

**AR401611 BITUMINOUS SURFACE COURSE – METHOD 1**

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 "BITUMINOUS SURFACE COURSE-METHOD 1" AS STATED ON PAGE 185 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING SUCCESSIVE LIFTS OF BITUMINOUS LEVELING COURSE (2-1/2 INCH MAXIMUM LIFTS) ON TOP OF THE PROPOSED POROUS FRICTION COURSE. A 1-1/2 INCH LIFT OF BITUMINOUS SURFACE COURSE WILL BE PLACED ON TOP OF THE FINAL LIFT OF BITUMINOUS LEVELING COURSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE.

PRIOR TO STARTING THE AGGREGATE BITUMINOUS SURFACE COURSE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

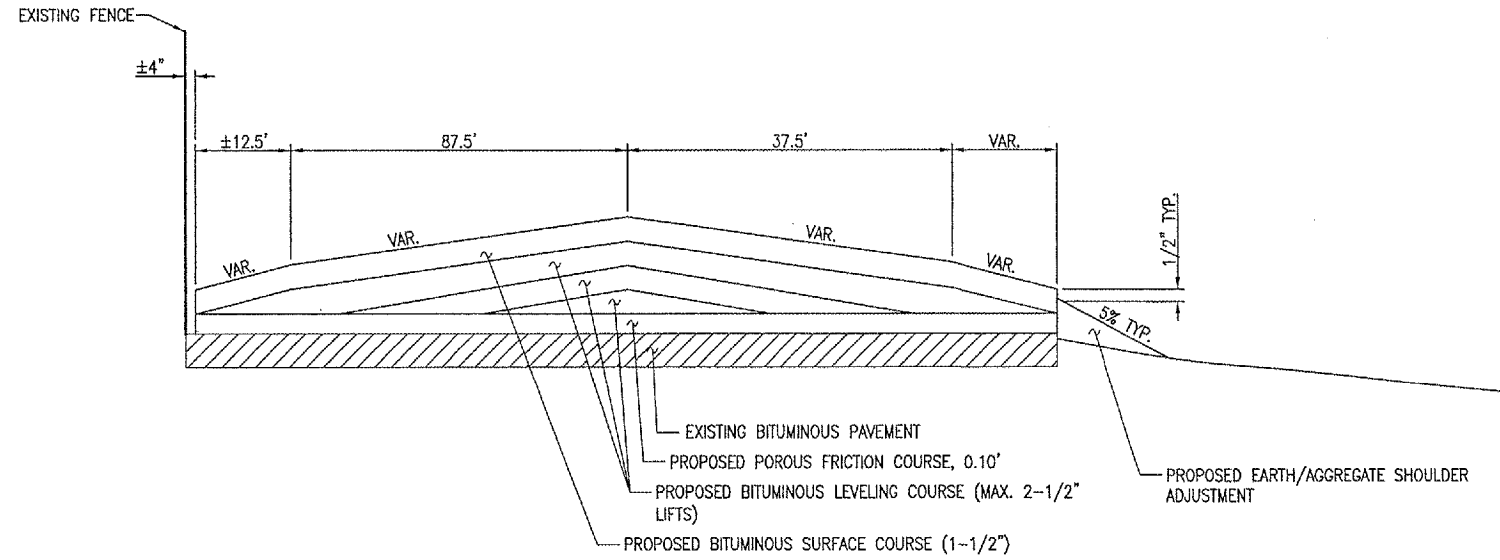
THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN OF LESS THAN 60,000 POUNDS.

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

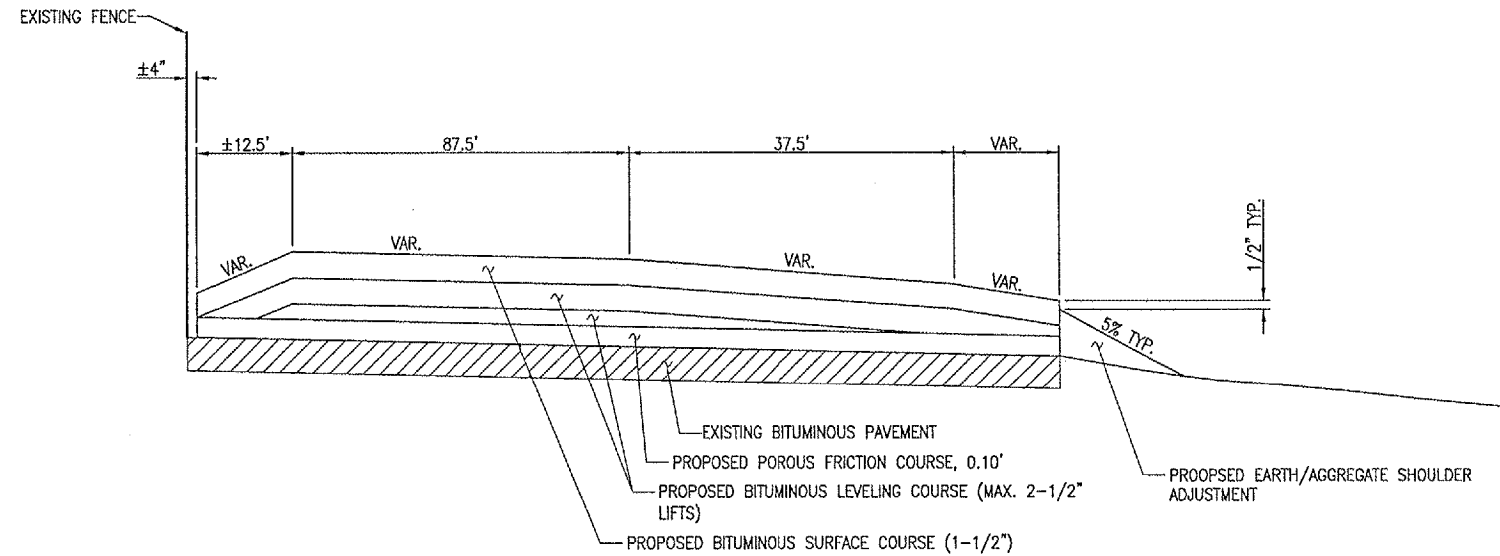
WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 ADD THE FOLLOWING TO THIS PARAGRAPH:

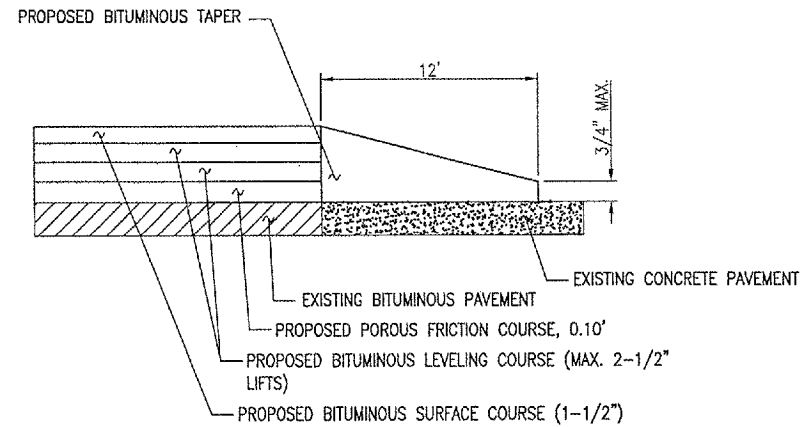
ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.



**PROPOSED TYPICAL SECTION A-A**  
NOT TO SCALE



**PROPOSED TYPICAL SECTION B-B**  
NOT TO SCALE



**PROPOSED TYPICAL SECTION C-C**  
NOT TO SCALE

BITUMINOUS TACK COAT SHALL BE APPLIED BETWEEN LIFTS OF PROPOSED PAVEMENT AT THE FOLLOWING RATES:

- ON EXISTING PRIOR TO PFC – 0.25 GAL/SY
  - 80% EMULSION/20% WATER
  - MINIMUM CURE TIME OF 48 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

- ON PFC PRIOR TO FIRST LIFT 401 – 0.25 GAL/SY
  - 80% EMULSION/20% WATER
  - MINIMUM CURE TIME OF 24 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

- ON 401 PRIOR TO EACH ADDITIONAL LIFT 401 – 0.15 GAL/SY
  - 50% EMULSION/50% WATER
  - MINIMUM CURE TIME OF 24 HOURS
  - TACK MUST BE DRY TO THE SATISFACTION OF THE RESIDENT ENGINEER

WATER SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION ALLOWED.

04-28-2005, 11:31 AM DPE

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BY	
REVISION	
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**DTMA**  
DEALB TAYLOR MUNICIPAL AIRPORT  
I.L. PROJ.: DKB-3467

HEL Project No.	803-050PND
Filename	R-521CON
Scale	NOT TO SCALE
Date	02/14/05
LAYOUT	JMB 02/14/05
DRAWN	JMB 02/14/05
REVIEWED	CAH ?

**HANSON**  
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RECONSTRUCT WEST APRON  
PROPOSED TYPICAL SECTIONS