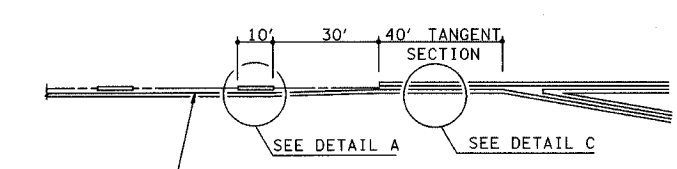


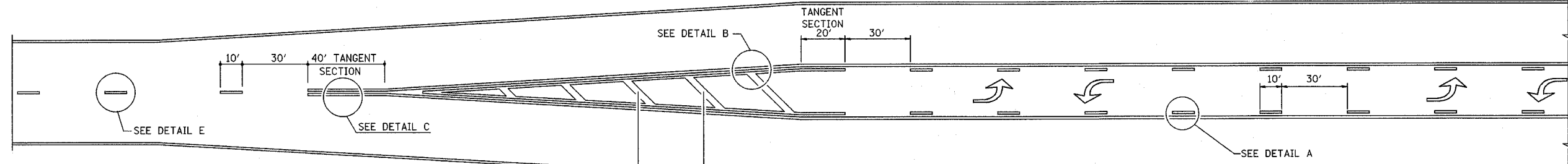
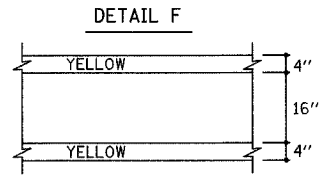
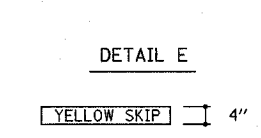
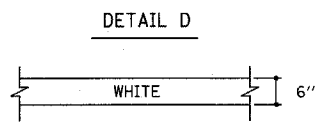
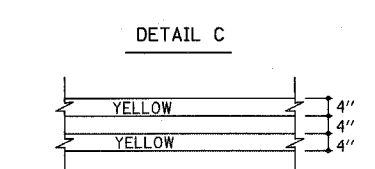
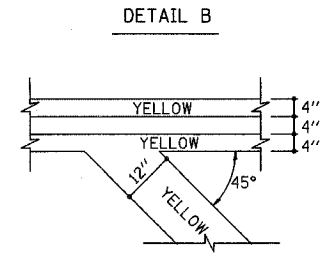
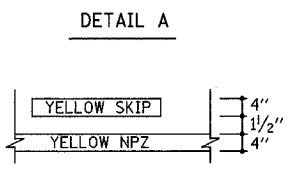
1 2 3 4 5 6 7 8 9 \$DATE\$
 10 11 12 13 14 15 16 17 18
 19 20 21 22 23 24 25 26 27
 28 29 30 31 32 33 34 35 36
 37 38 39 40 41 42 43 44 45
 46 47 48 49 50 51 52 53 54
 55 56 57 58 59 60 61 62 63

DCN-SPEC
 DATE



LENGTH OF NPZ
 SPEED LIMIT 35 TO 45 MPH = 350'
 SPEED LIMIT > 45 MPH = 500'

RURAL SECTION (NO CURB)
 TRANSITION APPROACH



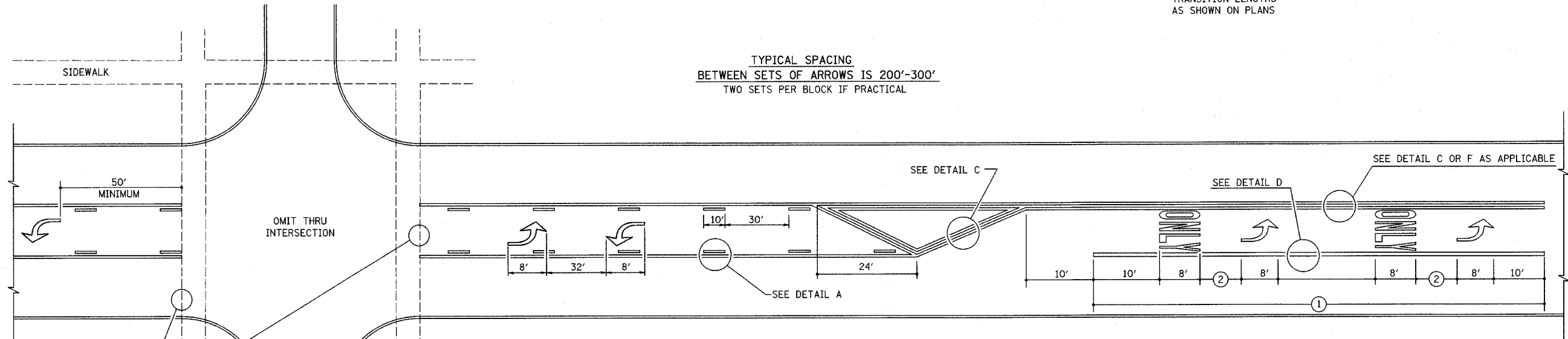
URBAN SECTION (CURBED)
 TRANSITION APPROACH

MEASURED PARALLEL TO
 CENTERLINE OF PAVEMENT

* SPACING OF DIAGONALS
 SPEED LIMIT < 30 MPH = 15'
 SPEED LIMIT 30 TO 45 MPH = 20'
 SPEED LIMIT > 45 MPH = 30'
 NO DIAGONALS SHORTER THAN 24"

LANE WIDTHS AND
 TRANSITION LENGTHS
 AS SHOWN ON PLANS

TYPICAL SPACING
 BETWEEN SETS OF ARROWS IS 200'-300'
 TWO SETS PER BLOCK IF PRACTICAL



SIDEWALK LINE
 EXISTING OR POTENTIAL

OMIT THRU
 INTERSECTION

TYPICAL BI-DIRECTIONAL TURN LANE MARKINGS
 (SHOWING TRANSITION TO EXCLUSIVE LEFT TURN LANE)

①	②	SETS OF SYMBOLS
< 100'	24'	1
100'-120'	16'	2
121'-180'	24'	
> 180'	24'	3

REVISIONS	
NAME	DATE
MMO	12-99

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL APPLICATION
 OF PAVEMENT
 MARKINGS - BI-DIRECTIONAL
 TURN LANE
 SCALE: VERT.
 HORIZ.
 DATE
 DRAWN BY DIST 7
 CHECKED BY BWC