6-17-05 Letting, Item 185

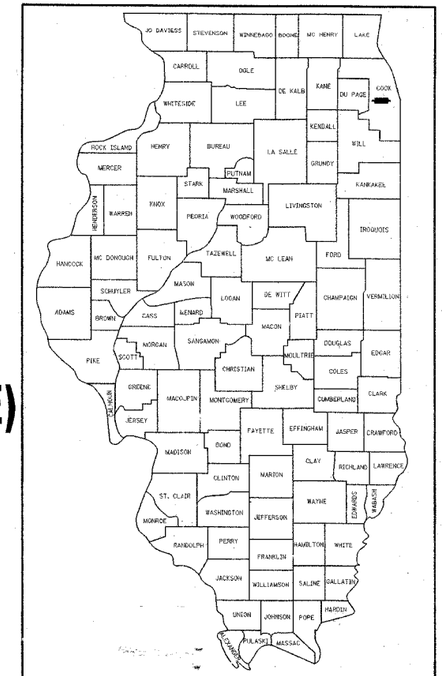
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	1
FED. ROAD DIST. NO. _	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

VILLAGE OF RIVERSIDE
F.A.U. ROUTE 1015 (ADDISON ROAD)
LONGCOMMON ROAD TO F.A.P. 348 (IL 43 / HARLEM AVENUE)
SECTION 04-00070-00-RS
ROADWAY RESURFACING
COOK COUNTY
PROJECT: M-8003(436)



LOCATION OF SECTION INDICATED THUS: -■-

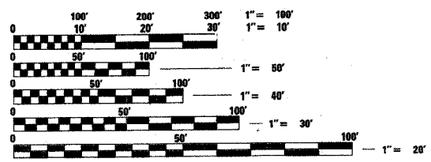
INDEX OF SHEETS

NO.	TITLE
1	TITLE SHEET
2	SUMMARY OF QUANTITIES, IDOT STANDARDS, GENERAL NOTES AND BENCHMARKS
3	TYPICAL SECTIONS
4-8	PLAN SHEETS
9	CONSTRUCTION DETAILS
10	DETECTOR LOOP DETAILS AND CONSTRUCTION SCHEDULES
11	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS
13	BUTT JOINT AND BITUMINOUS TAPER DETAILS
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
15	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

PROJECT LOCATED IN VILLAGE OF RIVERSIDE

DESIGN SPEED 30 mph
SPEED LIMIT = 25 mph
ADT = 2,000 (2030)

DESIGN DESIGNATION: COLLEGTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

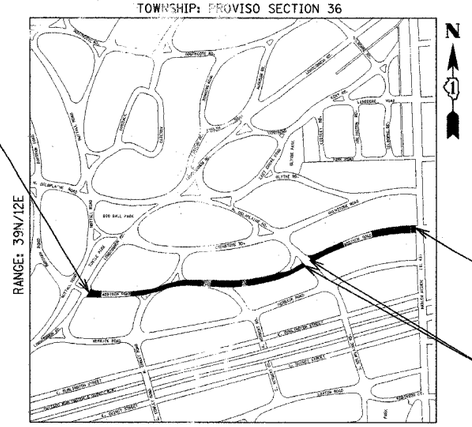
CONTRACT NO. 83791

COUNTY: COOK SECTION: F.A.U. ROUTE: 1015

IDOT FEDERAL AID PROGRAM ENGINEER: CHAD RIDDLE (847) 705-4406

C-91-321-04

TOWNSHIP: PROVISO SECTION 36



PROJECT BEGINS - ADDISON ROAD
AT LONGCOMMON ROAD
STA. 100+26.52

PROJECT ENDS - ADDISON ROAD
AT HARLEM AVENUE (IL 43)
STA. 138+61

PROJECT OMISSION - ADDISON ROAD
AT DELAPLAINE ROAD
STA. 125+68.75 TO STA. 126+31.44

LOCATION MAP
GROSS LENGTH OF PROJECT: 3835 FT. (0.726 MI.)
NET LENGTH OF PROJECT: 3,772 FT. (0.714 MI.)
39N/12E PROVISO TOWNSHIP SECTION 36
SCALE: NTS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED January 12, 20 05
Kathleen F. Rush
VILLAGE OF RIVERSIDE
VILLAGE MANAGER

APPROVED February 24, 20 05
ENGINEER OF LOCAL ROADS AND STREETS

APPROVED February 24, 20 05
Dave O'Neal, AP
DISTRICT ENGINEER

PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS

JAMES D. MIEDEMA
62994
REGISTERED PROFESSIONAL ENGINEER OF
ILLINOIS

James D Miedema, P.E.
EXPIRES 11-30-05

JAMES J. BENES & ASSOCIATES
CONSULTING ENGINEERS
950 WARRENVILLE ROAD, SUITE 101
LISLE, IL 60532

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	2
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791

GENERAL NOTES

1. ACCESS TO LOCAL RESIDENCES AND BUSINESSES SHALL BE MAINTAINED DURING CONSTRUCTION.
2. THE CONTRACTOR SHALL GIVE THE MUNICIPALITY AND JAMES J. BENES AND ASSOCIATES, INC. THREE (3) WORKING DAYS NOTICE PRIOR TO THE COMMENCEMENT OF WORK. (JAMES J. BENES AND ASSOCIATES, INC.: (630) 719-7570)
3. ALL ELEVATIONS ARE ON U.S.C.S. DATUM.
4. NEITHER THE ENGINEER, NOR THE OWNER, SHALL ASSUME ANY OF THE RESPONSIBILITIES OF THE CONTRACTOR'S SUPERINTENDENT OR OF SUBCONTRACTORS. ADDITIONALLY, NEITHER THE ENGINEER, NOR THE OWNER, SHALL ADVISE ON, OR ISSUE DIRECTIONS CONCERNING, ASPECTS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, OR SAFETY PRECAUTIONS AND/OR PROGRAMS IN CONNECTION WITH THE WORK.
5. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THEIR ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES. THE CONTRACTOR SHALL REPORT ANY ENCOUNTERED DISCREPANCIES TO THE ENGINEER AT ONCE. THE CONTRACTOR SHALL TAKE DUE CARE IN ALL PHASES OF CONSTRUCTION TO PROTECT ANY UTILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
6. THE CONTRACTOR SHALL KEEP THE CONSTRUCTION AREA FREE OF DEBRIS AND/OR OBJECTIONABLE MATERIALS DURING CONSTRUCTION.
7. THE CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE FACILITIES DURING CONSTRUCTION AND SHALL REPAIR ANY DRAINAGE FACILITIES DAMAGED DURING CONSTRUCTION. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND WILL NOT BE PAID FOR SEPARATELY.
8. THE CONTRACTOR SHALL VERIFY THE ELEVATIONS OF EXISTING STORM SEWERS PRIOR TO THE CONSTRUCTION OF PROPOSED STORM SEWER.
9. BEFORE STARTING AND EXCAVTION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
10. UNLESS OTHERWISE SHOWN, TRANSITIONS OF 10' SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF RIVERSIDE.
12. BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I AND TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
13. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
14. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINTS AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

	ITEM CODE NUMBER	ITEM NO.	ITEM DESCRIPTION	UNIT	QTY	CONSTRUCTION CODE NUMBER
	X4021000	1	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	30	I-000
	X4066414	2	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIXTURE C, N50	TON	832	I-000
	X4067100	3	POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50	TON	725	I-000
	XX004005	4	PORTLAND CEMENT CONCRETE SIDEWALK, 5", SPECIAL (PEA GRAVEL MIX)	SQ FT	1,500	I-000
	XX004740	5	PORTLAND CEMENT CONCRETE SIDEWALK, 7", SPECIAL (PEA GRAVEL MIX)	SQ FT	1,050	I-000
	XX004833	6	STABILIZED DRIVEWAYS SUPERPAVE, 7"	SQ YD	125	I-000
	20201200	7	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	314	I-000
	20700420	8	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	250	I-000
	20800150	9	TRENCH BACKFILL	CU YD	24.2	I-000
	21001000	10	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	750	I-000
	21101615	11	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2,000	I-000
	25000400	12	NITROGEN FERTILIZER NUTRIENTS	POUND	24.8	I-000
	25000500	13	PHOSPORUS FERTILIZER NUTRIENTS	POUND	24.8	I-000
	25000600	14	POTASSIUM FERTILIZER NUTRIENTS	POUND	24.8	I-000
	25200100	15	SODDING	SQ YD	2,000	I-000
	25200200	16	SUPPLEMENTAL WATERING	UNIT	5	I-000
	40600200	17	BITUMINOUS MATERIALS (PRIME COAT)	TON	8	I-000
	40600300	18	AGGREGATE (PRIME COAT)	TON	20	I-000
	40600400	19	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	10	I-000
	40600895	20	CONSTRUCTING TEST STRIP	EACH	1	I-000
	40600980	21	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	105.0	I-000
	40600990	22	TEMPORARY RAMP	SQ YD	50	I-000
	42001300	23	PROTECTIVE COAT	SQ YD	1,100	I-000
	42300300	24	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7"	SQ YD	750	I-000
	42400200	25	PORTLAND CEMENT CONCRETE SIDEWALK, 5"	SQ FT	200	I-000
	44000007	26	BITUMINOUS SURFACE REMOVAL, 2"	SQ YD	3,040	I-000
	44000009	27	BITUMINOUS SURFACE REMOVAL, 3"	SQ YD	6,861	I-000
	44000200	28	DRIVEWAY PAVEMENT REMOVAL	SQ YD	850	I-000
	44000600	29	SIDEWALK REMOVAL	SQ FT	2,750	I-000
	44001700	30	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,400	I-000
	56500600	31	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	15	I-000
	60218300	32	MANHOLES, TYPE A, 4' DIAM., TYPE 1 FRAME, OPEN LID	EACH	1	I-000
	60252800	33	CATCH BASINS TO BE RECONSTRUCTED	EACH	4	I-000
	60257900	34	MANHOLES TO BE RECONSTRUCTED	EACH	8	I-000
	60262700	35	INLETS TO BE RECONSTRUCTED	EACH	1	I-000
	60266100	36	VALVE VAULTS TO BE RECONSTRUCTED	EACH	1	I-000
	60266600	37	VALVE BOXES TO BE ADJUSTED	EACH	7	I-000
	60300105	38	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	I-000
	60300305	39	FRAMES AND LIDS TO BE ADJUSTED	EACH	9	I-000
	60300310	40	FRAMES AND LIDS TO BE ADJUSTED, SPECIAL	EACH	19	I-000
	60406000	41	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	8	I-000
	60406100	42	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	16	I-000
	60500040	43	REMOVING MANHOLES	EACH	1	I-000
	70102620	44	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	I-000
	70300100	45	SHORT TERM PAVEMENT MARKINGS	FOOT	225	I-000
	70301000	46	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	75	I-000
*	78000200	47	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,000	I-000
*	78000400	48	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	275	I-000
*	78000650	49	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	60	I-000
*	88600600	50	DETECTOR LOOP REPLACEMENT	FOOT	150	I-000
	XX006215	51	BRICK PAVERS REMOVAL AND REPLACEMENT	SQ YD	6	I-000
	XX006203	52	STORM SEWERS, PVC SDR 26, 8"	FOOT	8	I-000
	XX006209	53	STORM SEWERS, PVC SDR 26, 24"	FOOT	16	I-000
	XX006205	54	CLASS D PATCHES, SUPERPAVE, TYPE 1, 4 INCHES	SQ YD	75	I-000
	XX006206	55	CLASS D PATCHES, SUPERPAVE, TYPE 2, 4 INCHES	SQ YD	150	I-000
	XX006207	56	CLASS D PATCHES, SUPERPAVE, TYPE 3, 4 INCHES	SQ YD	225	I-000
	XX006208	57	CLASS D PATCHES, SUPERPAVE, TYPE 4, 4 INCHES	SQ YD	300	I-000
	XX004899	58	CLASS D PATCHES, SUPERPAVE, TYPE 1, 8 INCHES	SQ YD	50	I-000
	XX004850	59	CLASS D PATCHES, SUPERPAVE, TYPE 2, 8 INCHES	SQ YD	100	I-000
	XX004851	60	CLASS D PATCHES, SUPERPAVE, TYPE 3, 8 INCHES	SQ YD	150	I-000
	XX004838	61	CLASS D PATCHES, SUPERPAVE, TYPE 4, 8 INCHES	SQ YD	200	I-000

* DENOTES AS SPECIALTY ITEMS

IDOT HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-03	SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED
442201-01	CLASS C AND D PATCHES
602001	CATCH BASIN, TYPE A
602601	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-02	FRAMES AND LIDS, TYPE 1
606001-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
702001-05	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS
886001	DETECTOR LOOP INSTALLATIONS

BENCH MARKS

1) SE FLANGE BOLT ON FIRE HYDRANT AT THE SOUTHERNLY INTERSECTION OF LONGCOMMON ROAD AND AKENSIDE ROAD
ELEVATION = 618.24

2) NW FLANGE BOLT ON FIRE HYDRANT AT NORTHEAST CORNER OF LONGCOMMON ROAD AND ADDISON ROAD.
ELEVATION = 616.76

3) NW FLANGE BOLT ON FIRE HYDRANT AT #200 ADDISON ROAD.
ELEVATION = 614.31

4) NW FLANGE BOLT ON FIRE HYDRANT AT #260 ADDISON ROAD.
ELEVATION = 613.85

5) NW FLANGE BOLT ON FIRE HYDRANT AT NORTHEAST CORNER OF ADDISON ROAD AND DELAPLAINE ROAD.
ELEVATION = 613.33

6) NW FLANGE BOLT ON FIRE HYDRANT AT NORTHWEST CORNER OF ADDISON ROAD AND HARLEM AVENUE.
ELEVATION = 612.66

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

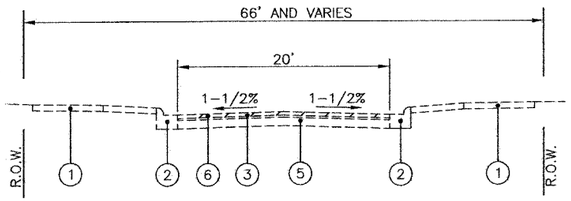
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

SUMMARY OF QUANTITIES, IDOT STANDARDS, GENERAL NOTES AND BENCH MARKS

SCALE: NTS
DATE: 10/15/04
DRAWN BY: BCD
CHECKED BY: JDM

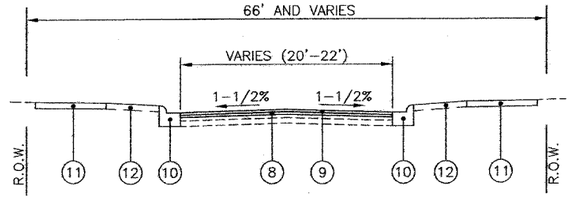
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	3
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791



EXISTING TYPICAL SECTION
ADDISON ROAD

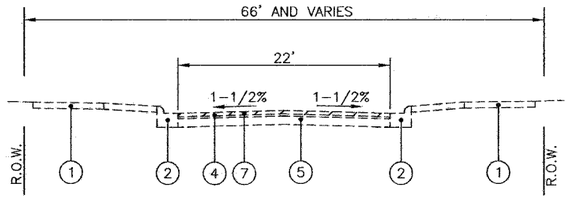
LONGCOMMON ROAD TO DELAPLAINE ROAD
STA. 100+26.52 TO STA 125+68.75



PROPOSED TYPICAL SECTION
ADDISON ROAD

LONGCOMMON ROAD TO HARLEM AVENUE (IL. 43)
STA. 100+26.52 TO STA 138+61.00

STATION	OFFSET		PROPOSED PATCH LENGTH (FOOT)	PROPOSED PATCH WIDTH (FOOT)	PATCH AREA (SQ YD)	PATCH TYPE	PATCH THICKNESS (IN)
102+05	5'	RT.	30	15	50.0	2	4
103+04	6'	LT.	20	10	22.2	1	4
105+58	4'	RT.	35	25	97.2	4	4
106+98	6'	LT.	30	20	66.7	3	4
108+86	4'	LT.	30	15	50.0	3	8
110+47	3'	LT.	12	10	13.3	1	8
110+96	6'	RT.	30	20	66.7	4	8
112+34	5'	RT.	20	15	33.3	2	8
112+85	3'	RT.	20	15	33.3	2	8
115+48	5'	RT.	30	20	66.7	4	8
117+54	5'	LT.	12	10	13.3	1	8
118+74	4'	LT.	30	15	50.0	3	8
120+94	6'	LT.	30	20	66.7	3	4
121+47	5'	RT.	35	25	97.2	4	4
122+87	4'	LT.	20	10	22.2	1	4
124+32	3'	RT.	30	15	50.0	2	4
127+33	6'	RT.	30	20	66.7	4	8
127+45	7'	RT.	20	15	33.3	2	8
129+86	6'	LT.	12	10	13.3	1	8
131+32	5'	LT.	30	15	50.0	3	8
133+46	6'	RT.	35	25	97.2	4	4
134+57	7'	LT.	30	20	66.7	3	4
135+84	5'	LT.	20	10	22.2	1	4
136+59	6'	RT.	30	15	50.0	2	4



EXISTING TYPICAL SECTION
ADDISON ROAD

DELAPLAINE ROAD TO HARLEM AVENUE (IL. 43)
STA. 126+31.44 TO STA 138+61.00

PAVEMENT DESIGN CALCULATIONS

DATE: January 30, 2005
IMPROVEMENT TYPE: FLEXIBLE PAVEMENT CROSS SECTION (RESURFACING)
LOCATION: Addison Road - Village of Riverside FAU 1015
Longcommon Road to Delaplaine Road

CLASSIFICATION OF ROADWAY:	=	CLASS II ROADWAY (ADT > 2000)	
TRAFFIC FACTOR = DP(((0.073*PV)+(44.530*SU)+(156.403*MU))/1000000)			
DESIGN LANE VOLUME % OF ADT	=	50 % TRUCKS	50 % PASS. VEHICLES
DESIGN PERIOD,YEARS (DP)	=	20 YEARS	
% OF PASSENGER VEHICLES (PV)	=	98.50 %	
% OF SINGLE UNIT TRUCKS (SU)	=	1.40 %	
% OF MULTI UNIT TRUCKS (MU)	=	0.10 %	

AVERAGE DAILY TRAFFIC	=	2000	TRAFFIC FACTOR.....	=	0.017035
DESIGN LANE VOLUME	=	1000			
NO. OF PASSENGER VEHICLES	=	985	I.B.R.....	=	2.5
NO. OF SINGLE UNIT TRUCKS	=	14			
NO. OF MULTI UNIT TRUCKS	=	1	STRUCTURAL NUMBER (Dt)	=	2.30

PROPOSED PAVEMENT CROSS SECTION

MATERIAL THICKNESS	STRUCTURAL MATERIAL	COEFFICIENT	Dt
1.50	BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX C, N50	0.40	0.60
1.50	POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50	0.40	0.60
1.00	EXISTING BITUMINOUS SURFACE COURSE	0.23	0.23
9.50	EXISTING AGGREGATE BASE COURSE	0.11	1.05
	TOTAL Dt PROVIDED =		2.48

PAVEMENT DESIGN CALCULATIONS

DATE: January 30, 2005
IMPROVEMENT TYPE: FLEXIBLE PAVEMENT CROSS SECTION (RESURFACING)
LOCATION: Addison Road - Village of Riverside FAU 1015
Delaplaine Road to Harlem Avenue (IL RT 43)

CLASSIFICATION OF ROADWAY	=	CLASS II ROADWAY (ADT > 2000)	
TRAFFIC FACTOR = DP(((0.073*PV)+(44.530*SU)+(156.403*MU))/1000000)			
DESIGN LANE VOLUME % OF ADT	=	50 % TRUCKS	50 % PASS. VEHICLES
DESIGN PERIOD,YEARS (DP)	=	20 YEARS	
% OF PASSENGER VEHICLES (PV)	=	98.50 %	
% OF SINGLE UNIT TRUCKS (SU)	=	1.40 %	
% OF MULTI UNIT TRUCKS (MU)	=	0.10 %	

AVERAGE DAILY TRAFFIC	=	2000	TRAFFIC FACTOR.....	=	0.017035
DESIGN LANE VOLUME	=	1000			
NO. OF PASSENGER VEHICLES	=	985	I.B.R.....	=	2.5
NO. OF SINGLE UNIT TRUCKS	=	14			
NO. OF MULTI UNIT TRUCKS	=	1	STRUCTURAL NUMBER (Dt)	=	2.30

PROPOSED PAVEMENT CROSS SECTION

MATERIAL THICKNESS	STRUCTURAL MATERIAL	COEFFICIENT	Dt
1.50	BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX C, N50	0.40	0.60
0.50	POLYMERIZED LEVELING BINDER (MM), SUPERPAVE, IL-4.75, N50	0.40	0.20
3.30	EXISTING BITUMINOUS SURFACE COURSE	0.23	0.76
9.00	EXISTING AGGREGATE BASE COURSE	0.11	0.99
	TOTAL Dt PROVIDED =		2.55

LEGEND

1. EXISTING SIDEWALK
2. EXISTING CURB AND GUTTER, TYPE M-6.12
3. EXISTING BITUMINOUS CONCRETE SURFACE COURSE, (VARIES 4" TO 7")
4. EXISTING BITUMINOUS CONCRETE SURFACE COURSE, (VARIES 4-1/2" TO 5-1/2")
5. EXISTING BASE COURSE,* (VARIES 3-1/2" TO 5-1/2")
6. PROPOSED BITUMINOUS SURFACE REMOVAL, 3"
7. PROPOSED BITUMINOUS SURFACE REMOVAL, 2"
8. PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE, IL-4.75, N50, VARIES (1/2" TO 1-1/2")**
9. PROPOSED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50, 1-1/2"
10. PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
11. PROPOSED PCC SIDEWALK, 5 INCH, PROPOSED PCC SIDEWALK, 5 INCH (PEA GRAVEL MIX), OR PROPOSED PCC SIDEWALK, 7 INCH (PEA GRAVEL MIX). NOTE: MINIMUM 2 INCH CA-6 SUBBASE REQUIRED (COST INCLUDED WITH SIDEWALK)
12. TOPSOIL FURNISH AND PLACE 4" AND SODDING (AS DIRECTED BY THE ENGINEER)

*APPROXIMATE LIMITS OF BASE COURSE MATERIAL

STA. 100+26 TO STA. 113+00 BITUMINOUS BASE
STA. 113+00 TO STA. 120+00 PCC BASE
STA. 120+00 TO STA. 125+69 BITUMINOUS BASE
STA. 126+31 TO STA. 132+00 PCC BASE
STA. 132+00 TO STA. 138+61 BITUMINOUS BASE

** APPROX. 1 1/2" THICK (STA. 100+26.52 TO STA. 125+68.75)
APPROX. 1/2" THICK (STA. 126+31.44 TO STA. 138+61.00)

BITUMINOUS MIXTURE REQUIREMENT

ITEM	AC TYPE	VOIDS	MAX RAP %
POLYMERIZED LEVELING BINDER (MACHINE METHOD), SUPERPAVE IL-4.75 N50	SBS/SBR PG 76-28	2.5%@50 GYR	0
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX C, N50	PG 64-22	4%@50 GYR	15
BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE IL 19.0, N70 (CLASS D PATCHES, 4" & 8")	PG 64-22	4%@70 GYR	15
BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE IL 19.0, N50 (DRIVEWAY BASE)	PG 58-22	4%@50 GYR	25
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE MIX C, N50 (DRIVEWAYS)	PG 64-22	4%@50 GYR	15

UNIT WEIGHT FOR ALL BITUMINOUS SURFACE MIX IS 112 LBS/SY/IN

REVISIONS NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

TYPICAL SECTIONS

SCALE: NTS
DATE: 10/15/04
DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	4
STA. 100+00	TO STA. 110+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791

LONGCOMMON ROAD

BEGIN IMPROVEMENTS STA. 100+26.52

STOP BAR, 12 FT.–24" LINE STA. 100+39

CENTER LINE SKIP DASH 250 FT. – 4" (YELLOW)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP. M–6.12) (AS DIRECTED BY THE ENGINEER)

EXISTING R.O.W.

ADDISON ROAD

CROSSWALK LINES 42 FT.– 6" LINE

MATCH LINE STA. 105+00

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP. M–6.12) (AS DIRECTED BY THE ENGINEER)

LIMIT OF IMPROVEMENTS

MATCH LINE STA. 110+00

CROSSWALK LINES 43 FT.– 6" LINE

COWLEY ROAD

LIMIT OF IMPROVEMENTS

(S–1) STA. 106+86, 13' RT
MANHOLES, TYPE A, 4' DIAM.,
TYPE 1 FRAME, OPEN LID
RIM=613.22
INV=607.09(S), 606.60(E), 606.69(W)

STORM SEWERS

P–1	8 FT	PVC SDR 26	24"	TBF=8.7 CY
P–2	8 FT	PVC SDR 26	24"	TBF=8.7 CY
P–2	8 FT	PVC SDR 26	8"	TBF=6.8 CY

LEGEND

A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
ASP	STRUCTURE TO BE ADJUSTED, SPECIAL
(ASP)	STRUCTURE TO BE ADJUSTED, SPECIAL WITH NEW FRAME & LID
REM.	STRUCTURE TO BE REMOVED

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

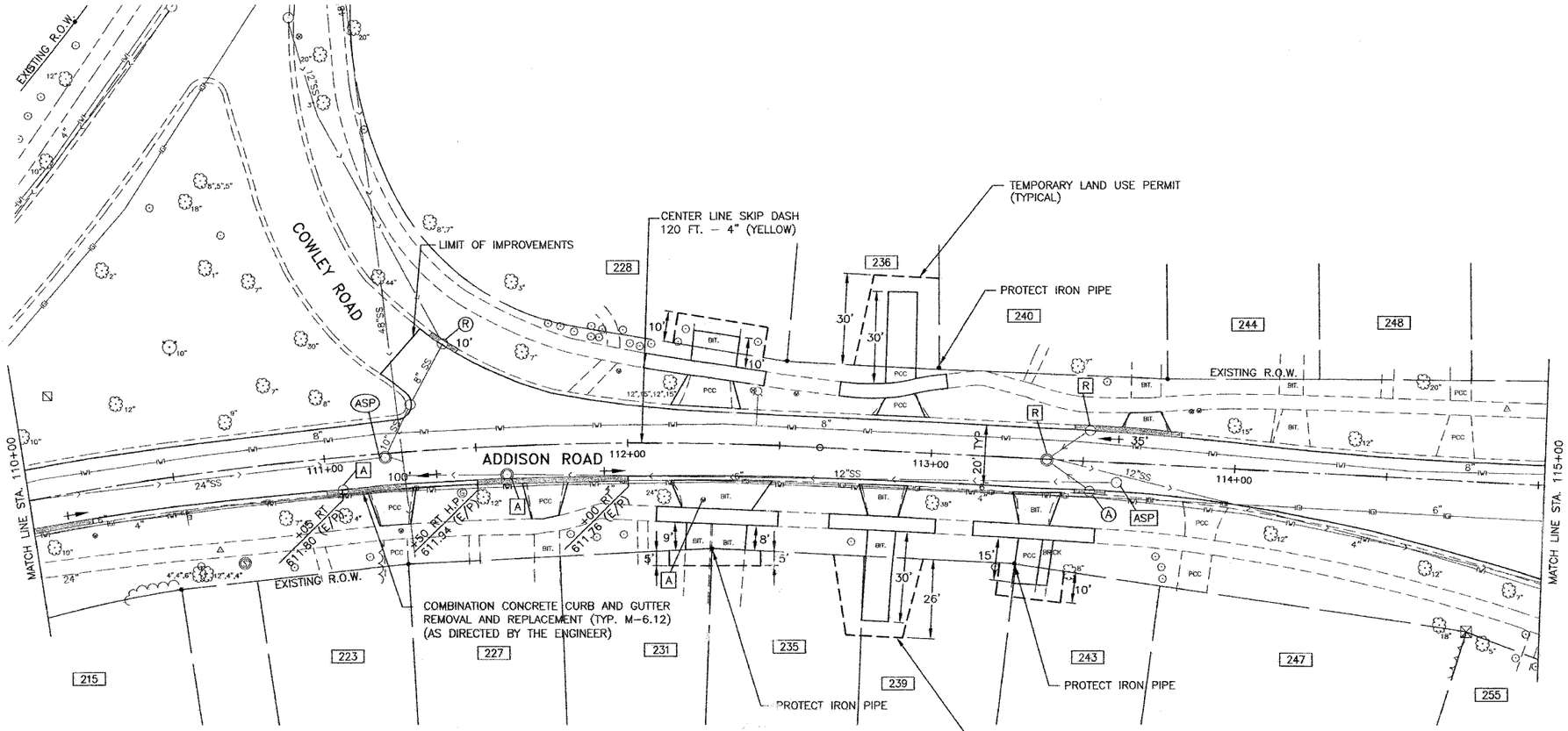
PLAN SHEET
STA. 100+26.52 TO STA. 110+00

SCALE: 1"=20'
DATE: 10/15/04

DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	5
STA. 110+00	TO STA. 115+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791



LEGEND

A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
ASP	STRUCTURE TO BE ADJUSTED, SPECIAL
(ASP)	STRUCTURE TO BE ADJUSTED, SPECIAL WITH NEW FRAME & LID
REM.	STRUCTURE TO BE REMOVED

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

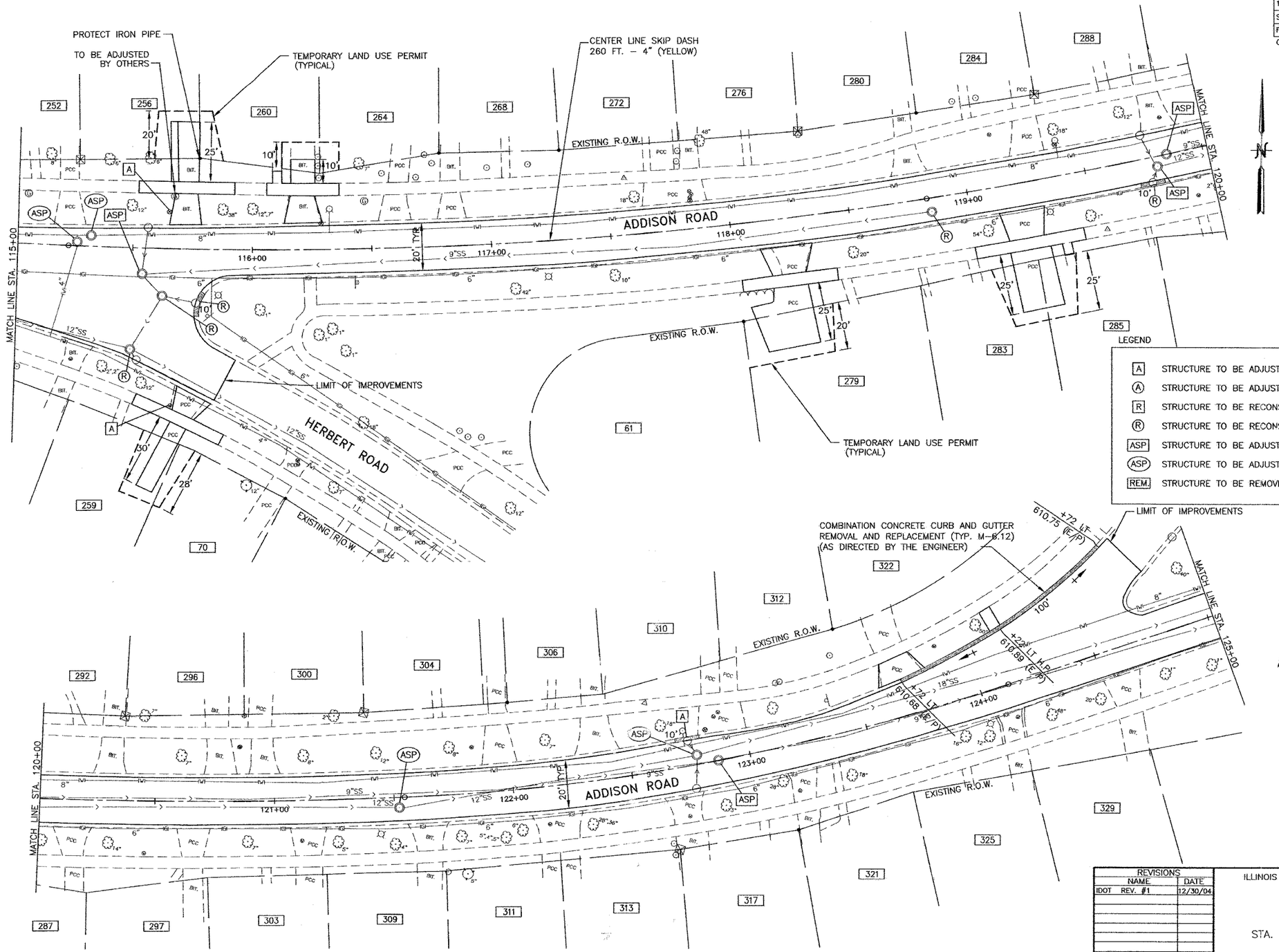
PLAN SHEET
STA. 110+00 TO STA. 115+00

SCALE: 1"=20'
DATE: 10/15/04

DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	6
STA. 115+00	TO STA. 125+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791



LEGEND

A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
ASP	STRUCTURE TO BE ADJUSTED, SPECIAL
(ASP)	STRUCTURE TO BE ADJUSTED, SPECIAL WITH NEW FRAME & LID
REM.	STRUCTURE TO BE REMOVED

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

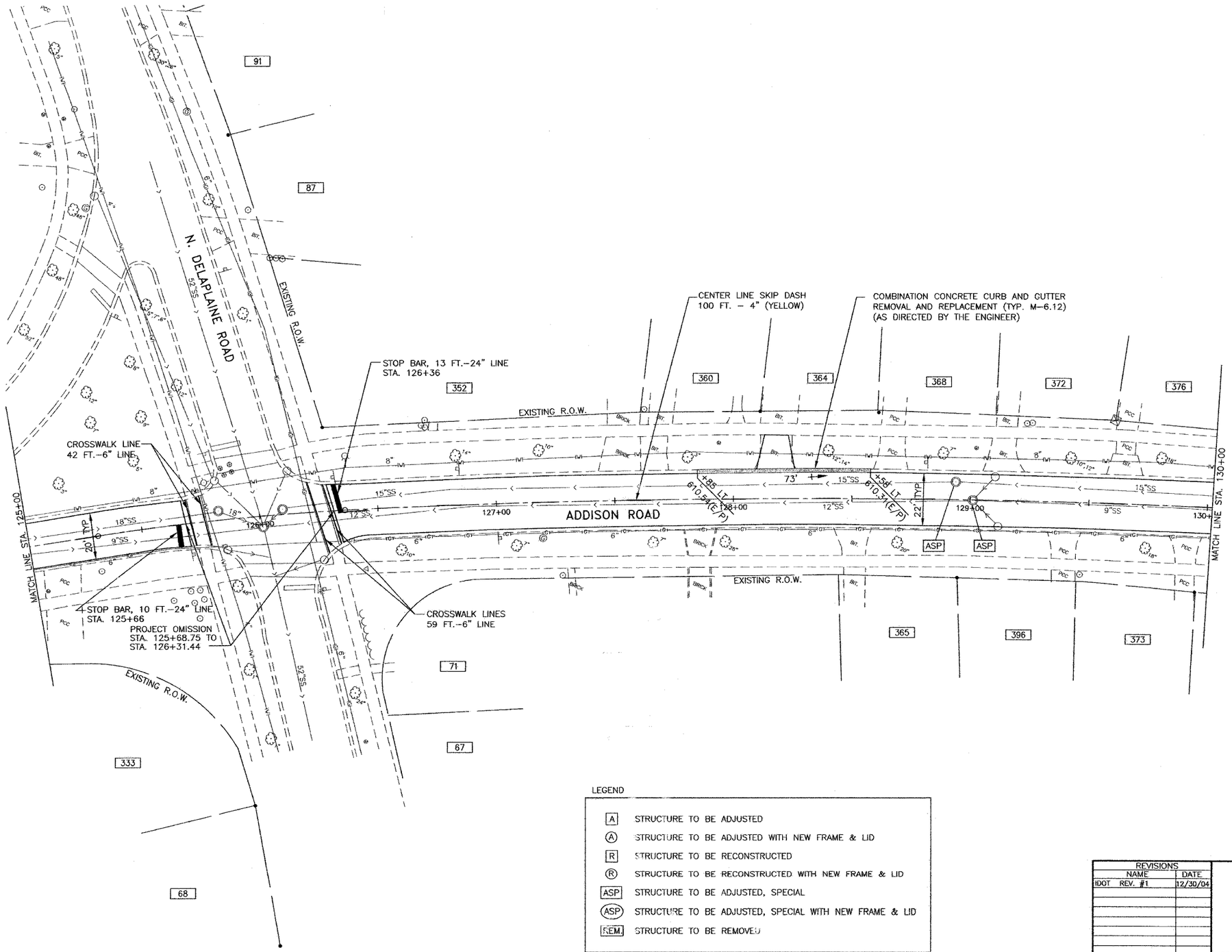
PLAN SHEET
STA. 115+00 TO STA. 125+00

SCALE: 1"=20'
DATE: 10/15/04

DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	7
STA. 125+00	TO STA. 130+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791



LEGEND

A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
ASP	STRUCTURE TO BE ADJUSTED, SPECIAL
(ASP)	STRUCTURE TO BE ADJUSTED, SPECIAL WITH NEW FRAME & LID
REM.	STRUCTURE TO BE REMOVED

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

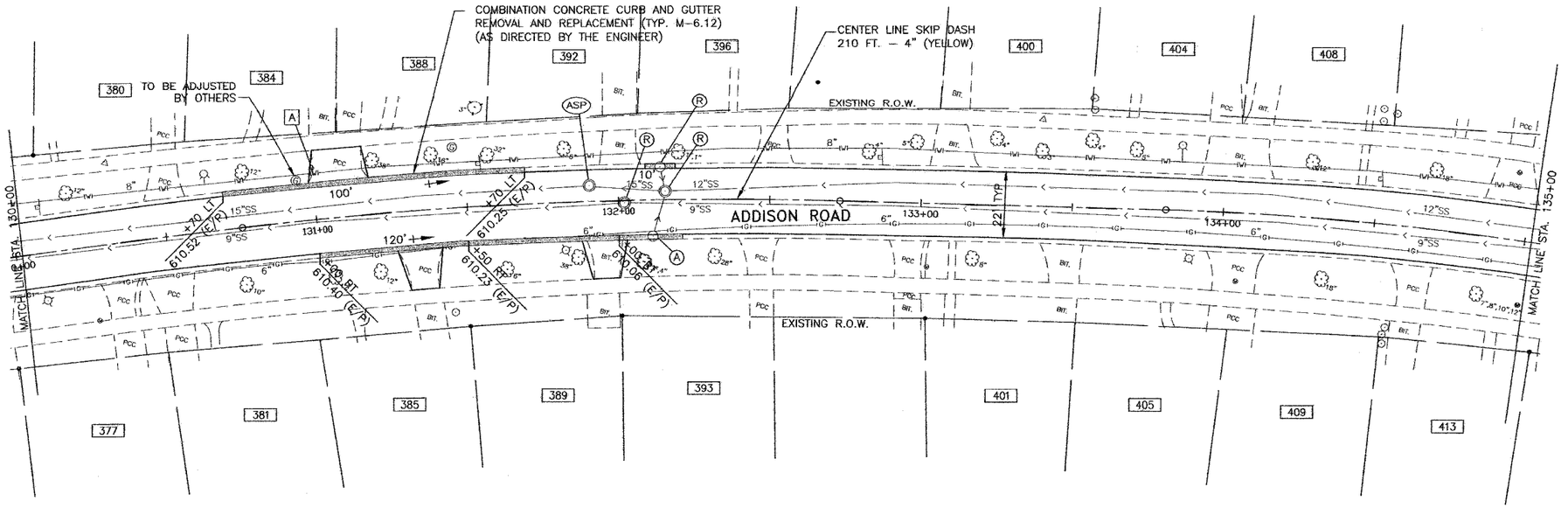
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

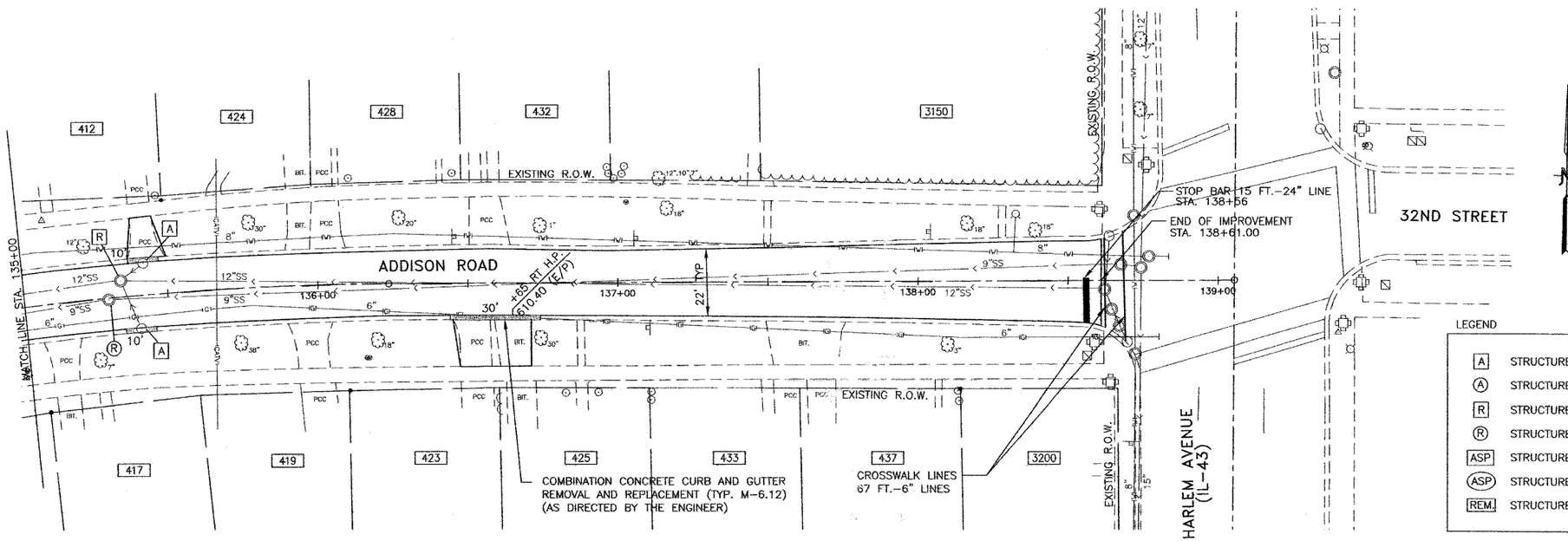
PLAN SHEET
STA. 125+00 TO STA. 130+00

SCALE: 1"=20'
DATE: 10/15/04
DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04--00070--00--RS	COOK	16	8
STA. 130+00	TO STA. 139+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791





LEGEND

A	STRUCTURE TO BE ADJUSTED
(A)	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
R	STRUCTURE TO BE RECONSTRUCTED
(R)	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
ASP	STRUCTURE TO BE ADJUSTED, SPECIAL
(ASP)	STRUCTURE TO BE ADJUSTED, SPECIAL WITH NEW FRAME & LID
REM.	STRUCTURE TO BE REMOVED

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

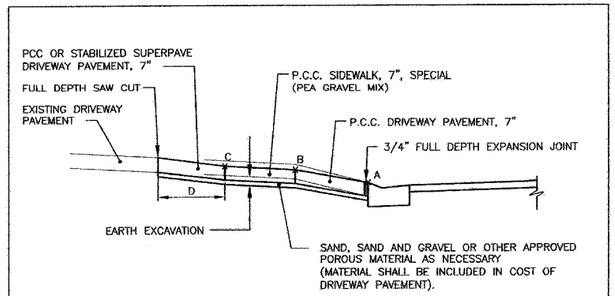
PLAN SHEET
STA. 130+00 TO STA. 138+61.00

SCALE: 1"=20'
DATE: 10/15/04

DRAWN BY: BCD
CHECKED BY: JDM

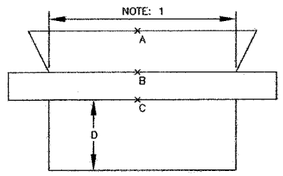
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	9
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO: 83791



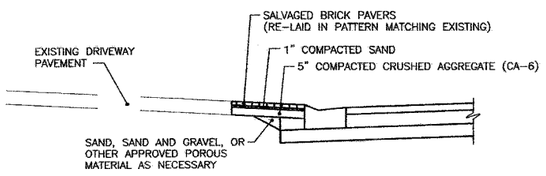
CROSS SECTION THROUGH DRIVEWAYS
RECONSTRUCTION REQUIRED TO DECREASE SLOPE

STATION	OFFSET	ADDRESS	A	B	C	D (ft)
108+75	27' RT.	# 211 W	612.55	613.29	613.39	15.0
109+76	10' RT.	# 215	612.23	612.99	613.09	10.0
112+26	10.5' LT.	# 228	611.85	612.64	612.74	10.0
112+31	10' RT.	# 231/235	611.75	612.50	612.60	8.0
112+86	10' RT.	# 236	611.61	612.13	612.23	30.0
112+89	10' LT.	# 239	611.60	612.48	612.58	30.0
113+36	10' RT.	# 243	611.40	612.23	612.33	15.0
115+70	8.5' LT.	# 256	610.73	611.76	611.86	25.0
115+77	59' RT.	# 259	610.53	611.68	611.78	30.0
116+21	10' LT.	# 260	611.03	611.95	612.05	10.0
118+24	10' RT.	# 279	611.29	612.20	612.30	25.0
119+22	10' RT.	# 283/285	610.91	611.86	611.96	25.0

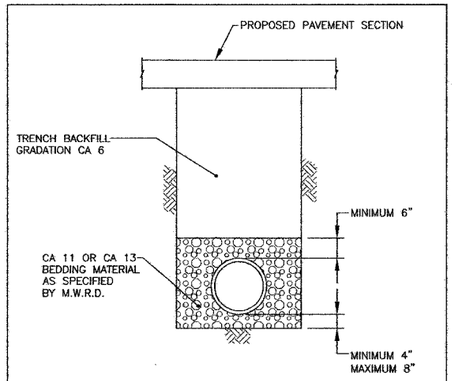


NOTE 1 : 10' UNLESS NOTED OTHERWISE ON THE PLANS (MAXIMUM WIDTHS ARE DETERMINED BY VILLAGE ORDINANCE)

DRIVEWAY GRADE MODIFICATIONS

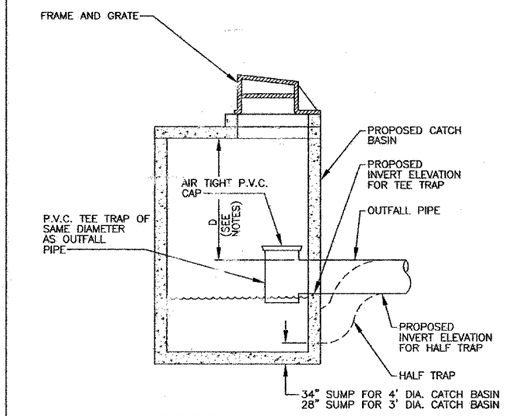


TYPICAL BRICK PAVER DETAIL



NOTE: THE BEDDING DETAIL SHOWN SHALL BE APPLICABLE TO ALL SEWERS CONSTRUCTED AS PART OF THIS PROJECT. CA 11 OR CA 13 BEDDING MATERIAL WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR TRENCH BACKFILL. NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR CONSTRUCTING SEWERS AS SHOWN IN THIS DETAIL.

STORM, SANITARY AND SERVICE SEWER BEDDING DETAIL



TRAP DETAIL

- TRAPS OR HALF TRAPS SHALL BE INSTALLED ON STORM SEWER PIPES EXITING CATCH BASINS.
- THE COST OF CONSTRUCTING TRAPS OR HALF TRAPS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CATCH BASINS.
- NO VARIATIONS FROM THIS DETAIL WILL BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE VILLAGE OF RIVERSIDE PUBLIC WORKS DEPARTMENT.
- TEE TRAP SHALL BE USED WHEN D ≥ 12" HALF TRAPS SHALL BE USED WHEN D < 12"

M.W.R.D. GENERAL NOTES

1. THE MWRD SEWER PERMIT SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) DAYS PRIOR TO THE COMMENCEMENT OF WORK (CALL (708) 588-4055)

2. ELEVATION DATUM IS U.S.G.S.

3. ALL CONCRETE, REINFORCED CONCRETE AND PVC SANITARY AND STORM SEWER IN COMBINED SEWER AREA, AND SEWER PIPE JOINTS SHALL CONFORM TO:

MATERIAL	JOINT SPEC	MATERIAL	JOINT SPEC
Concrete Pipe (C-14)	C-443	PVC Gravity Sewer Pipe 6" to 15" Diameter SDR 26 D-3034	
RCP (C-76)	C-443	18" to 27" Diameter F/dy=46 F-697	
		Joints - Gasket	D-3212
		- Solvent	D-2855

4. ALL SANITARY, AND STORM IN COMBINED SEWER AREAS, SEWER CONSTRUCTION REQUIRES STONE BEDDING ¼" TO 1" IN SIZE, WITH A MINIMUM THICKNESS EQUAL TO ¼ THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 2" ABOVE THE TOP OF THE PIPE WHEN USING PVC.

5. "BAND-SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPE OF DISSIMILAR MATERIALS.

6. WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:

   A. CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS ("SEWER-TAP" MACHINE OR SIMILAR) AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE.
   B. REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH WYE OR TEE BRANCH SECTION.
   C. WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING A "BAND-SEAL" OR SIMILAR COUPLINGS TO HOLD IT FIRMLY IN PLACE.

7. WHEREVER A SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY SEWERS AND WATERMAINS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTRUBED EARTH, KEEPING A MINIMUM 18 INCH VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATERMAIN, THE SEWER SHALL BE CONSTRUCTED TO WATERMAIN STANDARDS.

8. ALL MANHOLES SHALL BE PRECAST CONSTRUCTED AND FITTED WITH RUBBER GASKETED BOOT STYLE COUPLINGS.

REVISIONS	
NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

CONSTRUCTION DETAILS

SCALE: NTS
DATE: 10/15/04
DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791

STRUCTURE ADJUSTMENT SCHEDULE

STATION	O/S (FT)	LT / RT	DESCIPTION	TYPE 1 FRAME
100+35	5.5	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	
101+19	8	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	
102+49	10	LT	FRAME & GRATE TO BE ADJUSTED	
102+51	10	RT	FRAME & GRATE TO BE ADJUSTED	
105+52	10	LT	FRAME & LID TO BE ADJUSTED	
105+52	10	RT	FRAME & LID TO BE ADJUSTED	O/L
106+30	14	RT	FRAME & LID TO BE ADJUSTED	
106+42	7	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
107+72	4.5	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
107+77	7	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
108+42	13	RT	FRAME & GRATE TO BE ADJUSTED	
108+56	33	RT	FRAME & LID TO BE ADJUSTED	O/L
109+32	11	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	
109+49	11	LT	FRAME & GRATE TO BE ADJUSTED	
111+06	10	RT	FRAME & LID TO BE ADJUSTED	
111+20	1.5	RT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
111+60	9	RT	FRAME & LID TO BE ADJUSTED	
113+53	10	RT	FRAME & LID TO BE ADJUSTED	O/L
113+61	6.5	RT	FRAME & LID TO BE ADJUSTED, SPECIAL	
115+25	1.5	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
115+31	4	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
115+52	11	RT	FRAME & LID TO BE ADJUSTED, SPECIAL	
119+80	4	RT	FRAME & LID TO BE ADJUSTED, SPECIAL	
119+85	0	C/L	FRAME & LID TO BE ADJUSTED, SPECIAL	
121+52	4	RT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
122+78	4	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
122+82	0	C/L	FRAME & LID TO BE ADJUSTED, SPECIAL	
128+94	7.5	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	
129+01	0	C/L	FRAME & LID TO BE ADJUSTED, SPECIAL	
131+90	5.5	LT	FRAME & LID TO BE ADJUSTED, SPECIAL	C/L
132+11	11	RT	FRAME & LID TO BE ADJUSTED	O/L
135+38	11	RT	FRAME & GRATE TO BE ADJUSTED	
135+40	11	LT	FRAME & LID TO BE ADJUSTED	

STRUCTURE RECONSTRUCTION SCHEDULE

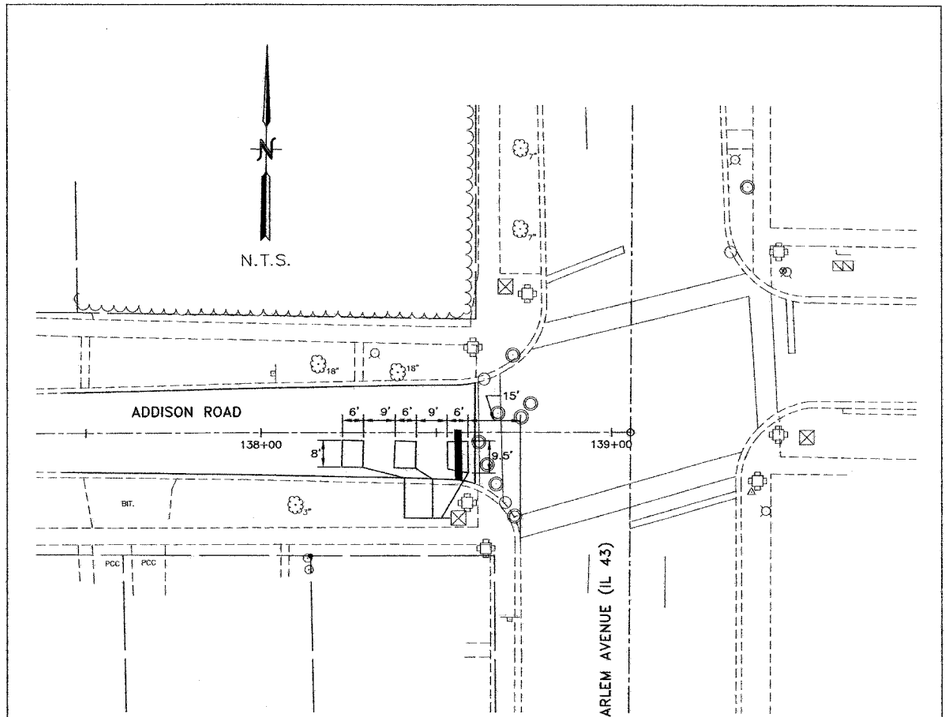
STATION	O/S (FT)	LT / RT	DESCIPTION	TYPE 1 FRAME
106+30	3	LT	VALVE VAULTS TO BE RECONSTRUCTED	C/L
111+40	35	LT	CATCH BASINS TO BE RECONSTRUCTED	O/L
113+38	0	C/L	MANHOLES TO BE RECONSTRUCTED	
113+52	10	LT	CATCH BASINS TO BE RECONSTRUCTED	
115+55	42	RT	MANHOLES TO BE RECONSTRUCTED	C/L
115+63	19.5	RT	MANHOLES TO BE RECONSTRUCTED	C/L
115+76	22	RT	CATCH BASINS TO BE RECONSTRUCTED	O/L
118+85	6	RT	MANHOLES TO BE RECONSTRUCTED	C/L
119+77	10	RT	CATCH BASINS TO BE RECONSTRUCTED	O/L
132+02	1	RT	MANHOLES TO BE RECONSTRUCTED	C/L
132+14	11	LT	INLETS TO BE RECONSTRUCTED	O/L
132+16	2.5	LT	MANHOLES TO BE RECONSTRUCTED	C/L
135+28	1	RT	MANHOLES TO BE RECONSTRUCTED	C/L
135+33	5.5	LT	MANHOLES TO BE RECONSTRUCTED	

DRIVEWAY SCHEDULE

STATION	OFFSET		ADDRESS	PROPOSED WIDTH AT SIDEWALK (FOOT)	PROPOSED WIDTH AT BACK / CURB (FOOT)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	EARTH * EXCAVATION (CU YD)	STABILIZED DRIVEWAYS SUPERPAVE, 7" (SQ YD)	P.C.C. DRIVEWAY PVMT, 7" (SQ YD)
101+12	10	LT	# 142 W	11	22	19.3			19.3
102+15	10	LT	# 142 E	15	23	12.7			12.7
103+01	10	LT	# 150	10	16	9.0			8.7
103+36	10	RT	# 163	10	16	13.9			14.5
103+95	10	LT	# 156/168	22	26	24.0			24.0
104+53	10	LT	# 174	9	15	14.0			14.0
108+75	27	RT	# 211 W	13	19	29.6	6.12		30.0
109+19	15.5	RT	# 211 E	16	24	25.6			25.6
109+76	10	RT	# 215	10	16	27.0	5.92		24.8
111+21	10	RT	# 223	9	15	15.3			15.3
111+73	10	RT	# 227	9	15	12.8			14.7
112+26	10.5	LT	# 228	15	23	32.5	6.39	16.7	20.1
112+31	10	RT	# 231/235	23	33	48.2	5.23	18.7	29.6
112+86	10	RT	# 239	10	16	43.3	7.23	30.0	14.5
112+89	10	LT	# 236	10	16	41.8	4.69		41.8
113+36**	10	RT	# 243**	10	16	28.5	6.38		28.5
113+71	10	LT	# 240	10	16	7.5			8.0
115+70	8.5	LT	# 256	9	13	39.5	7.59	23.6	15.9
115+77	59	RT	# 259	10	16	37.7	10.84		38.7
116+21	10	LT	# 260	10	16	27.0	4.58	11.1	15.9
118+24	10	RT	# 279	14	22	84.0	8.91		91.1
119+22	10	RT	# 283/285	15	19	69.6	15.73		69.6
123+73	14	LT	# 322	13	21	15.5			17.0
128+18	11	LT	# 364	10	16	22.6			20.2
131+07	11	LT	# 384/388	16	20	19.5			19.5
131+34	11	RT	# 385	9	15	17.3			17.3
131+95	11	RT	# 389	9	15	13.6			16.7
135+41	11	LT	# 412	7	13	14.3			15.0
136+58	11	RT	# 423/425	23	30	37.3			43.5
TOTALS						803	89.61	100	727

* THE EARTH EXCAVATION TOTAL INCLUDES REGRADING PARKWAYS ADJACENT TO DRIVEWAYS AND SIDEWALKS AND IS MEASURED AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

** LOCATION OF BRICK PAVERS REMOVAL AND REPLACEMENT



DETECTOR LOOP REPLACEMENT DETAIL
ADDISON ROAD AND HARLEM AVENUE

REVISIONS NAME	DATE
IDOT REV. #1	12/30/04

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1015
ADDISON ROAD

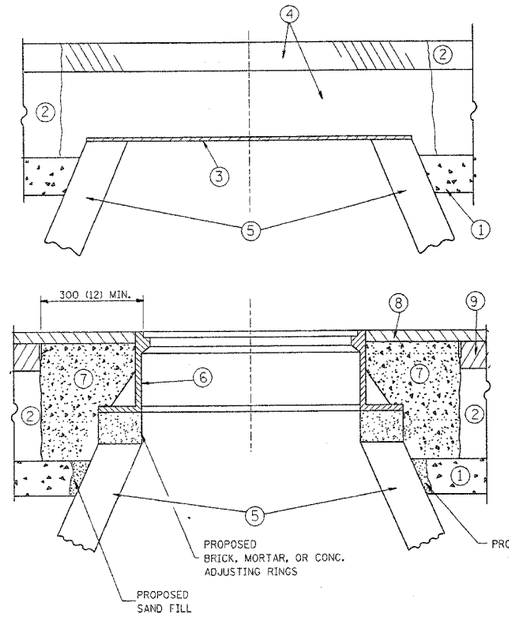
DETECTOR LOOP DETAILS
AND CONSTRUCTION SCHEDULES

SCALE: N.T.S.
DATE: 10/15/04

DRAWN BY: BCD
CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	11
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 300 (12) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 900 (36) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 40 (1½) THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

(1) SUB-BASE GRANULAR MATERIAL

(2) EXISTING PAVEMENT

(3) 900 (36) DIAMETER METAL PLATE

(4) PROPOSED CRUSHED STONE AND BITUMINOUS MATERIAL

(5) EXISTING STRUCTURE

(6) FRAME AND LID (SEE NOTES)

(7) CLASS SI CONCRETE, BITUMINOUS CONCRETE SURFACE OR BINDER COURSE MATERIAL

(8) PROPOSED BITUMINOUS CONCRETE SURFACE COURSE

(9) PROPOSED BITUMINOUS CONCRETE BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE RECONSTRUCTION PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: FRAMES AND LIDS TO BE ADJUSTED, SPECIAL EACH

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE DRAWN BY

DATE: 11/20/01 CHECKED BY

BD600-03 (BD-8)

Tue Nov 20 16:06:53 2001
\\dicaddl\projects\diststd\bd08.dgn LV=35
VI=BD8

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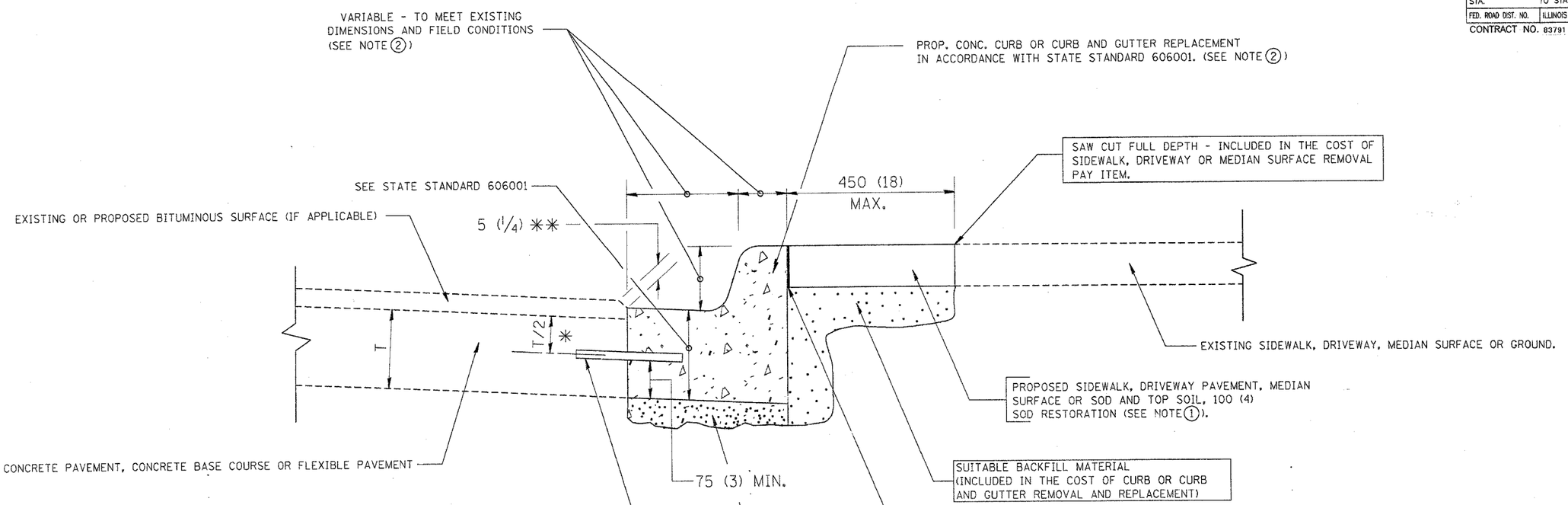
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791



* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

(2) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(3) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(4) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(5) THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(7) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

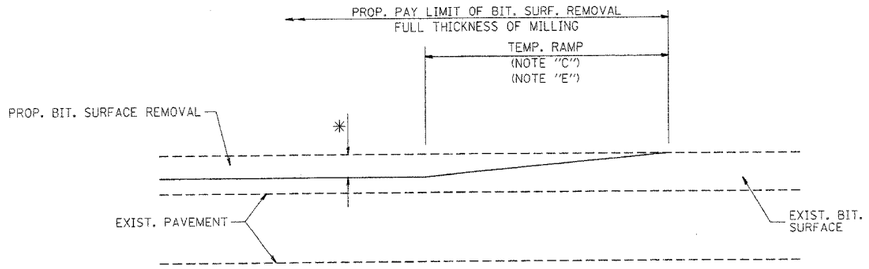
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE DRAWN BY

DATE 11/20/01 CHECKED BY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	13
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

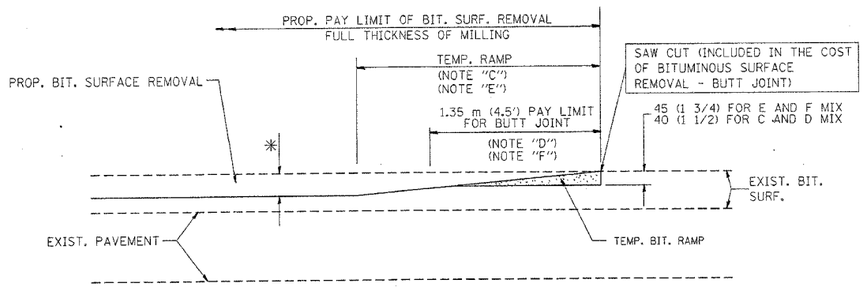
CONTRACT NO. 83791



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

OPTION 1

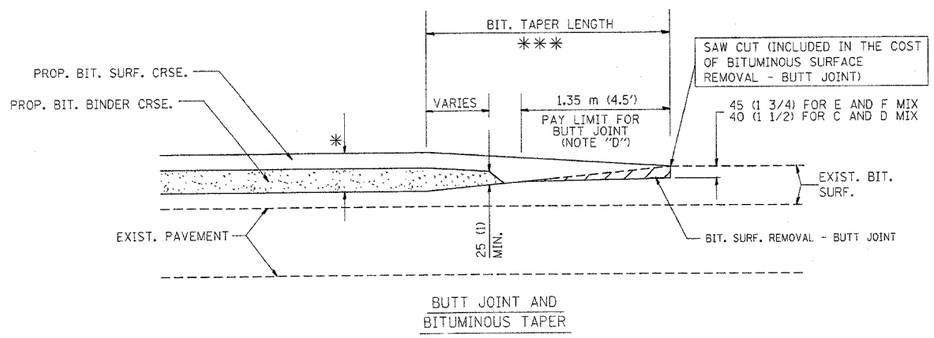


BITUMINOUS CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

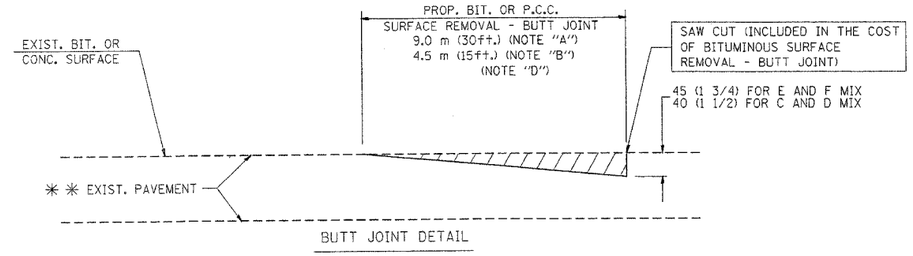
OPTION 2

TYPICAL TEMPORARY RAMP

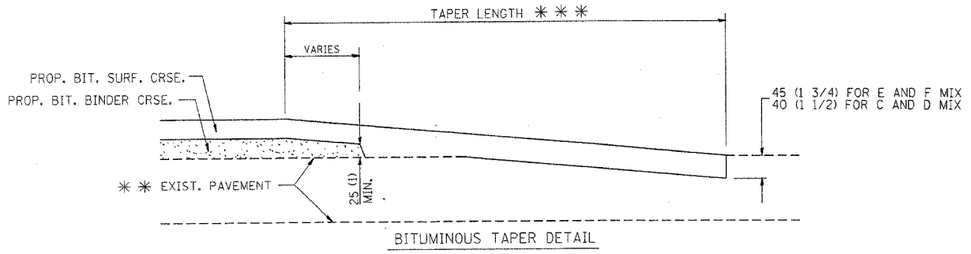


BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



BITUMINOUS TAPER DETAIL

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".

G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

SCALE: NONE DRAWN BY

DATE PLOTTED: 11/20/01 CHECKED BY

BD400-05 (VI=BD32)

Tue Nov 20 16:07:35 2001
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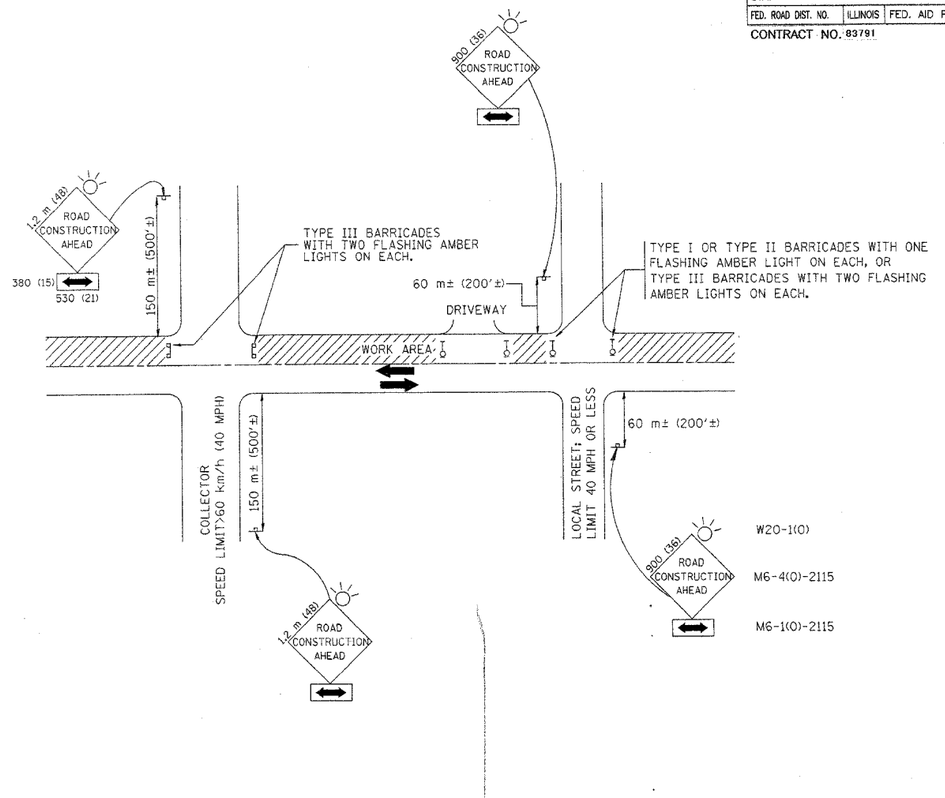
VI=BD32

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	14
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83791



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE DRAWN BY

DATE: 11/20/01 CHECKED BY

Tue Nov 20 16:07:55 2001
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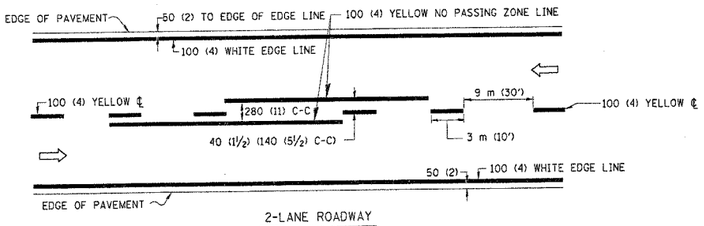
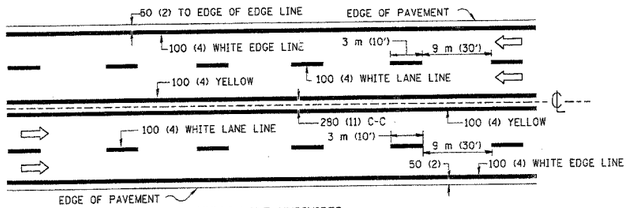
VI=TC10

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-RS	COOK	16	15
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

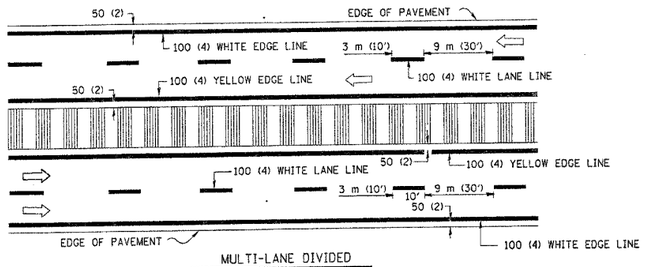
CONTRACT NO. 83791



2-LANE ROADWAY



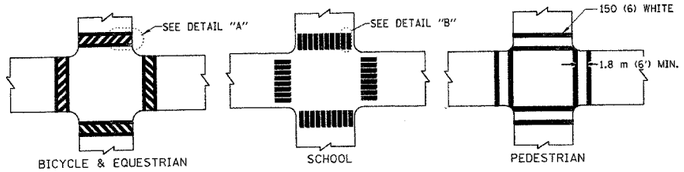
MULTI-LANE UNDIVIDED

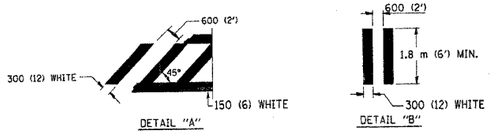


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

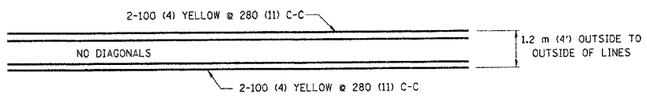
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

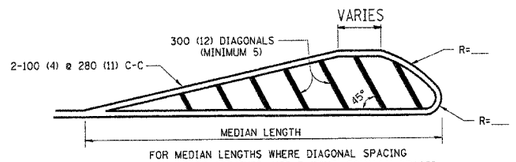




TYPICAL CROSSWALK MARKING



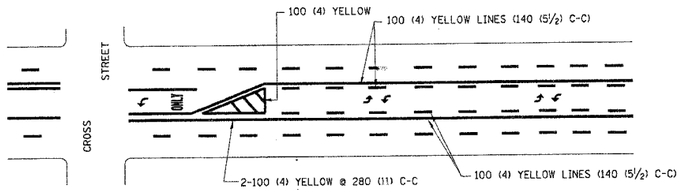
1.2 m (4') WIDE MEDIANS ONLY



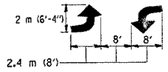
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

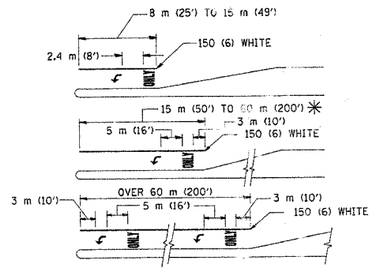


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

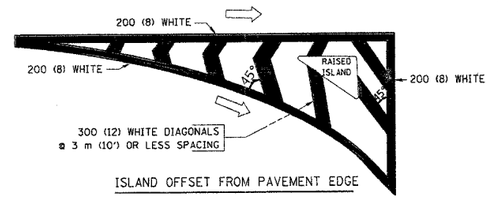


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
↰ AREA = 1.5 m² (15.6 SQ. FT.) ONLY AREA = 1.9 m² (20.8 SQ. FT.)

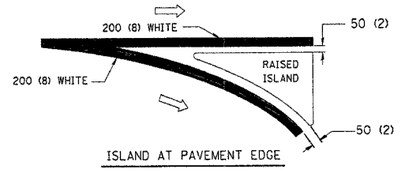
✳ TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5½) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5½) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m² (3.6 SQ. FT.) EACH "X"=5.0 m² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

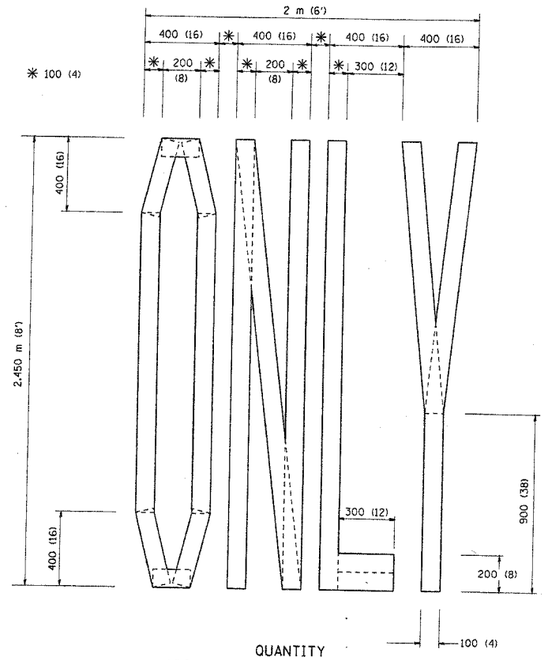
SCALE: NONE
DRAWN BY CADD
DATE 11/20/01
CHECKED BY

TC-13

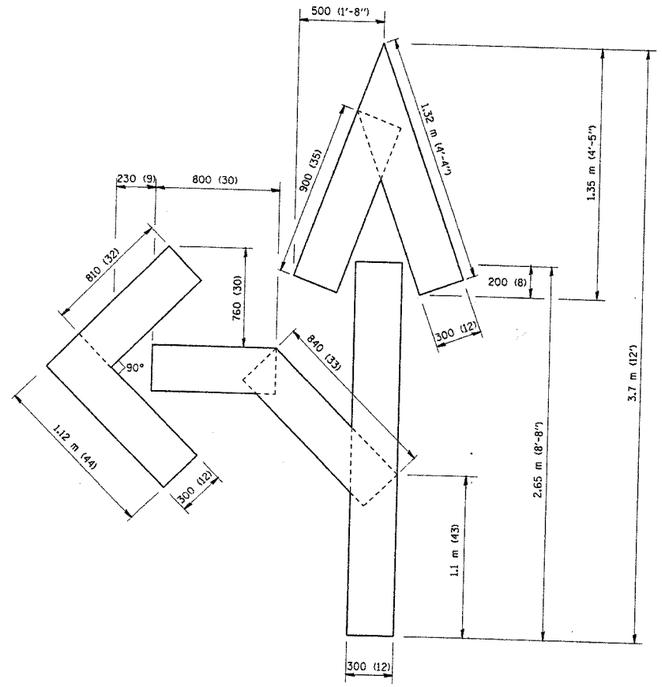
Tue Nov 20 16:08:31 2001
\\dloaddl\projects\diststd\tc13.dgn LV=35
VI=TC13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1015	04-00070-00-SS	COOK	16	16
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

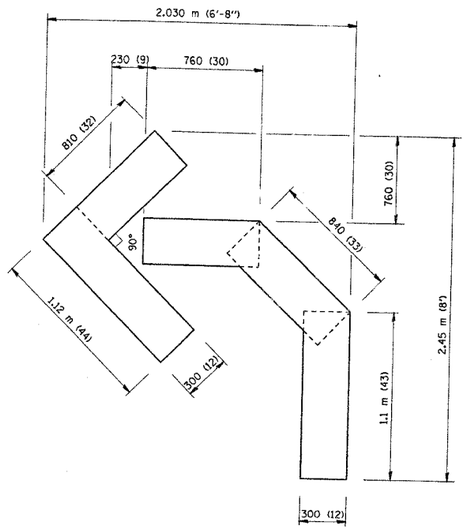
CONTRACT NO. 83791



QUANTITY
100 (4) LINE = 19.7 m (64.1 ft.)
1.97 sq. m (21.1 sq. ft.)



QUANTITY
100 (4) LINE = 25.3 m (82.5 ft.)
2.53 sq. m (27.5 sq. ft.)



QUANTITY
100 (4) LINE = 13.9 m (45.5 ft.)
1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE
DATE 11/20/01
DRAWN BY CADD
CHECKED BY

TC-16

Tue Nov 20 16:09:07 2001
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VI=TC16

sh\tc16.prk: 15:50:45 11/20/2001