

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	1

D-7 JOINT REPAIR 2005-1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 726 (IL 148)

D-7 JOINT REPAIR 2005-1

JEFFERSON COUNTY

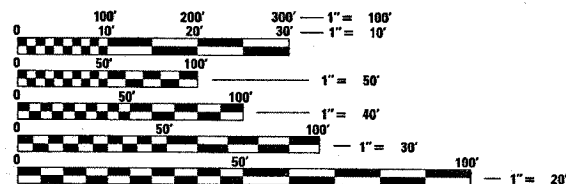
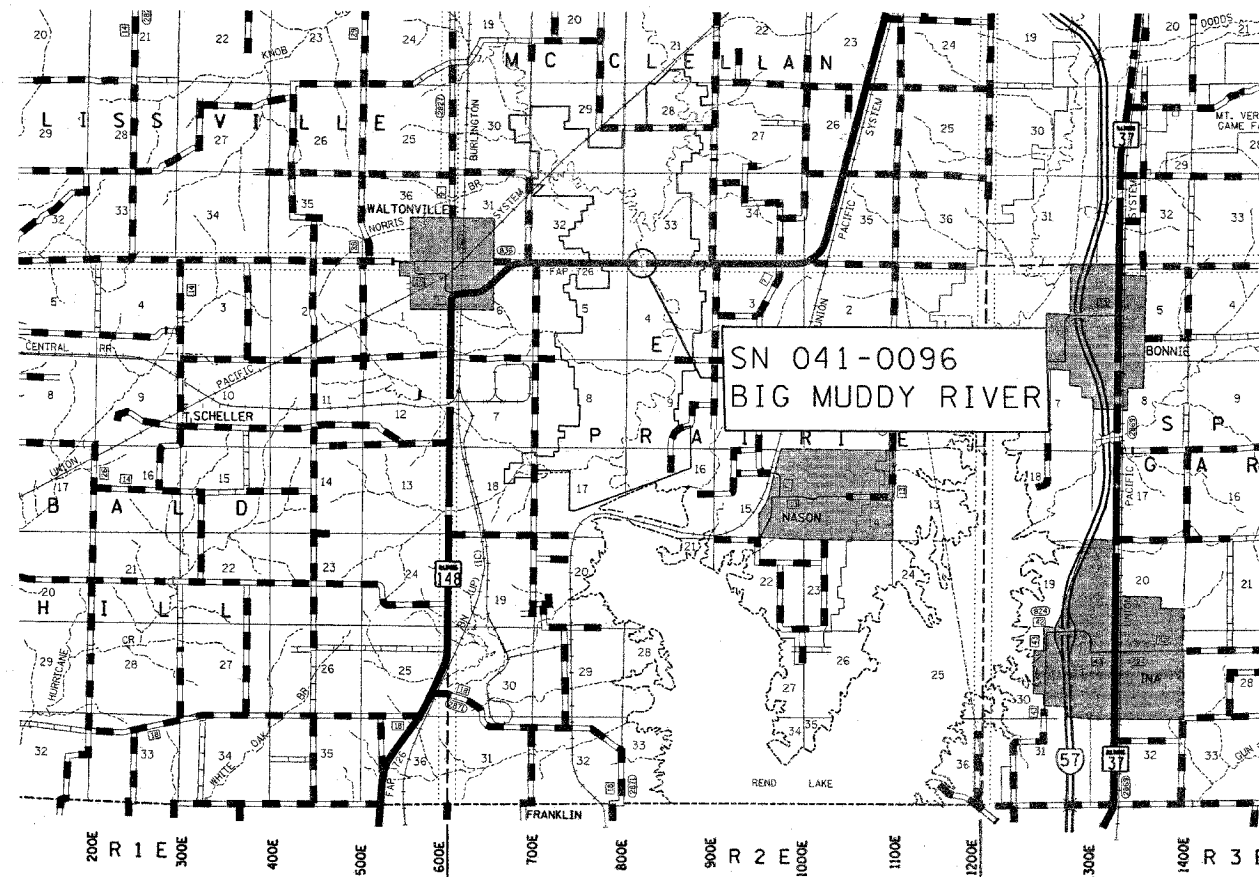
C-97-094-04

SEE SHEET 2 FOR INDEX OF SHEETS

D-97-054-04



LOCATION OF SECTION INDICATED THIS: - [black box] -



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

PROJECT ENGINEER : MIKE ALLEN
SQUAD LEADER :
DESIGNER : ALEC RING
TELEPHONE : 217/342-3951 EX 316

CONTRACT NO. 74073

2004 ADT = 2650

GROSS LENGTH = 646.7 FT
NET LENGTH = 646.7 FT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 29 2005
Christine M. Reed
DEPUTY DIRECTOR OF HIGHWAYS, REGION FOUR ENGINEER

May 13, 2005
Mike Allen
ENGINEER OF DESIGN AND ENVIRONMENT

May 13, 2005
Victor Madarik
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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CONTRACT NO. 74073

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	2
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* D-7 JOINT REPAIRS 2005-1

GENERAL NOTES

This section shall be constructed in accordance with the plans; the STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, adopted January 1, 2002; the SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, adopted January 1, 2005; and the SPECIAL PROVISIONS included in the proposal.

This project is located south of Mt. Vernon on IL. Route 148 in Jefferson County. The project includes the following structure: Structure Number 041-0096 over the Big Muddy River.

The work included in this project consists of the removal and replacement of the existing neoprene expansion joints, pavement striping, and all other work necessary to complete this section.

The reflective lenses of all raised reflective markers located between the stop bars and the end of the approach pavements shall be removed prior to stage I construction. This work will be paid for at the contract unit price for RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL. Replacement of the bi-directional amber reflectors at the completion of stage 2 will be paid for at the contract unit price for REPLACEMENT REFLECTOR.

The quantities of paint pavement marking line were computed as follows:

144	Feet (yellow)
1798	Feet (white)
1942	Feet (Total)

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVERSHEET
2	GENERAL NOTES & INDEX OF SHEETS
3	SUMMARY OF QUANTITY
4	QUANTITY SCHEDULES
5	GENERAL PLAN AND ELEVATION
6	STAGE CONSTRUCTION DETAILS
7-8	EXPANSION JOINT DETAILS
9	NEOPRENE EXPANSION JOINTS
10	BAR SPLICER ASSEMBLY DETAIL

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 10:

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
701006-02	OFF-ROAD OPERATIONS, 2L 2W, 15' TO 24" AWAY, SPEEDS > 45 MPH
701011-01	OFF-ROAD MOVING OPERATIONS, 2L 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701201-02	LANE CLOSURE, 2L 2W, DAY ONLY, ON-ROAD TO 600 MM OFF-ROAD, SPEEDS > 45 MPH
701301-02	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
701311-02	LANE CLOSURE, 2L 2W, MOVING DAY ONLY OPERATIONS
701316-03	LANE CLOSURE, 2L 2W, BRIDGE REPAIR, FOR SPEEDS > 45 MPH
701326-02	LANE CLOSURE, 2L 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
702001-05	TRAFFIC CONTROL DEVICES
720001	SIGN PANEL MOUNTING DETAILS
720006	SIGN PANEL ERECTION DETAILS
780001-01	TYPICAL PAVEMENT MARKINGS
781001-02	TYPICAL APPLICATIONS OF RAISED REFLECTIVE PAVEMENT MARKERS
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUT FOR DETECTION LOOPS

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 PLOT SCALE = #SCALE#
 REFERENCE = #REF#

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ILLINOIS DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	3
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		SFTY-2A		
50102400	CONCRETE REMOVAL	CU YD	34.1	34.1		
50300155	NEOPRENE EXPANSION JOINT 2 1/2"	FOOT	68	68		
50300160	NEOPRENE EXPANSION JOINT 4"	FOOT	34	34		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	34.3	34.3		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	6296	6296		
51500200	RELOCATING NAME PLATES	EACH	1	1		
67100100	MOBILIZATION	L SUM	1	1		
70100100	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316	EACH	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	4	4		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	57	57		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5940	5940		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1386	1386		
*78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1942	1942		
*78100300	REPLACEMENT REFLECTOR	EACH	8	8		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	637	637		
X0322050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	8	8		
Z0002600	BAR SPLICERS	EACH	40	40		

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*SPECIALTY ITEMS

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SUMMARY OF QUANTITIES

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CONTRACT NO. 74073

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	•	JEFFERSON	10	4
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• D-7 JOINT REPAIRS 2005-1				

BRIDGE REPAIR SCHEDULE

041-0096	LENGTH	CONCRETE REMOVAL		REINFORCEMENT BARS, EPOXY COATED	BAR SPLICERS	NEOPRENE EXPANSION JOINT 2 1/2"	NEOPRENE EXPANSION JOINT 4"
	FEET	CU FT	CU FT				
Stage I	646.7	17.07	17.14	3148	40	34	17
Stage II		17.07	17.14	3148		34	17
TOTAL	646.7	34.1	34.3	6296	40	68	34

PAVEMENT MARKING SCHEDULE

041-0096	PAINT PAVEMENT MARKING - LINE 4"	PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 4"	SHORT TERM PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	RAISED REFLECTOR PAVEMENT MARKER, REFLECTOR REMOVAL	REPLACEMENT REFLECTOR
041-0096	FEET	SQ FT	FEET	FEET	SQ FT	EACH	EACH
TOTAL	1942	637	5940	57	1386	8	8

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QUANTITY SCHEDULES

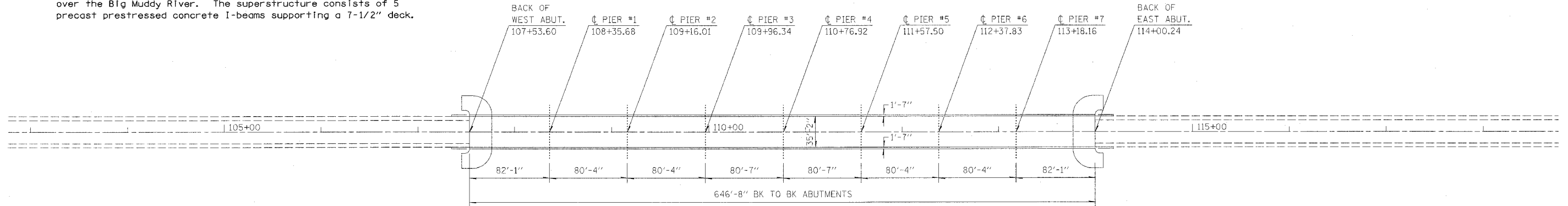
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	5
STA.		TO STA.		
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Structure 041-0096 was constructed in 1980 as an 8 span bridge over the Big Muddy River. The superstructure consists of 5 precast prestressed concrete I-beams supporting a 7-1/2" deck.



PLAN

GENERAL NOTES

Prior to pouring the new concrete deck, all loose rust, loose mill scale, and other loose potentially detrimental foreign material shall be removed from the surfaces of the beams or girders in contact with concrete. The cost of this work will be included in the pay item covering removal of the existing concrete. All heavy rust and other tightly adhered potentially detrimental foreign matter shall be removed from the surfaces of the beams or girders in contact with concrete. Tightly adhered paint may remain unless otherwise noted. This removal shall be accomplished by methods that will not damage the steel. The cost of the work will be paid for according to Article 109.04 of the Standard Specifications.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beams.

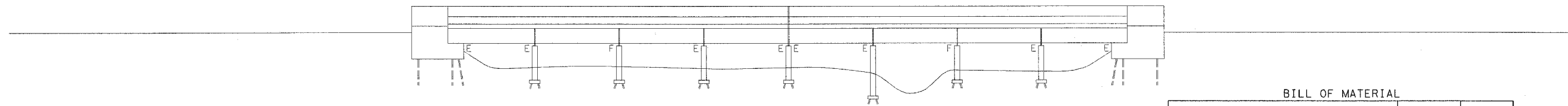
The removal and reinstallation of the first section of guardrail at each abutment to allow construction of the neoprene expansion joint shall be included in the contract unit price for concrete superstructure.

Reinforcement bars shall conform to the requirements of AASHTO M-31, OR M322 Grade 60.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work; however, the contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars shall be cleaned and ^{STRAIGHTENED} incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system (cost included in CONCRETE REMOVAL).

Joint openings shall be adjusted according to Article 503.10(c) of the STANDARD SPECIFICATIONS when the deck is poured at an ambient temperature other than 50 degrees Fahrenheit.



ELEVATION

BILL OF MATERIAL

Concrete Removal	CU YD	34.1
Neoprene Expansion Joint 2 1/2"	FOOT	68
Neoprene Expansion Joint 4"	FOOT	34
Concrete Superstructure	CU YD	34.1
Reinforcement Bars, Epoxy Coated	POUND	6296
Relocating Name Plates	EACH	1
Bar Splacers	EACH	40

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ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL PLAN
AND ELEVATION
STR #041-0096**

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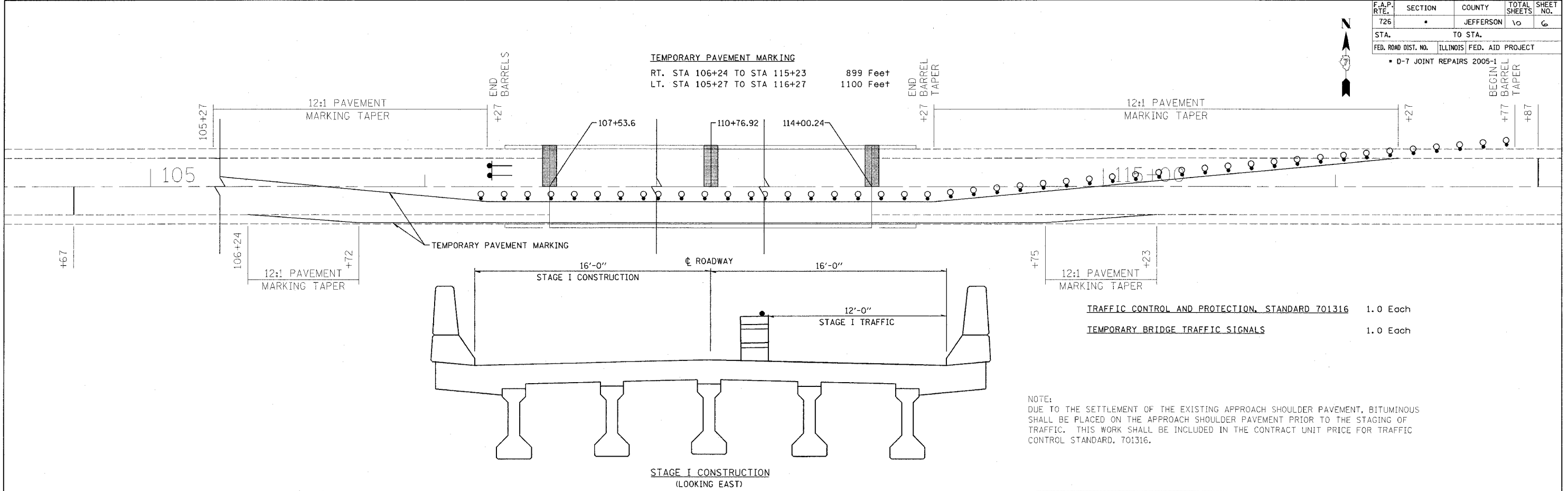
STAGE

CONTRACT NO. 74073

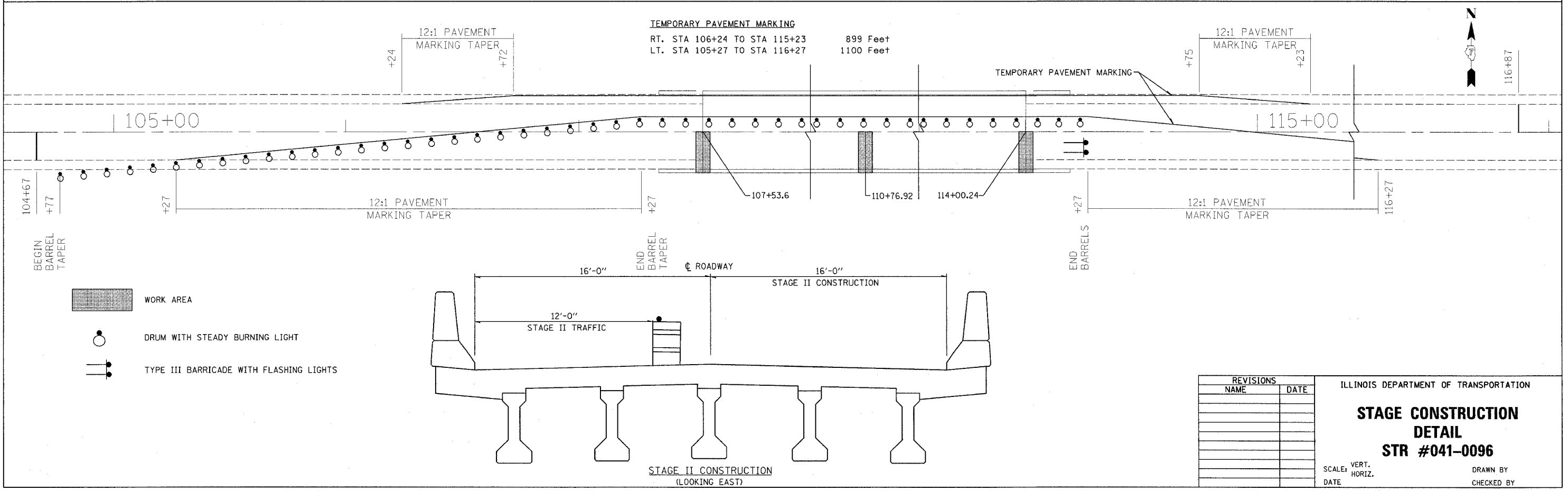
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	60	6
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



• 0-7 JOINT REPAIRS 2005-1



NOTE:
DUE TO THE SETTLEMENT OF THE EXISTING APPROACH SHOULDER PAVEMENT, BITUMINOUS SHALL BE PLACED ON THE APPROACH SHOULDER PAVEMENT PRIOR TO THE STAGING OF TRAFFIC. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL STANDARD, 701316.



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ILLINOIS DEPARTMENT OF TRANSPORTATION
**STAGE CONSTRUCTION
DETAIL
STR #041-0096**
SCALE: VERT. _____
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PLOT DATE = 3/29/2005
PLOT SCALE = AS SHOWN
REFERENCE = REF

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

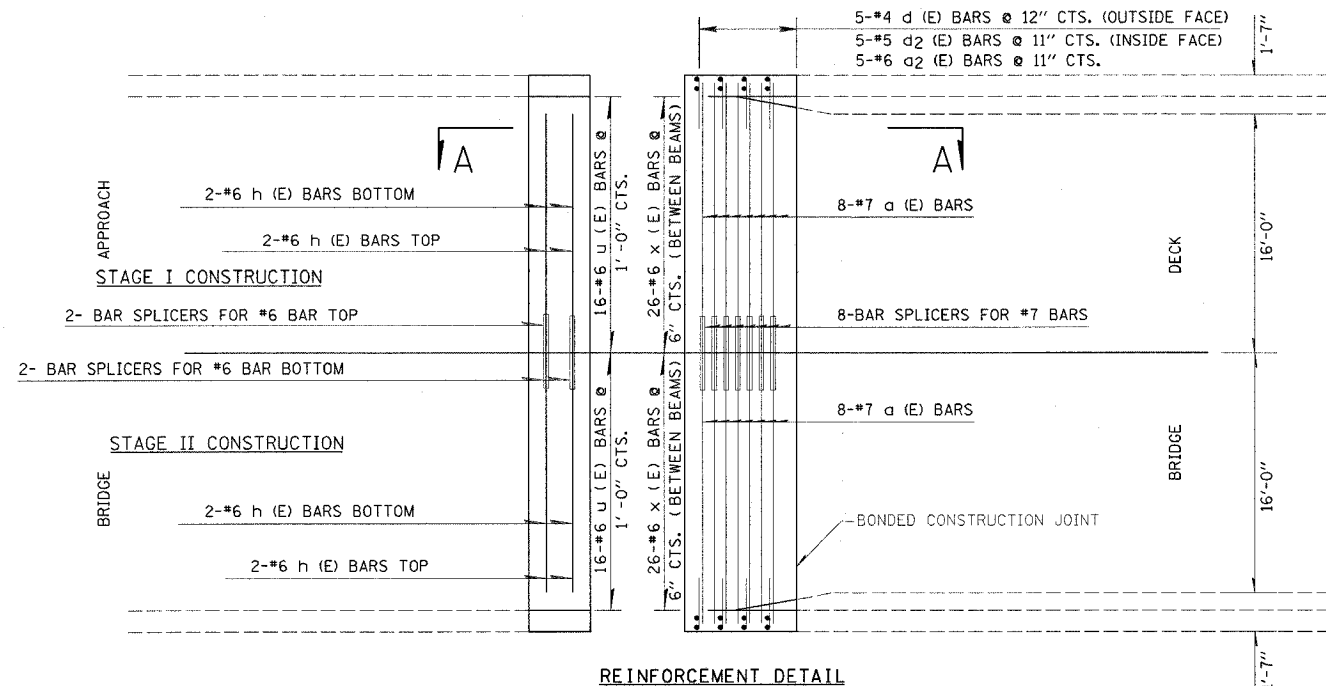
D-7 JOINT REPAIRS 2005-1

BAR LIST - PER ABUTMENTS - STR #041-0096

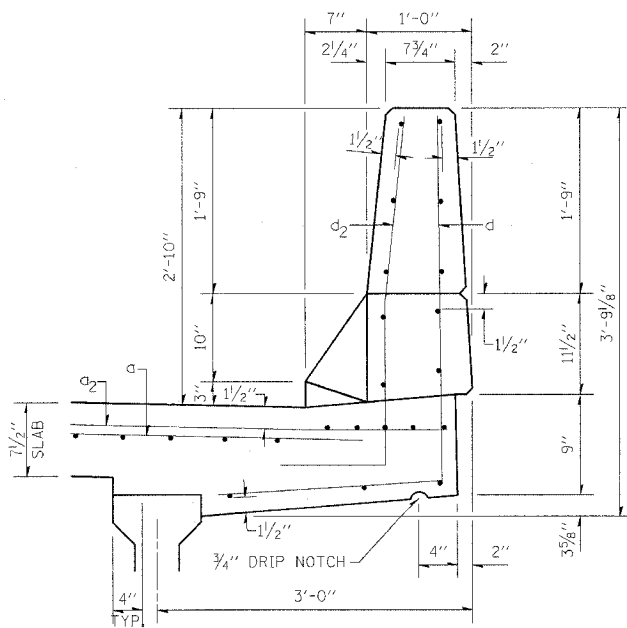
BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE
	STAGE I	STAGE II				
a (E)	8	8	16	#7	15'-1"	—
a1(E)	10	10	20	#6	6'-6"	—
a2(E)	5	5	10	#6	4'-0"	—
d(E)	5	5	10	#4	4'-10"	J
d2(E)	5	5	10	#5	4'-1"	J
h(E)	4	4	8	#6	15'-0"	—
x(E)	26	26	52	#6	6'-11"	—
u(E)	16	16	32	#6	2'-8"	□

CONCRETE REMOVAL (LIMITS ARE FROM OUT TO OUT OF DECK)

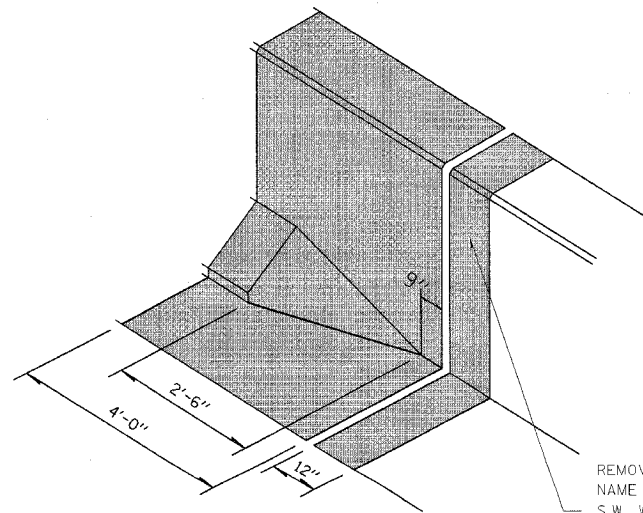
REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED



REINFORCEMENT DETAIL EAST ABUTMENT (WEST ABUTMENT SIMILAR)

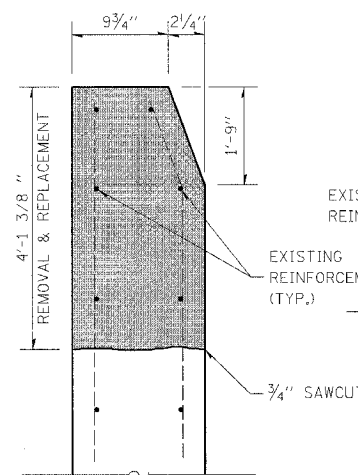


PARAPET DETAIL

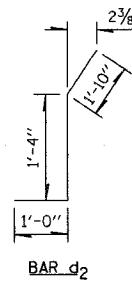


TYPICAL CONCRETE REPLACEMENT AT EACH ABUTMENT

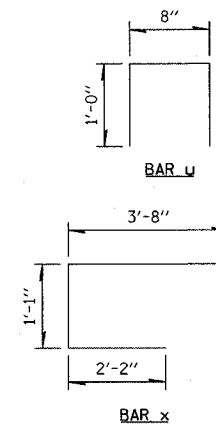
REMOVE AND REINSTALL NAME PLATE LOCATED ON S.W. WING STR. 041-0096, PAID FOR AS RELOCATING NAME PLATE 1.0 EACH



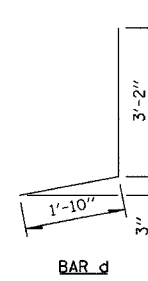
WINGWALL DETAIL



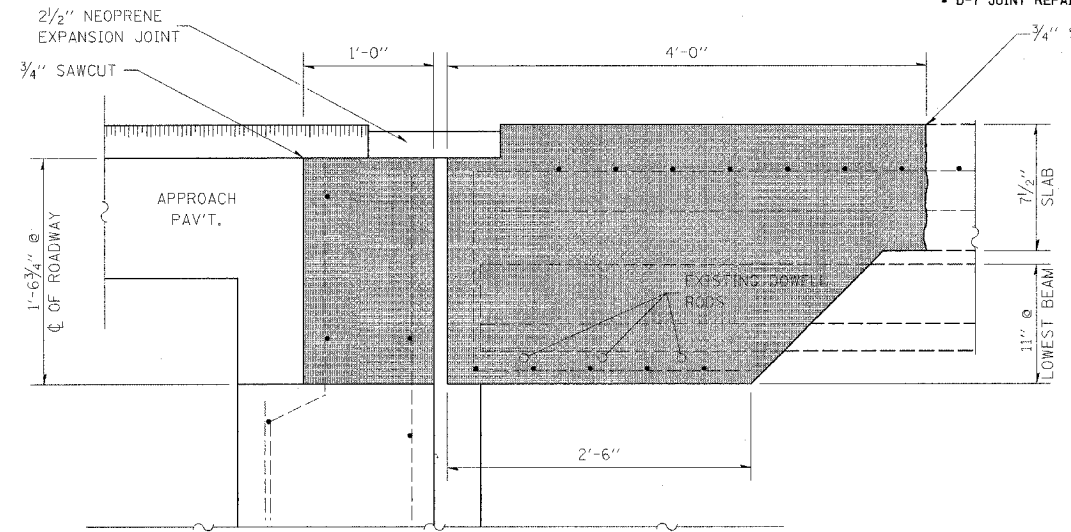
BAR d2



BAR x

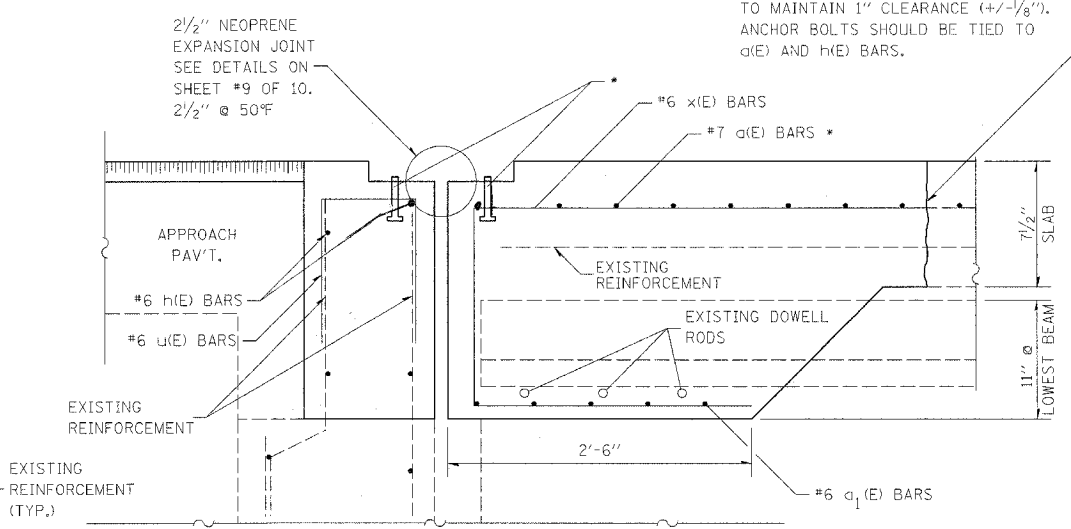


BAR d



EXISTING EXPANSION JOINTS AT ABUTMENTS

PLACE a(E) AND h(E) BARS IN BACK OF ANCHOR BOLT AS SHOWN IF REQUIRED TO MAINTAIN 1" CLEARANCE (+/- 1/8"). ANCHOR BOLTS SHOULD BE TIED TO a(E) AND h(E) BARS.



PROPOSED EXPANSION JOINTS AT ABUTMENTS

NOTE: EXISTING REINFORCEMENT BARS AND DOWELL RODS ARE TO BE CLEANED, STRIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM (COST INCLUDED IN CONCRETE REMOVAL).

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT DETAILS
EAST & WEST ABUTMENTS
STR #041-0096

SCALE: VERT.
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CONTRACT NO. 74073

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726	*	JEFFERSON	10	8
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

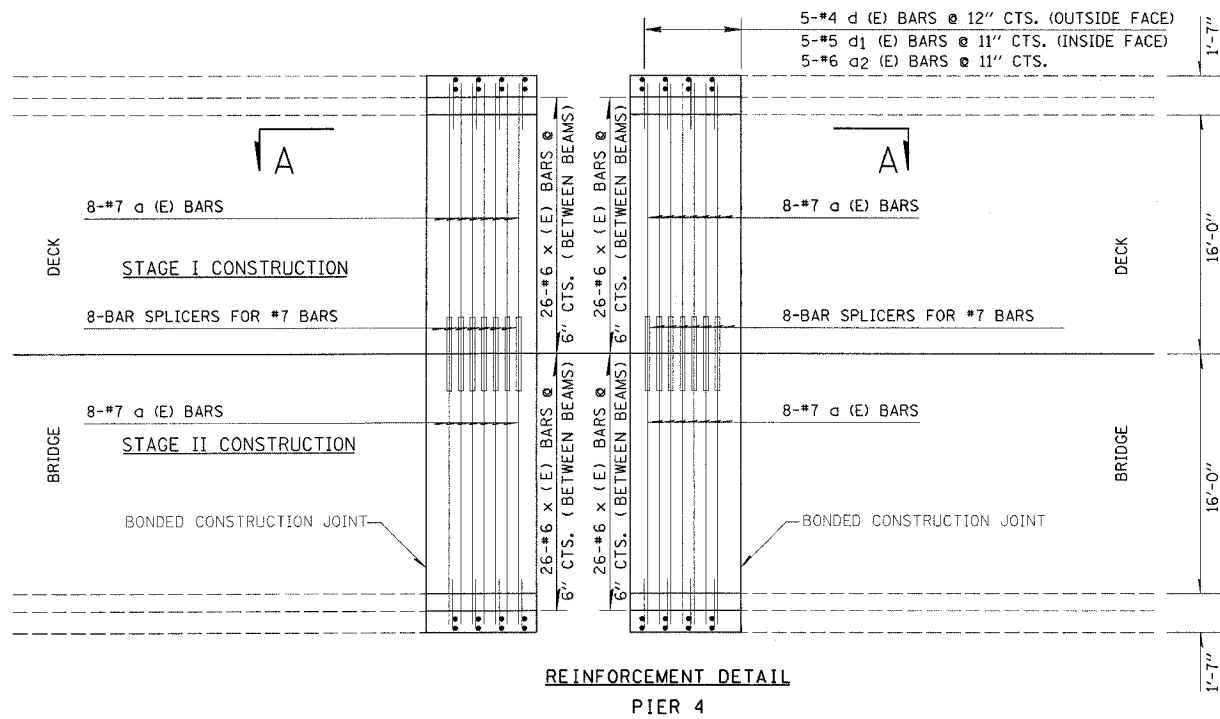
* D-7 JOINT REPAIRS 2005-1

BAR LIST - PIER 4 - STR #041-0096

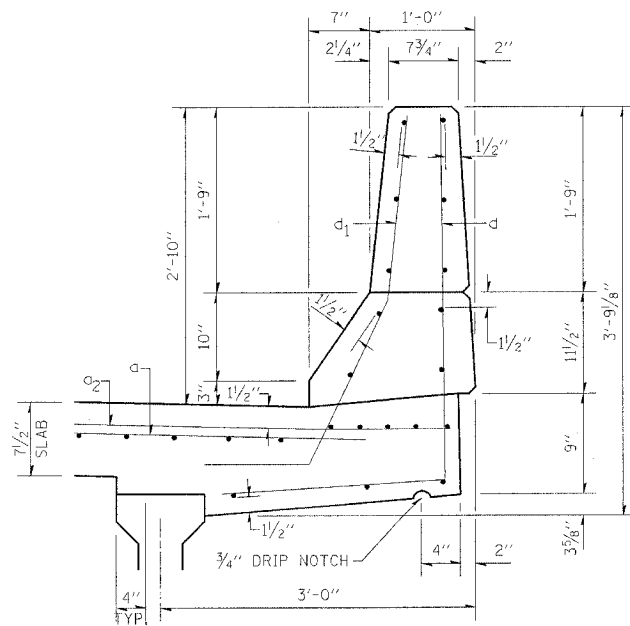
BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE
	STAGE I	STAGE II				
a (E)	16	16	32	#7	15'-1"	—
a1 (E)	20	20	40	#6	6'-6"	—
a2 (E)	10	10	20	#6	4'-0"	—
d (E)	10	10	20	#4	4'-10"	J
d1 (E)	10	10	20	#5	4'-1"	J
x (E)	52	52	104	#6	6'-11"	—

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED

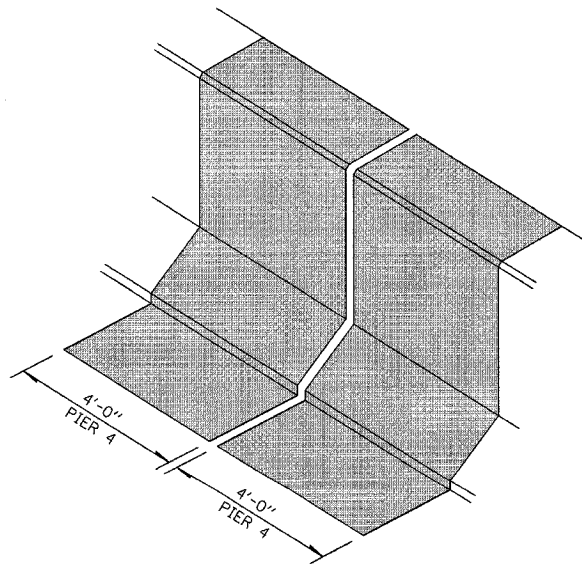
CONCRETE REMOVAL (LIMITS ARE FROM OUT TO OUT OF DECK)



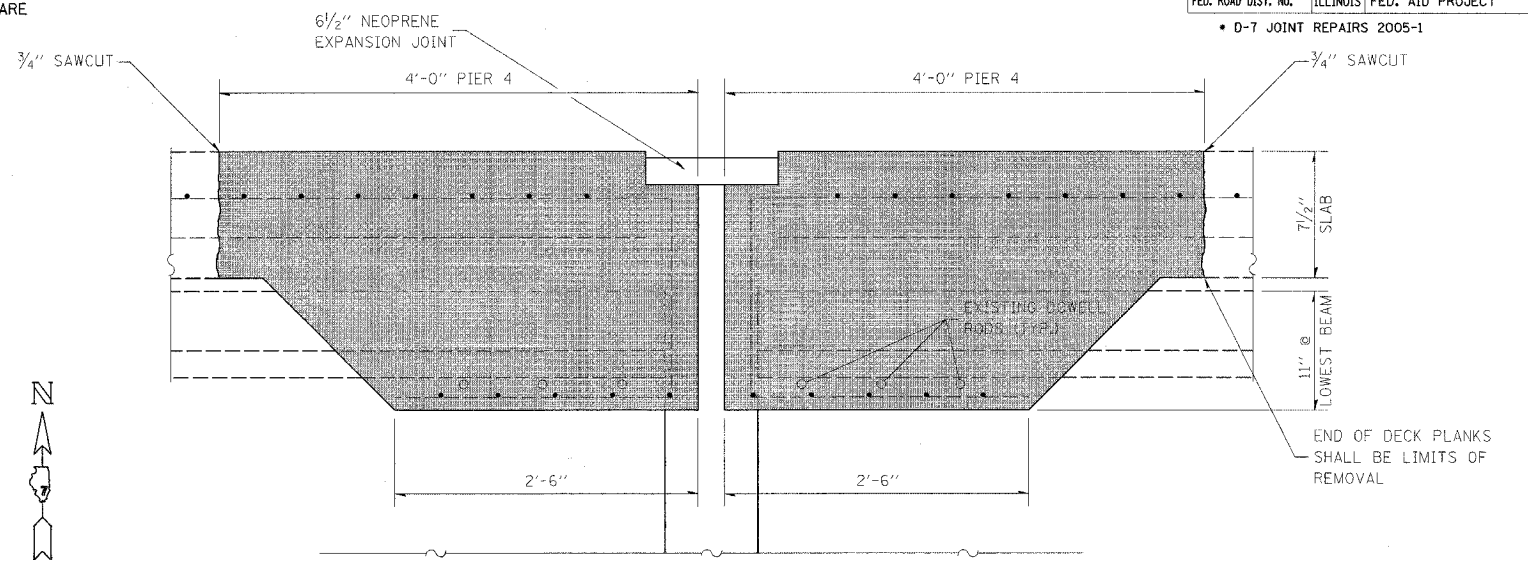
REINFORCEMENT DETAIL
PIER 4



PARAPET DETAIL

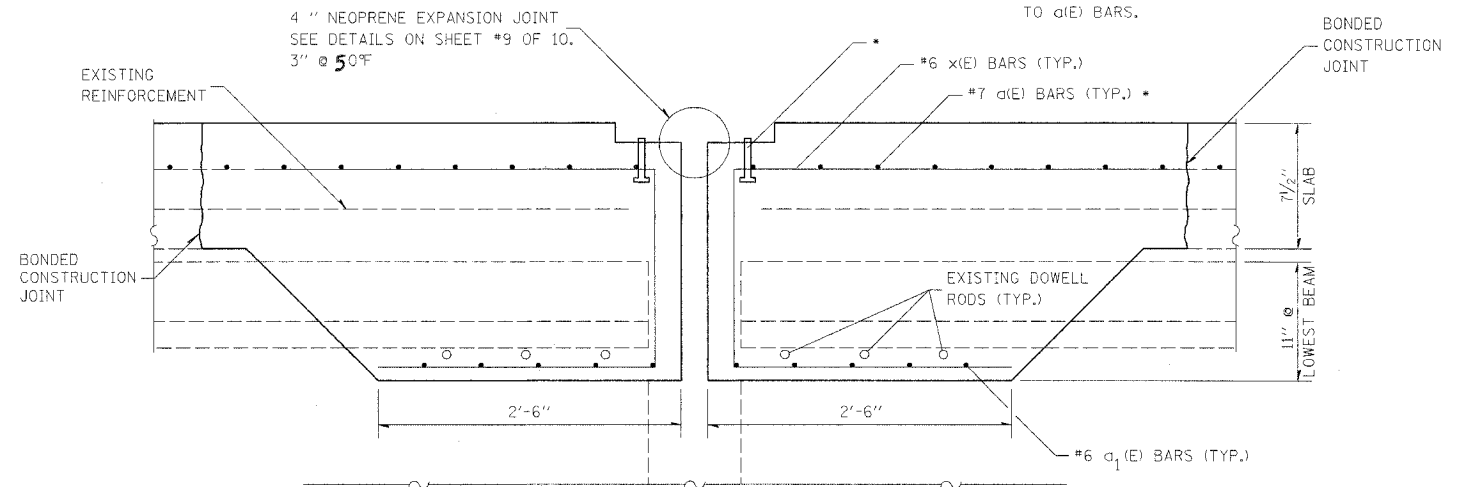


TYPICAL CONCRETE REPLACEMENT
PIER 4



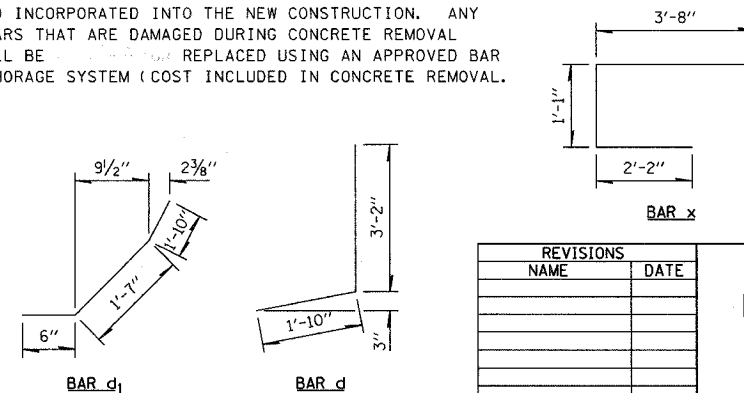
EXISTING EXPANSION JOINTS AT PIER 4

* PLACE a1(E) BARS IN BACK OF ANCHOR BOLT AS SHOWN IF REQUIRED TO MAINTAIN 1" CLEARANCE (+/- 1/8"). ANCHOR BOLTS SHOULD BE TIED TO a1(E) BARS.



PROPOSED EXPANSION JOINTS AT PIER 4

NOTE: EXISTING REINFORCEMENT BARS AND DOWELL RODS ARE TO BE CLEANED, STRIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL OPERATIONS SHALL BE REPLACED USING AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM (COST INCLUDED IN CONCRETE REMOVAL).



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT DETAILS
PIER 4
STR #041-0096

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PLOT SCALE = #SCALE*
REFERENCE = #REF*

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

D-7 JOINT REPAIRS 2005-1

GENERAL NOTES

Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.
 The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.
 The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.
 Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
 The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.

Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1 1/2" Min.
2 1/2"	2 1/2"	1 3/4" Min.
4"	3"	2 1/2" Min.

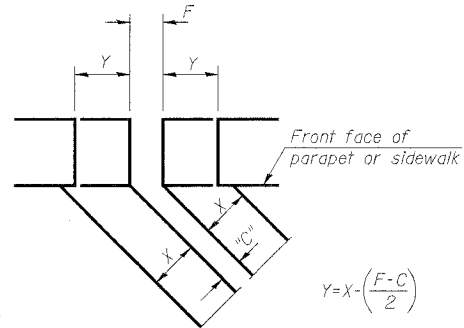
INSTALLATION NOTES

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

Note A:
Maximum spacing of anchor bolts shall be 12" centers.

SKREW LIMITATIONS

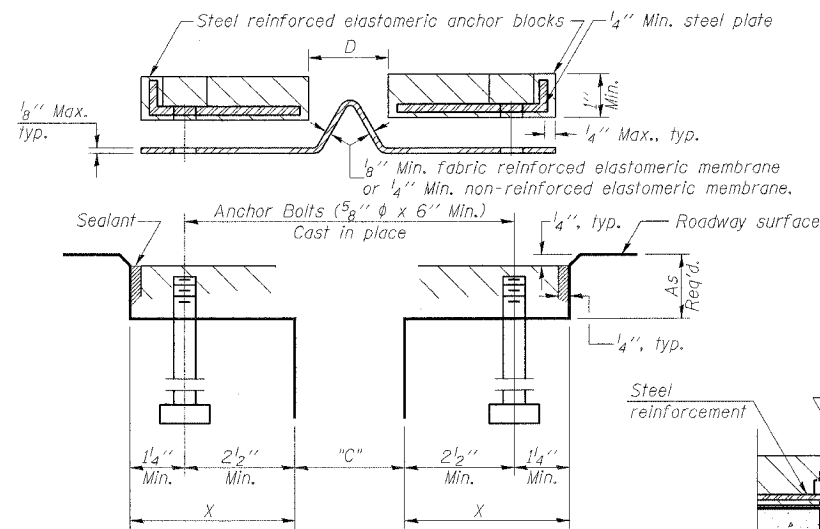
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1/2" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



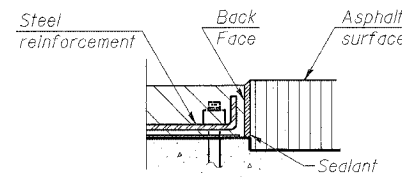
$$Y = X - \left(\frac{F - C}{2} \right)$$

For dimension "F" see sheet # 7-8

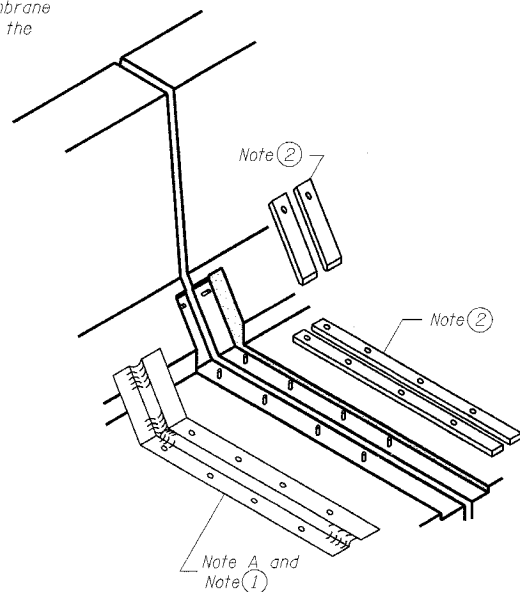
FORMING BLOCKOUT SKETCH



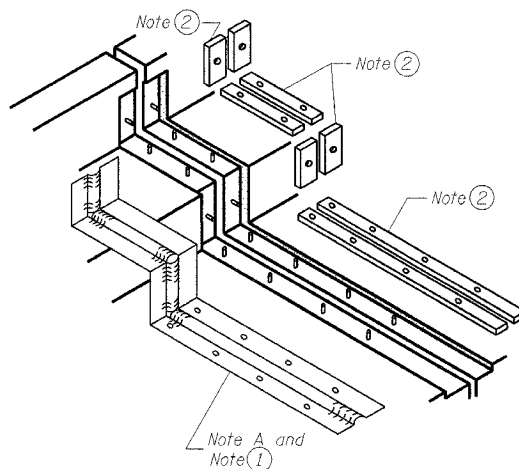
CROSS SECTION



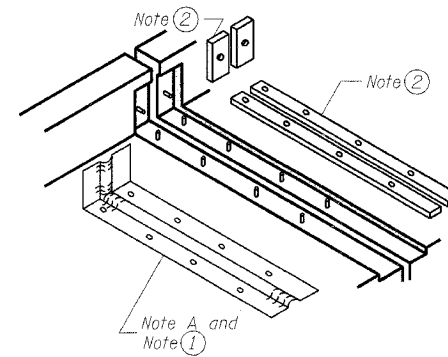
ANCHOR BLOCK WITH ASPHALT SURFACE



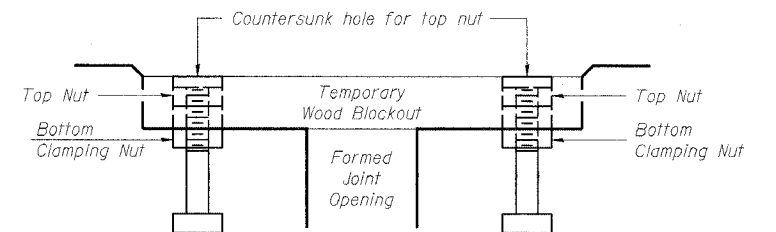
AT PARAPET



AT SIDEWALK OR MEDIAN



AT WALL

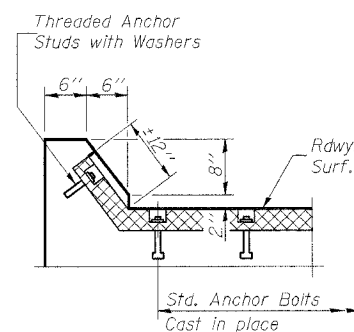


RECOMMENDED BLOCKOUT DETAIL

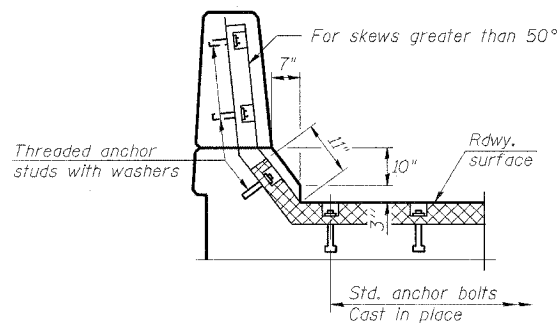
Note:
Stud needs to be threaded lower to allow for use of clamping nut.

Anchor studs should be stainless

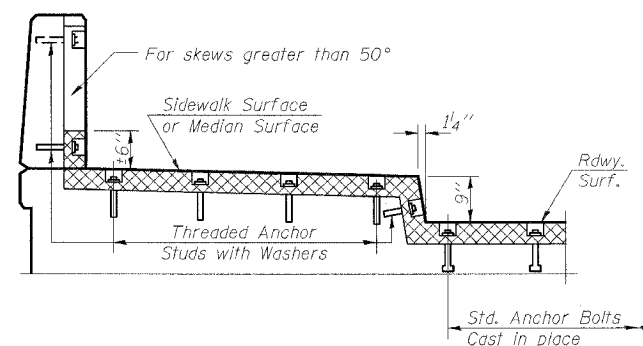
CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS



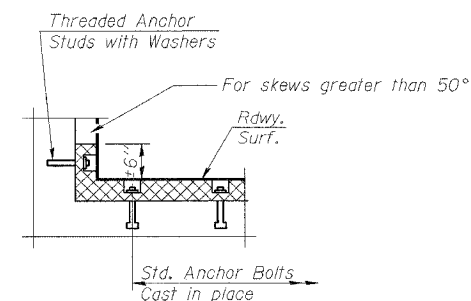
AT CURB



AT PARAPET



AT SIDEWALK OR MEDIAN TYPICAL END TREATMENTS



AT WALL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS STR #041-0096

SCALE: VERT. HORIZ. DATE

DRAWN BY CHECKED BY

steffenmk
 3/29/2005
 c:\projects\74073\d05404pa.dgn
 EJ CS

PLOT DATE = 3/29/2005
 FILE NAME = #FILES#
 PLOT SCALE = #SCALE#
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
726		JEFFERSON	10	10
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

D-7 JOINT REPAIRS 2005-1

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity = $1.25 \times f_y \times A_t$
(Tension in kips)
- ② Minimum *Pull-out Strength = $1.25 \times f_{s_{allow}} \times A_t$
(Tension in kips)

Where f_y = Yield strength of lapped reinforcement bars in ksi.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in ksi (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	5.9
#5	2'-0"	23.0	9.2
#6	2'-7"	33.1	13.3
#7	3'-5"	45.1	18.0
#8	4'-6"	58.9	23.6
#9	5'-9"	75.0	30.0
#10	7'-3"	95.0	38.0
#11	9'-0"	117.4	46.8

Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."

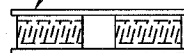
The diameter of this part is equal or larger than the diameter of bar spliced.

ROLLED THREAD DOWEL BAR



** ONE PIECE

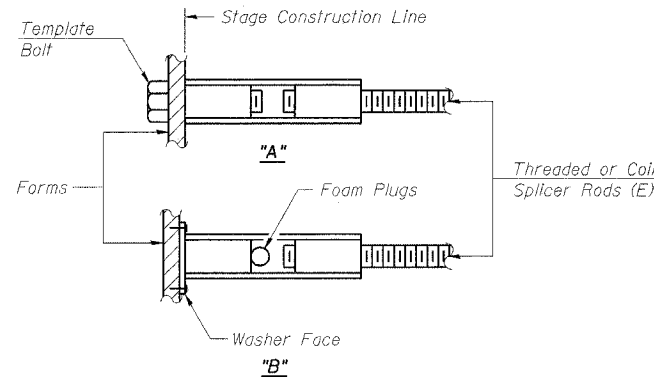
Wire Connector



WELDED SECTIONS

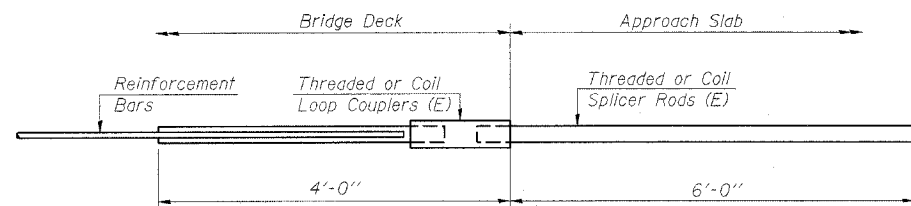
BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



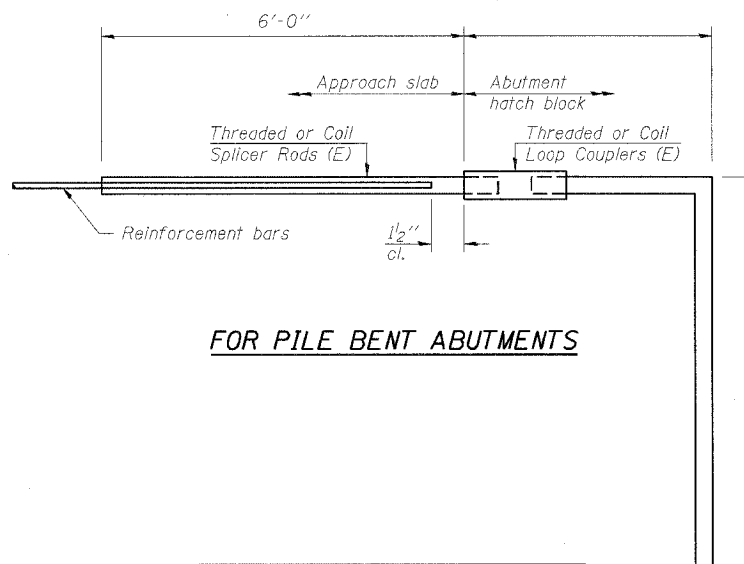
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



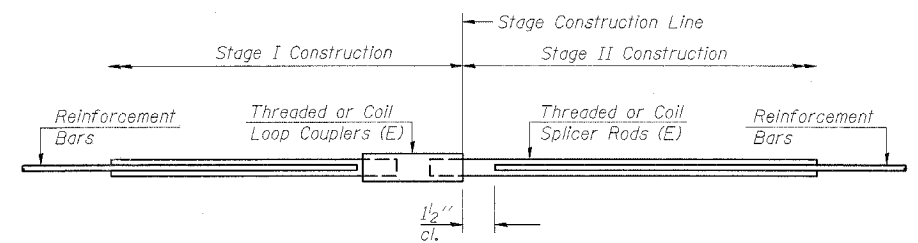
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



FOR PILE BENT ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 9.2 kips - tension
No. Required =



STANDARD

Bar Size	No. Assemblies Required	Location
#6	8	E. & W. ABUTMENTS
#7	16	ABUTMENTS
#7	16	PIER 4

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY DETAILS STR #041-0096

SCALE: VERT. DATE
 HORIZ.

DRAWN BY
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steffernmk
 3/29/2005
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