

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	1

+2
70

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

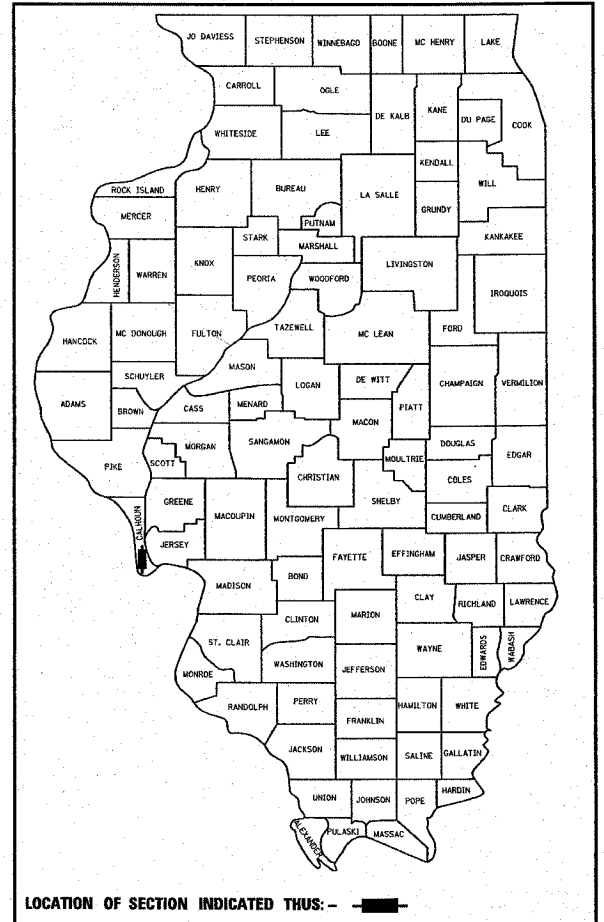
FAP ROUTE 304 (IL 100)
SECTION 5BR-2
STRUCTURE REPLACEMENT OVER CRAWFORD CREEK
CALHOUN COUNTY

C-98-115-05

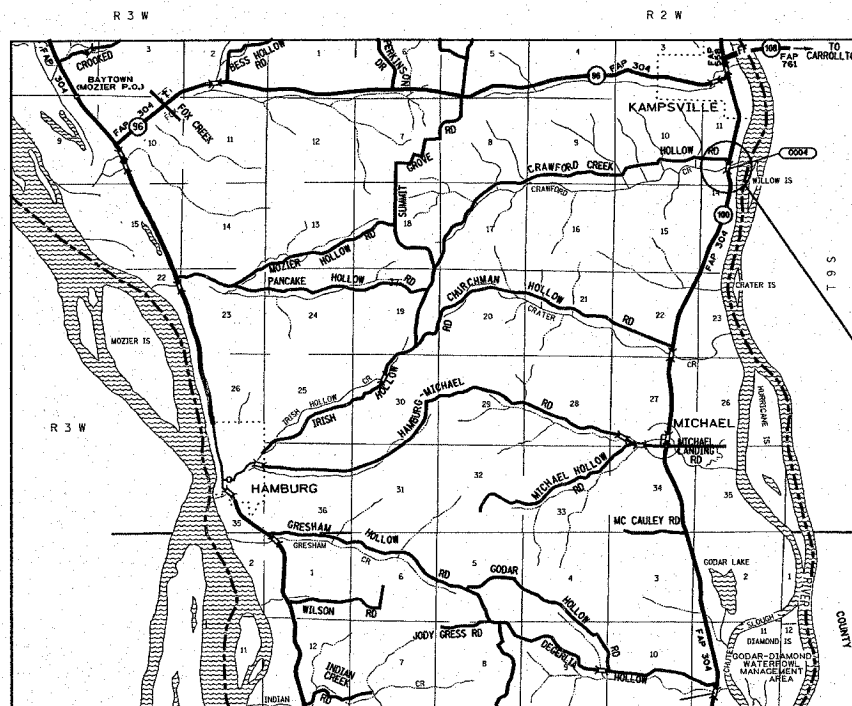
PROJECT: BRF-0304(036)

FOR INDEX OF SHEETS, SEE SHEET NO. 2

D-98-090-05



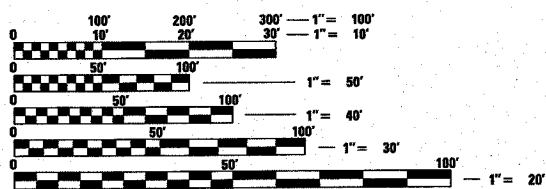
LOCATION OF SECTION INDICATED THUS: —■—



PROJECT LOCATION
IL 100 OVER CRAWFORD CREEK
SN 007-0004(E) 0027(P)
STA 467+59.00



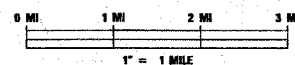
MICROFILMED _____
REEL NUMBER _____
AWARDED _____
RESIDENT ENGINEER _____
AS BUILT CHANGES WERE MADE
ON THE FOLLOWING SHEETS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

TRAFFIC DATA:
ADT: 1450 (2008)
1650 (2028)
SU: 2.9%
MU: 4.3%



LATITUDE: 39.2813 LONGITUDE: 90.6096

GROSS LENGTH (ABUT. TO ABUT.) = 116.03 FT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Jan 31 20 08
Mark C. Adams
DEPUTY DIRECTOR OF HIGHWAYS
REGION FIVE ENGINEER

March 21 20 08
Eric E. Harms
SENIOR ENGINEER OF DESIGN AND ENVIRONMENT

March 21 20 08
Christine M. Reed
DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: PATTI LEBEAU (618) 346-3179
SQUAD CONTACT: ART MUEHLFELD (618) 346-3209

CONTRACT NO. 76948

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A FED 80% STATE 20%	X071-2A FED 80% STATE 20%
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	99	99	
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	824	824	
20200100	EARTH EXCAVATION	CU YD	200	200	
20200500	EARTH EXCAVATION (WIDENING)	CU YD	80	80	
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	126		126
28000255	TEMPORARY EROSION CONTROL SEEDING	ACRE	0.25	0.25	
28100107	STONE RIPRAP, CLASS A4	SQ YD	255	255	
28000300	TEMPORARY DITCH CHECKS	EACH	3	3	
28100109	STONE RIPRAP, CLASS A5	SQ YD	1479	1479	
28100807	STONE DUMPED RIPRAP, CLASS A4	TON	4021	4021	
28200200	FILTER FABRIC	SQ YD	1734	255	1479
35600712	HOT-MIX ASPHALT BASE COURSE WIDENING, 9"	SQ YD	306	306	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.5	0.5	
40600300	AGGREGATE (PRIME COAT)	TON	2.1	2.1	
40600990	TEMPORARY RAMP	SQ YD	60	60	
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	370	370	
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	118	118	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	11	11	
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	247	247	
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	50	50	
44000100	PAVEMENT REMOVAL	SQ YD	620	620	
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	605	605	
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	380	380	
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	365	365	
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1		1
50105220	PIPE CULVERT REMOVAL	FOOT	70	70	
50200100	STRUCTURE EXCAVATION	CU YD	424		424
50300100	FLOOR DRAINS	EACH	7		7
50300225	CONCRETE STRUCTURES	CU YD	168		168
50300255	CONCRETE SUPERSTRUCTURE	CU YD	155.4		155.4
50300260	BRIDGE DECK GROOVING	SQ YD	402		402
50300280	CONCRETE ENCASEMENT	CU YD	10.8		10.8
50300300	PROTECTIVE COAT	SQ YD	576		576
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1		1
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	4420		4420
50500505	STUD SHEAR CONNECTORS	EACH	2376		2376
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	52150		52150
50800515	BAR SPLICERS	EACH	517		517

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A FED 80% STATE 20%	X071-2A FED 80% STATE 20%
51201600	FURNISHING STEEL PILES HP12X53	FOOT	575		575
51201900	FURNISHING STEEL PILES HP14X89	FOOT	620		620
51202305	DRIVING PILES	FOOT	1195		1195
51203600	TEST PILE STEEL HP12X53	EACH	2		2
51203900	TEST PILE STEEL HP14X89	EACH	2		2
51205200	TEMPORARY SHEET PILING	SQ FT	673		673
51500100	NAME PLATES	EACH	1		1
52100520	ANCHOR BOLTS, 1"	EACH	48		48
54213453	END SECTIONS 18"	EACH	2	2	
542D1063	PIPE CULVERTS, CLASS D, TYPE 2 18"	FOOT	72.5	72.5	
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	79		79
60109580	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	106		106
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	650	650	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	968.5	968.5	
* 63300205	REMOVAL AND REINSTALLATION OF EXISTING STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	601	601	
66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	16	16	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	7	7	
67100100	MOBILIZATION	L SUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1	
70101205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1		1
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4442	4442	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1620	1620	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	72	72	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1625	1625	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	870	870	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	750	750	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	10	10	
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	24	24	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3574	3574	

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* SPECIALTY ITEMS

Rev. 4-3-08

Rev

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

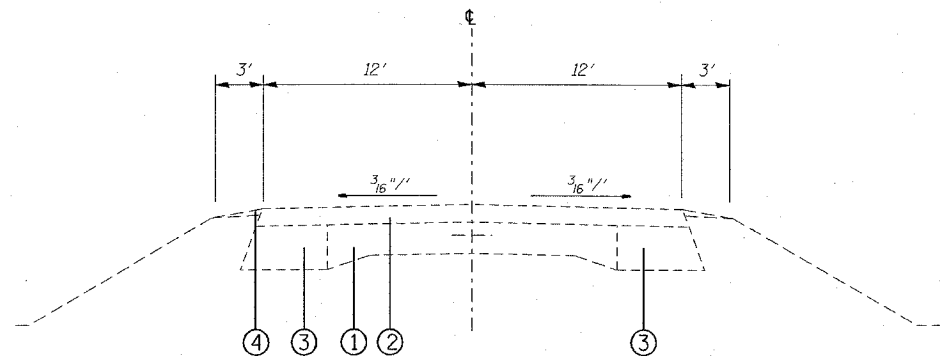
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		1000-2A FED 80% STATE 20%	X071-2A FED 80% STATE 20%
* 7800650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	20	20	
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	705		705
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	12	12	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	2		2
* 78200200	BIDIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	4	4	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	12	12	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	840	840	
X5020501	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 1	EACH	1		1
X5020502	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 2	EACH	1		1
X7200200	WIDE LOAD SIGNING	L SUM	1	1	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
<i>Z0001900</i>	<i>ASBESTOS BEARING PAD REMOVAL</i>	<i>EACH</i>	<i>48</i>		<i>48</i>

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*SPECIALTY ITEMS

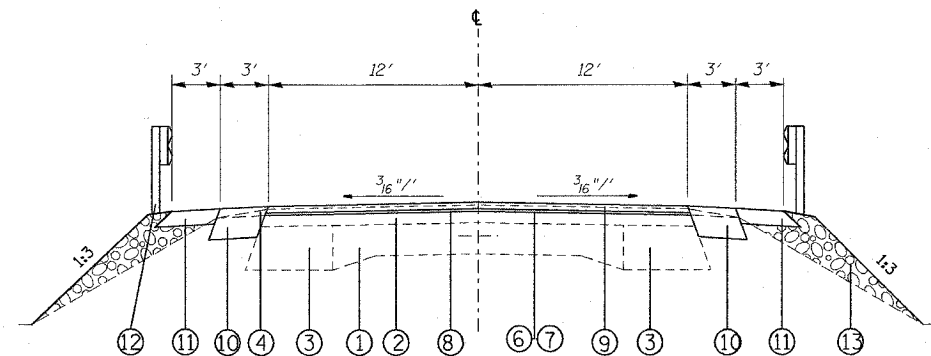
Rev. 4-3-08

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	SBR-2	CALHOUN	68	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



EXISTING TYPICAL SECTION

STA. 463+70.00 TO STA. 471+65.00



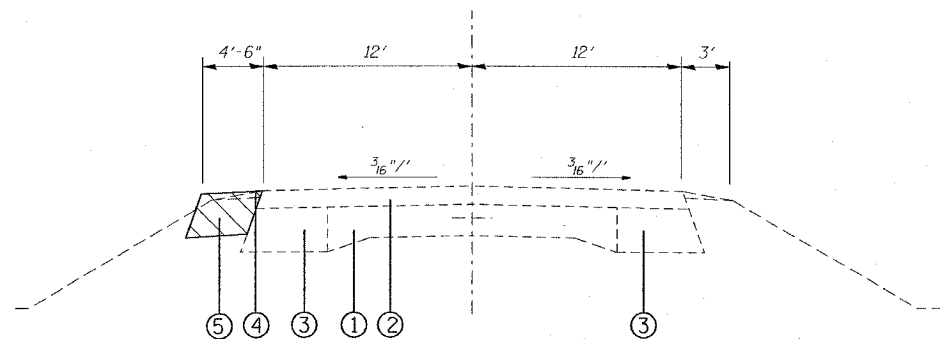
PROPOSED TYPICAL SECTION

STA. 464+15.00 TO STA. 470+15.00

STONE RIPRAP:

STA. 463+58.00 TO STA. 471+13.00 (LT)

STA. 464+15.00 TO STA. 471+05.00 (RT)



PROPOSED TYPICAL SECTION

STA. 463+70.00 TO STA. 471+21.00 - LT

PAVEMENT REMOVAL

LEGEND

- ① EXISTING P.C.C. PAVEMENT 9-7-9
- ② EXISTING BITUMINOUS OVERLAY 6" (±)
- ③ EXISTING BASE COURSE WIDENING 8"
- ④ EXISTING AGGREGATE SHOULDERS
- ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING, 9"
- ⑥ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ PROPOSED AGGREGATE (PRIME COAT)
- ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE VARIABLE DEPTH (0.75" TO 14.3")
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- ⑩ PROPOSED HOT-MIX ASPHALT SHOULDER, 8 "
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B 6"
- ⑫ PROPOSED GUARDRAIL
- ⑬ PROPOSED STONE RIPRAP

MIXTURE REQUIREMENTS

MIXTURE USE	SURFACE	INCIDENTAL SURF	BINDER	WIDENING COURSE	SHOULDERS	TOP LIFT SHOULDERS
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 58-22	PG 58-22
RAP % (MAX)	10%	10%	15%	15%	30%	30%
DESIGN AIR VOIDS	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	2.0% @ Ndes=30	**2.0% @ Ndes=30
MIX COMPOSITION						
(GRADATION MIXTURE)						
FRICITION AGG	MIXTURE "D"	MIXTURE "C"	MIXTURE "B"	MIXTURE "B"	BAM	BAM

** TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%.

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS).

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION TYPICAL SECTIONS MIXTURE REQUIREMENT
NAME	DATE	
		FAP ROUTE 304 SECTION 5BR-2 CALHOUN COUNTY

SCALE: VERT. _____
HORIZ. _____

DATE _____ DRAWN BY _____
CHECKED BY _____

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	6
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

RESURFACING SCHEDULE

STATION	RT/LT	HOT-MIX ASPHALT BS WIDENING 9" (SQ YD)	AGG. PRIME COAT (TON)	BIT. MAT'L PRIME COAT (TON)	INCIDENTAL HMA SURF. (TON)	BIT. CONC. BINDER SUPER. MIX "B" (TON)	BIT. CONC. SURF. CSE. SUPER. MIX "C", N70 (TON)	HMA SHOULDERS 8" (SQ YD)	AGG. SHLD TYPE B 6" (SQ YD)
463+70.00 TO 466+71.50	LT	150.75							
464+15.00 TO 466+71.39	RT/LT		1.03	0.21			57.43	170.93	170.93
464+15.00 TO 465+04.86	RT/LT					10.06			
465+04.86 TO 466+71.39	RT/LT					156.67			
468+11.50 TO 471+21.00	LT	154.75							103.17
468+42.80 TO 471+19.00	LT							92.07	
468+47.42 TO 471+15.00	RT/LT		1.07	0.22			59.94		
468+47.42 TO 470+11.81	RT/LT					191.48			
470+11.81 TO 471+15.00	RT/LT					11.56			
468+47.42 TO 471+65.00	RT								105.86
468+59.78 TO 471+65.00	RT							101.74	
471+44.97	SIDE RD LT				10.33				
TOTAL		305.50	2.10	0.43	10.33	369.77	117.37	364.74	379.96

RIPRAP SCHEDULE

LOCATION					RIPRAP A4 (SQ YD)	STONE DUMPED RIPRAP A4 (TON)	RIPRAP A5 (SQ YD)	FILTER FABRIC (SQ YD)
STA	463+58.00	TO	STA	466+81.00	LT	945.12		
STA	465+15.00	TO	STA	466+99.00	RT	711.12		
BRIDGE							1479	1479
STA	468+19.00	TO	STA	471+25.00	LT	1894.90		
STA	468+38.90	TO	STA	471+05.00	RT	469.06		
STA	469+75.00	TO	STA	471+10.00	LT	255.00		255.00
TOTAL					255.00	4020.19	1479.00	1734.00

TEMPORARY PAVEMENT MARKING SCHEDULE

STATION	RT/LT	STOP BARS	PAVEMENT MARKING			WORK ZONE PVMT REMOVAL (SQ FT)	PVMT MRKG REMOVAL (SQ FT)
			LINE 4" PAVEMENT (FT)	LINE 6" (TEMP. BARR.) (FT)	LINE 24" PAVEMENT (FT)		
462+30.00	RT	STOP BARS			12	24.0	
462+30.00 TO 473+48.00		STAGE 1	2236.00			745.3	
462+30.00 TO 473+50.00	RT/LT						746.7
462+30.00 TO 473+50.00	CL						93.3
463+30.00 TO 472+00.00		STAGE 1		870			
463+57.50 TO 471+17.50		STAGE 2		750			
473+48.00	LT	STOP BARS			12.0	24.0	
462+47.00	RT	STOP BARS			12	24.0	
462+47.00 TO 473+50.00		STAGE 2	2206.00			735.3	
473+50.00	LT	STOP BARS			12.0	24.0	
471+44.97	SIDE ROAD LT	STOP BARS			24.0	48.0	
SUB-TOTAL							
TOTAL			4442.0	1620.0	72.0	1624.7	840.0

GUARDRAIL SCHEDULE

STATION	RT/LT	SPBGR (FT)	TBT - T1 (SPECIAL) (EA)	TBT-T6 (EA)	GUARDRAIL MRKS TY-A (EA)	PRISMATIC BAR. REFLEC. (EA)
463+75.25 TO 466+94.00	LT	225.0	1	1	4	
466+94.00 TO 468+09.50	LT					2
468+09.50 TO 471+03.25	LT	200.0	1	1	4	
465+02.25 TO 467+08.50	RT	112.5	1	1	3	
467+08.50 TO 468+26.00	RT					2
468+26.00 TO 470+32.25	RT	112.5	1	1	3	
TOTAL		650.0	4	4	14	4

PAVEMENT MARKING SCHEDULE

STATION	RT/LT	PAVEMENT					BRIDGE		
		4" WHITE LINE (FT)	4" YELLOW LINE (FT)	YELLOW SKIP DASH LINE 4" (FT)	4" DBL YELLOW LINE (FT)	DOUBLE AMBER RSD REFL PMK (EA)	4" WHITE LINE (FT)	4" DBL YELLOW LINE (FT)	DOUBLE AMBER RSD REFL PMK (EA)
462+30.00 TO 466+71.39	RT/LT	882.78				6			
462+30.00 TO 465+00.00	CL		270.00	67.50					
465+00.00 TO 466+71.39	CL				342.78				
466+71.39 TO 468+47.42	RT/LT					352.06	352.06	2	
468+47.42 TO 473+50.00	RT/LT	1005.16							
468+47.42 TO 473+50.00	CL				1005.16	6			
SUB-TOTAL		1887.94	270.00	67.50	1347.94	12	352.06	352.06	2
TOTAL			3573.38			12	704.12		2

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULE OF QUANTITIES FAP ROUTE 304 SECTION 5BR-2 CALHOUN COUNTY
NAME	DATE	
		SCALE: VERT. _____ HORIZ. _____ DATE _____ DRAWN BY _____ CHECKED BY _____

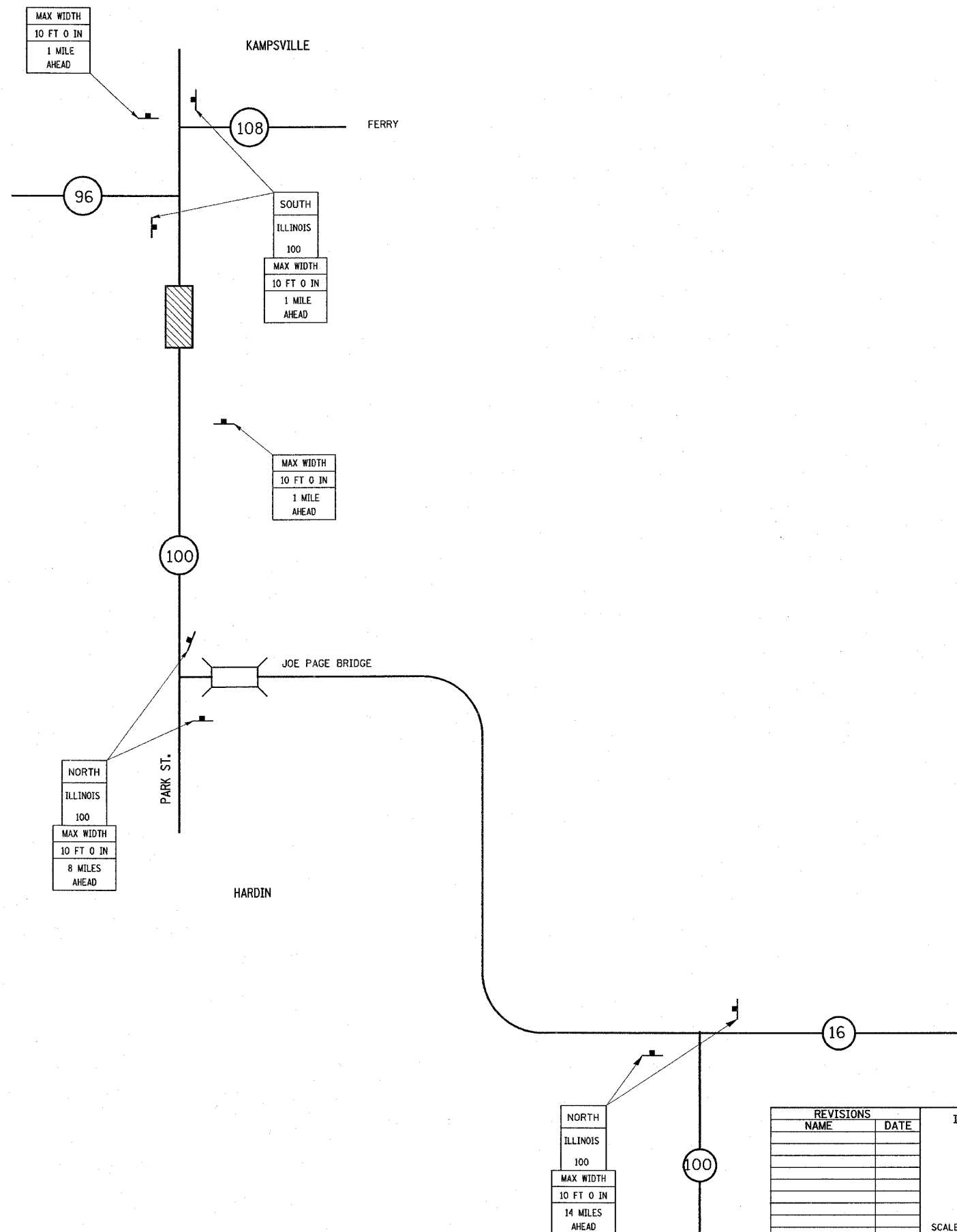
Rev. Sheet 4-3-08

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTES

1. ALL SIGNS REQUIRED WILL BE SUPPLIED TO THE CONTRACTOR BY I.D.O.T.
2. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THIS SHEET, AS DIRECTED BY THE RE/RT. THE POSTS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL GIVE ILLINOIS DEPARTMENT OF TRANSPORTATION, BUREAU OF OPERATIONS TWO WEEKS NOTICE FOR SIGNS. THE CONTRACTOR SHALL PICK UP THE SIGNS AT THE T.M. BUILDING IN FAIRVIEW HEIGHTS, AND RETURN THEM UPON COMPLETION OF THE CONTRACT. CONTACT JEAN SLAPE @ (618) 346-3289.
4. THE ABOVE NOTED WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE, LUMP SUM, FOR WIDE LOAD SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. SIGN SPACING WILL BE 400' OR TO FIT FIELD CONDITIONS.
6. THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT BE LESS THAN 6'.



SIGNS REQUIRED			
MAX WIDTH 10 FT 0 IN 1 MILE AHEAD	(4)	NORTH	(4)
MAX WIDTH 10 FT 0 IN 8 MILES AHEAD	(2)	SOUTH	(2)
MAX WIDTH 10 FT 0 IN 14 MILES AHEAD	(2)	ILLINOIS 100	(6)

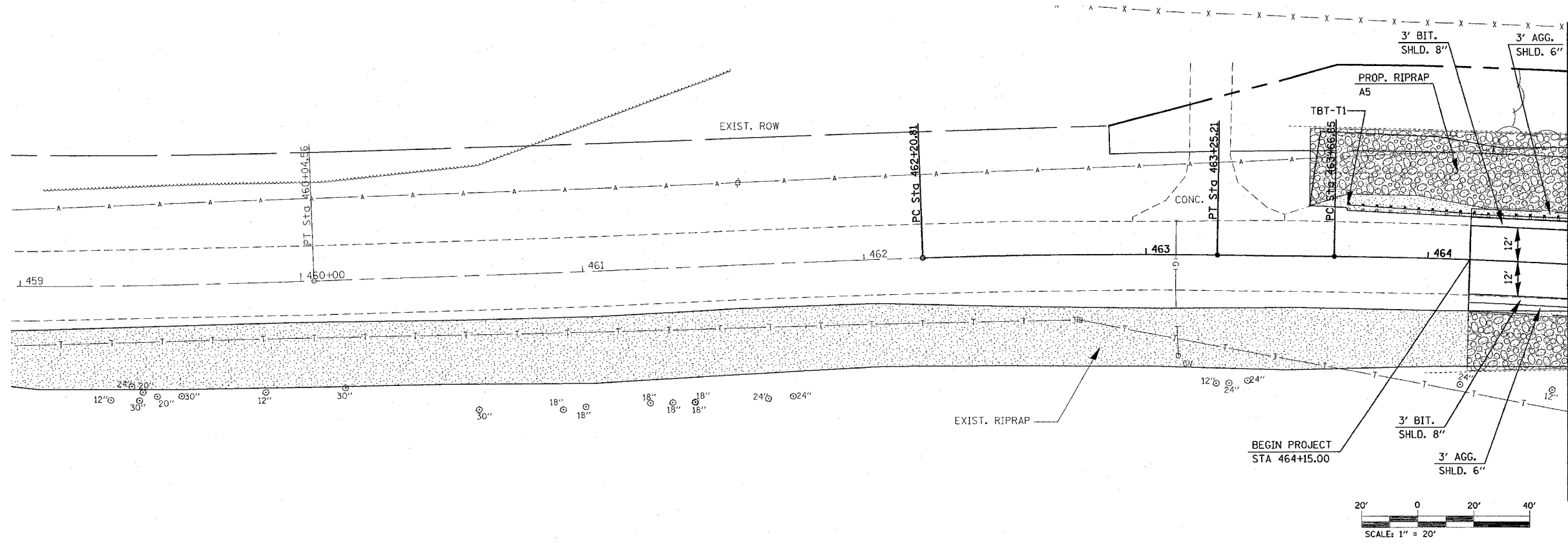
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
WIDE LOAD SIGNING
 FAP ROUTE 304
 SECTION 5BR-2
 CALHOUN COUNTY

SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
 DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	10
STA. 459+00.00 TO STA. 464+50.00				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



MATCHLINE STA 464+50.00

PLAN

DATE	BY

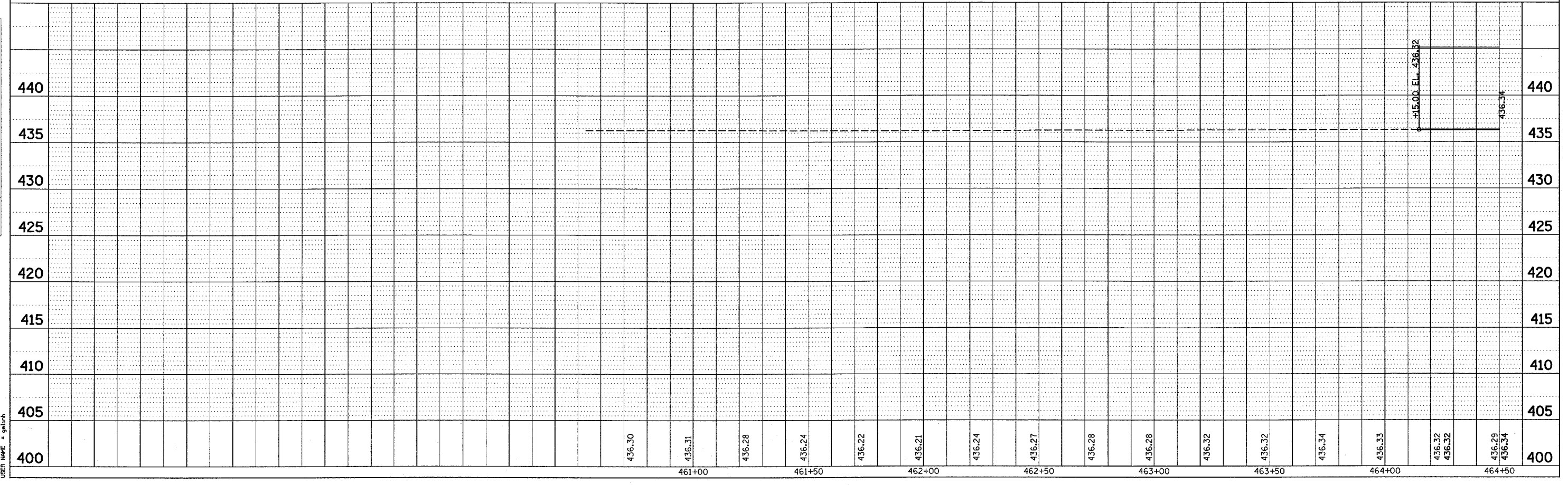
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 ALIGNED BY: _____
 CHECKED BY: _____
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PROFILE

DATE	BY

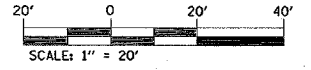
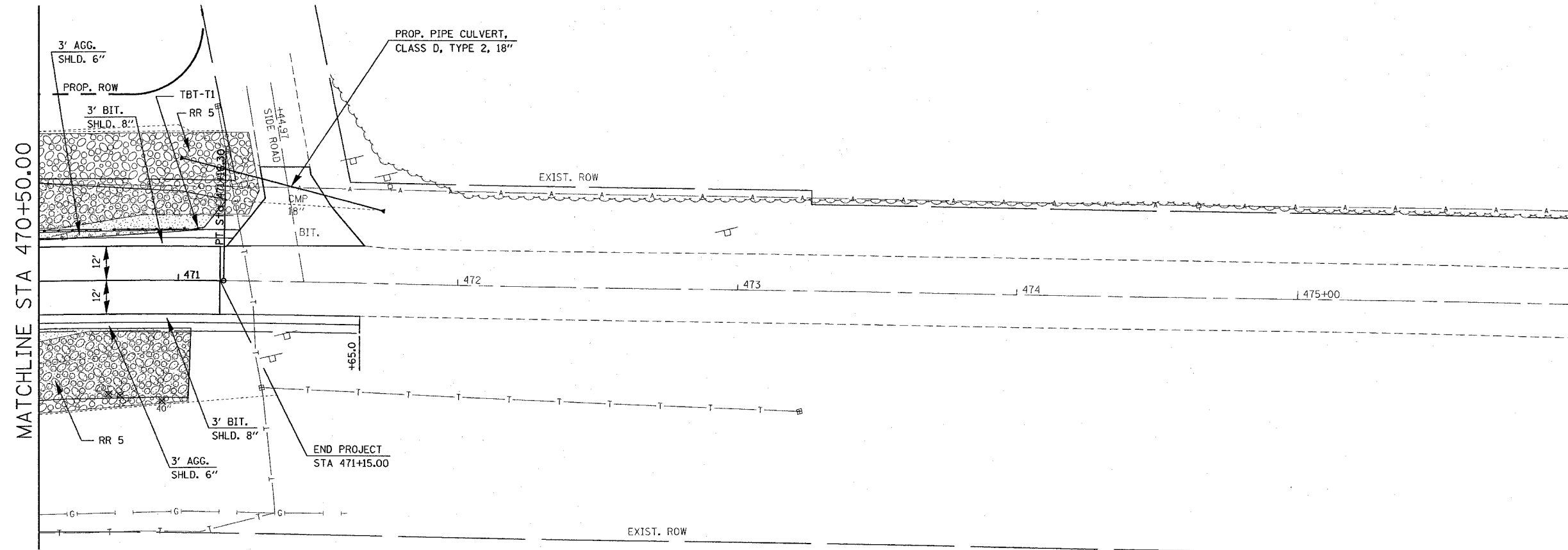
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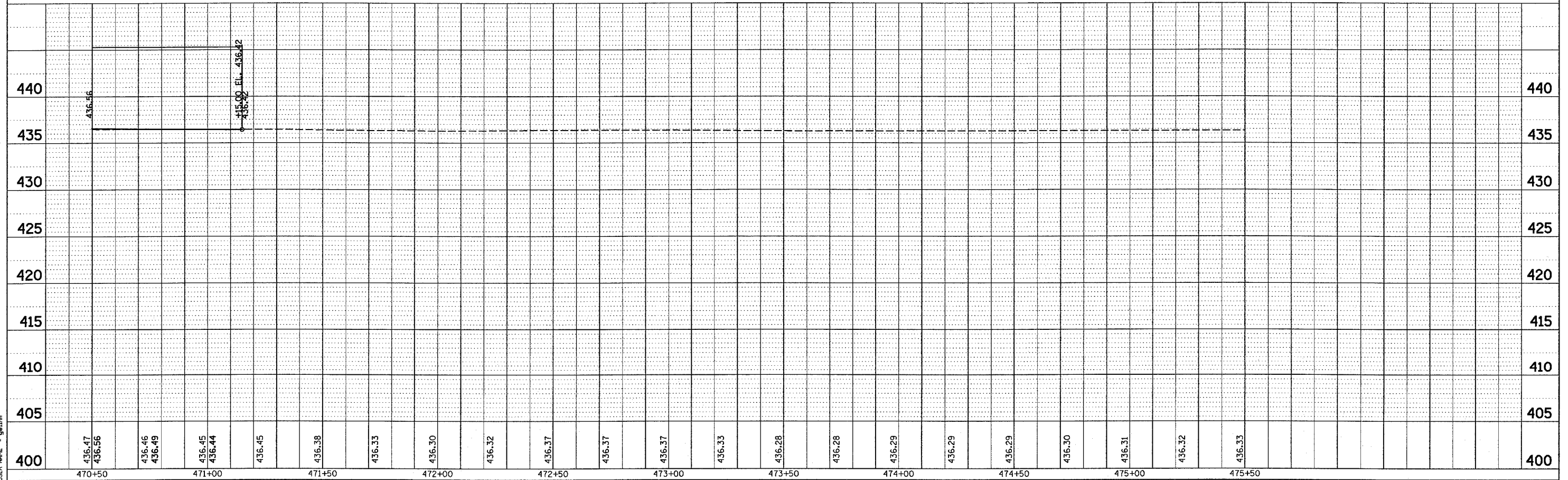


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	12
STA. 470+50.00		TO STA. 475+00.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PLAN	REVISION	DATE
NO.	BY	
NO.	BY	
NO.	BY	
NO.	BY	



PROFILE	REVISION	DATE
NO.	BY	
NO.	BY	
NO.	BY	



PLOT DATE = 1/26/2006
 PLOT SCALE = 20.0000 / IN.
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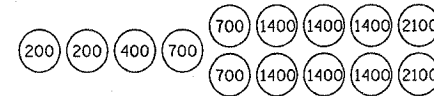
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	13
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

PRE-STAGE I CONSTRUCTION:

- PRE-STAGE I CONSTRUCTION SHALL CONSIST OF THE RELOCATION OF THE EXISTING GUARD RAIL, CONSTRUCTION OF THE 4 1/2' PAVEMENT WIDENING ON THE NORTHEAST AND NORTHWEST CORNERS OF THE STRUCTURE, AND PLACEMENT OF THE W33X118 STEEL BEAM UNDER BEAM 3 OF THE EXISTING STRUCTURE AS SHOWN ON PAGE 24A OF THE PLANS. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, STANDARD 701325.

STAGE I CONSTRUCTION:

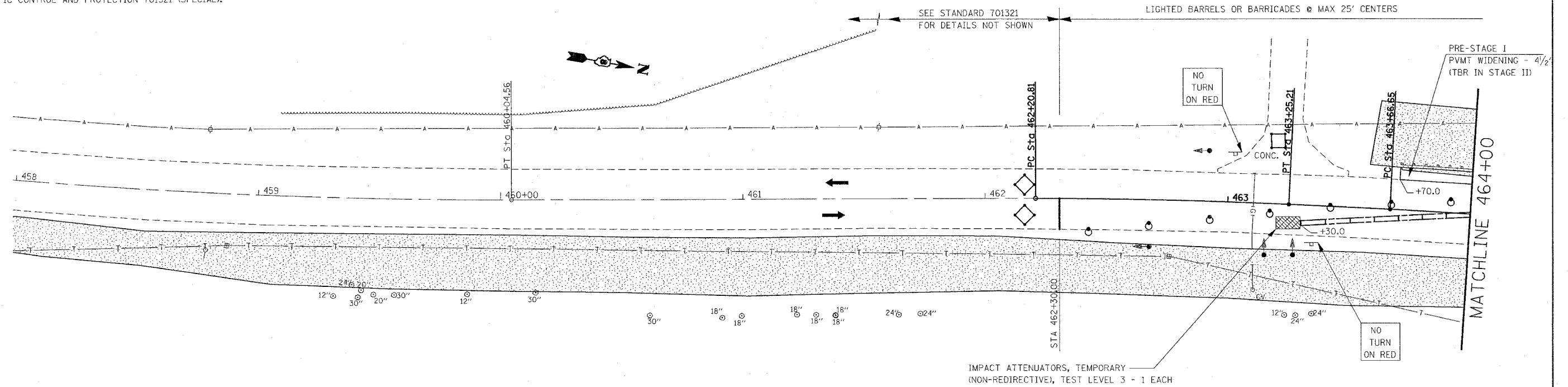
- STAGE I CONSTRUCTION SHALL CONSIST OF STAGE I REMOVAL OF THE EXISTING STRUCTURE, AND STAGE I CONSTRUCTION OF THE REPLACEMENT STRUCTURE, PAVEMENT REMOVAL, GRADING, RESURFACING THE RIGHT SIDE PAVEMENT NORTH AND SOUTH OF THE STRUCTURE, GUARDRAIL, RIPRAP, ETC. STAGE I CONSTRUCTION SHALL BE DONE ACCORDING TO STAGE CONSTRUCTION AS DETAILED IN THE BRIDGE PLANS. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STANDARD 701321 AND AS DETAILED IN THE STAGE CONSTRUCTION PLANS. THIS TRAFFIC CONTROL SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION 701321 (SPECIAL).



SAND MODULE IMPACT ATTENUATOR LAYOUT (IF OPTION USED)

NOTES:

- THE CONTRACTOR SHALL MAINTAIN ACCESS TO PRIVATE AND FIELD ENTRANCES LOCATED WITHIN THE LIMITS OF THE PROJECT.
- TRAFFIC CONTROL & PROTECTION, STANDARD 701321 (SPECIAL) INCLUDES BOTH STAGE I & II AND ANY ADDITIONAL SIGNING OR TRAFFIC CONTROL DEVICES SHOWN ON THE STAGE CONSTRUCTION PLANS.
- ALL ADDITIONAL TRAFFIC SIGNAL HEADS, LOOP DETECTORS AND ASSOCIATED EQUIPMENT REQUIRED TO MAINTAIN ACCESS AT THE FIELD AND DRIVEWAY ENTRANCES SHALL BE INCLUDED IN THE COST OF "TEMPORARY BRIDGE TRAFFIC SIGNALS"
- THE COST OF "BARRICADES, TYPE III" SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".
- ALL SIDEROADS AND ENTRANCES WITHIN TRAFFIC CONTROL SHALL HAVE "NO RIGHT TURN ON RED" AND "STOP HERE ON RED" SIGNS. THE COST SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".



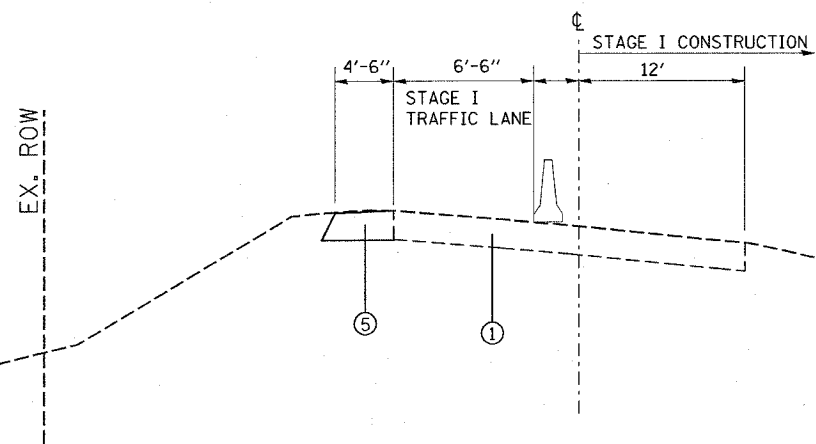
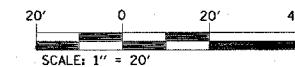
IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 - 1 EACH

LEGEND:

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE

LEGEND

- ① EXISTING PAVEMENT
- ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"



STAGE I TYPICAL SECTION
STA. 463+00.0 TO STA. 464+00.0
(NTS)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED STAGE I CONSTRUCTION

FAP ROUTE 304
SECTION 5BR-2
CALHOUN COUNTY

SCALE: VERT. DATE HORIZ. DRAWN BY CHECKED BY

PLOT DATE = 5/2/2008
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REFERENCE = SHEET # 13

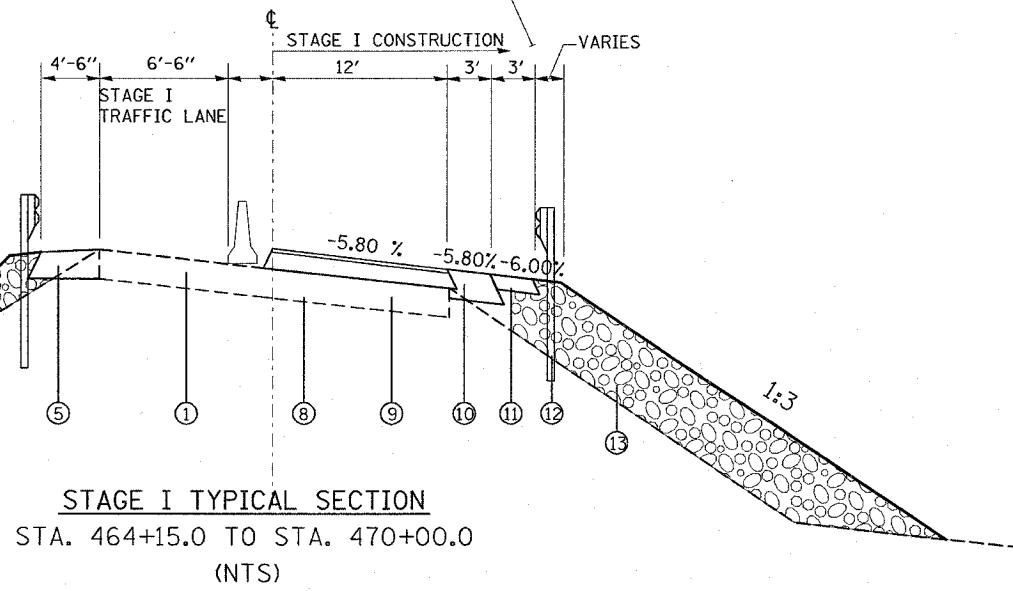
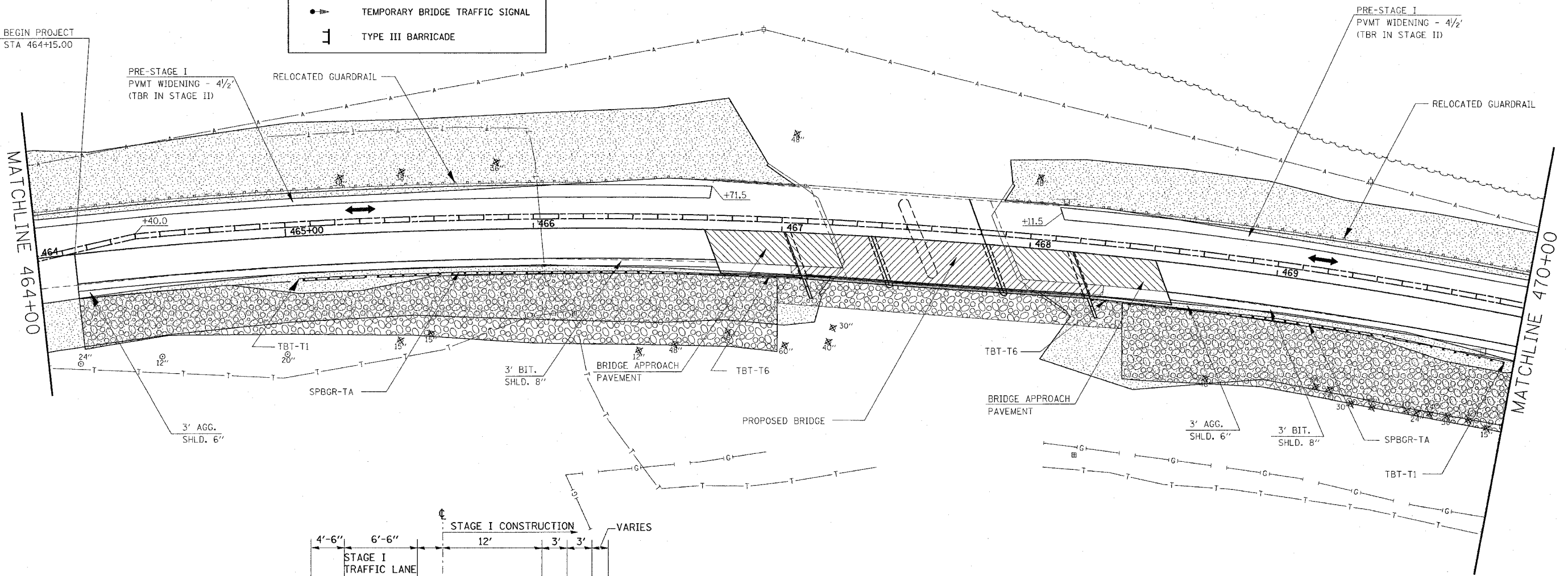
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	14
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

LEGEND:

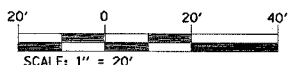
- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



BEGIN PROJECT
STA 464+15.00



- LEGEND**
- ① EXISTING PAVEMENT
 - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
 - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
 - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
 - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑪ PROPOSED AGGREGATE SHOULDER 6"
 - ⑫ PROPOSED GUARDRAIL
 - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE










ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED STAGE I CONSTRUCTION
 FAP ROUTE 304
 SECTION 5BR-2
 CALHOUN COUNTY

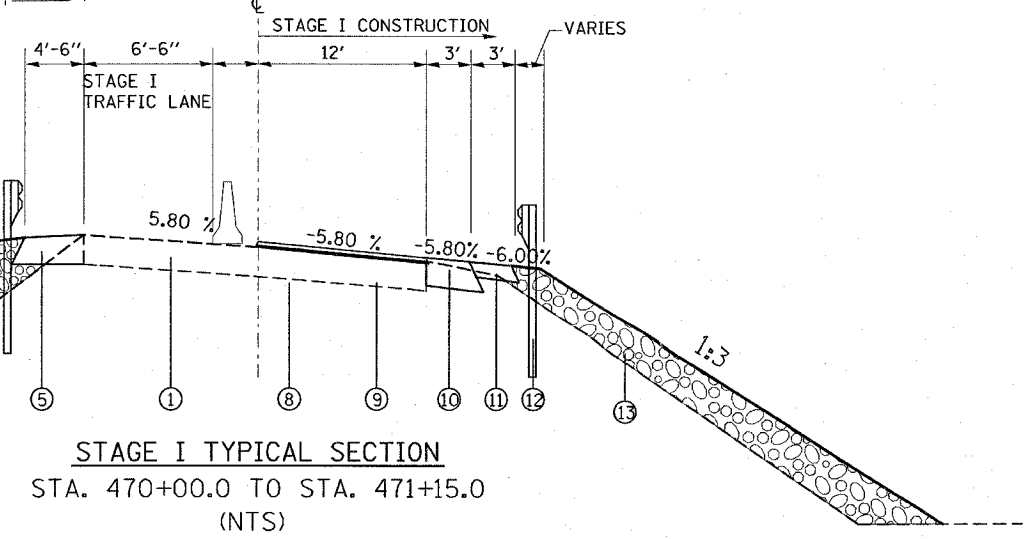
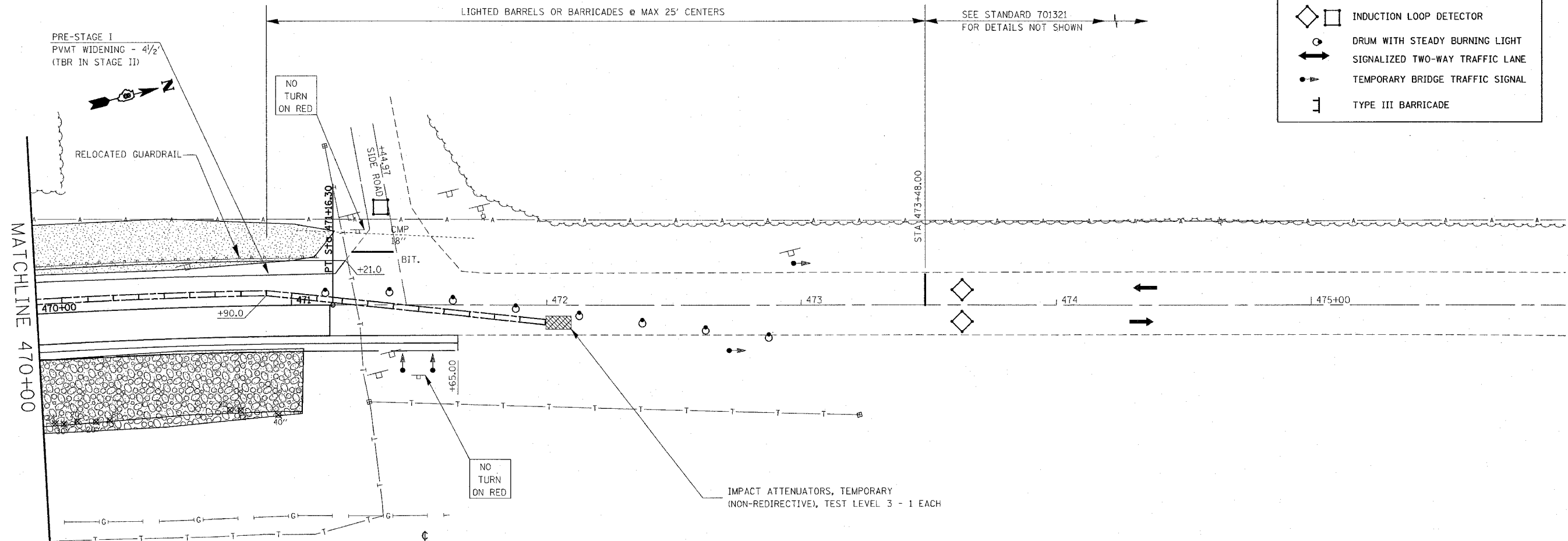
SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

PLOT DATE = 1/30/2008
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 REFERENCE = \$REF\$

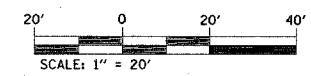
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	15
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

LEGEND:

-  STRUCTURE REMOVAL
-  PAVEMENT REMOVAL
-  TEMPORARY CONCRETE BARRIER
-  IMPACT ATTENUATOR
-  INDUCTION LOOP DETECTOR
-  DRUM WITH STEADY BURNING LIGHT
-  SIGNALIZED TWO-WAY TRAFFIC LANE
-  TEMPORARY BRIDGE TRAFFIC SIGNAL
-  TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
 - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
 - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
 - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
 - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑪ PROPOSED AGGREGATE SHOULDER 6"
 - ⑫ PROPOSED GUARDRAIL
 - ⑬ PROPOSED RIPRAP



REVISIONS		NAME	DATE
NO.	DESCRIPTION		

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED STAGE I CONSTRUCTION

FAP ROUTE 304
SECTION 5BR-2
CALHOUN COUNTY

SCALE: VERT. _____
HORIZ. _____

DATE _____

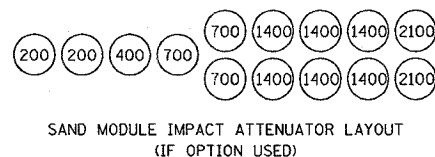
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CHECKED BY _____

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	16
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

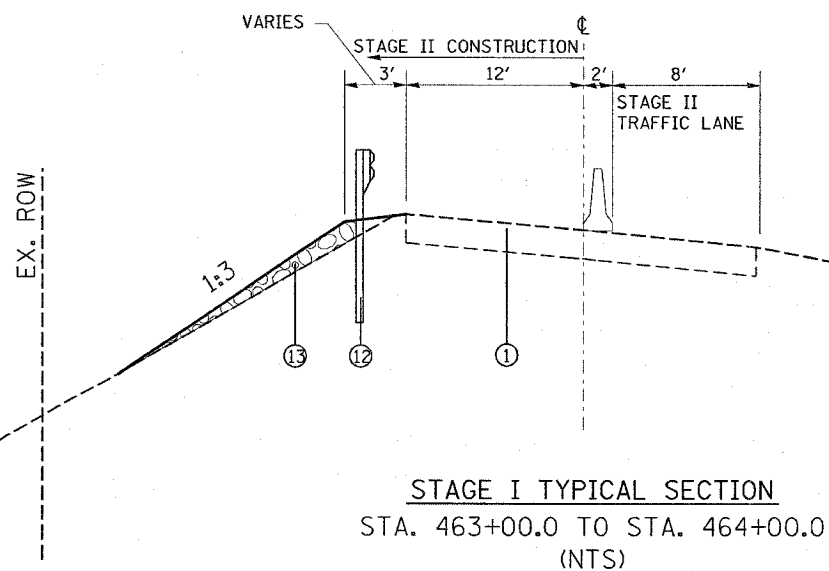
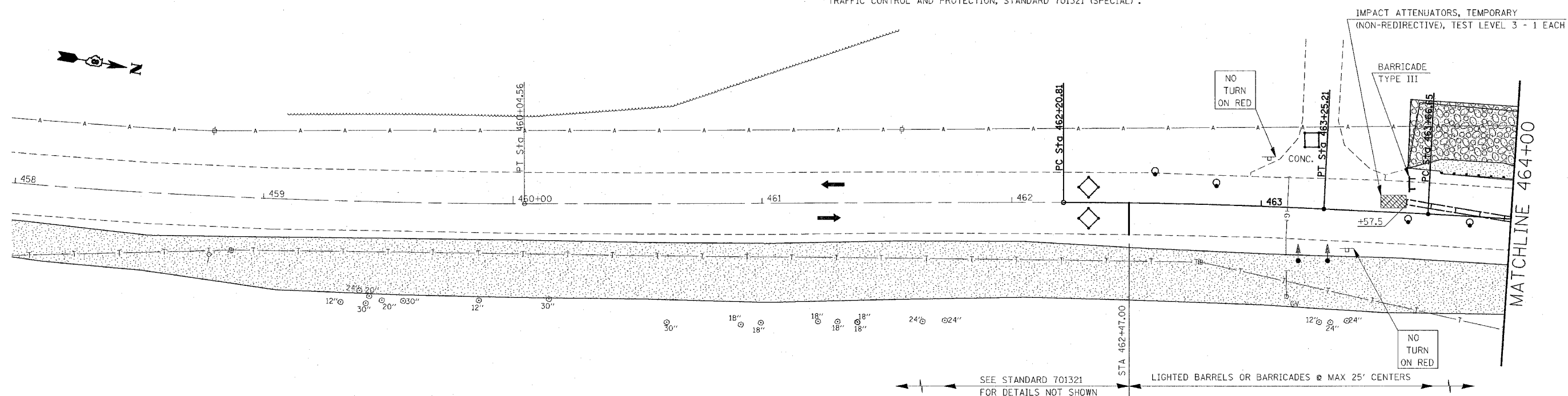
STAGE II CONSTRUCTION:

- STAGE II CONSTRUCTION SHALL CONSIST OF STAGE II REMOVAL OF THE EXISTING STRUCTURE, AND STAGE II CONSTRUCTION OF THE REPLACEMENT STRUCTURE, REMOVE PRE-STAGE I PAVEMENT WIDENING, PAVEMENT REMOVAL, GRADING, RESURFACING THE LEFT SIDE PAVEMENT NORTH AND SOUTH OF THE STRUCTURE, GUARDRAIL, RIPRAP, ETC. STAGE I CONSTRUCTION SHALL BE DONE ACCORDING TO STAGE CONSTRUCTION AS DETAILED IN THE BRIDGE PLANS. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STANDARD 701321 AND AS DETAILED IN THE STAGE CONSTRUCTION PLANS. THIS TRAFFIC CONTROL SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION 701321 (SPECIAL).



NOTES:

- THE CONTRACTOR SHALL MAINTAIN ACCESS TO PRIVATE AND FIELD ENTRANCES LOCATED WITHIN THE LIMITS OF THE PROJECT.
- TRAFFIC CONTROL & PROTECTION, STANDARD 701321 (SPECIAL) INCLUDES BOTH STAGE I & II AND ANY ADDITIONAL SIGNING OR TRAFFIC CONTROL DEVICES SHOWN ON THE STAGE CONSTRUCTION PLANS.
- ALL ADDITIONAL TRAFFIC SIGNAL HEADS, LOOP DETECTORS AND ASSOCIATED EQUIPMENT REQUIRED TO MAINTAIN ACCESS AT THE FIELD AND DRIVEWAY ENTRANCES SHALL BE INCLUDED IN THE COST OF "TEMPORARY BRIDGE TRAFFIC SIGNALS"
- THE COST OF "BARRICADES, TYPE III" SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".
- ALL SIDEROADS AND ENTRANCES WITHIN TRAFFIC CONTROL SHALL HAVE "NO RIGHT TURN ON RED" AND "STOP HERE ON RED" SIGNS. THE COST SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".



LEGEND

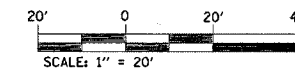
- ① EXISTING PAVEMENT
- ② PROPOSED GUARDRAIL
- ③ PROPOSED RIPRAP

LEGEND:

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED STAGE II CONSTRUCTION
 FAP ROUTE 304
 SECTION 5BR-2
 CALHOUN COUNTY



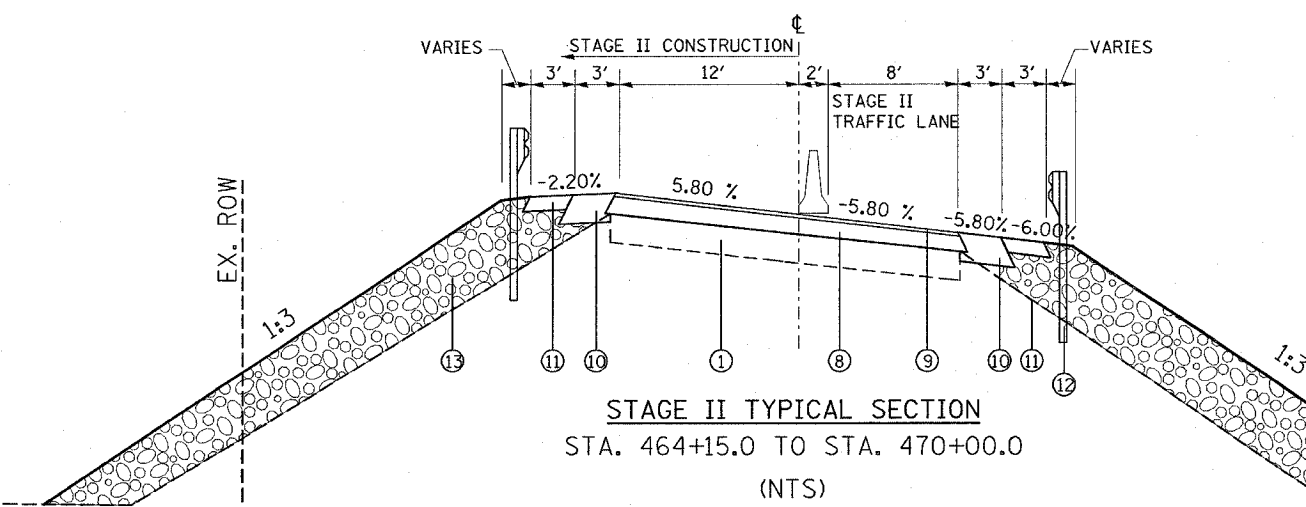
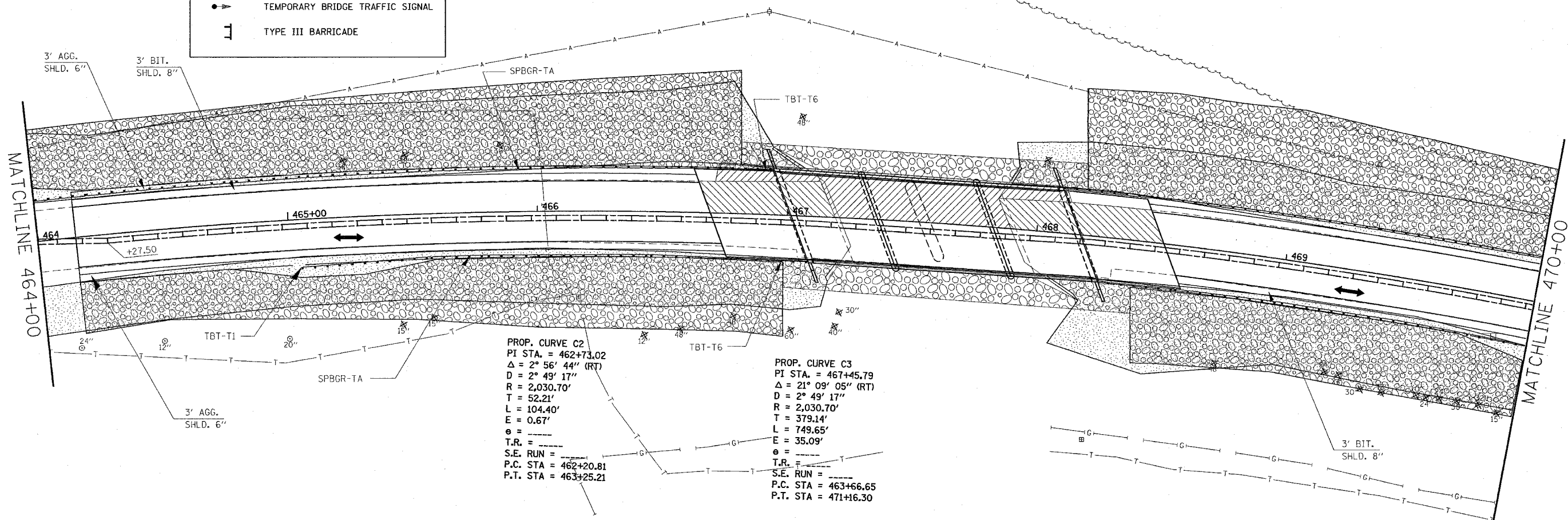
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 DATE
 DRAWN BY
 CHECKED BY

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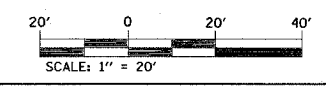
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	17
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

LEGEND:

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
 - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
 - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
 - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
 - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑪ PROPOSED AGGREGATE SHOULDER 6"
 - ⑫ PROPOSED GUARDRAIL
 - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUGGESTED STAGE II CONSTRUCTION
 FAP ROUTE 304
 SECTION 5BR-2
 CALHOUN COUNTY

SCALE: VERT. _____
 HORIZ. _____
 DATE _____

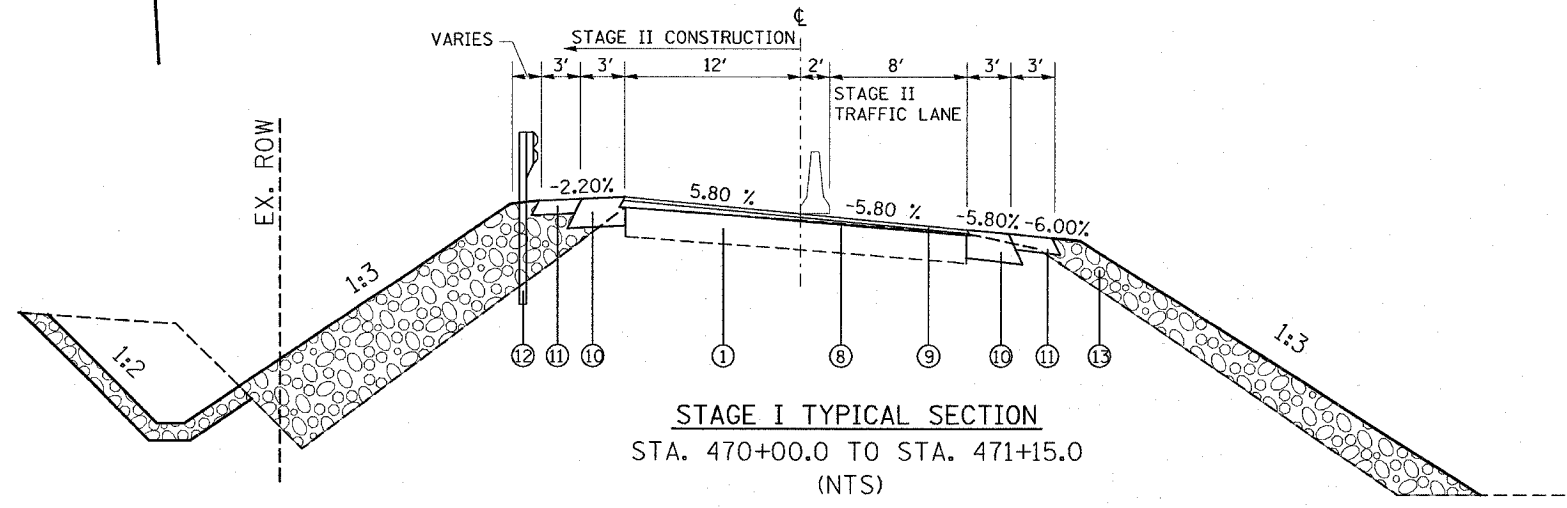
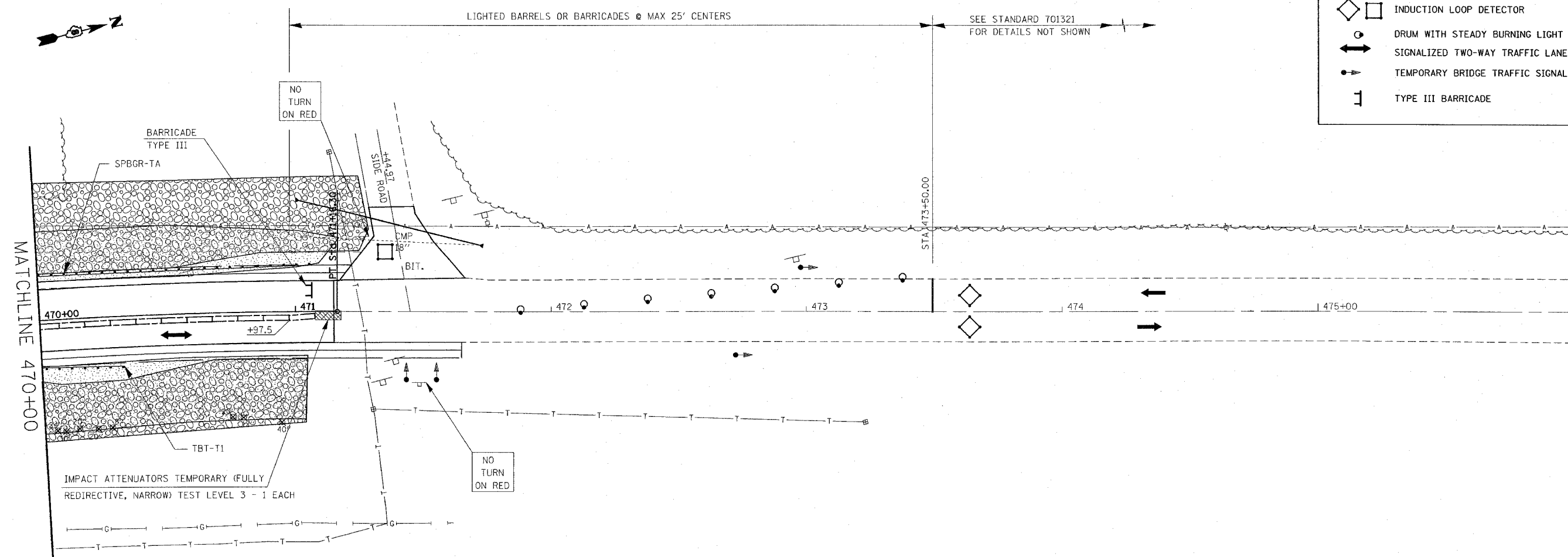
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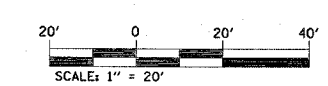
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	18
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		

LEGEND:

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
 - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
 - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
 - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
 - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
 - ⑪ PROPOSED AGGREGATE SHOULDER 6"
 - ⑫ PROPOSED GUARDRAIL
 - ⑬ PROPOSED RIPRAP



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SUGGESTED STAGE II CONSTRUCTION FAP ROUTE 304 SECTION 5BR-2 CALHOUN COUNTY
NAME	DATE	
		SCALE: VERT. HORIZ. DATE

DRAWN BY _____
CHECKED BY _____

PLOT DATE = 1/26/2006
 PLOT SCALE = 20.0000 / IN.
 REFERENCE = #REF#

PART OF THE S.W. 1/4 OF SECTION 11 T. 9 S., R. 2 W., OF THE 4RD PM, CALHOUN COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

STATION	OFFSET	NORTH	EAST
STA. 460+17.19 (PT STA. 461+20.00)	0.23' LT (EXIST. C. ROW)	951713.1924	171346.6443
STA. 462+87.38 (PC STA. 463+90.20)	0.41' LT (EXIST. C. ROW)	951979.3722	171300.2287
STA. 466+90.56 (PI STA. 467+99.80)	46.09' LT (N/A)		
STA. 470+93.70 (PT STA. 471+97.48)	0.67' LT (EXIST. C. ROW)	952780.2911	171329.1183

STATION	OFFSET	NORTH	EAST
STA. 458+69.65	128.72' RT	951582.8370	171496.7814
STA. 458+70.43	28.72' RT	951571.6212	171397.4121
STA. 460+16.75	129.77' RT	951735.5244	171474.7118
STA. 460+17.34	45.23' LT	951705.4621	171302.3133
STA. 462+86.09	45.39' LT	951971.6419	171255.8976
STA. 462+91.47	129.53' RT	952001.7042	171428.2961
STA. 463+66.65	67.24' LT	952049.6787	171223.7128
STA. 465+03.42	67.97' LT	952190.3285	171210.1602
STA. 467+44.53	68.10' LT	952439.3686	171210.5626
STA. 467+73.04	68.01' LT	952468.7590	171212.6621
STA. 467+92.64	87.06' RT	952475.7552	171368.7955
STA. 468+12.17	127.14' RT	952490.5822	171410.3746
STA. 470+83.62	65.79' LT	952785.2310	171263.3874
STA. 470+92.63	89.32' RT	952758.4828	171416.4361
STA. 471+07.27	95.87' LT	952816.1327	171239.8338
STA. 482+55.11	30.00' LT		

STATION	OFFSET	NORTH	EAST
PC STA. 455+00.00	CONST. C.		
PI STA. 457+53.20	N/A		
PT STA. 460+04.56	CONST. C.		
PC STA. 462+20.81	CONST. C.	951913.7095	171311.2132
PI STA. 462+73.02	N/A		
PT STA. 463+25.21	CONST. C.	952016.9224	171295.5825
PC STA. 463+66.65	CONST. C.	952059.0350	171290.4335
PI STA. 467+45.79	N/A		
PT STA. 471+16.30	CONST. C.	952802.0976	171335.1211
POT STA. 480+00.00	CONST. C.	953659.5010	171549.0970

CORNER DESCRIPTION	NORTH	EAST
N.W. COR. E. 1/2 S.W. 1/4 SEC. 11	953807.7731	170864.6536

PROP. CURVE C1
PI STA. = 457+53.20
Δ = 11° 55' 28" (LT)
D = 2° 21' 48"
R = 2,424.37'
T = 253.20'
L = 504.56'
E = 13.19'
P.C. STA. = 455+00.00
P.T. STA. = 460+04.56

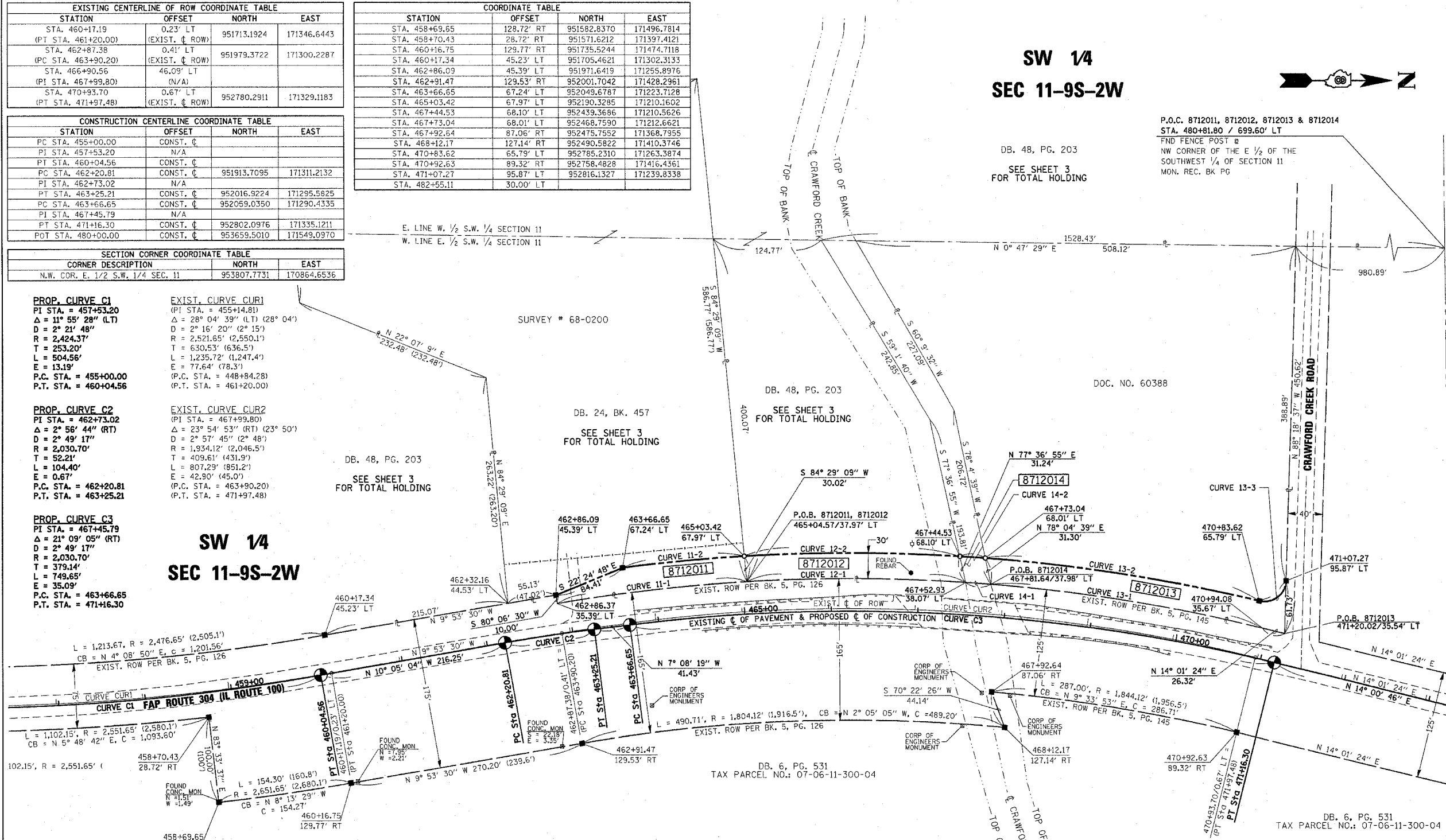
EXIST. CURVE CUR1
PI STA. = 455+14.81
Δ = 28° 04' 39" (LT) (28° 04')
D = 2° 16' 20" (2° 15')
R = 2,521.65' (2,550.1')
T = 630.53' (636.5')
L = 1,235.72' (1,247.4')
E = 77.64' (78.3')
(P.C. STA. = 448+84.28)
(P.T. STA. = 461+20.00)

PROP. CURVE C2
PI STA. = 462+73.02
Δ = 2° 56' 44" (RT)
D = 2° 49' 17"
R = 2,030.70'
T = 52.21'
L = 104.40'
E = 0.67'
P.C. STA. = 462+20.81
P.T. STA. = 463+25.21

EXIST. CURVE CUR2
PI STA. = 467+99.80
Δ = 23° 54' 53" (RT) (23° 50')
D = 2° 57' 45" (2° 48')
R = 1,934.12' (2,046.5')
T = 409.61' (431.9')
L = 807.29' (851.2')
E = 42.90' (45.0')
(P.C. STA. = 463+90.20)
(P.T. STA. = 471+97.48)

PROP. CURVE C3
PI STA. = 467+45.79
Δ = 21° 09' 05" (RT)
D = 2° 49' 17"
R = 2,030.70'
T = 379.14'
L = 749.65'
E = 35.09'
P.C. STA. = 463+66.65
P.T. STA. = 471+16.30

**SW 1/4
SEC 11-9S-2W**



FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	19

LEGEND

- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SAME OWNERSHIP
- EXISTING BUILDING

STAKING OF PROPOSED RIGHT OF WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

STATE OF KENTUCKY)
COUNTY OF McCRACKEN)

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED _____

RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008

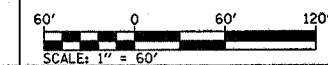


500 South 17th Street
Paducah, Kentucky 42003
Phone: (270) 443-1896
Fax: (270) 443-1904

401 North Market
Marion, IL 62959
Phone: (618) 997-9190

REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 304 (IL ROUTE 100)
SECTION 5BR-2
CALHOUN COUNTY
JOB NO. R-98-012-07
STA. 457+00 TO STA. 473+00



SHEET 2 OF 4

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINSVILLE, ILLINOIS 62234-6198

SHEET 1 IS A COVER SHEET

PARCEL NO.	OWNER	TOTAL HOLDING ACRES	FEE SIMPLE ACQUISITION				REMAINDER ACRES	PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY		
			GROSS ACRES	SQ. FT.	PREVIOUSLY DEDICATED ACRES	NET SQ. FT.					
8712011	VILLAGE OF KAMPSVILLE BOOK 24, PAGE 457 TITLE REPORT NUMBER CN-227	4.9970	0.1339	5,831	0	0	0.1339	5,831	4.8631	07-06-11-300-002-A	
8712012	SANDRA A. CLENNENY, A MARRIED PERSON BOOK 48, PAGE 203 TITLE REPORT NUMBER CN-226	72.2148	0.1729	7,533	0	0	0.1729	7,533	72.0418	07-06-11-300-002	
8712013	DONNA JANE RICHTER AS TRUSTEE OF THE DONNA JANE RICHTER DECLARATION OF TRUST DATED NOVEMBER 1, 2006 DOCUMENT NO. 60388 TITLE REPORT NUMBER CN-225	3.8727	0.2447	10,661	0	0	0.2447	10,661	3.6280	07-06-11-300-006	
8712014	THE ESTATE OF EMIEL HEIDENREICH, DECEASED TITLE REPORT NUMBER CN-330	0.3046	0.0202	882	0	0	0.0202	882	0.2844	NO PIN ASSIGNED	

CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
11-1	1,969.12' (2,081.5')	221.46'	N 6° 40' 10" W	221.35'
11-2	1,999.12'	141.33'	S 5° 30' 14" E	141.30'
12-1	1,969.12' (2,081.5')	253.03'	S 0° 14' 01" W	252.86'
12-2	1,999.12'	249.20'	N 0° 05' 33" E	249.04'
13-1	1,969.12' (2,081.5')	318.16'	S 9° 23' 40" W	317.81'
13-2	1,999.12'	320.86'	N 9° 06' 22" E	320.51'
13-3	25.00'	44.50'	N 37° 18' 54" W	38.85'
14-1	1,969.12' (2,081.5')	29.24'	N 4° 20' 25" E	29.24'
14-2	1,999.12'	29.47'	S 4° 05' 09" W	29.47'

COMPLETION DATE OF FIELD WORK PERFORMED
LAND SURVEY: 5/23/07
RIGHT OF WAY STAKING:

8/24/2008
 12/30/2008
 1/20/2009
 2/10/2009
 3/10/2009
 4/10/2009
 5/10/2009
 6/10/2009
 7/10/2009
 8/10/2009
 9/10/2009
 10/10/2009
 11/10/2009
 12/10/2009

PART OF THE S.W. 1/4 OF SECTION 11 T. 9 S., R. 2 W., OF THE 4RD PM, CALHOUN COUNTY, ILLINOIS

JOB NAME	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	20
STA. 455+00		TO STA. 480+00		
CONTRACT NO.:				

LEGEND

- SECTION CORNERS
- QUARTER SECTION CORNERS
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
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- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667(1) (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET OUT CROSS
- SAME OWNERSHIP
- EXISTING BUILDING

STAKING OF PROPOSED RIGHT OF WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

STATE OF KENTUCKY)
COUNTY OF MCCRACKEN)

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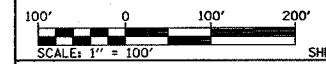
DATED _____

RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008

GEOTECH
ENGINEERING & TESTING, INC.

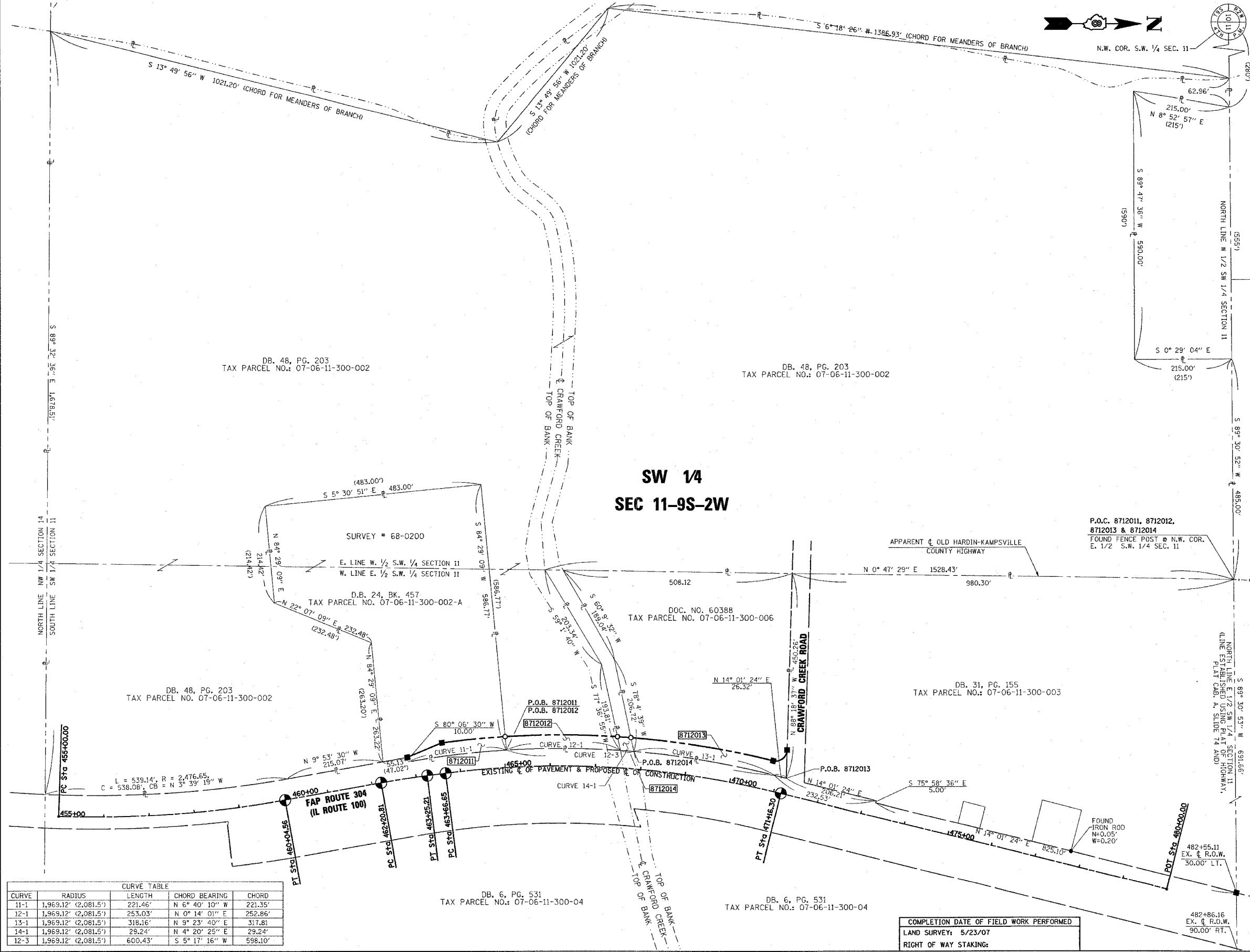
500 South 17th Street Paducah, Kentucky 42003
Phone: (270) 443-1995 Fax: (270) 443-1904
401 North Market Macon, IL 62559
Phone: (618) 997-9190
REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 304 (IL ROUTE 100)
SECTION 5BR-2
CALHOUN COUNTY
JOB NO. R-98-012-07
STA. 455+00 TO STA. 480+00



ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-6198
SHEET 1 IS A COVER SHEET

NO.	DATE	DESCRIPTION	BY

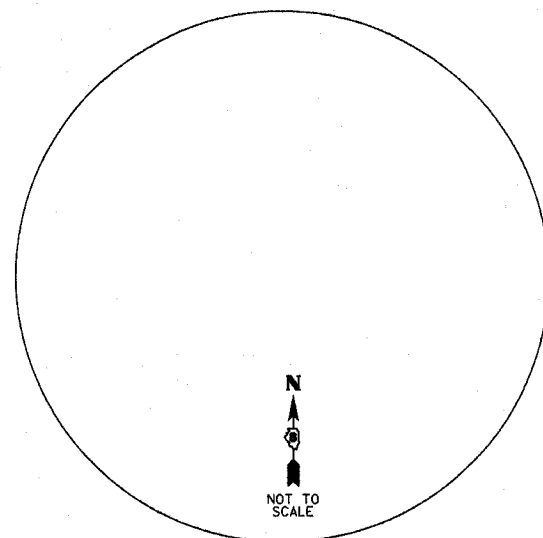


CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
11-1	1,969.12' (2,081.5')	221.46'	N 6° 40' 10" W	221.35'
12-1	1,969.12' (2,081.5')	253.03'	N 0° 14' 01" E	252.86'
13-1	1,969.12' (2,081.5')	318.16'	N 9° 23' 40" E	317.81'
14-1	1,969.12' (2,081.5')	29.24'	N 4° 20' 25" E	29.24'
12-3	1,969.12' (2,081.5')	600.43'	S 5° 17' 16" W	598.10'

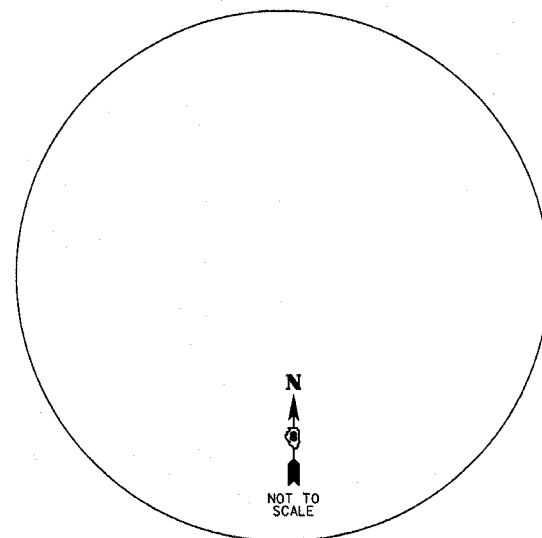
DATE: 5/23/07
DRAWN BY: R. BACON
CHECKED BY: R. BACON
DATE: 5/23/07

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	20A
STA. N/A		TO STA. N/A		
CONTRACT NO.:				

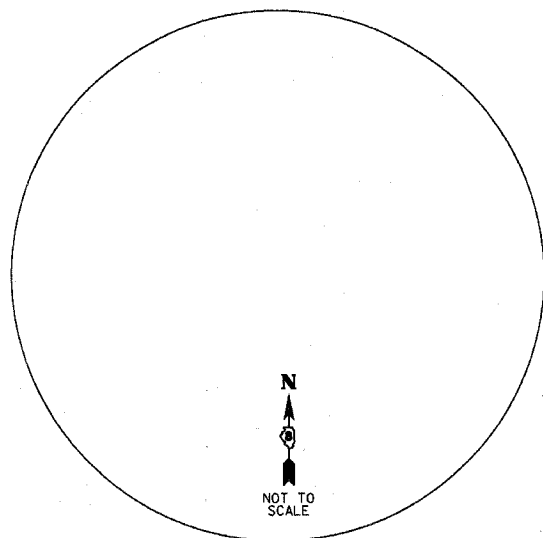
PROPOSED CENTERLINE OF CONSTRUCTION CONTROL TIES



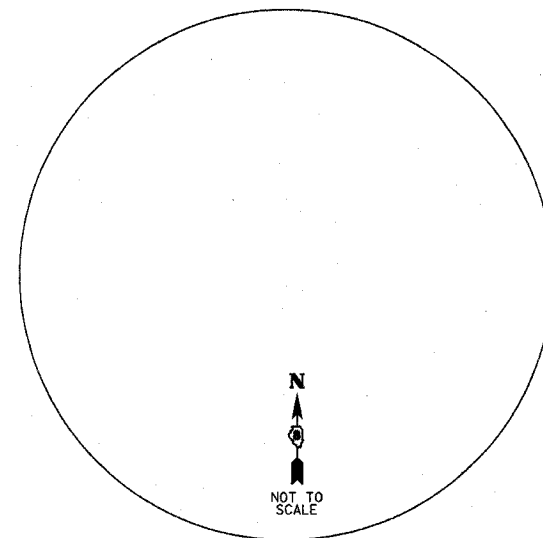
P.T. STA. 460+04.56 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



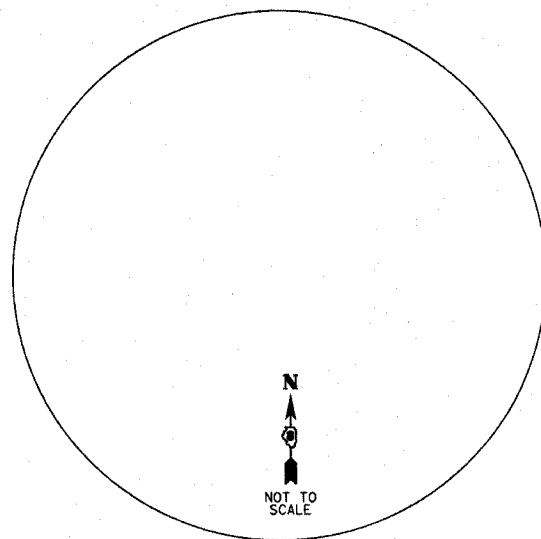
P.C. STA. 462+20.81 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



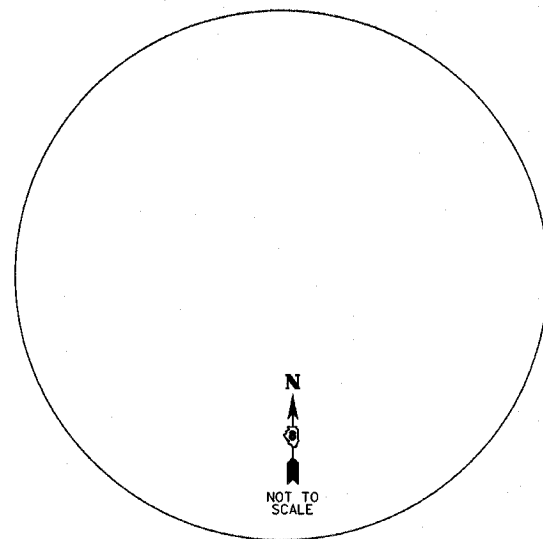
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FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



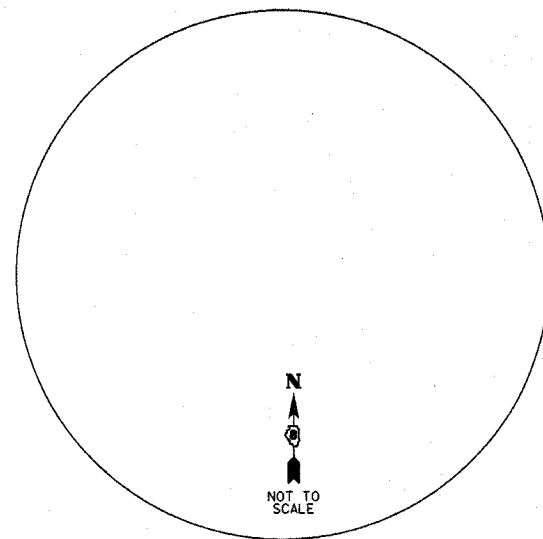
P.T. STA. 463+25.21 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



P.C. STA. 463+66.65 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



P.I. STA. 467+45.79 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL



P.C. STA. 471+16.30 PROPOSED ϕ OF CONSTRUCTION
FAP ROUTE 304 / IL RT. 100
SET MAGNAIL

LEGEND

- SECTION CORNERS
- QUARTER SECTION CORNERS
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
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- MEASURED DIMENSION
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- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667(1) (TO BE SET BY OTHERS)
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STATE OF KENTUCKY)
) SS
COUNTY OF McCRACKEN)

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DATED _____

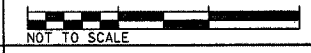
RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008

GEOTECH
ENGINEERING & TESTING, INC.

500 South 17th Street Paducah, Kentucky 42003
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REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 304 (IL ROUTE 100)
SECTION 5BR-2
CALHOUN COUNTY
JOB NO. R-98-012-07
TIE SHEET



NOT TO SCALE SHEET 4 OF 4

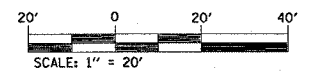
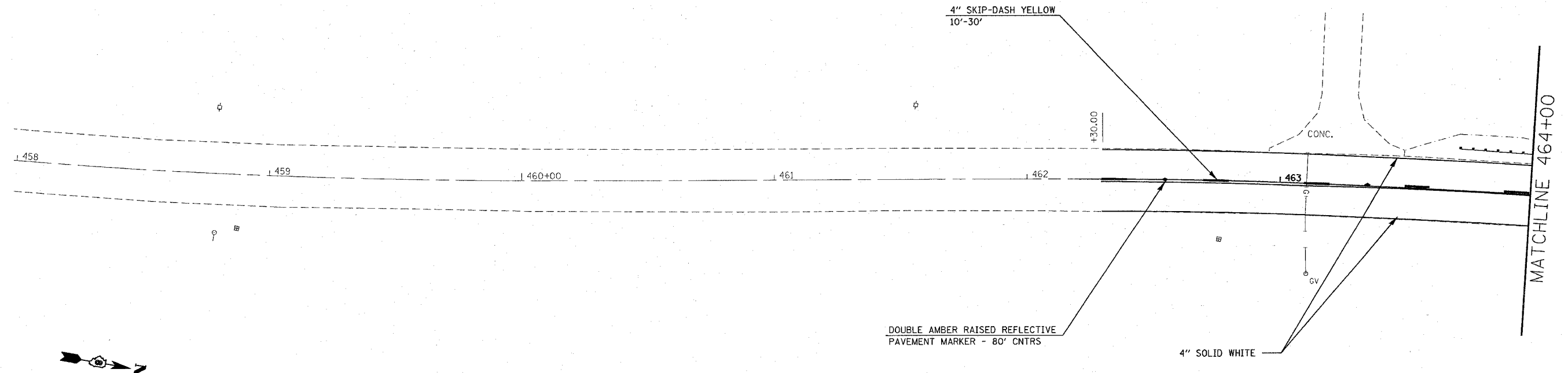
COMPLETION DATE OF FIELD WORK PERFORMED
LAND SURVEY: 5/23/07
RIGHT OF WAY STAKING:

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-6198
SHEET 1 IS A COVER SHEET

NO.	DATE	DESCRIPTION	BY

\$DATE\$ 1/30/2008
 C:\Users\ronaldb\Documents\GIS\180718-07\Subm\180718-07.dgn
 REF- REF- REF- REF-

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	21
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO. _		ILLINOIS	FED. AID PROJECT	



REVISIONS	
NAME	DATE

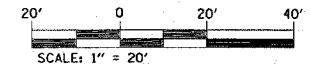
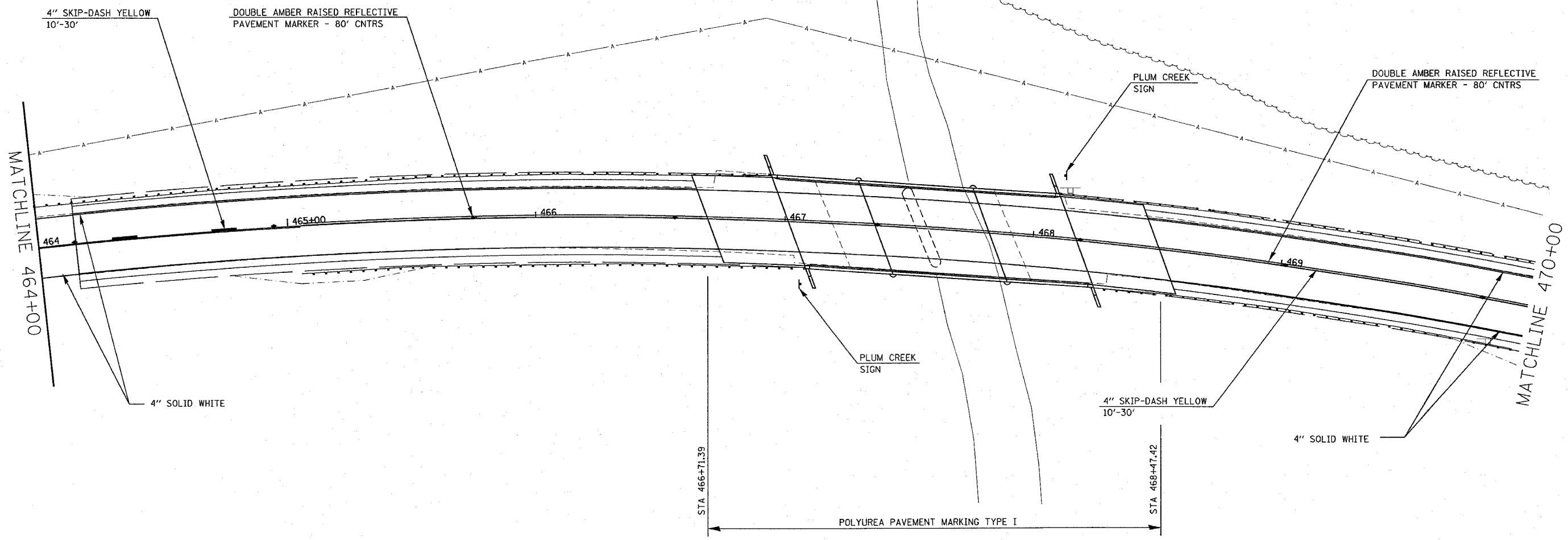
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING DETAILS

FAP ROUTE 304
SECTION 5BR-2
CALHOUN COUNTY

SCALE: VERT. _____
HORIZ. _____
DATE: _____ DRAWN BY _____
CHECKED BY _____

PLOT DATE = 1/28/2008
FILE NAME = c:\projects\79648\5br\stg\p1025a.dgn
PLOT SCALE = 20.00000 / IN.
REFERENCE = #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	22
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

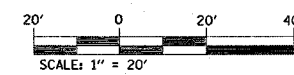
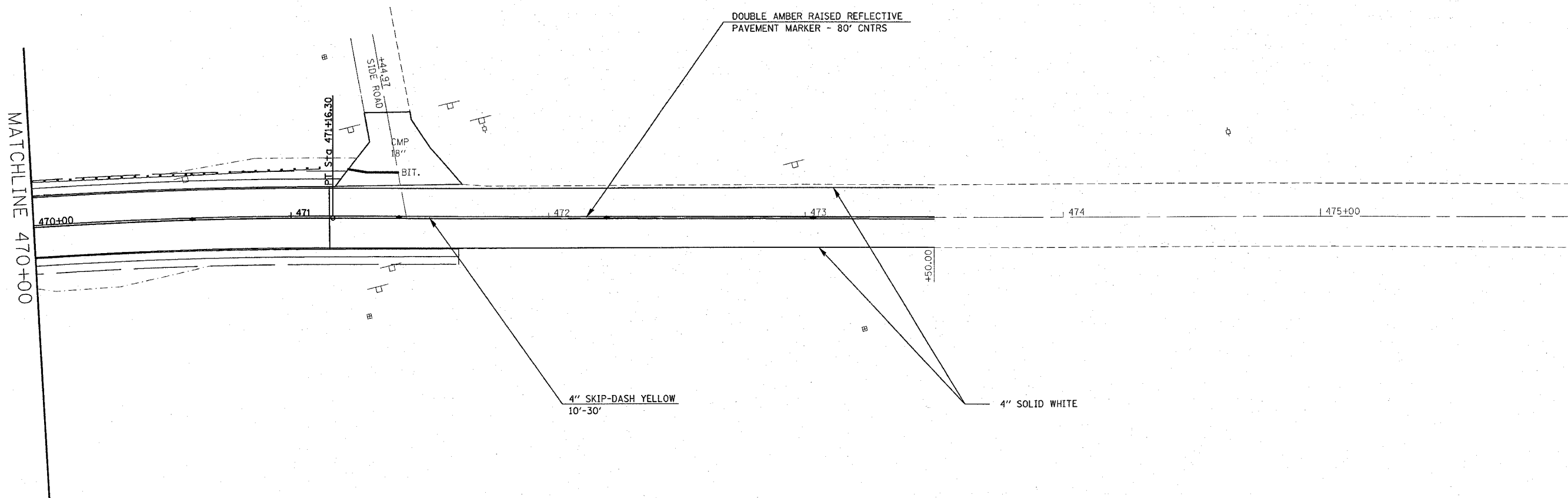
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING DETAILS
 FAP ROUTE 304
 SECTION 5BR-2
 CALHOUN COUNTY

SCALE: VERT. _____
 HORIZ. _____

DATE _____ DRAWN BY _____
 CHECKED BY _____

PLOT DATE = 1/30/2008
 FILE NAME = c:\pms\proj\46470\pav\pav.dgn
 PLOT SCALE = 1/8\"/>

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	23
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO. _		ILLINOIS	FED. AID PROJECT	



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION PAVEMENT MARKING DETAILS FAP ROUTE 304 SECTION 5BR-2 CALHOUN COUNTY
NAME	DATE	
		SCALE: VERT. HORIZ. DATE DRAWN BY CHECKED BY

PLOT DATE = 1/30/2009
 PLOT SCALE = 20' = 1" IN.
 REFERENCE = #REF#

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	STATE SHEET NO.	SHEET NO.
FAP 304		CALHOUN	68 24A	1
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		1 SHEETS

Contract Number: 76948

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

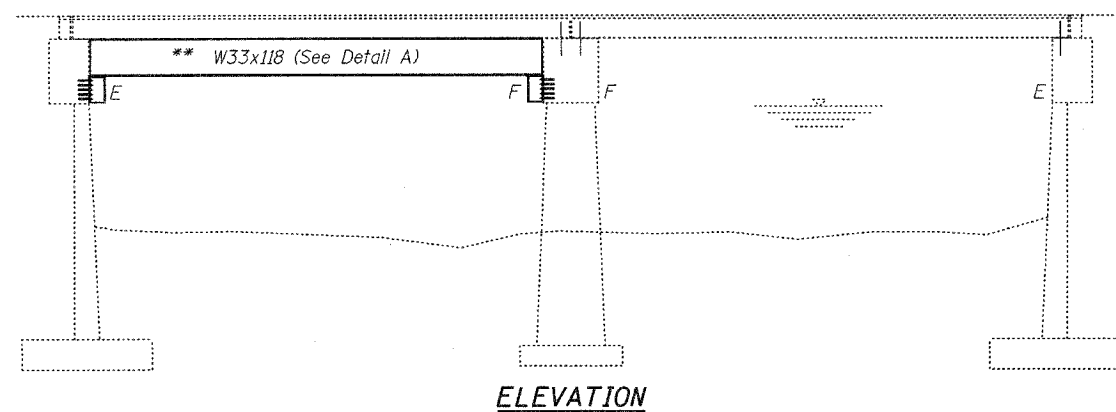
See Section 584 of the Standard Specifications for Epoxy Grouting of Threaded Rods.

The cost of epoxy grouting threaded rods shall be included with Furnishing and Erecting Structural Steel.

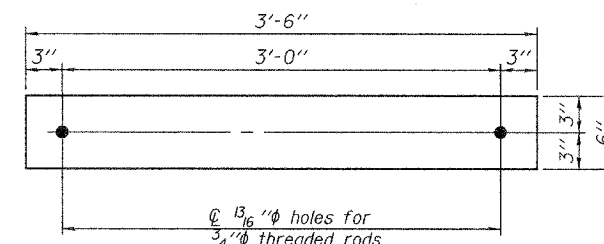
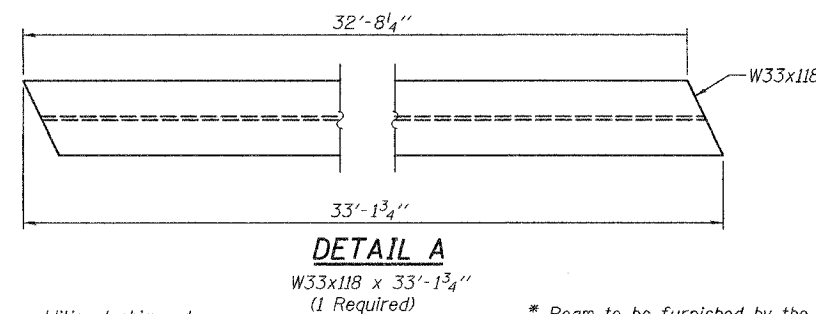
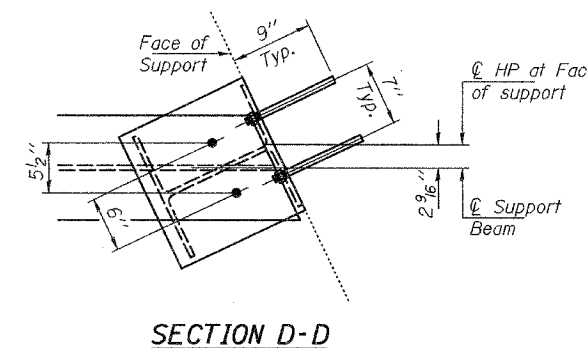
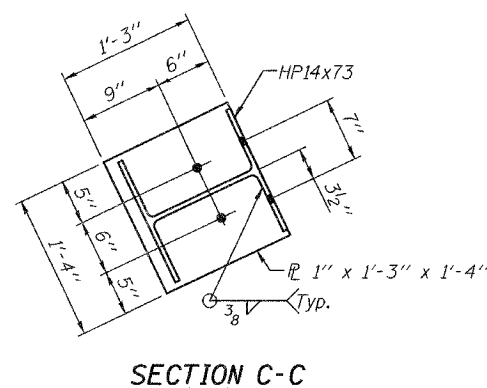
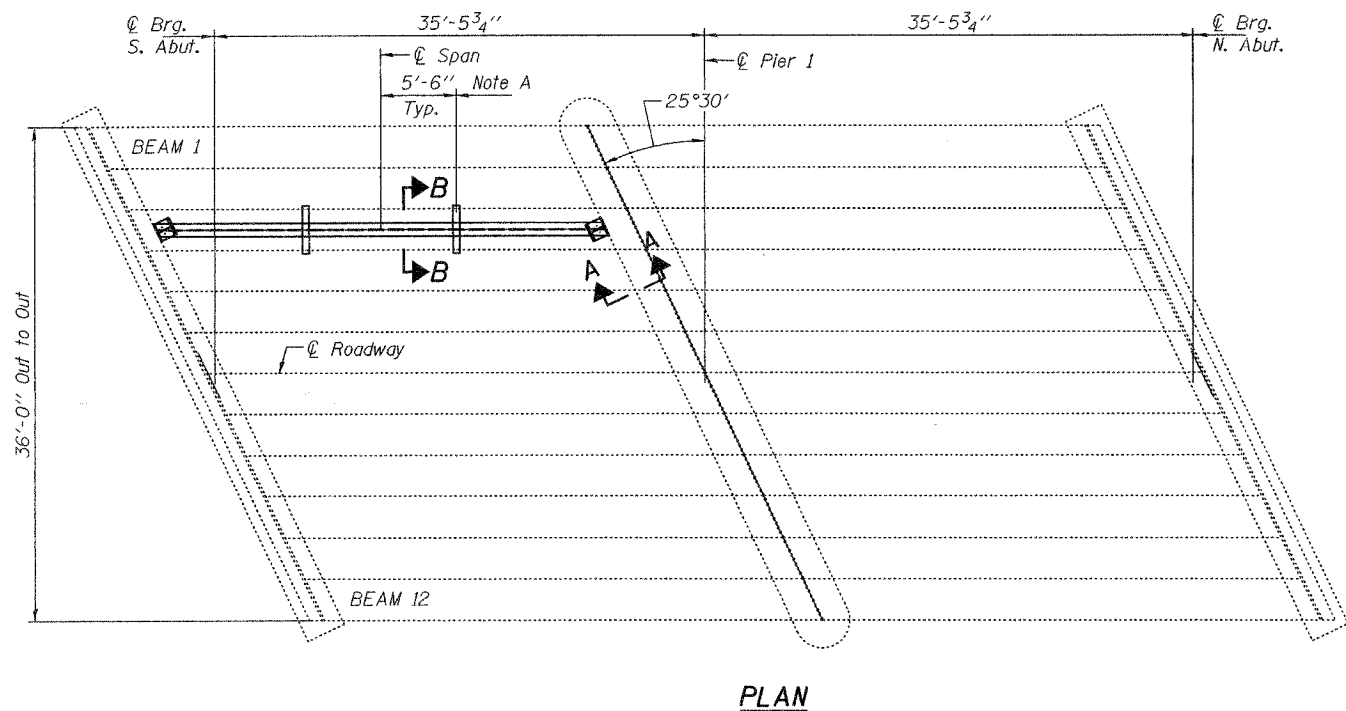
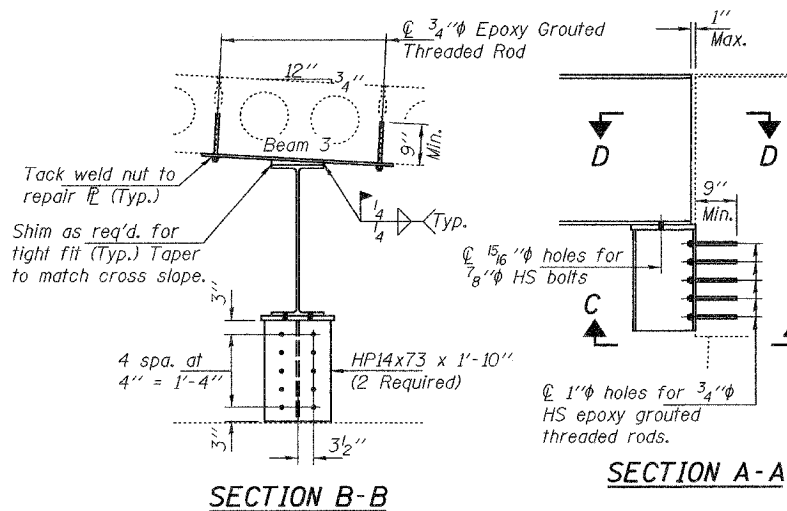
The Contractor has the option of using used steel. See special provisions.

The contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the contractor's responsibility to account for the condition of the beams when developing construction procedures.

If the contractor's procedure for placement of beams involves placement of cranes or other heavy equipment on the bridge, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, prepared and sealed by an Illinois Licensed Structural Engineer, verifying that the equipment and procedure used will not overstress the existing beams. To distribute load to multiple beams and protect the existing surface, in all cases a double layer mat of heavy timbers shall be used at all times under crane tracks or wheels and any outriggers in the down position. If necessary, shims shall be used under the crane mat to ensure uniform contact with the underlying beams.



** To be installed prior to switching traffic over for Stage I.



TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
* Furnishing and Erecting Structural Steel	Pound	4420

REPAIR DETAILS

FAP 304 OVER CRAWFORD CREEK
CALHOUN COUNTY
SN 007-0004 (EXISTING)
SN 007-0027 (PROPOSED)

Note A:
Transverse tie plates. Place additional shims at midpoints between tie plates. Securely weld shims to top flange of support beam. Minimum shim size is 6" x flange width.

* Beam to be furnished by the District. See Special Provisions.

DESIGNED	Victor H. Veliz
CHECKED	Kevin Baliva
DRAWN	baliva
CHECKED	VHV

MAY 6, 2008
EXAMINED
PASSED



Expires: November 30, 2008

Bench Mark: BM 206, chiseled square on top of north end of west concrete curb on existing structure. Elevation 438.261

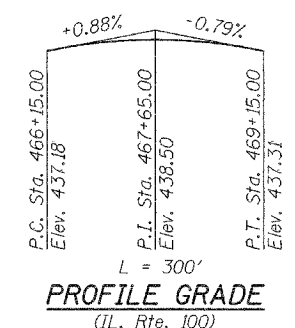
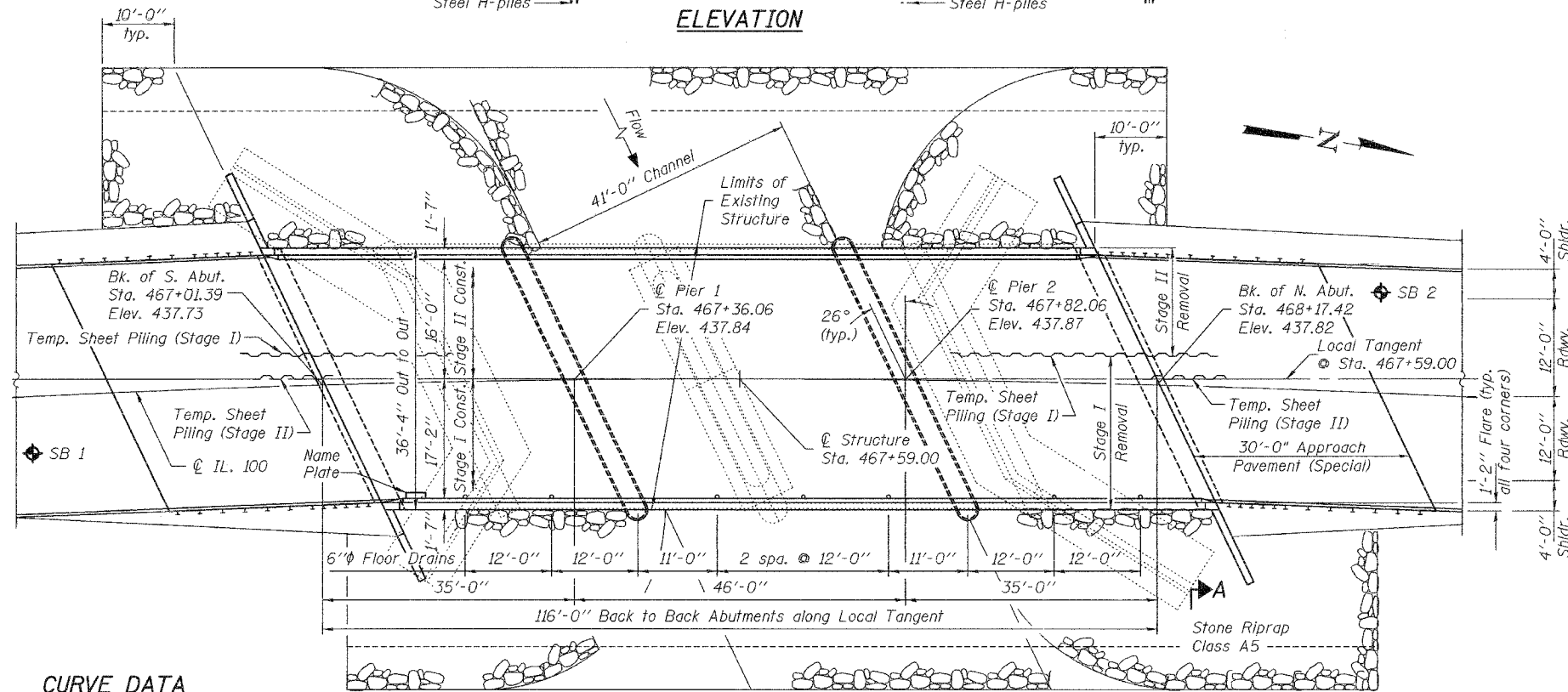
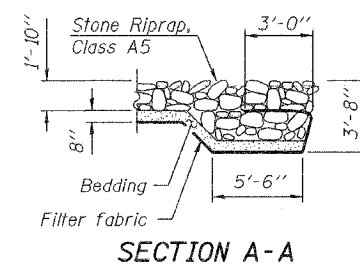
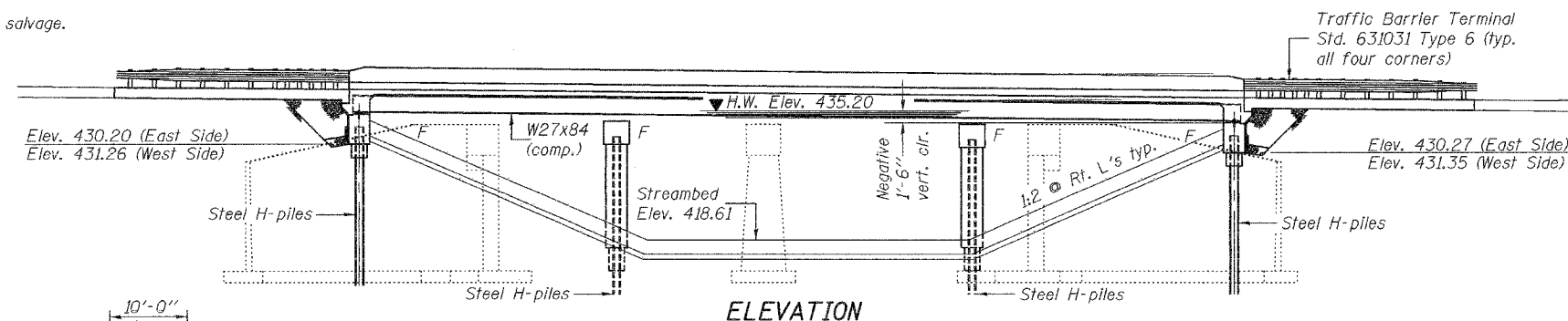
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
F.A.P. 304	5BR-2	CALHOUN	68	25	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

Existing Structure: S.N. 007-0004, originally built in 1924 as S.B.I. Route 38, Section 5B. The superstructure was replaced and the structure was widened in 1981. The existing structure is a two span deck beam bridge supported on closed abutments and a solid wall pier all on pile supported footings. The back to back abutment length is 72'-7" and the out to out bridge width is 36'-0". Traffic is to be maintained utilizing stage construction.

No salvage.



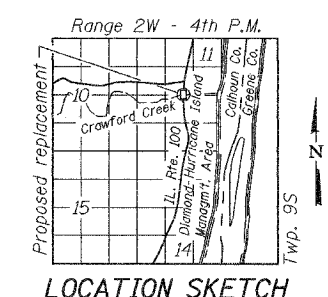
INDEX OF SHEETS

- General Plan and Elevation
- General Structure Data
- Foundation Layout
- Stage Construction Details
- Temporary Concrete Barrier For Stage Construction
- Top of Slab Elevations
- Top of S. Approach Slab Elevations
- Top of N. Approach Slab Elevations
- Superstructure
- Superstructure Details
- Diaphragm Details
- Framing Plan and Design Data
- Beam Details
- Bearing Details
- South Abutment Details
- North Abutment Details
- Pier 1 Details
- Pier 2 Details
- Miscellaneous Pier Details
- Steel H-Pile Details
- Bar Splicer Assembly Details
- Cantilever Forming Brackets for Superstructure
- Concrete Parapet Slipforming Option
- Boring Logs

STATION 467+59.00
BUILT 200 BY
STATE OF ILLINOIS
F.A.P. RT. 304 SEC. 5BR-2
LOADING HL93
STRUCTURE NO. 007-0027

NAME PLATE
See Std. 515001

APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Robert E. Anderson
ENGINEER OF BRIDGES AND STRUCTURES



LOADING HL-93
Allow 50#/#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS
2007 AASHTO LRFD Bridge Design Specifications, 4th Edition

DESIGN STRESSES
 $f_c = 3,500$ psi
 $f_y = 60,000$ psi (reinforcement)
 $f_y = 50,000$ psi (M270 Grade 50 structural steel)

SEISMIC DATA
Seismic Performance Zone (SPZ) = 1
Bedrock Acceleration Coefficient (A) = 0.054g
Site Coefficient (S) = 2.0

CURVE DATA
PI Sta. = 467+41.98
 $\Delta = 20^\circ-56'-30''$ (RT)
 $D = 2^\circ-49'-17''$
 $R = 2,030.70'$
 $T = 375.30'$
 $L = 742.22'$
 $E = 34.39'$
 $S.E. = 5.8\%$
P.C. Sta. = 463+66.68
P.T. Sta. = 471+08.91

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (feet)	S. Abut.	Pier 1	Pier 2	N. Abut.
	430.0	416.6	416.6	430.1

WATERWAY INFORMATION

Drainage Area = 5.57 sq. mi. Low Grade Elev. 436.21 @ Sta. 462+00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Nat. Prop.	H.W.E. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
Design	50	3,081	743.82	997.32	435.20	0.30	0.15	435.50
Base	100	3,664	743.82	997.32	435.20	0.43	0.22	435.63
Overtopping	N/A	-	-	-	-	-	-	0.00
Max. Calc.	500	5,107	743.82	997.32	435.19	0.83	0.43	436.02
Scour	10	1,848	743.82	997.32	435.20	0.11	0.06	435.31



David W. Petermeier
DAVID W. PETERMEIER
EDWARDSVILLE, ILLINOIS
ILLINOIS LICENSED STRUCTURAL
ENGINEER NO. 081-005642
EXPIRES NOV. 30, 2008

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

GENERAL PLAN AND ELEVATION
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	60	26	28 SHEETS
FED. ROAD DIST. NO. 7	ILL. ENDS	FED. AID PROJECT-			

Contract #76948

GENERAL NOTES

Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts.
Bolts $\frac{7}{8}$ in. ϕ , holes $\frac{9}{16}$ in. ϕ , unless otherwise noted.
Calculated weight of Structural Steel = 60,570 lbs. of Grade 50 and 5,150 lbs. of Grade 36.
No field welding is permitted except as specified in the contract documents.
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions
Reinforcement bars designated (E) shall be epoxy coated.
If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.
Bearing seat surfaces shall be constructed or adjusted to their designated elevations within a tolerance of $\frac{1}{8}$ inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shiming the bearings.
The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5 G 4/8. See Special Provision for "Cleaning and Painting New Metal Structures".
Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
The Contractor shall drive test piles to 110% of the nominal required bearing specified in production locations at substructures specified or approved by the Engineer before ordering the remainder of piles.
The Contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. Yd.		126	126
Stone Riprap, Class A5	Sq. Yd.		1,479	1,479
Filter Fabric	Sq. Yd.		1,479	1,479
Removal of Existing Structures	Each			1
Structure Excavation	Cu. Yd.		424	424
Floor Drains	Each	7		7
Concrete Structures	Cu. Yd.		168.0	168.0
Concrete Superstructure	Cu. Yd.	155.4		155.4
Bridge Deck Grooving	Sq. Yd.	402		402
Concrete Encasement	Cu. Yd.		10.8	10.8
Protective Coat	Sq. Yd.	576		576
Furnishing and Erecting Structural Steel	L. Sum	1		1
Stud Shear Connectors	Each	2,376		2,376
Reinforcement Bars, Epoxy Coated	Pound	35,670	16,480	52,150
Bar Splicers	Each	415	102	517
Furnishing Steel Piles HP 12x53	Foot		575	575
Furnishing Steel Piles HP 14x89	Foot		620	620
Driving Piles	Foot		1,195	1,195
Test Pile Steel HP 12x53	Each		2	2
Test Pile Steel HP 14x89	Each		2	2
Temporary Sheet Piling	Sq. Ft.		673	673
Name Plates	Each	1		1
Anchor Bolts, 1"	Each		48	48
Geocomposite Wall Drain	Sq. Yd.		79	79
Pipe Underdrains for Structures 4"	Foot		106	106
Underwater Structure Excavation Protection, Location #1	Each		1	1
Underwater Structure Excavation Protection, Location #2	Each		1	1
Asbestos Bearing Pad Removal	Each	48		48

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



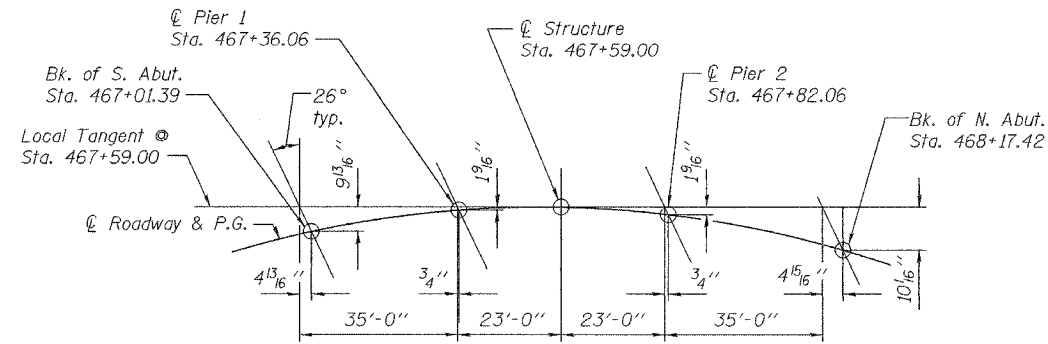
02/27/08

GENERAL STRUCTURE DATA
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

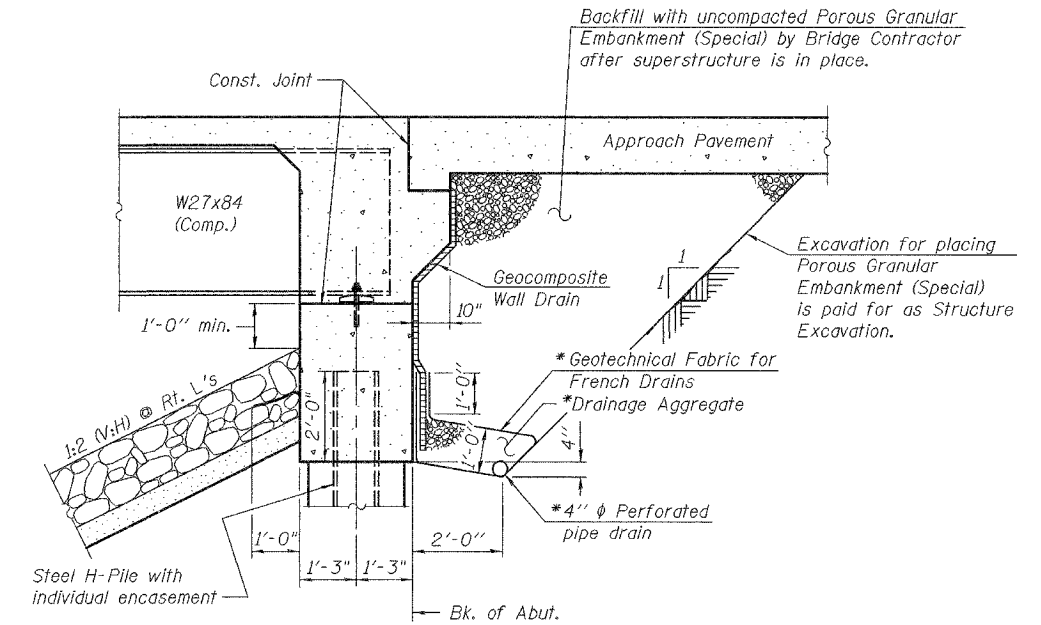
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 3 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	27	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948



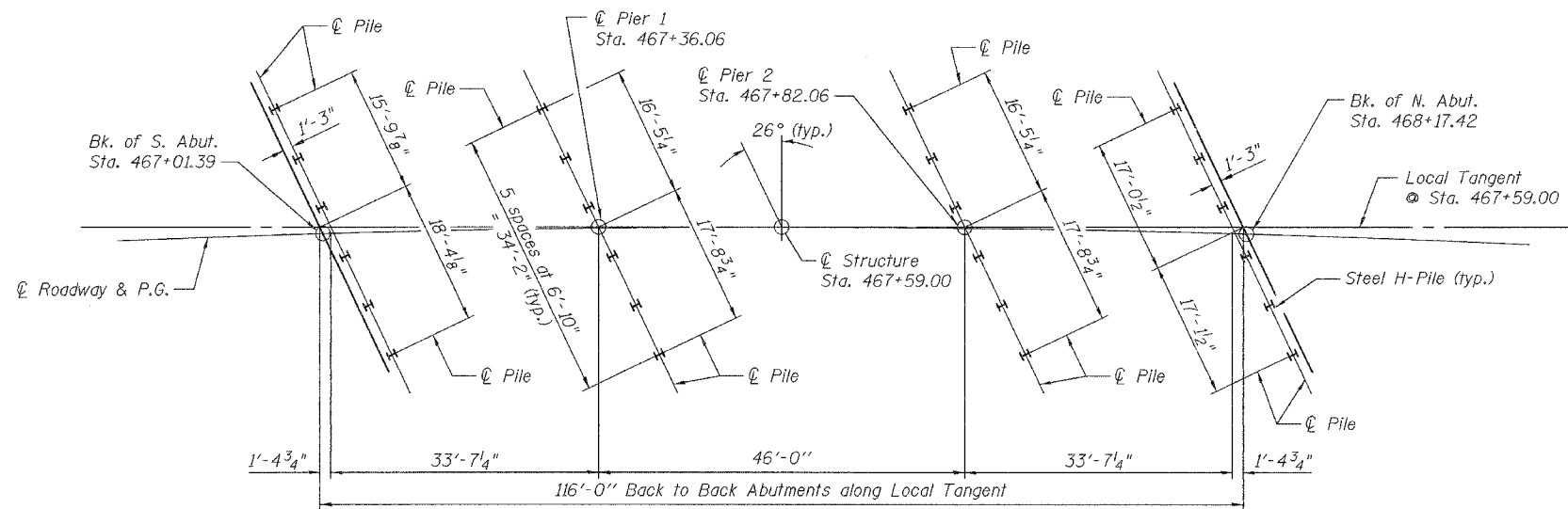
GEOMETRIC LAYOUT



SECTION THRU INTEGRAL ABUTMENT
(Horiz. dim. @ Rt. L's)

* Included in the cost of Pipe Underdrains for Structures 4".

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



FOUNDATION LAYOUT

Note:
Pile encasement not shown for clarity.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



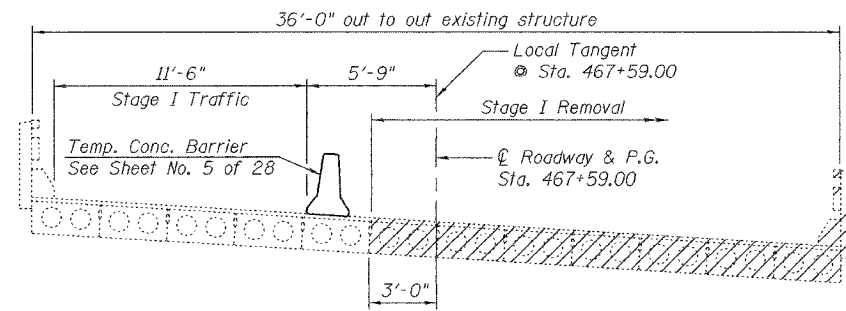
02/27/08

FOUNDATION LAYOUT
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

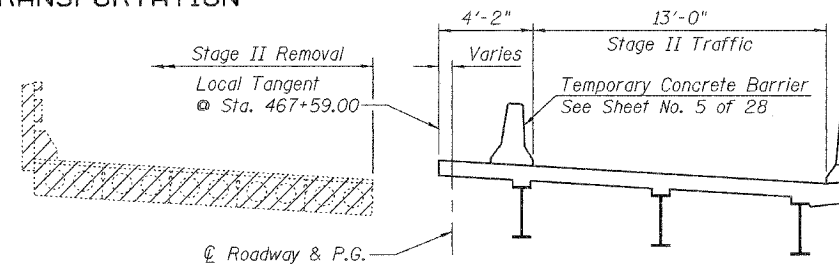
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	28	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

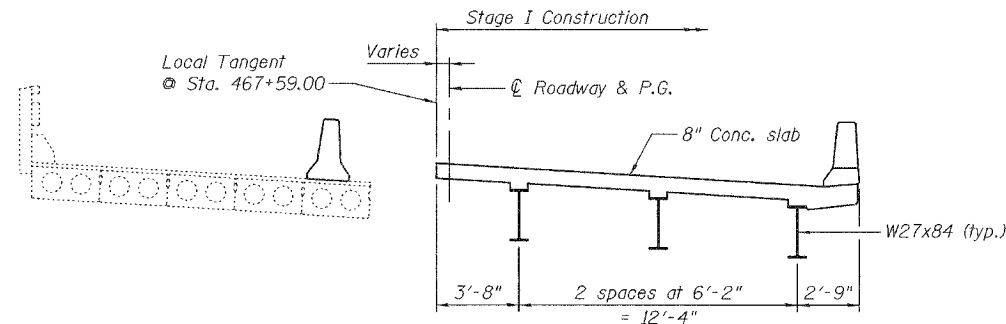
Contract #76948



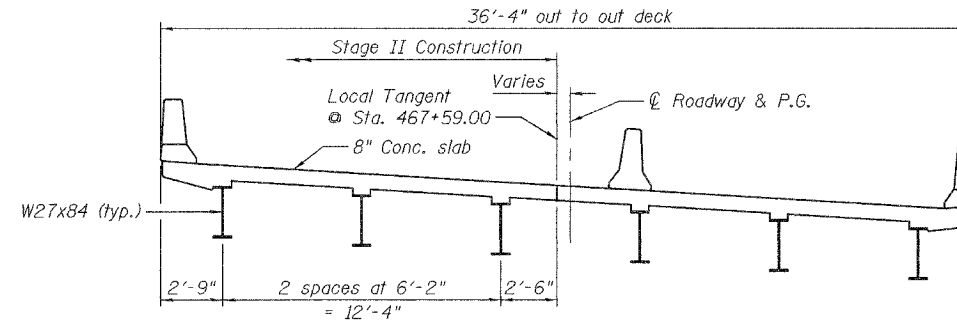
STAGE I REMOVAL
(Looking upstation)



STAGE II REMOVAL
(Looking upstation)

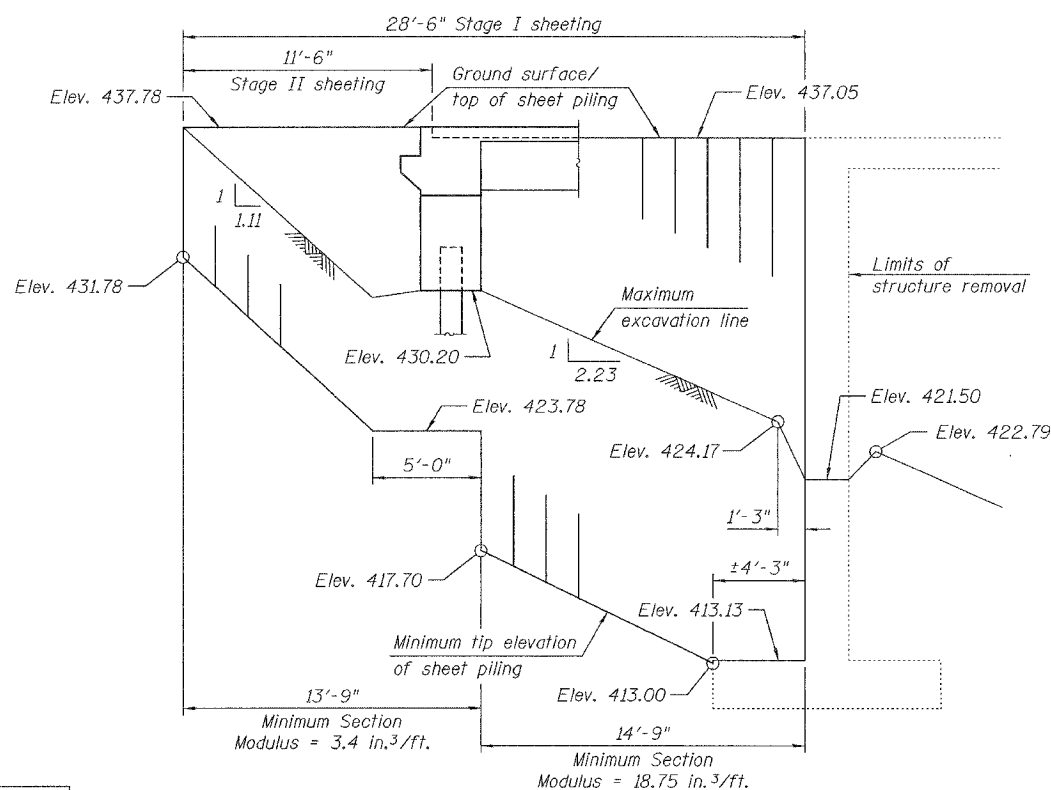


STAGE I CONSTRUCTION
(Looking upstation)

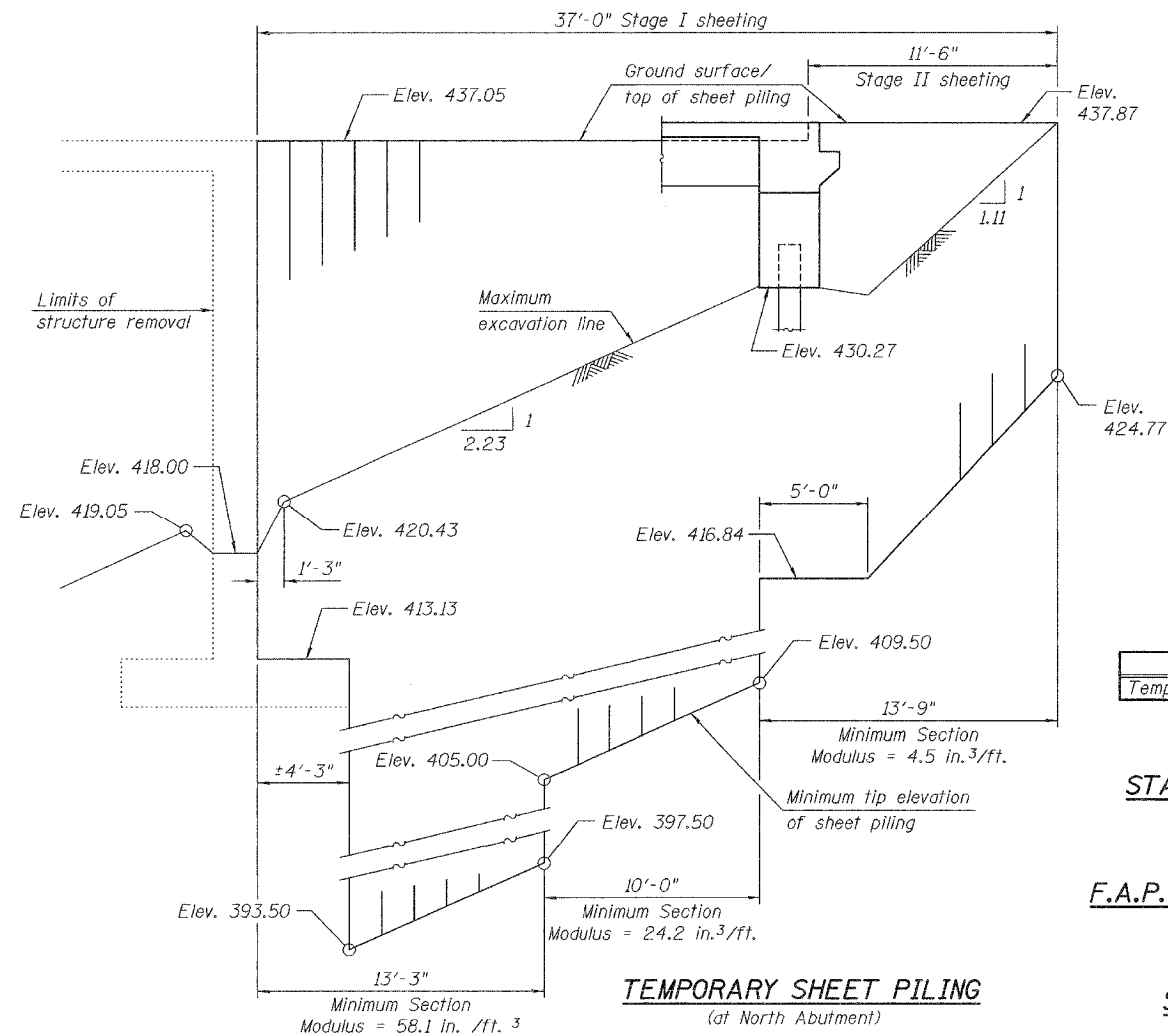


STAGE II CONSTRUCTION
(Looking upstation)

Notes:
Excavation behind existing abutment walls shall be performed to balance front and back soil pressure before removing the existing superstructure. The Contractor shall sawcut the upper portion of the existing abutment at the stage removal line before Stage I removal to ensure the remaining portion will not be prematurely damaged.
If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.
The Contractor shall connect the first sheet to the existing abutment wall to ensure stability of sheets driven to the top of the existing footing. This connection shall be reviewed and accepted by the Engineer and included in the cost for Temporary Sheet Piling.



TEMPORARY SHEET PILING
(at South Abutment)



TEMPORARY SHEET PILING
(at North Abutment)

BILL OF MATERIAL

Item	Unit	Total
Temporary Sheet Piling	Sq. Ft.	673

STAGE CONSTRUCTION DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



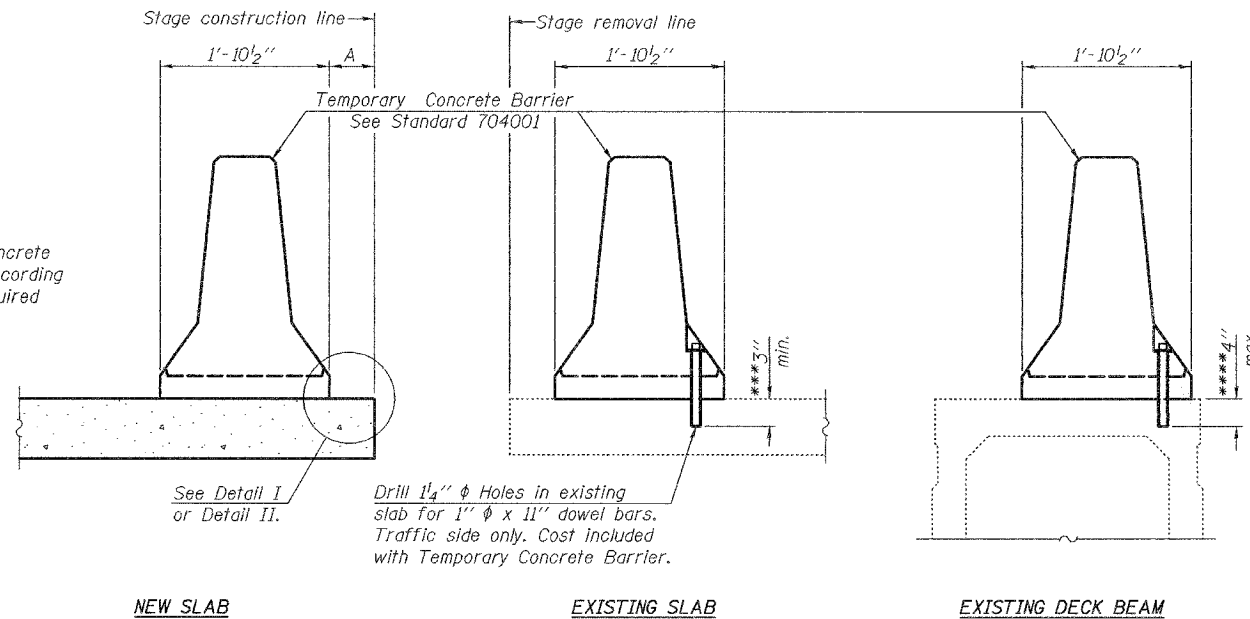
02/27/08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 5 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	29	
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT			

Contract #76948

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

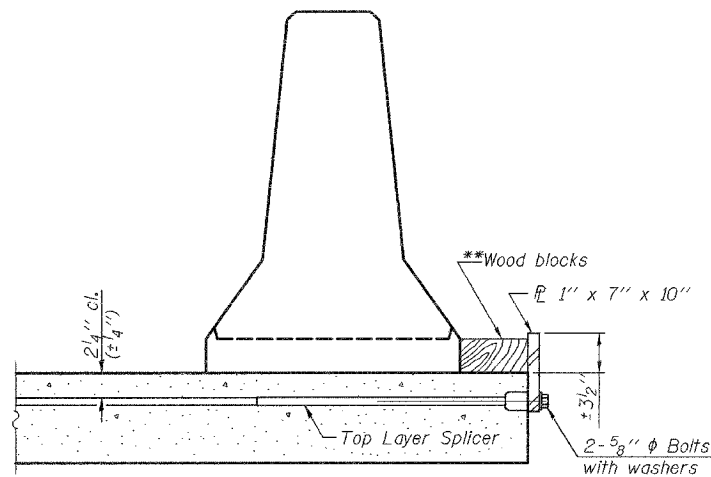


SECTIONS THRU SLAB OR DECK BEAM

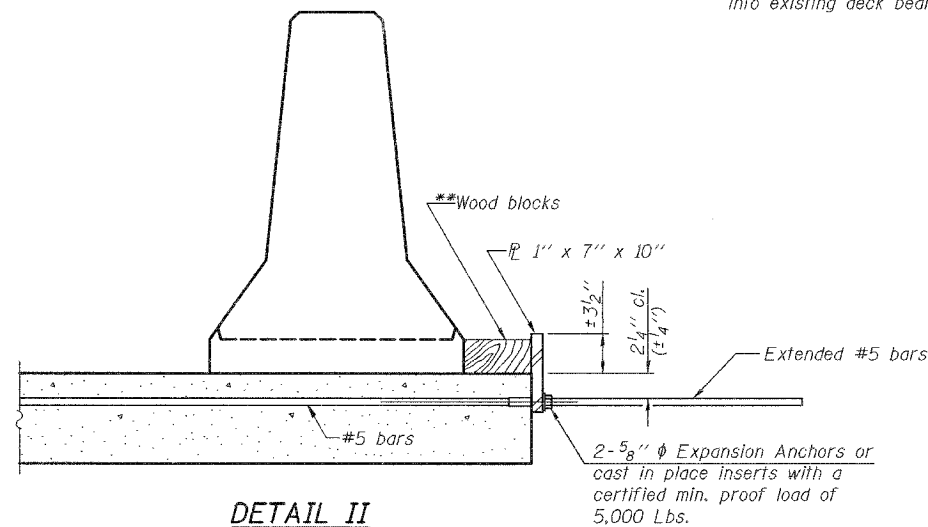
NOTES

- Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.
- Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.
- See Roadway Plans for quantity of Temporary Concrete Barrier.

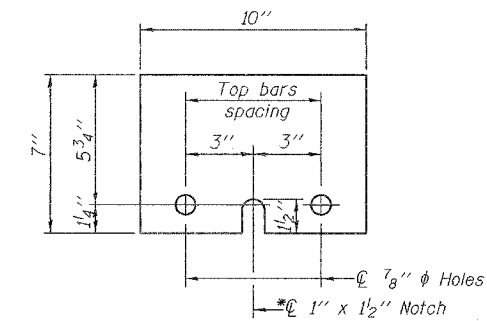
- ***Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.
- ***If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{L} 1" x 7" x 10"

* Required only with Detail II

**Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



R-27

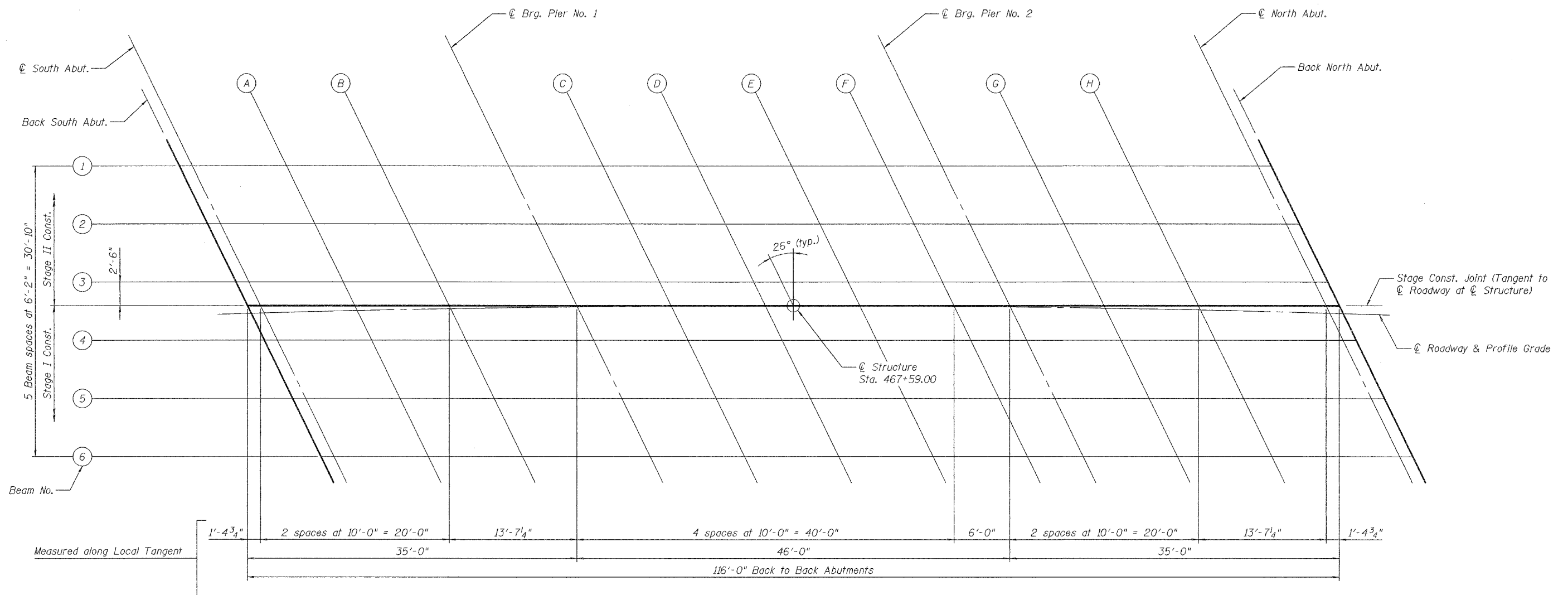
9-3-07

TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 30	SHEET NO. 6 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT	

Contract #76948



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM

02/27/08



TOP OF SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 7
F.A.P. 304	5BR-2	CALHOUN	68	91	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+94.26	-15.87	438.62	438.62
☉ South Abut.	466+95.64	-15.83	438.63	438.63
A	467+05.56	-15.54	438.65	438.66
B	467+15.48	-15.30	438.67	438.68
☉ Brg. Pier No. 1	467+28.99	-15.06	438.69	438.69
C	467+38.91	-14.93	438.71	438.72
D	467+48.84	-14.86	438.72	438.74
E	467+58.77	-14.83	438.73	438.75
F	467+68.69	-14.86	438.74	438.74
☉ Brg. Pier No. 2	467+74.65	-14.89	438.74	438.74
G	467+84.58	-15.00	438.74	438.75
H	467+94.50	-15.15	438.74	438.75
☉ North Abut.	468+08.01	-15.43	438.74	438.74
Bk. North Abut.	468+09.39	-15.46	438.74	438.74

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+97.06	-9.62	438.27	438.27
☉ South Abut.	466+98.44	-9.57	438.28	438.28
A	467+08.39	-9.30	438.30	438.31
B	467+18.34	-9.08	438.32	438.32
☉ Brg. Pier No. 1	467+31.89	-8.85	438.34	438.34
C	467+41.85	-8.74	438.36	438.37
D	467+51.80	-8.68	438.37	438.38
E	467+61.76	-8.67	438.37	438.39
F	467+71.72	-8.71	438.38	438.39
☉ Brg. Pier No. 2	467+77.69	-8.75	438.38	438.38
G	467+87.65	-8.87	438.38	438.39
H	467+97.60	-9.04	438.38	438.39
☉ North Abut.	468+11.15	-9.34	438.38	438.38
Bk. North Abut.	468+12.53	-9.38	438.38	438.38

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+99.87	-3.36	437.92	437.92
☉ South Abut.	467+01.26	-3.32	437.92	437.92
A	467+11.24	-3.06	437.95	437.95
B	467+21.22	-2.85	437.97	437.97
☉ Brg. Pier No. 1	467+34.81	-2.64	437.99	437.99
C	467+44.80	-2.55	438.00	438.01
D	467+54.79	-2.50	438.01	438.03
E	467+64.77	-2.51	438.02	438.03
F	467+74.76	-2.56	438.02	438.03
☉ Brg. Pier No. 2	467+80.75	-2.62	438.03	438.03
G	467+90.74	-2.75	438.03	438.03
H	468+00.72	-2.93	438.02	438.03
☉ North Abut.	468+14.31	-3.25	438.02	438.02
Bk. North Abut.	468+15.70	-3.29	438.02	438.02

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

TOP OF SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 8 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	32	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+01.02	-0.83	437.78	437.78
☉ South Abut.	467+02.41	-0.79	437.78	437.78
A	467+12.40	-0.53	437.80	437.81
B	467+22.39	-0.33	437.82	437.83
☉ Brg. Pier No. 1	467+36.00	-0.13	437.84	437.84
C	467+46.00	-0.04	437.86	437.87
D	467+56.00	0.00	437.87	438.89
E	467+66.00	-0.01	437.87	438.89
F	467+76.00	-0.07	437.88	437.88
☉ Brg. Pier No. 2	467+81.98	-0.13	437.88	437.88
G	467+92.00	-0.27	437.88	437.88
H	468+01.99	-0.46	437.88	437.89
☉ North Abut.	468+15.59	-0.79	437.87	437.87
Bk. North Abut.	468+16.98	-0.83	437.87	437.87

☉ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+01.39	0.00	437.73	437.73
☉ South Abut.	467+02.76	0.00	437.74	437.74
A	467+12.64	0.00	437.77	437.78
B	467+22.55	0.00	437.80	437.81
☉ Brg. Pier No. 1	467+36.06	0.00	437.84	437.84
C	467+46.02	0.00	437.86	437.87
D	467+56.00	0.00	437.87	438.89
E	467+66.01	0.00	437.87	438.89
F	467+76.03	0.00	437.88	437.88
☉ Brg. Pier No. 2	467+82.06	0.00	437.87	437.87
G	467+92.13	0.00	437.87	437.87
H	468+02.23	0.00	437.85	437.86
☉ North Abut.	468+16.05	0.00	437.82	437.82
Bk. North Abut.	468+17.42	0.00	437.82	437.82

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+02.70	2.89	437.57	437.57
☉ South Abut.	467+04.09	2.93	437.57	437.57
A	467+14.11	3.17	437.59	437.60
B	467+24.12	3.37	437.61	437.62
☉ Brg. Pier No. 1	467+37.75	3.56	437.63	437.63
C	467+47.77	3.64	437.65	437.66
D	467+57.79	3.67	437.66	437.67
E	467+67.80	3.65	437.66	437.68
F	467+77.82	3.58	437.67	437.67
☉ Brg. Pier No. 2	467+83.83	3.52	437.67	437.67
G	467+93.85	3.37	437.67	437.67
H	467+03.86	3.17	437.67	437.67
☉ North Abut.	468+17.49	2.83	437.66	437.66
Bk. North Abut.	468+18.88	2.79	437.66	437.66

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



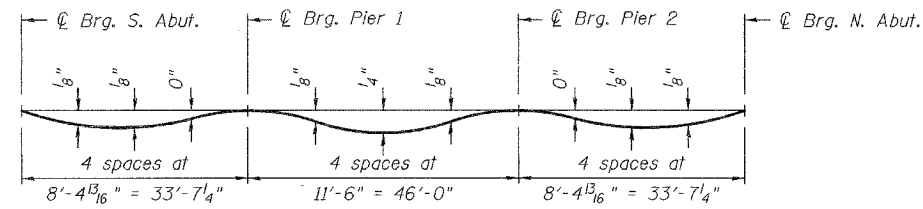
02/27/08

TOP OF SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 9 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	48	33	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #76948

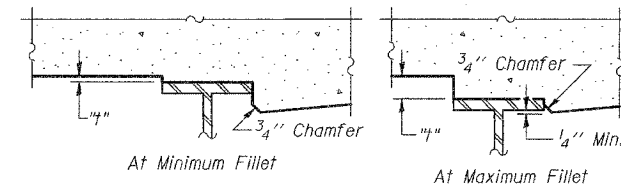


DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheet Nos. 7 thru 9 of 28.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown on Sheet No. 6 of 28. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on Sheet Nos. 7 thru 9 of 28, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+05.55	9.13	437.22	437.22
☉ South Abut.	467+06.95	9.17	437.22	437.22
A	467+16.99	9.40	437.24	437.25
B	467+27.03	9.58	437.26	437.27
☉ Brg. Pier No. 1	467+40.71	9.75	437.28	437.28
C	467+50.76	9.82	437.29	437.30
D	467+60.80	9.83	437.30	437.32
E	467+70.85	9.80	437.31	437.32
F	467+80.90	9.72	437.31	437.32
☉ Brg. Pier No. 2	467+86.93	9.64	437.31	437.31
G	467+96.98	9.48	437.31	437.31
H	468+07.02	9.27	437.31	437.31
☉ North Abut.	468+20.68	8.90	437.30	437.30
Bk. North Abut.	468+22.08	8.86	437.29	437.29

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+08.42	15.37	436.87	436.87
☉ South Abut.	467+09.82	15.41	436.87	436.87
A	467+19.89	15.63	436.89	436.90
B	467+29.97	15.79	436.91	436.91
☉ Brg. Pier No. 1	467+43.68	15.94	436.93	436.93
C	467+53.76	15.99	436.94	436.95
D	467+63.84	15.99	436.95	436.96
E	467+73.92	15.95	436.95	436.97
F	467+84.00	15.85	436.95	436.96
☉ Brg. Pier No. 2	467+90.05	15.76	436.95	436.95
G	468+00.12	15.59	436.95	436.95
H	468+10.20	15.36	436.95	436.95
☉ North Abut.	468+23.90	14.97	436.94	436.94
Bk. North Abut.	468+25.30	14.93	436.93	436.93

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

TOP OF SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET	SHEET NO. 10 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	34	
FED. ROAD DIST. NO. 7		LENGTHS	FEE. AND PROJECT		

Contract #76948

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+64.97	-16.00	438.48
A	466+74.50	-16.45	438.56
B	466+84.03	-16.94	438.64
Bk. South Abut.	466+93.54	17.48	438.71

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+66.69	-12.00	438.26
A	466+76.44	-12.00	438.31
B	466+86.20	-12.00	438.36
Bk. South Abut.	466+95.99	-12.00	438.41

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+71.05	-1.91	437.70
A	466+81.04	-1.50	437.73
B	466+91.03	-1.14	437.75
Bk. South Abut.	467+01.02	-0.83	437.78

☉ ROADWAY & P.G.

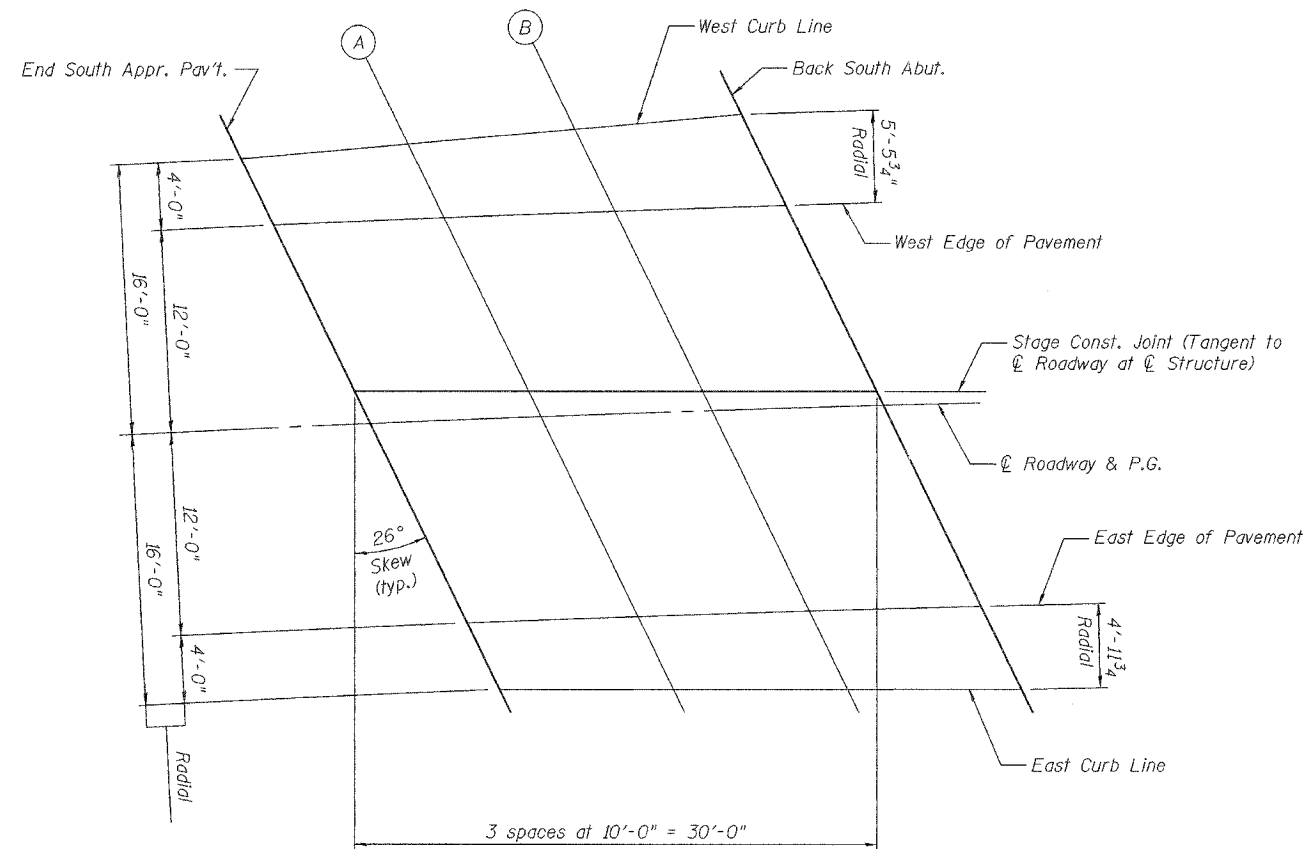
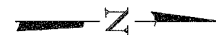
Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+71.88	0.00	437.59
A	466+81.70	0.00	437.64
B	466+91.53	0.00	437.69
Bk. South Abut.	467+01.39	0.00	437.73

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+77.15	12.00	436.92
A	466+87.03	12.00	436.97
B	466+96.94	12.00	437.02
Bk. South Abut.	467+06.86	12.00	437.06

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+78.92	16.00	436.70
A	466+88.99	16.38	436.73
B	466+99.07	16.70	436.75
Bk. South Abut.	467+09.15	16.98	436.78



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

**TOP OF SOUTH APPROACH
SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027**

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 11 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	35	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+08.58	-17.03	438.83
A	468+18.84	-16.63	438.78
B	468+29.10	-16.29	438.73
End North Appr. Pav't.	468+39.37	-16.00	438.68

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+11.17	-12.00	438.53
A	468+21.26	-12.00	438.51
B	468+31.37	-12.00	438.48
End North Appr. Pav't.	468+41.50	-12.00	438.44

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+16.98	-0.83	437.87
A	468+26.97	-1.14	437.86
B	468+36.96	-1.50	437.85
End North Appr. Pav't.	468+46.94	-1.91	437.83

☉ ROADWAY & P.G.

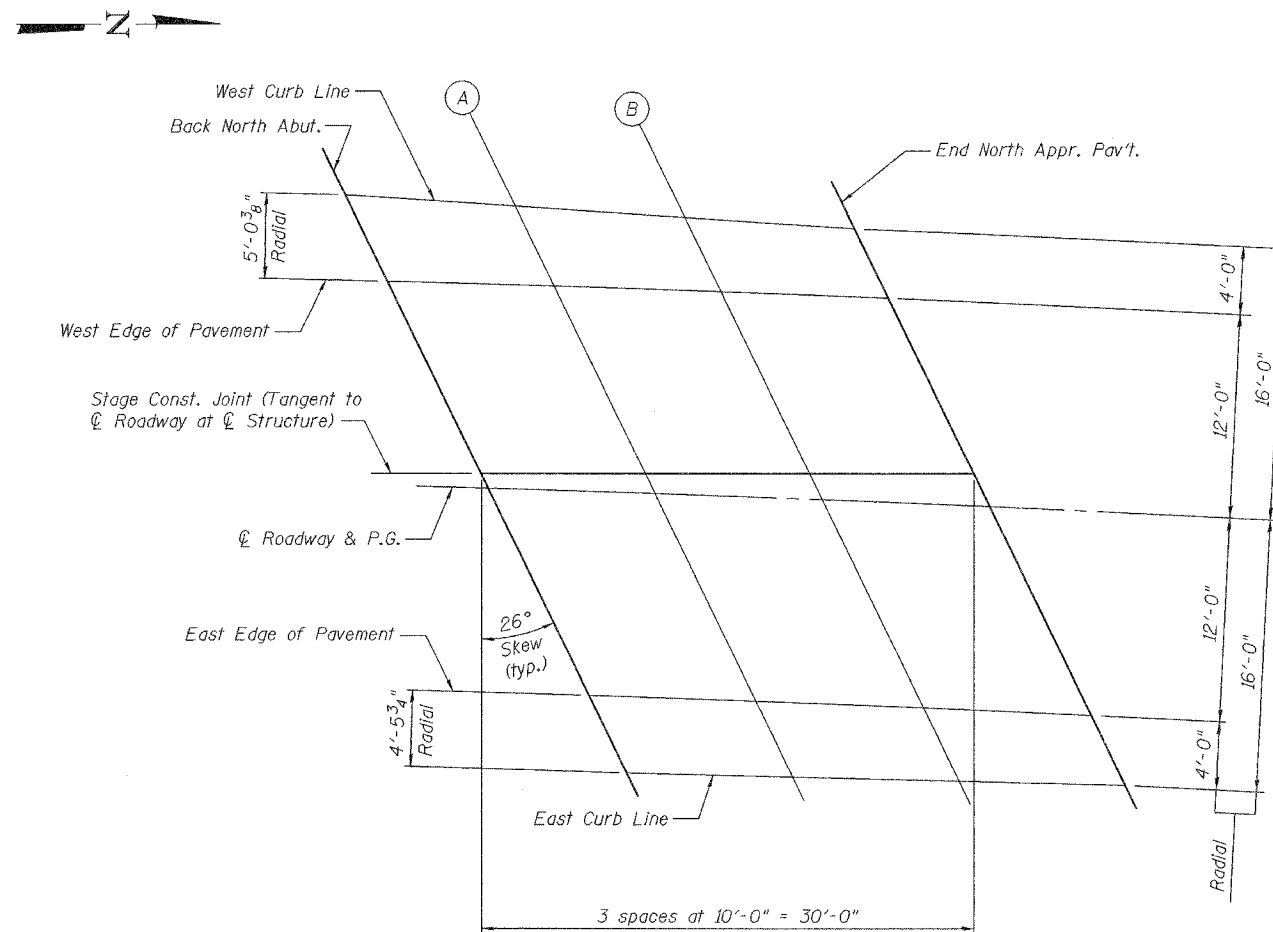
Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+17.42	0.00	437.82
A	468+27.58	0.00	437.79
B	468+37.76	0.00	437.76
End North Appr. Pav't.	468+47.98	0.00	437.72

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+23.75	12.00	437.11
A	468+33.98	12.00	437.08
B	468+44.25	12.00	437.04
End North Appr. Pav't.	468+54.54	12.00	436.99

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+26.13	16.48	436.84
A	468+36.34	16.37	436.81
B	468+46.55	16.21	436.79
End North Appr. Pav't.	468+56.75	16.00	436.75



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



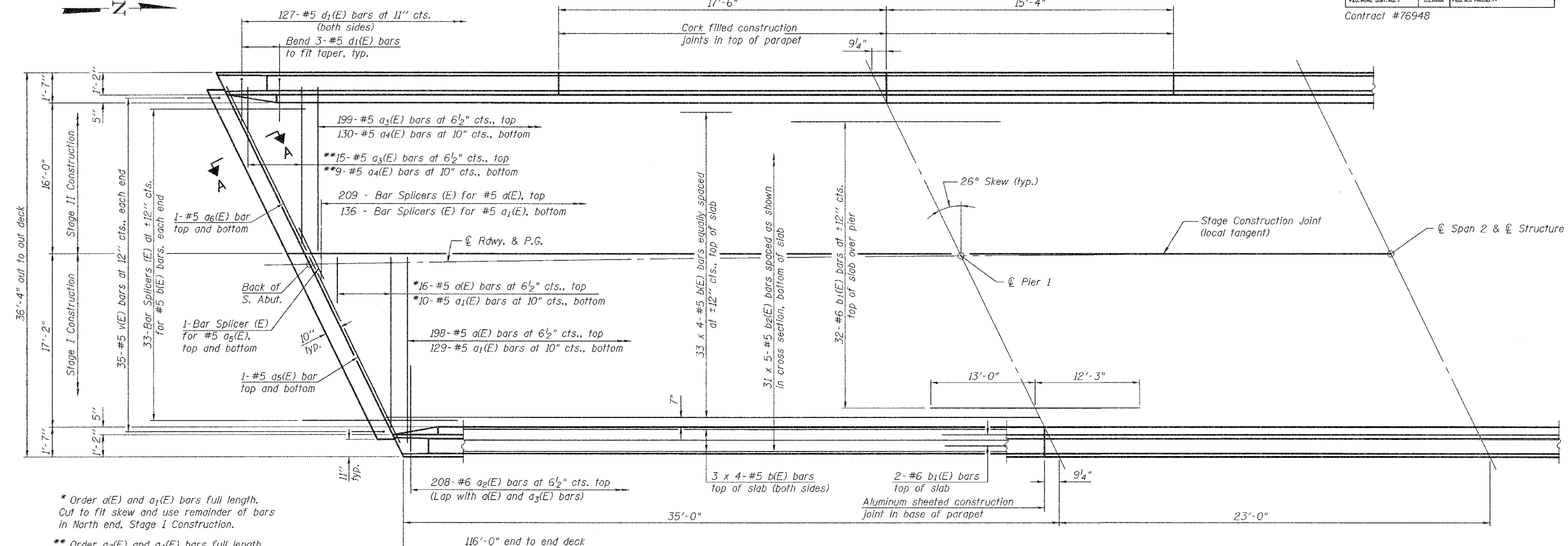
02/27/08

TOP OF NORTH APPROACH
SLAB ELEVATIONS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	36
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

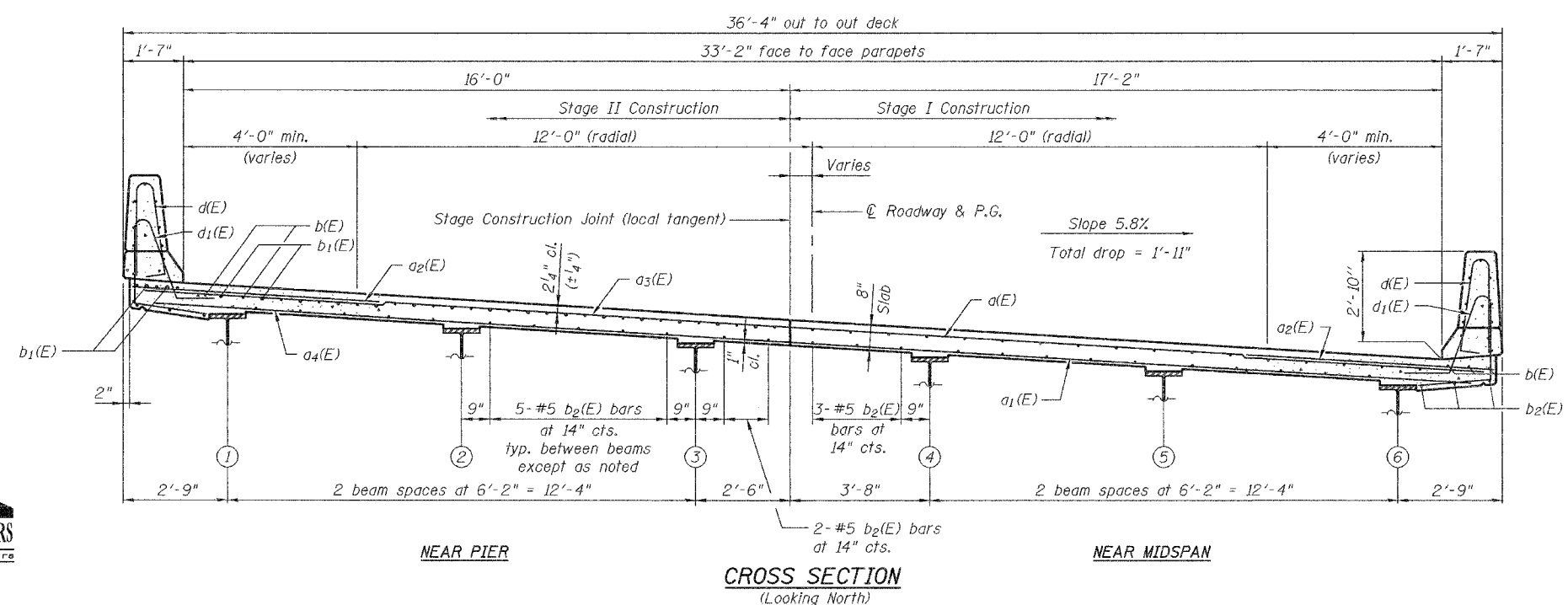
SHEET NO. 12
28 SHEETS
Contract #76948



* Order a(E) and a1(E) bars full length. Cut to fit skew and use remainder of bars in North end, Stage I Construction.

** Order a3(E) and a4(E) bars full length. Cut to fit skew and use remainder of bars in North end, Stage II Construction.

PARTIAL PLAN



MINIMUM BAR LAP
(Slab)
#5 bar = 1'-8"

Notes:
See Sheet 13 of 28 for superstructure details and Superstructure Bill of Material.
Bars indicated thus 33 x 4-#5 etc. indicates 33 lines of bars with 4 lengths per line.
See Sheet 13 of 28 for parapet reinforcement.
For details of Section A-A, see Sheet No. 14 of 28.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



SUPERSTRUCTURE
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

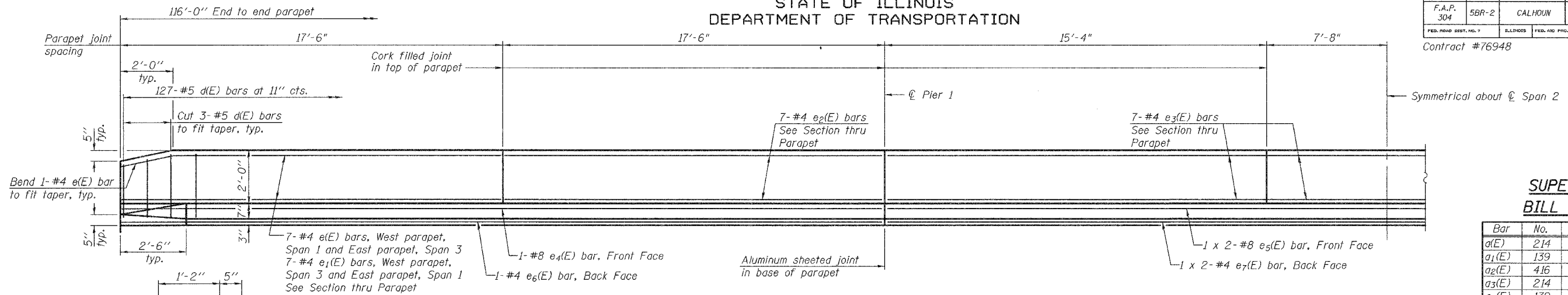
02/27/08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	37
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

SHEET NO. 13
28 SHEETS

Contract #76948

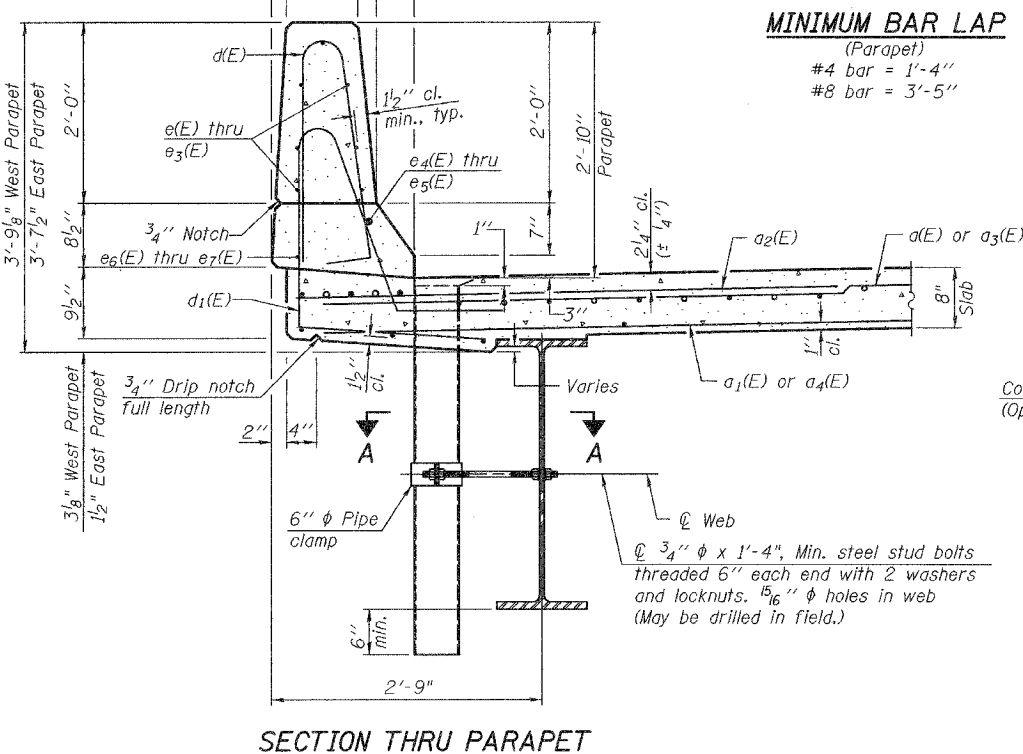


INSIDE ELEVATION OF PARAPET

SUPERSTRUCTURE
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	214	#5	18'-2"	—
a1(E)	139	#5	17'-5"	—
a2(E)	416	#6	6'-0"	—
a3(E)	214	#5	16'-11"	—
a4(E)	139	#5	16'-2"	—
a5(E)	4	#5	19'-5"	—
a6(E)	4	#5	18'-0"	—
b(E)	156	#5	30'-3"	—
b1(E)	72	#6	25'-3"	—
b2(E)	155	#5	24'-6"	—
d(E)	254	#5	5'-7"	—
d1(E)	254	#5	7'-7"	—
e(E)	14	#4	17'-8"	—
e1(E)	14	#4	16'-10"	—
e2(E)	28	#4	17'-3"	—
e3(E)	42	#4	15'-1"	—
e4(E)	4	#8	34'-9"	—
e5(E)	4	#8	24'-7"	—
e6(E)	4	#4	34'-9"	—
e7(E)	4	#4	23'-7"	—
m(E)	4	#6	19'-5"	—
m1(E)	6	#6	20'-7"	—
m2(E)	12	#6	8'-9"	—
m3(E)	8	#6	6'-6"	—
m4(E)	4	#6	2'-9"	—
m5(E)	2	#6	3'-9"	—
m6(E)	4	#6	18'-1"	—
m7(E)	6	#6	19'-4"	—
m8(E)	12	#6	8'-4"	—
m9(E)	2	#6	2'-5"	—
s(E)	72	#5	6'-9"	—
s1(E)	62	#4	8'-4"	—
v(E)	70	#5	3'-4"	—
Reinforcement Bars, Epoxy Coated		Pound		35,670
Concrete Superstructure		Cu. Yds.		155.4

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line. For details of bar splicers, see Sheet No. 24 of 28.

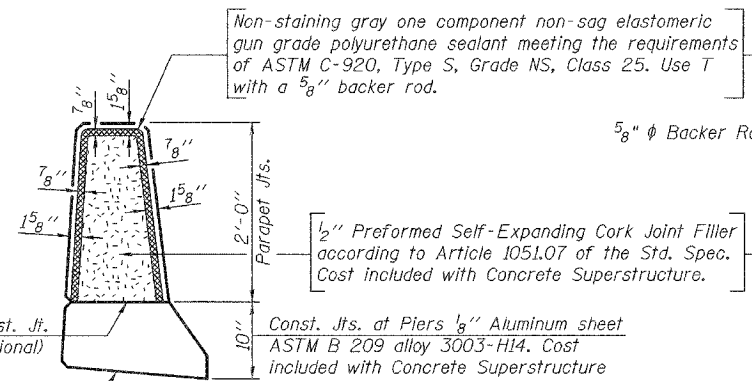


SECTION THRU PARAPET

MINIMUM BAR LAP

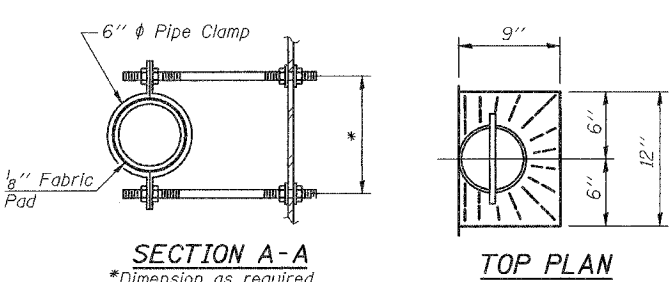
(Parapet)
#4 bar = 1'-4"
#8 bar = 3'-5"

Const. Jt. (Optional)
Const. Jt. (Mandatory)

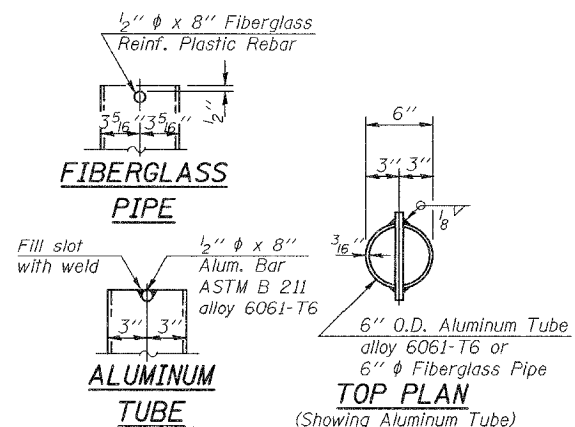


PARAPET JOINT DETAILS

Notes:
All edges of concrete structures shall have a standard 3/4" chamfer, except as noted.
The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to Steel Structures Painting Council's Spec. SSPC-SPI prior to painting.
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
The clamping device and inserts shall be galvanized according to AASHTO M 232.
For concrete parapet slipforming option, see Sheet No. 26 of 28.

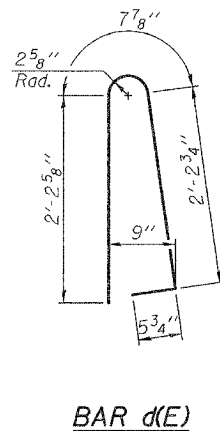


SECTION A-A
*Dimension as required by Pipe Clamp

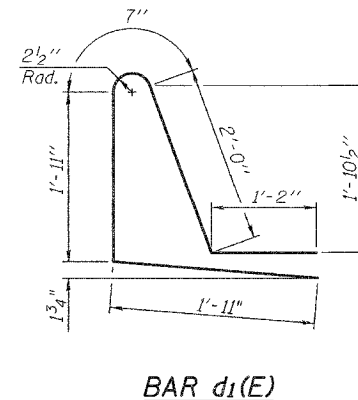


ALUMINUM TUBE

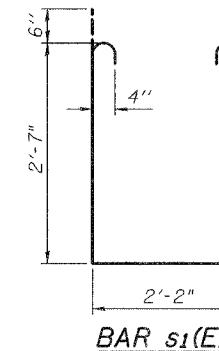
TOP PLAN (Showing Aluminum Tube)



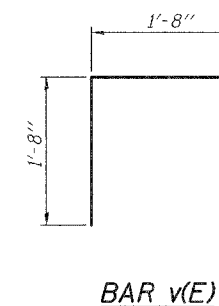
BAR d(E)



BAR d1(E)



BAR s1(E)



BAR v(E)

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



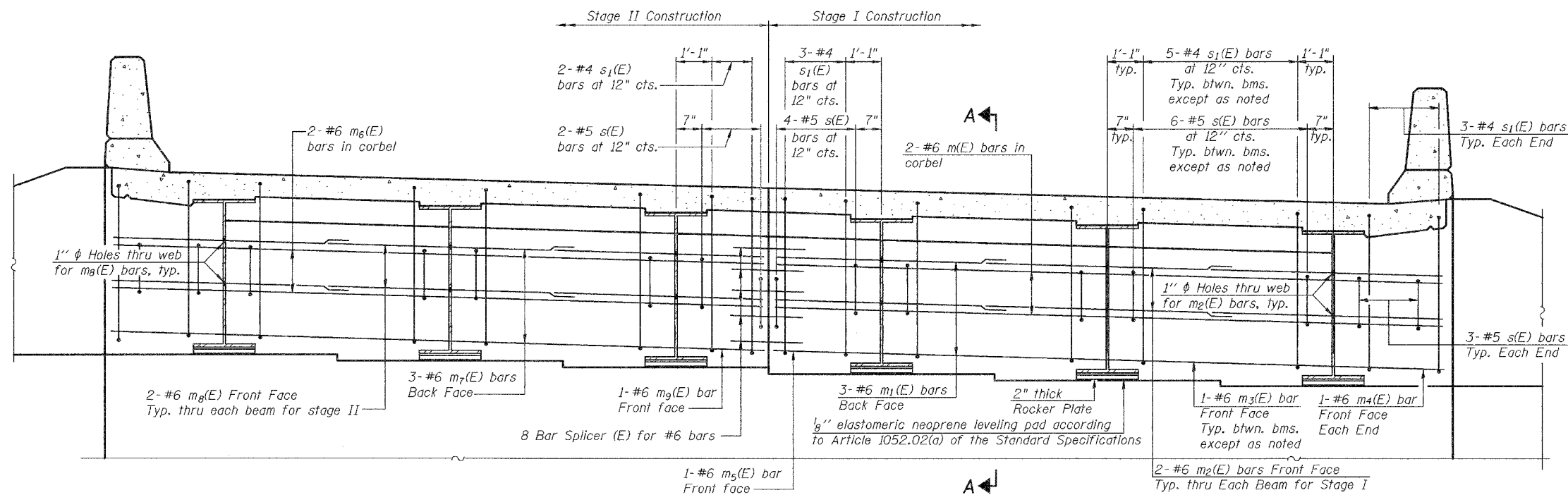
02/27/08

SUPERSTRUCTURE DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 14
F.A.P. 304	5BR-2	CALHOUN	68	38	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

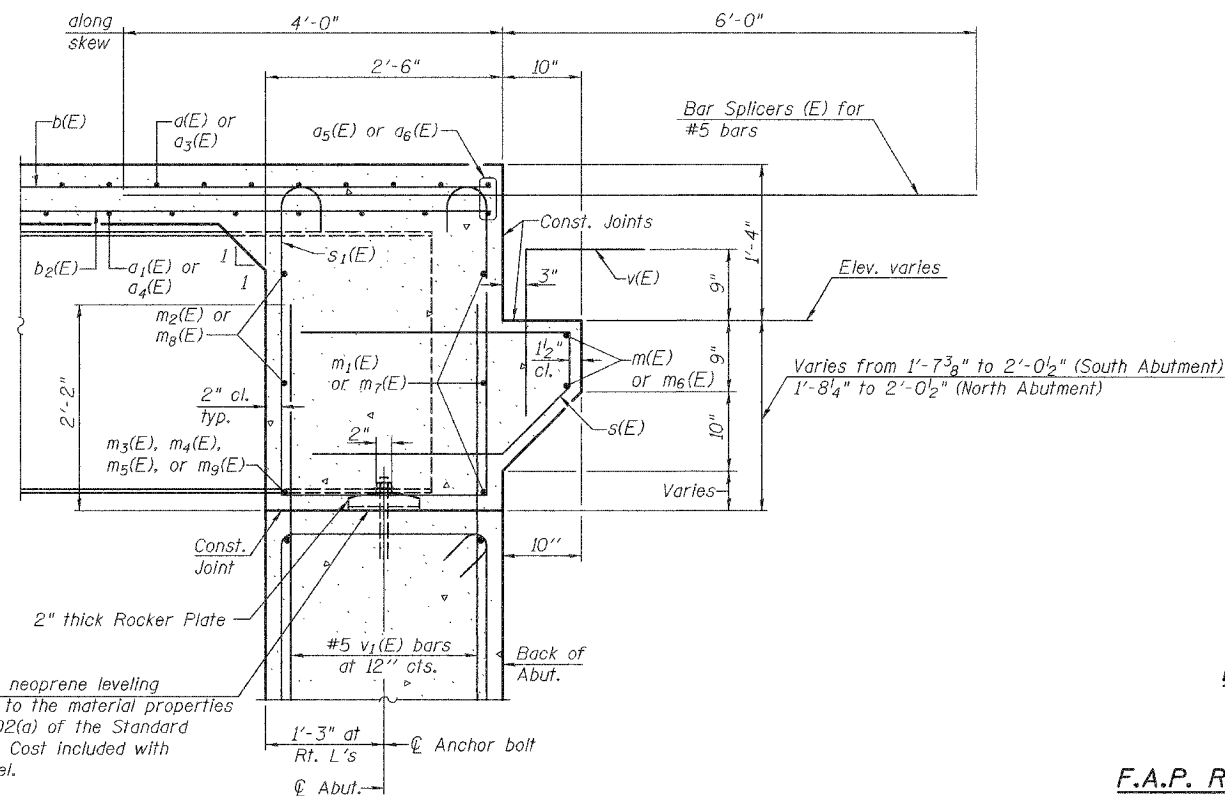


DIAPHRAGM ELEVATION AT ABUTMENT

(North Abutment shown, looking North. South Abutment similar)

Notes:
Reinforcement bars in diaphragm are billed with superstructure on Sheet No. 13 of 28.
Concrete in diaphragm is included with Concrete Superstructure on Sheet No. 13 of 28.
For details of bars s(E) & s1(E) see Sheet No. 13 of 28.
The s(E) and s1(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.

MIN. BAR LAP
(Diaphragms)
#6 bar = 2'-9"



SECTION A-A

Dimensions at right angles to abutment, except as shown.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	YSS



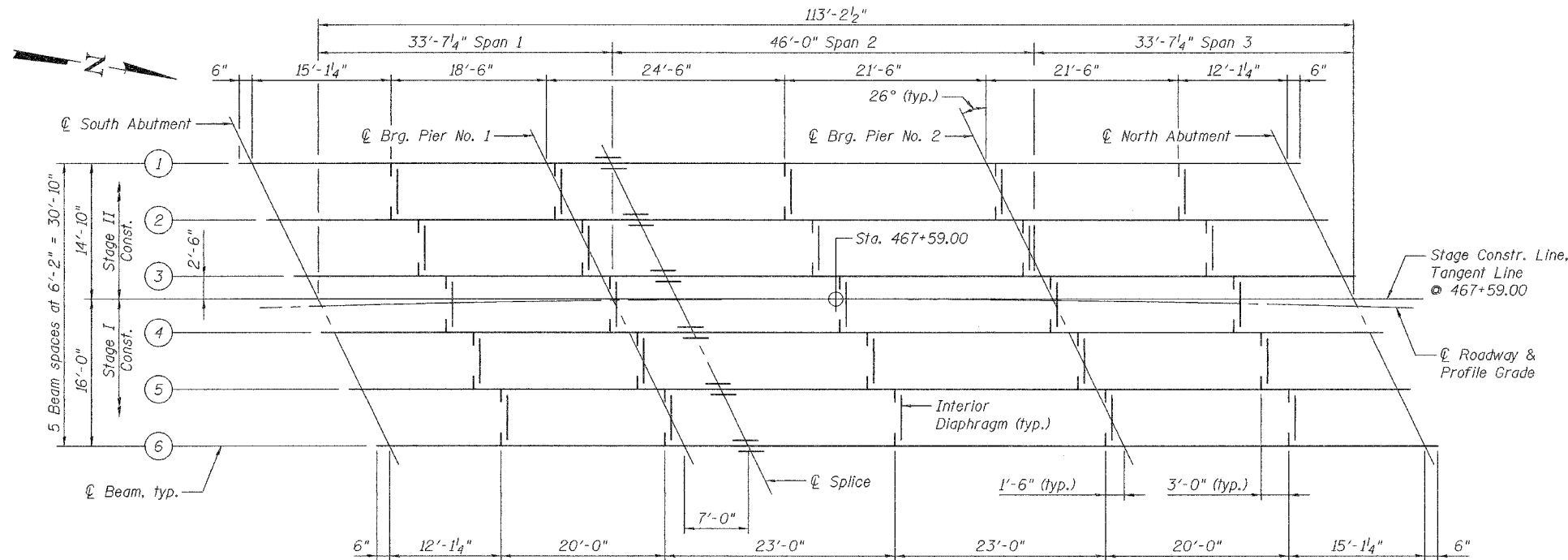
02/27/08

DIAPHRAGM DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	48	39	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948



FRAMING PLAN

Note:
For Beam Elevation, Field Splice and Interior Diaphragm Details, See Sheet No. 16 of 28.
For Details of Diaphragms at the Abutments, see Sheet No. 14 of 28.

	0.4 Sp. 1 0.6 Sp. 3	Pier 1 or Pier 2	0.5 Sp. 2
I_s	(in ⁴) 2,850	2,850	2,850
$I_c(n)$	(in ⁴) 8,549	-	8,625
$I_c(3n)$	(in ⁴) 6,390	-	6,472
S_s	(in ³) 213	213	213
$S_c(n)$	(in ³) 331	-	332
$S_c(3n)$	(in ³) 300	-	301
Z	(in ³) -	-	-
DC1	(k/ft) 0.728	0.728	0.728
M _{DC1}	(k) 51	120	73
DC2	(k/ft) 0.149	0.149	0.149
M _{DC2}	(k) 11	24	15
DW	(k/ft) 0.308	0.308	0.308
M _{DW}	(k) 22	50	31
M _{Δ + Imp}	(k) 310	272	327
M _u (Strength I)	(k) 653	731	729
$\phi_r M_n, \phi_r M_{nc}$	(k) 1,741	820	1,723
f_s DC1	(ksi) 2.873	6.761	4.113
f_s DC2	(ksi) 0.620	1.352	0.845
f_s DW	(ksi) 0.880	2.817	1.236
f_s 1.3(I)	(ksi) 14.610	19.921	15.365
f_s (Service II)	(ksi) 18.983	30.851	21.559
f_s (Total)(Strength I)	(ksi) 25.354	41.183	28.735
V _r	(k) 16.2	20.0	16.8

	S. Abut. or N. Abut.	Pier 1 or Pier 2
R _{DC1}	(k) 8.7	32.5
R _{DC2}	(k) 1.8	6.7
R _{DW}	(k) 3.7	13.8
R _{Δ + Imp}	(k) 56.3	80.0
R _{Total}	(k) 117.2	209.7

- I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total-Strength I, and Service II) due to non-composite dead loads (in⁴ and in³).
- $I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total-Strength I, and Service II) due to short-term composite live loads (in⁴ and in³).
- $I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total-Strength I, and Service II) due to long-term composite (superimposed) dead loads (in⁴ and in³).
- Z: Plastic section modulus of the steel section in non-composite areas. Omit line in Moment Table if not used in design calculations (in³).
- DC1: Un-factored non-composite dead load (kips/ft.).
- M_{DC1}: Un-factored moment due to non-composite dead load (kip-ft.).
- DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).
- M_{DC2}: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).
- DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).
- M_{DW}: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).
- M_{Δ + Imp}: Un-factored live load moment plus dynamic load allowance (Impact)(kip-ft.).
- M_u (Strength I): Factored design moment (kip-ft.).
1.25 (M_{DC1} + M_{DC2}) + 1.5 M_{DW} + 1.75 M_{Δ + Imp}
- $\phi_r M_n$: Compact composite positive moment capacity computed according to Article 6.10.7.1 (kip-ft.).
- $\phi_r M_{nc}$: Non-Compact non-composite negative moment capacity computed according to Article A6.1.1 (kip-ft.).
- f_s (Service II): Sum of stresses as computed from the moments below (ksi).
M_{DC1} + M_{DC2} + M_{DW} + 1.3 M_{Δ + Imp}
- f_s (Total)(Strength I): Sum of stresses as computed from the moments below on non-compact section (ksi).
1.25 (M_{DC1} + M_{DC2}) + 1.5 M_{DW} + 1.75 M_{Δ + Imp}
- V_r: Factored shear range computed according to Article 6.10.10.

Notes:
All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchor rods.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM

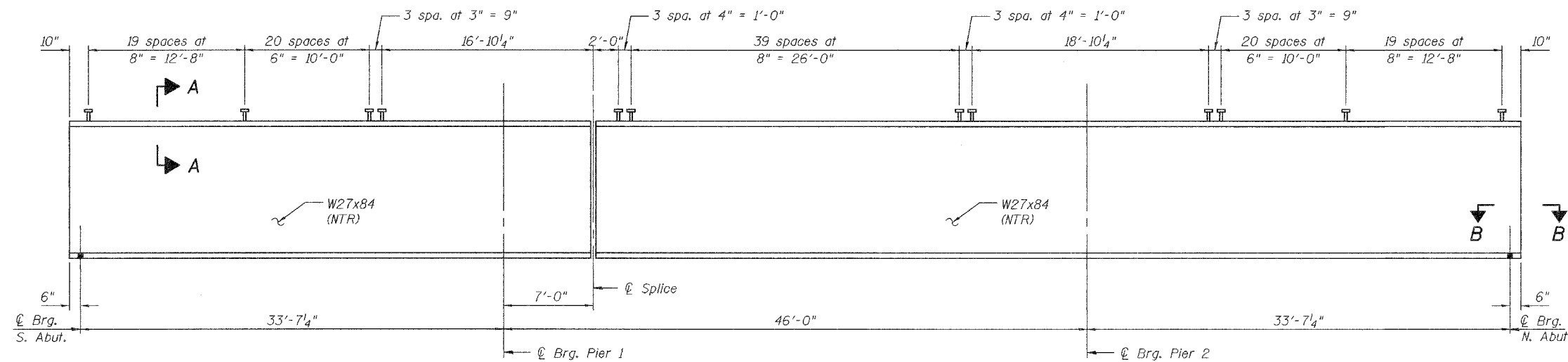


FRAMING PLAN AND DESIGN DATA
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 16 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	40	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

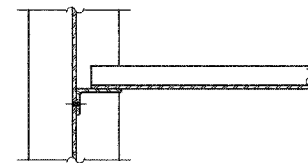


BEAM ELEVATION
(6 Required)

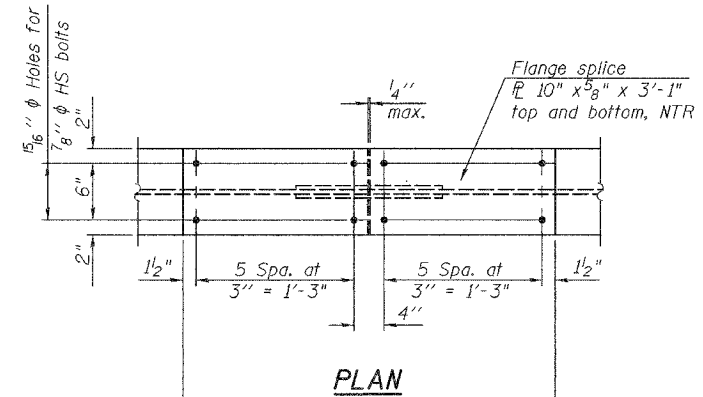
***TOP OF BEAM ELEVATIONS**

Location	Gdr. #1	Gdr. #2	Gdr. #3	Gdr. #4	Gdr. #5	Gdr. #6
℄ Brg. at South Abut.	437.87	437.52	437.16	436.81	436.46	436.11
℄ Brg. at Pier 1	437.93	437.58	437.23	436.87	436.52	436.17
℄ Splice	437.94	437.59	437.24	436.88	436.53	436.17
℄ Brg. at Pier 2	437.98	437.62	437.27	436.91	436.55	436.19
℄ Brg. at North Abut.	437.98	437.62	437.26	436.90	436.54	436.18

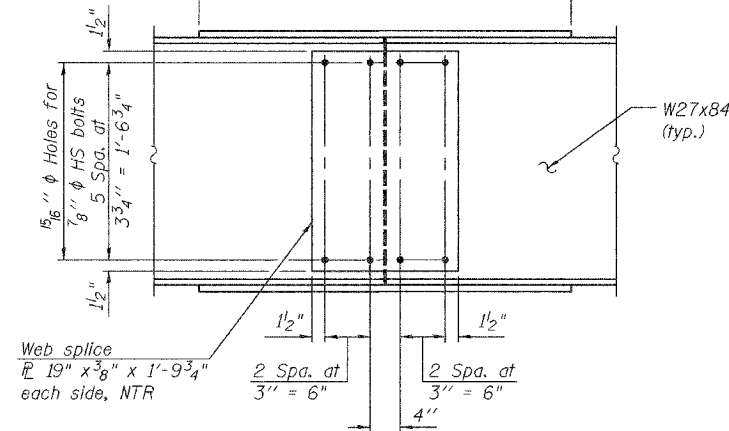
* For fabrication only



SECTION C-C



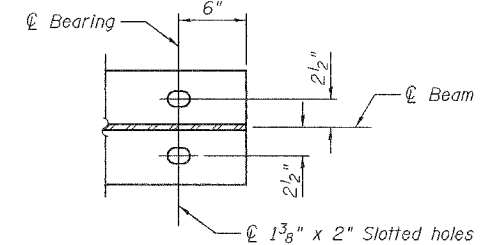
PLAN



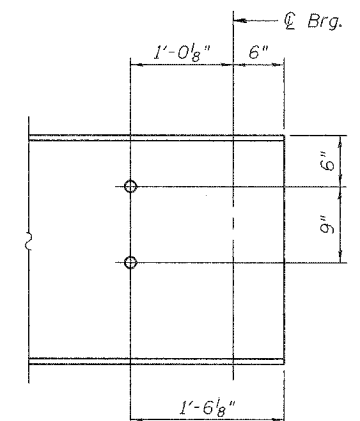
ELEVATION

SPLICE DETAIL
(6 Required)

Note:
Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.



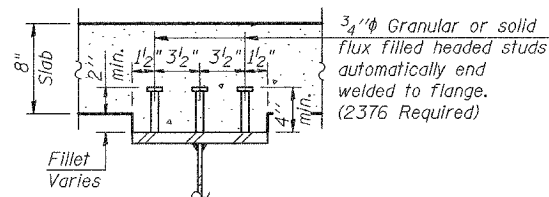
SECTION B-B
(N. Abut. end shown
S. Abut. end similar)



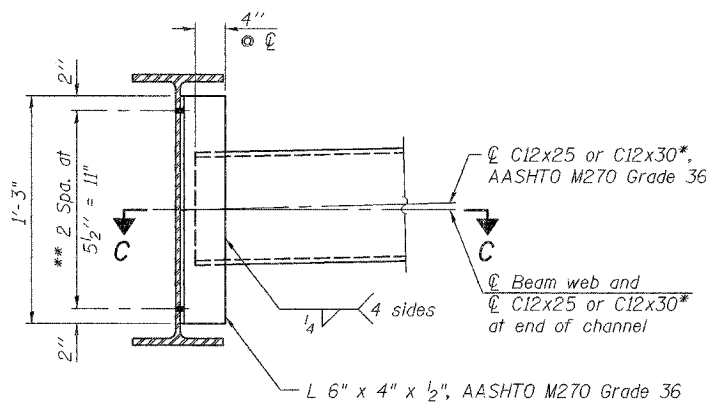
END OF GIRDER ELEVATION

(N. Abut. end shown
S. Abut. end similar)

Note:
Structural steel shall be AASHTO M270 Grade 50, except as noted.



SECTION A-A



INTERIOR DIAPHRAGM
(25 Required)

Notes:
Two hardened washers required for each set of oversized holes with one washer on each side of the connection.
See the Framing Plan on Sheet No. 15 of 28 for locations of interior diaphragms.
* Alternate channels are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section.
** 3/4" φ HS bolts, 15/16" φ holes

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



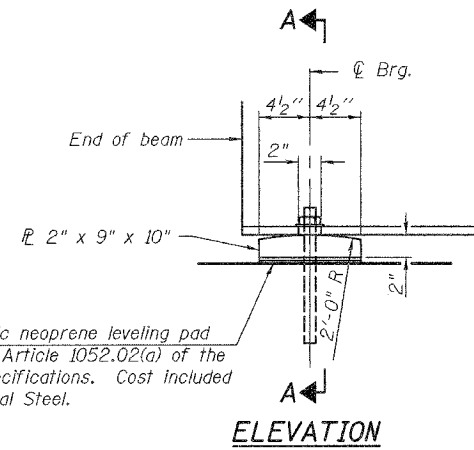
02/27/08

BEAM DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

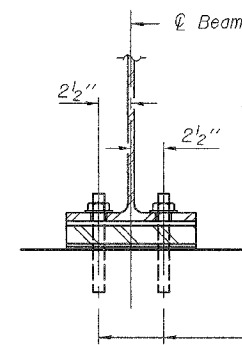
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 41	SHEET NO. 17 28 SHEETS
FED. ROAD DIST. NO. 7		FED. AID PROJECT			

Contract #76948



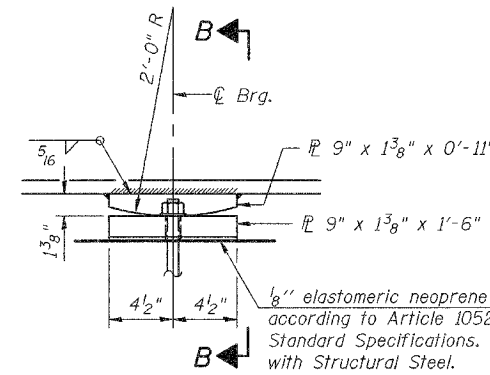
ELEVATION



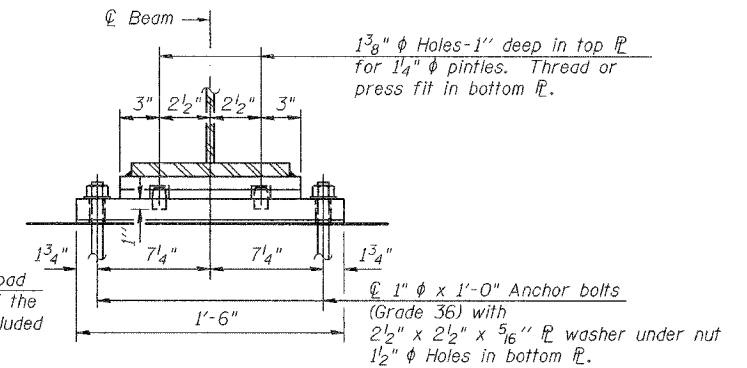
SECTION A-A

FIXED BEARING AT ABUTMENT

(12 Required)



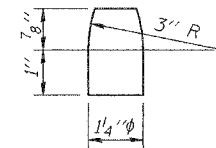
ELEVATION



SECTION B-B

FIXED BEARING AT PIER

(12 Required)



PINTLE

(24 Required)

Notes:

Bearing plates and pintles shall be AASHTO M270 Grade 50.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

BILL OF MATERIAL

Item	Unit	Total
Anchor Bolts, 1"	Each	48

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	YSS



02/27/08

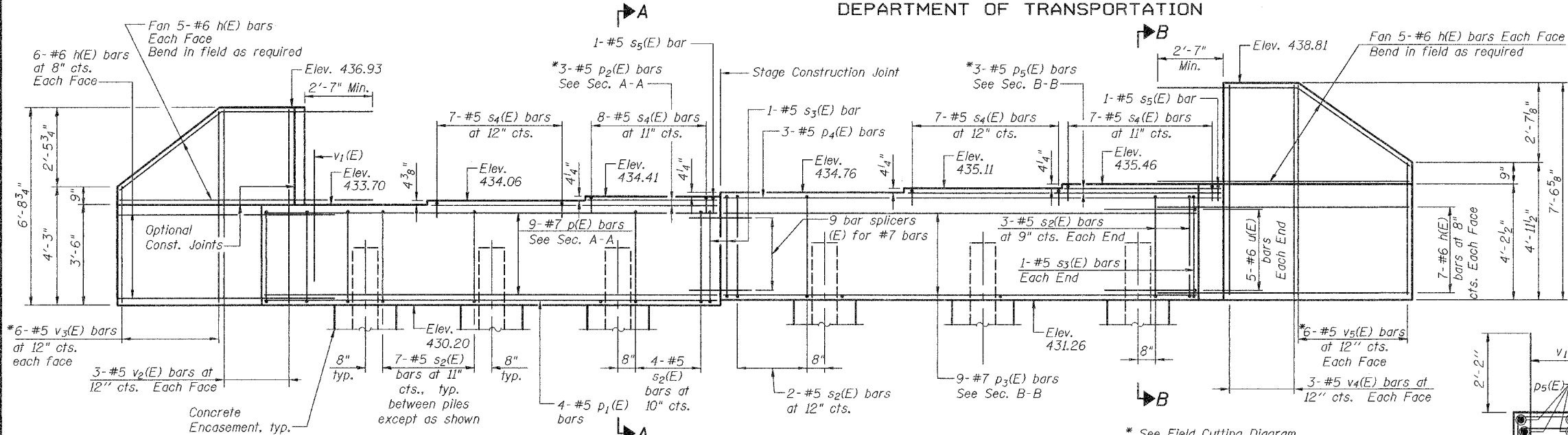
BEARING DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

Notes: Four steps monolithically with cap.
Space reinforcement in cap to miss anchor bolts.

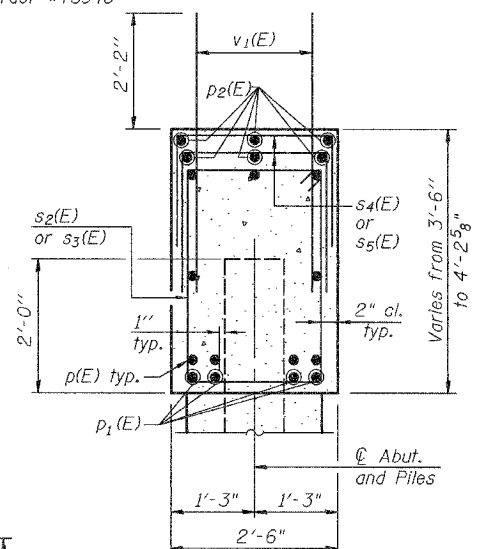
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	42	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948



ELEVATION



SEC. A-A

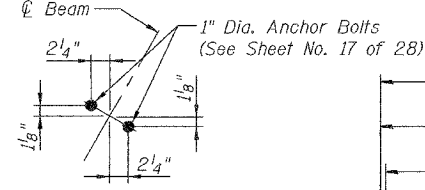
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	46	#6	11'-7"	
p(E)	9	#7	20'-6"	
p1(E)	4	#5	20'-6"	
p2(E)	3	#5	21'-2"	
p3(E)	9	#7	19'-3"	
p4(E)	3	#5	19'-3"	
p5(E)	3	#5	19'-2"	
s2(E)	40	#5	11'-7"	
s3(E)	4	#5	12'-1"	
s4(E)	29	#5	6'-6"	
s5(E)	2	#5	6'-9"	
u(E)	10	#6	9'-5"	
v1(E)	82	#5	4'-4"	
v2(E)	6	#5	6'-5"	
v3(E)	6	#5	10'-3"	
v4(E)	6	#5	7'-3"	
v5(E)	6	#5	11'-9"	
Structure Excavation	Cu. Yd.	159		
Concrete Structures	Cu. Yd.	18.4		
Reinforcement Bars, Epoxy Coated	Pound	3,290		
Furnishing Steel Piles HP 12x53	Foot	265		
Driving Piles	Foot	265		
Test Pile Steel HP 12x53	Each	1		
Concrete Encasement	Cu. Yd.	2.1		

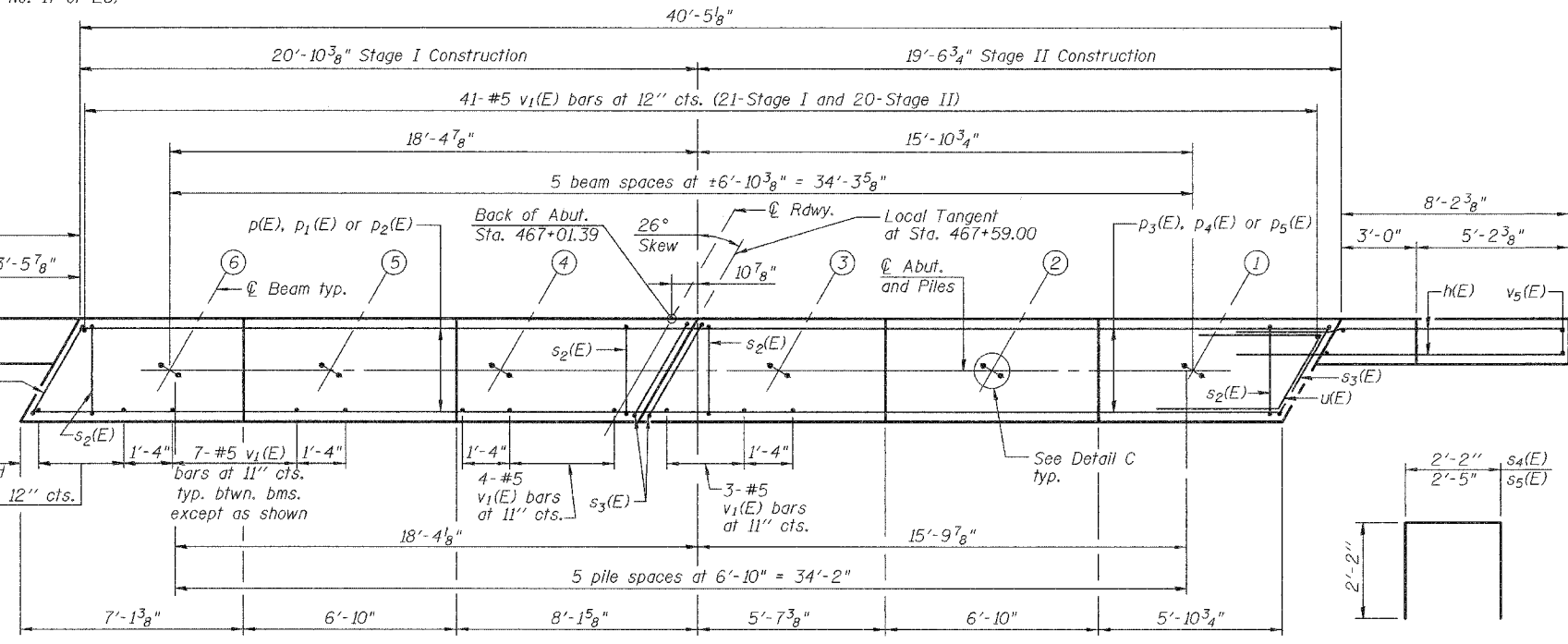
For details of Bar Splicers, see Sheet No. 24 of 28.
For details of piles and Concrete Encasement, see Sheet No. 23 of 28.

SOUTH ABUTMENT DETAILS

IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

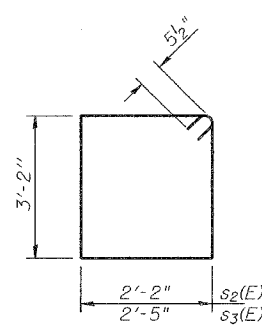


DETAIL C

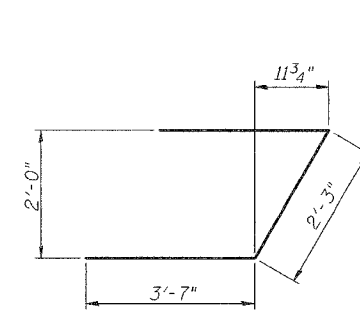


PLAN

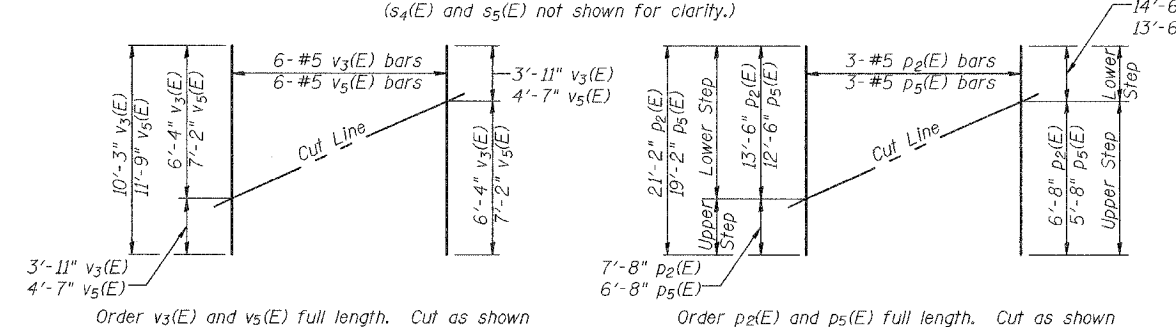
BARS s4(E) & s5(E)



BARS s2(E) & s3(E)



BAR u(E)



FIELD CUTTING DIAGRAM

PILE DATA

Type: Steel HP 12x53
Nominal Required Bearing: 340 kips
Factored Resistance Available: 170 kips
Est. Length: 53 ft
No. Production Piles: 5
No. Test Piles: 1

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	YSS



02/27/08

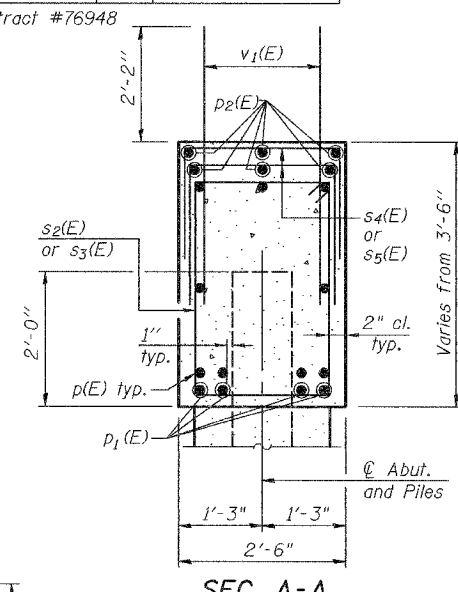
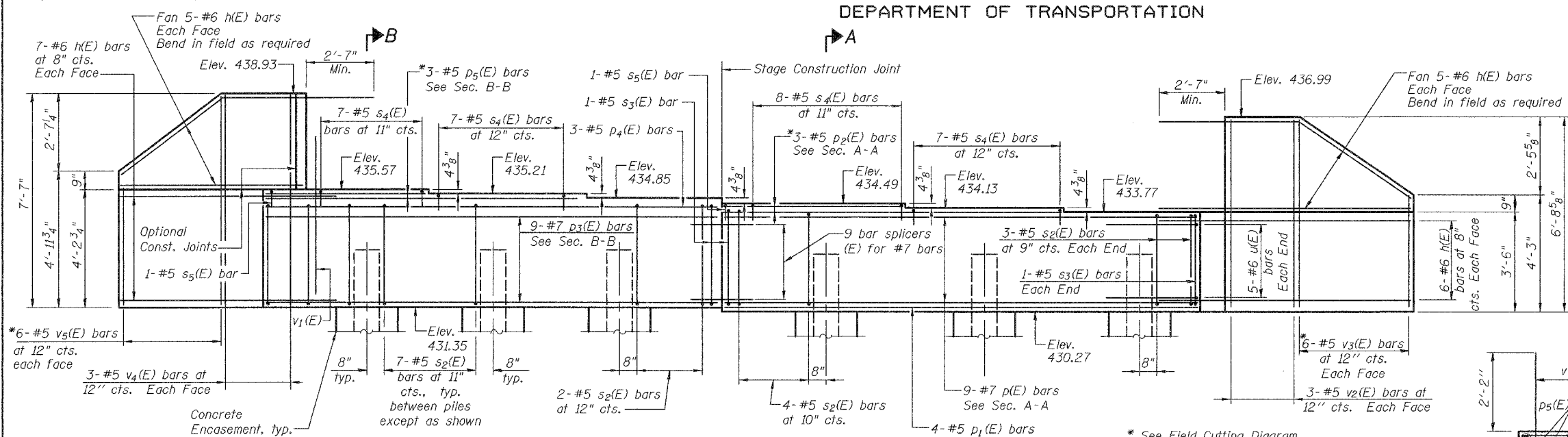
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	48	43
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

SHEET NO. 19
28 SHEETS

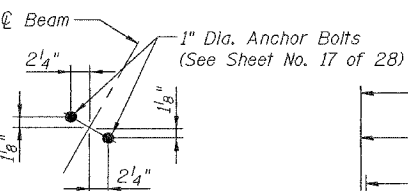
Contract #76948

Notes: Four steps monolithically with cap.
Space reinforcement in cap to miss anchor bolts.

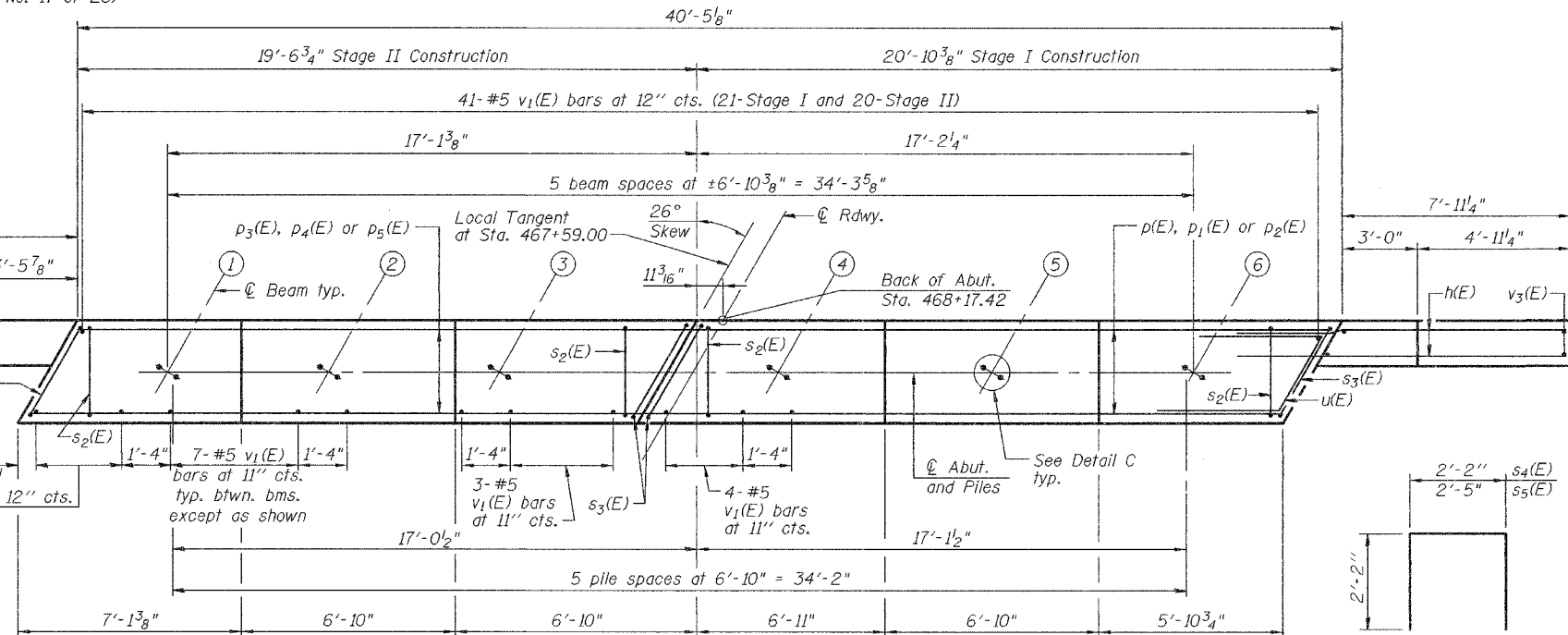


SEC. A-A
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	46	#6	11'-7"	
p(E)	9	#7	20'-6"	
p1(E)	4	#5	20'-6"	
p2(E)	3	#5	21'-2"	
p3(E)	9	#7	19'-3"	
p4(E)	3	#5	19'-3"	
p5(E)	3	#5	19'-2"	
s2(E)	40	#5	11'-7"	
s3(E)	4	#5	12'-1"	
s4(E)	29	#5	6'-6"	
s5(E)	2	#5	6'-9"	
u(E)	10	#6	9'-5"	
v1(E)	82	#5	4'-4"	
v2(E)	6	#5	6'-5"	
v3(E)	6	#5	10'-3"	
v4(E)	6	#5	7'-3"	
v5(E)	6	#5	11'-9"	
Structure Excavation		Cu. Yd.	158	
Concrete Structures		Cu. Yd.	18.5	
Reinforcement Bars, Epoxy Coated		Pound	3,290	
Furnishing Steel Piles HP 12x53		Foot	310	
Driving Piles HP 12x53		Foot	310	
Test Pile Steel HP 12x53		Each	1	
Concrete Encasement		Cu. Yd.	2.1	

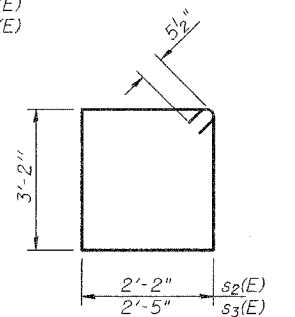


DETAIL C

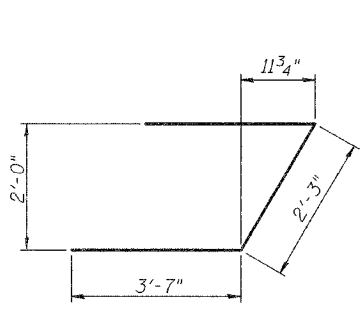


PLAN

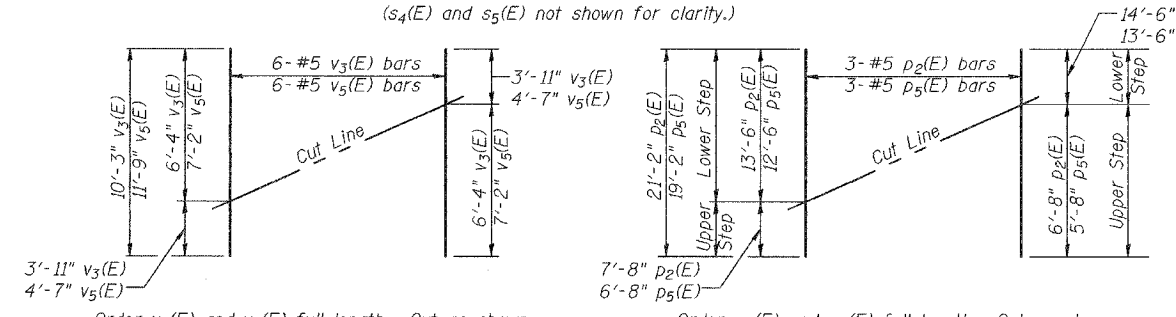
BARS s4(E) & s5(E)



BARS s2(E) & s3(E)



BAR u(E)



FIELD CUTTING DIAGRAM

PILE DATA

Type: Steel HP 12x53
Nominal Required Bearing: 340 kips
Factored Resistance Available: 170 kips
Est. Length: 62 ft
No. Production Piles: 5
No. Test Piles: 1

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	YSS



02/27/08

For details of Bar Splicers, see Sheet No. 24 of 28.
For details of piles and Concrete Encasement, see Sheet No. 23 of 28.

NORTH ABUTMENT DETAILS

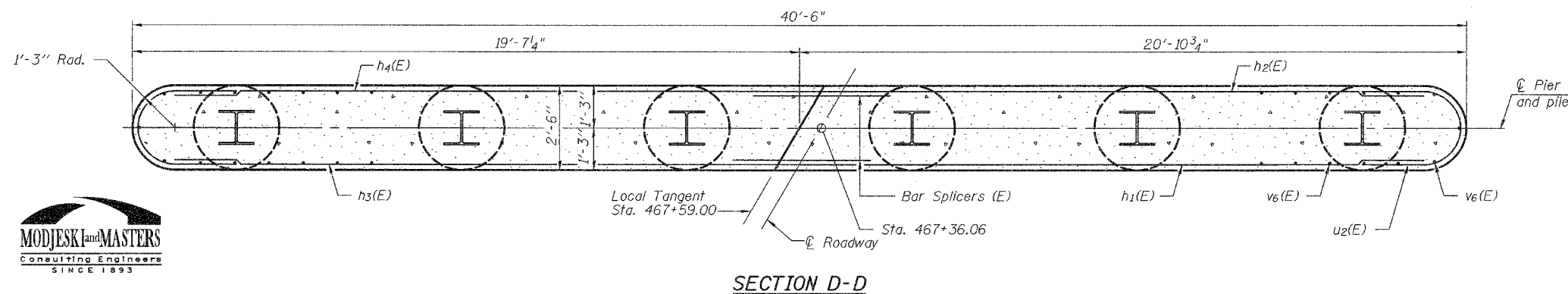
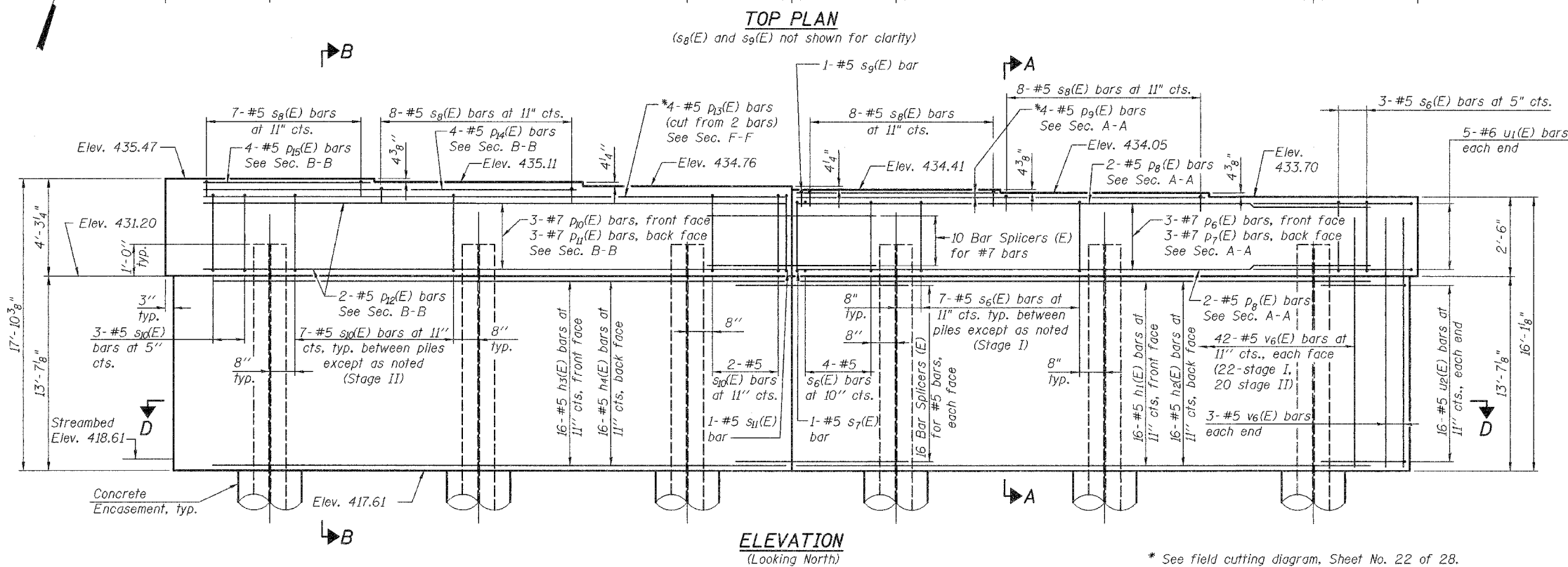
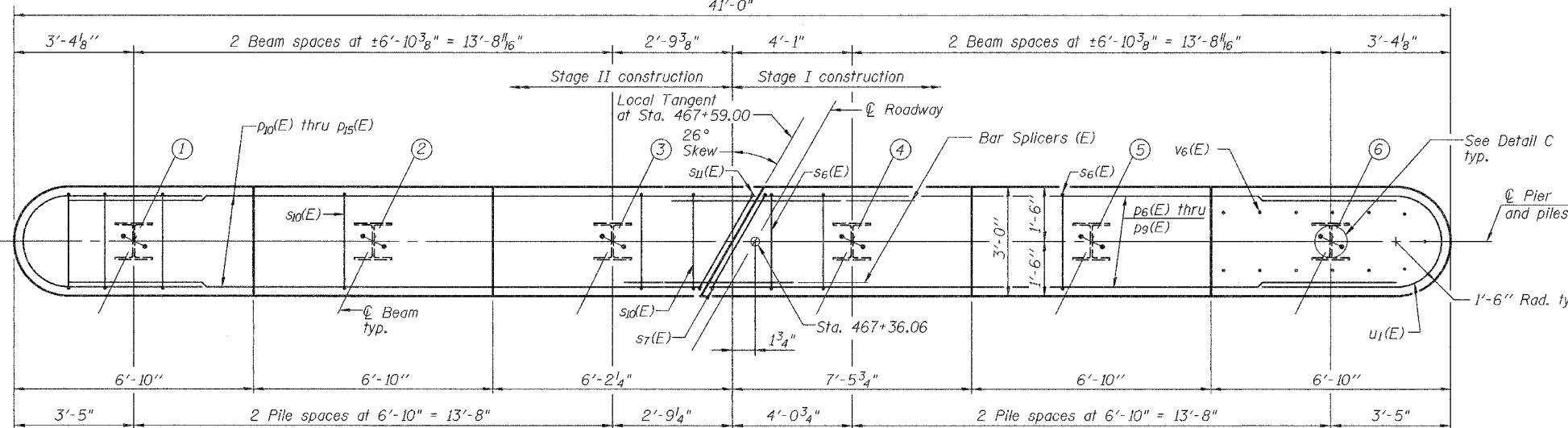
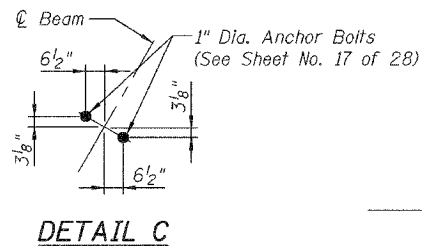
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
41'-0"

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 44	SHEET NO. 20 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #76948

Notes:
Pour steps monolithically with cap.
Space reinforcement in cap to miss anchor bolts.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	16	#5	20'-0"	—
h2(E)	16	#5	19'-0"	—
h3(E)	16	#5	17'-9"	—
h4(E)	16	#5	18'-9"	—
p6(E)	3	#7	20'-1"	—
p7(E)	3	#7	18'-11"	—
p8(E)	4	#7	19'-4"	—
p9(E)	*4	#5	21'-4"	—
p10(E)	3	#7	17'-7"	—
p11(E)	3	#7	18'-10"	—
p12(E)	4	#7	18'-0"	—
p13(E)	*2	#5	36'-5"	—
p14(E)	4	#5	12'-1"	—
p15(E)	4	#5	5'-3"	—
s6(E)	21	#5	10'-7"	□
s7(E)	1	#5	11'-3"	□
s8(E)	31	#5	7'-0"	□
s9(E)	1	#5	7'-4"	□
s10(E)	19	#5	12'-9"	□
s11(E)	1	#5	13'-5"	□
u1(E)	10	#6	11'-1"	U
u2(E)	32	#5	10'-7"	U
v6(E)	90	#5	15'-1"	—
Structure Excavation			Cu. Yd.	70
Concrete Structures			Cu. Yd.	65.5
Reinforcement Bars, Epoxy Coated			Pound	4,950
Furnishing Steel Piles HP 14x89			Foot	310
Driving Piles			Foot	310
Test Pile Steel HP 14x89			Each	1
Concrete Encasement Underwater Structure			Cu. Yd.	3.3
Excavation Protection Location #1			Each	1

For details of bar splicer, see Sheet No. 24 of 28.
For details of piles and concrete encasement, see Sheet No. 23 of 28.
For details of Section A-A and B-B, see Sheet No. 22 of 28.
For bar dimensions, see Sheet No. 22 of 28.

PILE DATA

Type: Steel HP 14x89
Nominal Required Bearing: 600 kips
Allowable Resistance Available: 240 kips
Est. Length: 62 ft
No. Production Piles: 5
No. Test Piles: 1

PIER 1 DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



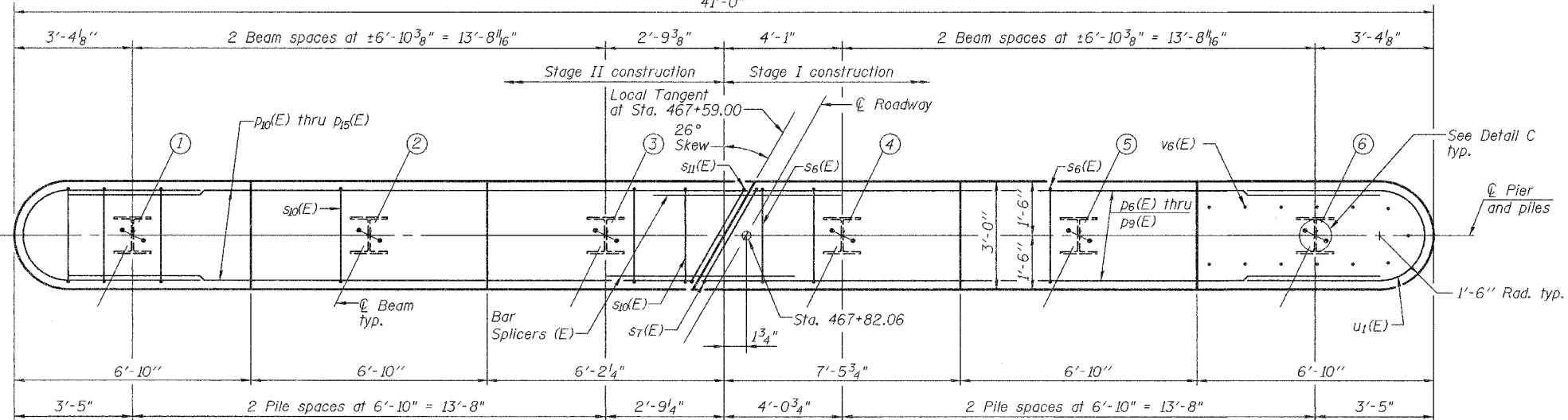
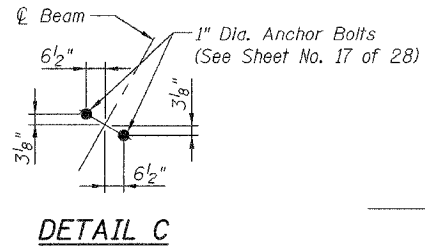
02/27/08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
41'-0"

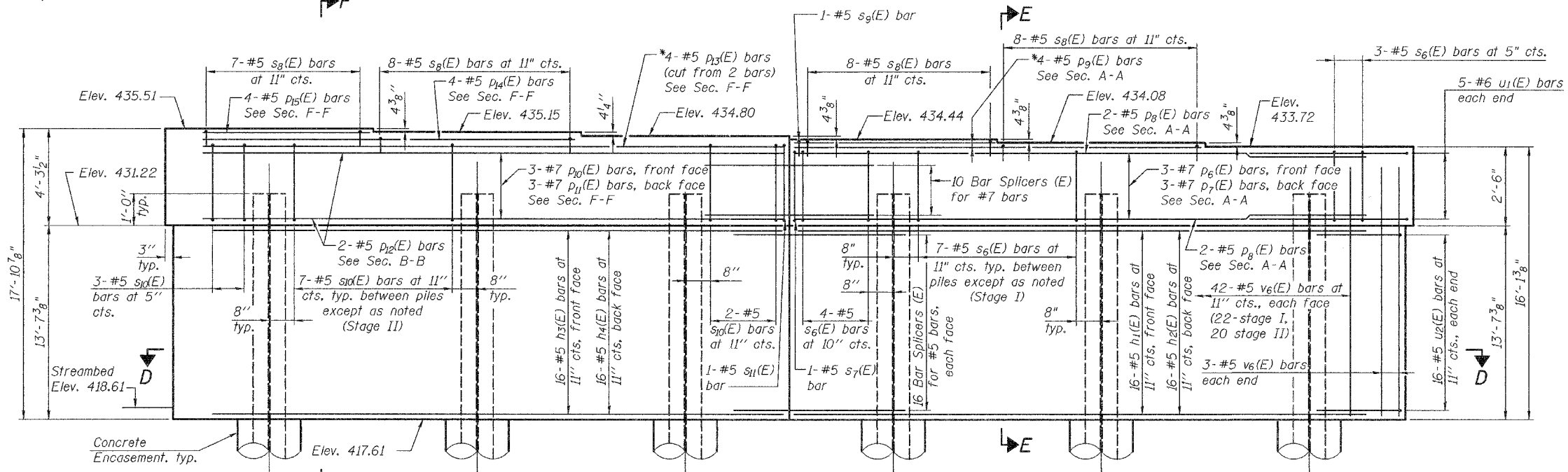
ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	45	28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76948

Notes:
Pour steps monolithically with cap.
Space reinforcement in cap to miss anchor bolts.

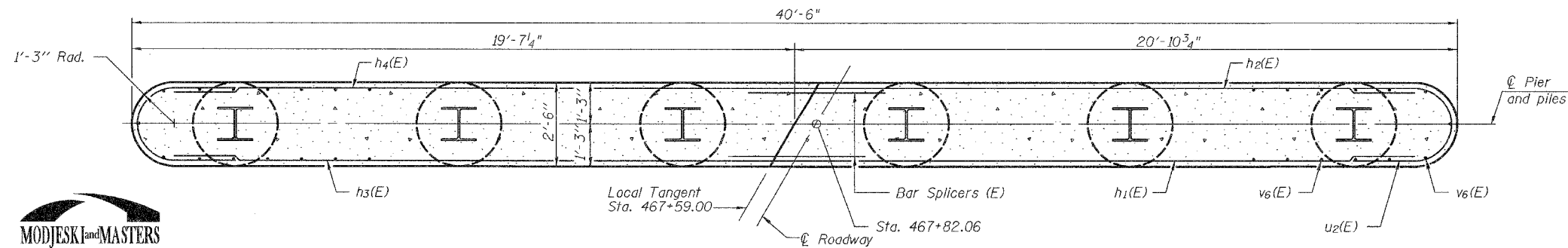


TOP PLAN
(s8(E) and s9(E) not shown for clarity)



ELEVATION
(Looking North)

* See field cutting diagram, Sheet No. 22 of 28.



SECTION D-D

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1(E)	16	#5	20'-0"	
h2(E)	16	#5	19'-0"	
h3(E)	16	#5	17'-9"	
h4(E)	16	#5	18'-9"	
p6(E)	3	#7	20'-1"	
p7(E)	3	#7	18'-11"	
p8(E)	4	#7	19'-4"	
p9(E)	*4	#5	21'-4"	
p10(E)	3	#7	17'-7"	
p11(E)	3	#7	18'-10"	
p12(E)	4	#7	18'-0"	
p13(E)	*2	#5	36'-5"	
p14(E)	4	#5	12'-1"	
p15(E)	4	#5	5'-3"	
s6(E)	21	#5	10'-7"	
s7(E)	1	#5	11'-3"	
s8(E)	31	#5	7'-0"	
s9(E)	1	#5	7'-4"	
s10(E)	19	#5	12'-9"	
s11(E)	1	#5	13'-5"	
u1(E)	10	#6	11'-1"	
u2(E)	32	#5	10'-7"	
v6(E)	90	#5	15'-1"	
Structure Excavation	Cu. Yd.		37	
Concrete Structures	Cu. Yd.		65.6	
Reinforcement Bars, Epoxy Coated	Pound		4,950	
Furnishing Steel Piles HP 14x89	Foot		310	
Driving Piles	Foot		310	
Test Pile Steel HP 14x89	Each		1	
Concrete Encasement Underwater Structure Excavation Protection-Location #2	Cu. Yd.		3.3	
	Each		1	

For details of bar splicer, see Sheet No. 24 of 28.
For details of piles and concrete encasement, see Sheet No. 23 of 28.
For details of Section E-E and F-F, see Sheet No. 22 of 28.
For bar dimensions, see Sheet No. 22 of 28.

PILE DATA

Type: Steel HP 14x89
Nominal Required Bearing: 600 kips
Allowable Resistance Available: 240 kips
Est. Length: 62 ft
No. Production Piles: 5
No. Test Piles: 1

PIER 2 DETAILS
IL. ROUTE 100 OVER

CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



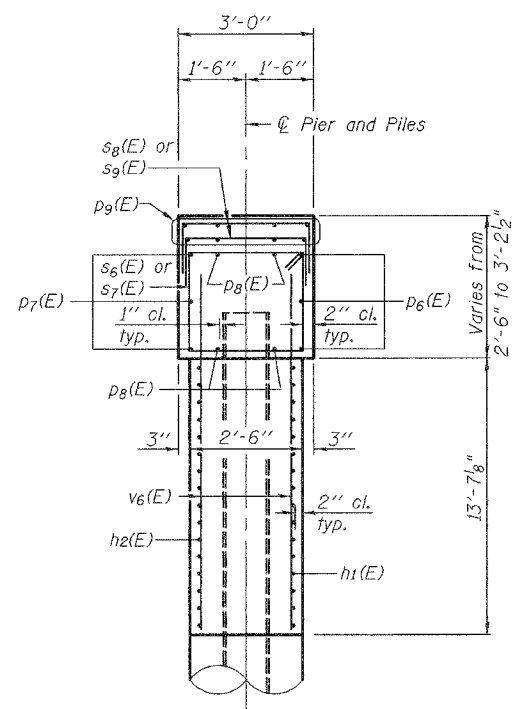
02/27/08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

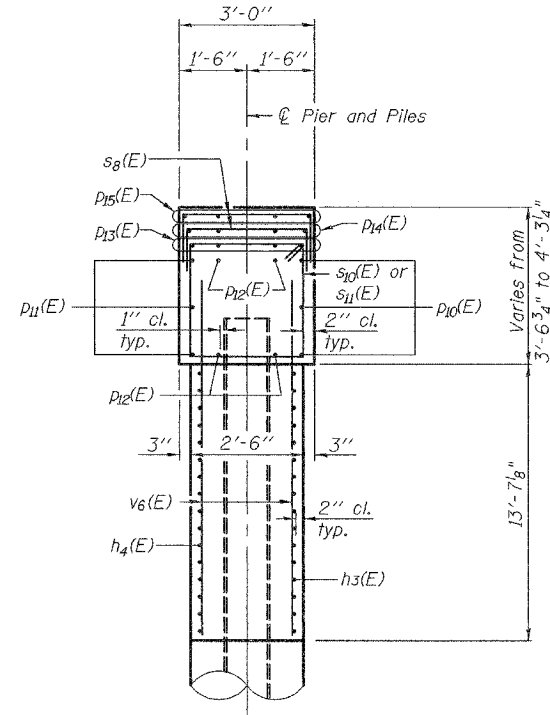
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	46
FED. ROAD DIST. NO. 7		FED. AID PROJECT		

Contract #76948

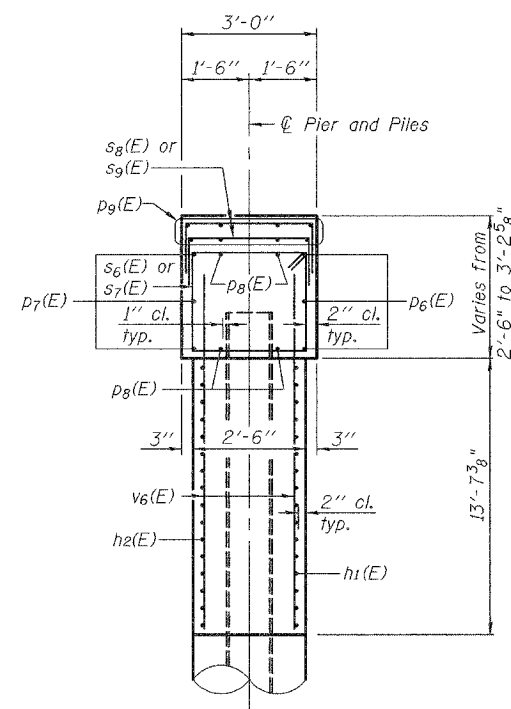
SHEET NO. 22
28 SHEETS



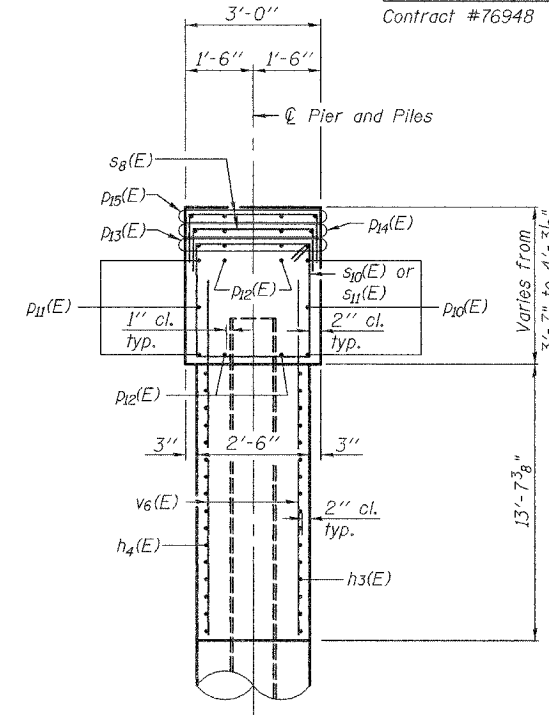
SECTION A-A



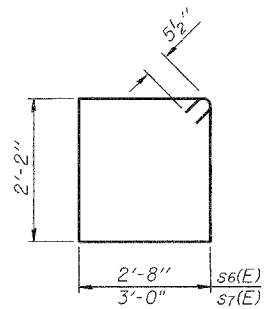
SECTION B-B



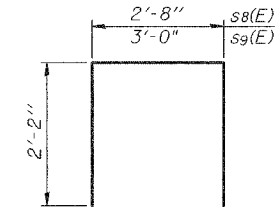
SECTION E-E



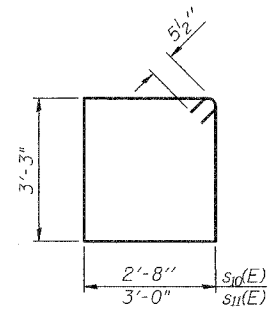
SECTION F-F



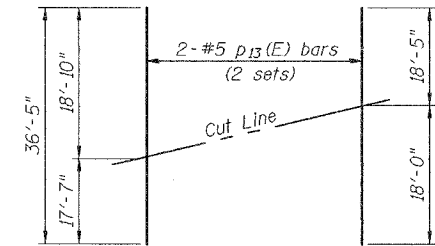
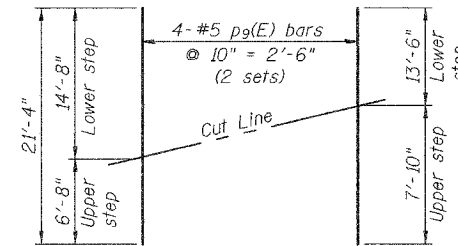
BARS s6(E) & s7(E)



BARS s8(E) & s9(E)



BARS s10(E) & s11(E)

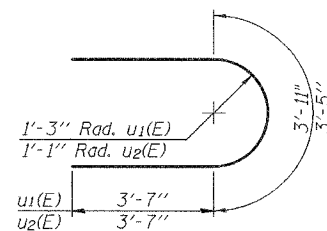


Order p9(E) full length. Cut as shown and place one set of bars as shown in Section A-A and one set of bars as shown in Section E-E (Stage I Constr.).

Order p13(E) full length. Cut as shown and place one set of bars as shown in Section B-B and one set of bars as shown in Section F-F (Stage II Constr.).

FIELD CUTTING DIAGRAM

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



BARS u1(E) & u2(E)

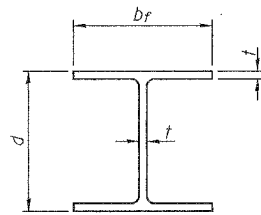
02/27/08

MISCELLANEOUS PIER DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

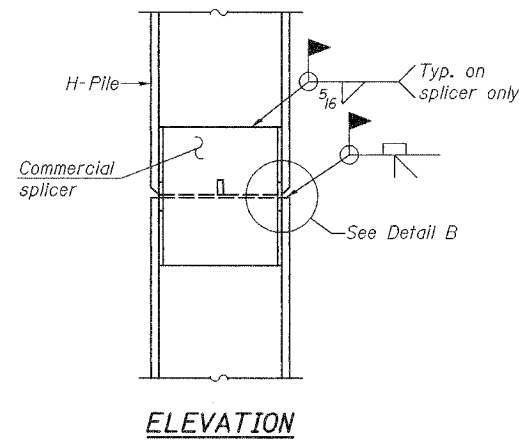
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 23 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	47	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

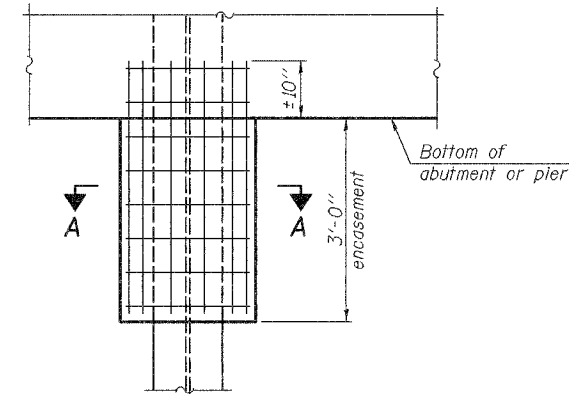


STEEL PILE TABLE

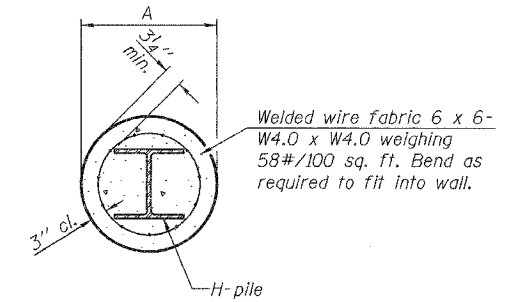
Designation	Depth d	Flange width bf	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION



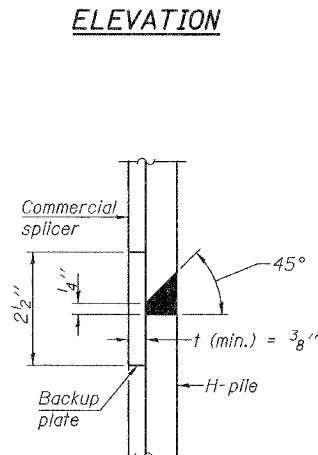
ELEVATION



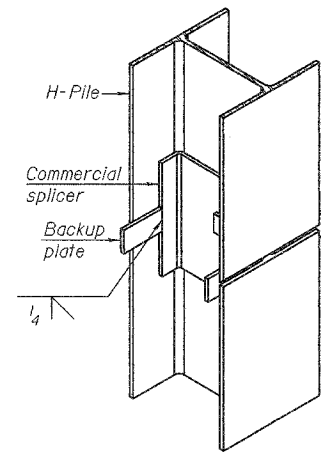
SECTION A-A

Note:
Forms for encasement may be omitted when soil conditions permit.

PILE ENCASEMENT

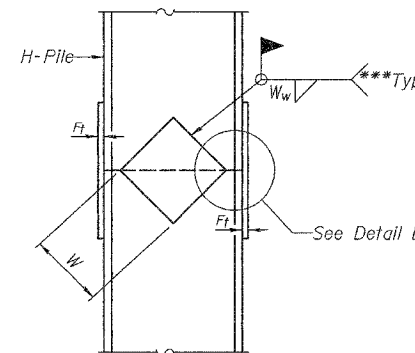


DETAIL "B"

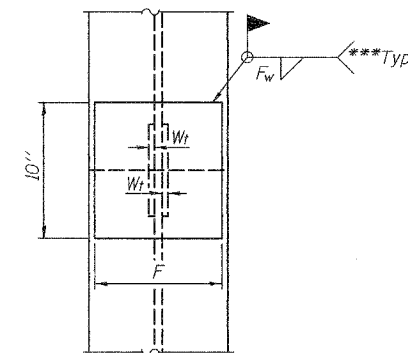


ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE

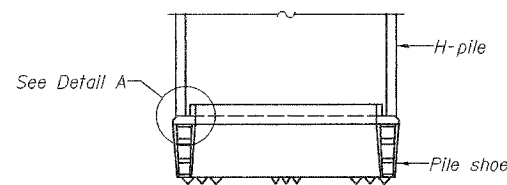


ELEVATION

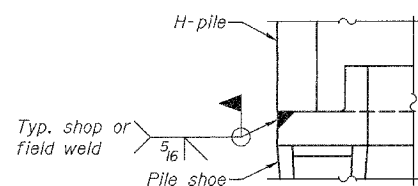


END VIEW

WELDED PLATE FIELD SPLICE

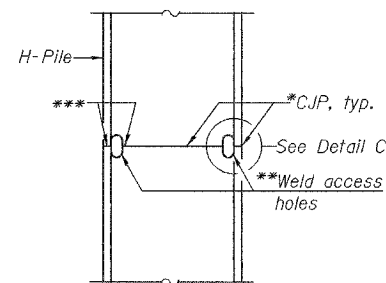


ELEVATION

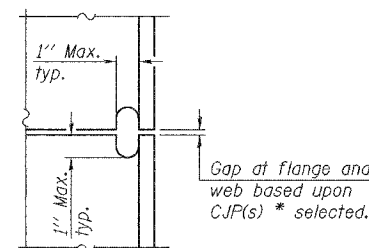


DETAIL A

H-PILE SHOE ATTACHMENT

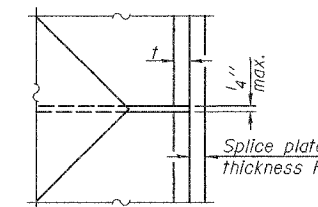


ELEVATION



DETAIL C

COMPLETE PENETRATION WELD SPLICE



DETAIL D

Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

STEEL H-PILE DETAILS

IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



F-HP 9-3-07

*Use joint conforming to Figure 3.4 in AWS D1.1, Structure Welding Code - Steel.

**Preparation per Fig. 5.2 in AWS D1.1, Structure Welding Code - Steel.

***Interrupt welds 1/4" from end of each pile.

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

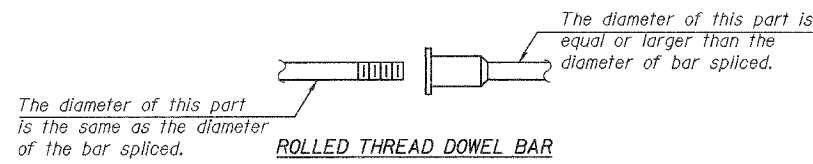
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	48	48	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

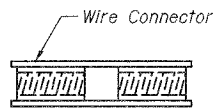
- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_t$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_t = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete



ROLLED THREAD DOWEL BAR



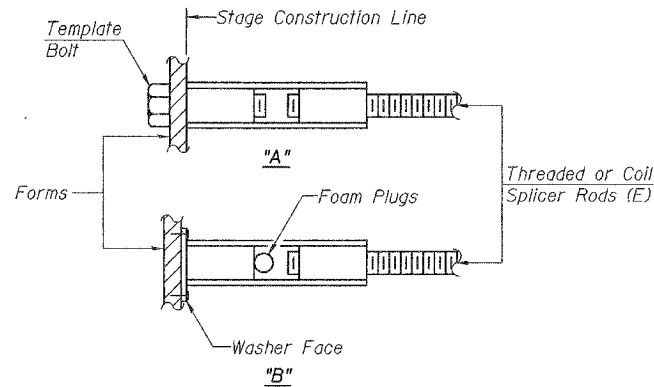
** ONE PIECE



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

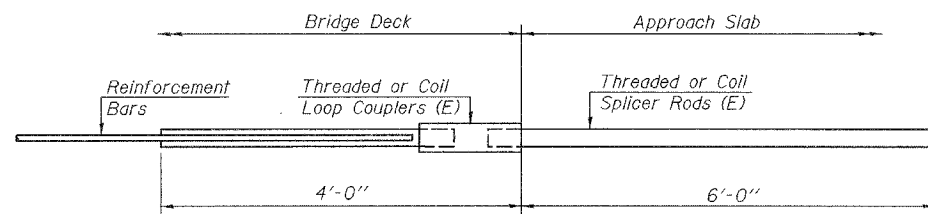
**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

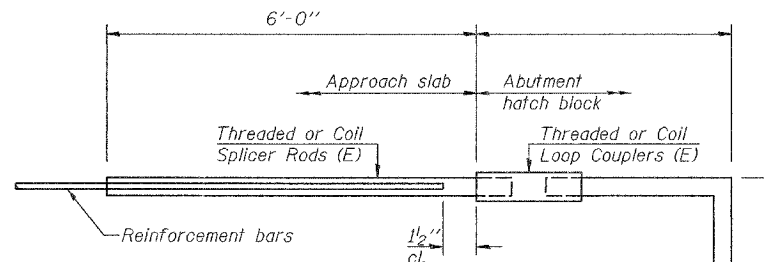
"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



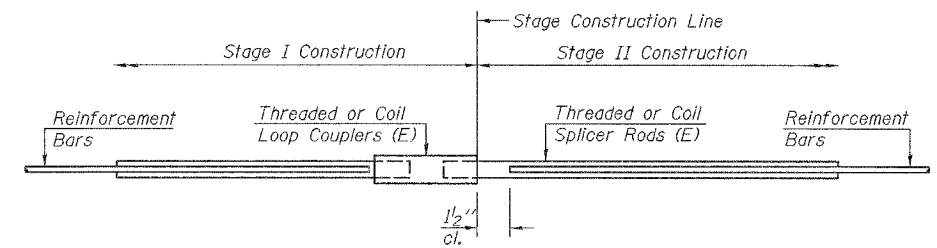
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 66



FOR STUB ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 0



STANDARD

Bar Size	No. Assemblies Required	Location
#5	349	Slab
#7	9	S. Abut.
#7	9	N. Abut.
#5	32	Pier 1
#7	10	Pier 1
#5	32	Pier 2
#7	10	Pier 2

**BAR SPLICER ASSEMBLY DETAILS
IL. ROUTE 100 OVER
CRAWFORD CREEK**

F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



BSD-1

11-1-06

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 25 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	49	
FED. ROAD DIST. NO. 7	ILLINOIS		FED. AID PROJECT		

Contract #76948

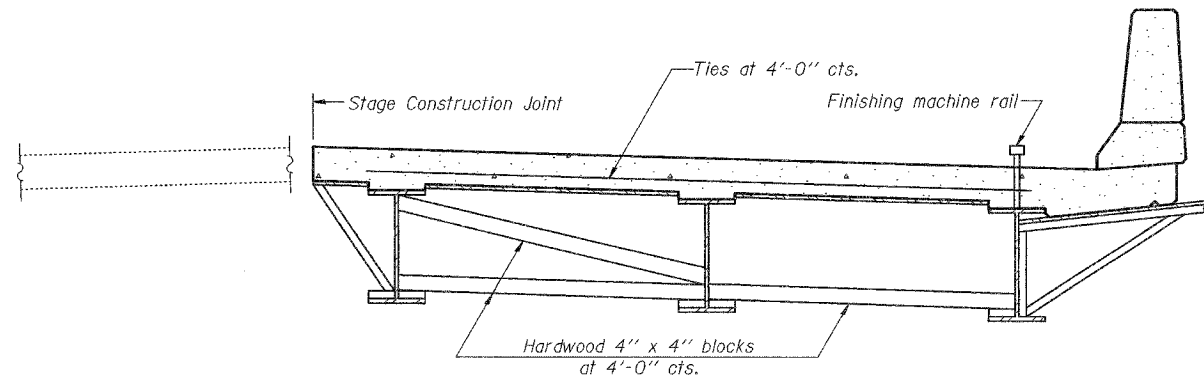
When cantilever forming brackets are used, the work shall be done according to Article 503.06(b) of the Standard Specifications, except as modified below and in the details shown on this sheet.

The finishing machine rails shall be placed on the top flange of the exterior beams.

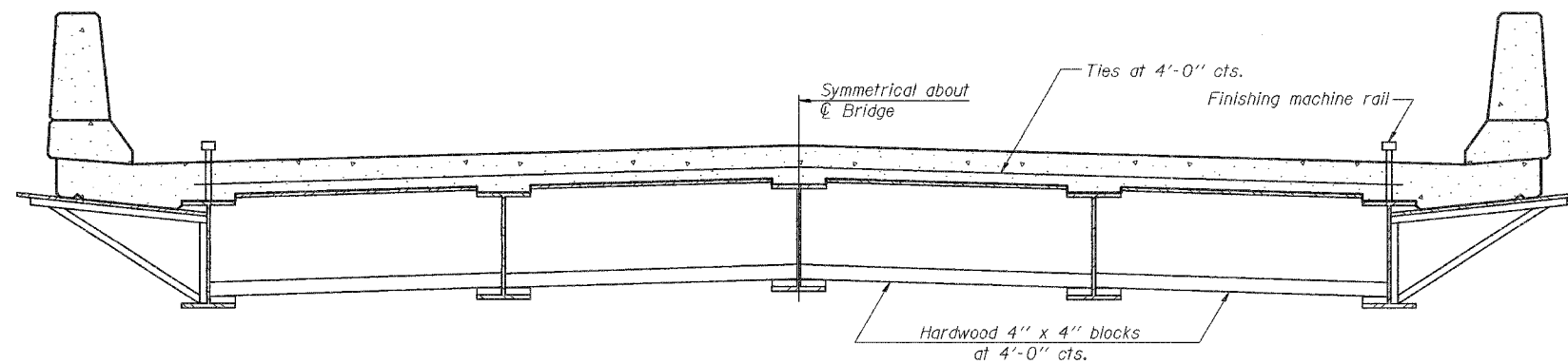
The beams or girders, supporting cantilever forming brackets, shall be tied together at 4 foot intervals.

For Standard construction, or Stage Construction the Hardwood bracing materials shall be placed as shown between webs of beams in each bay.

See Sheet No. 4 of 28 for Stage Construction Details.



**FORM BRACES FOR
STAGE CONSTRUCTION**



**FORM BRACES FOR
STANDARD CONSTRUCTION**

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



SB-1

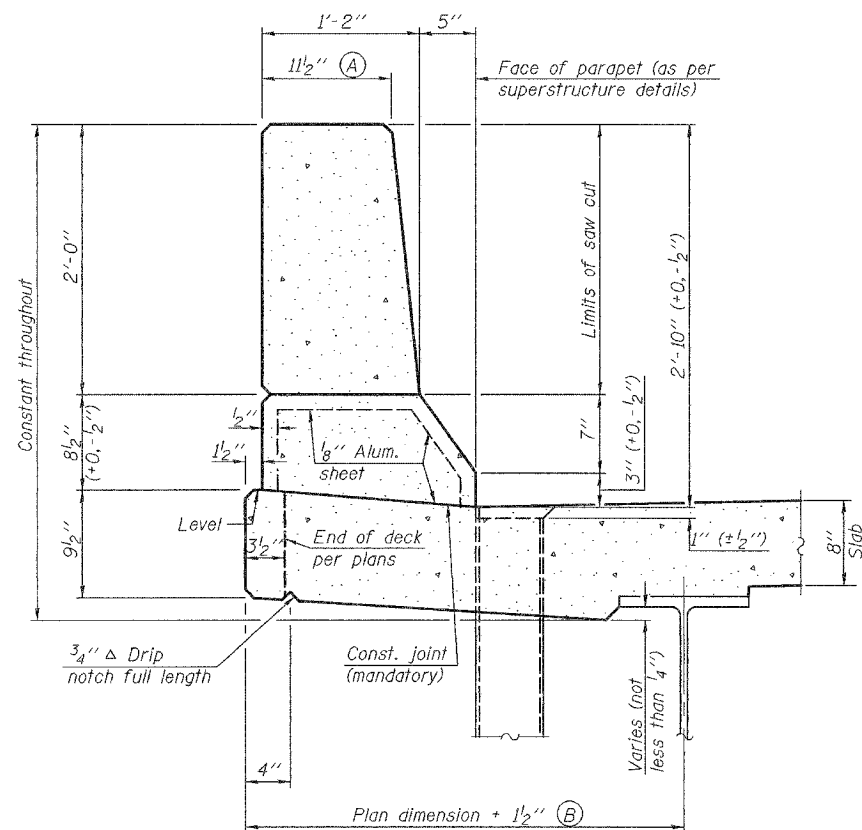
11-1-06

**CANTILEVER FORMING BRACKETS
FOR SUPERSTRUCTURE
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027**

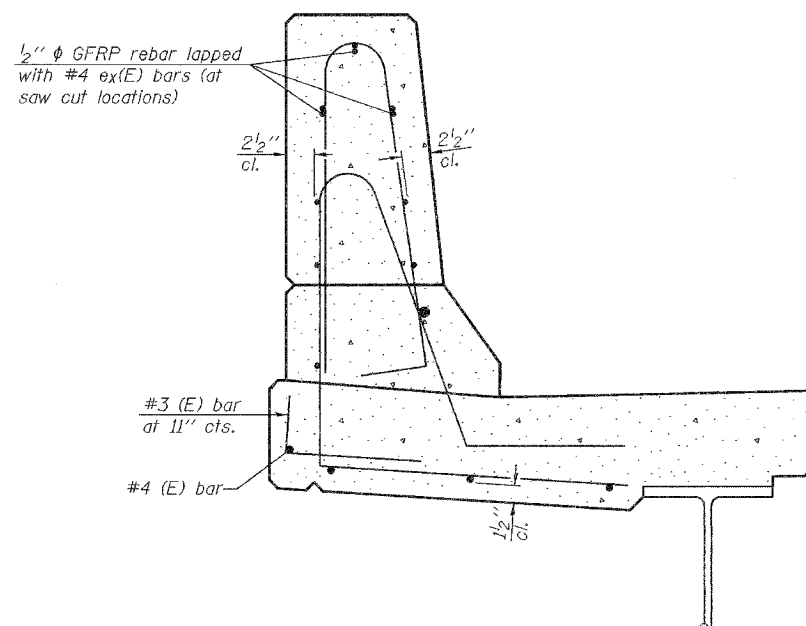
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 08	SHEET NO. 50	SHEET NO. 26 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT			

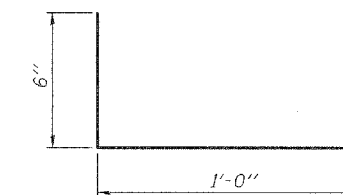
Contract #76948



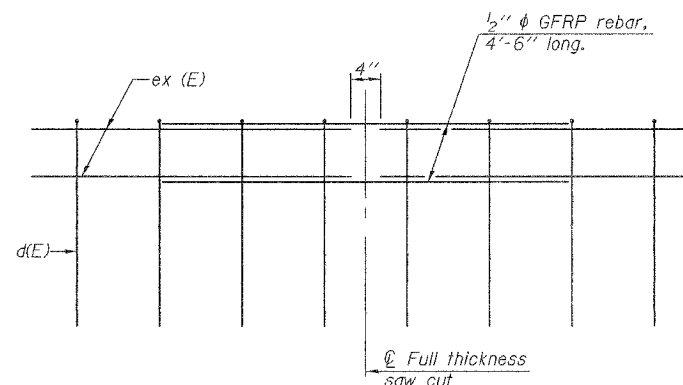
SECTION
(Showing dimensions)



SECTION
(Showing reinforcement clearances for slip forming and additional reinforcement bars)



#3 (E) BAR



GFRP REBAR STIFFENING DETAIL
(Place as shown in parapet section at each parapet joint location.)

CONCRETE PARAPET
SLIPFORMING OPTION
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



SFP-34

9-3-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 28 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	60	52	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT		

Contract #76948



Illinois Department of Transportation
Division of Highways
State Department of Transportation

SOIL BORING LOG

Page 1 of 2

Date 11/20/06

ROUTE FAP 304 DESCRIPTION IL 100 over Crawford Creek LOGGED BY E. Stewart

SECTION 5BR-2 LOCATION SW 1/4, SEC. 11, TWP. 05, RNG. 2W, 4 PM

COUNTY Calhoun DRILLING METHOD Hollow Stem Auger HAMMER TYPE 140# Automatic

STRUCT. NO. 007-0004 (E) / 007-0027 (P)
Station 467+55
BORING NO. SB 2 N. Abut
Station 469+43.5
Offset 14.500 Left
Ground Surface Elev. 436.5 ft

D E P T H H S	B L O W S	U C S	M O S	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev. ft	First Encounter Upon Completion After Hrs.	D E P T H H S	B L O W S	U C S	M O S	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev. ft	First Encounter Upon Completion After Hrs.
4	4	0.51	19					3	0.48	33					
5	5	S/20						3	B/20						
6	4	0.58	20					2	0.30	32					
7	5	G/15						3	B/20						
8	6							3							
9	7	1.88	18					2	0.88	25					
10	6	S/20						1	0/20						
11	5							3							
12	7	0.82	21					2	0.48	25					
13	8	S/15						3	B/20						
14	4							4							
15	4	0.59	24					3	B/20						
16	3							4							
17	5	0.59	25					4							
18	5	B/20						3	NC						
19	3							4							
20	3	0.45	31					16							
21	2	B/20						30	1.16	8					
22	2	0.30	32					25	S/15						
23	3														
24	3														

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS Form 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
State Department of Transportation

SOIL BORING LOG

Page 2 of 2

Date 11/20/06

ROUTE FAP 304 DESCRIPTION IL 100 over Crawford Creek LOGGED BY E. Stewart

SECTION 5BR-2 LOCATION SW 1/4, SEC. 11, TWP. 05, RNG. 2W, 4 PM

COUNTY Calhoun DRILLING METHOD Hollow Stem Auger HAMMER TYPE 140# Automatic

STRUCT. NO. 007-0004 (E) / 007-0027 (P)
Station 467+55
BORING NO. SB 2 N. Abut
Station 469+43.5
Offset 14.500 Left
Ground Surface Elev. 436.5 ft

D E P T H H S	B L O W S	U C S	M O S	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev. ft	First Encounter Upon Completion After Hrs.	D E P T H H S	B L O W S	U C S	M O S	Surface Water Elev. ft	Stream Bed Elev. ft	Groundwater Elev. ft	First Encounter Upon Completion After Hrs.
1	3							3	0.48	33					
2	3	B/20						3	B/20						
3	3							3							
4	2	0.30	32					2	0.30	32					
5	3	B/20						3	B/20						
6	3							3							
7	3							3							
8	3							3							
9	3							3							
10	3							3							
11	3							3							
12	3							3							
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58	3							3							
59	3							3							
60	3							3							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS Form 137 (Rev. 8-99)

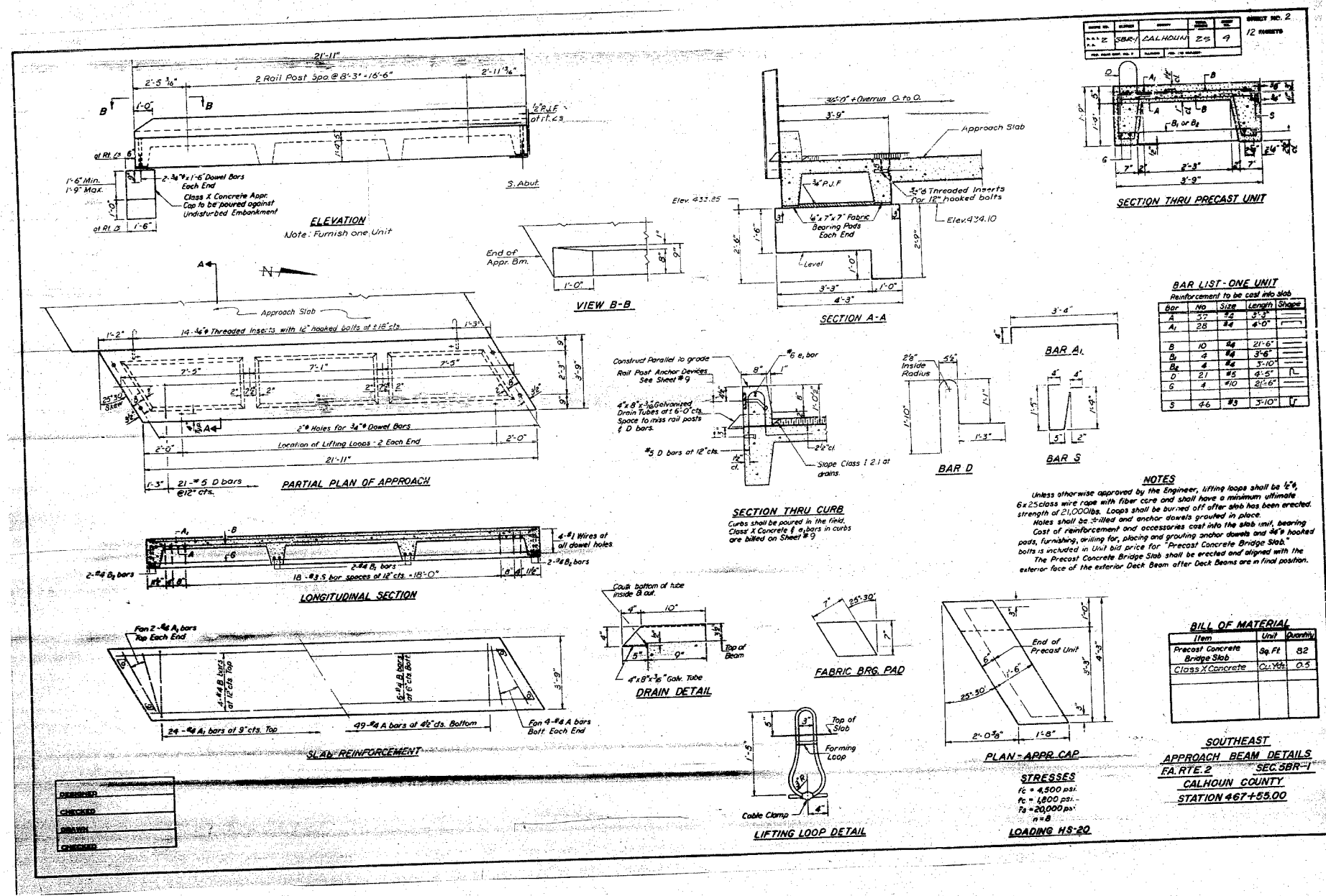
DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



02/27/08

BORING LOGS
IL. ROUTE 100 OVER
CRAWFORD CREEK
F.A.P. ROUTE 304 - SECTION 5BR-2
CALHOUN COUNTY
STATION 467+59.00
STRUCTURE NO. 007-0027

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	54
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



BAR LIST - ONE UNIT
Reinforcement to be cast into slab

Bar	No	Size	Length	Shape
A	37	#4	3'-3"	
A ₁	28	#4	4'-0"	
B	10	#4	21'-6"	
B ₁	4	#4	3'-6"	
B ₂	4	#4	3'-10"	
D	21	#5	4'-5"	
S	4	#10	21'-6"	
S	46	#3	3'-10"	

BILL OF MATERIAL

Item	Unit	Quantity
Precast Concrete Bridge Slab	Sq. Ft.	82
Class X Concrete	Cu. Yds.	0.5

STRESSES
 f_c = 4,500 psi.
 f_t = 4,800 psi.
 f_s = 20,000 psi.
 n = 8
LOADING HS-20

SOUTHEAST APPROACH BEAM DETAILS
 F.A. RTE 2 SEC. 5BR-1
 CALHOUN COUNTY
 STATION 467+55.00

DESIGNED	
CHECKED	
APPROVED	
DATE	

REVISIONS	
NAME	DATE

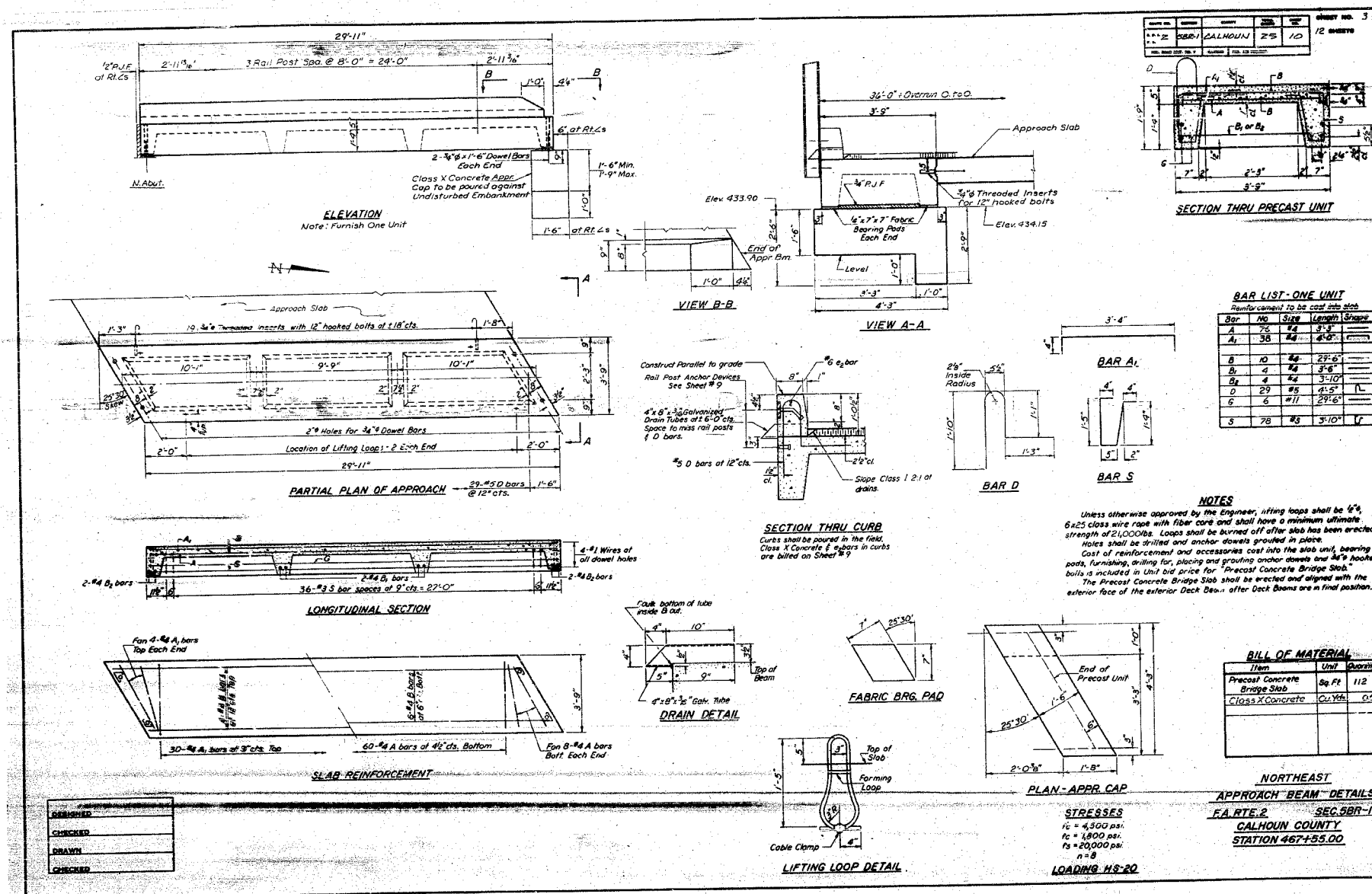
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING STRUCTURE PLANS
 FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY
 SCALE: VERT. HORIZ.
 DATE DATE DRAWN BY CHECKED BY

FOR INFORMATION ONLY

PLOT DATE = 1/24/2008
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	SBR-2	CALHOUN	68	55

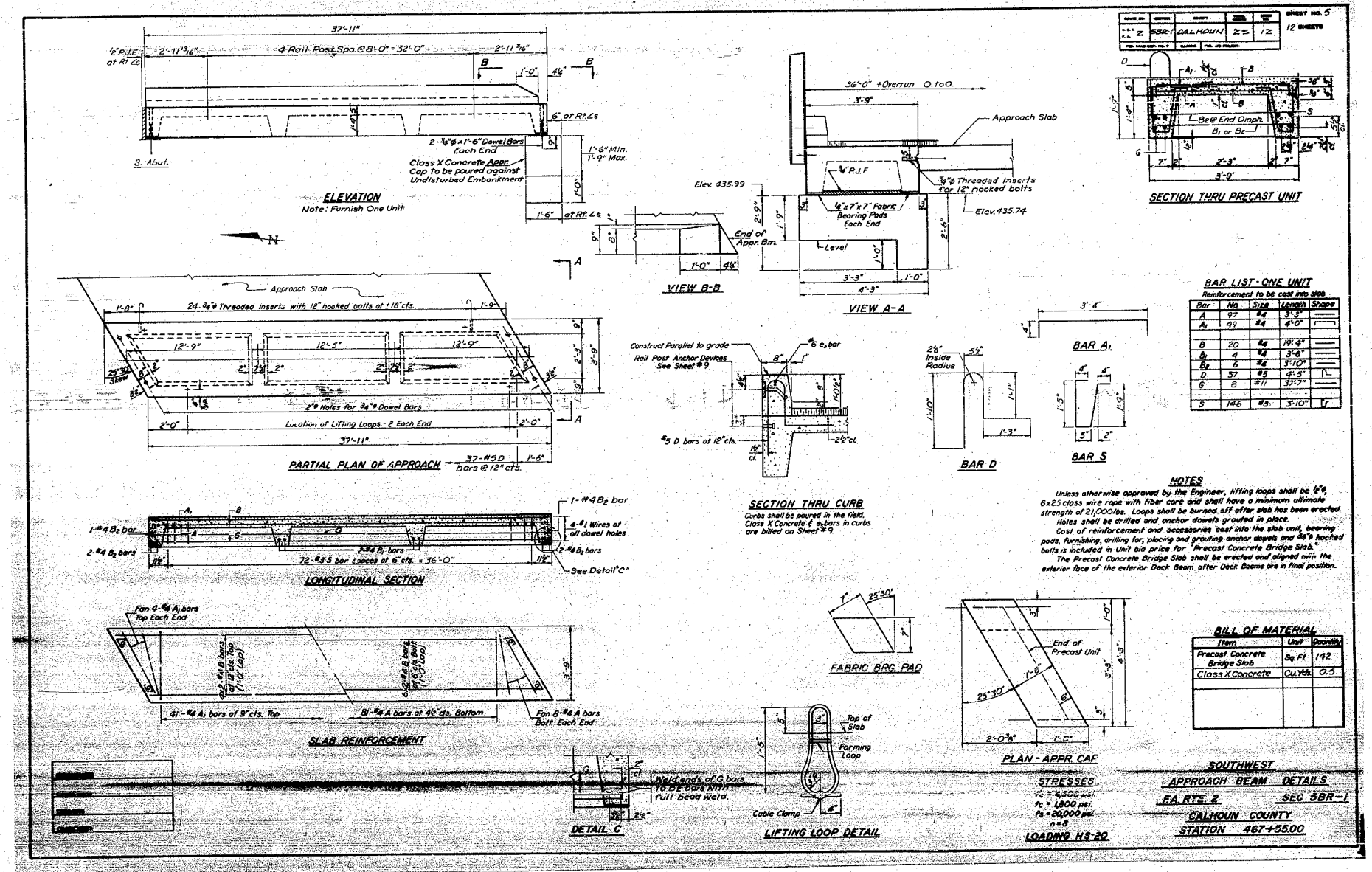
STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



FOR INFORMATION ONLY

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ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING STRUCTURE PLANS
FAP ROUTE 304
SECTION 5BR-2
CLINTON COUNTY
SCALE: VERT. HORIZ.
DATE DRAWN BY CHECKED BY



FOR INFORMATION ONLY

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REFERENCE = SHEET 9

REVISIONS		DATE
NAME		

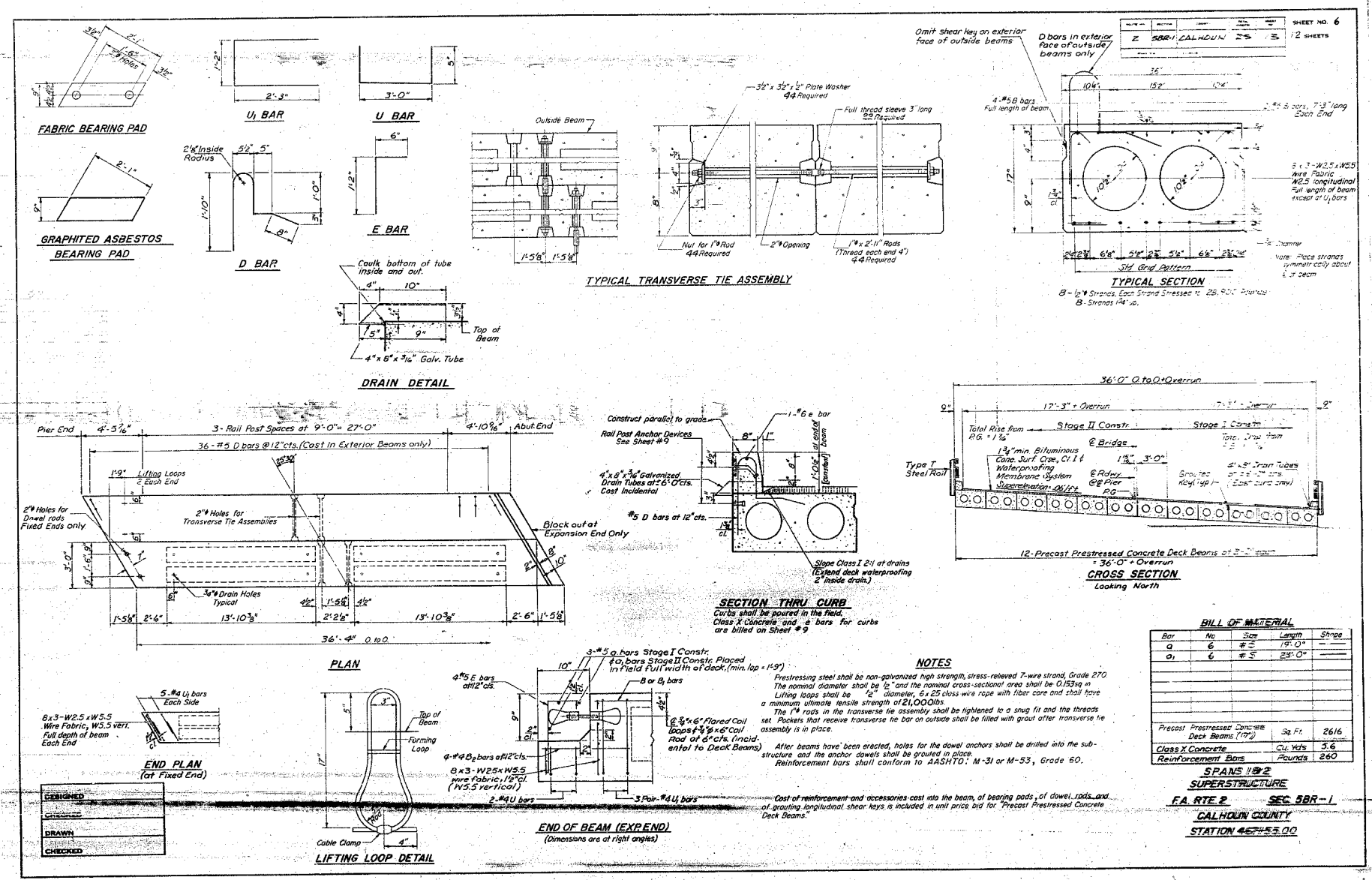
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING STRUCTURE PLANS

FAP ROUTE 304
SECTION 5BR-2
CLINTON COUNTY

SCALE: VERT. HORIZ.
DATE

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	58
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

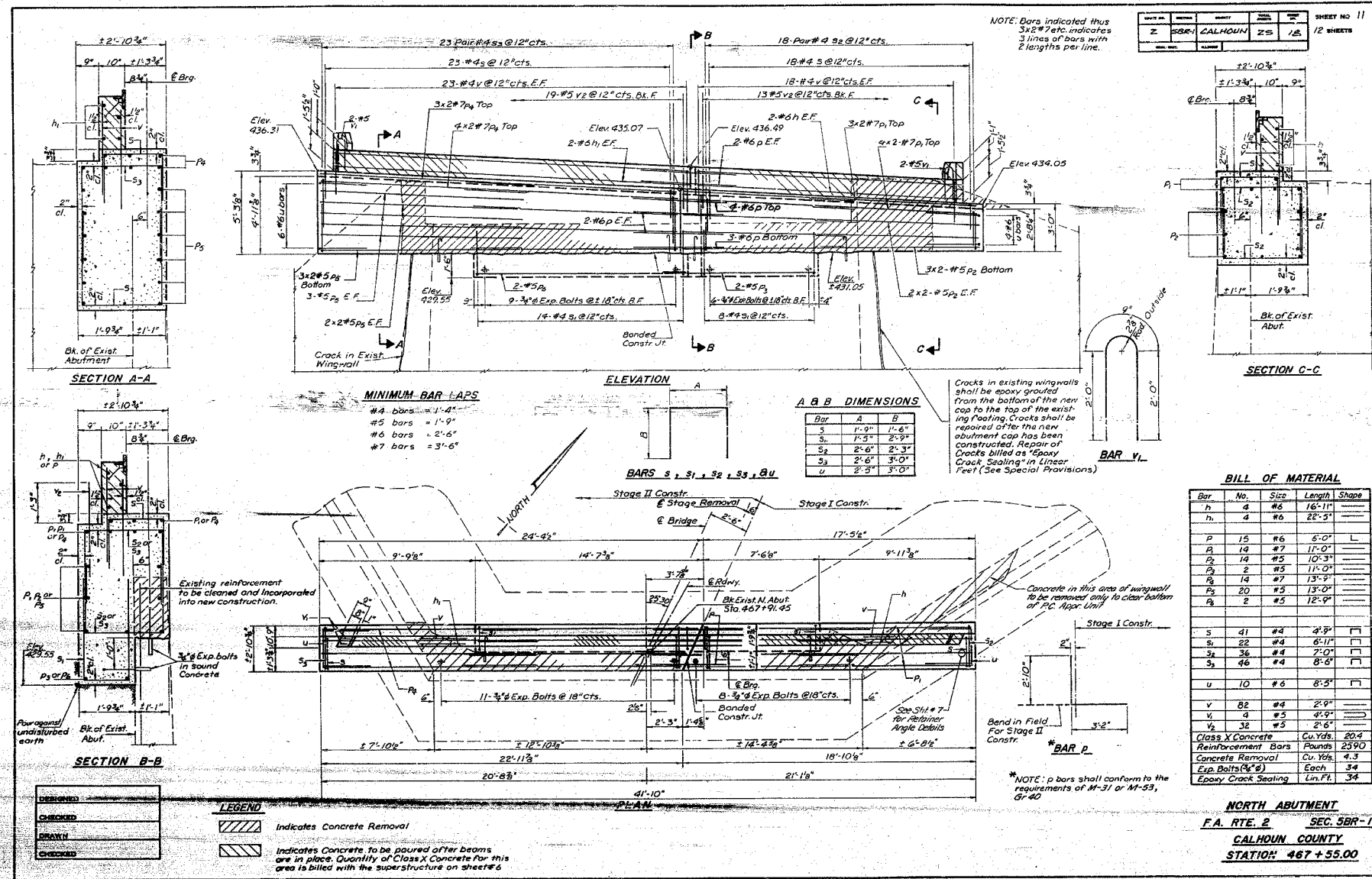


FOR INFORMATION ONLY

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		EXISTING STRUCTURE PLANS
		FAP ROUTE 304 SECTION 5BR-2 CLINTON COUNTY
		SCALE: VERT. HORIZ.
		DATE
		DRAWN BY
		CHECKED BY

PLOT DATE = 1/24/2008
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REFERENCE = REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	SBR-2	CALHOUN	68	59
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FOR INFORMATION ONLY

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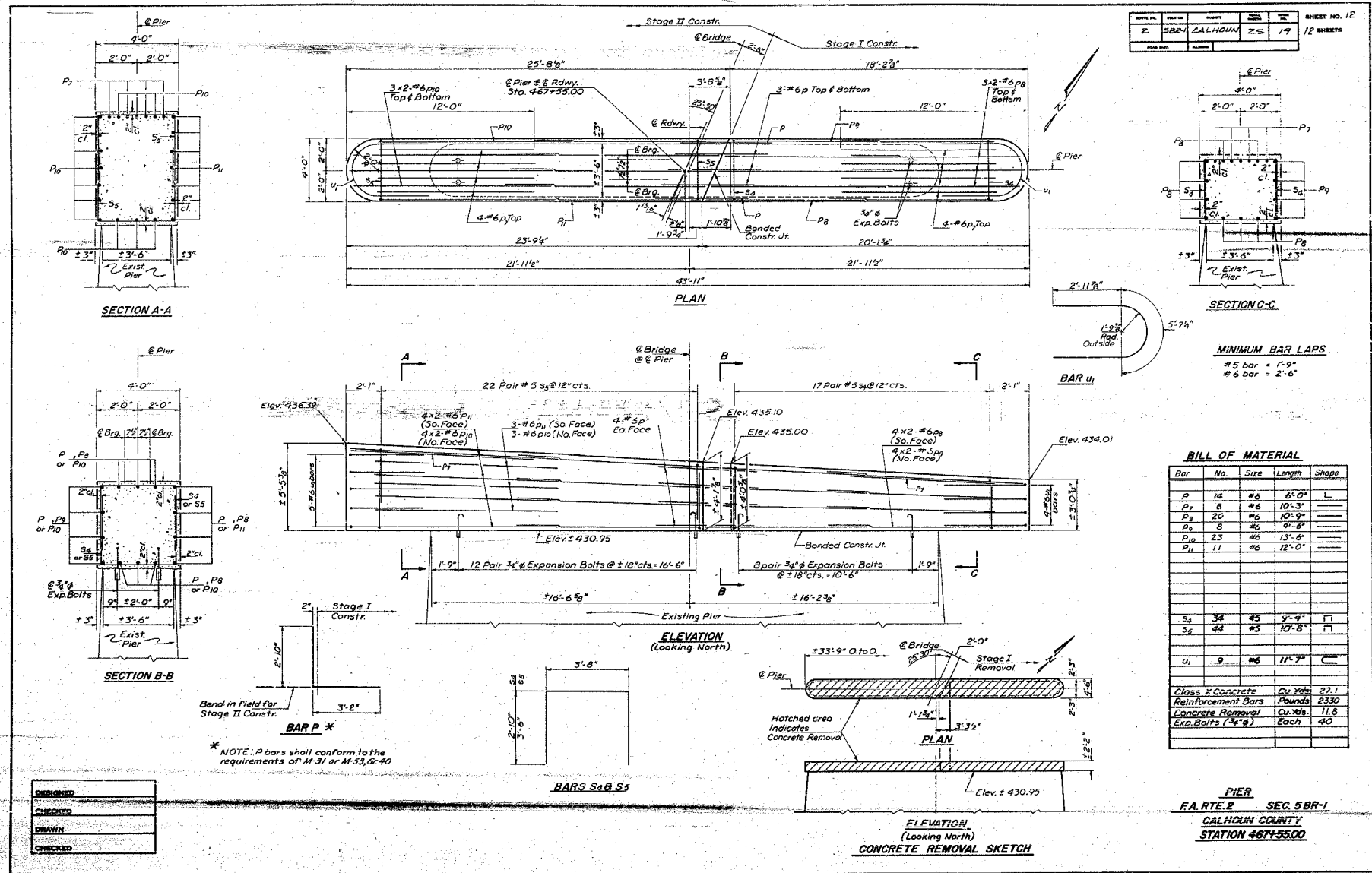
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING STRUCTURE PLANS
 FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY

SCALE: VERT. _____
 HORIZ. _____
 DATE _____

DRAWN BY _____
 CHECKED BY _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	60

STA. TO STA.
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



FOR INFORMATION ONLY

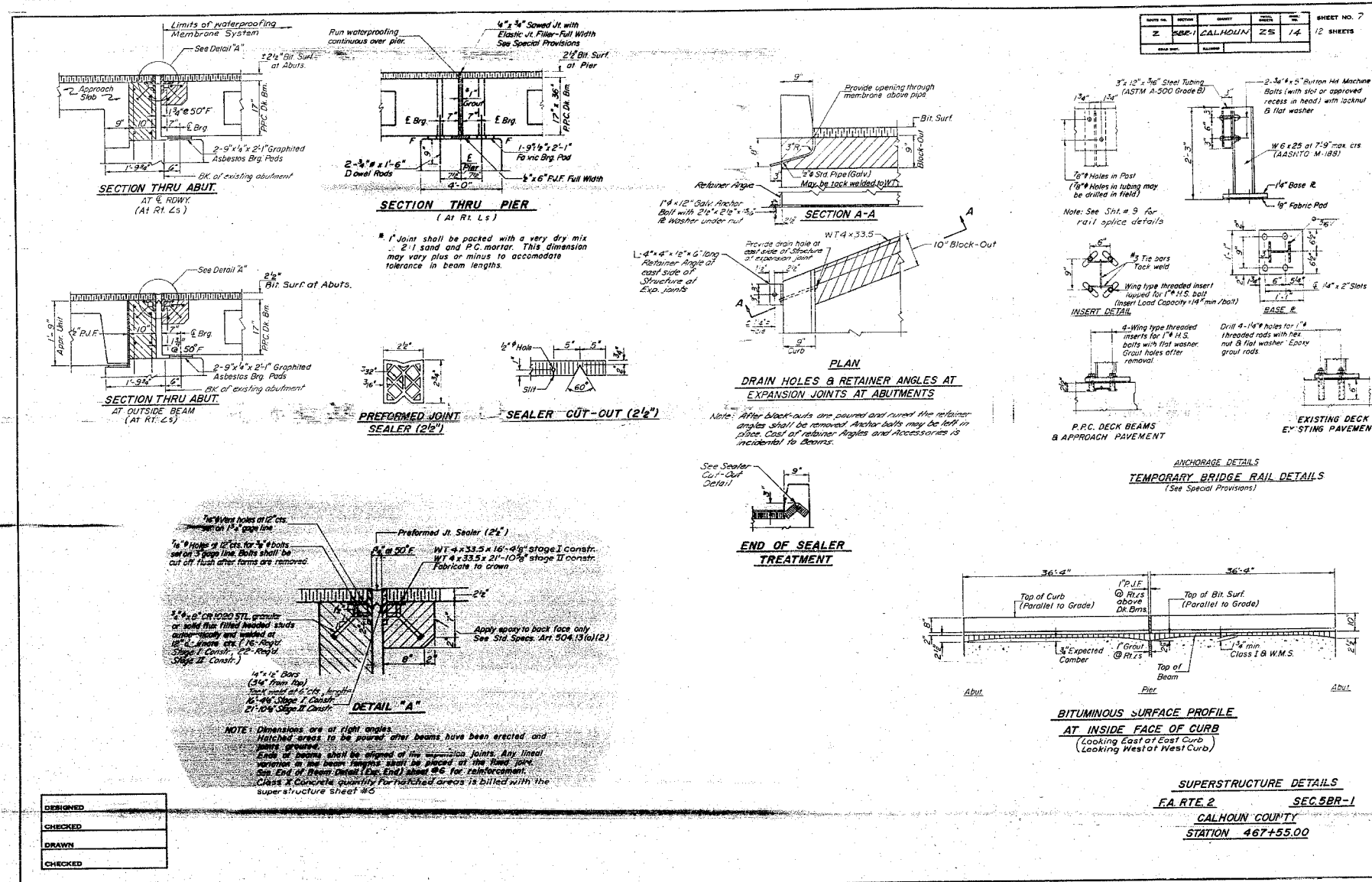
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING STRUCTURE PLANS

FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY

SCALE: VERT. DATE: HORIZ. DRAWN BY: CHECKED BY:

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	61
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



FOR INFORMATION ONLY

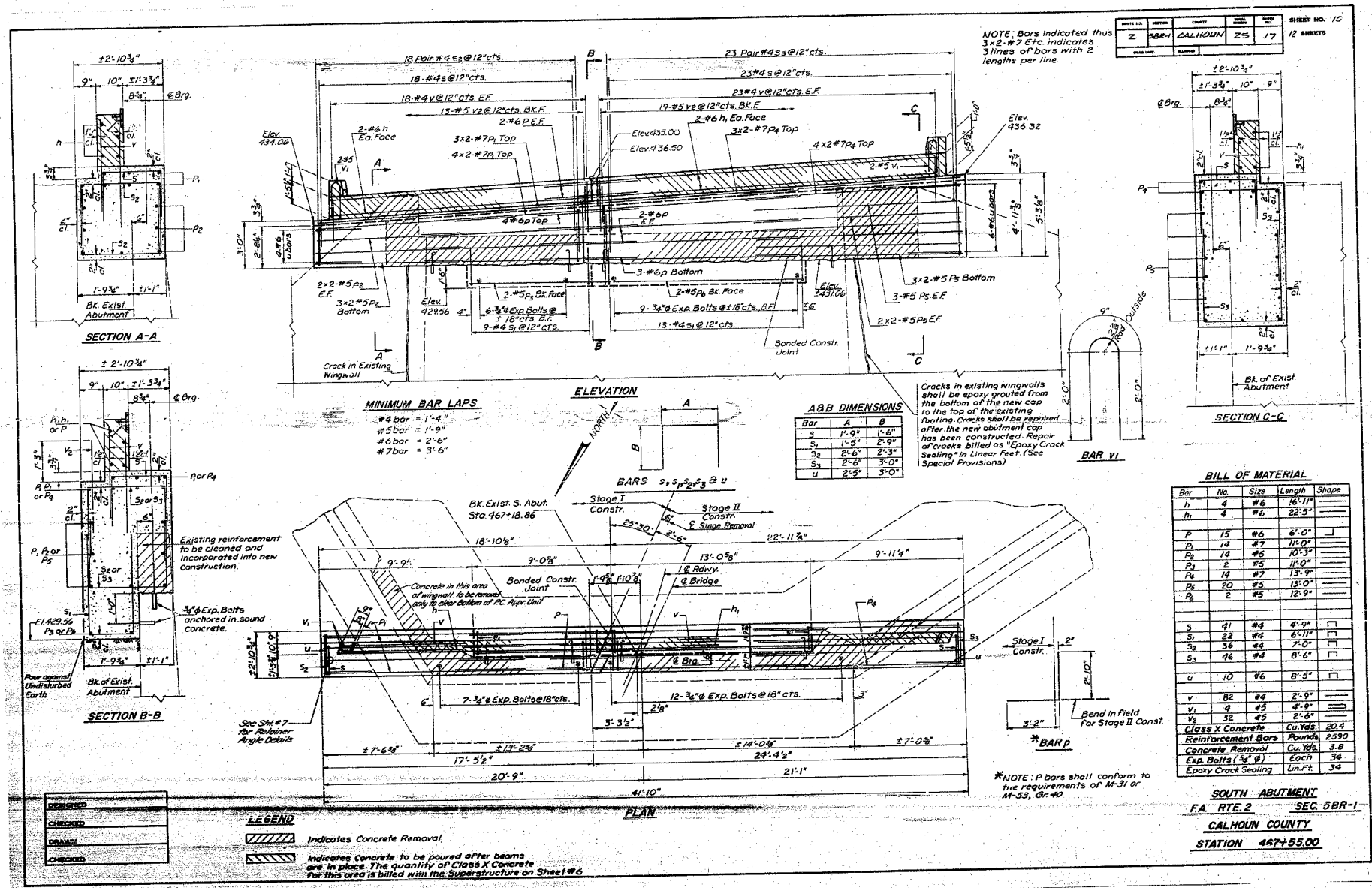
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	EXISTING STRUCTURE PLANS	

FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY

SCALE: VERT.
 HORIZ.
 DATE

DRAWN BY
 CHECKED BY

PLT DATE = 1/21/2008
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FOR INFORMATION ONLY

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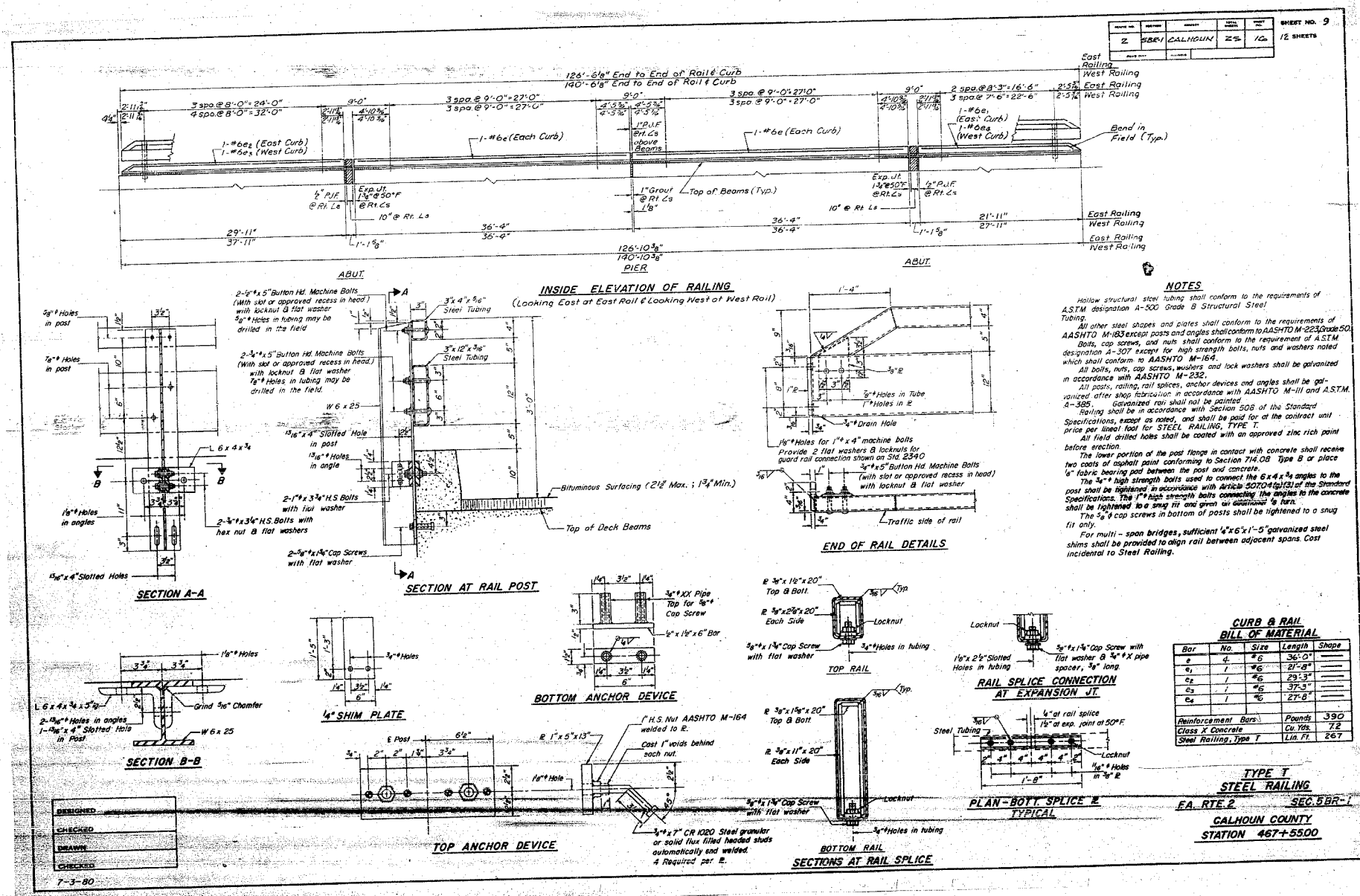
ILLINOIS DEPARTMENT OF TRANSPORTATION

EXISTING STRUCTURE PLANS

FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY

SCALE: VERT. DATE
 HORIZ. DATE

DRAWN BY
 CHECKED BY



NO.	DATE	BY	CHKD.	APP'D.
2	5/24/80	WALSH	ZS	TC

SHEET NO. 9
12 SHEETS

NOTES

Hollow structural steel tubing shall conform to the requirements of ASTM designation A-500 Grade B Structural Steel Tubing.

All other steel shapes and plates shall conform to the requirements of AASHTO M-183 except posts and angles shall conform to AASHTO M-223 Grade 50. Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A-307 except for high strength bolts, nuts and washers noted which shall conform to AASHTO M-164.

All bolts, nuts, cap screws, washers and lock washers shall be galvanized in accordance with AASHTO M-532.

All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication in accordance with AASHTO M-111 and ASTM A-335. Galvanized rail shall not be painted.

Railing shall be in accordance with Section 508 of the Standard Specifications, except as noted, and shall be paid for at the contract unit price per lineal foot for STEEL RAILING, TYPE I.

All field drilled holes shall be coated with an approved zinc rich paint before erection.

The lower portion of the post flange in contact with concrete shall receive two coats of epoxy paint conforming to Section 714.08 Type B or place 1/2" fabric bearing pad between the post and concrete.

The 1/2" high strength bolts used to connect the 6 x 4 x 1/2" angles to the post shall be galvanized in accordance with Article 507.04 (1)(3) of the Standard Specifications. The 1/2" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/2 turn.

The 3/8" cap screws in bottom of posts shall be tightened to a snug fit only.

For multi-span bridges, sufficient 1/2" x 6" x 1/2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost incidental to Steel Railing.

CURB & RAIL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
e	4	#6	36'-0"	
e ₁	1	#6	27'-0"	
e ₂	1	#6	23'-3"	
e ₃	1	#6	37'-3"	
e ₄	1	#6	27'-8"	

Reinforcement Bars	Pounds	39.0
Class X Concrete	Cu. Yds.	7.2
Steel Railing, Type I	Lin. Ft.	26.7

TYPE I STEEL RAILING

EA. RTE. 2	SEC. 5BR-1
CALHOUN COUNTY	
STATION 467+55.00	

DESIGNED	
CHECKED	
DRAWN	
CHECKED	
7-3-80	

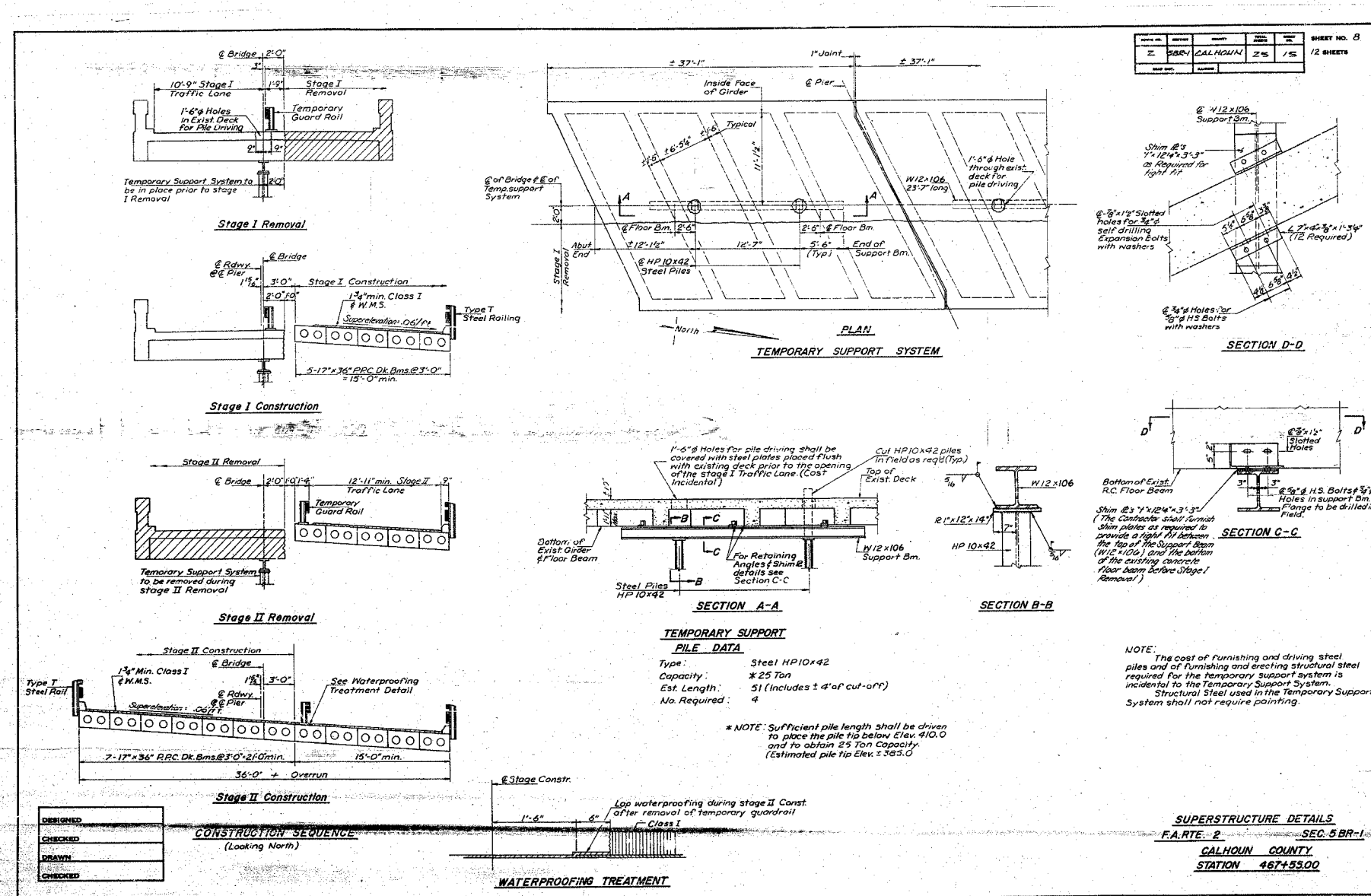
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 EXISTING STRUCTURE PLANS
 FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY
 SCALE: VERT. _____
 HORIZ. _____
 DATE _____
 DRAWN BY _____
 CHECKED BY _____

FOR INFORMATION ONLY

PLOT DATE = 1/24/2008
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	64
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FOR INFORMATION ONLY

PLOT DATE = 1/24/2008
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 REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 EXISTING STRUCTURE PLANS

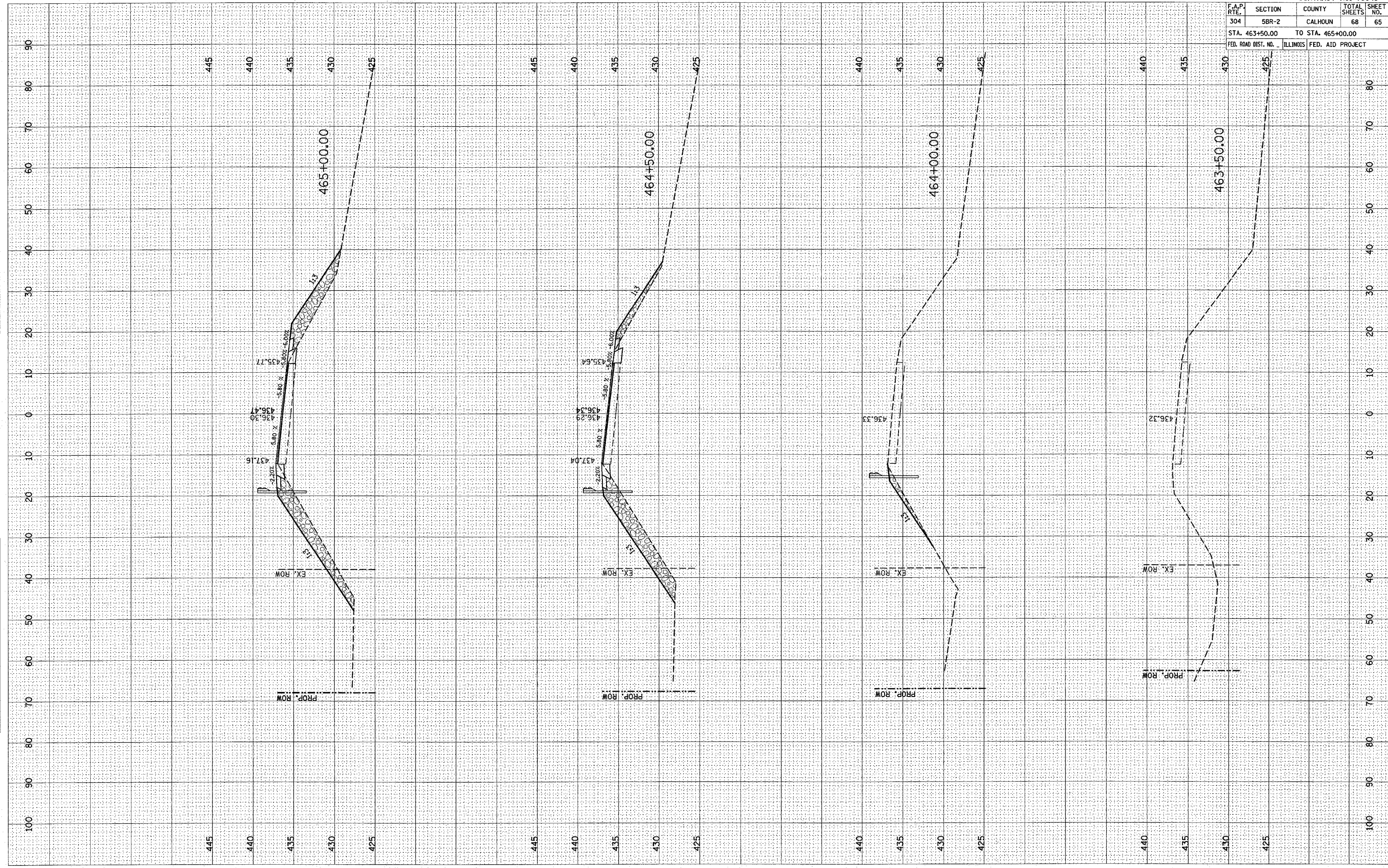
FAP ROUTE 304
 SECTION 5BR-2
 CLINTON COUNTY

SCALE: VERT. DATE
 HORIZ. DRAWN BY
 CHECKED BY

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ORIGINAL SURVEY
 DATE
 BY
 NO. AREAS CHECKED

FINAL SURVEY
 DATE
 BY
 NO. AREAS CHECKED



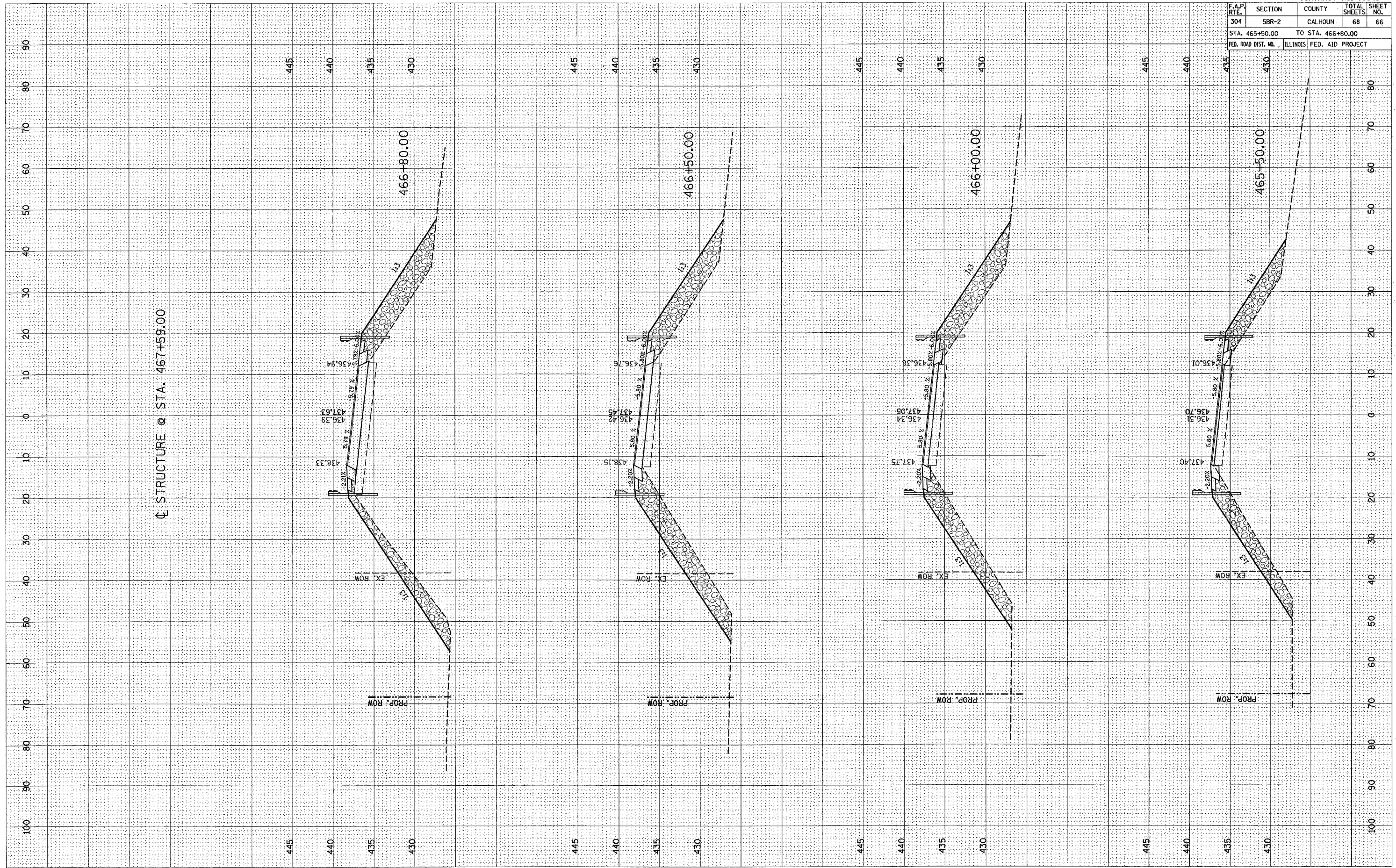
CONTRACT NO. 76948

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	65
STA. 463+50.00		TO STA. 465+00.00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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ORIGINAL SURVEY NO. _____
 CHECKED BY _____
 DATE _____

FINAL SURVEY NO. _____
 CHECKED BY _____
 DATE _____



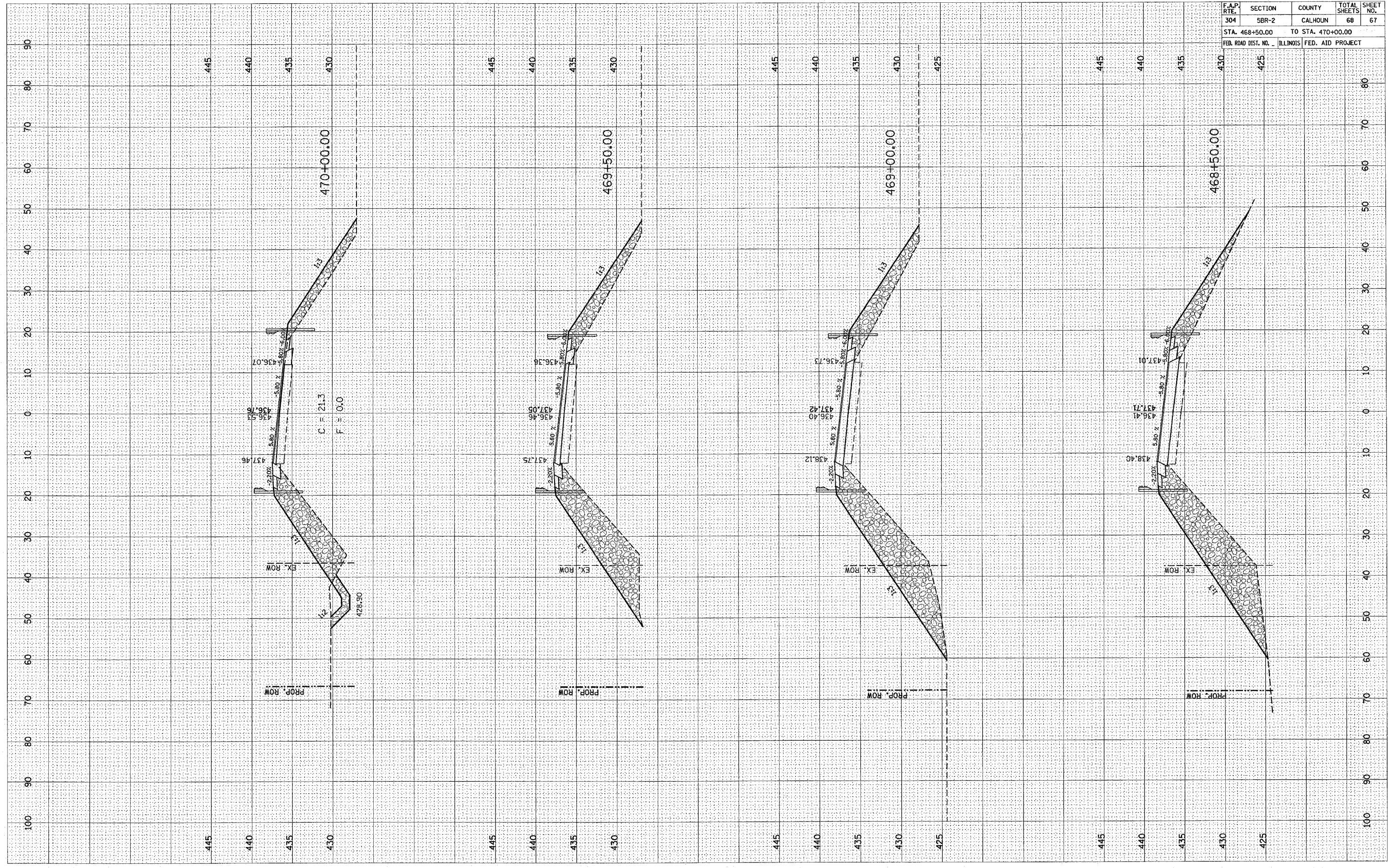
☉ STRUCTURE @ STA. 467+59.00

CONTRACT NO. 76948				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	66
STA. 465+50.00		TO STA. 466+80.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PLOT DATE = 1/30/2008
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ORIGINAL SURVEY
 FIELD DATE
 DATE
 BY
 NO.
 AREAS CHECKED

FINAL SURVEY
 REVISIONS
 DATE
 BY
 NO.
 AREAS CHECKED

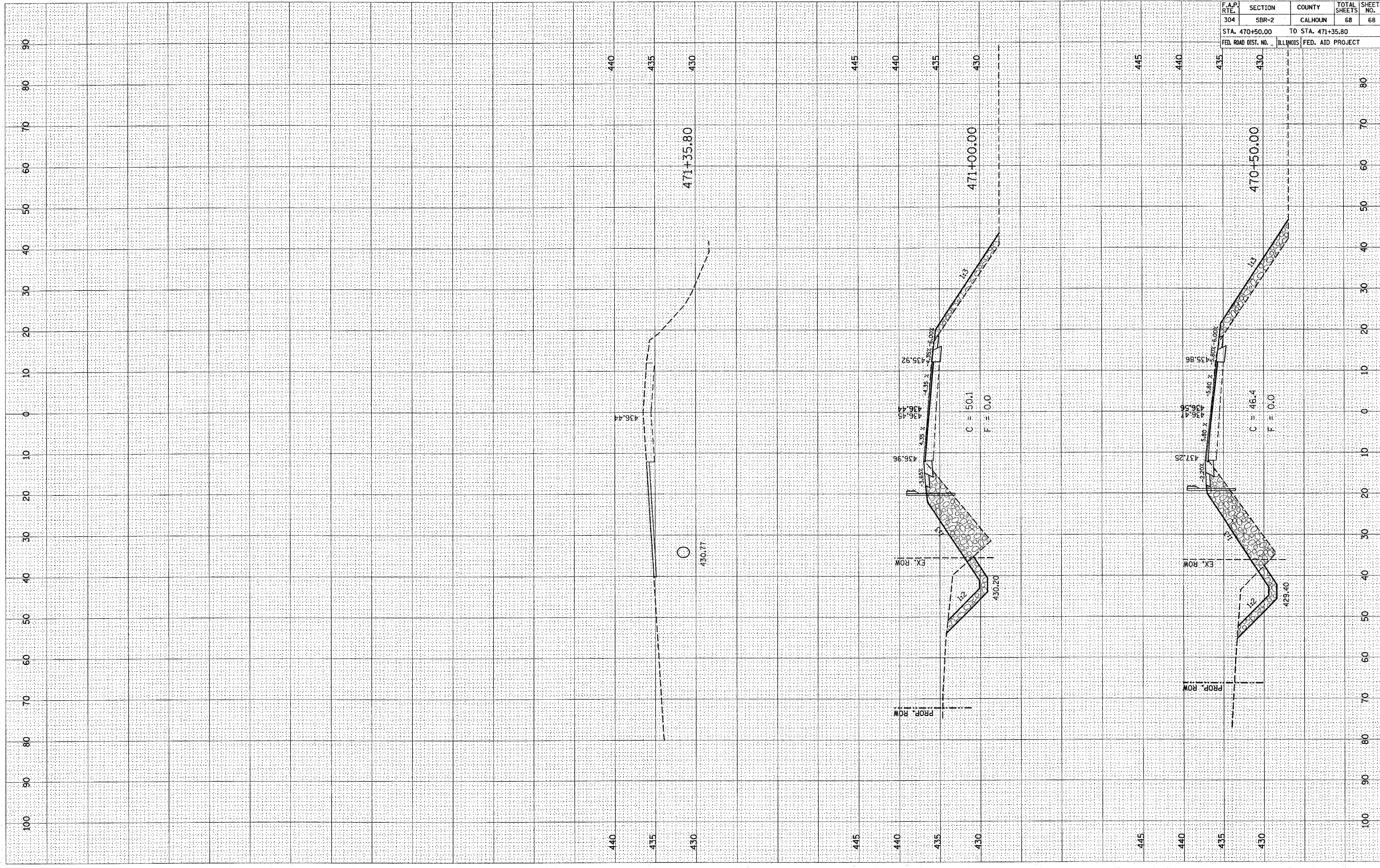


CONTRACT NO. 76948				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	67
STA. 468+50.00 TO STA. 470+00.00				
FED. ROAD DIST. NO. ILLINOIS			FED. AID PROJECT	

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ORIGINAL SURVEY PLOTTED
 NO. DATE AREAS CHECKED

FINAL SURVEY PLOTTED
 NO. DATE AREAS CHECKED



CONTRACT NO. 76948				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	68
STA. 470+50.00		TO STA. 471+35.80		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				