

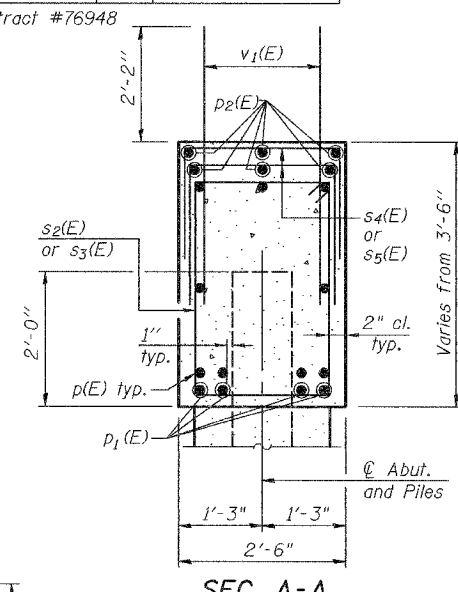
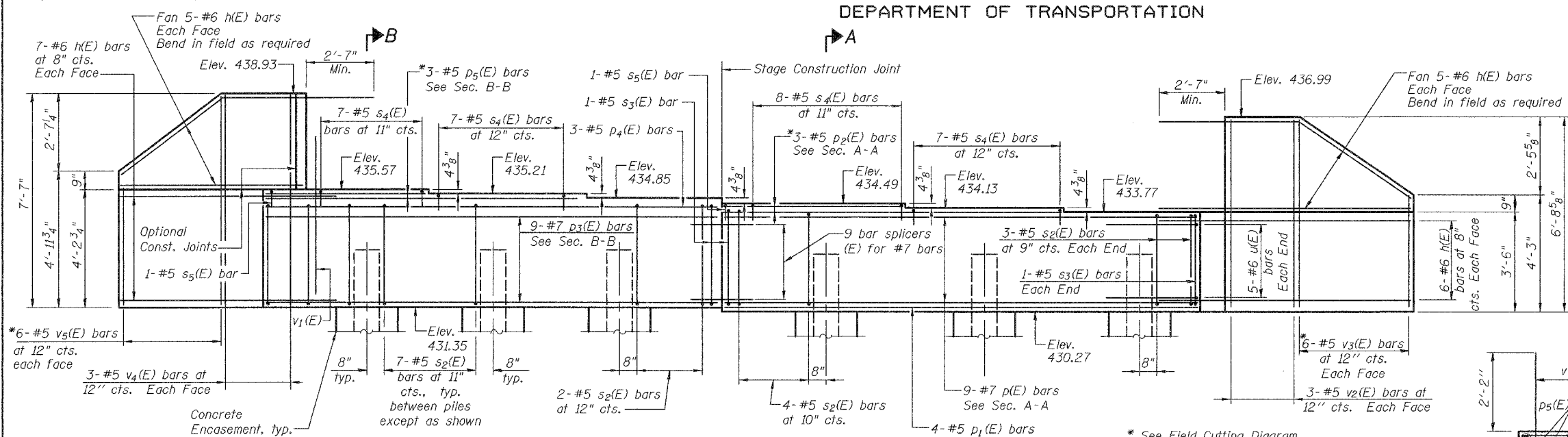
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	48	43
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

SHEET NO. 19  
28 SHEETS

Contract #76948

Notes: Four steps monolithically with cap.  
Space reinforcement in cap to miss anchor bolts.



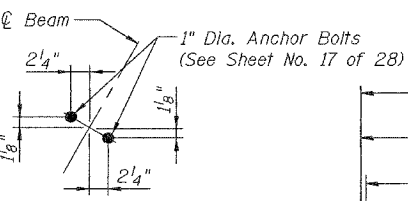
SEC. A-A

BILL OF MATERIAL

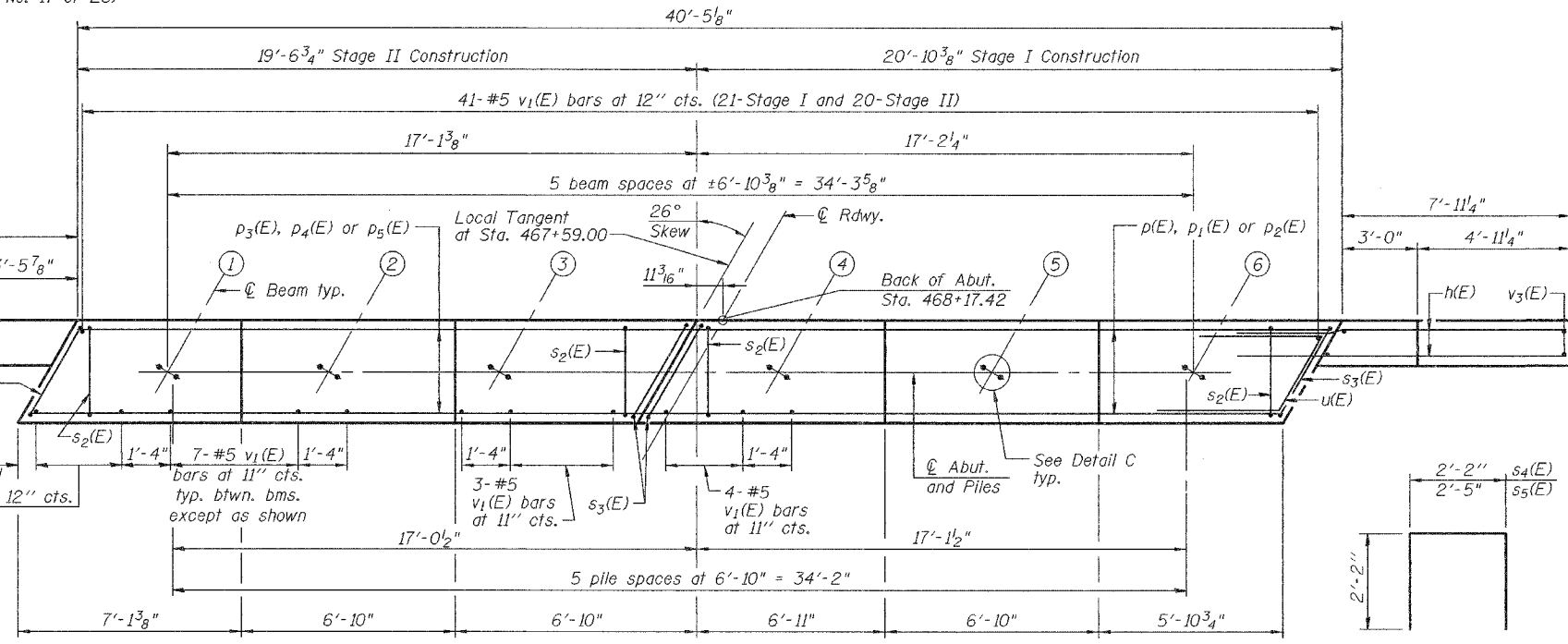
Bar	No.	Size	Length	Shape
h(E)	46	#6	11'-7"	—
p(E)	9	#7	20'-6"	—
p1(E)	4	#5	20'-6"	—
p2(E)	3	#5	21'-2"	—
p3(E)	9	#7	19'-3"	—
p4(E)	3	#5	19'-3"	—
p5(E)	3	#5	19'-2"	—
s2(E)	40	#5	11'-7"	□
s3(E)	4	#5	12'-1"	□
s4(E)	29	#5	6'-6"	□
s5(E)	2	#5	6'-9"	□
u(E)	10	#6	9'-5"	┘
v1(E)	82	#5	4'-4"	—
v2(E)	6	#5	6'-5"	—
v3(E)	6	#5	10'-3"	—
v4(E)	6	#5	7'-3"	—
v5(E)	6	#5	11'-9"	—
Structure Excavation		Cu. Yd.	158	
Concrete Structures		Cu. Yd.	18.5	
Reinforcement Bars, Epoxy Coated		Pound	3,290	
Furnishing Steel Piles HP 12x53		Foot	310	
Driving Piles HP 12x53		Foot	310	
Test Pile Steel HP 12x53		Each	1	
Concrete Encasement		Cu. Yd.	2.1	

For details of Bar Splicers, see Sheet No. 24 of 28.  
For details of piles and Concrete Encasement, see Sheet No. 23 of 28.

DETAIL C



ELEVATION



PLAN

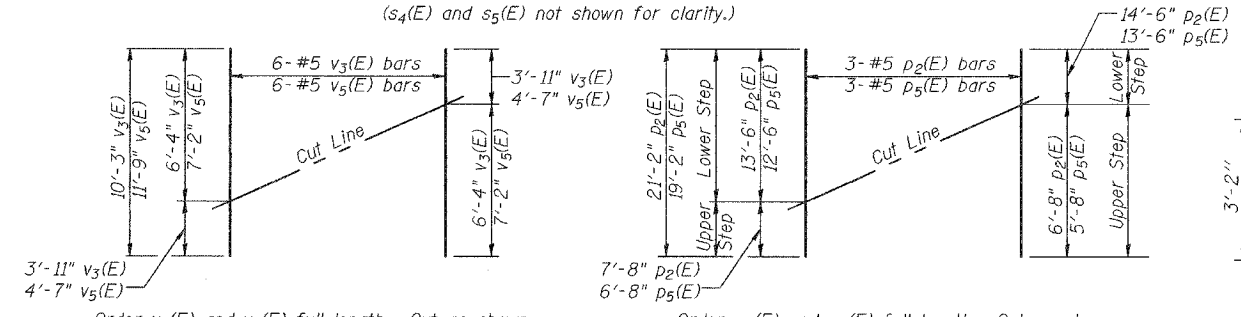
PILE DATA

Type: Steel HP 12x53  
Nominal Required Bearing: 340 kips  
Factored Resistance Available: 170 kips  
Est. Length: 62 ft  
No. Production Piles: 5  
No. Test Piles: 1

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	YSS

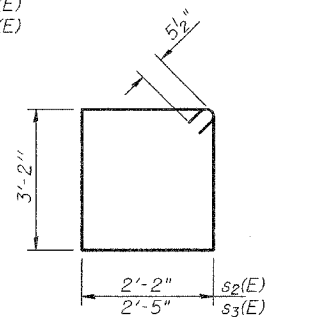


02/27/08

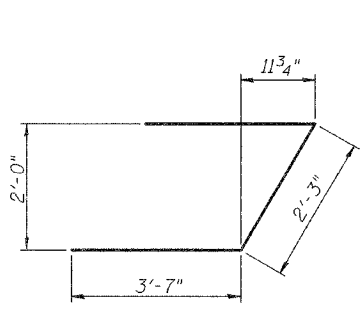


FIELD CUTTING DIAGRAM

BARS s4(E) & s5(E)

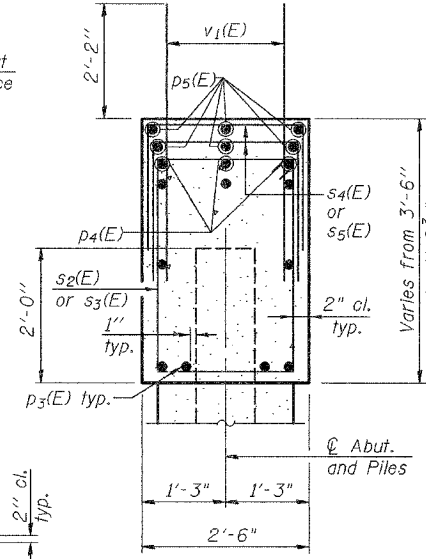


BARS s2(E) & s3(E)



BAR u(E)

SEC. B-B



**NORTH ABUTMENT DETAILS**  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027