60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	1

6-13-08 Letting, Item 258

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

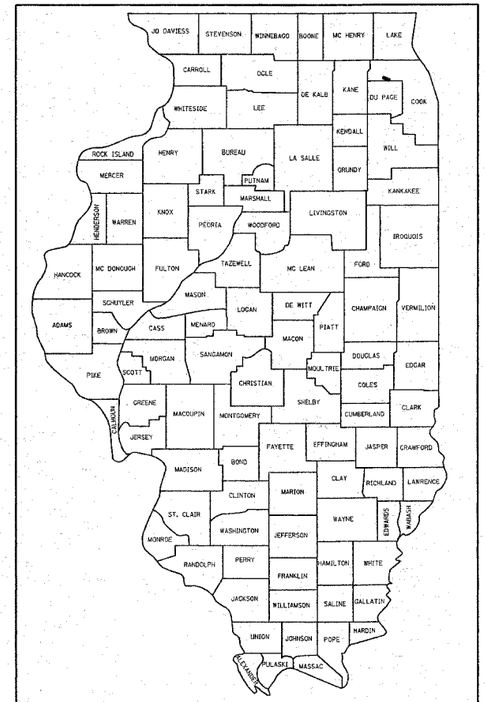
FAU ROUTE 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK – DUPAGE COUNTY LINE
SECTION: 0711 RS–2
RESURFACING (MAINTENANCE)
PROJECT NO.: ACM-1321(015)
COOK COUNTY
C–91–062–06

IMPROVEMENT IS LOCATED IN THE VILLAGES OF HANOVER PARK, ROSELLE, AND SCHAUMBURG IN COOK COUNTY

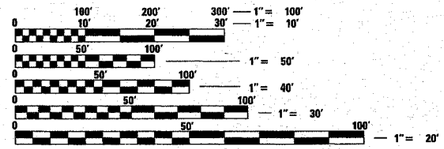
TRAFFIC DATA

2003 ADT = 28900
SPEED LIMIT = 35 MPH – 45 MPH

D–91–062–06



LOCATION OF SECTION INDICATED THUS: –

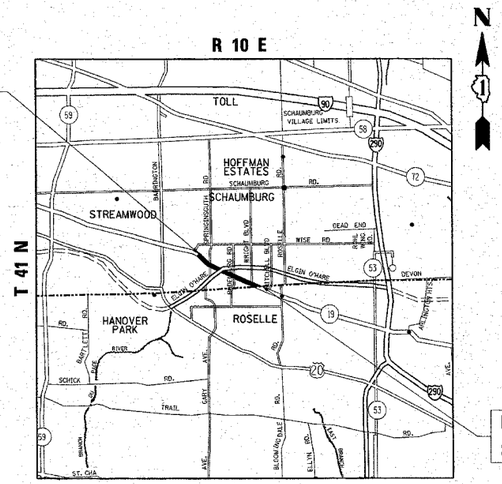


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123 OR 811

CONTRACT NO. 60A49

IMPROVEMENT BEGINS STATION 11+29



IMPROVEMENT ENDS STATION 100+66

SCHAUMBURG / BLOOMINGDALE TOWNSHIPS

GROSS & NET LENGTH OF IMPROVEMENT = 8,937 FEET = 1.69 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 25 20 08
Dan Olk
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 9, 20 08
Eric E. Harm PE
Interim ENGINEER OF DESIGN AND ENVIRONMENT

May 9, 20 08
Christine M. Reed PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

DISTRICT ONE — DESIGN AND PLAN PREPERATION ENGINEER — KEN ENGJ. P. CHANG (847) 705-4432

3/24/2008
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	2
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-13	EXISTING AND PROPOSED TYPICAL SECTIONS
14-17	ROADWAY AND PAVEMENT MARKING PLANS
18-23	DETECTOR LOOP REPLACEMENT PLANS
24	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
25	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
26	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
27	BUTT JOINT AND HMA TAPER DETAILS
28	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
29	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
30	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
31	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
32	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
33	ARTERIAL ROAD INFORMATION SIGN
34-37	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS
38	DISATRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF STATE STANDARDS:

STANDARD NO.	DESCRIPTION
000001-05	TYPICAL SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-02	FRAMES AND LIDS, TYPE 1
606001-03	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
701006-02	OFF-RD OPERATIONS, 3L , 2W 4.5 m (15') TO 600mm (24") FROM PAVEMENT EDGE
701011-01	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-01	OFF-RD OPERATIONS, MULTILANE, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE
701201-02	LANE CLOSURE, 2L, 2W DAY ONLY FOR SPEEDS >= 45 MPH
701301-02	LANE CLOSURE, 2L, 2W SHORT TIME OPERATION
701306-01	LANE CLOSURE, 2L, 2W SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-02	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701426-02	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS >= 45 MPH
701501-04	URBAN LANE CLOSURE 2L, 2W UNDIVIDED
701502-02	URBANE LANE CLOSURE 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701601-05	URBAN LANE CLOSURE MULTILANE 1W, OR 2W WITH NONTRAVERSABLE MEDIAN
701701-05	URBAN LANE CLOSURE MULTI INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OR BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF HANOVER PARK, SCHAUMBURG AND ROSELLE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNTI PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)

THE CONTRACTOR SHALL CONTACT DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (848) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING AND RESURFACING.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT 847-741-9857 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

ILL 19 (IRVING PARK ROAD)
WISE RD TO DUPAGE COUNTY LINE

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES

SCALE: VERT.
HORIZ.
DATE 4/22/2008

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CHECKED BY

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	3
FED. ROAD DIST. NO. 1	ILLINOIS	HIGHWAY PROJECT		

CONTRACT NO. 60A49

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN 80% FED. 20% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 1000-2A
20201006	GRADING AND SHAPING SHOULDERS	UNIT	45	45
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	26	26
40600300	AGGREGATE (PRIME COAT)	TON	135	135
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20	20
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	1120	1120
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	232	232
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	6120	6120
42001300	PROTECTIVE COAT	SQ YD	45	45
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	60620	60620
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	120	120
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	1025	1025
44004610	SIDEWALK REMOVAL AND REPLACEMENT (SPECIAL)	SQ FT	5	5
44201785	CLASS D PATCHES, TYPE I, 12 INCH	SQ YD	110	110
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	202	202
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	104	104
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	242	242
48102100	AGGREGATE WEDGE SHOULDERS, TYPE B	TON	305	305
▪ 55037800	STORM SEWERS TO BE CLEANED 12"	FOOT	940	940
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	2	2
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1
60253100	CATCH BASINS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	20	20
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	5	5
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	13760	13760
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	775	775
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25990	25990
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	520	520
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	275	275
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	670	670
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	15900	15900
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	775	775
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	25990	25990
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	520	520
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	275	275
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	670	670
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	715	715
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	648	648
* 81400115	HANDHOLE TO BE ADJUSTED	EACH	1	1
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2300	2300
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	282.7	282.7

* SPECIALITY ITEMS
▪ NON-PARTICIPATING ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
SUMMARY OF QUANTITIES

PLOT DATE: 3/29/2008

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	4
FED. ROAD DIST. NO. 1	ILLINOIS	HIGHWAY PROJECT		

CONTRACT NO. 60A49

SUMMARY OF QUANTITIES

URBAN 80% FED. 20% STATE

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: I000-2A					
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2485	2485					
* Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	5	5					
G Z0076600	TRAINEES	HOUR	1500	1500					

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					

* SPECIALITY ITEMS

• NON-PARTICIPATING ITEMS

G Y080

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

SUMMARY OF QUANTITIES

PLOT DATE: 3/11/2008

3/11/2008
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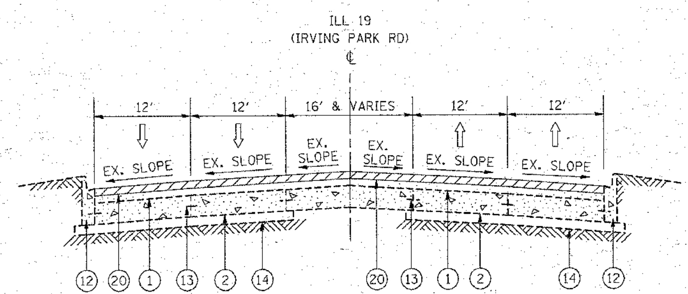
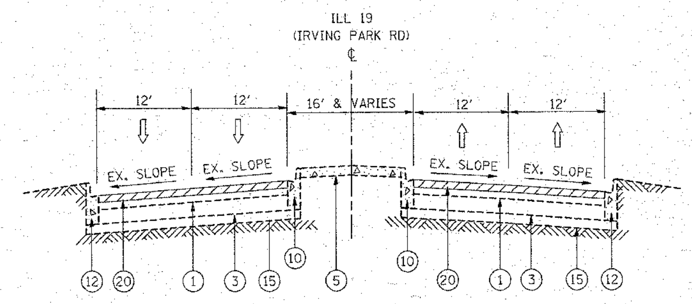
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	5
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

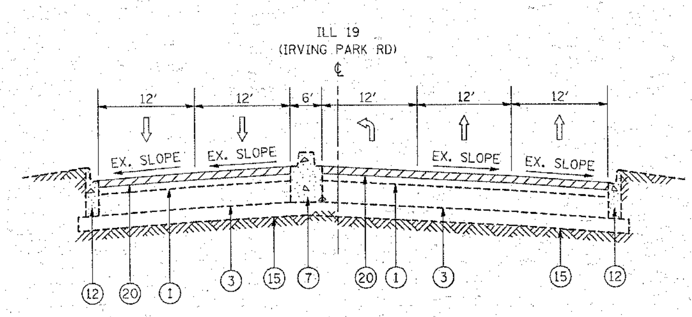
CONTRACT NO. 60A49



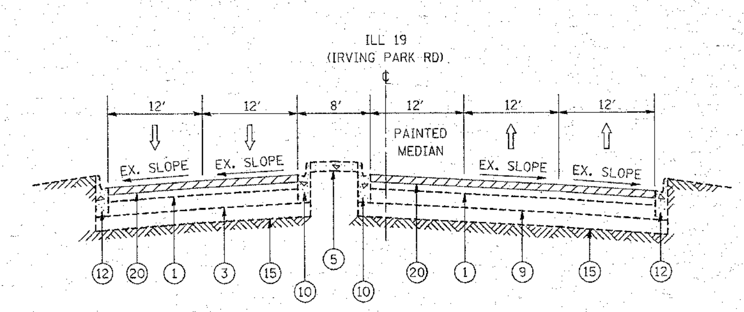
EXISTING TYPICAL SECTION
STA. 11+29 TO STA. 11+52



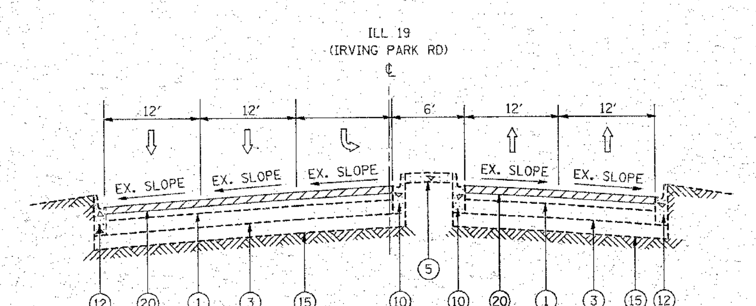
EXISTING TYPICAL SECTION
STA. 17+05 TO STA. 23+58



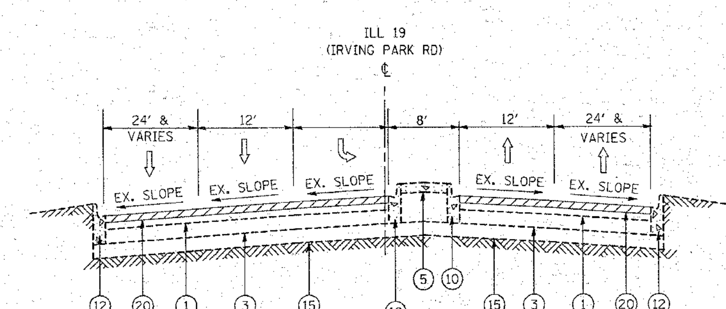
EXISTING TYPICAL SECTION
STA. 11+52 TO STA. 14+76



EXISTING TYPICAL SECTION
STA. 23+58 TO STA. 26+78



EXISTING TYPICAL SECTION
STA. 14+76 TO STA. 17+05



EXISTING TYPICAL SECTION
STA. 26+78 TO STA. 28+76

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED
TYPICAL SECTIONS

SCALE: VERT.
HORIZ.
DATE

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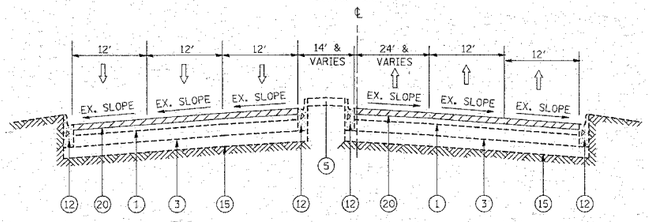
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	6
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

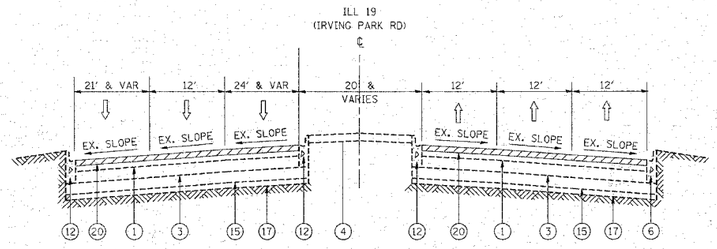
CONTRACT NO. 60A49

ILL 19
(IRVING PARK RD)



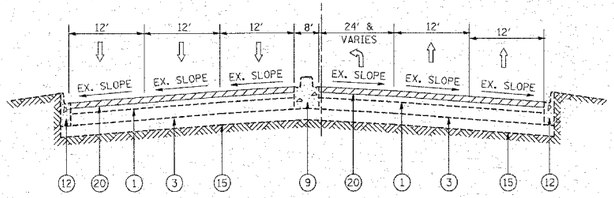
EXISTING TYPICAL SECTION
STA. 28+76 TO STA. 32+33

ILL 19
(IRVING PARK RD)



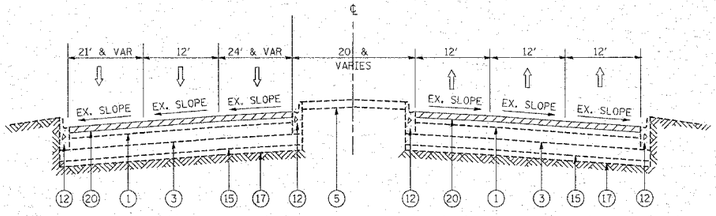
EXISTING TYPICAL SECTION
STA. 37+15 TO STA. 39+88

ILL 19
(IRVING PARK RD)



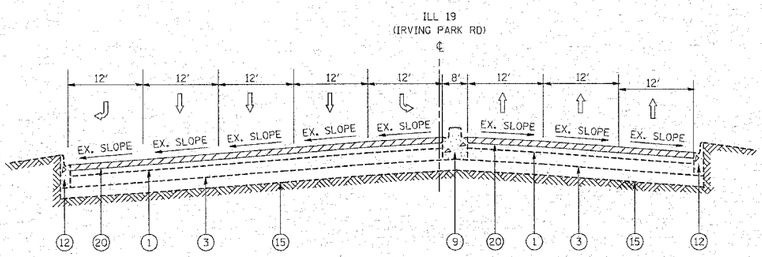
EXISTING TYPICAL SECTION
STA. 32+33 TO STA. 35+39

ILL 19
(IRVING PARK RD)



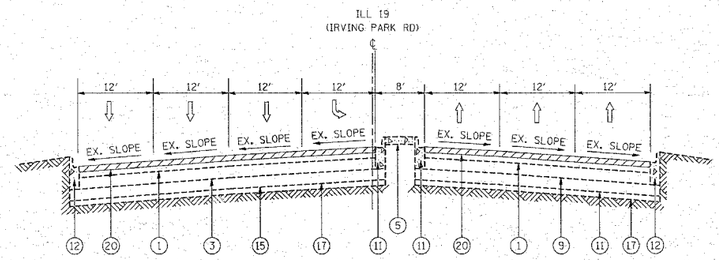
EXISTING TYPICAL SECTION
STA. 39+88 TO STA. 42+27

ILL 19
(IRVING PARK RD)



EXISTING TYPICAL SECTION
STA. 35+39 TO STA. 37+15

ILL 19
(IRVING PARK RD)



EXISTING TYPICAL SECTION
STA. 42+27 TO STA. 43+45

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
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14. EXISTING STABILIZED SUB BASE, 4"
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16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
- ✱ 23. PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

✱ LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT.
HORIZ.
DATE

DRAWN BY
CHECKED BY

PLOT DATE = 4/2/2008
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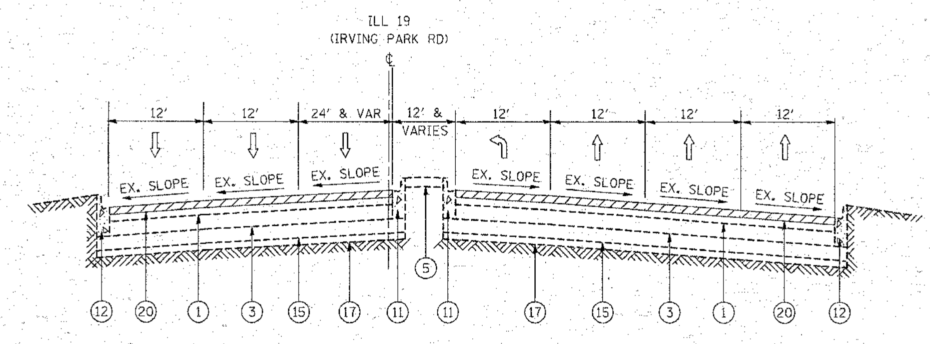
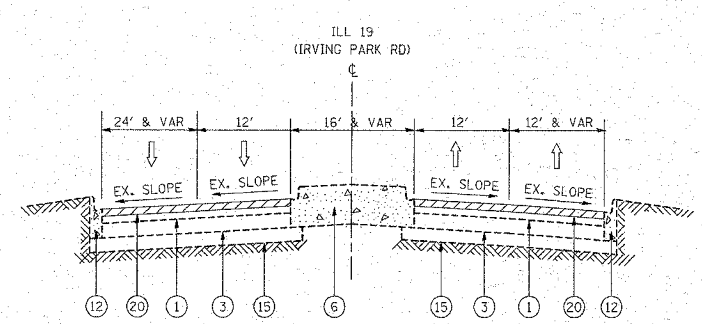
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	7
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

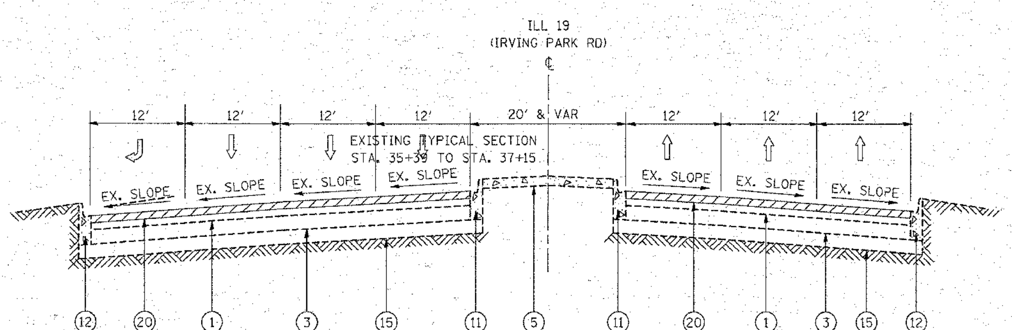
CONTRACT NO. 60A49



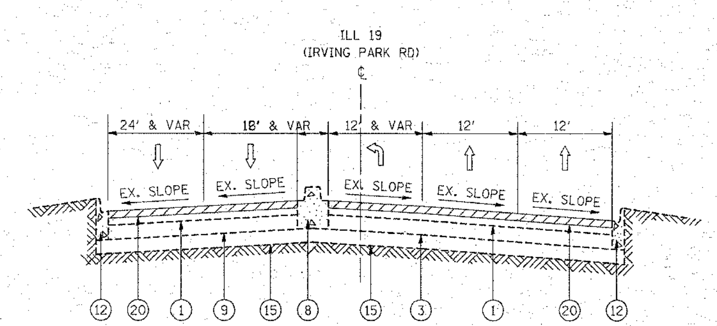
EXISTING TYPICAL SECTION
STA. 43+45 TO STA. 50+27



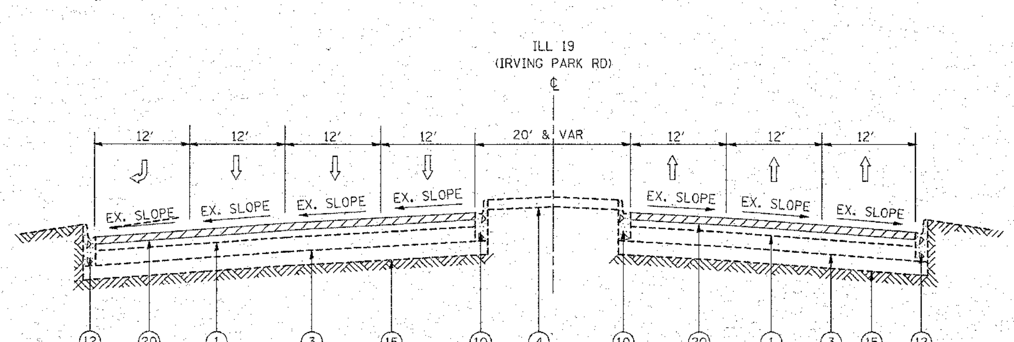
EXISTING TYPICAL SECTION
STA. 52+82 TO STA. 61+94



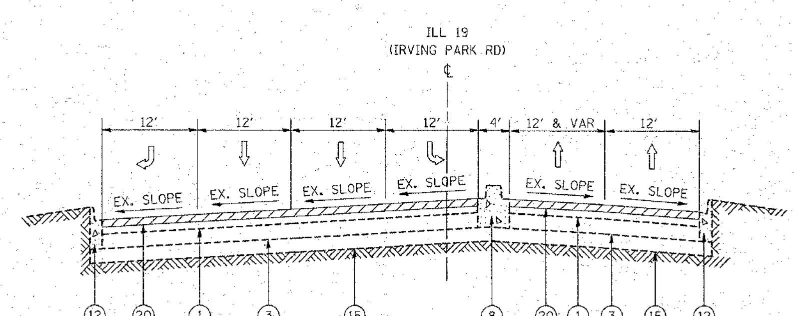
EXISTING TYPICAL SECTION
STA. 50+27 TO STA. 51+03



EXISTING TYPICAL SECTION
STA. 61+94 TO STA. 64+38



EXISTING TYPICAL SECTION
STA. 51+03 TO STA. 62+82



EXISTING TYPICAL SECTION
STA. 64+38 TO STA. 66+37

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL, TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321:ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT. HORIZ. DRAWN BY
DATE CHECKED BY

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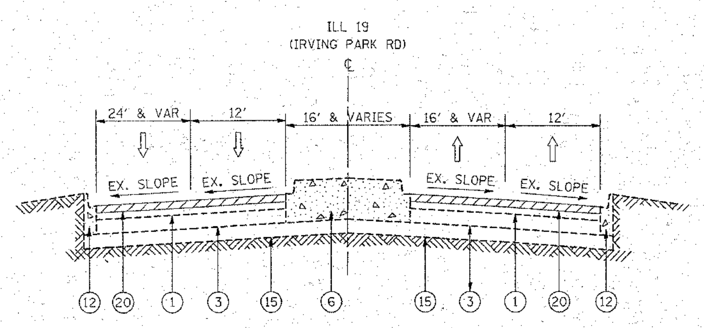
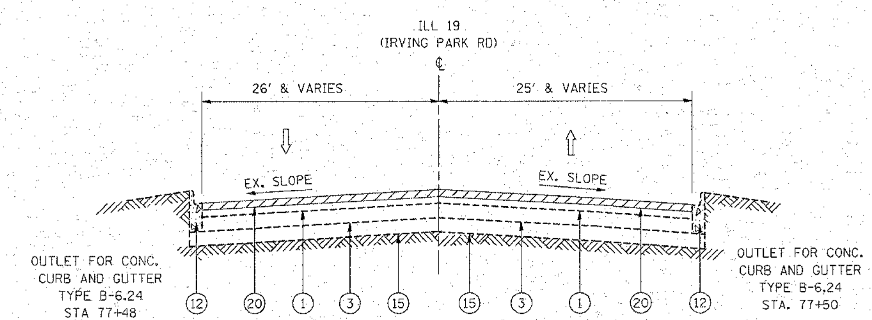
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	8
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

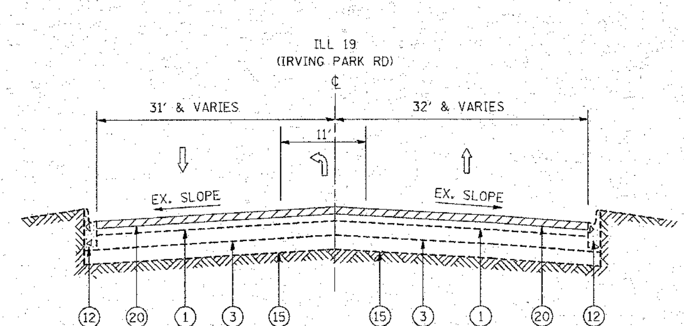
CONTRACT NO. 60A49



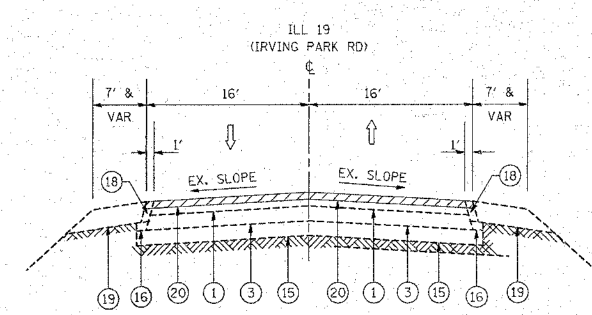
EXISTING TYPICAL SECTION
STA. 66+37 TO STA. 69+84



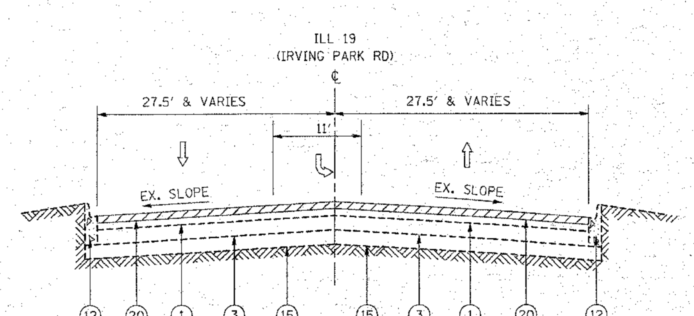
EXISTING TYPICAL SECTION
STA. 73+09 TO STA. 77+50



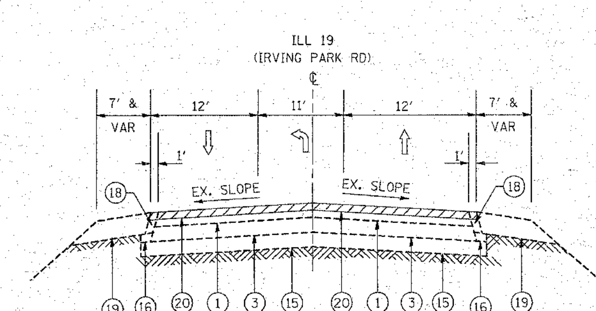
EXISTING TYPICAL SECTION
STA. 69+84 TO STA. 71+98



EXISTING TYPICAL SECTION
STA. 77+50 TO STA. 84+65



EXISTING TYPICAL SECTION
STA. 71+98 TO STA. 73+09



EXISTING TYPICAL SECTION
STA. 84+65 TO STA. 87+22
STA. 91+07 TO STA. 93+30

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARKROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED
TYPICAL SECTIONS

SCALE: VERT. HORIZ.
DATE

DRAWN BY
CHECKED BY

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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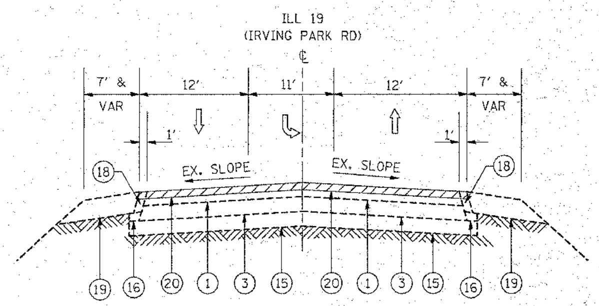
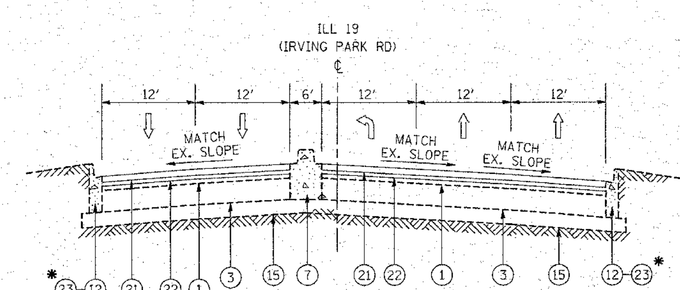
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

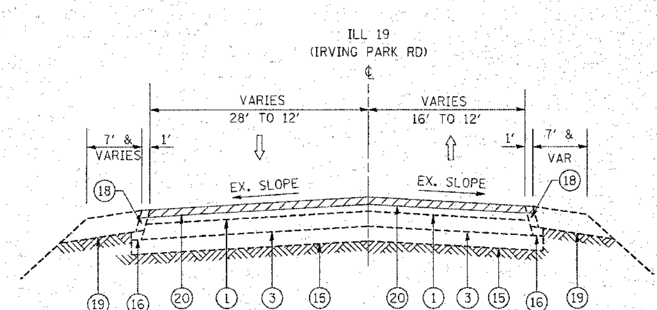
CONTRACT NO. 60A49



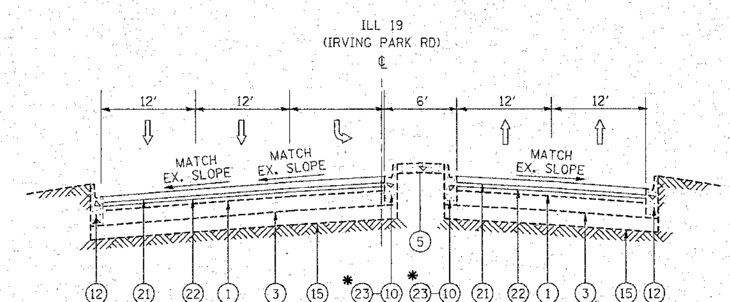
EXISTING TYPICAL SECTION
STA. 87+22 TO STA. 89+06



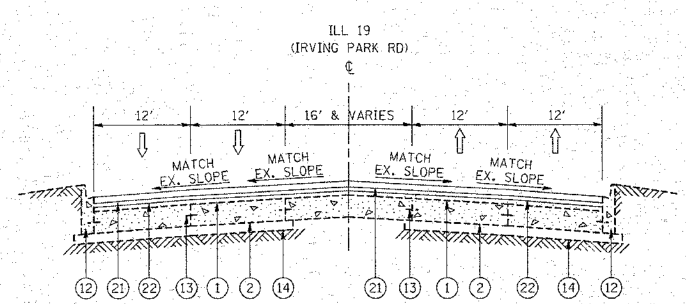
PROPOSED TYPICAL SECTION
STA. 11+52 TO STA. 14+76



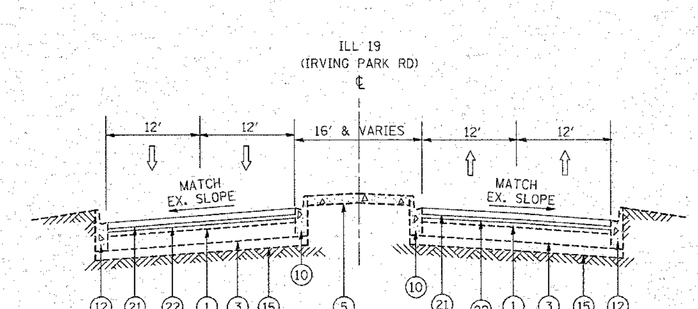
EXISTING TYPICAL SECTION
STA. 89+06 TO STA. 91+07
STA 93+30 TO STA 100+66



PROPOSED TYPICAL SECTION
STA. 14+76 TO STA. 17+05



PROPOSED TYPICAL SECTION
STA. 11+29 TO STA. 11+52



PROPOSED TYPICAL SECTION
STA. 17+05 TO STA. 23+58

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED
TYPICAL SECTIONS

SCALE: VERT.
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DATE

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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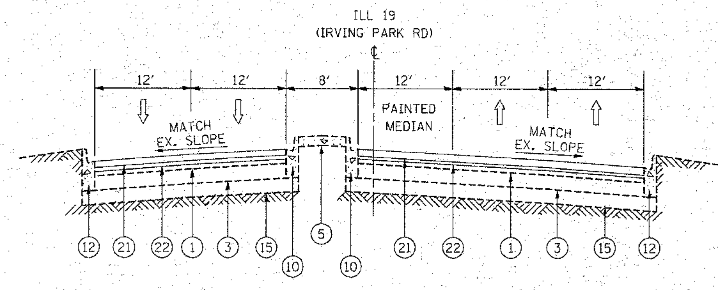
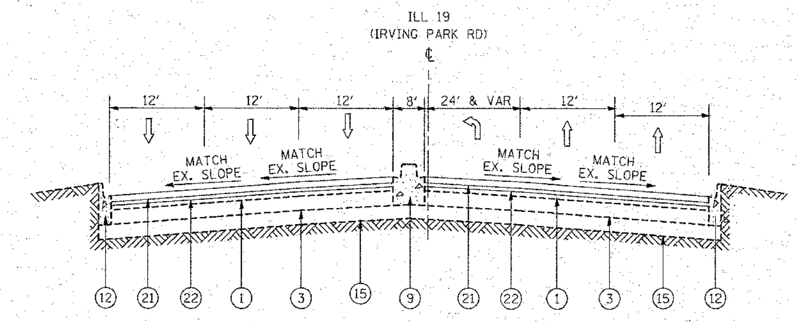
STA.		TO STA.		
FED. ROAD DIST. 1	ILLINOIS	FED. AID PROJECT		

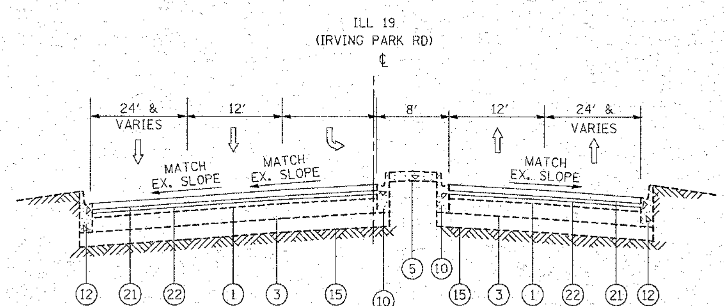
CONTRACT NO. 60A49



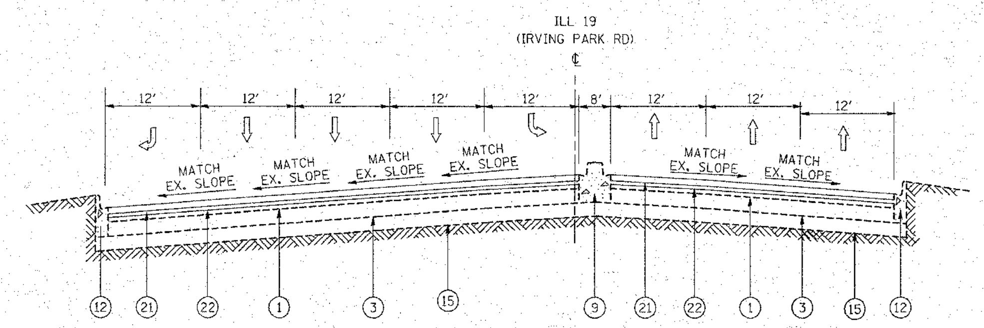
EXISTING TYPICAL SECTION
STA. 23+58 TO STA. 26+78



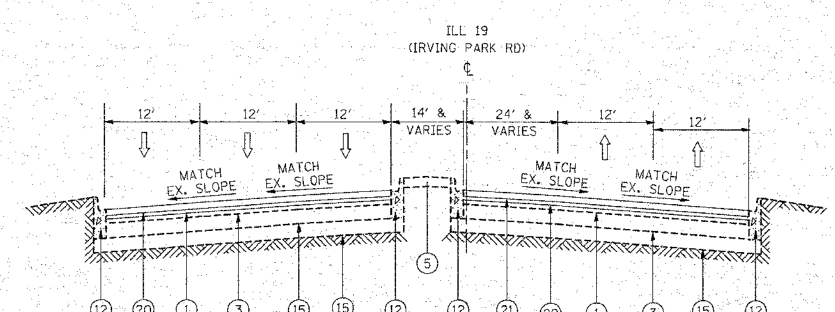
PROPOSED TYPICAL SECTION
STA. 32+33 TO STA. 35+39



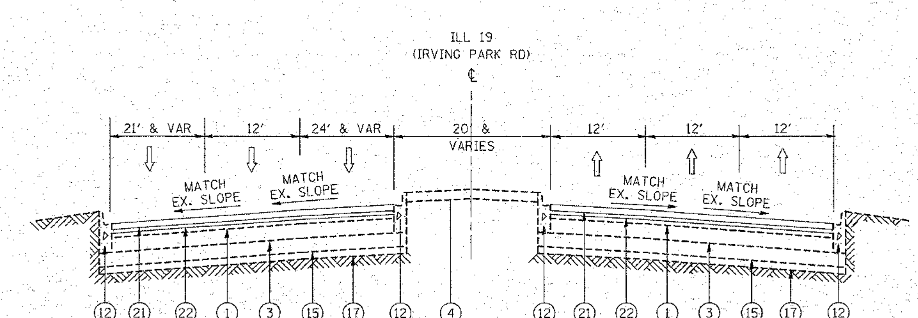
PROPOSED TYPICAL SECTION
STA. 26+78 TO STA. 28+76



PROPOSED TYPICAL SECTION
STA. 35+39 TO STA. 37+15



PROPOSED TYPICAL SECTION
STA. 28+76 TO STA. 32+33



PROPOSED TYPICAL SECTION
STA. 37+15 TO STA. 39+88

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT. HORIZ.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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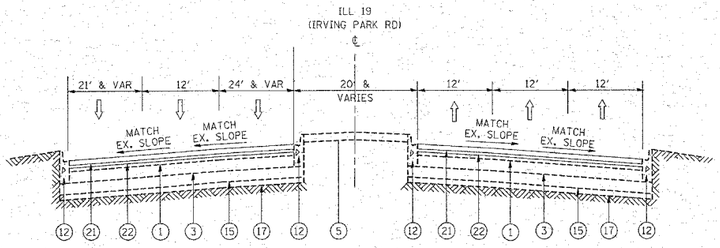
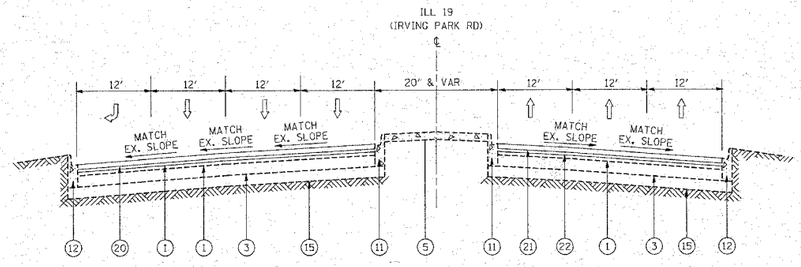
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

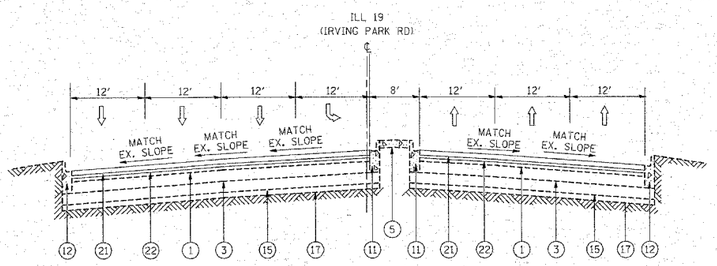
CONTRACT NO. 60A49



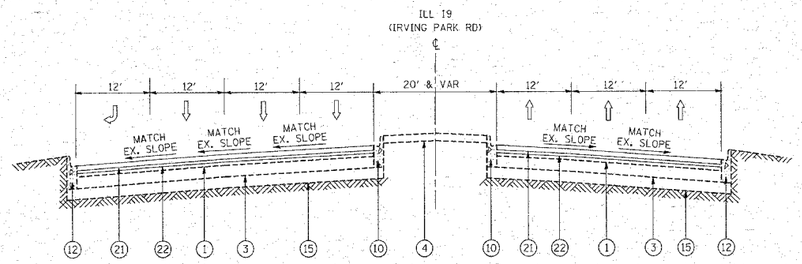
PROPOSED TYPICAL SECTION
STA. 39+88 TO STA. 42+27



PROPOSED TYPICAL SECTION
STA. 50+27 TO STA. 61+03



PROPOSED TYPICAL SECTION
STA. 42+27 TO STA. 43+45



PROPOSED TYPICAL SECTION
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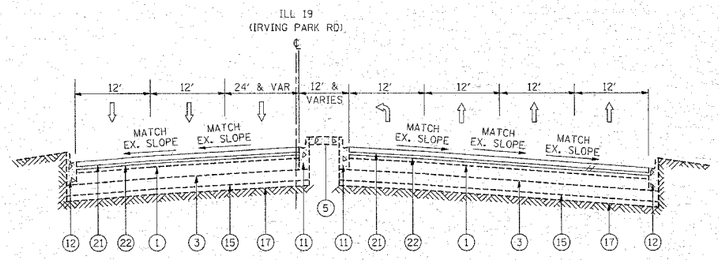
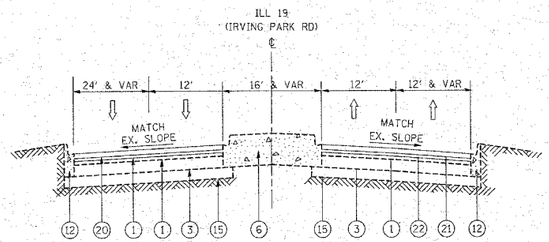


PROPOSED TYPICAL SECTION
STA. 43+45 TO STA. 50+27



PROPOSED TYPICAL SECTION
STA. 52+82 TO STA. 61+94

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT. / HORIZ.
DRAWN BY
DATE
CHECKED BY

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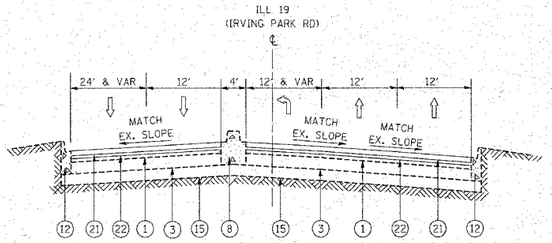
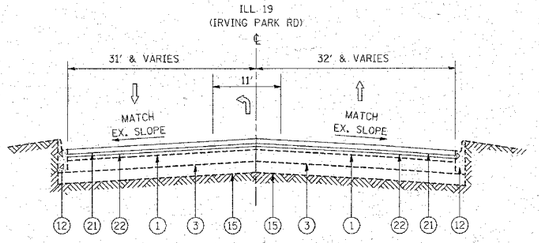
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	12
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

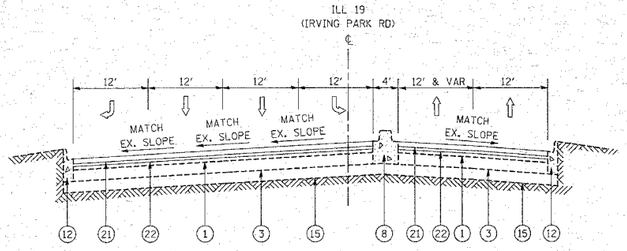
CONTRACT NO. 60A49



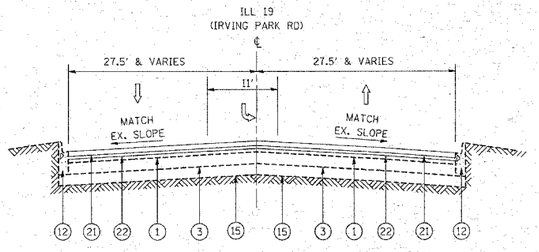
PROPOSED TYPICAL SECTION
STA. 61+94 TO STA. 64+38



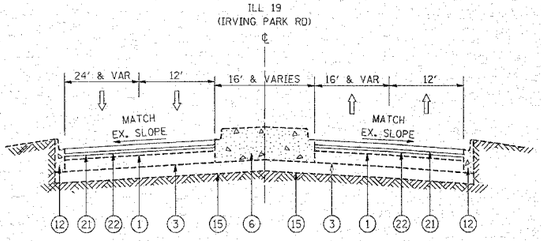
PROPOSED TYPICAL SECTION
STA. 69+84 TO STA. 71+98



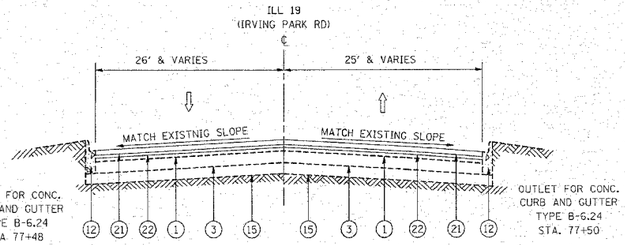
PROPOSED TYPICAL SECTION
STA. 64+38 TO STA. 66+37



PROPOSED TYPICAL SECTION
STA. 71+98 TO STA. 73+09



PROPOSED TYPICAL SECTION
STA. 66+37 TO STA. 69+84



OUTLET FOR CONC. CURB AND GUTTER TYPE B-6.24 STA. 77+48

OUTLET FOR CONC. CURB AND GUTTER TYPE B-6.24 STA. 77+50

PROPOSED TYPICAL SECTION
STA. 73+09 TO STA. 77+50

PROPOSED TYPICAL SECTION
STA. 52+82 TO STA. 61+94

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
8. EXISTING CONCRETE MEDIAN, TYPE SB-6.12
9. EXISTING CONCRETE MEDIAN, TYPE SB-6.24
10. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
11. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
13. EXISTING #5 DEFORMED TIE BARS, 18" LONG 30" CENTER
14. EXISTING STABILIZED SUB BASE, 4"
15. EXISTING AGGREGATE SUBGRADE, 12"
16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
- ✱ 23. PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

✱ LOCATIONS TO BE DETERMINED BY THE ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT. HORIZ.
DRAWN BY
DATE
CHECKED BY

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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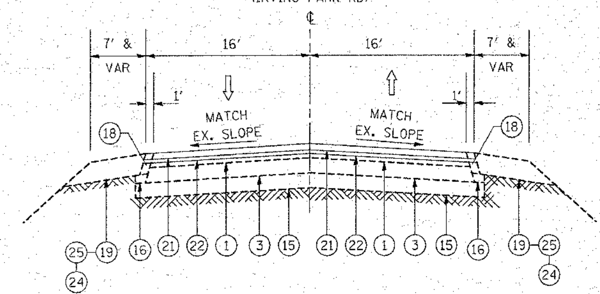
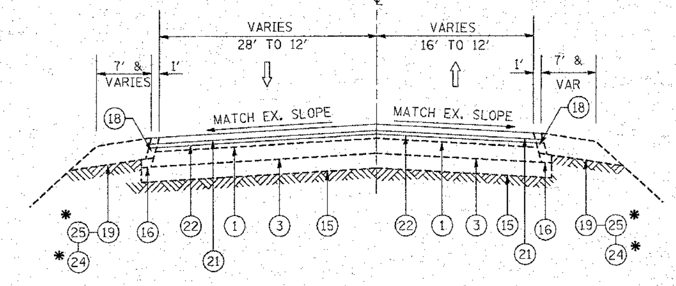
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

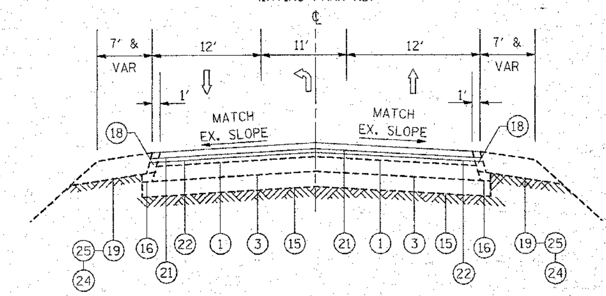
CONTRACT NO. 60A49



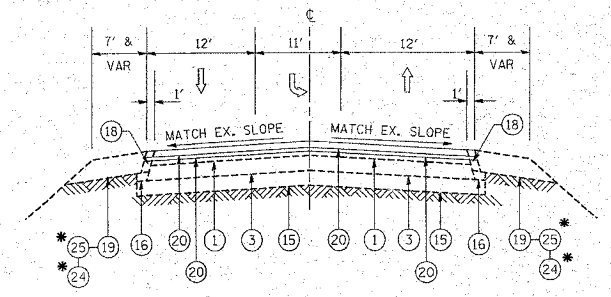
PROPOSED TYPICAL SECTION
STA. 77+50 TO STA. 84+65



PROPOSED TYPICAL SECTION
STA. 89+06 TO STA. 91+07
STA. 93+30 TO STA. 100+66



PROPOSED TYPICAL SECTION
STA. 84+65 TO STA. 87+22
STA. 91+07 TO STA. 93+30



PROPOSED TYPICAL SECTION
STA. 87+22 TO STA. 89+06

LEGEND

1. EXISTING HOT-MIX ASPHALT SURFACING
2. EXISTING PCC BASE COURSE, 9" (±)
3. EXISTING HOT-MIX ASPHALT BASE COURSE, 12" (±)
4. EXISTING STABILIZED MEDIAN SURFACE
5. EXISTING CONCRETE MEDIAN SURFACE, 4" (±)
6. EXISTING CONCRETE MEDIAN, TYPE SM-2.12
7. EXISTING CONCRETE MEDIAN, TYPE SB-9.12
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12. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
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16. EXISTING SUB BASE GRANULAR MATERIAL , TYPE B
17. EXISTING POROUS GRANULAR EMBANKMENT, SPECIAL
18. EXISTING HOT-MIX ASPHALT SHOULDER, 8"
19. EXISTING AGGREGATE WEDGE SHOULDER, TYPE B
20. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
21. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
22. PROPOSED POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50, 3/4"
23. * PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
24. PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
25. PROPOSED GRADING AND SHAPING SHOULDERS

* LOCATIONS TO BE DETERMINED BY THE ENGINEER

MIXTURE REQUIREMENTS

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

MIXTURE TYPE	AC/PG	AIR VOIDS (%)	THICKNESS
POLYMERIZED LEVELING BINDER (MM) IL-4.75, N50	SBS/SBR PG 76-28/-22	4% @ 50 GYR	3/4"
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 MM)	SBS/SBR PG 70-22	4% @ 90 GYR	1-3/4"
CLASS D PATCHES BINDER IL-19.0 MM	PG 64-22*	4% @ 70 GYR	12"
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, BINDER IL-19.0 MM	PG 64-22*	4% @ 70 GYR	4"

NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURE QUANTITIES IS 112 POUNDS PER SQUARE YARD PER INCH.

* WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

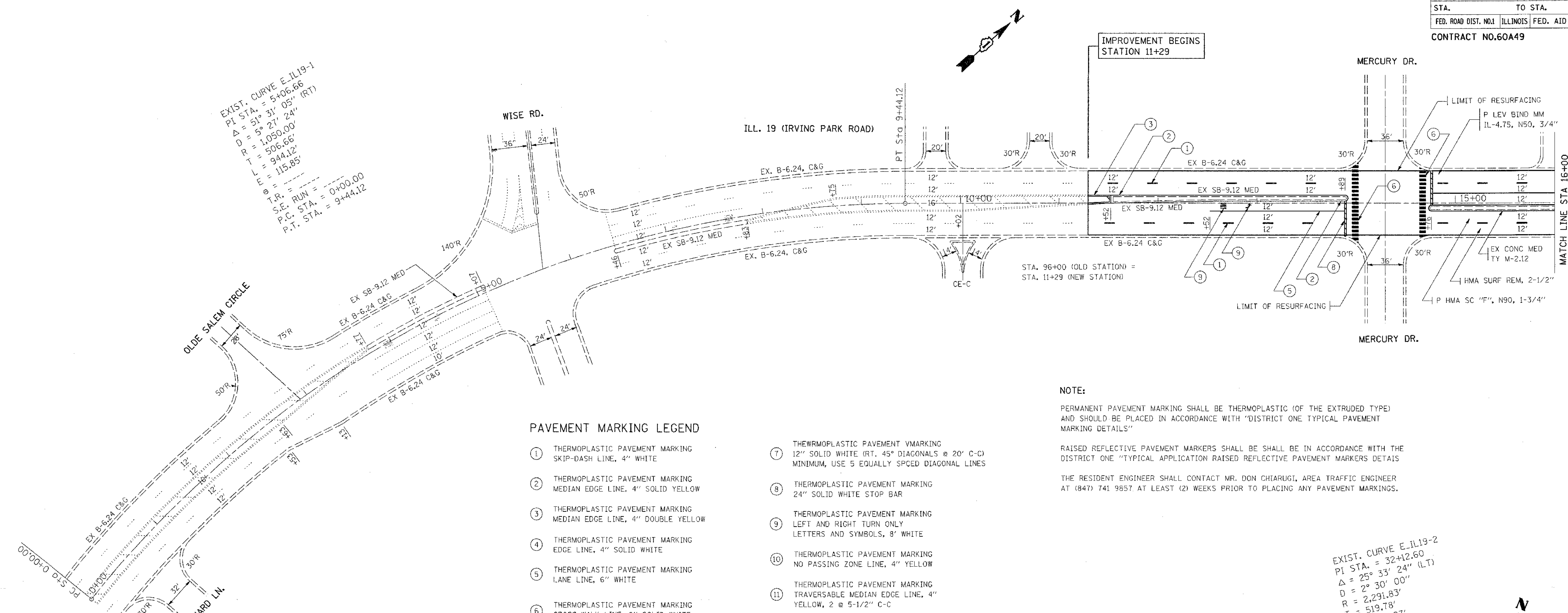
EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: VERT. HORIZ.
DATE
DRAWN BY
CHECKED BY

PLOT DATE = 4/2/2008

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	14
STA.		TO STA.		
FED. ROAD DIST. NO.1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO.60A49



PAVEMENT MARKING LEGEND

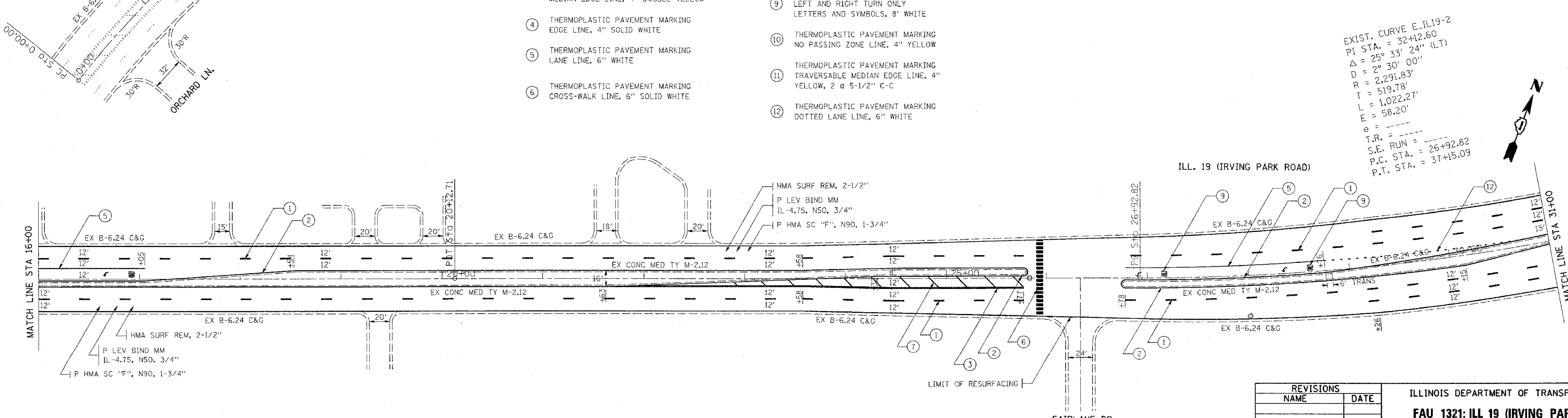
1. THERMOPLASTIC PAVEMENT MARKING SKIP-DASH LINE, 4″ WHITE
2. THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGE LINE, 4″ SOLID YELLOW
3. THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGE LINE, 4″ DOUBLE YELLOW
4. THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4″ SOLID WHITE
5. THERMOPLASTIC PAVEMENT MARKING LANE LINE, 6″ WHITE
6. THERMOPLASTIC PAVEMENT MARKING CROSS-WALK LINE, 6″ SOLID WHITE
7. THEWRMOPLASTIC PAVEMENT VMARKING 12″ SOLID WHITE (RT. 45° DIAGONALS @ 20′ C-C) MINIMUM, USE 5 EQUALLY SPCED DIAGONAL LINES
8. THERMOPLASTIC PAVEMENT MARKING 24″ SOLID WHITE STOP BAR
9. THERMOPLASTIC PAVEMENT MARKING LEFT AND RIGHT TURN ONLY LETTERS AND SYMBOLS, 8′ WHITE
10. THERMOPLASTIC PAVEMENT MARKING NO PASSING ZONE LINE, 4″ YELLOW
11. THERMOPLASTIC PAVEMENT MARKING TRAVERSABLE MEDIAN EDGE LINE, 4″ YELLOW, 2 @ 5-1/2″ C-C
12. THERMOPLASTIC PAVEMENT MARKING DOTTED LANE LINE, 6″ WHITE

NOTE:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAIS

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC ENGINEER AT (847) 741 9857 AT LEAST (2) WEEKS PRIOR TO PLACING ANY PAVEMENT MARKINGS.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN

SCALE: VERT. HORIZ.
DATE
DRAWN BY
CHECKED BY

PLOT DATE = 4/22/2008
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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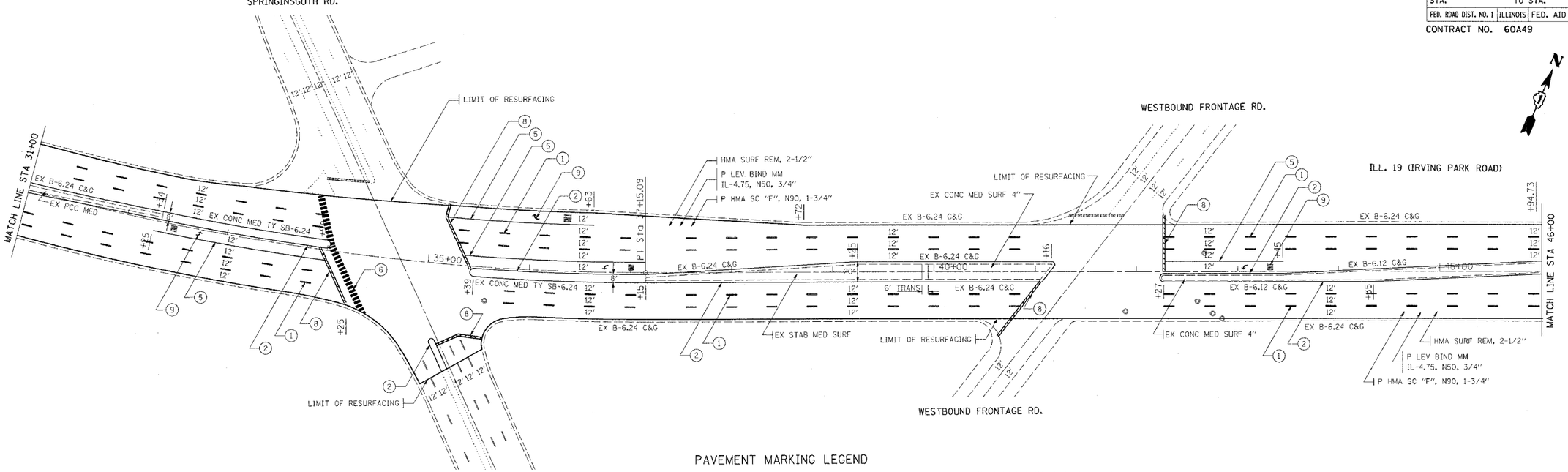
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



NOTE:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"

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PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING SKIP-DASH LINE, 4" WHITE
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5. THERMOPLASTIC PAVEMENT MARKING LANE LINE, 6" WHITE
6. THERMOPLASTIC PAVEMENT MARKING CROSS-WALK LINE, 6" SOLID WHITE, 2@6' C-C AND 4' IN ADVANCE OF STOP BAR
7. THEWRMOPLASTIC PAVEMENT VMARKING 12" SOLID WHITE (RT. 45° DIAGONALS @ 20' C-C) MINIMUM, USE 5 EQUALLY SPCED DIAGONAL LINES
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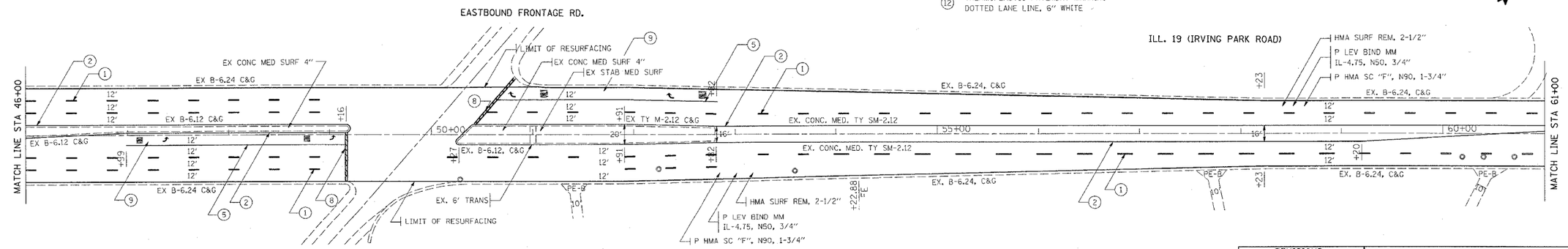

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD) WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN

SCALE: VERT. HORIZ. DRAWN BY

DATE CHECKED BY

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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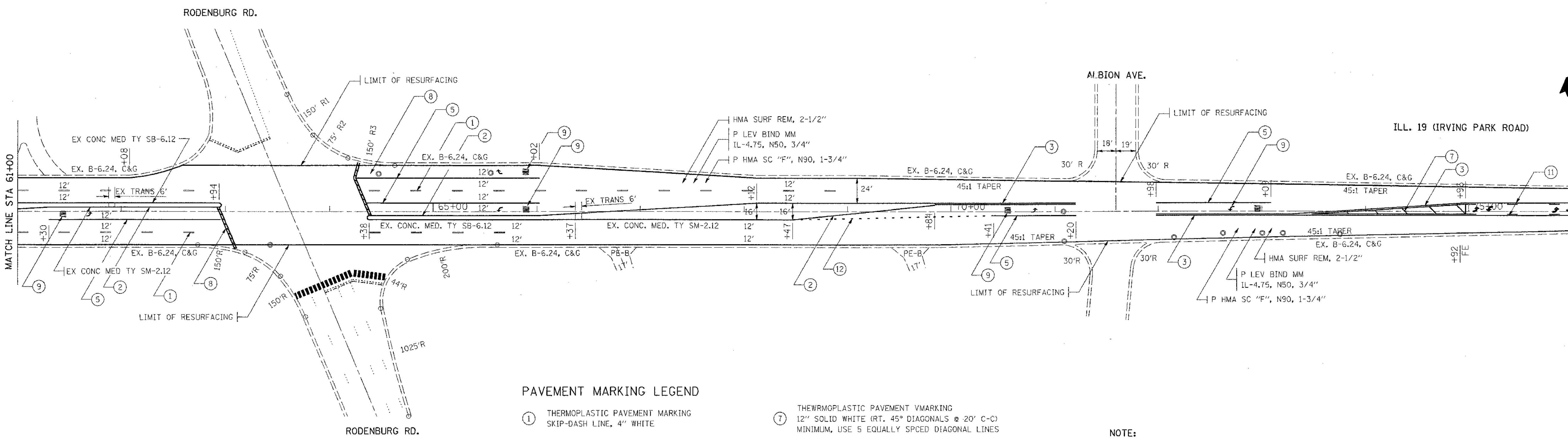
STA.		TO STA.		
FED. ROAD DIST. NO.1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



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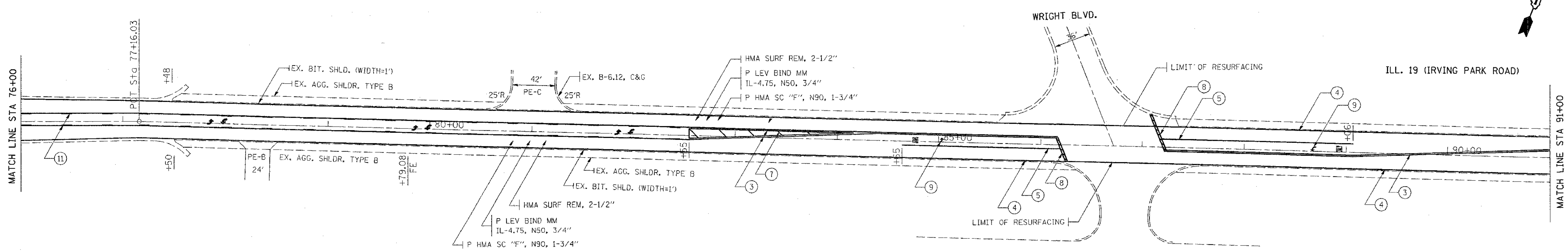
NOTE:

PERMANENT PAVEMENT MARKING SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKING DETAILS"

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAIS

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC ENGINEER AT (847) 741 9857 AT LEAST (2) WEEKS PRIOR TO PLACING ANY PAVEMENT MARKINGS.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE

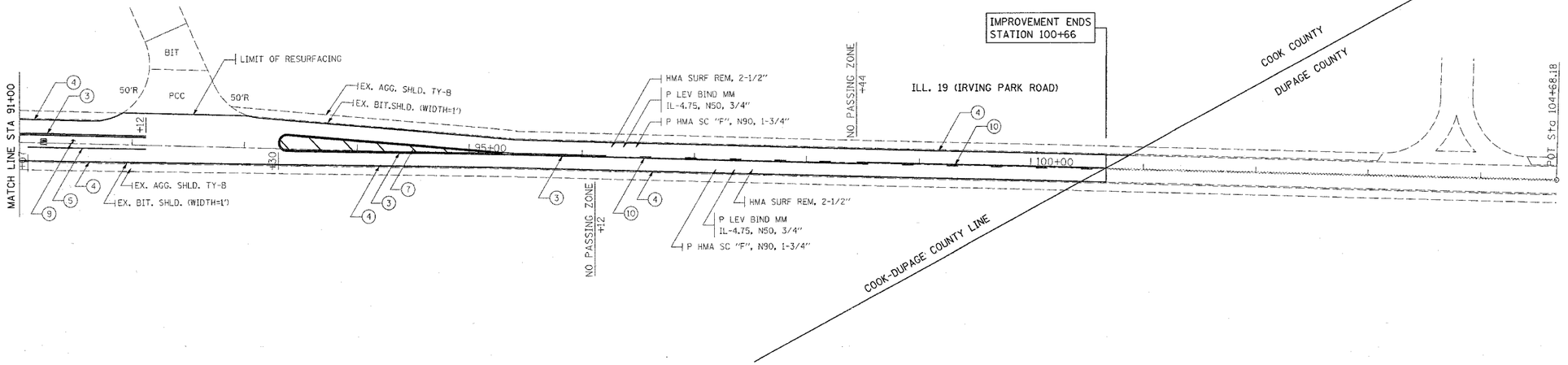
EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN

SCALE: VERT. HORIZ.
DATE
DRAWN BY
CHECKED BY

PLOT DATE = 4/22/2008
PLOT SCALE = 50.0000 ' / IN.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	17
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



PAVEMENT MARKING LEGEND

1. THERMOPLASTIC PAVEMENT MARKING SKIP-DASH LINE, 4" WHITE
2. THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGE LINE, 4" SOLID YELLOW
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7. THEWRMOPLASTIC PAVEMENT VMARKING 12" SOLID WHITE (RT. 45° DIAGONALS @ 20' C-C) MINIMUM, USE 5 EQUALLY SPCED DIAGONAL LINES
8. THERMOPLASTIC PAVEMENT MARKING 24" SOLID WHITE STOP BAR
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NOTE:

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THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC ENGINEER AT (847) 741 9857 AT LEAST (2) WEEKS PRIOR TO PLACING ANY PAVEMENT MARKINGS.

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD) WISE ROAD TO COOK-DUPAGE COUNTY LINE

EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLAN

SCALE: VERT. HORIZ. DRAWN BY

DATE CHECKED BY

PLOT DATE = 4/22/2008
FILE NAME = o:\projects\d86206\design_ea.dgn
PLOT SCALE = 50.0000 ' / IN.

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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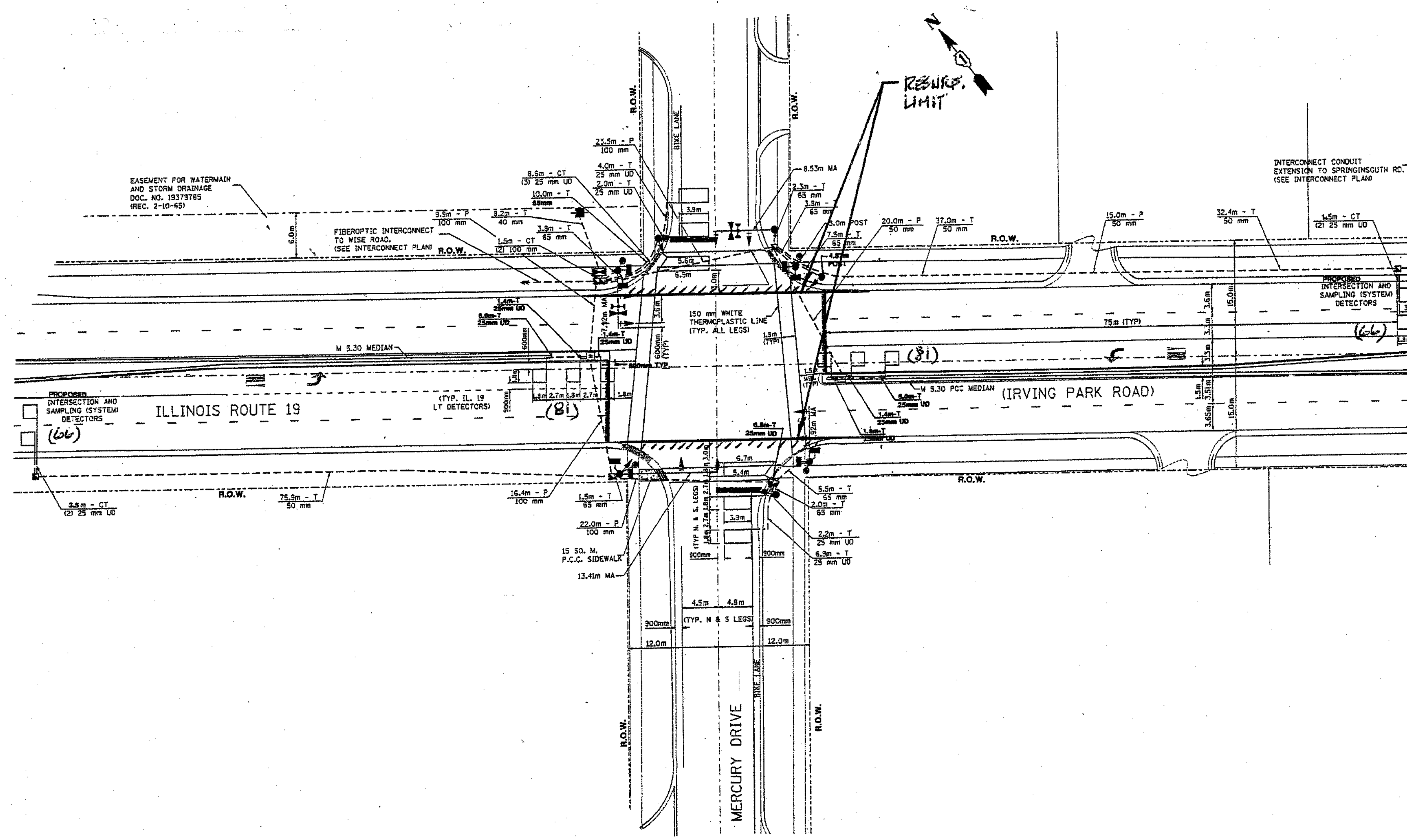
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	294	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ MERCURY DRIVE

SCALE: VERT. HORIZ. DRAWN BY
DATE CHECKED BY

PLOT DATE = 9/20/2006
FILE NAME = o:\projects\d1062006\design-ee.dgn
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USER NAME = gelbannb

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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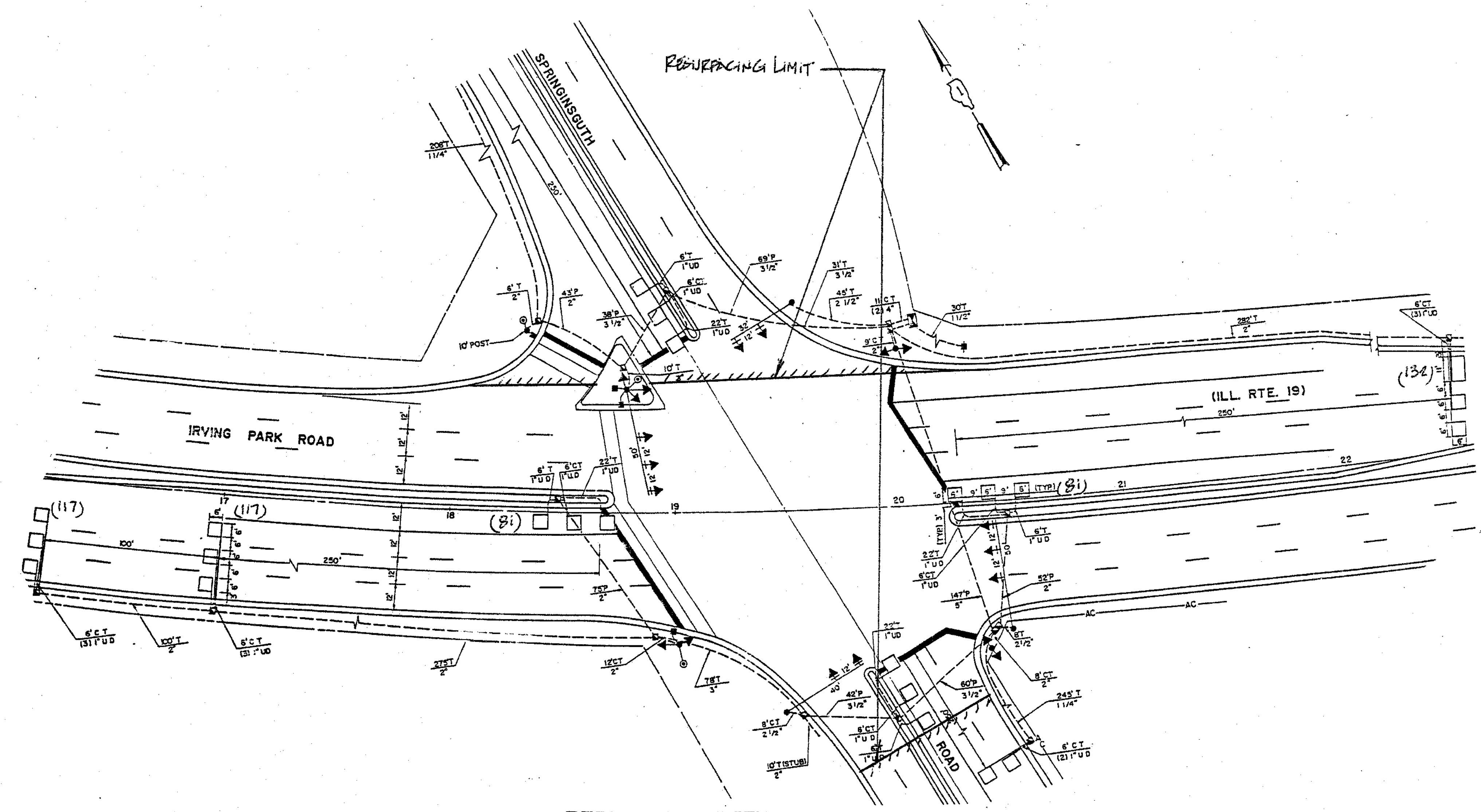
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	528	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

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FILE NAME = c:\projects\d106206\design-oa.dgn
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USER NAME = galbeano

REVISIONS	
NAME	DATE

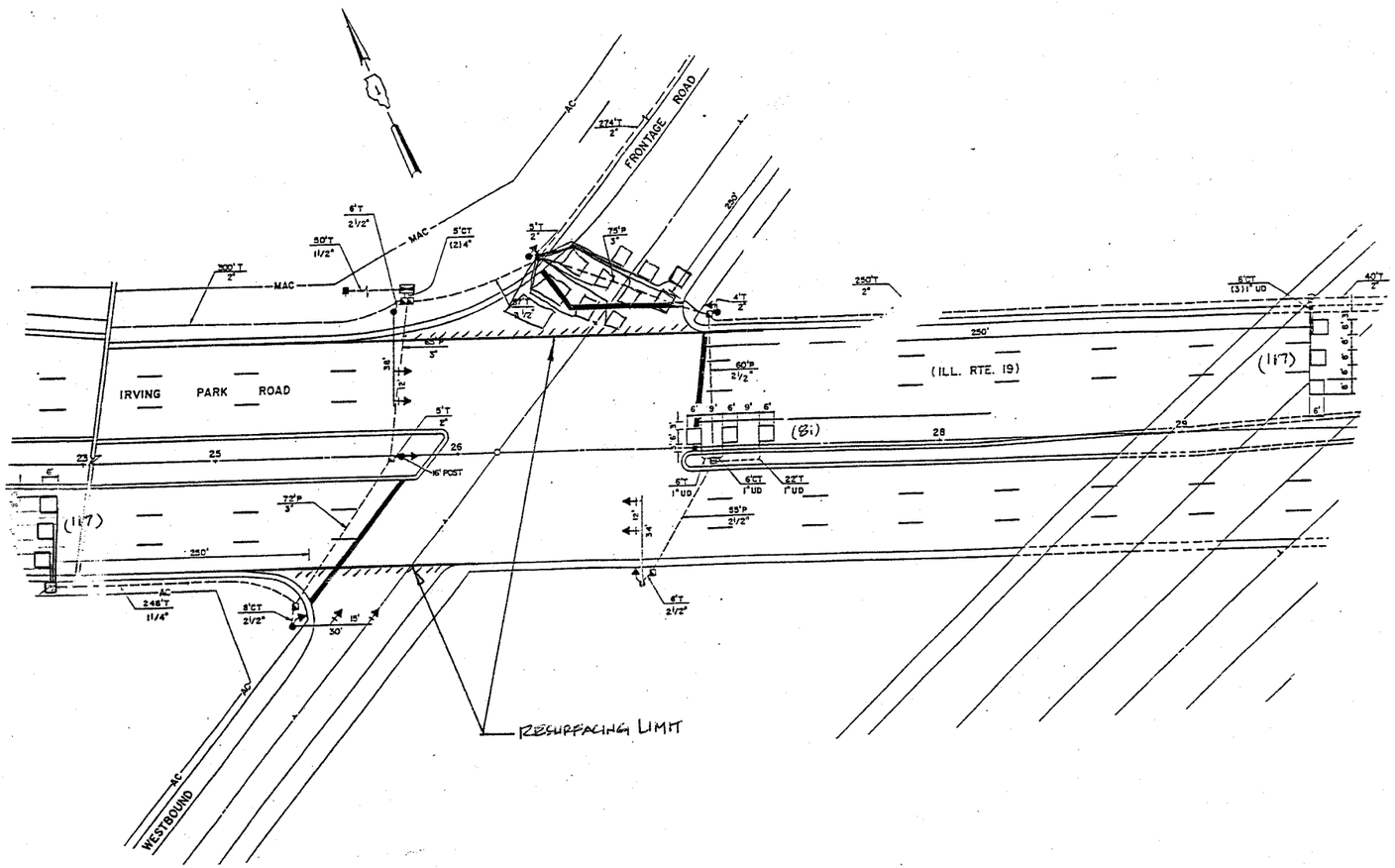
ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ SPRINGINSGUTH ROAD

SCALE: VERT. HORIZ. | DRAWN BY
DATE | CHECKED BY

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	20
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	315	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

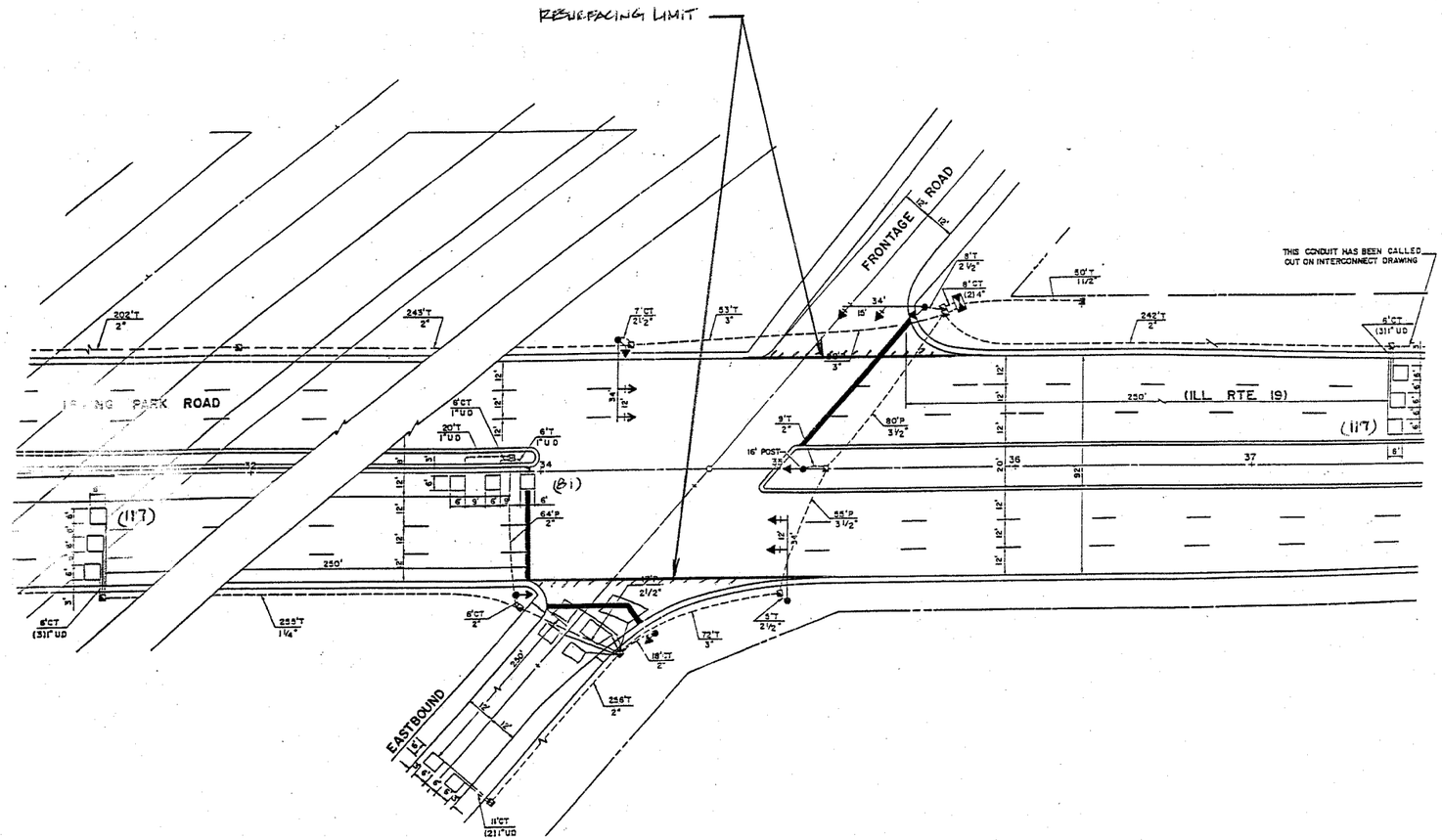
FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ WEST FRONTAGE ROAD

SCALE: VERT.
HORIZ.
DRAWN BY
DATE
CHECKED BY

PLOT DATE = 9/20/2006
FILE NAME = c:\projects\d1062006\design-aa.dgn
PLOT SCALE = 50.0000 ' / IN.
USER NAME = galbanmb

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	21
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	315	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

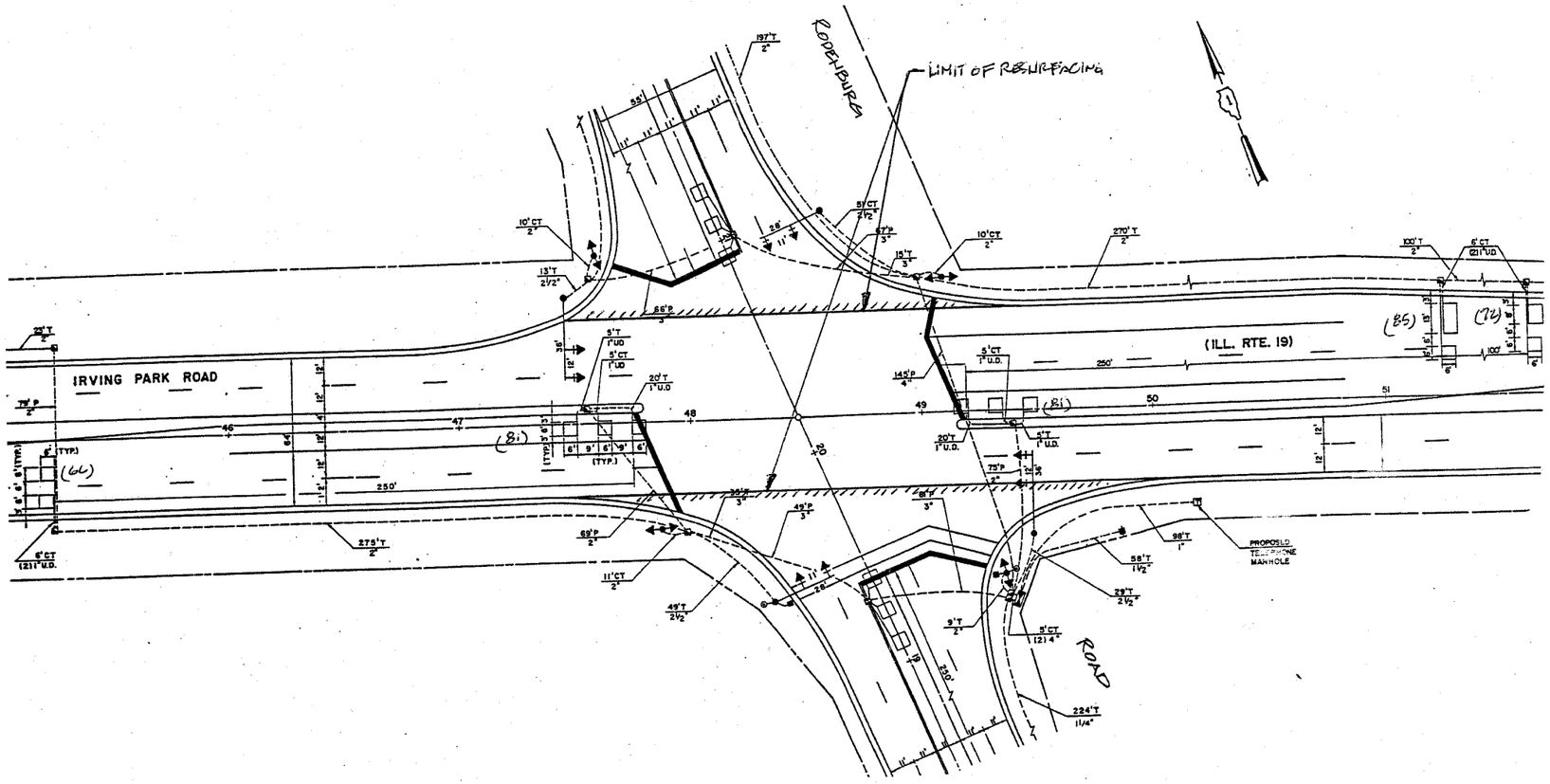
FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ EAST FRONTAGE ROAD

SCALE: VERT. HORIZ. DRAWN BY
DATE CHECKED BY

PLOT DATE = 9/28/2006
FILE NAME = c:\projects\d0828b\design-aa.dgn
PLOT SCALE = 50.0000 ' / IN.
USER NAME = galbenb

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	22
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	385	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ RODENBURG ROAD

SCALE: VERT. HORIZ. DRAWN BY

DATE CHECKED BY

PLOT DATE = 9/28/2006
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USER NAME = gstevens

CONTRACT NO. 60A49

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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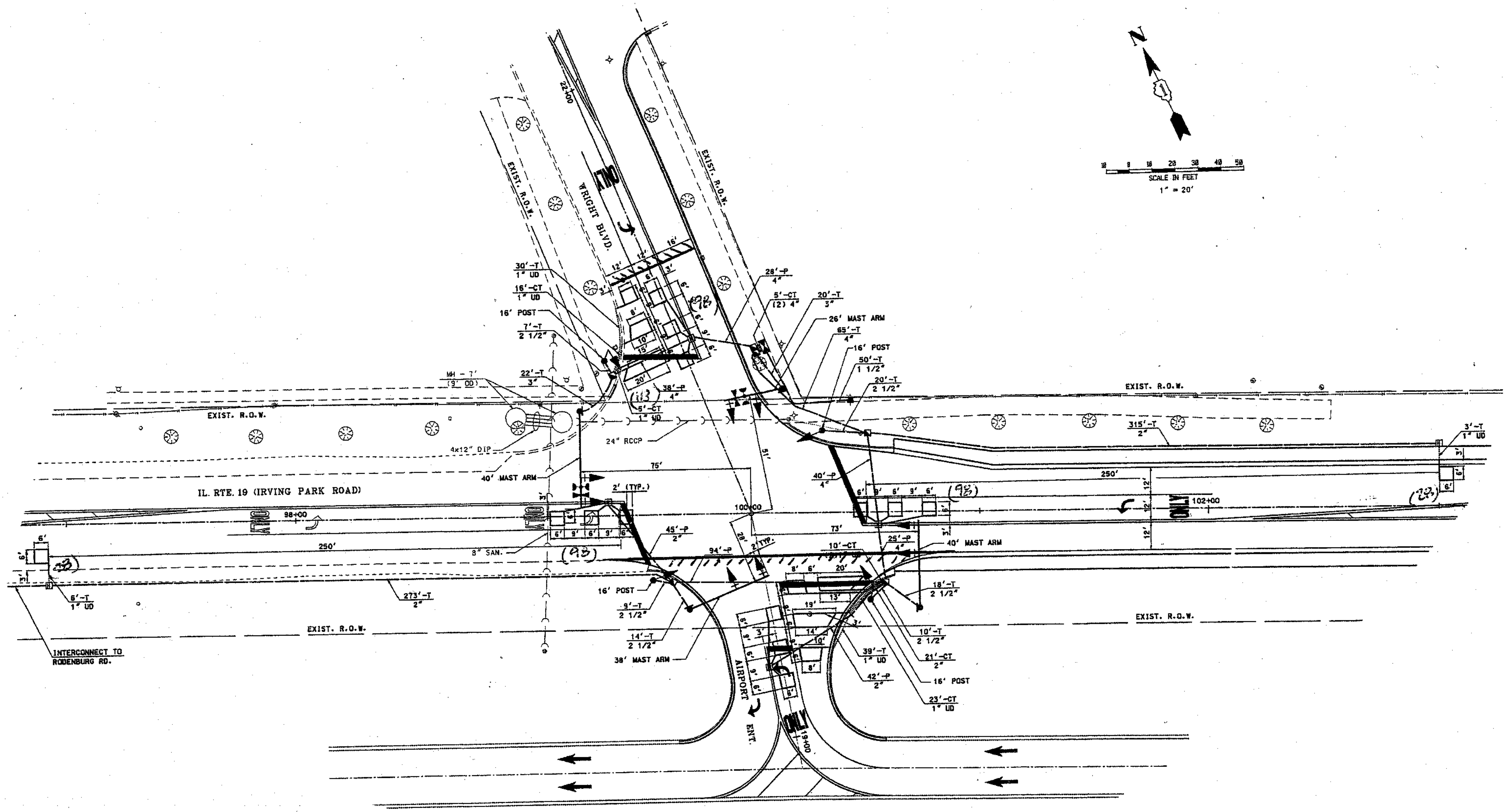
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
	463	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

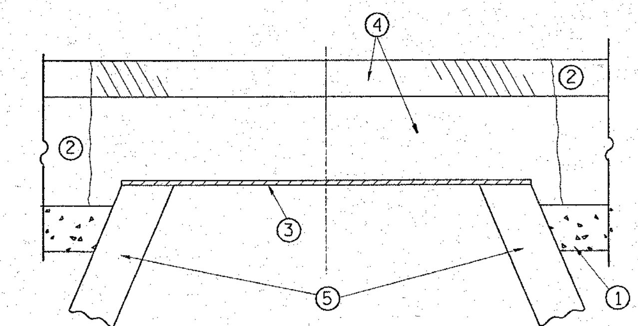
FAU 1321: ILL 19 (IRVING PARK ROAD)
WISE ROAD TO COOK-DUPAGE COUNTY LINE
DETECTOR LOOP REPLACEMENT
ILL RTE 19 (IRVING PARK ROAD)
@ WRIGHT BLVD

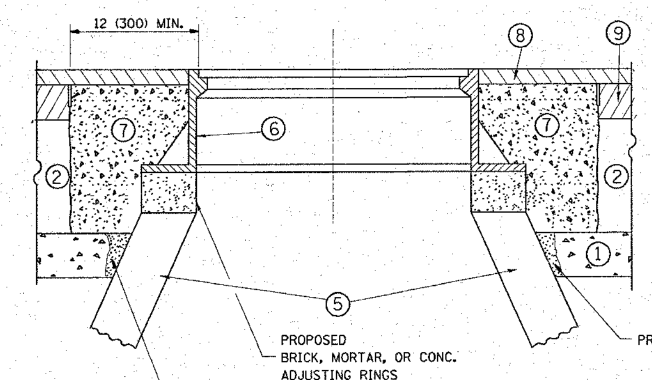
SCALE: VERT. HORIZ. DRAWN BY
DATE CHECKED BY

PLOT DATE = 9/20/2006
FILE NAME = c:\projects\d106206\design-ea.dgn
PLOT SCALE = 50.0000 ' / IN.
USER NAME = golbenvb

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	24
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49





CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. HORIZ. NONE

DRAWN BY

CHECKED BY

BD600-03 (BD-8)

PLOT DATE = 3/10/2008
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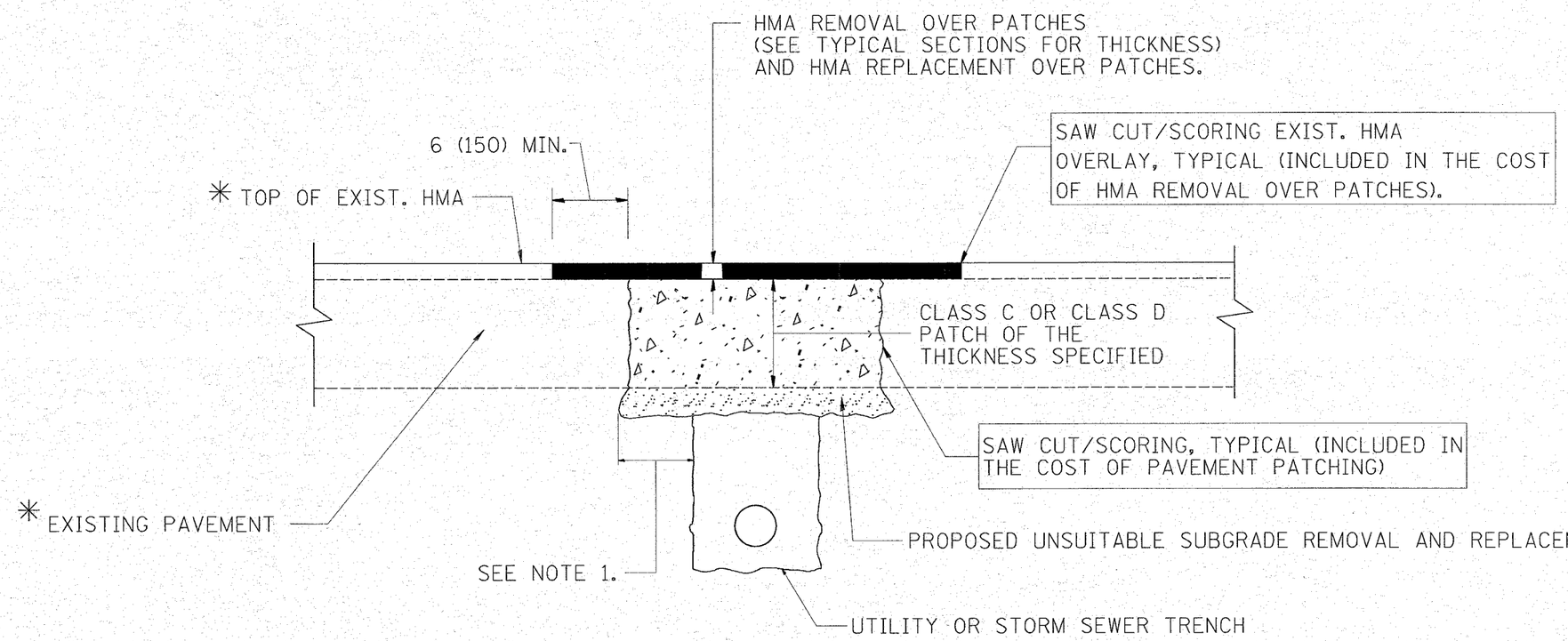
PLOT SCALE = 50.0000 ' / IN.
USER NAME = galbannb

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	25
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07
R. BORO	09/04/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE HORIZ.

DRAWN BY

CHECKED BY

BD400-04 (BD-22)

PLOT DATE = 3/18/2009
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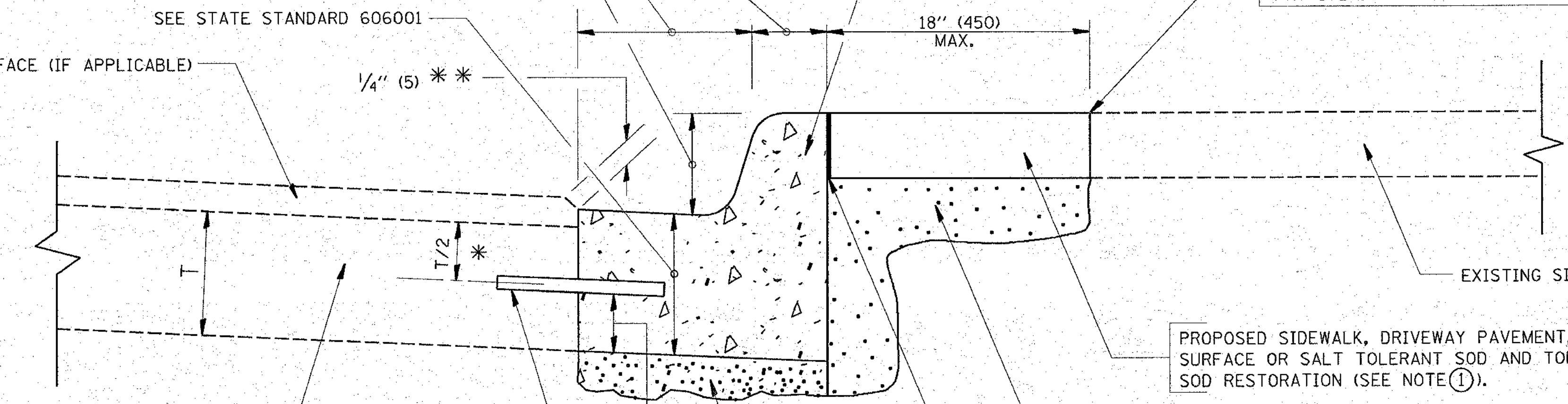
PLOT NUMBER = 1:1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	26
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



✱ 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

✱✱ IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR
CURB AND GUTTER
REMOVAL AND REPLACEMENT

SCALE: VERT. HORIZ. NONE

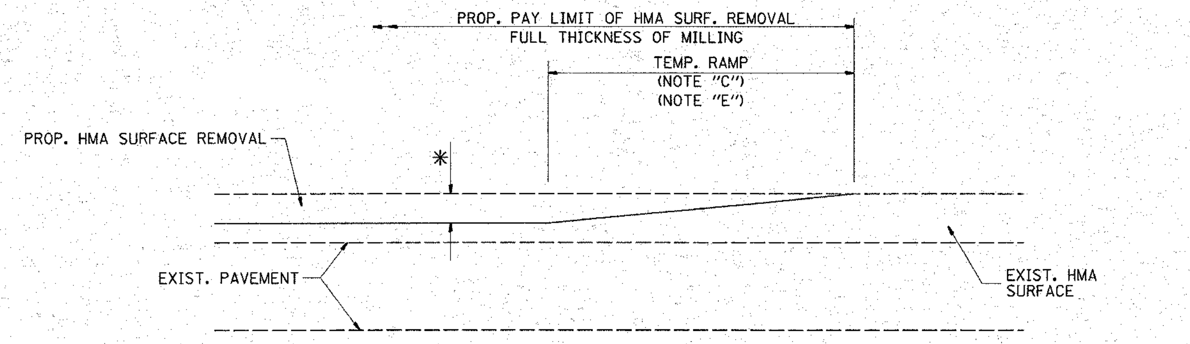
DRAWN BY

CHECKED BY

BD600-06 (BD-24)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	<0711 RS-2>>	COOK	38	27
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

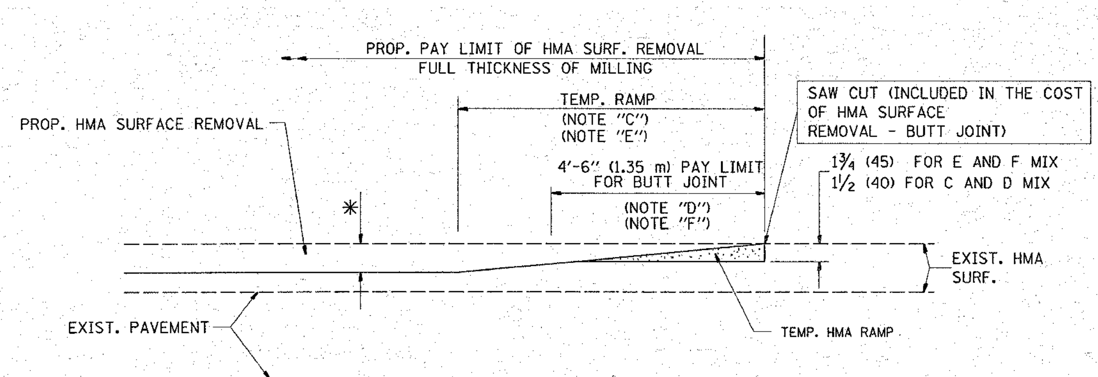
CONTRACT NO. 62703



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

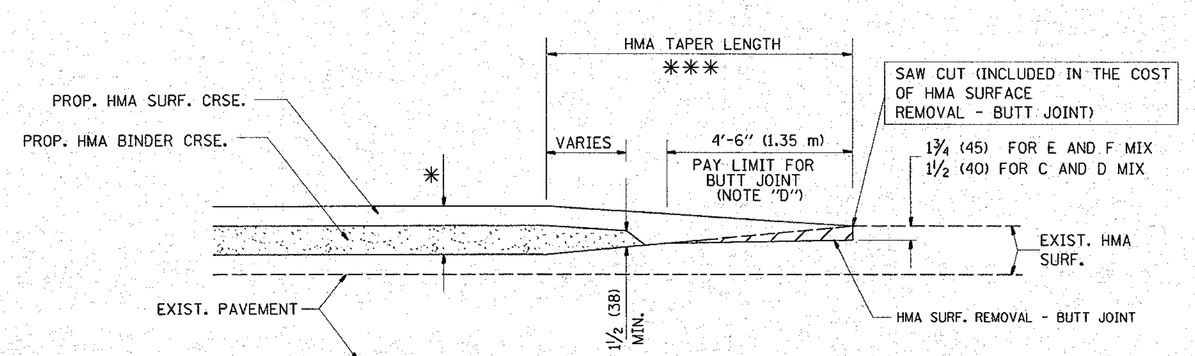


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

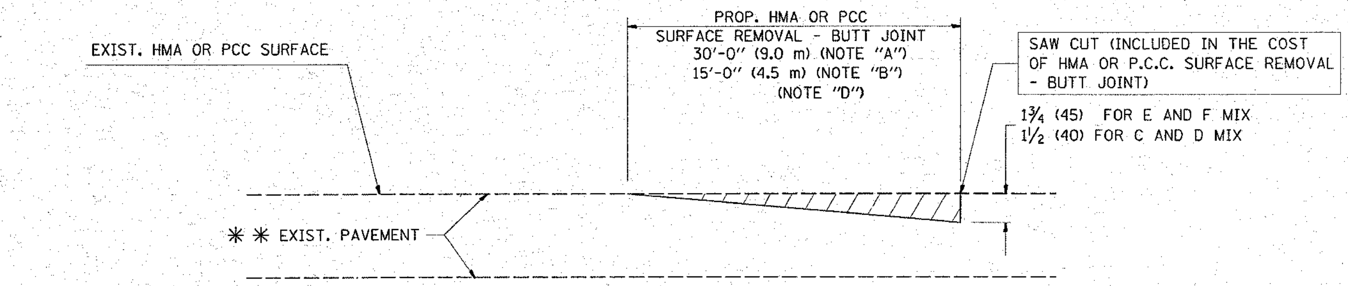
OPTION 2

TYPICAL TEMPORARY RAMP

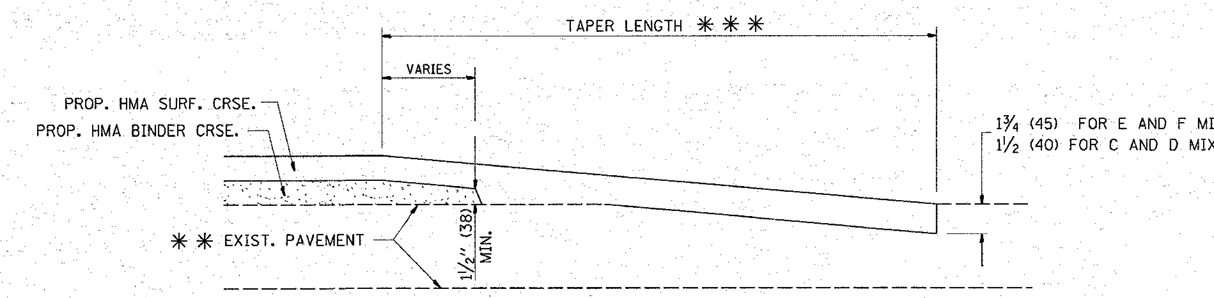


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

REVISIONS NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. HORIZ. NONE

DRAWN BY

CHECKED BY

BD400-05 (VI=BD32)

PLOT DATE = 3/10/2008
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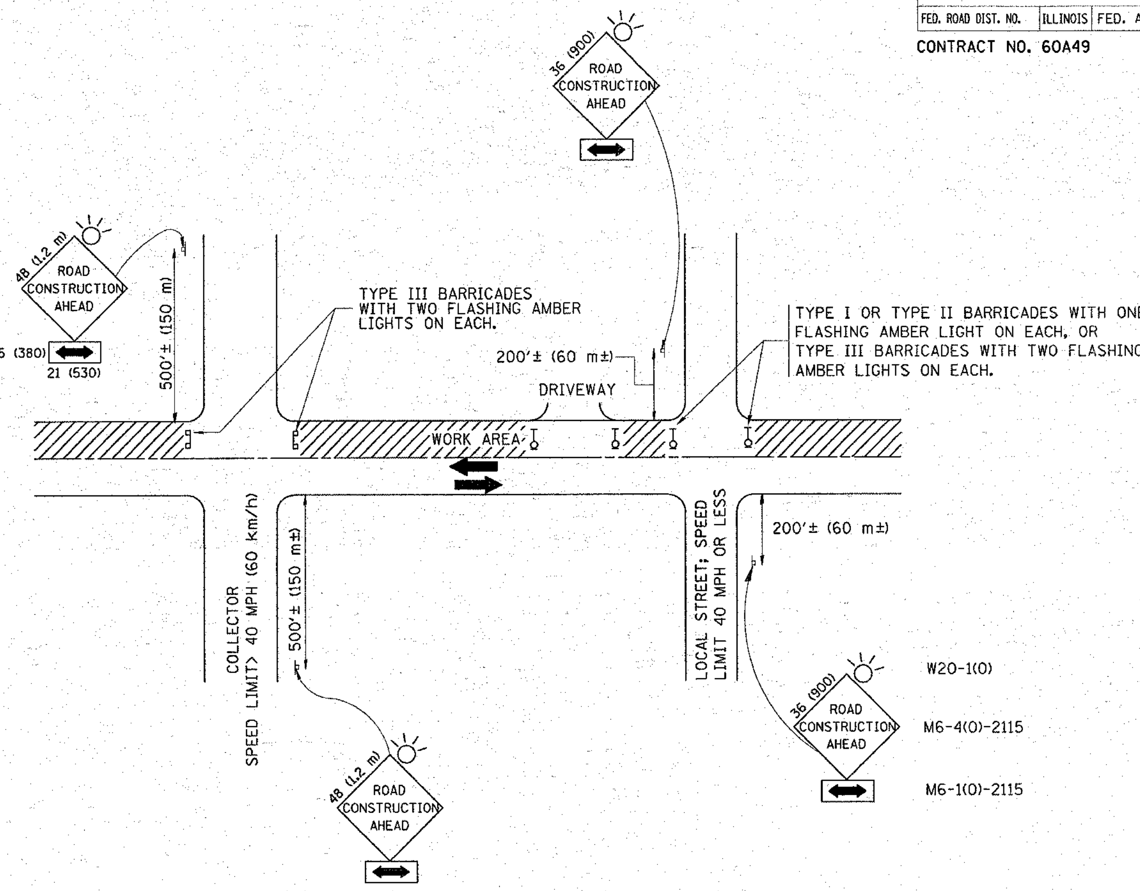
USER NAME = gallbennb

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	28
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE

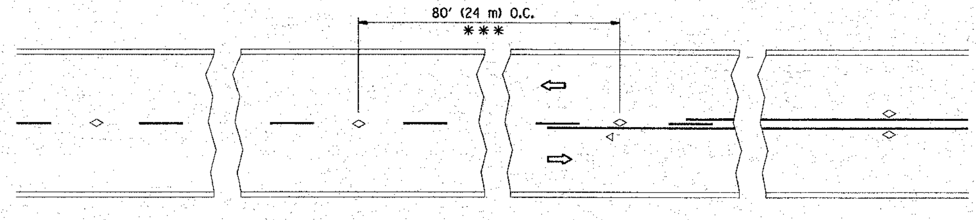
DRAWN BY

CHECKED BY

TC-10

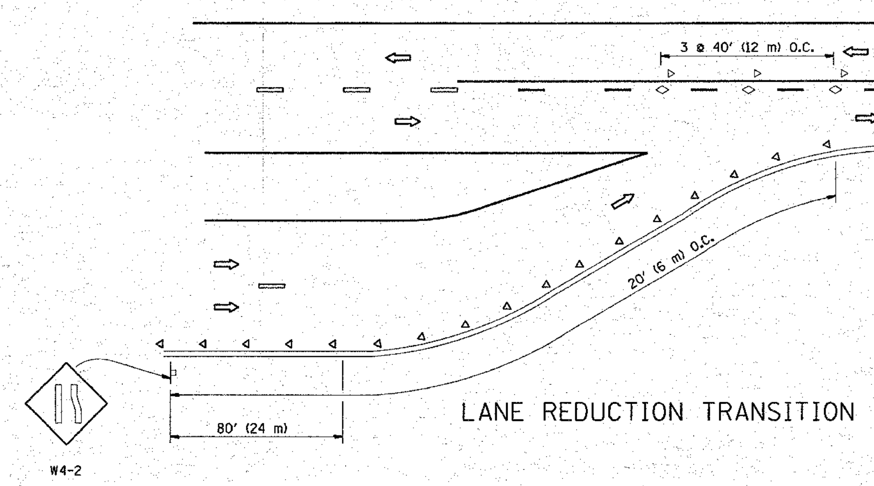
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	29
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49

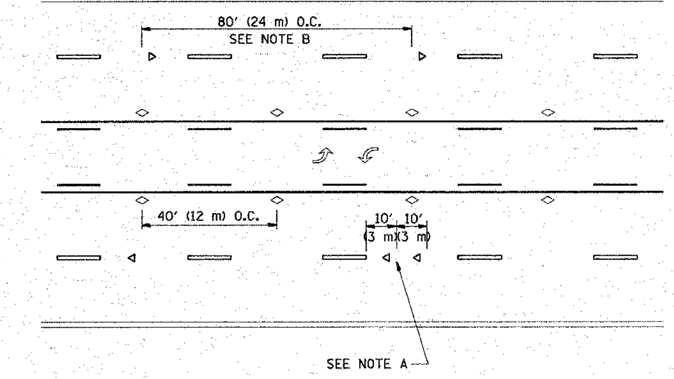


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

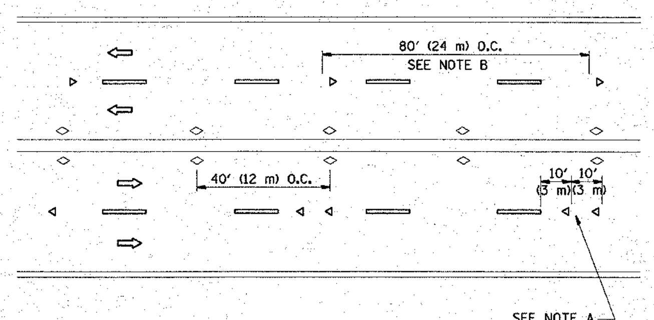
TWO-LANE/TWO-WAY



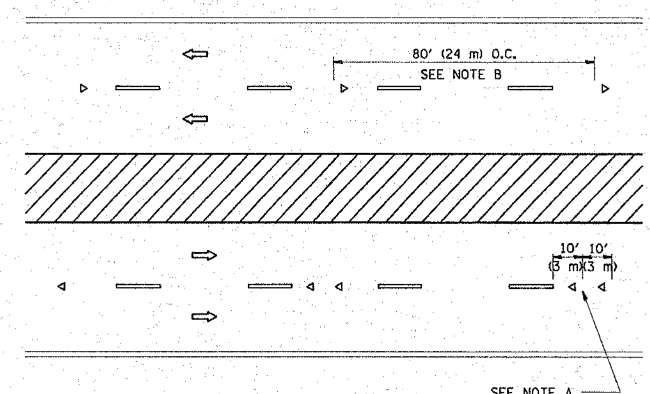
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

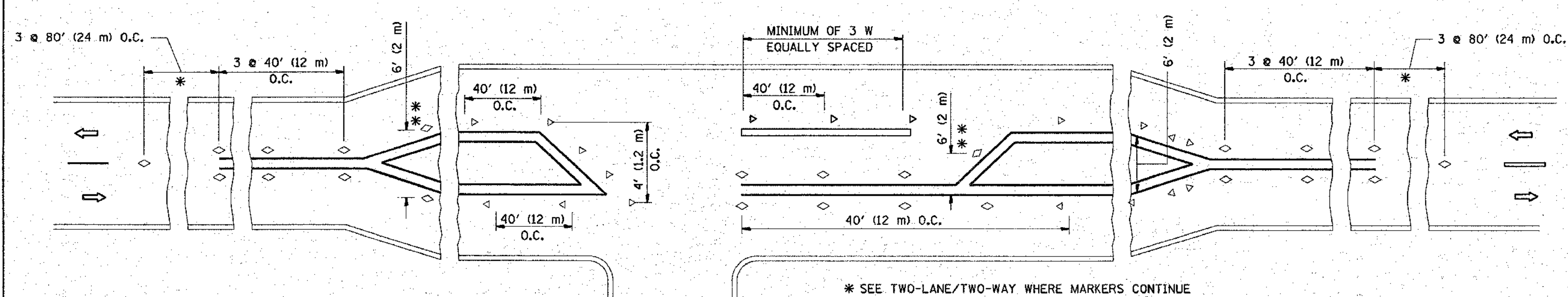
A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

DRAWN BY CADD

CHECKED BY

TC-11

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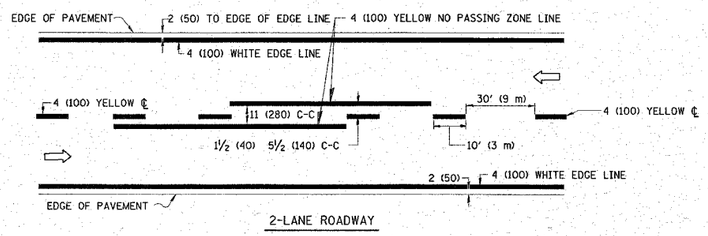
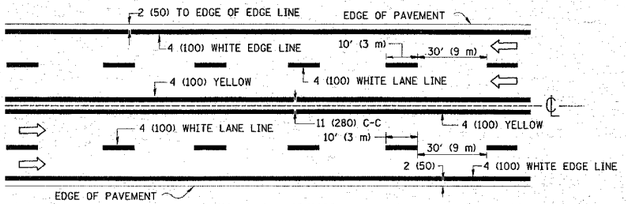
USER NAME = galbannb

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	30
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

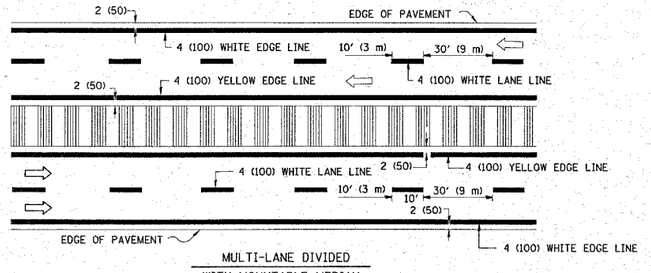
CONTRACT NO. 60A49



2-LANE ROADWAY



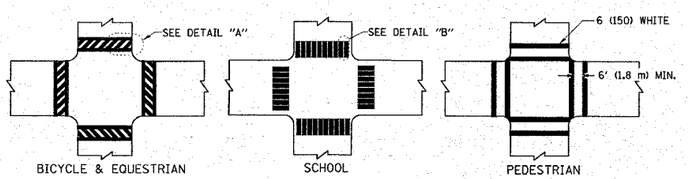
MULTI-LANE UNDIVIDED



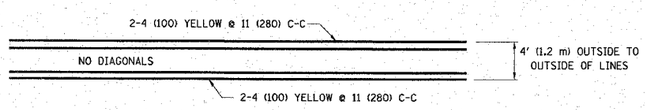
MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

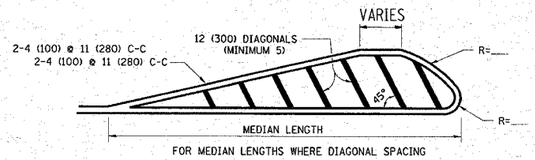
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



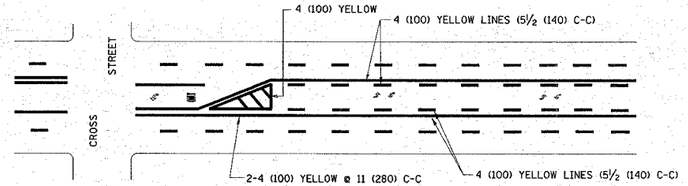
4' (1.2 m) WIDE MEDIANS ONLY



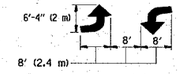
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

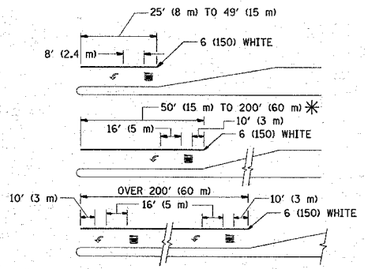


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

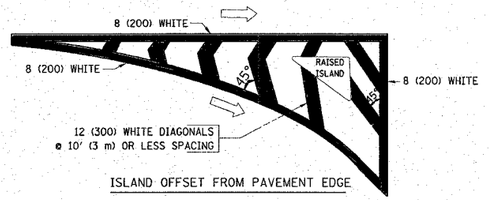


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) ONLY AREA = 20.8 SQ. FT. (1.9 m^2)

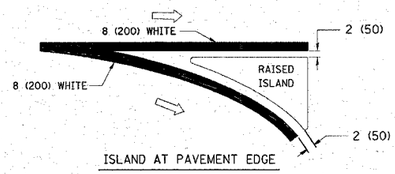
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE

DRAWN BY CADD

CHECKED BY

TC-13

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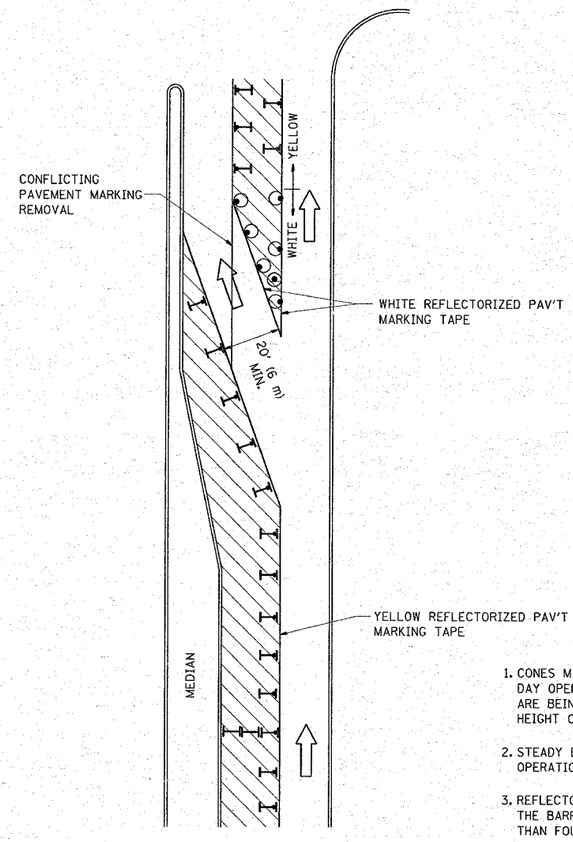
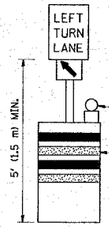
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	31
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49





R 3-1100L 24 x 24 (600 x 600)

M6-2L 21 x 15 (530 x 380)

OPTIONAL FLASHING LIGHT

STANDARD 702001
DRUM FILLED WITH ENOUGH
SAND (BAGS) FOR STABILIZATION

GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

WORK AREA

LANE OPEN TO TRAFFIC

TYPE I OR II BARRICADE WITH STEADY BURN LIGHT

DRUM WITH STEADY BURN LIGHT

DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL

TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE

DRAWN BY

CHECKED BY LHA

TC-14

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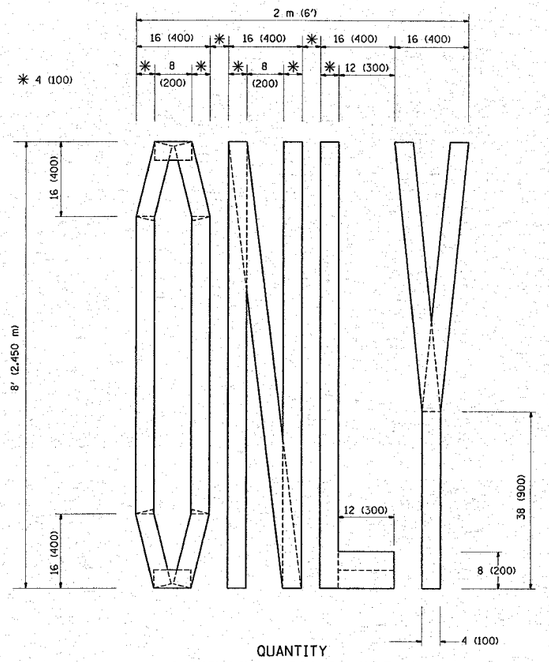
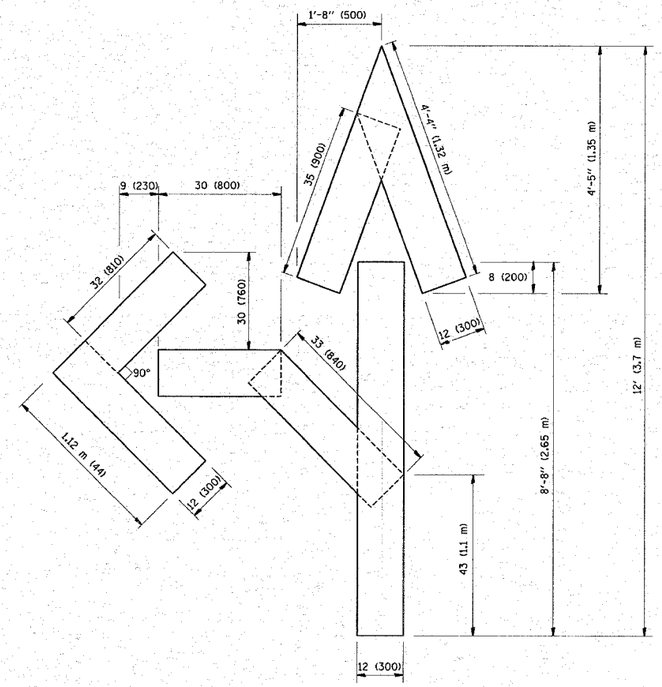
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	32
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

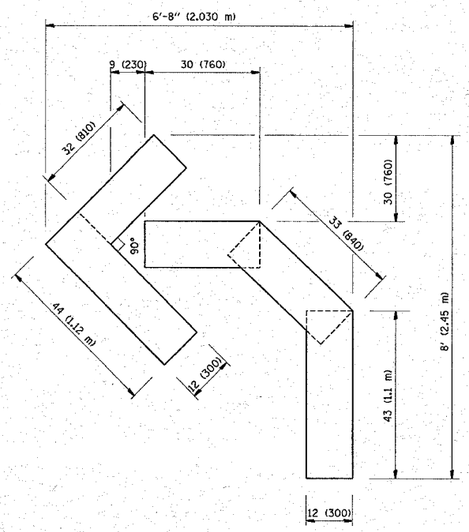
CONTRACT NO. 60A49



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE

DRAWN BY CADD
CHECKED BY

TC-16

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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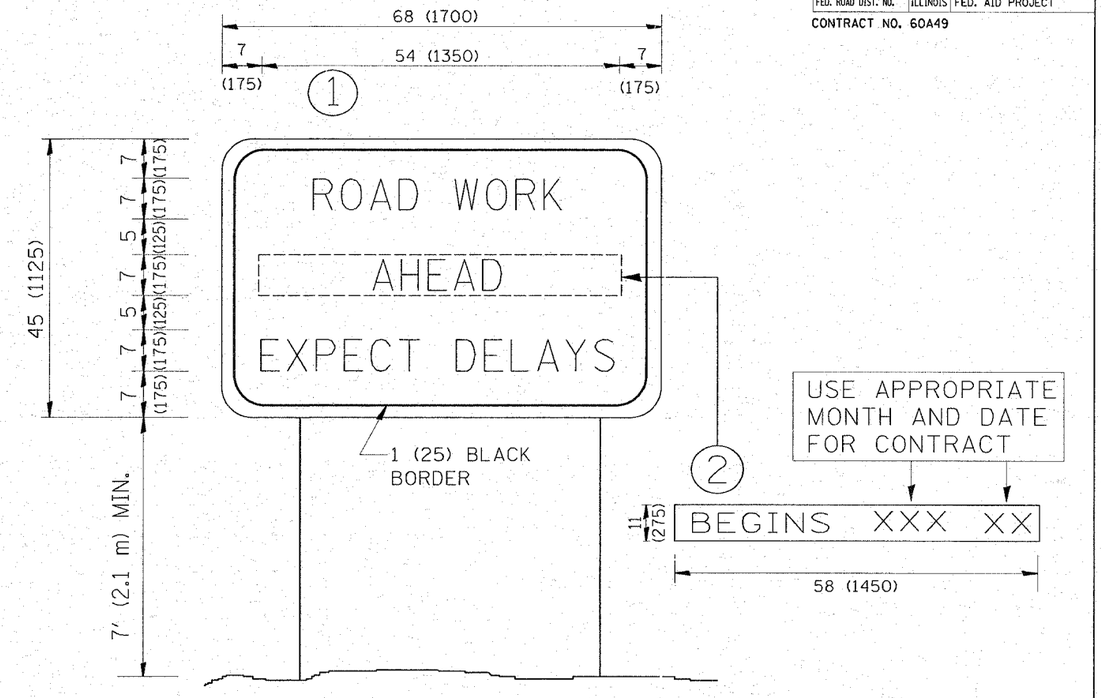
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN

CHECKED BY

TC22

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	34
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

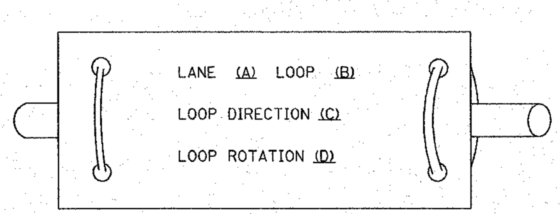
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

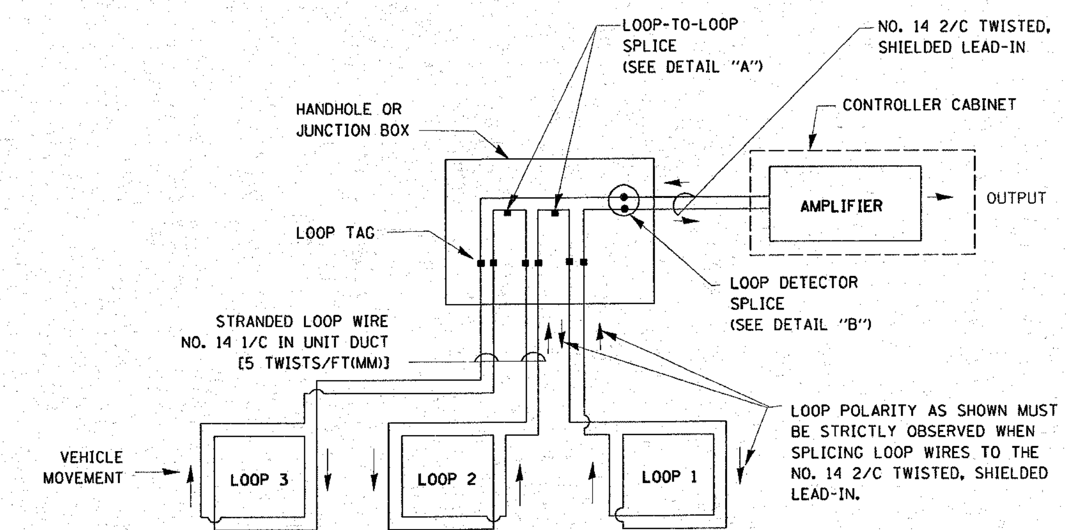


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

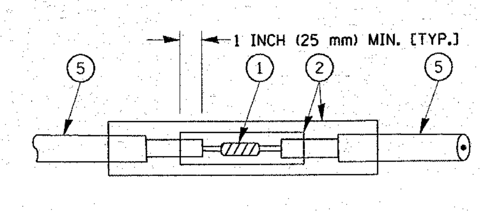
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

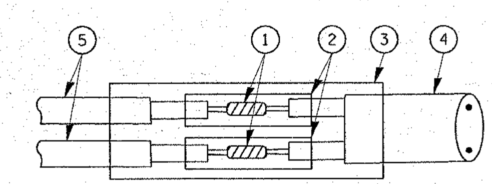


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

TS05

PLOT DATE = 3/10/2008
FILE NAME = \\distlntfs2\users\galbannb\Desktop\ts05.dgn
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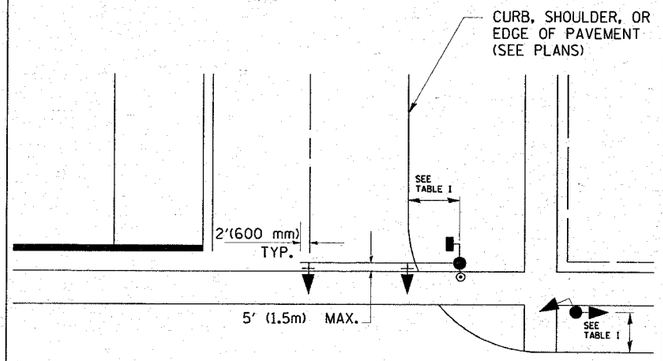
USER NAME = galbannb

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	35
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

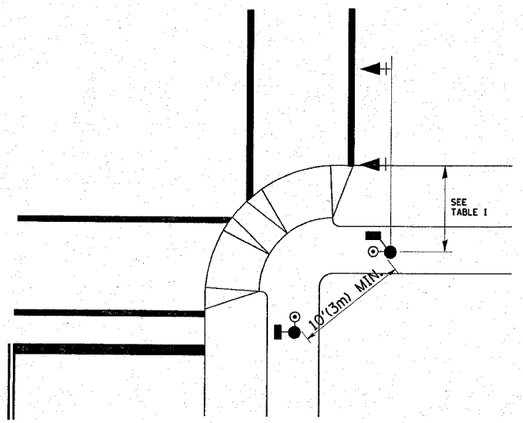
CONTRACT NO. 60A49

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

1. AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

   AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

   PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:

   A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.

   B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.

   C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

   D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).

   E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK

2. PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.

3. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.

4. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

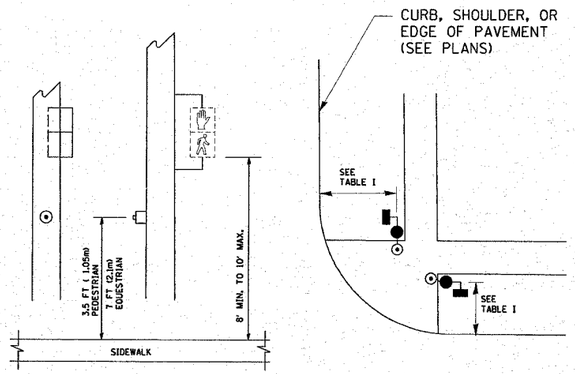


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

REVISIONS NAME	DATE
BUREAU OF TRAFFIC	1/01/02

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 2 OF 4

TS05

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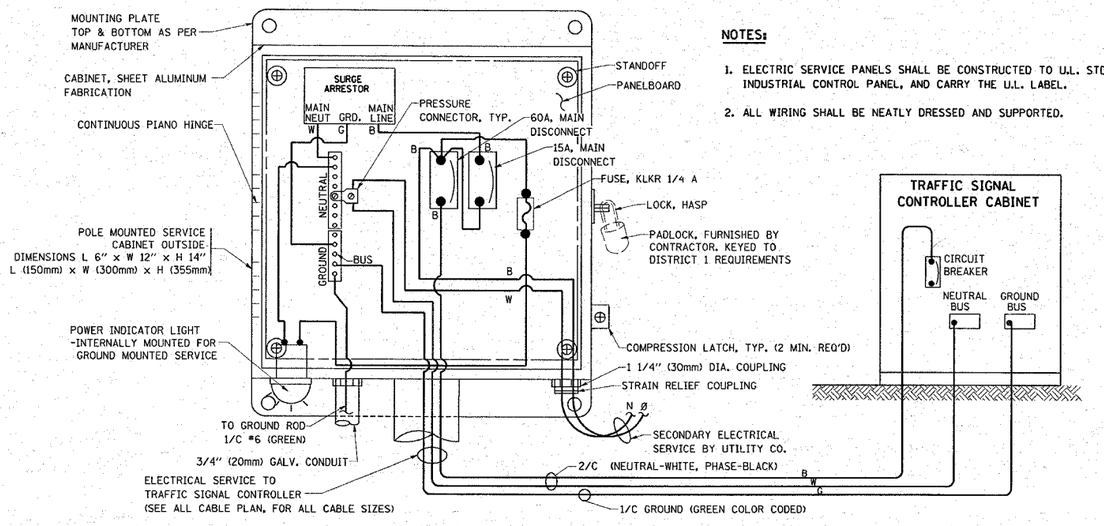
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	36
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49



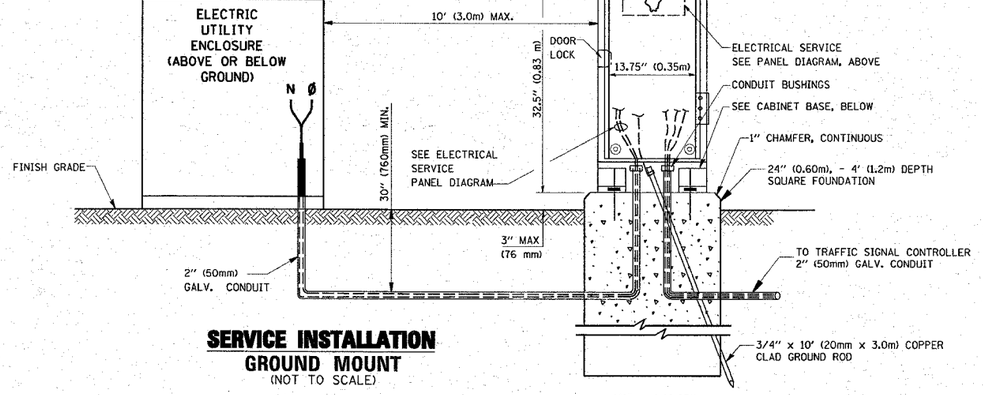
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

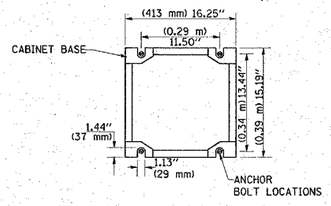
SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)



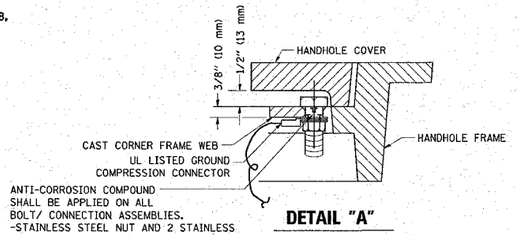
SERVICE INSTALLATION GROUND MOUNT

(NOT TO SCALE)

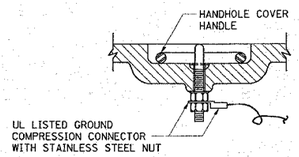


CABINET – BASE BOLT PATTERN

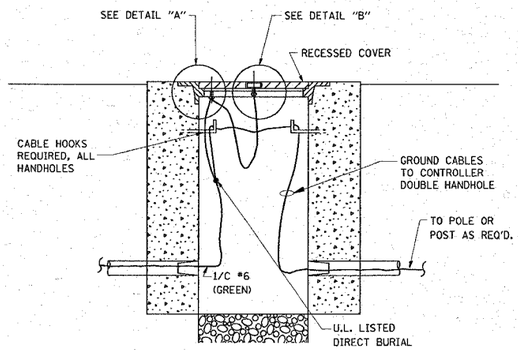
(NOT TO SCALE)



DETAIL "A"

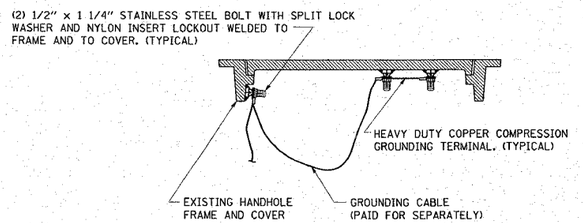


DETAIL "B"



HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



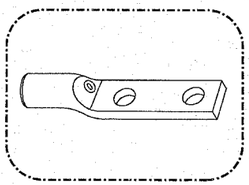
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

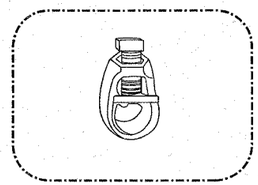
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



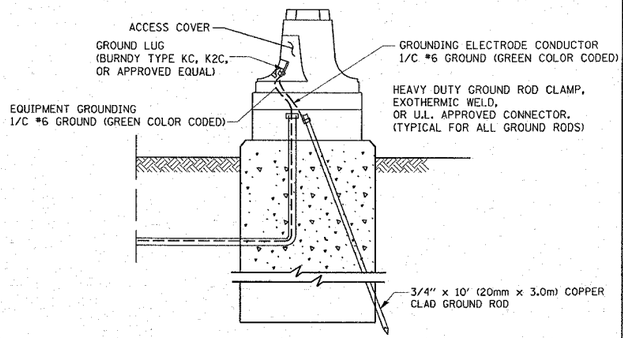
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

REVISIONS	
NAME	DATE
CADD	5/30/00
CADD	3/15/01
BUREAU OF TRAFFIC	1/01/02

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 3 OF 4

TS05

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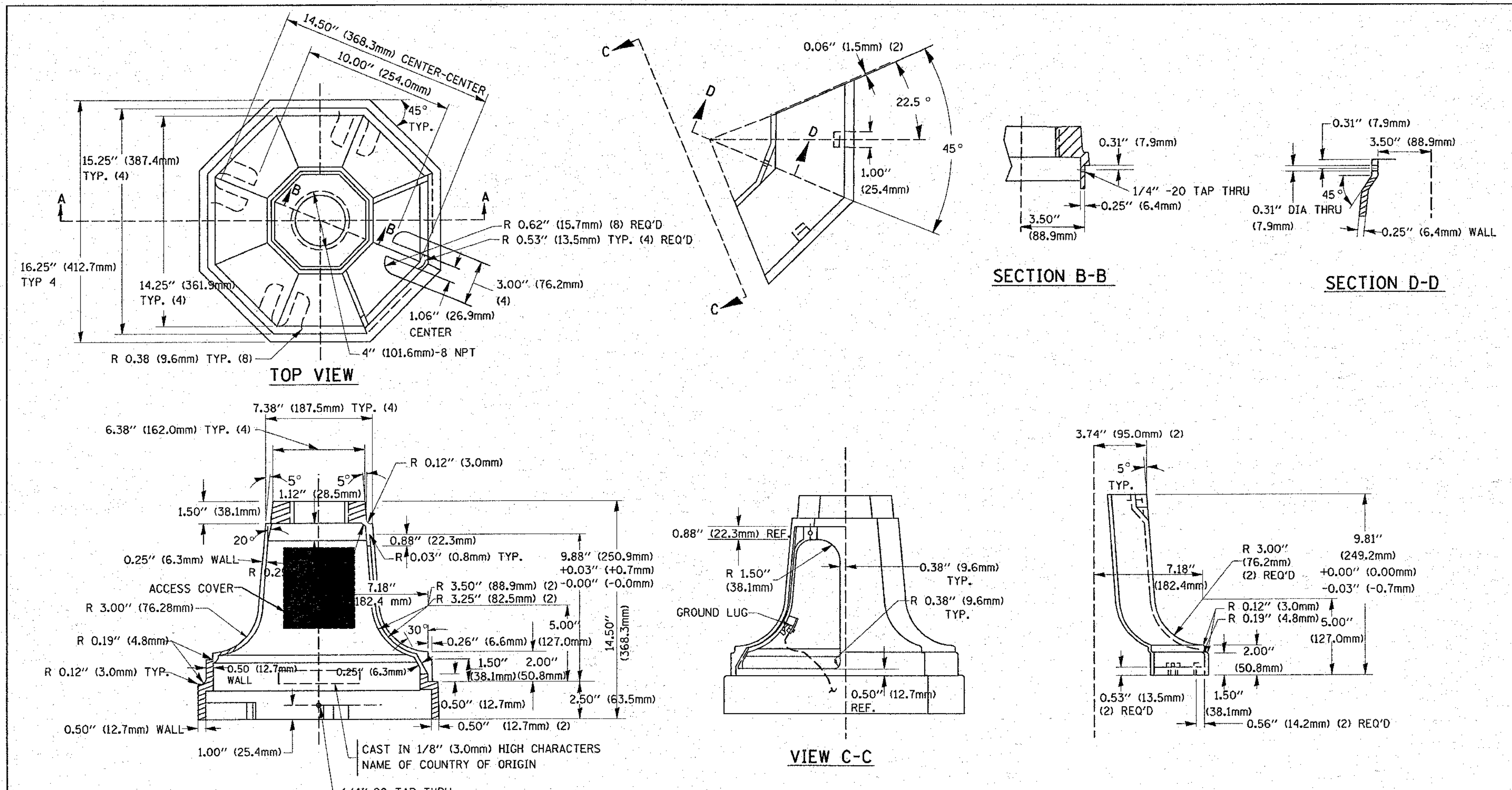


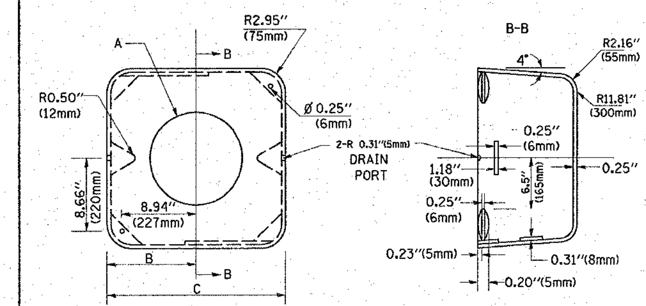
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	37

STA. TO STA.

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

CONTRACT NO. 60A49



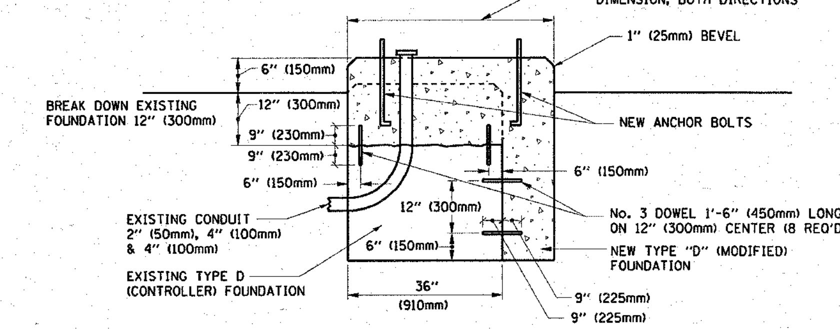
MATERIAL:

- ASTM A48 CLASS 30 GREY IRON
- ASTM A123 HOT DIPPED GALVANIZED

TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125"(257mm)	9.5"(241mm)	19"(483mm)	12" (300mm)	24kg
II	Ø 11.125"(283mm)	10.75"(273mm)	21.5"(546mm)	12" (300mm)	26kg

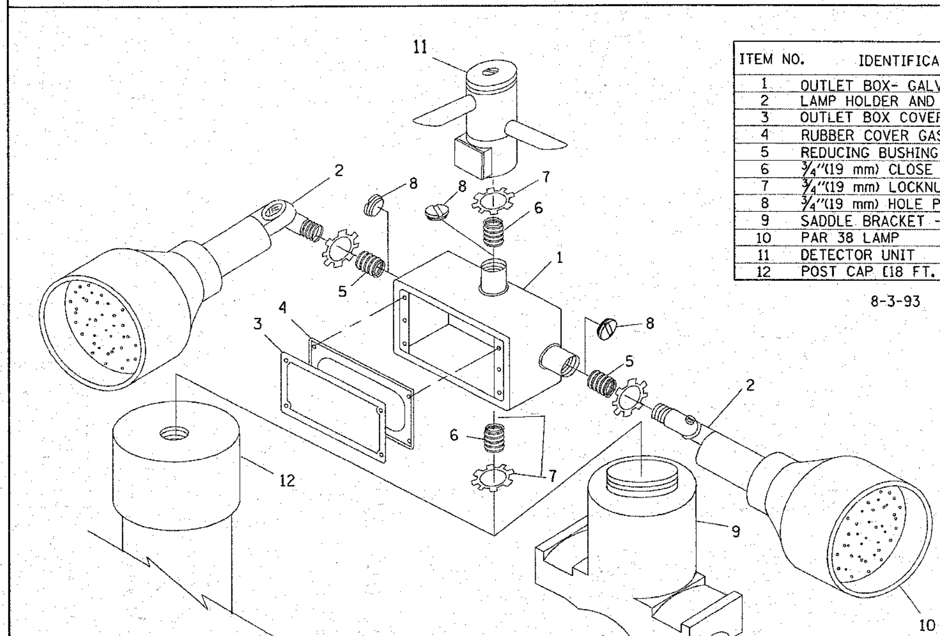
SHROUD DETAIL

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)



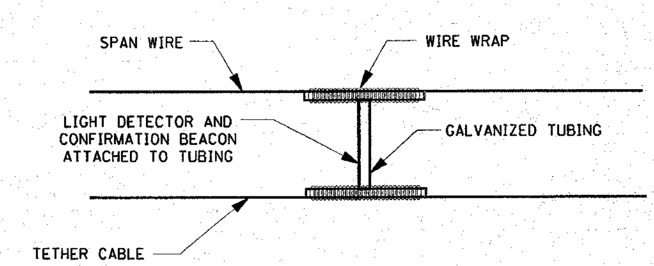
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

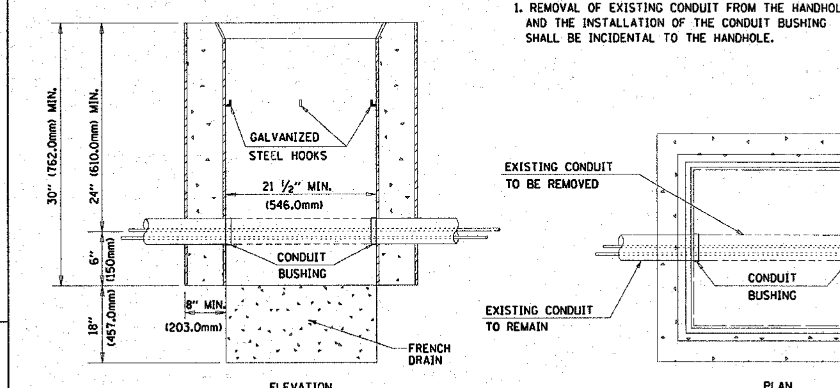
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

(NOT TO SCALE)

NOTES:
1. REMOVAL OF EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHING SHALL BE INCIDENTAL TO THE HANDHOLE.



DETAIL
HANDHOLE TO INTERCEPT EXISTING CONDUIT
N.T.S.

REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	5/30/00
BUREAU OF TRAFFIC	3/15/01
BUREAU OF TRAFFIC	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 4 OF 4

TS05

PLOT DATE = 3/10/2008
FILE NAME = \\distlntfs2\users\galbannb\Desktop\ts05.dgn
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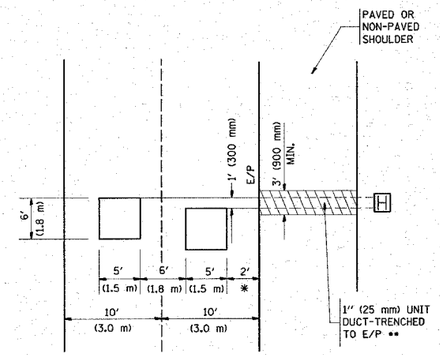
LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

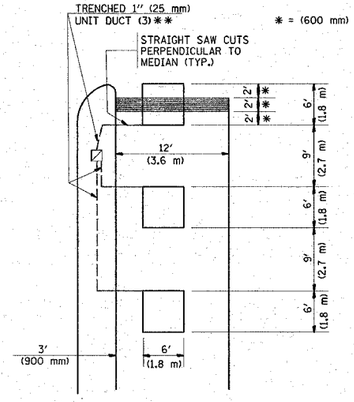


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

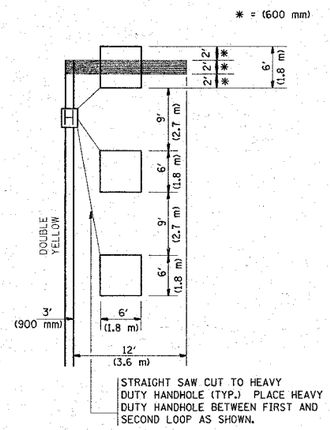
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	0711 RS-2	COOK	38	38
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A49

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

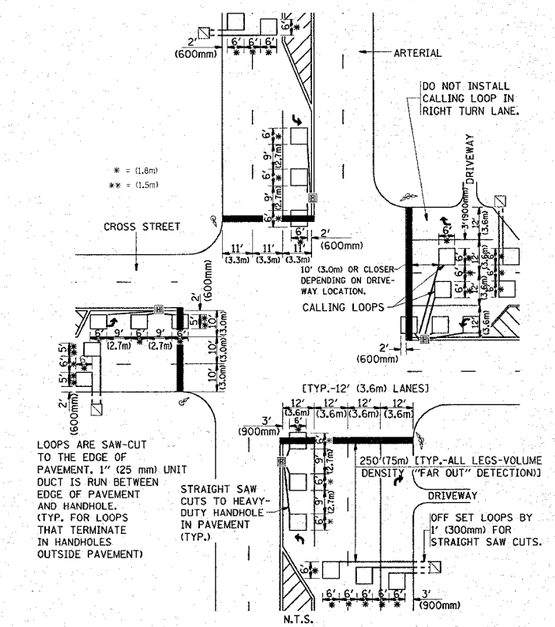
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

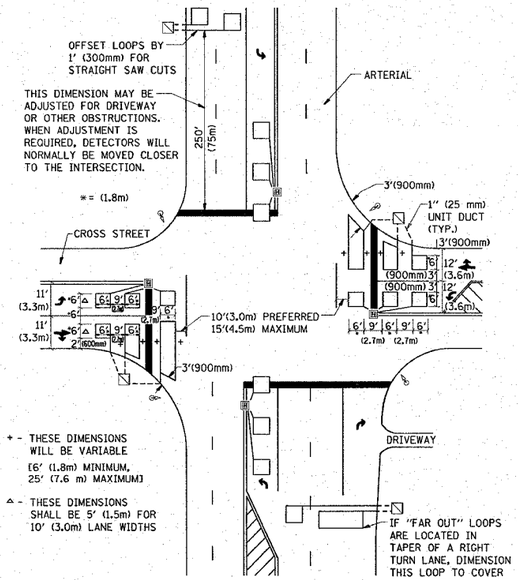
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1
DETECTOR LOOP
INSTALLATION DETAILS
FOR ROADWAY RESURFACING

SCALE: NONE

DESIGNED BY
DRAWN BY CADD
CHECKED BY R.K.F.

TS07

PLOT DATE = 3/10/2006
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