PLAN PREPARATION ENGIN	, who were the second of the s
DISTRICT ONE - DESIGN - PLAN	KEN ENG / (847) 705-4247

			TOTAL	SHEET
ROUTE	SECTION	COUNTY	SHEETS	NUMBER
VARIOUS	2010-015 RS	COOKFWILL	26	1

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS DISTRICT ONE PROPOSED HIGHWAY PLANS

**FOR INDEX OF SHEETS SEE SHEET 2** 

VARIOUS ROUTES

SECTION: 2010-015 RS

VARIOUS LOCATIONS IN SOUTHERN COOK COUNTY & WILL COUNTY

INTERMITTENT PAVEMENT RESURFACING

COOK & WILL COUNTY

C-91-473-10

McHenry Lake

DuPage
Cook

Will

**CONTRACT NO. 60K34** 

D-91-473-10

LOCATION OF IMPROVEMENT INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED: FEBRUARY 16, 20 10

Diane M. O'lle

March 19 20 1 Scott E Stitl P. Value Engineer of Design and Environ

DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION (312) 744-7000

**CONTRACT NO. 60K34** 



### INDEX OF SHEETS

### STATE STANDARDS

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	TITLE SHEET	000001-05 TYPICAL SYMB	OLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	701201-03 LANE CLOSURE	, 2L, 2W, DAY ONLY
3	SUMMARY OF QUANTITIES	701301-03 LANE CLOSURE	., 2L, 2W, SHORT TIME OPERATIONS
4	GENERAL LOCATION MAP		. 2L, 2W. SLOW MOVING OPERATIONS - DAY ONLY
5	SUMMARY OF PATCHING SCHEDULE		
6-19	PATCHING SCHEDULE	101336-03 LANE CLOSURE	, 2L, 2W, WORK AREAS IN SERIES
20	BUTT JOINT AND HMA TAPER DETAILS	701501-05 URBAN LANE C	CLOSURE, 2L, 2W, UNDIVIDED
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701601-06 URBAN LANE C NONTRAVERSAB	CLOSURE, MULTILANE, 1W OR 2W WITH BLE MEDIAN
22	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT	701606 <i>-06</i> URBAN LANE C	CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
22	MARKERS (SNOW-PLOW RESISTANT)	701701-06 URBAN LANE C	CLOSURE, MULTILANE INTERSECTION
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	701901~ <i>01</i> TRAFFIC CONT	POL DEVICES
24	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	TOISOI OF TRAFFIC CONT	NOL DEVICES
25	ARTERIAL ROAD INFORMATION SIGN		
26	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING		

### HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N7O (IL 9.5MM), 2"	PG 64-22	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

### GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE FIGURER

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MS. PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET.

NO PATCHING OR RESURFACING IS TO BE DONE WITHIN FIFTY (50) FEET OF ANY RAILROAD CROSSING WITHOUT OBTAINING THE PROPER RAILROAD PROTECTIVE LIABILITY INSURANCE.

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK.

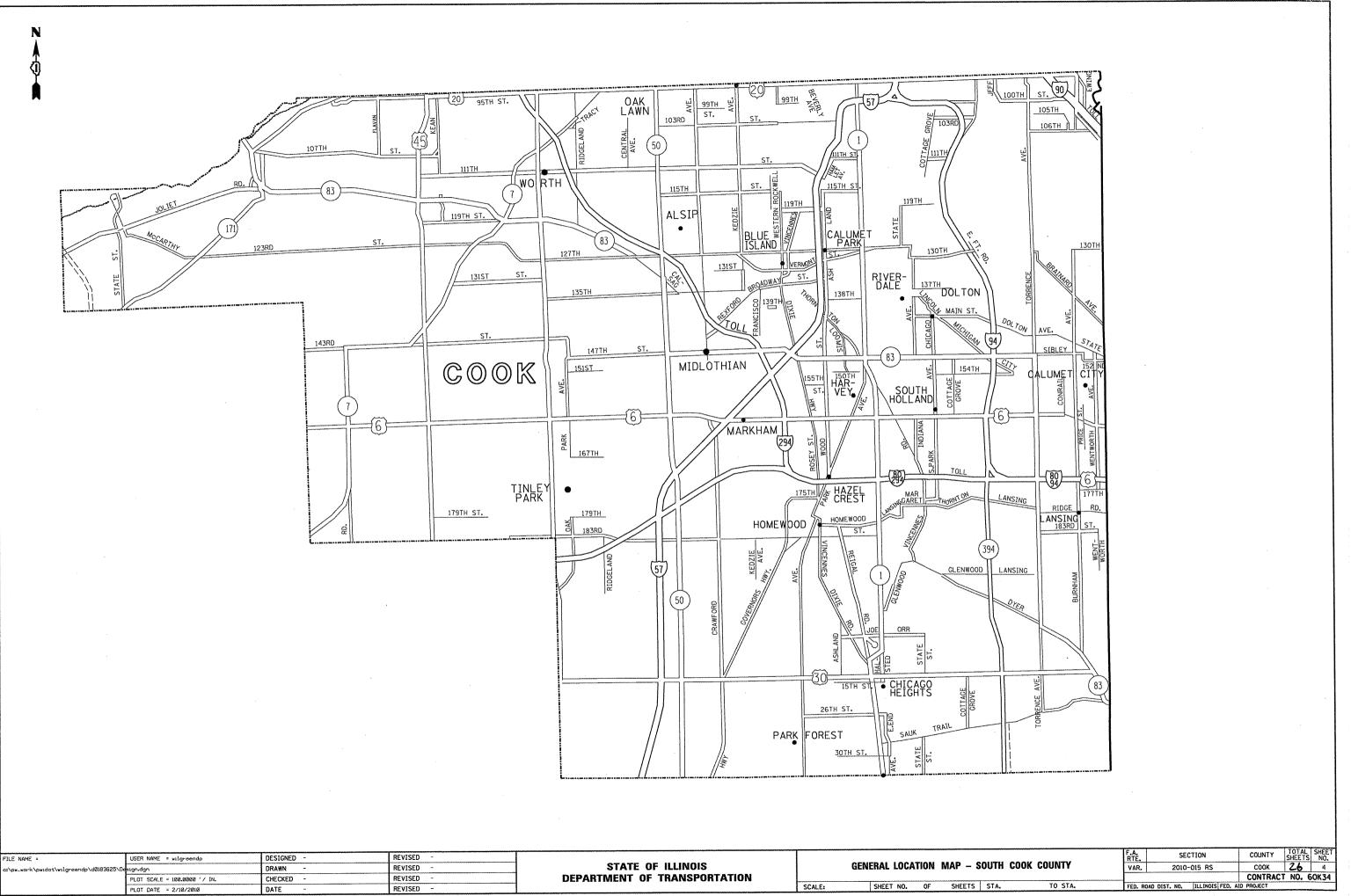
THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEV OF	eurre e	TATE	STANDARD	C AND	CENEDAL	NOTES	RTE.
HADEY OF	onecio, a	IMIE	SIMMUN	9 WILD	UCHERAL	MOTES	VAR.
SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.	FED.

	SUMMARY OF QUANTITIES		URBAN 100% STATE		C	ONSTRUCTI	ON TYPE	CODE			SUMMA	RY OF QUANTITIES	5				C	ONSTRUCT	ION TYPE	CODE	
	·		TOTAL	соок	WILL										TOTAL						
CODE NO	ITEM	UNIT	QUANTITIES	IOOO	COUNTY 1000					CODE NO		ITEM		UNIT	OUANTITIES			·			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	11	9	2																
40600300	AGGREGATE (PRIME COAT)	TON	56	45	11						-										
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	42	- 33	9																
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	834	663	171 .				·												
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3113	2476	637																
44000157	HOT-MIX ASPHALT SURFACE REMOVAL. 2"	SO YD	27786	22103	5683																
67000400	ENGINEER'S FIELD OFFICE. TYPE A	CAL MO	6	5	1																
67100100	MOBILIZATION	L SUM	1	0.9	0. 1																
·		FOOT	6824	5066	1758																
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	2275	1689	586								•								
	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	100	100			,						•								
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	34823	25223	9600		•														
<b>*</b> 78000300	THERMOPLASTIC PAVEMENT MARKING - LINE 5"	FOOT	1650		1650																
	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	740	740																	
<b>*</b> 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100																	
<b>*</b> 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	150											-						
<b>*</b> 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1118	956	162																
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1118	956	162									,							
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	675	500	175																
x0322256	TEMPORARY INFORMATION SIGNING	SO FT	874	822	52									,							
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	* SPECIALTY ITEM																				
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6:43:34 AM User≃wilgreend	PLOT DATE = 2/19/2010 DA	TE -		REVISED								SCALE: SHE	EET NO. OF	SHEETS STA	. T(	STA.	FED. RO	DAD DIST. NO. 1	ILLINOIS FED. AID		



CHARAADV COUTHEDN COOK COUNTY DOLLTES	HMA 2" MILL
SUMMARY - SOUTHERN COOK COUNTY ROUTES	& RESURFACE (SY)
	(31)
111TH ST. (RIDGELAND AVE. TO HARLEM AVE.)	1696
127TH ST (CENTRAL AVE. TO RIDGELAND AVE.)	143
135TH ST. (CENTRAL AVE. TO RIDGELAND AVE.)	3765
159TH ST. (CICERO AVE. TO I-294)	1155
CICERO AVE. (147TH ST. TO 159TH ST.)	493
CRAWFORD AVE. (159TH ST. TO 175TH ST.)	323
HALSTED ST. (JOE ORR RD. TO IL 1 CUTOFF)	225
IL 83 (127TH ST. TO ARCHER AVE.)	3593
PARK AVE. (154TH ST. TO 167TH ST.)	748
LAGRANGE RD. (112TH ST. TO 142ND ST.)	4345
LOOMIS ST. (THORNTON RD. TO 150TH ST.)	145
WOOD ST. (127TH ST. TO THORNTON RD.)	92
MAIN ST. IN LEMONT (IL 83 AT IL 171 TO PARKER RD.)	3072
SIBLEY BLVD. (I-94 TO TORRENCE AVE.)	419
TORRENCE AVE. (RIDGE RD. TO I-80/94)	1207
US 30 (I-394 TO HALSTED ST.)	682
IL 394 NB (STEGER RD. TO RELOCATED IL 1) - LOCATED IN WILL COUNTY	2484
IL 394 SB (STEGER RD. TO RELOCATED IL 1) - LOCATED IN WILL COUNTY	3199
SOUTHERN COOK COUNTY TOTALS =	27786
SOUTHERN COUNTY TOTALS =	2//86 SY

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CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
	en engagnasan yan a basan yayan basah ar juu sa beragyi Mangarisaan di membianan pambanda bili	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Ridgeland		WB	2	12	12	144	16
10901010		WB	2	12	12	144	16
		WB	2	6	100	600	67
		WB	2	6	12	300	33
`		WB	2	6	50	300	33
		WB	2	12	12	144	16
	·	WB	2	12	12	144	16
······································		WB	2	12	12	144	16
		WB	2	6	50	300	33
	Harlem	WB	2	12	12	144	16
Jodom	nanem	EB		12	20	240	27
Harlem	· · · · · · · · · · · · · · · · · · ·	EB	2	6	50	300	33
		EB	2	12	20	240	27
		EB	2	12	12	144	16
		EB	2	12	100	1200	133
		EB	2	12	50	300	33
		EB	2	6	50	300	33
		EB	2	12	20	240	27
······································		EB	2	12	12	144	16
		EB	2	6	100	600	67
· · · · · · · · · · · · · · · · · · ·		EB	2	6	100	600	67
		EB	2	12	12	144	16
	****	EB	2	12	50	600	67
		EB	2	12	12	144	16
		EB	2	12	50	600	67
		EB	2	12	20	240	27
······	Ridgeland	EB	2	12	12	144	16
Ridgeland		WB	1	6	12	72	8
		WB	1	12	50	600	67
		WB	1	12	20	240	27
		WB	1	12	75	900	100
		WB	1	12	50	600	67
		WB	11	6	12	72	8
		WB	1	6	12	72	8
		WB	1	12	20	240	27
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		WB	1	12	12	144	16
	Harlem	WB	1	12	15	180	20
-larlem		EB	1	6	12	72	8
		EB	1	12	12	144	16
		EB	1	12	12	144	16
	<u> </u>	EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	1	12	12	144	16
	<del>~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~</del>	EB	1	12	12	144	16
		EB	1	12	12	144	16
		EB	<del>                                     </del>	12	12	144	16
		EB	1	12	75	900	100
		EB	1	12	12	144	16
		EB	1	12	12	. 144	16
		EB	1	12	15	180	20
····	Didgeland	EB	1	12	12	144	16
	Ridgeland	ED	1	14	12	144	10
	- С. Буду дудь и комунутивного сительного и инфинутываний и солости положения поставления и поставления и пост	man in a company	and a second second		i more announcement of the contract of	reconstruction and forest on the contract of the contract of	
AND THE PROPERTY OF THE PROPER		TOTALS:			1558		1696

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CR	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
e spekken in men mengangkangan kemilikan dan mengangkan kemilikan dan melakunya berkan dan mengan dan mengan d	and a second second of the second sec	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Ridgeland		EB	1	12	200	2400	267
		EB	1	6	400	2400	267
		EB	1	12	20	240	27
		EB	1 1	12	20	240	27
		EB	1	6	100	600	67
		EB	1	12	12	144	16
	······	EB	1	12	12	144	16
		EB	1	6	200	1200	133
	***************************************	EB	1	12	12	144	16
	***************************************	EB	1	12	15	180	20
	······································	EB	1	6	300	1800	200
		EB	1	12	12	144	16
		EB	1	/ 12	12	144	16
		EB	1	12	12	144	16
		EB	1	12	12	144	16
**************************************		EB	1	12	12	144	16
······	······	EB	1 1	6	300	1800	200
		EB	1	6	50	300	33
		EB	1	6	100	600	67
		EB	1	12	20	240	. 27
	***************************************	EB	1	12	20	240	27
······································		EB	1	12	30	360	40
		EB	1	6	50	300	33
		EB	1	6	100	600	67
		EB	1	6	200	1200	133
	***************************************	EB	1	12	20	240	27
		EB	1	12	40	480	53
		EB	1	6	200	1200	133
		EB	1	12	12	144	16
	Central	EB	1	6	300	1800	200
Centrall	Ochila	WB	1	6	100	600	67
Ochtran		WB	1	6	50	300	33
		WB	1	12	20	240	27
		WB	1	12	12	144	16
		WB	1	12	12	144	16
		. WB	1	12	12	144	16
		WB	1	6	100	600	67
		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
		WB	1	6	12	72	8
		WB	. 1	12	12	144	16
		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
······································		WB	<del>- i</del>	12	20	240	27
		WB	1	12	75	900	100
		WB	1	12	100	1200	133
·		WB	1	12	100	1200	133
		WB	1	12	25	300	33
		WB	1	6	400	2400	267
		WB	1	12	12	144	16
	Ridgeland	WB	1	12	50	600	67
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gangan agawan yi isam panga inggapan ari in ngabon biy ingdabiyar ago manar	основник и не с не с населения и почетор сенеципрорательность при и проведен и из учественность населения. 	TOTALS:			4305 FT		3765 SY

gram of the common approximation and the second management of the common second	·							
ROUTE:	127th	St. (	Central	Ave.	to	Ridgeland	Ave.	)

CR	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
Adams on the second	and the state of t	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Central Ave.	Ridgeland Ave.	WB	1	12	6	72	8
		WB	1	6	120	720	80
		EB	1	12	10	120	14
		EB	1	- 6	10	60	7
		EB	1	6	20	120	14
		EB	1	6	30	180	20
A LIFE CONTRACTOR CONTRACTOR AND		TOTALS:	AND THE PROPERTY OF THE PARTY O		196	gar ng distancegan" njamang ngangda magada yang 1 isancegad ngangantar ya	143
entropropropries dependantes : describates entropressons en el departe, l'existent en		kalenda kantan kant 	Supergravement member op a company of a co	e propriores antes carros en estra como conserva en estra en el conserva en estra el conserva en el conserva e La conserva en el conserva en	FT	a diferit seján serit it des adjah naszeli megsedikátása reaszlepátásatás az in szesze	SY

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TION

PATCHING SCHEDULE 127TH ST. AND 135TH ST. SHEET NO. OF SHEETS STA.

TO STA.

SCALE:

2010-015 RS COOK Z-6 7

CONTRACT NO. 60K34

FEO. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT F.A. RTE. VAR. SECTION

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
TITOWI Maring and a marino and a silver a silver a silver a silver	eren omgreson minoren opprøder om hande er emerker b	(NB/SB)	(1, 2, 3)	WDTH	LENGTH	(SQ FT)	(SQ YD)
Cinoro	Dulanki Crauford			<b></b>	<del> </del>	72	
Cicero	Pulaski - Crawford	EB	1 1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6		8
Cicero	Pulaski - Crawford	EB	1	12	12	144	16
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	10	120	13
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
Pulaski - Crawford	I - 57	EB	1 .	12	6	72	8
Pulaski - Crawford	I - 57	EB	1	12	6	72 .	8.
Pulaski - Crawford	I - 57	EB	1	12	6	72	8
1 - 57	Kedzie	EB	<del>                                     </del>	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
1-57	Kedzie	EB	1	12	6	72	8
		EB	1	12	6	72	8
l - 57	Kedzie					72	
1 - 57	Kedzie	EB	1	12	6		8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
I - 57	Kedzie	EB	1	12	6	72	8
l - 57	Kedzie	EB	1	12	10	120	13
I - 57	Kedzie	EB	1	12	6	72	8
Kedzie	1 - 294	EB	1	12	6	72	8
Kedzie	I - 294	EB	1	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	6	72	8
Cicero	Pulaski - Crawford	EB	2	12	10	120	13
Cicero	Pulaski - Crawford	EB	2	12	8	96	11
	.,	EB	2	12	6	72	8
Cicero	Pulaski - Crawford			12	L	72	8
Cicero	Pulaski - Crawford	EB	2		6		
Cicero	Pulaski - Crawford	EB	2	12	10	120	13
Pulaski - Crawford	I - 57	EB	2	12	6	72	8
Pulaski - Crawford	I - 57	EB	2	12	6	72	8
Pulaski - Crawford	I - 57	EB	2	12	12	144	16
I ~ 57	Kedzie	EB	2	12	6	72	8
I - 57	Kedzie	EB	2	12	6	72	8
I - 57	Kedzie	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	6	20	120	13
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	6	20	120	13
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	1 - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	1 - 294	EB	2	12	6	72	8
Kedzie	I - 294	EB	2	12	6	72	8
Kedzie	Richmond	EB	3	6	20	120	13
Kedzie	Richmond	EB	3	12	6	72	8
	~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~		L		I	72	
Kedzie Kedzie	Richmond Richmond	EB EB	3	12	6 20	120	8 13

AND DESCRIPTION OF THE PARTY OF						
ROUTE:	159th	St.	(Cicero	Ave.	to	1-294)

			† <b>†</b>	·			
	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
l - 294	Kedzie	WB	1	12	6	72	8
I ~ 294	Kedzie	WB	1	12	6	72	8
l - 294	Kedzie	WB	1	6	25	150	17
l - 294	<u>Kedzie</u>	WB	1	6	25	150	17
l - 294	Kedzie	WB	1	6	15	90	10
l - 294	Kedzie	WB	1	6	40	240	27
l - 294	Kedzie	WB	1	12	6	72	8
l - 294	Kedzie	WB	1	6	20	120	13
Kedzie	l - 57	WB	1	12	6	72	8
Kedzie	l - 57	WB	1	12	. 6	72	8
Kedzie	l - 57	WB	1	12	8	96	11
Kedzie	l - 57	WB	1	12	6	72	8
Kedzie	l - 57	WB	1	12	12	144	16
Kedzie	l - 57	WB	1	12	6	72	8
I - 57	Pulaski - Crawford	WB	1	12	6	72	8
l - 57	Pulaski - Crawford	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	8	96	11
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
Pulaski - Crawford	Cicero	WB	1	12	6	72	8.
Pulaski - Crawford	Cicero	WB	1	12	6	72	8
I - 294	Kedzie	WB	2	12	6	72	8
I - 294	Kedzie	WB	2	12	6	72	8
l - 294	Kedzie	WB	2	12	6	72	8
I - 294	Kedzie	WB	2	6	20	120	13
I - 294	Kedzie	WB	2	6	20	120	13
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	10	120	13
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	12	8	96	11
Kedzie	l - 57	WB	2	12	6	72	8
Kedzie	l - 57	WB	2	6	30	180	20
Kedzie	l - 57	WB	2	6	50	300	33
Kedzie	l - 57	WB	2	6	20	120	13
I - 57	Pulaski - Crawford	WB	2	. 12	6	72	8
l - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	12	12	144	16
I - 57	Pulaski - Crawford	WB	2	6	40	240	27
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
I - 57	Pulaski - Crawford	WB	2	12	6	72	8
l - 57	Pulaski - Crawford	WB	2	6	30	180	20
I - 57	Pulaski - Crawford	WB	2	6	30	180	20
Pulaski - Crawford	Cicero	WB	2	6	50	300	33
Pulaski - Crawford	Cicero	WB	2	6	50	300	- 33
Pulaski - Crawford	Cicero	WB	2	6	30	180	20
Pulaski - Crawford	Cicero	WB	2	12	8	96	11
Pulaski - Crawford	Cicero	WB	2	6	30	180	20

and the second s	minum a	 es comme en en en entre entre en en entre entre	ing. The state of the state of			
TOTALS:			1169			1155
and constitution to the state of the state o		 and the second contract of the second contrac	man of the state of many or proper and department of the state of the	and the contract of the contra	···· · · · · · · · · · · · · · · · · ·	A commence of the second
	1		FT			SY

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o:\pw_work\PWIDOT\WILGREENDP\dØ183625\D	esign.dgn	DRAWN -	REVISED	-	
	PLOT SCALE = 100,0000 '/ IN.	CHECKED -	REVISED	-	
	PLOT DATE = 2/17/2010	DATE -	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

		PA	TCHING	3 SCHE	DULE		
			159	TH ST.			
CALE:	SHEET	NO.	OF	SHEETS	STA.	TO	STA.

 F.A. RTE. RTE. VAR.
 SECTION
 COUNTY SHEET SHEET SHEET SHOO.

 VAR.
 2010-015 RS
 COOK
 26 8

 CONTRACT NO. 60K34

 FED. ROAD DIST. NO. 1 | ILLINOIS| FED. AID PROJECT

FROM 159th	S STREETS TO	DIRECTION (EB/WB) (NB/SB) NB NB	LANE NO. (1, 2, 3)	PAVEMENT PATCH	PAVEMENT PATCH	REPAIR	REPAIR
FROM	TO	(EB/WB) (NB/SB) NB	NO.		Commence of the commence of th	The track of the first of the second section of the s	- The Art Commission of March State (Annales of Annales
and a consideration profession for the second state of the second	energe i stande entagener, ny steppener i sepergentite pommy vend politik, forse, schrift nepe engazeleg, escent i ken	(NB/SB) NB	- normal soundman Shradgement, 4 p. 1544	1 1711011 1	PAILM	AREA	AREA
159th	147th	NB	(', ~, ~,	WIDTH	LENGTH	(SQ FT)	(SQ YD)
	177701		2	4	50	200	22
			2	12	6	72	8
		NB NB	2	12	10	12	13
	·	NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	6	72	8
		NB	2	12	4	48	5
		NB	2	12	10	120	13
		NB	2	12	4	48	5
·	,	NB	2	4	100	400	44
147th	159th	SB	2	12	6	72	8
147 UI	19901	SB	2	12	6	72 72	8
· · · · · · · · · · · · · · · · · · ·		SB	2	12	40	480	53
		SB	2	12	6	480 72	8
		SB	2	12	6	72 72	8
		SB	2	12	8	96	11
	<del> </del>	SB	2	12	6	72	. 8
		SB	2	12	6	72	8
		SB	2	12	4	48	5
		SB	2	4	50	200	22
		SB	2	4	100	400	44
		SB	2	12		72	8
1E0+h	1.4.7+b	NB		12	6	96	
159th	147th	NB	1	12	8	72	11 8
		NB	1	12	6	72	
	1504h		1	12	6	72	8
147th	159th	SB SB	1	12	6	72	8 8
		SB	1	12	4	72 48	5
######################################		SB	1	12	6	72	8
		SB	1	12	8	96	11
		SB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	1	12	4	48	5
		SB	1	12	8	96	11
		SB	1	12	24	288	32
		SB	1	12	8	96	11
		SB	1	12	8	96	11
		SB		12	4	48	
		SB	1	12	6	72	5 8
		l op	1	14	0	12	<u> </u>
and all the facility of the country of the country of the facilities of the federal and the first of the federal of the federa	A STATE A STATE OF THE PROPERTY OF CASE ASSAULT STATE OF THE PROPERTY OF THE P	TOTALS:	and we have a second se	En ar um someon - serverus perigangagaisanan T	570	agenticity of the control of a substitute of the control of the co	493
Zanngas 2-648501-Cannon Shadi si Presideded — wiki 1651 Shiki 1654 ANSA 1545 (1665) (1664) (1664)	s (a Marie and sense and sense a consent of the advantage of the consent of the c	······································	TO CONTROL OF THE PARTY OF THE	ana	5/U FT		493 SY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -				DATCUI	ING SCH	EDIN E		F.A. RTF.	SECTION	COUNTY TOTAL SHEETS	HEET
c:\pw_work\PWIDOT\WILGREENDP\d0183625\D	esign,dgn	DRAWN -	REVISED -	STATE OF ILLINOIS							VAR.	2010-015 RS	соок 2.6	₹ <u>`</u>
	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			LI	CERO AV	E,		<u> </u>		CONTRACT NO. 60M	<b>434</b>
	PLOT DATE = 2/17/2010	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1   ILLINOIS FED. A	L	

ROLL  TO A CONTROL	JTE: Crawford Ave. (159th	St. to 175th St.) - F	IMA Section	on .	Makes and a contraction of the c	e construente de la c El construente de la	
CRO	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
-autorit in Strausummen <del>(Anthropida) (Anthropida) (Anthropida) (A</del> nthropida) (Anthropida) (Anth	Casamegar's command. It is the responsition packets and only the above short state of the circle of the circle short of the ci	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
167TH	l-57	NB	2	12	6	72	8
167TH	I-57	NB	2	12	20	240	27
167TH	I-57	NB	2	12	10	120	13
I-57	159TH	SB	2	12	200	2400	267
I-57	159TH	SB	2	12	6	72	8
33 kilorusayinahar 2 area neki eye 2 x reanimenta adaptetin 1913 komunh erir kollas ye damisteksilikadi.	TO THE MELT TO THE STATE OF THE STATE OF THE STATE OF THE STATE STATES AND THE ST	TOTALS:	THE STATE OF THE S	E continue e e e e e e e e e e e e e e e e e e	242	dergeneration description (com product in demonstrative activities and constraints	323
omericans (Me.), estima est	More An experience process and experience pro		g en ministration en l'écologie del recons antidendes en .		**************************************	de hale allem et mensem et til stoder har i de hale åden et per til et hale til per til se til se til se til s	SY

			1	·			
	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	8	96	11
Joe Orr	ILL 1 Cutoff	SB	2	15	8	120	13
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	12	144	16
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	2	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	2	12	12	144	16
ILL 1 Cutoff	Joe Orr	NB	2	12	10	120	13
ILL 1 Cutoff	Joe Orr	NB	2	12	25	300	33
ILL 1 Cutoff	Joe Orr	NB	2	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	. 2	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	1	12	6	72	8
Joe Orr	ILL 1 Cutoff	SB	1	12	10	120	13
Joe Orr	ILL 1 Cutoff	SB	1	12	12	144	16
Joe Orr	ILL 1 Cutoff	SB	1	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	1	12	6	72	8
ILL 1 Cutoff	Joe Orr	NB	1	12	10	120	13
annogenius, anna mandaga ga rai ga saine, i ar se eithean de i seann ann e eithe eil ea chean de ear an an ear	Takan da kanam dayanggan manama dang pan- sagai saka sahisi - dan kanis kanis kanaman ganam sakanan kanam	TOTALS:	the action will represent the second	e e e e e e e e e e e e e e e e e e e	167	e meeringade englissission opinisensissipanisti meetineen — ye c.e.	225
en en far general franco frank frank en en de til skale frank frank frank frank frank frank frank frank frank f		5 			FT		SY

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c:\pwwwork\PWIDOT\WILGREENDP\dØ183625\D	es1gn.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS			VAR. 2010-015 RS	COOK Z6 10
	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	CRAWFORD AVE. AND HALSTED ST.			CONTRACT NO. 60K34
	PLOT DATE = 2/17/2010	DATE ~	REVISED -		SCALE:	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FEE	D. AID PROJECT

ROUTE: IL 83 (127th St. / Cal Sag to Archer Ave.)

CR	CROSS STREETS		LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	DIRECTION (EB/WB)	NO.	PATCH	PATCH	AREA	AREA
$in (g_{\theta}(x)) = (x - g_{\theta}(x)) + (x - $	de Sources d'agra : La condition ender de la contrata del contrata de la contrata de la contrata del contrata de la contrata del la contrata del la contrata de la contrata del la contrata del la contrata de la contrata de la contrata del la contr	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
127th/Cal-Sag	Archer Ave.	WB	1	4	500	2000	223
		WB	1,2,3	36	6	216	24
	·····	WB	1,2	24	6	144	16
		WB	1,2	24	6	144	16
,	······	WB	1	6	300	1800	200
		WB	1	6	250	1500	167
		WB	1	4	100	400	45
		WB	1	4	200	800	89
		WB	1	6	150	900	100
		WB	1	12	6	72	8
		WB	1	4	150	600	67
		WB	1	4	200	800	89
		WB	1	12	5	60	7
		WB	1	12	6	72	8
		WB	1	12	12	144	16
,		WB	1	12	6	72	8
		WB	1	50	4	200	22
		WB	1	12	8 ,	96	11
		WB	1	12	50	600	67
· · · · · · · · · · · · · · · · · · ·		WB	1	12	10	120	13
		WB	1	12	25	300	33
·		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	6	72	8
		WB	1	12	300	3600	400
		WB	1	6	50	300	33
······································		WB	1	12	50	600	67
		WB	1	6	200	1200	133
		WB	1	12	25	300	33
		WB	1	12	200	2400	267
		WB	11	6	200	1200	133
		WB	11	6	200	1200	133
		WB	1	12	20	240	27
Archer	107th/Cal Cas	WB EB	1	6 12	30 4	180	20 5
Aicher	127th/Cal-Sag	EB	1	12	20	48 240	27
· · · · · · · · · · · · · · · · · · ·		EB	1	4	150	600	67
		EB	1	4	125	500	56
		EB	1	12	123	144	16
		EB	1	4	30	120	13
		EB	1	4	70	280	31
		EB	1	4	100	400	44
		EB	1	4	50	200	22
		EB	1	4	50	200	22
		EB	1	12	8	96	11
		EB	1	12	8	96	11
		EB	1	12	100	1200	133
	,	EB	1	12	60	720	80
		EB	1	4	50	200	22
······································		EB	1	4	100	400	44
	1						
	***************************************		1	4	200	800	89
		EB EB		4	200 100	800 400	89 44

3593 SY TOTALS:

ROUTE: Park Ave. (154th St. to 167th St.) - HMA Section

	STREETS	DIRECTION		and the second of the second	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
AF 415-	APPU-	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD
154th	155th	NB	2	6	12	72	8
155th	156th	NB	2	6	12	72	8
155th	156th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	12	12	144	16
156th	157th	NB	2	6	12	72	8
156th	157th	NB	2	6	12	72	8
154th	155th	SB	2	12	70	840	93
155th	156th	SB	2	6	12	72	8
155th	156th	SB	2	6	12	72	8
155th	156th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
156th	157th	SB	2	6	12	72	8
157th	159th	SB	2	12	20	240	27
157th	159th	ŞB	2	6	12	72	8
157th	159th	\$B	2	6	12	72	8
157th	159th	SB	2	6	12	72	8
157th	159th	SB	2	12	15	180	20
157th	156th	NB	1	6	12	72	8
157th	156th	NB	1	6	12	72	8
157th	156th	NB	1	12	12	144	16
157th	156th	NB	1	6	12	72	8
157th	156th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
156th	155th	NB	1	6	12	72	8
155th	154th	NB	1	12	12	144	16
155th	154th	NB	1	6	12	72	8
155th	154th	NB	1	12	50	600	67
154th	155th	SB	1	12	100	1200	133
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
155th	156th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8
156th	157th	SB	1	6	12	72	8

TOTALS:

831 FT

TO STA.

748

FILE NAME :

REVISED -DESIGNED c:\pw\_work\pwidot\wilgreendp\dØ183625\ DRAWN REVISED -CHECKED ~ REVISED -PLOT SCALE = 100.0000 '/ IN. PLOT DATE = 2/18/2010 DATE REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PATCHING SCHEDULE IL 83 AND PARK AVE. SHEET NO. OF SHEETS STA. SCALE:

VAM. | 2010-015 RS | COOK | Z-b | //

FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT | SHEE SNO. |

CONTRACT NO. 60K34 F.A. RTE. VAR. SECTION

CROS	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
I12th		SB	2	12	20	240	27
		SB	2	12	200	2400	267
		SB	2	12	100	1200	133
		SB	2	12	12	144	16
·		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
· · · · · · · · · · · · · · · · · · ·		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
		SB	2	12	12	144	16
	142nd	SB	2	12	100	1200	133
42nd		NB	2	12	12	144	16
		NB	2	12	12	144	16
		NB	2	12	50	600	67
· · · · · · · · · · · · · · · · · · ·		NB	2	12	50	600	67
	·	NB	2	6	100	600	67
		NB	2	12	150	1800	200
		NB	2	12	100	1200	133
		NB	2	12	12	144	16
		NB	2	12	12	144	16
		NB	2	12	20	240	27
		NB	2	6	300	1800	200
		NB	2	6	300	1800	200
		NB	2	12	20	240	27
		NB	2	12	20	240	27
		NB	2	6	200	1200	133
		NB	2	6	100	600	67
	112th	NB	2	12	12	144	16
12th		SB	1	6	300	1800	200
		SB	1	6	100	600	67
		SB	1	6	400	2400	267
		SB	1	12	20	240	27
		SB	1	12	20	240	27

	000 OTDEETO	T m.m.=====		I m a) m			
	DSS STREETS	DIRECTION	LANE	PAVEMENT	Committee of the commit	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD
112th		SB	11	12	20	240	27
		SB	1	12	12	144	16
		· SB	1	12	12	144	16
		SB	1	12	12	144	16
		SB	1	12	30	360	40
		SB	1	12`	40	480	53
		SB	1	12	40	480	53
		SB	1	12	20	240	27
	-	SB	1	6	400	2400	267
		SB	1 .	12	12	144	16
		SB	1	12	12	144	16
		SB	1	12	12	144	16
		SB	1	12	60	720	80
		SB	1	12	60	720	80
		SB	1	12	60	720	80
	142nd	SB	1	12	25	300	33
42nd		NB	1	12	12	144	16
		NB	1	12	12	144	16
		NB	1	12	12	144	16
		NB	1	12	12	144	16
		NB	1	6	250	1500	167
		NB	1	12	20	240	27
		NB	1	12	20	240	27
		NB	1	12	20	240	27
		NB	1	12	30	360	40
		NB	1	12	12 .	144	16
		NB	1	12	12	144	16
· · · · · · · · · · · · · · · · · · ·		NB	1	6	200	1200	133
	······································	NB	1	12	75	900	100
·		NB	1	12	50	600	67
		NB	1	12	90	1080	120
	112th	NB	1	12	20	240	27
тументумпунта такоболарды айтатту да үзбүктөөтүмпунунун артарауулуулуу да	gan jura di manghapa yan karakan karan ki sayahildan kamanga. Asaraya di karaya di karakan kata da	TOTALS:	Security of the contract of th	The stage properties of the second se	4584	з эмиминического систем с интерментор систем с и отка	4345
	aanaanaluuntaan markii ja konsuurriitaan markiin markiin kari oo markiin ka k I			/	FT		SY

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PATCHING SCHEDULE					F.A. RTÉ.	SEC	TION		COUNTY	TOTAL	SHE S NO
LAGRANGE RD.				VAR.	2010-	015 RS		COOK	26	12	
 	L-T-C	IIIAIIGE II							CONTRACT	NO.	60K3
SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. F	ROAD DIST, NO. 1	ILLINOIS	FED. AI	D PROJECT		

150th	NB TOTALS:	1	10	131	40	4 145
	NB	1	10	4	40	4
150th	NB	1	10	4	40	4
- 150th	NB	11	1	L		17
	NB			L		4
150th	NB	1	<u> </u>	4		4
150th	NB	1		4	40	4
150th	SB	1	10	15	150	17
150th	SB	1	10	4	40	4
150th	SB	1	10	4	40	4
150th	SB .	1	10	4	40	4
150th	SB	1	10	4	40	4
147th	NB	1	10	15	150	17
147th	NB	1	10 ·	10	100	11
147th	NB	1	10	4	40	4
147th	NB	1 /	10	4	40	4
147th	NB	1	10	4	40	4
147th	NB	1	10	4	40	4
147th	NB .	1	10	4	40	4
147th	SB	1	10	4	40	4
147th	SB	1	10	4	40	4
147th	SB	1	10	4	40	4
147th	SB	1	10	4	40	4
147th	SB	1	. 10	4	40	4.
a humanda ( 👫 - Antayahanga Ada) i du un gilipun i hasuni innagar Aglaninini dupung di dupung dagan unda sulpun pulas dipinini	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD
TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
SSSTREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
	147th 150th 150th 150th 150th 150th 150th 150th 150th	TO (EB/WB) (NB/SB)  147th SB 147th SB 147th SB 147th SB 147th SB 147th SB 147th NB 147th NB 147th NB 147th NB 147th NB 147th NB 147th SB 150th SB 150th SB 150th SB 150th SB 150th NB 150th NB 150th NB 150th NB	TO         (EB/WB)         NO.           (NB/SB)         (1, 2, 3)           147th         SB         1           147th         NB         1           150th         SB         1           150th         SB         1           150th         SB         1           150th         SB         1           150th         NB         1           150t	TO         (EB/WB)         NO.         PATCH (NB/SB)           147th         SB         1         10           147th         NB         1         10           150th         SB         1         10           150th         SB         1         10           150th         SB         1         10	TO         (EB/WB)         NO.         PATCH (NB/SB)         PATC	TO         (EB/WB)         NO.         PATCH (N2,3)         PATCH (SQ FT)           147th         SB         1         10         4         40           147th         NB         1

ROL	JTE: Wood St. (127th St. t	to Thornton Rd.)					and the second s
CRO	DSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
a control of the cont	er rando-formanismo (Propries and Propries and Artifacture and Artifacture Artifacture and Art	. (NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
127th	Vermont	SB	1	10	4	40	4
127th	Vermont	SB	1	10	4	40	4
127th	Vermont	SB	1	10	6	60	7
127th	Vermont	SB	1	10	8	80	9
127th	Vermont	SB	2	10	4	40	4
Vermont	Broadway	NB	2	12	8	96	11
Broadway	Thornton Rd	SB	2	10	8	80	9
Broadway	Thornton Rd	SB	1	10	4	40	4
Broadway	Thornton Rd	SB	1	10	4	40	4
Broadway	Thornton Rd	NB	1	10	4	40	4
Broadway	Thornton Rd	NB	1	10	4	40	4
Broadway	Thornton Rd	NB	. 1	10	4	40	4
Broadway	Thornton Rd	SB	2	12	8	96	11
Broadway	Thornton Rd	SB	2	12	8	96	11
talan indonesia dan disenta di meneringka da urig geradangki waka seguna semulukan melapan diselagi bida agukan	том в не возменяться в принципрительной принципрительной в него принципрительной в постоя в него него него него него него него него	TOTALS:			78		92
er han verse filment van hannet terminisje verse, de trade en dette trade severable det the destrict de new des		uniterated destinations on a first control in relative residue space on two parts of curricularity once in a processor.	Management of the Control of Control of State (1997)	The state of the s	FT	and the second of the second o	SY

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

PATCHING SCHEDULE	F.A. RTE.	SECTIO
LOOMIS ST. AND WOOD ST.	VAR.	2010-015
SHEET NO. OF SHEETS STA. TO	D STA. FED. F	ROAD DIST. NO. 1 ILL

, CDC	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
y dia mandana y y programa y li an sy drigan manada and it dia mana ang amana manada dia mandan	with a wind - could come the country of the country and the country of the countr	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Rt 83 @ ILL171		WB	1	12	10	120	13
		WB	1	12	6	72	8
		WB	1	12	25	300	33
		WB	1	12	15	180	20
		WB	1	12	30	360	40
		WB	1	12	55	660	73
		WB	2	12	10	120	13
		WB	2	12	6	72	8
<u></u>		WB	2	12	25	300	33
		WB	2	12	15	180	20
		WB	2	12	30	360	40
	End of Dbl Lane, Conc Median	WB	2	12	55	660	73
Conc Median	End of Doi Lane, Conc Median	WB	1	12	10	120	13
CONC Median		WB	1	12	25	300	33
		WB	1	12	10	120	13
		WB	1	12	45	540	60
		WB	1	12	45 55	660	73
	Maley drive	WB	1	2	250	500	73 56
Malay Driva	ivialey unive	WB	1	12	30	360	40
Maley Drive		WB	1	12	60	720	80
•	K-Five Drive	WB	1	2	150	300	33
K-Five drive	IV-LIAE DIIAE	· WB	1	2	75	150	33 17
N-Five unive		WB	1	12	100	1200	133
		WB	1	12	100	1200	133
		WB	1	12	10	120	13
		WB	1	12	10	120	13
		WB	1	12	10	120	13
	St Mary's Drive	WB	1	12	30	360	40
St. Mary's Drive	3t Mary's Drive	WB	<del></del>	2	75	150	17
St. Mary's Drive		WB	1	12	100	1200	133
·		WB	1	12	100	1200	133
		WB	1	12	10	120	13
		WB	1	12	40	480	53
	Walker Road	WB	1	2	180	360	40
Walker Road	Walker Noau	EB	1	2	50	100	11
valkei Ruau		EB	1	2	200	400	44
		EB	1	2	150	300	33
		EB	1	12	20	240	27
		EB	1	12	50	600	67
		EB	1	12	40	480	53
		EB	1	12	30	360	40
		EB	1	12	50	600	67
	Mt. Assisi Drive	EB	1	12	100	1200	133
VIt. assisi Drive	IVIL. ASSISI DIIVE	EB	1	2	50	100	11
VIL. GOOISI DIIVO	·	EB	1	2	200	400	44
· · · · · · · · · · · · · · · · · · ·		EB	1	2	150	300	33
		EB	1	12	40	480	53
		EB	1	12	30	360	40
		EB	1	12	100	1200	133
		EB	1	12	20	240	27
		EB	1	12	300	3600	400
		EB	1	12	60	720	80
		EB	1	12	75	900	100
	Maley Drive	EB	1	2	250	500	56
Malay Drive	Maley Drive	EB	1	2	60	120	13
Maley Drive		EB		2	150	300	33
			11		40	480	53
·		EB	11	12	30	360	40
	Cana Madian	EB	11	12			
Oana Madian	Conc Median	EB	11	12	10	120	13 13
Conc Median		EB		12	10	120	
······	DI 00 @ " 474	EB	1	12	45 75	540	60
	Rt 83 @ IL 171	EB	. 1	12	75	900	100

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	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			WIA	Alla 21					CONTRACT NO. 60K34
	PLOT DATE = 2/17/2010	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED. A	ID PROJECT

00000	OTOFFTO	DIDECTION		I DAY (FA IFE)	D0\ #= \$ #== \ 1==	DEDAID	DEDAIR
	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
194	west of Torrence	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	12	144	16
		EB	1	12	8	96	11
		EB	1	12	6	72	8
		EB	1	12	6	72	8
	,	EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	8	96	11
west of Torrence	1 94	WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	6	72	8
		WB	1	-12	6	72	8
		WB	1	12	6	72	8
		WB	1	12	12	144	16
l 9 <del>4</del>	west of Torrence	EB	2	12	6	72	8
· · · · · · · · · · · · · · · · · · ·		EB	2	12	8	96	11
		EB	2	12	8	96	11
		• EB	2	12	6	72	8
		EB	2	12	6	72	8
<del> </del>		EB	2	12	6	72	8
***************************************		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	20	240	27
		EB	2	12	6	72	8
		EB	2	12	6	72	8
	<u> </u>	EB	2	12	10	120	13
		EB	2	12	10	120	13
		EB	2	12	6	72	8
west of Torrence	1 94	WB	2	12	6	72	8
		WB	2	12	20	240	27
		WB	2	12	6	72	8
		WB	2	12	10	120	13
		WB	2	12	6	72	8
		WB	2	12	20	240	27
There's and the state of the st		TOTALO	Augustonia (Marterior Agrango) and		ARTON AND A BOUND AND A WAY TO THE TO THE AND	AND AND ADMINISTRATION OF THE PARTY OF THE P	448
		TOTALS:			314		419

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	PLOT DATE = 2/17/2010	DATE -	REVISED ~		SCALE:	SHEET NO.	OF	SHEET	STA.	TO STA.	FED. ROAD	DIST. NO. 1 ILLINOIS FED. A		

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
glightere ikan kan kan kan kan kan kan kan kan kan	and the state of the state of the second state	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
178TH STREET	THORTON LANSING RD	SB	1	2	36	72	8
		SB	1	2	30	60	7
		SB	1	2	400	800	89
	· · · · · · · · · · · · · · · · · · ·	SB	1	5	12	60	7
		SB	1	1	12	12	1
		SB	1	4	12	48	5
		SB	1	2	200	400	44
		SB	1	2	12	24	3
		ŞB	1	2	12	24	3
THORTON LANSING RD	RIDGE RD	SB	1	2	12	24	3
		SB	1	2	12	24	3
·		SB	1	2	12	24	3
		SB	1	2	50	100	11
		SB	1	2	100	200	22
		SB	1	4	12	48	5
		SB	1	2	24	48	5
		SB	1	2	24	48	5
178TH STREET	THORTON LANSING RD	SB	2	4	30	120	13
178th / TORRENCE INTE		SB	2	4	14	56	6
170til7 TOTALITOL IIIIL	I	SB	2	2	400	800	89
		SB	2	12	12	144	16
		SB	2	4	50	200	22
·		SB	2	2	12	24	3
		SB	2	2	20	40	4
·		SB	2	2	15	30	3
		SB	2	4	12	48	5
THORTON LANSING RD	PIDGE PD	SB	2	2	12	24	3
INORTON LANGING ND	RIDGE RD	SB	2	2	12	24	3
		SB	2	1	8	8	1
		SB	2	2	12	24	3
		SB	2	4	4	16	<u>3</u> 2
		SB	2	4	4	16	2
		SB	2	2	2	4	0
		SB	2		12	48	5
VIII.		SB	2	4	4	16	2
		SB	2		4	16	<u>2</u> 2
		SB	2	4	4	16	2
		SB	2	4		8	<u>∠</u> 1
		SB	2	2 2	4	16	
DUDOED KINO DADILIO					8 15		. 2
BURGER KING RADIUS	THORTON LANGING DD	SB	2	8	15	120	13
RIDGE RD	THORTON LANSING RD	NB ND	1	2	200	400	44
	,	NB	1	4	12	48	5
		NB	1	2	50	100	11
		NB	1	4	100	400	44
		NB	1	2	60	120	13
	,	NB	1	2	20	40	4
		NB	1	2	80	160	18

ROUTE:	Torrence Ave.	(Ridge Rd.	to I-80/94)

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
- Holoman with the field thousands in which as those only must use a mind which is the original trapping to the data is a solid fe	- Control State (Control of State August 1 - State Install Install August 1 - State Install Install Install Install Install Install Install Install Install	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
RIDGE RD	THORTON LANSING RD	NB	1	2	12	24	3
		NB	1	2	12	24	3
		NB	1	4	12	48	5
		NB	1	12	12	144	16
·		NB	1	2	60	120	13
		NB	1	2	12	24	3
		NB	1	2	12	24	3
	······································	NB	1	2	12	24	3
		NB	1	2	12	24	3
<del></del>		NB	1 1	2	12	24	3
***************************************		NB	1 1	2	12	24	3
		NB	1	2	12	24	3
		NB	1 1	2	12	24	3
		NB	1 1	2	12	24	3
		NB	<del>                                     </del>	2	12	24	3
		NB	1	2	50	100	11
RIDGE RD	THORTON LANSING RD	NB	2	12	60	720	80
TABOL TO	THE RESIDENCE TO	NB	2	4	60	240	27
		NB	2	12	40	480	53
		NB	2	2	100	200	22
		NB	2	4	20	80	9
		NB	2	4	12	48	5
		NB	2	2	12	24	3
		NB	2	3	30	90	10
		NB NB	2	2	6	12	10
		NB	2	4	50	200	22
		NB	2	2	20	40	4
		NB	2	2	10	20	2
		NB NB	2	4	12	48	5
		NB NB	2	6	12	72	8
		NB	2	4	10	40	4
		NB	2	2	8	16	2
THORTON LANSING RD	170TU STDEET	NB NB	2	4	8	32	4
THORTON LANGING RD	17011131KLL1	NB	2	2	6	12	1
••••••••••••••••••••••••••••••••••••••		NB	2	4	12	48	5
		NB	2	2	12	24	3
L ALONG CL FROM 179TH	I TO MORTH LIMITS	NB	2	2	900	1800	200
L LONG OF LUCINI 1/81L	I TO NORTH LIMITS	NB	2	4	12	48	200 5
		NB NB	2	2	12		3
		NB NB	2	1		24	
				4	50 50	200	22
		NB NB	2	4	50 30	200	22
		NB NB	2	4	30 10	120	13
		NB	2	2	10	20	2
	l	NB	2	4	50	200	22

TOTALS: 4056 1207 FT SY

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	PLOT DATE = 2/17/2010	DATE ~	REVISED -	ĺ

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

			NG SCHI RENCE A		
 SHEET	NO.	OF	SHEETS	STA.	то :

makerine antonomin kantalanda kantana a pomininanjan i a sina transmini di pramining di pramining sa asia	Benerally springers (make to play to play a series of the content	TOTALS:	Control control to the control of th	/ :	652	lation and approximation of the state of the	682
Voodlawn	I-394	WB	2	6	100	600	67
Ellis	Woodlawn	WB	-	-	-	-	
Park	Ellis	WB	-	-	-		
Ford Entrance	Park	WB	-	-	-	-	′
ndian Ct	Ford Entrance	WB	2	12	6	72	8
		WB	2	6	75	450	50
State	Indian Ct	WB	2	6	100	600	67
		WB	2	12	6	72	8
Wentworth	State	WB	2	12	8	96	11
		WB	2	12	8	96	11
***************************************		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	6	72	8
East End Ave	Wentworth	WB	2	12	8	96	11
Halsted	East End Ave	WB	2	12	6	72	8
East End Ave	Halsted	EB		-	-	_	
		EB	2	12	6	72	8
		EB	2	12	6	72	8
Wentworth	East End Ave	EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
State	Wentworth	EB	2	12	75	900	100
Indian Ct	State	EB	2	12	150	1800	200
		EB	2	12	6	72	8
		EB	2	8	6	48	5
		EB	2	10	6	60	7
		EB	2	12	6	72	8
Ford Entrance	Indian Ct	EB	2	12	6	72	8
		EB	2	12	6	72	8
Park	Ford Entrance	EB	2	12	6	72	8
Ellis	Park	EB	2	12	6	72	8
Woodlawn	Ellis	EB	2	12	6	72	8
I-394	Woodlawn	EB EB	2	12	8	96	11
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
CR	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -		PATCHING SCHEDULE	F.A. SECTION	COUNTY TOTAL SHEET
c:\pw_work\PWIDOT\WILGREENDP\dØ183625\De	sign.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	FATORING SUREDULE	VAR. 2010-015 RS	COOK 26 17
	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	US 30	1	CONTRACT NO. 60K34
	PLOT DATE = 2/17/2010	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. A	

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
ick f. dy reing (christ Site Chalander allen der 1900) alle der Arreitander der Site der Park Site Arreitan is Arreitander (b. 1904).	- 400-000-00-000-00-00-00-00-00-00-00-00-0	(NB/SB)	(1, 2, 3)	WDTH	LENGTH	(SQ FT)	(SQ YD)
Relocated Route 1	Steger Rd	NB	2	6	20	120	13
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB NB	2	12	20	240	27
			2	12	10	120	13
Relocated Route 1	Steger Rd	NB					
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	8	96	11
Relocated Route 1	Steger Rd	NB	2	4	75	300	33
Relocated Route 1	Steger Rd	NB	2	12	20	240	27
	Steger Rd	NB	2	4	15	60	7
	Steger Rd	NB	2	12	. 10	120	13
	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	4	100	400	44
Relocated Route 1	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	4	500	2000	222
	Steger Rd	NB	2	12	200	2400	267
	Steger Rd	NB	2	12	10	120	13
	Steger Rd	NB	2	12	15	180	20
	Steger Rd	NB	2	12	10	120	13
Relocated Route 1	Steger Rd	NB	2	4	125	500	56
	Steger Rd	NB	2	4	120	480	53
Relocated Route 1	Steger Rd	NB	2	12	20	240	27
	Steger Rd	NB	2	4	50	200	22
Relocated Route 1	Steger Rd	NB	2	12	8	96	11
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	15	180	20
	Steger Rd	NB	2	4	50	200	22
	Steger Rd	NB	2	12	8	96	11
	Steger Rd	NB	2	4	100	400	44
	Steger Rd	NB	2	4	75	300	33
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	10	120	13
	Steger Rd	NB NB	2	12	6	72	8
	Steger Rd				50		22
		NB NB	2	4		200	
	Steger Rd	NB NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72	8
	Steger Rd	NB	2	12	6	72 .	8
Relocated Route 1	Steger Rd Steger Rd	NB	2	4	50	200 200	22
Relocated Route 1		NB	1	4	50		22

NO. (1, 2, 3) 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	PATCH WIDTH 12 12 12 12 12 12	PATCH LENGTH 6 6 6 6	AREA (SQ FT) 72 72	AREA (SQ YD
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1 1 1 1 1	12 12 12 12	6 6		^
1 1 1 1	12 12 12	6	72	-8
1 1 1	12 12		1	8
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1		6	72	8
	4	50	200	22
1 1	12	25	300	33
1 1	12	6	72	8
1	12	6	72	8
1	12	6	72	8
1	12	8	96	11
1	12	8	96	11
1	12	10	120	13
$\frac{1}{1}$	12	10	120	13
<del>-i-</del>	4	75	300	33
_i	12	6	72	- 8
1	4	25	100	11
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<del>- i -  </del>	12	8	96	11
$\frac{1}{1}$	12	6	72	8
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<del>- i - l</del>	12	15	180	20
1	12	8	96	11
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	4	50	200	22
1 1	4	150	600	67
1	12	25	300	33
1	12	6	72	8
1	12	25	300	33
<del>- i - l</del>	4	75	300	33
<del>- i -  </del>	12	15	180	20
<del>-i-</del>	12	6	72	8
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1	4	50	200	22
1	4	100	400	44
1	12	6	72	8
1	4	100	400	44
1	12	20	240	27
1	12	15	180	20
1	4	25	100	11
1	12	15	180	20
1	4	75	300	33
1	4	100	400	44
1	12	6	72	8
1	12	6	72	8
1	12	6	72	8
		50		33
1	12	30	360	40
		<u>-</u>		
	1 1 1 1	1 4 1 12 1 12 1 12 1 6	1     4     100       1     12     6       1     12     6       1     12     6       1     6     50	1     4     100     400       1     12     6     72       1     12     6     72       1     12     6     72       1     6     50     300       1     12     30     360

ROUTE: NB IL 394 (Steger Rd to Relocated Route 1)

NOTE: THIS SECTION OF IL 394 IS LOCATED IN WILL COUNTY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED ~		PATCHING SCHEDULE		F.A.	SECTION	COUNTY TOTAL SHEET	
c:\pw_work\PWIDOT\WILGREENDP\d0183625\	Design.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS				VAR.	2010-015 RS	COOK 26 18
	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	IL 394				CONTRACT NO. 60K34	
	PLOT DATE = 2/17/2010	DATE -	REVISED -		SCALE:	SHEET NO. OF SHEETS STA.	TO STA.	FED. ROAD	DIST. NO. 1   ILLINOIS FED. A	

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
r reger a crossocara a feasoaradeos estito e procarrendos suppen anaces estito difere constituiro	i tyrograpit der i 1907, jogani seine adeistani jude sendipedi i seddi jedajesto dogasa ugblandiska	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	. 2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	1 4	50	200	22
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	4	100	400	44
Steger Rd	Relocated Route 1	SB	2	4	75	300	33
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	6	50	300	33
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	6	50	300	33
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	6	60	360	40
	Relocated Route 1	SB	2	6	125	750	83
Steger Rd Steger Rd	Relocated Route 1	SB	2	6	125	750 750	83
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	300	1200	133
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	6	300	1800	200
Steger Rd	Relocated Route 1	SB	2	12	20	240	27
Steger Rd	Relocated Route 1	SB	2	12	50	600	67
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	250	1000	111
Steger Rd	Relocated Route 1	SB	2	4	500	2000	222
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	8	96	11
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	150	600	67
Steger Rd	Relocated Route 1	SB	2	12	35	420	47
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	4	50	200	22
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	6	72	8
Steger Rd	Relocated Route 1	SB	2	12	10	120	13
Steger Rd	Relocated Route 1	SB	2	12	10	120	13
Steger Rd	Relocated Route 1	SB	2	4	25	100	11
Steger Rd	Relocated Route 1	SB	2	4	25 25	100	11
Steger Rd	Relocated Route 1	SB	1	12	6	72	8
	<b>4</b>	SB	1	4	50	200	22
Relocated Route 1 Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Neiocated Noute 1	Steger Rd	SB		12	6	72	8

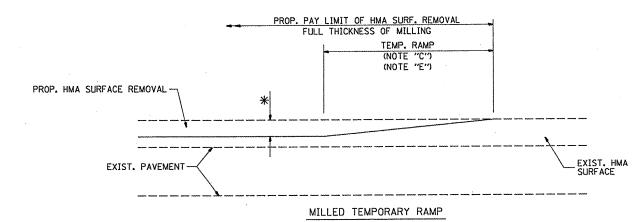
ROUTE:	ISB	IL	394	(Steaer	Rd 1	to Re	located	Route	1)

	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	10	120	13
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	6	50	300	33
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	15	60	7
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	- 8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	75	300	33
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	4	150	600	67
Relocated Route 1	Steger Rd	SB	1	4	75	300	33
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	250	1000	111
Relocated Route 1	Steger Rd	SB	1	4	250	1000	111
Relocated Route 1	Steger Rd	SB	1	4	500	2000	222
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	35	420	47
Relocated Route 1	Steger Rd	SB	1	4	200	800	89
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	4	50	200	22
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	4	100	400	44
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	4	25	100	11
Relocated Route 1	Steger Rd	SB	1	12	16	192	21
Relocated Route 1	Steger Rd	SB	1	12	16	192	21
Relocated Route 1	Steger Rd	SB	1	6	50	300	33
Relocated Route 1	Steger Rd	SB	1	12	6	72	8
Relocated Route 1	Steger Rd	SB	1	12	6	72	8

TOTALS: 5881 3199
FT SY

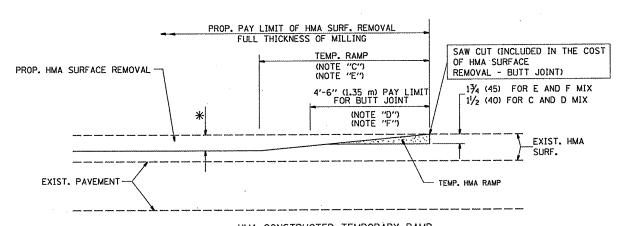
NOTE: THIS SECTION OF IL 394 IS LOCATED IN WILL COUNTY

FILE NAME =	USER NAME = wilgreendp	DESIGNED -	REVISED -		PATCHING SCHEDULE	F.A. SECTION	COUNTY SHEET
c:\pw_work\PWIDOT\WILGREENDP\d0183625\D	esign,dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	FAIGHING SCHLDULE	VAR. 2010-015 RS	соок 2.6 19
·	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	IL 394		CONTRACT NO. 60K34
	PLOT DATE = 2/17/2010	DATE -	REVISED -	]	SCALE: SHEET NO. OF SHEETS STA. TO STA.	EED BOAD DIST NO 1 THE INDISES	TOU AID PROJECT



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

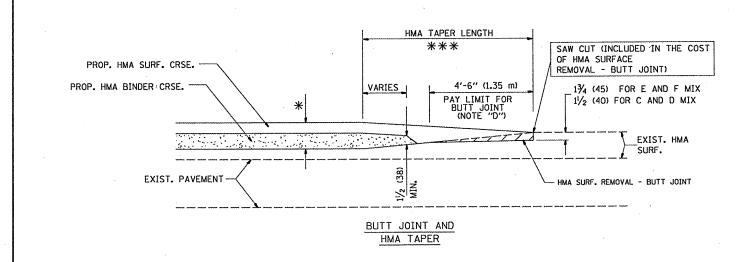
### OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP (FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

### OPTION 2

### TYPICAL TEMPORARY RAMP

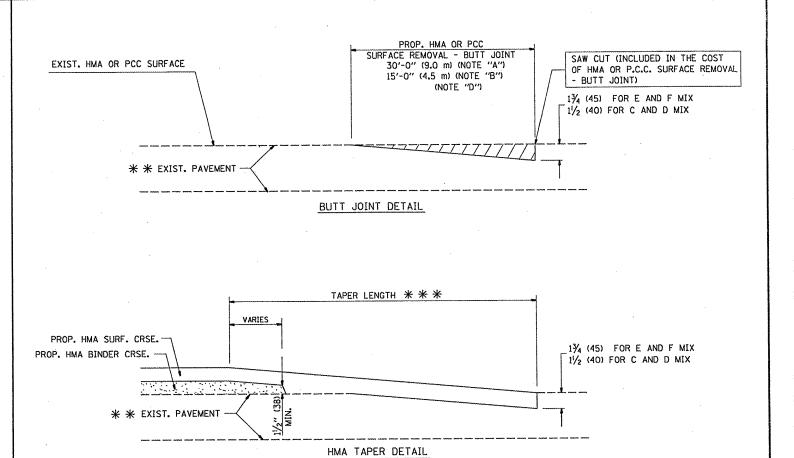


### TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

REVISED - R. SHAH 10-25-94 FILE NAME = USER NAME = whiteter DESIGNED - M. DE YONG DRAWN REVISED - A. ABBAS 03-21-97 CHECKED -REVISED - M. GOMEZ 04-06-01 PLOT SCALE = 100.0000 '/ IN. PLOT DATE = 2/10/2010 DATE - 06-13-90 REVISED - R. BORO 01-01-07

## STATE OF ILLINOIS

SECTION COLINTY COUNTY SHEETS NO. **BUTT JOINT AND** 2010-015 RS HMA TAPER DETAILS CONTRACT NO. 60K34 BD400-05 BD32 SHEET NO. 1 OF 1 SHEETS STA. TO STA. SCALE: NONE FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



### TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

### NOTES

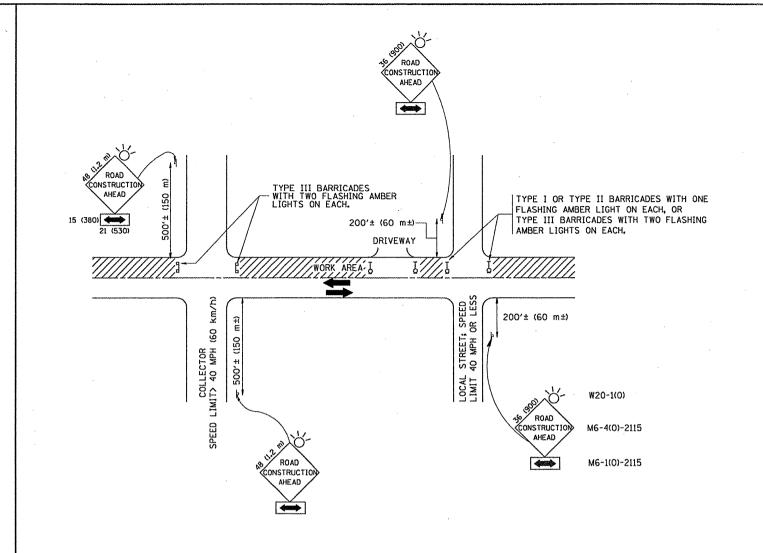
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

### BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER)
FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

**DEPARTMENT OF TRANSPORTATION** 



### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- 0) ONE ROAD CONSTRUCTION AHEAD SIGN  $36\times36$  ( $900\times900$ ) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h)
  AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- d) ONE ROAD CONSTRUCTION AHEAD SIGN  $48 \times 48$  (1.2 m  $\times$  1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

SCALE: NONE

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = USER NAME = whitetor DESIGNED - LHA REVISED - J. OBERLE 10-18-95
c1\pw.work\PWIDOT\WHITETAR\d0175544\D13
Std.dgn DRAWN - REVISED - A. HOUSEH 03-06-96
PLOT SCALE = 188.8000 '/ IN. CHECKED - REVISED - A. HOUSEH 10-15-96
PLOT DATE = 2/10/2010 DATE - 06-89 REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

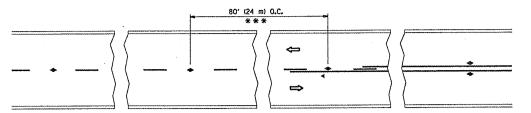
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SHEET NO. 1 OF 1 SHEETS STA. TO

F.A. SECTION COUNTY SHEETS NO.

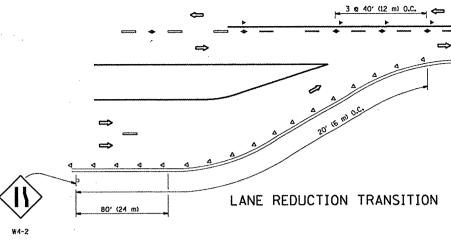
VAR 2010-015 RS COOK Z6 Z1

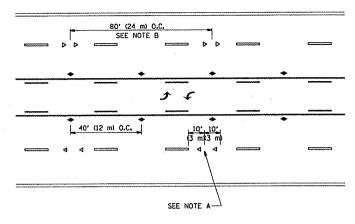
TC-10 CONTRACT NO. 60K34



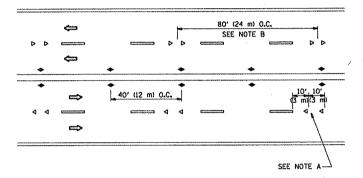
\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

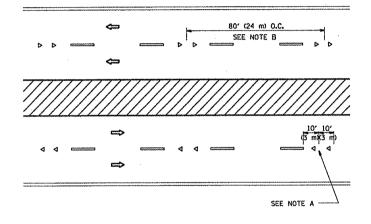




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

### GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

### LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

### SYMBOLS

YELLOW STRIPE

WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (₩/O)
- ◆ TWO-WAY AMBER MARKER

### DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

### 

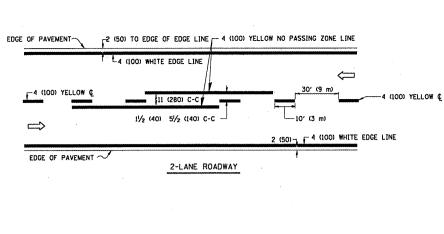
LEFT TURN

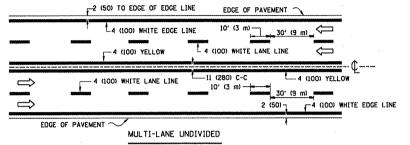
All dimensions are in inches (millimeters) unless otherwise shown.

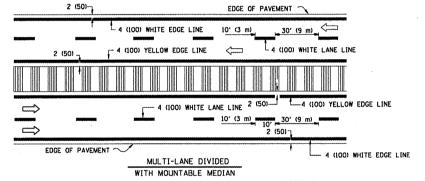
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	TYPICAL APPLICATIONS									
	RAISED	REFLECTIVE	PAVEMENT	MARKERS	(SNOW-PLOW	RESISTANT)				
CALE:	NONE	SHEET NO.	. 1 OF 1	SHEETS	STA.	TO STA.	_			

F.A RTE.	SEC	Τ	COUNTY	TOTAL	SHEET NO.		
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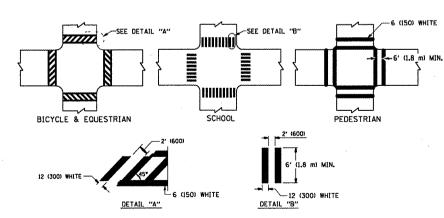




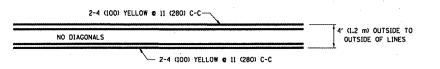


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

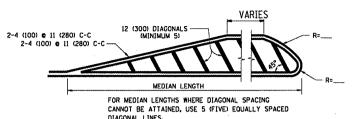
### TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

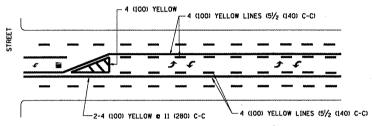


### 4' (1.2 m) WIDE MEDIANS ONLY

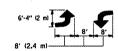


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

### MEDIANS OVER 4' (1.2 m) WIDE

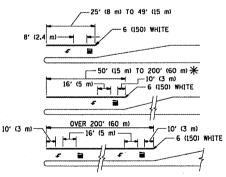


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

### TYPICAL PAINTED MEDIAN MARKING

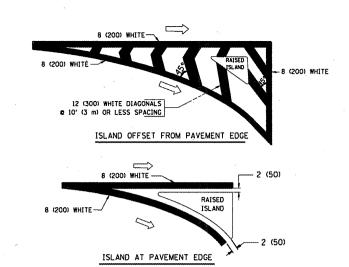


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  $\P$  AREA = 15.6 SO. FT. (1.5 m<sup>2</sup>)  $\P$  AREA = 20.8 SO. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF

TYPICAL LEFT (OR RIGHT) TURN LANE

### TYPICAL TURN LANE MARKING



### TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 0 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 <b>e</b> 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS).	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ¢ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
,	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 <b>c</b> 6 (150) 12 (300) <b>c</b> 45° 12 (300) <b>c</b> 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (500) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT OSSIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 e 4 (100) WITH 12 (300) DIAGONALS e 45°	SOLID	YELLOW: TWO WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS		WHITE: ONE WAY TRAFFIC	
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DTAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 ml LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) <b>c</b> 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

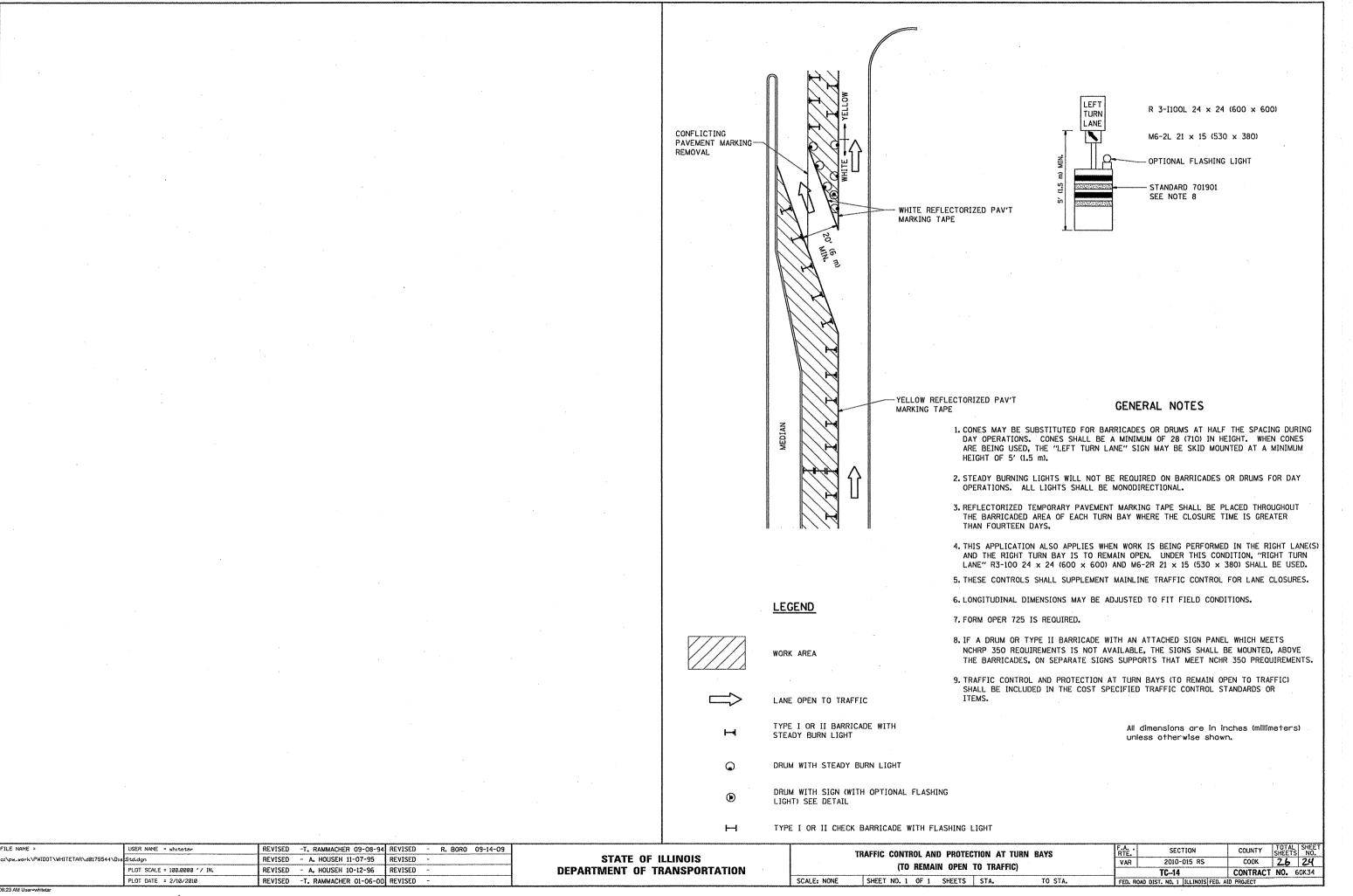
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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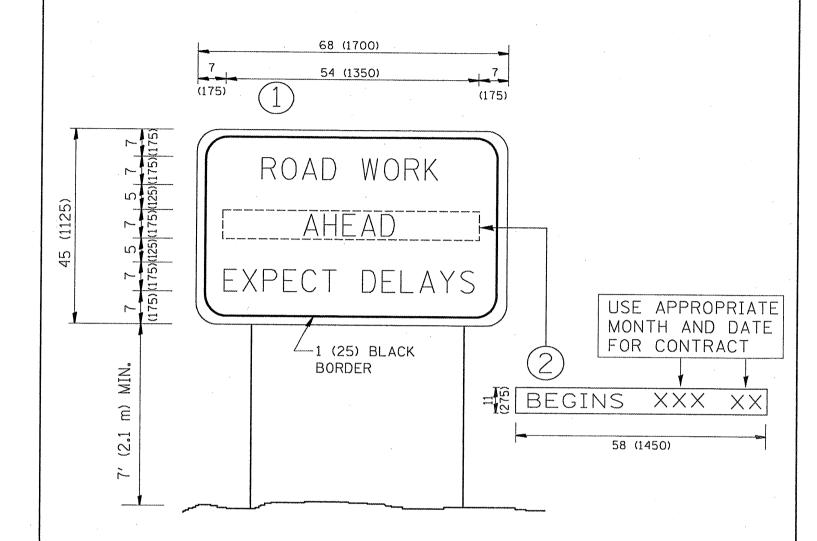
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TYPICAL PAVEMENT MARKINGS						VAR.	2010-015 RS	соок	26	23
						TC-13		CONTRACT	NO.	60K34
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### NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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# PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL. 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAVED SHOULDER. PAVED OR NON-PAVED SHOULDER I'' (25 mm) UNIT DUCT-TRENCHED TO E/P \*\*\*

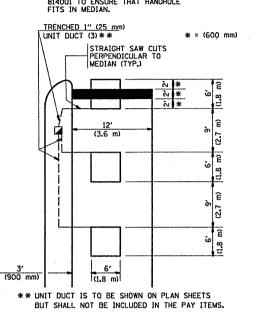
\* \* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS

BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

# LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

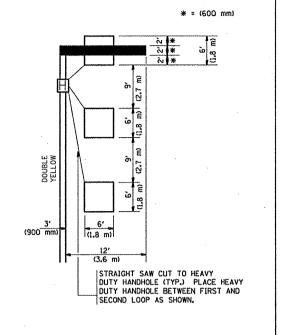


NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

# LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

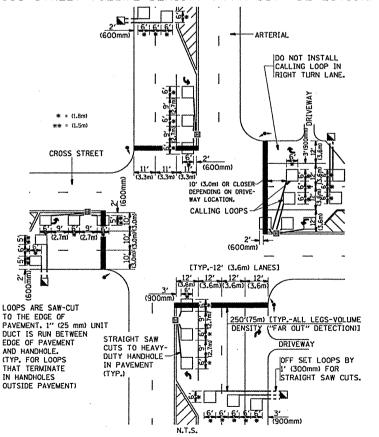


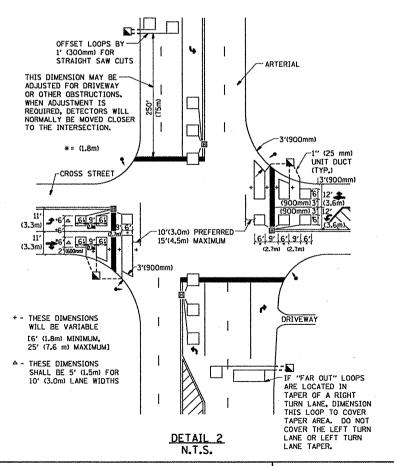
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





### NOTES:

### VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

### PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON  $\underline{\text{ALL}}$  SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

### NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

DETAIL 1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION

DETAILS FOR ROADWAY RESURFACING

SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD 12

 F.A. THE.
 SECTION
 COUNTY SHEET NO.

 VAR
 2010-015 RS
 COOK
 26
 26

 TS-07
 CONTRACT
 NO.
 60K34