

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS	DUPAGE	52	1
ILLINOIS			CONTRACT NO. 80B25	

* 52 + 8 = 64 TOTAL SHEETS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED WITHIN THE VILLAGES OF GLEN ELLYN AND LOMBARD

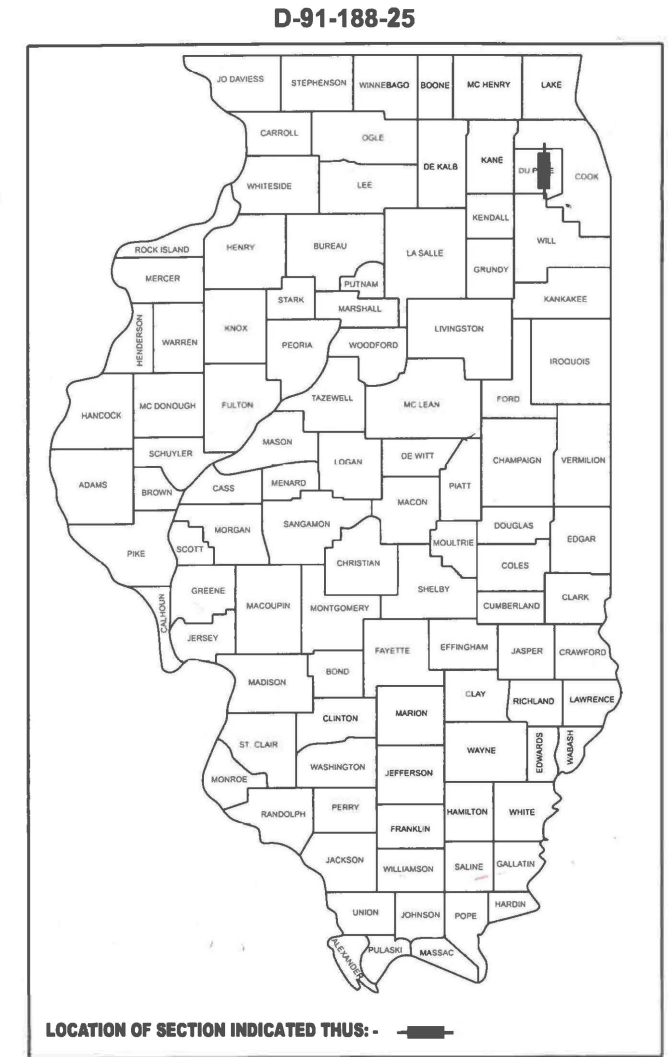
TRAFFIC DATA:

IL ROUTE 53 (OTHER PRINCIPAL ARTERIAL):

- BRYANT AVE TO N OF WILSON RD
ADT (2023) = 15300 VPD TO 15800 VPD
SPEED LIMIT = 40 MPH
- N OF WILSON RD TO S OF GLEN OAK RD
ADT (2023) = 15800 VPD TO 20800 VPD
SPEED LIMIT = 45 MPH
- S OF GLEN OAK RD TO S OF ST CHARLES RD
ADT (2023) = 20800 VPD TO 21600 VPD
SPEED LIMIT = 35 MPH

FAP ROUTE 870: IL 53 (COLUMBINE AVENUE)
SOUTH OF ST CHARLES ROAD (PHILLIPS CT) TO BAKER HILL DRIVE
SECTION: 2025-2006-RS
PROJECT: NHPP-8X19(066)
STANDARD OVERLAY, ADA IMPROVEMENTS, NEW SHOULDERS
DUPAGE COUNTY

C-91-261-25



LOCATION OF SECTION INDICATED THUS: - [Symbol] -

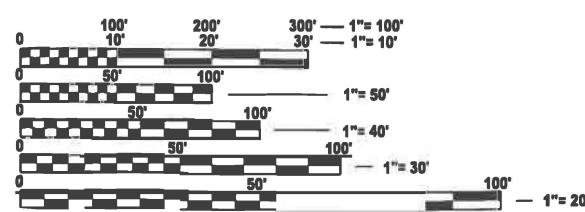
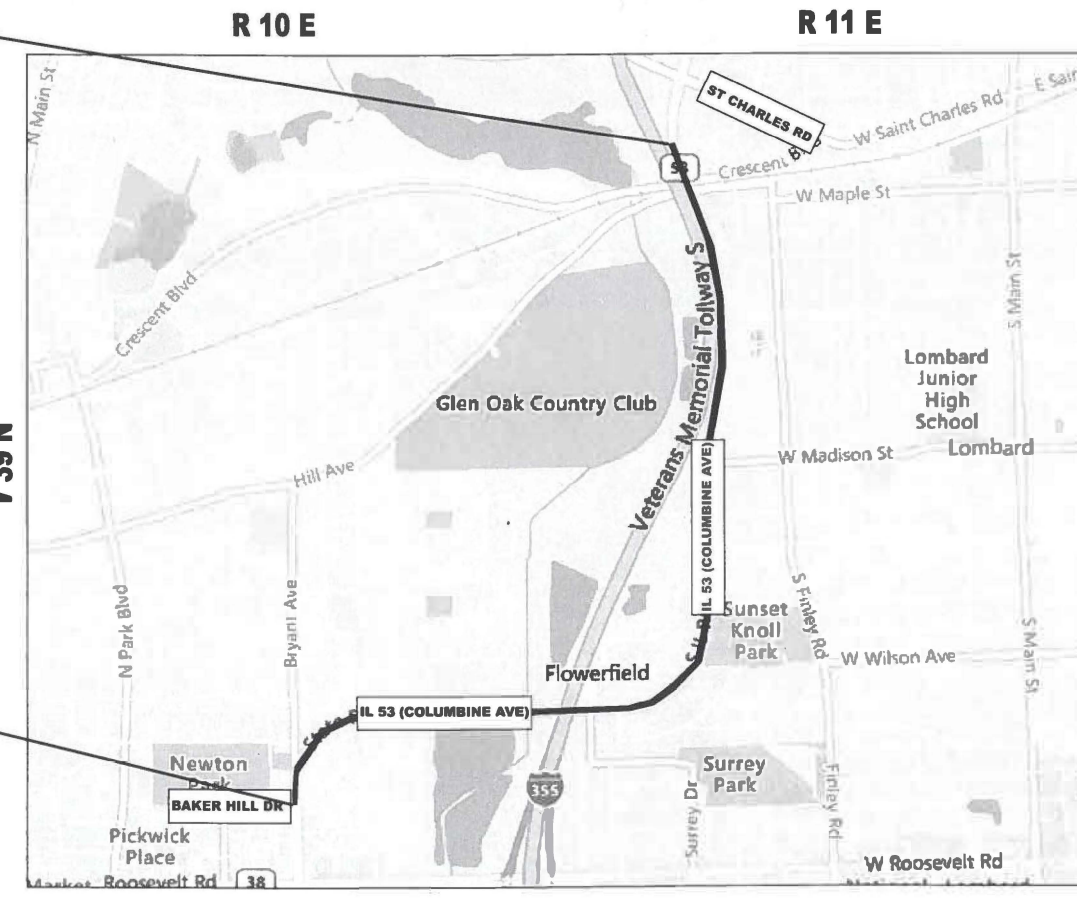
PROJECT ENDS
STA. 144+00

RESURFACING BEGINS:
STA 12+94

RESURFACING OMISSION:
STA 12+05
TO STA 12+94;
STA 49+22
TO STA 50+80

RESURFACING ENDS:
STA 144+00

PROJECT BEGINS
STA. 12+05



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL

GROSS LENGTH = 13,195 FT. = 2.499 MILE
NET LENGTH = 12,948 FT. = 2.452 MILE

CONTRACT NO. 80B25

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 9th 2025
Jacobson
REGIONAL ENGINEER

January 23, 2026
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

January 23, 2026
[Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

SHEET NO.

DESCRIPTION

STANDARD NO.

DESCRIPTION

10. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
11. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
12. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
13. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
14. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE A FIELD LABORATORY FOR USE FOR ANY ON-SITE TESTING BY THE ENVIRONMENTAL FIRM. NO TESTING OF ANY KIND CONTAMINATED OR NON-CONTAMINATED, FLUID OR SOLID SHALL BE PERMITTED IN THE ENGINEER'S FIELD OFFICE.
15. ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.
16. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN, THE TRANSITIONS SHALL BE PAID FOR THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
17. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MIN. 1:3 (V:H).
18. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS SHALL BE PAID FOR ACCORDING TO THE ARTICLE 109.04 OF THE STANDARD UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
19. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
20. ALL MILLED SURFACES SHALL BE UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
21. TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
22. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
23. TEMPORARY PAVEMENT MARKINGS OR SHORT TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED, UNLESS DIRECTED BY THE ENGINEER.

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GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGES OF GLEN ELLYN AND LOMBARD.
3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
4. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
5. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
6. THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
7. ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
8. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
9. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

GEOTECHNICAL NOTES

1. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
2. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.
3. THE AGGREGATE GRADATION FOR THE LOWER 9 INCHES OF AGGREGATE SUBGRADE IMPROVEMENT 12" SHALL BE CS 1 OR RR1.

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	USER NAME = Aya.Eikhatib	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES IL ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -			870	2025-2006-RS, 8X19(066)	DUPAGE	56	2
		CHECKED -	REVISED -							CONTRACT NO. 80B25
	PLOT DATE = 12/2/2025	DATE -	REVISED -		SCALE:	SHEET 1	OF 2	SHEETS	STA.	TO STA.
										ILLINOIS FED. AID PROJECT

MAINTENANCE OF TRAFFIC NOTES

WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.

Table 2, Condition II
Drop-off Near the Edge of Traveled Way

Existing Road Type	Normal Posted Speed Limit, NPSL (mph)	Drop-off Depth, D (in.)	TCB is Warranted(2)	Use of TCB may be warranted, based on traffic exposure.(2)	Maximum Allowable Total Traffic (Both Directions) Without TCB (3)
2L2W	Up to 35	12 ≤ D ≤ 18		Yes(1)	3.02
2L2W	Up to 35	18 < D ≤ 24		Yes(1)	2.39
2L2W	Up to 35	24 < D ≤ 36		Yes(1)	2.08
2L2W	Up to 35	D > 36	Yes(1)		
2L2W	35 < NPSL ≤ 45	12 ≤ D ≤ 18		Yes(1)	1.42
2L2W	35 < NPSL ≤ 40	18 < D ≤ 24		Yes(1)	1.12
2L2W	> 45	D > 12	Yes(1)		
4L2W	Up to 35	12 ≤ D ≤ 18		Yes(1)	9.31
4L2W	Up to 35	18 < D ≤ 24		Yes(1)	7.30
4L2W	Up to 35	24 < D ≤ 36		Yes(1)	6.25
4L2W	Up to 35	> 36	Yes(1)		
4L2W	35 < NPSL ≤ 45	12 ≤ D ≤ 18		Yes(1)	3.43
4L2W	35 < NPSL ≤ 40	18 < D ≤ 24		Yes(1)	2.94
4L2W	> 45	D ≥ 12	Yes(1)		
All	>45	D < 12	No (2)		
All	>45	D ≥ 12	Yes		

RAILROAD NOTES

1. THE FOLLOWING ITEMS SHALL BE COMPLETED PRIOR TO THE CONTRACTOR COMMENCING WORK ON UPRR PROPERTY:

- THE CONTRACTOR MUST CONTACT A FLAGGING COMPANY DIRECTLY AND SCHEDULE FLAGGERS TO BE PRESENT DURING THE CONTRACTOR'S WORK OPERATIONS. BELOW IS A LINK TO INFORMATION ON THIRD-PARTY APPROVED UPRR FLAGGING COMPANIES:

UP: THIRD PARTY FLAGGING POLICY

- THE CONTRACTOR AND SUB-CONTRACTOR(S) SHALL PARTICIPATE IN A SHORT PRE-CONSTRUCTION MEETING WITH REPRESENTATIVES OF THE ENGINEER AND THE UPRR. THE TOPICS TO BE DISCUSSED INCLUDE, BUT ARE NOT LIMITED TO, FLAGGING, TRAFFIC CONTROL, SAFETY, AND BADGING REQUIREMENTS.

- ALL CONTRACTOR AND SUB-CONTRACTOR EMPLOYEES THAT INTEND TO ACCESS THE UPRR ROW MUST SUCCESSFULLY COMPLETE ERAILSAFE TRAINING. THE CONTRACTOR AND SUB-CONTRACTOR(S) SHALL PROVIDE COPIES OF EACH EMPLOYEE'S SUCCESSFULLY COMPLETED TRAINING CERTIFICATE TO THE APPROPRIATE UPRR REPRESENTATIVE. BELOW IS A LINK TO INFORMATION ON THE ERAILSAFE TRAINING:

[HTTPS://WWW.UP.COM/ABOUTUP/COMMUNITY/SAFETY/ERAILSAFE/UP-PAT/INDEX.HTM](https://www.up.com/aboutup/community/safety/erailsafe/up-pat/index.htm)

2. THE AUDIT NUMBER REFERENCED IN THE TRAINING IS THE REMS/PROJECT NUMBER AT THE TOP RIGHT ON THE MAINTENANCE CONSENT LETTER INCLUDED IN THE SPECIAL PROVISIONS.
3. THE CONTRACTOR MUST REFERENCE UP PROJECT NUMBER **552799** IN THEIR COMMUNICATION WITH THE UP WHEN THE CONTRACTOR APPLIES FOR THE RIGHT-OF-ENTRY PERMIT

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USER NAME = Aya.Eikhalib	DESIGNED -	REVISED -
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PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	3
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				TYPE CODE					
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
				ROADWAY	ROADWAY	TRAFFIC SIGNALS			
				80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0005	0021			
20200100	EARTH EXCAVATION	CU YD	4185	4185					
20800150	TRENCH BACKFILL	CU YD	5	5					
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	320	320					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	115	115					
21400100	GRADING AND SHAPING DITCHES	FOOT	2520	2520					
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	0.25	0.25					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	0.25	0.25					
25100630	EROSION CONTROL BLANKET	SQ YD	1125	1125					
25200110	SODDING, SALT TOLERANT	SQ YD	115	115					
25200200	SUPPLEMENTAL WATERING	UNIT	1.15	1.15					
28000305	TEMPORARY DITCH CHECKS	FOOT	150	150					
28000500	INLET AND PIPE PROTECTION	EACH	10	10					
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	1125	1125					
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	6925	6925					
35400100	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 6"	SQ YD	105	105					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	20	20					
35600708	HOT-MIX ASPHALT BASE COURSE WIDENING, 8"	SQ YD	5920	5920					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	46055	46055					
40600370	LONGITUDINAL JOINT SEALANT	FOOT	27720	27720					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	95	95					

SUMMARY OF QUANTITIES				TYPE CODE					
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
				ROADWAY	ROADWAY	TRAFFIC SIGNALS			
				80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0005	0021			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	307	307					
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	2895	2895					
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	2	2					
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	6849	6849					
42001300	PROTECTIVE COAT	SQ YD	615	615					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1560	1560					
42400800	DETECTABLE WARNINGS	SQ FT	125	125					
44000100	PAVEMENT REMOVAL	SQ YD	640	640					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	62495	62495					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	20	20					
44000600	SIDEWALK REMOVAL	SQ FT	1560	1560					
44003100	MEDIAN REMOVAL	SQ FT	2705	2705					
44004250	PAVED SHOULDER REMOVAL	SQ YD	2800	2800					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	160	160					
44201773	CLASS D PATCHES, TYPE I, 11 INCH	SQ YD	25	25					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	795	795					
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	255	255					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	640	640					
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	1260	1260					
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	1005	1005					
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	125	125					
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SQ YD	620	620					
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SQ YD	495	495					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	240	240					

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USER NAME = Aya.Elkhathib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
IL ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	4
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				TYPE CODE					
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
				ROADWAY	ROADWAY	TRAFFIC SIGNALS			
				80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0005	0021			
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	565	565					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	10	10					
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	190	190					
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1					
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1					
60255500	MANHOLES TO BE ADJUSTED	EACH	5	5					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1					
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	210	210					
60920018	PIPE CULVERTS TO BE CLEANED 18"	FOOT	100		100				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	4300	4300					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	20	20					
67100100	MOBILIZATION	L SUM	1	1					
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1					
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					

SUMMARY OF QUANTITIES				TYPE CODE					
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
				ROADWAY	ROADWAY	TRAFFIC SIGNALS			
				80% FED 20% STATE	100% STATE	80% FED 20% STATE			
Code No.	Item	Unit	Total Quantity	0005	0005	0021			
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	25365	25365					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2850	2850					
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	1250	1250					
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	105450	105450					
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	5310	5310					
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	300	300					
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	4600	4600					
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	920	920					
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	6345	6345					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	625	625					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	52725	52725					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2655	2655					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	150	150					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2300	2300					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	460	460					
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	190	190					
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1120	1120					

* SPECIALTY ITEM

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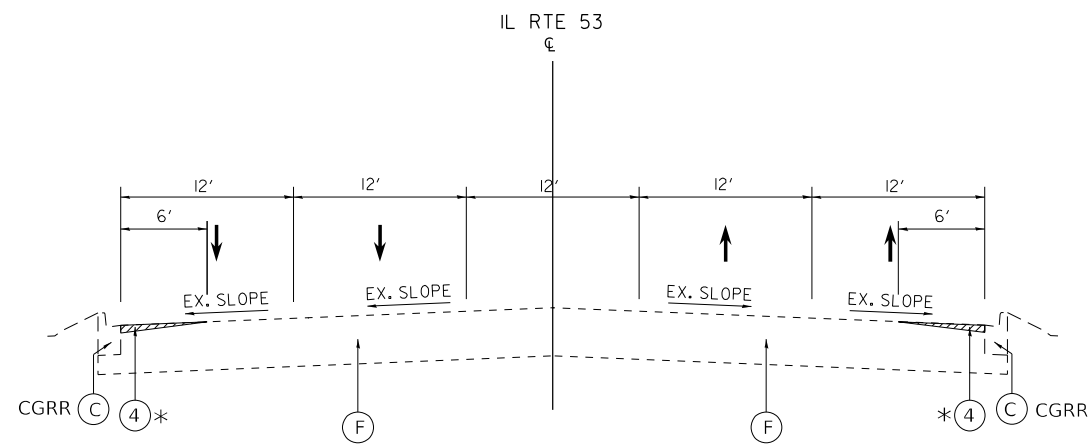
USER NAME = Aya.Eikhalib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

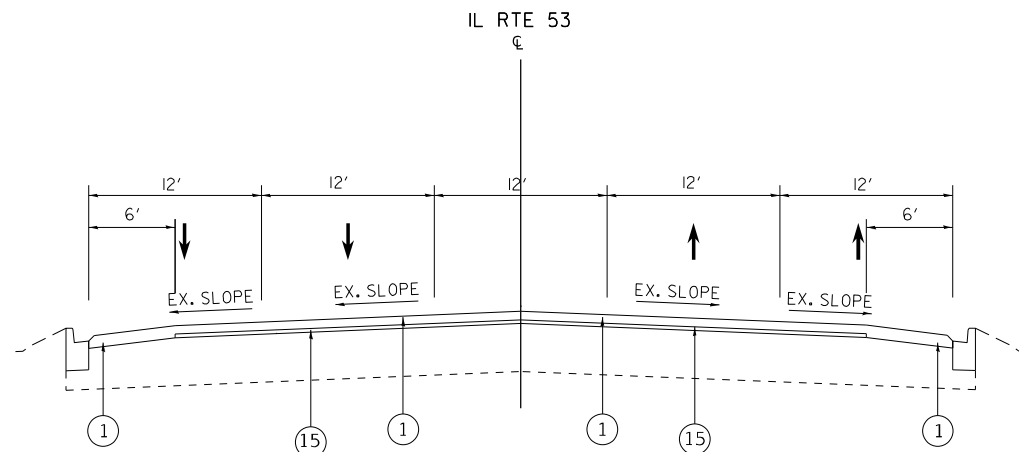
**SUMMARY OF QUANTITIES
IL ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	5
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.



EXISTING TYPICAL SECTION
STA 12+05 to STA 12+94



PROPOSED TYPICAL SECTION
STA 12+05 to STA 12+94

LEGEND - EXISTING

- (A) HMA PAVEMENT ±10.5"
- (B) HMA SHOULDERS ±6"
- (C) COMB. CONC. CURB & GUTTER
- (D) AGGREGATE SHOULDER
- (E) CORRUGATED MEDIAN
- (F) PCC PAVEMENT 10"
- (G) HMA SHOULDERS
- (H) HMA PAVEMENT 5"
- (I) 5/8" EXPANSION TIE ANCHORS - INSTALLED ALONG THE CENTERLINE OF THE EXISTING CORRUGATED MEDIAN AT 30" C-C EACH WAY
- (J) EXISTING SOD

LEGEND - PROPOSED

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1.75"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- (3) HOT-MIX ASPHALT BASE COURSE WIDENING, 8"
- (4) PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH (SEE DISTRICT DETAIL BD-33)
- (5) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (9) PAVEMENT REMOVAL
- (10) MEDIAN REMOVAL
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 6"
- (13) NO. 6 X 2' LONG DEFORMED TIE BARS, GROUTED-IN-PLACE (EPOXY COATED) AT 2' C-C (STANDARD 420001-08, INCLUDED IN THE COST OF COMBINATION CURB AND GUTTER, OF THE TYPE SPECIFIED)
- (14) SOD RESTORATION (PAID AS TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT)
- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"

CGRR CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)

* MILLING AT GUTTER FLAG IS 1.5"

** SAW CUTTING IS INCLUDED IN THE COST OF PAVEMENT REMOVAL

NOTES:

- (1) THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING
- (2) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLY. HMA BINDER COURSE, IL-4.75, N50 THROUGH RESURFACING LIMITS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS (%) @ NDES	
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70; 1.75"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"	3.5% @ 50 GYR.	QCP
PCC PAVEMENT OVERLAY		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70; 1.75"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 1"	3.5% @ 50 GYR.	QCP
DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"	4% @ 50 GYR.	QC/QA
HMA BASE COURSE (HMA BINDER IL-19.0); 6"	4% @ 50 GYR.	QC/QA
MEDIAN IMPROVEMENT AND PAVEMENT WIDENING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70; 1.75"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; 0.75"	3.5% @ 50 GYR.	QCP
HMA BASE COURSE WIDENING (HMA BINDER IL-19.0); 8"	4% @ 70 GYR.	QC/QA
PATCHING		
CLASS D PATCH (HMA BINDER IL-19.0)	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

WHEN SHOULDER RESURFACING OF 6 (SIX) FEET OR LESS IS ALLOWED TO BE PLACED SIMULTANEOUSLY WITH THE ADJACENT TRAFFIC LANE, THE MAINLINE QUALITY MANAGEMENT PROGRAM WILL BE ENFORCED FOR THE MAINLINE AND SHOULDER.

MODEL: Typical 1 (Sheet)
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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS			
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)			
SCALE:	SHEET 1 OF 4 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	7
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

LEGEND - EXISTING

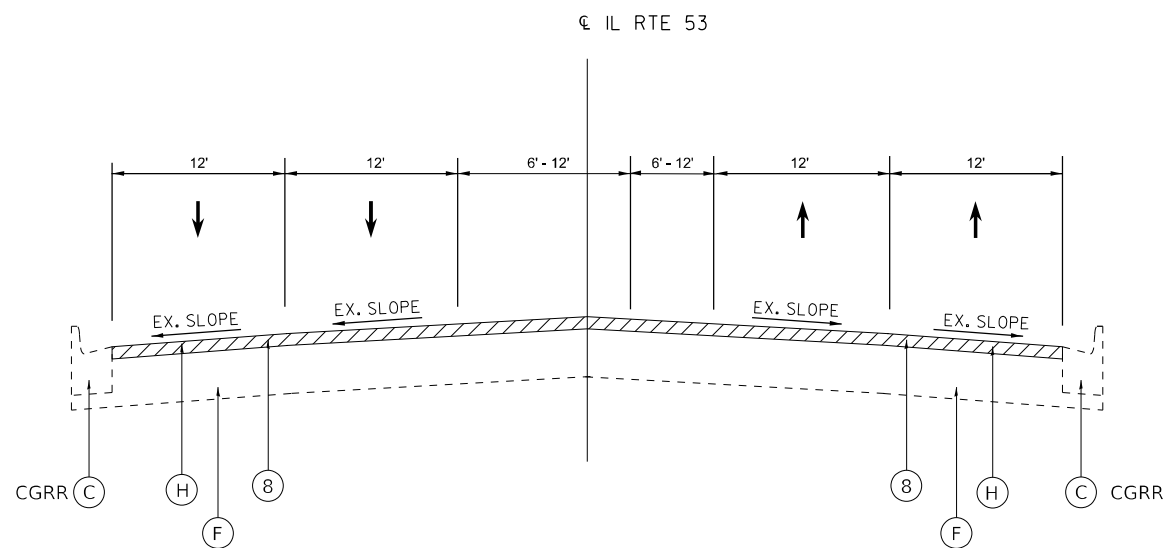
- (A) HMA PAVEMENT ±10.5"
- (B) HMA SHOULDERS ±6"
- (C) COMB. CONC. CURB & GUTTER
- (D) AGGREGATE SHOULDER
- (E) CORRUGATED MEDIAN
- (F) PCC PAVEMENT 10"
- (G) HMA SHOULDERS
- (H) HMA PAVEMENT 5"
- (I) 5/8" EXPANSION TIE ANCHORS - INSTALLED ALONG THE CENTERLINE OF THE EXISTING CORRUGATED MEDIAN AT 30" C-C EACH WAY
- (J) EXISTING SOD

LEGEND - PROPOSED

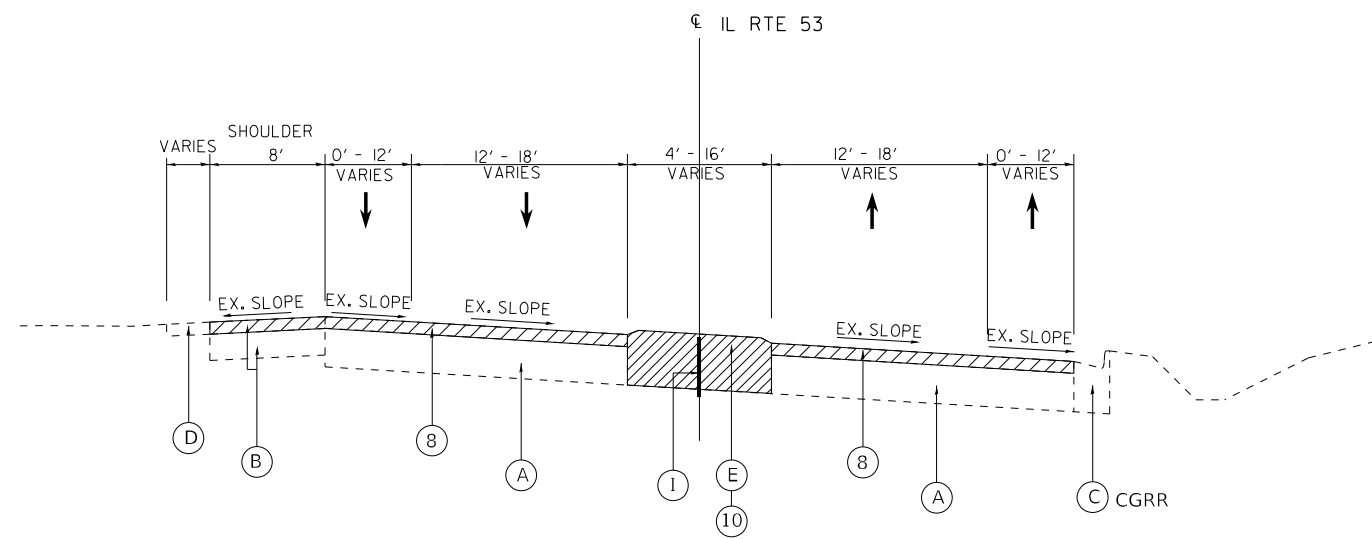
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1.75"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- (3) HOT-MIX ASPHALT BASE COURSE WIDENING, 8"
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- (5) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDERS, TYPE B

- (8) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
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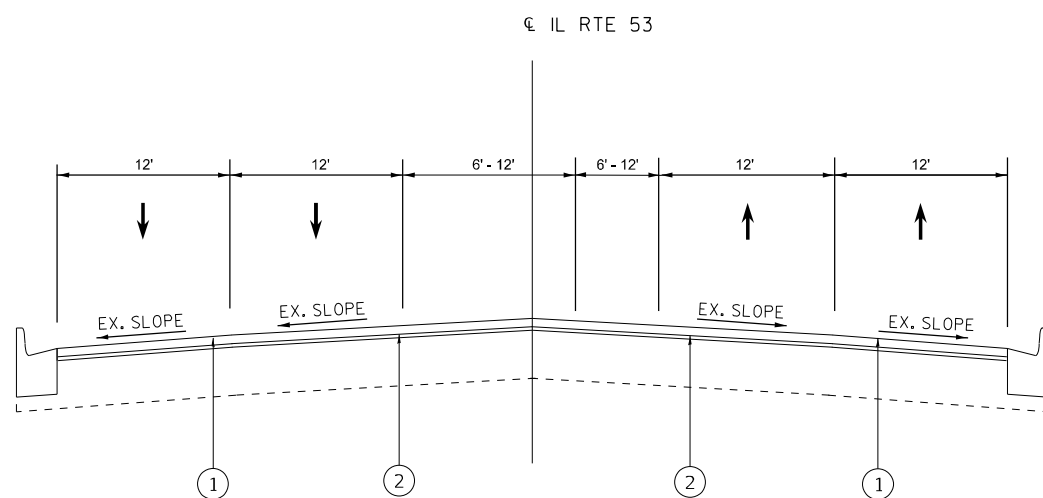
- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- CGRR CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)
- ** SAW CUTTING IS INCLUDED IN THE COST OF PAVEMENT REMOVAL



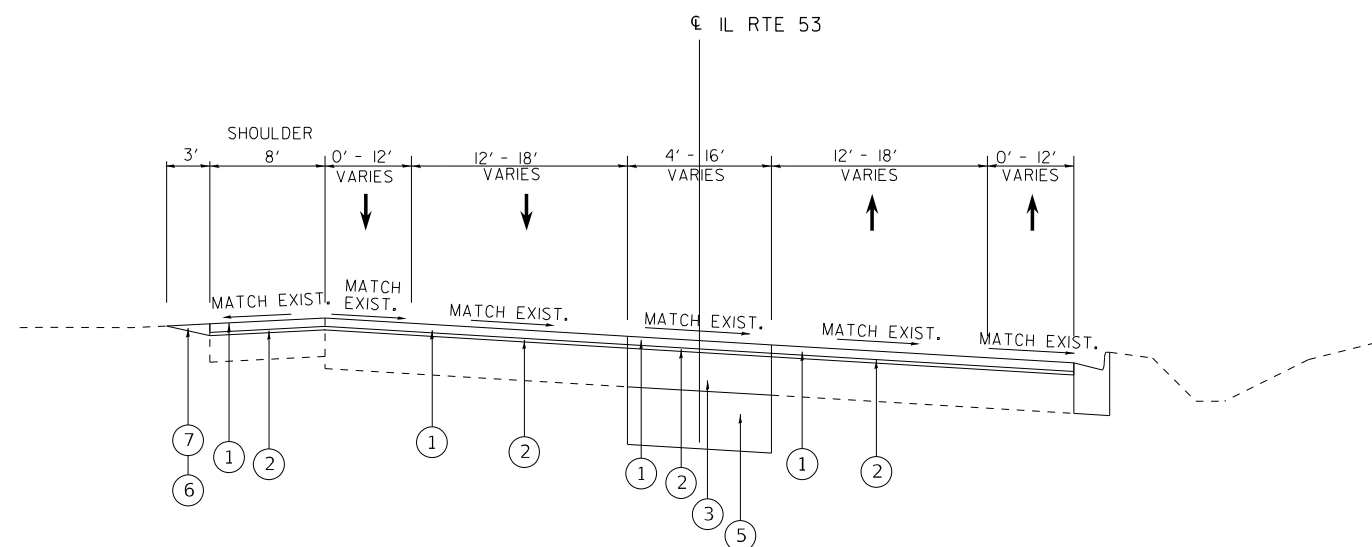
EXISTING TYPICAL SECTION
STA 12+94 TO STA 18+38



EXISTING TYPICAL SECTION
STA 18+38 TO STA 23+49



PROPOSED TYPICAL SECTION
STA 12+94 TO STA 18+38



PROPOSED TYPICAL SECTION
STA 18+38 TO STA 23+49

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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 12/2/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS	
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)	
SCALE:	SHEET 2 OF 4 SHEETS STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)		56	8
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

LEGEND - EXISTING

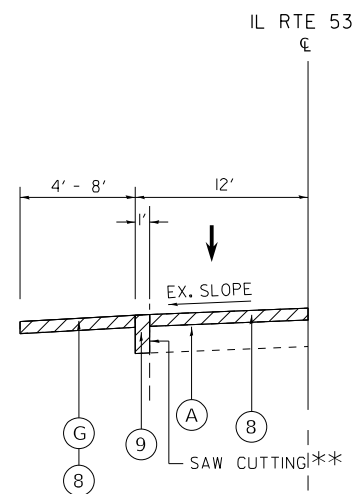
- (A) HMA PAVEMENT ±10.5"
- (B) HMA SHOULDERS ±6"
- (C) COMB. CONC. CURB & GUTTER
- (D) AGGREGATE SHOULDER
- (E) CORRUGATED MEDIAN
- (F) PCC PAVEMENT 10"
- (G) HMA SHOULDERS
- (H) HMA PAVEMENT 5"
- (I) 5/8" EXPANSION TIE ANCHORS - INSTALLED ALONG THE CENTERLINE OF THE EXISTING CORRUGATED MEDIAN AT 30" C-C EACH WAY
- (J) EXISTING SOD

LEGEND - PROPOSED

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1.75"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
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- (5) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDERS, TYPE B

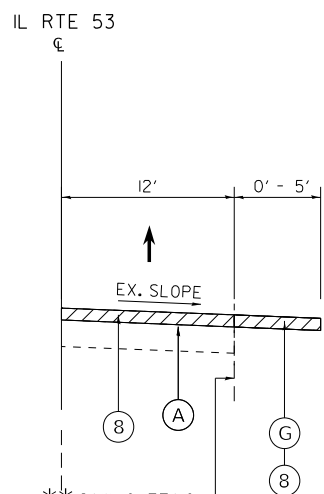
- (8) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (9) PAVEMENT REMOVAL
- (10) MEDIAN REMOVAL
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- (14) SOD RESTORATION (PAID AS TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT)

- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- CGRR CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)
- ** SAW CUTTING IS INCLUDED IN THE COST OF PAVEMENT REMOVAL



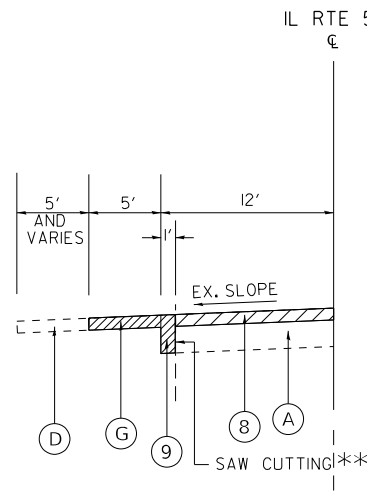
EXISTING TYPICAL SECTION

STA 23+49 TO STA 26+85
 STA 38+57 TO STA 42+19
 STA 56+20 TO STA 57+82
 STA 58+25 TO STA 65+74



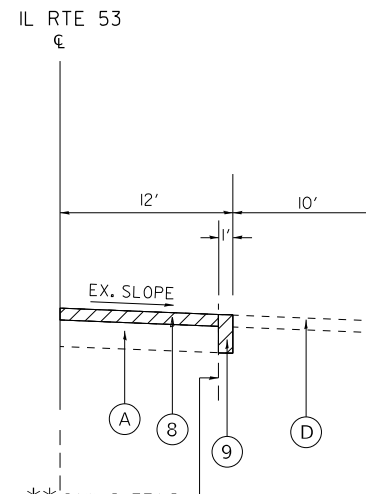
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STA 27+60 TO STA 29+72



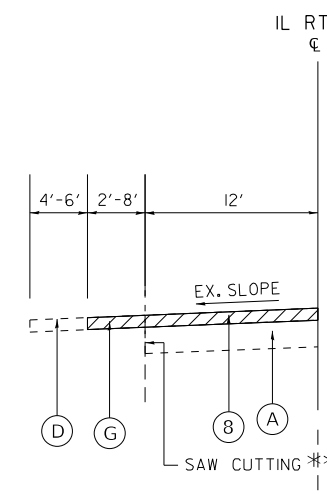
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STA 65+74 TO STA 84+29



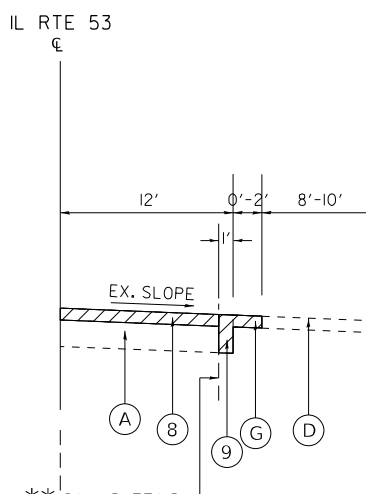
EXISTING TYPICAL SECTION

STA 38+57 TO STA 41+58
 STA 42+09 TO STA 44+00



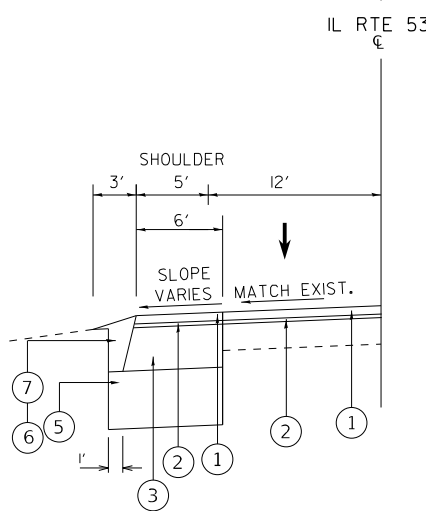
EXISTING TYPICAL SECTION

STA 91+45 TO STA 93+31



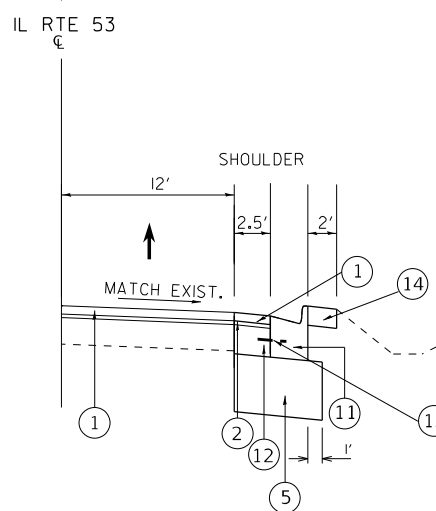
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STA 78+61 TO STA 87+30
 STA 88+25 TO STA 89+00
 STA 89+60 TO STA 92+67



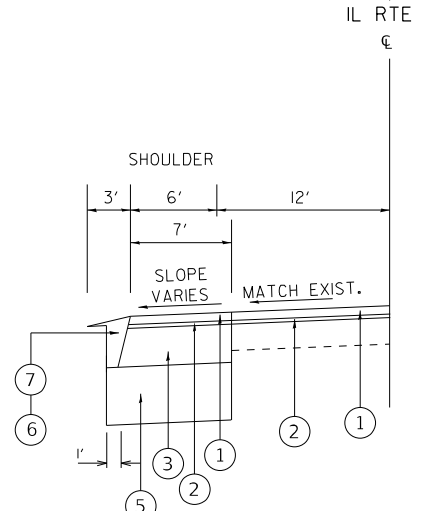
PROPOSED TYPICAL SECTION

STA 23+49 TO STA 26+85
 STA 38+57 TO STA 42+19
 STA 56+20 TO STA 57+82
 STA 58+25 TO STA 65+74



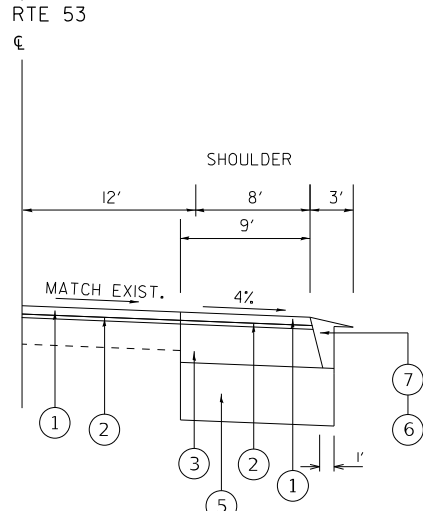
PROPOSED TYPICAL SECTION

STA 27+60 TO STA 29+72



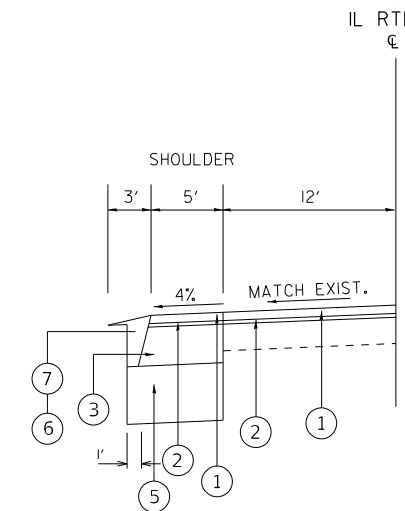
PROPOSED TYPICAL SECTION

STA 65+74 TO STA 84+29



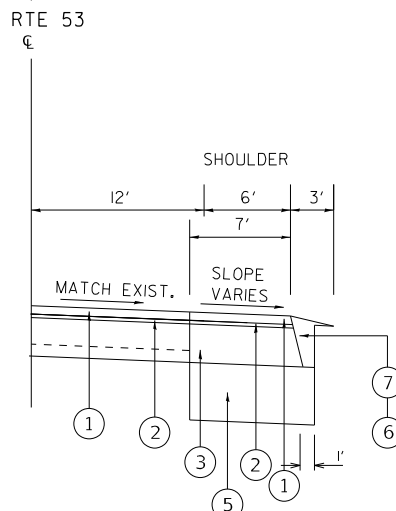
PROPOSED TYPICAL SECTION

STA 38+57 TO STA 41+58
 STA 42+09 TO STA 44+00



PROPOSED TYPICAL SECTION

STA 91+45 TO STA 93+31



PROPOSED TYPICAL SECTION

STA 78+61 TO STA 87+30
 STA 88+25 TO STA 89+00
 STA 89+60 TO STA 92+67

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USER NAME = Aya,Elkhalib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED TYPICAL SECTIONS			
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)			
SCALE:	SHEET 3	OF 4 SHEETS	STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	9
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

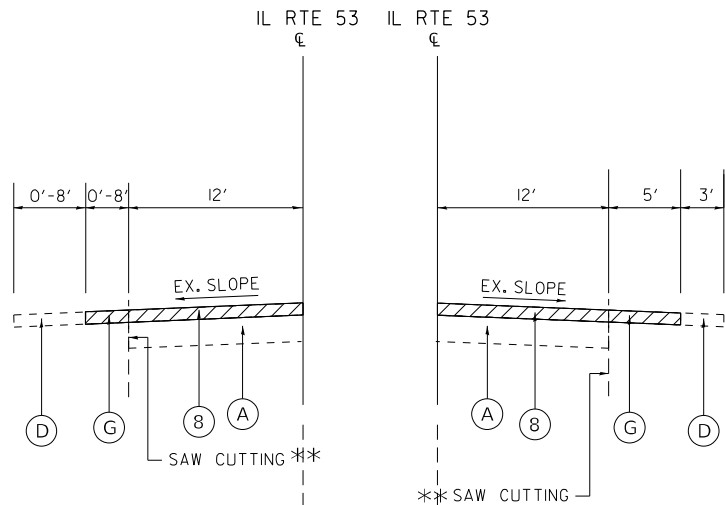
LEGEND - EXISTING

- (A) HMA PAVEMENT ±10.5"
- (B) HMA SHOULDERS ±6"
- (C) COMB. CONC. CURB & GUTTER
- (D) AGGREGATE SHOULDER
- (E) CORRUGATED MEDIAN
- (F) PCC PAVEMENT 10"
- (G) HMA SHOULDERS
- (H) HMA PAVEMENT 5"
- (I) 5/8" EXPANSION TIE ANCHORS - INSTALLED ALONG THE CENTERLINE OF THE EXISTING CORRUGATED MEDIAN AT 30" C-C EACH WAY
- (J) EXISTING SOD

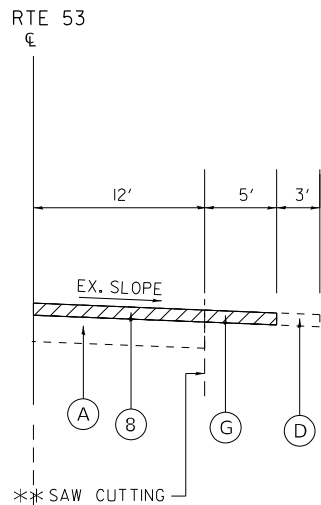
LEGEND - PROPOSED

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1.75"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- (3) HOT-MIX ASPHALT BASE COURSE WIDENING, 8"
- (4) PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH (SEE DISTRICT DETAIL BD-33)
- (5) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (9) PAVEMENT REMOVAL
- (10) MEDIAN REMOVAL
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 6"
- (13) NO. 6 X 2' LONG DEFORMED TIE BARS, GROUTED-IN-PLACE (EPOXY COATED) AT 2' C-C (STANDARD 420001-08, INCLUDED IN THE COST OF COMBINATION CURB AND GUTTER, OF THE TYPE SPECIFIED)
- (14) SOD RESTORATION (PAID AS TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT)
- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"

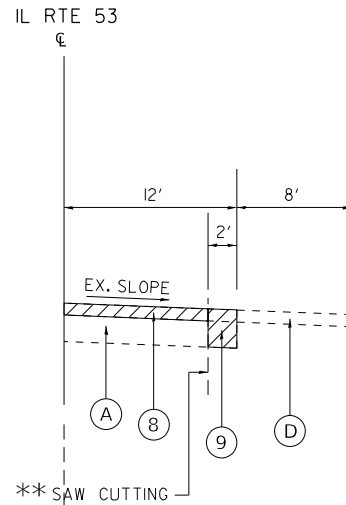
- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- CGRR CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)
- ** SAW CUTTING IS INCLUDED IN THE COST OF PAVEMENT REMOVAL



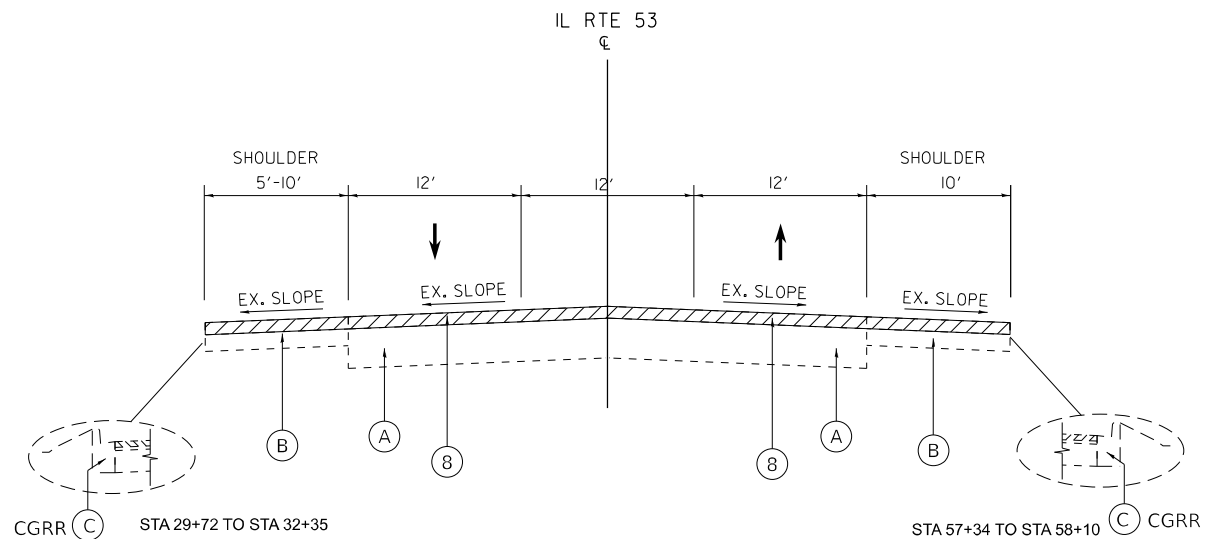
EXISTING TYPICAL SECTION
STA 86+87 TO STA 91+45
STA 93+76 TO STA 95+09



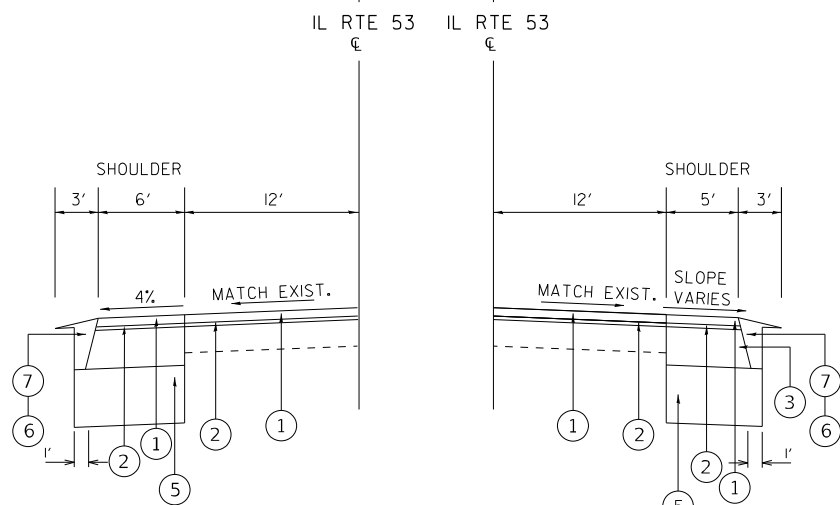
EXISTING TYPICAL SECTION
STA 62+15 TO STA 73+23
STA 73+72 TO STA 77+83



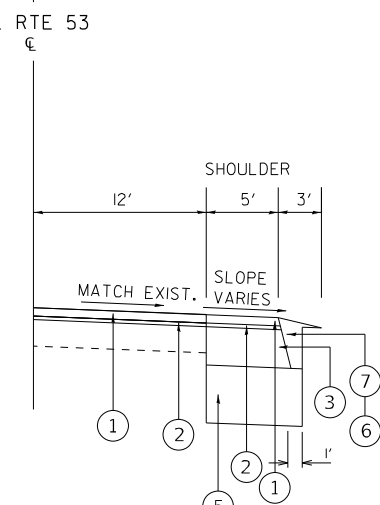
EXISTING TYPICAL SECTION
STA 58+87 TO STA 59+77
STA 59+92 TO STA 61+49



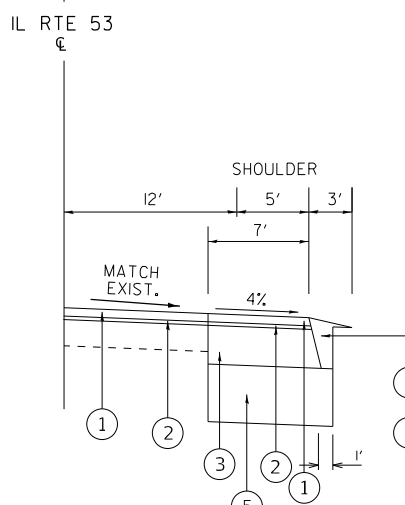
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STA 29+72 TO STA 38+57
STA 44+00 TO STA 55+70
OMMISSION: STA 49+22 TO STA 50+81



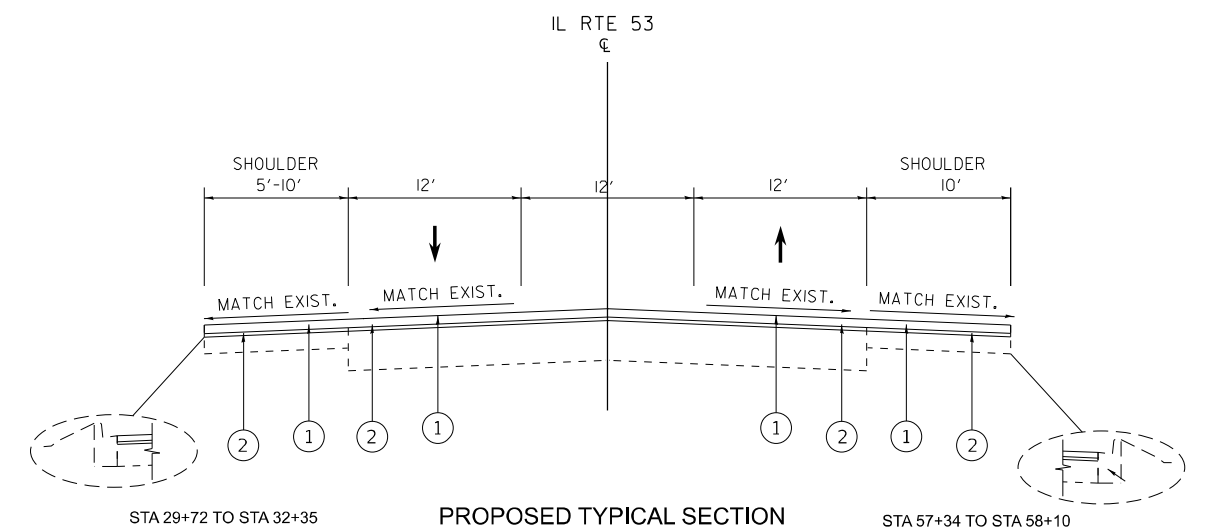
PROPOSED TYPICAL SECTION
STA 86+87 TO STA 91+45
STA 93+76 TO STA 95+09



PROPOSED TYPICAL SECTION
STA 62+15 TO STA 73+23
STA 73+72 TO STA 77+83



PROPOSED TYPICAL SECTION
STA 58+87 TO STA 59+77
STA 59+92 TO STA 61+49



PROPOSED TYPICAL SECTION
STA 29+72 TO STA 38+57
STA 44+00 TO STA 55+70
OMMISSION: STA 49+22 TO STA 50+81

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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED TYPICAL SECTIONS			
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)			
SCALE:	SHEET 4	OF 4 SHEETS	STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	10
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

LEGEND - EXISTING

- (A) HMA PAVEMENT ±10.5"
- (B) HMA SHOULDERS ±6"
- (C) COMB. CONC. CURB & GUTTER
- (D) AGGREGATE SHOULDER
- (E) CORRUGATED MEDIAN
- (F) PCC PAVEMENT 10"
- (G) HMA SHOULDERS
- (H) HMA PAVEMENT 5"

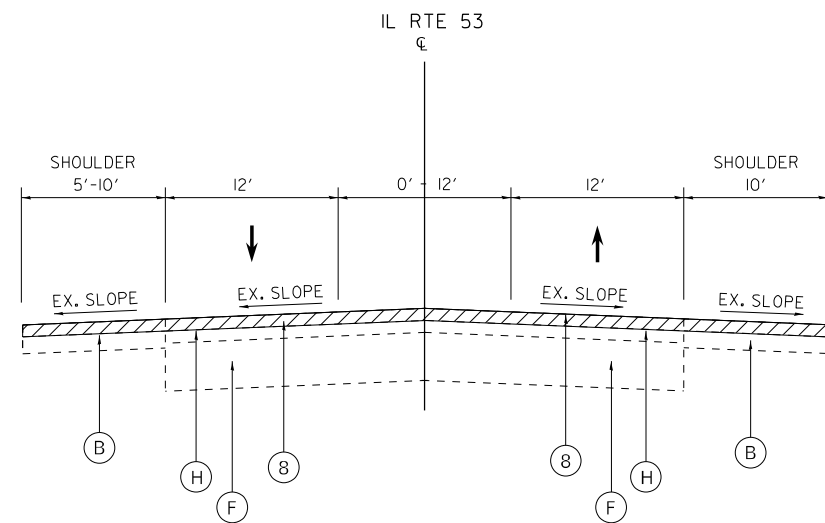
- (I) 5/8" EXPANSION TIE ANCHORS - INSTALLED ALONG THE CENTERLINE OF THE EXISTING CORRUGATED MEDIAN AT 30" C-C EACH WAY
- (J) EXISTING SOD

LEGEND - PROPOSED

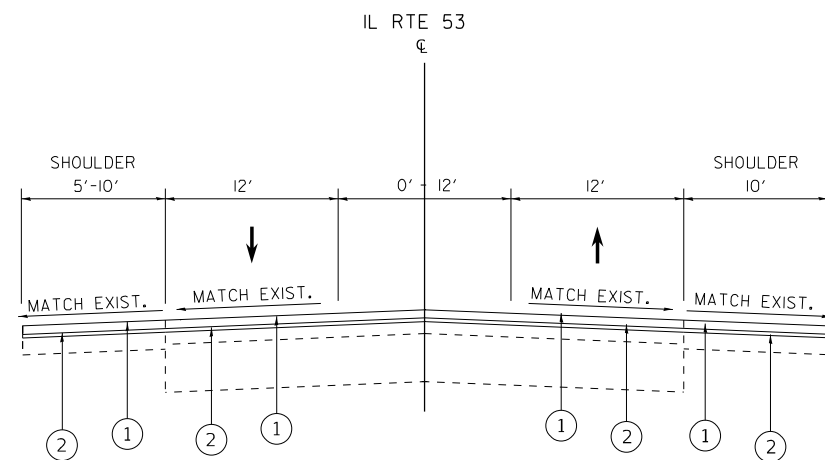
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70, 1.75"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 0.75"
- (3) HOT-MIX ASPHALT BASE COURSE WIDENING, 8"
- (4) PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH (SEE DISTRICT DETAIL BD-33)
- (5) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDERS, TYPE B

- (8) HOT-MIX ASPHALT SURFACE REMOVAL, 2.5"
- (9) PAVEMENT REMOVAL
- (10) MEDIAN REMOVAL
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 6"
- (13) NO. 6 X 2' LONG DEFORMED TIE BARS, GROUTED-IN-PLACE (EPOXY COATED) AT 2' C-C (STANDARD 420001-08, INCLUDED IN THE COST OF COMBINATION CURB AND GUTTER, OF THE TYPE SPECIFIED)
- (14) SOD RESTORATION (PAID AS TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT)

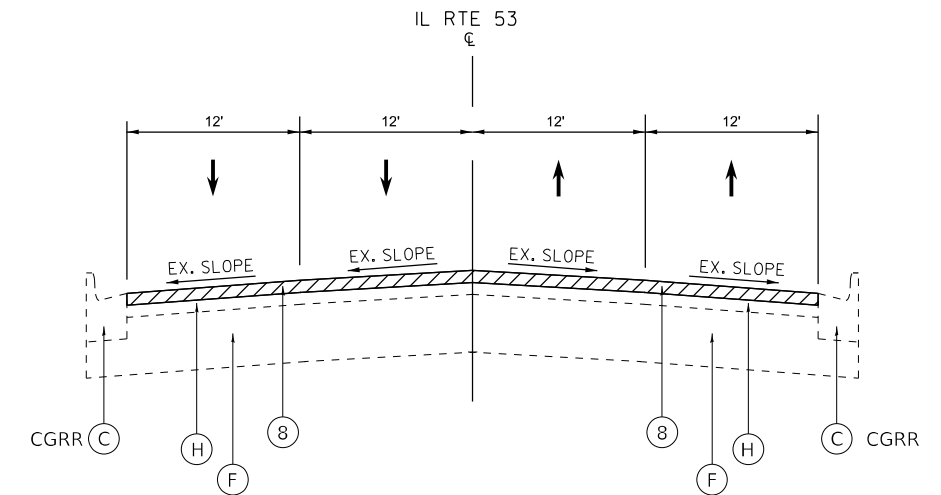
- (15) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- CGRR CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATION AS DIRECTED BY THE ENGINEER)
- ** SAW CUTTING IS INCLUDED IN THE COST OF PAVEMENT REMOVAL



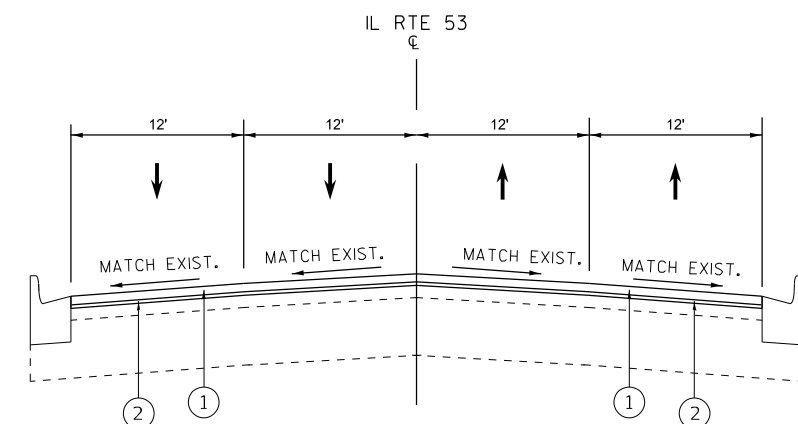
EXISTING TYPICAL SECTION
STA 95+09 TO STA 120+00



PROPOSED TYPICAL SECTION
STA 95+09 TO STA 120+00



EXISTING TYPICAL SECTION
STA 120+00 TO STA 144+00



PROPOSED TYPICAL SECTION
STA 120+00 TO STA 144+00

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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS	
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)	
SCALE:	SHEET Untitled-8 (Sheet) SHEETS
STA. 0+00.00	TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	10A
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

SCHEDULE OF QUANTITIES - ADA IMPROVEMENT

STATE ROUTE	CROSS STREET	CORNER	20200100	21101615	25200110	25200200	42001300	42400200	42400800	44000600	85000200	89502376	X1400450	X4400501	X4400503	
			EARTH EXCAVATION	TOPSOIL FURNISH AND PLACE, 4"	SODDING, SALT TOLERANT	SUPPLEMENTAL WATERING	PROTECTIVE COAT	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	DETECTABLE WARNINGS	SIDEWALK REMOVAL	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	REBUILD EXISTING HANDHOLE	REBUILD EXISTING HEAVY-DUTY HANDHOLE	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	
			CU YD	SQ YD	SQ YD	UNIT	SQ YD	SQ FT	SQ FT	SQ FT	EACH	EACH	EACH	FOOT	FOOT	
IL 53 (COLUMBINE AVE)	BAKER HILL DR	NE	2.25	8.0	8.0	0.08	38.3	225.0	30.0	225.0	1	0.0		20.0	20.0	
		SE	2.25	14.0	14.0	0.14	33.0	225.0	15.0	225.0		1.0	12.0	12.0		
		NW	2.25	9.0	9.0	0.09	53.0	225.0	15.0	225.0		0.0	42.0	42.0		
	SPRING AVE	NW	2.00	15.0	15.0	0.15	28.9	200.0	10.0	200.0	1	0.0		10.0	10.0	
		SW	2.63	10.0	10.0	0.10	42.6	263.1	34.6	263.1		0.0	20.0	20.0		
	HARRISON RD	NE	2.10	11.0	11.0	0.11	28.7	210.0	10.0	210.0	0	0.0		8.0	8.0	
		SE	2.10	12.0	12.0	0.12	28.7	210.0	10.0	210.0		0.0	8.0	8.0		
	MADISON ST	NE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1	0.0	2.0	0.0	0.0	
		SE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		0.00	0.00	0.00		
	TOTAL			20	79	79	0.80	253	1560	125	1560	3	1	2	120	120

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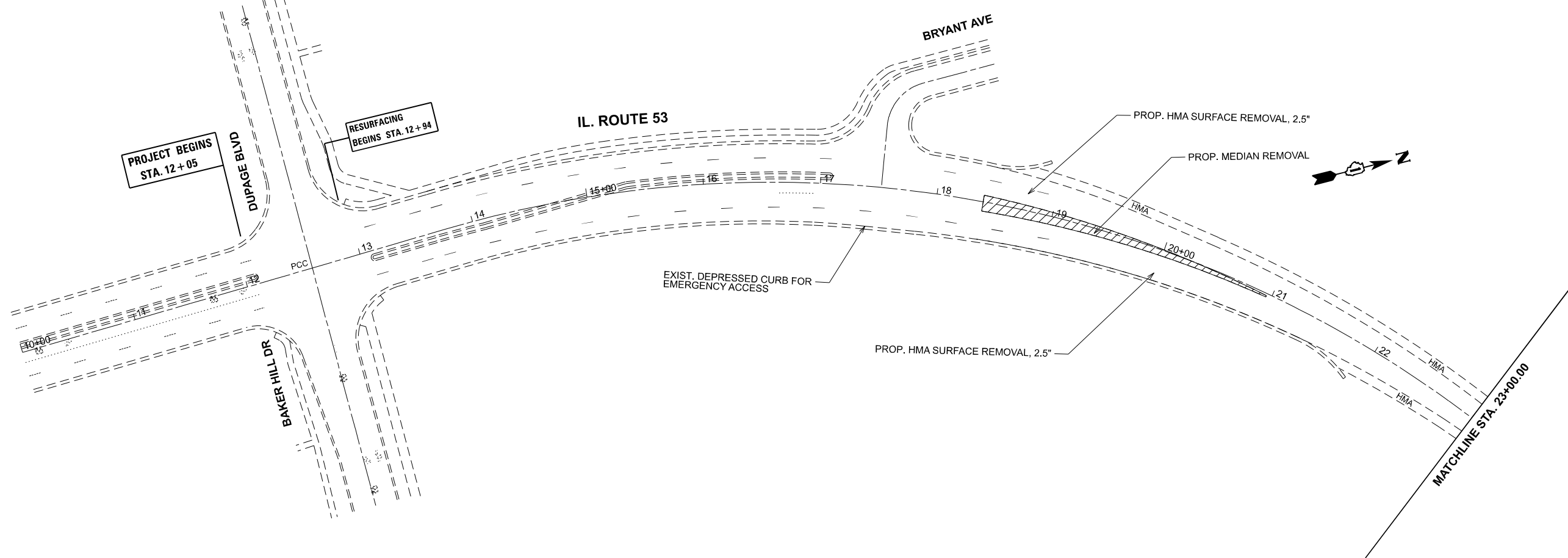
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES - ADA IMPROVEMENT
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	58	11
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

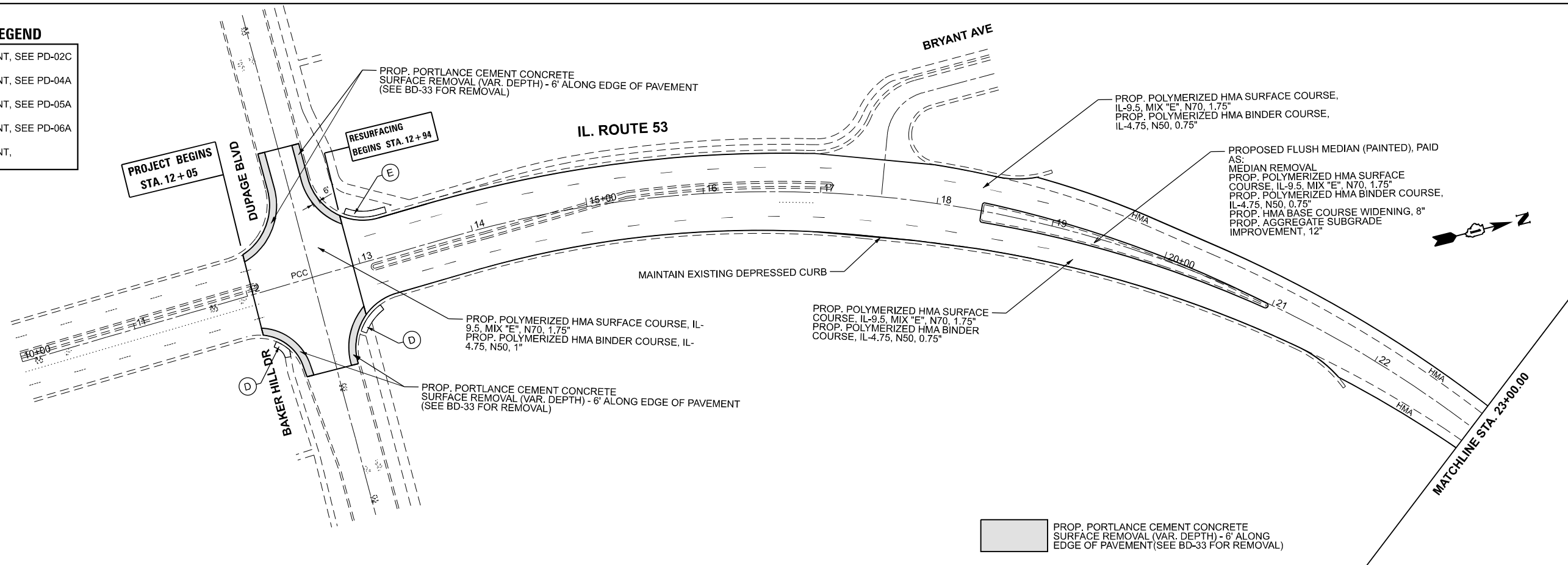
SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.



EXISTING

ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
- (E) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN



PROPOSED

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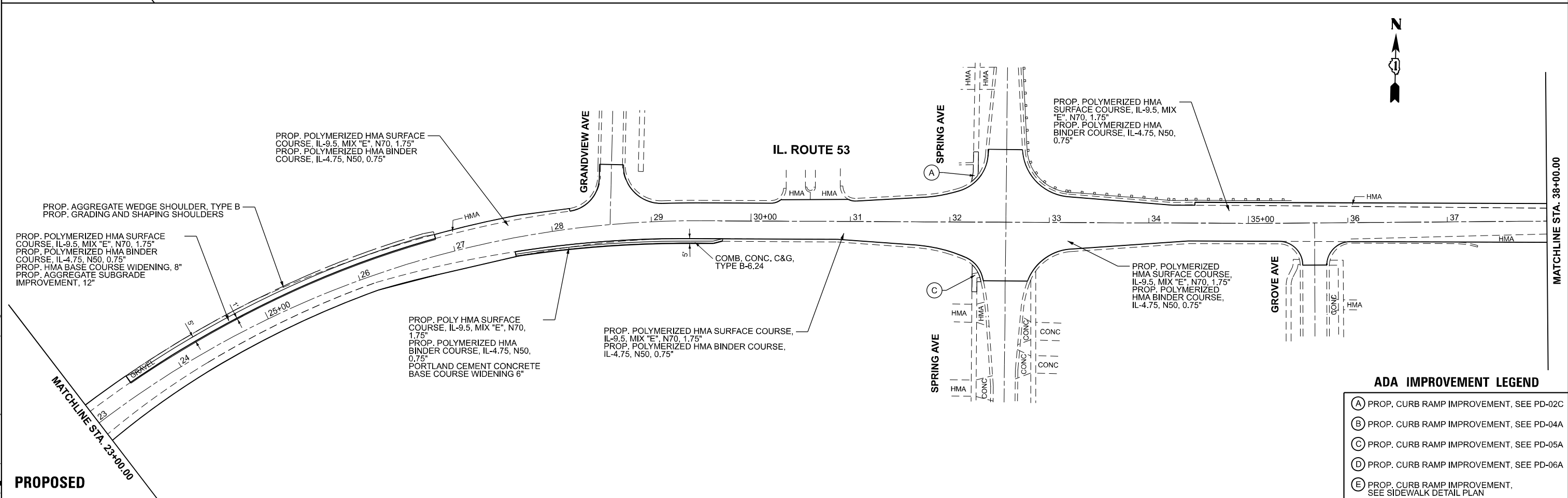
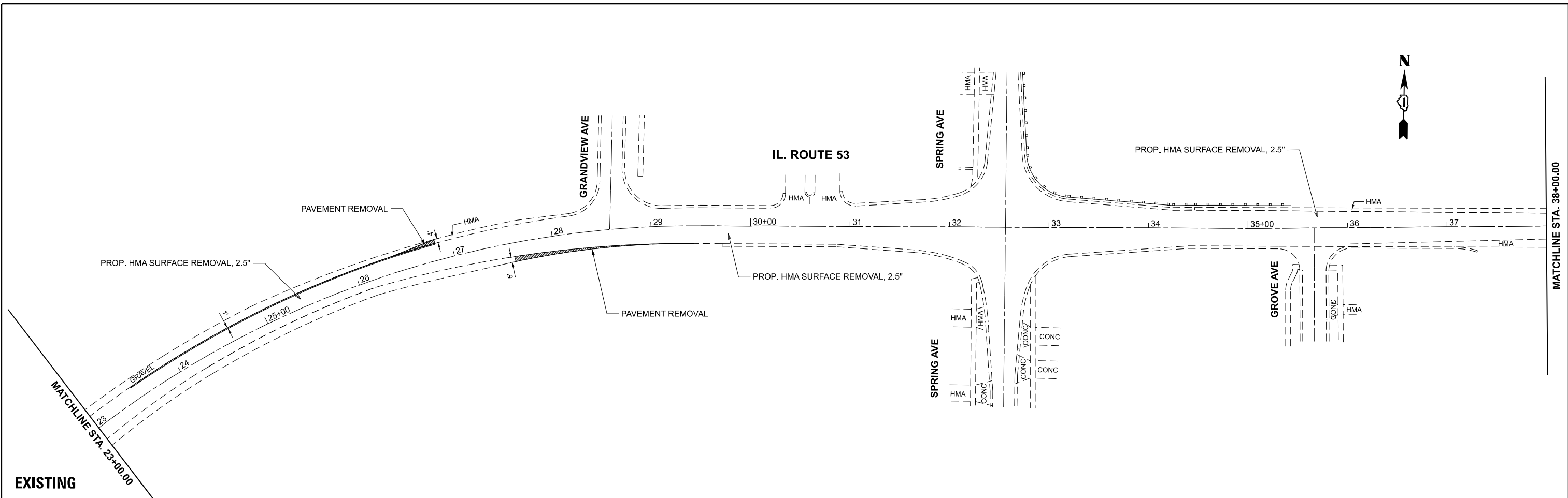
USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 1 OF 10 SHEETS STA. 8+00.00 TO STA. 23+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	12
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
- (E) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

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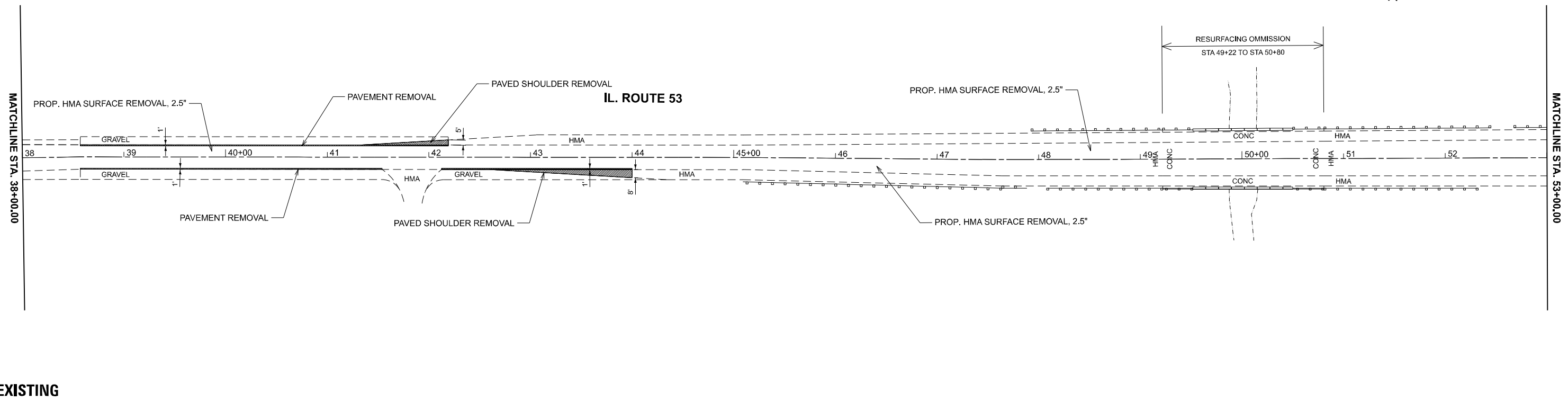
USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

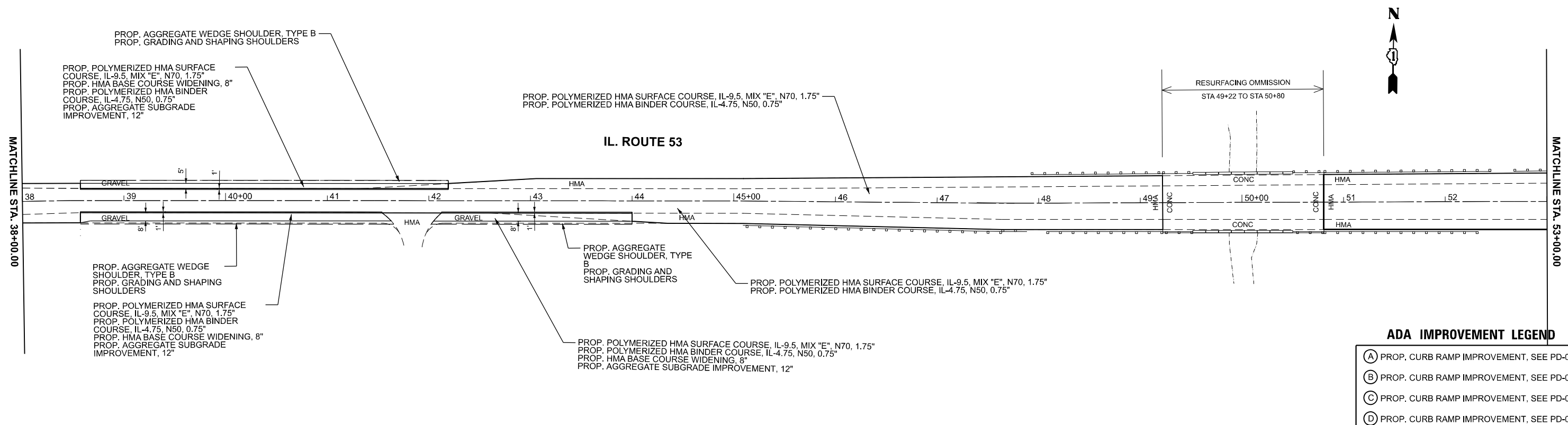
EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)

SCALE: 1"=50' SHEET 2 OF 10 SHEETS STA. 23+00.00 TO STA. 38+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	13
				CONTRACT NO. 80B25
ILLINOIS FED. AID PROJECT				



EXISTING



PROPOSED



ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
- (E) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

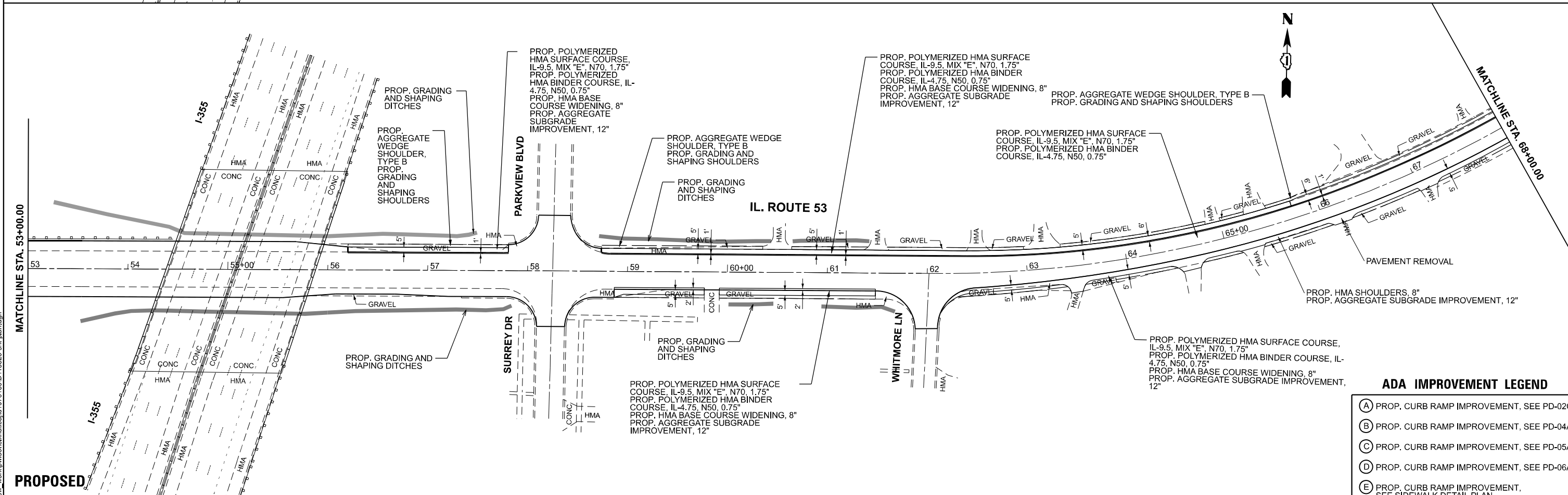
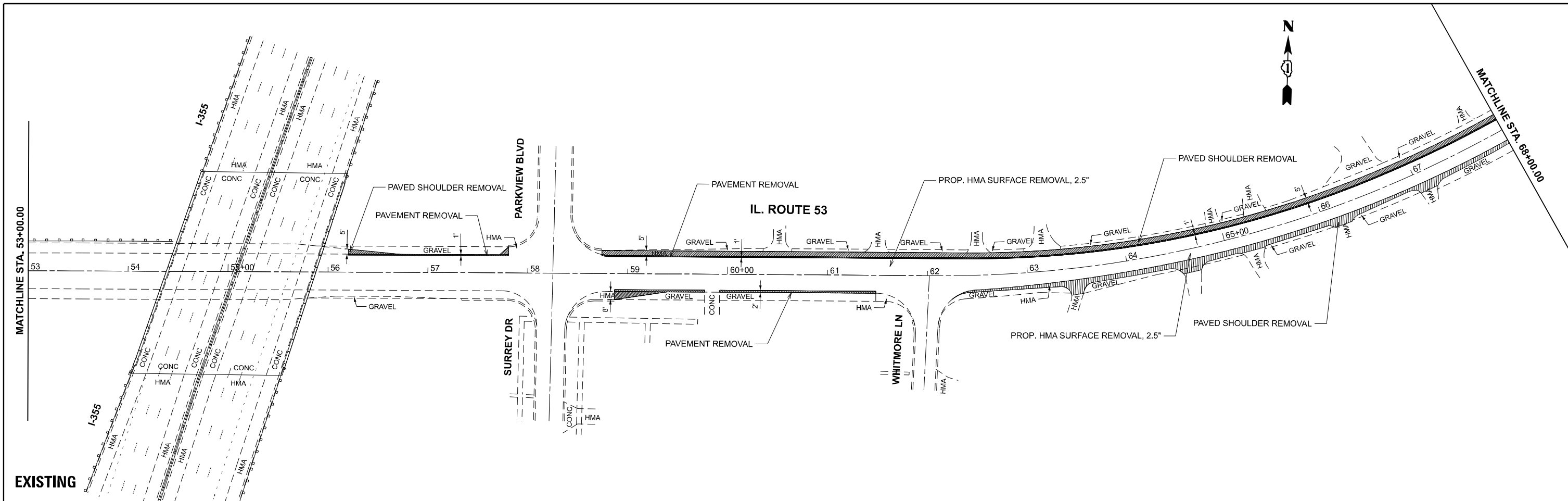
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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE	
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)	
SCALE: 1"=50'	SHEET 3 OF 10 SHEETS
STA. 38+00.00	TO STA. 53+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	14
CONTRACT NO. 80B25			ILLINOIS FED. AID PROJECT	



ADA IMPROVEMENT LEGEND

(A)	PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
(B)	PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
(C)	PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
(D)	PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
(E)	PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

MODEL: IL53_RoadwayPlan4
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USER NAME	= Aya.Elkhalti	DESIGNED	-	REVISED	-
DRAWN	-	REVISED	-	REVISED	-
CHECKED	-	REVISED	-	REVISED	-
PLOT DATE	= 12/2/2025	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

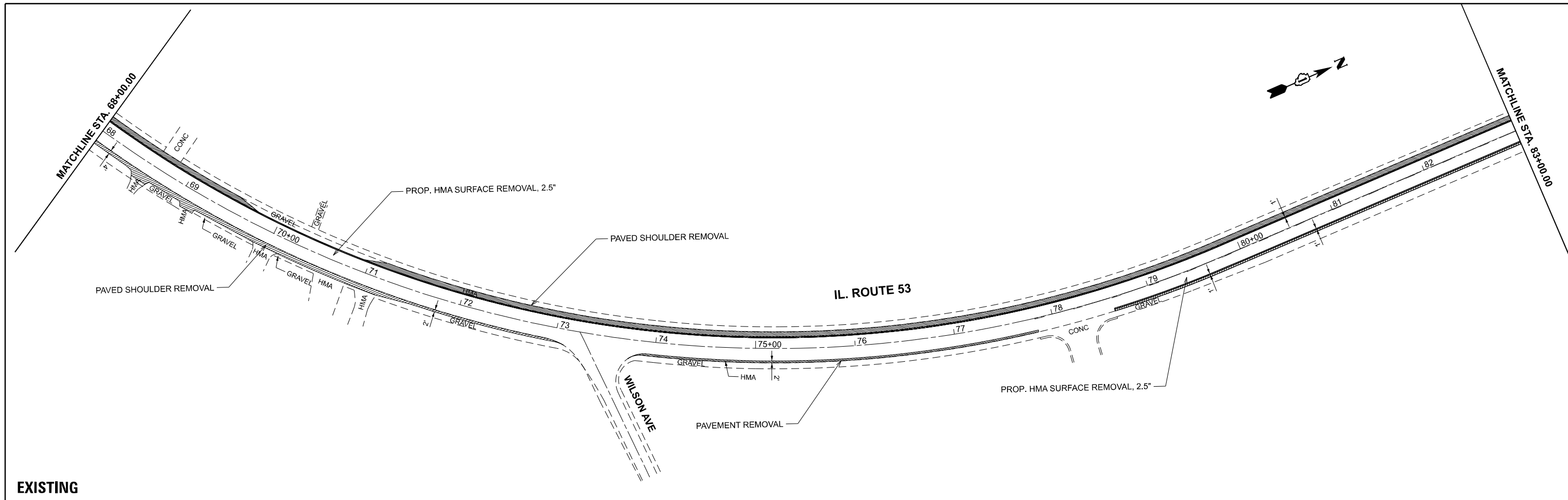
EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)

SCALE: 1"=50' SHEET 4 OF 10 SHEETS STA. 53+00.00 TO STA. 68+00.00

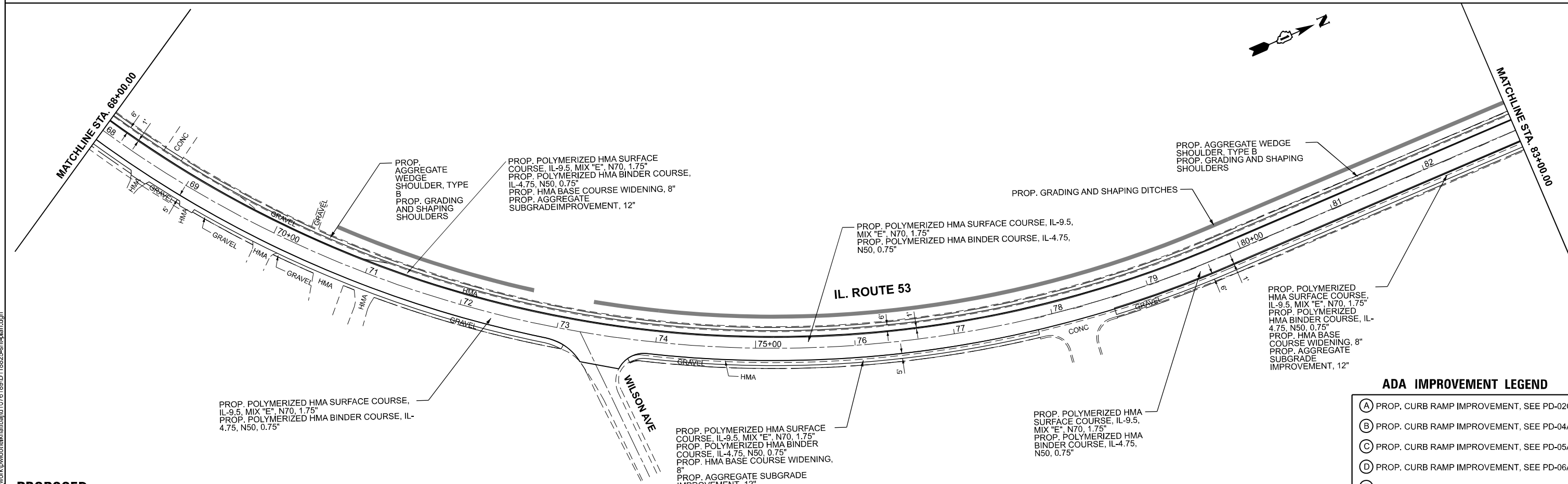
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	15

CONTRACT NO. 80B25

ILLINOIS FED. AID PROJECT



EXISTING



PROPOSED

ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
- (E) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

MODEL: IL53_RoadwayPlans5
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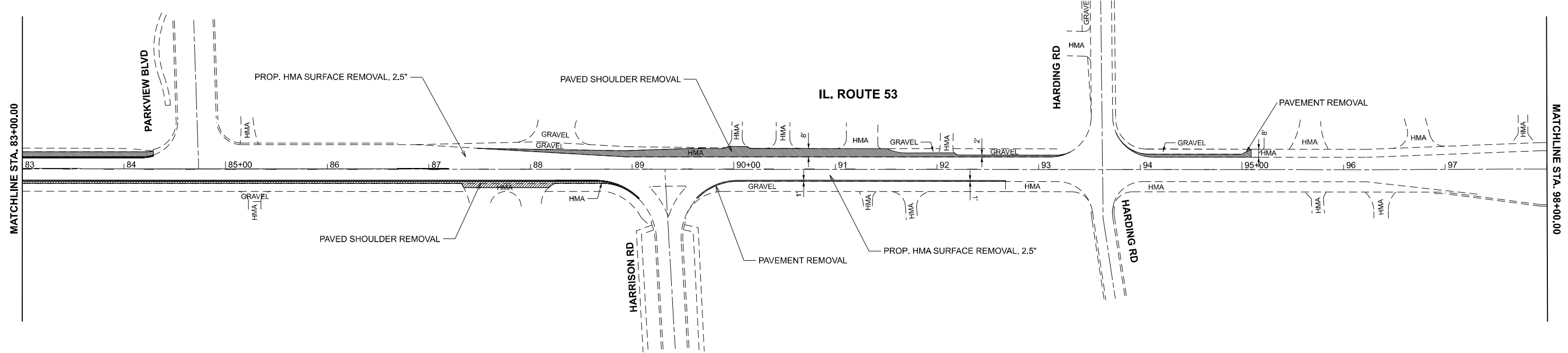
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

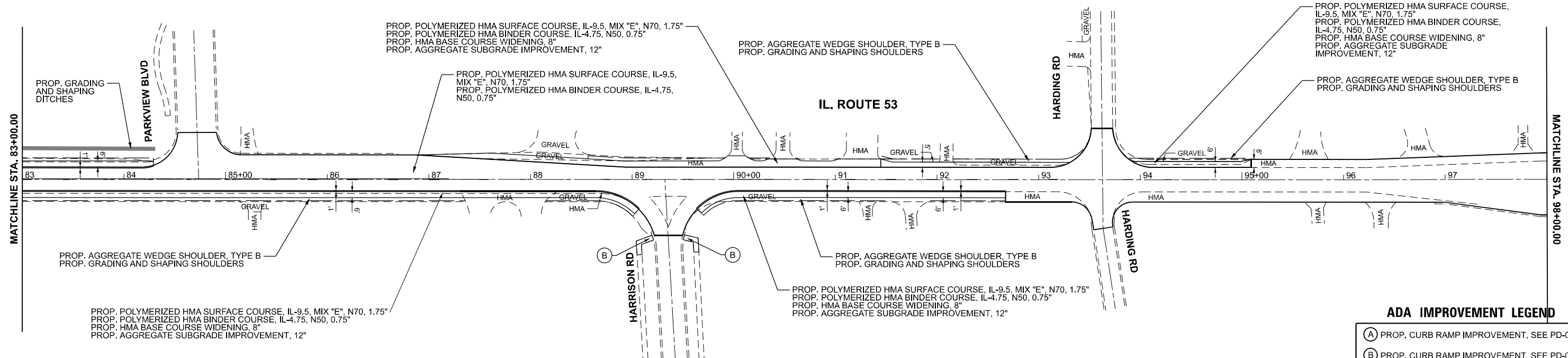
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 5 OF 10 SHEETS STA. 68+00.00 TO STA. 83+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	16
CONTRACT NO. 80B25			ILLINOIS FED. AID PROJECT	



EXISTING



PROPOSED

ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A
- (E) PROP. CURB RAMP IMPROVEMENT, SEE SIDEWALK DETAIL PLAN

MODEL: IL53_RoadwayPlan6
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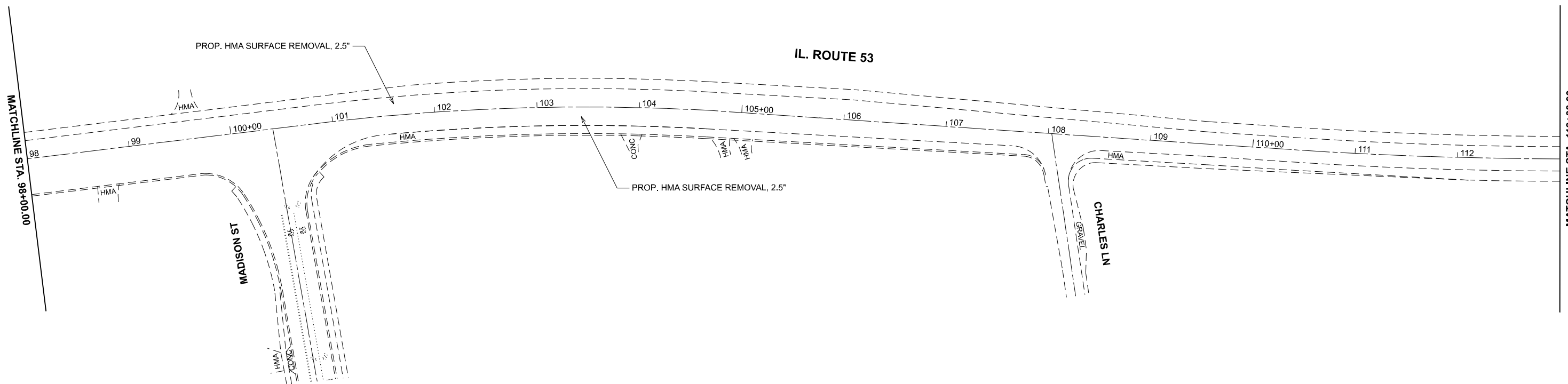
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	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

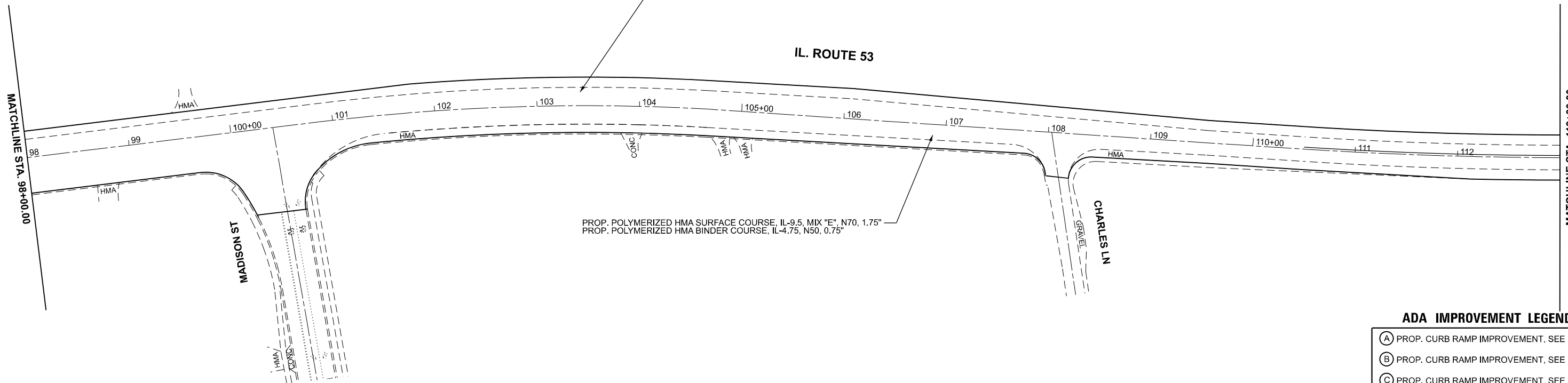
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 6 OF 10 SHEETS STA. 83+00.00 TO STA. 98+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	17
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



EXISTING



PROPOSED

ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A

MODEL: IL53_RoadPlan7
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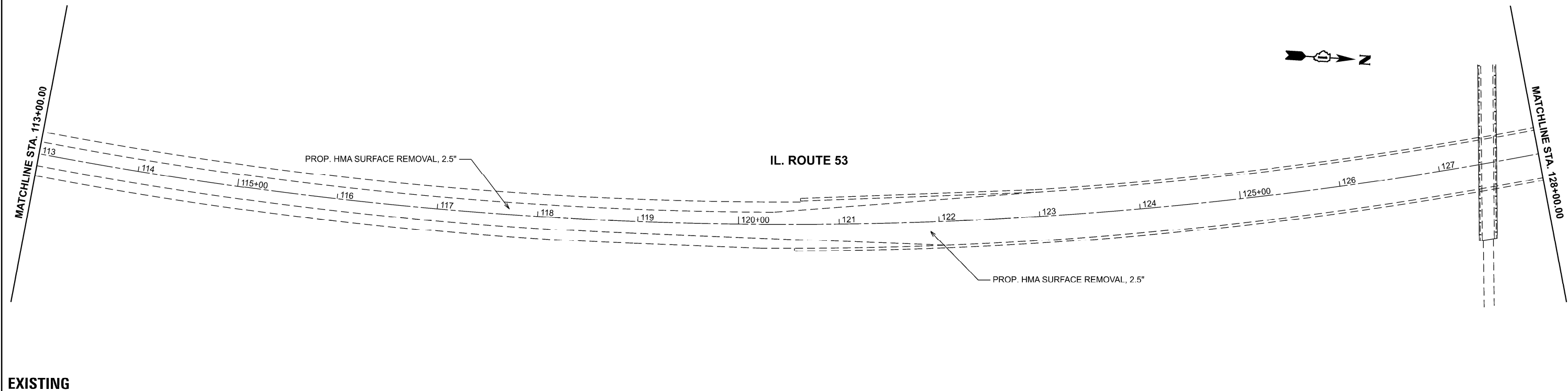
USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

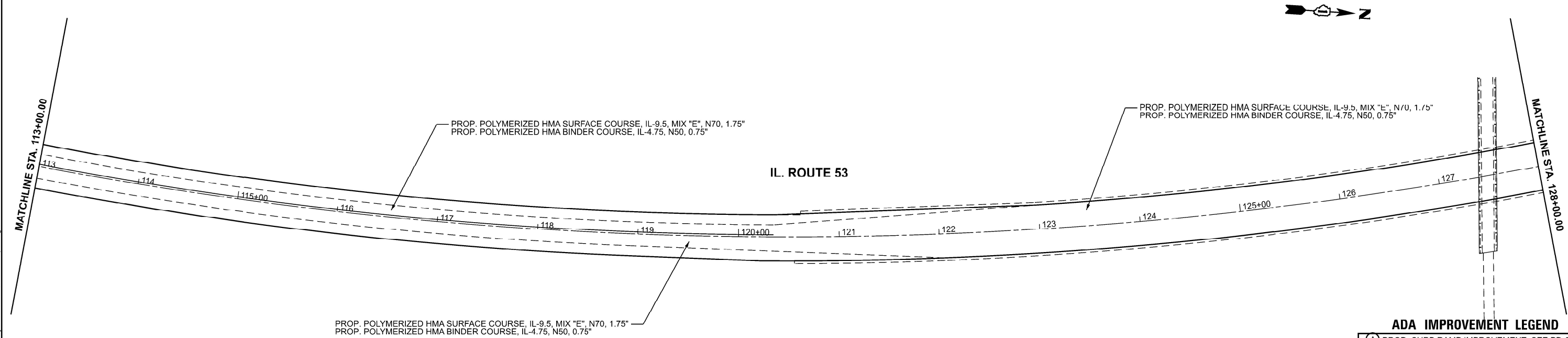
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL, ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 7 OF 10 SHEETS STA. 98+00.00 TO STA. 113+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	18
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



EXISTING



PROPOSED



- ADA IMPROVEMENT LEGEND**
- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
 - (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
 - (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
 - (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A

MODEL: IL53_RoadwayPlan8
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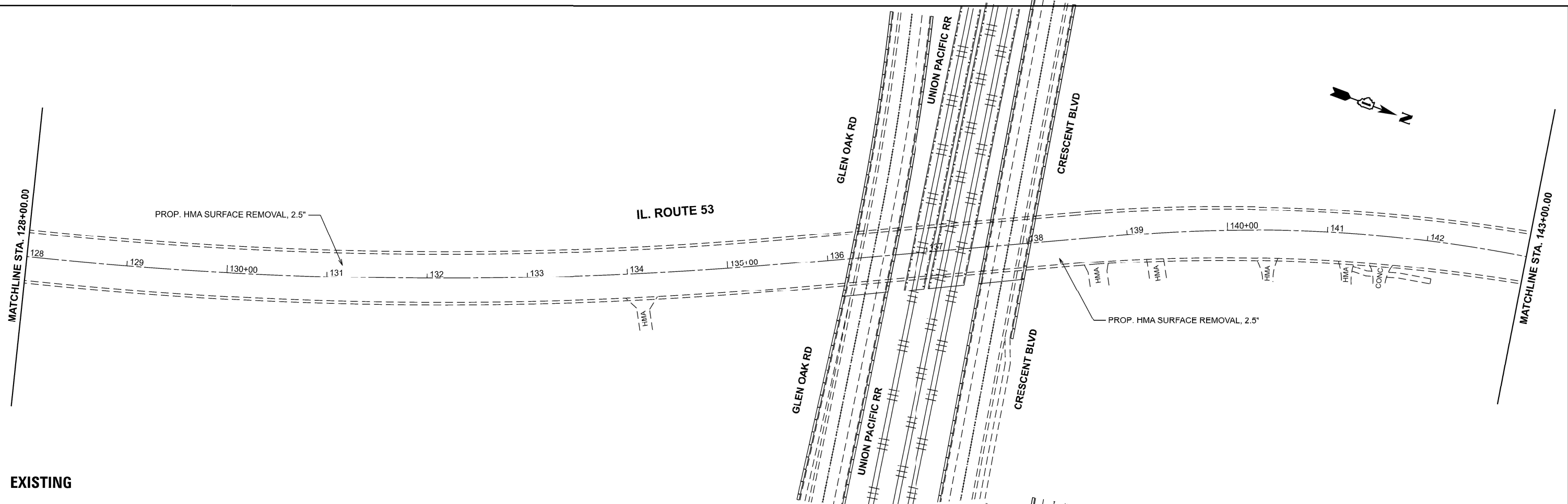
USER NAME = Aya.Eikhalib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

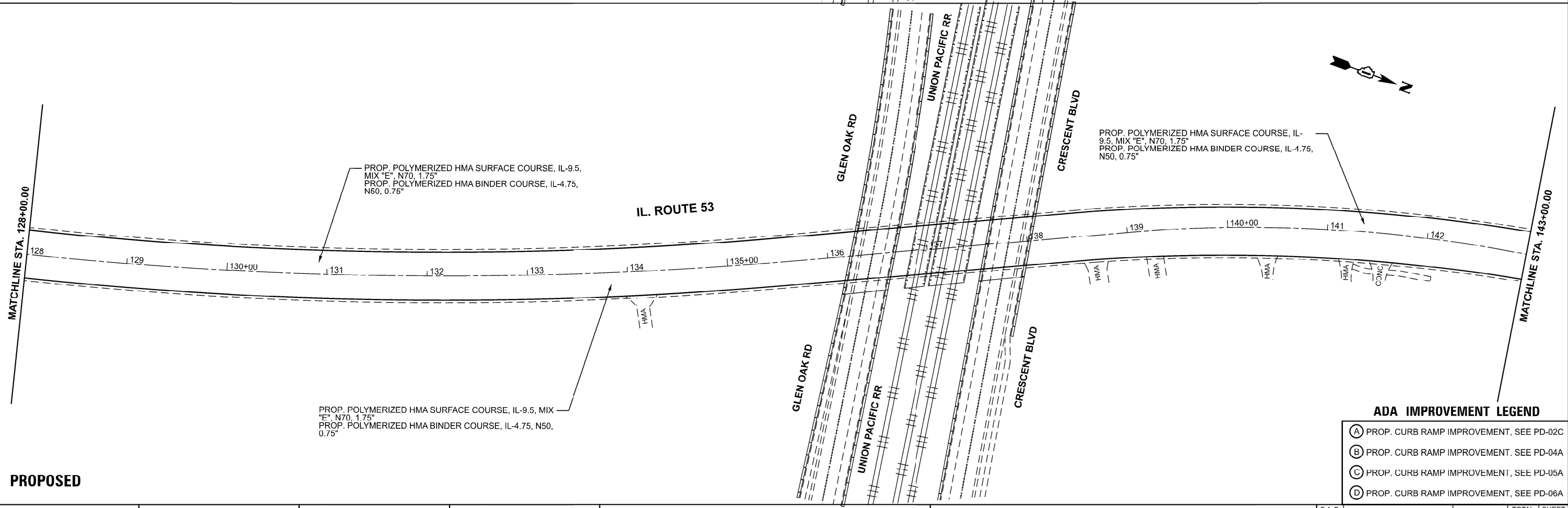
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 8 OF 10 SHEETS STA. 113+00.00 TO STA. 128+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	19
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



EXISTING



PROPOSED

ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A

MODEL: IL53_RdwyPlan9
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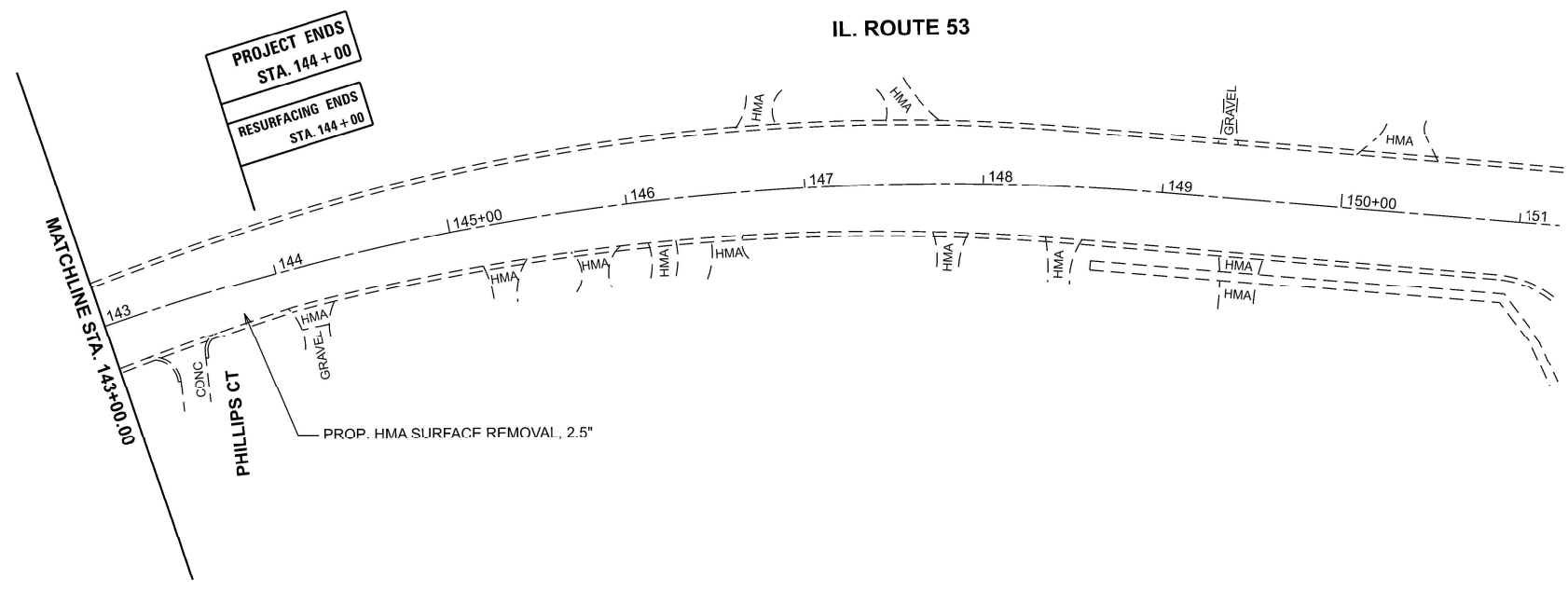
USER NAME = Aya.Eikhalib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

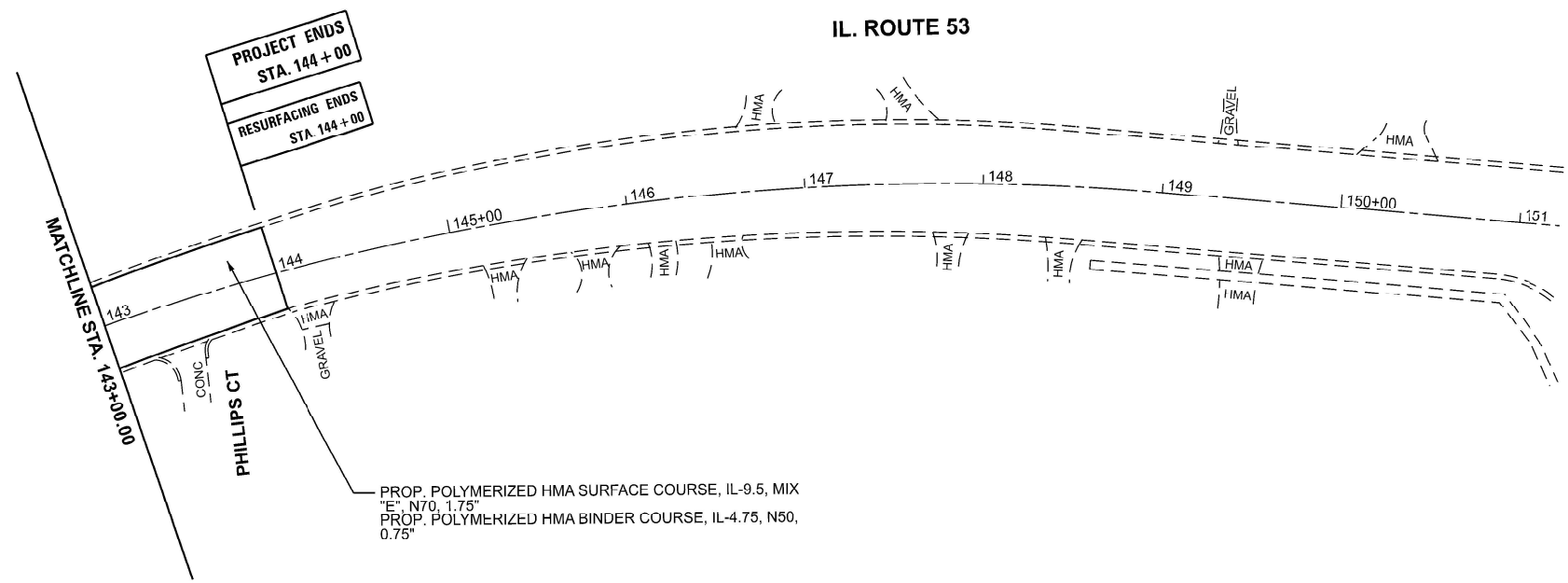
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 9 OF 10 SHEETS STA. 128+00.00 TO STA. 143+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	20
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



EXISTING



PROPOSED



ADA IMPROVEMENT LEGEND

- (A) PROP. CURB RAMP IMPROVEMENT, SEE PD-02C
- (B) PROP. CURB RAMP IMPROVEMENT, SEE PD-04A
- (C) PROP. CURB RAMP IMPROVEMENT, SEE PD-05A
- (D) PROP. CURB RAMP IMPROVEMENT, SEE PD-06A

MODEL: IL53_RoadPlan10
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USER NAME = Aya.Eikihalib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

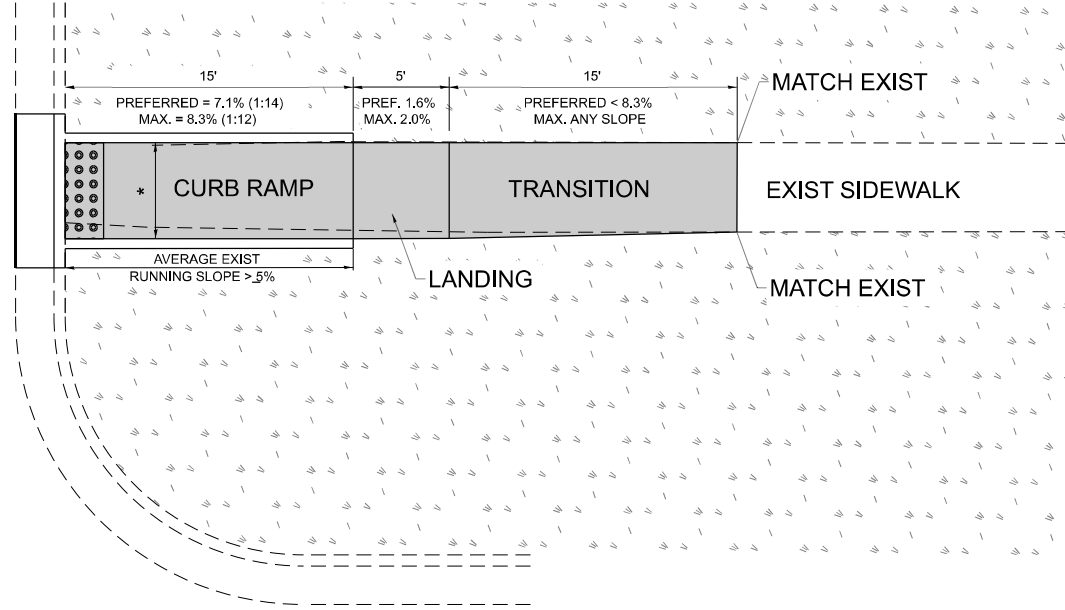
**EXISTING AND PROPOSED ROADWAY PLANS AND PROFILE
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 10 OF 10 SHEETS STA. 143+00.00 TO STA. 158+00.00

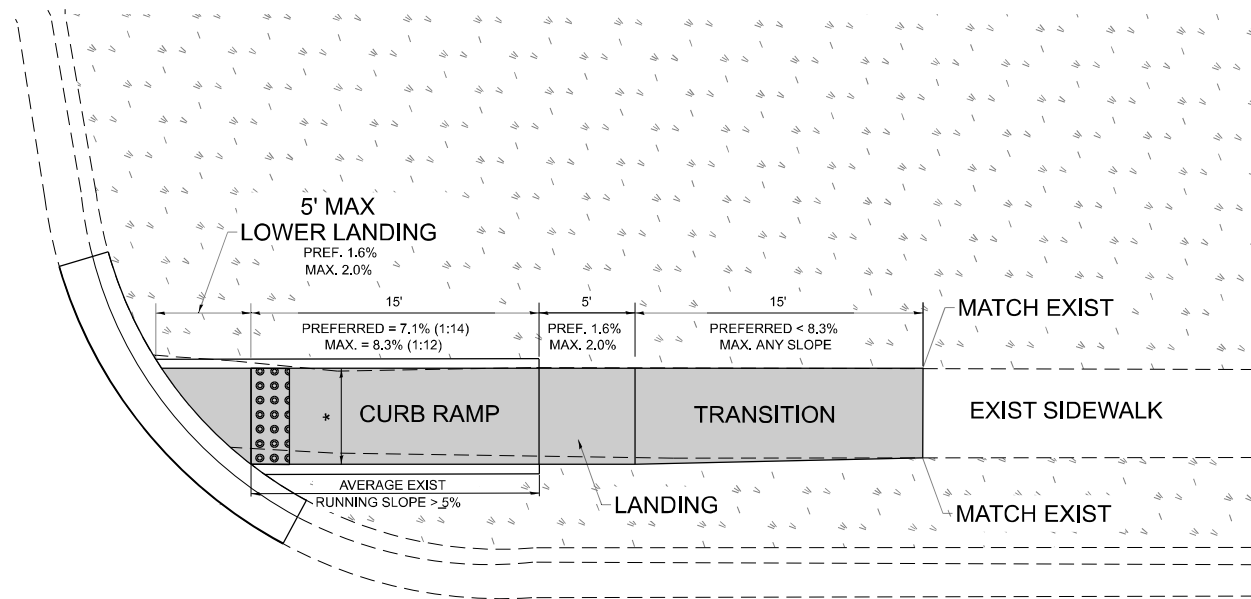
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	21
			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

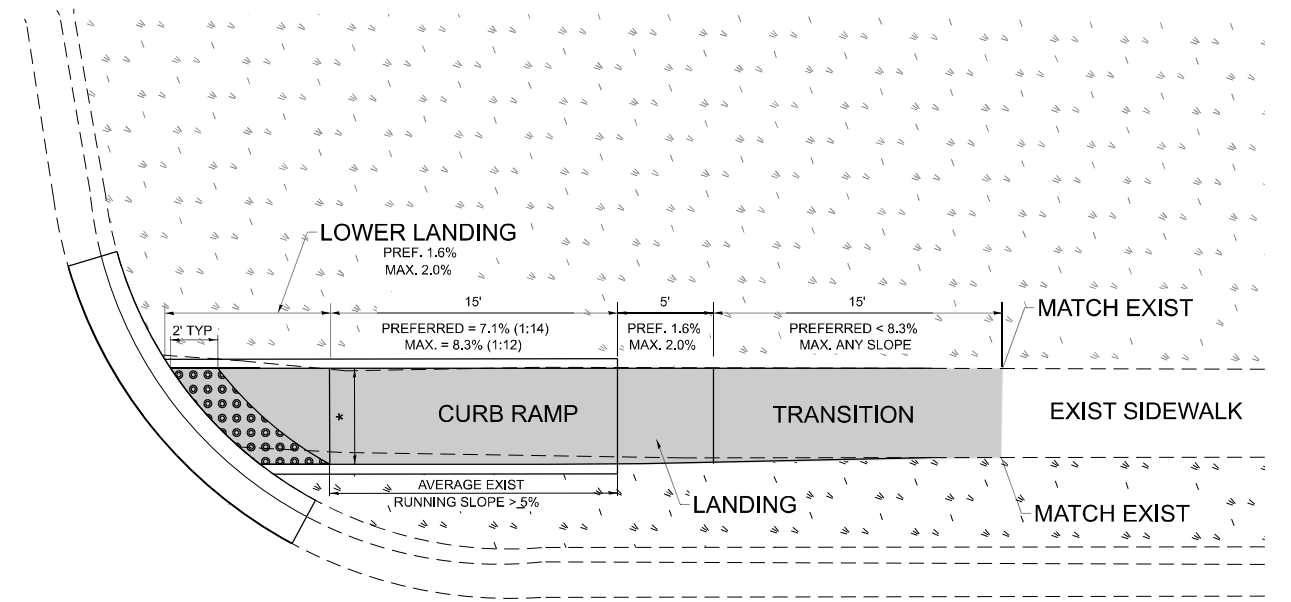
PD-02A



PD-02B



PD-02C



LEGEND

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-02 (Sheet)
FILE NAME: c:\pwworking\dwg\118825-sh1-DistSigs2.dgn

USER NAME = Aya,Elkhalib	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

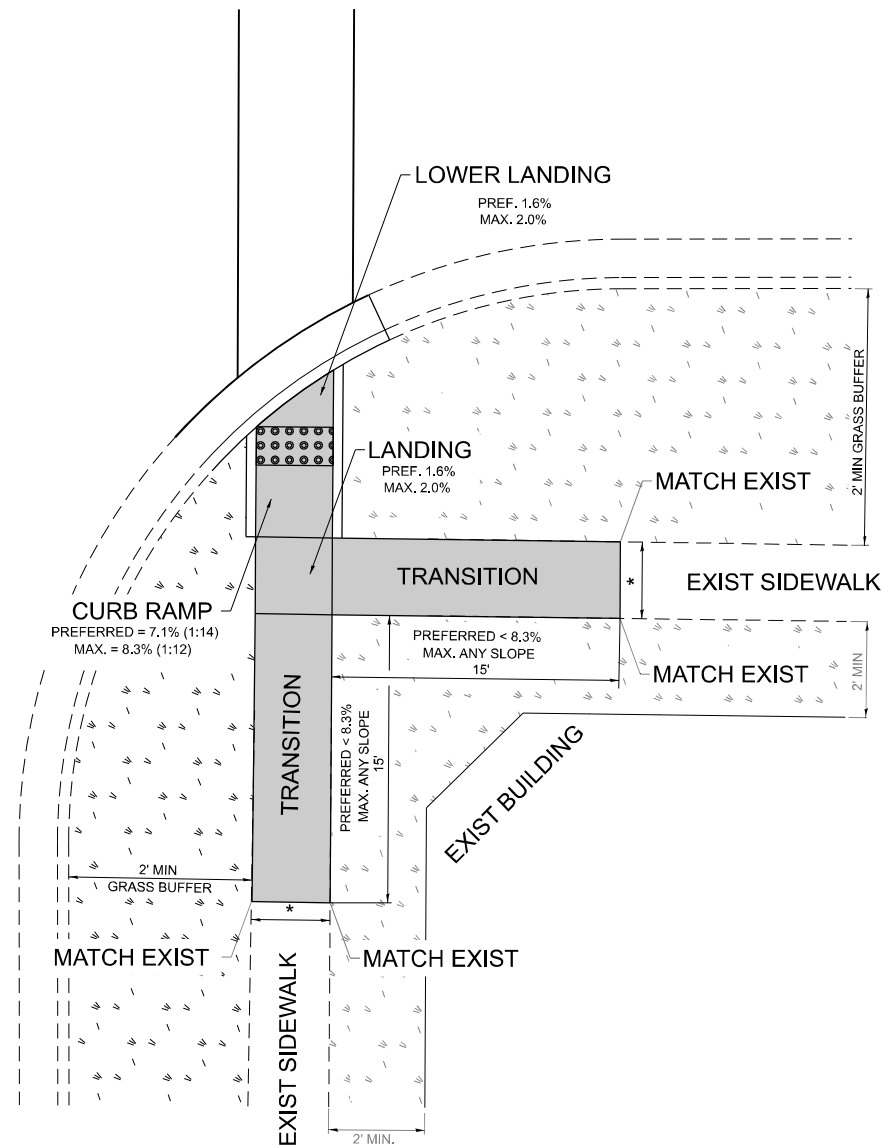
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-02)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

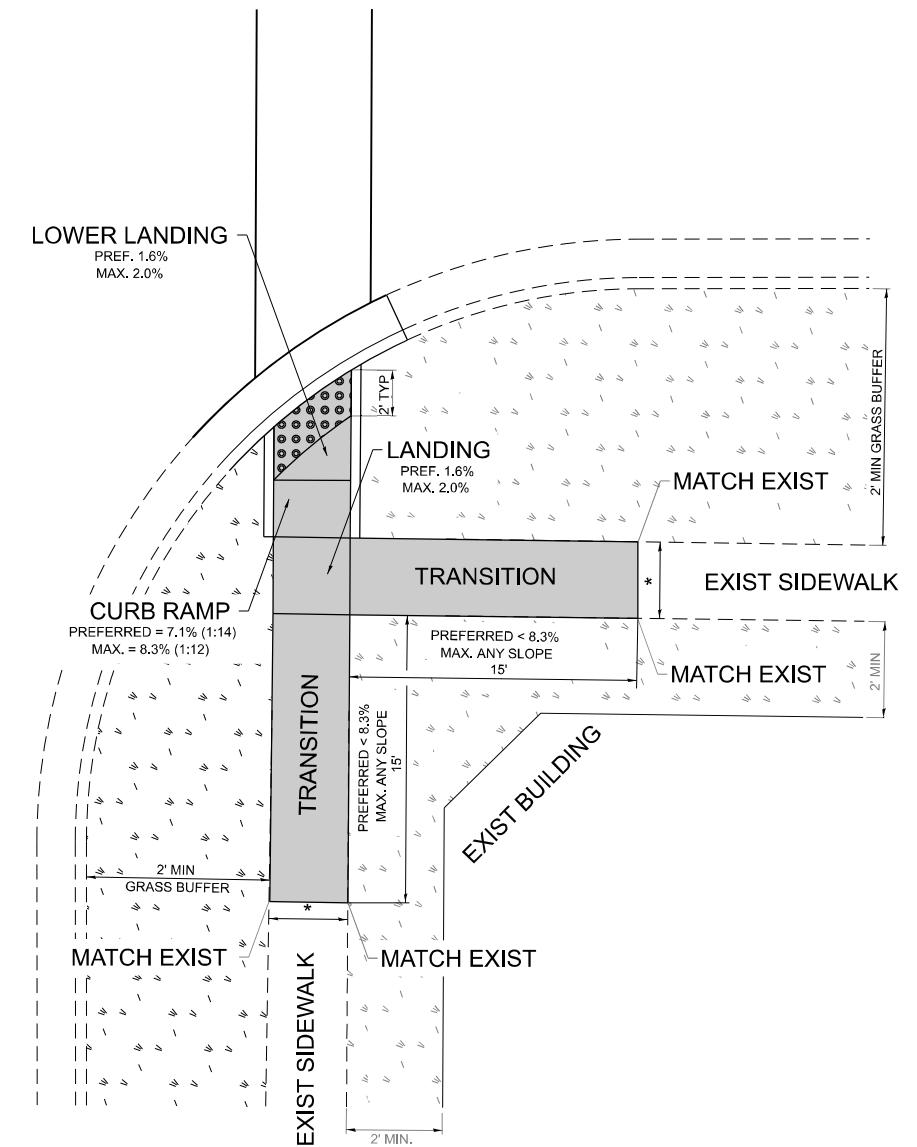
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870	2025-2006-RS, 8X19(066)	DUPAGE	56	22
PD-02			CONTRACT NO. 80B25	
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-04 (Sheet)
FILE NAME: c:\pwworking\elkhalibaj\176189\118925-sh+Dis\Std.dgn

USER NAME - Aya.Elkhailib	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -

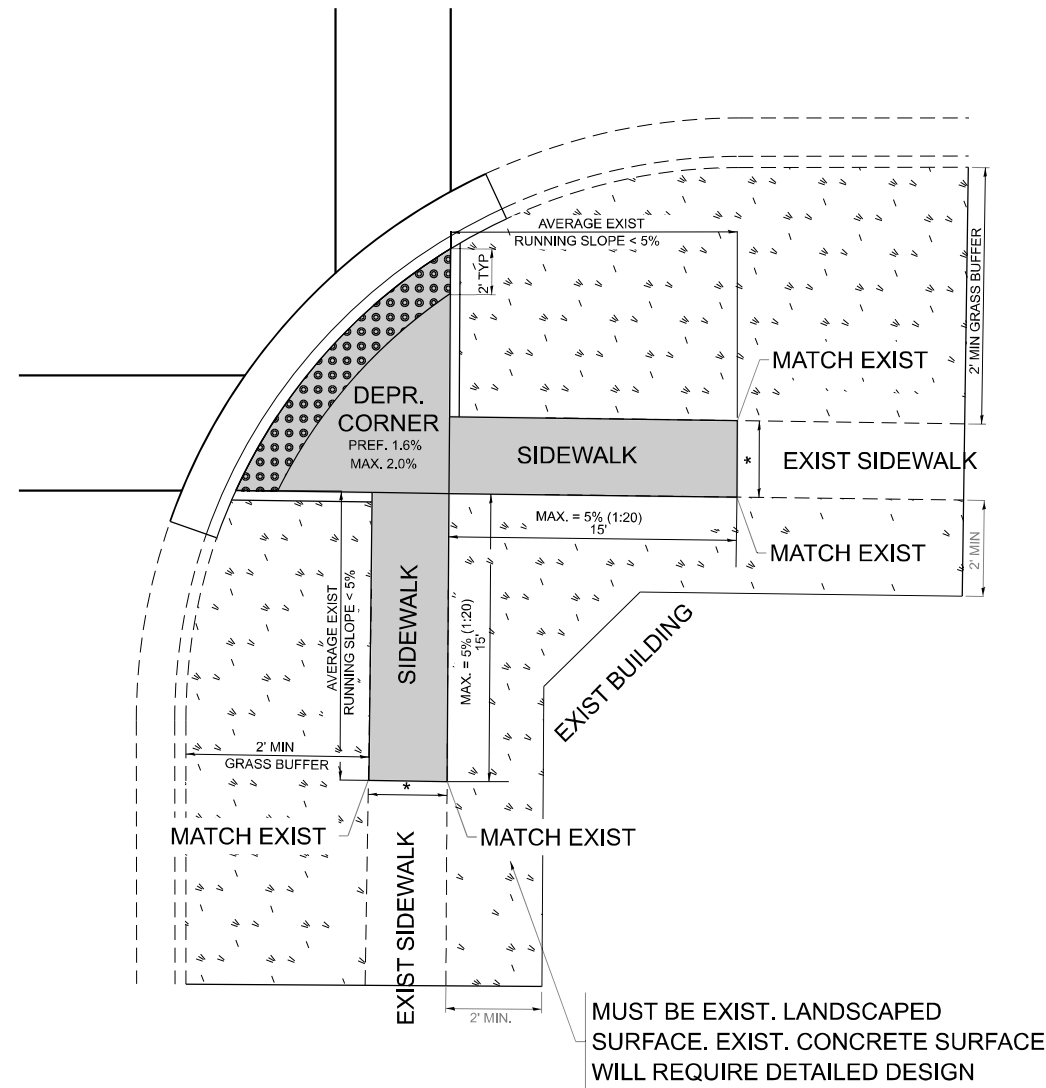
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)**

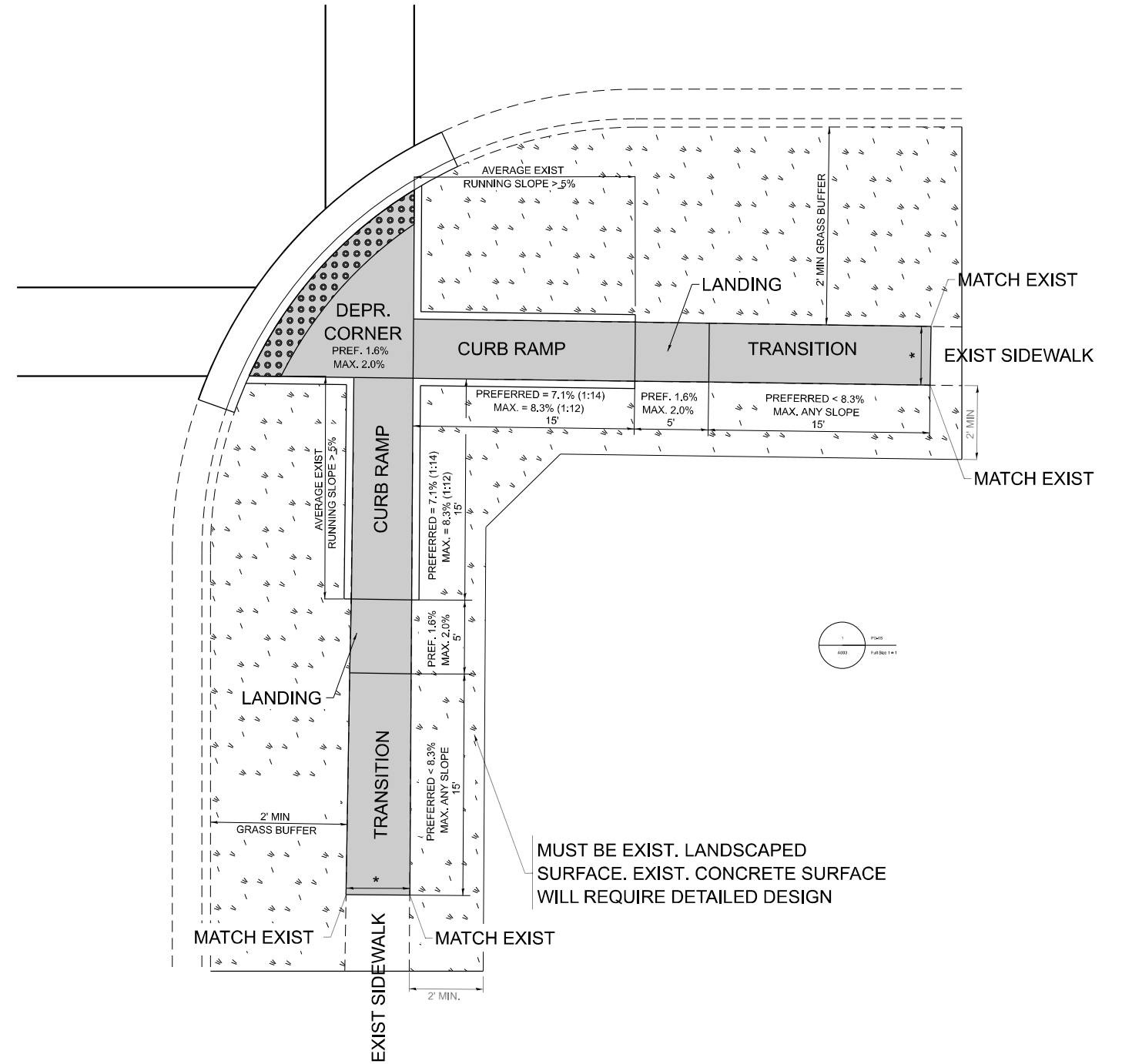
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870	2025-2006-RS, 8X19(066)	DUPAGE	56	23
PD-04			CONTRACT NO. 80B25	

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-05 (Sheet)
FILE NAME: c:\pwworking\elkhalibaj\1076189\118825-shr-HisSigs.dgn

USER NAME = Aya.Elkhailib	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

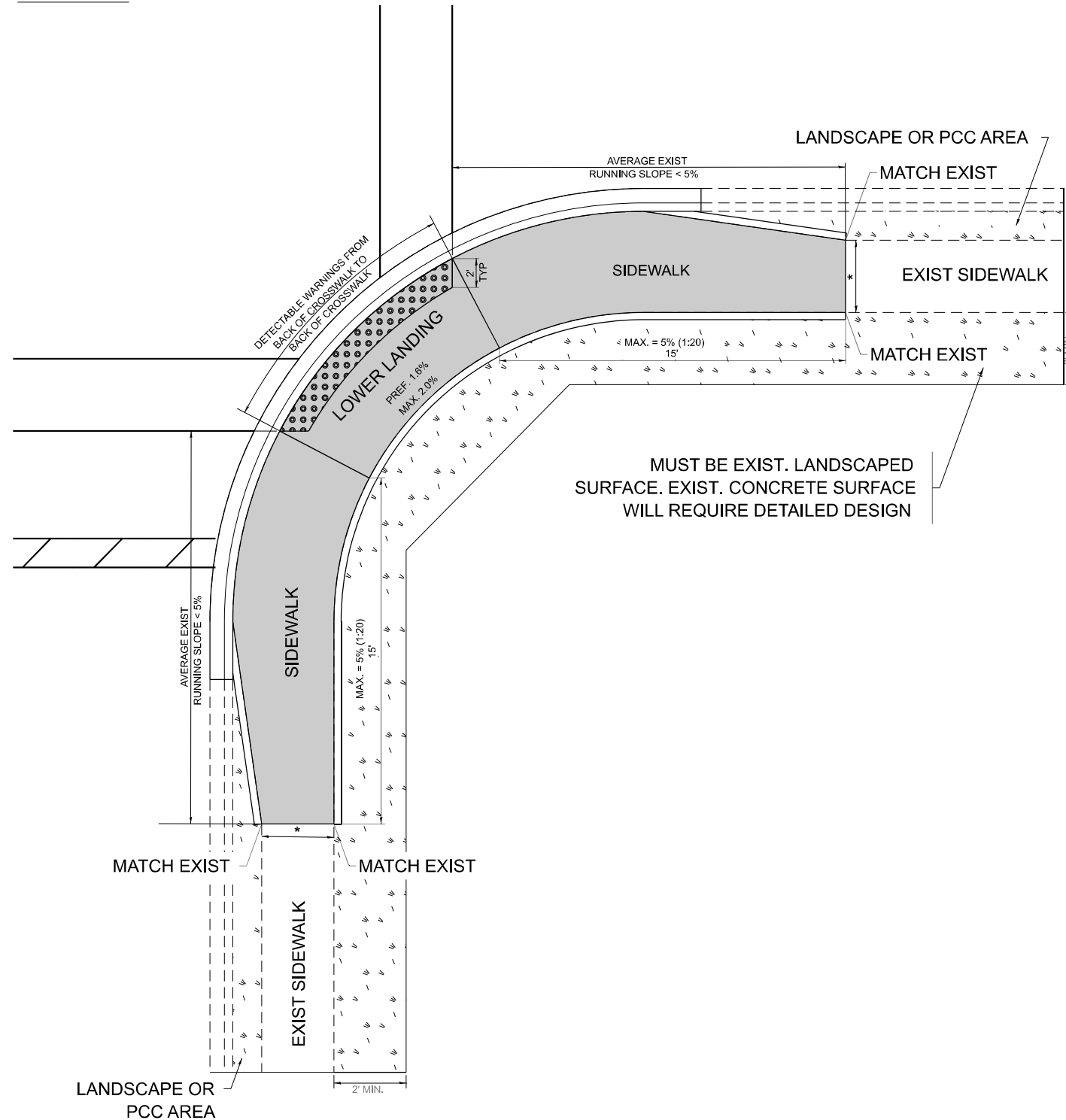
**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

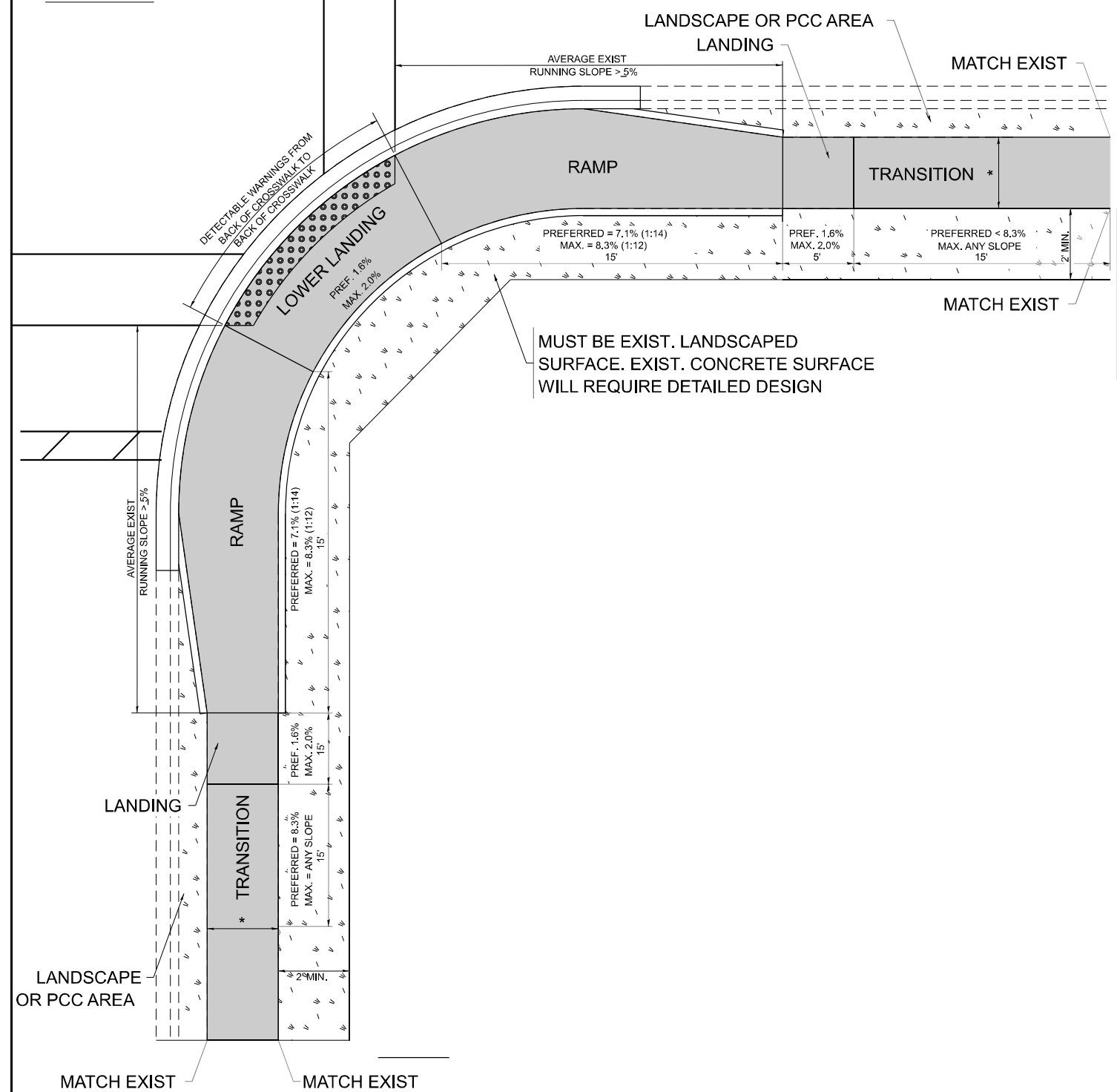
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870	2025-2006-RS_8X19(066)	DUPAGE	56	24
PD-05			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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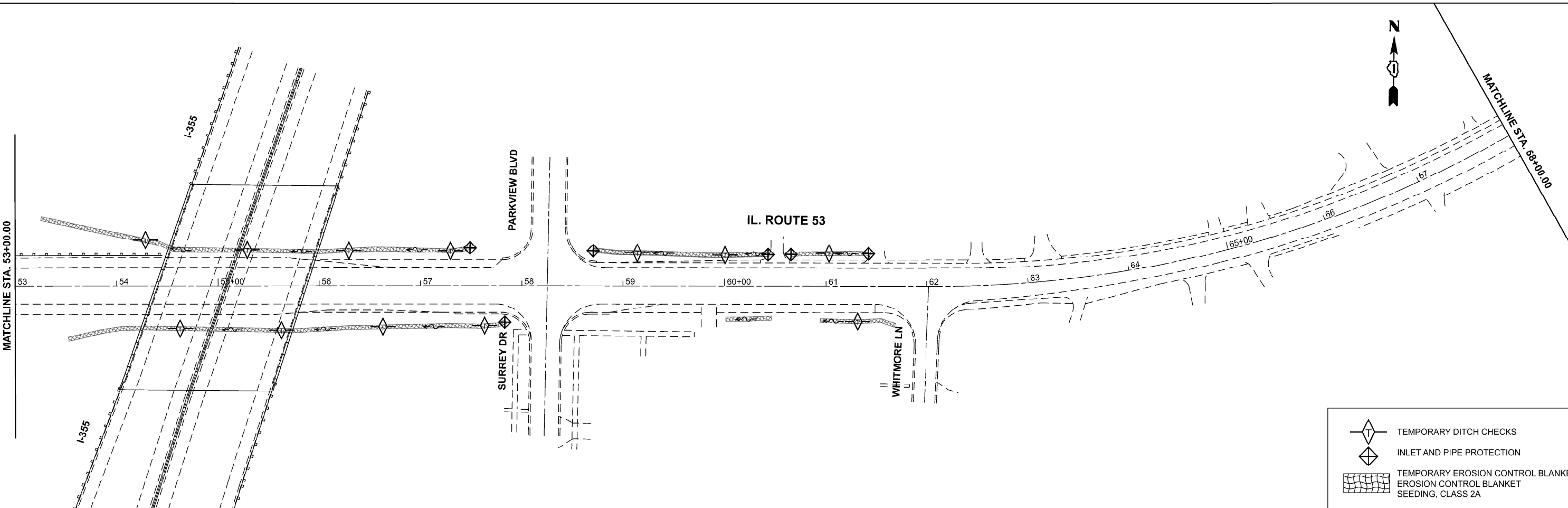
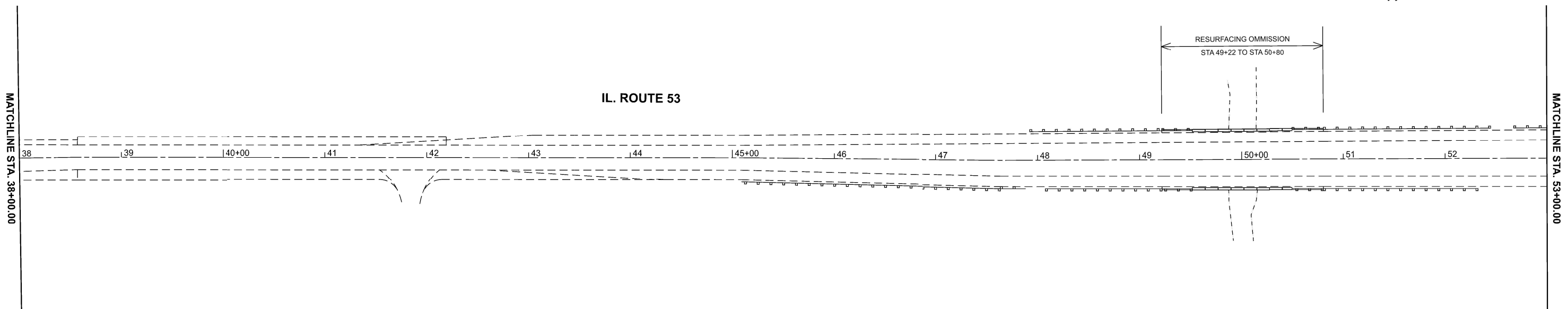
USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS_8X19(066)	DUPAGE	56	25
PD-06			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



- TEMPORARY DITCH CHECKS
- INLET AND PIPE PROTECTION
- TEMPORARY EROSION CONTROL BLANKET
- EROSION CONTROL BLANKET SEEDING, CLASS 2A

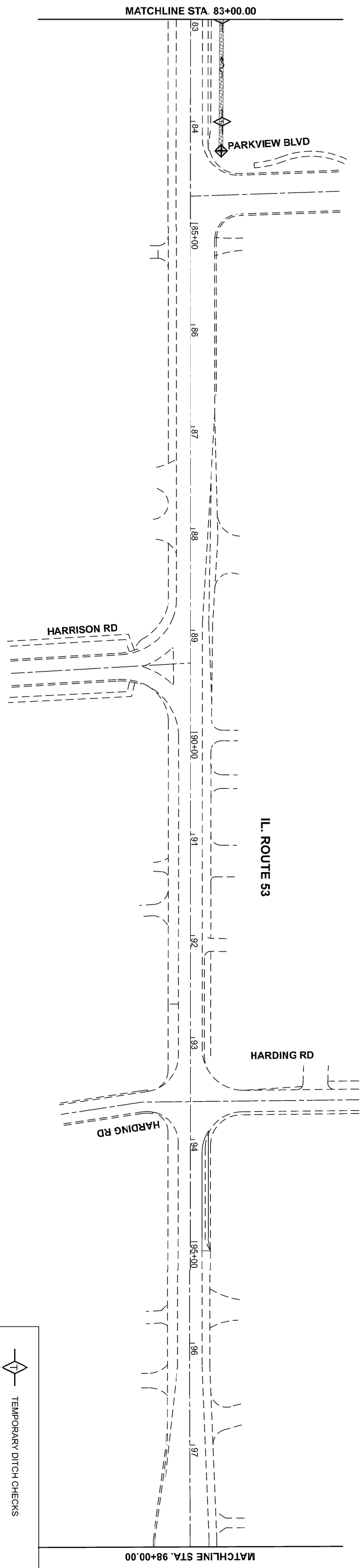
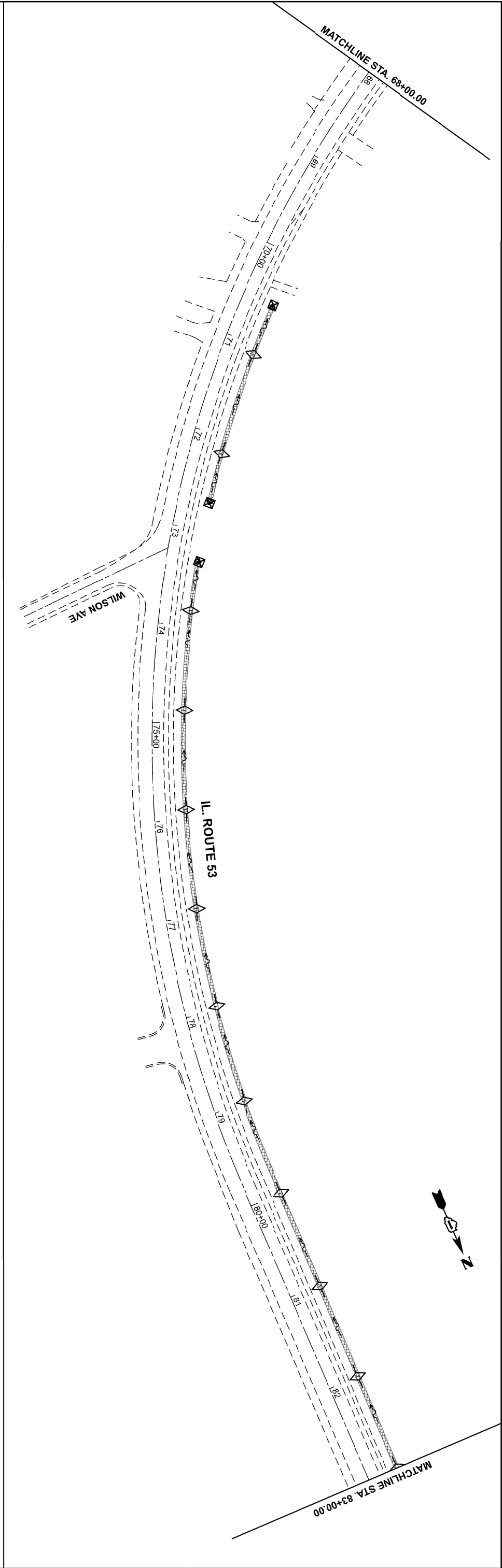
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USER NAME = Aya.EIKI@itb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL AND LANDSCAPING PLAN
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)
 SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 38+00.00 TO STA. 68+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	26
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

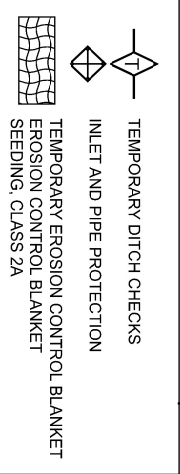


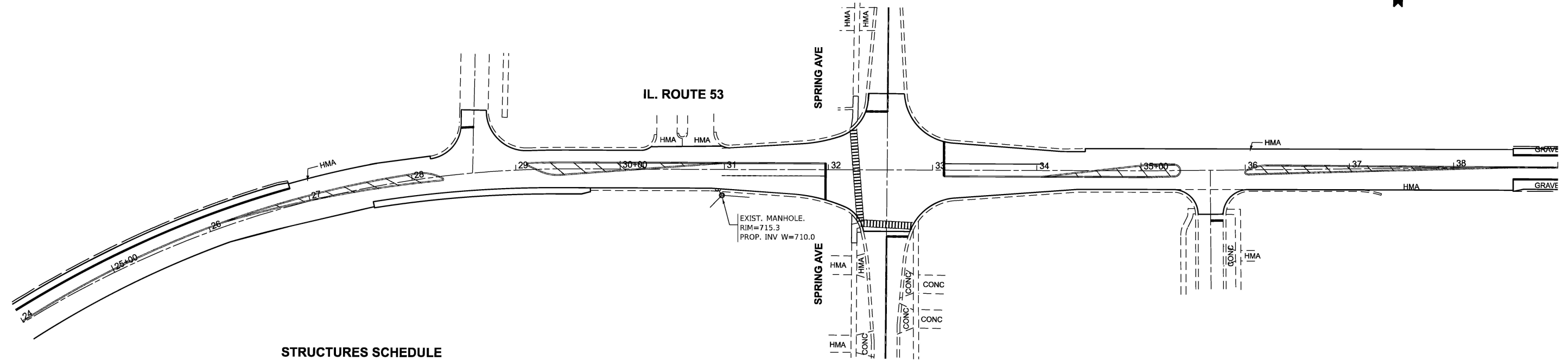
USER NAME = AYALEKHIDU	DESIGNED -	REVISED -
DRAWN -	CHECKED -	REVISED -
DATE -	DATE -	REVISED -
PLT DATE = 10/17/2025		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL AND LANDSCAPING PLAN
ILL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS
870	2025-2006-RS_8X19(068)	DUPAGE	56
SCALE: 1"=50'	SHEET 2	OF 2 SHEETS	NO. 27
	STA. 68+00.00	TO STA. 98+00.00	CONTRACT NO. 80B25
	ILLINOIS FED. AID PROJECT		

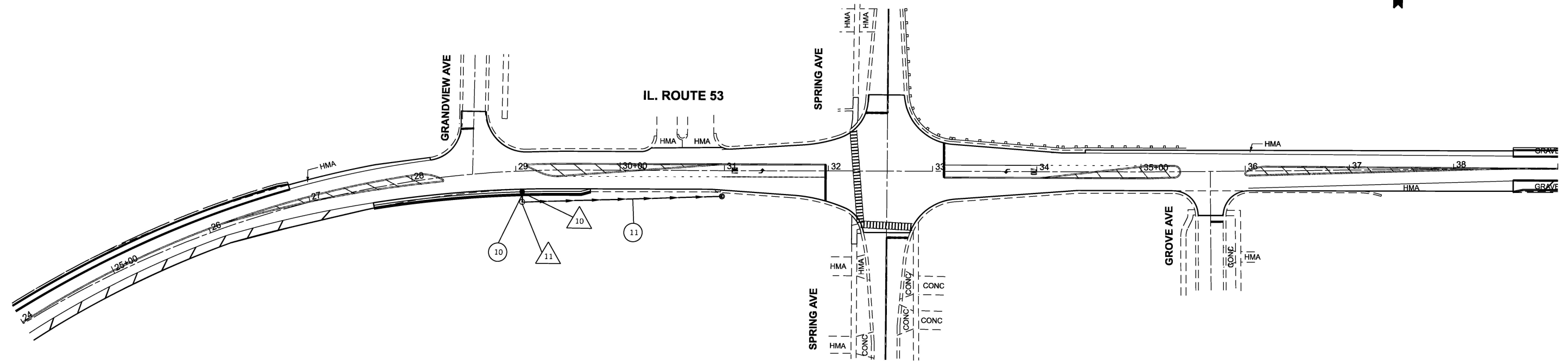




STRUCTURES SCHEDULE

STORM SEWER SCHEDULE

PIPE NUMBER	PIPE TYPE	DIAMETER (IN)	LENGTH (FT)	SLOPE (%)	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT
10	STORM SEWER, CLASS A, TYPE 1	12	6	0.50	10	11	719.32	719.29
11	STORM SEWER, CLASS A, TYPE 1	15	188	4.90	11	EXIST. MANHOLE	719.20	710.00



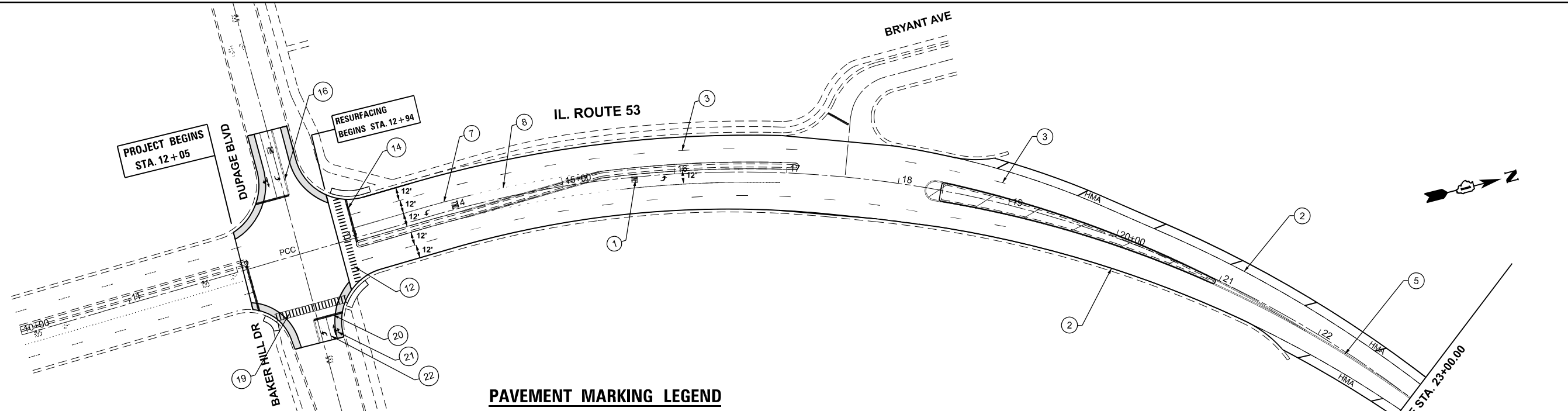
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USER NAME = Aya.Eikhatib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

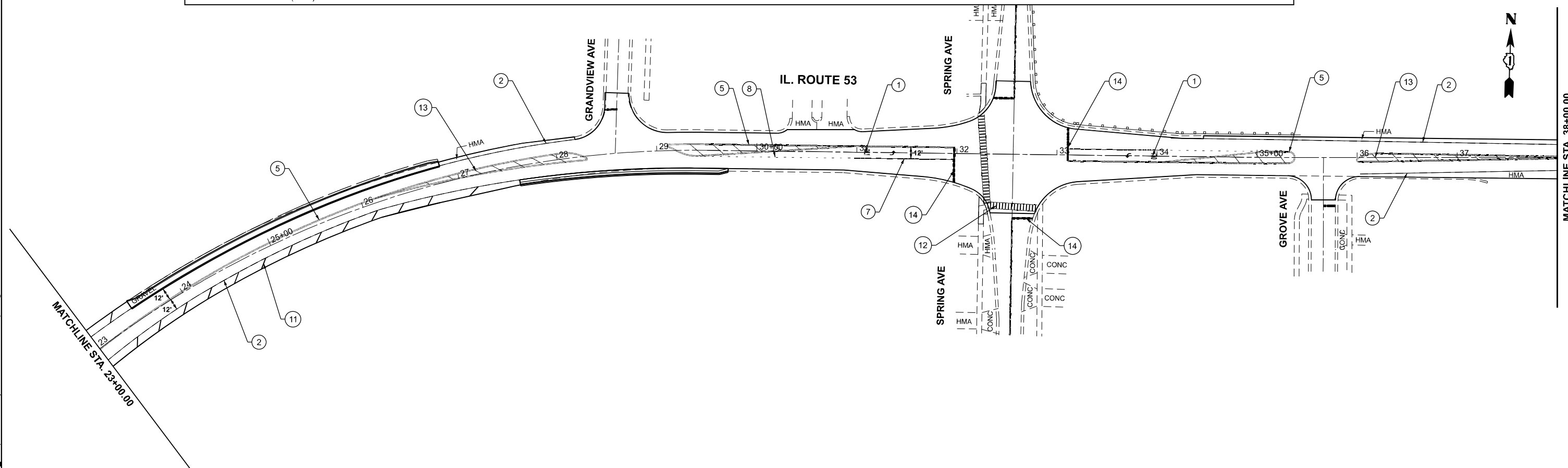
**EXISTING AND PROPOSED DRAINAGE PLANS
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**
SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 24+00.00 TO STA. 39+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	28
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

① PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)	⑧ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.)	⑮ PROP. MODIFIED URETHANE PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.)	⑳ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.)
② PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.)	⑨ PROP. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.)	⑯ PROP. MODIFIED URETHANE PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.)	㉑ PROP. MODIFIED URETHANE PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
③ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.)	⑩ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE MARKING @ 45 DEG. (TYP.)	⑰ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.)	㉒ PROP. MODIFIED URETHANE PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.)
④ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE @ 30' SKIP & 10' DASH (TYP.)	⑪ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.)	⑱ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.)	
⑤ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.)	⑫ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.)	㉑ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.)	
⑥ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID AND SKIP DASH YELLOW NO PASSING ZONE LINES FOR ONE DIRECTION @ 5 1/2 C-C (TYP.)	⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.)	㉒ PROP. MODIFIED URETHANE PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)	
⑦ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE (TYP.)	⑭ PROP. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)	㉓ PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.)	



MODEL: IL53_E-Rdwy Plan 1 (Sheet)
 FILE NAME: c:\p\work\pav\il53\plan1\8825-shp-pmk.dgn

USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

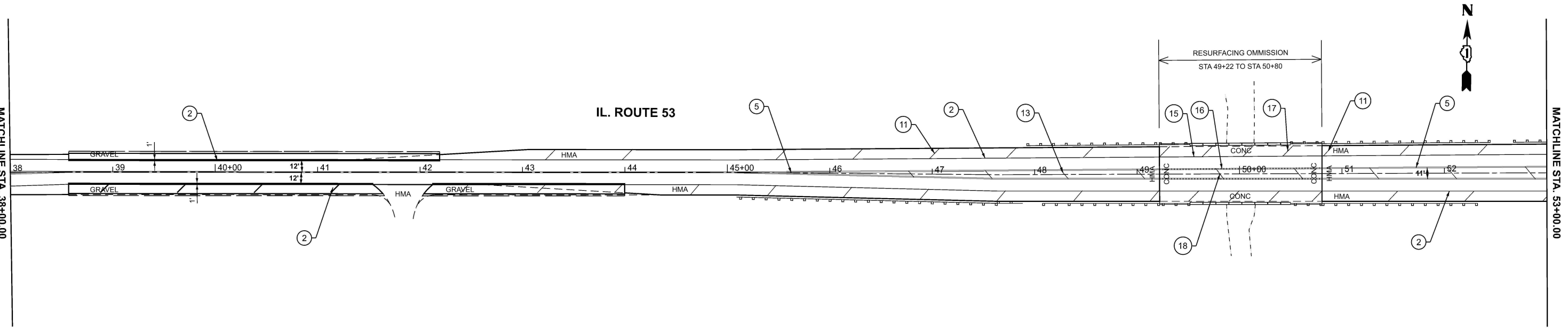
**PAVEMENT MARKING PLANS
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**

SCALE: 1"=50' SHEET 1 OF 5 SHEETS STA. 8+00.00 TO STA. 38+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	29
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 38+00.00

MATCHLINE STA. 53+00.00

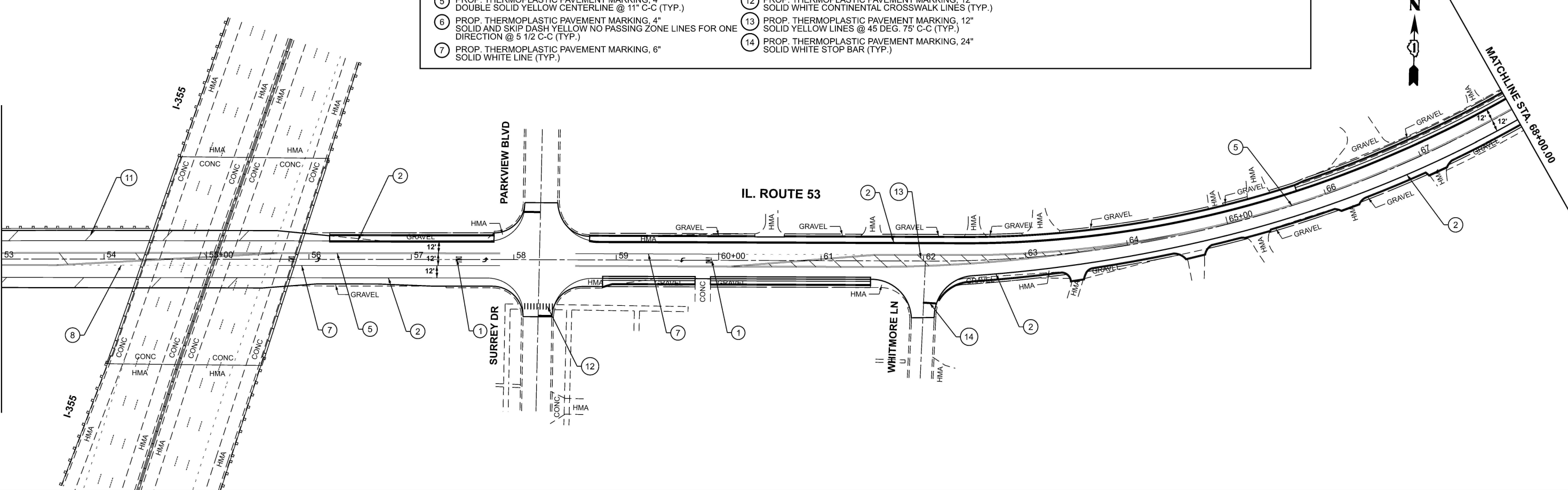


PAVEMENT MARKING LEGEND

- | | | |
|--|---|--|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.) | ⑮ PROP. MODIFIED URETHANE PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.) |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.) | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.) | ⑯ PROP. MODIFIED URETHANE PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.) |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.) | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE MARKING @ 45 DEG. (TYP.) | ⑰ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.) |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE @ 30' SKIP & 10' DASH (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.) | ⑱ PROP. MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.) |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.) | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.) | |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID AND SKIP DASH YELLOW NO PASSING ZONE LINES FOR ONE DIRECTION @ 5 1/2 C-C (TYP.) | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.) | |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) | |

MATCHLINE STA. 53+00.00

MATCHLINE STA. 68+00.00



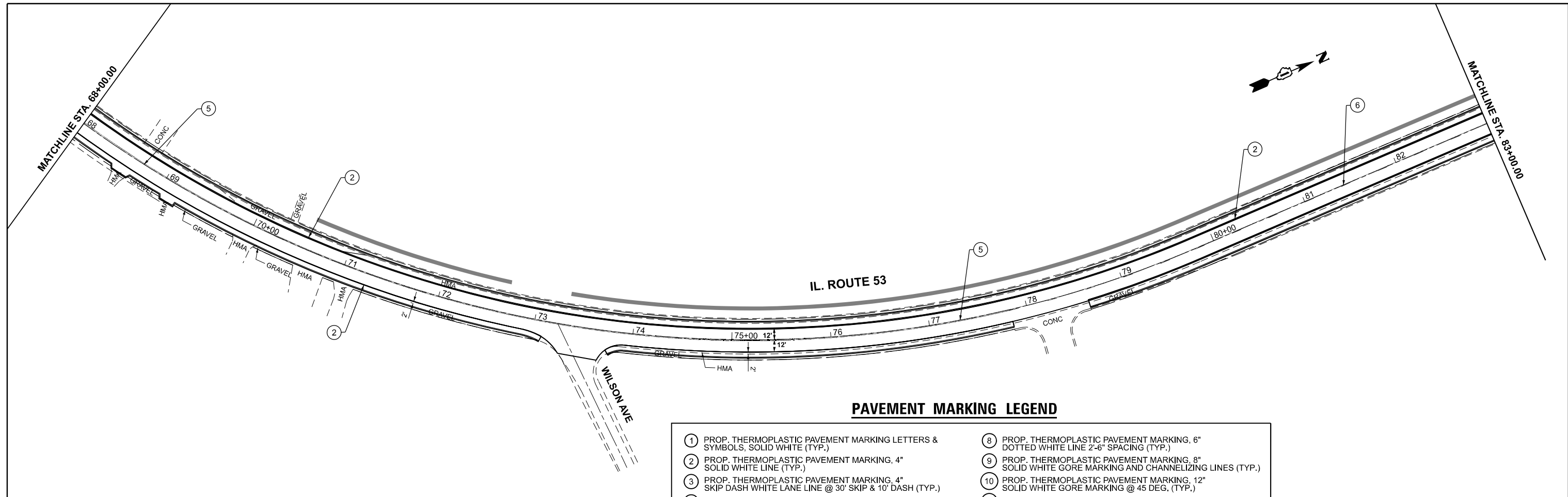
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USER NAME = Aya,Elkhatib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

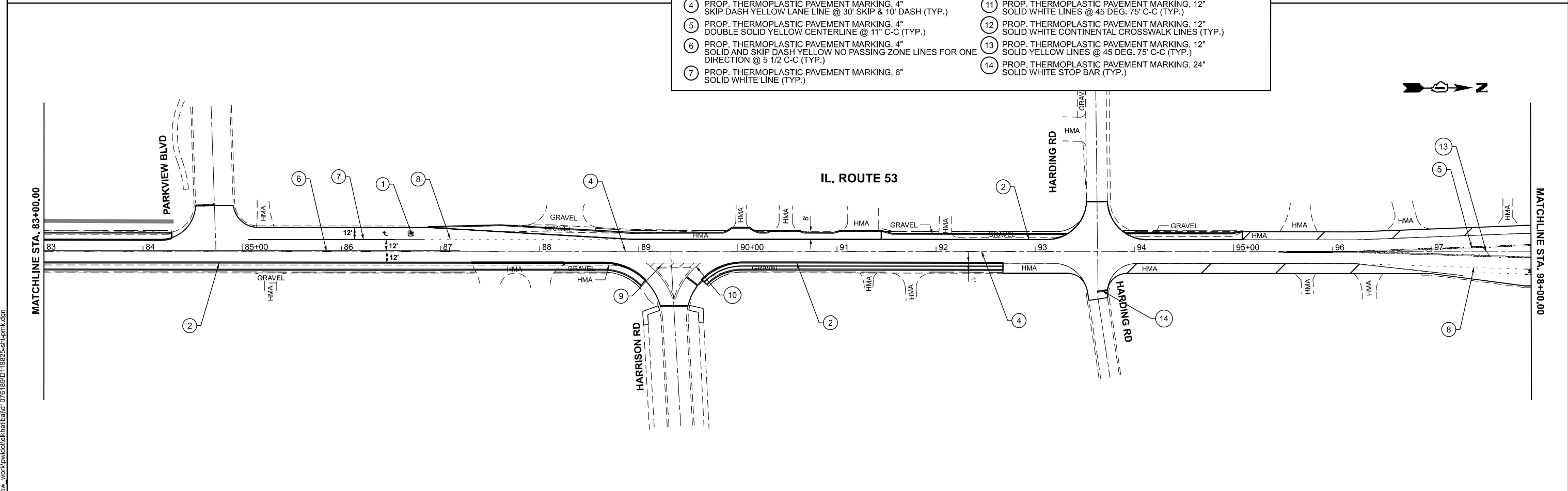
PAVEMENT MARKING PLANS	
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)	
SCALE: 1"=50'	SHEET 2 OF 5 SHEETS STA. 38+00.00 TO STA. 68+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	30
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- | | |
|--|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.) |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.) | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.) |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.) | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE MARKING @ 45 DEG. (TYP.) |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE @ 30' SKIP & 10' DASH (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.) |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.) | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.) |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID AND SKIP DASH YELLOW NO PASSING ZONE LINES FOR ONE DIRECTION @ 5 1/2 C-C (TYP.) | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.) |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) |



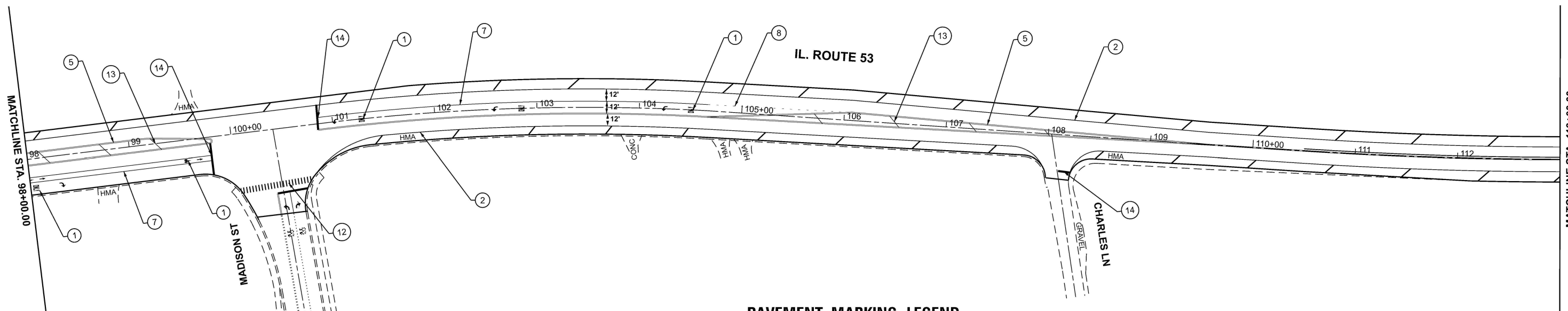
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	DRAWN -	REVISED -
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PLOT DATE = 12/2/2025	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

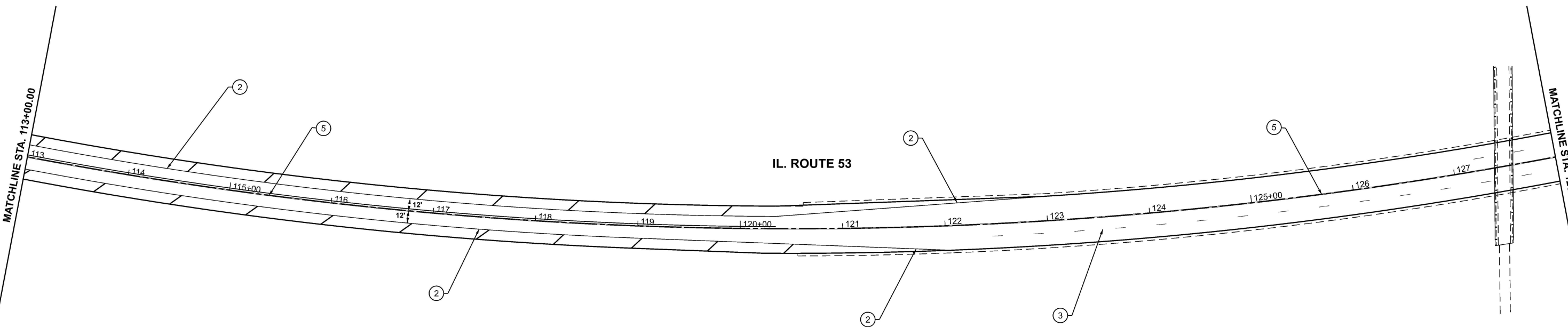
PAVEMENT MARKING PLANS			
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)			
SCALE: 1"=50'	SHEET 3	OF 5 SHEETS	STA. 68+00.00 TO STA. 98+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	31
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- | | |
|---|---|
| <ul style="list-style-type: none"> ① PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.) ② PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.) ③ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE @ 30' SKIP & 10' DASH (TYP.) ④ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE @ 30' SKIP & 10' DASH (TYP.) ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.) ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID AND SKIP DASH YELLOW NO PASSING ZONE LINES FOR ONE DIRECTION @ 5 1/2 C-C (TYP.) ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE (TYP.) | <ul style="list-style-type: none"> ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.) ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.) ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE GORE MARKING @ 45 DEG. (TYP.) ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LINES @ 45 DEG. 75' C-C (TYP.) ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.) ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.) ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) |
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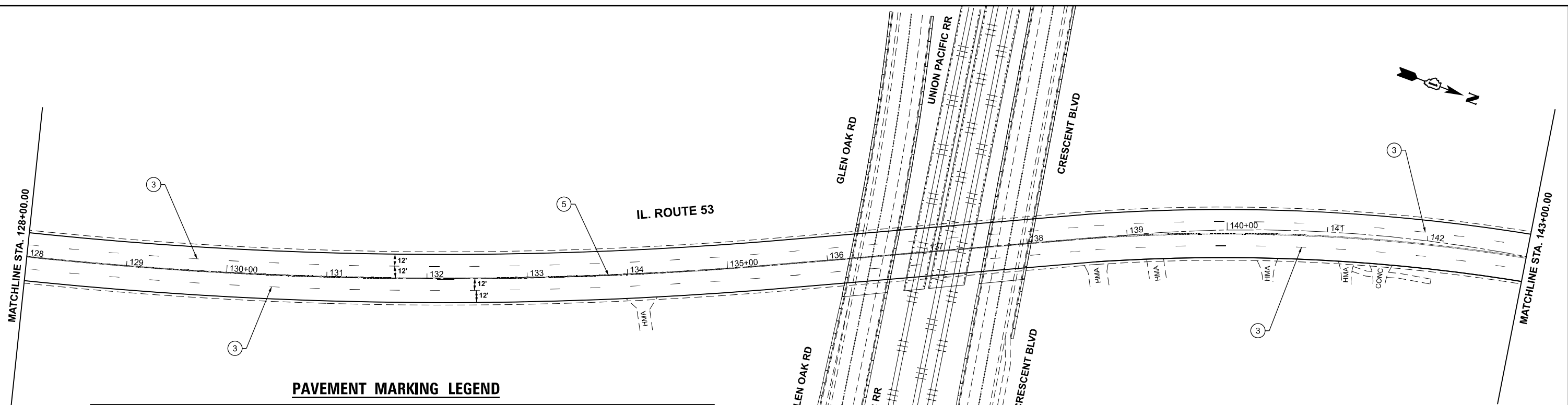
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USER NAME = Aya.Eikhatib	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

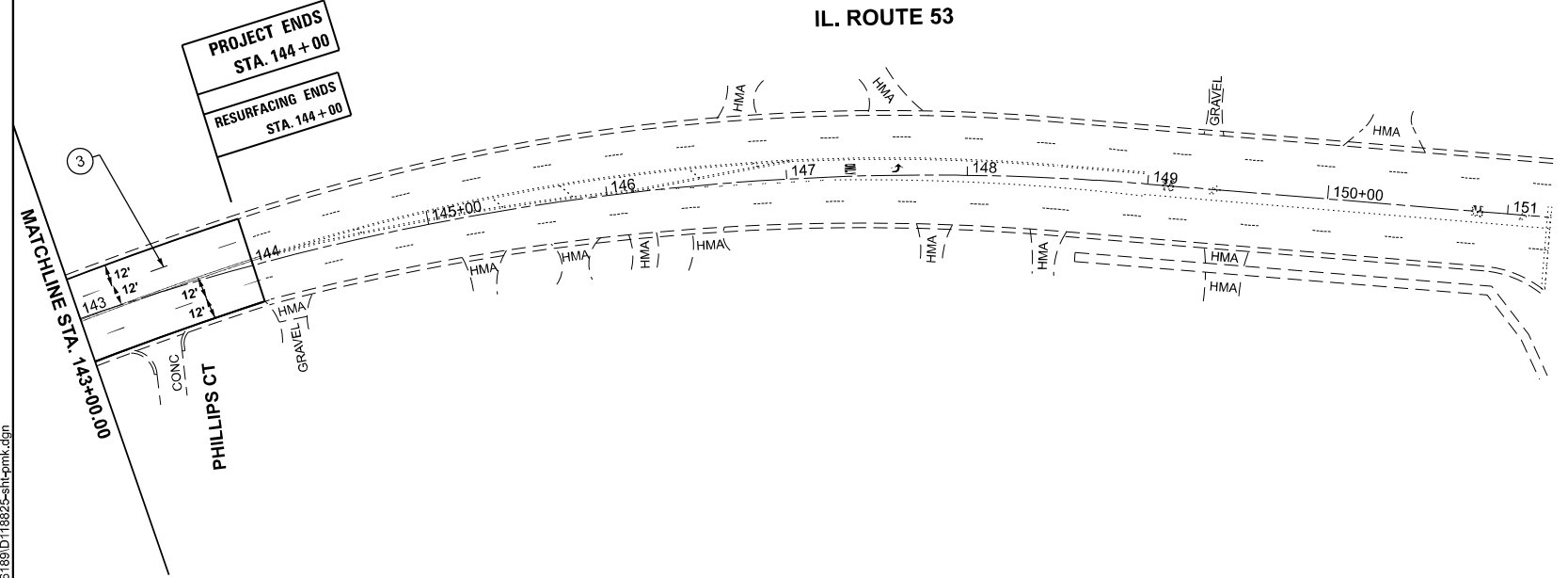
PAVEMENT MARKING PLANS			
IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)			
SCALE: 1"=50'	SHEET 4	OF 5 SHEETS	STA. 98+00.00 TO STA. 128+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	32
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- | | |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, SOLID WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED WHITE LINE 2'-6" SPACING (TYP.) |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE (TYP.) | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.) |
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| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE @ 11" C-C (TYP.) | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE CONTINENTAL CROSSWALK LINES (TYP.) |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING, 4" SOLID AND SKIP DASH YELLOW NO PASSING ZONE LINES FOR ONE DIRECTION @ 5 1/2' C-C (TYP.) | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW LINES @ 45 DEG. 75' C-C (TYP.) |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) |



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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 12/2/2025	DATE -	REVISED -

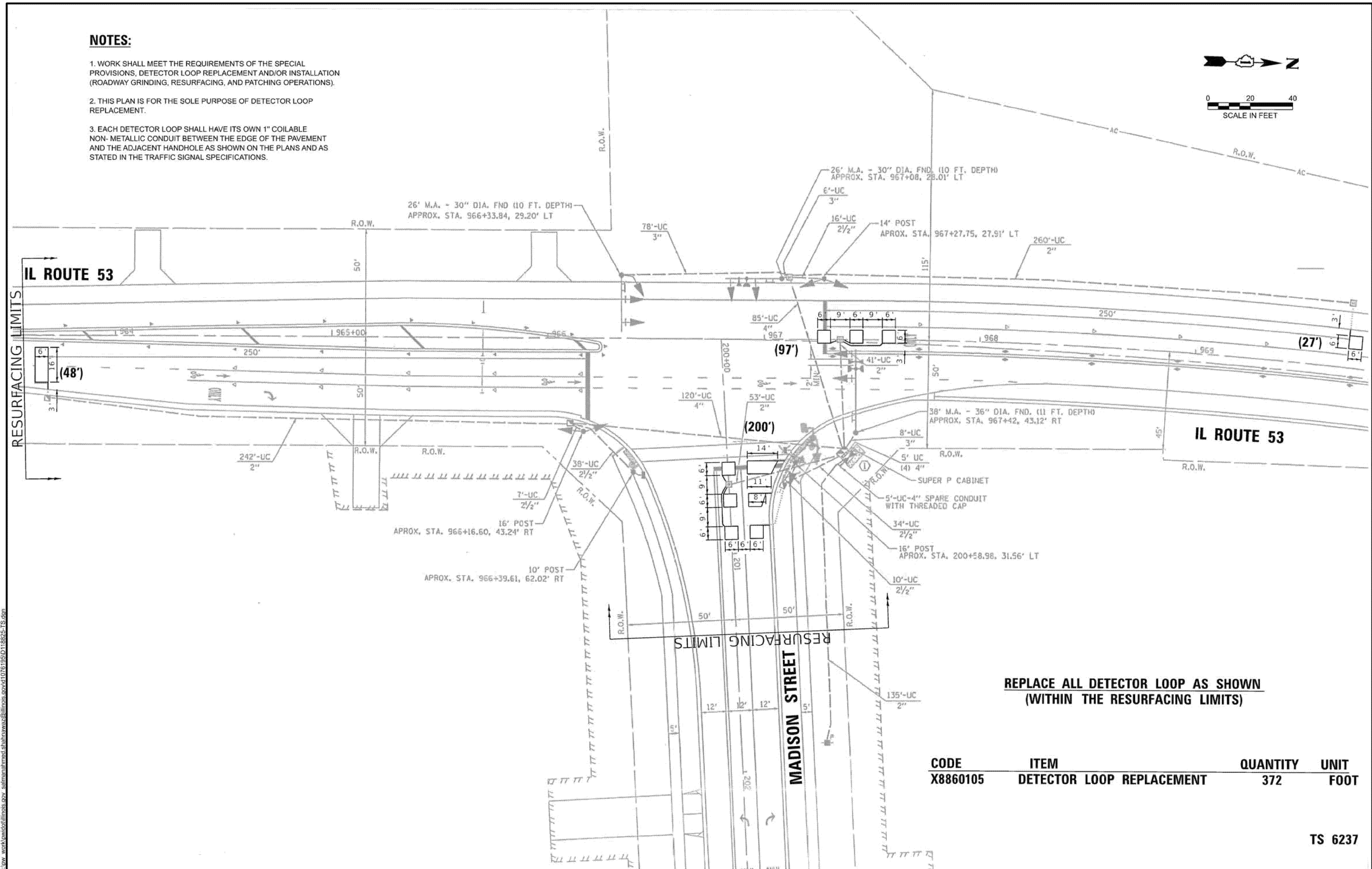
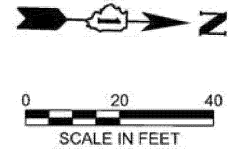
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLANS
 IL. ROUTE 53 (S OF CHARLES ST (PHILLIPS CT) TO BAKER HILL DR)**
 SCALE: 1"=50' SHEET 5 OF 5 SHEETS STA. 128+00.00 TO STA. 158+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.
3. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.



**REPLACE ALL DETECTOR LOOP AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
X8860105	DETECTOR LOOP REPLACEMENT	372	FOOT

TS 6237

MODEL: Default
FILE NAME: c:\pwworkspace\illinois.gov_salmanahmed.shahnewaz@illinois.gov\1076195D118826-TS.dgn

USER NAME = SalmanAhmed.Shahnewaz	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/30/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
IL ROUTE 53 AT MADISON STREET**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33A
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

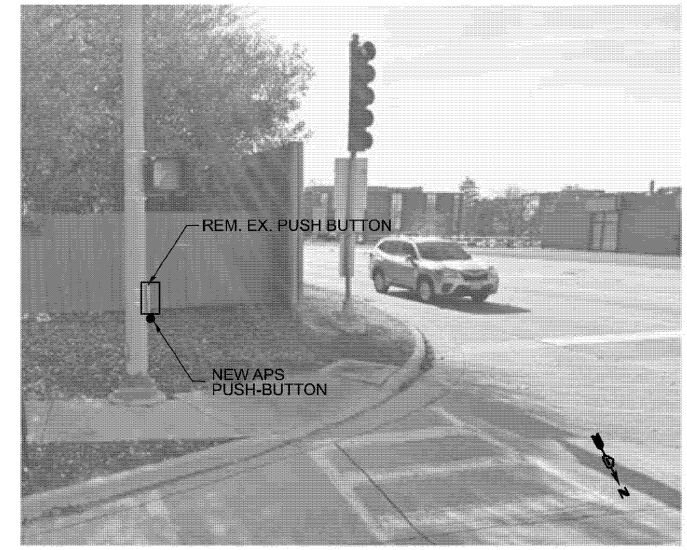
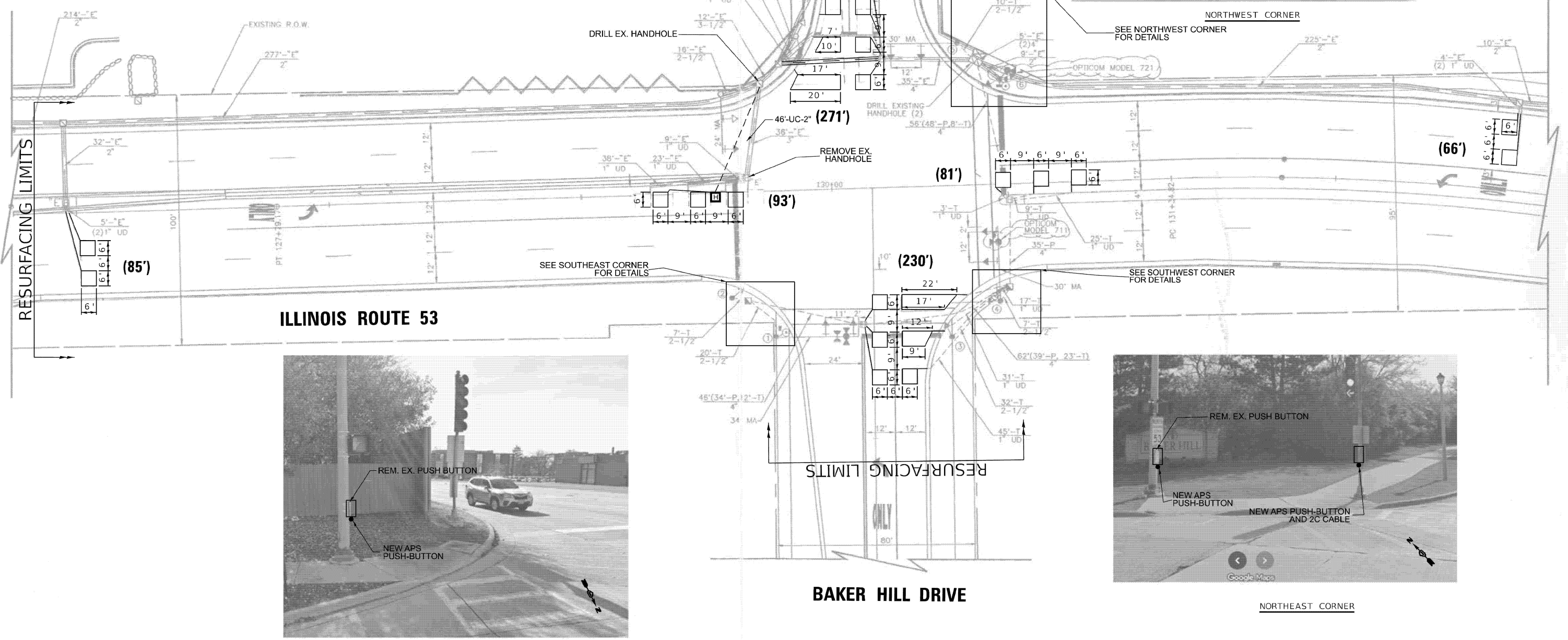
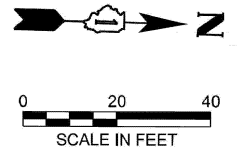
REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH PEDESTRIAN PUSH-BUTTON

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
2. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
3. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
4. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
5. ALL PUSH BUTTONS SHALL BE APS



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USER NAME = SalmanAhmed.Shahnavaz	DESIGNED -	REVISED -
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PLOT DATE = 11/25/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

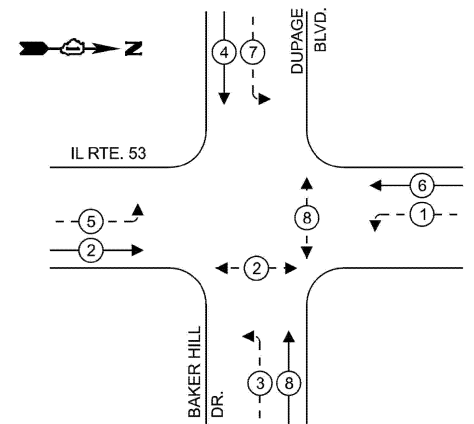
**TRAFFIC SIGNAL MODERNIZATION PLAN
IL ROUTE 53 AT DUPAGE BLVD / BAKER HILL DR**

SCALE: SHEET OF SHEETS STA. TO STA.

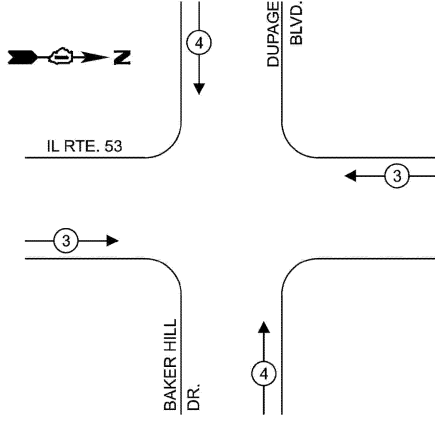
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33B
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

**TS 20330
ECON 104**

EXISTING CONTROLLER SEQUENCE



EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND:

- ← (⊛) → PROTECTED PHASE
- ← (⊛) - - PROTECTED/PERMITTED PHASE
- ← (⊛) → PEDESTRIAN PHASE

SCHEDULE OF QUANTITIES

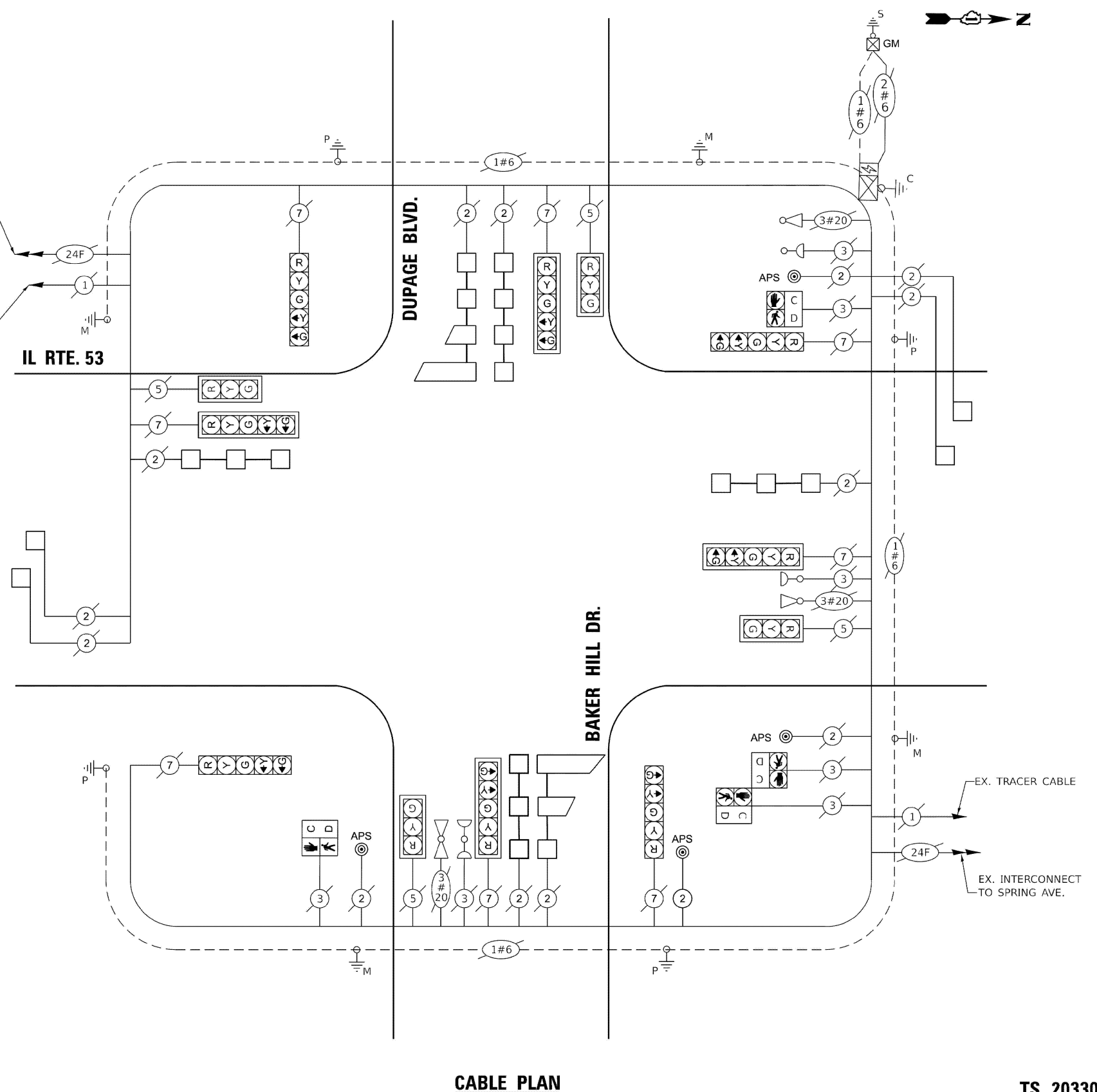
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UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA	FOOT	42
HEAVY-DUTY HANDHOLE	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO.14 1 PAIR	FOOT	163
DRILL EXISTING HANDHOLE	EACH	1
DETECTOR LOOP, TYPE I	FOOT	826
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	4	11	44
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL CONTROLLER	4	15	60
MASTER CONTROLLER	1	150	150
UPS	1	25	25
DETECTION VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
PTZ CAMERA	-	75	-
TOTAL UPS SIZING			383
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			988

ENERGY COSTS TO:
 VILLAGE OF GLEN ELLYN
 535 DUANE STREET
 GLEN ELLYN, IL 60137

ENERGY SUPPLY: CONTACT: ANTONIO RIOS
 PHONE: 630-696-6855
 COMPANY: COMED
 ACCOUNT NUMBER: 32544-65000
 METER NUMBER: ---



CABLE PLAN

**TS 20330
 ECON 104**

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 USER: SalmaAhmed.Shahinawa

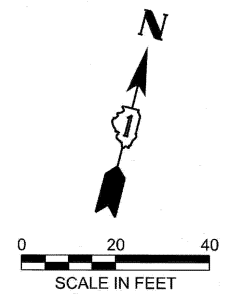
USER NAME	DESIGNED	REVISION
SalmaAhmed.Shahinawa	SS	-
	SS	-
	-	REVIS
	-	REVIS
	-	REVIS
	-	REVIS

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, SCHEDULE OF QUANTITIES
 AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
 IL ROUTE 53 AT DUPAGE BLVD / BAKER HILL DR**

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS. 8X19(066)	DUPAGE	58	33C
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



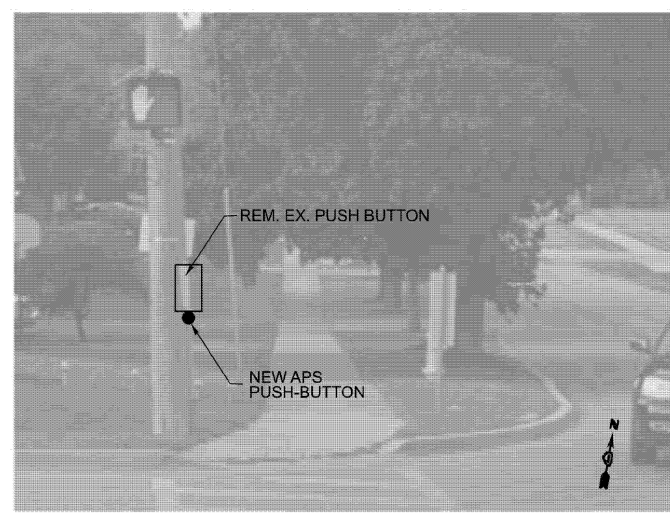
REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

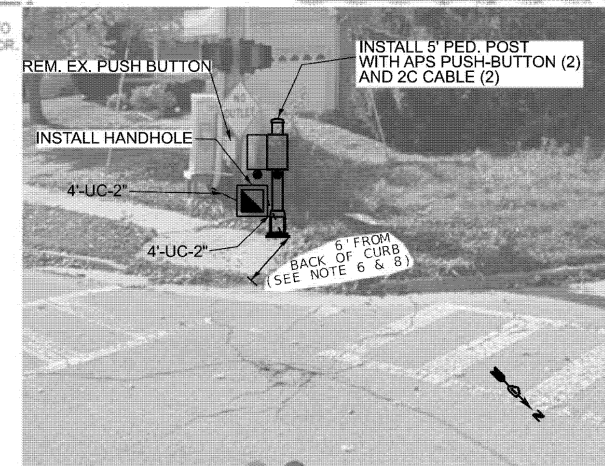
- 3 EACH PEDESTRIAN PUSH-BUTTON

NOTES:

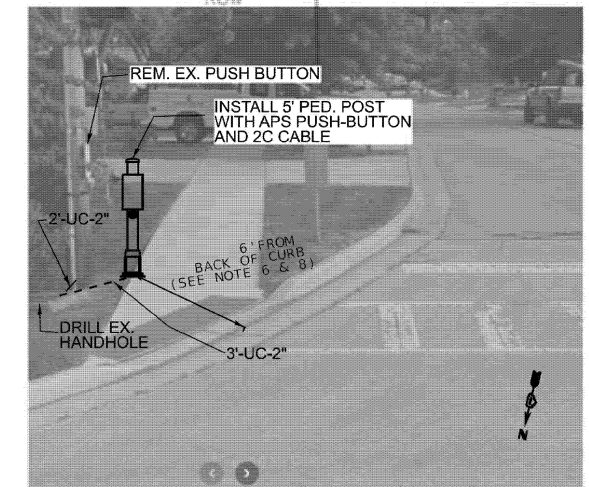
- 1. THE PROPOSED PEDESTRIAN SIGNAL POST, 5 FT (EACH 2) SHALL BE GROUNDED.



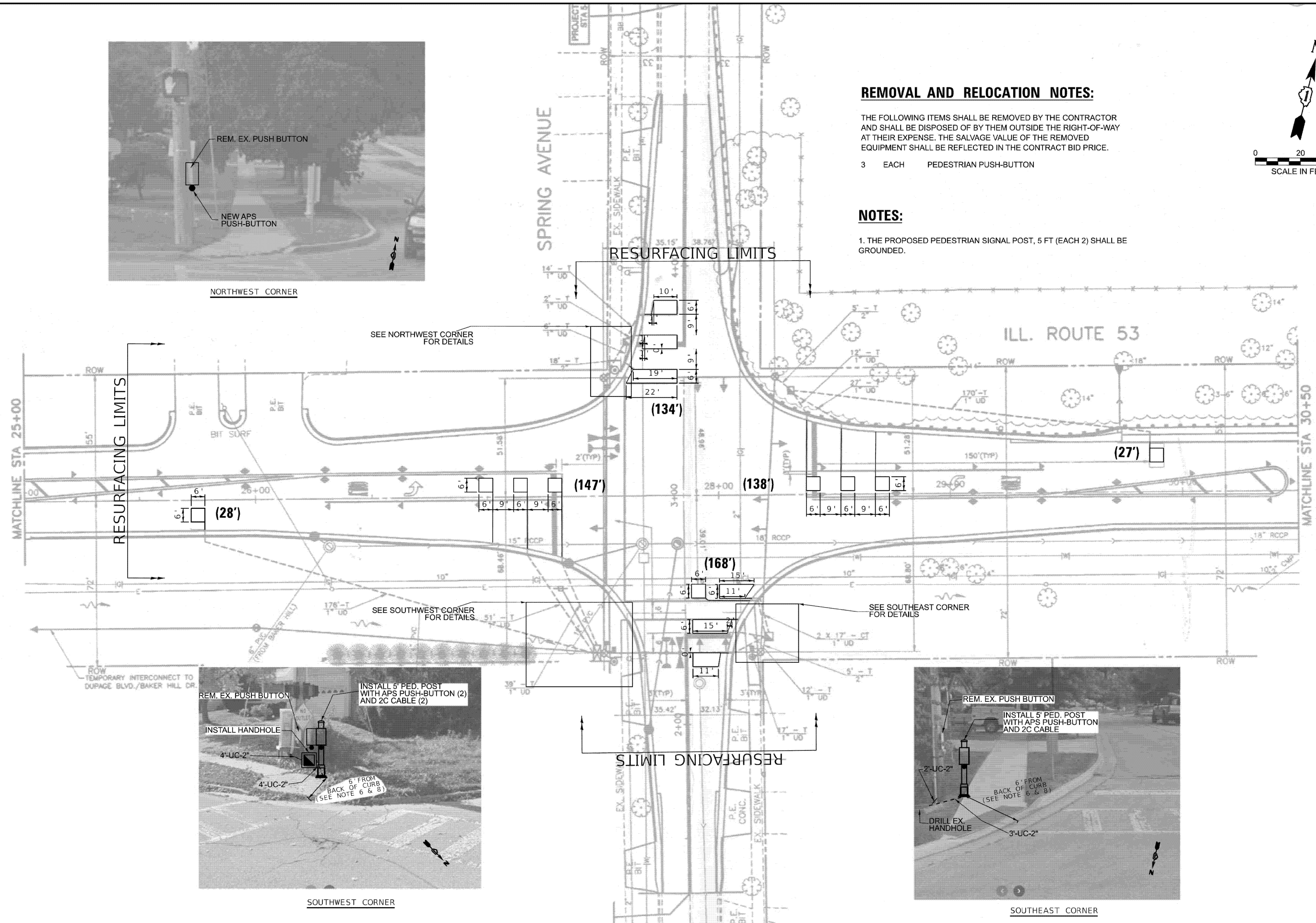
NORTHWEST CORNER



SOUTHWEST CORNER



SOUTHEAST CORNER



MODEL: Default
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USER NAME = SalmanAhmed.Shahnavaz	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 11/25/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 1 OF 2)
IL ROUTE 53 AT SPRING AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33D
CONTRACT NO. 80B25				
ILLINOIS FED. AID PROJECT				

TS 20333
ECON 104

NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.

3. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.

4. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).

5. ALL PUSH BUTTONS SHALL BE APS

6. NO PROPOSED PEDESTRIAN POST SHALL EXCEED 10 FT FROM BACK OF CURB

7. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED

8. THERE SHALL BE A MINIMUM OF 4' SIDEWALK CLEARANCE NEXT TO TRAFFIC SIGNAL FOUNDATIONS TO BE ADA COMPLIANT

**TS 20333
ECON 104**

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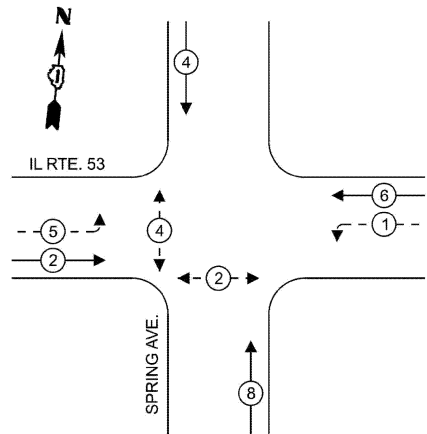
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN (SHEET 1 OF 2)
IL ROUTE 53 AT SPRING AVE.**

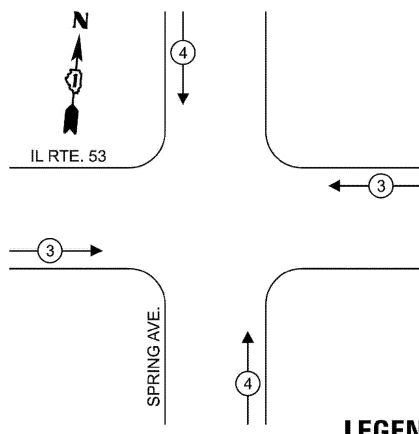
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33E
CONTRACT NO. 80B25				
		ILLINOIS	FED. AID PROJECT	

EXISTING CONTROLLER SEQUENCE



EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND:

- ← * → PROTECTED PHASE
- ← * - PROTECTED/PERMITTED PHASE
- ← * ▷ PEDESTRIAN PHASE

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA	FOOT	13
HANDHOLE	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	231
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	168
DRILL EXISTING HANDHOLE	EACH	1
DETECTOR LOOP, TYPE I	FOOT	642
REMOVE AERIAL CABLE	FOOT	143
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

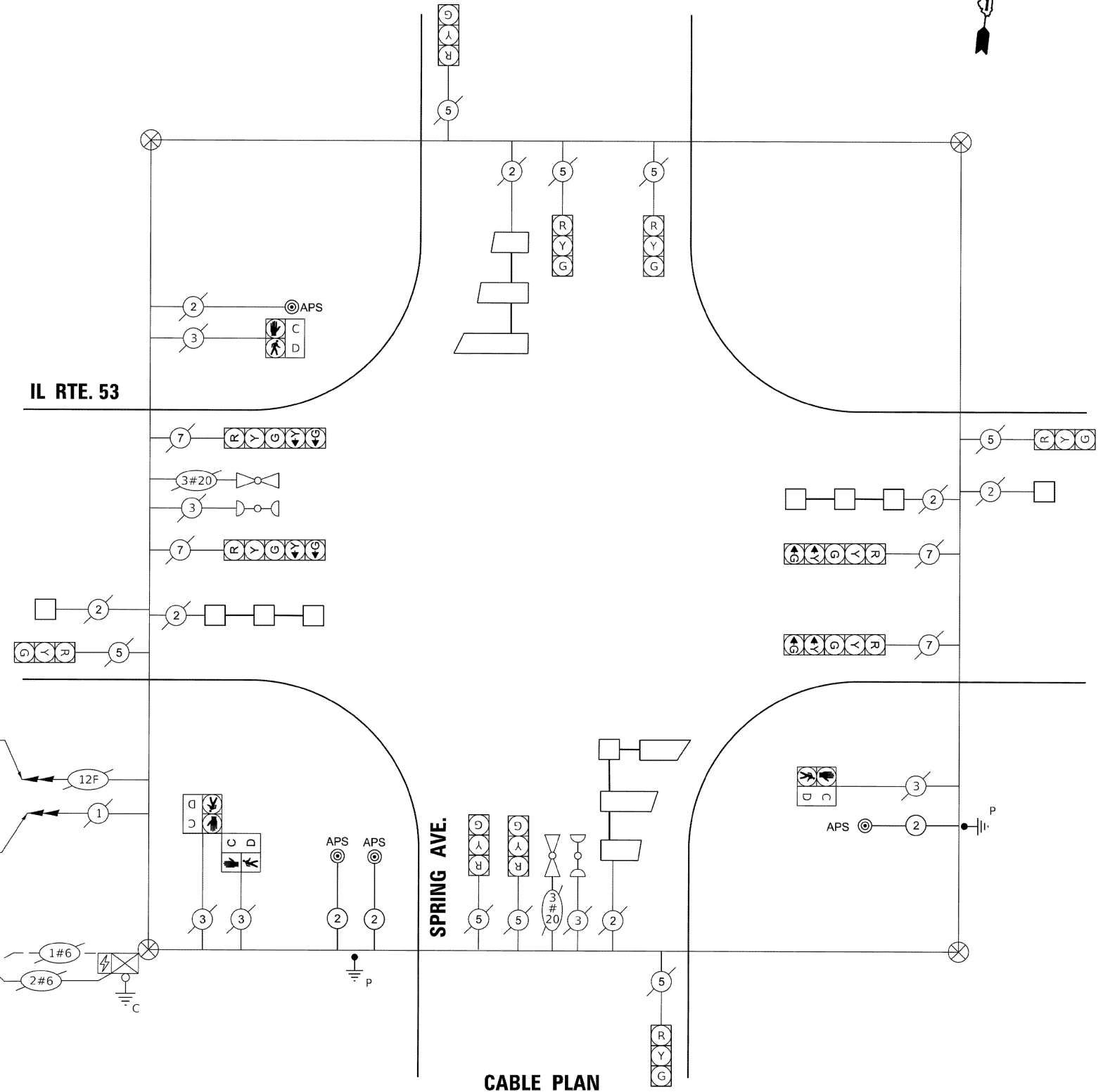
TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	8	11	88
4-SECTION	-	14	-
5-SECTION	4	13	52
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL CONTROLLER	4	15	60
MASTER CONTROLLER	1	150	150
UPS	-	100	-
DETECTION VIDEO	1	25	25
BLANK-OUT SIGN	-	20	-
NETWORK SWITCH II OR III	-	25	-
CELLULAR MODEM	-	35	-
PTZ CAMERA	-	15	-
	-	75	-
TOTAL UPS SIZING		375	
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING		980	

ENERGY COSTS TO:
VILLAGE OF GLEN ELLYN
 535 DUANE STREET
 GLEN ELLYN, IL 60137

ENERGY SUPPLY: CONTACT: ANTONIO RIOS
 PHONE: 630-696-6855
 COMPANY: COMED
 ACCOUNT NUMBER: 32544-65000
 METER NUMBER: ---

IL RTE. 53



CABLE PLAN

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, SCHEDULE OF QUANTITIES
 AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
 IL ROUTE 53 AT SPRING AVE.**

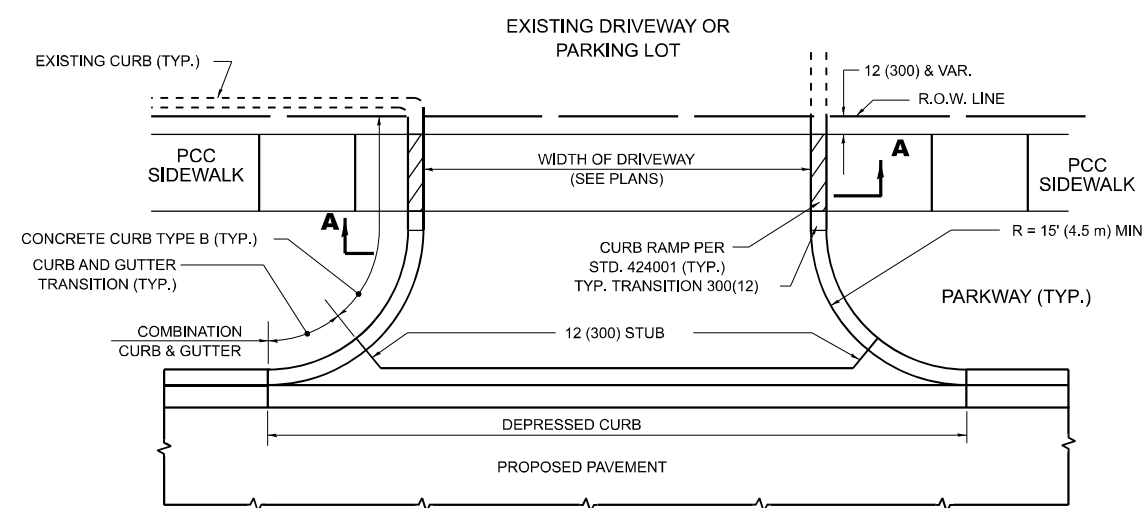
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	33F

CONTRACT NO. 80B25

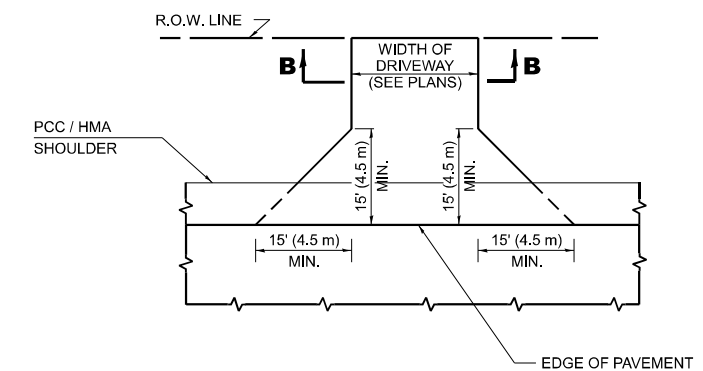
ILLINOIS FED. AID PROJECT

**TS 20333
 ECON 104**

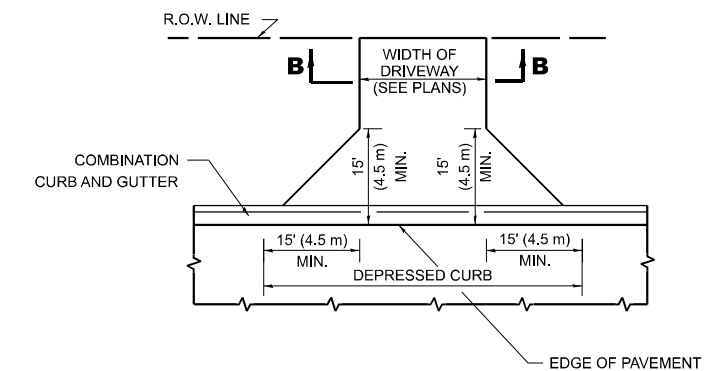
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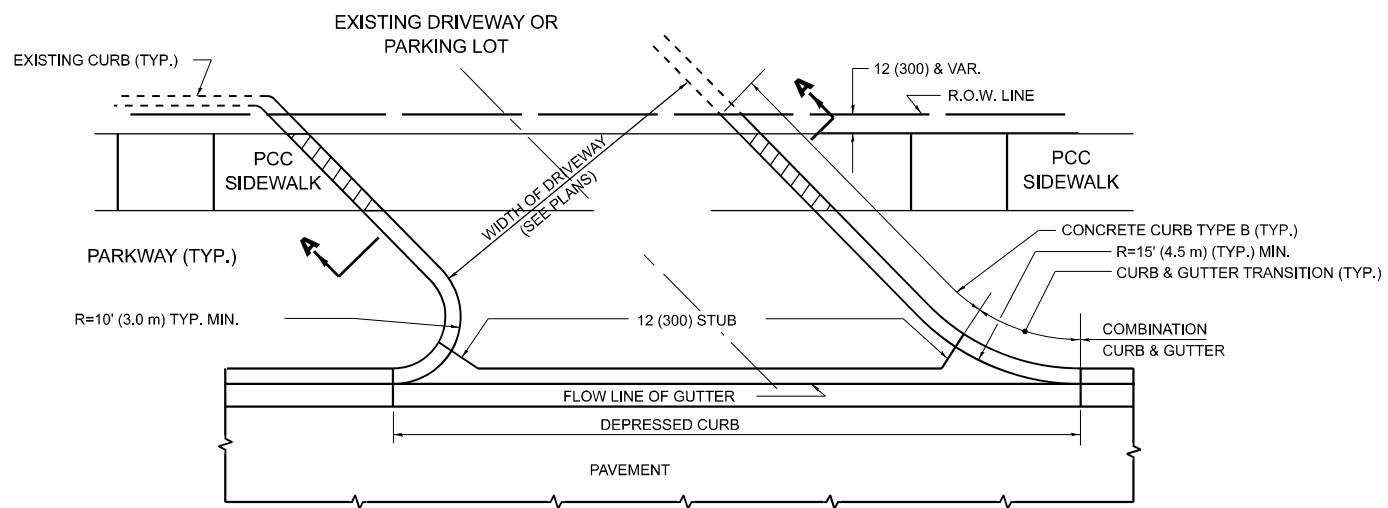
WITH CONCRETE CURB, TYPE B



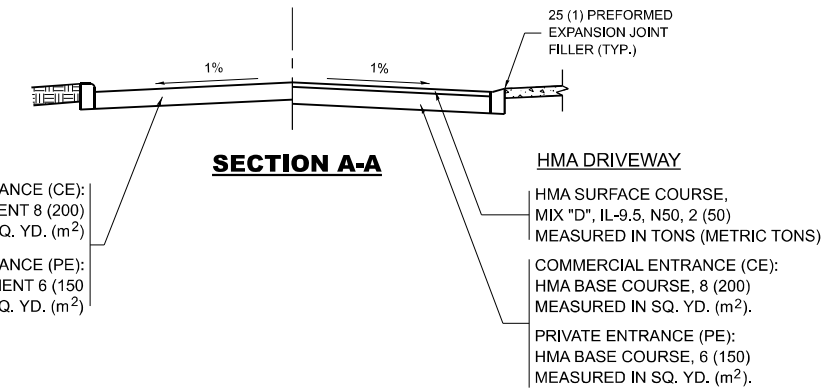
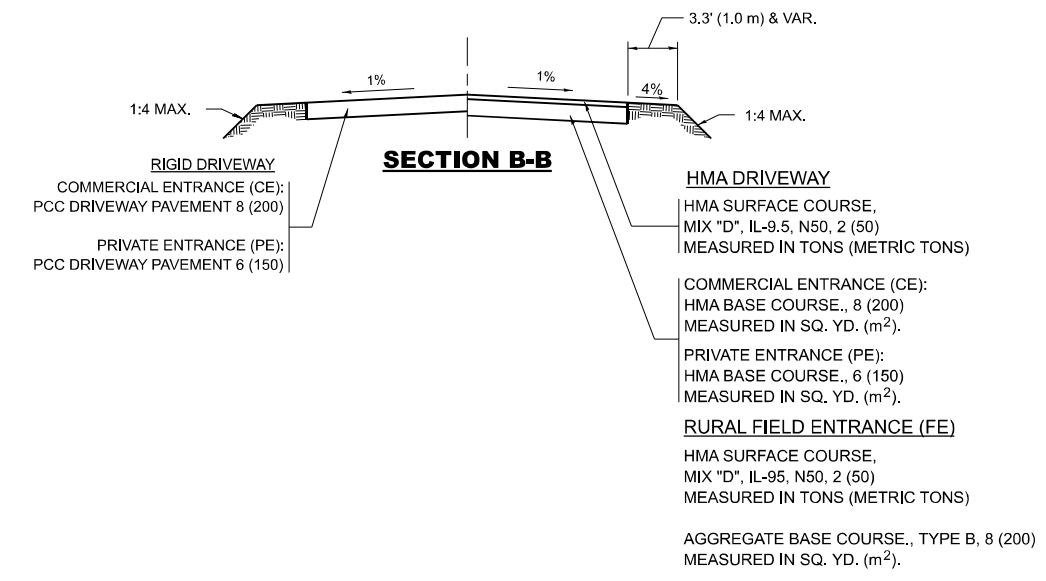
ADJACENT TO PCC / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B



GENERAL NOTES

1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.
2. COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

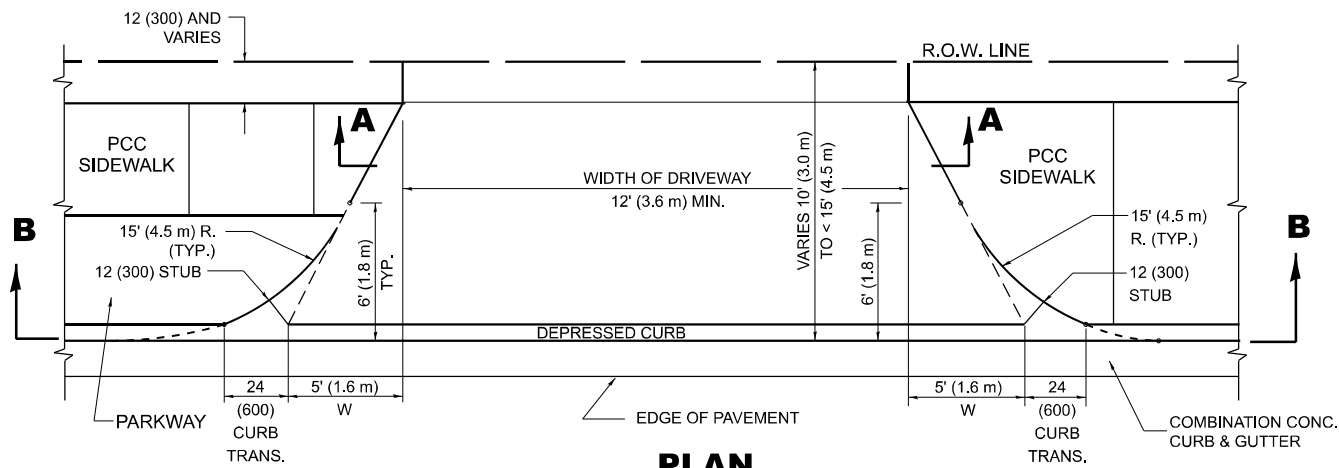
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USER NAME = Aya.Elkhallib	DESIGNED - R. SHAH	REVISED - R. BORO 06-11-08
	DRAWN - R. BORO 09-06-11	REVISED - K. SMITH 08-28-19
	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 10/20/2025	DATE - 11-04-95	

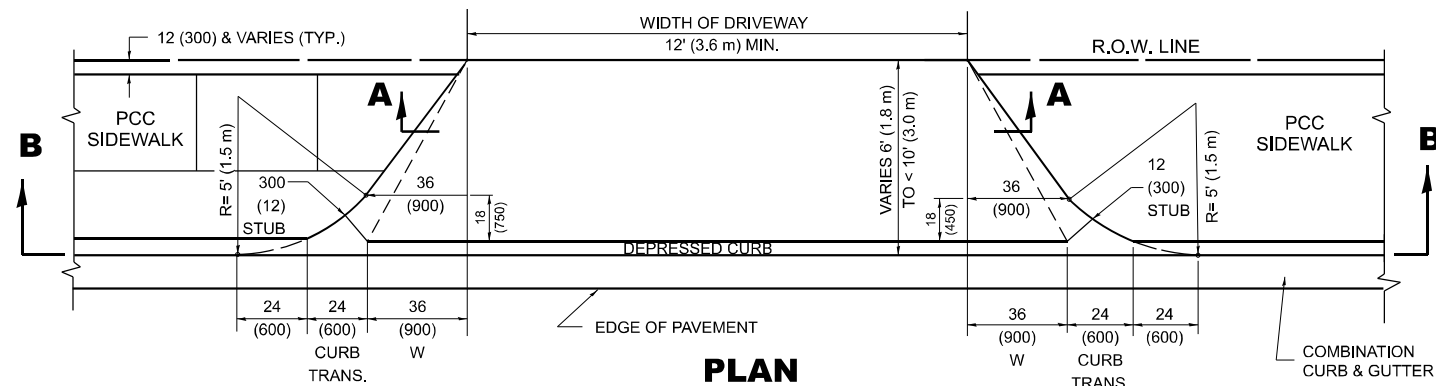
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >15'(4.5m)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

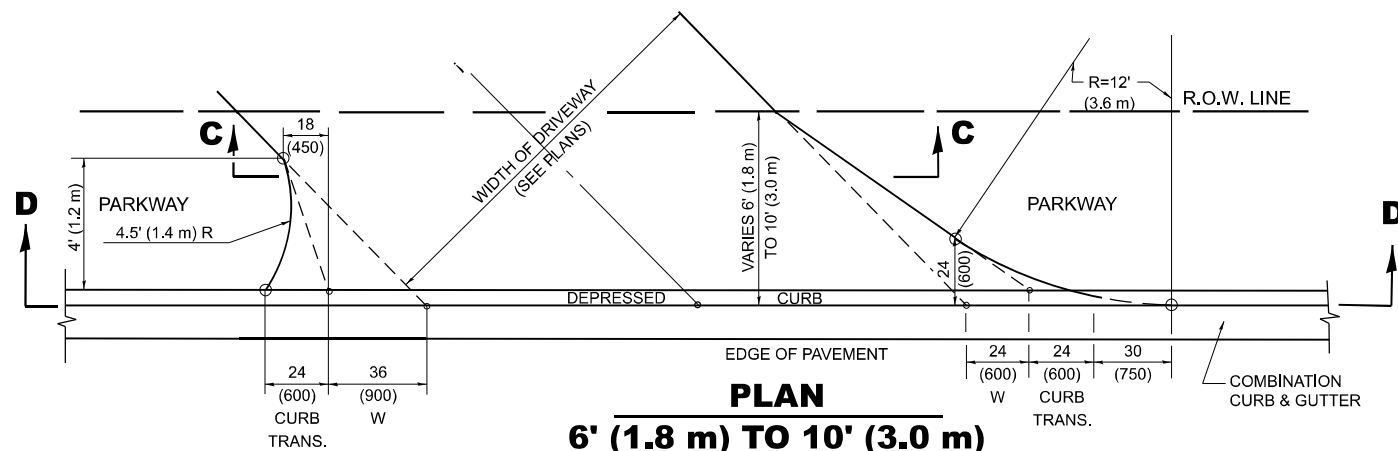
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-01 (BD-01)			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



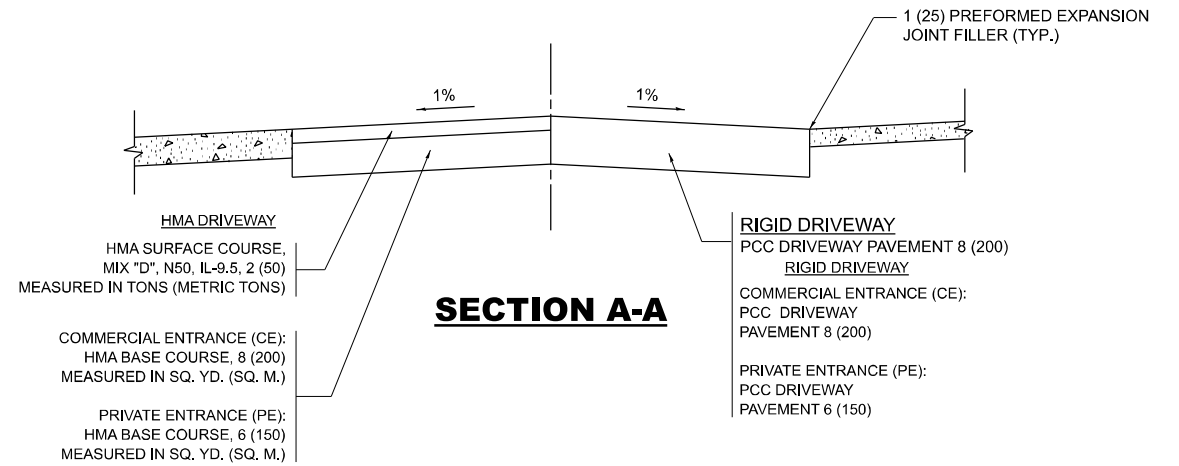
PLAN
10' (3.0 m) TO < 15' (4.5 m)



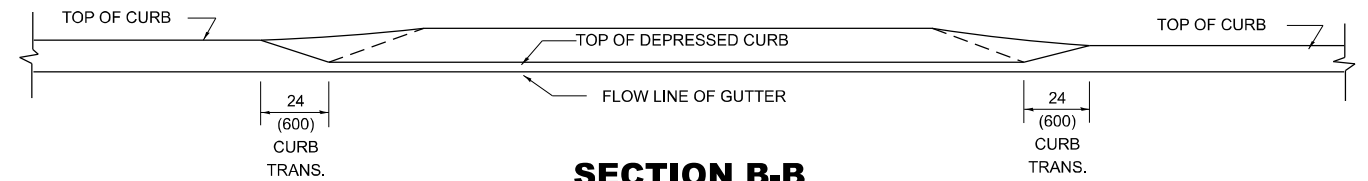
PLAN
6' (1.8 m) TO < 10' (3.0 m)



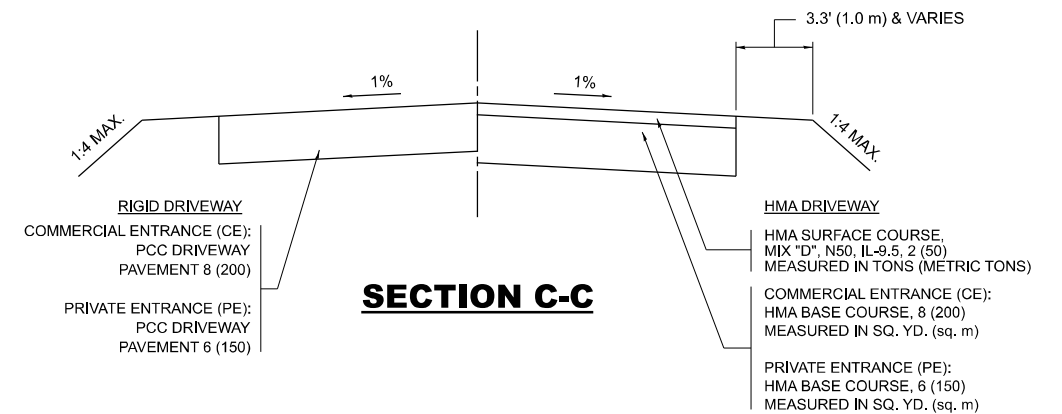
PLAN
6' (1.8 m) TO 10' (3.0 m)



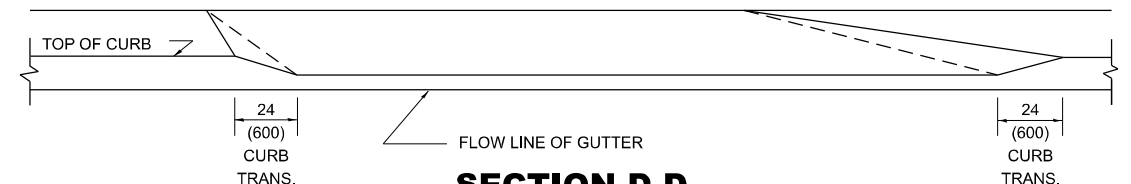
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
- WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
- "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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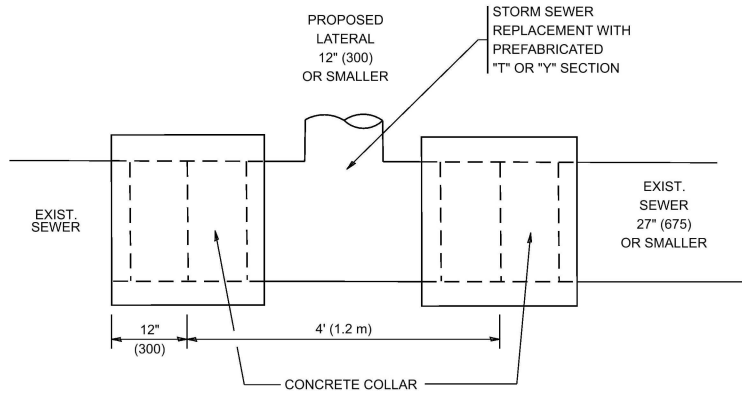
USER NAME = Aya.Elkhailb	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN - R. BORO 09-06-11	REVISED - K. SMITH 08-27-19
	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 10/20/2025	DATE - 11-06-95	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)

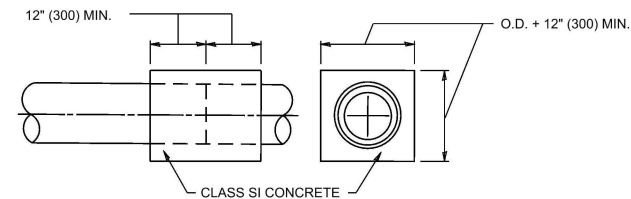
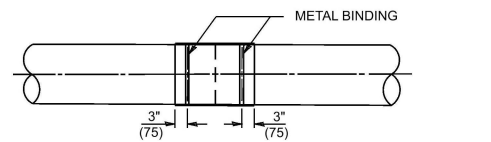
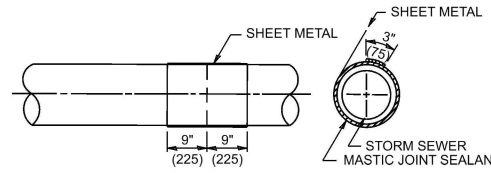
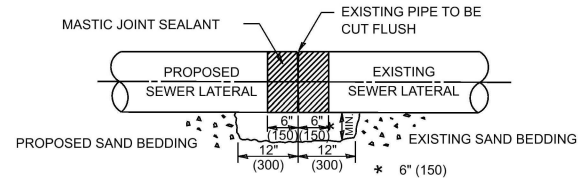
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-01 (BD-02)			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



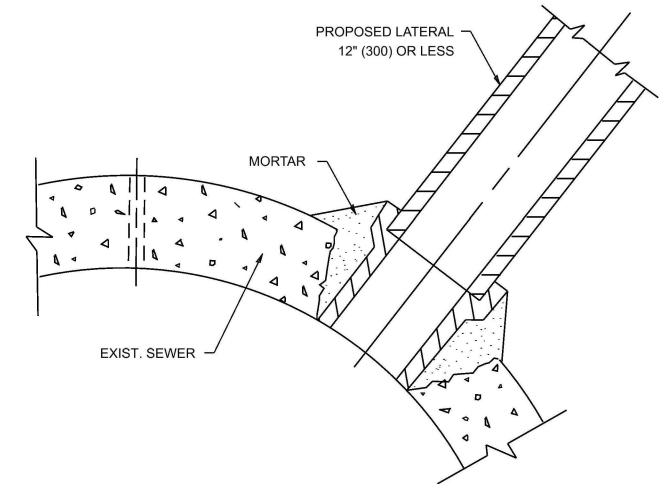
DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



DETAIL "B"

CLASS SI CONCRETE COLLAR



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

1. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

- A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
- B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

1. CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
2. CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

1. TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.
2. REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.
3. TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.
4. CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Aya.Eikhatib	DESIGNED - M. DE YONG	REVISED - R. SHAH 09-09-94
	DRAWN -	REVISED - R. SHAH 10-25-94
	CHECKED -	REVISED - R. SHAH 06-12-96
PLOT DATE = 10/20/2025	DATE - 07-25-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

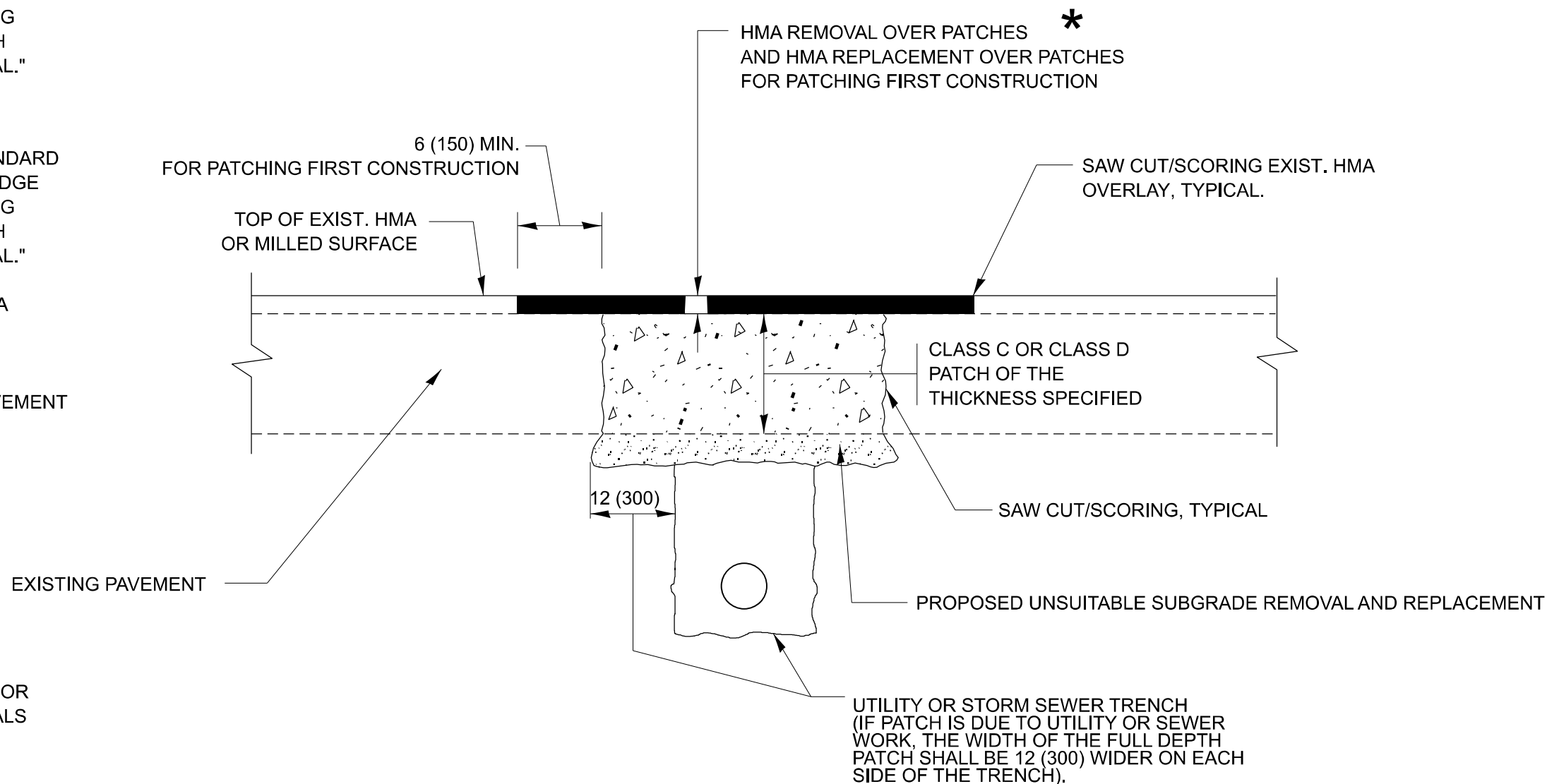
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	36
BD500-01 (BD-07)			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 ½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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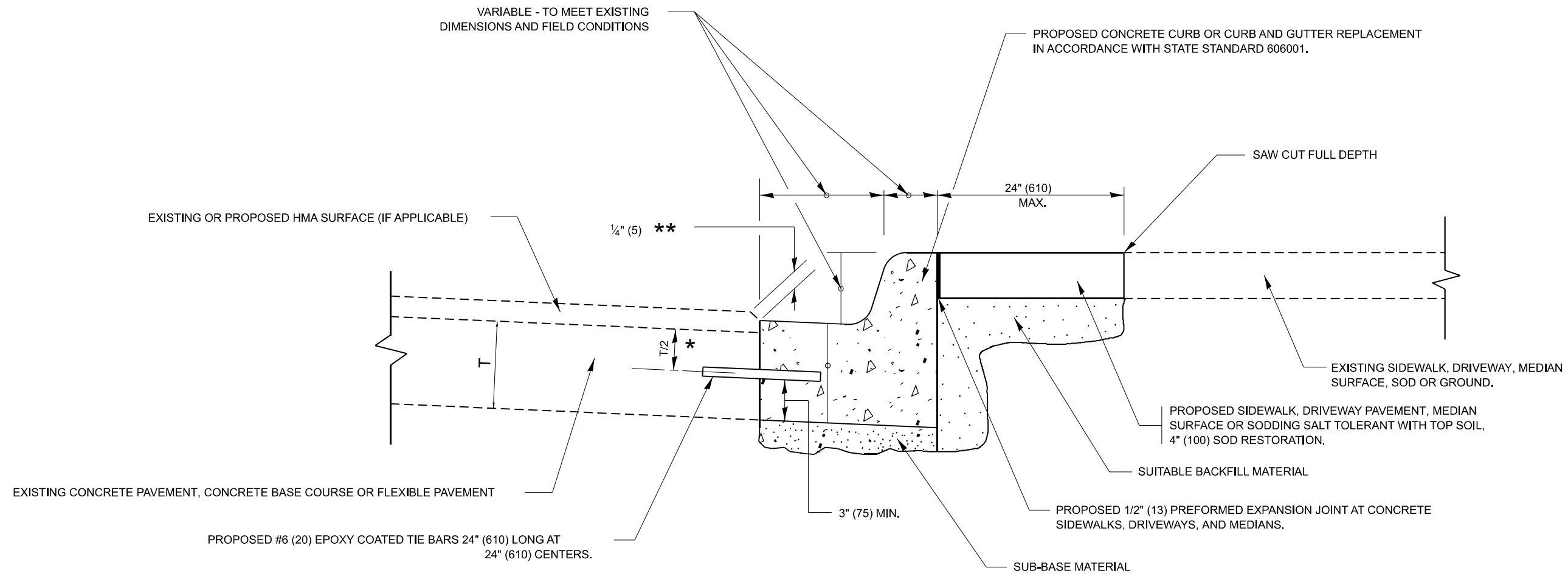
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	DRAWN -	REVISED - R. BORO 09-04-07
	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 10/20/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	38
BD400-04 (BD-22)			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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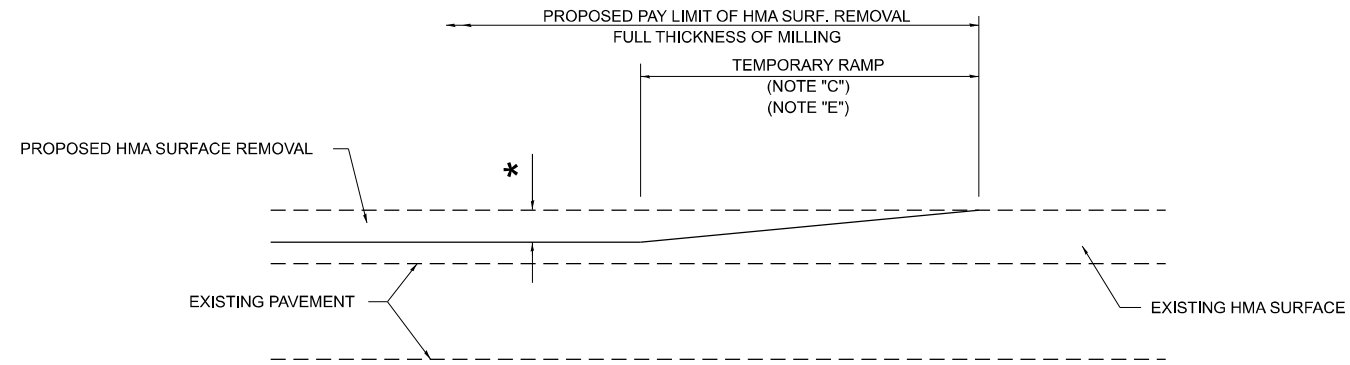
USER NAME = Aya.Elkhalti	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 01-22-01
	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 10/20/2025	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

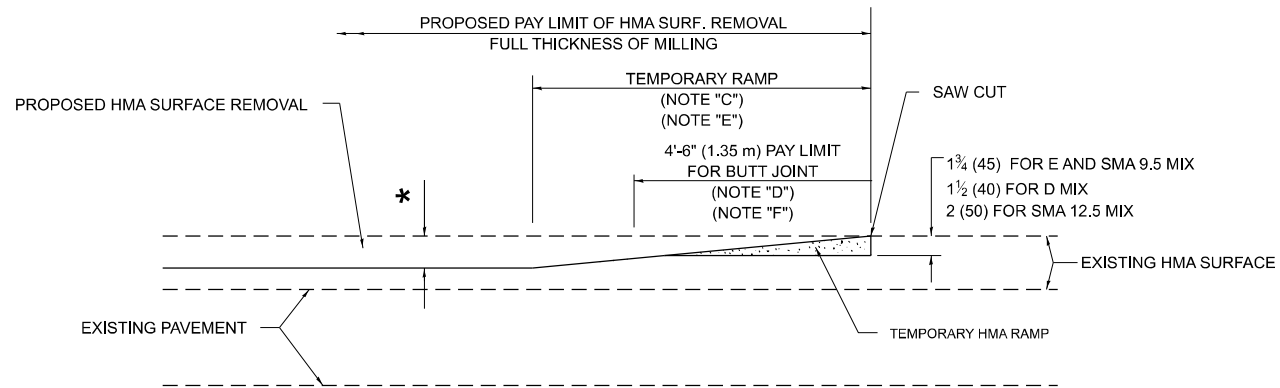
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870	2025-2006-RS, 8X19(066)	DUPAGE	56	39
BD600-06 (BD-24)			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

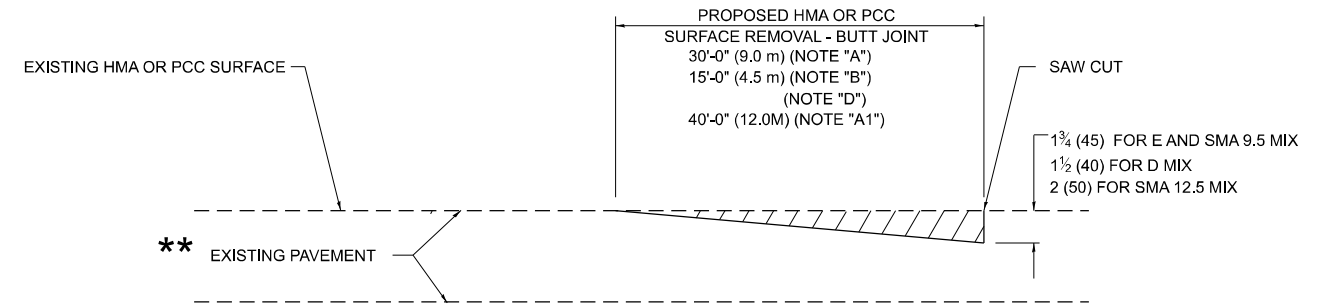


HMA CONSTRUCTED TEMPORARY RAMP

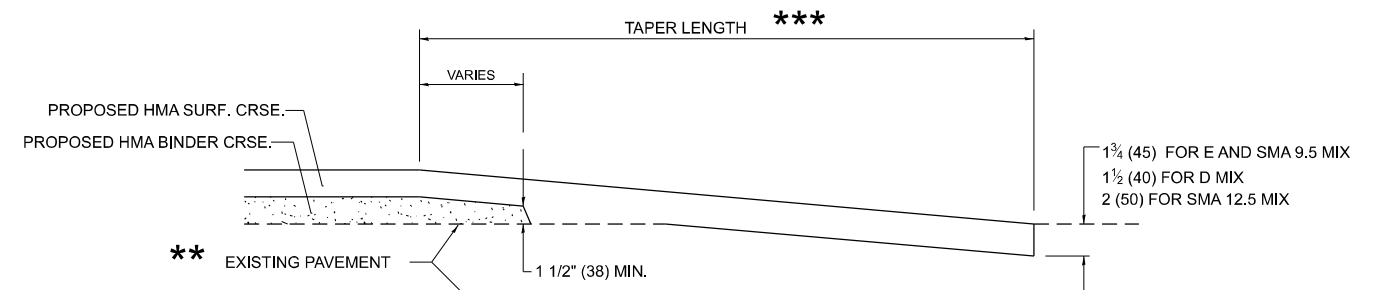
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



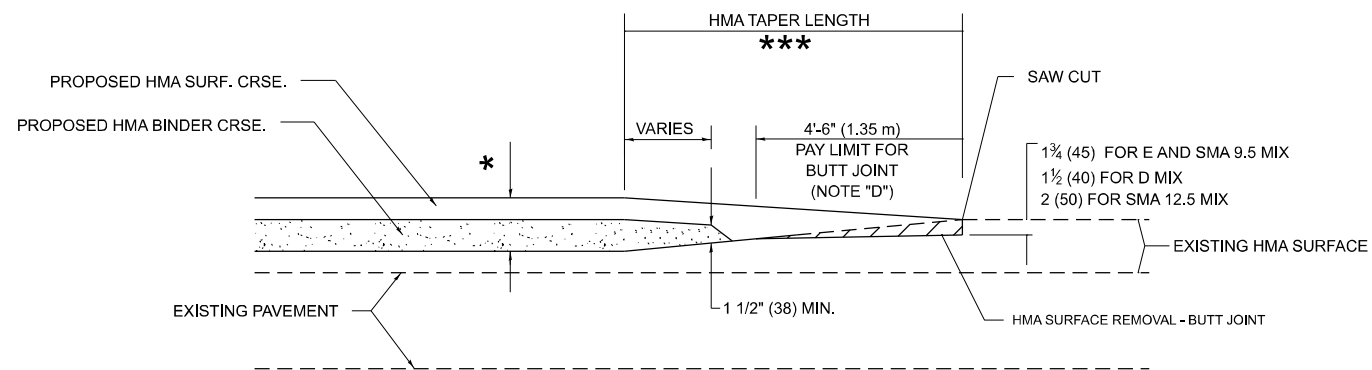
BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Aya,Elkhalib	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/20/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

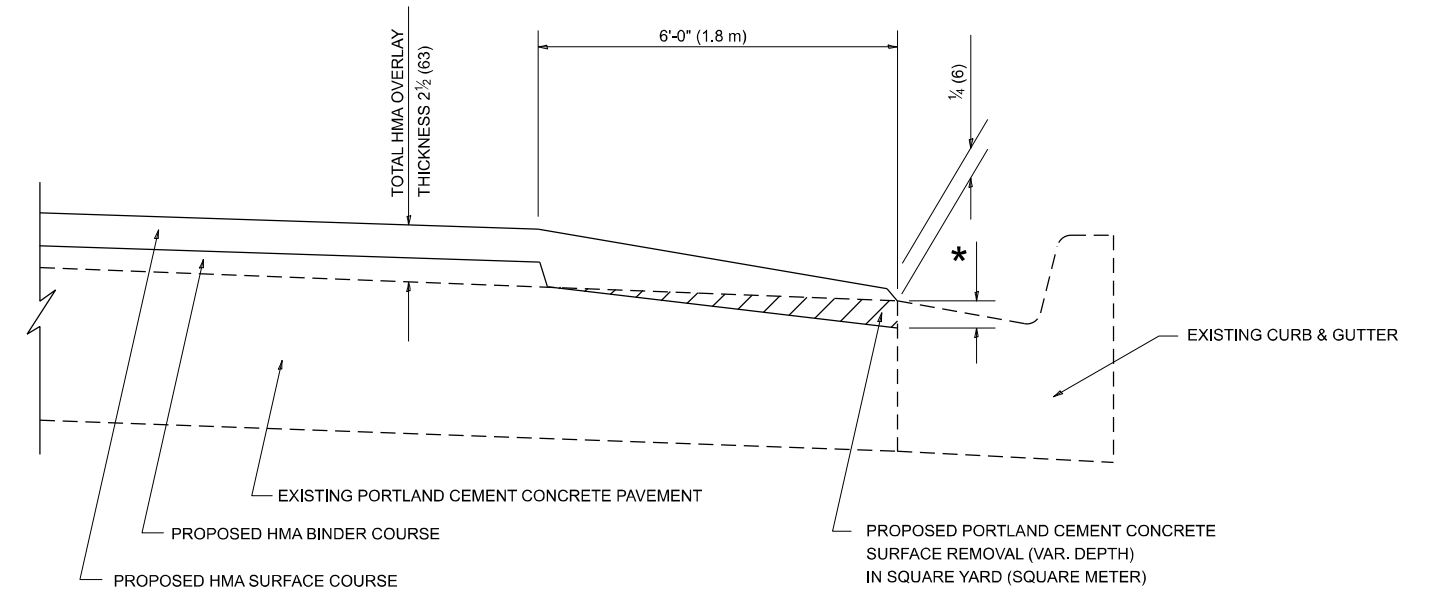
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	40
BD400-05 BD-32			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

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HMA TAPER AT EDGE OF P.C.C. PAVEMENT

HMA SURFACE COURSE	HMA BINDER COURSE	* MILLING AT GUTTER FLAG	
MIX	THICKNESS	THICKNESS	
D	1½ (38)	1 (25)	1¼ (33)
E OR SMA 9.5	1¾ (44)	¾ (19)	1½ (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

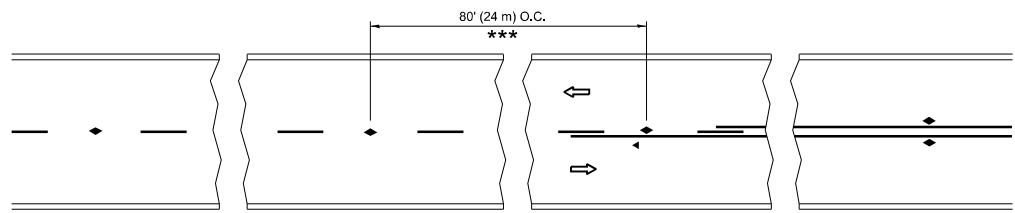
USER NAME = Eric.L.thomas	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07
	DRAWN - JIS	REVISED - JP CHANG 07-08-16
	CHECKED - A. ABBAS	REVISED - K. SMITH 02-01-22
PLOT DATE = 12/2/2024	DATE - 09-10-94	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C. PAVEMENT

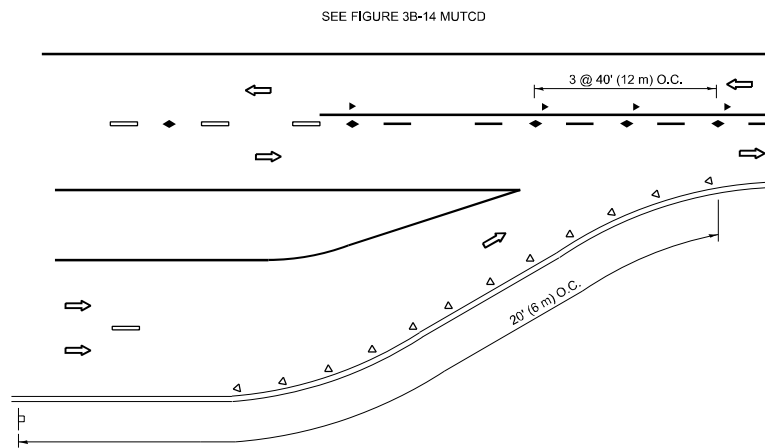
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-06 BD-33		CONTRACT NO. 80B25		
ILLINOIS FED. AID PROJECT				

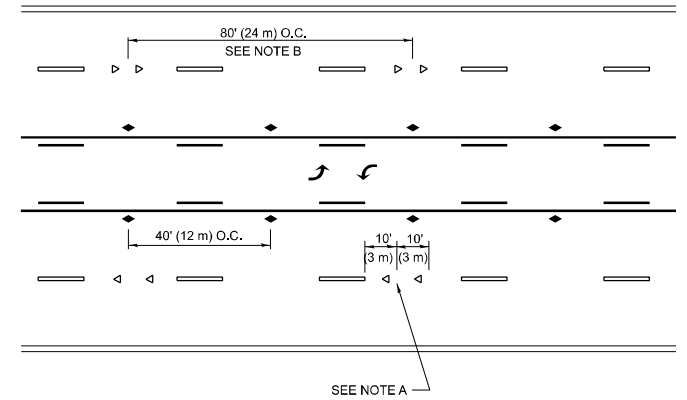


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

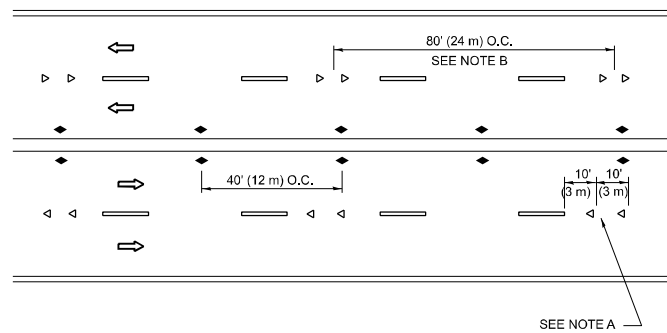
TWO-LANE/TWO-WAY



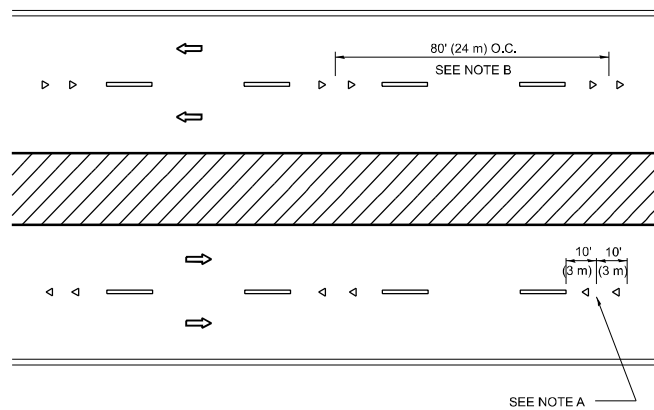
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

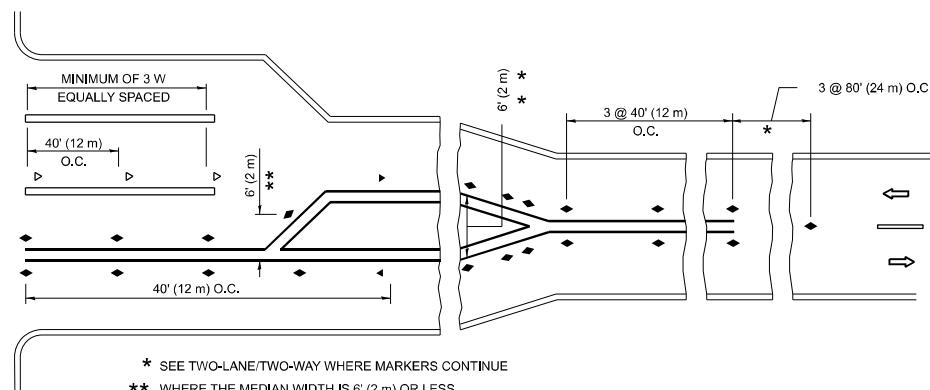
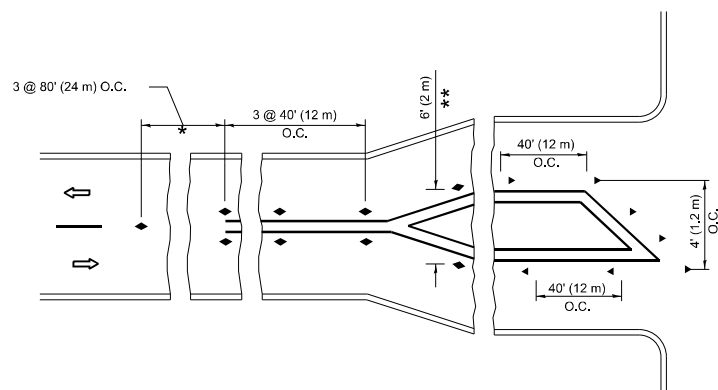
- YELLOW STRIPE
- WHITE STRIPE
- ◄ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = Aya.Elkhailb	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 10/20/2025	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS_8X19(066)	DUPAGE	56	42
TC-11			CONTRACT NO. 80B25	
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

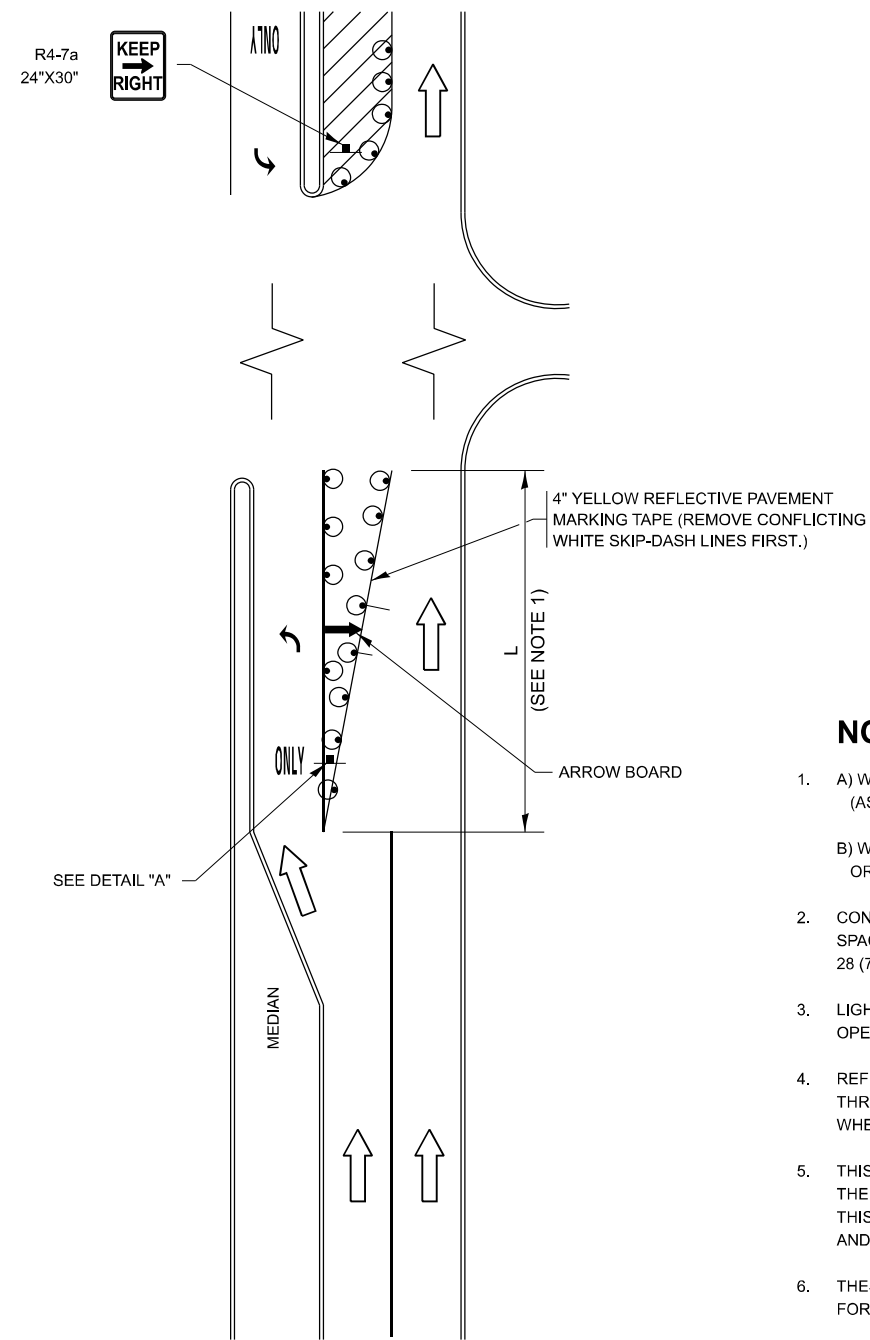


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

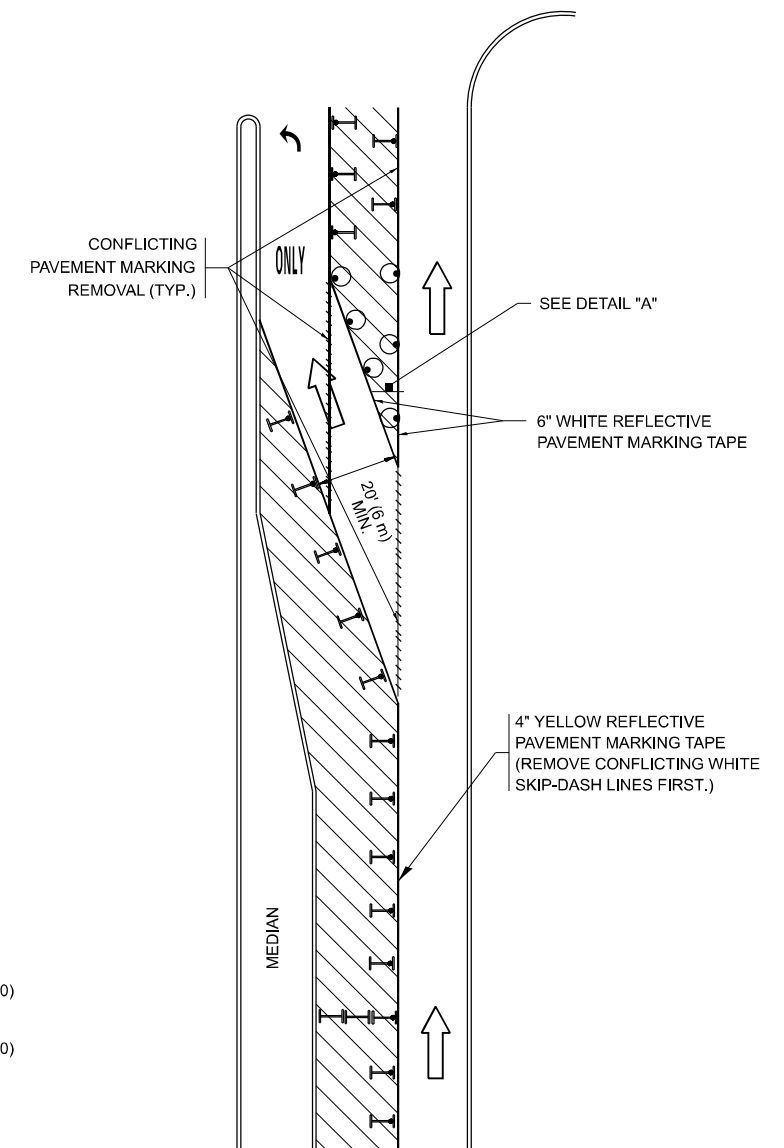
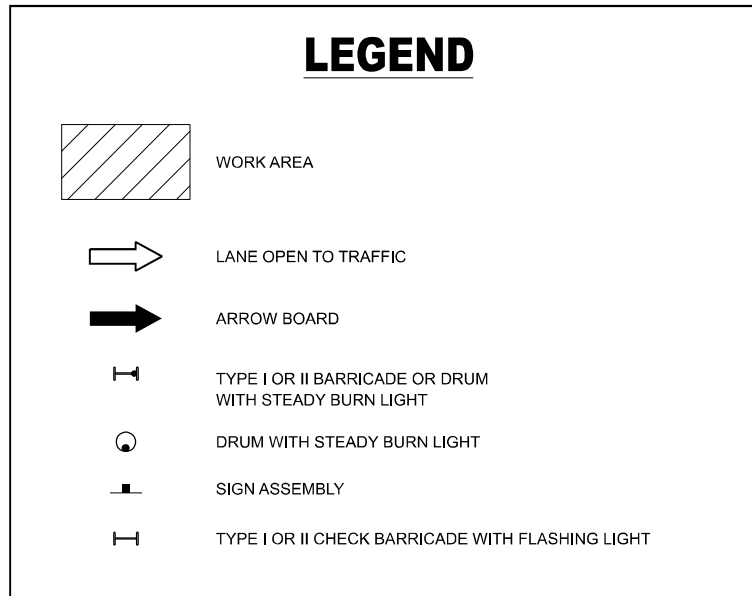
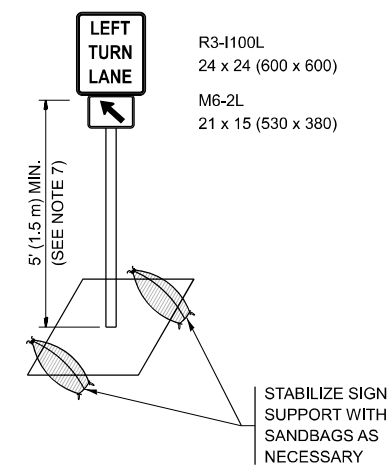


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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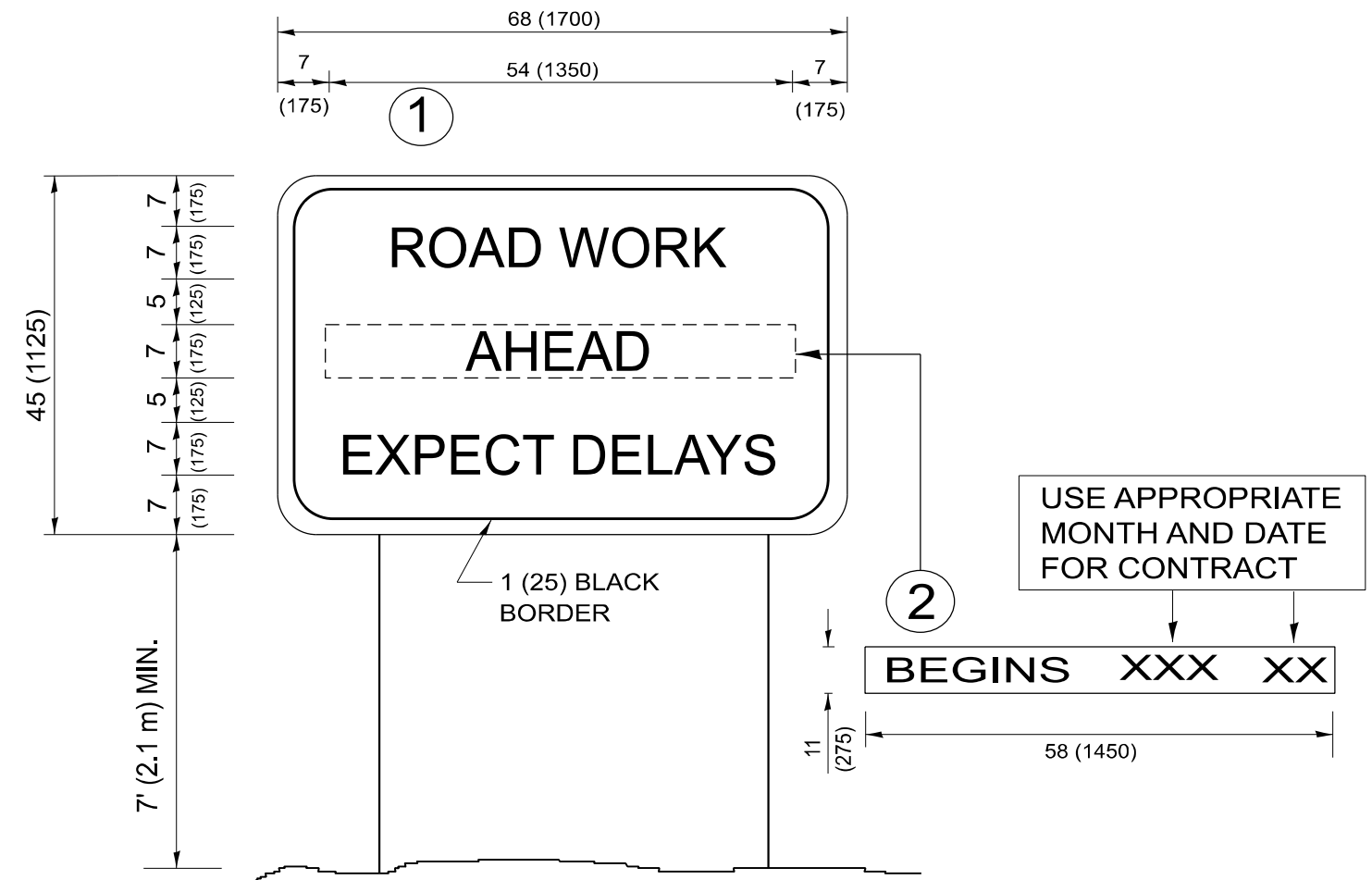
USER NAME = Aya.Elkhailb	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 10/20/2025	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)		56	44
TC-14			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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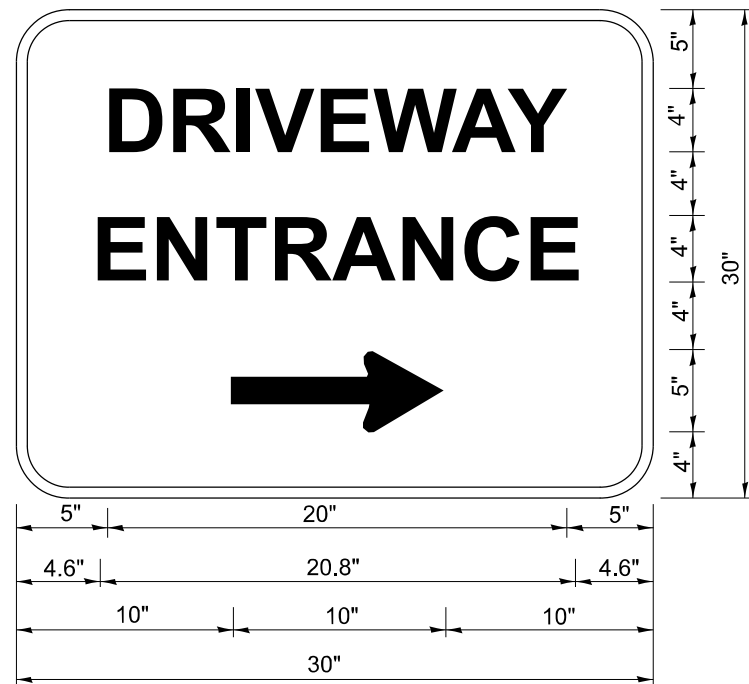
USER NAME = Aya,Elkhalib	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 10/20/2025	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	46
TC-22			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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USER NAME = Aya,Elkhalib	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	47
TC-26			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

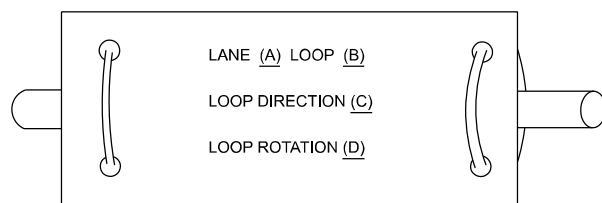
ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM	S	SP	FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM	I	IP	GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM		R			
SIGNAL HEAD			RELOCATE ITEM		RL			
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM		A			
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF			
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF			
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF			
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

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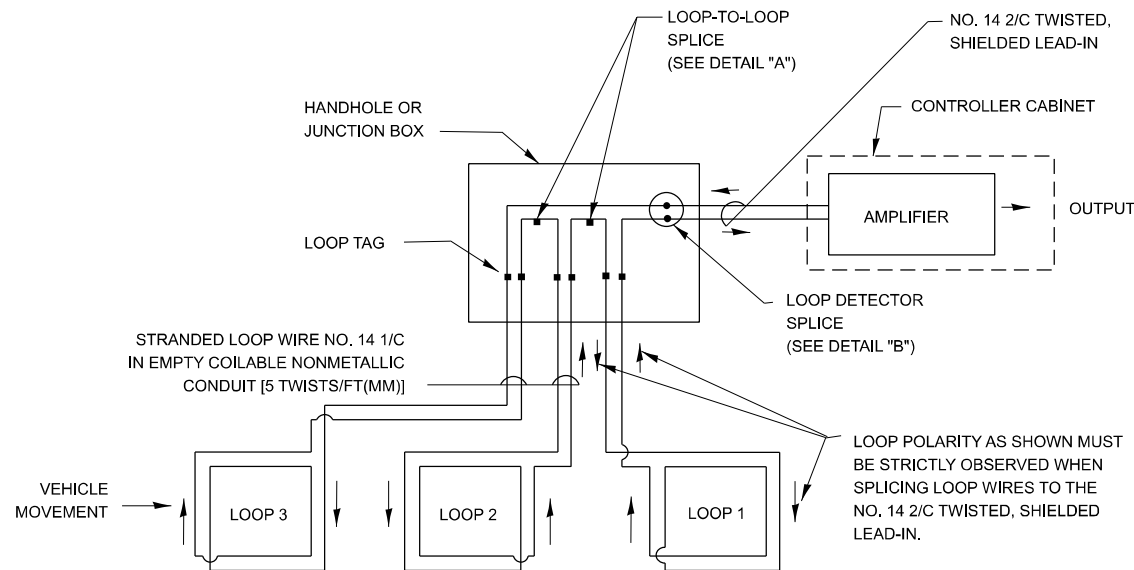
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

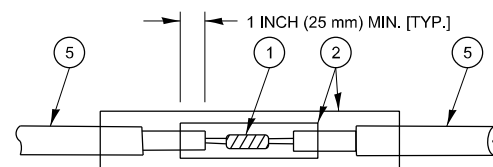


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

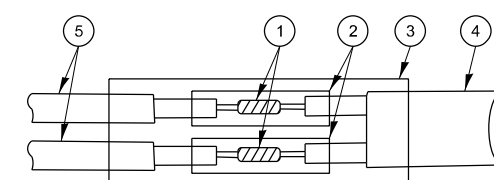


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

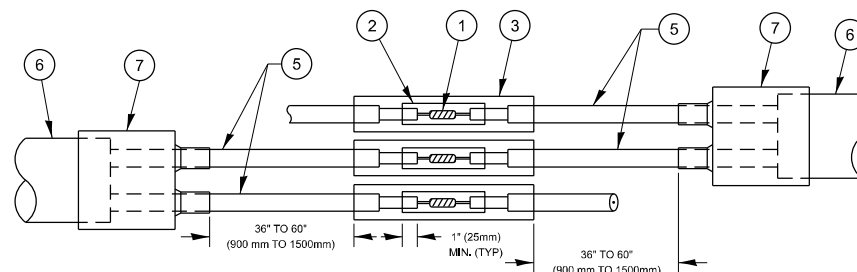


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

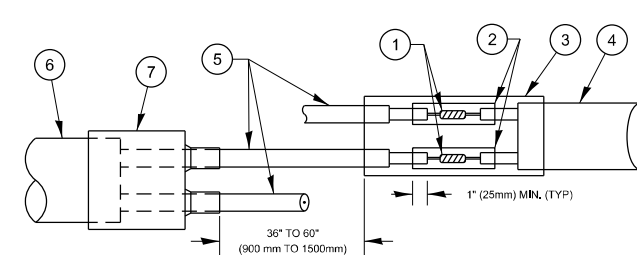


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR
- BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

MODEL: TS-05b (Sheet)
FILE NAME: c:\p\work\project\hsh\hsh\1076189\118825-shh-DistSds2.dgn

USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

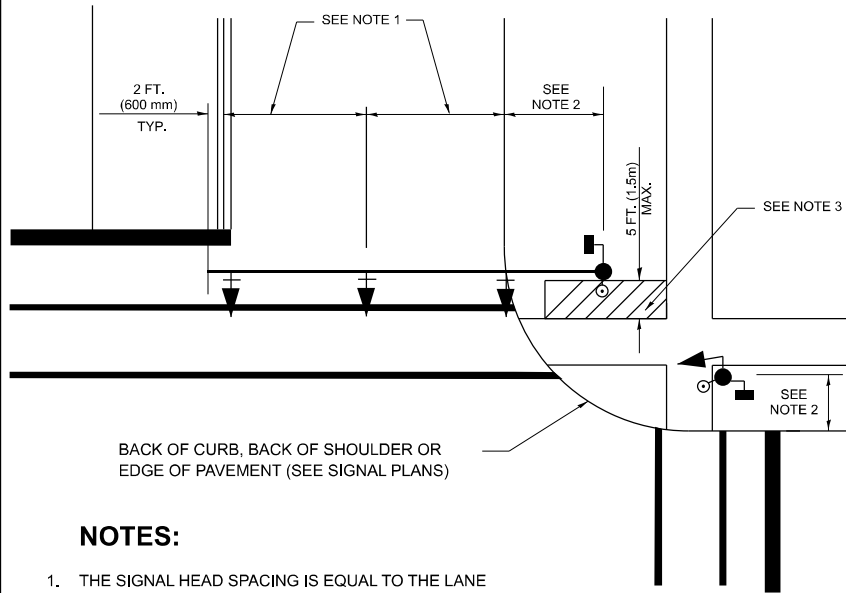
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	50
TS-05			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

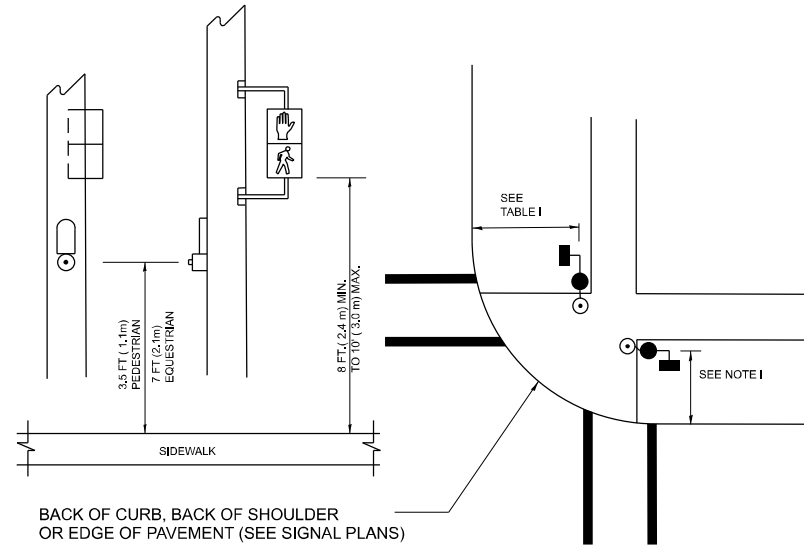
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

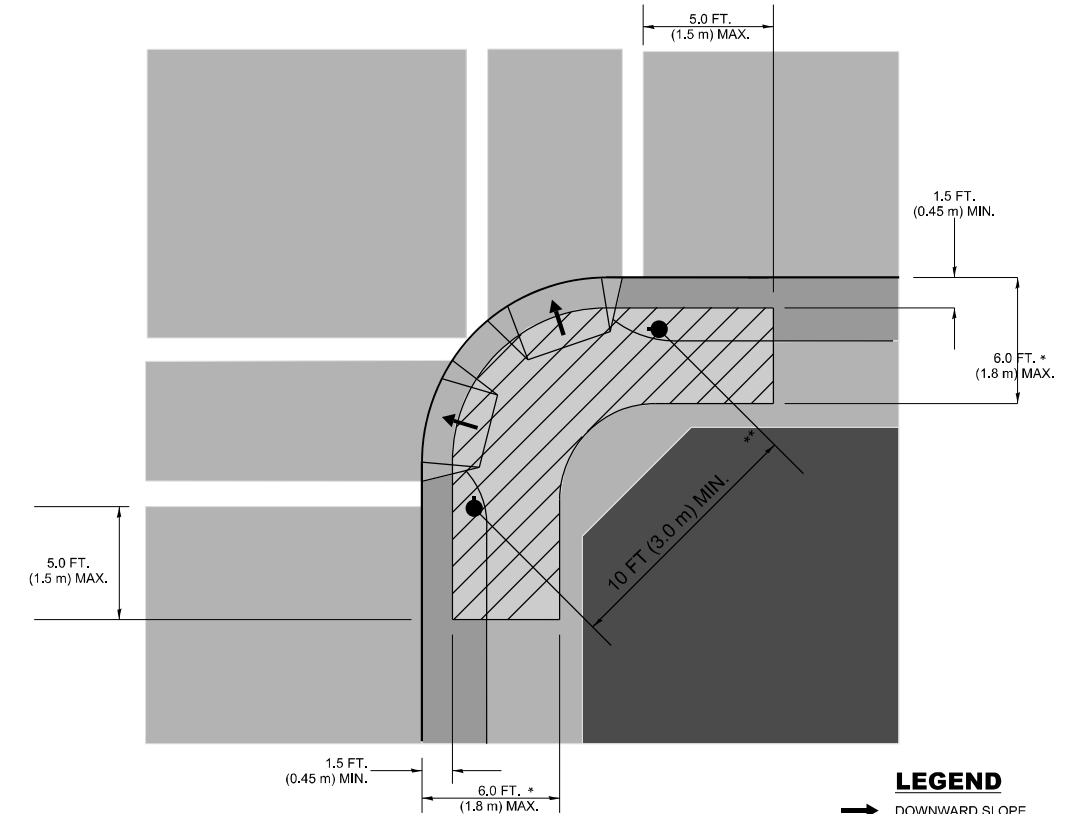
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

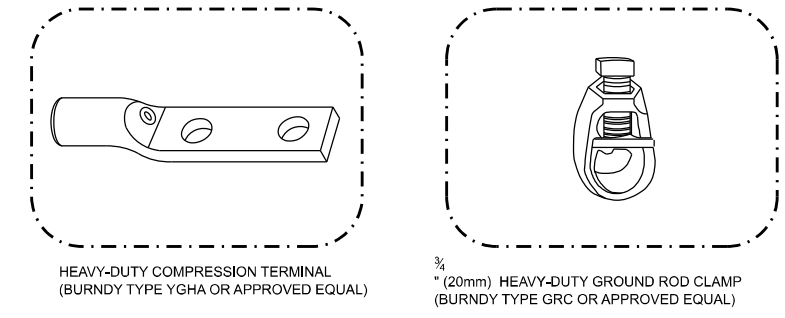
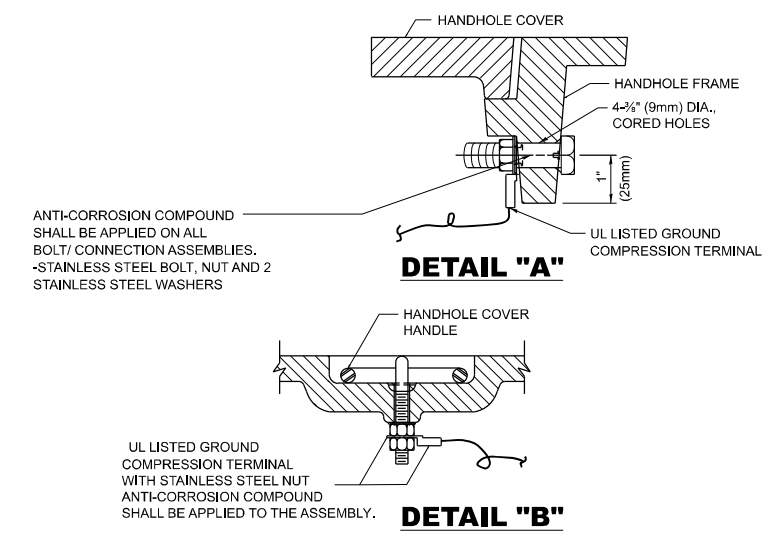
SCALE: NONE SHEET 3 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

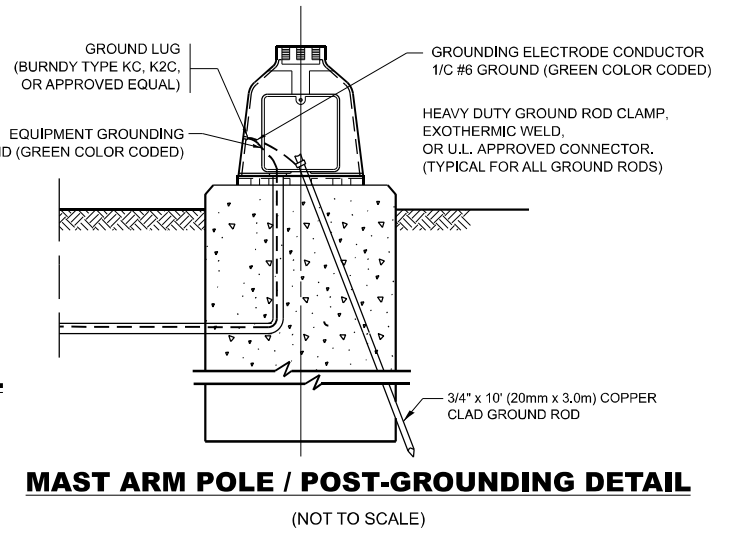
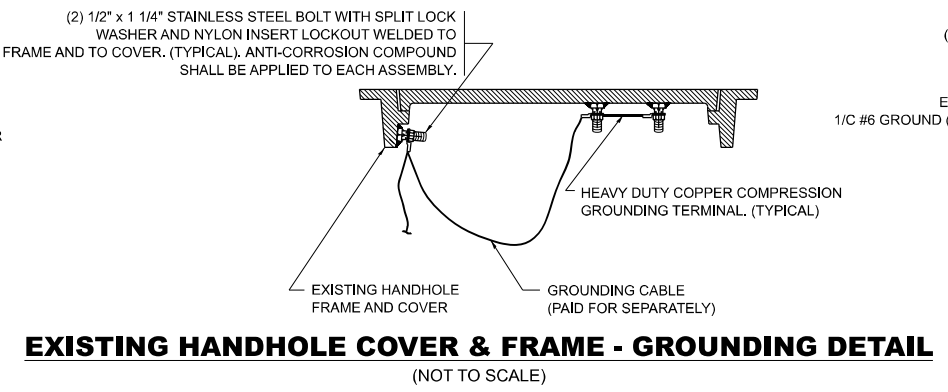
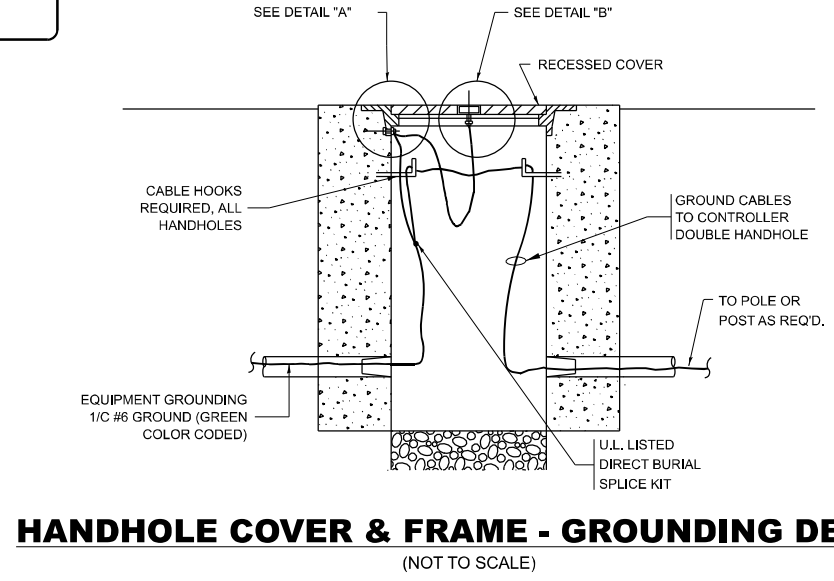
USER NAME = Aya.Elkhailb	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

NOTES:
GROUNDING SYSTEM

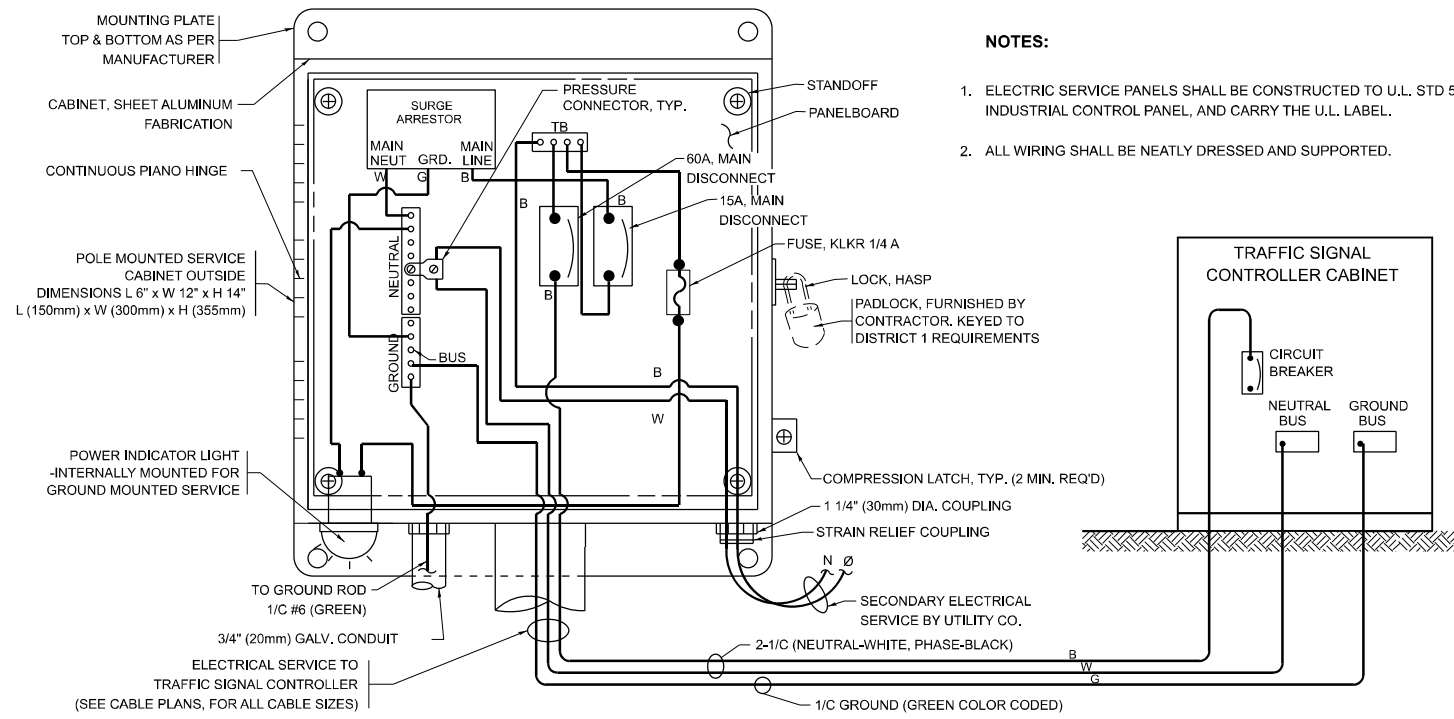
- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



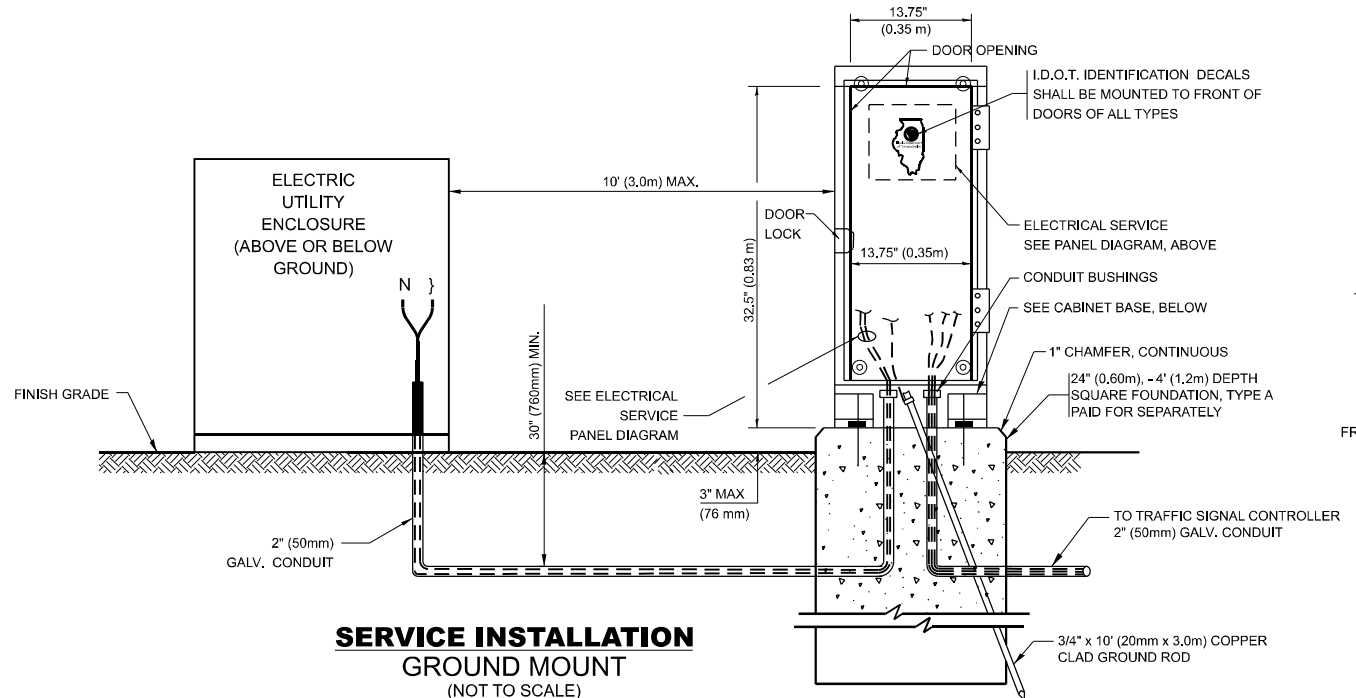
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



- NOTES:**
- ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
 - ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

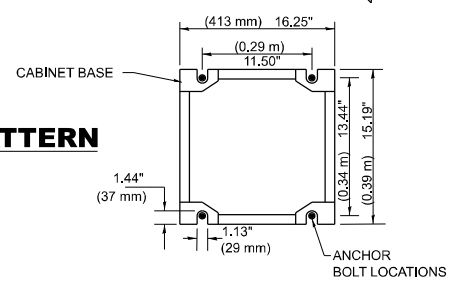


ELECTRICAL SERVICE - PANEL DIAGRAM
(TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET - BASE BOLT PATTERN
(NOT TO SCALE)



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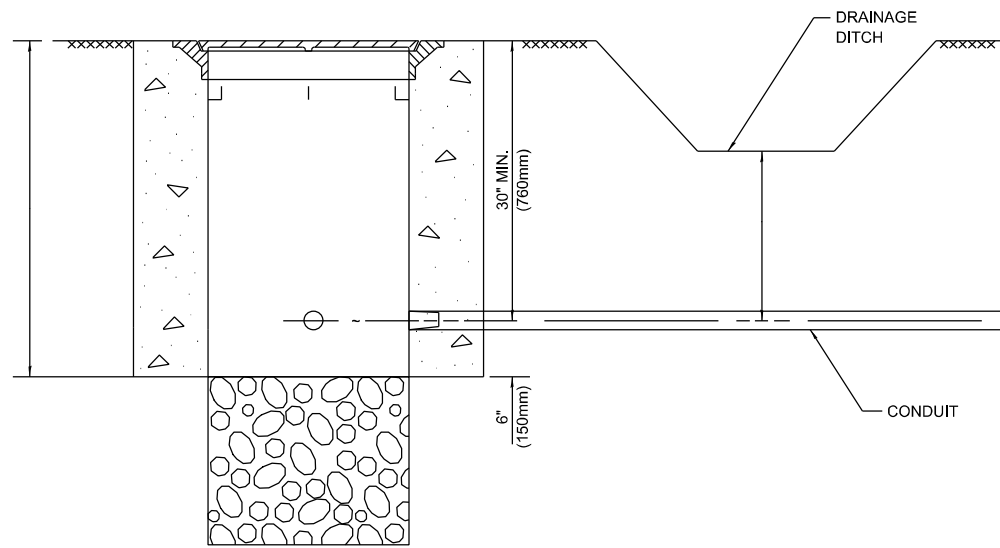
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	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

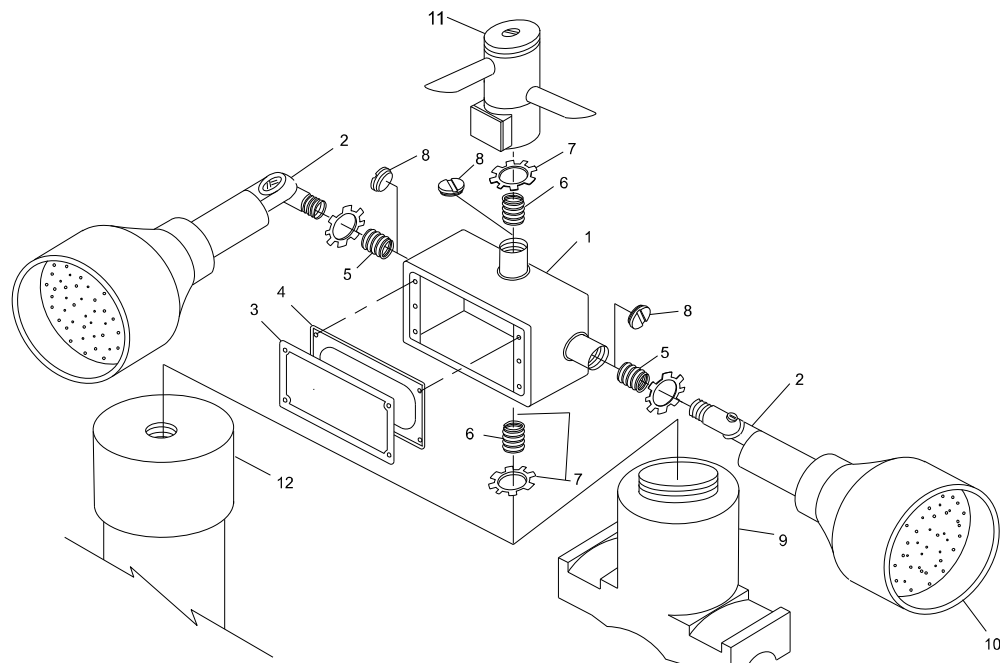
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05		CONTRACT NO. 80B25		
ILLINOIS FED. AID PROJECT				



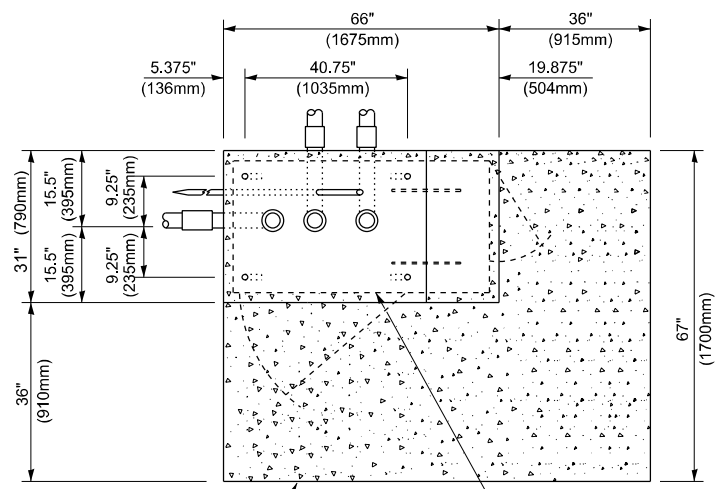
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

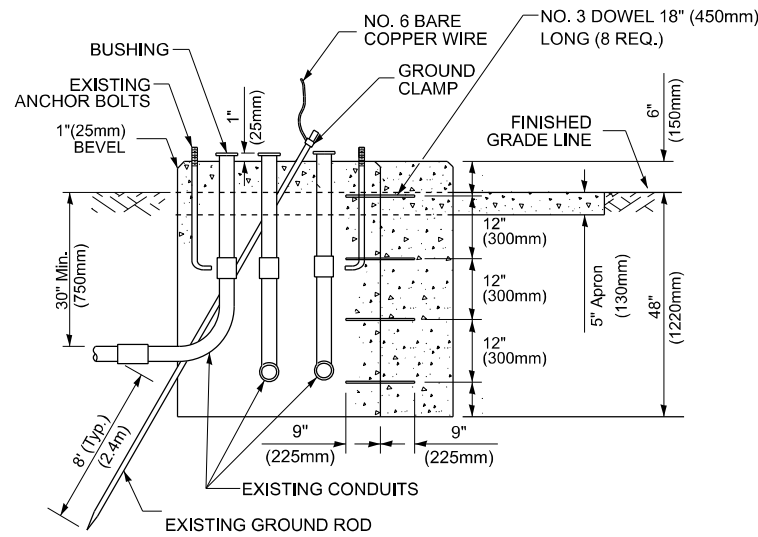
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



POST CAP MOUNT MAST ARM MOUNT
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

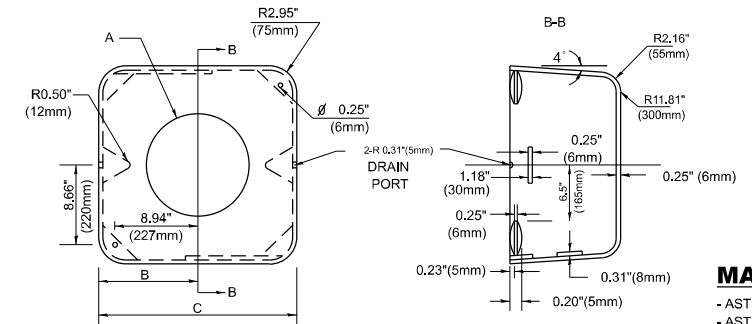


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

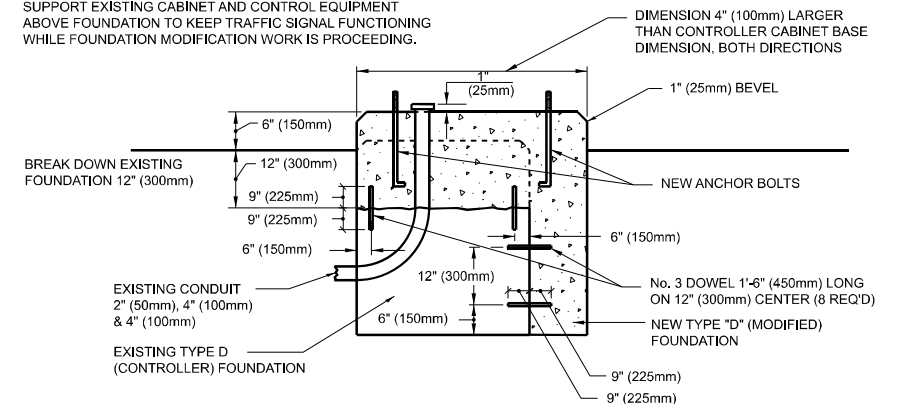
SHROUD

NOTES:

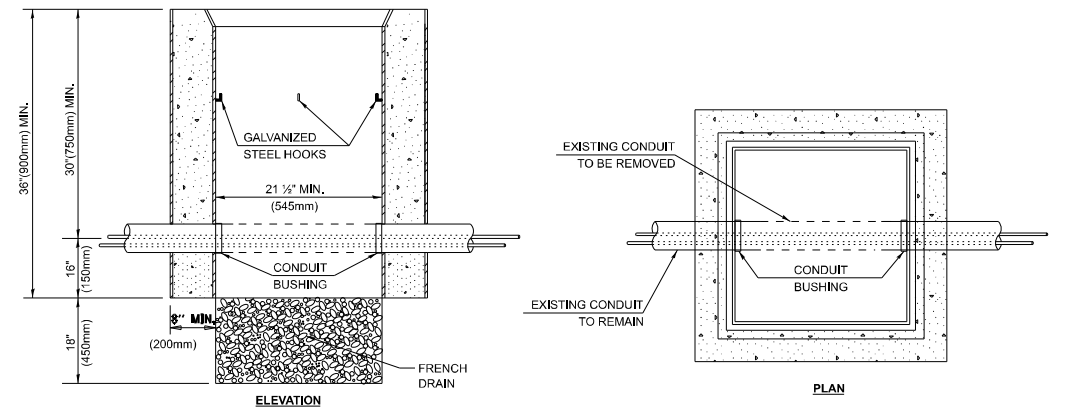
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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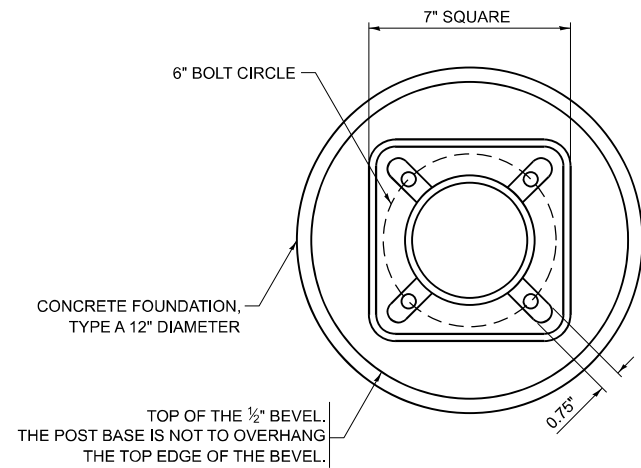
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/20/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

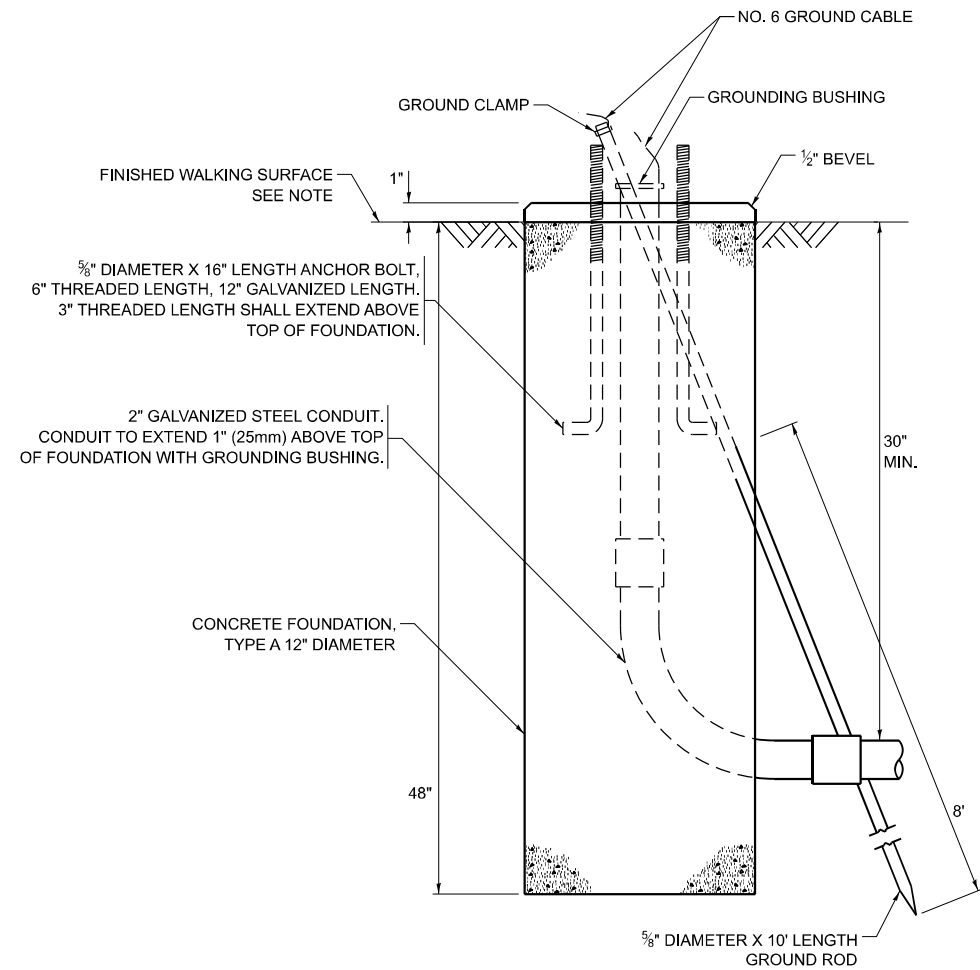
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TS-05		CONTRACT NO. 80B25		
ILLINOIS FED. AID PROJECT				



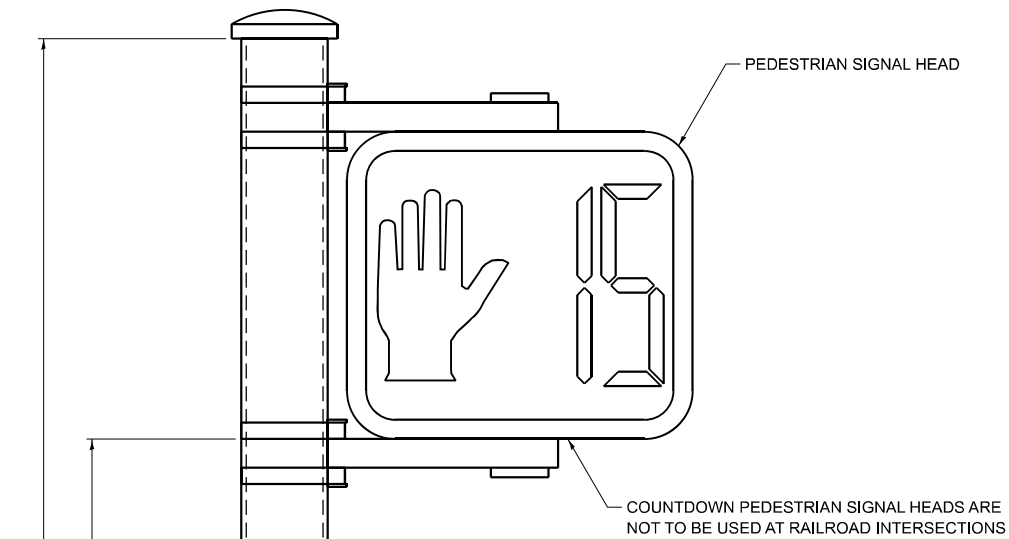
BOLT PATTERN

NOTE:

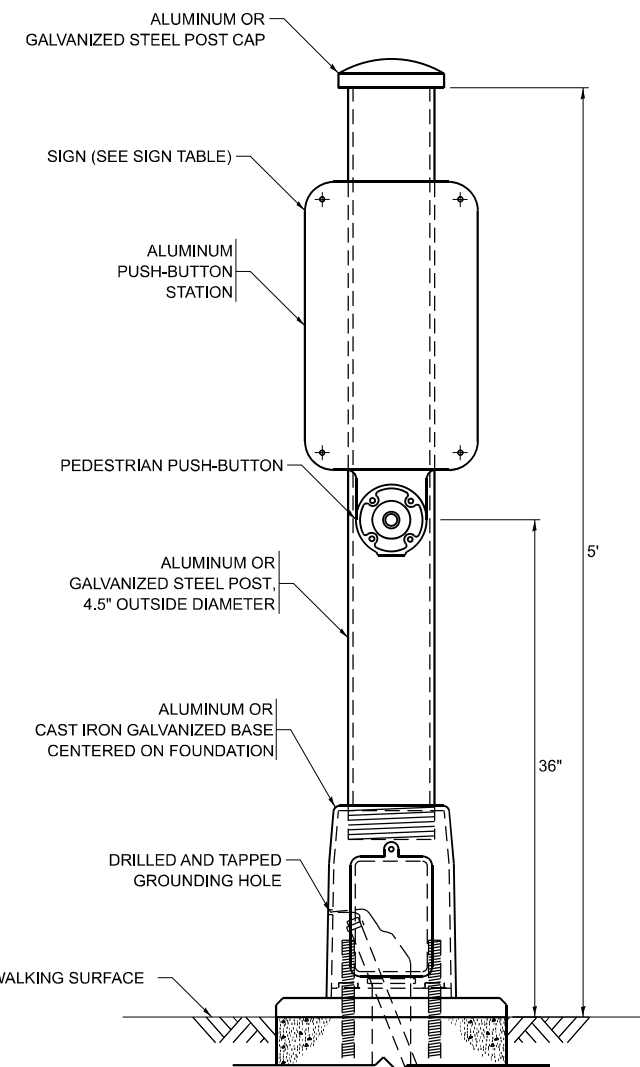
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



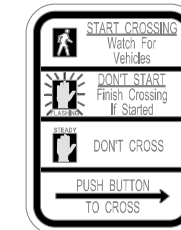
**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



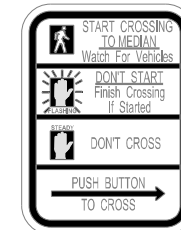
PEDESTRIAN SIGNAL POST, 10 FT.



PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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PLOT DATE = 10/20/2025	DATE - 10-15-2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

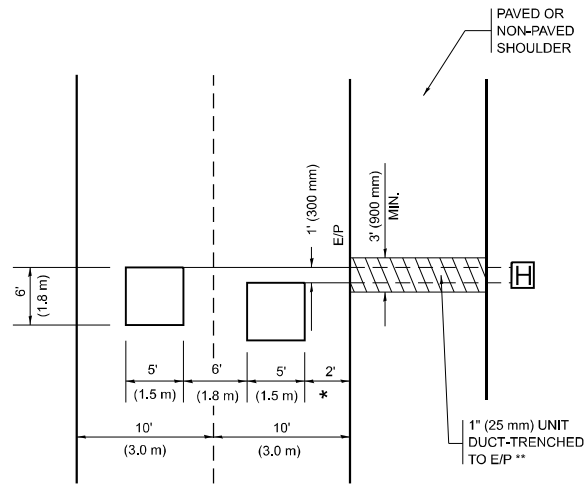
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 07 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS_8X19(066)	DUPAGE	56	55
TS-05			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



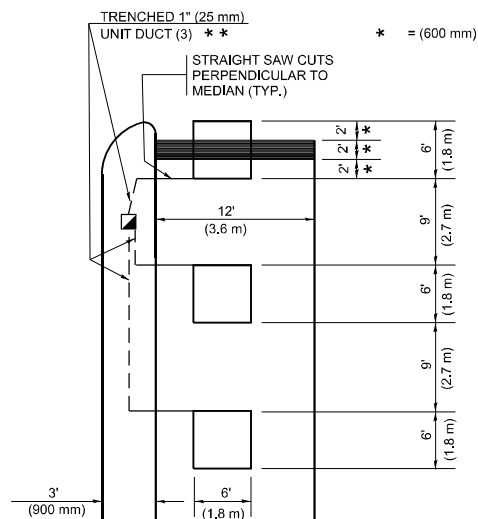
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



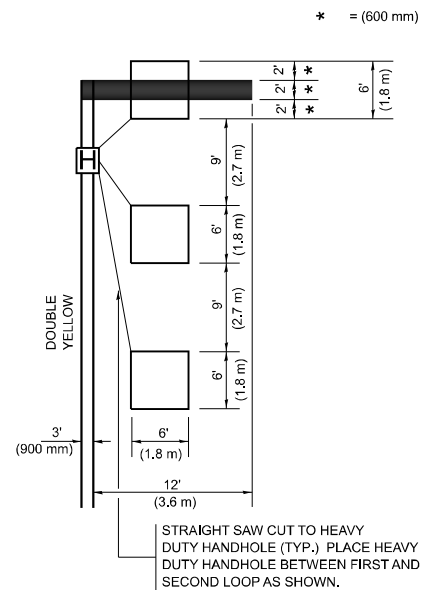
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPES OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

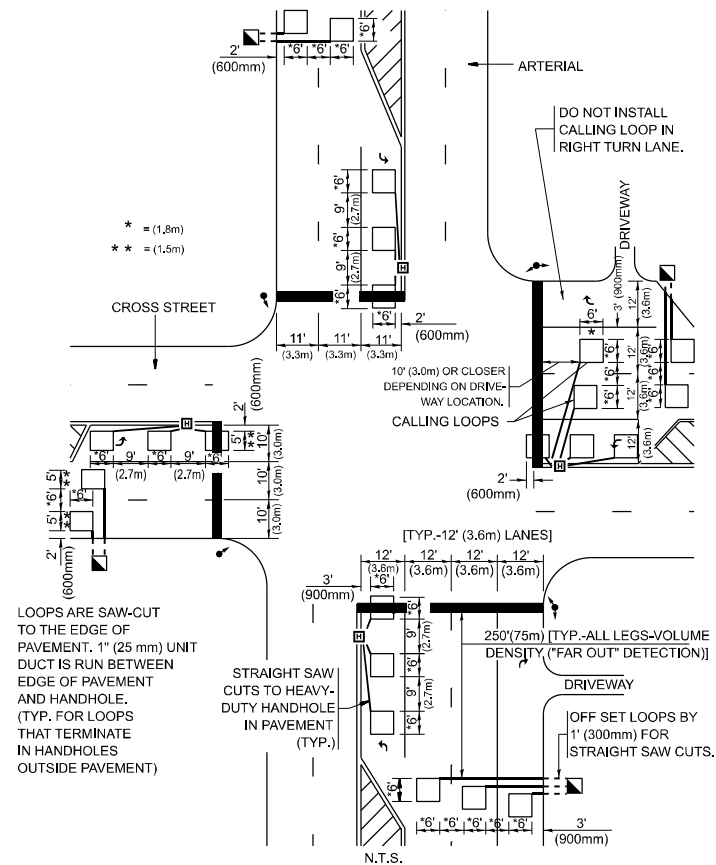
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

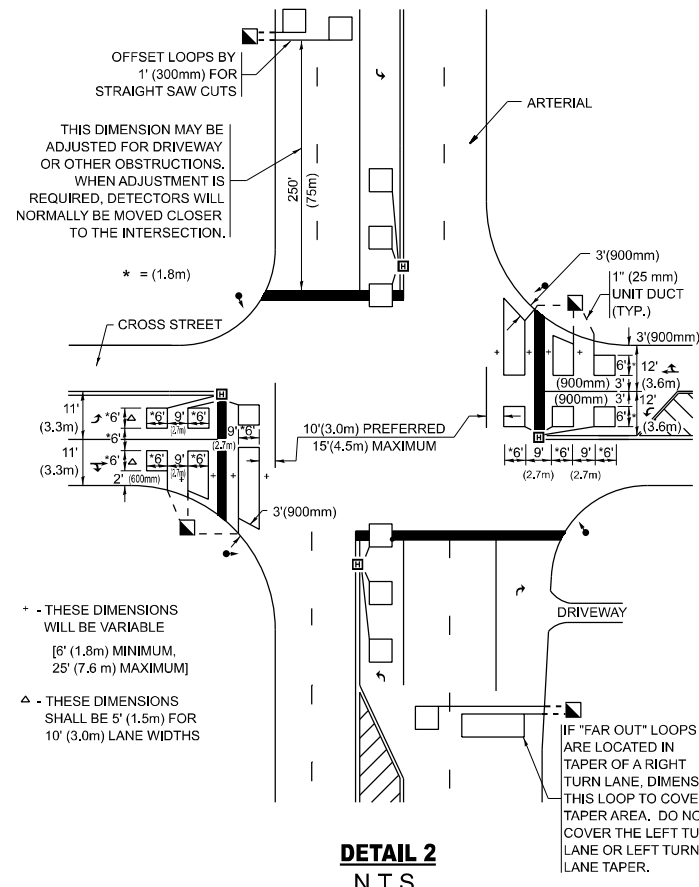
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



* - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	2025-2006-RS, 8X19(066)	DUPAGE	56	56
TS-07			CONTRACT NO. 80B25	
ILLINOIS FED. AID PROJECT				

MODEL: TS-07 (Sheet)
FILE NAME: c:\p\work\ts07\ts07.dgn