

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	1
		ILLINOIS	CONTRACT NO. 80B19	

* 33 + 7 = 40 TOTAL SHEETS

D-91-172-25

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN:
THE VILLAGES OF GLENWOOD
& HOMEWOOD

TRAFFIC DATA:
ADT = 34,900 (2023)

POSTED SPEED LIMIT = 35 & 40 MPH

DESIGN DESIGNATION = OTHER PRINCIPAL ARTERIAL

**PROPOSED
HIGHWAY PLANS**
FAP ROUTE 876: IL 1 (HALSTED STREET)
SOUTH OF RIDGE ROAD TO STRIEFF LANE
SECTION: 2025-1093-RS
PROJECT: NHPP-WWFK(227)
STANDARD OVERLAY & ADA IMPROVEMENTS
COOK COUNTY

C-91-252-25



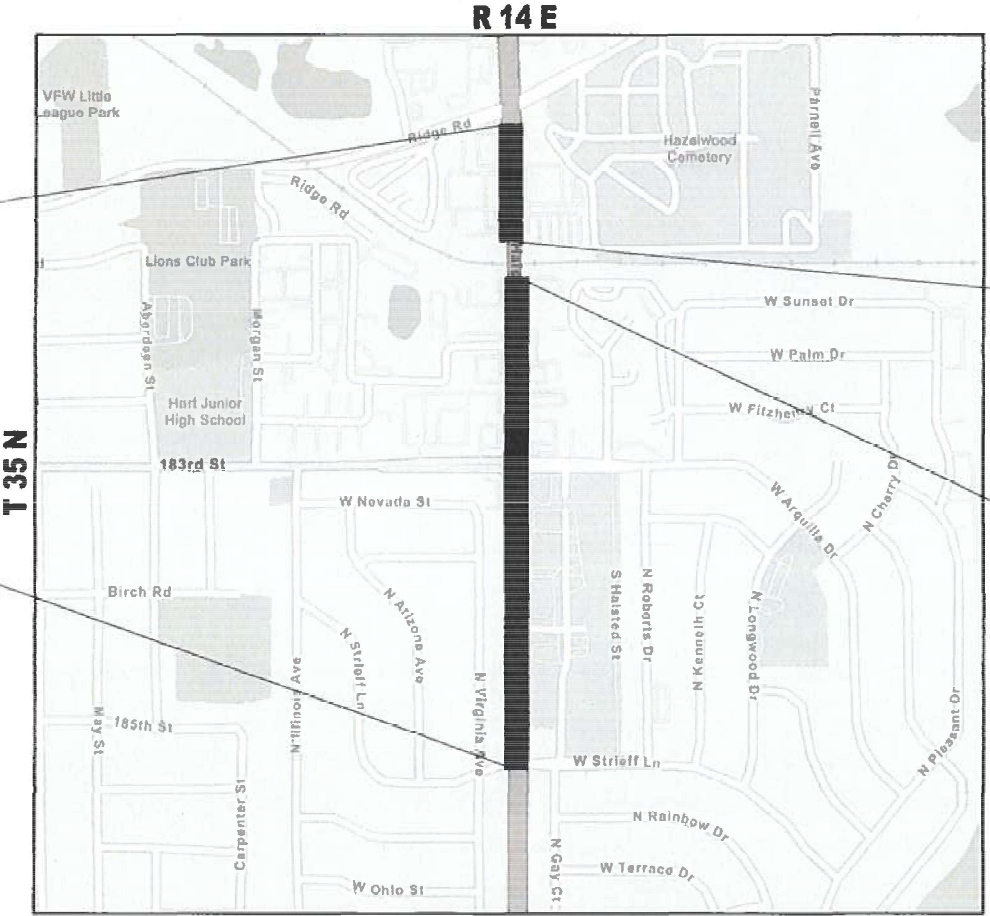
LOCATION MAP
(NOT TO SCALE)

OMMISSION ENDS
STATION 43 + 22

OMMISSION BEGINS
STATION 42 + 15

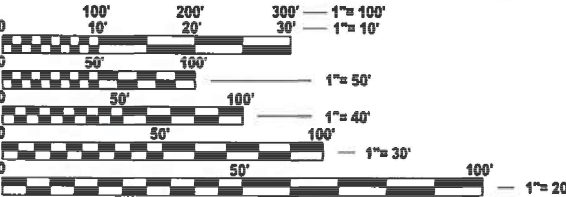
PROJECT ENDS
STATION 49 + 68

PROJECT BEGINS
STATION 14 + 56



BLOOM & THORNTON TOWNSHIP

GROSS LENGTH = 3,512 FT. = 0.67 MILES
NET LENGTH = 3,405 FT. = 0.64 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN, P.E. (847)-705-4240
PROJECT MANAGER: J. ALAIN MIDY, P.E. (847)-221-3063

CONTRACT NO. 80B19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Aug 11th 2025
Joseph IR REGIONAL ENGINEER

October 3, 2025
Scott A. Etkin ENGINEER OF DESIGN AND ENVIRONMENT

October 3, 2025
Cherry DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION	
1	COVER SHEET	000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	1. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
2	INDEX OF SHEETS. STATE STANDARDS,AND GENERAL NOTES	424001-12	PERPINDICULAR CURB RAMPS FOR SIDEWALKS	2. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES, ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
3-5	SUMMARY OF QUANTITIES	424011-05	DEPRESSED CORNER FOR SIDEWALKS	3. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
6	TYPICAL SECTIONS	424021-07	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS	4. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
7-8	ROADWAY PLANS	442201-04	CLASS C AND D PATCHES	5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
9-12	APS AND DETECTOR LOOP INSTALLATION PLAN	606001-09	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	6. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
13-15	ADA RAMP DESIGNS	701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM EDGE OF PAVEMENT	7. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
16	MEDIAN RE-ADJUSTMENT DETAIL	701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH	8. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
17	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)	701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN	9. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
18	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE	10. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
19	CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION	11. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
20	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE	12. THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701901-11	TRAFFIC CONTROL DEVICES	13. ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
22	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	780001-05	TYPICAL PAVEMENT MARKINGS	14. ALL PAVEMENT MARKINGS ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING DETAIL TC-13.
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS	15. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
24	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	886001-01	DETECTOR LOOP INSTALLATIONS	16. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
25	ARTERIAL ROAD INFORMATION SIGN (TC-22)	886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS	17. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.
26	DRIVEWAY ENTRANCE SIGNING (TC-26)			18. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INSTALLED ACCORDING TO IDOT D1 RRPM DETAIL TC-11.
26A-G	TRAFFIC SIGNAL DESIGN DETAILS (TS-05)			19. CONTRACTOR MUST NOT ENCROACH, PERFORM ANY CONSTRUCTION ACTIVITIES, OR PARK ANY CONSTRUCTION EQUIPMENT OR VEHICLES IN THE AREA WITHIN 30' FROM THE CENTERLINE OF THE NEAREST TRACK.
27	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING (TS-07)			20. THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT AT LEAST 1 WEEK IN ADVANCE OF BEGINNING FORESTRY WORK, WEED SPRAYING AND SEEDING.
28-33	ADA RAMP PD STANDARDS			21. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITES COMPANIES AND THE VILLAGES OF GLENWOOD & HOMEWOOD.
				22. TEMPORARY PAVEMENT MARKINGS OR SHORT TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED, UNLESS DIRECTED BY THE ENGINEER.
				23. PATCHING LOCATIONS TO BE DETERMINED BY THE RE AND AS SHOWN IN PLANS
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES IL-1 (HALSTED ST) FROM SOUTH OF RIDGE RD TO STRIEFF LN
				SCALE: SHEET OF 1 SHEETS STA. TO STA.
				F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
				876 2025-1093-RS COOK 33 2
				CONTRACT NO. 80B19
				ILLINOIS FED. AID PROJECT

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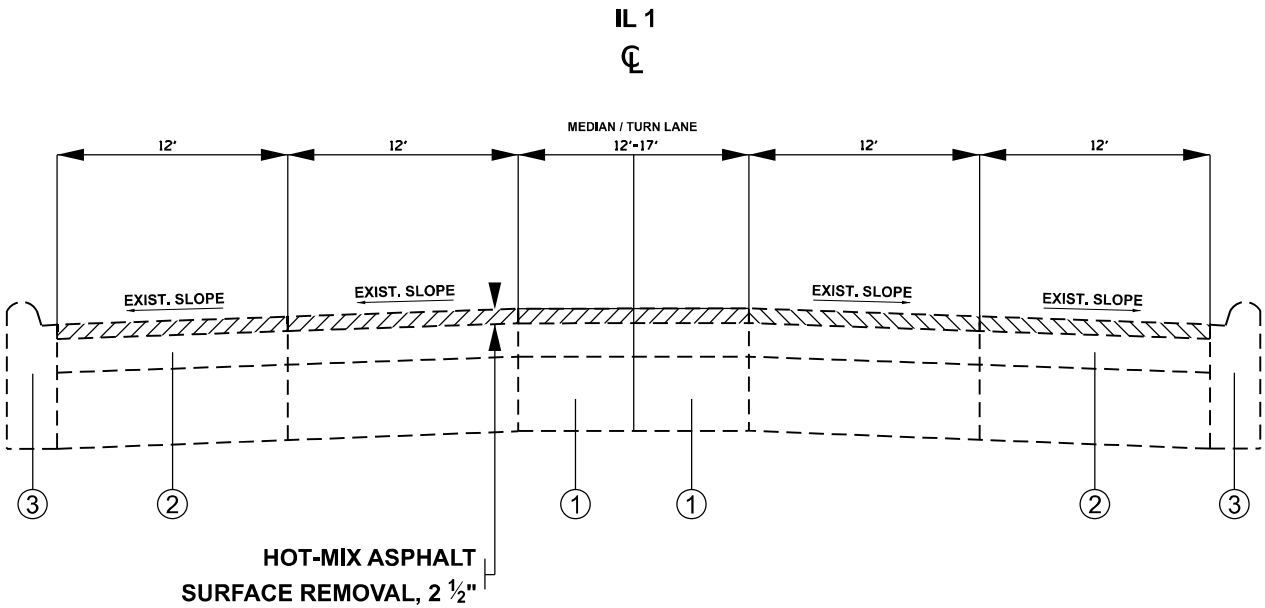
SUMMARY OF QUANTITIES					TYPE CODE					
					IL-1 COOK	IL-1 COOK	IL-1 COOK			
					ROADWAY	ROADWAY	APS SIGNALS			
					80% FED 20% STATE	100% STATE	80% FED 20% STATE			
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	0005	0005	0021			
	20200100	EARTH EXCAVATION	CU YD	20	20					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	122	122					
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1	1					
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1	1					
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1	1					
	25200110	SODDING, SALT TOLERANT	SQ YD	126	126					
	25200200	SUPPLEMENTAL WATERING	UNIT	0.8	0.8					
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	10	10					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17368	17368					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	13713	13713					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	39	39					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	218	218					
	40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	86	86					
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1053	1053					
	40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	2501	2501					
	42001300	PROTECTIVE COAT	SQ YD	415	415					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1320	1320					
	42400800	DETECTABLE WARNINGS	SQ FT	100	100					
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	25511	25511					
	44000600	SIDEWALK REMOVAL	SQ FT	1485	1485					
	44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	380	380					
✱ = SPECIALTY ITEM										
		USER NAME = mohammad.hamwi	DESIGNED -	REVISED -	STATE OF I DEPARTMENT OF TR					
			DRAWN -	REVISED -						
			CHECKED -	REVISED -						
		PLOT DATE = 8/14/2025	DATE -	REVISED -						

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LEGEND - EXISTING

- ① PORTLAND CEMENT CONCRETE PAVEMENT ±9"
- ② HOT MIX ASPHALT PAVEMENT ±4"
- ③ EXISTING CURB AND GUTTER

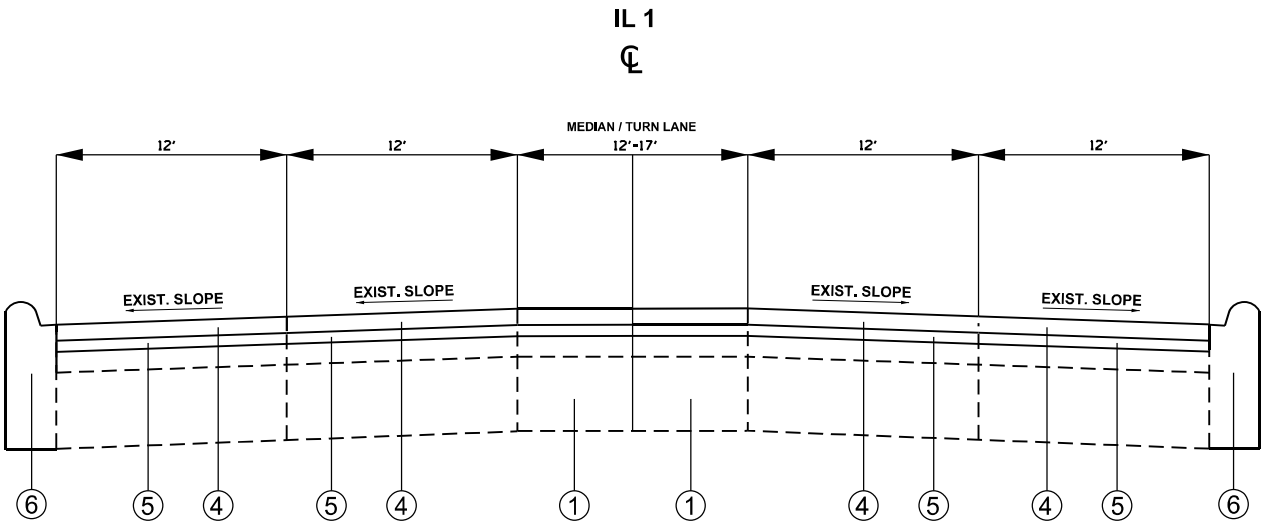


EXISTING TYPICAL SECTION

STATION:
14+56 TO 49+68

LEGEND - PROPOSED

- ④ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- ⑥ COMBINATION CURB & GUTTER (REMOVAL & REPLACEMENT DETERMINED BY RE)



PROPOSED TYPICAL SECTION

STATION:
14+56 TO 49+68

LOCATION	HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
	MIXTURE TYPE	AIR VOIDS @ Ndesign	
	PAVEMENT RESURFACING		
IL-1	POLYMERIZED HMA SURFACE COURSE, STONE MIX ASPHALT, 9.5 MIX "F", N80 1¾"	3.5% @ 80 GYR	QCP
IL-1	POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, ¾"	3.5% @ 50 GYR	QC/QA
	PATCHING		
IL-1	CLASS D PATCHES (HMA BINDER IL-19.0 MM: 9")	4% @ 70 GYR	QC/QA
IL-1	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0)	4% @ 70 GYR	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)			

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE COURSE MIXTURES IS 112 LBS/SQYD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HMA BC IL-4.75 N50
- THE CONTRACTOR SHALL PATCH THEN MILL.

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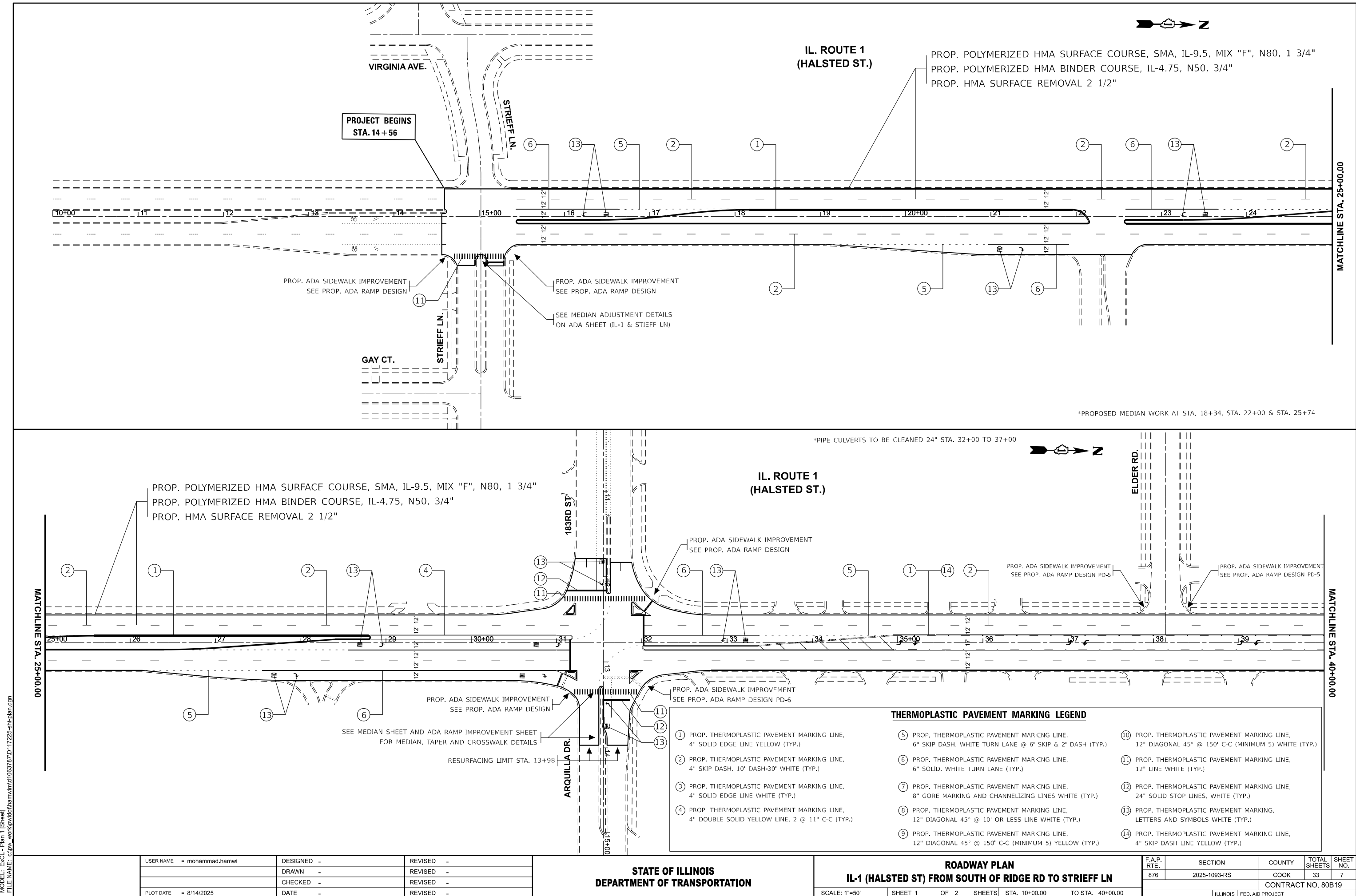
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

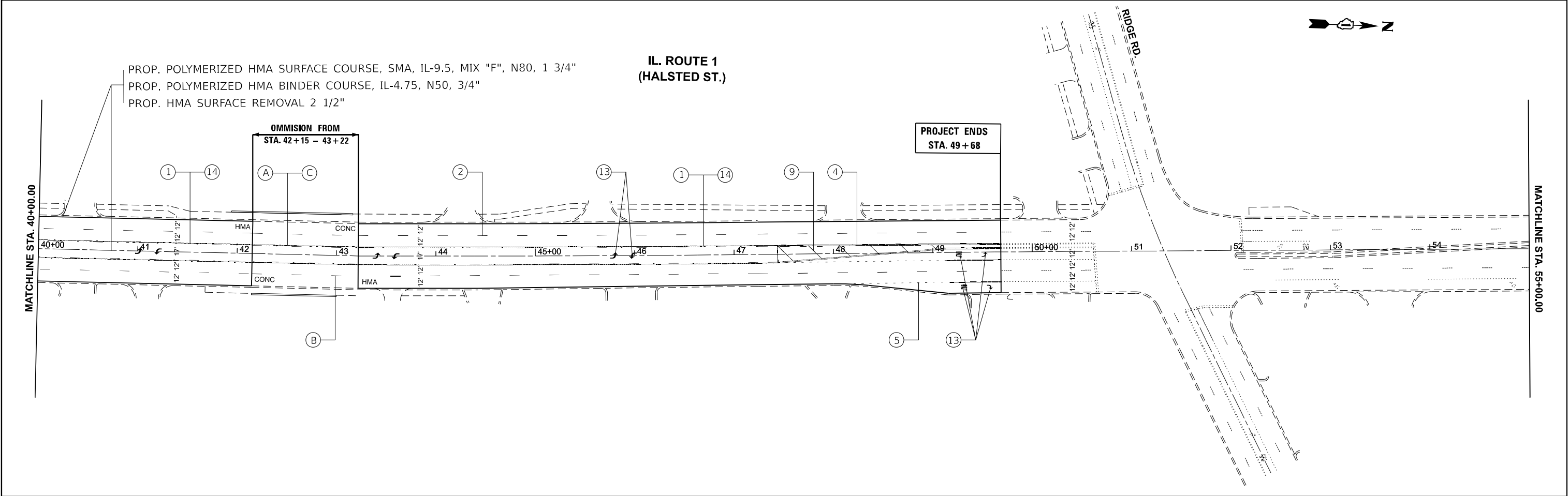
TYPICAL SECTION
IL-1 (HALSTED ST) FROM SOUTH OF RIDGE RD TO STRIEFF LN

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	6
CONTRACT NO. 80B19				
ILLINOIS FED. AID PROJECT				

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IL. ROUTE 1
(HALSTED ST.)

MATCHLINE STA. 55+00.00

THERMOPLASTIC PAVEMENT MARKING LEGEND

① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SOLID EDGE LINE YELLOW (TYP.)	⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SKIP DASH, WHITE TURN LANE @ 6' SKIP & 2' DASH (TYP.)	⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL 45° @ 150' C-C (MINIMUM 5) WHITE (TYP.)
② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SKIP DASH, 10' DASH-30' WHITE (TYP.)	⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" SOLID, WHITE TURN LANE (TYP.)	⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LINE WHITE (TYP.)
③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SOLID EDGE LINE WHITE (TYP.)	⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" GORE MARKING AND CHANNELIZING LINES WHITE (TYP.)	⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" SOLID STOP LINES, WHITE (TYP.)
④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE SOLID YELLOW LINE, 2 @ 11" C-C (TYP.)	⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL 45° @ 10' OR LESS LINE WHITE (TYP.)	⑬ PROP. THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS WHITE (TYP.)
	⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL 45° @ 150' C-C (MINIMUM 5) YELLOW (TYP.)	⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" SKIP DASH LINE YELLOW (TYP.)

MODIFIED URETHANE PAVEMENT MARKING LEGEND

(A) PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID EDGE LINE YELLOW (TYP.)
(B) PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH, 10' DASH-30' WHITE (TYP.)
(C) PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH LINE YELLOW (TYP.)

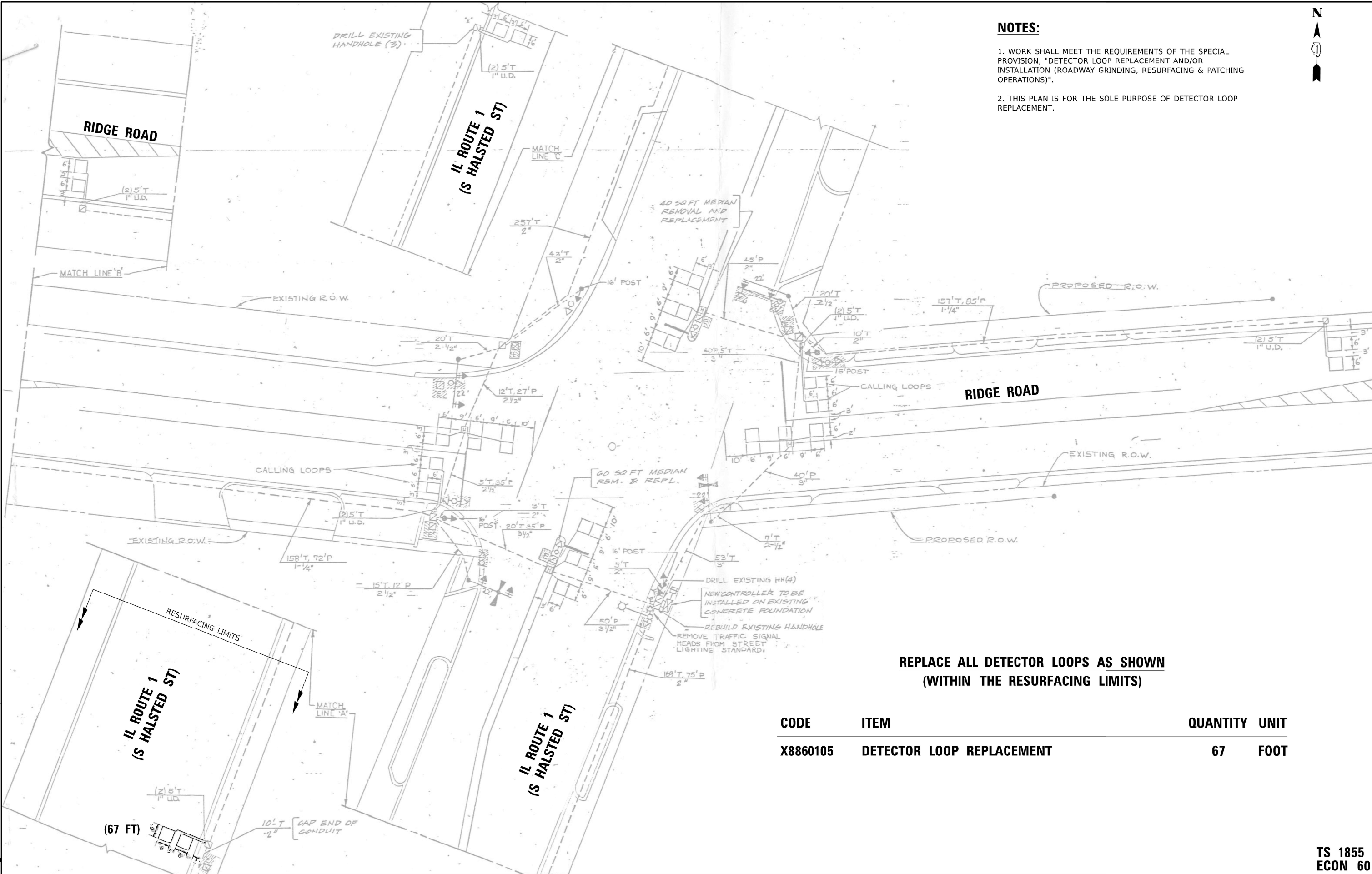
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN IL-1 (HALSTED ST) FROM SOUTH OF RIDGE RD TO STRIEFF LN		F.A.P. RTE. 876	SECTION 2025-1093-RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 8
	SCALE: 1"=50'		SHEET 2 OF 2 SHEETS		STA. 40+00.00 TO STA. 70+00.00		ILLINOIS FED. AID PROJECT

MODEL: EXCL - Plan 3 [Sheet]
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NOTES:

- 1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)".
- 2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
X8860105	DETECTOR LOOP REPLACEMENT	67	FOOT

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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
IL-ROUTE 1 (HALSTED STREET) AT RIDGE ROAD

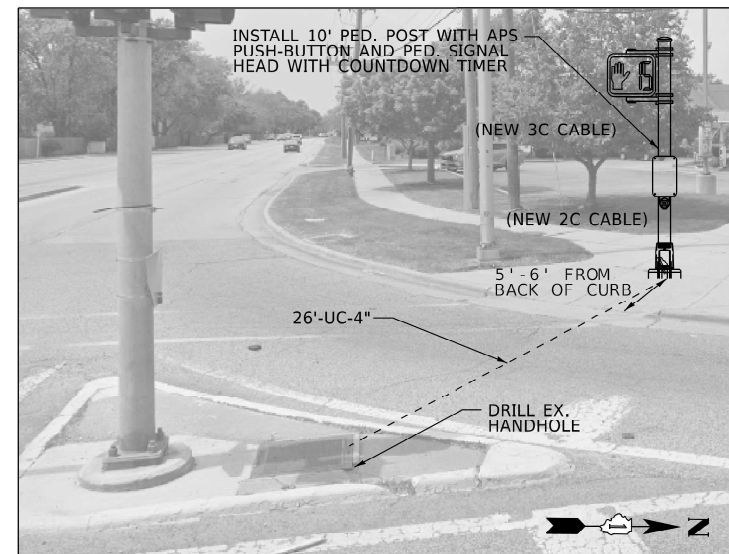
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	9
CONTRACT NO. 80B19				
ILLINOIS FED. AID PROJECT				

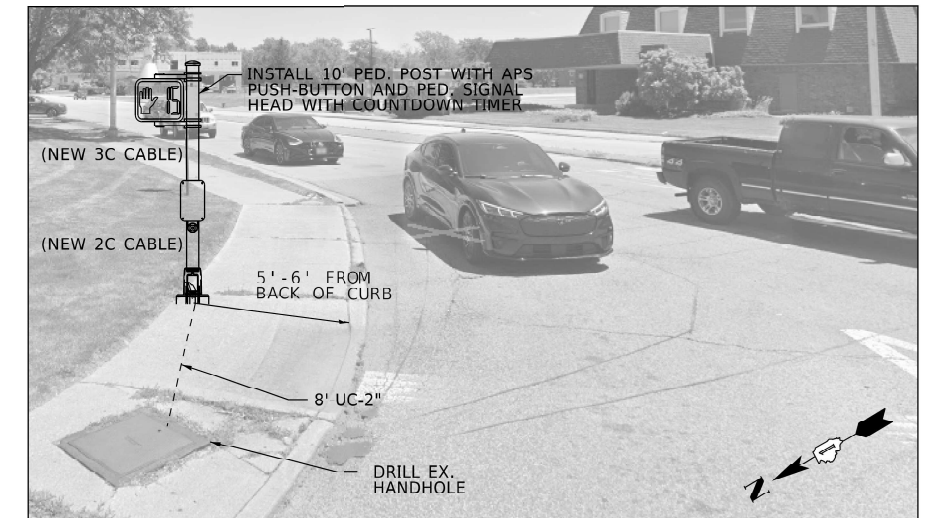
TS 1855
ECON 60

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

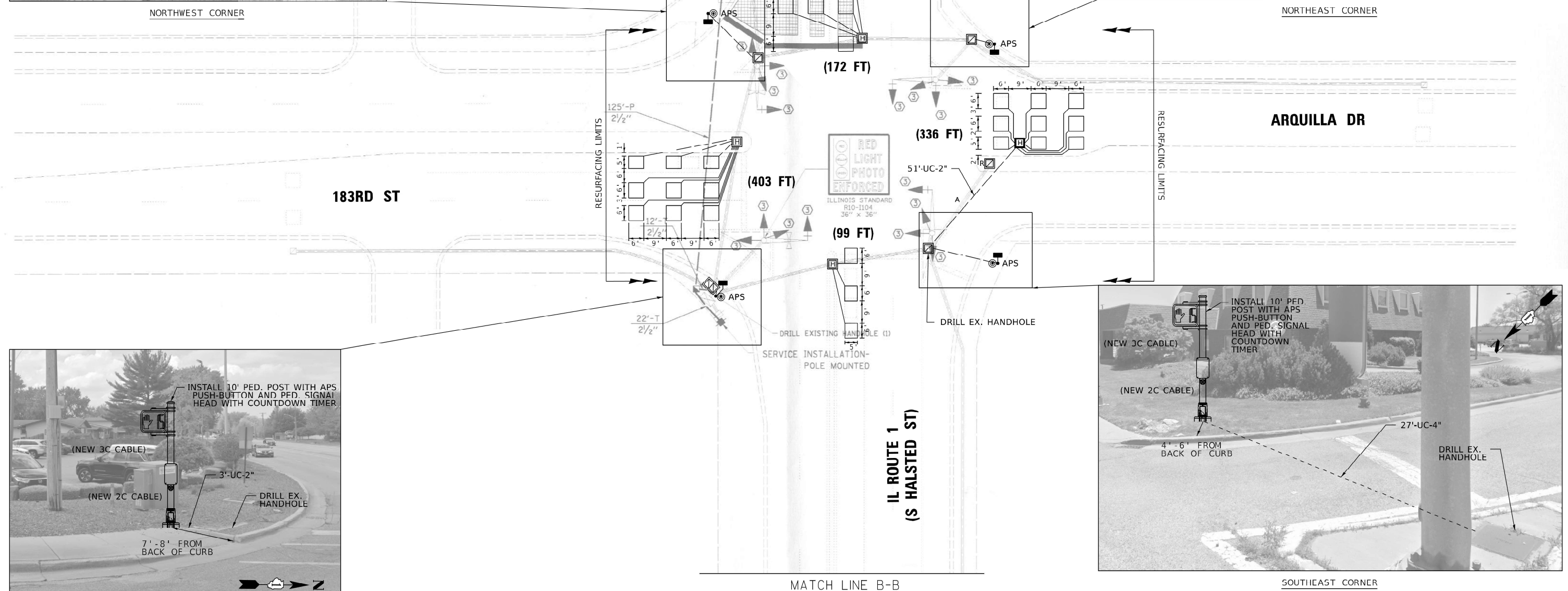
1. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
3. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
4. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
5. ALL PUSH BUTTONS SHALL BE APS.
6. ALL PUSH BUTTONS SHALL BE LOCATED ADJACENT TO 4' BY 2.5' OF CLEAR SIDEWALK SPACE TO MEET ADA. ALL PEDESTRIAN POSTS SHALL BE LOCATED A MAXIMUM OF 10' FROM EDGE OF PAVEMENT AND MINIMUM OF 4' FROM BACK OF CURB.
7. SIDEWALK AND SIDEWALK RAMPS ARE TO BE IMPROVED AT SW, NW, SE, AND NE CORNERS OF THE INTERSECTION. SEE ADA DETAIL SHEETS.
8. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.



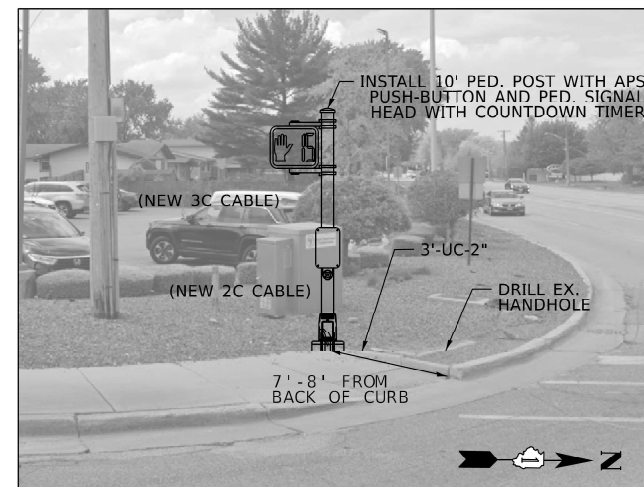
NORTHWEST CORNER



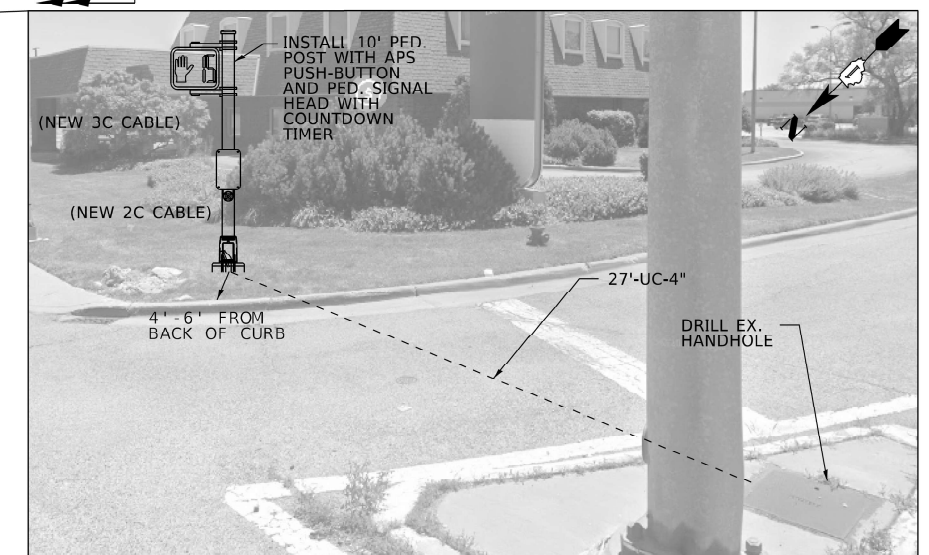
NORTHEAST CORNER



MATCH LINE B-B

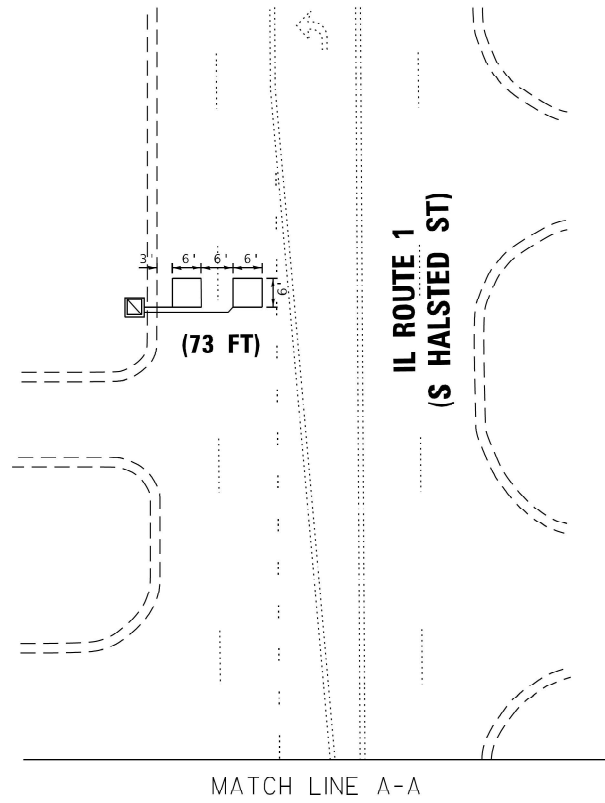
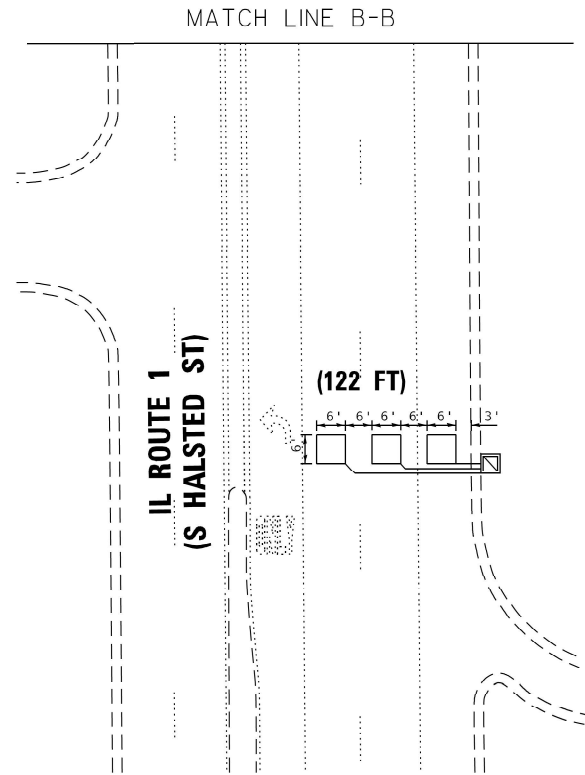


SOUTHWEST CORNER



SOUTHEAST CORNER

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TS 1830
ECON 60

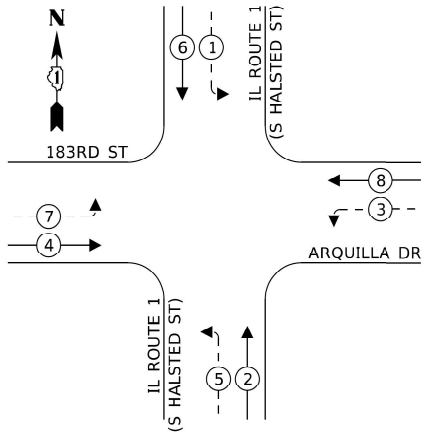
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PLOT DATE = 8/14/2025	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

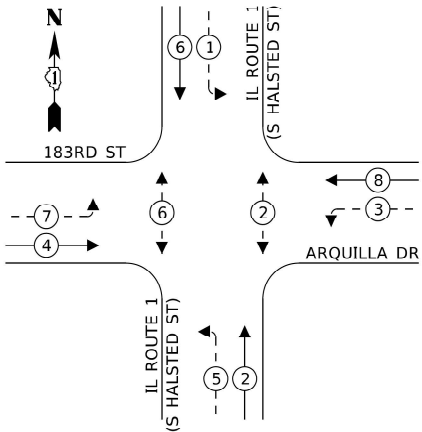
TRAFFIC SIGNAL MODERNIZATION PLAN IL-1 (HALSTED STREET) AT 183RD STREET / ARQUILLA DRIVE			
SCALE:	SHEET	OF 4 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	11
CONTRACT NO. 80B19				
ILLINOIS FED. AID PROJECT				

EXISTING CONTROLLER SEQUENCE



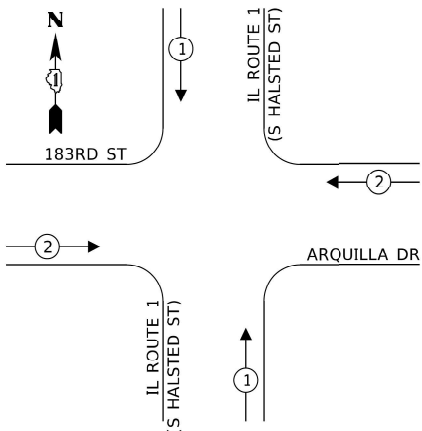
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← ⊛ → PROTECTED PHASE
- ← ⊛ → PROTECTED/PERMITTED PHASE
- ← ⊛ → PEDESTRIAN PHASE
- ← ⊛ OL → OVERLAP

EXISTING AND PROPOSED EMERGENCY VEHICLE
PREEMPTION SEQUENCE



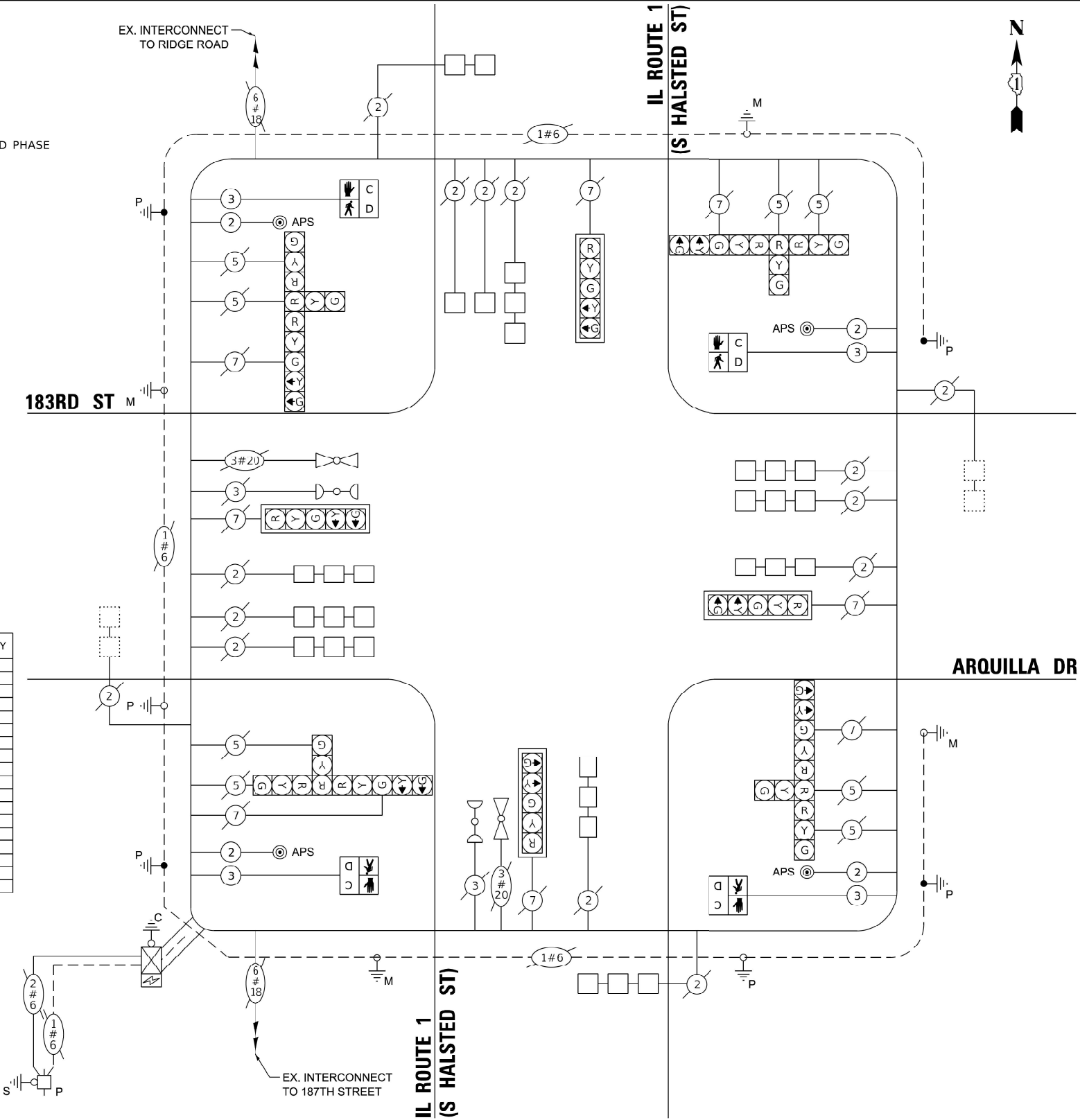
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	62
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	53
HEAVY-DUTY HANDHOLE	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	642
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	656
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	545
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	84
DRILL EXISTING HANDHOLE	EACH	5
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
DETECTOR LOOP, TYPE 1	FOOT	1205
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	186
REMOVE EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	4
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	4
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	16
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	8	11	88
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	4	15	60
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			427
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1032

ENERGY COSTS TO:	
ILLINOIS DEPARTMENT OF TRANSPORTATION	
201 WEST CENTER COURT	
SCHAUMBURG, ILLINOIS 60196	
ENERGY SUPPLY:	CONTACT: <u>NEW BUSINESS DEPARTMENT</u>
	PHONE: <u>(866) 639-3532</u>
	COMPANY: <u>COMED</u>
ACCOUNT NUMBER:	<u>---</u>
METER NUMBER:	<u>---</u>



CABLE PLAN
(NOT TO SCALE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION
SEQUENCE, AND SCHEDULE OF QUANTITIES IL-1 (HALSTED STREET) AT
183RD STREET / ARQUILLA DRIVE

SCALE: SHEET OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	12
CONTRACT NO. 80B19				

ILLINOIS FED. AID PROJECT

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ECON 60

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IL-1 (HALSTED STREET)

STRIEFF LANE

ADJUST EX. MEDIAN, TO BE PAID AS:

- MEDIAN REMOVAL
- CLASS D PATCHES, TYPE II, 9 INCH
- HMA REPLACEMENT OVER PATCHES
- HMA REMOVAL OVER PATCHES, 4"
- AGGREGATE BASE COURSE TYPE B 4"
- CONCRETE CURB, TYPE B
- REMOVE AND REINSTALL BRICK PAVER

LEGEND

xx.xx'

EXISTING LENGTH

PROPOSED SIDE CURB

() EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR DRIVEWAYS, REALIGNED SIDEWALK UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS.
- BENCHMARK: NORTHEAST BOLT AT FH @ SOUTHEAST CORNER OF HALSTED ST AND STRIEFF LN

CONSTRUCTION NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISITING SIDEWALK.
- 2) MATCH EXISTING SIDEWALK WIDTH.

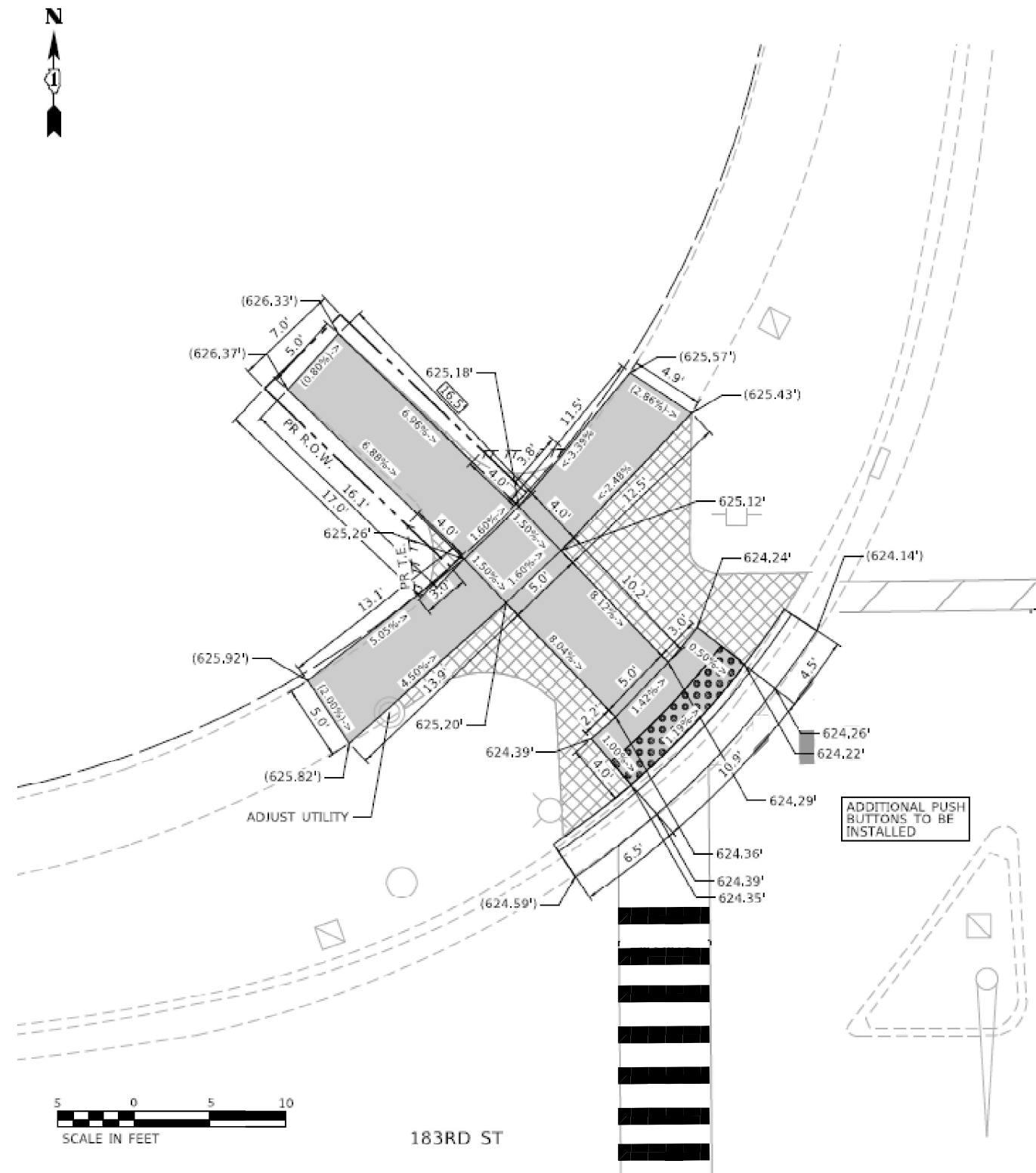
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB RAMP IMPROVEMENT
NE AND SE QUADRANTS OF IL-1 (HALSTED STREET) & STRIEFF LANE

SCALE: SHEET 1 OF 4 SHEETS STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	13
CONTRACT NO. 80B19				
ILLINOIS FED. AID PROJECT				





DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR DRIVEWAYS, REALIGNED SIDEWALK UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS.
- REF. BENCHMARK: 625.434 / BENCHMARK: CUT SQUARE ON SOUTH SIDE OF CONCRETE FOUNDATION FOR LIGHT POLE

LEGEND

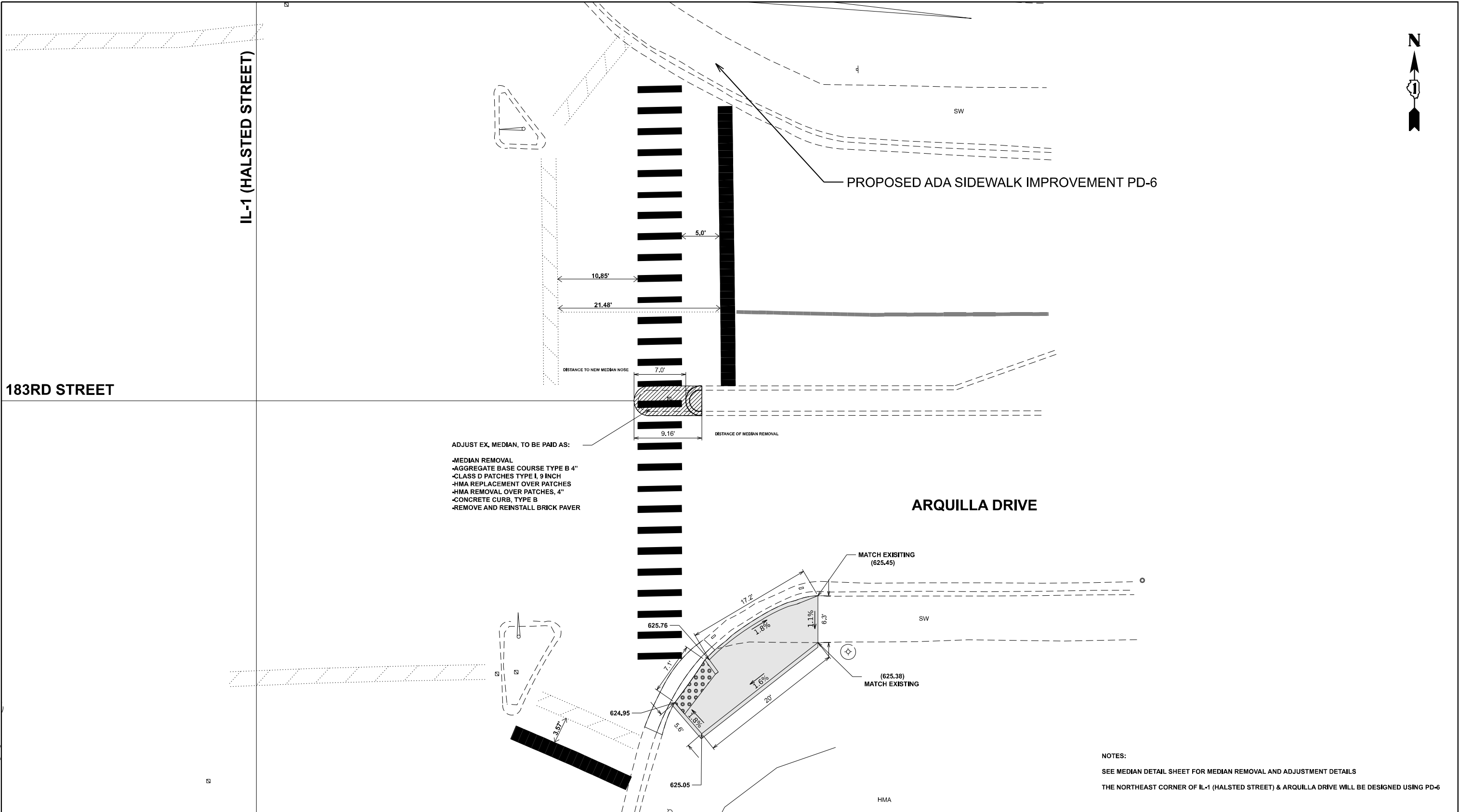
- | | | | |
|--|---------------------------------|--|---|
| | EXISTING LENGTH | | DETECTABLE WARNINGS |
| | PROPOSED SIDE CURB | | SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD |
| | EXISTING ELEVATION/SLOPE | | |

CONSTRUCTION NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN
TRANSITIONING TO EXISTING SIDEWALK.
- 2) MATCH EXISTING SIDEWALK WIDTH.

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB RAMP IMPROVEMENT NW QUADRANT OF IL-1 (HALSTED STREET) & 183RD STREET				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -						876	2025-1093-RS	COOK	33	14
	CHECKED -	REVISED -		CONTRACT NO. 80B19								
PLOT DATE = 11/19/2025	DATE -	REVISED -		SCALE:	SHEET 8	OF	SHEETS	STA. 0+00.00	TO STA. 0+00.00	ILLINOIS FED. AID PROJECT		

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DESIGNER NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
 - 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN
 - 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR DRIVEWAYS, REALIGNED SIDEWALK UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
 - 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS.
- BENCHMARK: AT ELEVATION 626.459 FT. "□" NEAST CORNER AT TCB OF SWEST CORNER OF HALSTED AND 183RD STREET.

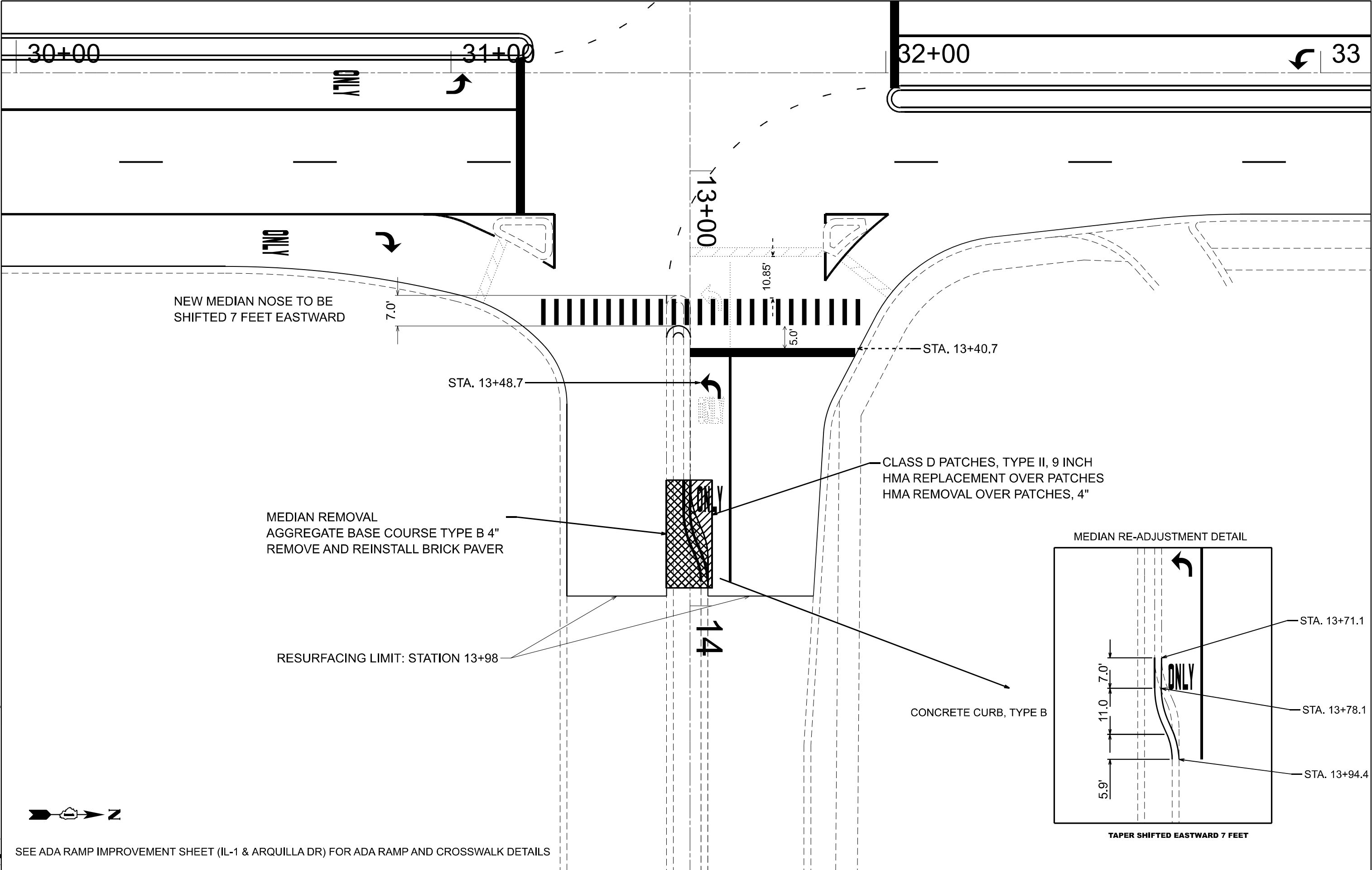
LEGEND

- XX.XX' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

CONSTRUCTION NOTES

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISITING SIDEWALK.
- 2) MATCH EXISTING SIDEWALK WIDTH.

	USER NAME = mohammad.hamwi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB RAMP IMPROVEMENT SE QUADRANT OF IL-1 (HALSTED STREET) & ARQUILLA DRIVE					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -							876	2025-1093-RS	COOK	33	15	
		CHECKED -	REVISED -							CONTRACT NO. 80B19					
	PLOT DATE = 8/9/2025	DATE -	REVISED -		SCALE:		SHEET 3	OF 4	SHEETS	STA. 0+00.00	TO STA. 0+00.00		ILLINOIS FED. AID PROJECT		



SEE ADA RAMP IMPROVEMENT SHEET (IL-1 & ARQUILLA DR) FOR ADA RAMP AND CROSSWALK DETAILS

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USER NAME	= mohammad.hamwi	DESIGNED	-	REVISED	-
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		CHECKED	-	REVISED	-
PLOT DATE	= 8/14/2025	DATE	-	REVISED	-

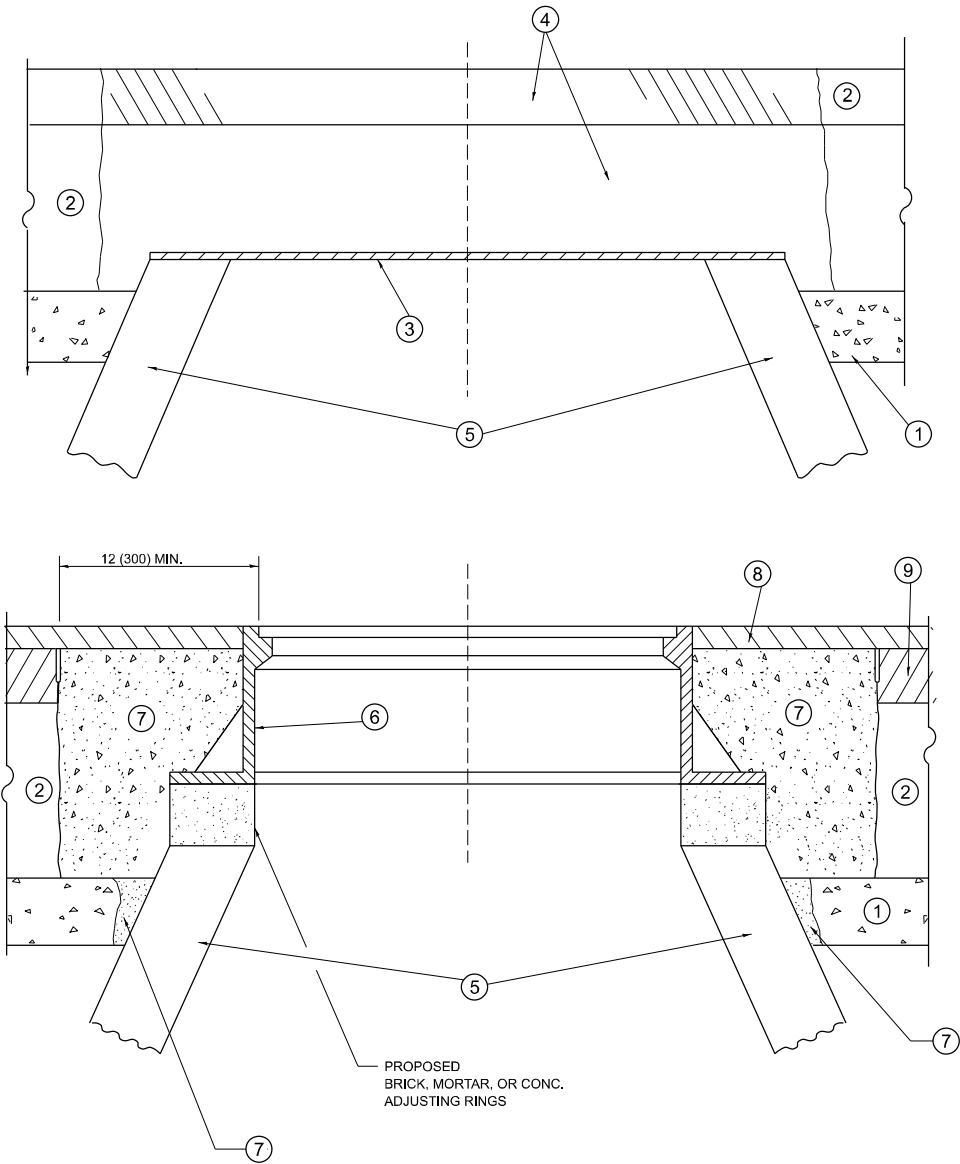
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MEDIAN RE-ADJUSTMENT DETAIL
IL-1 (HALSTED STREET) & ARQUILLA DRIVE

SCALE: SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	16
CONTRACT NO. 80B19				
ILLINOIS FED. AID PROJECT				

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**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

	USER NAME = mohammad.hamwi	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 12-06-11					876	2025-1093-RS	COOK	33	17
		CHECKED -	REVISED - K. SMITH 11-18-22		BD600-03 (BD-08)			CONTRACT NO. 80B19				
	PLOT DATE = 8/9/2025	DATE - 10-25-94	REVISED - K. SMITH 09-15-23		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

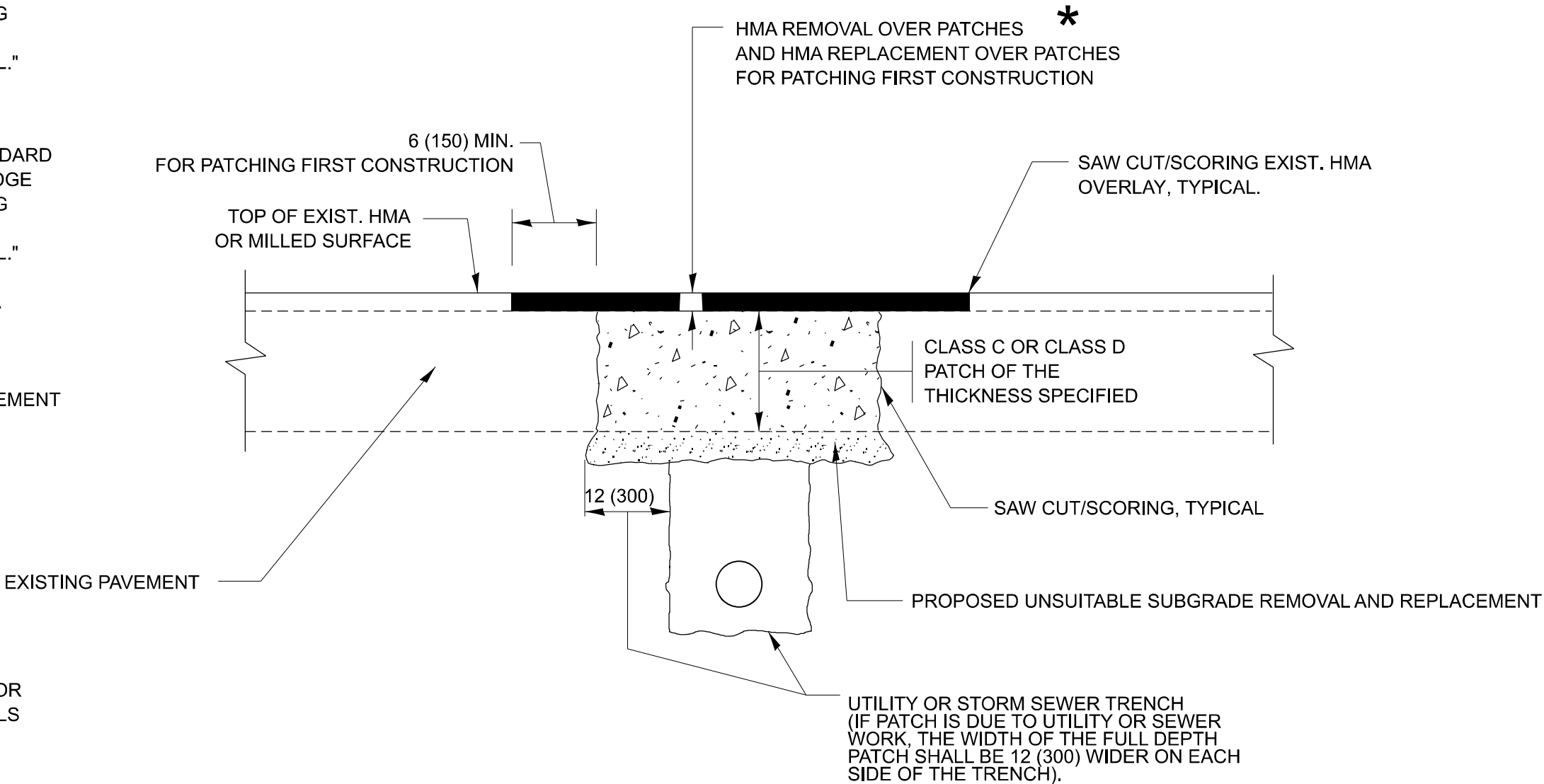
METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.

* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

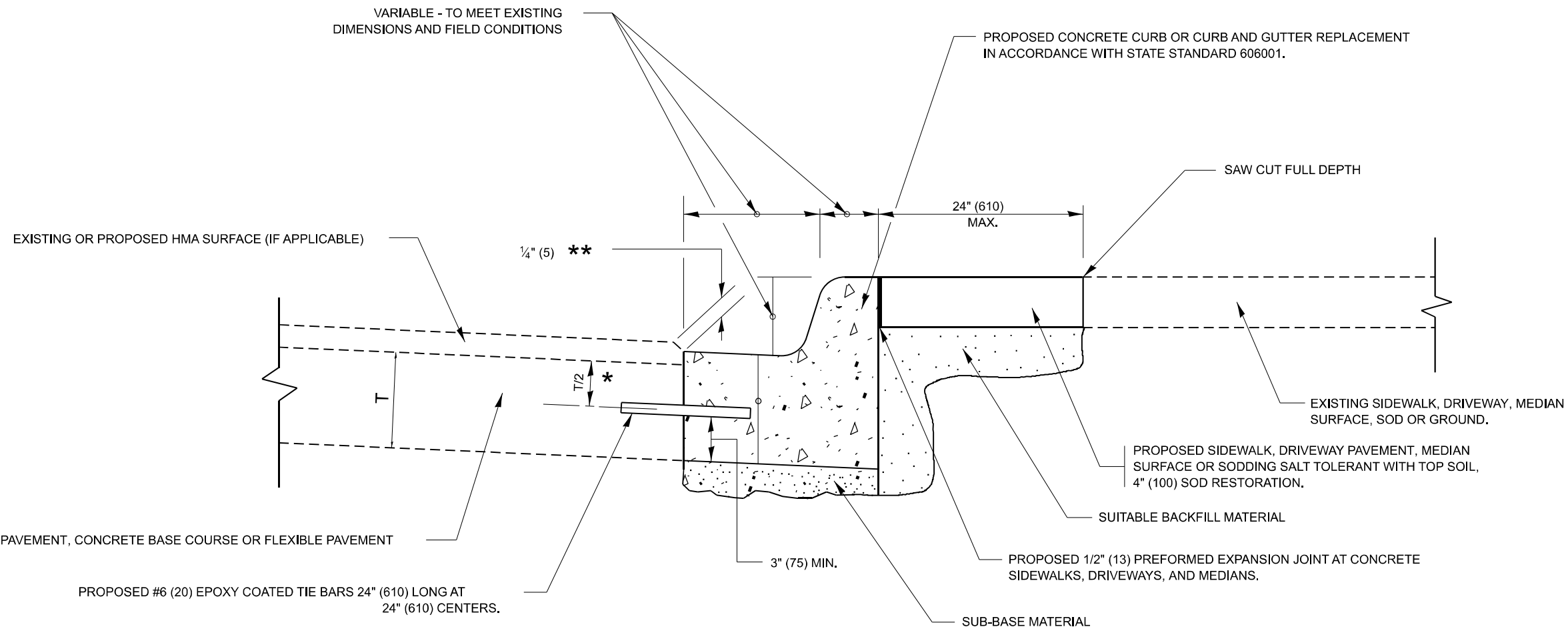
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	USER NAME = mohammad.hamwi	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 09-04-07						876	2025-1093-RS	COOK	33	18
		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22)				
	PLOT DATE = 8/9/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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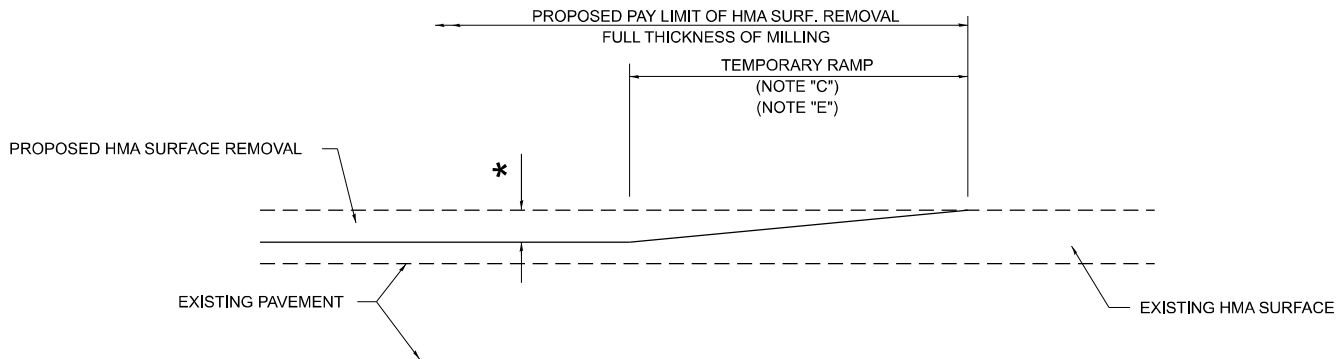
USER NAME	= mohammad.hamwi	DESIGNED -	A. HOUSEH	REVISED -	A. ABBAS 03-21-97
		DRAWN -		REVISED -	M. GOMEZ 01-22-01
		CHECKED -		REVISED -	R. BORO 12-15-09
PLOT DATE	= 8/14/2025	DATE	= 03-11-94	REVISED -	K. SMITH 07-11-19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT

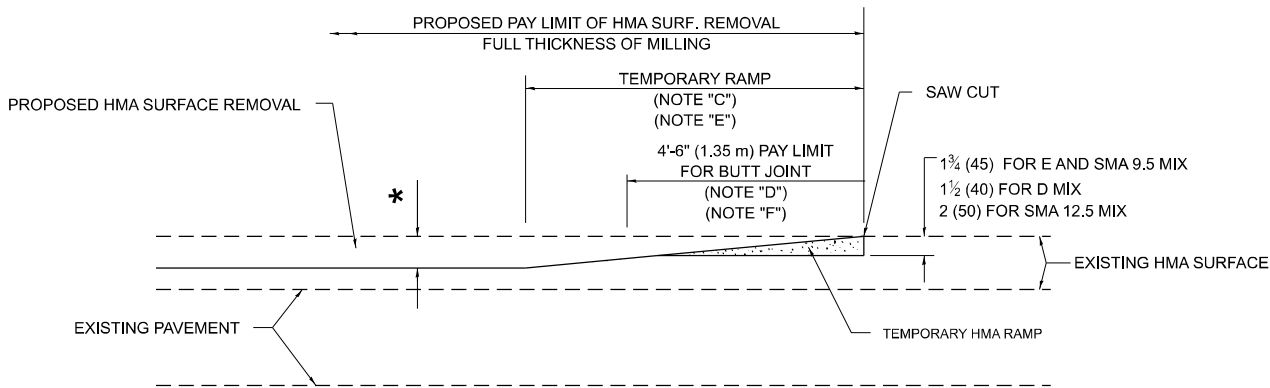
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	19
BD600-06 (BD-24)		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

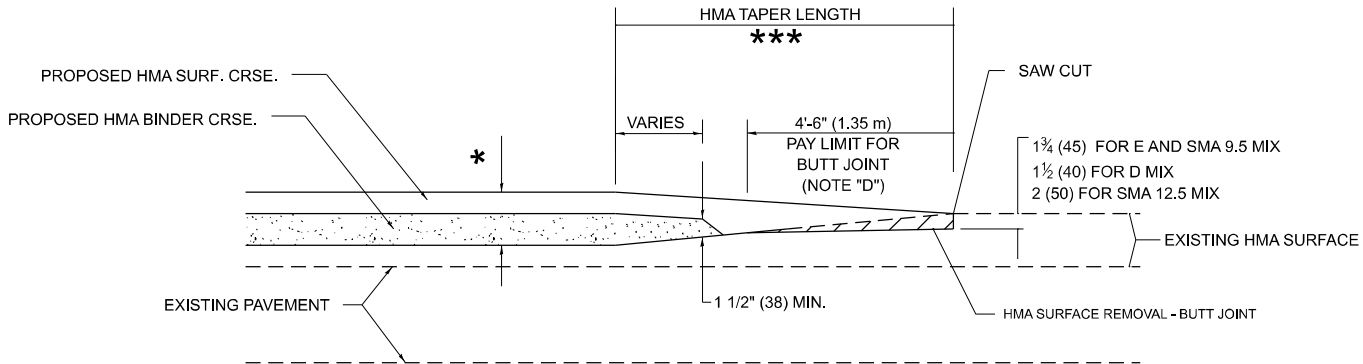
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

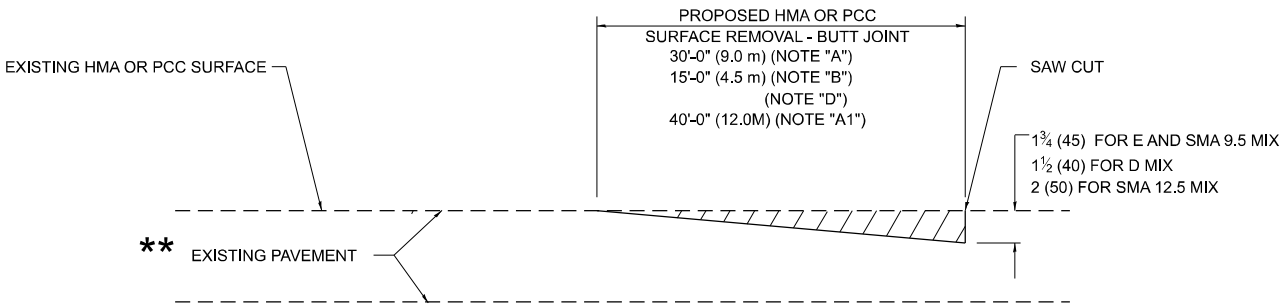
OPTION 2

TYPICAL TEMPORARY RAMP

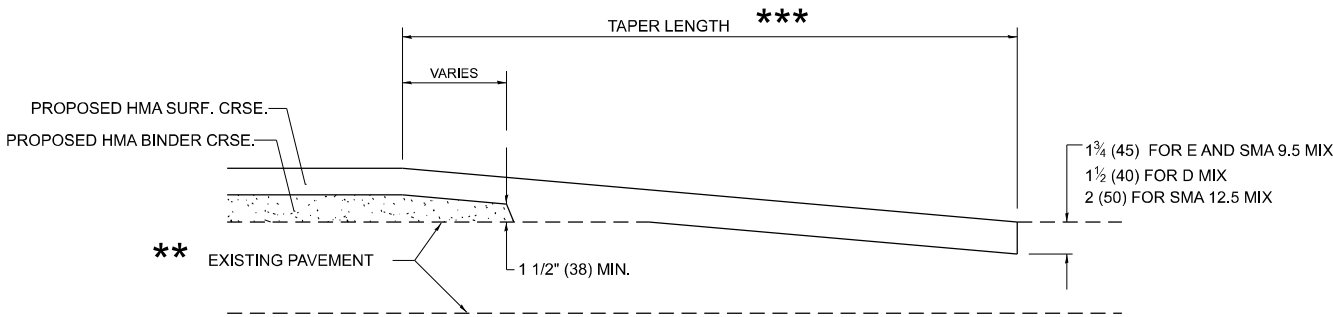


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: BD-32-1 (Sheet)
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USER NAME = mohammad.hamwi	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN - M. GOMEZ 04-06-01	REVISED - M. GOMEZ 04-06-01
	CHECKED - R. BORO 01-01-07	REVISED - R. BORO 01-01-07
PLOT DATE = 8/14/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

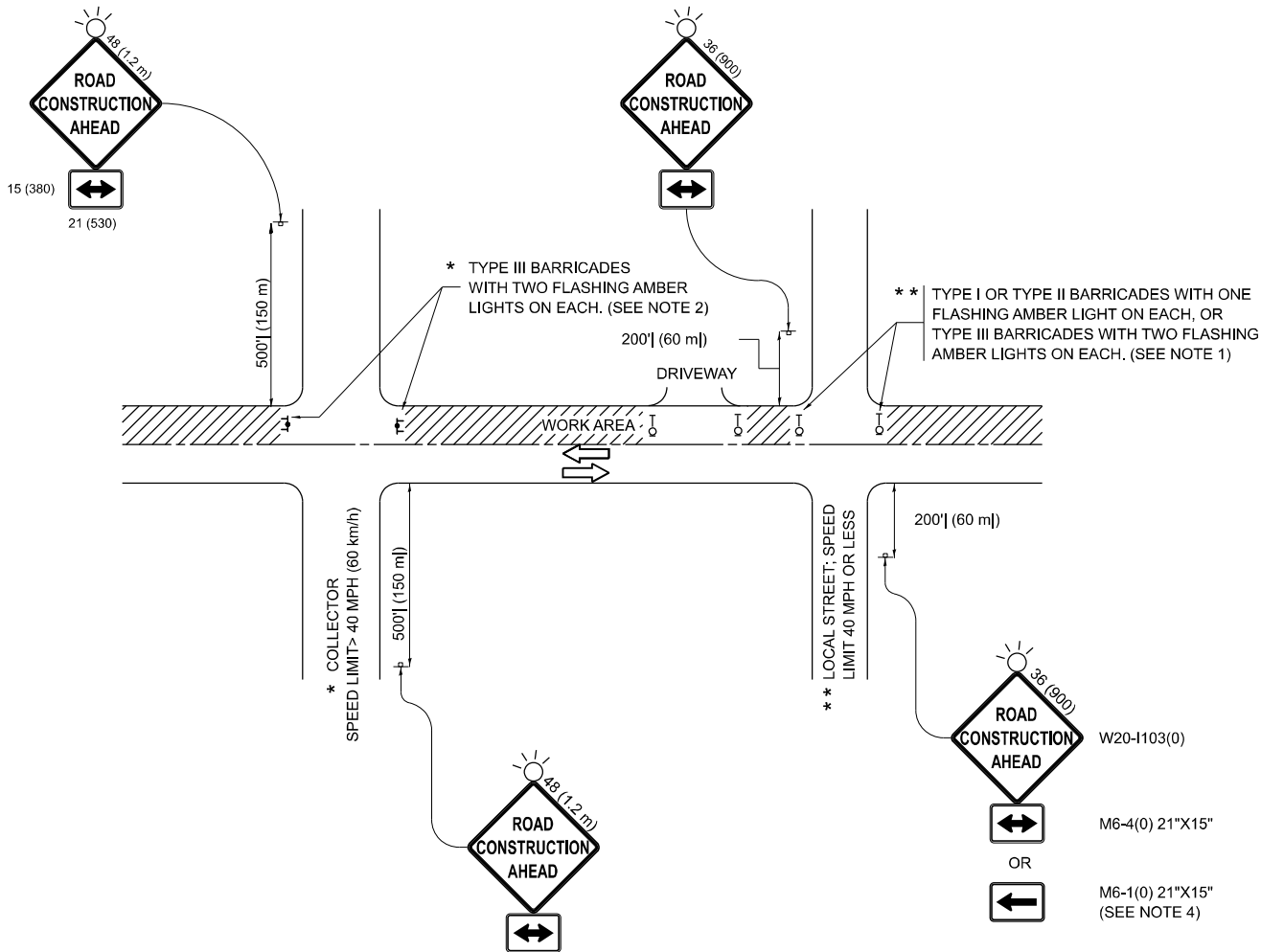
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	20
BD400-05 BD-32		CONTRACT NO. 80B19		
		ILLINOIS FED. AID PROJECT		

MODEL: TC-10-1 [Sheet]
FILE NAME: c:\p\work\p\work\hammad\1063787\117225-shh-DiaStus.dgn



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

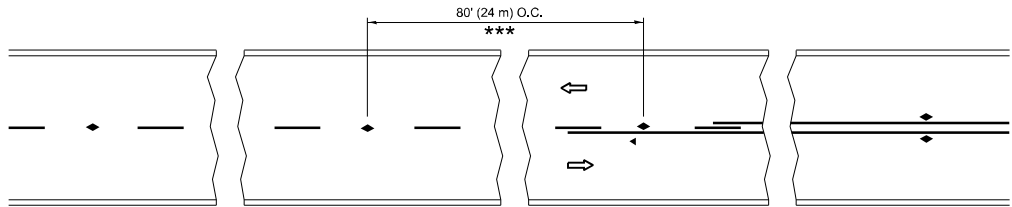
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.

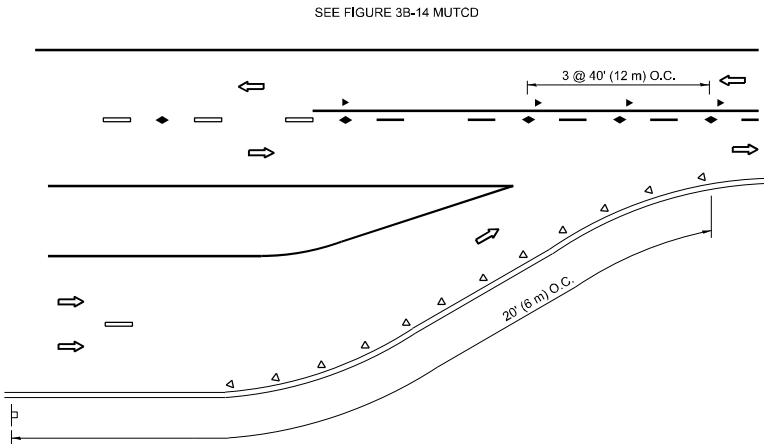
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.

7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
- All dimensions are in inches (millimeters)
unless otherwise shown.
- | | | | | | | | | | | | | | | | | |
|--|----------------------------|-------------------|---------------------------------|---|--|--------|-------|----|--------|------|---------|-----------------------------|--------------|--------------------|-----------------|--------------|
| | USER NAME = mohammad.hamwi | DESIGNED - L.H.A. | REVISED - T. RAMMACHER 01-06-00 | STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | SCALE: | SHEET | OF | SHEETS | STA. | TO STA. | F.A.P.
RTE. | SECTION | COUNTY | TOTAL
SHEETS | SHEET
NO. |
| | | DRAWN - | REVISED - A. SCHUETZE 07-01-13 | | | | | | | | | 876 | 2025-1093-RS | COOK | 33 | 21 |
| | | CHECKED - | REVISED - A. SCHUETZE 09-15-06 | | | | | | | | | TC-10 | | CONTRACT NO. 80B19 | | |
| | PLOT DATE = 8/9/2025 | DATE - 06-89 | REVISED - D. SENDERAK 05-03-24 | | | | | | | | | ILLINOIS FED. AID PROJECT | | | | |

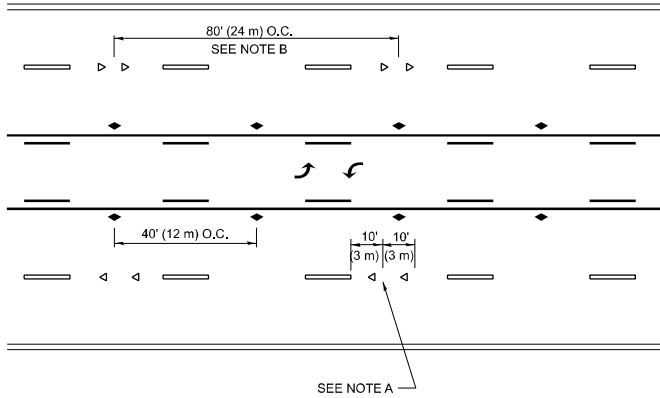


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

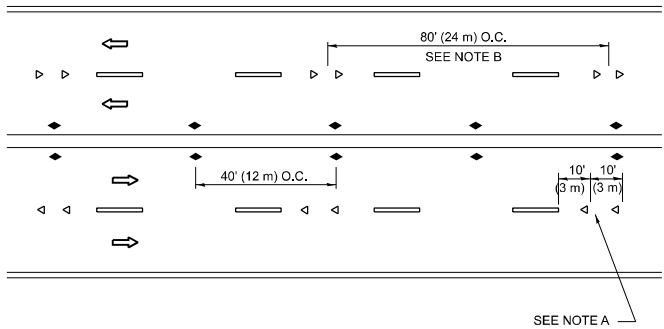
TWO-LANE/TWO-WAY



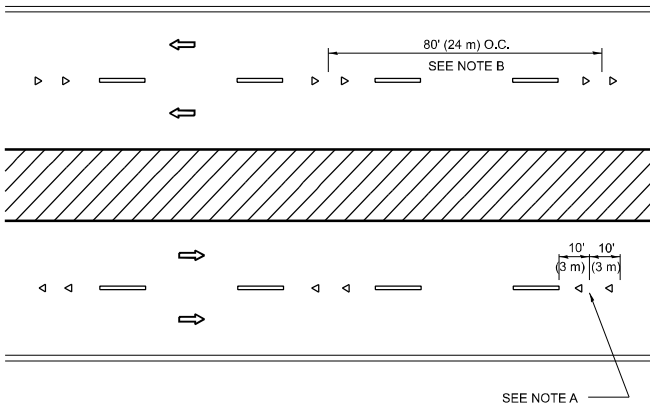
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

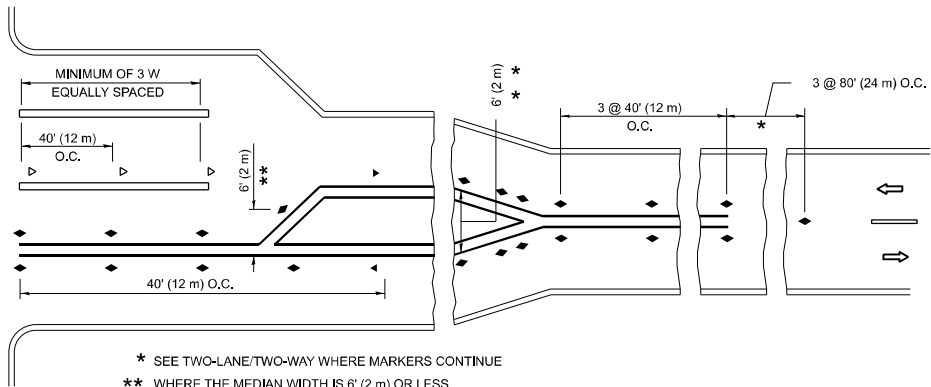
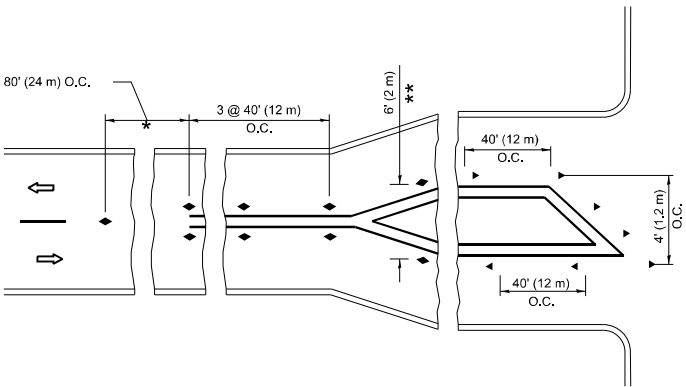
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

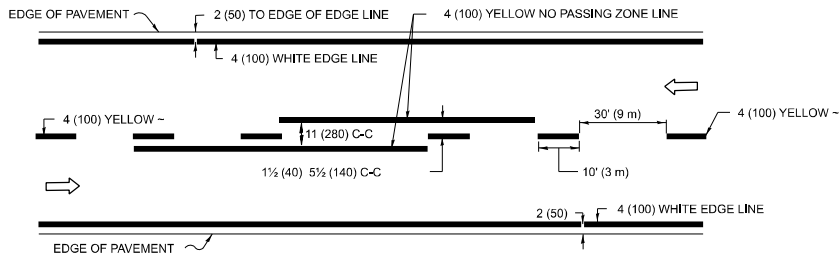
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

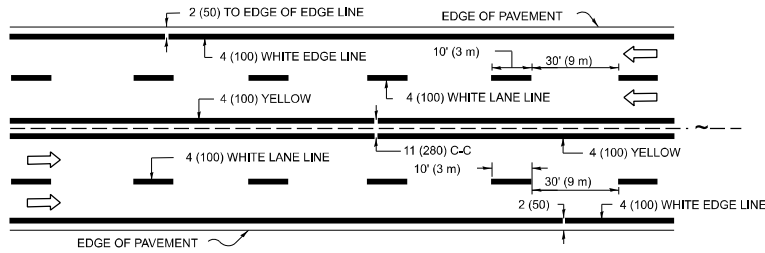
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	22
TC-11		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		

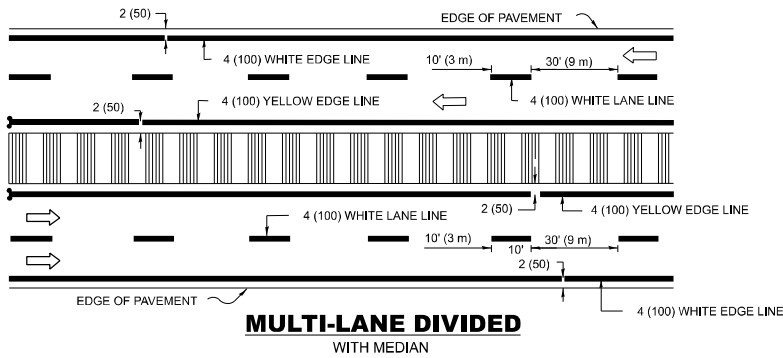
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FILE NAME: c:\p\work\dwg\hamw\md1063787\ID117225-shh-DistStus.dgn



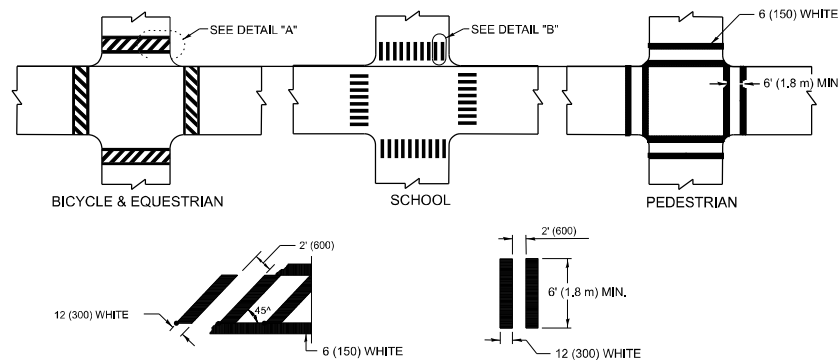
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING

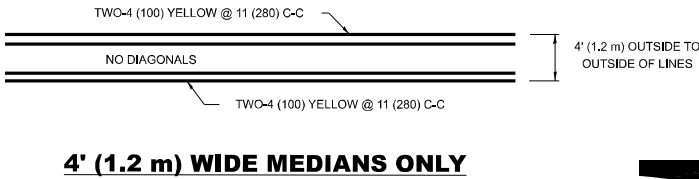


DETAIL "A"

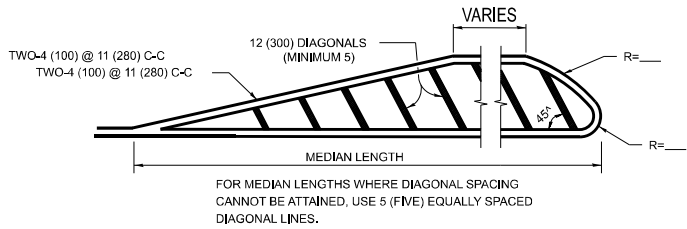
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

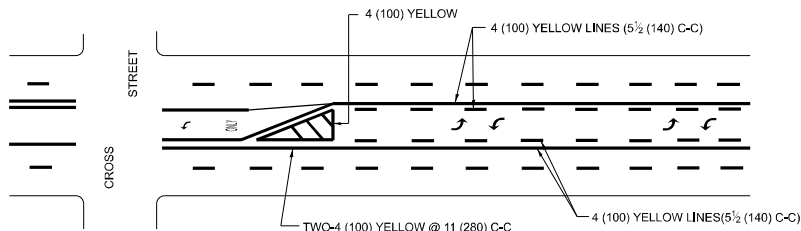


4' (1.2 m) WIDE MEDIANS ONLY



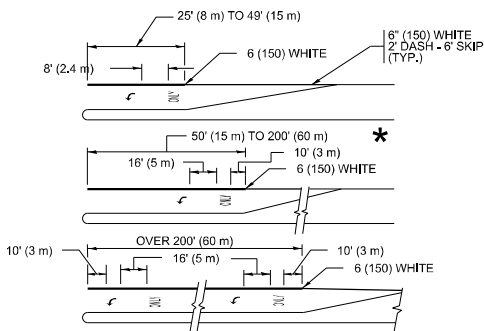
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

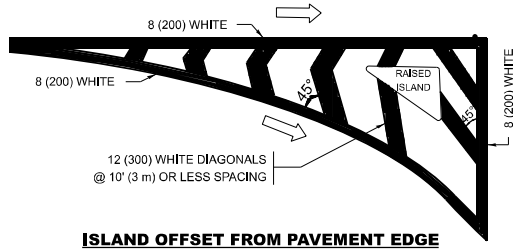


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

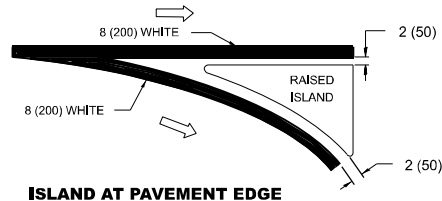
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

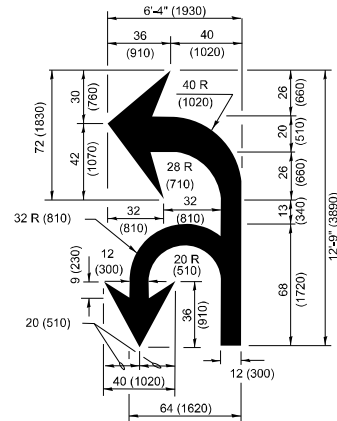


ISLAND OFFSET FROM PAVEMENT EDGE

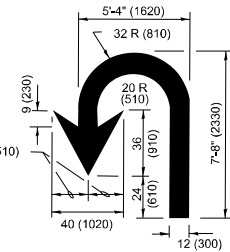


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE-FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters)
unless otherwise shown.

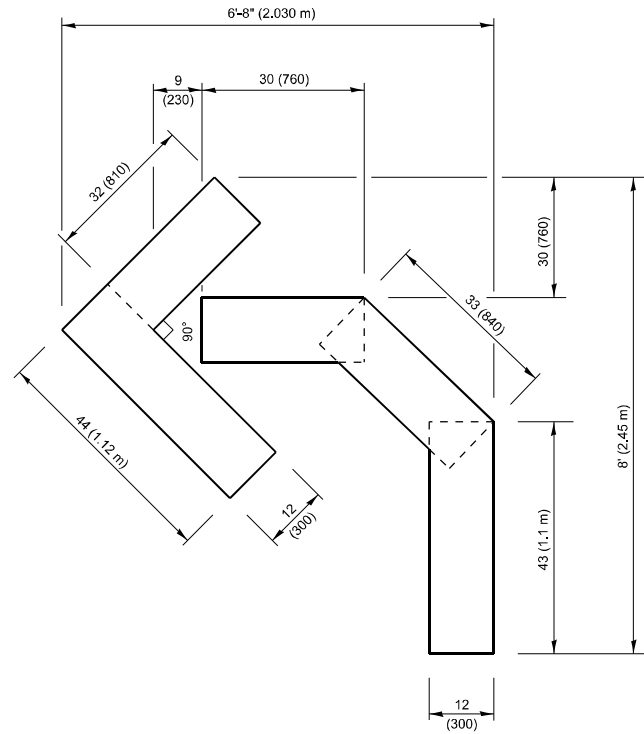
USER NAME = mohammad.hamwi	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 8/14/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

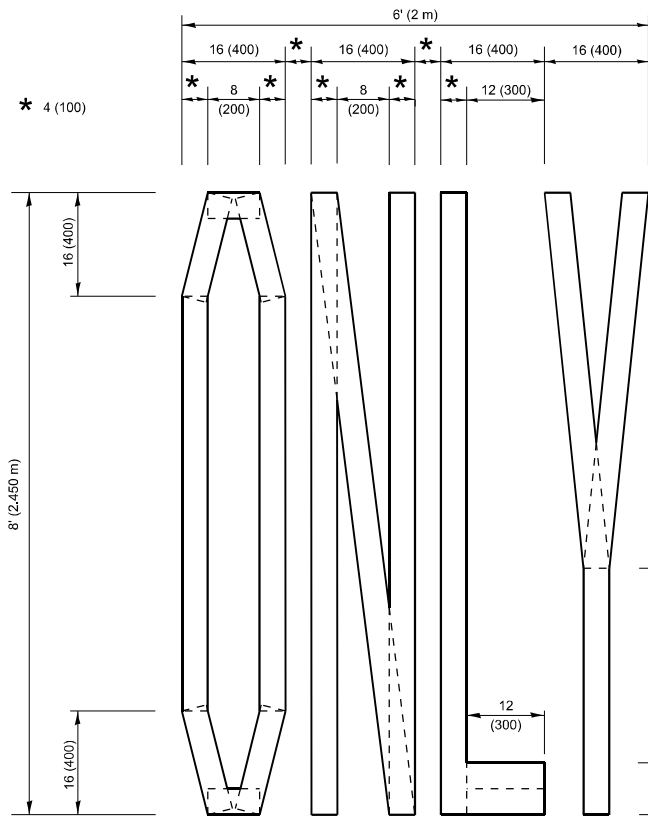
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	23
TC-13		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		



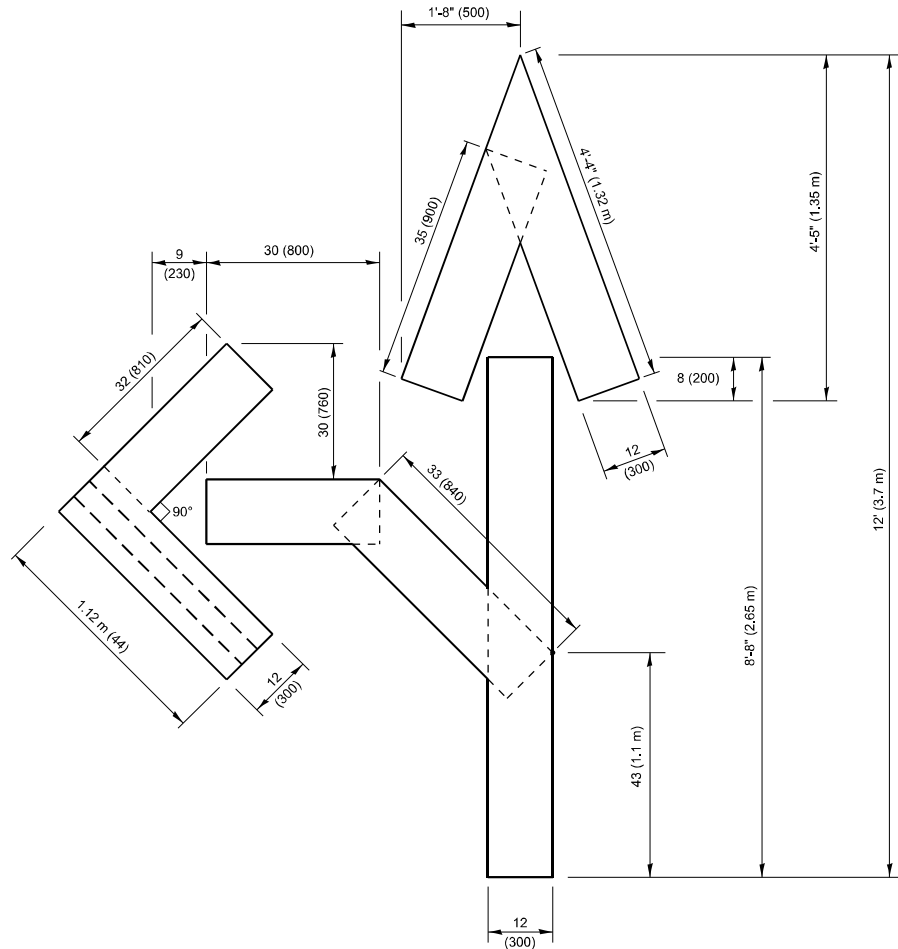
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

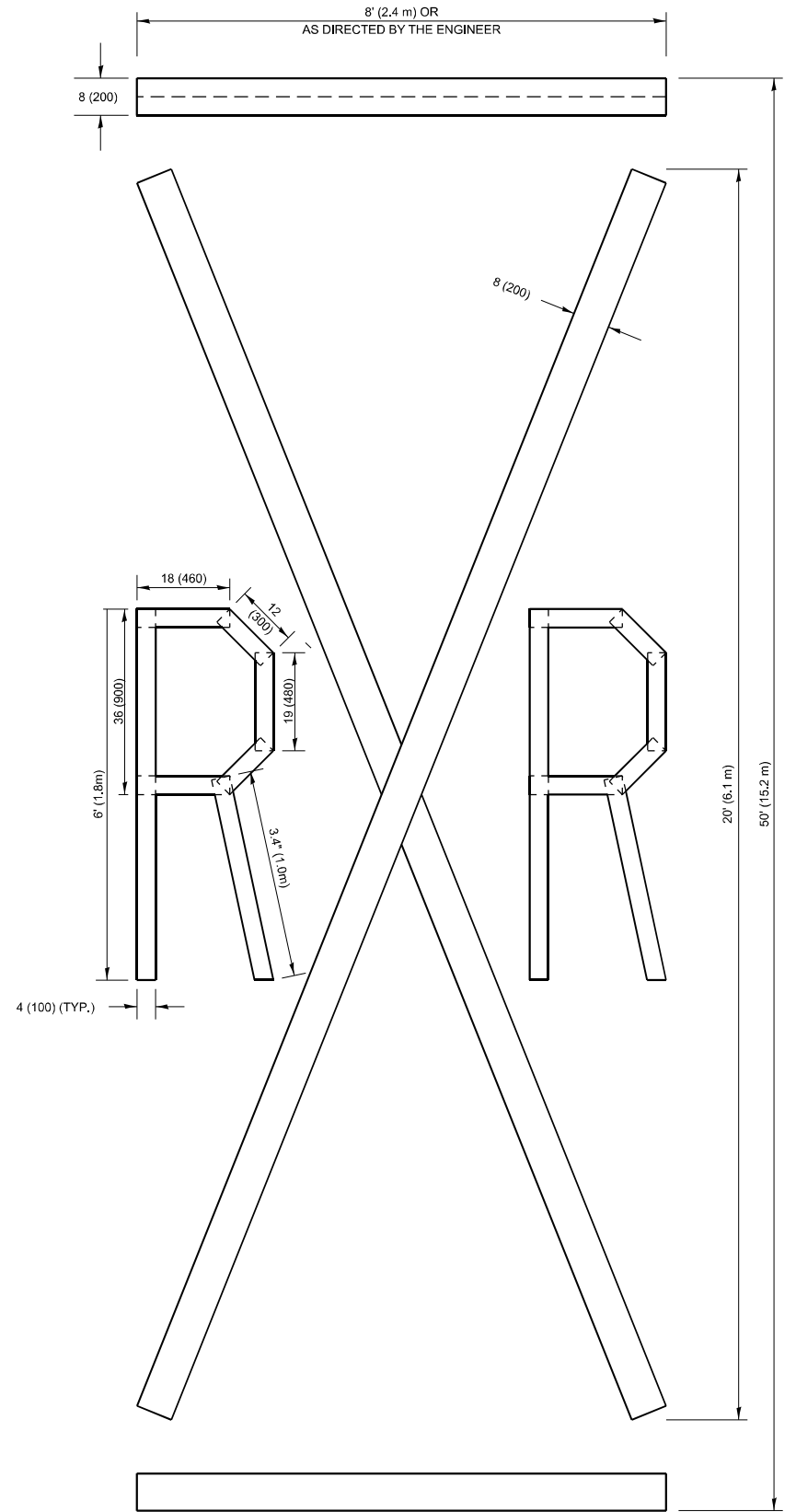


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: TC-16-1 [Sheet]
FILE NAME: c:\p\work\pav\hamw\md1063787\ID117225-sht-DiaStatus.dgn

PLOT DATE = 8/9/2025	USER NAME = mohammad.hamwi	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
		DRAWN -	REVISED - E. GOMEZ 08-28-00
		CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94		REVISED - A. SCHUETZE 09-15-16

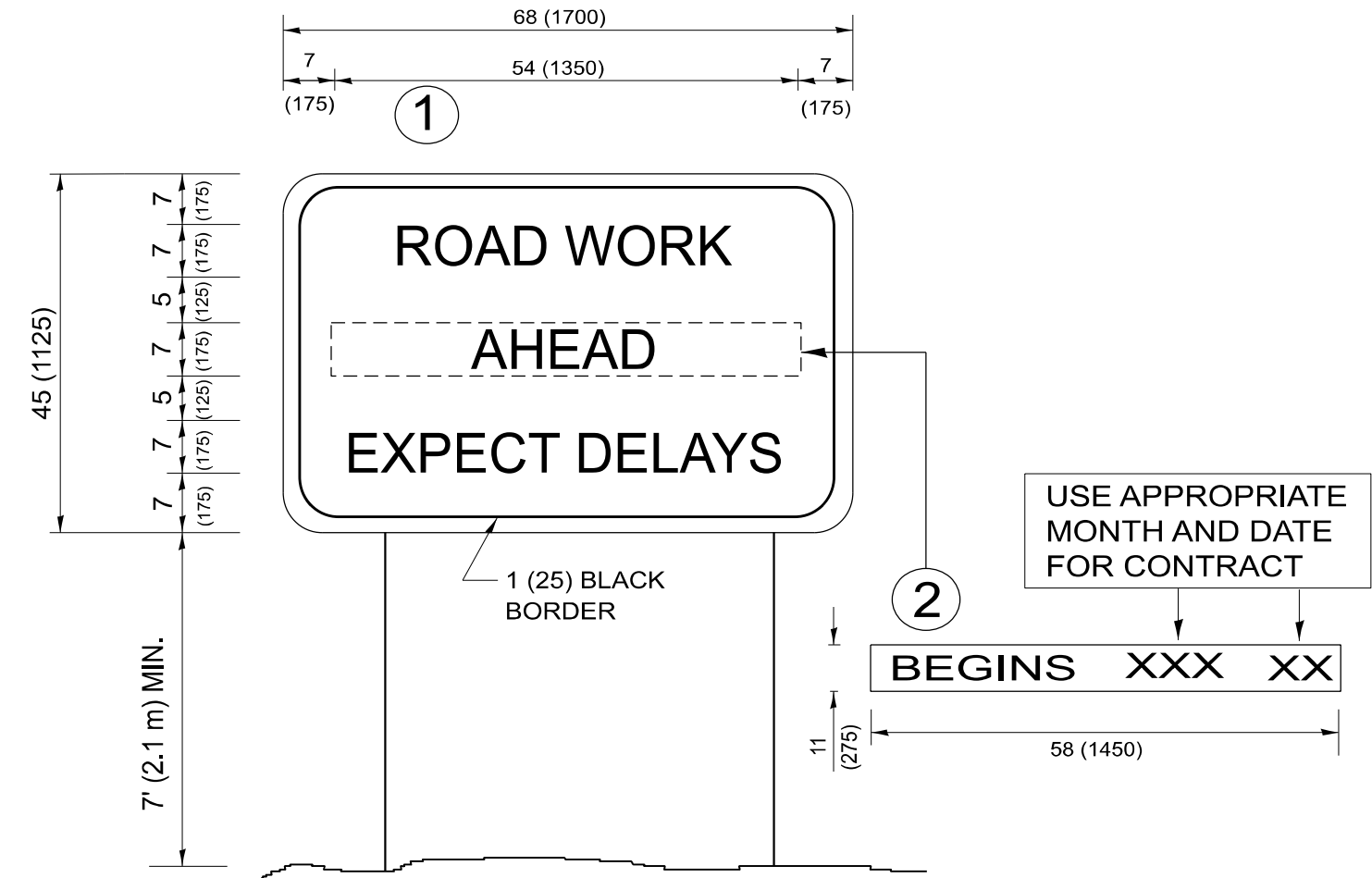
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	24
TC-16		CONTRACT NO. 80B19		
		ILLINOIS	FED. AID PROJECT	

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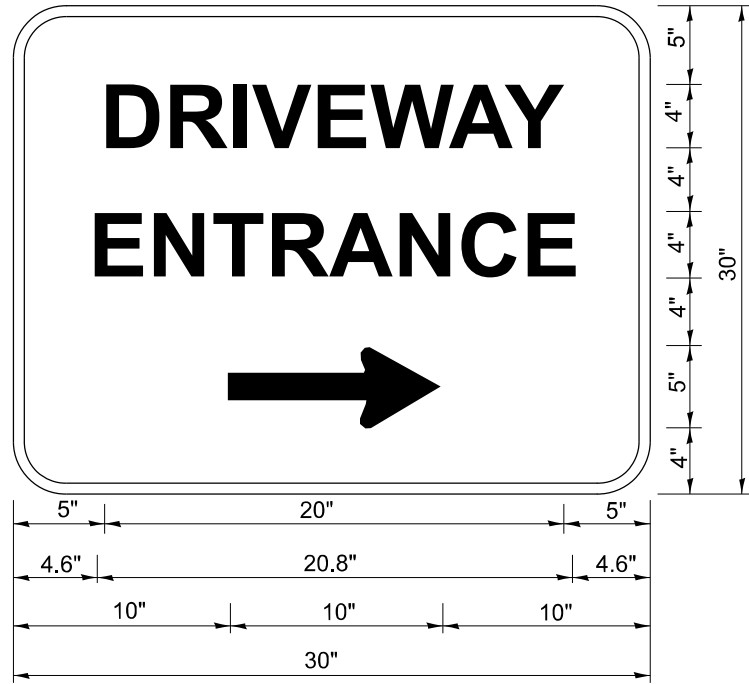


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = mohammad.hamwi	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97						876	2025-1093-RS	COOK	33	25
		CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22		CONTRACT NO. 80B19		
	PLOT DATE = 8/14/2025	DATE -	REVISED - C. JUCIUS 01-31-07						ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:**
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26-1 [Sheet]
FILE NAME: c:\p\work\p\work\hamw\md1063787\ID117225-sht-DistSign.dgn

	USER NAME = mohammad.hamwi	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					876	2025-1093-RS	COOK	33	26
		CHECKED -	REVISED -		TC-26			CONTRACT NO. 80B19				
	PLOT DATE = 8/9/2025	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

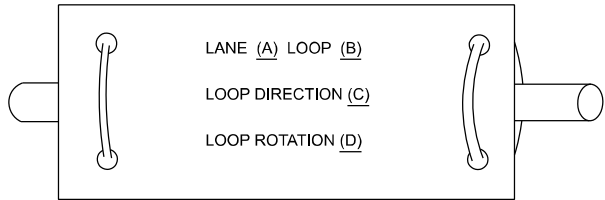
ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

MODEL: TS-05A [Sheet] FILE NAME: c:\pdk_work\pdktohamwi\md1063787\ID117225-shh-DistSigus.dgn	USER NAME = mohammad.hamwi		DESIGNED - IP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN - IP	REVISED -					876	2025-1093-RS	COOK	33	26A
			CHECKED - LP	REVISED -					TS-05		CONTRACT NO. 80B19		
	PLOT DATE = 11/19/2025		DATE - 9/29/2016	REVISED -		SCALE: NONE	SHEET 1	OF 7 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT	

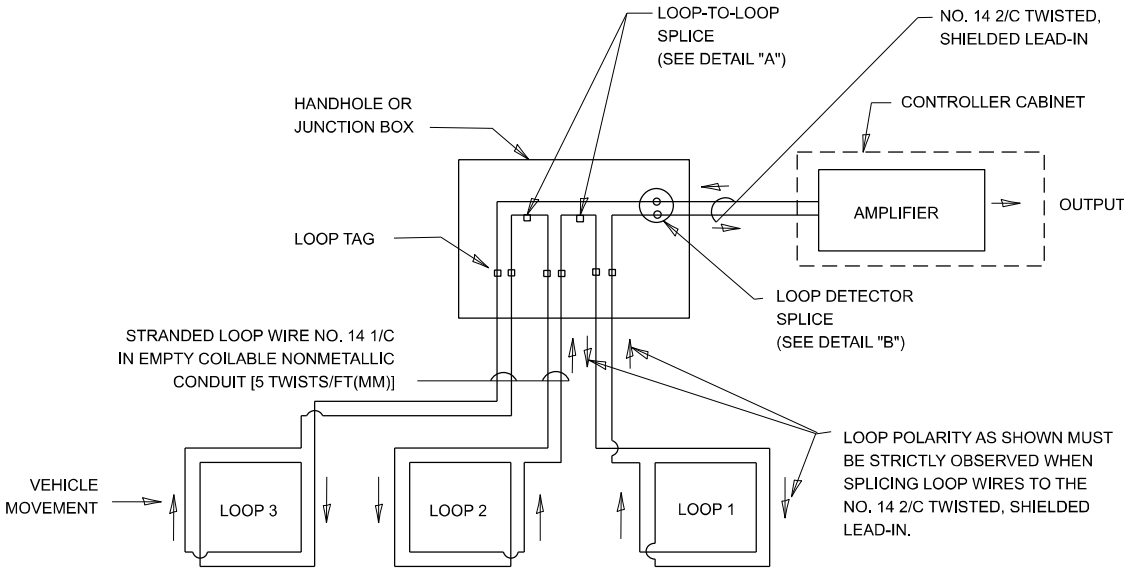
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE, SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE
7. PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

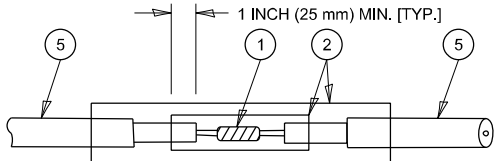


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

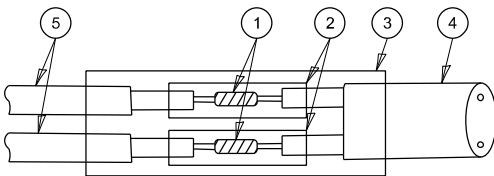


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

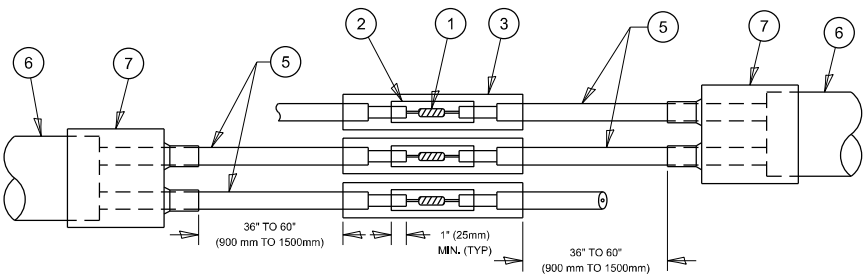


DETAIL "A"
LOOP-TO-LOOP SPLICE

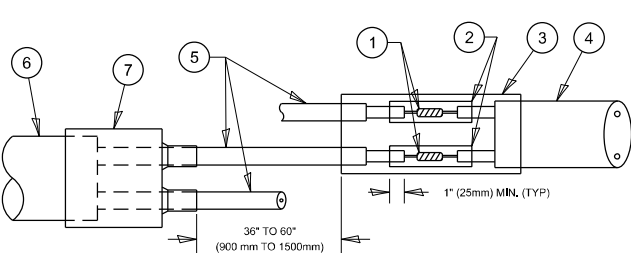


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

1. WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
2. WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
3. WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
4. NO. 14 2/C TWISTED, SHIELDED CABLE.
5. LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
6. XL POLYOLEFIN 2 CONDUCTOR
7. BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

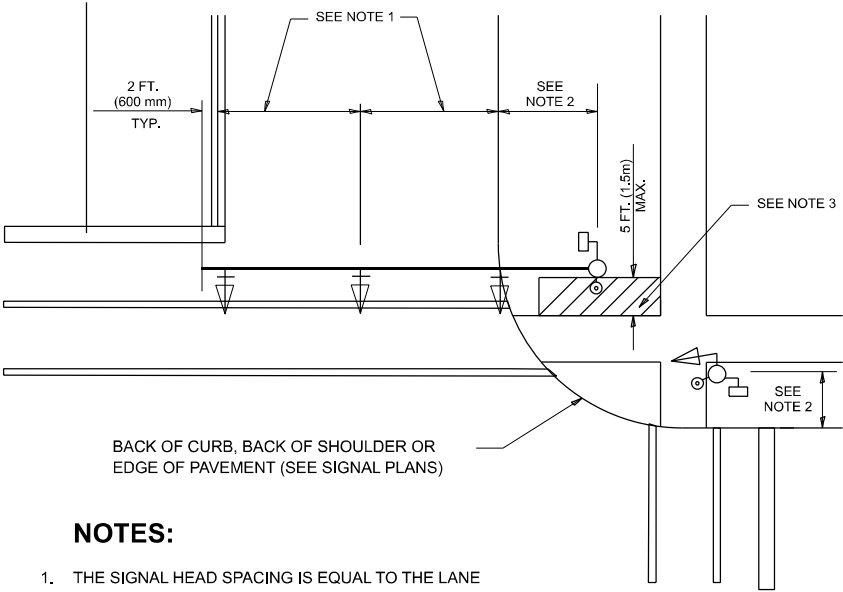
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	26B
TS-05		CONTRACT NO. 80B19		
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TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

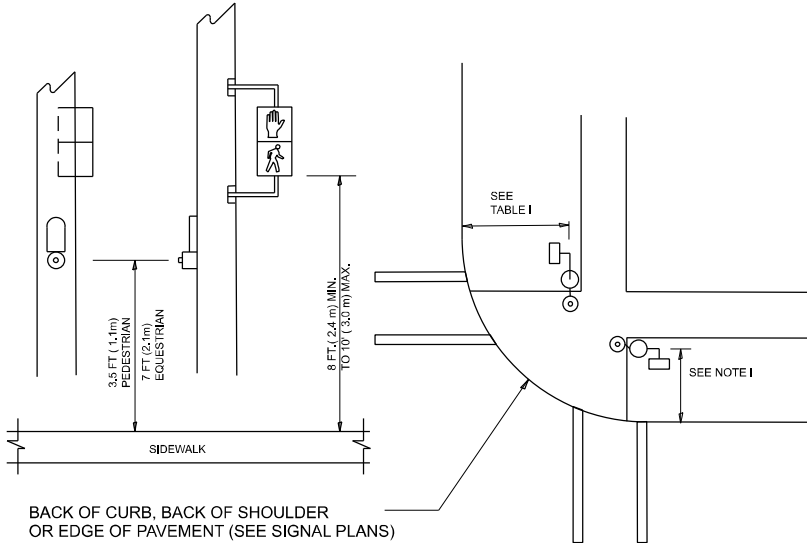
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

- 1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

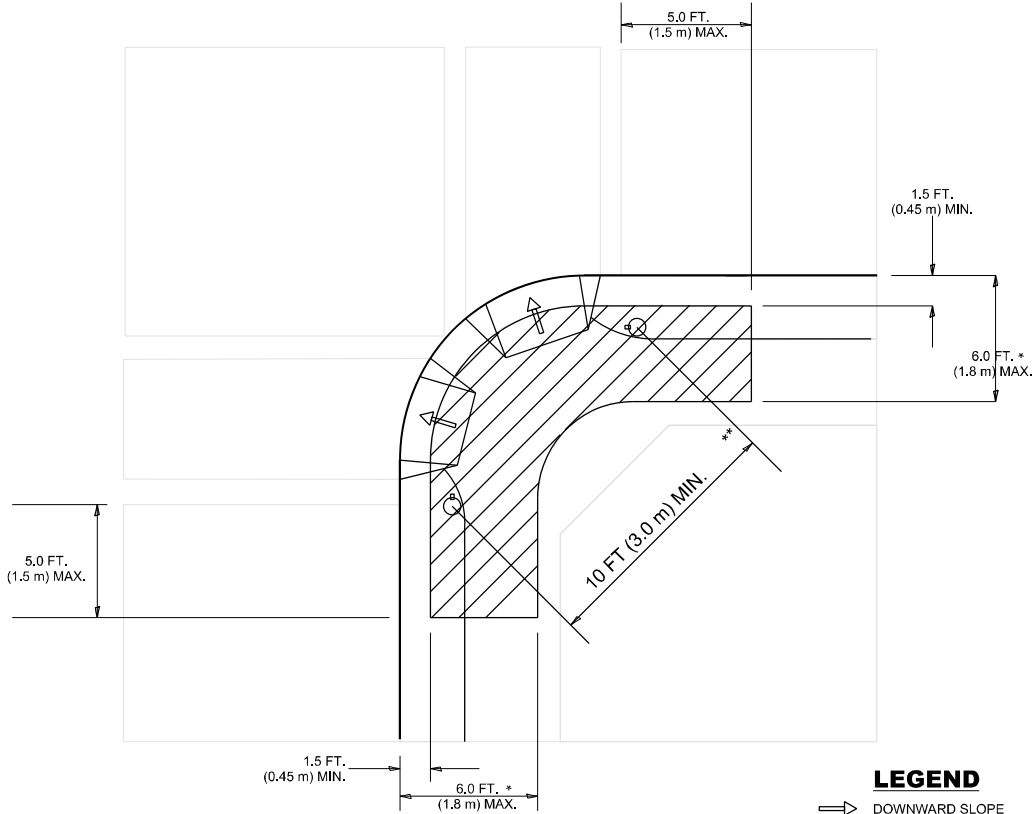
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- 1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- 2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

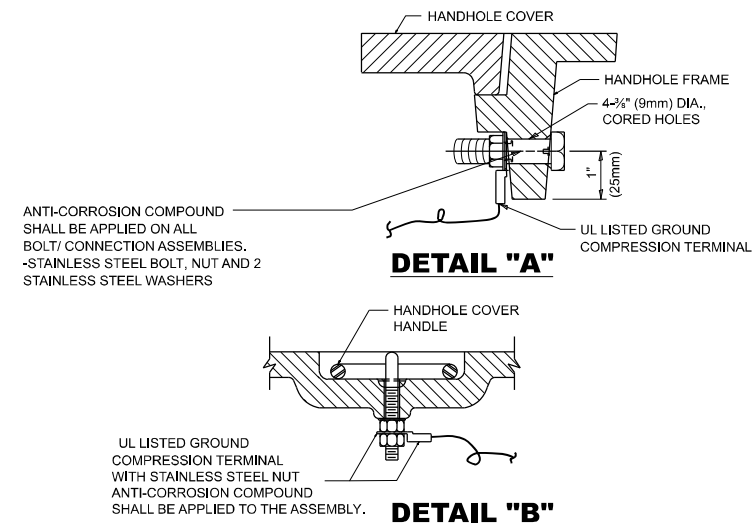
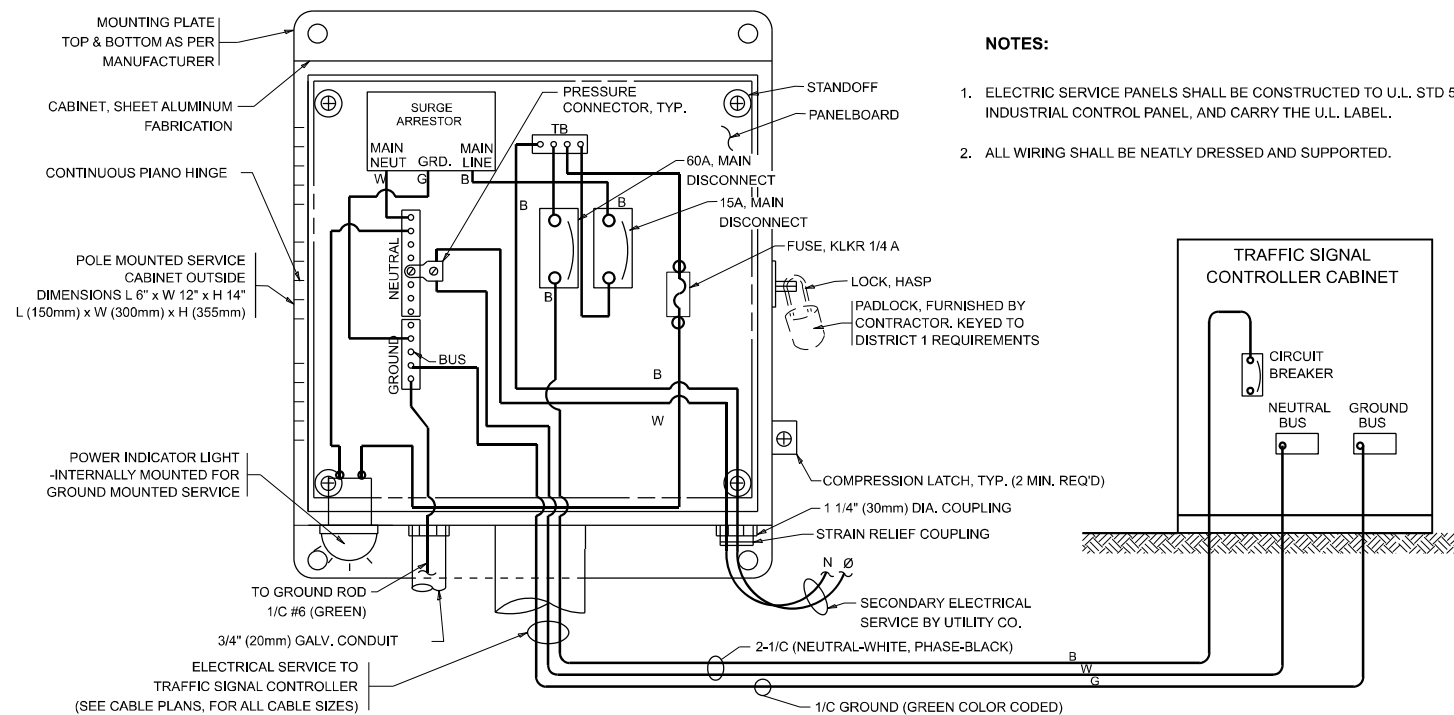
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

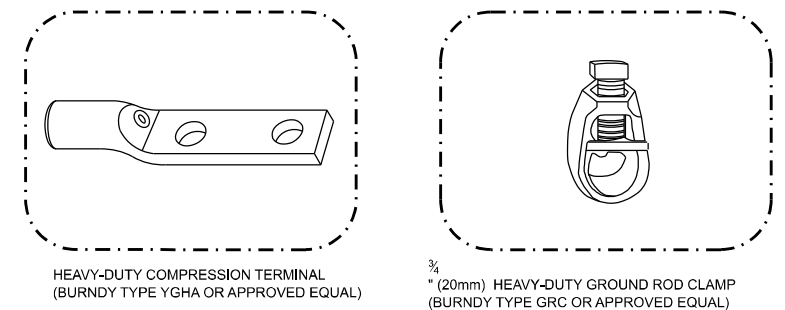
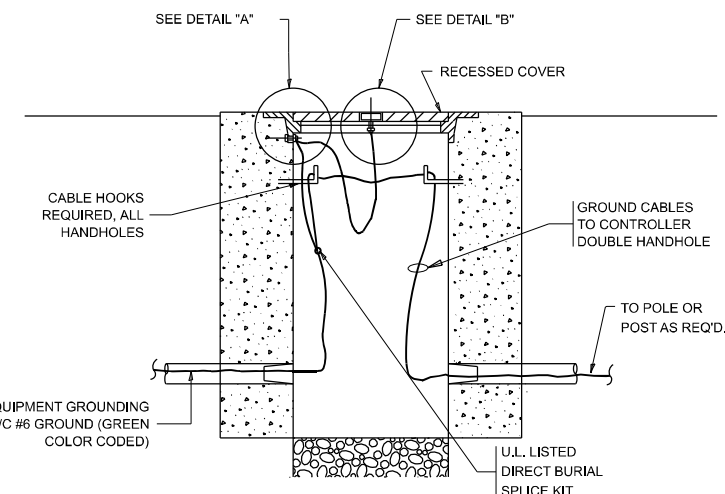
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		DRAWN -	REVISED -						876	2025-1093-RS	COOK	33	26C	
		CHECKED -	REVISED -						TS-05					
	PLOT DATE = 11/19/2025	DATE -	REVISED -		SCALE: NONE		SHEET 3	OF 7 SHEETS	STA.	TO STA.		CONTRACT NO. 80B19		
											ILLINOIS		FED. AID PROJECT	



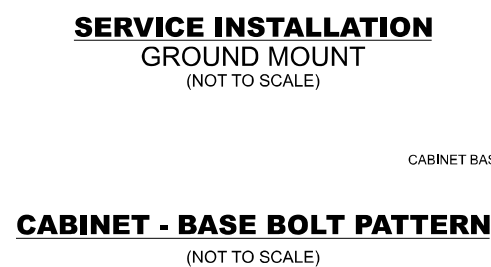
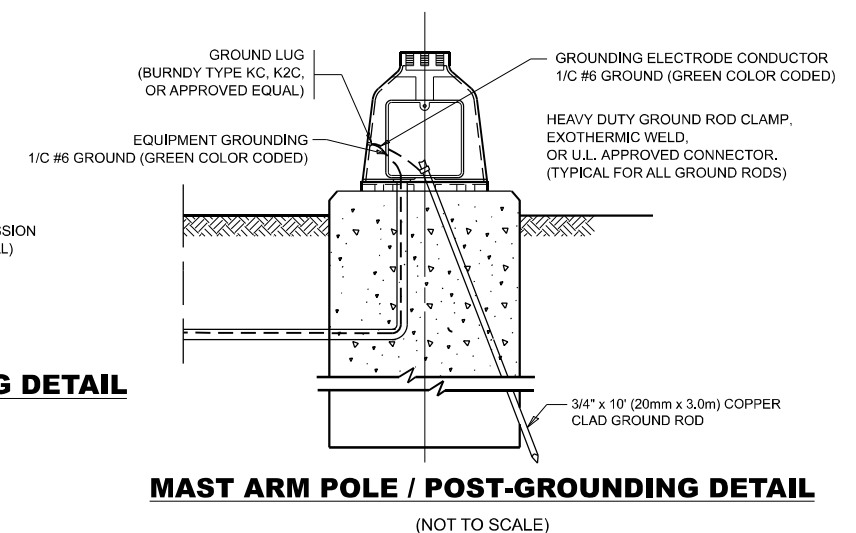
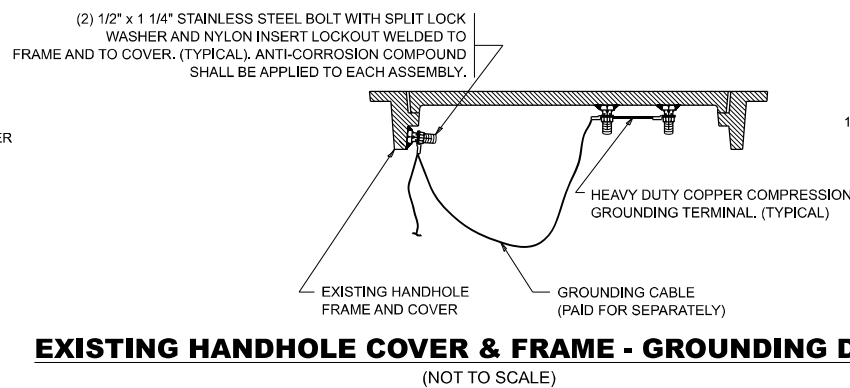
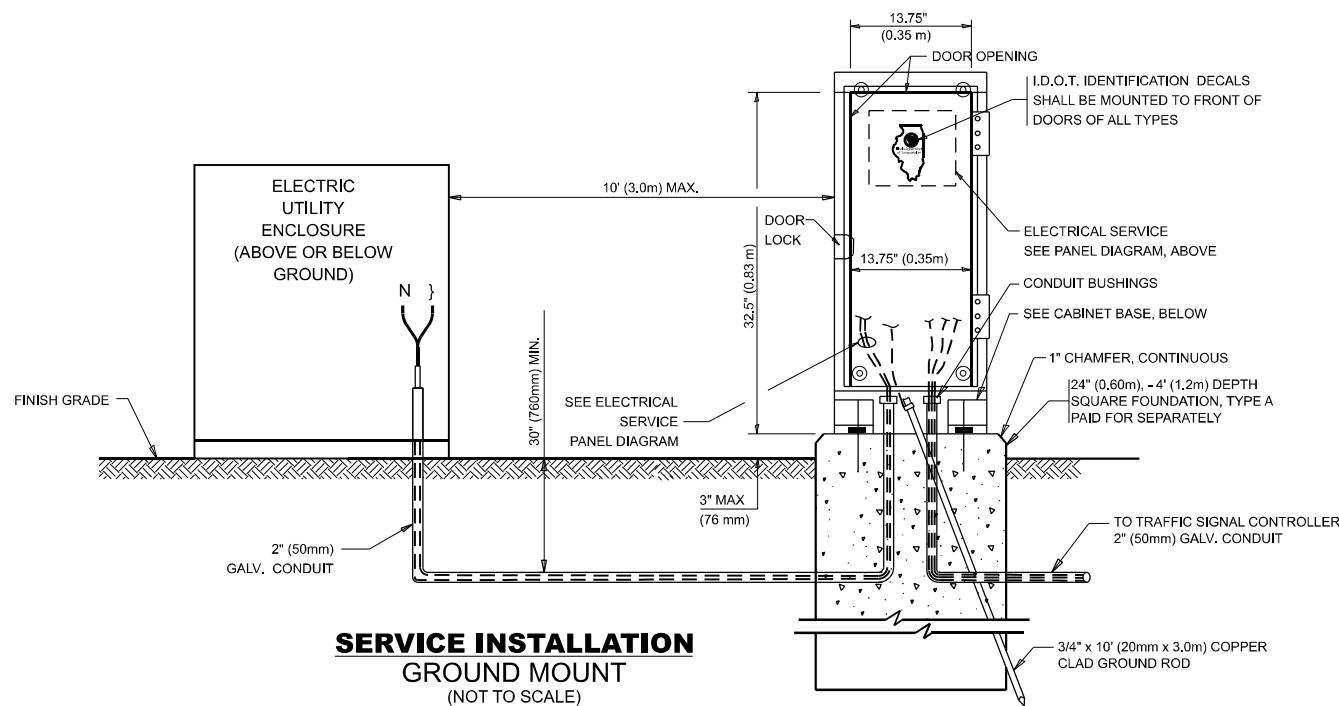
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MASTER ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



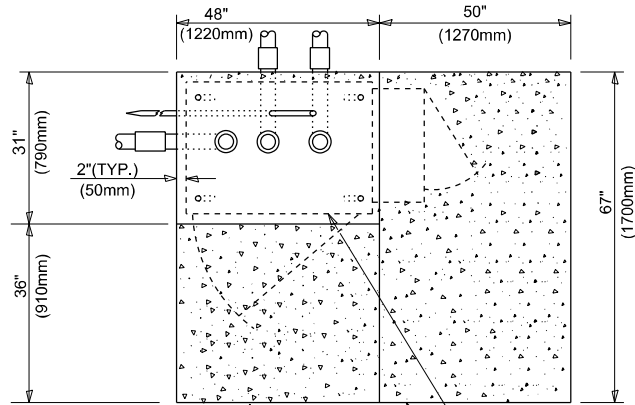
USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
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PLOT DATE = 11/19/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

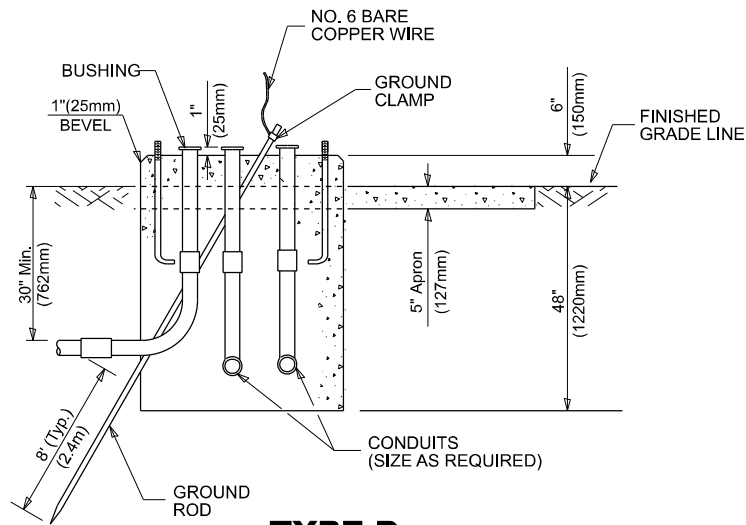
DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE	SHEET 4	OF 7	SHEETS	STA.	TO STA.
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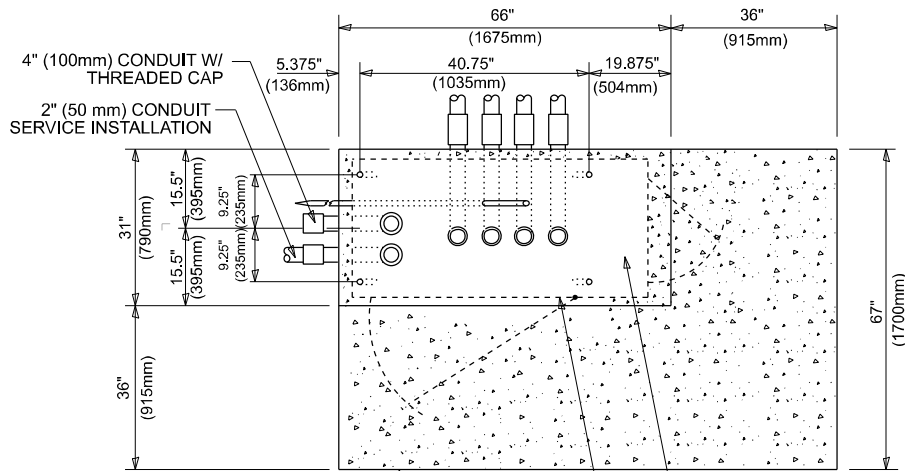
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	26D
TS-05		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		



TOP VIEW



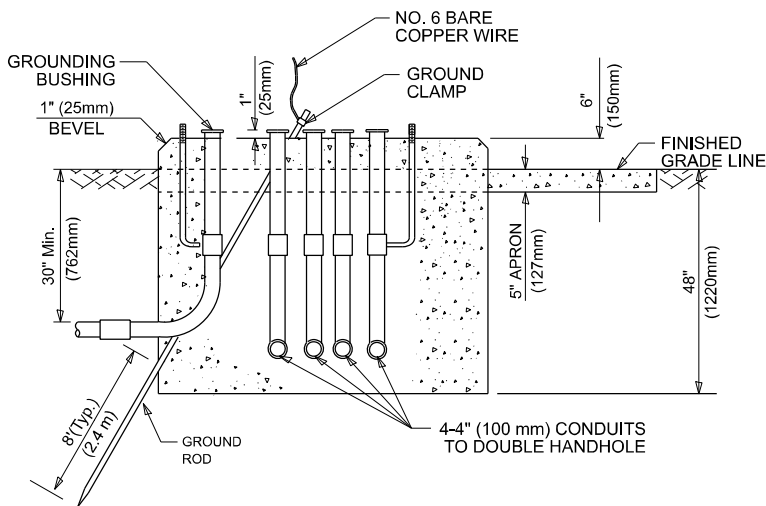
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



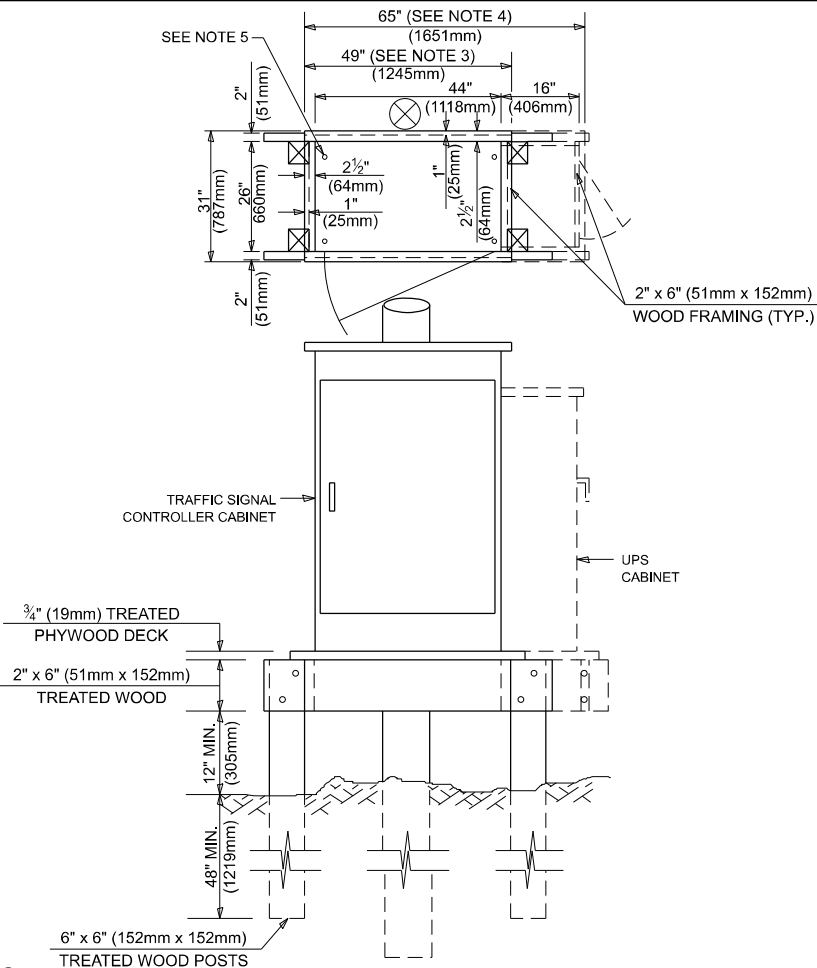
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL
BE HIGHER THAN TOP OF
DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

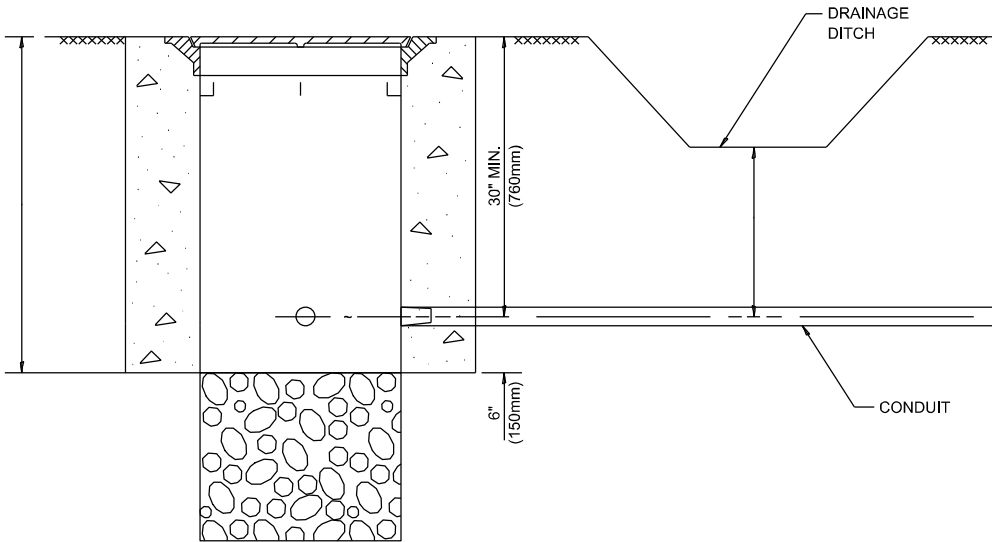
NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

MODEL: TS-05E [Sheet]
FILE NAME: c:\p\work\tdh\hamwi\1063787\1063787-011725-shd-Distatus.dgn

	USER NAME = mohammad.hamwi	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						876	2025-1093-RS	COOK	33	26E
		CHECKED -	REVISED -						TS-05		CONTRACT NO. 80B19		
	PLOT DATE = 11/19/2025	DATE -	REVISED -						ILLINOIS		FED. AID PROJECT		
	SCALE: NONE		SHEET 5						OF 7 SHEETS	STA.	TO STA.		

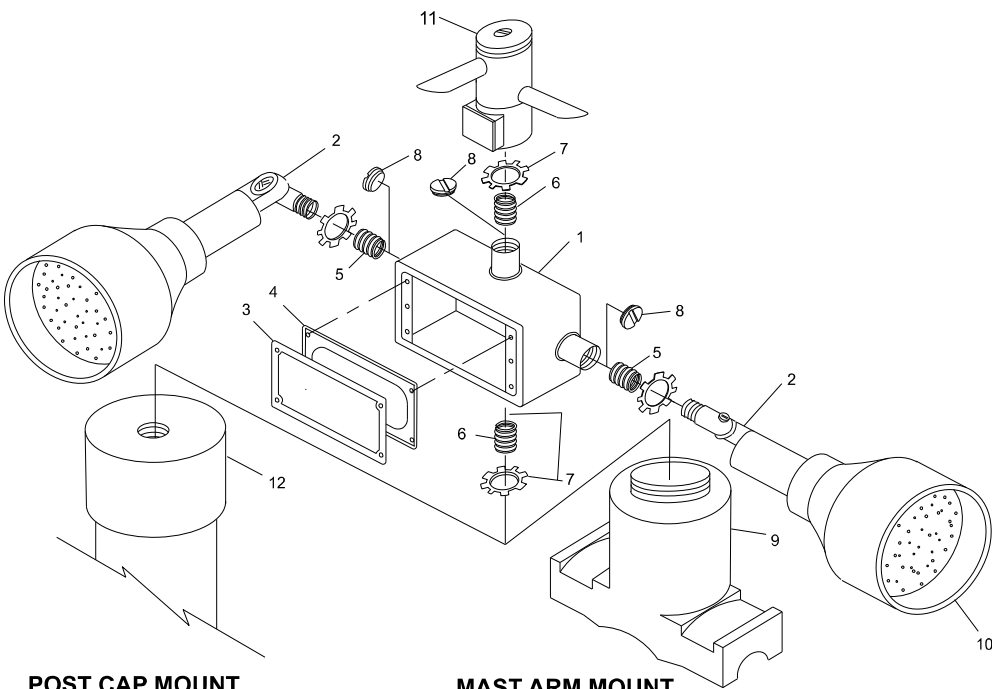


NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

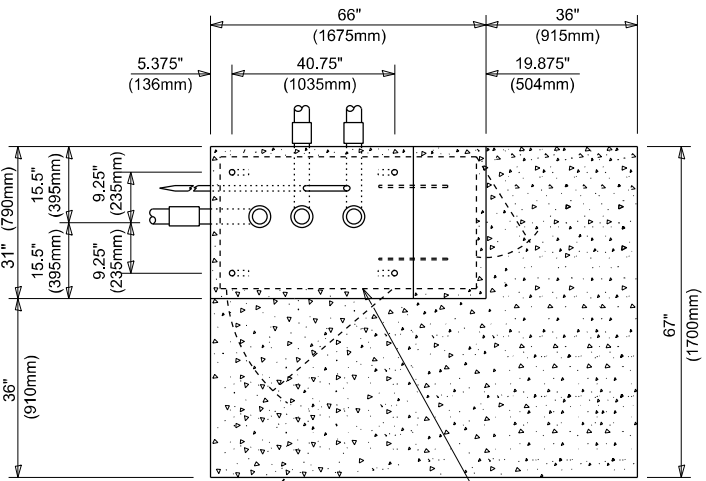
(NOT TO SCALE)



POST CAP MOUNT

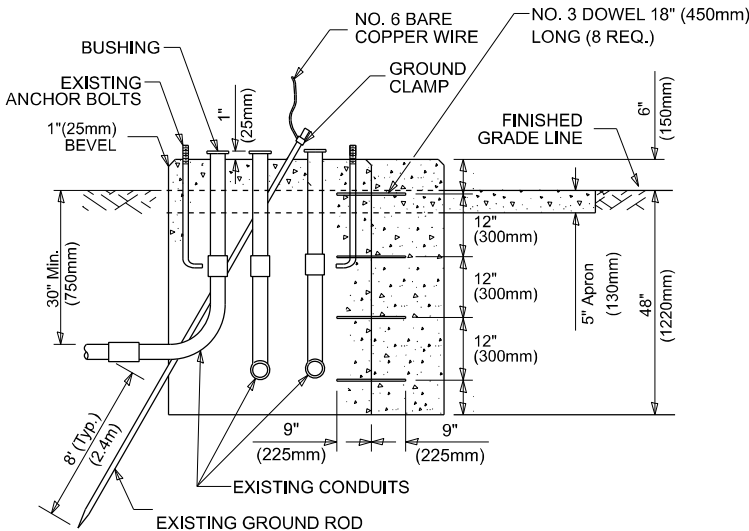
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW

(NOT TO SCALE)



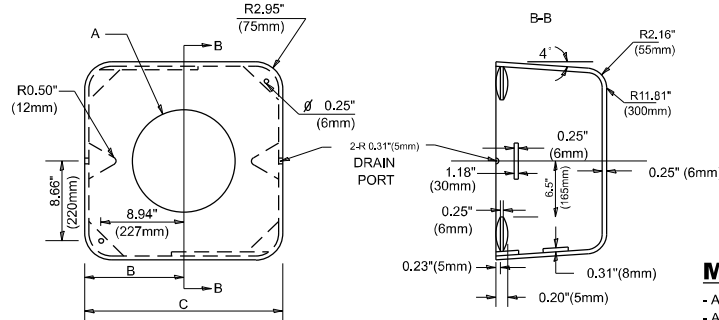
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9-"BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

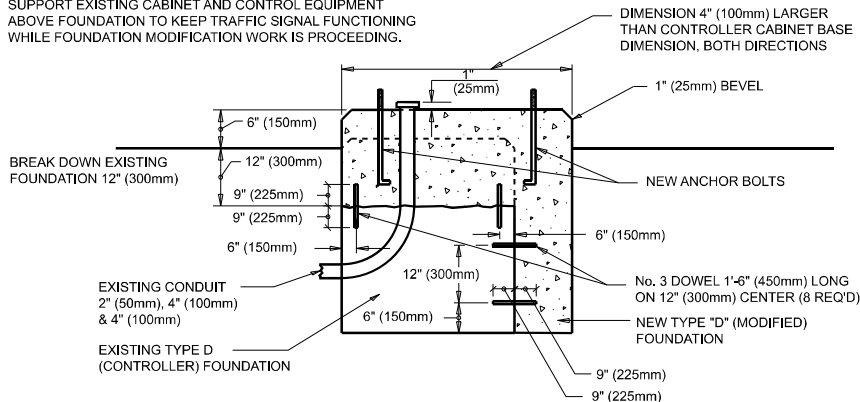
SHROUD

NOTES:

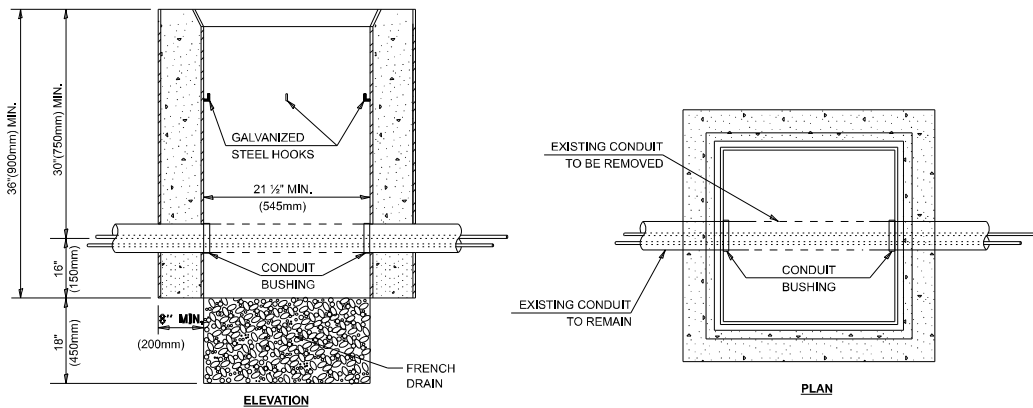
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

MODEL: TS-05F [Sheet]
FILE NAME: c:\pdk_work\pdk\hamwil\1063787\1063787-117225-sht-Distatus.dgn

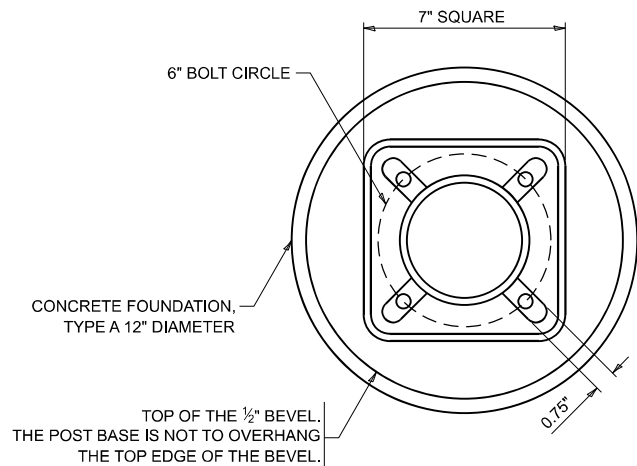
USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 11/19/2025	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

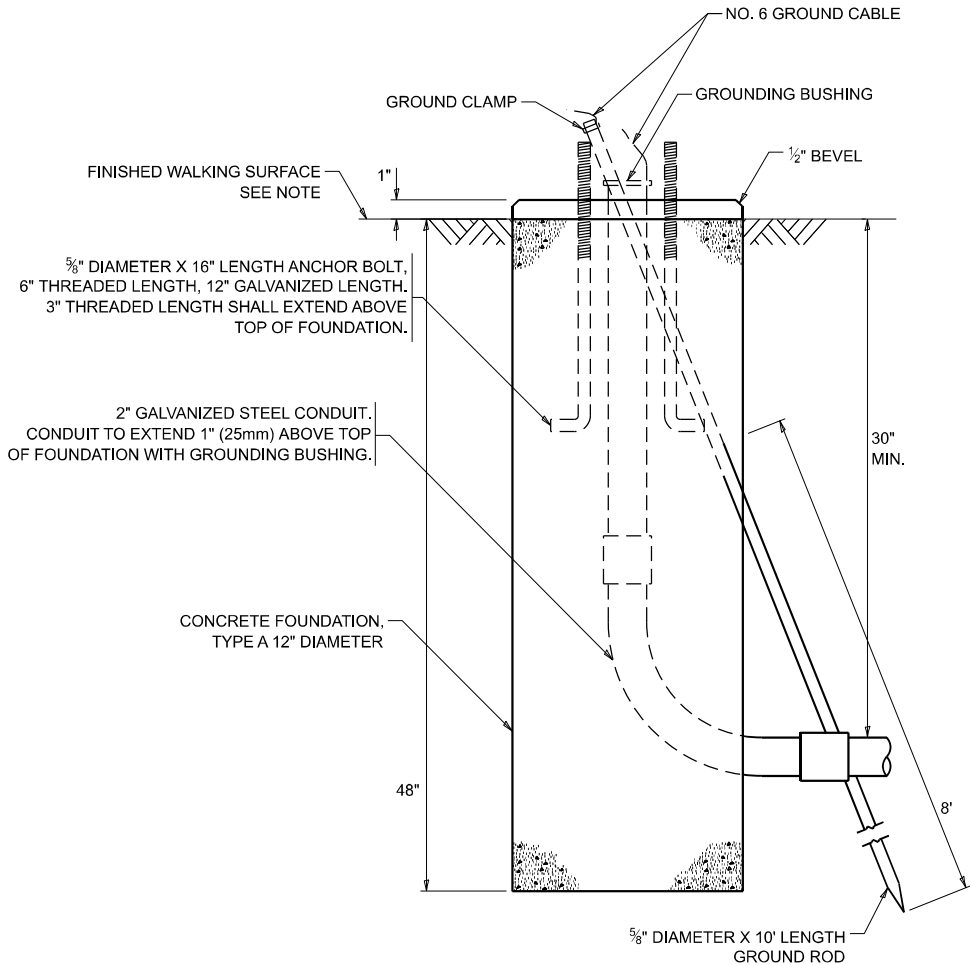
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	26F
TS-05		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		



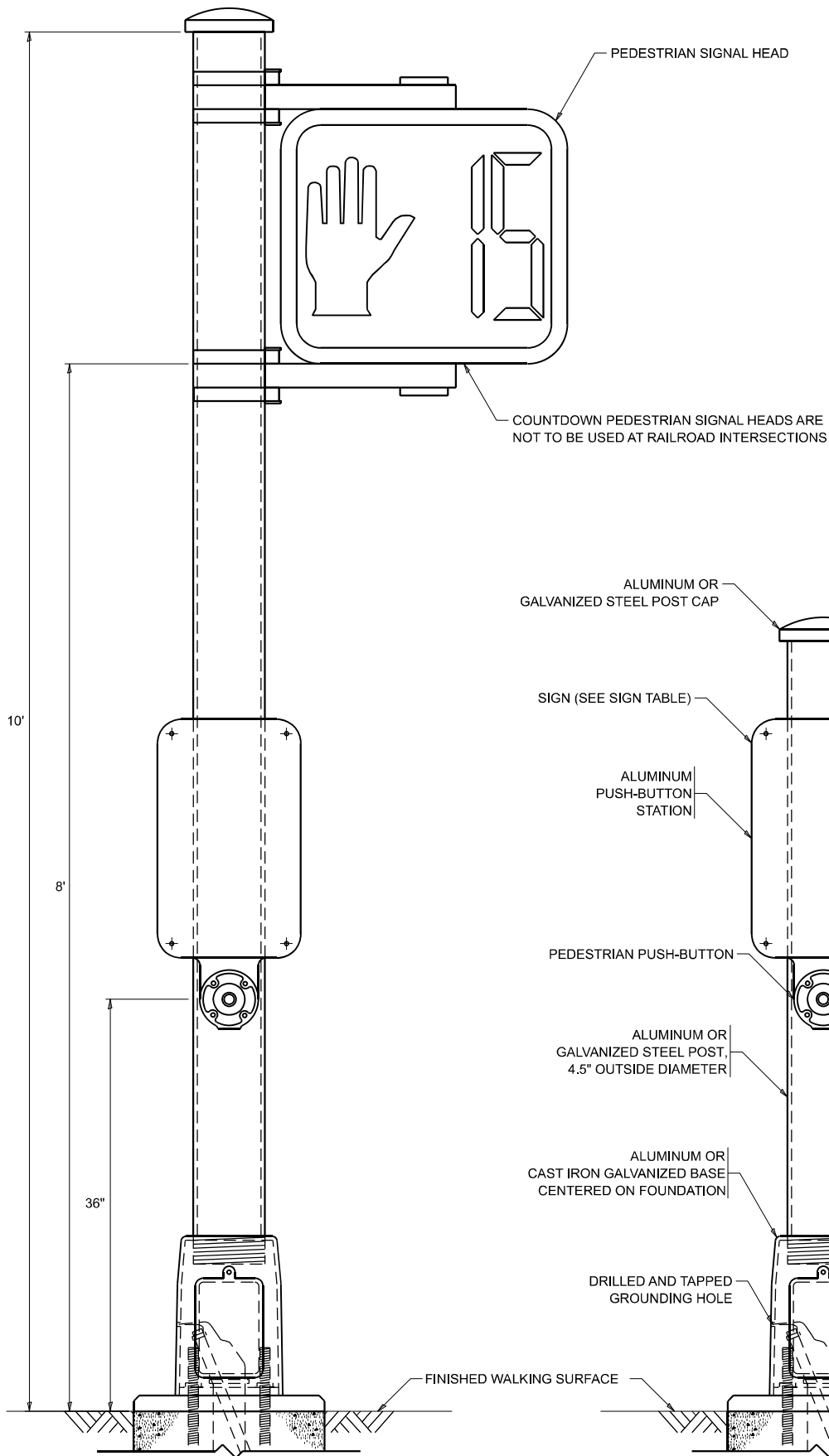
BOLT PATTERN

NOTE:

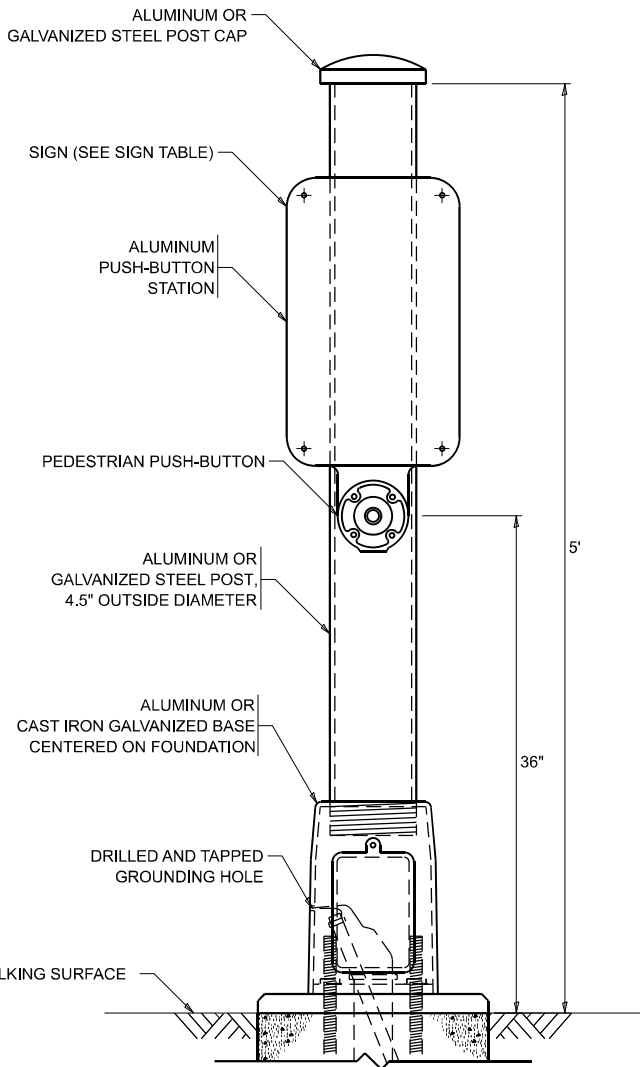
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



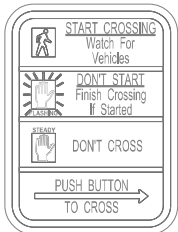
**CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER**



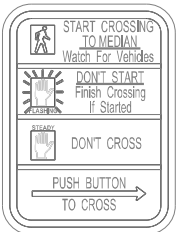
PEDESTRIAN SIGNAL POST, 10 FT.



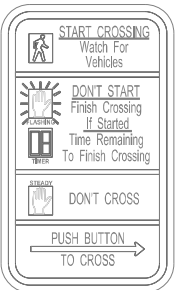
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

PEDESTRIAN SIGNAL HEAD

COUNTDOWN PEDESTRIAN SIGNAL HEADS ARE NOT TO BE USED AT RAILROAD INTERSECTIONS

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

MODEL: TS-05G (Sheet)
FILE NAME: c:\p\work\hamwi\1063787\1063787-117225-sht-DistSig.dgn

USER NAME = mohammad.hamwi	DESIGNED - IP	REVISED - 10-15-2020
	DRAWN - IP	REVISED -
	CHECKED - LP	REVISED -
PLOT DATE = 11/19/2025	DATE - 10-15-2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

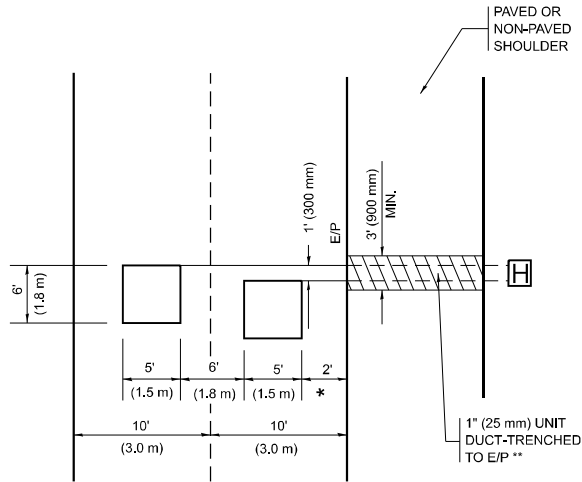
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	26G
TS-05		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

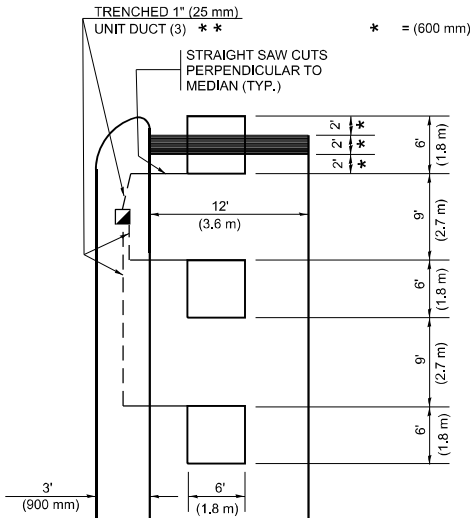


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

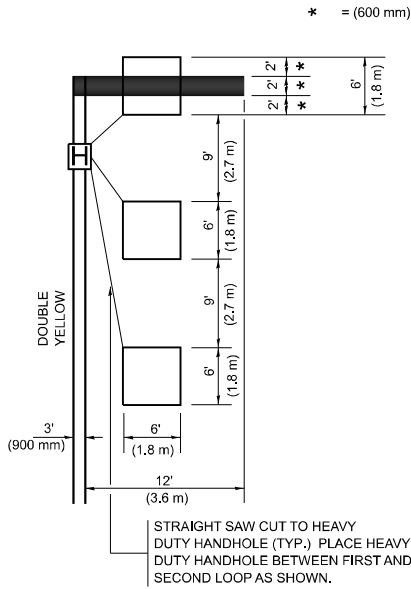


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

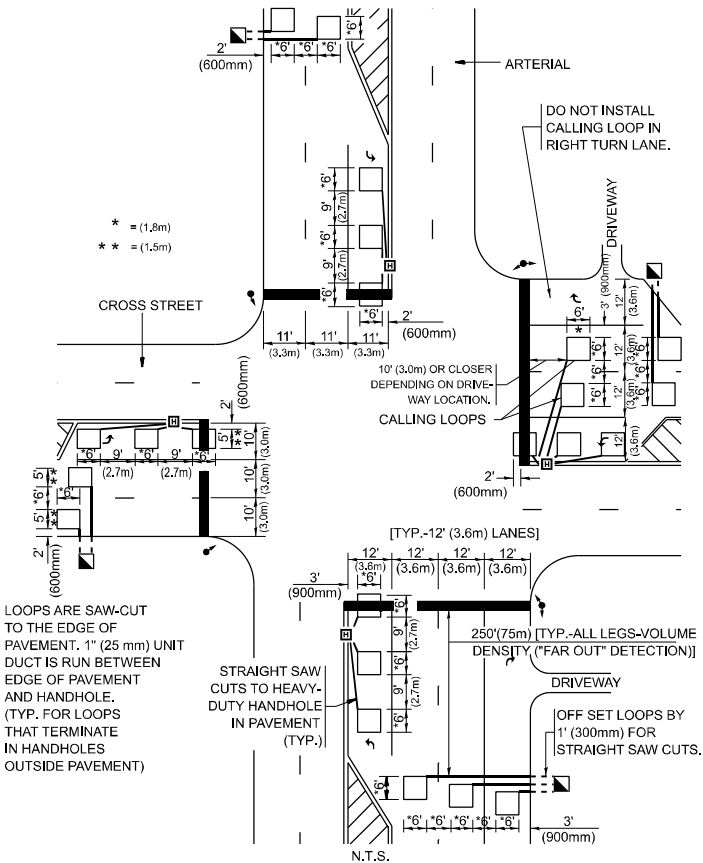
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



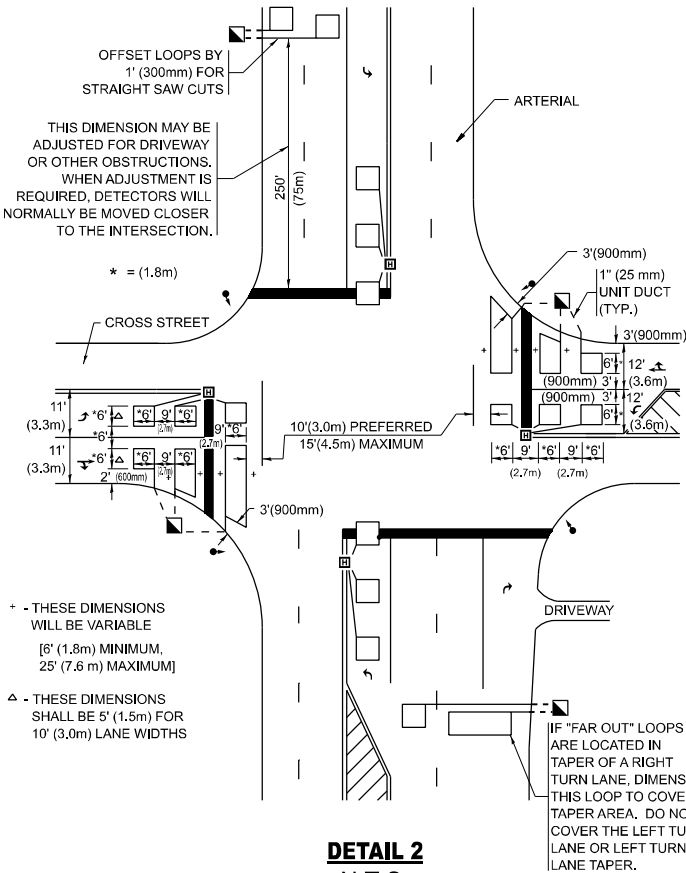
* = (1.8m)
** = (1.5m)

LOOPS ARE SAW-CUT
TO THE EDGE OF
PAVEMENT. 1" (25 mm) UNIT
DUCT IS RUN BETWEEN
EDGE OF PAVEMENT
AND HANDHOLE.
(TYP. FOR LOOPS
THAT TERMINATE
IN HANDHOLES
OUTSIDE PAVEMENT)

STRAIGHT SAW
CUTS TO HEAVY-
DUTY HANDHOLE
IN PAVEMENT
(TYP.)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



OFFSET LOOPS BY
1' (300mm) FOR
STRAIGHT SAW CUTS

THIS DIMENSION MAY BE
ADJUSTED FOR DRIVEWAY
OR OTHER OBSTRUCTIONS.
WHEN ADJUSTMENT IS
REQUIRED, DETECTORS WILL
NORMALLY BE MOVED CLOSER
TO THE INTERSECTION.

* = (1.8m)

- THESE DIMENSIONS
WILL BE VARIABLE
[6' (1.8m) MINIMUM,
25' (7.6 m) MAXIMUM]

- THESE DIMENSIONS
SHALL BE 5' (1.5m) FOR
10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

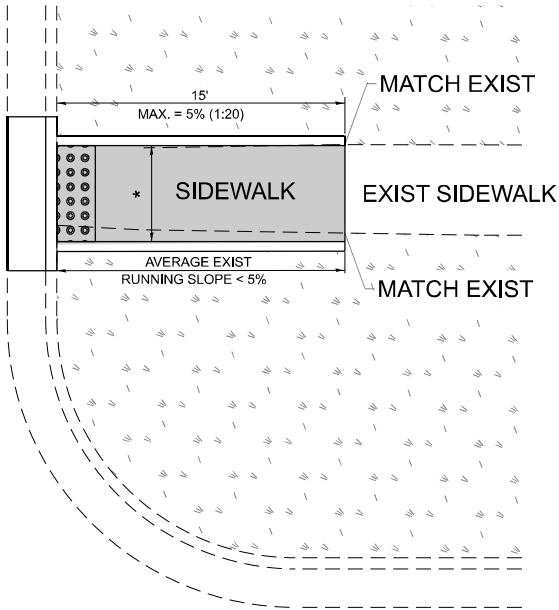
DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

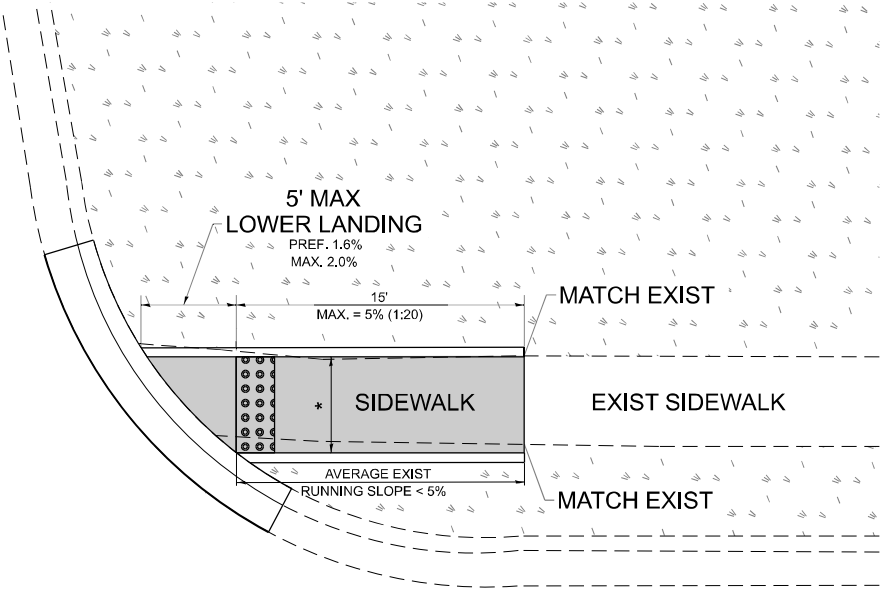
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	27
TS-07		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

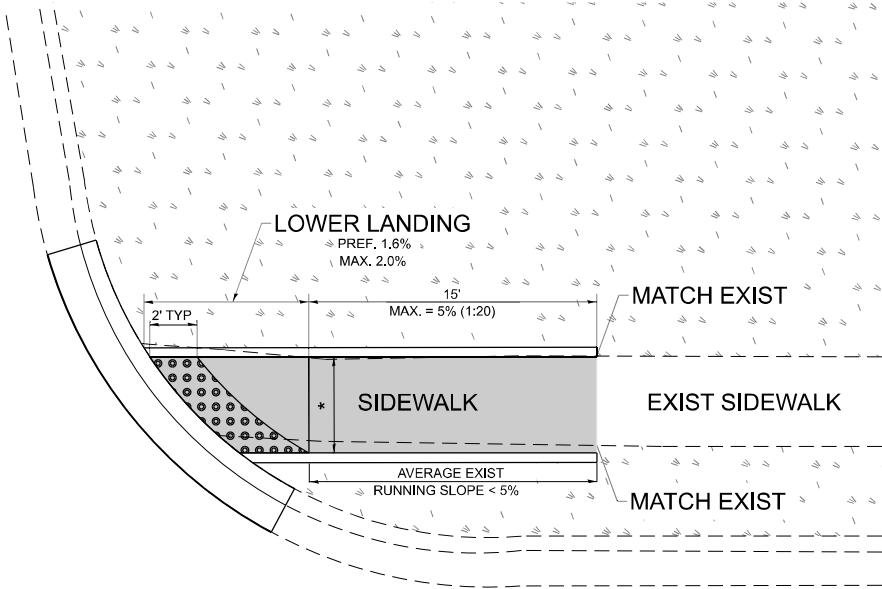
PD-01A



PD-01B



PD-01C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

EXIST. GRASS

PROPOSED SIDEWALK

PROPOSED SIDE CURB

DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

USER NAME	= mohammad.hamwi	DESIGNED	-	REVISED	-
DRAWN	-	R. LEDEZMA		REVISED	-
CHECKED	-			REVISED	-
DATE	-	10/02/2019		REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)

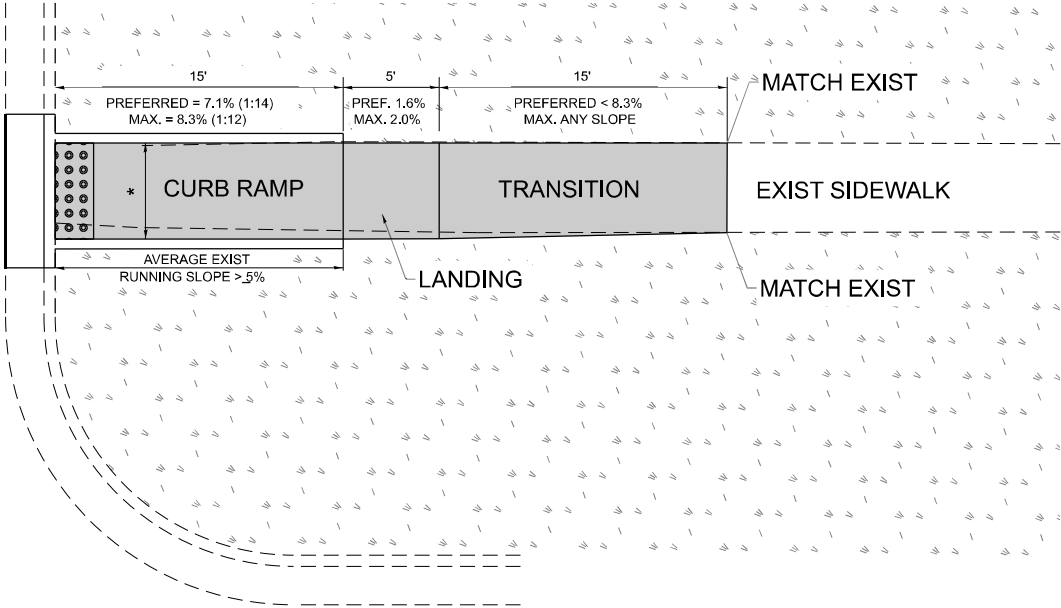
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	28
PD-01		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		

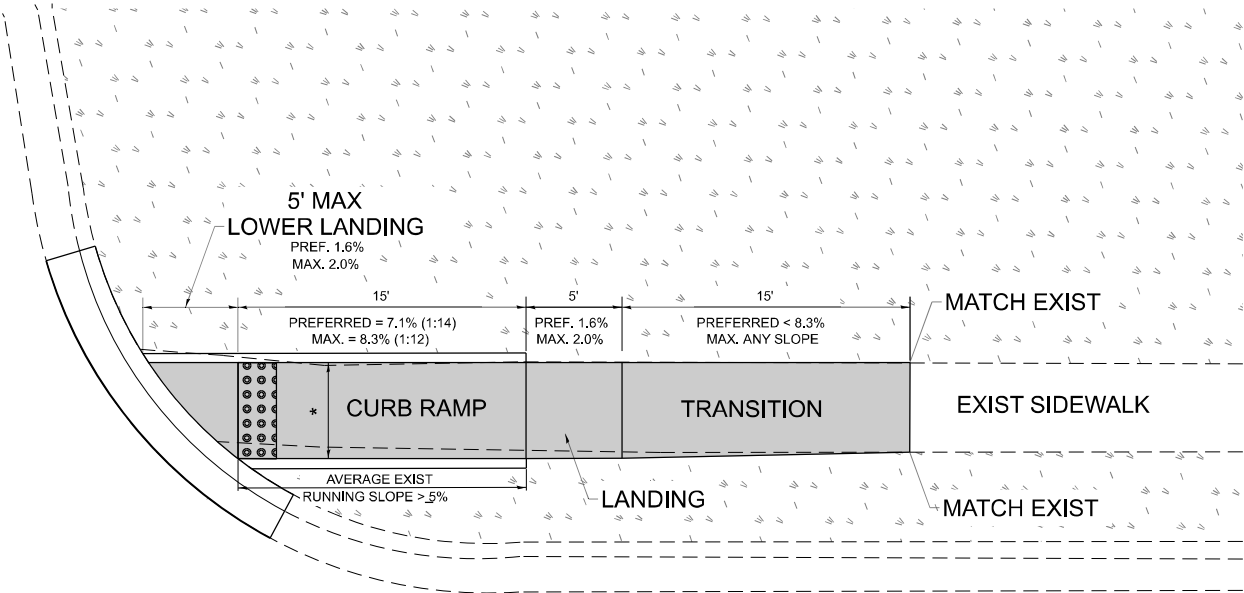
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FILE NAME: c:\p\work\1063787\11725-shh-DistShts.dgn

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

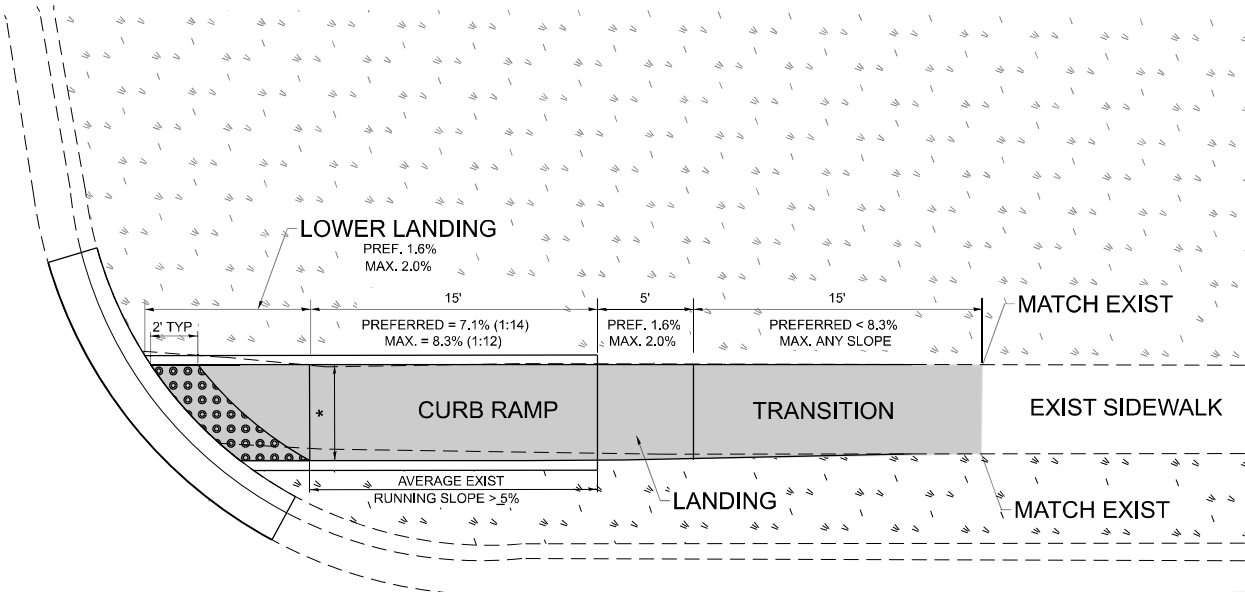
PD-02A



PD-02B



PD-02C



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-02)

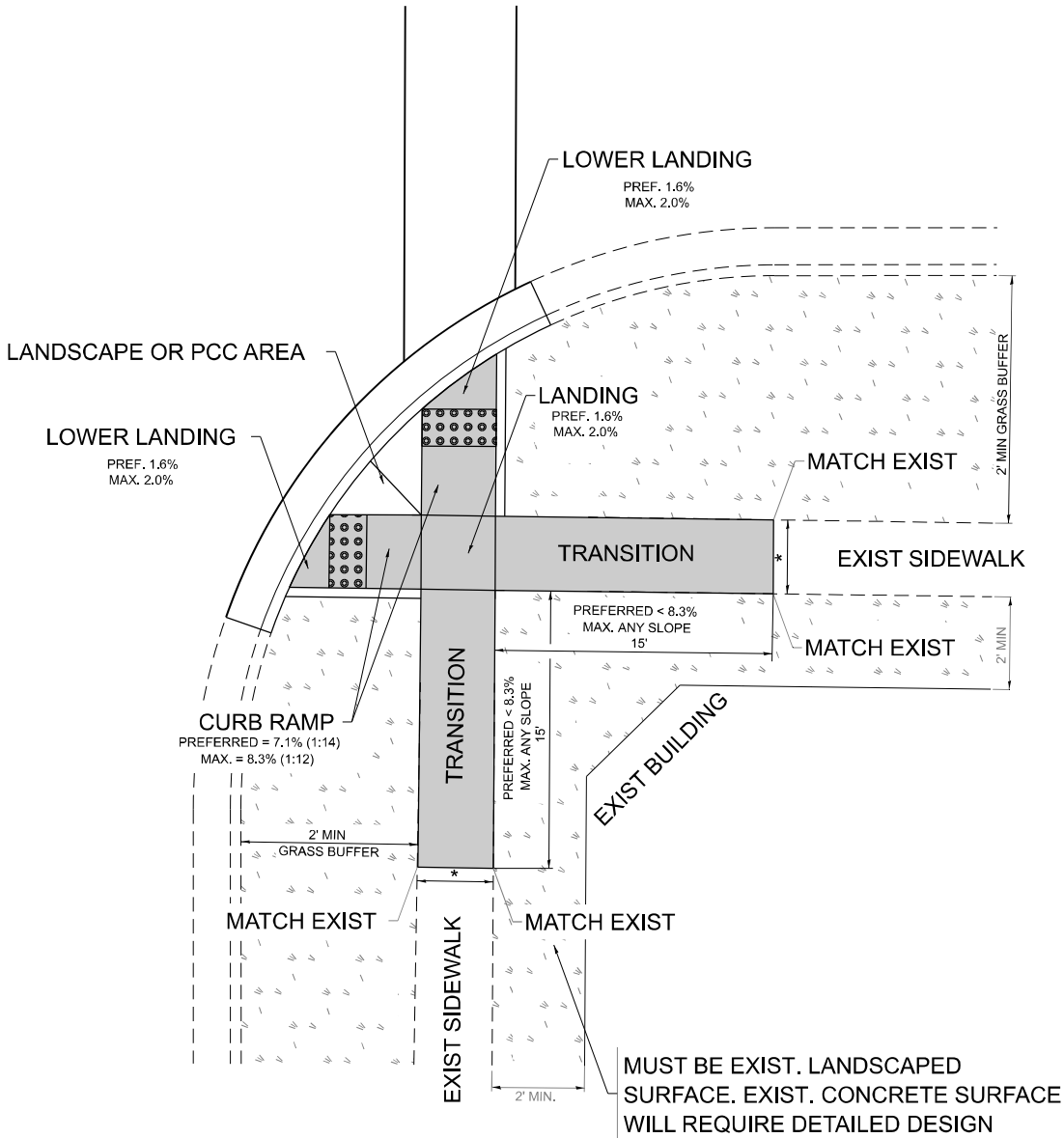
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	29
PD-02		CONTRACT NO. 80B19		
		ILLINOIS FED. AID PROJECT		

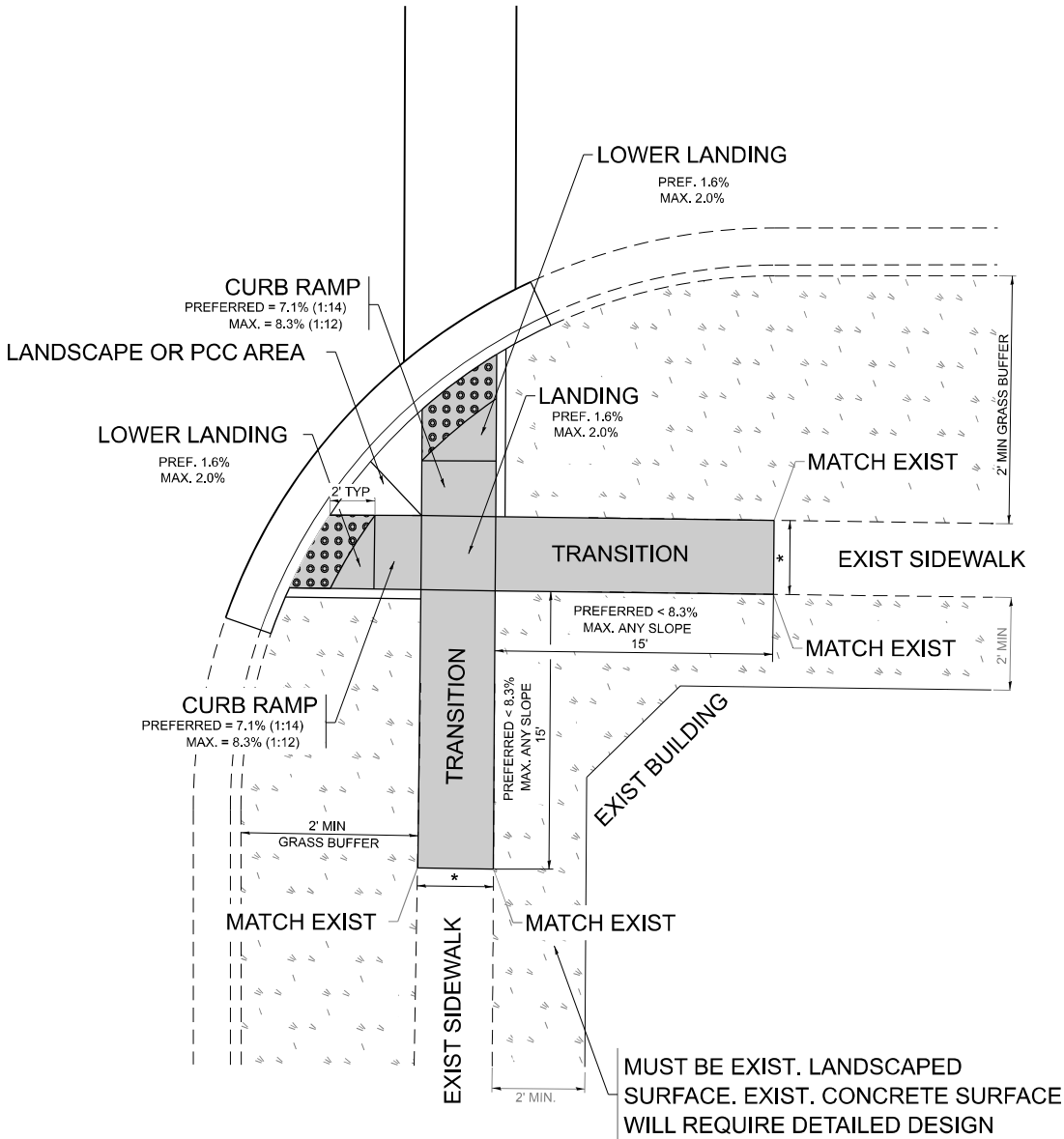
MODEL: pd-2-1 [Sheet]
FILE NAME: c:\p\work\hamwi\1063787\117225-shh-DistShts.dgn

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

MODEL: pd-3-1 [Sheet]
FILE NAME: c:\p\work\h\hamwi\1063787\117225-shh-DistShts.dgn

USER NAME	= mohammad.hamwi
DESIGNED	-
DRAWN	- R. LEDEZMA
CHECKED	-
PLOT DATE	= 8/14/2025
DATE	- 10/02/2019

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

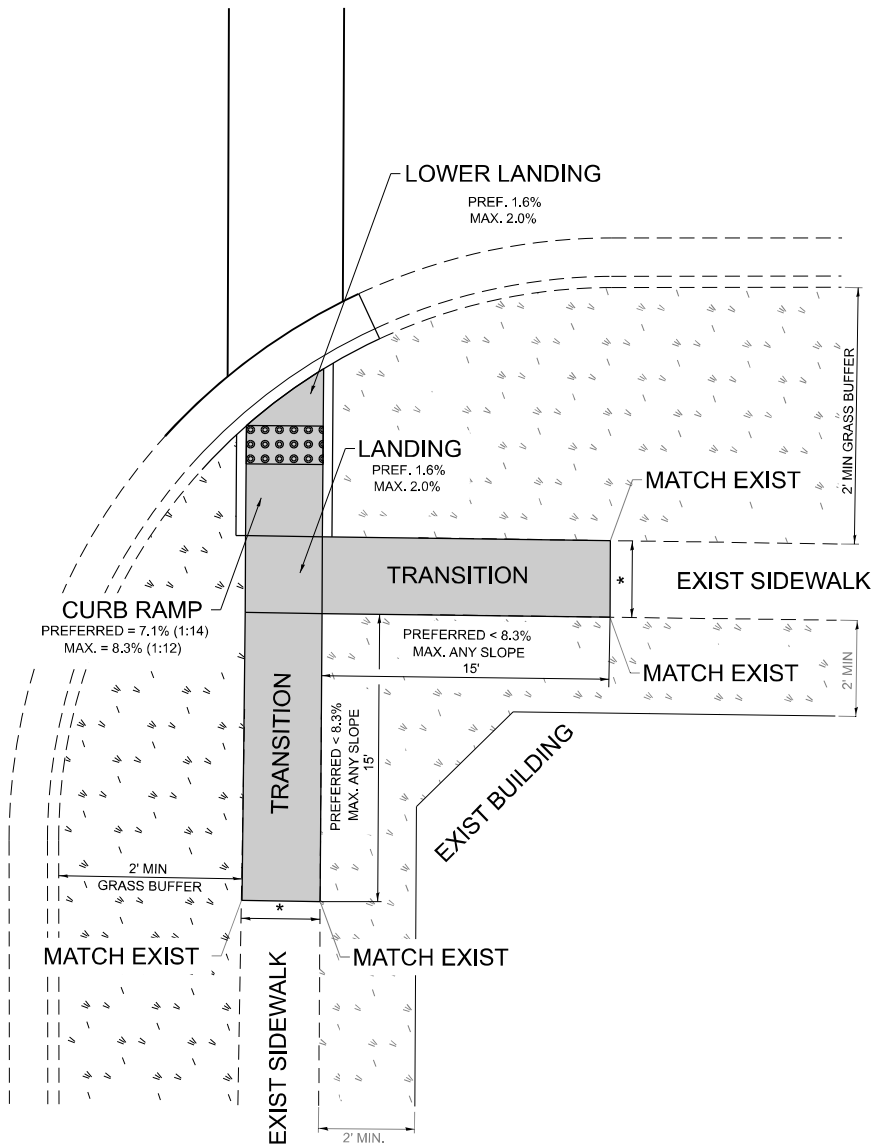
PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

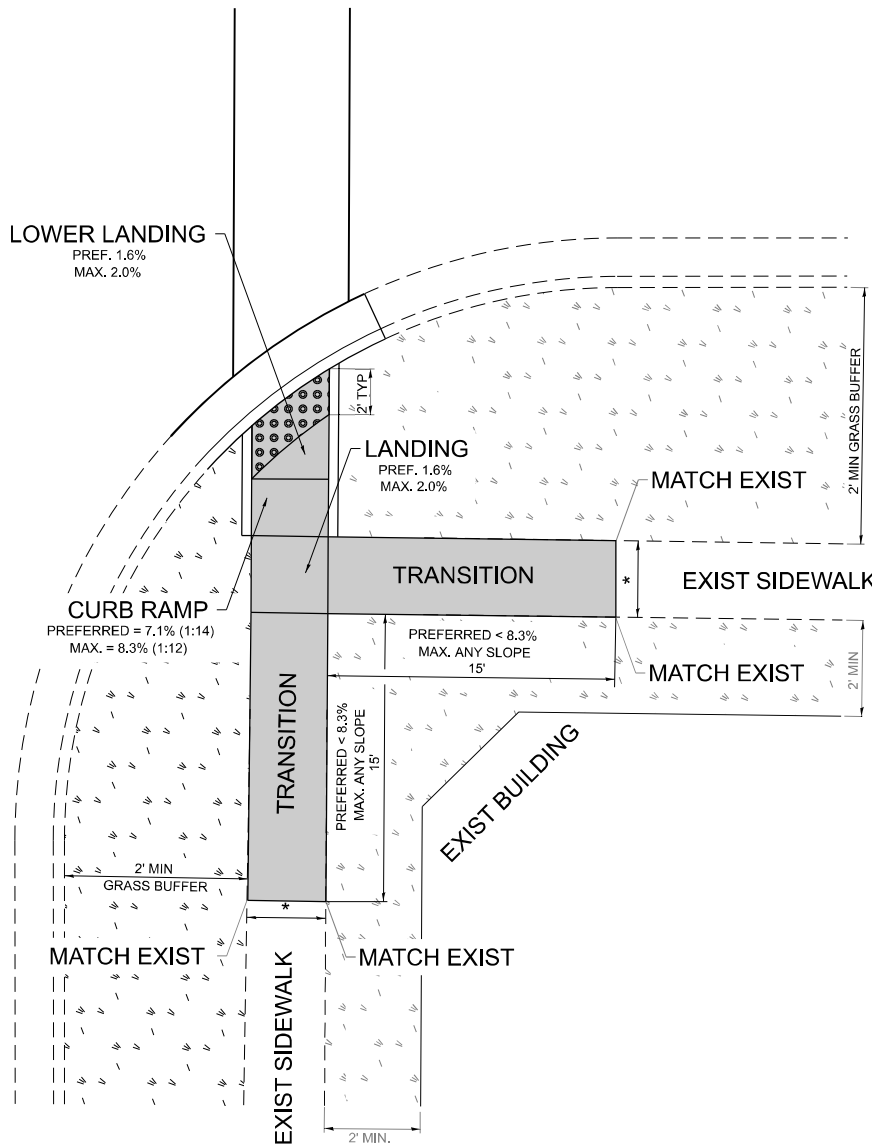
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	30
PD-03		CONTRACT NO. 80B19		
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



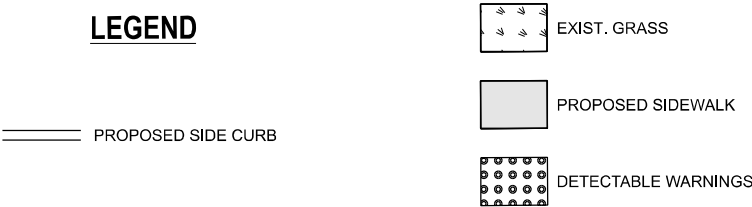
PD-04B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND



CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: pd-4-1 [Sheet]
FILE NAME: c:\p\work\hmd\hmd1063787\117225-shh-DistShts.dgn

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 8/14/2025	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

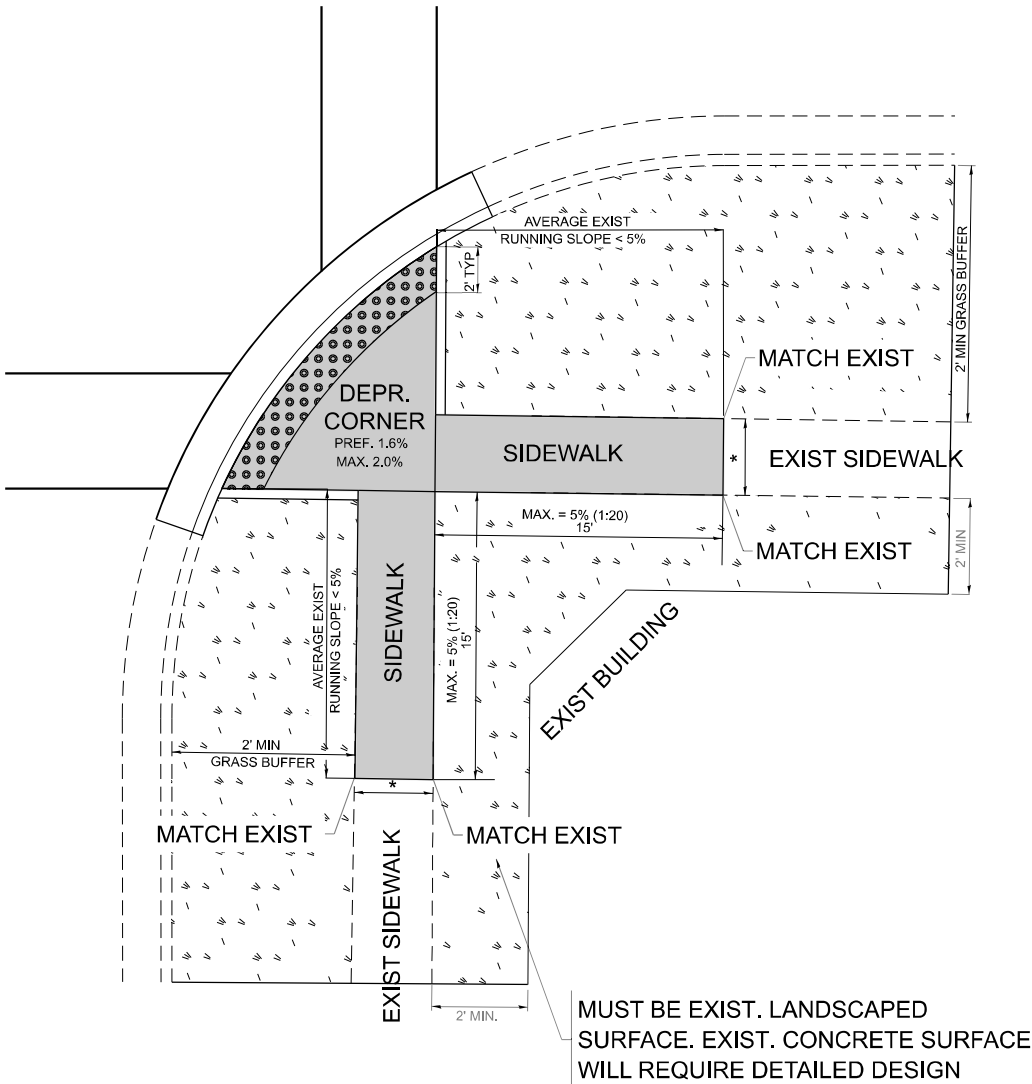
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

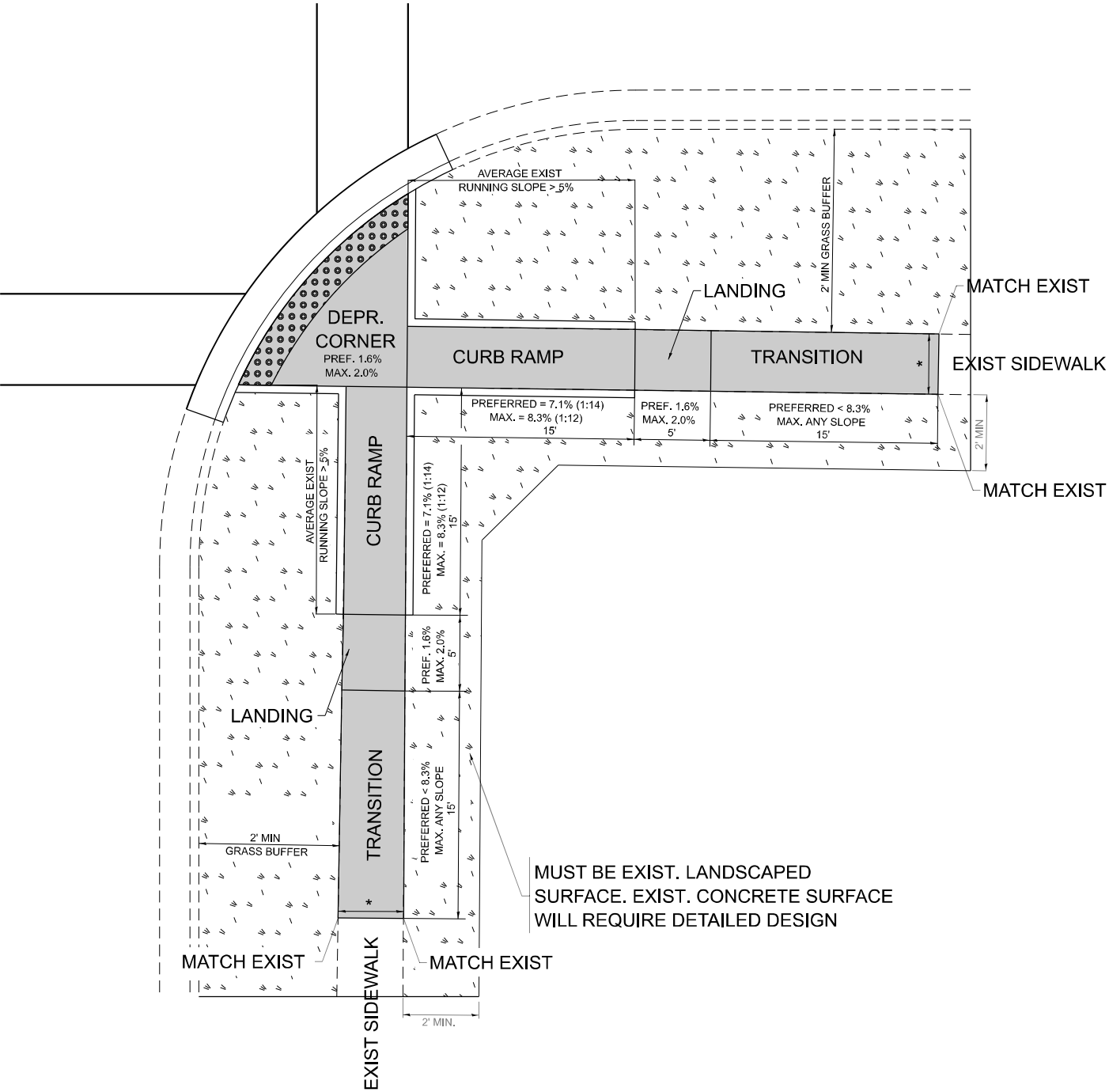
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876	2025-1093-RS	COOK	33	31
PD-04		CONTRACT NO. 80B19		
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: pd-5-1 [Sheet]
FILE NAME: c:\p\work\p\work\hamwi\1063787\117225-shh-DistShts.dgn

USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 8/14/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

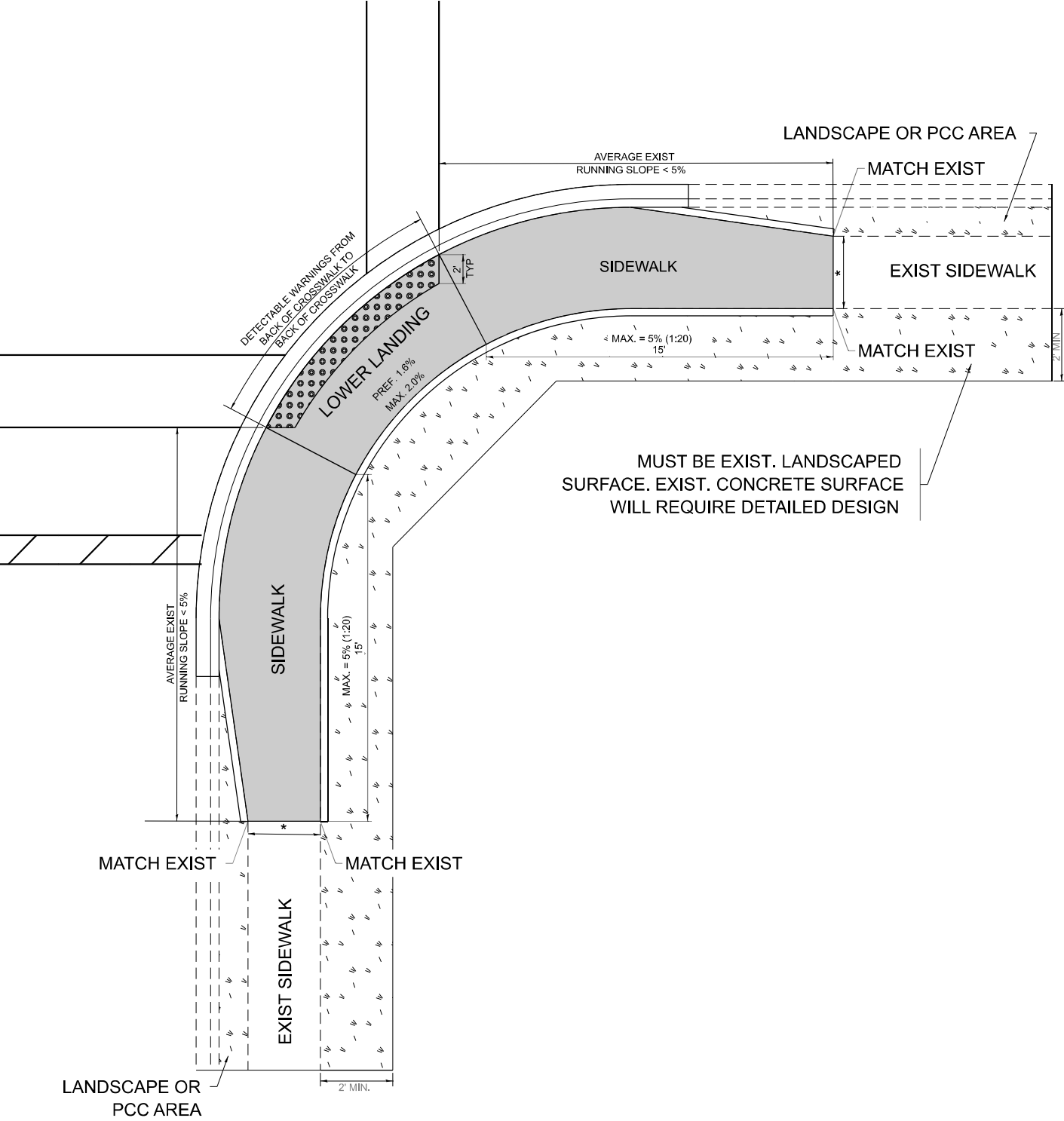
PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

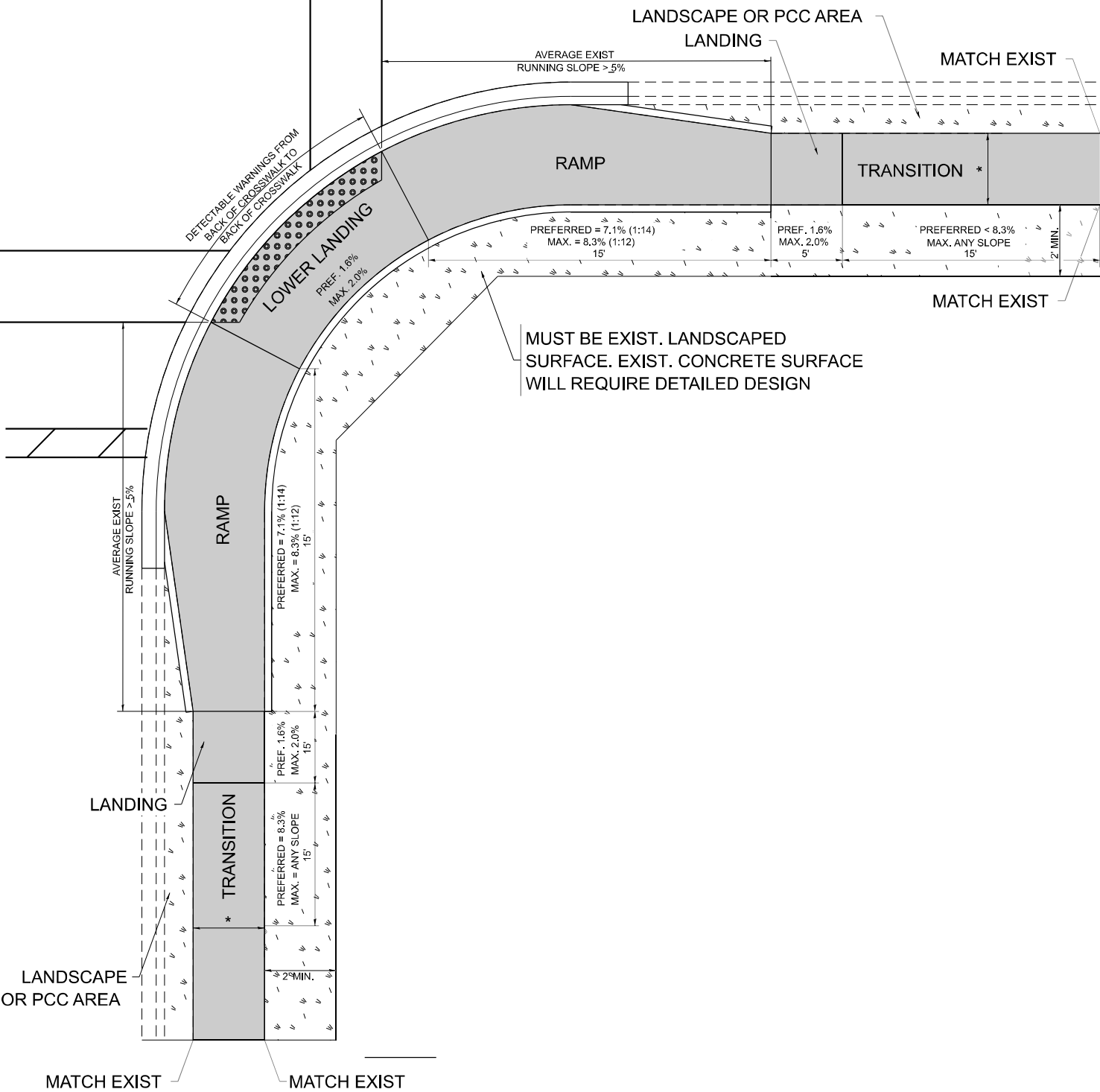
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	32
PD-05		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



DESIGNER NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50).
- 2) SIDEWALK REALIGNMENT WILL REQUIRE DETAILED DESIGN.
- 3) AREAS SURROUNDED BY PCC/ASPHALT, BUILDINGS, OR ARE NEAR TO DRIVEWAYS, REALIGNED SIDEWALK, UTILITY AND SIGNAL POLES, OR WHEN PRIVATE SIDEWALK TIES IN, WILL REQUIRE DETAILED SURVEY AND DESIGN.
- 4) ALL BRICK CORNERS WILL REQUIRE SUPERVISOR APPROVAL BEFORE USING PROJECT DETAILS

LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: pd-6-1 [Sheet]
FILE NAME: c:\p\work\hdm\1063787\117225-shh-DistShts.dgn

PLOT DATE = 8/9/2025	USER NAME = mohammad.hamwi	DESIGNED -	REVISED -
		DRAWN - R. LEDEZMA	REVISED -
		CHECKED -	REVISED -
	DATE - 10/02/2019	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
876	2025-1093-RS	COOK	33	33
PD-06		CONTRACT NO. 80B19		
ILLINOIS		FED. AID PROJECT		