

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	1
CONTRACT NO. 62V42				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGES OF
NILES AND MORTON GROVE

TRAFFIC DATA

FAP 348 (IL-43):
OAKTON ST FROM HARLEM AVE TO WAUKEGAN RD
ADT: 26,800 (2023)
SPEED LIMIT: 35 MPH
CLASS: OTHER PRINCIPAL ARTERIAL
WAUKEGAN RD FROM OAKTON ST TO MAIN ST
ADT: 13,500 (2023)
SPEED LIMIT: 35 MPH
CLASS: OTHER PRINCIPAL ARTERIAL
FAU 1332:
OAKTON ST FROM HARLEM AVE TO CALDWELL AVE (US 14)
ADT: 24,900 (2023)
SPEED LIMIT: 35 MPH
CLASS: MINOR ARTERIAL

FAP ROUTE 348: IL-43 (OAKTON ST & WAUKEGAN RD)
FROM SOUTH OF US 14 (CALDWELL AVE) TO OAKTON ST
& FROM HARLEM AVE TO WAUKEGAN RD
SECTION FAP 0348/1332 23 RES
PROJECT NHPP-D9CL(859)
STANDARD OVERLAY WITH ADA IMPROVEMENTS
COOK COUNTY

C-91-022-24

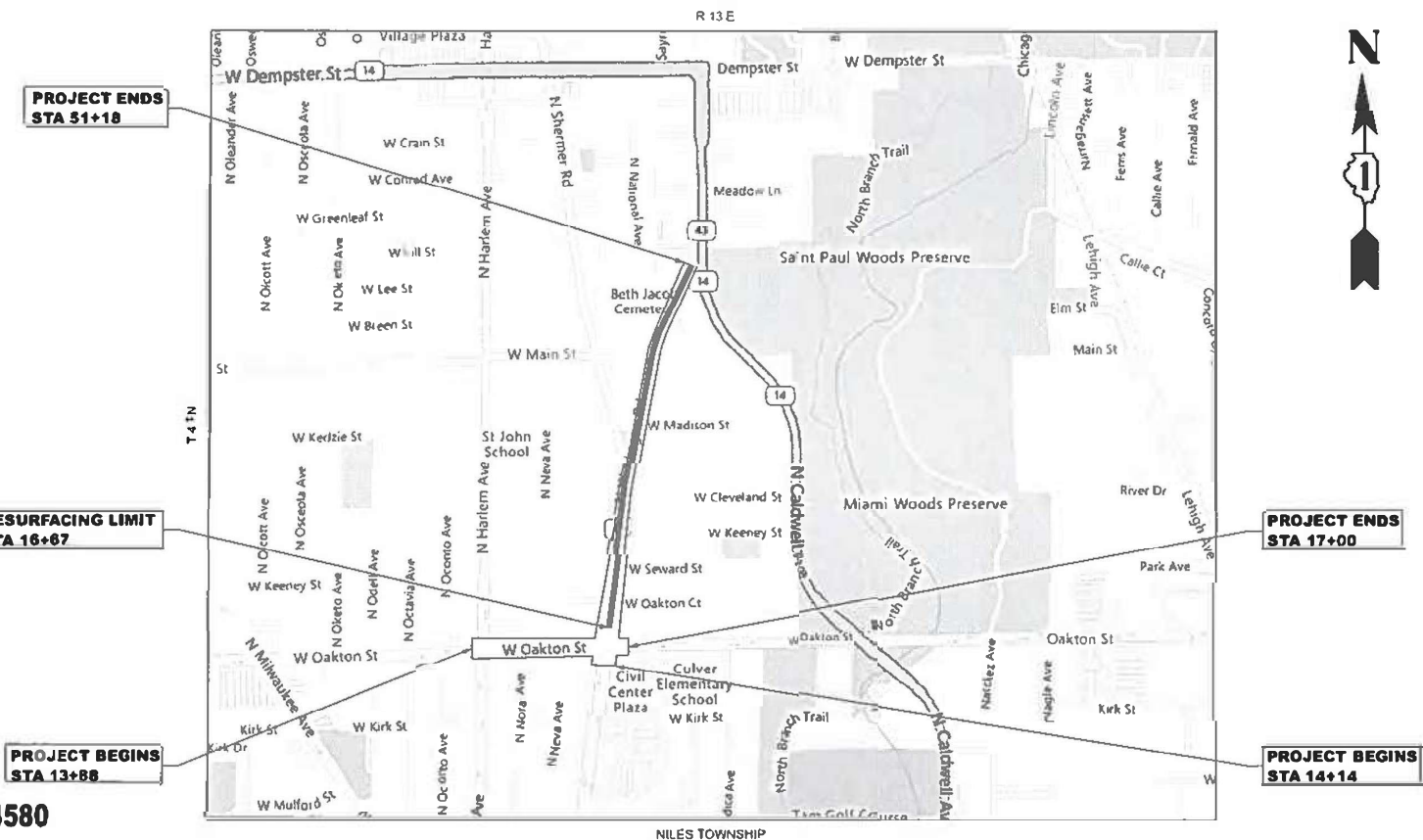


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: RODRIGO LEDEZMA (847) 705-4580
PROJECT MANAGER J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 62V42



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 20 25*
Joe R. Kunkin
REGIONAL ENGINEER

December 5 20 25
Scott E. H. H.
ENGINEER OF DESIGN AND ENVIRONMENT

December 5 20 25
John P. H.
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GROSS LENGTH = 4,061 FT. = 0.76 MILES
NET LENGTH = 3,451 FT. = 0.65 MILES

MODEL: GenNotesSh01 [Sheet]
FILE NAME: c:\paw_work\paw\illinois.gov\justin.king@illinois.gov\102024-shr-ge-note.dgn

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.		DESCRIPTION	STANDARD NO.	DESCRIPTION	
1	COVER SHEET		000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES		442201-04	CLASS C&D PATCHES	2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGES OF NILES, VILLAGE OF MORTON GROVE, AND COOK COUNTY
3-4	SUMMARY OF QUANTITIES		604001-05	FRAME AND LIDS TYPE 1	3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5	TYPICAL SECTIONS		604091-05	FRAME AND GRATE TYPE 24	4. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTEDAT NO COST TO THE DEPARTMENT.
6-8	ROADWAY PLANS		606001-09	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	5. BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
9-12	ADA IMPROVEMENT PLANS		701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE	6. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
13-17	APS AND DETECTOR LOOP SIGNAL PLANS		701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY	7. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
18	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)		701301-04	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS	8. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
19	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)		701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS < 40 MPH	9. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
20	CURB OR CURB AD GUTTER REMOVAL AND REPLACEMENT (BD-24)		701601-09	URBAN SINGLE LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN	10. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
21	BUTT JOINTS AND HMA TAPER (BD-32)		701602-10	URBAN LANE CLOSURE MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE	11. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
22-28	STANDARD TRAFFIC CONTROL SIGNAL DESIGN DETAILS (TS-05)		701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN	12. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
29	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY SURFACING (TS-07)		701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN	13. THE CONTRACTOR SHALL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURESACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK,THE CONTRACTOR SHALL DELIVER THE RECORD TO THE ENGINEER.
30	TRAFFIC CONTROL AND PROTECTION FOR SIDEROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)		701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION	14. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
31	RAISED REFLECTIVE PAVEMENT MARKERS (SNOWBLOW RESISTANT) (TC-11)		701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE	15. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
32	DISTRICT ONE PAVEMENT MARKINGS (TC-13)		701901-11	TRAFFIC CONTROL DEVICES	16. THE ENGINEER SHALL CONTACT FADI SULTAN, AREA TRAFFIC FIELD ENGINEER, AT FADI.SULTAN@ILLINOIS.GOV A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS
33	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)		886001-01	DETECTOR LOOP INSTALLATIONS	17. TEMPORARAY PAVEMENT MARKING OR SHORT TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED UNLESS DIRECTED BY THE ENGINEER
34	SHORT-TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)				18. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES
35	ARTERIAL ROAD INFORMATION SIGN (TC-22)				19. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
36	DRIVEWAY ENTRANCE SIGNING (TC-26)				20. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRENT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS
37	PROJECT DETAIL FOR SINGLE PERPENDICULAR RAMPS (PD-01)				21. BEFORE BEGGINING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
38	PROJECT DETAIL FOR SINGLE PERPENDICULAR RAMPS (PD-02)				
39	PROJECT DETAIL FOR DOUBLE PERPENDICULAR RAMPS (PD-03)				
40	PROJECT DETAIL FOR SINGLE PERPENDICULAR RAMPS WITH TURNING SPACE (PD-04)				
41	PROJECT DETAIL FOR DEPRESSED CURB RAMPS (PD-05)				
42	PROJECT DETAIL FOR PARALLEL CURB RAMPS (PD-06)				
43	PCC SIDEWALK 5 OR 8 INCH (SPECIAL) DETAIL				

	USER NAME	= justin.king	DESIGNED	-	REVISED	-	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES WAUKEGAN RD - IL-43 (OAKTON ST TO US 14)	F A P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			DRAWN	-	REVISED	-			348	FAP 0348/1332 23 RES	COOK	43	2	
			CHECKED	-	REVISED	-							CONTRACT NO. 62V42	
	PLOT DATE	= 11/21/2025	DATE	-	REVISED	-							ILLINOIS FED. AID PROJECT	
								SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.	

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SIGNALS				
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0044			
	20200100	EARTH EXCAVATION	CU YD	48	48					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	768.2	768.2					
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15					
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15	15					
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15	15					
	25200110	SODDING, SALT TOLERANT	SQ YD	768.2	768.2					
	25200200	SUPPLEMENTAL WATERING	UNIT	8	8					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	13723	13723					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	11484	11484					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	31	31					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	280	280					
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	839	839					
	40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1708	1708					
	42001300	PROTECTIVE COAT	SQ YD	1223.1	1223.1					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4500			4500			
	42400800	DETECTABLE WARNINGS	SQ FT	270			270			
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL,, 2 1/4"	SQ YD	20330	20330					
	44000600	SIDEWALK REMOVAL	SQ FT	4500			4500			
	44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	21	21					
	44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	436	436					
	44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	162	162					
* SPECIALTY ITEM										

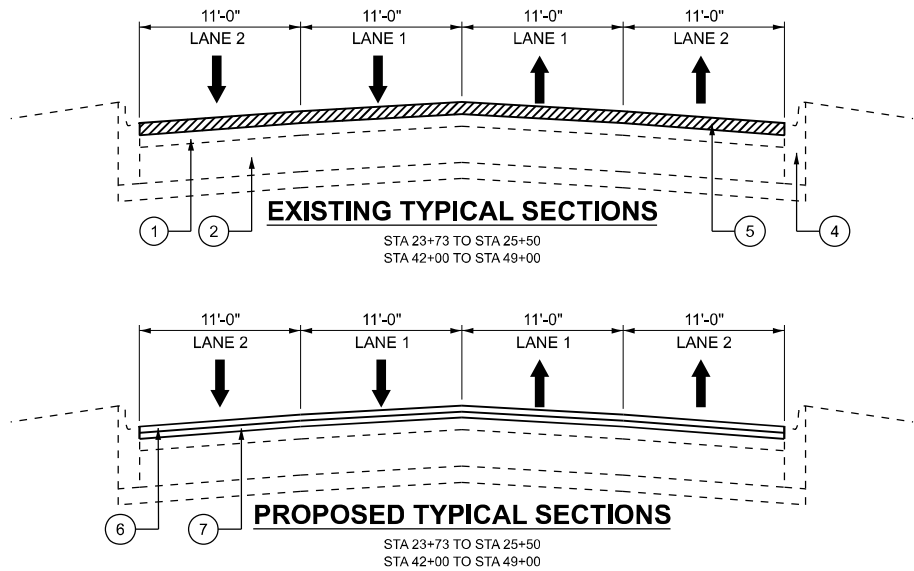
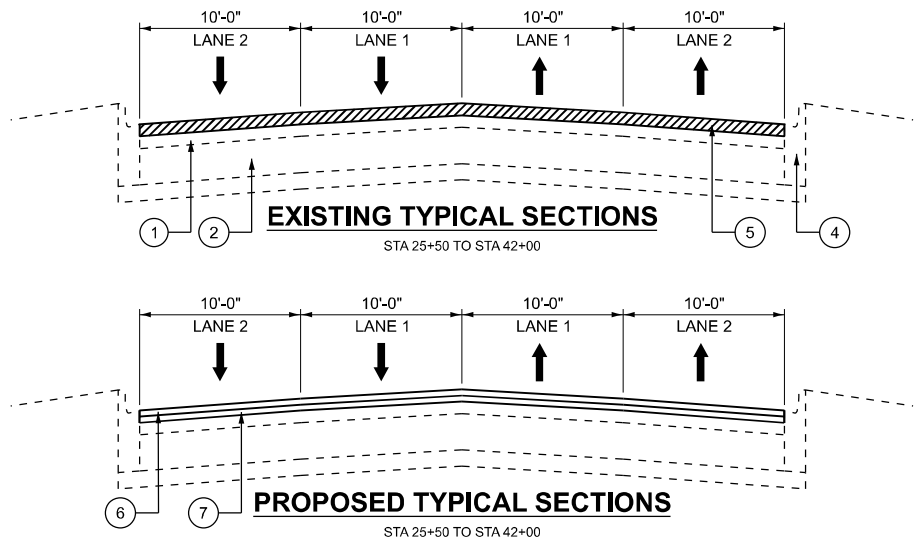
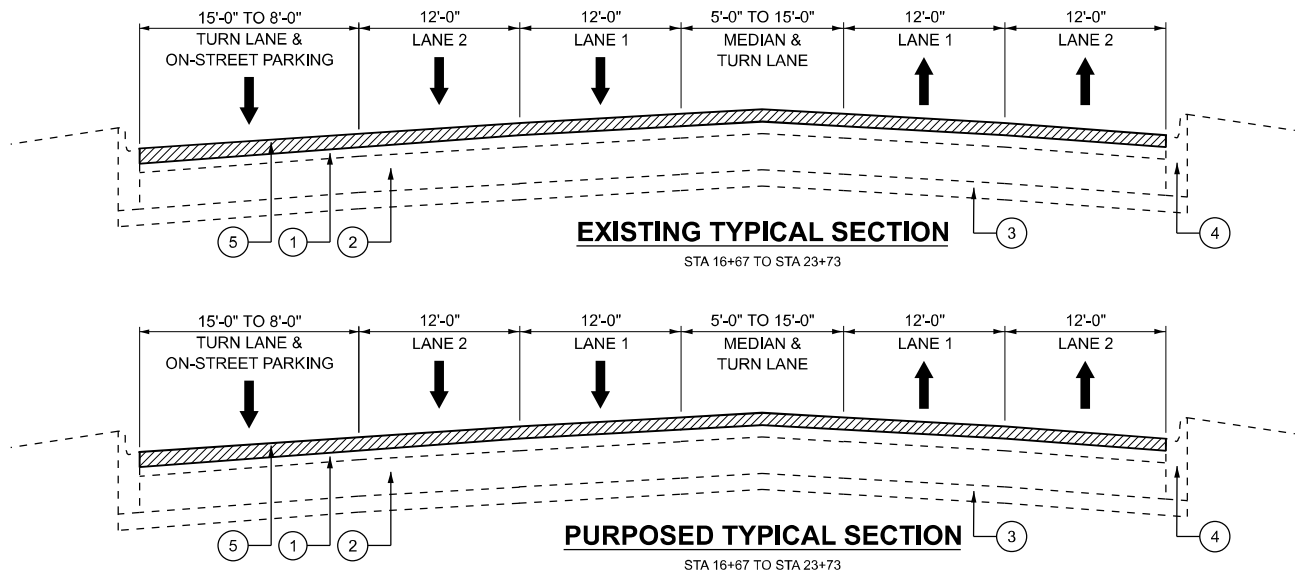
SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SIGNALS				
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0044			
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	272	272					
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	12	12					
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	9	9					
	60300405	VALVE BOX FRAMES TO BE ADJUSTED	EACH	2	2					
	60404950	FRAMES AND GRATES, TYPE 24	EACH	2	2					
	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	3	3					
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	45	45					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
*✱	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15					
	67100100	MOBILIZATION	L SUM	1	1					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
	70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	10982	10982					

	USER NAME = justin.king	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES WAUKEGAN RD - IL-43 (OAKTON ST TO US 14)				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -						348	FAP 0348/1332 23 RES	COOK	43	3	
		CHECKED -	REVISED -							CONTRACT NO. 62V42				
	PLOT DATE = 10/17/2025	DATE -	REVISED -		SCALE:	SHEET 1	OF 2	SHEETS	STA.	TO STA.				
					ILLINOIS FED. AID PROJECT									

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	SIGNALS				
					80% FED 20% STATE	80% FED 20% STATE	100% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0021	0044			
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	4797.3	4797.3					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	154.8	154.8					
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	19288	19288					
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	1922	1922					
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	312	312					
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	469	469					
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	1002	1002					
	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	9644	9644					
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	77.4	77.4					
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9644	9644					
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	961	961					
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	156	156					
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	234.5	234.5					
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	501	501					
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	340	340					
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	340	340					
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	16		16				
*	81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	74		74				
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1		1				
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	886		886				
*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	110		110				

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NOTES

- THE CONTRACTOR SHALL MILL BEFORE PATCHING
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLY. HMA BINDER COURSE, IL-4.75, N50

LEGEND

- ① EXISTING HMA SURFACE COURSE, 4½"±
- ② EXISTING PCC BASE COURSE, 8"±
- ③ EXISTING SUBBASE,
- ④ EXISTING CURB AND GUTTER
- ⑤ PROPOSED HOT-MIX ASPHALT REMOVAL, 2¼"±
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, N70, 1½"
- ⑦ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"

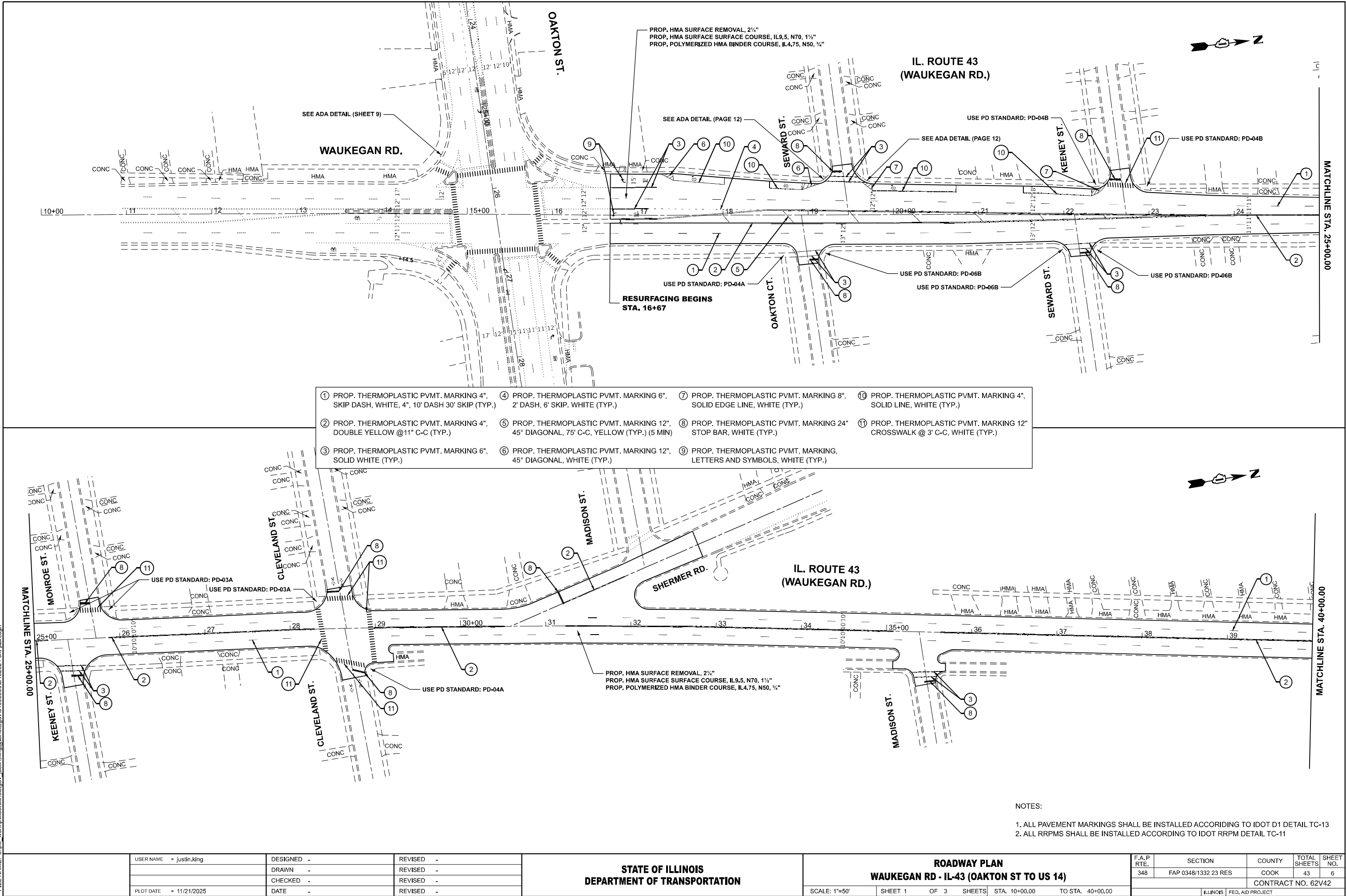
HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP
MIXTURE TYPE	AIR VOIDS @ Ndesign	
PAVEMENT RESURFACING (STANDARD OVERLAY)		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1½"	4% @ 70 Gyr.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, ¾"	3.5% @ 50 Gyr.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19.0)	4% @ 70 Gyr.	QC/QA
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

	USER NAME = justin.king	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS WAUKEGAN RD - IL-43 (OAKTON ST TO US 14)			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -					348	FAP 0348/1332 23 RES	COOK	43	5	
		CHECKED -	REVISED -		CONTRACT NO. 62V42								
	PLOT DATE = 10/17/2025	DATE -	REVISED -		SCALE:		SHEET 1	OF 1	SHEETS	STA.	TO STA.		
	ILLINOIS FED. AID PROJECT												

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NOTES:

1. ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT D1 DETAIL TC-13

2. ALL RRPMS SHALL BE INSTALLED ACCORDING TO IDOT RRP DETAIL TC-11

USER NAME	= justin.king	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
PLOT DATE	= 11/21/2025	DATE -	REVISED -

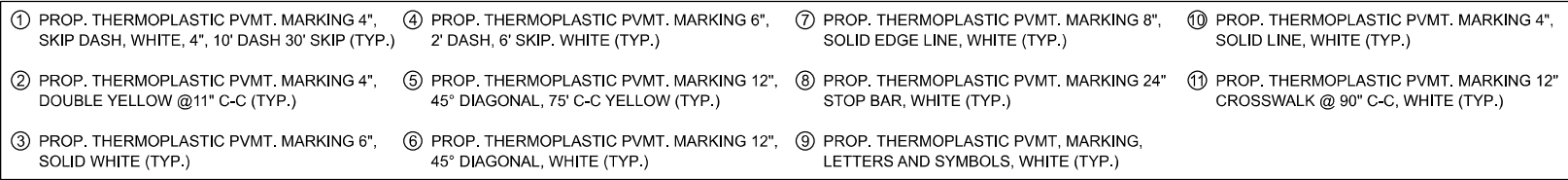
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
WAUKEGAN RD - IL-43 (OAKTON ST TO US 14)

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 10+00.00 TO STA. 40+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	6
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

FAU 1332

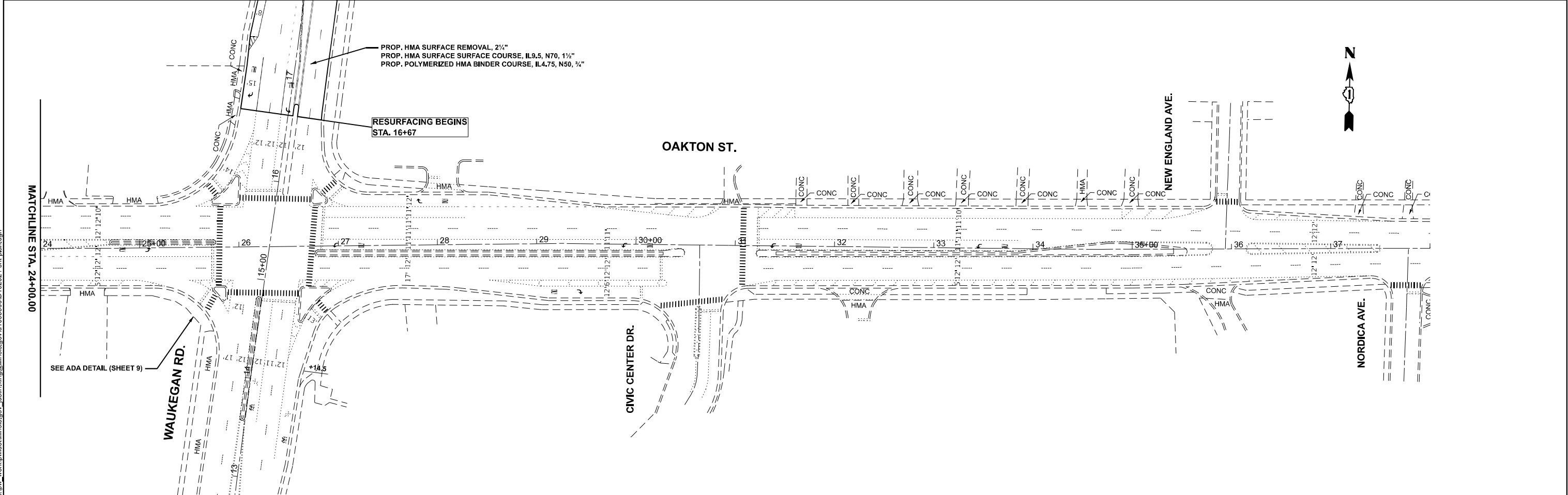
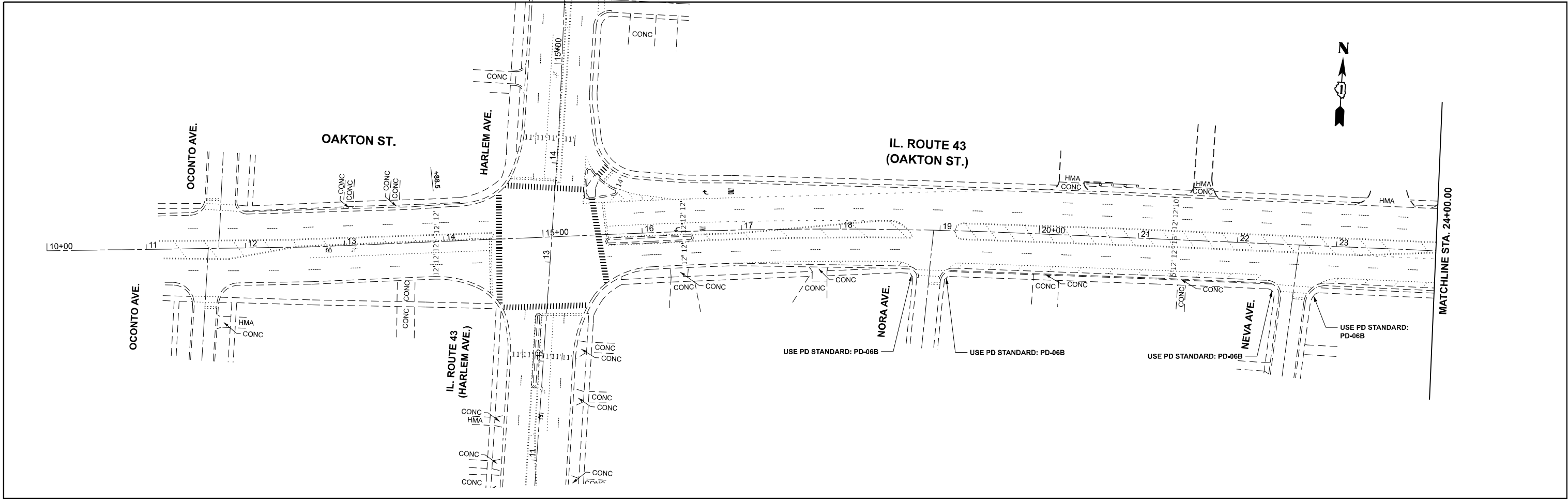


1. ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT D1 DETAIL TC-13
2. ALL RRPMS SHALL BE INSTALLED ACCORDING TO IDOT RRPM DETAIL TC-11

	USER NAME = justin.king	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN WAUKEGAN RD - IL-43 (OAKTON ST TO US 14)					F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -		348	FAP 0348/1332 23 RES	COOK	43	7					
		CHECKED -	REVISED -		CONTRACT NO. 62V42									
	PLOT DATE = 11/21/2025	DATE -	REVISED -		SCALE: 1"=50'	SHEET 2	OF 3	SHEETS	STA. 40+00.00	TO STA. 55+00.00	ILLINOIS FED. AID PROJECT			

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PLOT DATE = 10/17/2025	USER NAME = justin.king	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

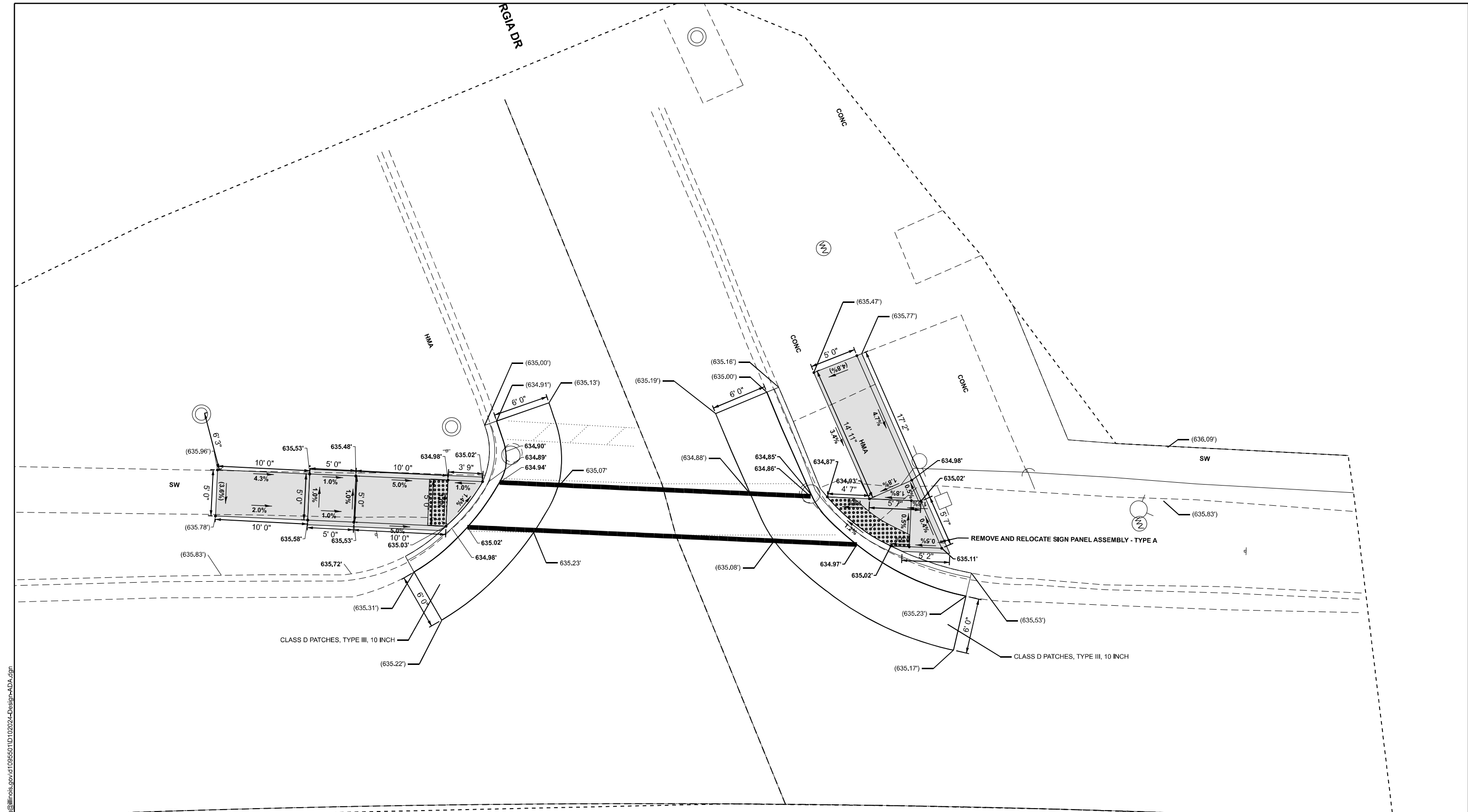
ROADWAY PLAN
OAKTON ST - IL-43 (HARLEM AVE TO US 14)

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 10+00.00 TO STA. 38+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	8
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

FAU 1332

MODEL: Georgia Drive [Sheet]
FILE NAME: c:\p\work\wid\illinois.gov\102024\Design-ADA.dgn



REFERENCE BENCHMARK ELEVATION: 637.93'
BENCHMARK: N.E. BOLT OF FIRE HYDRANT
LOCATION: N.W. CORNER OF WAUKEGAN RD AND GEORGIA DRIVE

LEGEND

XX.XX'

EXISTING LENGTH

PROPOSED SIDE CURB

() EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEVATION: 637.93'
BENCHMARK: N.E. BOLT OF FIRE HYDRANT
LOCATION: N.W. CORNER OF WAUKEGAN RD AND GEORGIA DRIVE

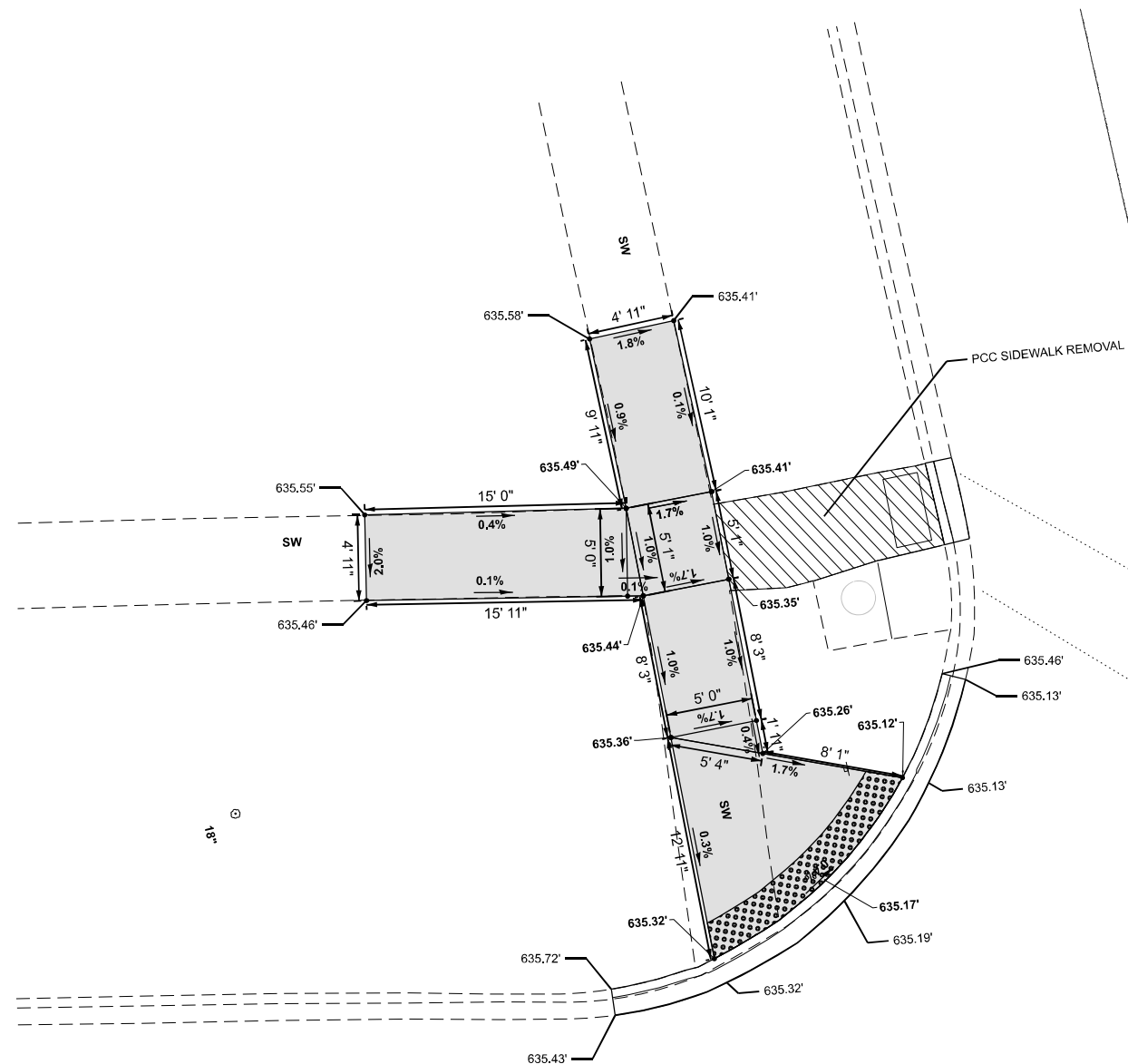
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	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA IMPROVEMENT DETAIL
NW AND SW CORNERS OF GEORGIA DR AND WAUKEGAN RD

SCALE: SHEET 2 OF 4 SHEETS STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	10
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				



REFERENCE BENCHMARK ELEVATION: 635.47'

BENCHMARK: BOLT OF FIRE HYDRANT

LOCATION: EAST SIDE OF WAUKEGAN RD ±30' SOUTH OF MAIN STREET

LEGEND

XX.XX' EXISTING LENGTH

===== PROPOSED SIDE CURB



PROPOSED SIDEWALK



DETECTABLE WARNINGS

REFERENCE BENCHMARK ELEVATION: 635.47'

BENCHMARK: BOLT OF FIRE HYDRANT

LOCATION: EAST SIDE OF WAUKEGAN RD ±30' SOUTH OF MAIN STREET

USER NAME = justin.king	DESIGNED -	REVISED -
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PLOT DATE = 10/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

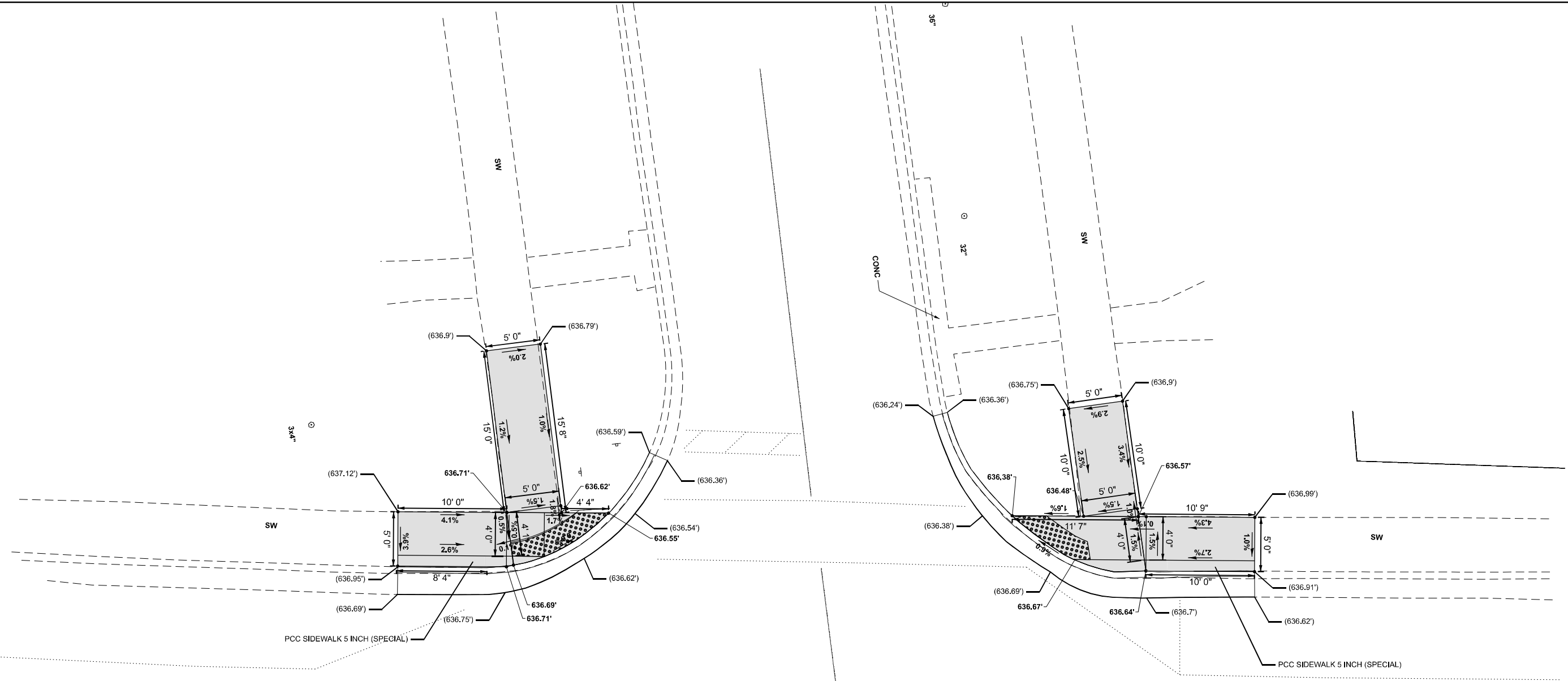
ADA IMPROVEMENT DETAIL
SW CORNER OF MAIN STREET & WAUKEGAN RD

SCALE:	SHEET 3	OF 4	SHEETS	STA. 0+00.00	TO STA. 0+00.00
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	11
CONTRACT NO. 62V42				
	ILLINOIS	FED. AID PROJECT		

MODEL: Seward St SW-1 (Sheet)
FILE NAME: c:\p\work\p\work\illinois.gov\109550\102024-Design-ADA.dgn

IL ROUTE 43
WAUKEGAN RD)



REFERENCE BENCHMARK ELEVATION: 639.01'

BENCHMARK: BOLT OF FIRE HYDRANT

LOCATION: N.W. CORNER OF WAUKEGAN RD AND SEWARD ST

LEGEND

XX.XX'

EXISTING LENGTH

=====

PROPOSED SIDE CURB



PROPOSED SIDEWALK



DETECTABLE WARNINGS

REFERENCE BENCHMARK ELEVATION: 639.01'

BENCHMARK: BOLT OF FIRE HYDRANT

LOCATION: N.W. CORNER OF WAUKEGAN RD AND SEWARD ST

	USER NAME = justin.king	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
	PLOT DATE = 10/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA IMPROVEMENT DETAIL
NW & SW CORNERS OF SEWARD ST & WAUKEGAN RD

SCALE: SHEET 4 OF 4 SHEETS STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	12
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

FAU 1332

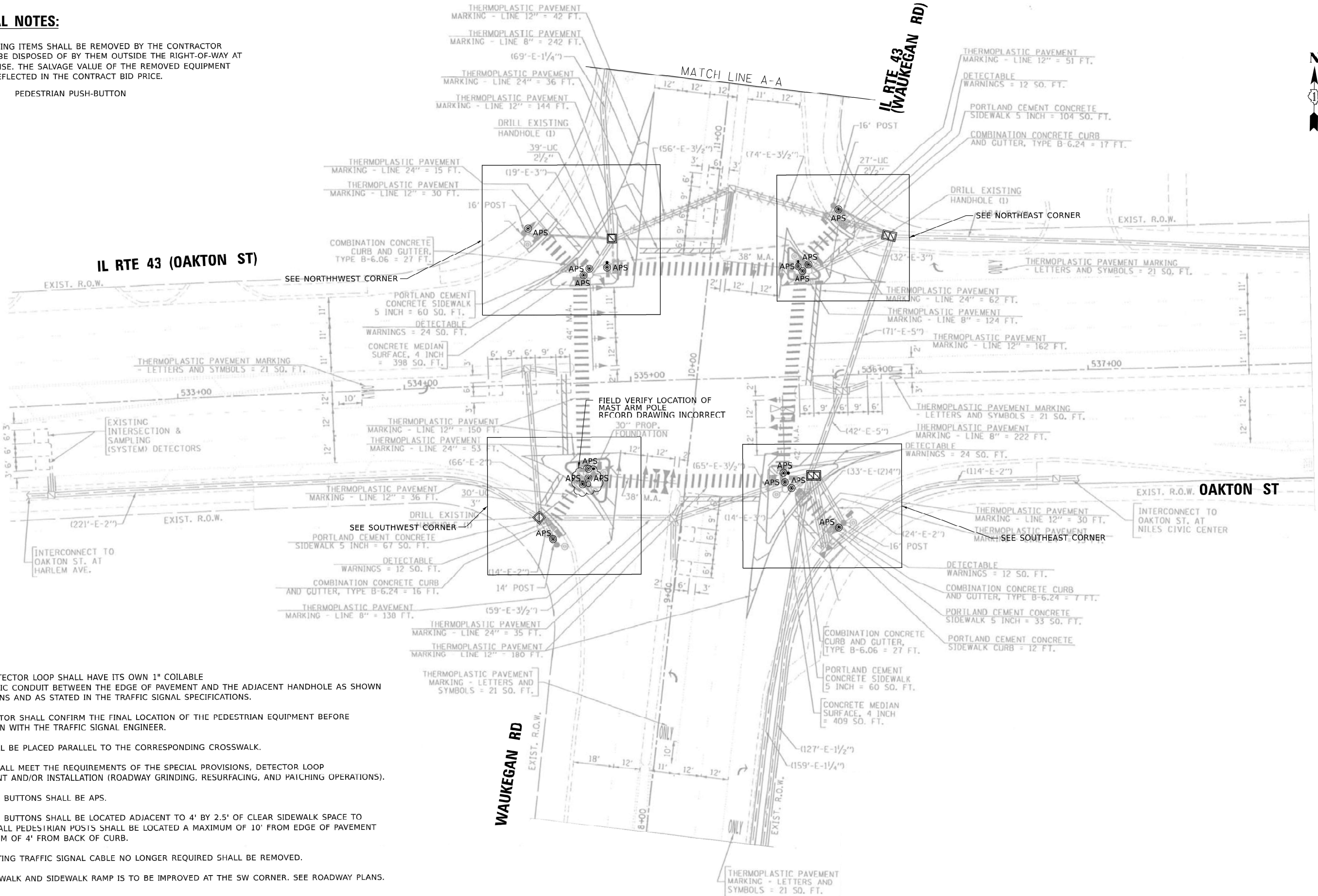
REMOVAL NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

12 EACH PEDESTRIAN PUSH-BUTTON

NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE PEDESTRIAN EQUIPMENT BEFORE INSTALLATION WITH THE TRAFFIC SIGNAL ENGINEER.
3. APS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.
4. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
5. ALL PUSH BUTTONS SHALL BE APS.
6. ALL PUSH BUTTONS SHALL BE LOCATED ADJACENT TO 4' BY 2.5' OF CLEAR SIDEWALK SPACE TO MEET ADA. ALL PEDESTRIAN POS'S SHALL BE LOCATED A MAXIMUM OF 10' FROM EDGE OF PAVEMENT AND MINIMUM OF 4' FROM BACK OF CURB.
7. ALL EXISTING TRAFFIC SIGNAL CABLE NO LONGER REQUIRED SHALL BE REMOVED.
8. THE SIDEWALK AND SIDEWALK RAMP IS TO BE IMPROVED AT THE SW CORNER. SEE ROADWAY PLANS.



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC MODERNIZATION PLAN
IL-43 (WAUKEGAN AVE) AT IL-43 (OAKTON ST)

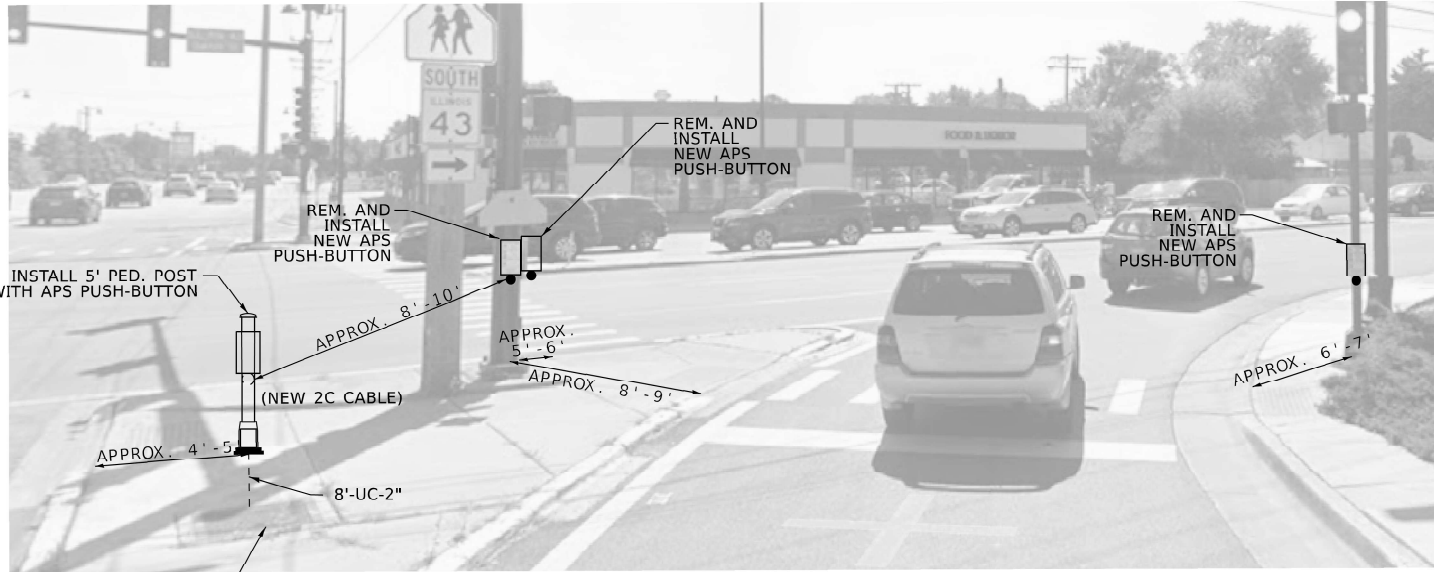
TS 2085
EAGLE 10

USER NAME = justin.king	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

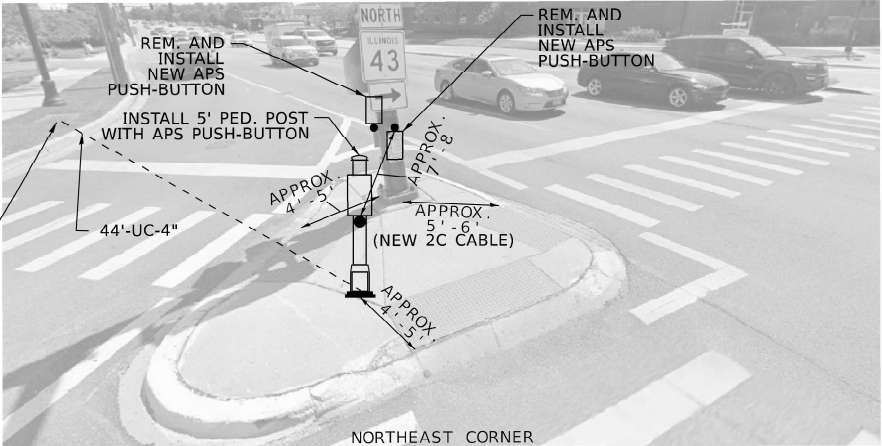
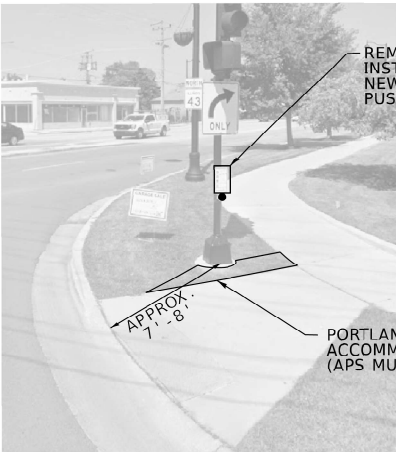
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

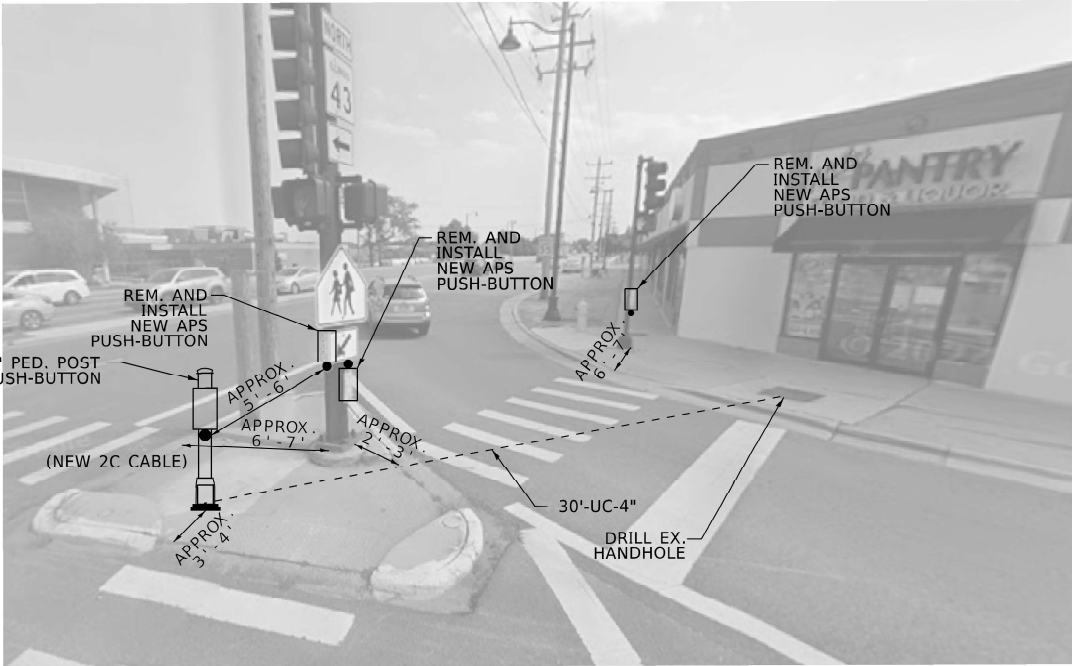
FAU 1332



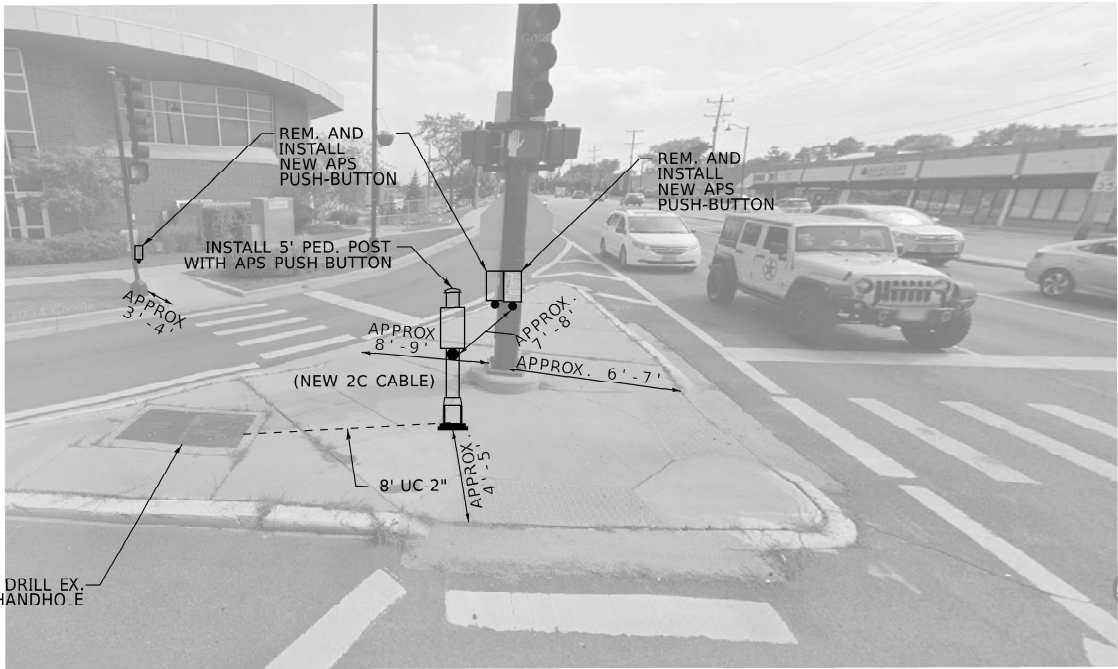
NORTHWEST CORNER



NORTHEAST CORNER



SOUTHWEST CORNER



SOUTHEAST CORNER

MODEL: Detector Loop Plan3 [Sheet]
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PLOT DATE = 10/17/2025	USER NAME = justin.king	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
	DATE -	REVISED -	

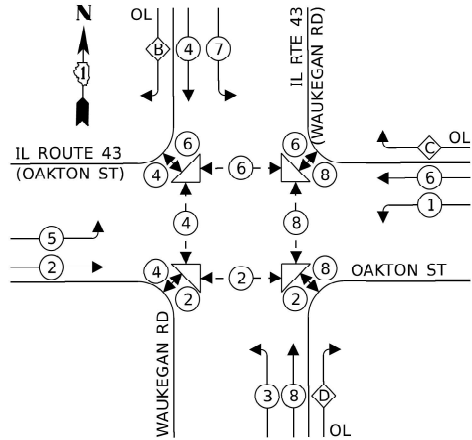
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC MODERNIZATION PLAN
IL-43 (WAUKEGAN AVE) AT IL-43 (OAKTON ST)

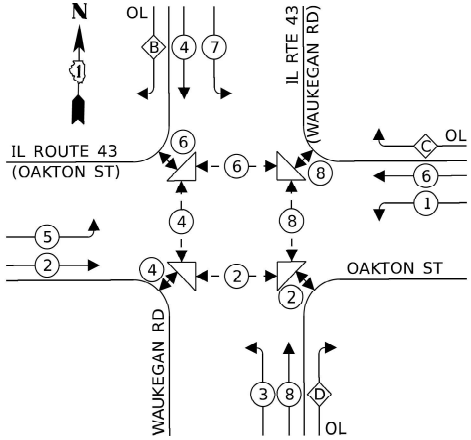
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	15
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

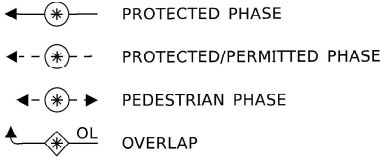
EXISTING
CONTROLLER SEQUENCE



PROPOSED
CONTROLLER SEQUENCE



LEGEND:

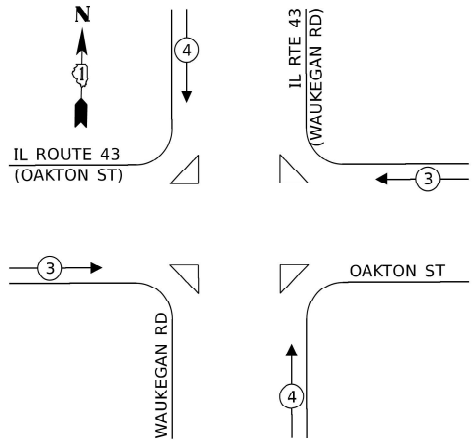


RIGHT TURN OVERLAP
PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4	+ 5
C	= 6	+ 7
D	= 8	+ 1

IL RTE 43
(OAKTON ST)

EXISTING AND PROPOSED EMERGENCY
VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	6	11	66
4-SECTION	-	14	-
5-SECTION	15	13	195
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	16	15	240
CONTROLLER	1	150	150
MASTER CONTROLLER	1	100	100
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			776
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1,381

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	QUANTITY
PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	12
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	16
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	74
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	886
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	110
DRILL EXISTING HANDHOLE	EACH	4
DETECTOR LOOP, TYPE I	FOOT	87
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	4
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	16
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	16
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT
PHONE: (866) 639-3532
COMPANY: COMED
ACCOUNT NUMBER: 73888-42222
METER NUMBER: ---

EX. TRACER CABLE
EX. INTERCONNECT TO OAKTON ST AT HARLEM AVE

EX. TRACER CABLE
EX. INTERCONNECT TO OAKTON ST AT HARLEM AVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE
PREEMPTION SEQUENCE, AND SCHEDULE OF QUANTITIES
IL-43 (WAUKGEGAN AVE) AT IL-43 (OAKTON ST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	16
CONTRACT NO. 62V42				

FAU 1332

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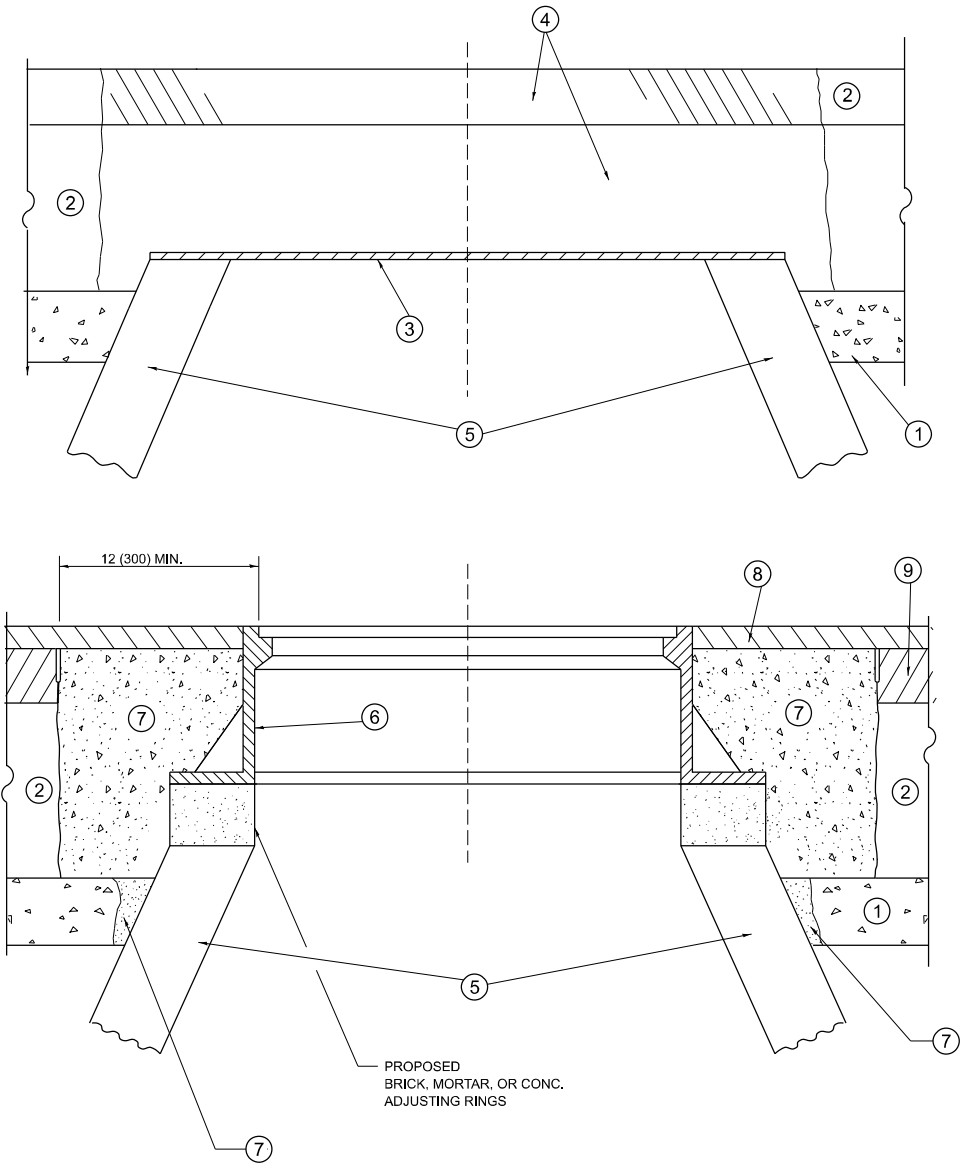
TS 2085
EAGLE 10

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)".

TS 1175
ECON 8

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	17
CONTRACT NO. 62V42				
		ILLINOIS	FED. AID PROJECT	



DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

- STAGE 1** (BEFORE PAVEMENT MILLING)
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

- STAGE 2** (AFTER PAVEMENT MILLING)
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD600-03 (BD-08)		CONTRACT NO. 62V42		
		ILLINOIS FED. AID PROJECT		

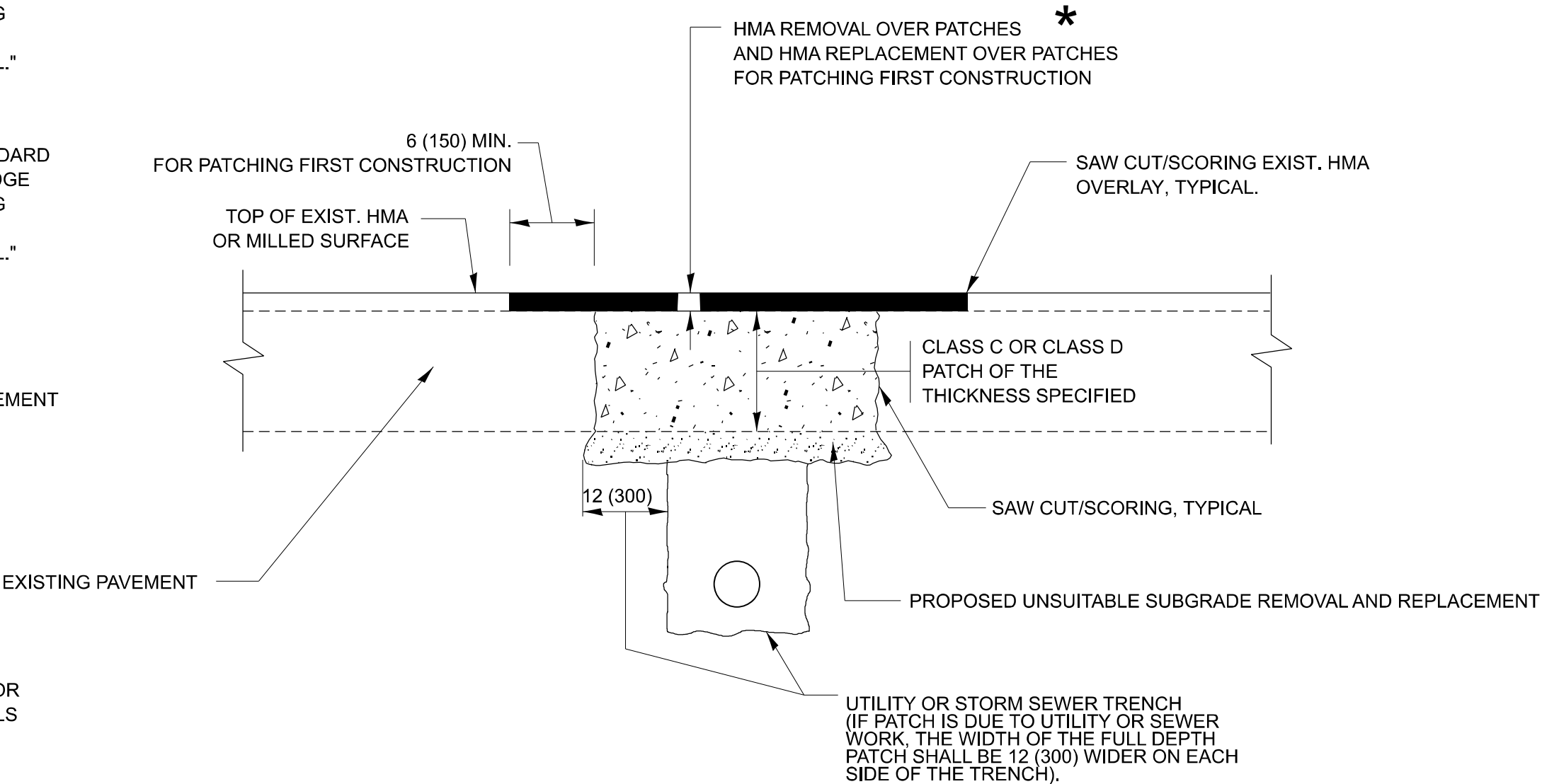
FAU 1332

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

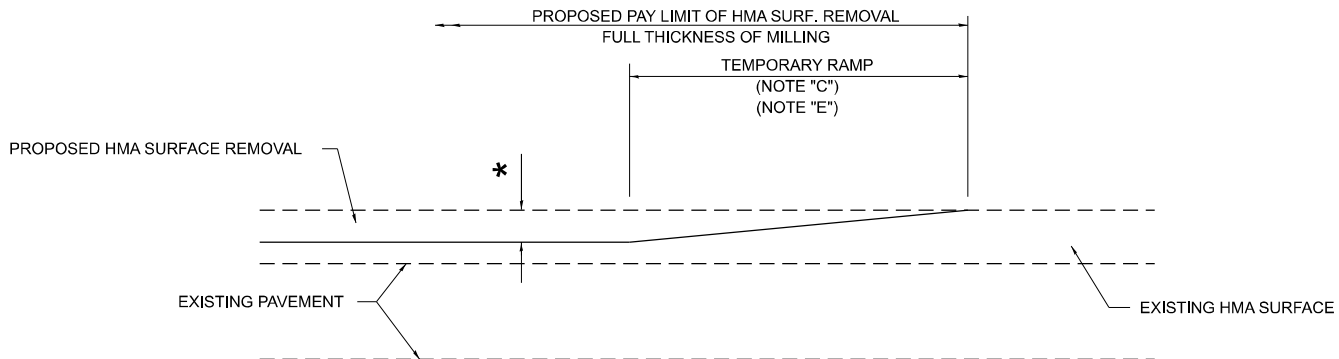
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

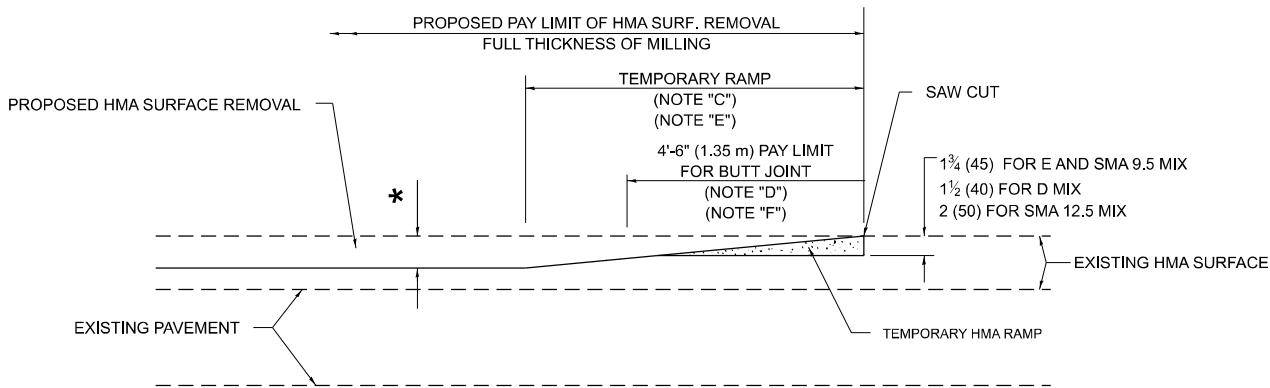
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		DRAWN -	REVISED - R. BORO 09-04-07						348	FAP 0348/1332 23 RES	COOK	43	19
		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22) CONTRACT NO. 62V42				
	PLOT DATE = 10/17/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		ILLINOIS FED. AID PROJECT								
							SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

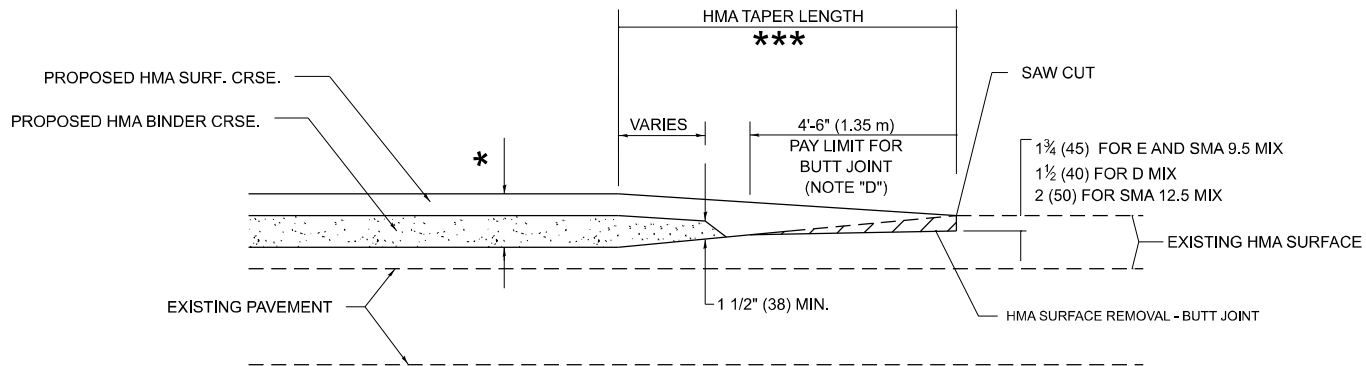
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

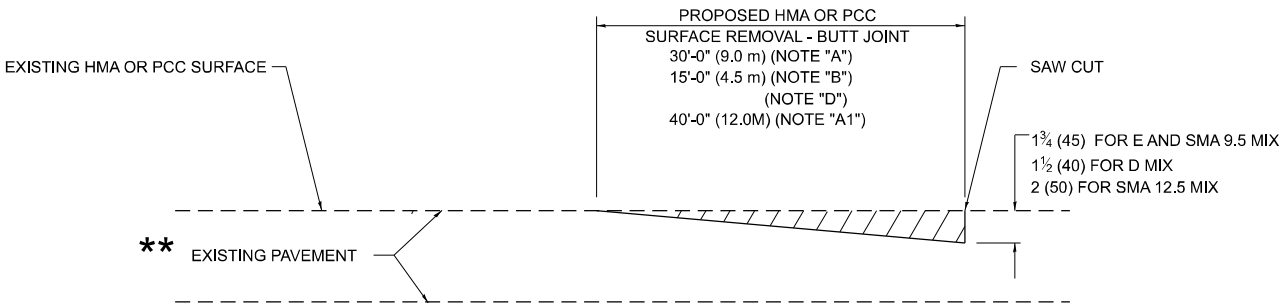
OPTION 2

TYPICAL TEMPORARY RAMP

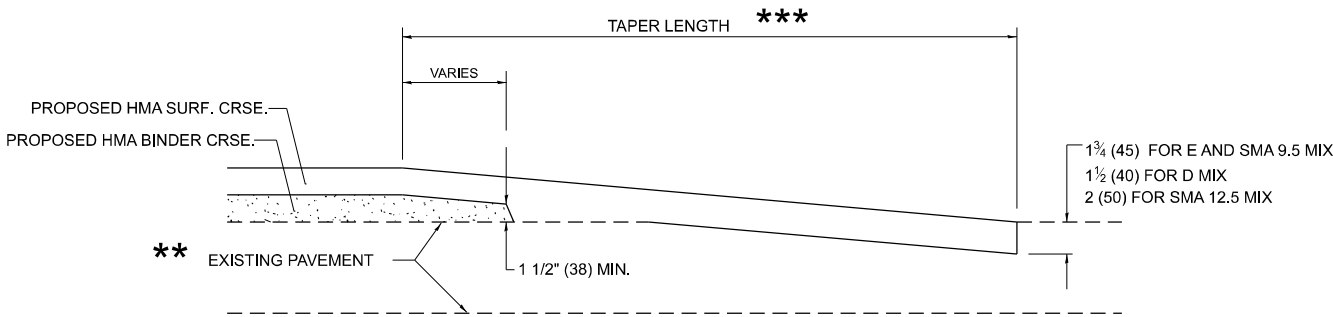


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = justin.king	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07
	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 10/17/2025	DATE - 06-13-90	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION





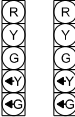
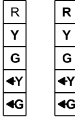












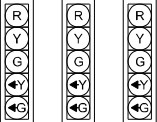
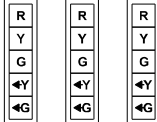






















































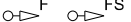


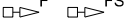





































**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	21
BD400-05 BD-32		CONTRACT NO. 62V42		
		ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

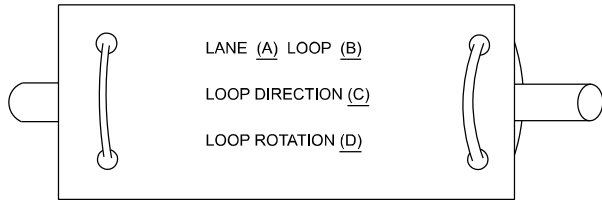
(NOT TO SCALE)

ITEM				ITEM				ITEM			
		EXISTING	PROPOSED			EXISTING	PROPOSED			EXISTING	PROPOSED
CONTROLLER CABINET				HANDHOLE				SIGNAL HEAD			
COMMUNICATION CABINET				-SQUARE				-(P) PROGRAMMABLE SIGNAL HEAD			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE							
MASTER MASTER CONTROLLER				-SQUARE							
UNINTERRUPTABLE POWER SUPPLY				-ROUND							
SERVICE INSTALLATION				DOUBLE HANDHOLE				SIGNAL HEAD WITH BACKPLATE			
-(P) POLE MOUNTED				JUNCTION BOX				-(P) PROGRAMMABLE SIGNAL HEAD			
SERVICE INSTALLATION				RAILROAD CANTILEVER MAST ARM				-(RB) RETROREFLECTIVE BACKPLATE			
-(G) GROUND MOUNTED				RAILROAD FLASHING SIGNAL							
-(GM) GROUND MOUNTED METERED				RAILROAD CROSSING GATE							
TELEPHONE CONNECTION				RAILROAD CROSSBUCK				PEDESTRIAN SIGNAL HEAD			
STEEL MAST ARM ASSEMBLY AND POLE				RAILROAD CONTROLLER CABINET				AT RAILROAD INTERSECTIONS			
ALUMINUM MAST ARM ASSEMBLY AND POLE				UNDERGROUND CONDUIT (UC), GALVANIZED STEEL				PEDESTRIAN SIGNAL HEAD			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				WITH COUNTDOWN TIMER			
SIGNAL POST				SYSTEM ITEM				ILLUMINATED SIGN			
-(BM) BARREL MOUNTED - TEMPORARY				INTERSECTION ITEM				"NO LEFT TURN"/"NO RIGHT TURN"			
WOOD POLE				REMOVE ITEM							
GUY WIRE				RELOCATE ITEM							
SIGNAL HEAD				ABANDON ITEM							
SIGNAL HEAD WITH BACKPLATE				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED							
SIGNAL HEAD OPTICALLY PROGRAMMED				MAST ARM POLE AND FOUNDATION TO BE REMOVED							
FLASHER INSTALLATION				SIGNAL POST AND FOUNDATION TO BE REMOVED							
-(FS) SOLAR POWERED				DETECTOR LOOP, TYPE I							
PEDESTRIAN SIGNAL HEAD				PREFORMED DETECTOR LOOP							
PEDESTRIAN PUSH BUTTON				SAMPLING (SYSTEM) DETECTOR							
-(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON				INTERSECTION AND SAMPLING (SYSTEM) DETECTOR							
RADAR DETECTION SENSOR				QUEUE AND SAMPLING (SYSTEM) DETECTOR							
VIDEO DETECTION CAMERA				WIRELESS DETECTOR SENSOR							
RADAR/VIDEO DETECTION ZONE				WIRELESS ACCESS POINT							
PAN, TILT, ZOOM (PTZ) CAMERA											
EMERGENCY VEHICLE LIGHT DETECTOR											
CONFIMATION BEACON											
WIRELESS INTERCONNECT											
WIRELESS INTERCONNECT RADIO REPEATER											
											</

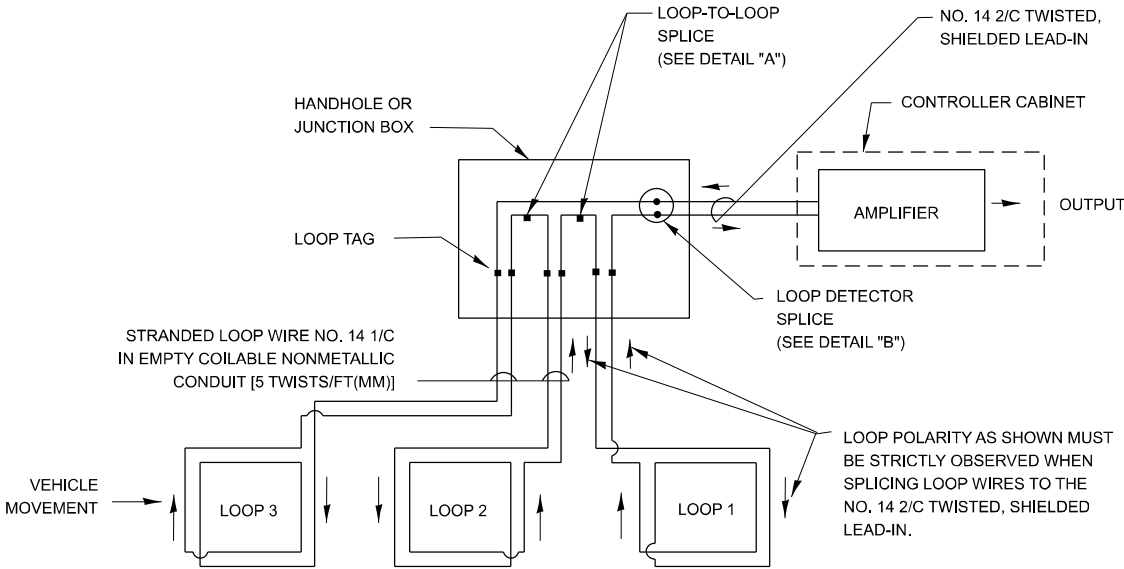
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE, SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

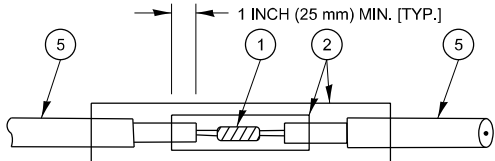


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

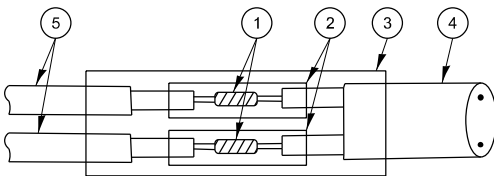


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

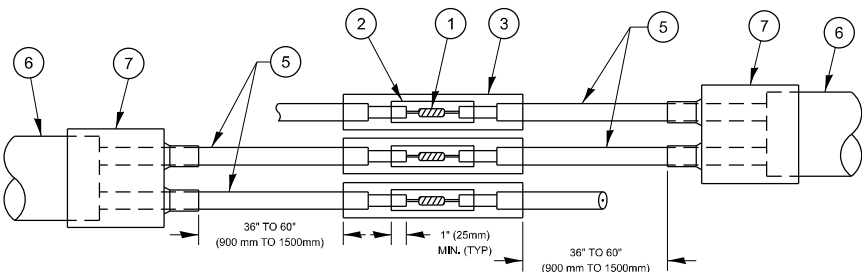


DETAIL "A"
LOOP-TO-LOOP SPLICE

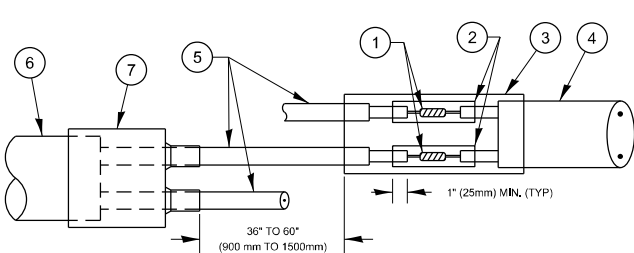


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

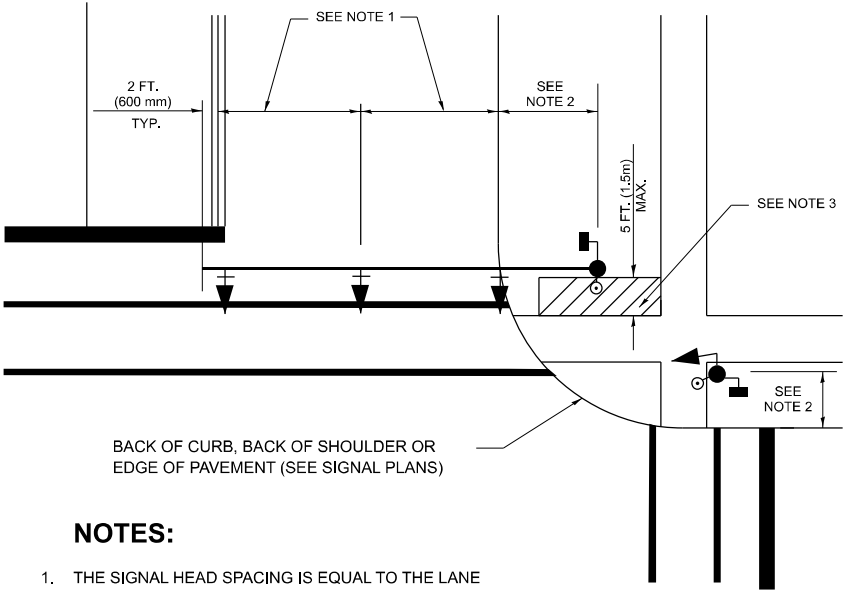
- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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		CHECKED -	REVISED -						TS-05		CONTRACT NO. 62V42			
	PLOT DATE = 10/17/2025	DATE -	REVISED -		SCALE: NONE		SHEET 2	OF 7 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

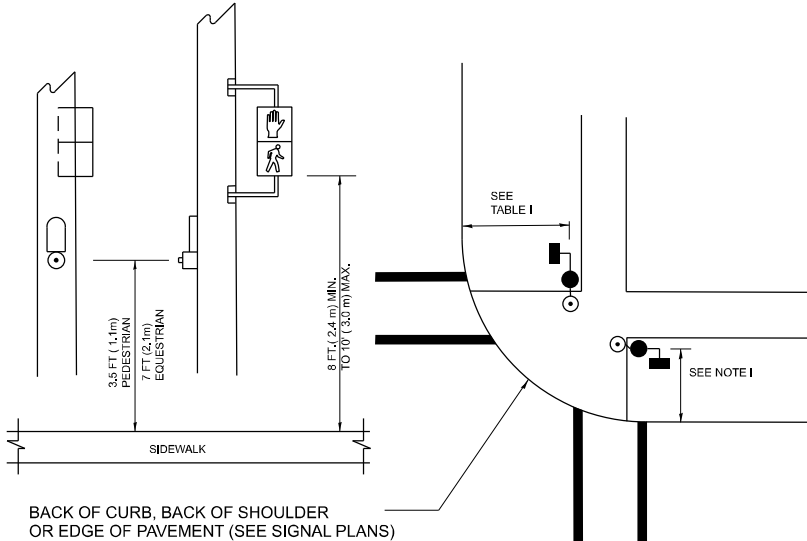
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

- 1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
- 2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
- 4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

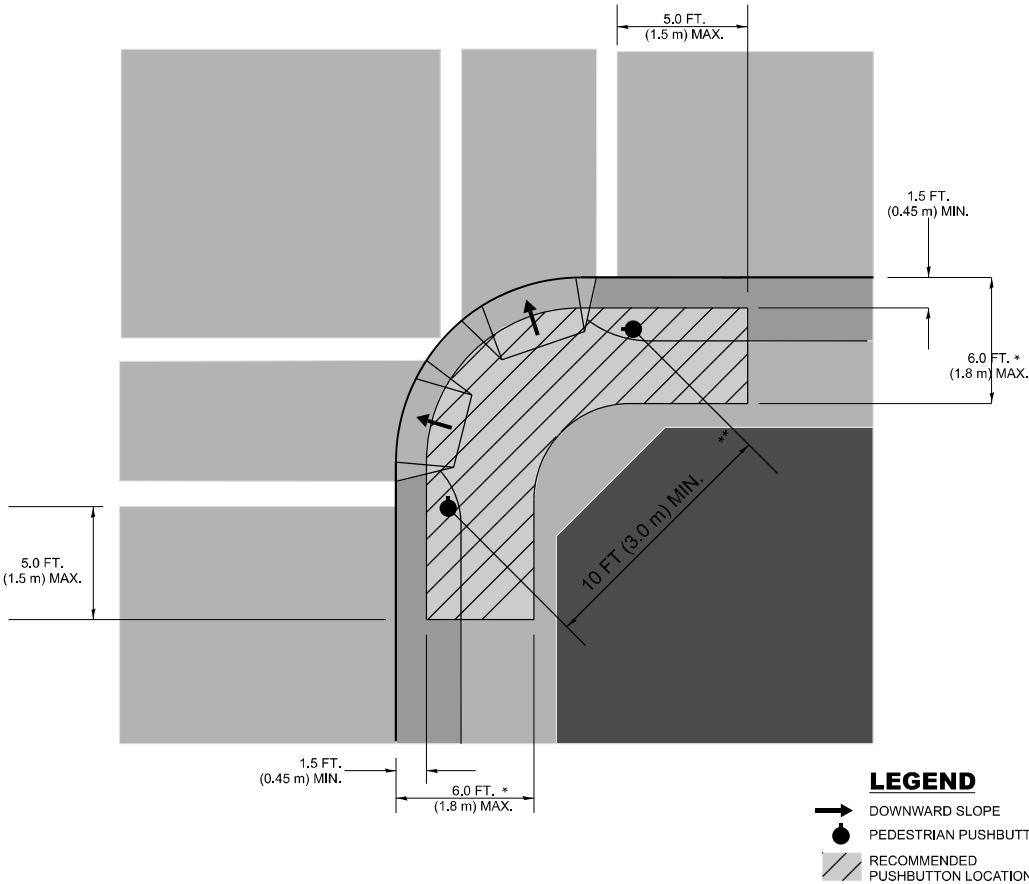
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

- 1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
- 2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
- 3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
- 4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

- 1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
- 2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
- 3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
- 5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

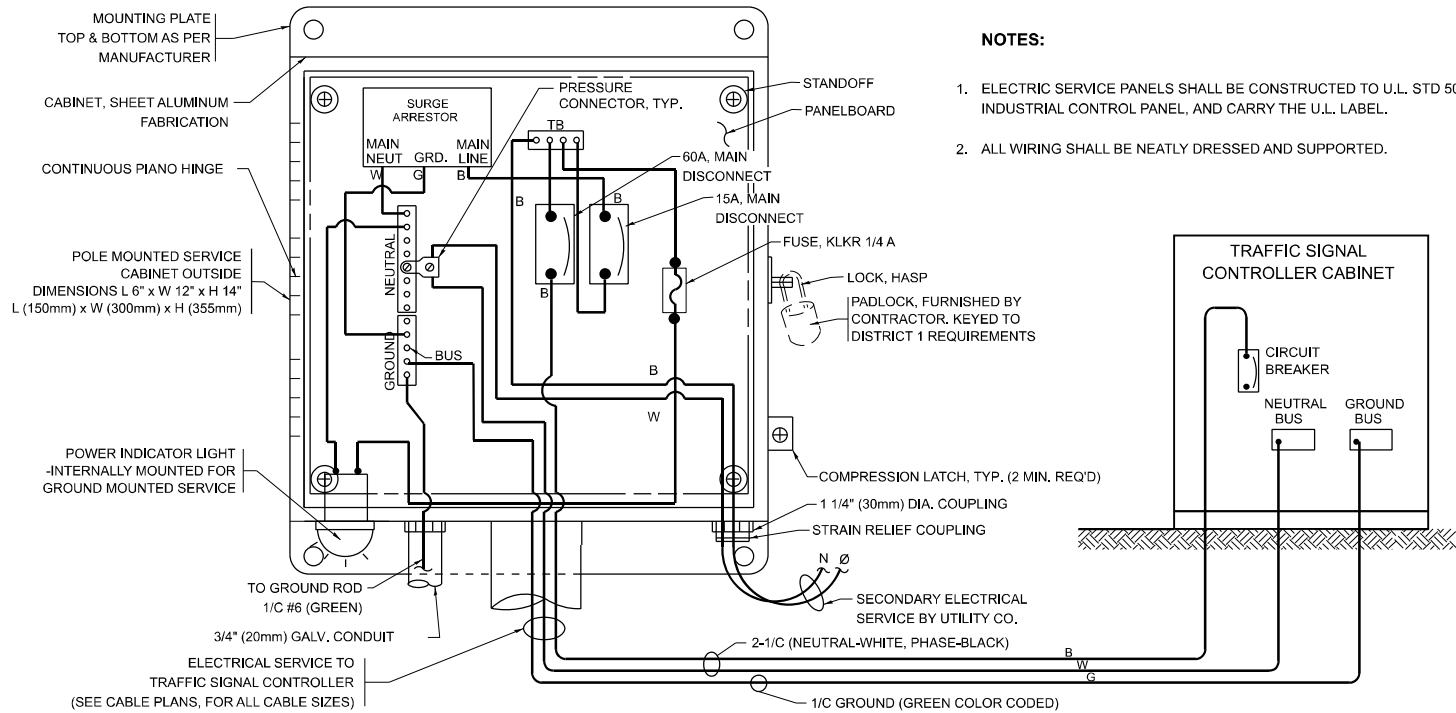
NOTES:

- 1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
- 2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
- 3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
- 4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

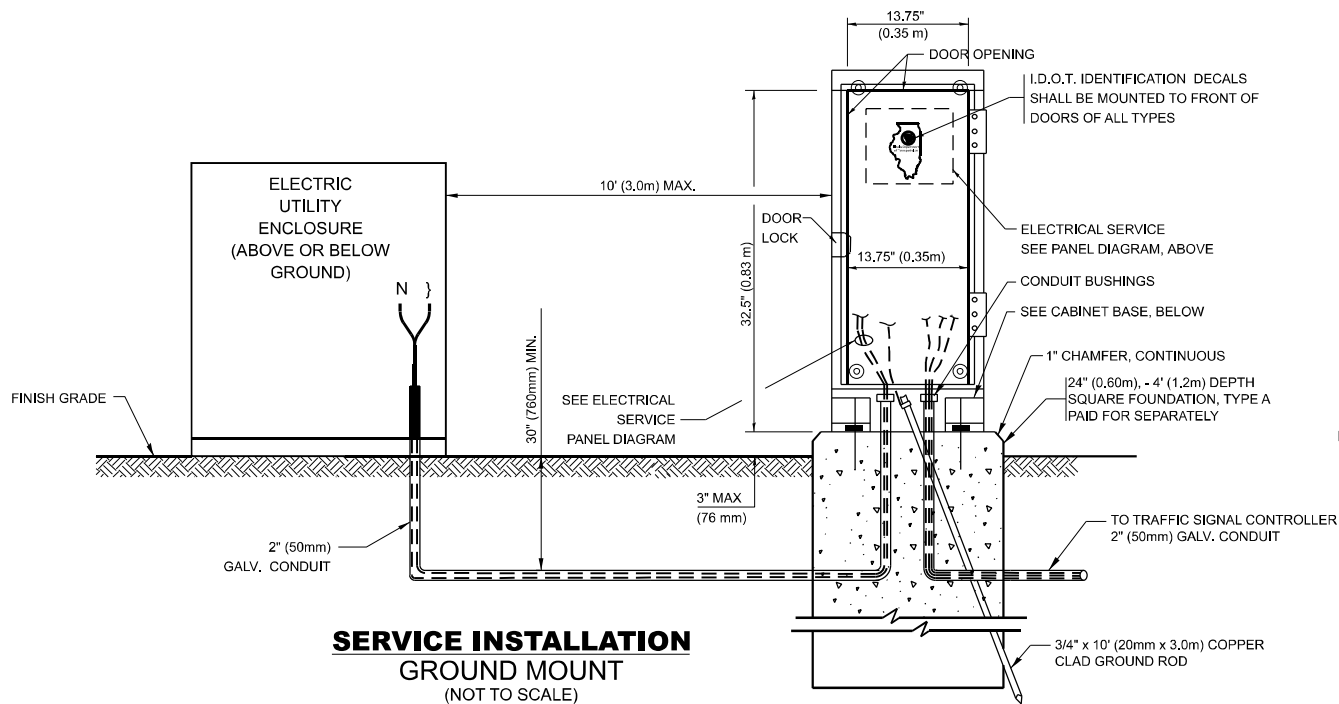
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		DRAWN -	REVISED -						348	FAP 0348/1332 23 RES		COOK	43	24
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	PLOT DATE = 10/17/2025	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									
					SCALE: NONE	SHEET 3	OF 7	SHEETS	STA.	TO STA.				

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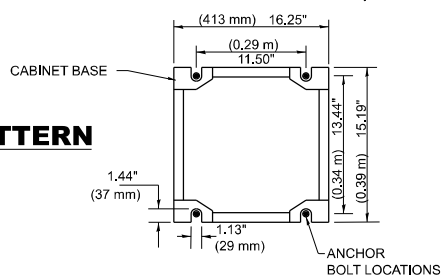


ELECTRICAL SERVICE - PANEL DIAGRAM
(TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



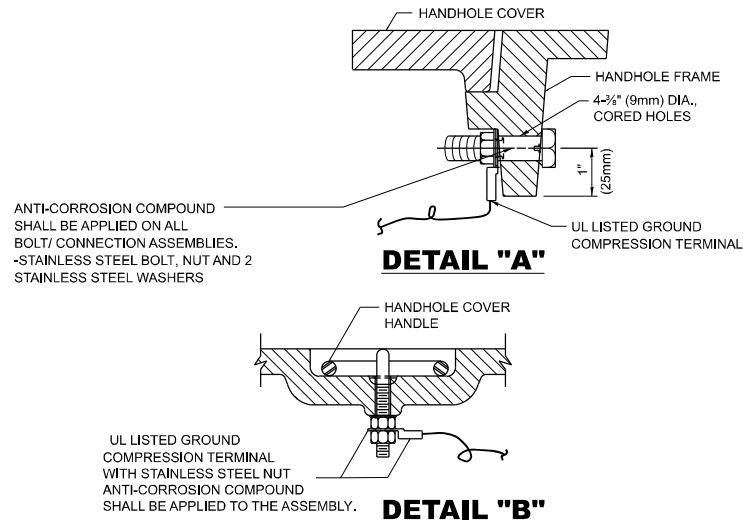
SERVICE INSTALLATION
GROUND MOUNT
(NOT TO SCALE)

CABINET - BASE BOLT PATTERN
(NOT TO SCALE)

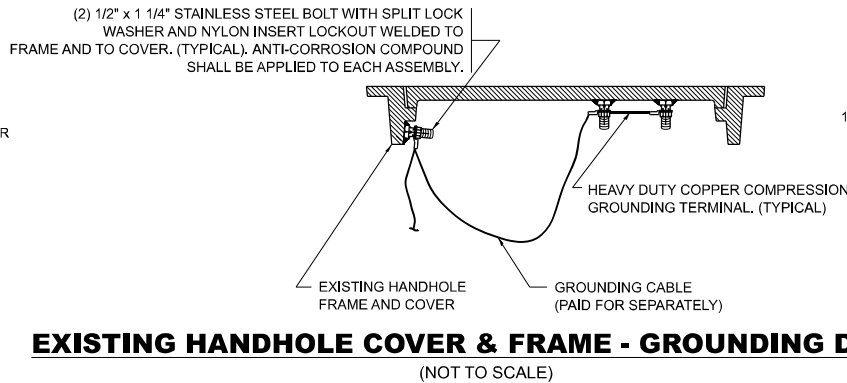


NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



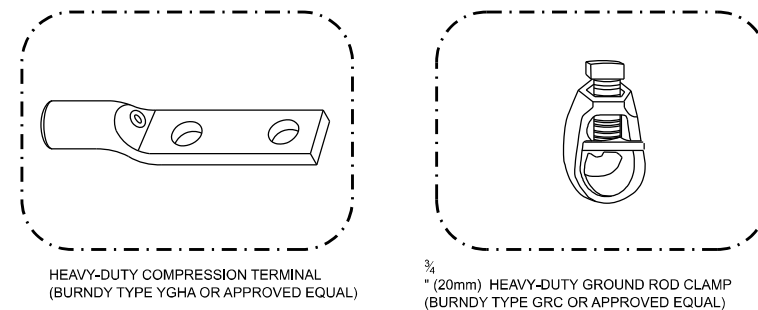
HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
(NOT TO SCALE)

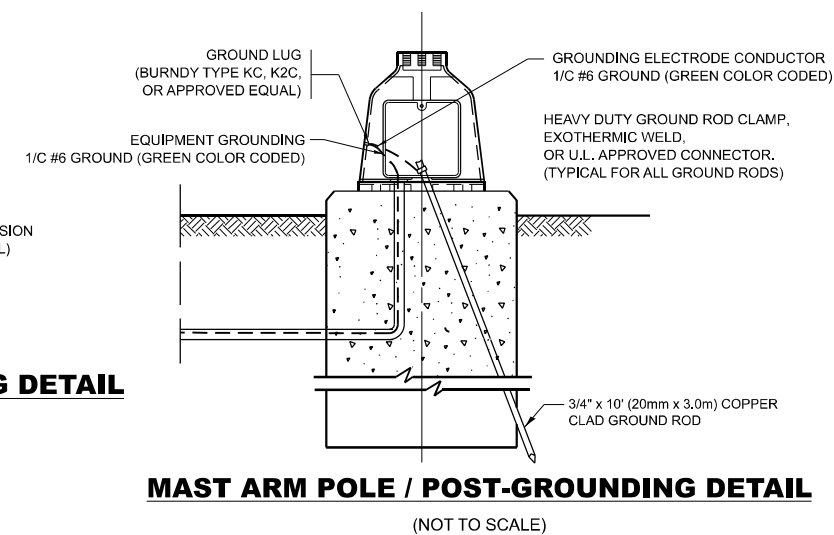
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST-GROUNDING DETAIL
(NOT TO SCALE)

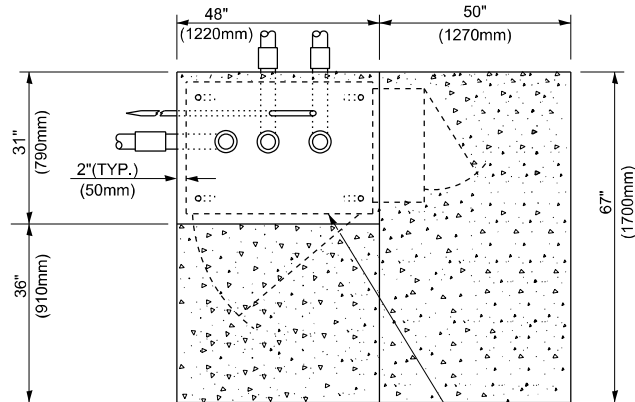
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

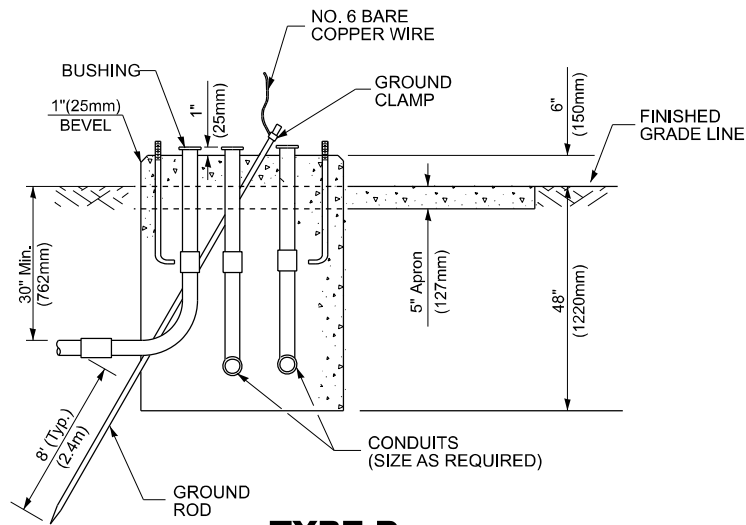
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

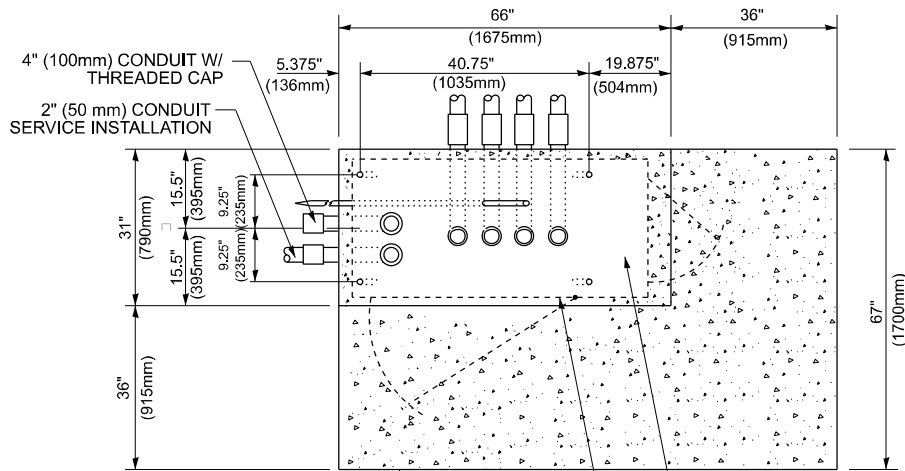
FAU 1332



TOP VIEW



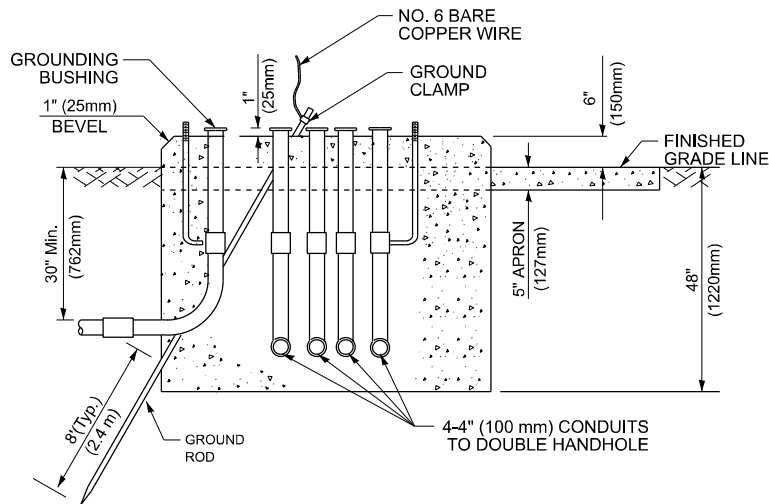
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



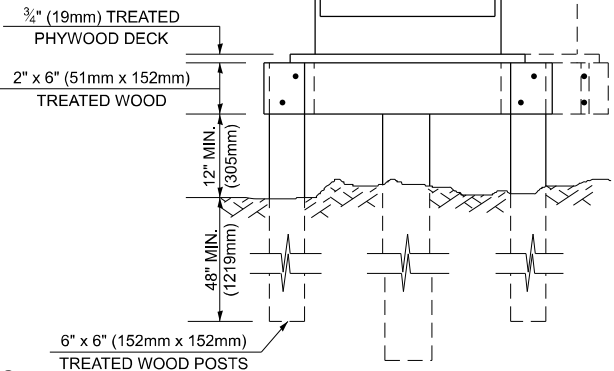
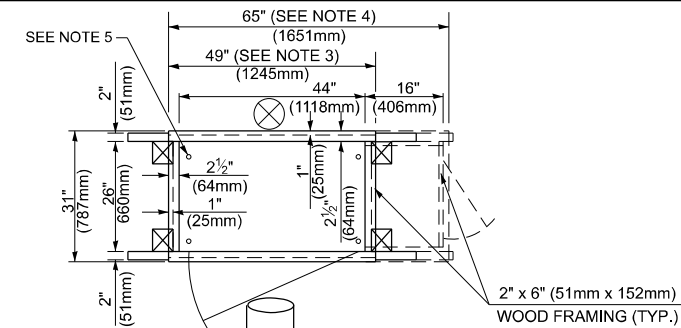
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD)		
(L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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PLOT DATE = 10/17/2025	DATE -	REVISED -

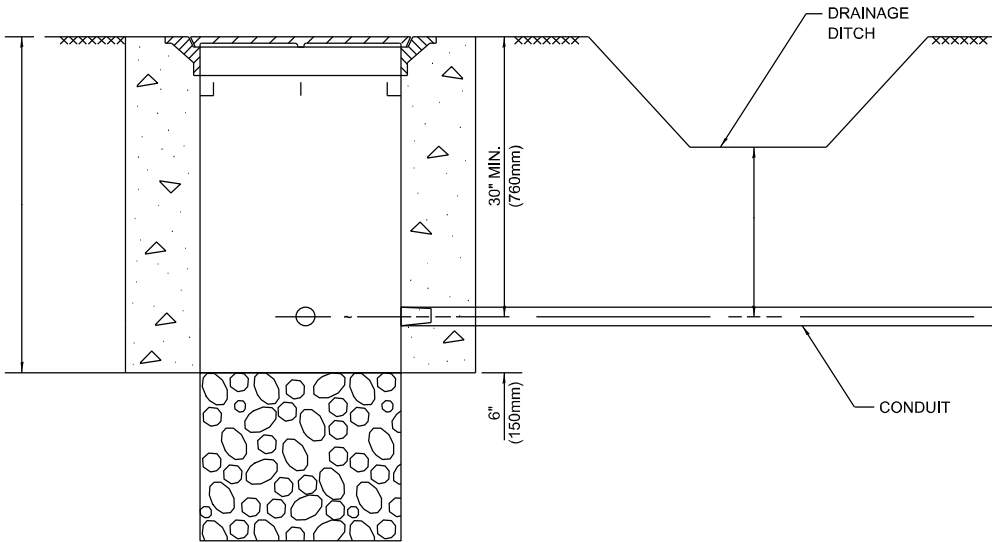
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05		CONTRACT NO. 62V42		
		ILLINOIS FED. AID PROJECT		

FAU 1332

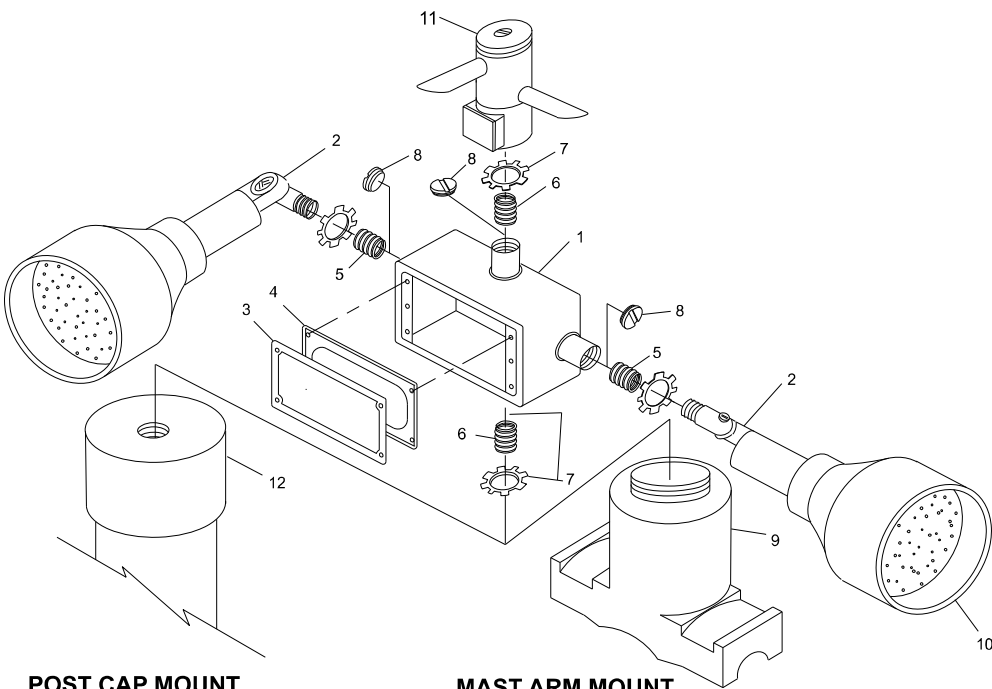


NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

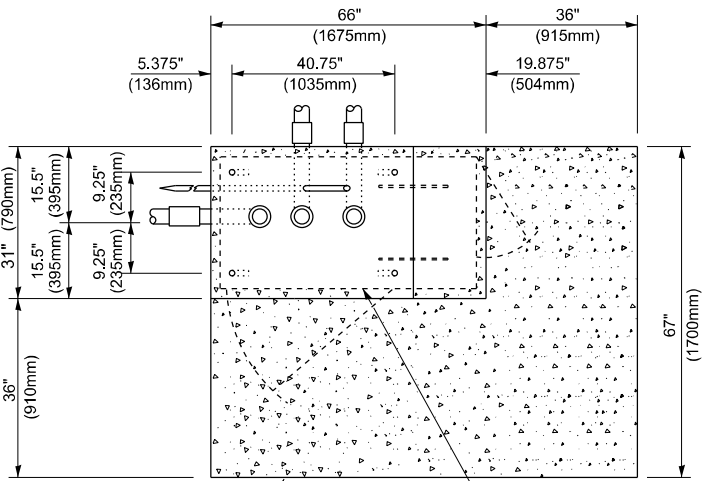
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POST CAP MOUNT

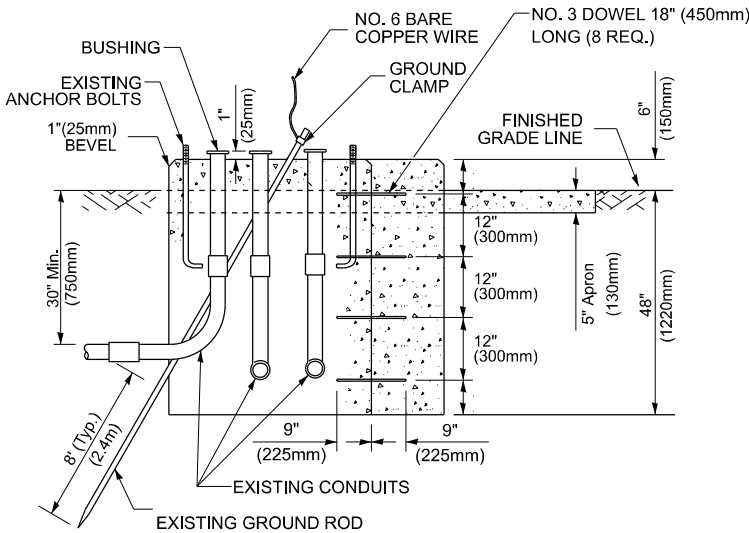
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW

(NOT TO SCALE)



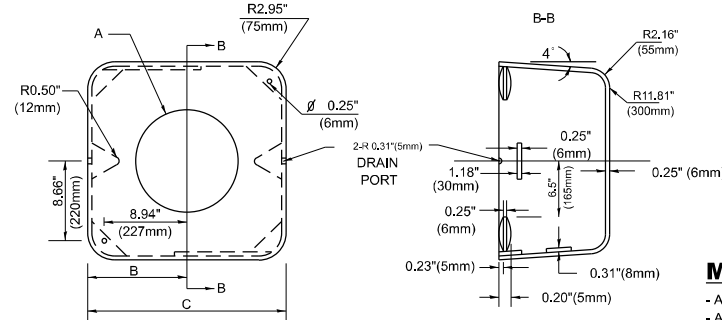
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

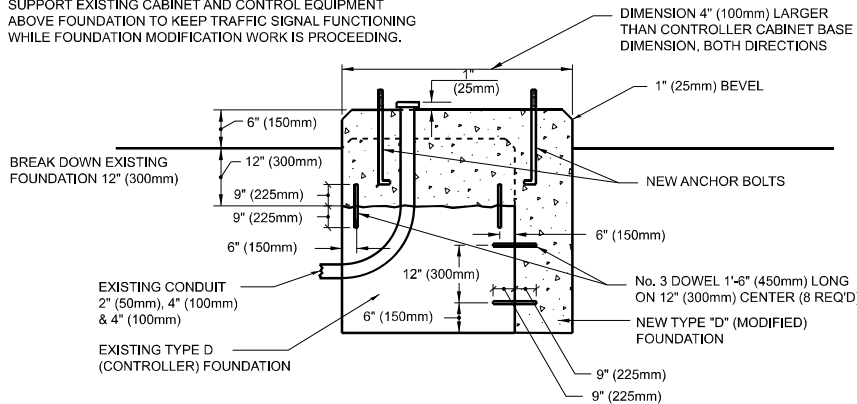
SHROUD

NOTES:

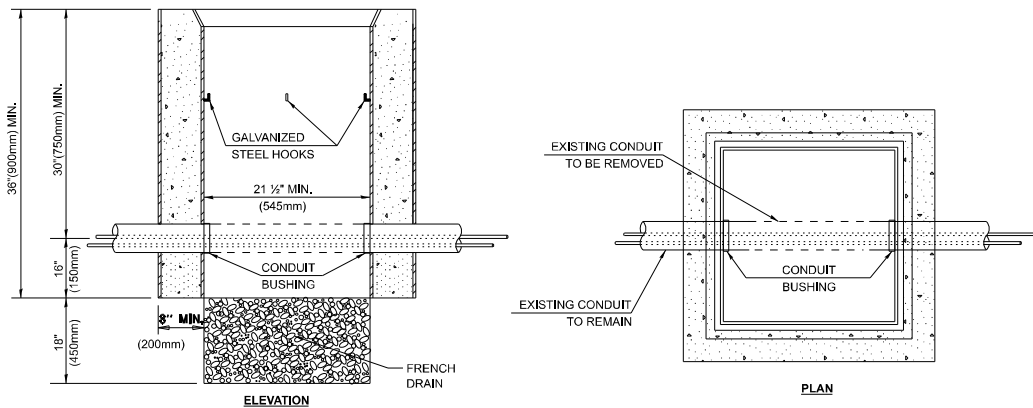
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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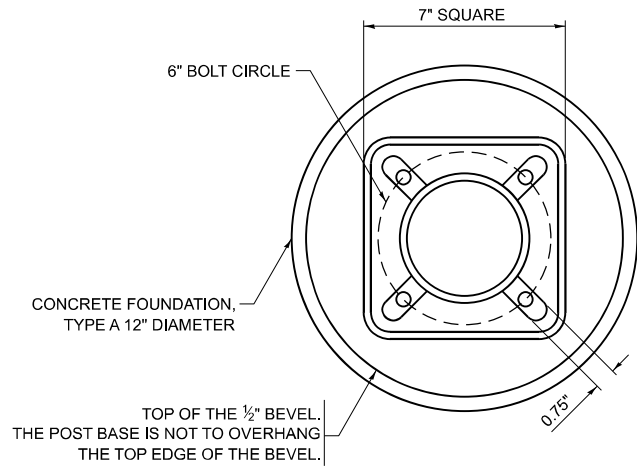
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-05		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

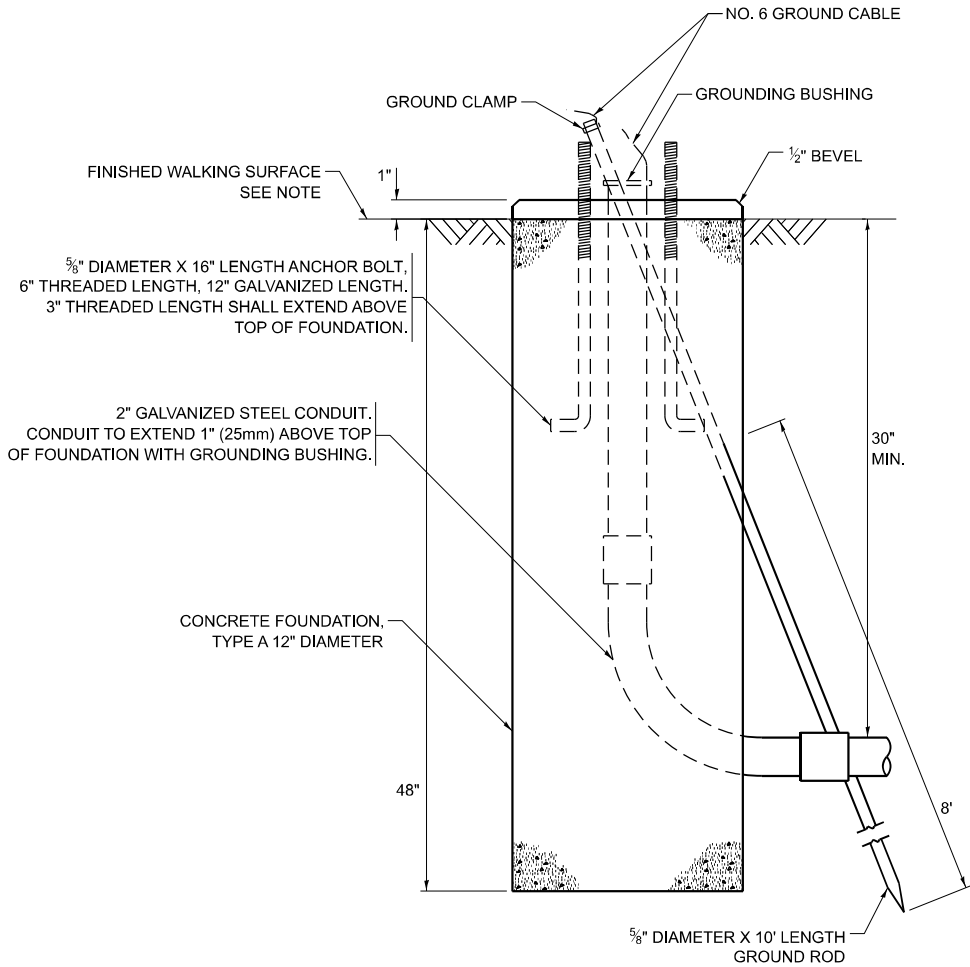
FAU 1332



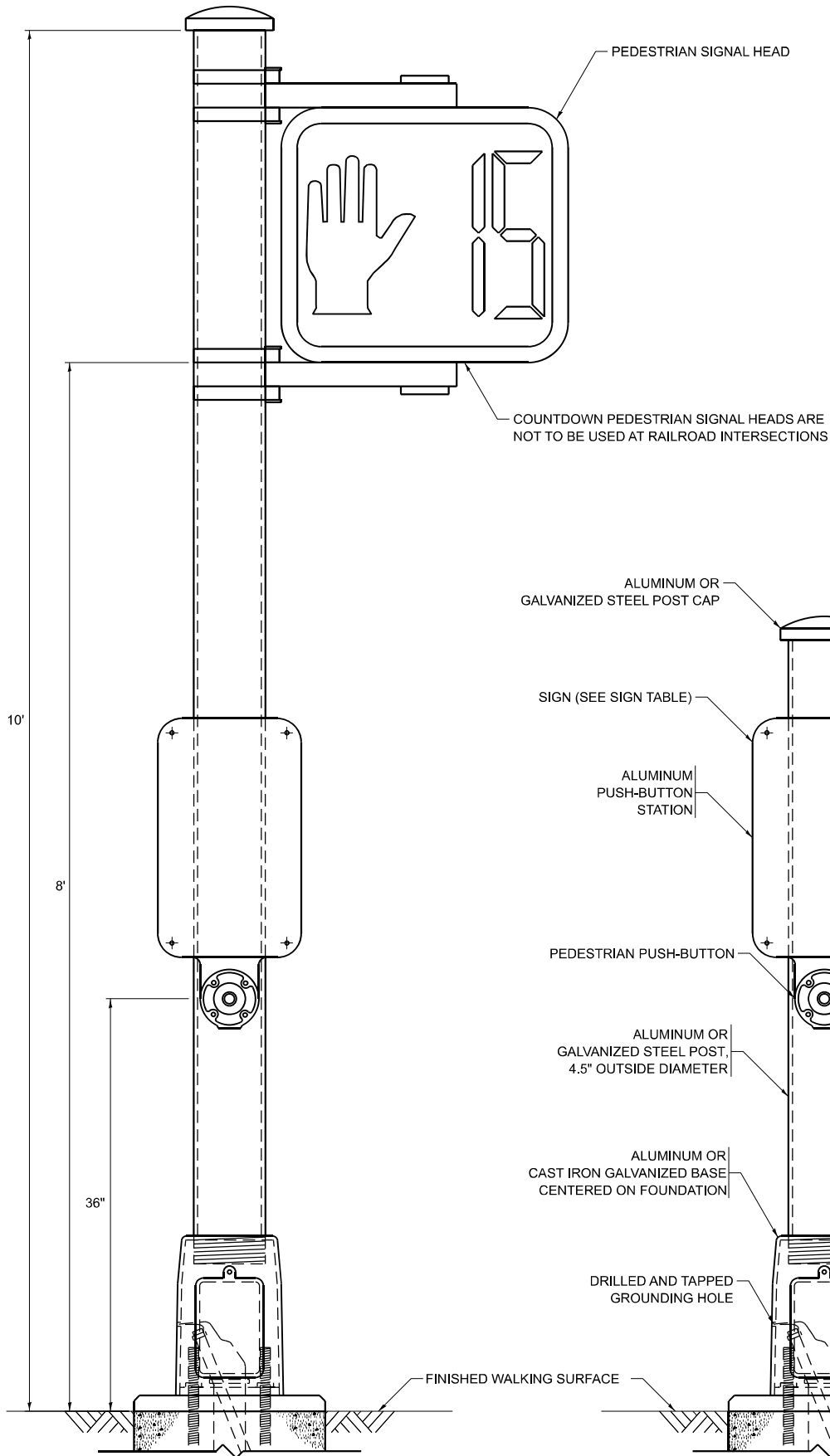
BOLT PATTERN

NOTE:

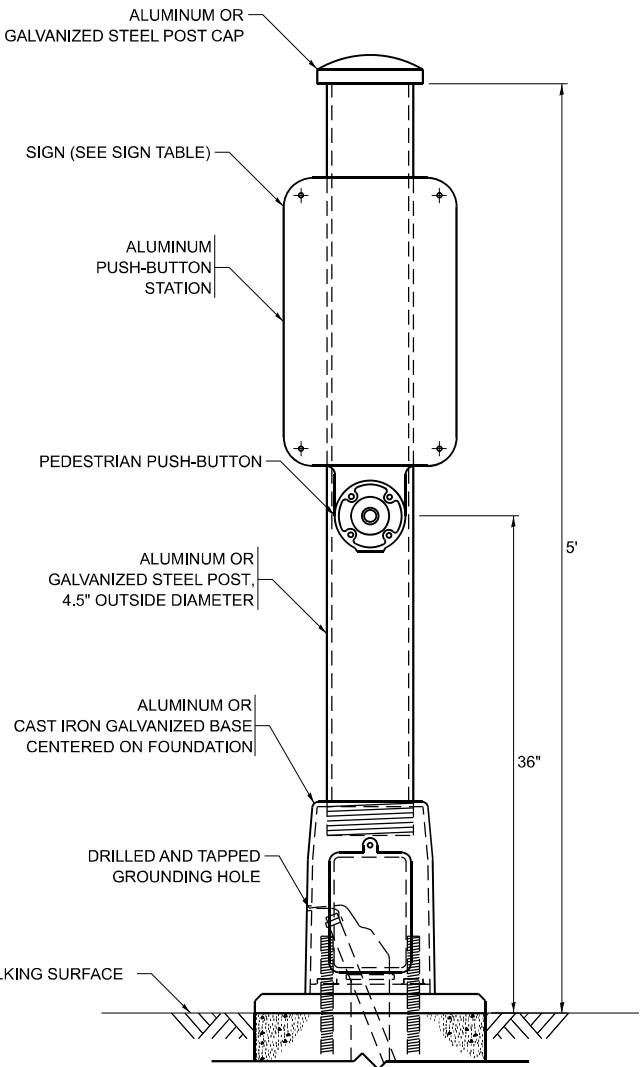
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



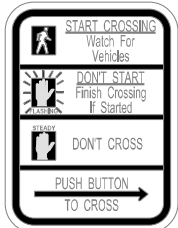
CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER



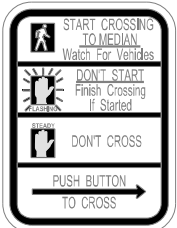
PEDESTRIAN SIGNAL POST, 10 FT.



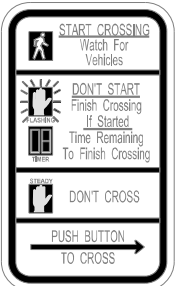
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

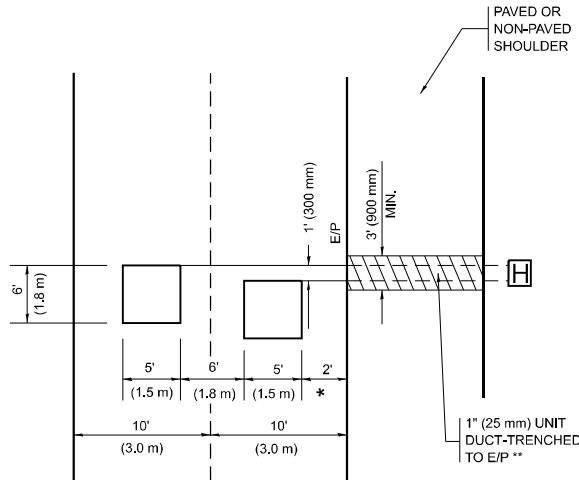
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	28
TS-05		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

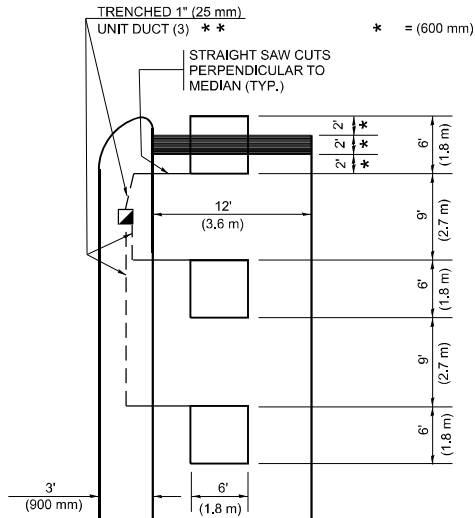


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

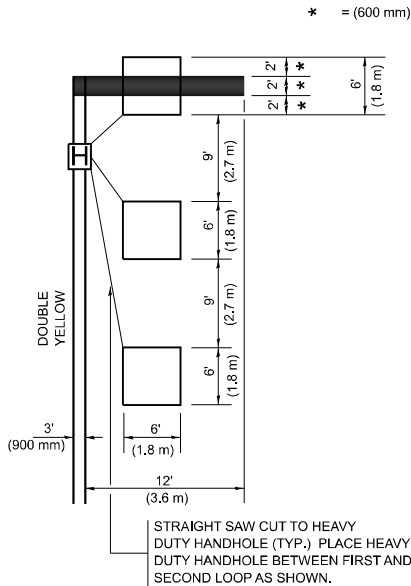
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

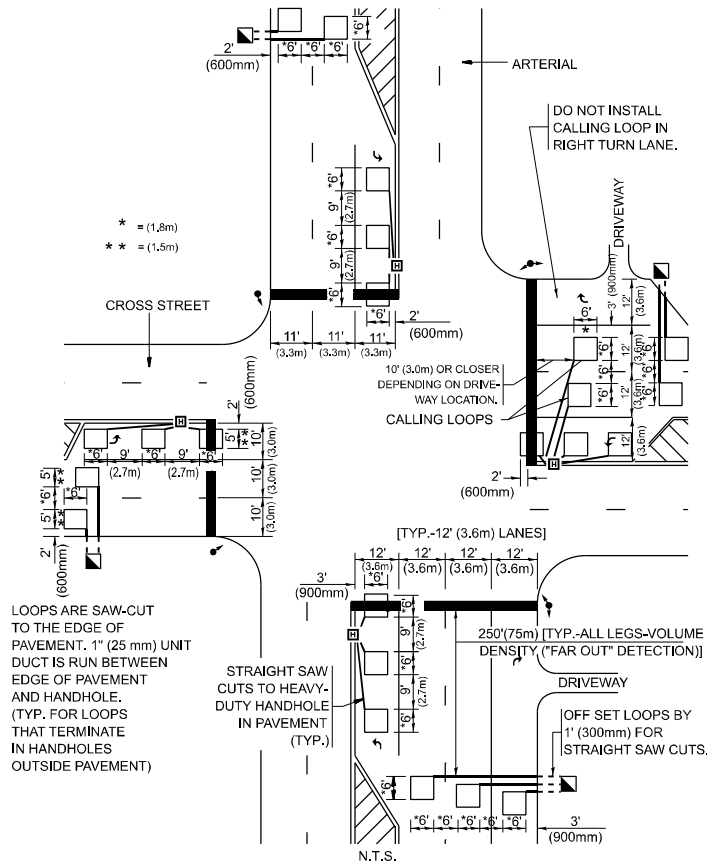
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



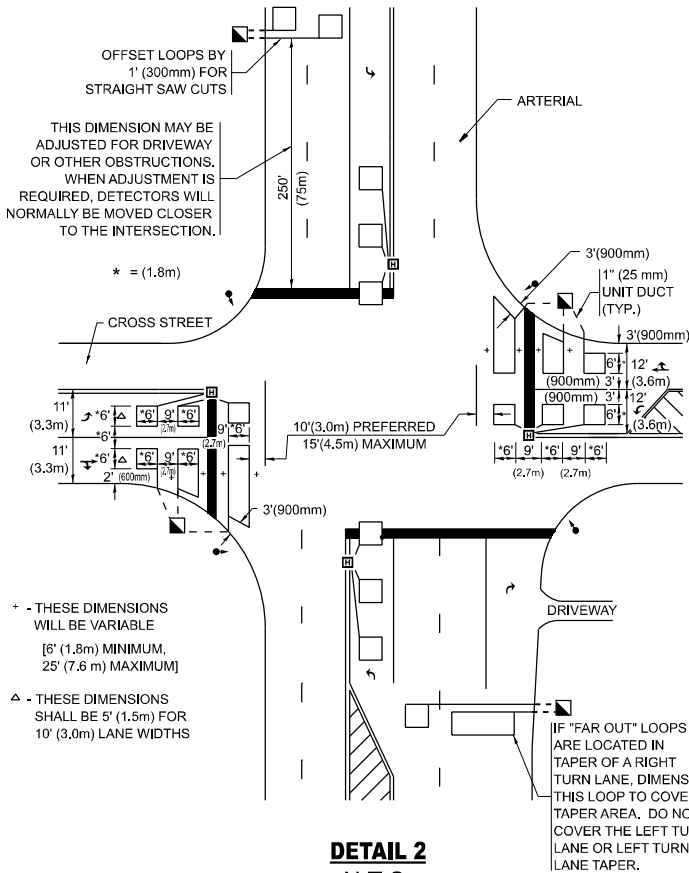
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

MODEL: TS-07 [Sheet]
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USER NAME = justin.king	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED - R.K.F.	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	29
TS-07		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

FAU 1332

MODEL: TC-10 (Sheet)
FILE NAME: c:\p\work\pwork\illinois.gov\justin.king@illinois.gov\1095500D\102024-shl-DistStd.dgn

USER NAME	= justin.king	DESIGNED	- L.H.A.	REVISED	- T. RAMMACHER 01-06-00
		DRAWN	-	REVISED	- A. SCHUETZE 07-01-13
		CHECKED	-	REVISED	- A. SCHUETZE 09-15-06
PLOT DATE	= 10/17/2025	DATE	- 06-89	REVISED	- D. SENDERAK 05-03-24

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

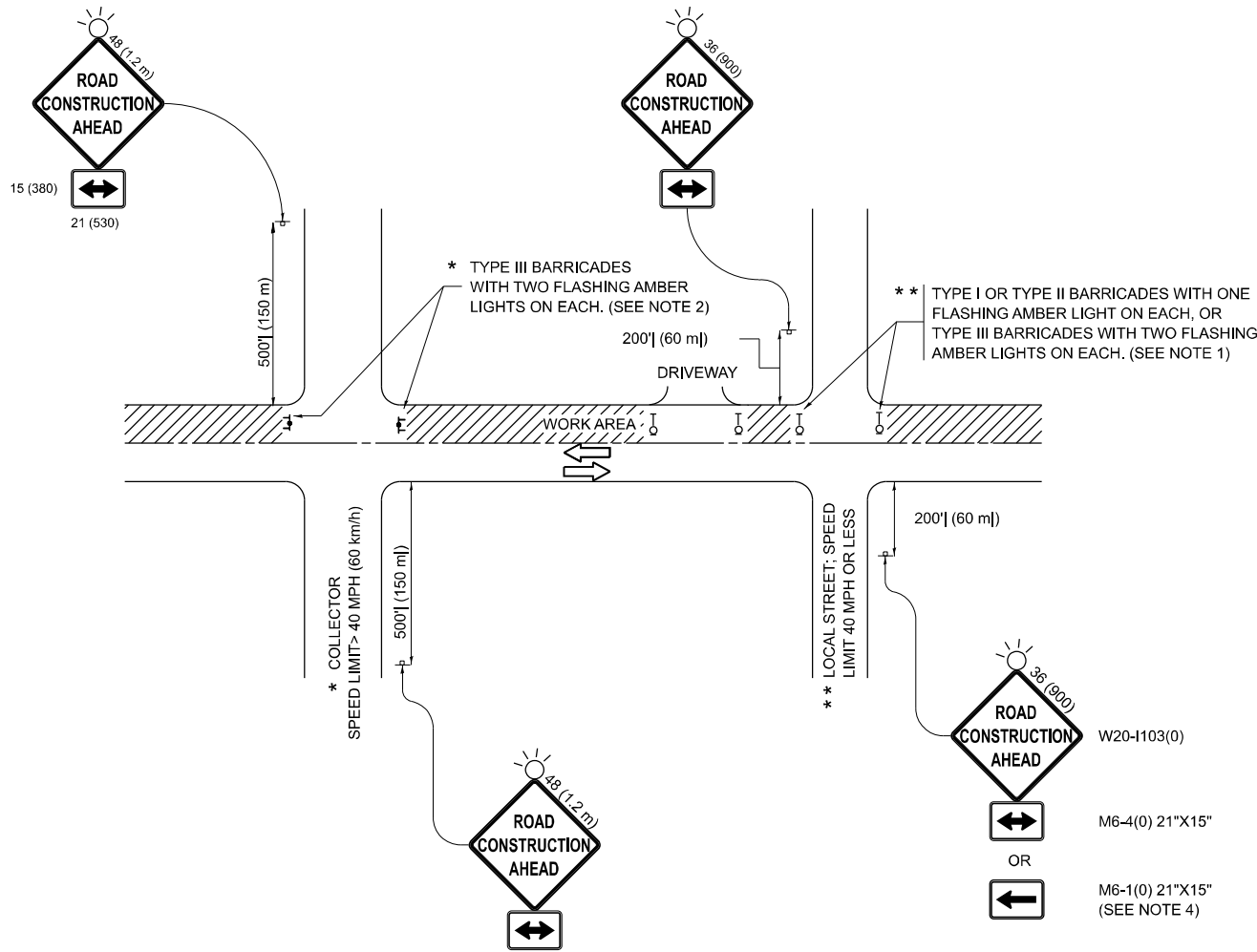
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: SHEET OF SHEETS STA. TO STA.

All dimensions are in inches (millimeters)
unless otherwise shown.

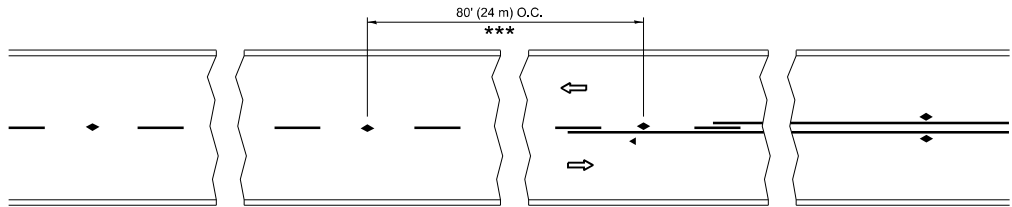
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TC-10		CONTRACT NO. 62V42		
		ILLINOIS FED. AID PROJECT		

FAU 1332

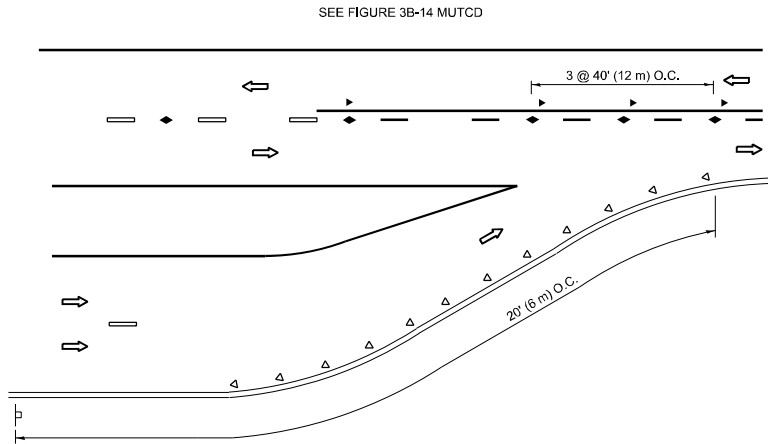


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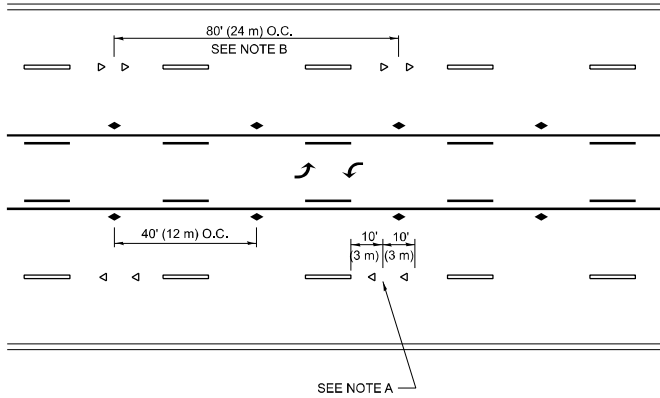
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



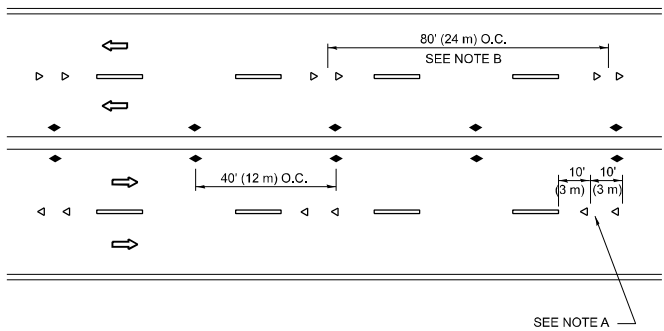
TWO-LANE/TWO-WAY



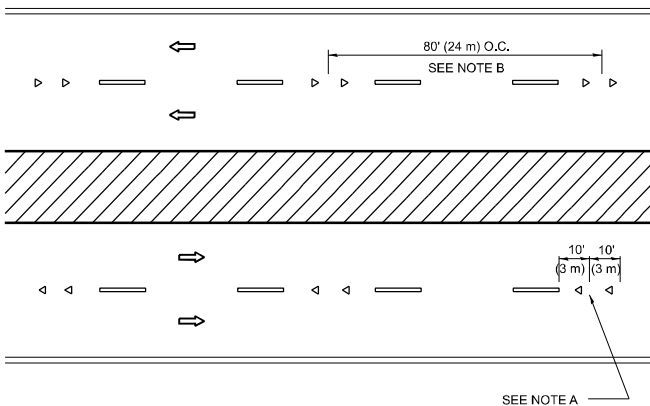
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

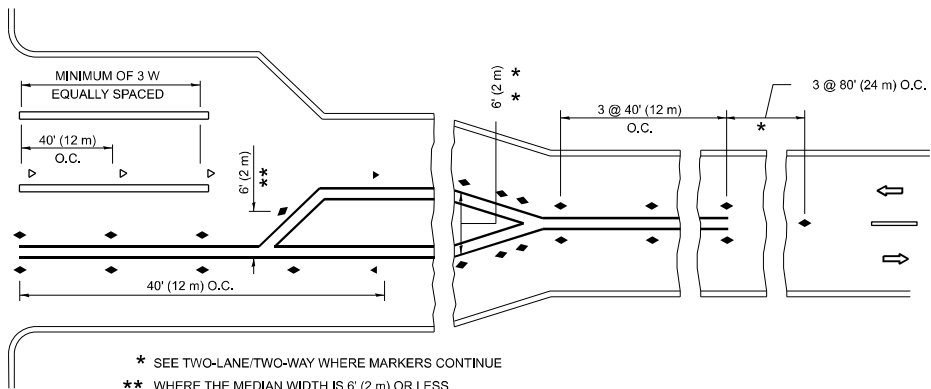
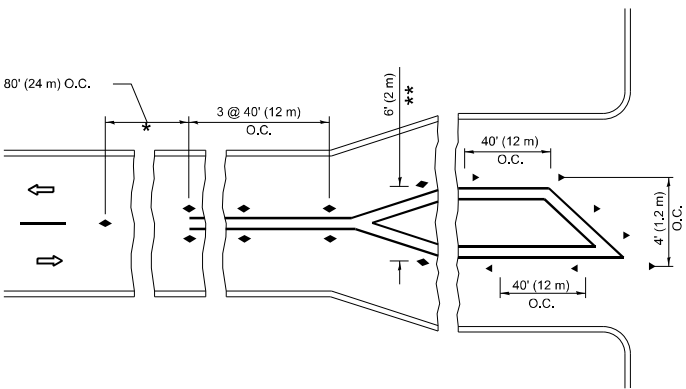
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

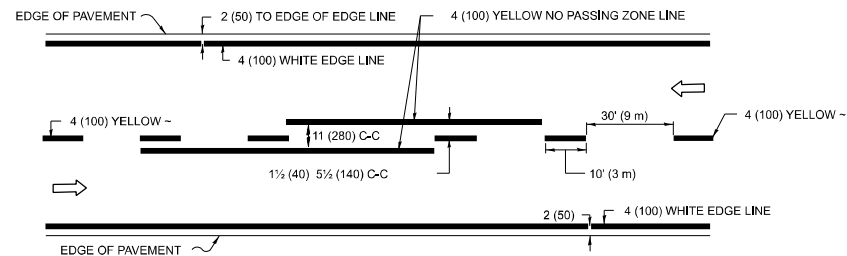
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

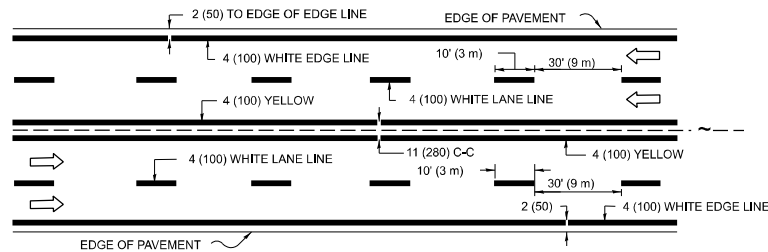
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	31
TC-11		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

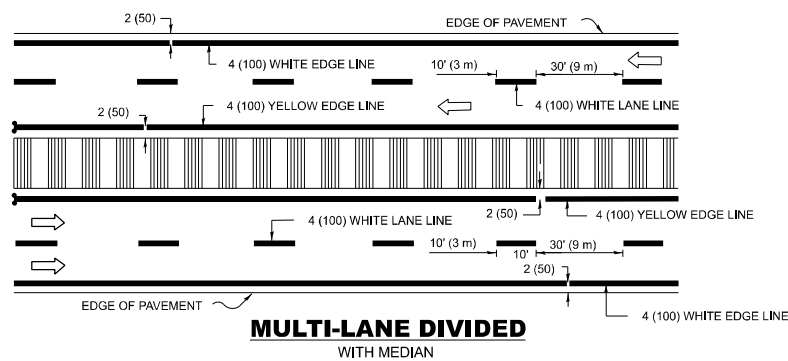
FAU 1332



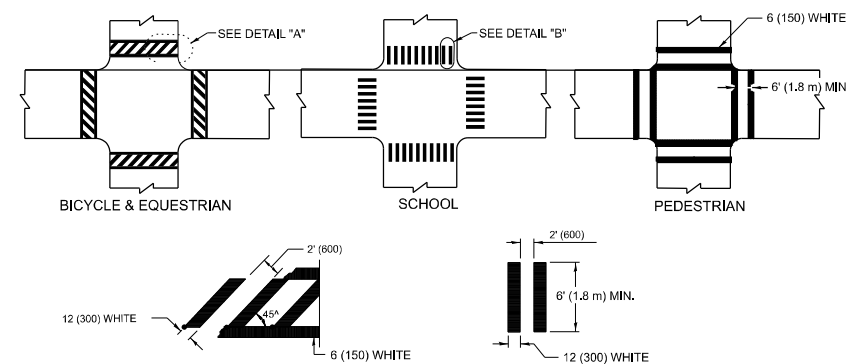
2-LANE ROADWAY



MULTI-LANE UNDIVIDED



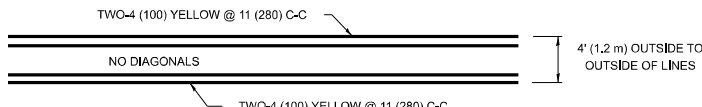
TYPICAL LANE AND EDGE LINE MARKING



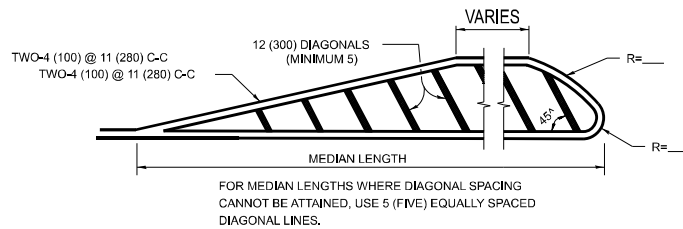
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

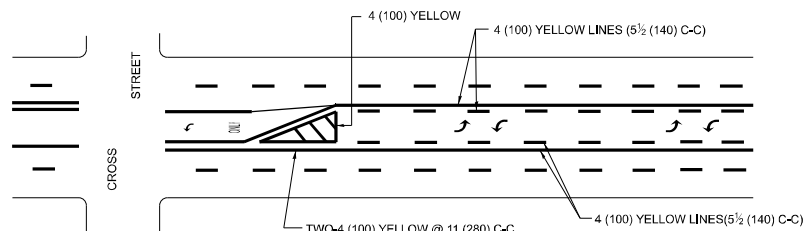


4' (1.2 m) WIDE MEDIANS ONLY

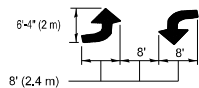


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

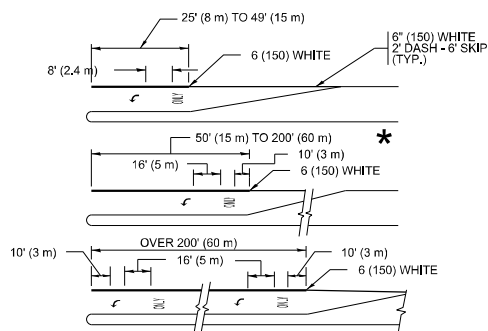
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING

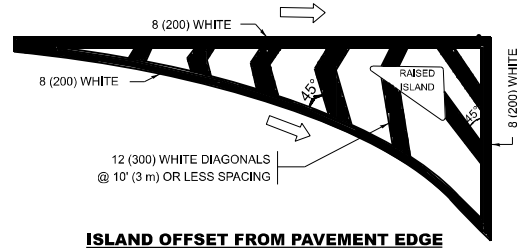


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

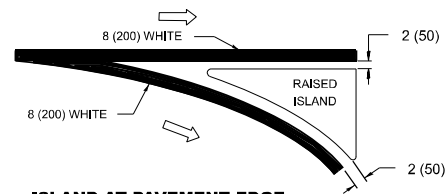
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

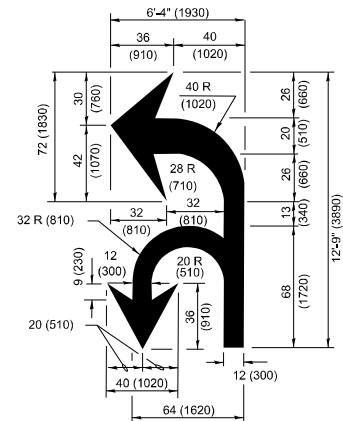


ISLAND OFFSET FROM PAVEMENT EDGE

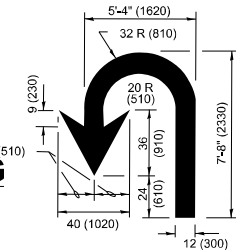


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION **LEFT AND U-TURN**



U-TURN

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FOR SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = justin.king	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
	DRAWN -	REVISED - C. JUCIUS 07-01-13
	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 10/17/2025	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

<div style="text-align: center;"> DISTRICT ONE TYPICAL PAVEMENT MARKINGS </div>					F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					348	FAP 0348/1332 23 RES	COOK	43	32
					TC-13		CONTRACT NO. 62V42		
					ILLINOIS			FED. AID PROJECT	
SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				

FAU 1332

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

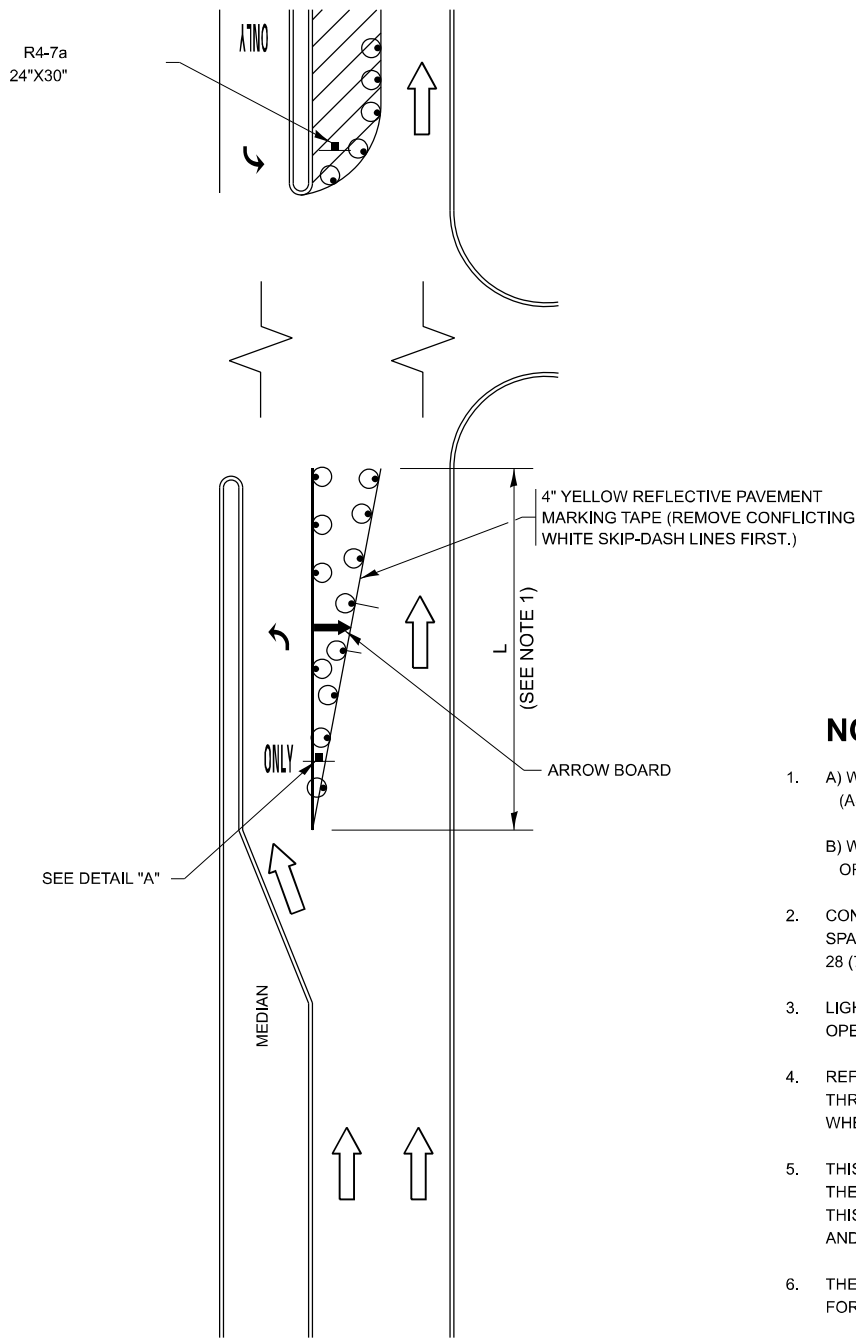
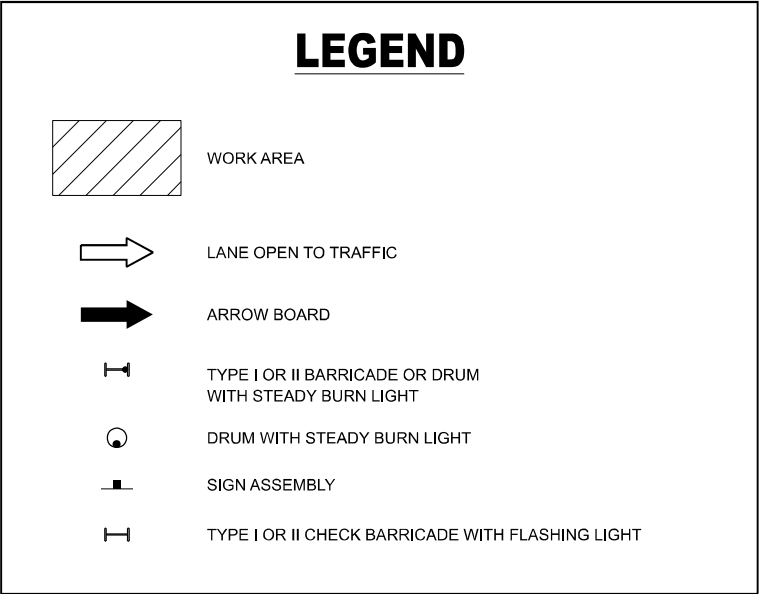


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

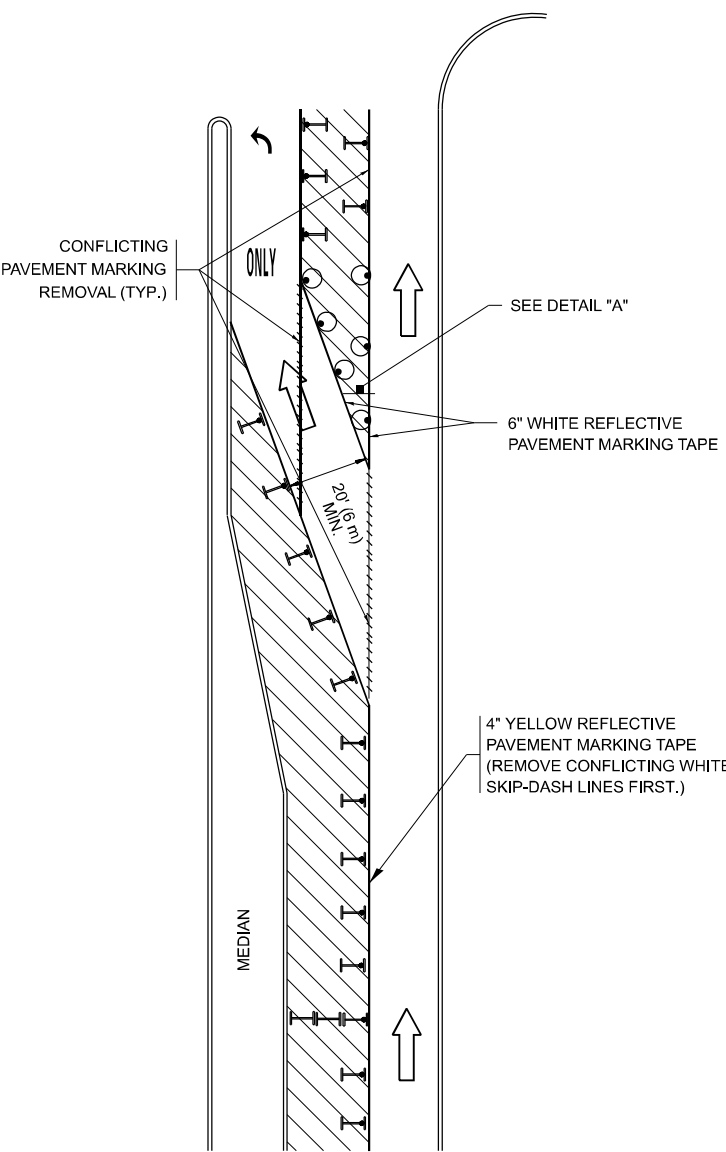
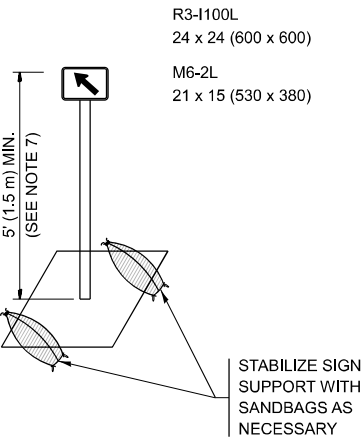


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
CHECKED	- A. HOUSEH 10-12-96
PLOT DATE	= 10/17/2025
DATE	- T. RAMMACHER 01-06-00

REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

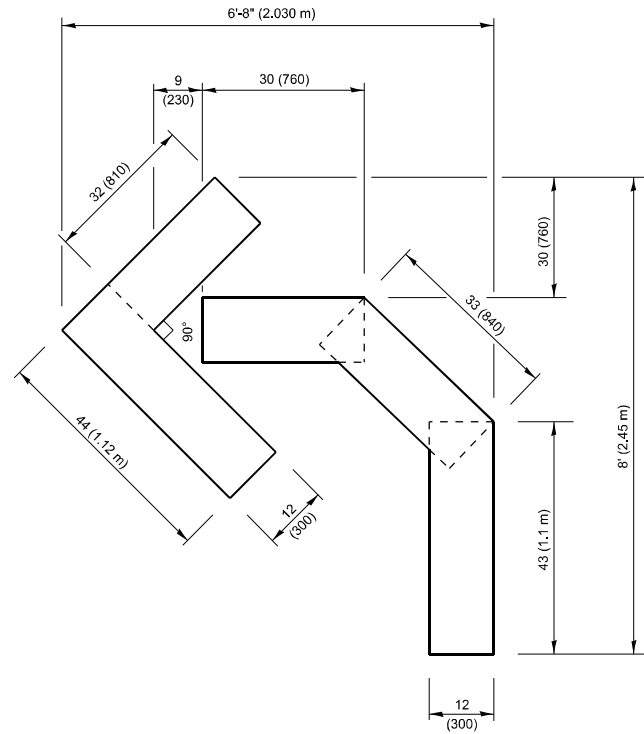
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

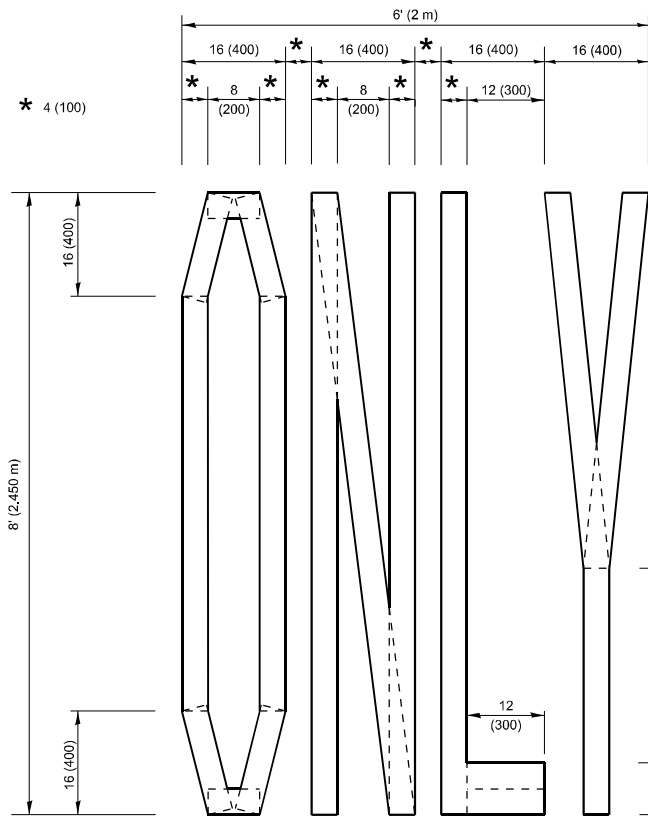
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	33
TC-14		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

FAU 1332



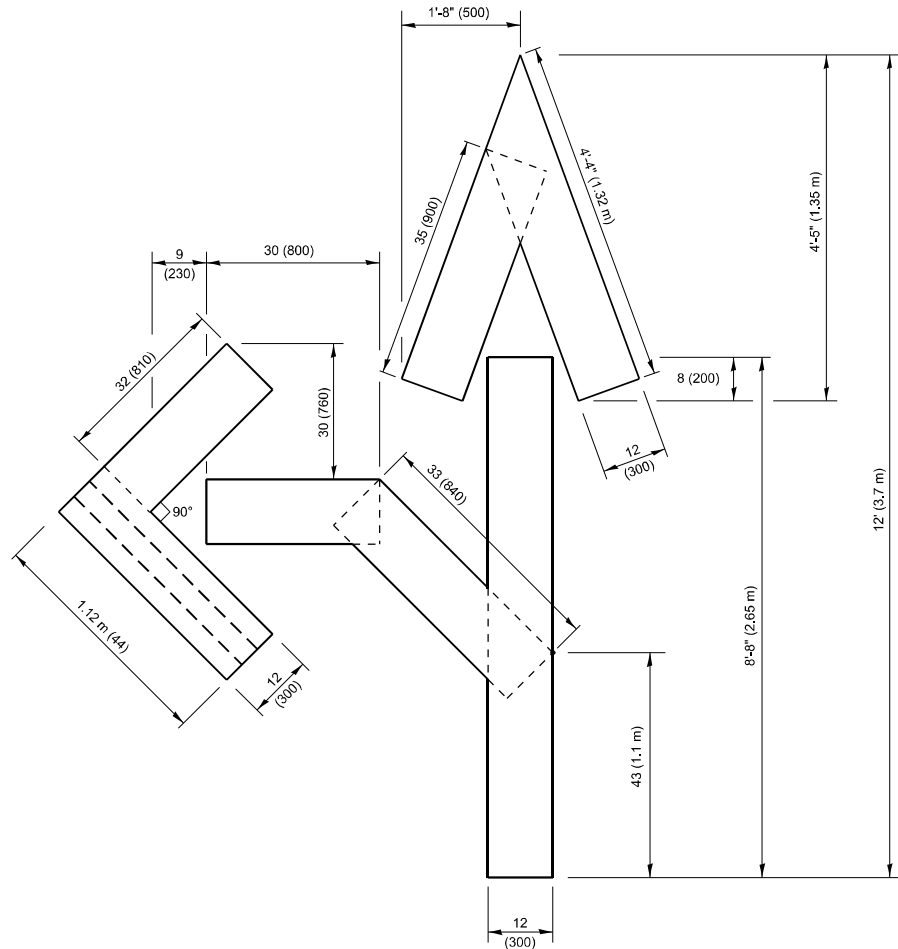
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

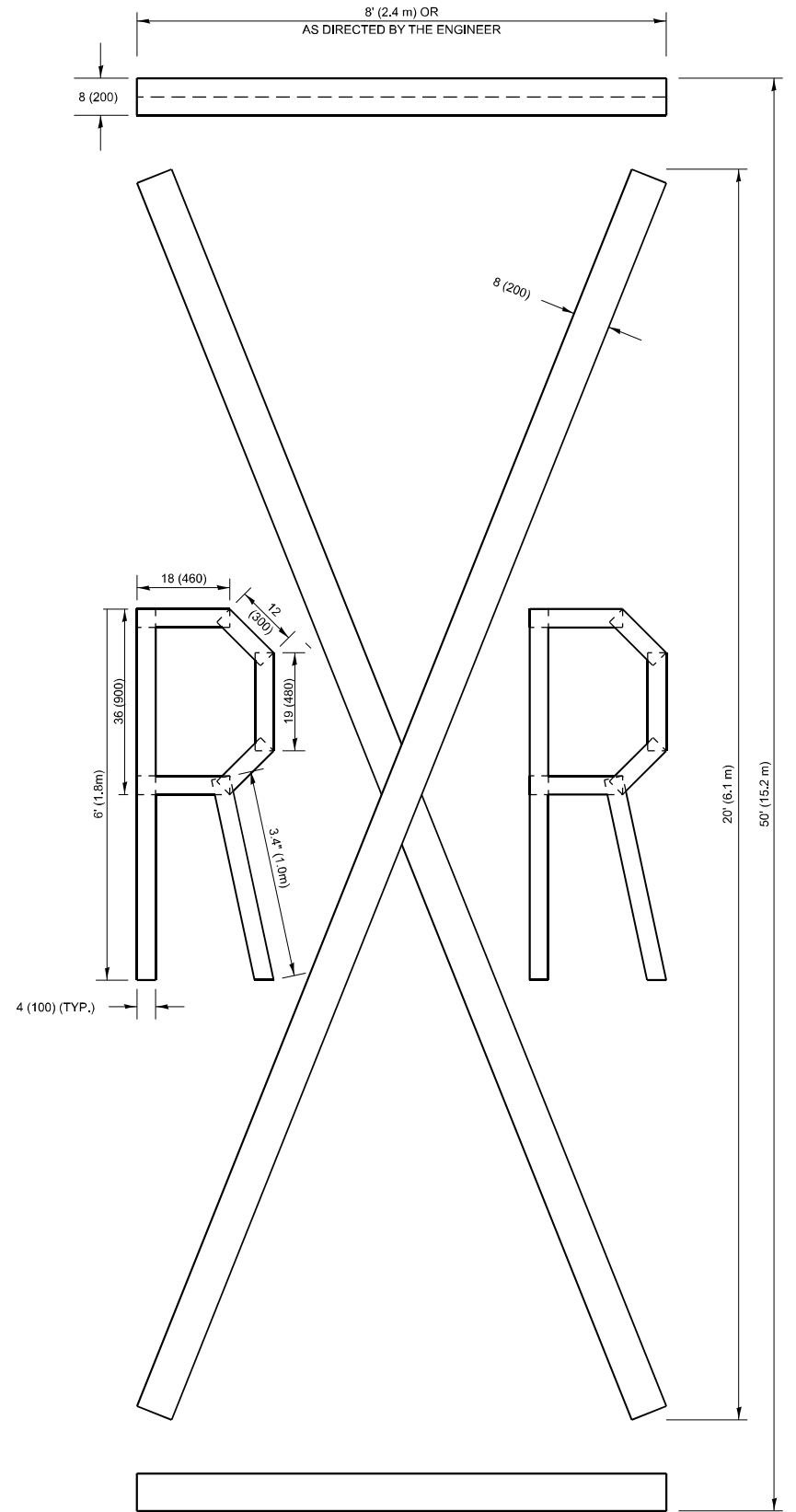


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

MODEL: TC-16 (Sheet)
FILE NAME: c:\pav_work\pav\illinois.gov\1095500D\102024-sh-TrafficStds.dgn

PLOT DATE = 10/17/2025	USER NAME = justin.king	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
		DRAWN -	REVISED - E. GOMEZ 08-28-00
		CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

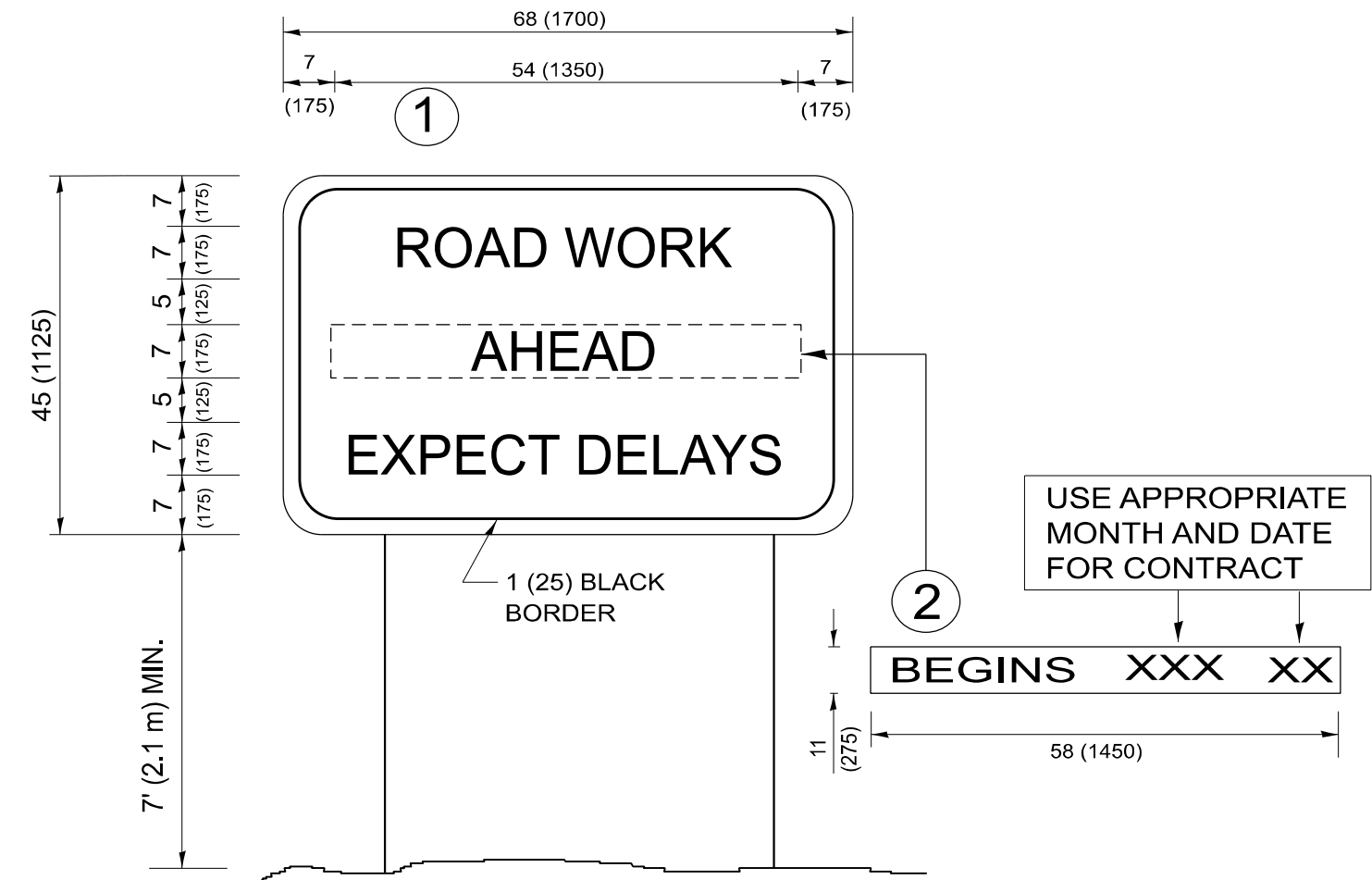
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	34
TC-16		CONTRACT NO. 62V42		
		ILLINOIS	FED. AID PROJECT	

FAU 1332

MODEL: TC-22 [Sheet]
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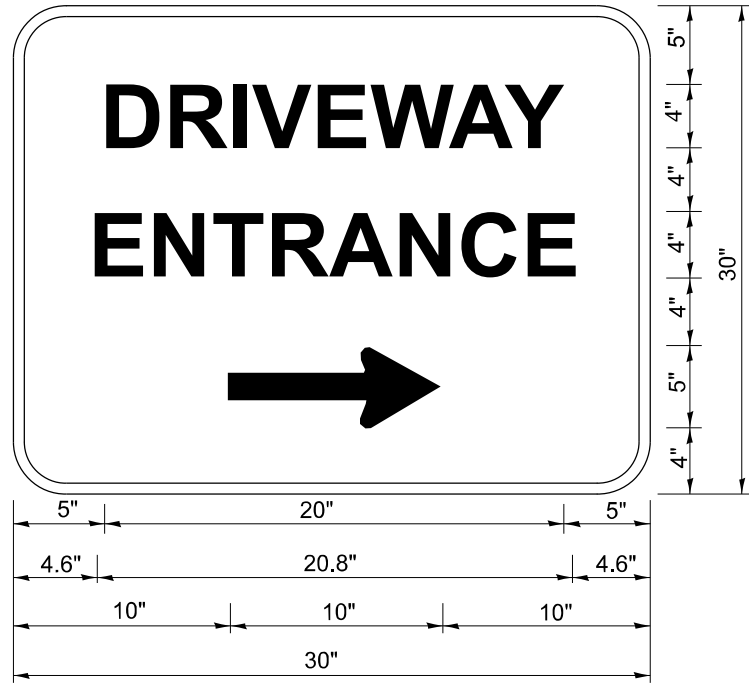


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = justin.king	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN					F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97							348	FAP 0348/1332 23 RES	COOK	43	35
		CHECKED -	REVISED - T. RAMMACHER 02-02-99							TC-22		CONTRACT NO. 62V42		
	PLOT DATE = 10/17/2025	DATE -	REVISED - C. JUCIUS 01-31-07		ILLINOIS FED. AID PROJECT									
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

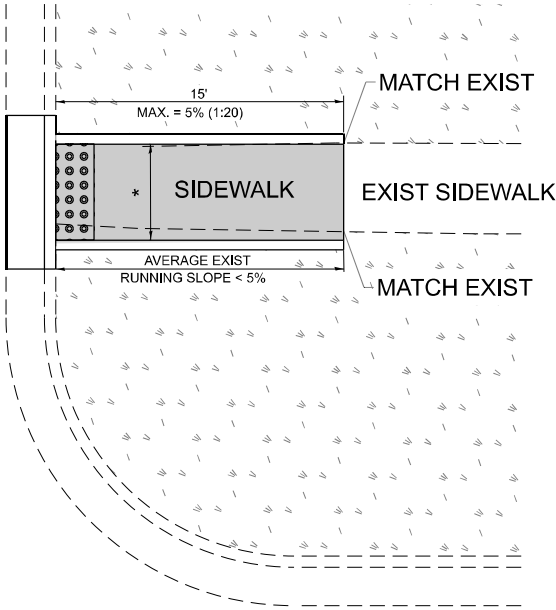
- NOTES:**
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26 (Sheet)
FILE NAME: c:\p\work\pwrtd\illinois.gov\justin.king@illinois.gov\1095500D102024-shh-DistStd.dgn

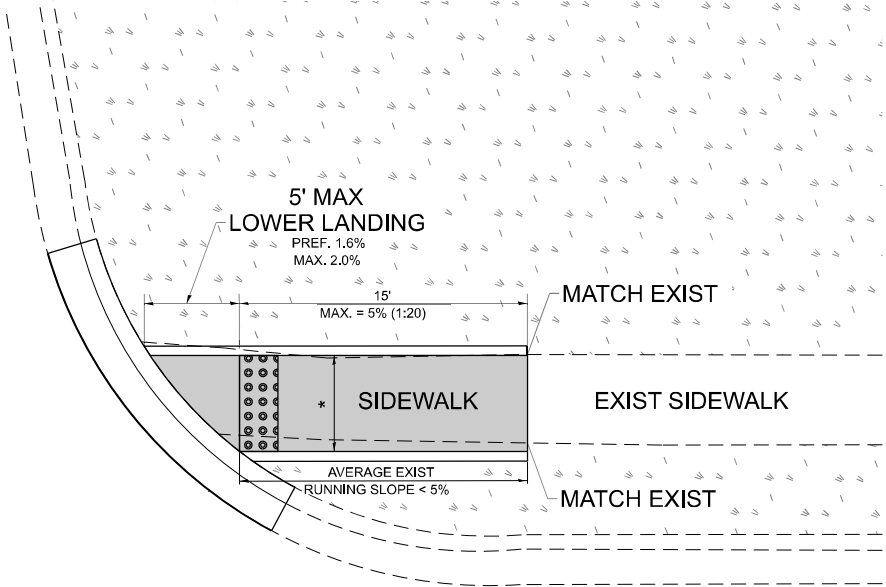
	USER NAME = justin.king	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					348	FAP 0348/1332 23 RES	COOK	43	36
		CHECKED -	REVISED -		TC-26			CONTRACT NO. 62V42				
	PLOT DATE = 10/17/2025	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

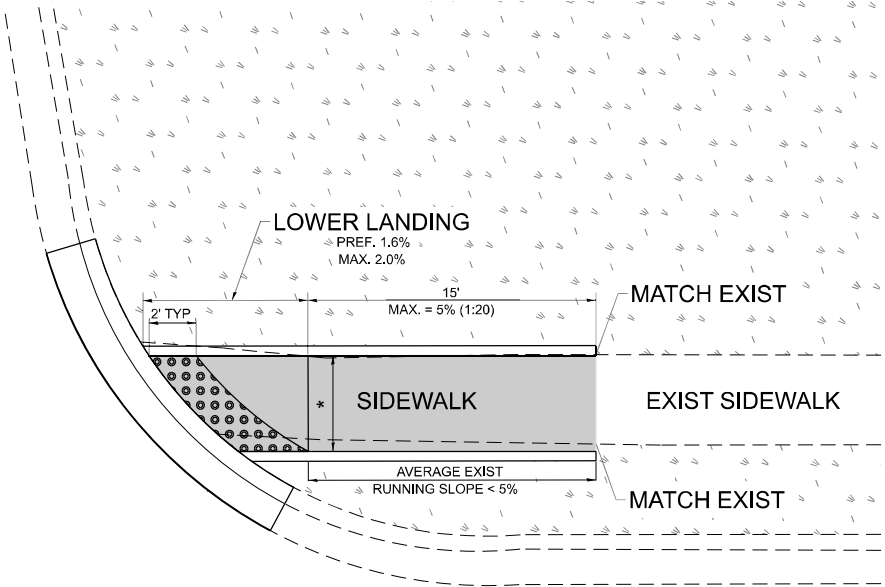
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-01 [Sheet]
FILE NAME: c:\pwwork\pwwork\illinois.gov\1095500D\102024-sh-DisStd.dgn

USER NAME = justin.king	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/02/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

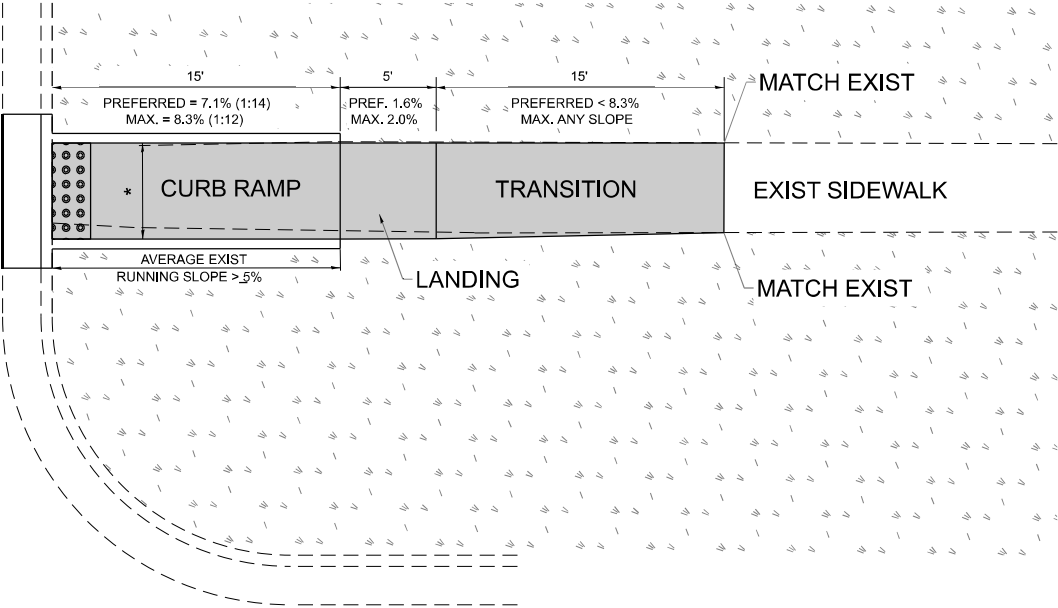
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

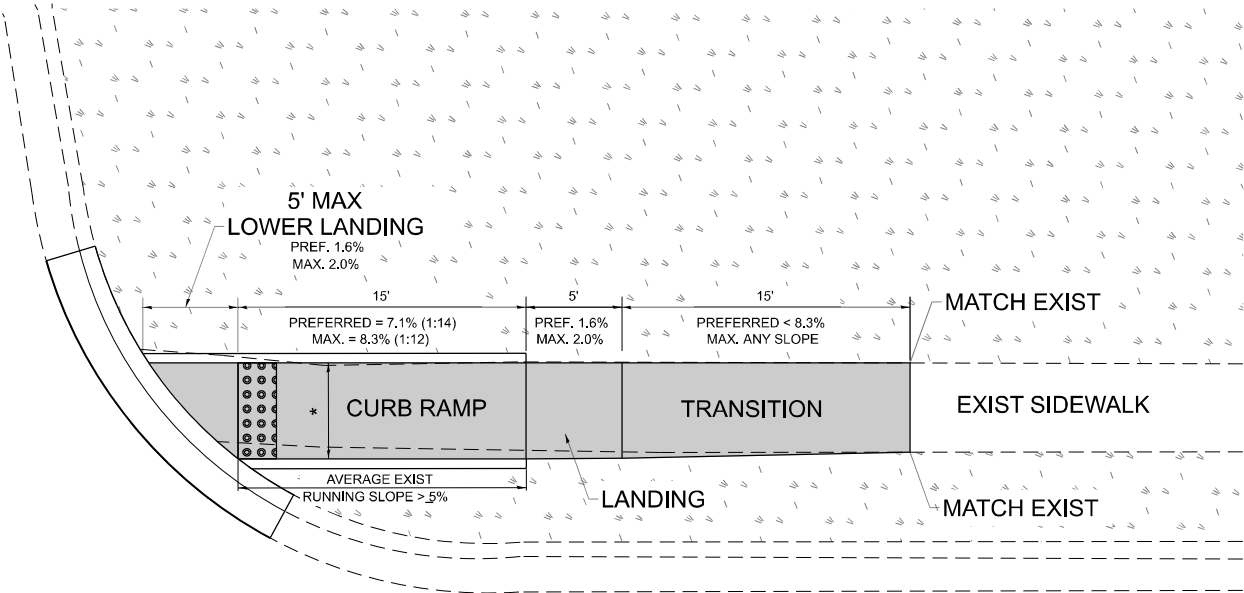
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	37
PD-01		CONTRACT NO. 62V42		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE

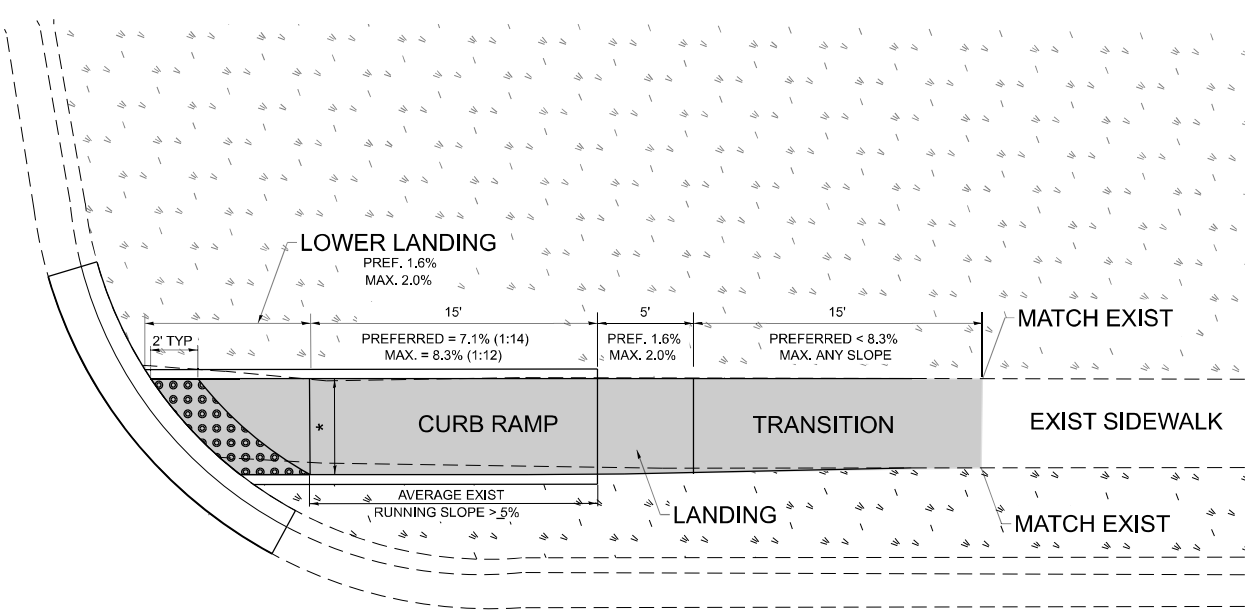
PD-02A



PD-02B



PD-02C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-02 [Sheet]
FILE NAME: c:\pawork\pawork\illinois.gov\1095500D\102024-sh-DistStd.dgn

	USER NAME	= justin.king	DESIGNED	-	REVISED	-
	DRAWN	-	R. LEDEZMA	REVISED	-	
	CHECKED	-		REVISED	-	
	PLOT DATE	= 10/17/2025	DATE	= 10/02/2019	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

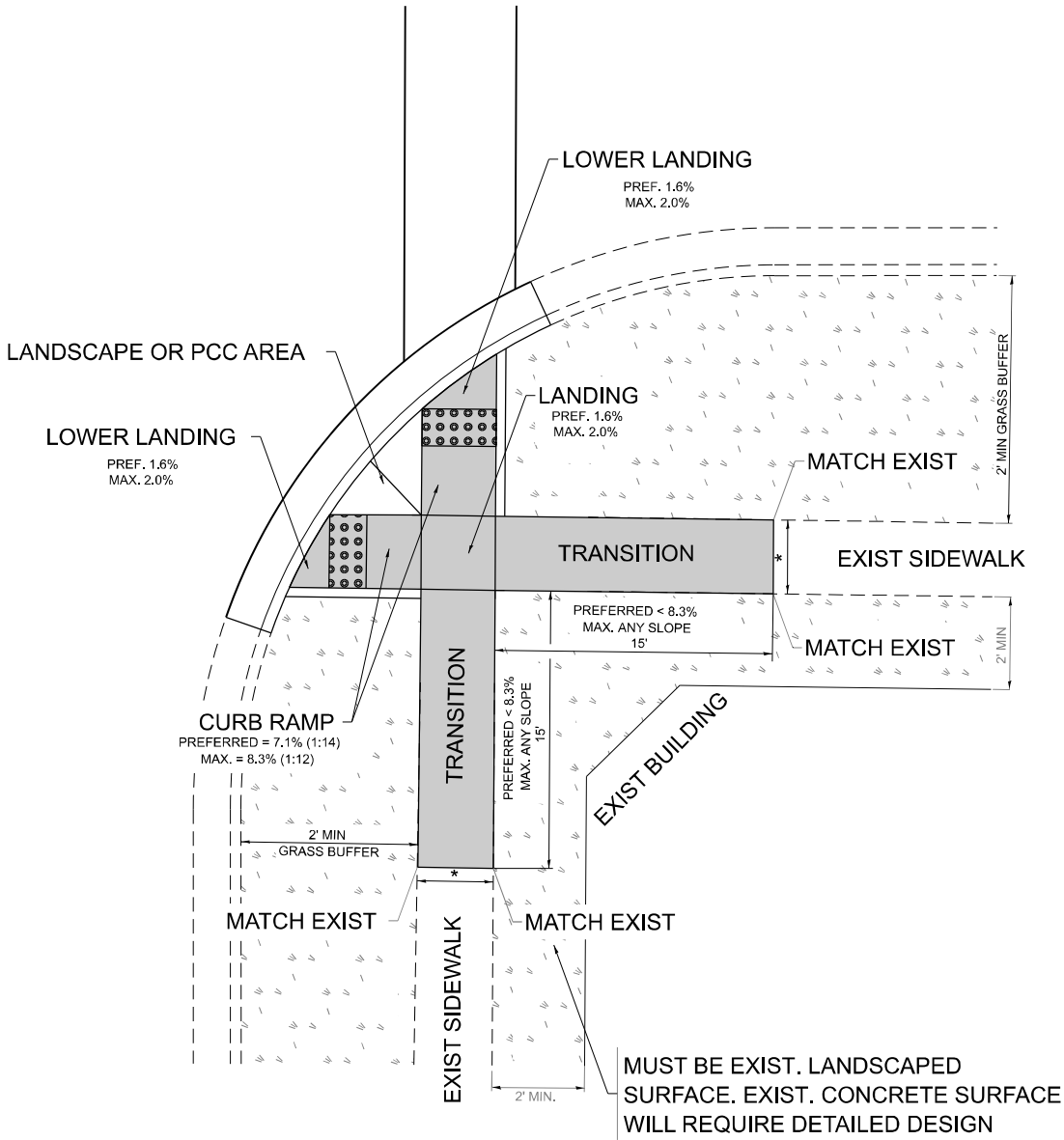
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-02)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

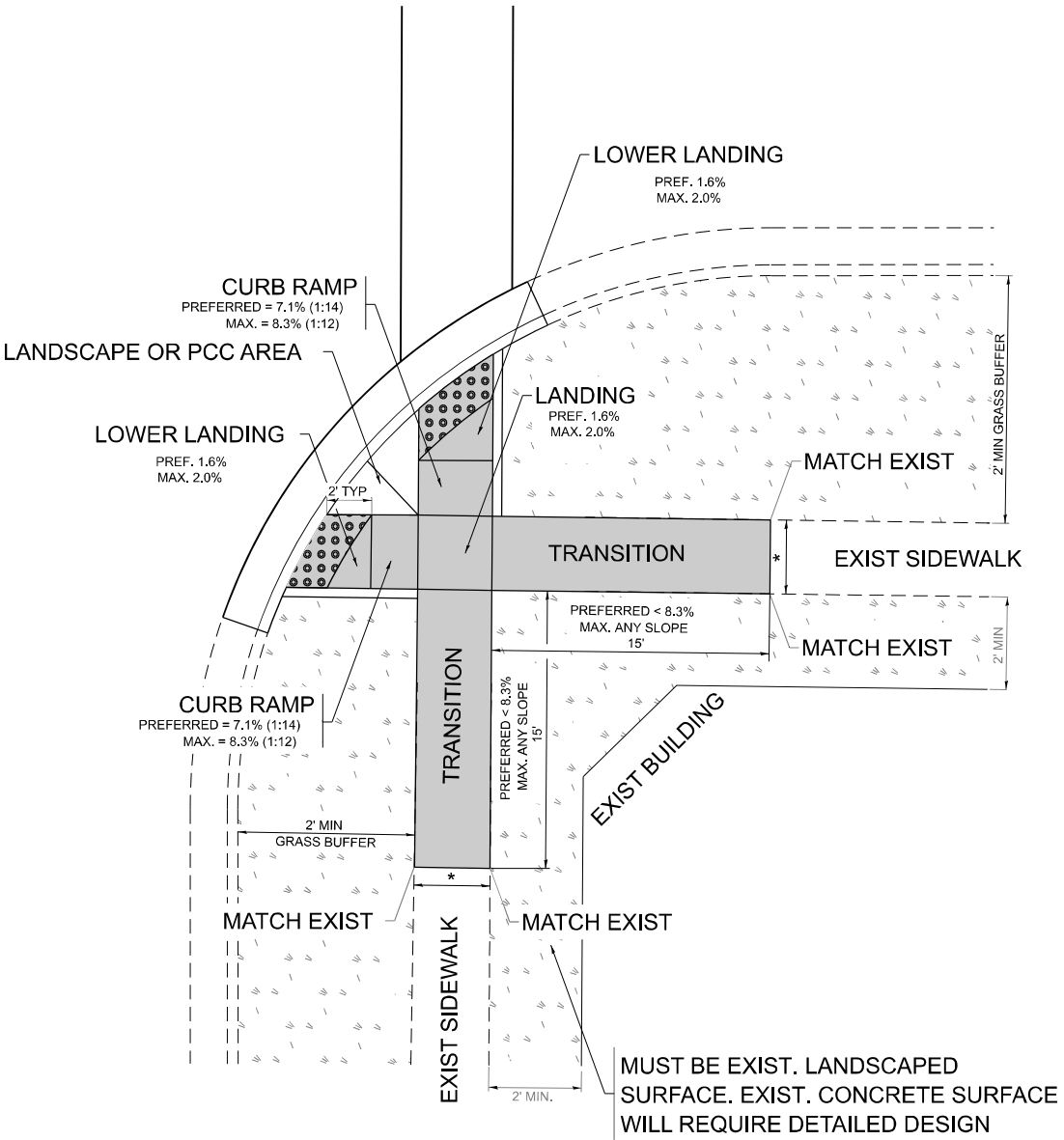
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	38
PD-02		CONTRACT NO. 62V42		
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

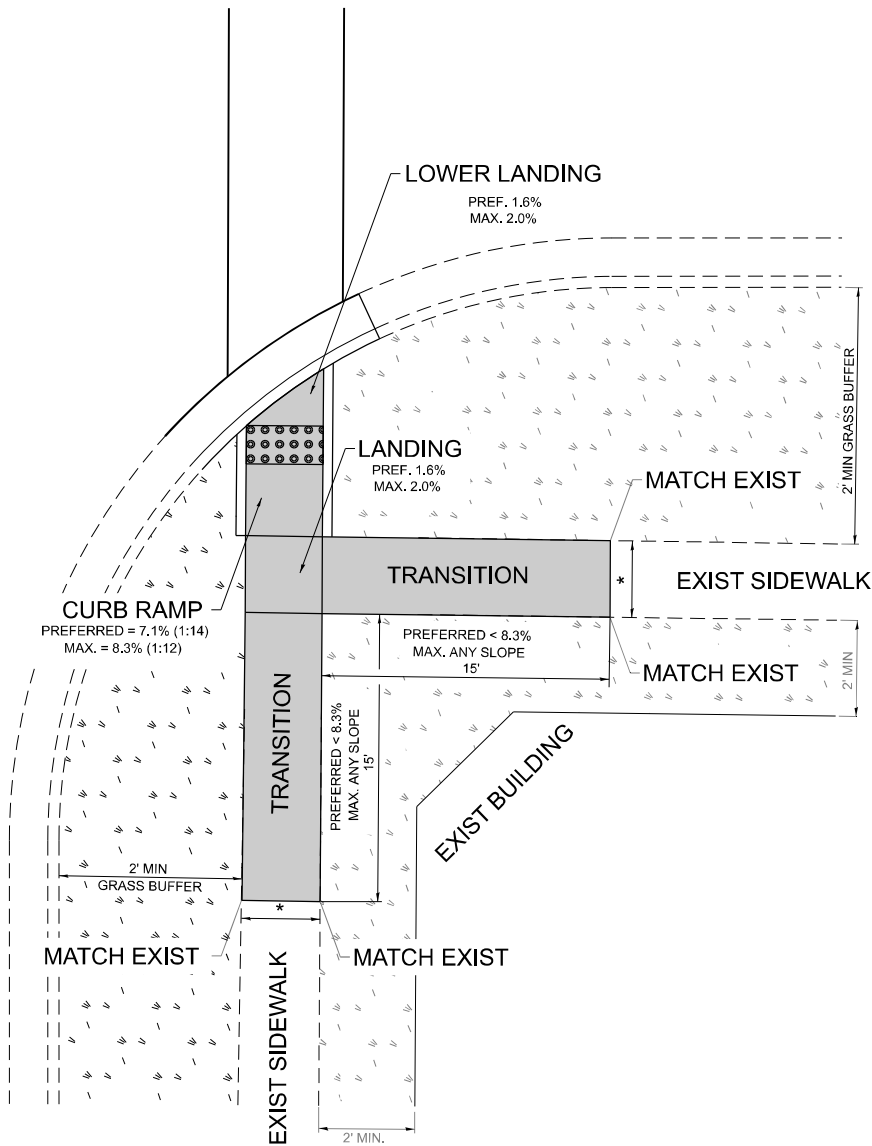
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	39
PD-03		CONTRACT NO. 62V42		
		ILLINOIS	FED. AID PROJECT	

FAU 1332

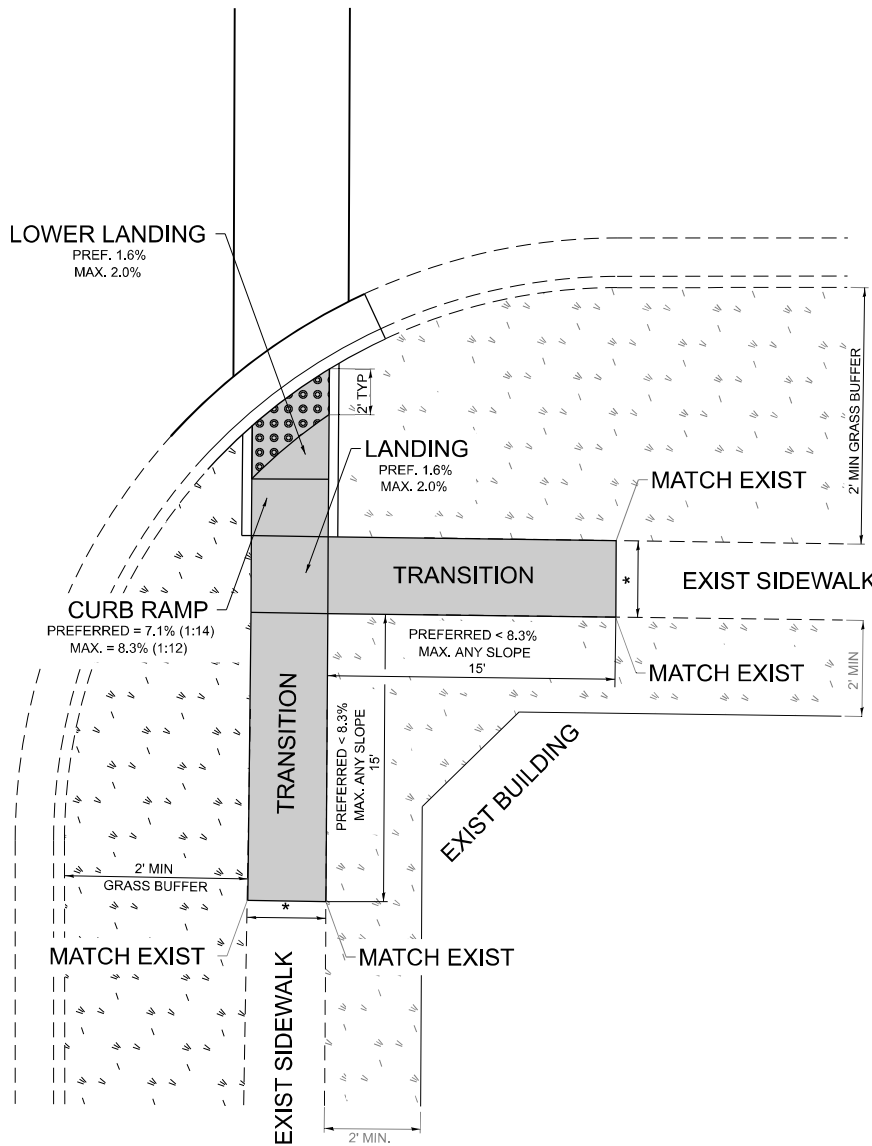
MODEL: PD-03 [Sheet]
FILE NAME: c:\pawork\pawork\illinois.gov\justin.king@illinois.gov\1095500D102024-sh-DistSids.dgn

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

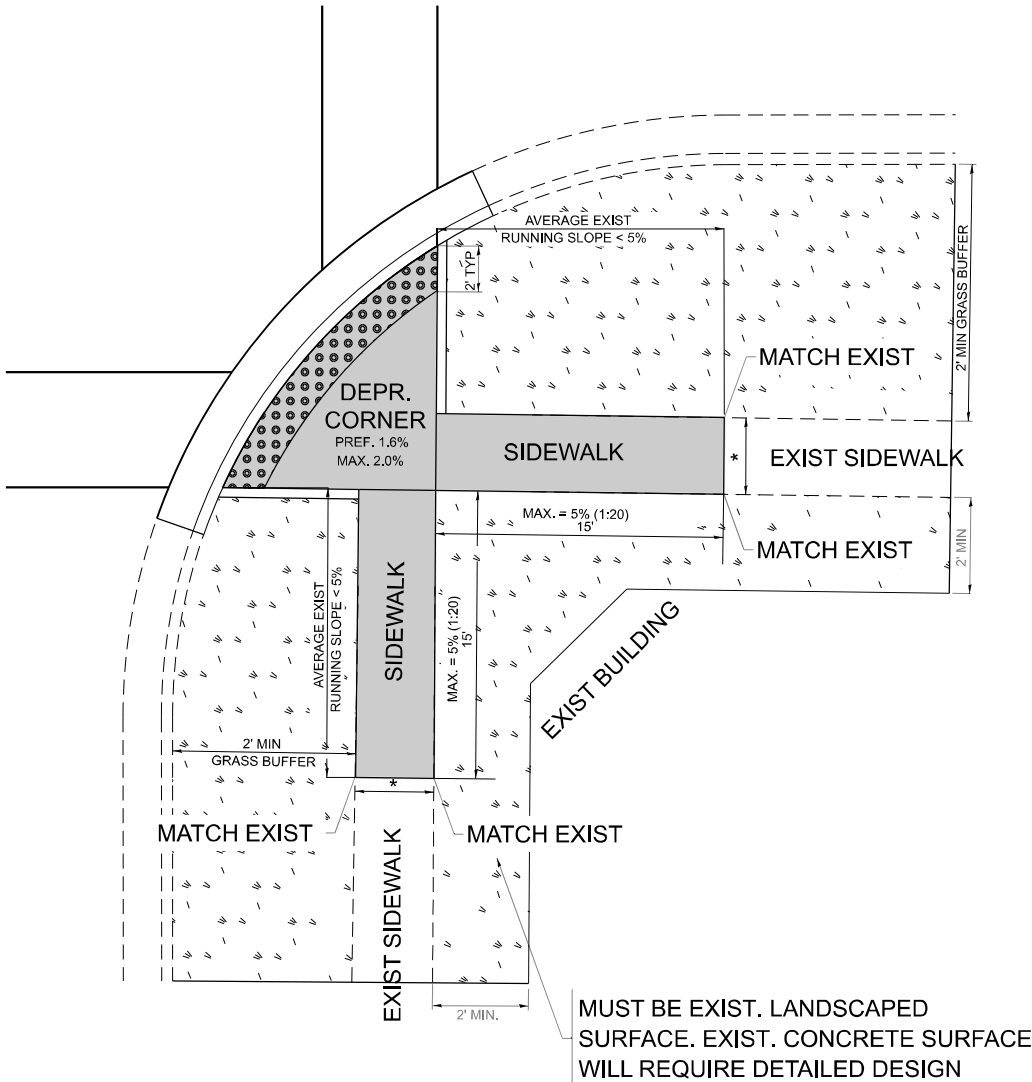
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348	FAP 0348/1332 23 RES	COOK	43	40
PD-04		CONTRACT NO. 62V42		
		ILLINOIS	FED. AID PROJECT	

FAU 1332

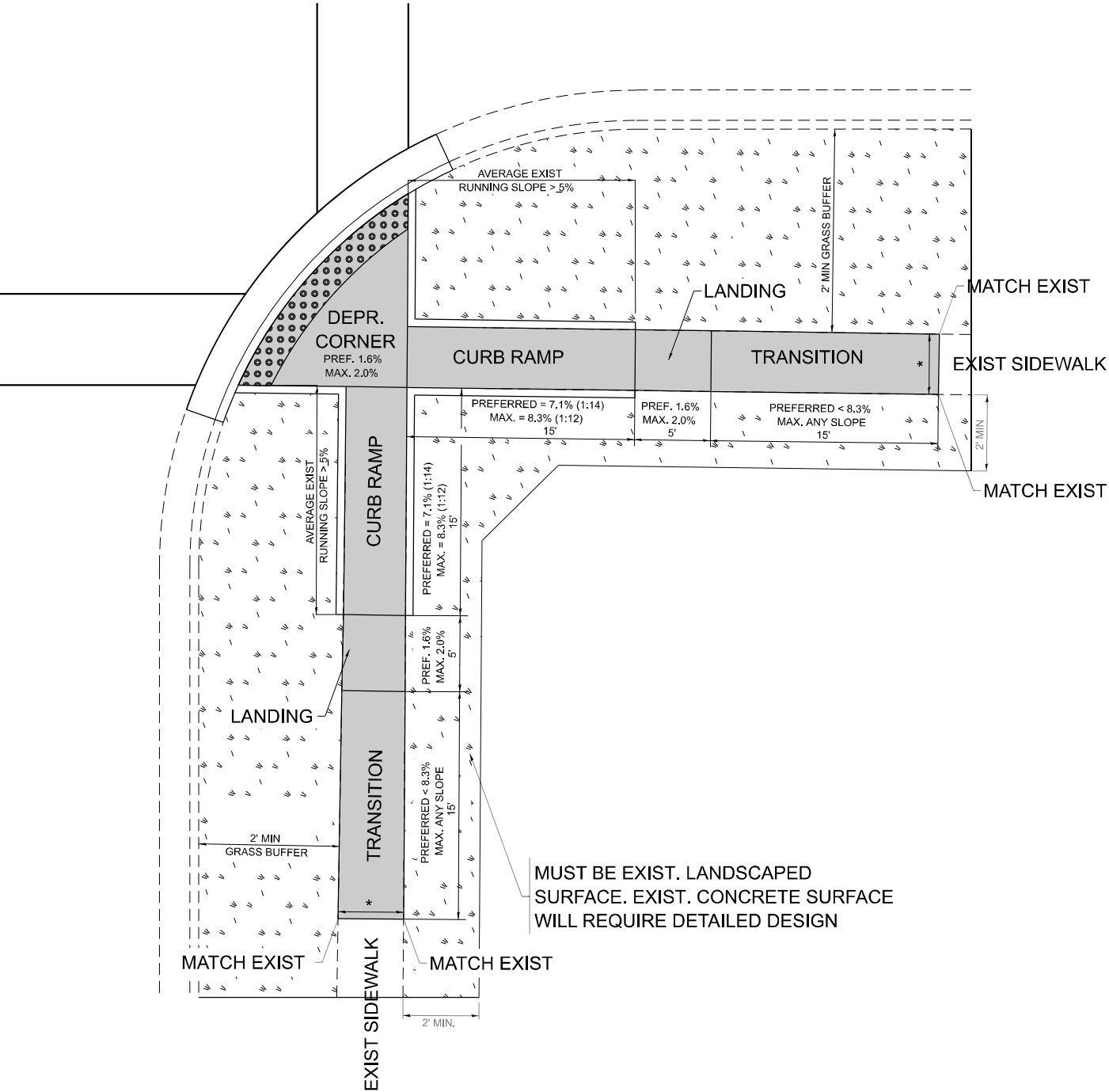
MODEL: PD-04 [Sheet]
FILE NAME: c:\pawork\pawork\illinois.gov\justin.king@illinois.gov\1095500D102024-sh-DistStd.dgn

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

PROPOSED SIDE CURB



EXIST. GRASS



PROPOSED SIDEWALK



DETECTABLE WARNINGS

CONSTRUCTION NOTES:

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

* MATCH EXISTING SIDEWALK WIDTH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)

SCALE: NONE 1 1 TO STA. SHEET OF SHEETS

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332-33 RES	COOK	43	41
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				

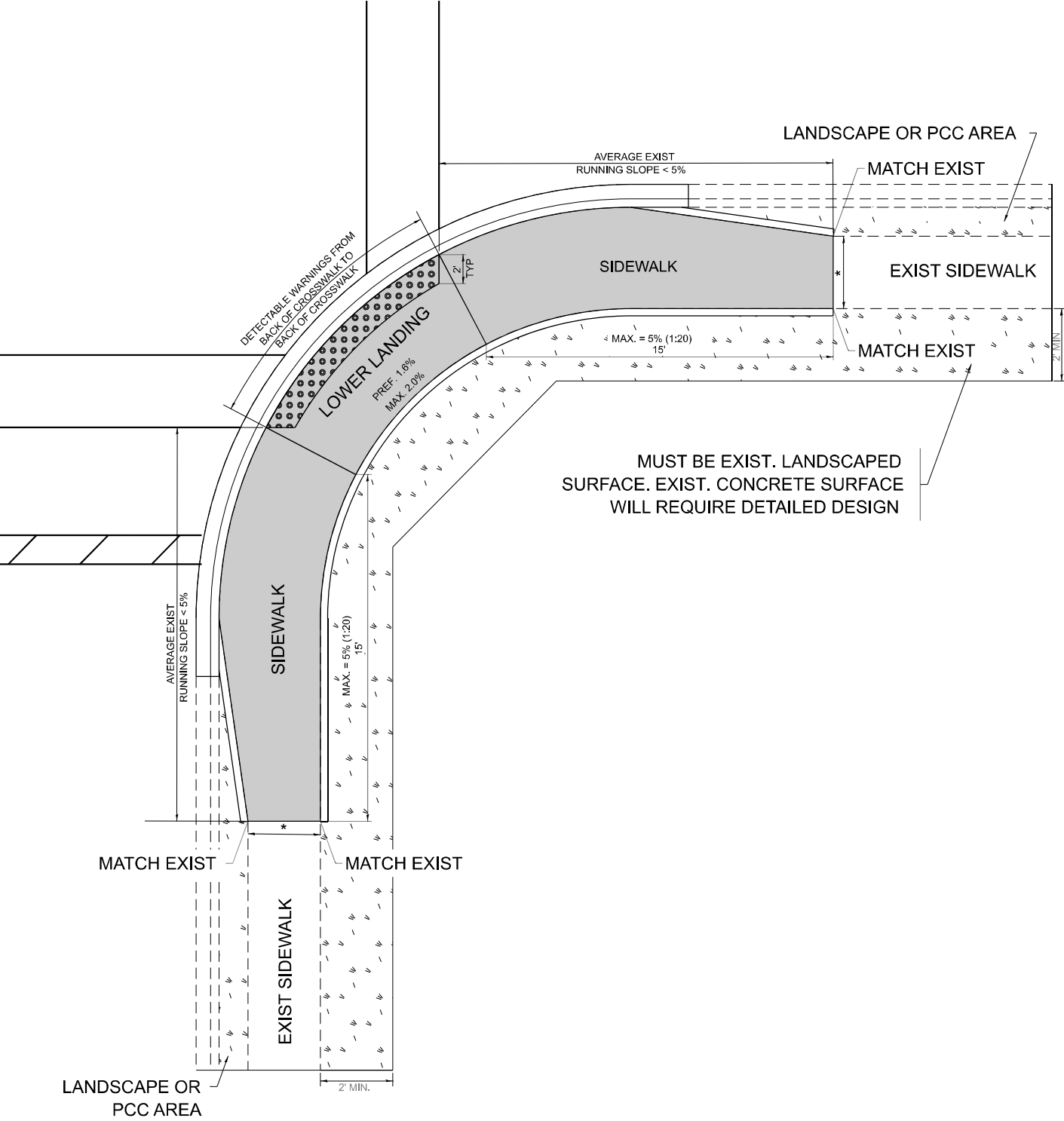
FAU 1332

MODEL: PD-05 (Sheet)
FILE NAME: c:\paw\work\paw\illinois.gov\justin.king@illinois.gov\1095500D\102024-sh-DistSids.dgn

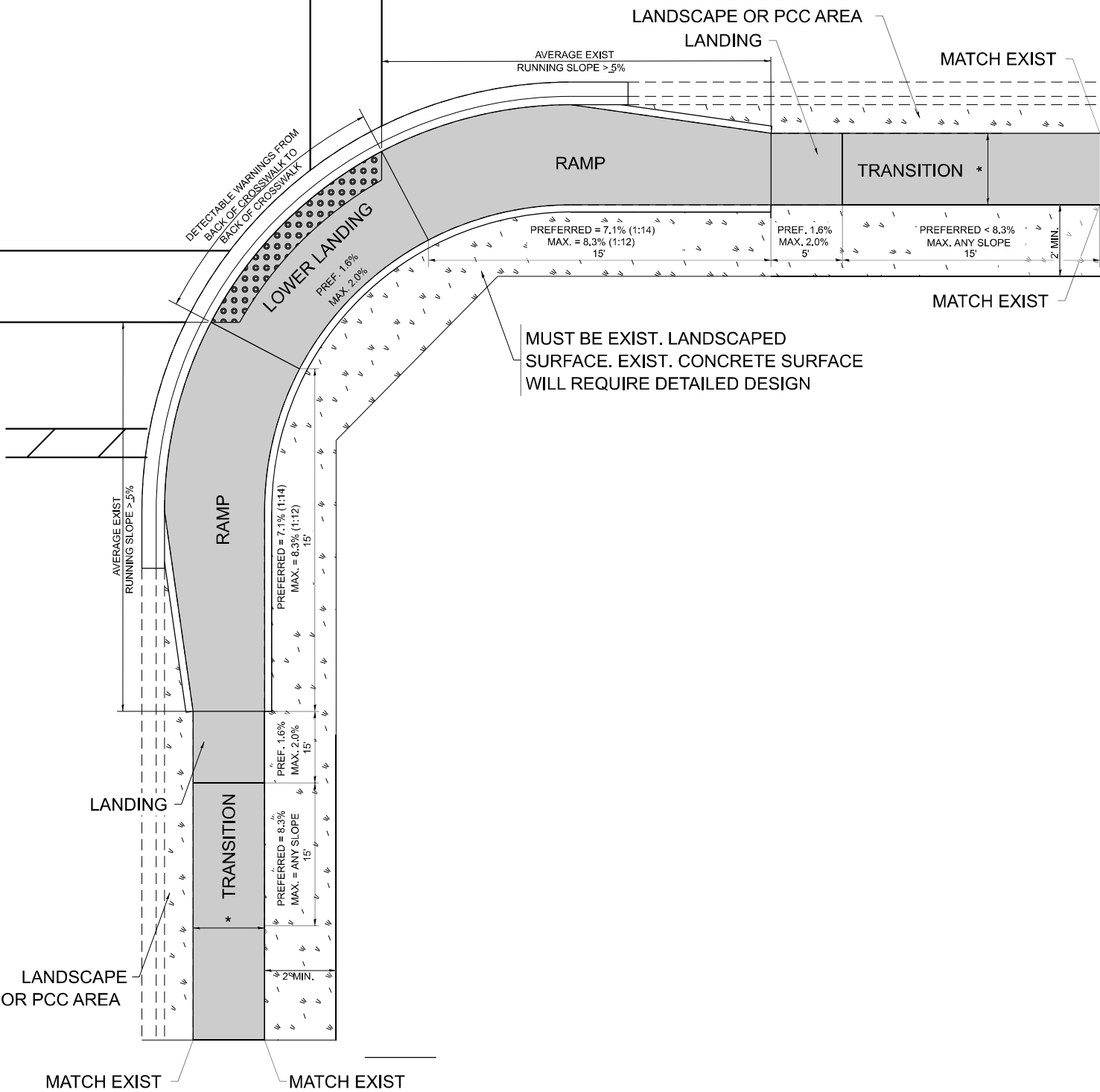
USER NAME = justin.king	DESIGNED - R. LEDEZMA	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/17/2025	DATE -	REVISED -

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

PROPOSED SIDE CURB

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-06 (Sheet)
FILE NAME: c:\paw\work\illinois.gov\1095500D\102024-sh-DistSds.dgn

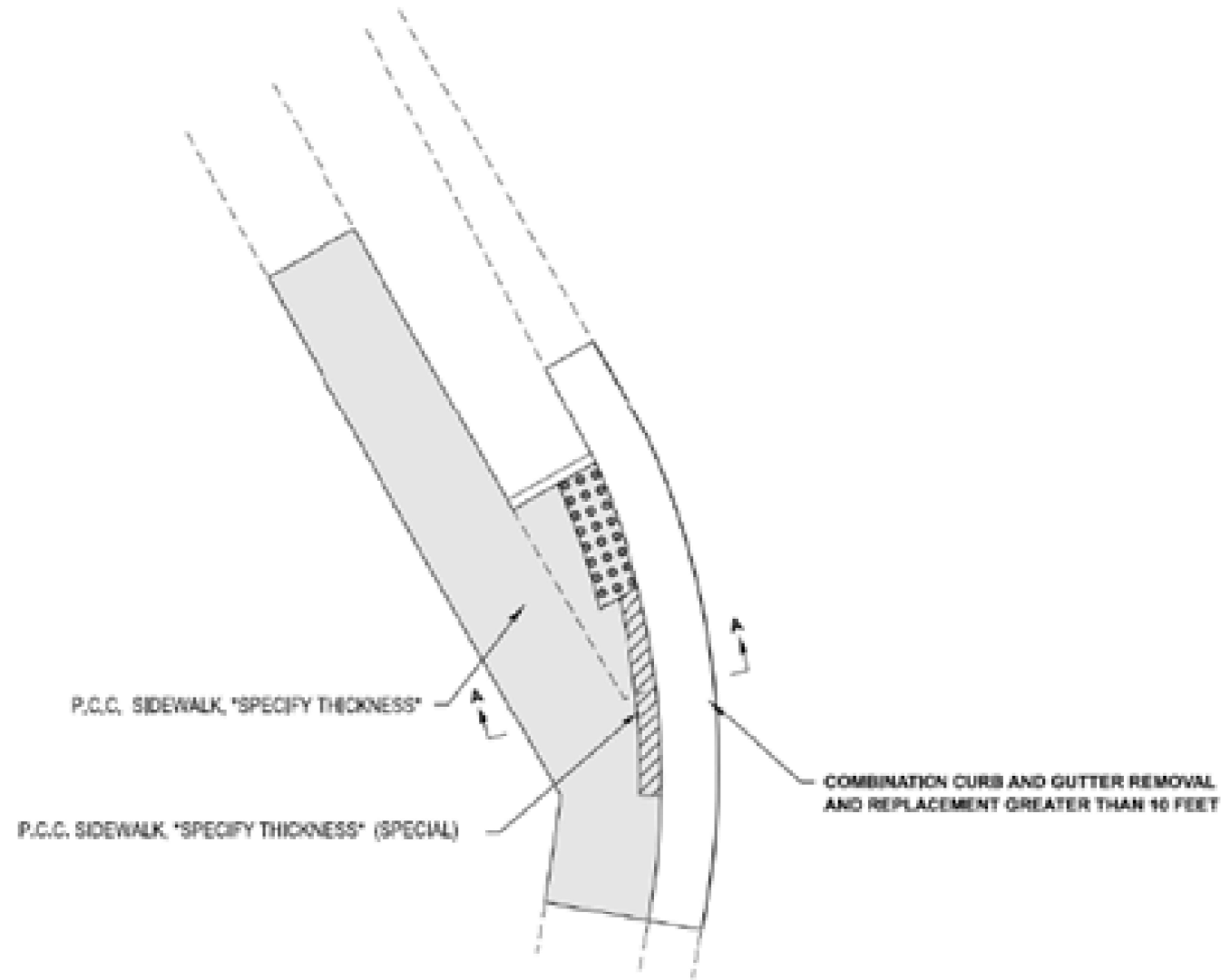
USER NAME	= justin.king	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 10/17/2025	DATE	= 10/02/2019	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

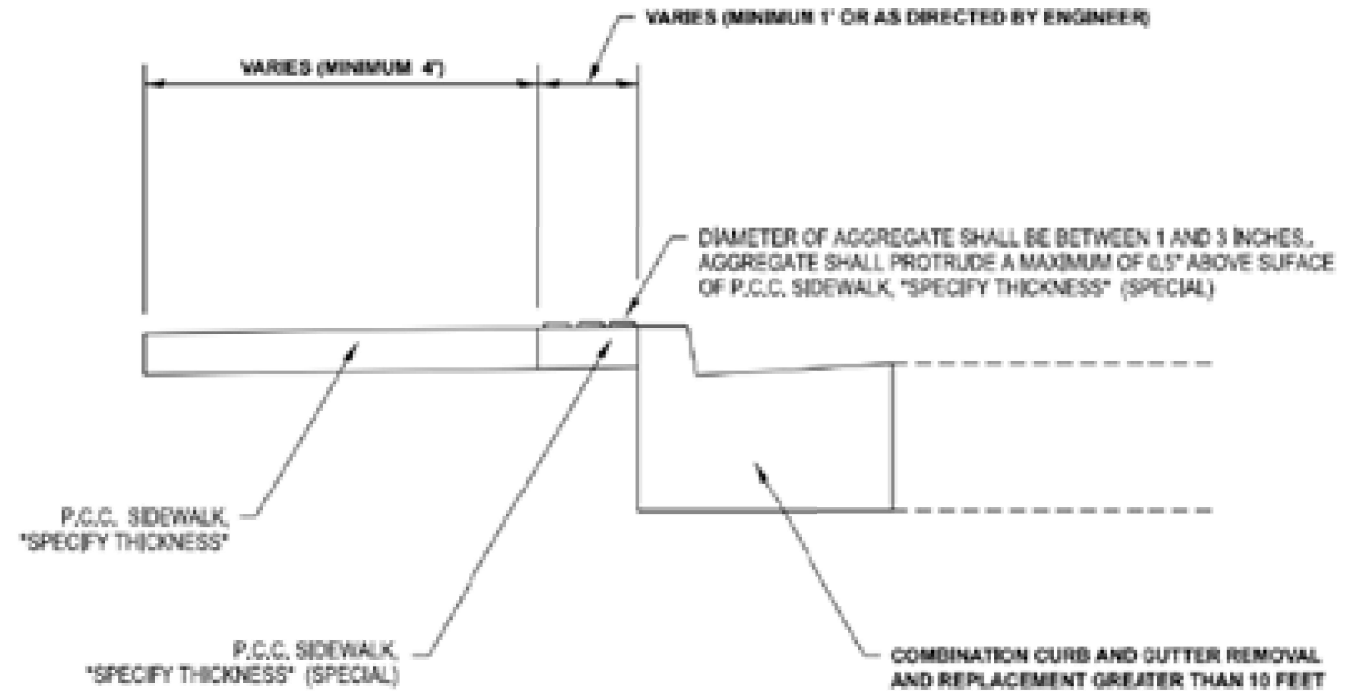
PROJECT DETAIL FOR PARALLEL CURB RAMPS
(PD-06)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	42
PD-06		CONTRACT NO. 62V42		
		ILLINOIS FED. AID PROJECT		



TYPICAL PLAN FOR PORTLAND CEMENT CONCRETE SIDEWALK, SPECIAL (THICKNESS SPECIFIED)



SECTION A-A

MODEL: PCC SIDEWALK SPECIAL [Sheet]
FILE NAME: c:\pawork\pawork\illinois.gov\justin.king@illinois.gov\102024-shl-DistSds.dgn

USER NAME	= justin.king	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 10/17/2025	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL FOR PCC SIDEWALK 5 OR 8 INCH (SPECIAL) NON-PREPARED SURFACE			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	FAP 0348/1332 23 RES	COOK	43	43
CONTRACT NO. 62V42				
ILLINOIS FED. AID PROJECT				