

FOR INDEX OF SHEETS, SEE SHEET NO. 2

01-16-2026 LETTING ITEM 194

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED  
HIGHWAY PLANS

PROJECT IS LOCATED IN THE  
CITY OF JOLIET

TRAFFIC DATA

CATON FARM ROAD:  
FUNCTION CLASS = MINOR ARTERIAL  
ADT (2023) = 18,100  
POSTED SPEED LIMIT = 35 MPH

FAI 55:  
FUNCTION CLASS = INTERSTATE  
ADT (2023) = 78,800  
POSTED SPEED LIMIT = 55 MPH

FAI 55 (I-55) AT CATON FARM ROAD  
(3 MILES N. OF US 52)  
SECTION FAI 55 22 BRIDGE REHAB  
PROJECT NO: NHPP-JM31(164)  
BRIDGE DECK OVERLAY AND JOINT REPAIR  
WILL COUNTY

  
ENGINEER DATE 8-6-25

HOWARD J. HAMILTON, PE, CFM, CPESC SHEETS 1-13  
ILLINOIS LICENSE NO. 062-047406  
EXPIRES 11/30/2025

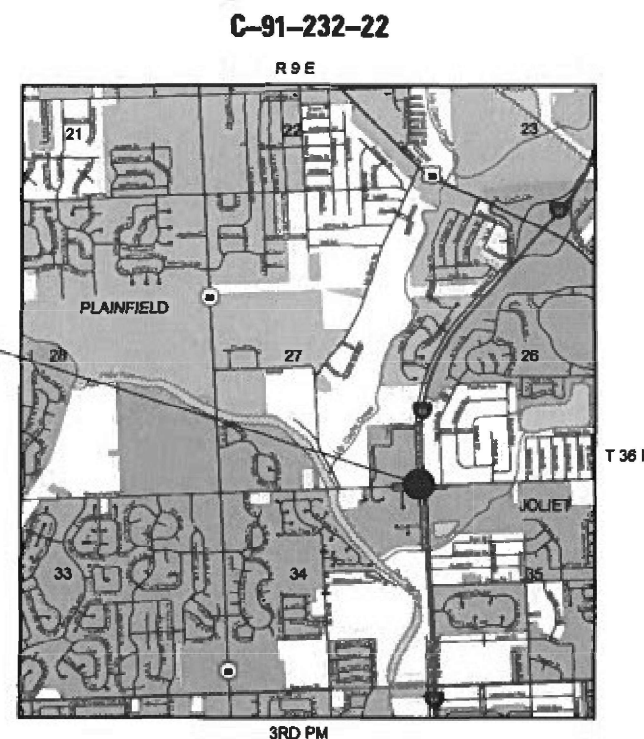


  
STRUCTURAL ENGINEER DATE 08/06/2025

MAJLINDA AGOJCI P.E., S.E. SHEETS 14-22  
ILLINOIS LICENSE NO. 081-006911  
EXPIRES 11/30/2026



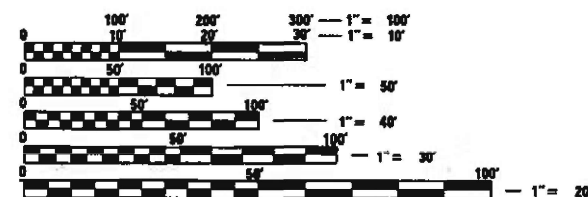
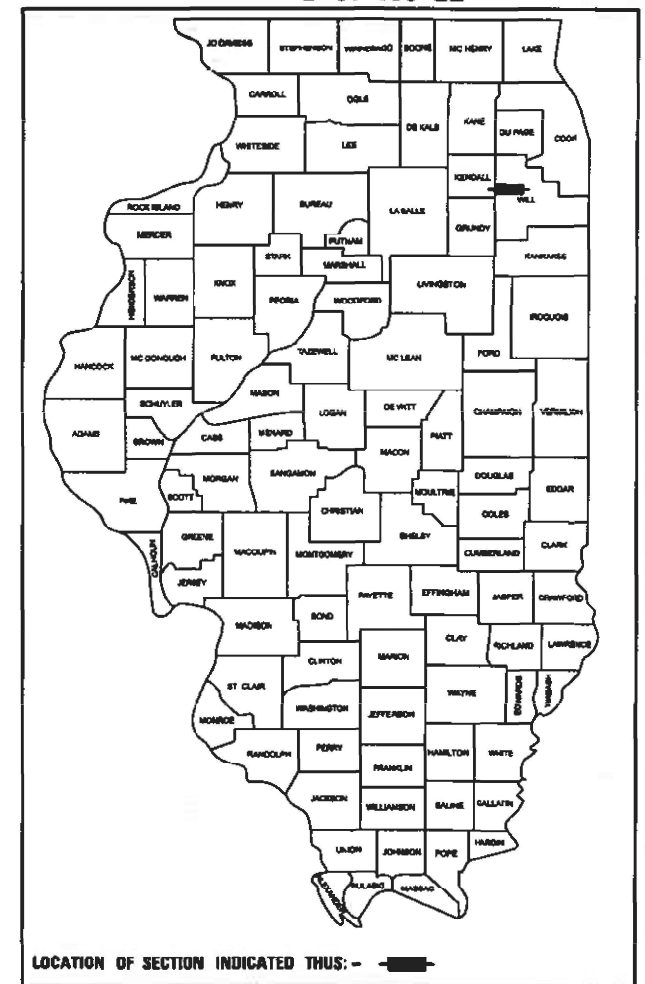
EXISTING STRUCTURE LOCATION  
SN 099-0210 4-SPAN MULTI-BEAM  
OVER I-55 WITH SPILL THROUGH  
ABUTS AND CONCRETE PIERS.  
BACK TO BACK LENGTH = 189'-0"  
OUT TO OUT WIDTH = 51'-0"



N  
NOT TO SCALE

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	1
ILLINOIS		CONTRACT NO. 62R76		

D-91-186-22



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT MANAGER, PRAVEEN KAINI, PE (847) 705-4237

CONTRACT NO. 62R76

GROSS LENGTH = 290.00 FT. = 0.055 MILE  
NET LENGTH = 290.00 FT. = 0.055 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED Aug 11<sup>th</sup> 2025 1R  
Joe A. Jones REGIONAL ENGINEER

October 3<sup>rd</sup> 2025  
Joe A. Jones ENGINEER OF DESIGN AND ENVIRONMENT

October 3<sup>rd</sup> 2025  
Joe A. Jones DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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000001-09	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
606001-09	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-13	STEEL PLATE BEAM GUARDRAIL
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701106-02	OFF ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701331-05	LANE CLOSURE, 2L, 2W, WITH RUNAROUND FOR SPEEDS $\geq 45$
701400-12	APPROACH TO LANE CLOSURE FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS $\geq 45$
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS $\leq 40$
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701901-11	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
HMA OVERLAY AND BUTT JOINT		
HOT MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1 1/2"	4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA), QUALITY CONTROL FOR PERFORMANCE (QCP), PAY FOR PERFORMANCE (PFP)		

- MODEL: General notes [Sheet]  
FILE NAME: L:\1980 Orton Engineers LLC\1980,005 PTB 199-013\Civil\03 CAD\01 Model\catonfam62R76 wo1\1D162R76-shit-gennote.dgn

## REV-SEP

MODEL: Default  
FILE NAME: L:\1980 Orion Engineers LLC\1980.005 PTB 199-013\Civil\03 CAD\01 Model\catonfarm2R76.wor11D162R76-shl-SQO.dgn

SUMMARY OF QUANTITIES				CONSTR. CODE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	90% FEDERAL 10% STATE
				BRIDGE
				0059
				SN 099-0210
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	212	212
40600982	HOT-MIX ASPHALT REMOVAL-BUTT JOINT	SQ YD	306	306
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	54	54
50102400	CONCRETE REMOVAL	CU YD	15	15
50157300	PROTECTIVE SHIELD	SQ YD	162	162
50300255	CONCRETE SUPERSTRUCTURE	CU YD	17	17
50300300	PROTECTIVE COAT	SQ YD	1115	1115
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2580	2580
50800515	BAR SPLICERS	EACH	24	24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	102	102
60260100	INLETS TO BE ADJUSTED	EACH	4	4
63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1	1
63200310	GUARDRAIL REMOVAL	FOOT	37.5	37.5
67100100	MOBILIZATION	L SUM	1	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	120	120

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33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603

Ph: (312) 345-1400 Fax: (312)345-0529   www.envdesignit.com

USER NAME	= mbuiting
PLOT DATE	= 6/25/2024

DESIGNED	-	MGB	REVISED	-
DRAWN	-	MGB	REVISED	-
CHECKED	-		REVISED	-
DATE	-		REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

I-55 AT CATON FARM ROAD  
SUMMARY OF QUANTITIES

SCALE: N.A.    SHEET 1    OF 3    SHEETS    STA.    TO STA.

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	3
CONTRACT NO. 62R76				
ILLINOIS FED. AID PROJECT				

MODEL: Default  
FILE NAME: L:\1980 Orion Engineers LLC\1980.005 PTB -199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-shit-SOO.dgn

				CONSTR. CODE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	90% FEDERAL 10% STATE
				BRIDGE
				0059
				SN 099-0210
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	3263	3263
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	9790	9790
70400100	TEMPORARY CONCRETE BARRIER	FOOT	614	614
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	614	614
70600241	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2
70600341	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3163	3163
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	35	35
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD LINE 7"	FOOT	48	48
* 78009004	MODIFIED URETHANE PAVEMENT MARKING, TYPE D - LINE 4"	FOOT	380	380
* 78009012	MODIFIED URETHANE PAVEMENT MARKING, TYPE D - LINE 12"	FOOT	35	35
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	48	48
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	84	84
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	32	32

\* SPECIALTY ITEM

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USER NAME	= mbuiting	DESIGNED	- MGB	REVISED	-
		DRAWN	- MGB	REVISED	-
		CHECKED	- HJH	REVISED	-
PLOT DATE	= 5/20/2024	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

I-55 AT CATON FARM ROAD  
SUMMARY OF QUANTITIES

SCALE: N.A.    SHEET 2    OF   3    SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL.	31	4
CONTRACT NO. 62R76				
ILLINOIS		FED. AID PROJECT		





MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE MOT PLANS.
3. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGNS SHALL BE REMOVED TWO WEEKS THEREAFTER UNLESS THEY ARE NEEDED IN A SUBSEQUENT FUTURE EVENT THAT WILL OCCUR WITHIN TWO WEEKS ON THE SAME APPROACH OF THE AFFECTED ROADWAY. CHANGEABLE MESSAGE SIGN LOCATION SHALL BE AS DIRECTED BY THE ENGINEER.
4. TYPE C BI-DIRECTIONAL CRYSTAL REFLECTORS INSTALLED ON EXISTING BRIDGE PARAPET SHALL BE REMOVED AT THE CONCLUSION OF MOT OPERATIONS AT NO ADDITIONAL COST TO THE DEPARTMENT.
5. ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF STAGING.
6. TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, FIXED OBJECTS, OR DROP-OFFS ARE LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL BE UNPINNED.

SUGGESTED MAINTENANCE OF TRAFFIC SEQUENCING - CATON FARM RD

STAGE 1

- REMOVE PERMANENT PAVEMENT MARKINGS TO THE LIMITS SHOWN IN THE PLANS AND THE SCHEDULE OF PAVEMENT MARKING REMOVAL.
- INSTALL ALL STAGE 1 CONSTRUCTION TRAFFIC CONTROL SIGNS, DRUMS, BARRICADES, TEMPORARY CONCRETE BARRIER, TEMPORARY IMPACT ATTENUATORS, AND TEMPORARY PAVEMENT MARKINGS AS DETAILED IN STAGE 1 PLANS AND APPROPRIATE STANDARD DRAWINGS.
- SHIFT ALL TRAFFIC TO THE NORTH OF THE CENTERLINE OF SN 099-0210.
- COORDINATE WORK TO BE COMPLETED IN THIS STAGE WITH DETAILS IN THE BRIDGE PLANS.

STAGE 2

- REMOVE STAGE 1 TEMPORARY PAVEMENT MARKINGS THAT WOULD CONFLICT WITH STAGE 2 TRAFFIC MOVEMENT.
- INSTALL ALL STAGE 2 CONSTRUCTION TRAFFIC CONTROL SIGNS, DRUMS, BARRICADES, TEMPORARY CONCRETE BARRIER, TEMPORARY IMPACT ATTENUATORS, AND TEMPORARY PAVEMENT MARKINGS AS DETAILED IN STAGE 1 PLANS AND APPROPRIATE STANDARD DRAWINGS.
- SHIFT ALL TRAFFIC TO THE SOUTH OF THE CENTERLINE OF SN 099-0210.
- COORDINATE WORK TO BE COMPLETED IN THIS STAGE WITH DETAILS IN THE BRIDGE PLANS.

POST STAGE

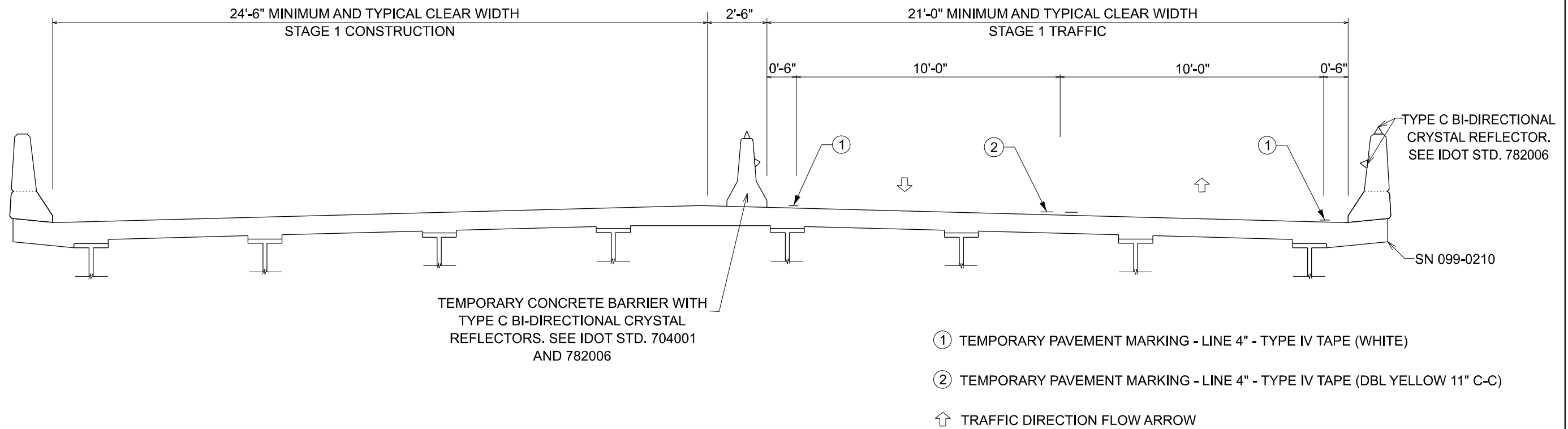
- REMOVE ALL CONSTRUCTION TRAFFIC CONTROL AND TEMPORARY CONSTRUCTION ITEMS.
- REMOVE ANY REMAINING EXISTING TEMPORARY PAVEMENT MARKINGS WITHIN THE PERMANENT PAVEMENT MARKING LIMITS.
- INSTALL PERMANENT PAVEMENT MARKINGS.

SUGGESTED MAINTENANCE OF TRAFFIC SEQUENCING - I-55

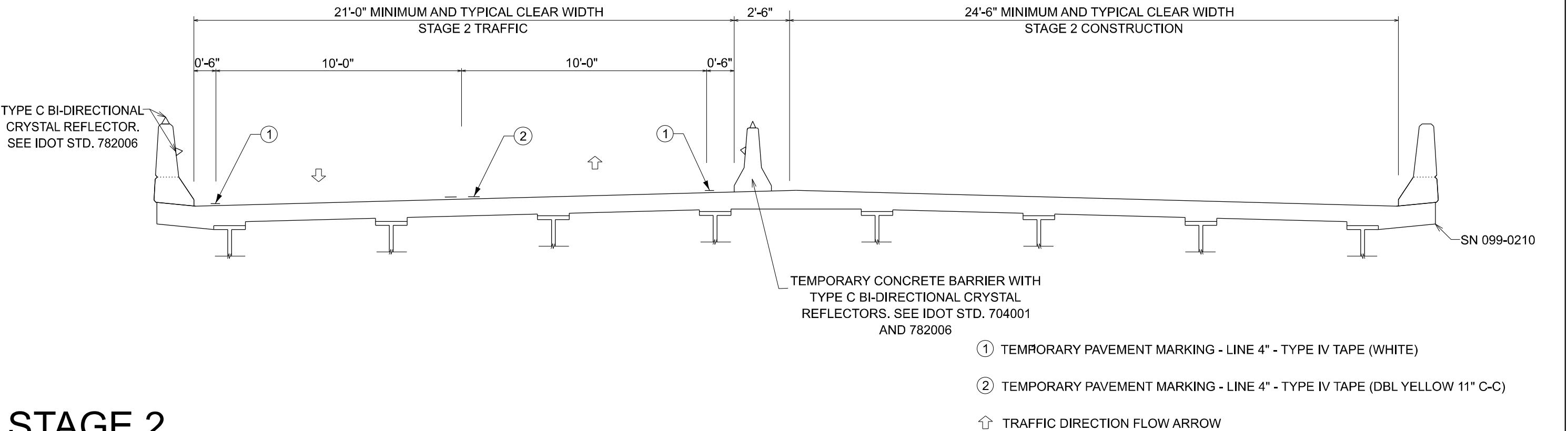
TEMPORARY SHOULDER/ LANE CLOSURES ARE REQUIRED TO PERFORM SUBSTRUCTURE REPAIRS. LANE CLOSURES ALONG I-55 SHALL BE IN ACCORDANCE WITH SPECIAL PROVISION "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)". LANE CLOSURES SHALL OCCUR ONLY DURING NIGHTTIME HOURS IN ACCORDANCE WITH SPECIAL PROVISION "KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC".

MODEL: MOT notes (Sheet)  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTE 199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11\1D162R76-shh-motnote.dgn

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		DRAWN   -   MGB	REVISED   -						55	FAI 55 22 BRIDGE REHAB	WILL	31	6
		CHECKED   -   HJH	REVISED   -						CONTRACT NO. 62R76				
	PLOT DATE    =   12/16/2024	DATE    -	REVISED   -		SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS   FED. AID PROJECT		



# STAGE 1



# STAGE 2

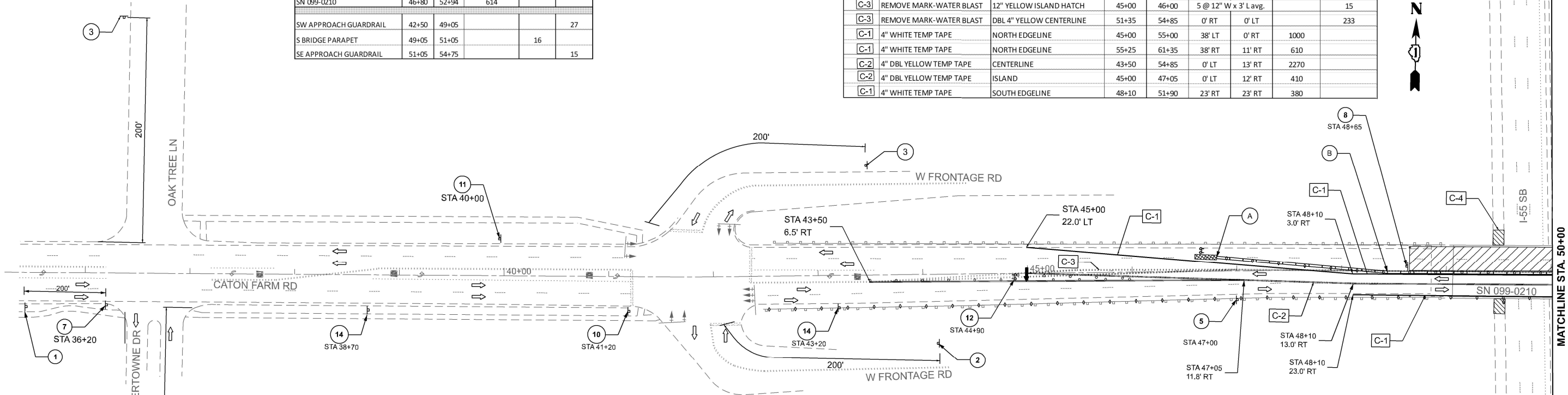
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			DRAWN    -    MGB		REVISED    -												55	FAI 55 22 BRIDGE REHAB		WILL	31	7		
			CHECKED    -		REVISED    -												CONTRACT NO. 62R76							
	PLOT DATE    =    12/17/2024		DATE		REVISED		SCALE: NTS						SHEET 1		OF 1 SHEETS		STA.		TO STA.		ILLINOIS		FED. AID PROJECT	

TEMPORARY CONCRETE BARRIER AND REFLECTORS - STAGE 1

LOCATION	STATION		SET BARRIER FOOT	REFL. TY C EACH	REFL. TY A EACH
	BEGIN	END			
SN 099-0210	46+80	52+94	614		
SW APPROACH GUARDRAIL	42+50	49+05			27
S BRIDGE PARAPET	49+05	51+05		16	
SE APPROACH GUARDRAIL	51+05	54+75			15

SCHEDULE OF PAVEMENT MARKING AND REMOVAL - STAGE 1								
SYMBOL	ITEM	LOCATION	STATION		OFFSET		PMK TEMPORARY FOOT (4")	PMK REMOVAL PERMANENT SQ FT
			BEGIN	END	MIN	MAX		
C-3	REMOVE MARK-WATER BLAST	DBL 4" YELLOW CENTERLINE	45+00	48+65	0' LT	5' RT		243
C-3	REMOVE MARK-WATER BLAST	DBL 4" YELLOW ISLAND BORDER	45+00	46+00	0' RT	5' LT		67
C-3	REMOVE MARK-WATER BLAST	12" YELLOW ISLAND HATCH	45+00	46+00	5 @ 12" W x 3' L Avg.			15
C-3	REMOVE MARK-WATER BLAST	DBL 4" YELLOW CENTERLINE	51+35	54+85	0' RT	0' LT		233
C-1	4" WHITE TEMP TAPE	NORTH EDGELINE	45+00	55+00	38' LT	0' RT	1000	
C-1	4" WHITE TEMP TAPE	NORTH EDGELINE	55+25	61+35	38' RT	11' RT	610	
C-2	4" DBL YELLOW TEMP TAPE	CENTERLINE	43+50	54+85	0' LT	13' RT	2270	
C-2	4" DBL YELLOW TEMP TAPE	ISLAND	45+00	47+05	0' LT	12' RT	410	
C-1	4" WHITE TEMP TAPE	SOUTH EDGELINE	48+10	51+90	23' RT	23' RT	380	



SHEET INDEX OF SIGNS, SYMBOLS AND CALLOUTS

1		W20-1103(0)-48
2		W20-1103(0)-48 M6-1R(0)3021
3		W20-1103(0)-48 M6-1L(0)3021
4		W21-1(0)-48
5		W6-3(0)-48

6		W20-5R(0)-48
7		W20-5(0)-48
8		W1-4R(0)-48
9		W1-4L(0)-48
10		W4-2L(0)-48

11		G20-1103-6036
12		W1-6R(0)-6030 (Above barricade)
13		W1-6L(0)-6030 (Above barricade)
14		W2-1115(0)-3618 R2-1-3648 R2-1106p-3618

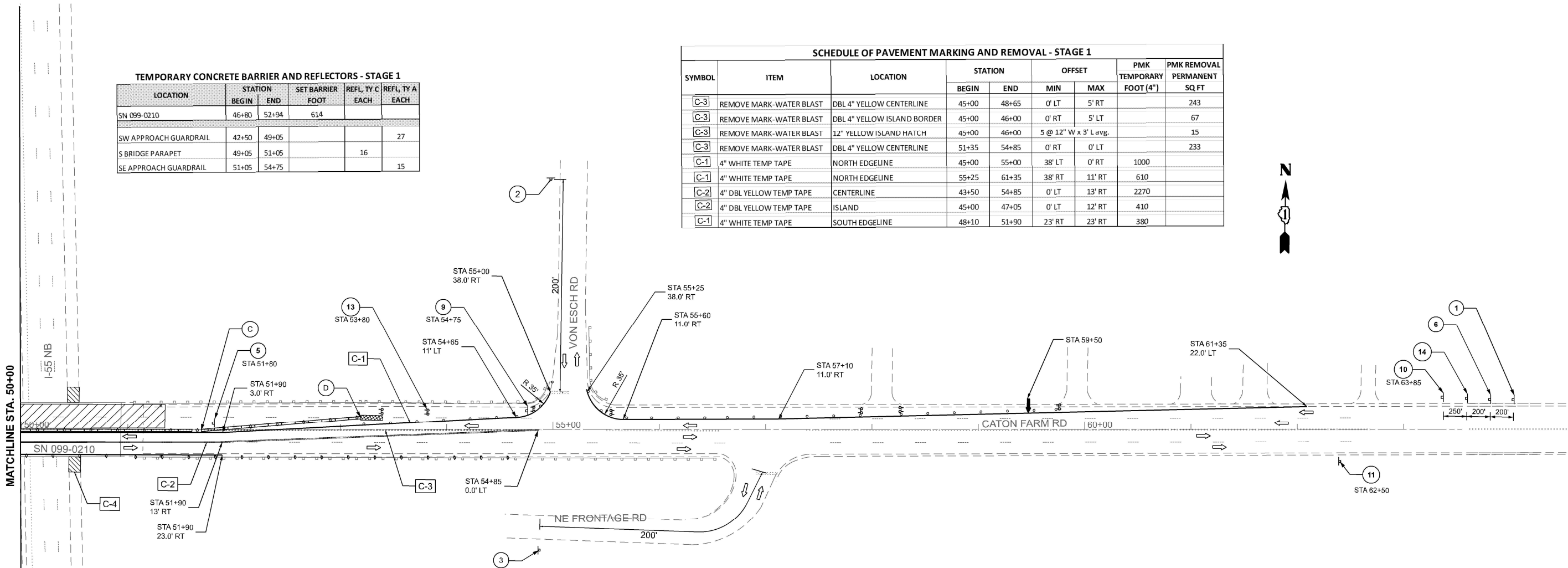
- WORK AREA
- DRUM
- CRYSTAL, BIDIRECTIONAL BRIDGE PARAPET REFLECTOR, TYPE C (SPECIAL)
- TEMPORARY IMPACT ATTENUATOR, NON-REDIRECTIVE, NARROW, TEST LEVEL 2
- TEMP. CONCRETE BARRIER w/ TYPE C REFLECTORS
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE III BARRICADE WITH FLASHING LIGHTS.
- TRAFFIC FLOW DIRECTION
- ARROW BOARD
- PAVEMENT MARKING TAPE, TYPE IV, 4" WHITE
- PAVEMENT MARKING TAPE, TYPE IV, 4" DOUBLE YELLOW.
- PAVEMENT MARKING REMOVAL-WATER BLASTING
- TEMPORARY SHOULDER/ LANE CLOSURES ARE REQUIRED TO PERFORM SUBSTRUCTURE REPAIRS. LANE CLOSURES ALONG I-55 SHALL BE IN ACCORDANCE WITH SPECIAL PROVISION "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)". LANE CLOSURES SHALL OCCUR ONLY DURING NIGHTTIME HOURS IN ACCORDANCE WITH SPECIAL PROVISION "KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC".

TEMP. BARRIER WALL OFFSETS		
A	STA 46+80	11.0' LEFT
B	STA 48+42	1.5' RIGHT
C	STA 51+67	1.5' RIGHT
D	STA 52+94	11.0' LEFT

MODEL: EXC14 - MOT-1 (Sheet 1)  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTE 199-013\civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-staging.dgn

TEMPORARY CONCRETE BARRIER AND REFLECTORS - STAGE 1					
LOCATION	STATION BEGIN	STATION END	SET BARRIER FOOT	REFL, TY C EACH	REFL, TY A EACH
SN 099-0210	46+80	52+94	614		
SW APPROACH GUARDRAIL	42+50	49+05			27
S BRIDGE PARAPET	49+05	51+05		16	
SE APPROACH GUARDRAIL	51+05	54+75			15

SCHEDULE OF PAVEMENT MARKING AND REMOVAL - STAGE 1								
SYMBOL	ITEM	LOCATION	STATION		OFFSET		PMK TEMPORARY	PMK REMOVAL
			BEGIN	END	MIN	MAX	FOOT (4")	SQ FT
[C-3]	REMOVE MARK-WATER BLAST	DBL 4" YELLOW CENTERLINE	45+00	48+65	0' LT	5' RT		243
[C-3]	REMOVE MARK-WATER BLAST	DBL 4" YELLOW ISLAND BORDER	45+00	46+00	0' RT	5' LT		67
[C-3]	REMOVE MARK-WATER BLAST	12" YELLOW ISLAND HATCH	45+00	46+00	5 @ 12" W x 3' L avg.			15
[C-3]	REMOVE MARK-WATER BLAST	DBL 4" YELLOW CENTERLINE	51+35	54+85	0' RT	0' LT		233
[C-1]	4" WHITE TEMP TAPE	NORTH EDGE LINE	45+00	55+00	38' LT	0' RT	1000	
[C-1]	4" WHITE TEMP TAPE	NORTH EDGE LINE	55+25	61+35	38' RT	11' RT	610	
[C-2]	4" DBL YELLOW TEMP TAPE	CENTERLINE	43+50	54+85	0' LT	13' RT	2270	
[C-2]	4" DBL YELLOW TEMP TAPE	ISLAND	45+00	47+05	0' LT	12' RT	410	
[C-1]	4" WHITE TEMP TAPE	SOUTH EDGE LINE	48+10	51+90	23' RT	23' RT	380	



TEMP. BARRIER WALL OFFSETS

(A)	STA 46+80	11.0' LEFT
(B)	STA 48+42	1.5' RIGHT
(C)	STA 51+67	1.5' RIGHT
(D)	STA 52+94	11.0' LEFT

SHEET INDEX OF SIGNS, SYMBOLS AND CALLOUTS

1		W20-1103(0)-48
2		W20-1103(0)-48 M6-1R(0)3021
3		W20-1103(0)-48 M6-1L(0)3021
4		W21-1(0)-48
5		W6-3(0)-48

6		W20-5R(0)-48
7		W20-5(0)-48
8		W1-4R(0)-48
9		W1-4L(0)-48
10		W4-2R(0)-48

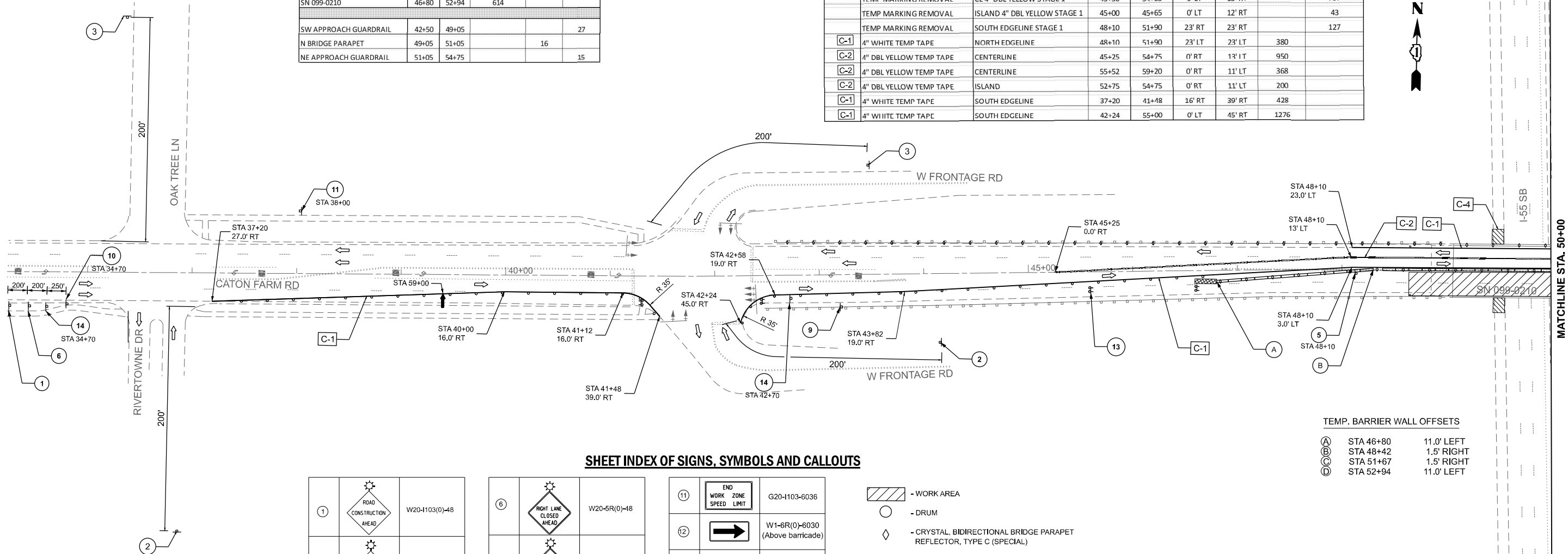
11		G20-1103-6036
12		W1-6R(0)-6030 (Above barricade)
13		W1-6L(0)-6030 (Above barricade)
14		W2-1115(0)-3618 R2-1-3648 R2-1106p-3618

- WORK AREA
- DRUM
- CRYSTAL, BIDIRECTIONAL BRIDGE PARAPET REFLECTOR, TYPE C (SPECIAL)
- TEMPORARY IMPACT ATTENUATOR, NON-REDIRECTIVE, NARROW, TEST LEVEL 2
- TEMP. CONCRETE BARRIER w/ TYPE C REFLECTORS
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE III BARRICADE WITH FLASHING LIGHTS.
- TRAFFIC FLOW DIRECTION
- ARROW BOARD
- [C-1] - PAVEMENT MARKING TAPE, TYPE IV, 4" WHITE
- [C-2] - PAVEMENT MARKING TAPE, TYPE IV, 4" DOUBLE YELLOW.
- [C-3] - PAVEMENT MARKING REMOVAL-WATER BLASTING
- [C-4] - TEMPORARY SHOULDER/ LANE CLOSURES ARE REQUIRED TO PERFORM SUBSTRUCTURE REPAIRS. LANE CLOSURES ALONG I-55 SHALL BE IN ACCORDANCE WITH SPECIAL PROVISION "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)". LANE CLOSURES SHALL OCCUR ONLY DURING NIGHTTIME HOURS IN ACCORDANCE WITH SPECIAL PROVISION "KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC".

MODEL: EXCL4 - MOT-1 [Sheet 2]  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTB 199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-stage1.dgn

TEMPORARY CONCRETE BARRIER AND REFLECTORS - STAGE 2					
LOCATION	STATION		RELOC BARRIER	REFL, TY C	REFL, TY A
	BEGIN	END	FOOT	EACH	EACH
SN 099-0210	46+80	52+94	614		
SW APPROACH GUARDRAIL	42+50	49+05			27
N BRIDGE PARAPET	49+05	51+05		16	
NE APPROACH GUARDRAIL	51+05	54+75			15

SCHEDULE OF PAVEMENT MARKING AND REMOVAL - STAGE 2							
SYMBOL	ITEM	LOCATION	STATION		OFFSET		PMK REMOVAL TEMPORARY SQ FT
			BEGIN	END	MIN	MAX	
	TEMP MARKING REMOVAL	NORTH EDGELINE STAGE 1	45+00	55+00	38' LT	0' RT	333
	TEMP MARKING REMOVAL	NORTH EDGELINE STAGE 1	55+25	61+35	38' RT	11' RT	203
	TEMP MARKING REMOVAL	CL 4" DBL YELLOW STAGE 1	43+50	54+85	0' LT	13' RT	757
	TEMP MARKING REMOVAL	ISLAND 4" DBL YELLOW STAGE 1	45+00	45+65	0' LT	12' RT	43
	TEMP MARKING REMOVAL	SOUTH EDGELINE STAGE 1	48+10	51+90	23' RT	23' RT	127
C-1	4" WHITE TEMP TAPE	NORTH EDGELINE	48+10	51+90	23' LT	23' LT	380
C-2	4" DBL YELLOW TEMP TAPE	CENTERLINE	45+25	54+75	0' RT	13' LT	950
C-2	4" DBL YELLOW TEMP TAPE	CENTERLINE	55+52	59+20	0' RT	11' LT	368
C-2	4" DBL YELLOW TEMP TAPE	ISLAND	52+75	54+75	0' RT	11' LT	200
C-1	4" WHITE TEMP TAPE	SOUTH EDGELINE	37+20	41+18	16' RT	39' RT	428
C-1	4" WHITE TEMP TAPE	SOUTH EDGELINE	42+24	55+00	0' LT	45' RT	1276



SHEET INDEX OF SIGNS, SYMBOLS AND CALLOUTS

①		W20-1103(0)-48	⑥		W20-5R(0)-48
②		W20-1103(0)-48 M6-1R(0)3021	⑦		W20-5(0)-48
③		W20-1103(0)-48 M6-1L(0)3021	⑧		W1-1R(0)-48
④		W21-1(0)-48	⑨		W1-1L(0)-48
⑤		W6-3(0)-48	⑩		W4-2R(0)-48
⑪		G20-1103-6036			
⑫		W1-6R(0)-6030 (Above barricade)			
⑬		W1-6L(0)-6030 (Above barricade)			
⑭		W2-III5(0)-3618 R2-1-3648 R2-1106p-3618			

- WORK AREA
- DRUM
- CRYSTAL, BIDIRECTIONAL BRIDGE PARAPET REFLECTOR, TYPE C (SPECIAL)
- TEMPORARY IMPACT ATTENUATOR, NON-REDIRECTIVE, NARROW, TEST LEVEL 2
- TEMP. CONCRETE BARRIER w/ TYPE C REFLECTORS
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE III BARRICADE WITH FLASHING LIGHTS.
- TRAFFIC FLOW DIRECTION
- ARROW BOARD
- C-1 - PAVEMENT MARKING TAPE, TYPE IV, 4" WHITE
- C-2 - PAVEMENT MARKING TAPE, TYPE IV, 4" DOUBLE YELLOW.
- C-3 - PAVEMENT MARKING REMOVAL-WATER BLASTING
- C-4 - TEMPORARY SHOULDER/ LANE CLOSURES ARE REQUIRED TO PERFORM SUBSTRUCTURE REPAIRS. LANE CLOSURES ALONG I-55 SHALL BE IN ACCORDANCE WITH SPECIAL PROVISION "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)". LANE CLOSURES SHALL OCCUR ONLY DURING NIGHTTIME HOURS IN ACCORDANCE WITH SPECIAL PROVISION "KEEPING THE EXPRESSWAYS OPEN TO TRAFFIC".

TEMP. BARRIER WALL OFFSETS		
A	STA 46+80	11.0' LEFT
B	STA 48+42	1.5' RIGHT
C	STA 51+67	1.5' RIGHT
D	STA 52+94	11.0' LEFT

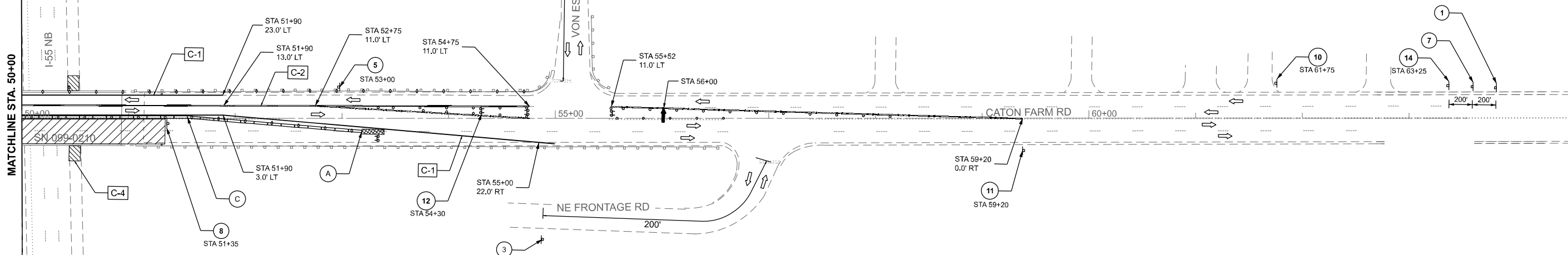
MODEL: EXC14 - MOT-2 (Sheet 1)  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTEB 199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-staging.dgn

# TEMPORARY CONCRETE BARRIER AND REFLECTORS - STAGE 2

LOCATION	STATION BEGIN	STATION END	RELOC BARRIER FOOT	REFL, TY C EACH	REFL, TY A EACH
SN 099-0210	46+80	52+94	614		
SW APPROACH GUARDRAIL	42+50	49+05			27
N BRIDGE PARAPET	49+05	51+05		16	
NE APPROACH GUARDRAIL	51+05	54+75			15

# SCHEDULE OF PAVEMENT MARKING AND REMOVAL - STAGE 2

SYMBOL	ITEM	LOCATION	STATION		OFFSET		PMK TEMPORARY FOOT (4")	PMK REMOVAL TEMPORARY SQ FT
			BEGIN	END	MIN	MAX		
	TEMP MARKING REMOVAL	NORTH EDGELINE STAGE 1	45+00	55+00	38' LT	0' RT		333
	TEMP MARKING REMOVAL	NORTH EDGELINE STAGE 1	55+25	61+35	38' RT	11' RT		203
	TEMP MARKING REMOVAL	CL 4" DBL YELLOW STAGE 1	43+50	54+85	0' LT	13' RT		757
	TEMP MARKING REMOVAL	ISLAND 4" DBL YELLOW STAGE 1	45+00	45+65	0' LT	12' RT		43
	TEMP MARKING REMOVAL	SOUTH EDGELINE STAGE 1	48+10	51+90	23' RT	23' RT		127
C-1	4" WHITE TEMP TAPE	NORTH EDGELINE	48+10	51+90	23' LT	23' LT	380	
C-2	4" DBL YELLOW TEMP TAPE	CENTERLINE	45+25	54+75	0' RT	13' LT	950	
C-2	4" DBL YELLOW TEMP TAPE	CENTERLINE	55+52	59+20	0' RT	11' LT	368	
C-2	4" DBL YELLOW TEMP TAPE	ISLAND	52+75	54+75	0' RT	11' LT	200	
C-1	4" WHITE TEMP TAPE	SOUTH EDGELINE	37+20	41+48	16' RT	39' RT	428	
C-1	4" WHITE TEMP TAPE	SOUTH EDGELINE	42+24	55+00	0' LT	45' RT	1276	



# TEMP. BARRIER WALL OFFSETS

A	STA 46+80	11.0' LEFT
B	STA 48+42	1.5' RIGHT
C	STA 51+67	1.5' RIGHT
D	STA 52+94	11.0' LEFT

# SHEET INDEX OF SIGNS, SYMBOLS AND CALLOUTS

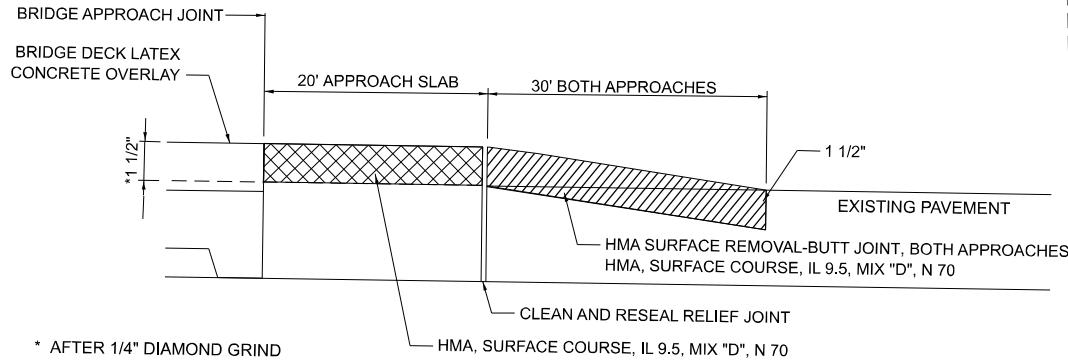
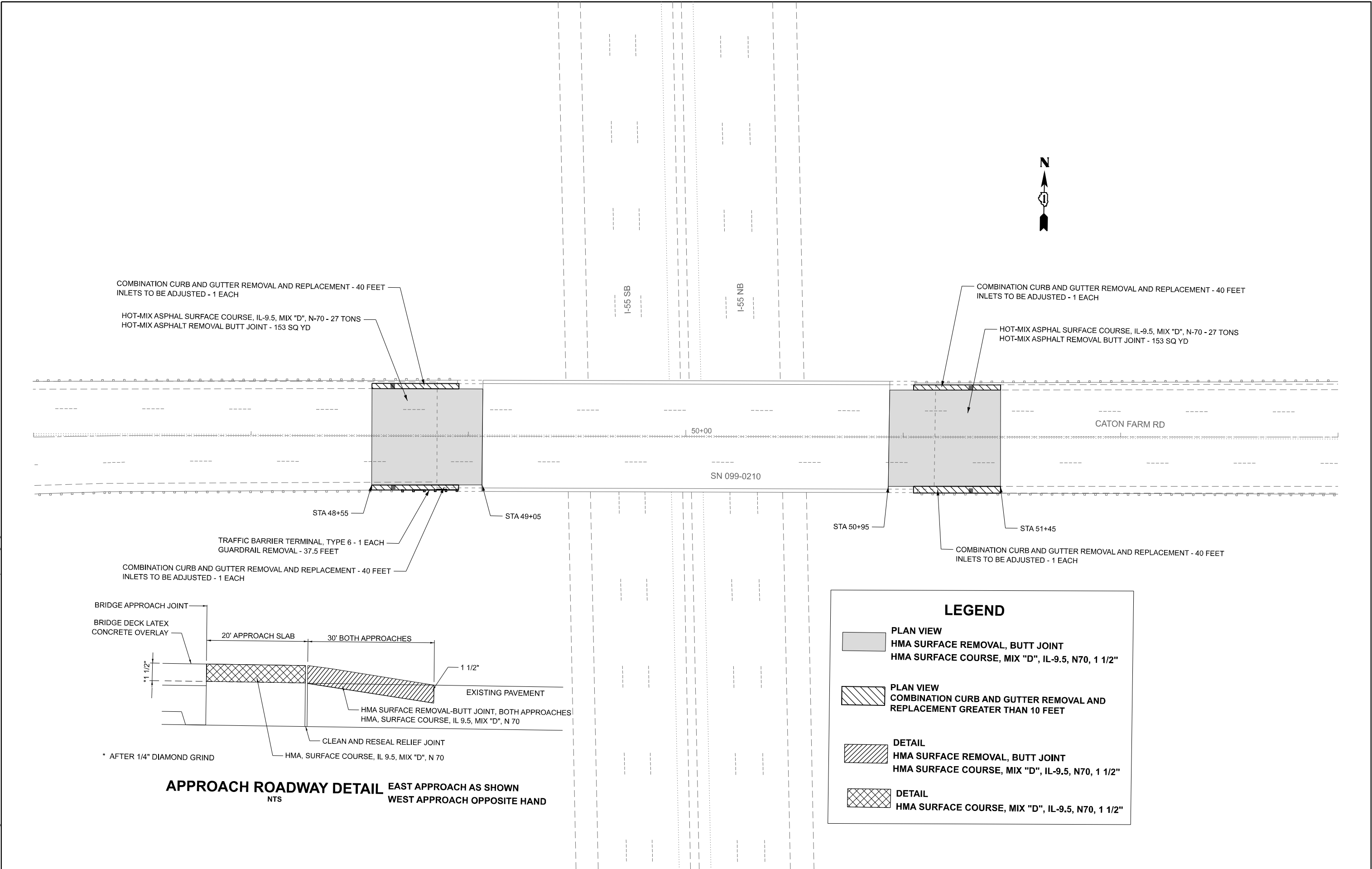
1		W20-1103(0)-48	11		G20-1103-6036
2		W20-1103(0)-48	12		W1-6R(0)-6030 (Above barricade)
3		M6-1R(0)3021	13		W1-6L(0)-6030 (Above barricade)
4		W20-1103(0)-48	14		W2-III5(0)-3618 R2-1-3648 R2-1106p-3618
5		M6-1L(0)3021			
6		W20-5R(0)-48			
7		W20-5(0)-48			
8		W1-4R(0)-48			
9		W1-4L(0)-48			
10		W4-2L(0)-48			

	- WORK AREA
	- DRUM
	- CRYSTAL, BIDIRECTIONAL BRIDGE PARAPET REFLECTOR, TYPE C (SPECIAL)
	- TEMPORARY IMPACT ATTENUATOR, NON-REDIRECTIVE, NARROW, TEST LEVEL 2
	- TEMP. CONCRETE BARRIER w/ TYPE C REFLECTORS
	- SIGN ON PORTABLE OR PERMANENT SUPPORT
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MODEL: EXCL4 - MOT-2 (Sheet 2)  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTEB 199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-staging.dgn

<b>EDI</b> Environmental Design International Inc. Civil, Survey, Environmental and Construction Inspection Services 33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603 Ph: (312) 345-1400 Fax: (312) 345-0529 www.ediintl.com	USER NAME = mbutling	DESIGNED - MGB	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>I-55 AT CATON FARM ROAD</b> <b>MAINTENANCE OF TRAFFIC PLAN STAGE 2</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MGB	REVISED -			55	FAI 55 22 BRIDGE REHAB	WILL	31	11
		CHECKED -	REVISED -			CONTRACT NO. 62R76				
	PLOT DATE = 12/16/2024	DATE -	REVISED -			SCALE: 1"=50'	SHEET OF SHEETS	STA. 50+00.00 TO STA. 67+35.00	ILLINOIS	FED. AID PROJECT

MODEL: EXC14 - rdwy (Sheet)  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 FTE 199-013\Civil\03 CAD\01 Model\catonfarm62R76 wo11D162R76-rwmdesign.dgn



**APPROACH ROADWAY DETAIL** EAST APPROACH AS SHOWN  
NTS WEST APPROACH OPPOSITE HAND

**LEGEND**

**PLAN VIEW**  
HMA SURFACE REMOVAL, BUTT JOINT  
HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"

**PLAN VIEW**  
COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET

**DETAIL**  
HMA SURFACE REMOVAL, BUTT JOINT  
HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"

**DETAIL**  
HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1 1/2"

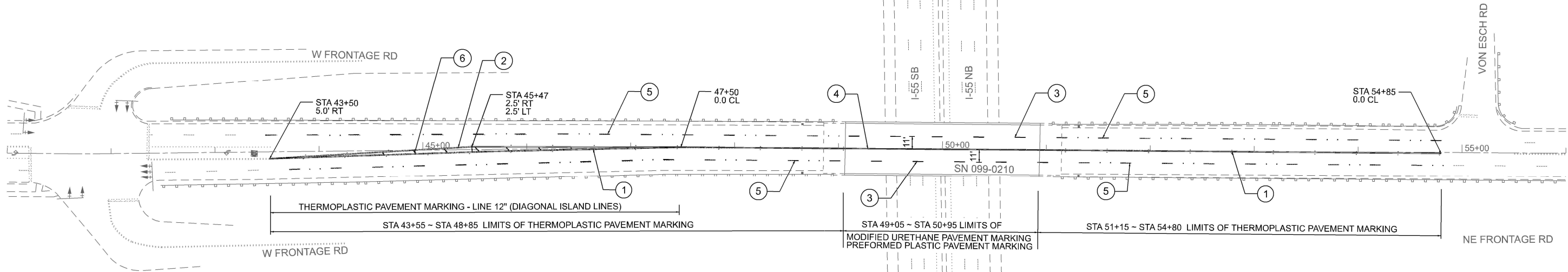
<div><div><div>EDI</div><div>Excellence, Dedication, Innovation</div><div>Environmental Design International Inc.</div><div>Civil, Survey, Environmental and Construction Inspection Services</div><div>33 W. MONROE STREET, SUITE 1825, CHICAGO, IL 60603</div><div>Ph. (312) 345-1400 Fax (312)345-0529</div><div>www.ediintl.com</div></div></div>	USER NAME = mbutling	DESIGNED - MGB	REVISED -	<div><div><div>STATE OF ILLINOIS</div><div>DEPARTMENT OF TRANSPORTATION</div></div></div>	<div><div><div>I-55 AT CATON FARM ROAD</div><div>ROADWAY PLAN</div></div></div>					F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	55	FAI 55 22 BRIDGE REHAB	WILL							31	12			
	DRAWN - MGB	REVISED -	REVISED -		CONTRACT NO. 62R76									
	CHECKED -	REVISED -	REVISED -											
	PLOT DATE = 11/19/2024	DATE -	REVISED -											
SCALE: 1"=20'				SHEET 1	OF 1	SHEETS	STA. 47+00.00	TO STA. 53+00.00	ILLINOIS FED. AID PROJECT					



MODEL: EXCL4 - Plan 1 [Sheet]  
FILE NAME: L:\1980 Orion Engineers LLC\1980.006 PTB 199-013\Civil03 CAD\01 Model\catonfarm62R76 wo11\Caton Farm Submittal 8-4-2025D162R76-pmk.dgn

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW CENTERLINE)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW ISLAND BORDER)
- ③ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7" (1.5" BLACK, 4" WHITE, 1.5" BLACK) (WHITE @ 10' DASH - 30' SKIP GROOVED)
- ④ MODIFIED URETHANE PAVEMENT MARKING, TYPE D - LINE 4" (DOUBLE YELLOW CENTERLINE)
- ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (10' DASH -30 SKIP WHITE LANE LINE)
- ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (DIAGONAL MEDIAN LINES AT 45 DEG. - 5 MIN)

SCHEDULE OF RRPm REMOVAL			
LOCATION	STATION		RRPM
	BEGIN	END	REMOVAL
SN 099-0210	49+05	50+95	12



SCHEDULE OF PAVEMENT MARKING AND REMOVAL - POST STAGE										
SYMBOL	ITEM	LOCATION	STATION		OFFSET		PERMANENT PMK			PMK REMOVAL
			BEGIN	END	MIN	MAX	YELLOW (4") FOOT	WHITE (4") FOOT	YELLOW (12") FOOT	TEMPORARY SQ FT
	TEMP MARKING REMOVAL	NORTH EDGELINE STAGE 2	48+10	51+90	23' LT	23' LT				127
	TEMP MARKING REMOVAL	CL 4" DBL YELLOW STAGE 2	43+50	54+85	0' LT	13' RT				757
	TEMP MARKING REMOVAL	CL 4" DBL YELLOW STAGE 2	55+52	59+20	0' RT	11' LT				245
	TEMP MARKING REMOVAL	ISLAND 4" DBL YELLOW STAGE 2	45+00	45+65	0' LT	12' RT				43
	TEMP MARKING REMOVAL	SOUTH EDGELINE STAGE 2	37+20	41+48	16' RT	39' RT				143
	TEMP MARKING REMOVAL	SOUTH EDGELINE STAGE 2	42+24	55+00	0' LT	45' RT				425
PERMANENT PAVEMENT MARKING -THERMOPLASTIC										
①	PAVEMENT MK 4"- DBL YELLOW	CENTERLINE	43+50	49+05	5' RT	0' LT	1110			
①	PAVEMENT MK 4"- DBL YELLOW	CENTERLINE	50+95	54+85	0' LT	0' RT	780			
⑤	PAVEMENT MK 4"- WHITE	DASHED LANE LINE LT & RT	43+50	49+05	11' LT & RT	16' RT		278		
⑤	PAVEMENT MK 4"- WHITE	DASHED LANE LINE LT & RT	50+95	54+85	11' LT & RT	11' LT & RT		195		
②	PAVEMENT MK 4"- DBL YELLOW	ISLAND BORDER	43+50	47+50	5' RT	0' LT	800			
⑥	PAVEMENT MK 12"- YELLOW	ISLAND HATCH	43+55	47+50	7 @ 12" W x 5' L avg.				35	
PREFORMED PLASTIC PAVEMENT MARK AND GROOVING										
③	PAVEMENT MK 4"- DASH WHITE	LANE LINE	49+05	50+95	35' LT	0' RT		48		
MODIFIED URETHANE PAVEMENT MARKING										
④	PAVEMENT MK 4"- DBL YELLOW	CFNTERLINE	49+05	50+95	35' LT	0' RT	380			

PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR ALL LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS.

THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON HMA PAVEMENT AND MODIFIED URETHANE PAVEMENT MARKINGS SHALL BE USED ON CONCRETE PAVEMENT EXCEPT AS OTHERWISE NOTED.

ALL PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT D1 PAVEMENT MARKING DETAIL TC-13.

Existing Structure:

SN 099-0210 was originally constructed in 1988. The bridge consists of 4-span reinforced concrete deck on steel rolled sections. The substructure consists of reinforced concrete piers and abutments.

Traffic Control:

Stage Construction will be utilized. Two lanes of traffic shall be maintained on the north side of Canton Farm Road for Stage I construction. Two lanes of traffic shall be maintained on the south side of Caton Farm Road for Stage II construction.

No Salvage.

DESIGN STRESSES

FIELD UNITS

EXISTING CAST IN PLACE CONCRETE

$f'c = 3,500 \text{ PSI}$  (Concrete)  
 $f_y = 50,000 \text{ PSI}$  (Reinforcement)

PROPOSED CAST IN PLACE CONCRETE

$f'c = 4,000 \text{ psi}$  (Superstructure)  
 $f'c = 3,500 \text{ psi}$  (Substructure)  
 $f_y = 60,000 \text{ psi}$  (Reinforcement)

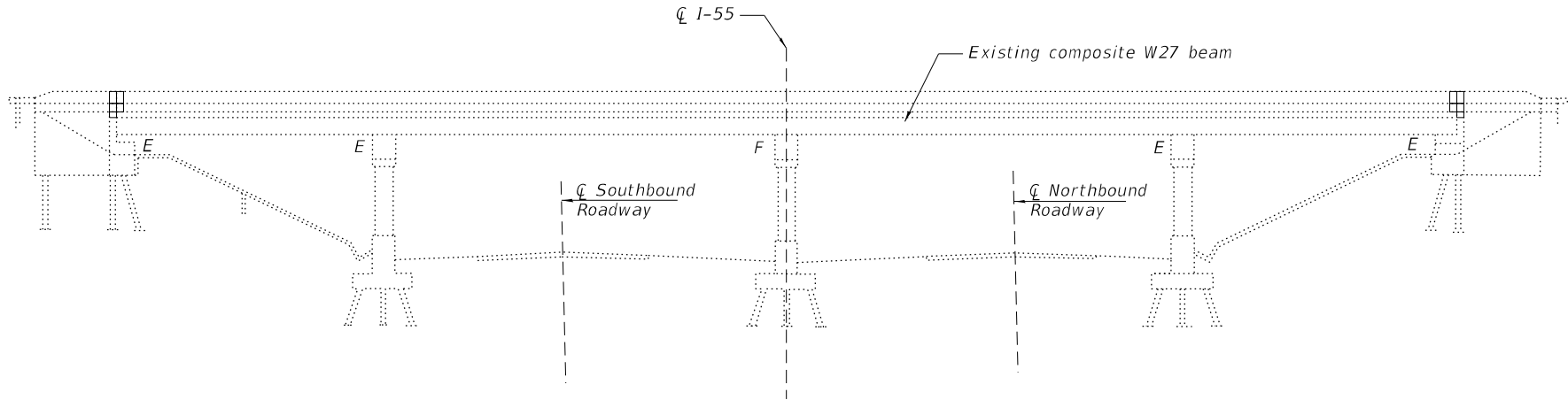
DESIGN SPECIFICATIONS

AASHTO LRFD Bridge Design Specifications, 17th Edition.

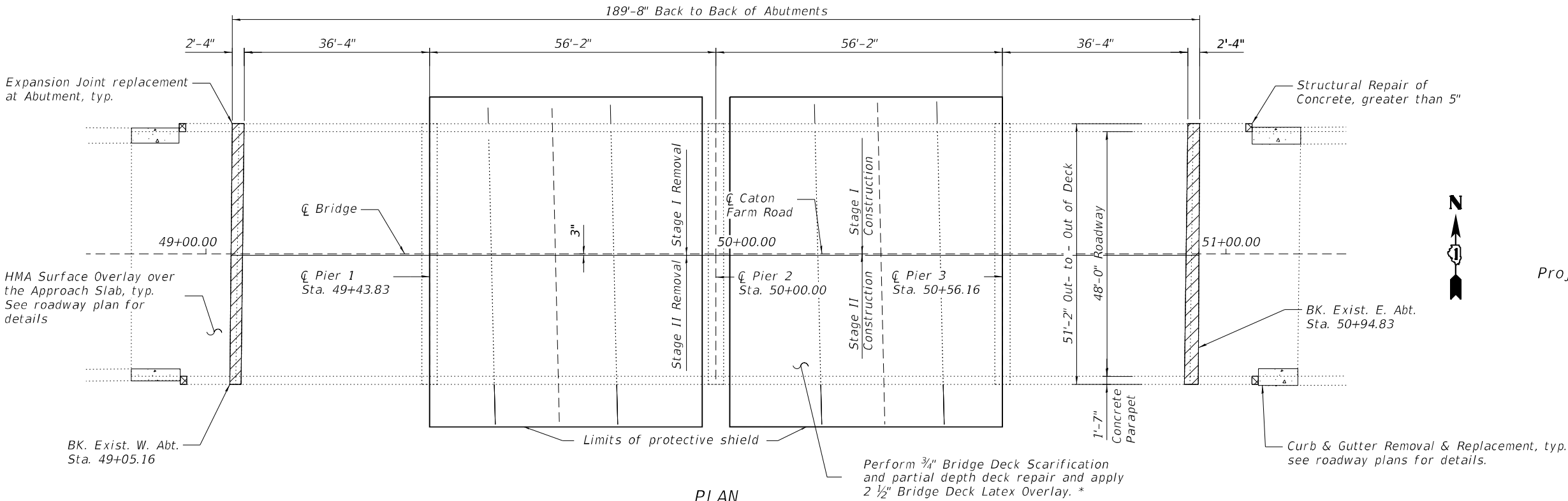
Illinois Department Of Transportation Bridge Manual Dated January 2025.

LOADING HS20-44

No future wearing surface allowed



ELEVATION



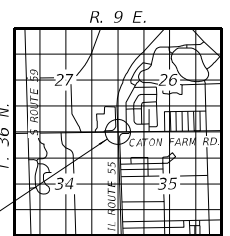
PLAN



EXP. DATE 11-30-2026

SIGNATURE

10/02/2025  
DATE



LOCATION SKETCH

GENERAL PLAN AND ELEVATION

I-55 AT CATON FARM ROAD

SEC. F.A.I. 55 22, BRIDGE REHAB

WILL COUNTY

STR NO. 099-0210

FILE NAME =  
PLOT SCALE =  
USER NAME =



USER NAME =	DESIGNED - AAA	REVISED -
DRAWN - AAA	REVISED -	
PLOT SCALE =	CHECKED - KZ	REVISED -
PLOT DATE =	DATE - 11/22/2024	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION  
STRUCTURE NO. 099-0210

SCALE: 1"=24' SHEET NO. 51 OF 59 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	31	14
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

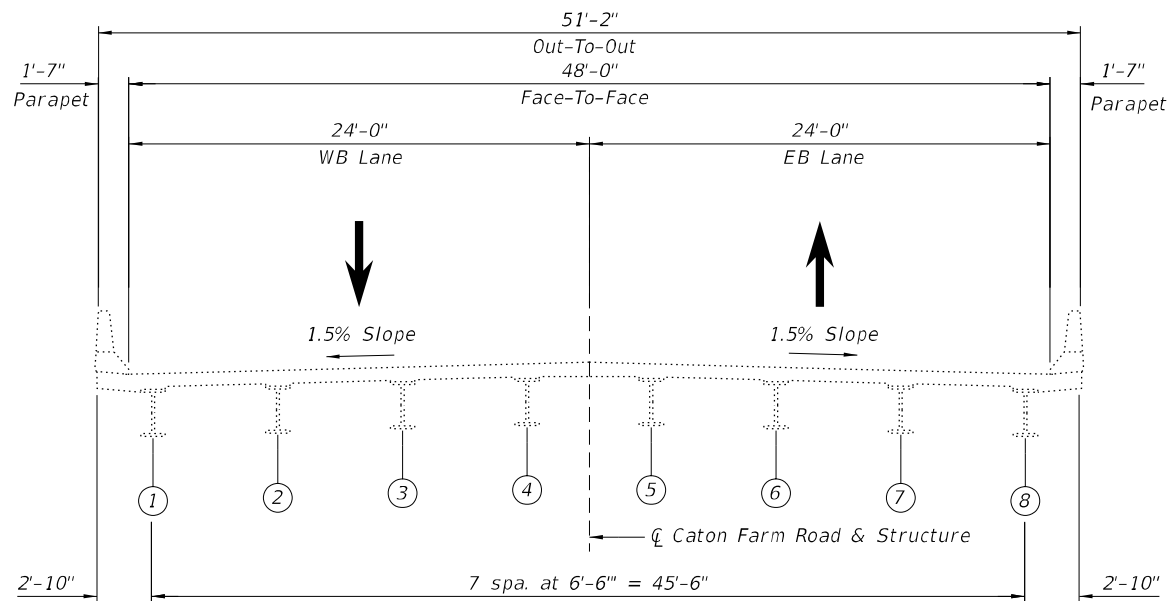
SHEET NO.

S1	GENERAL PLAN AND ELEVATION
S2	GENERAL NOTES, INDEX OF SHEETS, BILL OF MATERIALS
S3	STAGE CONSTRUCTION DETAILS
S4	DECK OVERLAY AND SLAB REPAIR PLANS
S5	EXPANSION JOINT DETAILS
S6	PREFORMED JOINT STRIP SEAL (BASE SHEET)
S7	ABUTMENT REPAIR DETAILS
S8	PIER REPAIR DETAILS
S9	BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER

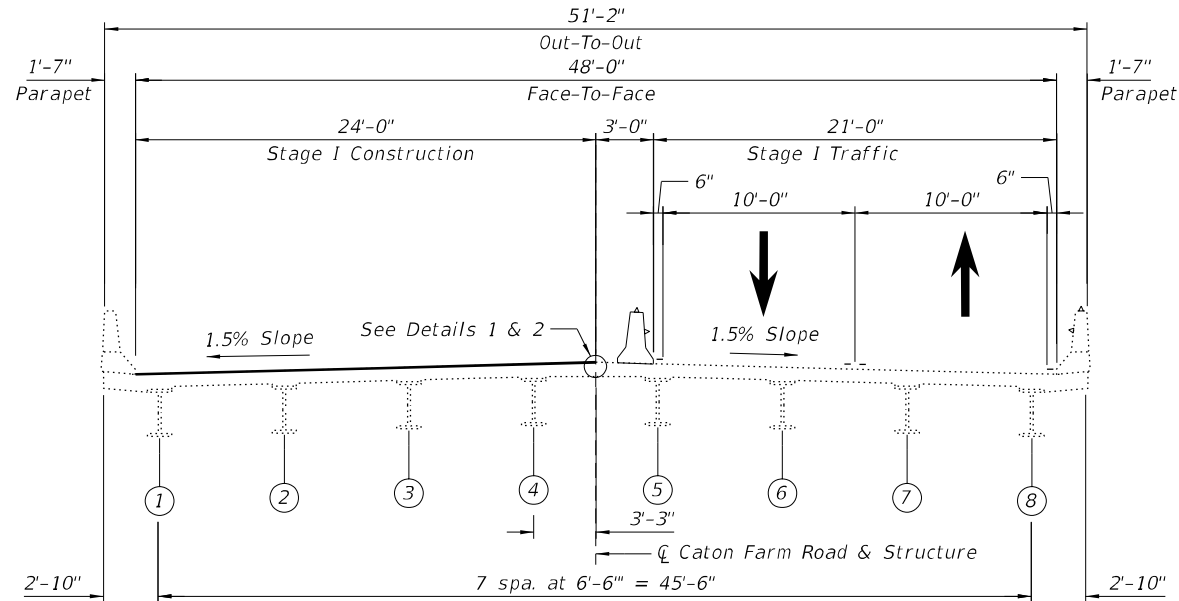
1. *Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.*
2. *Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.*
3. *Reinforcement bars designated "(E)" shall be epoxy coated.*
4. *Reinforcement bar bending details shall be in accordance with the latest "Manual of Standard Practice for Detailing Reinforced Concrete Structures, ACI 315".*
5. *Reinforcement bar bending dimensions are out to out.*
6. *Contractor shall not scale dimensions from the Contract Plans for construction purposes. Scales shown are for information only.*
7. *No construction joints except those shown on the plans shall be allowed unless approved by the Engineer.*
8. *No concrete cutting shall be permitted until the cutting limits have been outlined by the Contractor and approved by the Engineer.*
9. *Joint openings shall be adjusted according with Article 520.04 of the Standard Specs, when the deck is poured at an ambient temperature other than 50°F.*

- To be completed under stage construction:
1. Install Protective Shield over lane and shoulders of I-55.
2. Perform 3/4" Bridge Deck Scarification.
3. Reconstruct bridge deck expansion joints at the Abutments with preformed joint strip seal.
4. Perform Structural Repair on approach concrete barriers.
5. Perform partial depth deck patching as required and, apply 2½" Bridge Deck Latex Overlay and perform Bridge Deck Grooving, and diamond grinding.
6. Apply Protective Coat to the existing parapet concrete and to new concrete overlay.
7. Perform Structural Repair on Abutments and Piers 1 & 3.

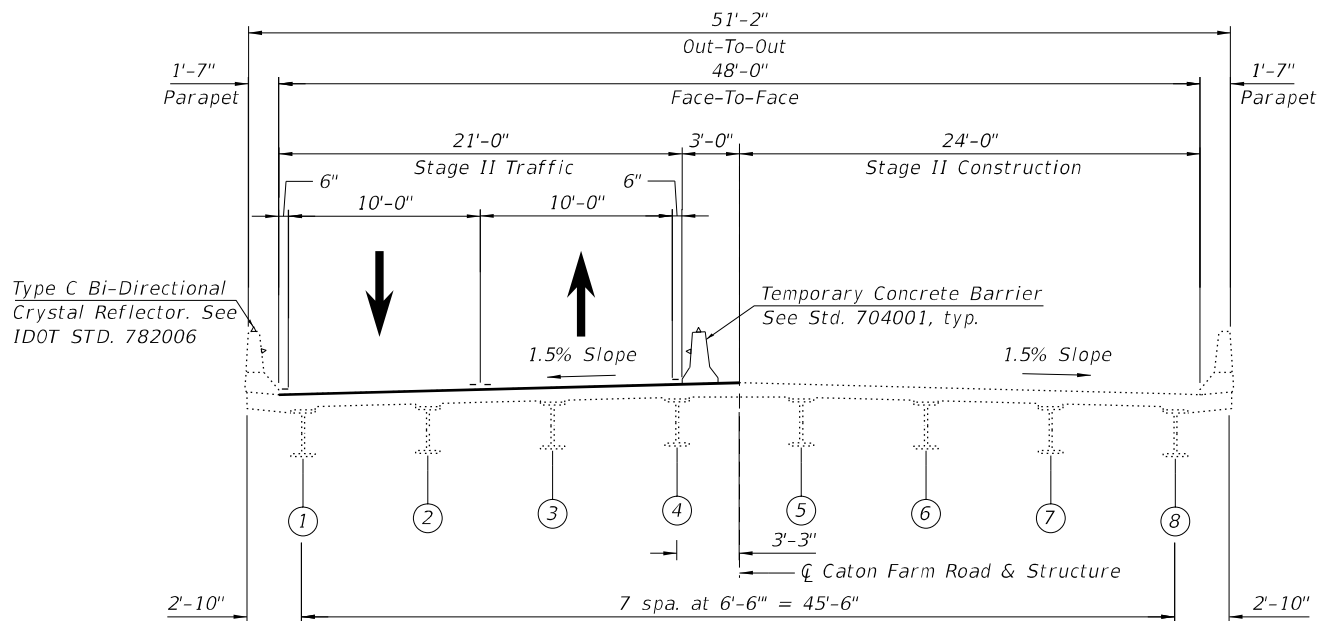
ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	15.0
Protective Shield	Sq Yd	162
Concrete Superstructure	Cu Yd	17.0
Protective Coat	Sq Yd	1115
Reinforcement Bars, Epoxy Coated	Pound	2580
Bar Splicers	Each	24
Preformed Joint Seal	Foot	102
Bridge Deck Grooving (Longitudinal)	Sq Yd	914
Approach Slab Repair (Partial Depth)	Sq Yd	14
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	Sq Yd	997
Bridge Deck Scarification 3/4"	Sq Yd	997
Structural Repair Of Concrete (Depth Equal to or Less Than 5 Inches)	Sq Ft	166
Structural Repair Of Concrete (Depth Greater than 5")	Sq Ft	8
Diamond Grinding (Bridge Section)	Sq Yd	914



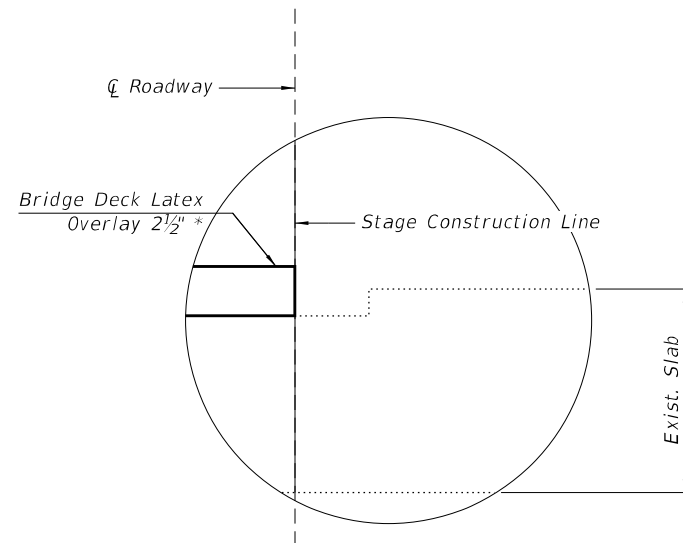
**EXISTING**  
(Looking East)



**STAGE I**  
(Looking East)

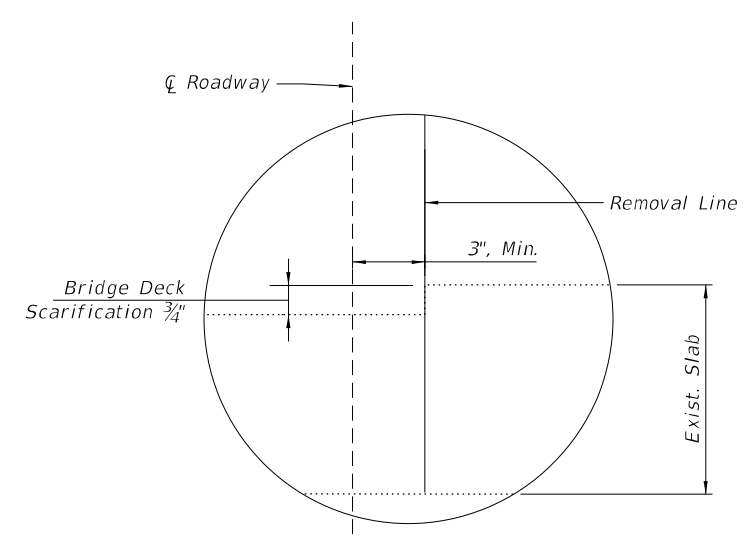


**STAGE II**  
(Looking East)



**DETAIL 2 (SHOWING PROPOSED)**

\* Prior to 1/4" Diamond Grinding



**DETAIL 1 (SHOWING REMOVAL)**

FILE NAME  
PLOT SCALE  
USER NAME

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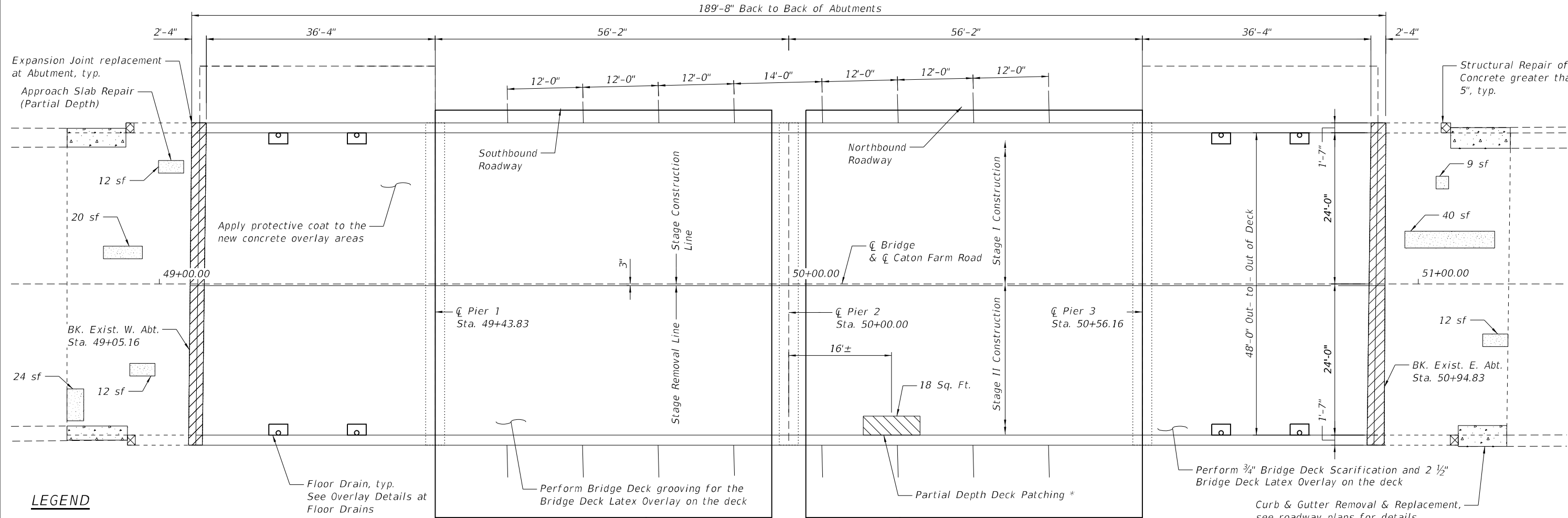
USER NAME	DESIGNED - AAA	REVISED -
DRAWN - AAA	REVISED -	
PLOT SCALE	CHECKED - KZ	REVISED -
PLOT DATE	DATE - 11/22/2024	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS**  
**STRUCTURE NO. 099-0210**

SCALE: 1"=10' SHEET NO. 53 OF 59 SHEETS STA. TO STA.

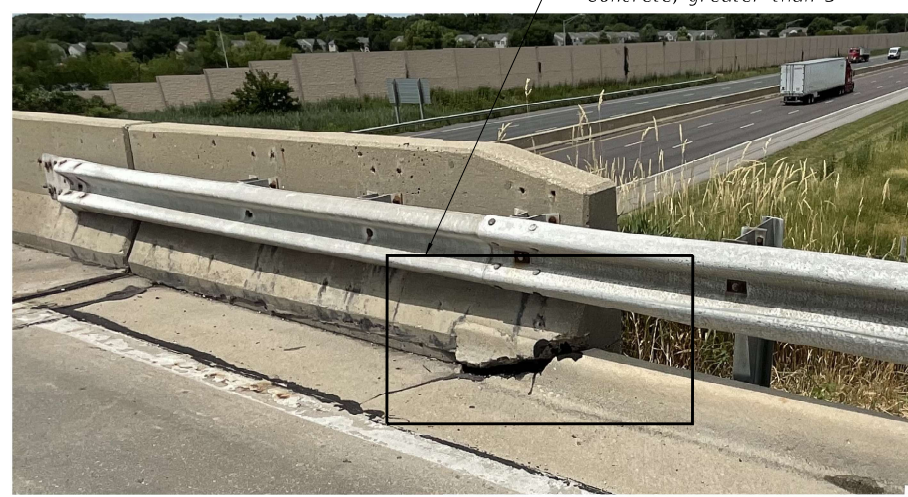
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	30	16
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LEGEND

- Curb & Gutter Removal and Replacement, see roadway plans for details.
- Structural Repair of Concrete greater than 5"
- Deck Slab Repair (Partial Depth)\* (For Information Only)
- Concrete Removal
- Approach Slab Repair (Partial Depth)

\* Deck Slab Repair (Partial Depth) to be paid under Bridge Deck Latex Concrete Overlay, 2 1/2".

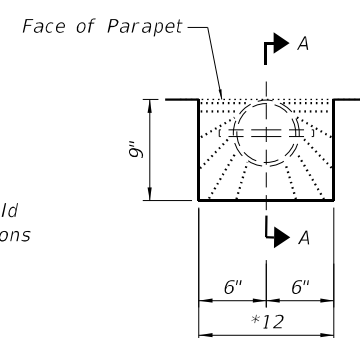


LOCATION OF STRUCTURAL REPAIR OF CONCRETE, GREATER THAN 5"

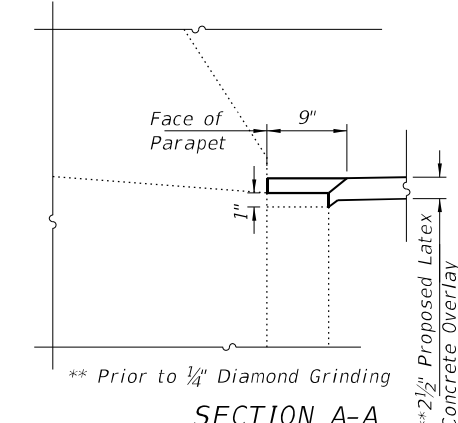
PLAN

NOTES

- Deck Slab Repair areas estimated based on District 1 Bureau of Maintenance field notes and the visual inspection completed in 2023 Actual repair areas and locations shall be determined by the Engineer and shown on the as-built plans.
- For Expansion Joint Reconstruction Details, see sheet S5.
- Bridge Deck Scarification, 3/4" and Bridge Deck Latex Overlay, 2 1/2" shall be performed over the limits of the bridge deck, excluding the transverse joint reconstruction areas.
- Diamond Grinding, 1/4" shall be performed over the limits of the bridge deck excluding exterior 2 feet.
- Protective coat shall be applied to the proposed concrete overlay, inside faces of the curbs, and the transverse joint reconstruction areas.



DRAIN DETAIL



SECTION A-A

OVERLAY DETAILS AT FLOOR DRAIN

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	15.0
Protective Shield	Sq Yd	162
Protective Coat	Sq Yd	1115
Bridge Deck Grooving (Longitudinal)	Sq Yd	914
Approach Slab Repair (Partial Depth)	Sq Yd	14
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	Sq Yd	997
Bridge Deck Scarification 3/4"	Sq Yd	997
Structural Repair of Concrete Greater than 5"	Sq Ft	8
Diamond Grinding (Bridge Section)	Sq Yd	914

USER NAME =	DESIGNED - AAA	REVISED -
PLOT SCALE =	DRAWN - AAA	REVISED -
PLOT DATE =	CHECKED - KZ	REVISED -
	DATE - 11/22/2024	REVISED -

FILE NAME =  
PLOT SCALE =  
USER NAME =



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DRAWN =  
PLOT SCALE =  
PLOT DATE =

DESIGNED - AAA  
DRAWN - AAA  
CHECKED - KZ  
DATE - 11/22/2024

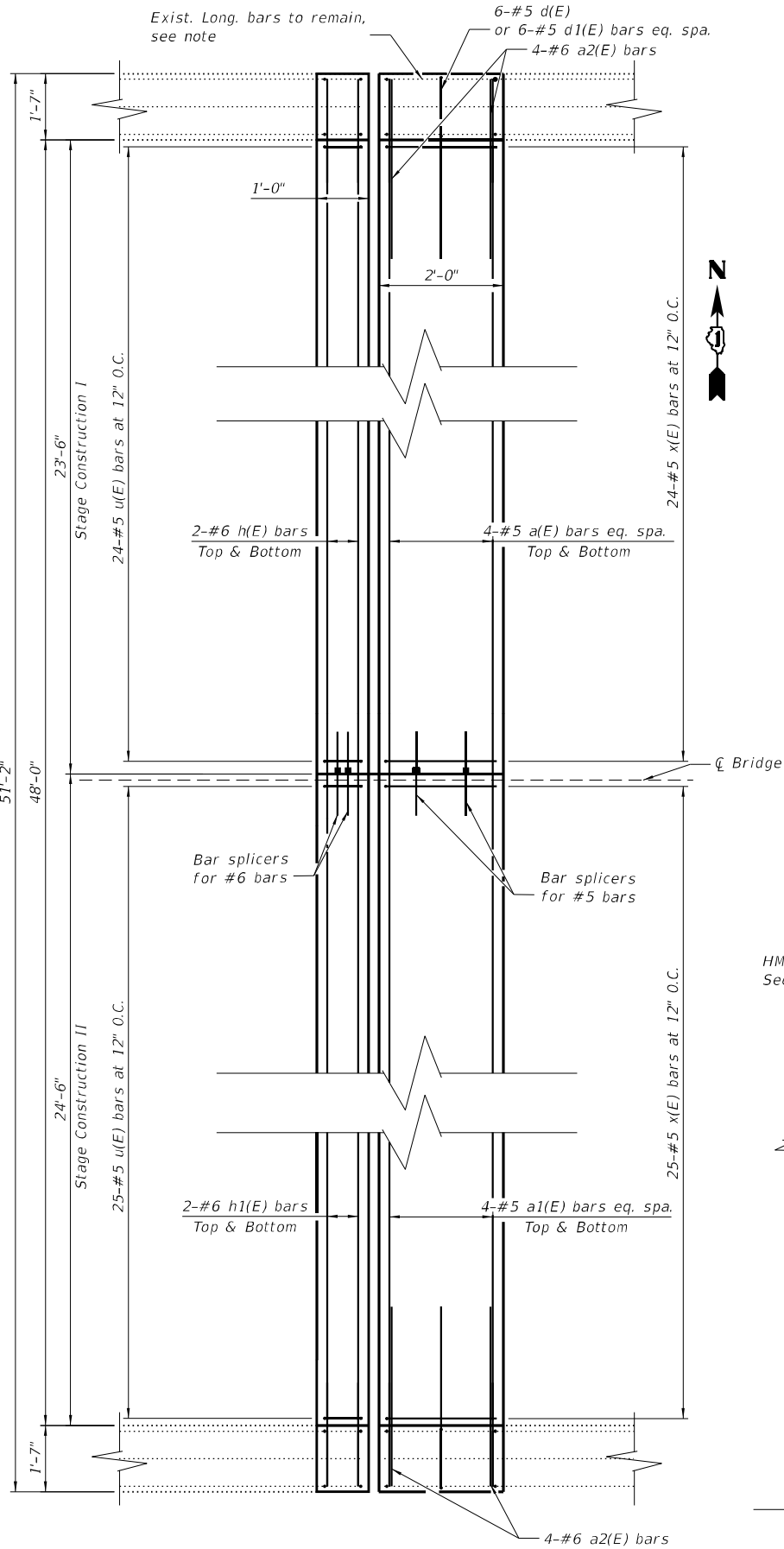
REVISED -  
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

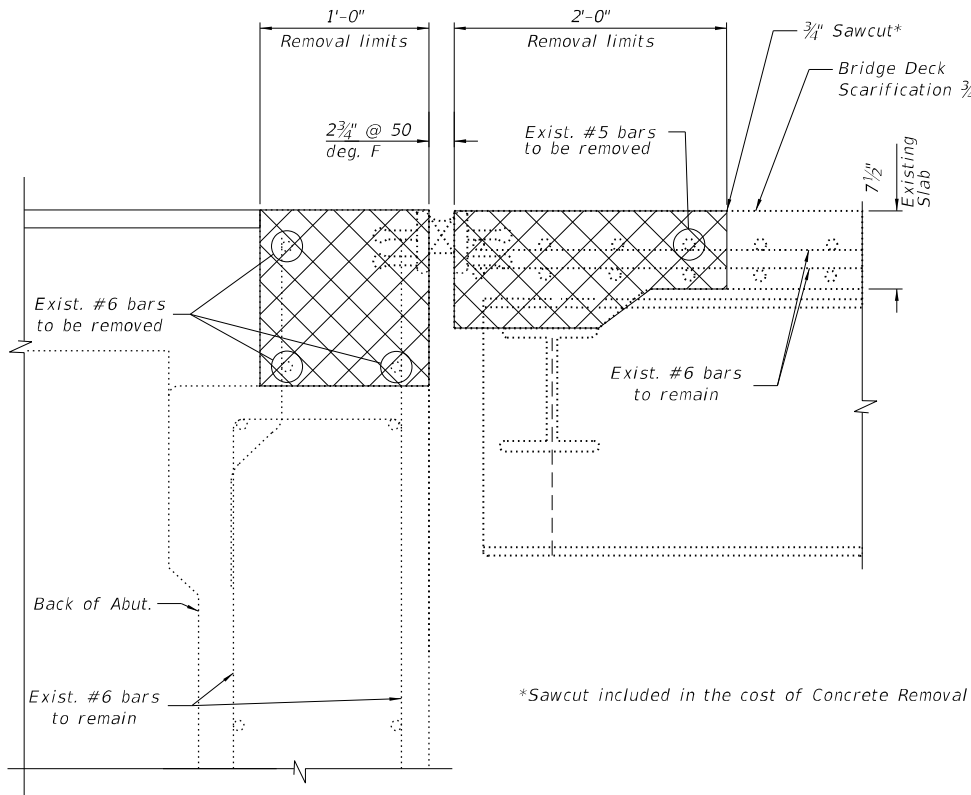
EXPANSION JOINT REPLACEMENT DETAILS  
STRUCTURE NO. 099-0210

SCALE: 1"=6' SHEET NO. 55 OF 59 SHEETS STA. TO STA.

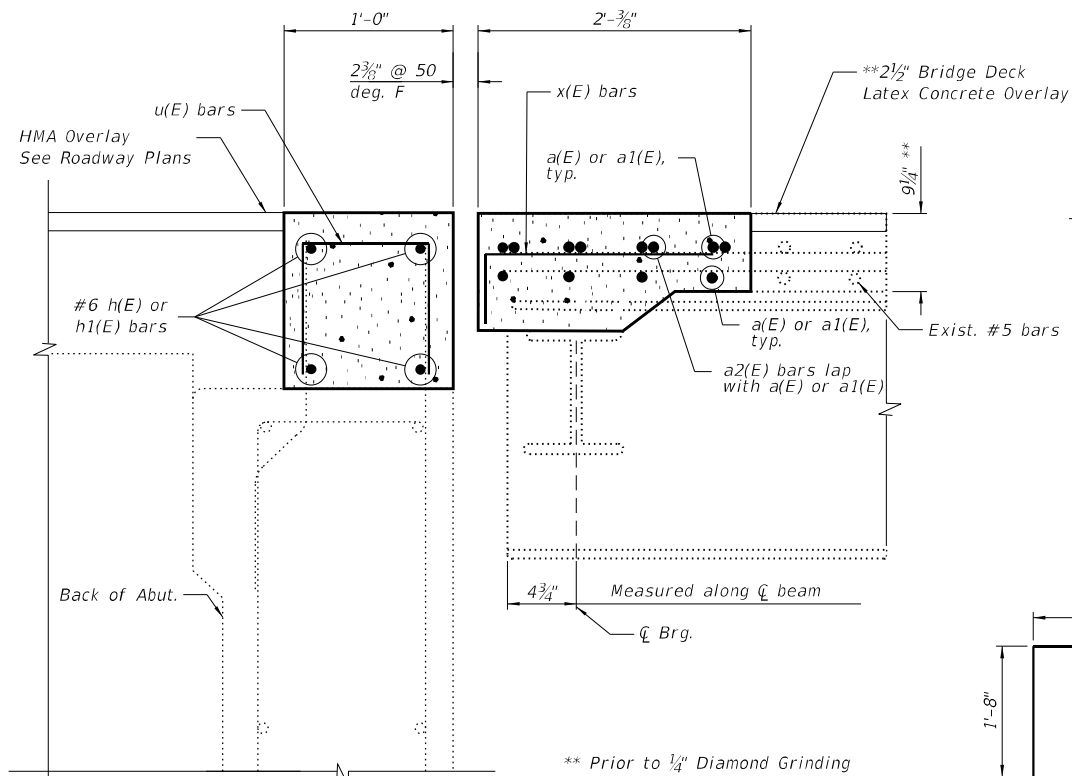
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	31	18
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



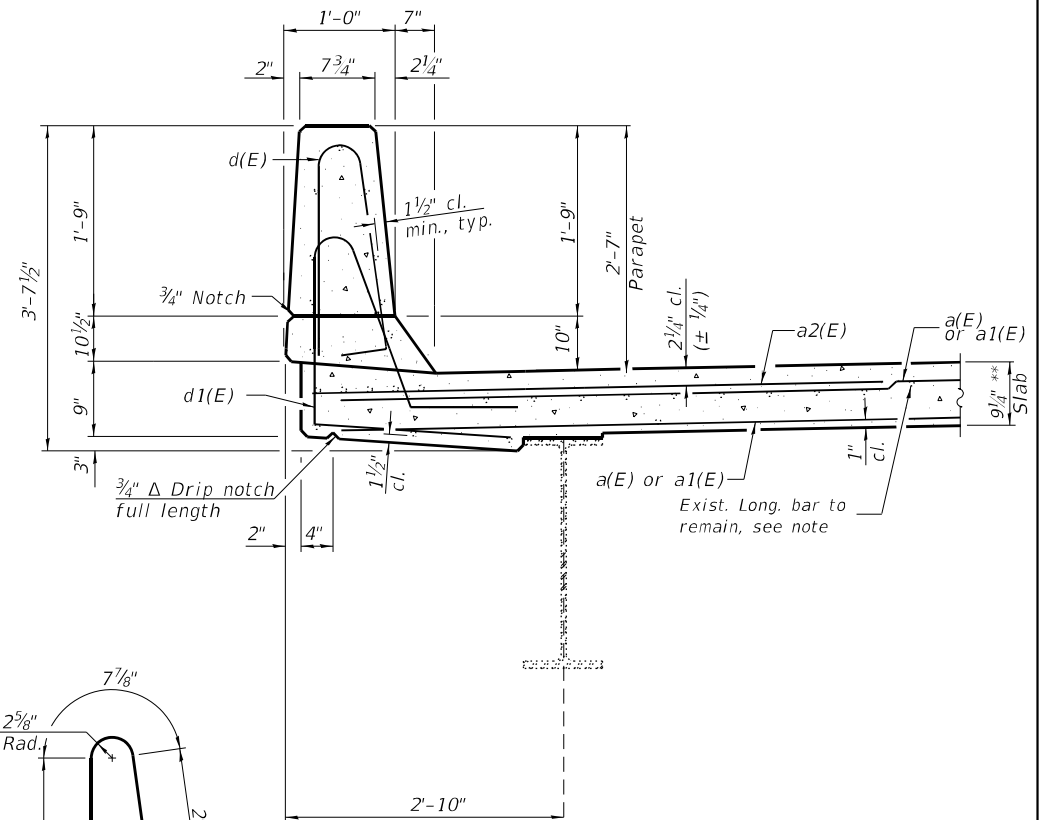
EXPANSION PLAN WEST ABUTMENT  
(EAST ABUTMENT OPPOSITE HAND)



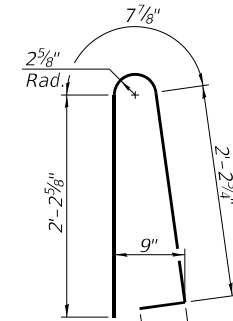
JOINT REMOVAL DETAILS



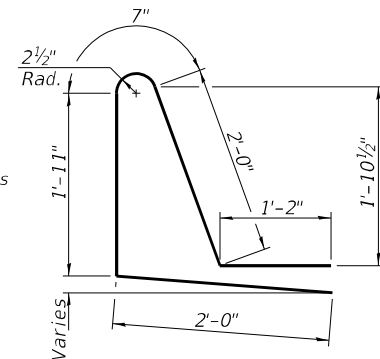
JOINT CONSTRUCTION DETAILS



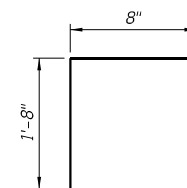
SECTION THRU PARAPET



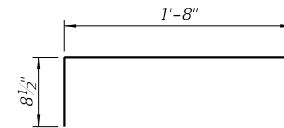
BAR d(E)



BAR d1(E)



BAR u(E)



BAR x(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	16	#5	24'-7"	—
a1(E)	16	#5	25'-7"	—
a2(E)	16	#6	6'-6"	—
d(E)	24	#5	5'-7"	⌋
d1(E)	24	#5	7'-8"	⌋
h(E)	8	#6	24'-7"	—
h1(E)	8	#6	25'-7"	—
u(E)	98	#5	4'-0"	⌋
x(E)	98	#5	2'-4 1/2"	⌋
Reinforcement Bars, Epoxy Coated			Lbs.	2580
Concrete Superstructure			Cu. Yds.	17.0

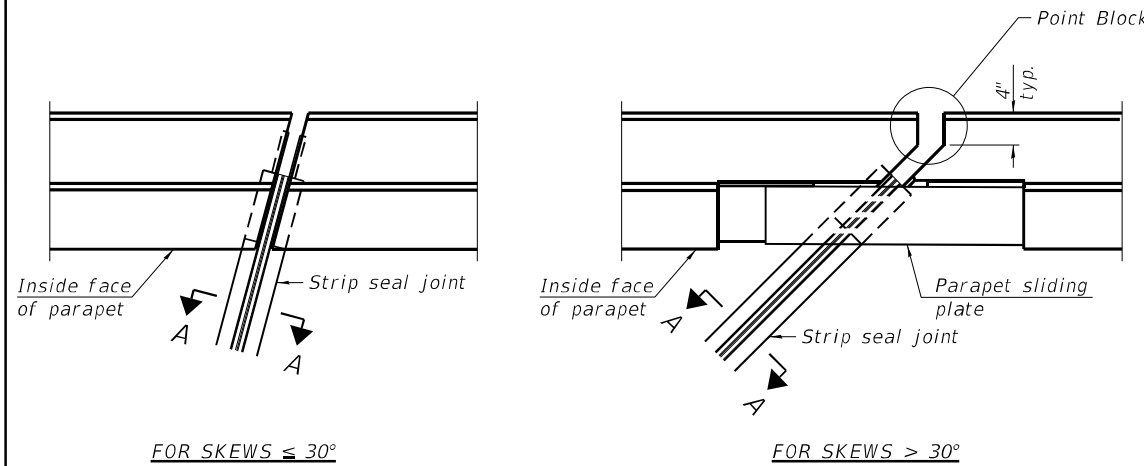
Bill of Material shown is for both joint repairs.

Note:

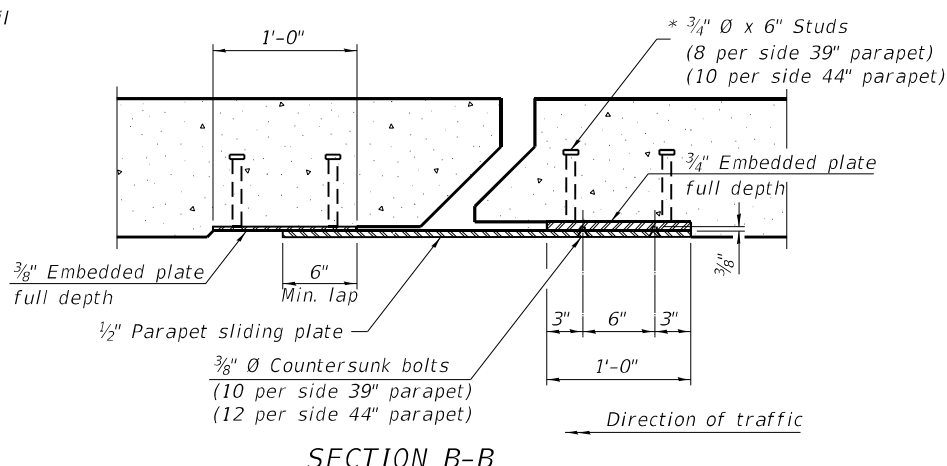
For details of Performed Joint Strip Seal Expansion Joints, see Sheet S6.

Existing reinforcement shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.

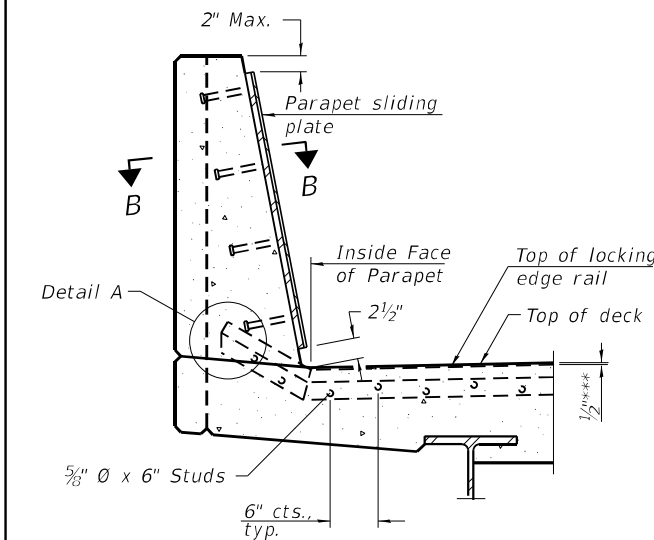




PLAN AT PARAPET

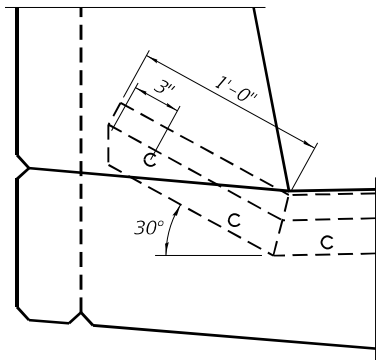


SECTION B-B

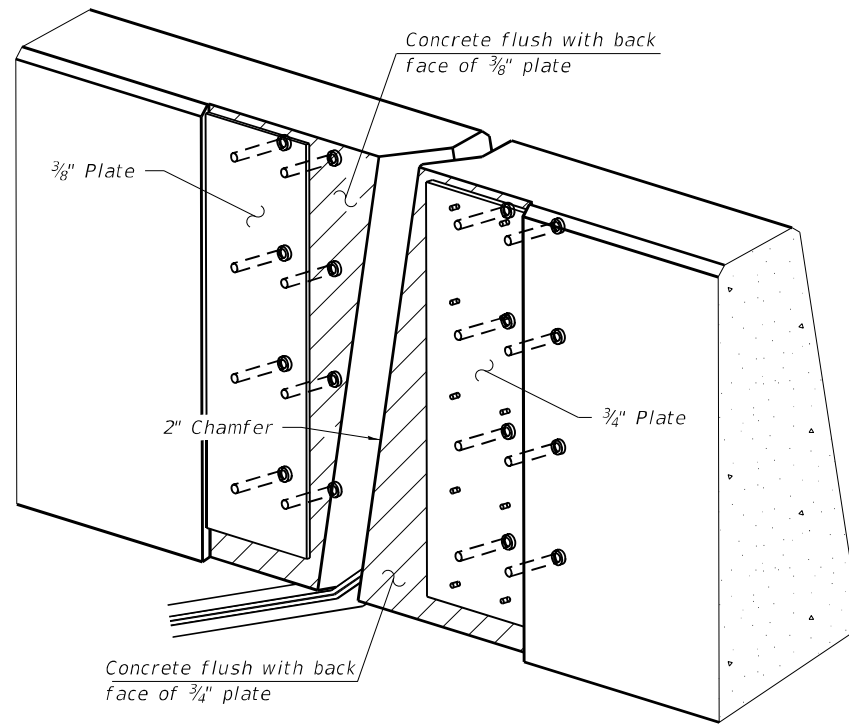


SECTION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

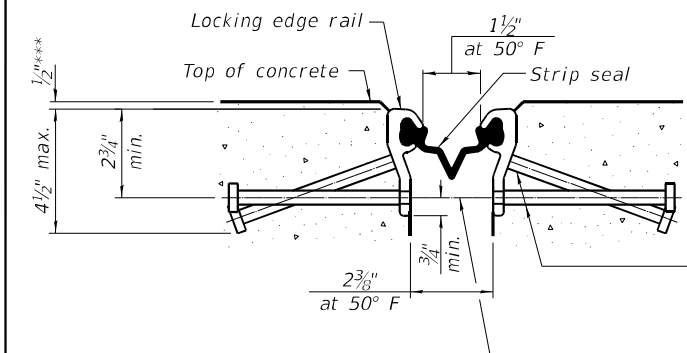


DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)



SHOWING ROLLED RAIL JOINT

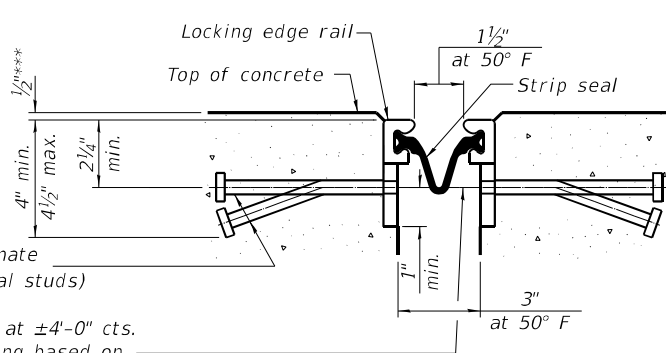
\* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

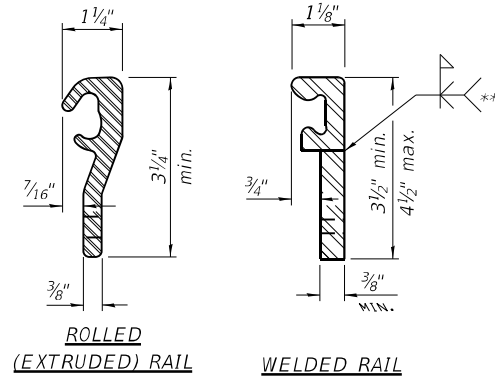
SECTION A-A

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

\*\*\* Prior to 1/4" Diamond Grinding



SHOWING WELDED RAIL JOINT



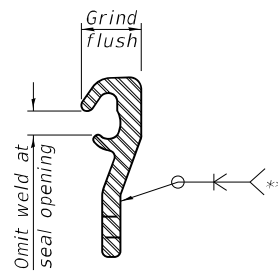
ROLLED

(EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	102

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

USER NAME	DESIGNED - AAA	REVISED -
PLOT SCALE	DRAWN - AAA	REVISED -
PLOT DATE	CHECKED - SK	REVISED -
	DATE - 11/22/2024	REVISED -

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	30	19
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME  
PLOT SCALE  
USER NAME  
SUBMIT DATE



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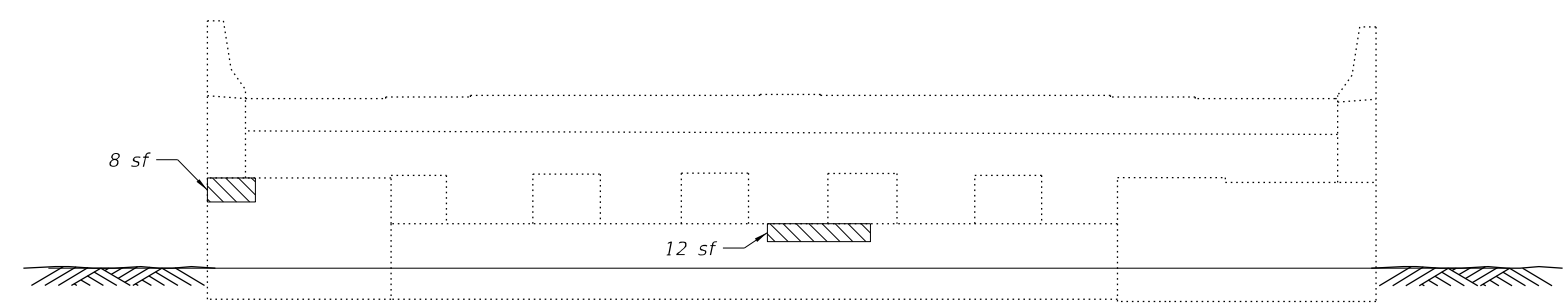
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USER NAME		DESIGNED	- AAA	REVISED	-
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PLOT DATE		DATE	- 11/22/2024	REVISED	-

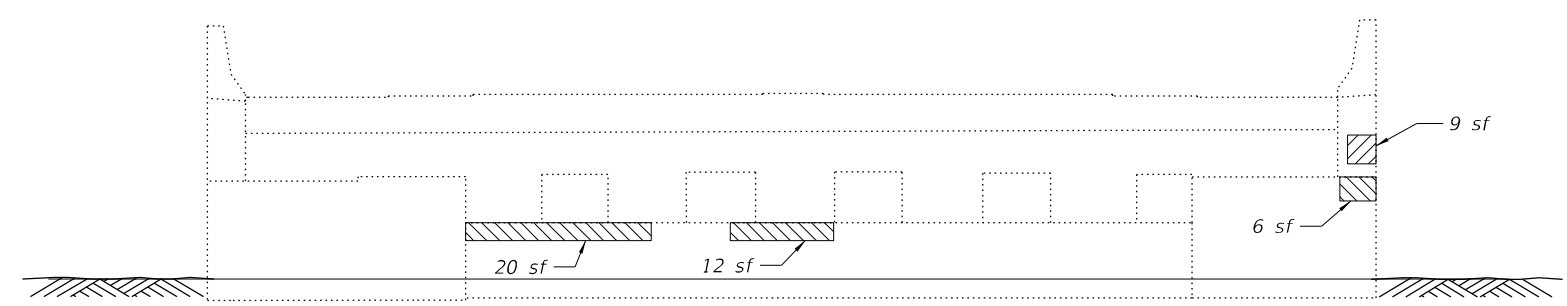
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ABUTMENT REPAIR DETAILS STRUCTURE NO. 099-0210	
SCALE: 1"=16'	SHEET NO. 57 OF 59 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	30	20
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

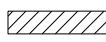


WEST ABUTMENT  
Looking East



EAST ABUTMENT  
Looking West

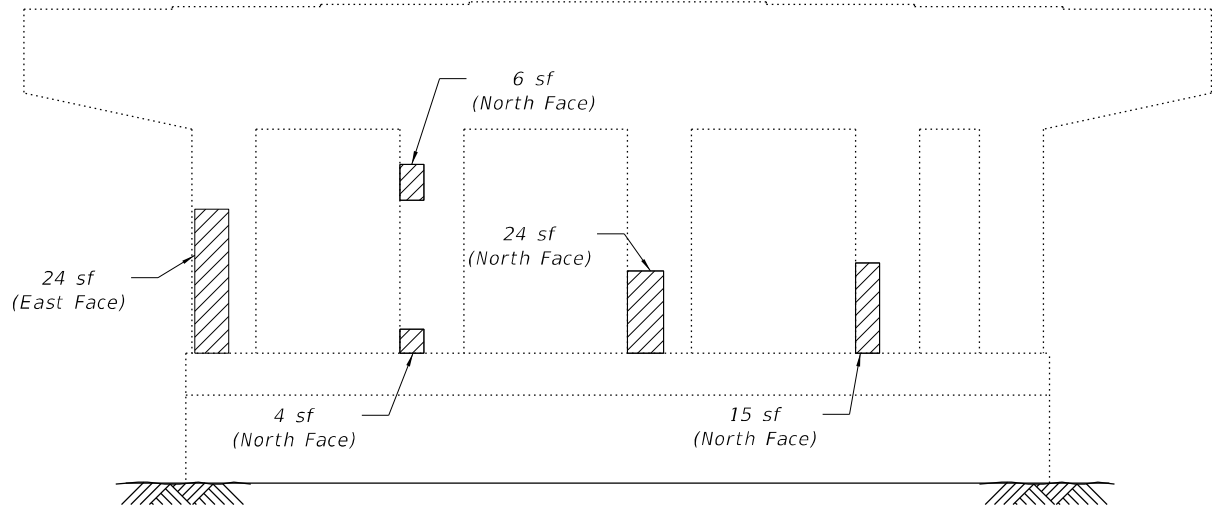
LEGEND

 Structural Repair of Concrete less than 5 Inches

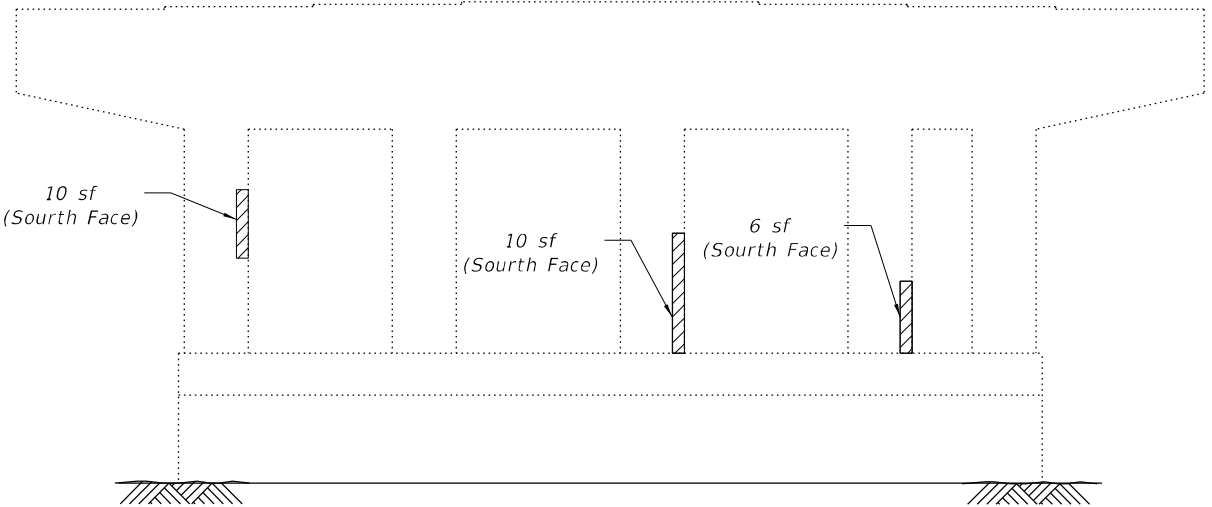
TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	67






**PIER 1**  
West Elevation



**PIER 3**  
West Elevation

**LEGEND**

 Structural Repair of Concrete less than 5 Inches

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	99

FILE NAME  
PLOT SCALE  
USER NAME

FILES  
SCALES  
SUBMITALS

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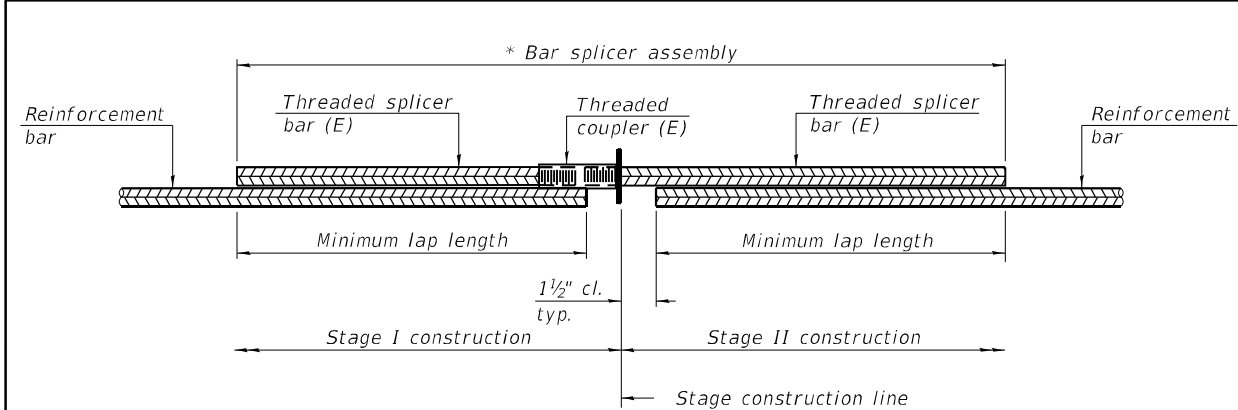
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PLOT SCALE	DRAWN - AAA	REVISED -
PLOT DATE	CHECKED - KZ	REVISED -
	DATE - 11/22/2024	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PIER REPAIR DETAILS**  
**STRUCTURE NO. 099-0210**

SCALE: N/A SHEET NO. 58 OF 59 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	28	21
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

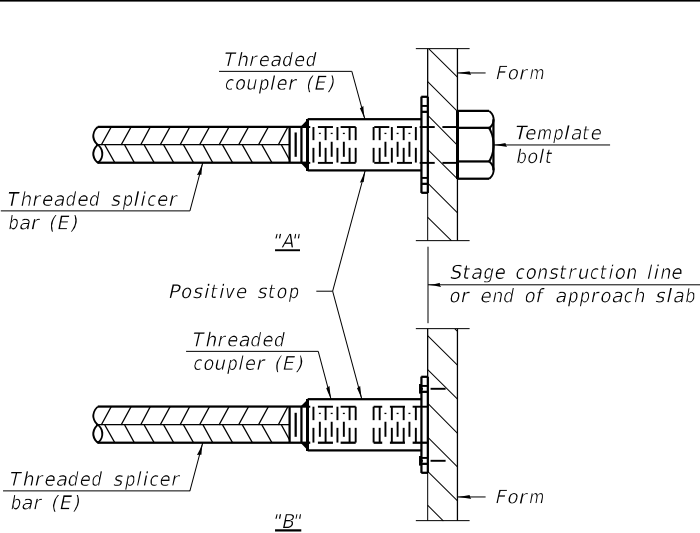


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1½" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	16	3'-4"
Abutment	#6	8	4'-0"

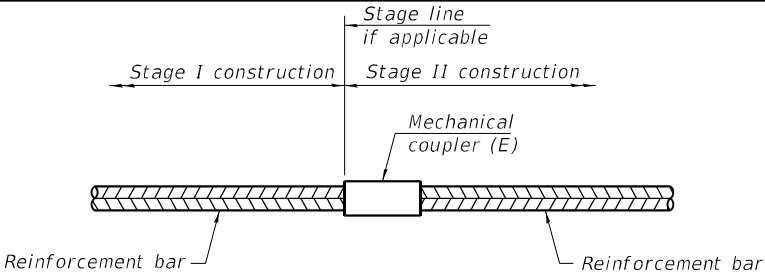


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

Notes:  
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
All reinforcement shall be lapped and tied to the splicer bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME  
PLOT SCALE  
USER NAME

BSD-1

2-1-2023

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USER NAME ■ AAA	DESIGNED -	REVISED -
	DRAWN - AAA	REVISED -
PLOT SCALE ■ 1"=2'	CHECKED - KZ	REVISED -
PLOT DATE ■	DATE - 11/22/2024	REVISED -

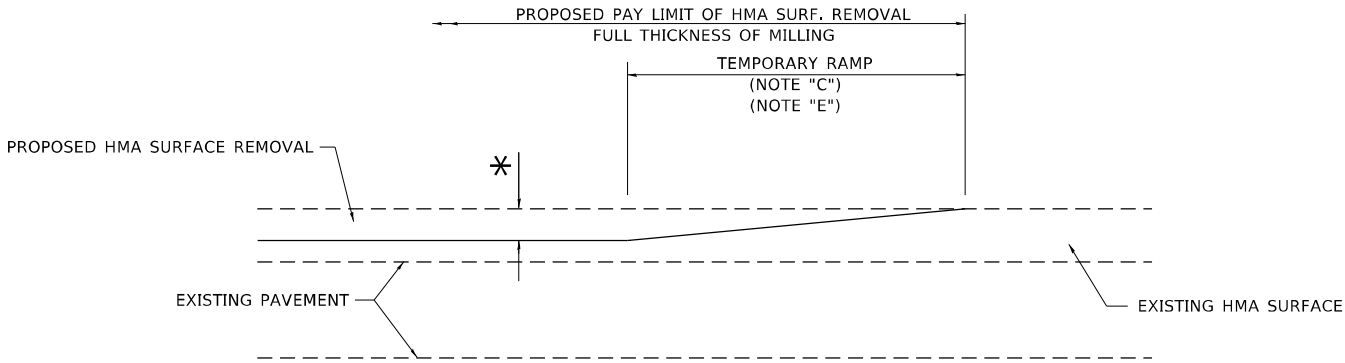
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS**  
**STRUCTURE NO. 099-0210**

SCALE: 1"=2' SHEET NO. 59 OF 59 SHEETS STA. TO STA.

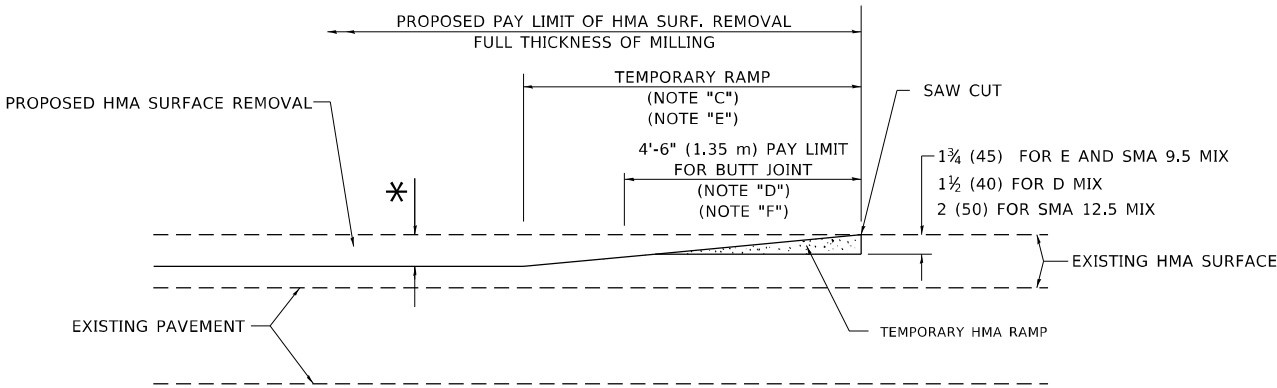
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	F.A.I. 55 22 BRIDGE REHAB	WILL	30	22
CONTRACT NO. 62R76				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

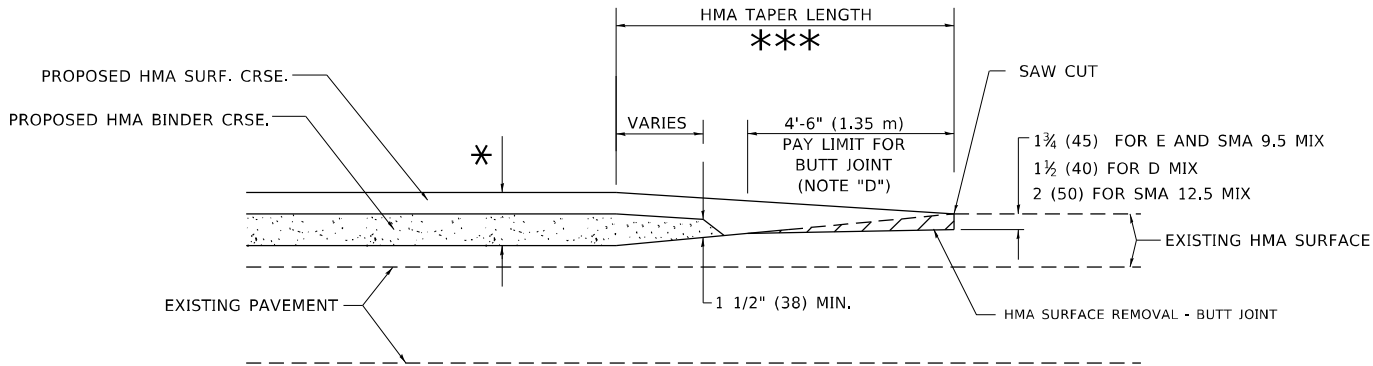
**OPTION 1**



**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

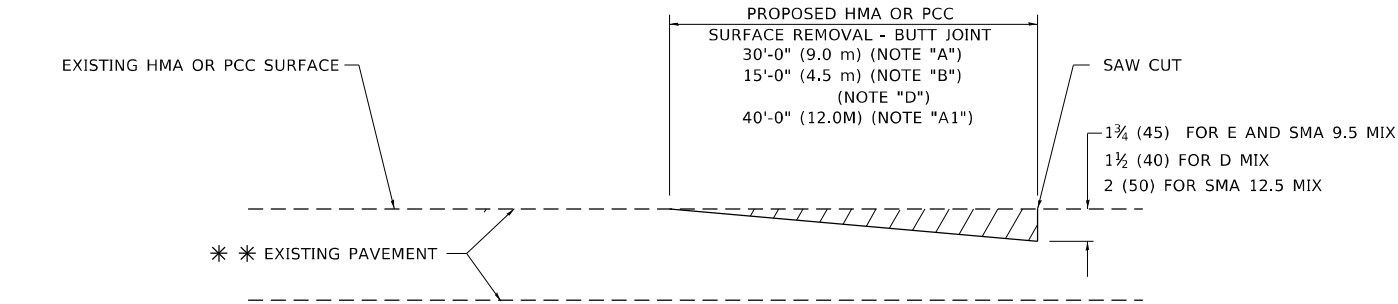
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

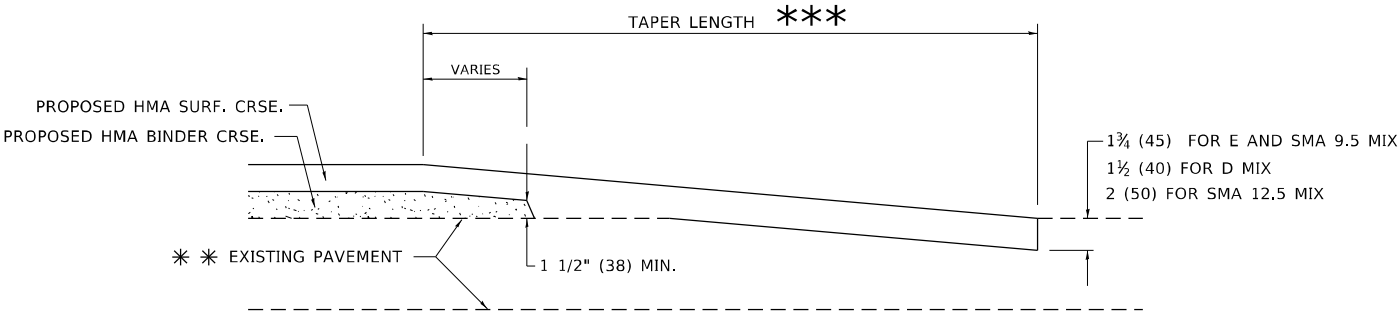


**BUTT JOINT AND  
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

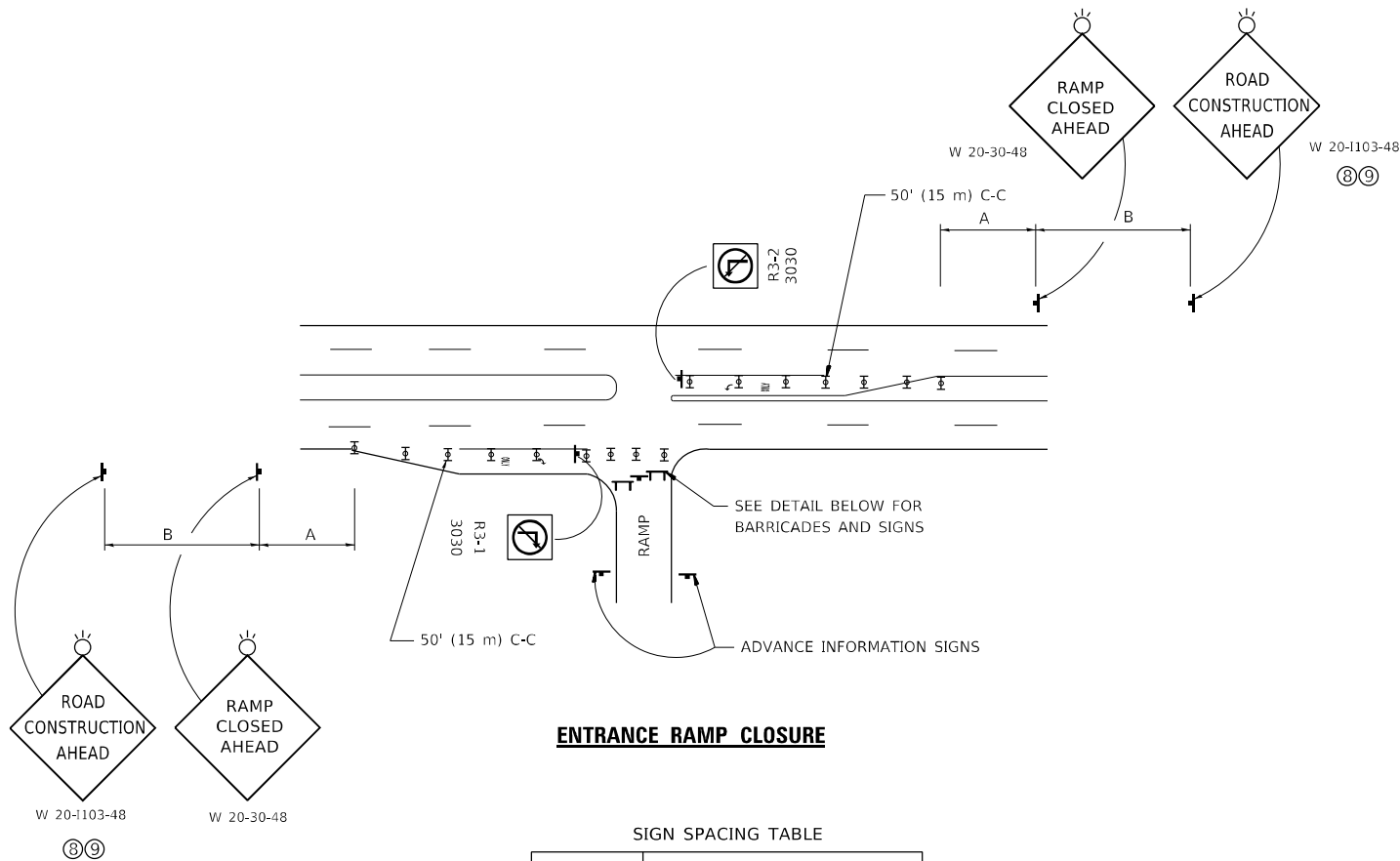
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	24
BD400-05 BD-32		CONTRACT NO. 62R76		
		ILLINOIS	FED. AID PROJECT	

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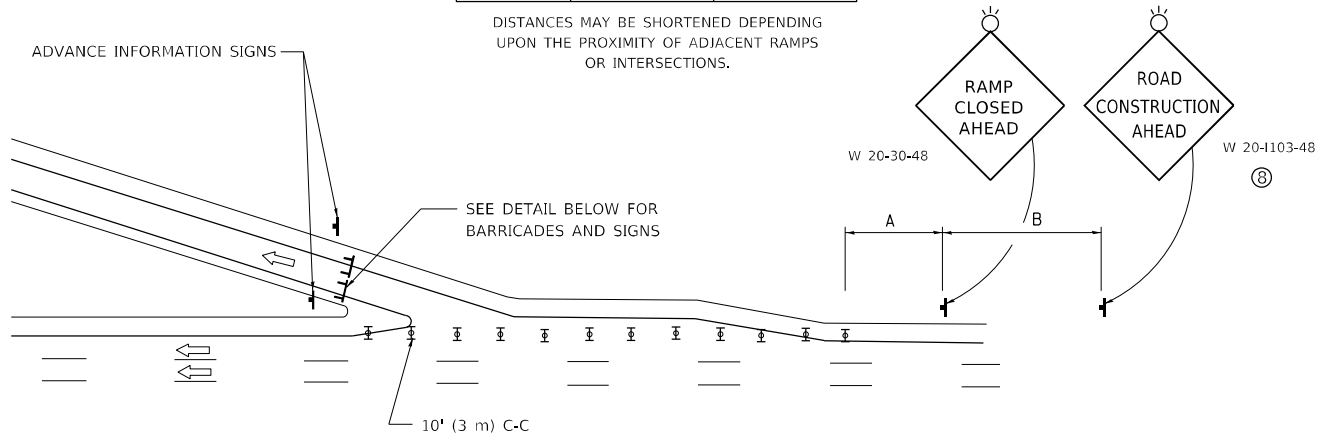


#### ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

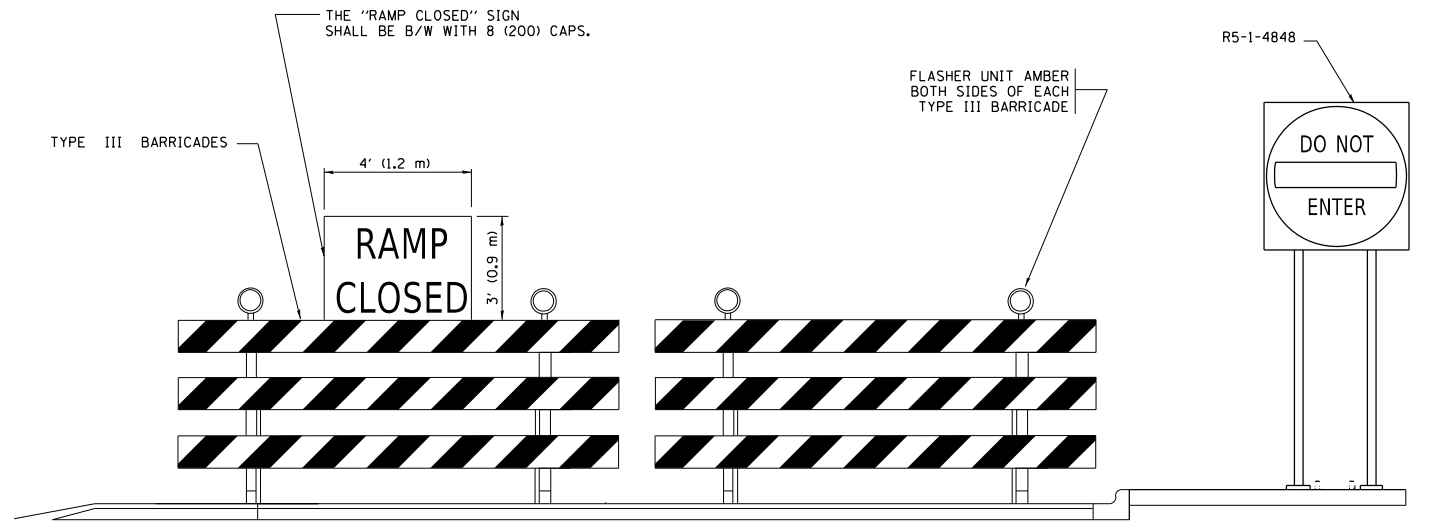
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



#### EXIT RAMP CLOSURE

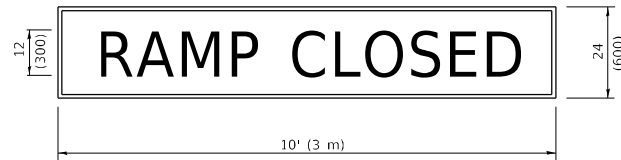
#### SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS



#### DETAIL FOR REQUIRED BARRICADES & SIGNS

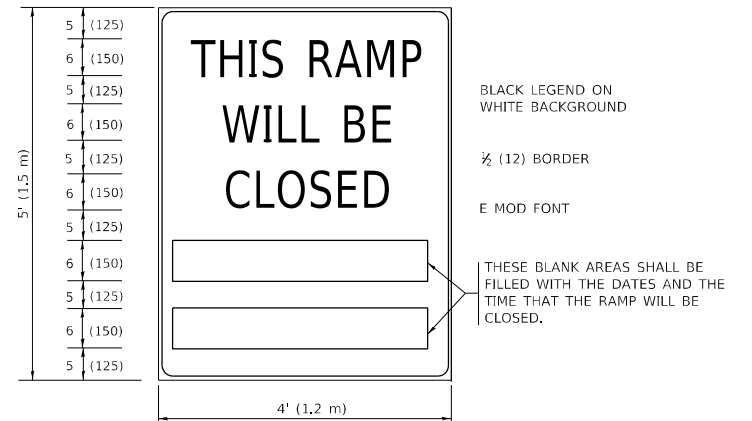
#### RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

#### RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

½ (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

#### GENERAL NOTES:

- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

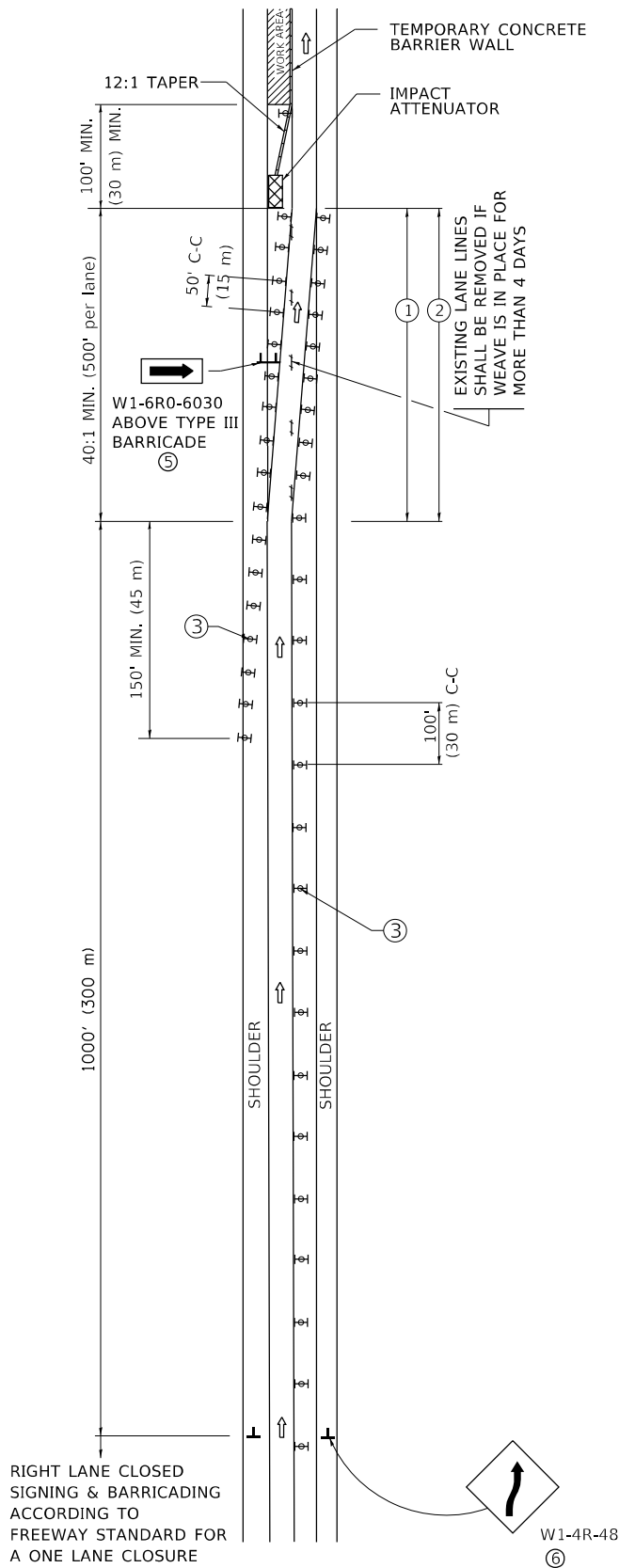
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ENTRANCE\_AND\_EXIT\_RAMP  
CLOSURE\_DETAILS

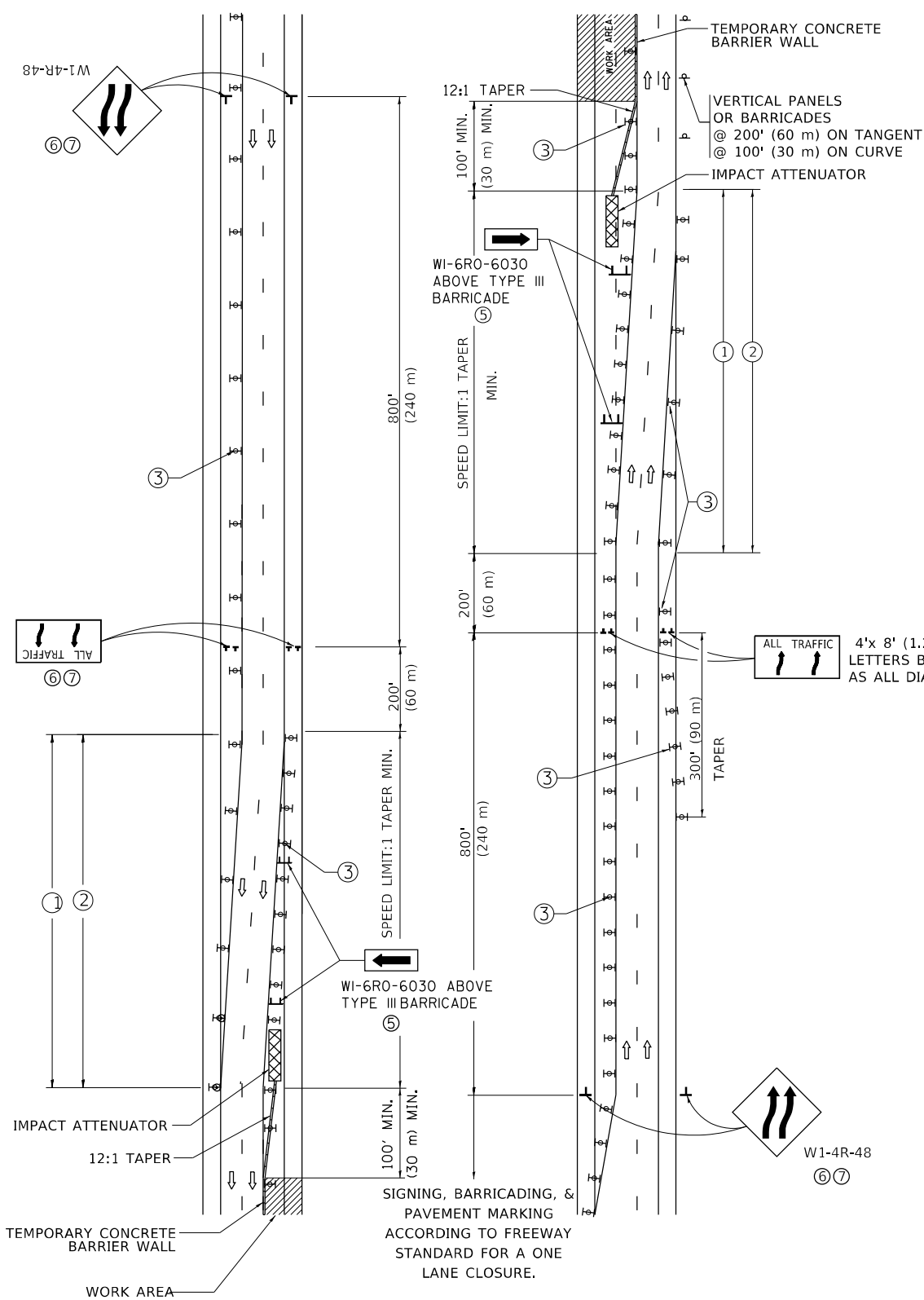
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	25
TC-08		CONTRACT NO. 62R76		
ILLINOIS		FED. AID PROJECT		

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES:

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

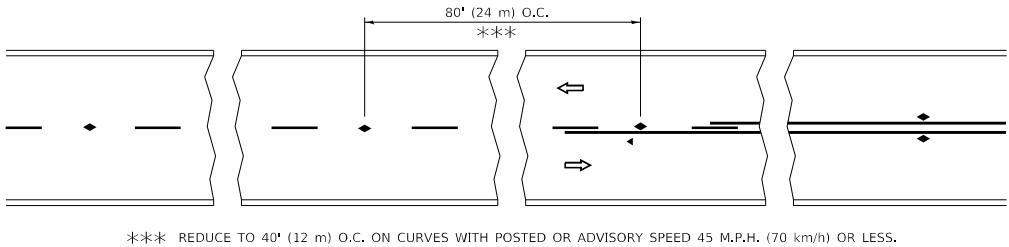
TRAFFIC CONTROL DETAILS FOR  
FREEWAY SINGLE & MULTI-LANE WEAVE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-09		CONTRACT NO. 62R76		
ILLINOIS		FED. AID PROJECT		

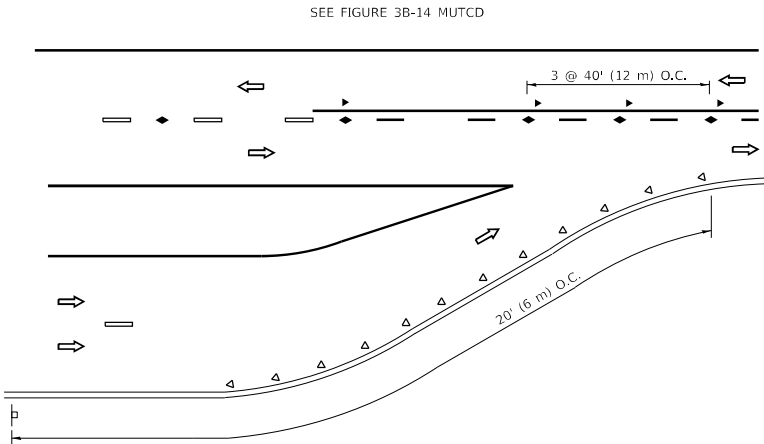
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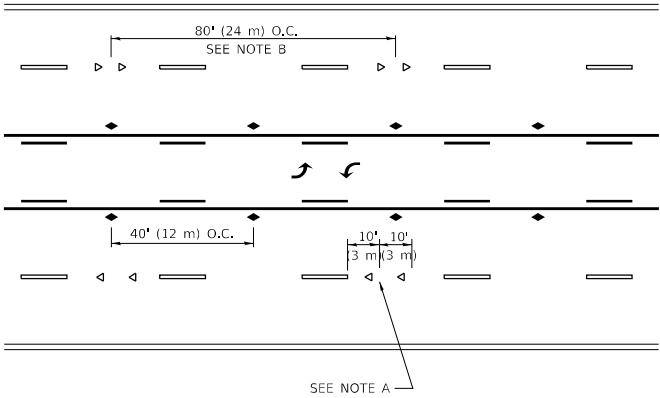
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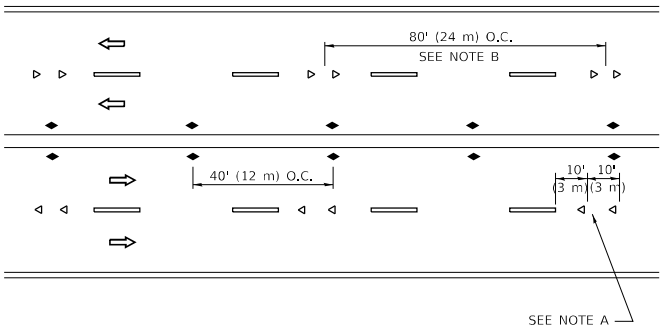
TWO-LANE/TWO-WAY



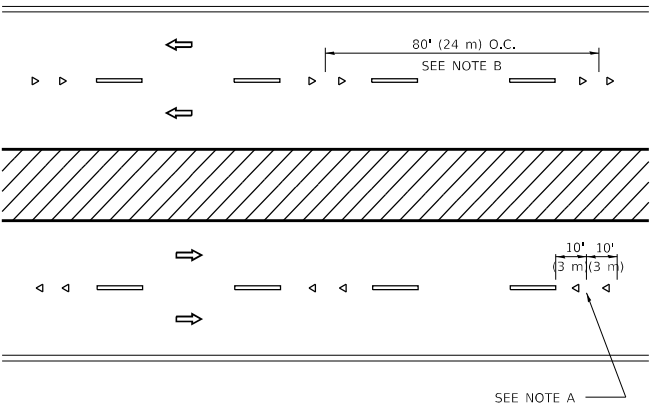
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

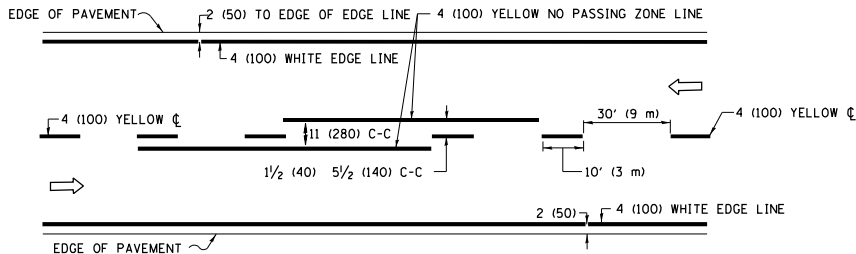
- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

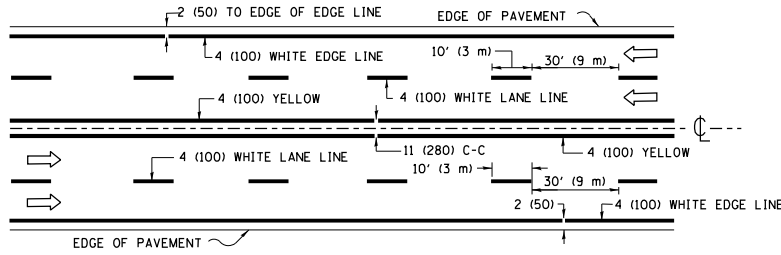
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		DRAWN -	REVISED - T. RAMMACHER 01-06-00						55	FAI 55 22 BRIDGE REHAB	WILL	31	28
	PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09		TC-11		CONTRACT NO. 62R76						
	PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	



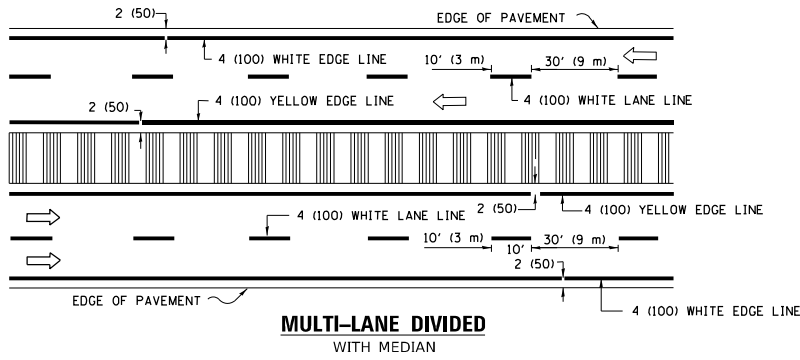
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2-LANE ROADWAY

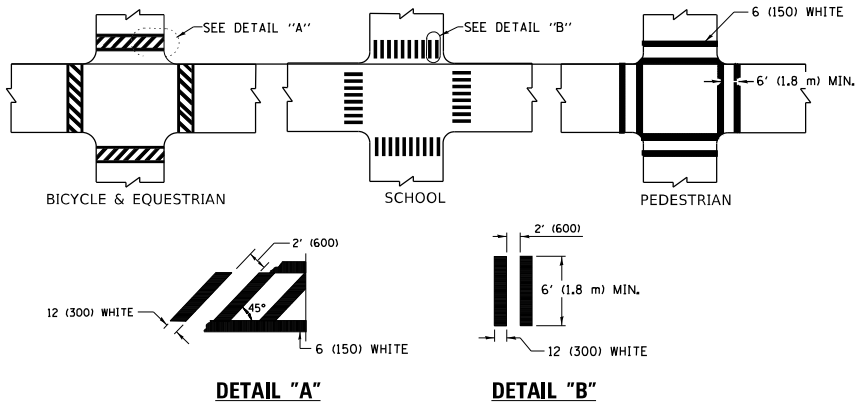


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED  
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

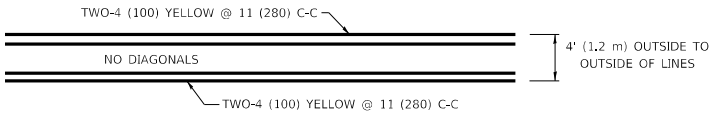


DETAIL "A"

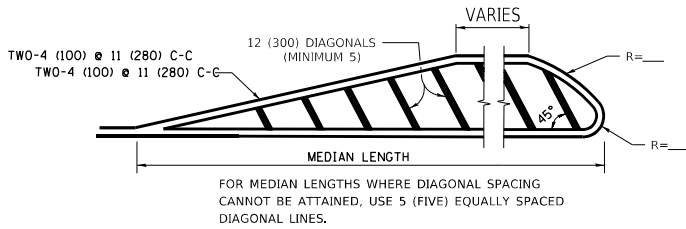
DETAIL "B"

TYPICAL CROSSWALK MARKING

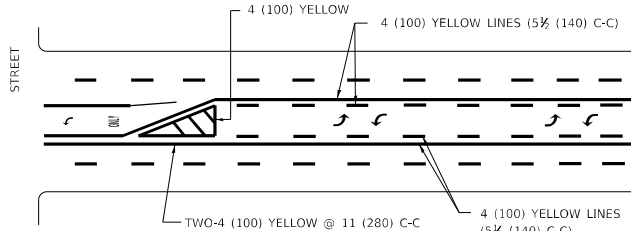
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



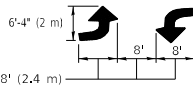
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

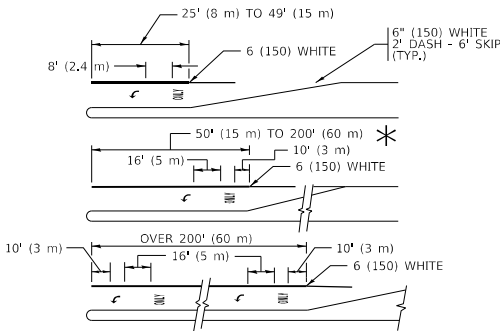


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

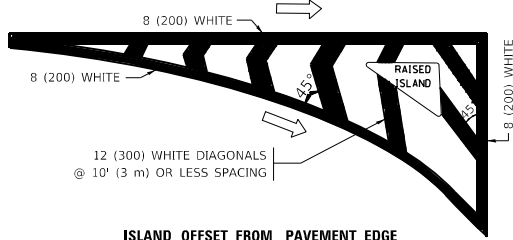
TYPICAL PAINTED MEDIAN MARKING



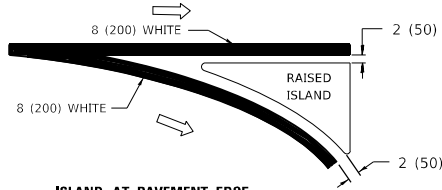
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

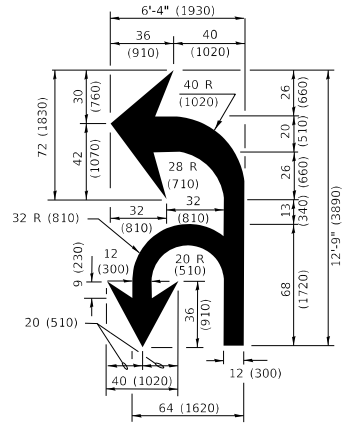


ISLAND OFFSET FROM PAVEMENT EDGE

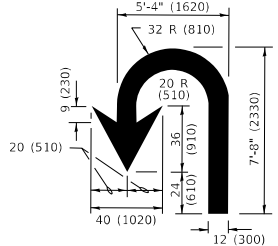


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION  
LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGTUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8' )	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem]	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

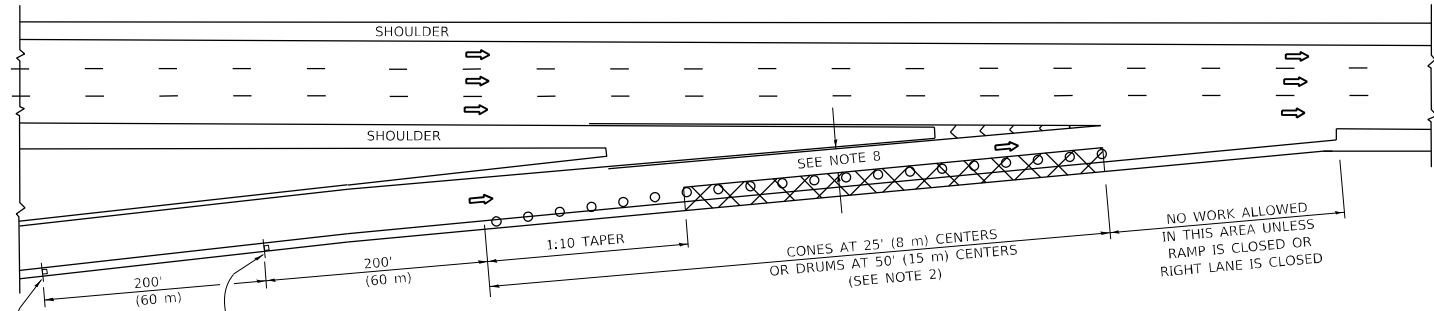
DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

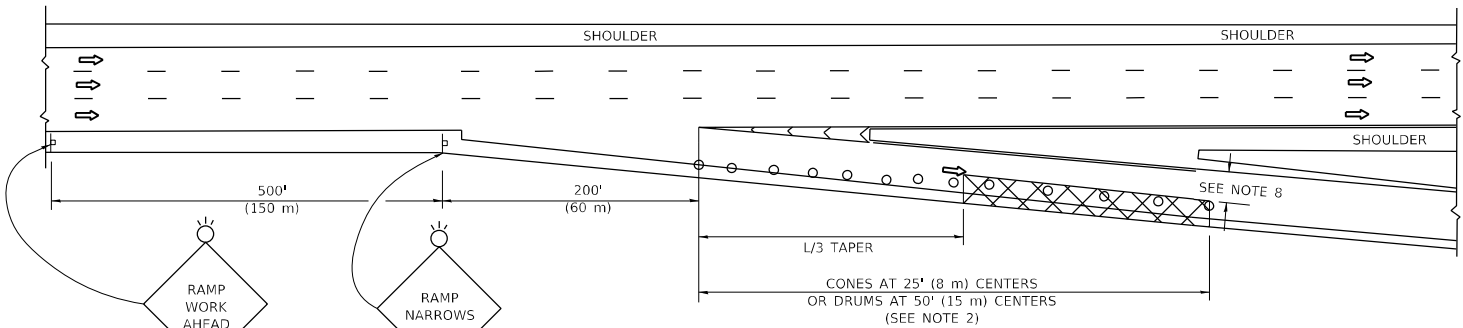
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	29
TC-13		CONTRACT NO. 62R76		
		ILLINOIS	FED. AID PROJECT	

PARTIAL RAMP CLOSURE DETAILS

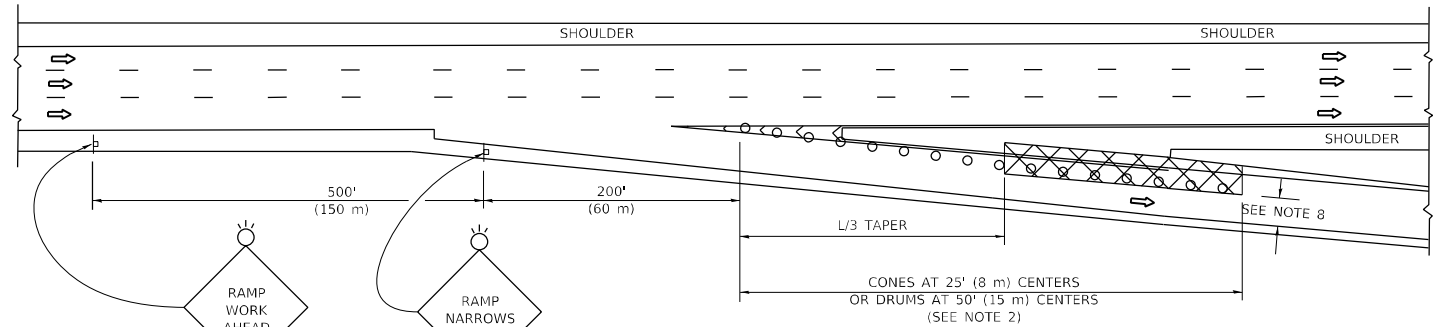
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



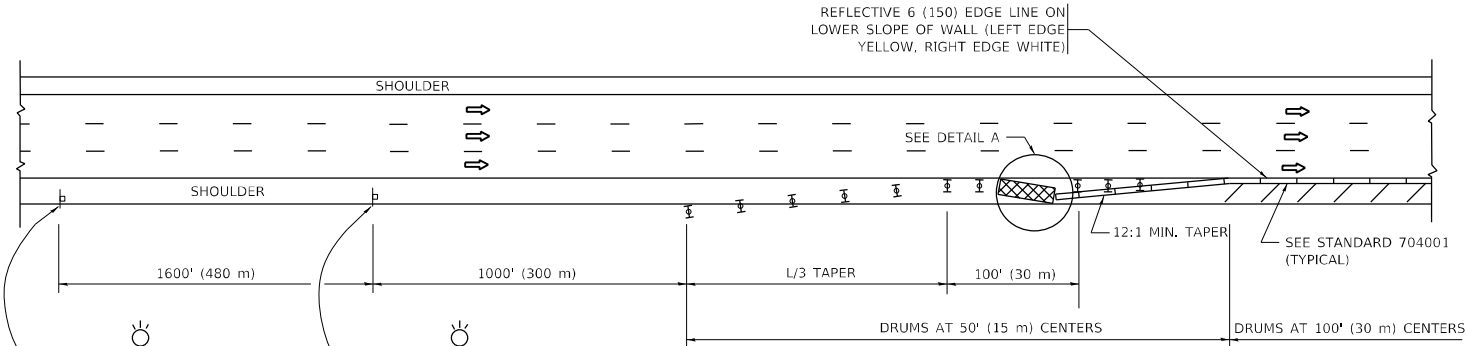
TYPICAL EXIT RAMP

SYMBOLS

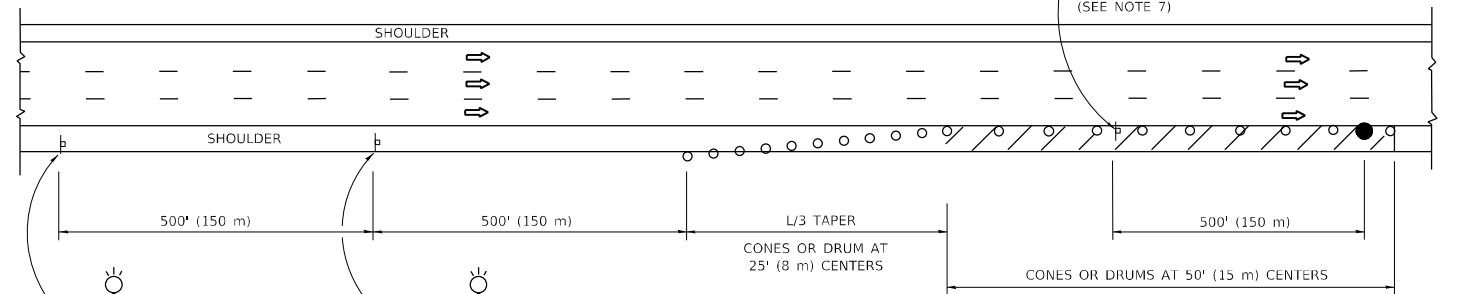
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

- THE "L" DISTANCE EQUALS:  
SPEED LIMIT  
45 mph (80 km/h)  
OR GREATER:  
W = WIDTH OF OFFSET IN FEET (METERS)  
S = NORMAL POSTED SPEED MPH (KM/H)  
FORMULAS  
METRIC ENGLISH  
L=0.65(W)(S) L=(W)(S)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES, TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

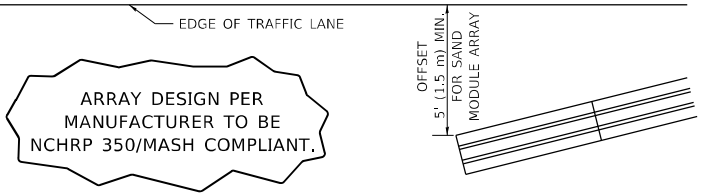


PERMANENT SHOULDER CLOSURE



TEMPORARY SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:  
a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD,  
b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.  
THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

- 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.
- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY  
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI 55 22 BRIDGE REHAB	WILL	31	30
TC-17		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

