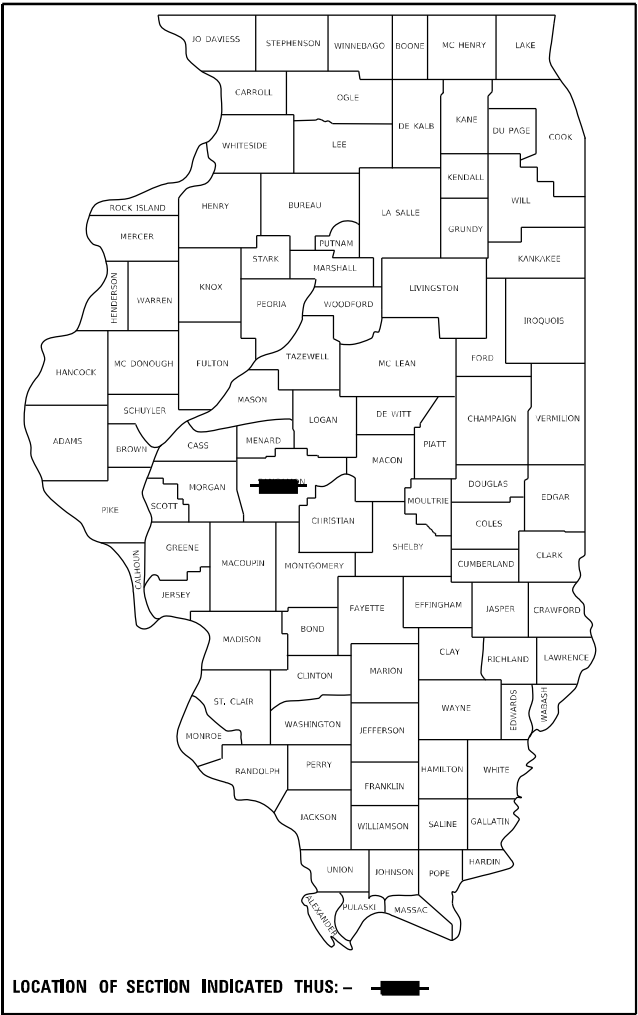


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-9-3)BP	SANGAMON	7	1
		ILLINOIS	CONTRACT NO. 72729	

D-96-017-24



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

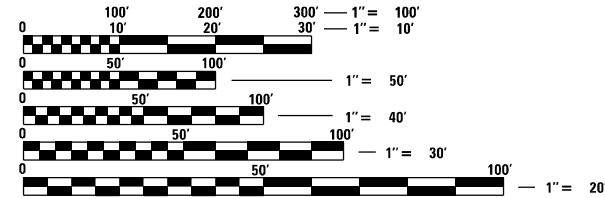
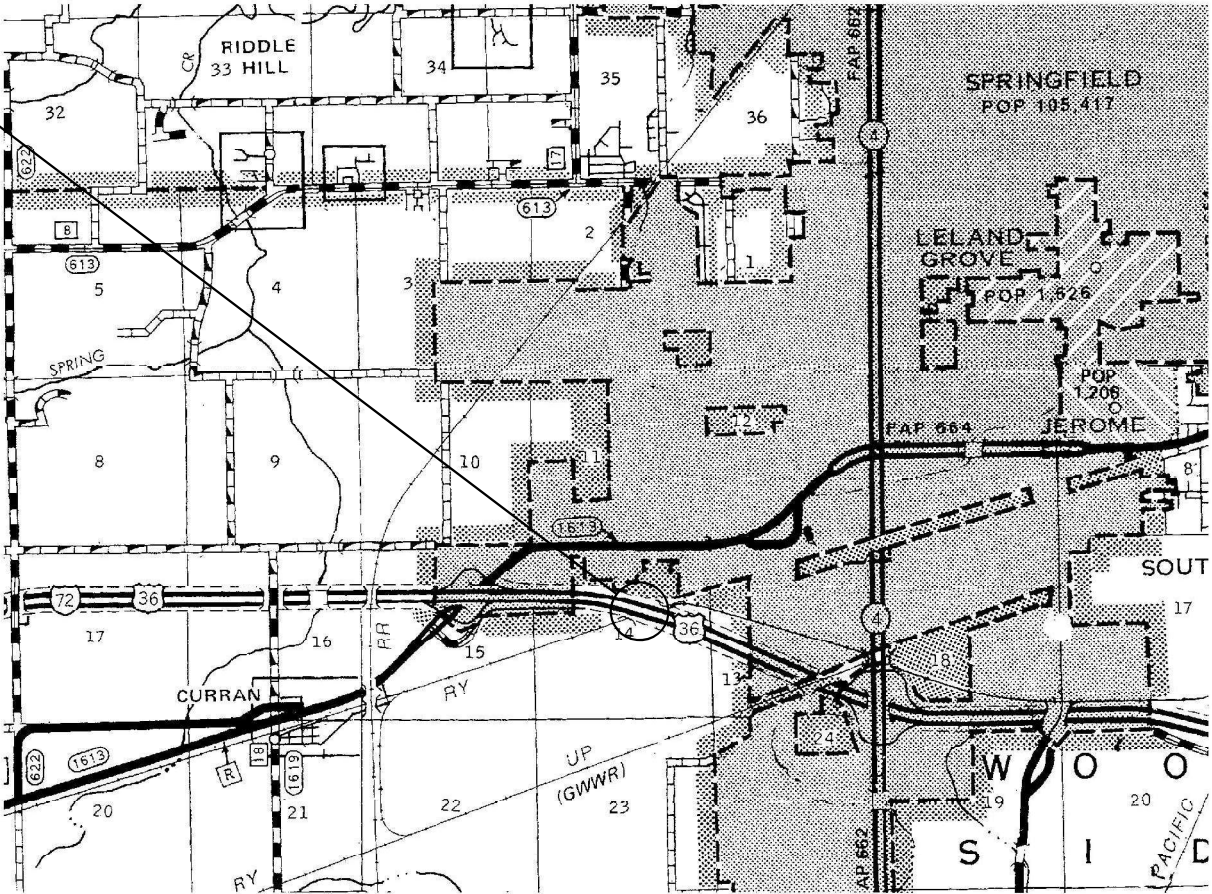
# PROPOSED HIGHWAY PLANS

FAI ROUTE 72 (I-72)  
SECTION (84-9-3)BP  
PROJECT NHPP-163E(650)  
BRIDGE PAINTING  
SANGAMON COUNTY

C-96-033-24



PROJECT LOCATION  
SN 084-0127 (EB) & 0128 (WB)  
I-72 OVER NSRR  
1.2 MI E WABASH AVE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

BRIDGE MAINTENANCE ENGINEER: BRANDON DUDLEY - (217) 785-9290

GROSS LENGTH = 224 FT. = 0.04 MILE  
NET LENGTH = 224 FT. = 0.04 MILE

CONTRACT NO. 72729

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 7 2025  
*Lora Ramsey* REGIONAL ENGINEER  
December 5 2025  
*Scott A. Etk* ENGINEER OF DESIGN AND ENVIRONMENT  
December 5 2025  
*Quarry* DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

HIGHWAY STANDARDS

INDEX OF SHEETS

- 1
- COVER SHEET
- 2
- INDEX, STANDARDS, GENERAL NOTES, & SIGNATURES
- 3
- SUMMARY OF QUANTITIES
- 4-7
- EXISTING BRIDGE PLANS, SN 084-0127 & 0128

000001-09  
001006  
701101-05  
701106-02  
701400-12  
701402-12  
701901-11  
704001-08  
782006-01

GENERAL NOTES:

1.
- WORK SHALL CONSIST OF BLASTING AND PAINTING STRUCTURAL STEEL AT LOCATIONS DESCRIBED IN THE SPECIAL PROVISIONS. CLEANING AND PAINTING OF THE EXISTING STRUCTURAL STEEL SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS FOR "CLEANING AND PAINTING EXISTING STEEL STRUCTURES". ALL AREAS TO BE PAINTED SHALL BE CLEANED PER NEAR WHITE BLAST CLEANING PER SSPC SP 10. ALL EXISTING STEEL CLEANED SHALL BE PAINTED ACCORDING TO THE REQUIREMENTS OF PAINT SYSTEM 1 - OZ/E/U. THE COLOR OF THE FINAL FINISH COATS SHALL BE AS DESCRIBED IN THE SPECIAL PROVISIONS.
- 2
- THE USE OF AIR MONITORS WILL BE REQUIRED AT LOCATIONS AS CALLED OUT IN THE SPECIAL PROVISIONS.
3.
- THE SSPC-QP-1 AND SSPC-QP2 PAINTING CONTRACTOR CERTIFICATIONS WILL BE REQUIRED.
4.
- CARE SHALL BE TAKEN NOT TO DAMAGE RUBBER BEARING OR JOINT COMPONENTS DURING BLASTING AND CLEANING OPERATIONS. ANY DAMAGE TO THESE COMPONENTS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. RUBBER COMPONENTS SHALL NOT BE PAINTED.
5.
- UPON COMPLETION OF PAINTING OPERATIONS, THE CONTRACTOR SHALL REMOVE ALL DEBRIS FROM PIER OR ABUTMENT CAPS UPON WHICH PAINTING OPERATIONS TOOK PLACE. FINAL CLEANUP SHALL BE CONSIDERED INCIDENTAL TO THE PAINT PAY ITEM FOR THE RESPECTIVE LOCATION. THE ENGINEER SHALL HAVE THE RIGHT TO WITHHOLD PAYMENT UNTIL SATISFACTORY CLEANUP IS ACHIEVED.
6.
- One Call services do not locate buried railroad signal and communications lines. The contractor shall contact the Railroad's representative two days in advance of work at those places where excavation, pile driving, or heavy loads may damage the Railroad's underground facilities. Upon request from the Contractor or Sponsor, Railroad forces will locate and paint mark or flag the Railroad's underground facilities. The Contractor shall avoid excavation or other disturbance of these facilities. If disturbance or excavation is required near a buried Railroad facility, the Contractor shall coordinate with the Railroad to have the facility potholed manually with careful hand excavation. The facility shall be protected by the Contractor during the course of the disturbance under the supervision and direction of the Railroad's representative. (See NS Public Projects Manual, Appendix E, Section 3.D)

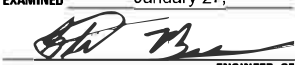
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
DISTRICT 6

EXAMINED

January 27,

20

25




ENGINEER OF OPERATIONS

EXAMINED

January 27,

20

25



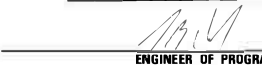
ENGINEER OF PROJECT IMPLEMENTATION

EXAMINED

January 27,

20

25



ENGINEER OF PROGRAM DEVELOPMENT

REV - MS

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		DRAWN -	REVISED -						72	(84-9-3)BP	SANGAMON	7	2
	PLOT SCALE = 100.0000" / in.	CHECKED -	REVISED -						CONTRACT NO. 72729				
	PLOT DATE = 8/20/2025	DATE -	REVISED -		ILLINOIS   FED. AID PROJECT								
						SCALE:	SHEET	OF	SHEETS	STA.	TO STA.		

MODEL Default  
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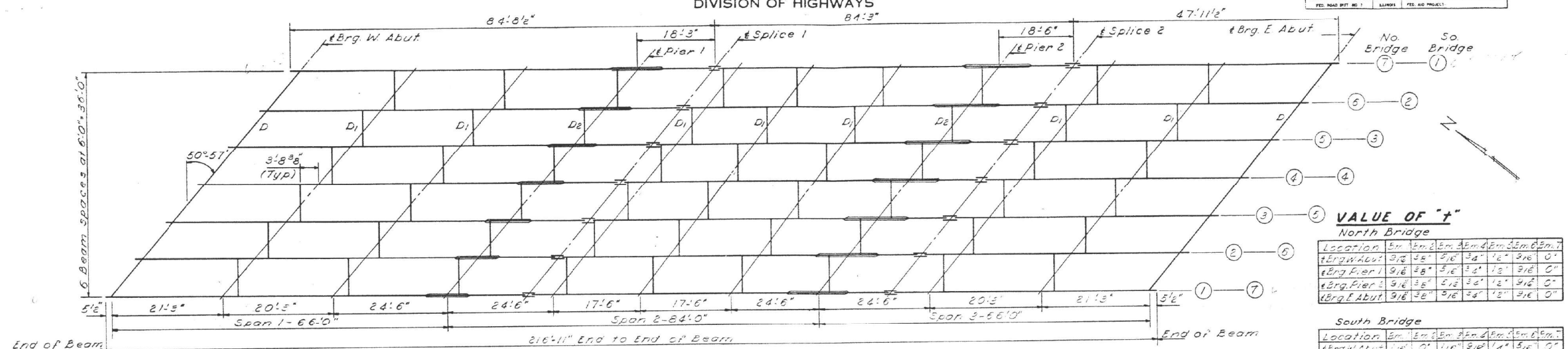
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				NHPP 90/10
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	SN 084-0127 & 0128
				0047-URBAN
				SANGAMON
67100100	MOBILIZATION	L SUM	1	1
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	2	2
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	14	14
70400100	TEMPORARY CONCRETE BARRIER	FOOT	800	800
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
Z0007101	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 1	L SUM	1	1
Z0007102	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO. 2	L SUM	1	1
Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	L SUM	1	1
Z0010502	CLEANING AND PAINTING STEEL BRIDGE NO. 2	L SUM	1	1
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1





STATE OF ILLINOIS  
DEPARTMENT OF PUBLIC WORKS & BUILDINGS  
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET TOTAL
196	IVB	SANGAMON	50	13
SHEET NO. 6				



VALUE OF "t"									
North Bridge									
Location	Em.	Em.	Em.	Em.	Em.	Em.	Em.	Em.	Em.
Brig. W. Abut.	31.6	45.0	31.6	34.0	12.0	31.6	0.0		
Brig. Pier 1	91.6	8.0	31.6	34.0	12.0	31.6	0.0		
Brig. Pier 2	91.6	35.0	31.6	34.0	12.0	31.6	0.0		
Brig. E. Abut.	91.6	35.0	31.6	34.0	12.0	31.6	0.0		

South Bridge									
Location	Em.	Em.	Em.	Em.	Em.	Em.	Em.	Em.	Em.
Brig. W. Abut.	11.0	0.0	11.0	91.6	14.0	51.6	0.0		
Brig. Pier 1	11.0	0.0	11.0	91.6	14.0	51.6	0.0		
Brig. Pier 2	11.0	0.0	11.0	91.6	14.0	51.6	0.0		
Brig. E. Abut.	11.0	0.0	11.0	91.6	14.0	51.6	0.0		

STRESS TABLE

Table of Moments & Reactions - Int. Bms.									
Moments					Reactions				
	4Sp1	Pier1/2	5Sp2	Wt. Abut. Pier1/2					
D.L.	257	557	252	21.9	77.9				
L.L.	430	402	430	36.4	48.1				
Imp.	112	101	103	9.5	15.8				
Total	799	1060	795	67.8	141.8				

Bridge is symm. about span 2.  
Moments are in ft kips.  
Reactions are in kips.

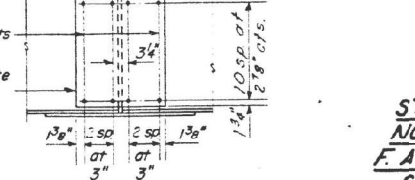
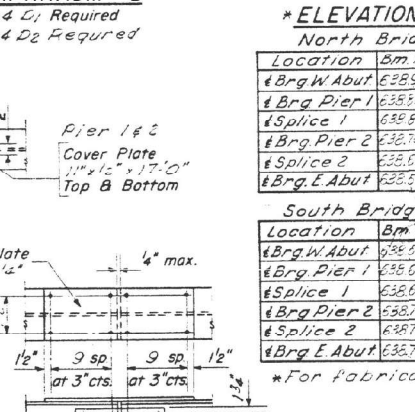
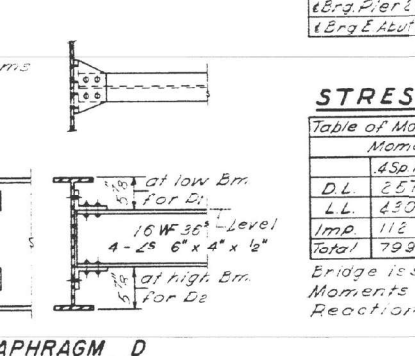
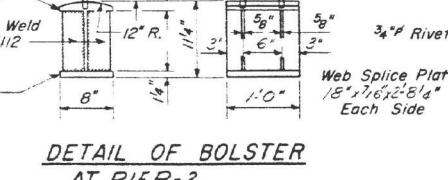
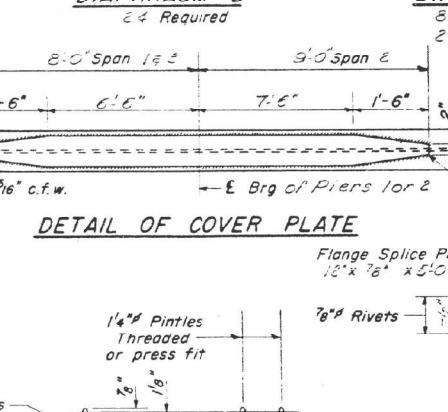
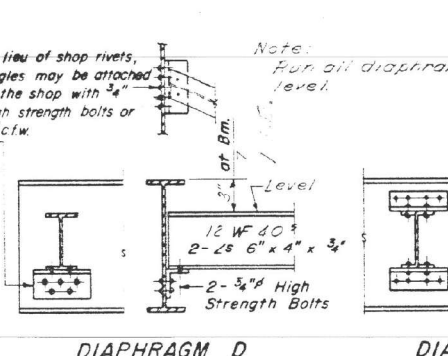
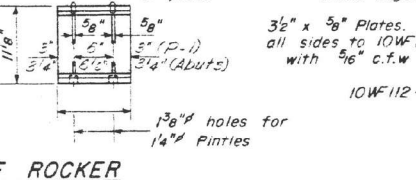
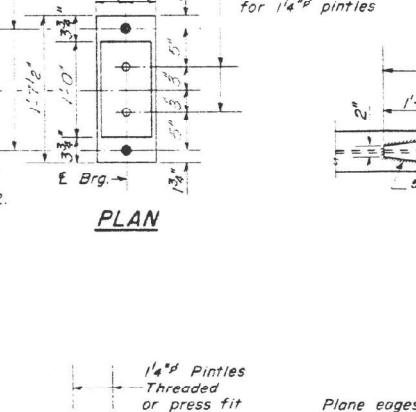
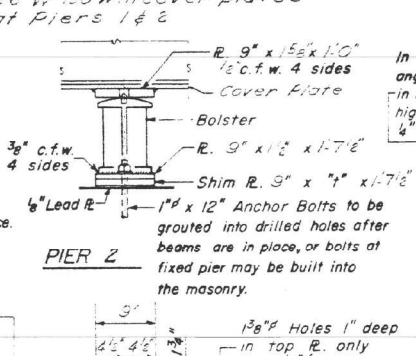
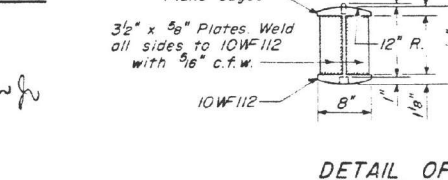
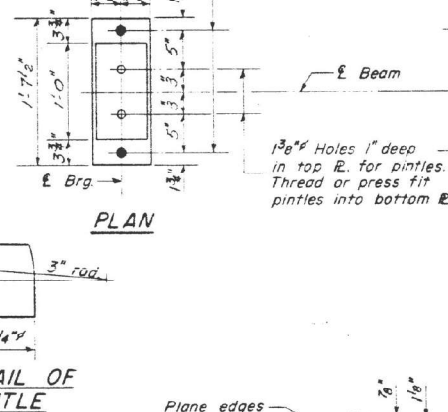
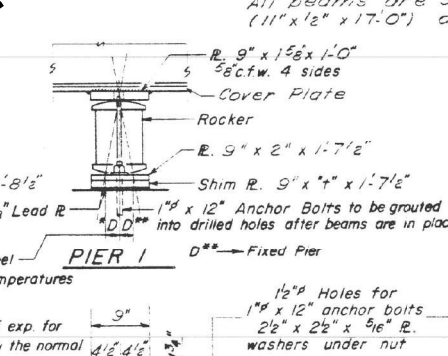
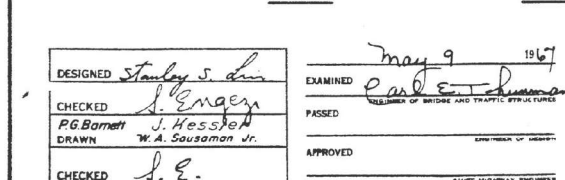
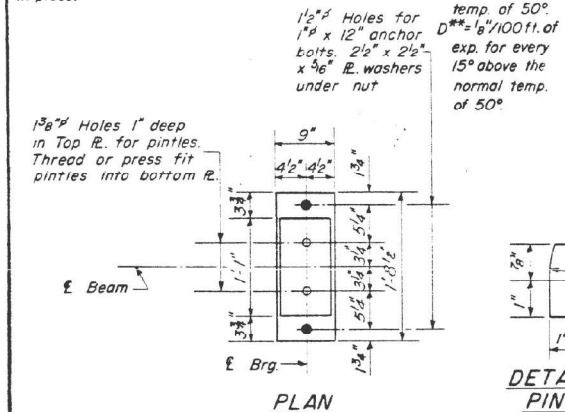
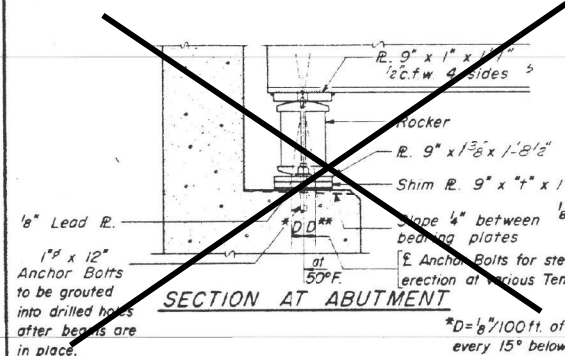
\*ELEVATION TOP OF WF

North Bridge									
Location	Bm 1	Bm 2	Bm 3	Bm 4	Bm 5	Bm 6	Bm 7		
Brig. W. Abut.	638.94	638.06	639.15	639.10	639.07	638.94	638.77		
Brig. Pier 1	638.98	639.00	639.00	639.14	639.01	638.88	638.71		
Splice 1	638.80	638.98	639.06	639.12	638.99	638.86	638.69		
Brig. Pier 2	638.70	638.61	638.91	638.90	638.82	638.70	638.53		
Splice 2	638.65	638.77	638.87	638.91	638.78	638.65	638.48		
Brig. E. Abut.	638.52	638.64	638.73	638.78	638.65	638.52	638.35		

South Bridge									
Location	Bm 1	Bm 2	Bm 3	Bm 4	Bm 5	Bm 6	Bm 7		
Brig. W. Abut.	638.08	638.08	638.94	639.00	638.79	638.66			
Brig. Pier 1	638.00	638.81	638.94	639.07	639.03	638.92	638.79		
Splice 1	638.69	638.84	638.97	639.10	639.06	638.95	638.82		
Brig. Pier 2	638.72	638.67	639.00	639.13	639.09	638.98	638.85		
Splice 2	638.73	638.68	639.01	639.14	639.10	638.99	638.86		
Brig. E. Abut.	638.74	638.69	639.03	639.15	639.11	639.05	638.92		

\*For Fabrication only

STRUCTURAL STEEL  
NORTH & SOUTH BRIDGES  
F.A. RT. 196 SEC. IVB  
SANGAMON COUNTY  
STATION 424+68.65



I-2-C 7-2-62 Rev. 11-9-62 Rev. 8-16-63 Rev. 12-10-63

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS, SN 084-0127 & 0128  
(FOR INFORMATION ONLY)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-9-3)BP	SANGAMON	7	5
CONTRACT NO. 72729				

ILLINOIS FED. AID PROJECT

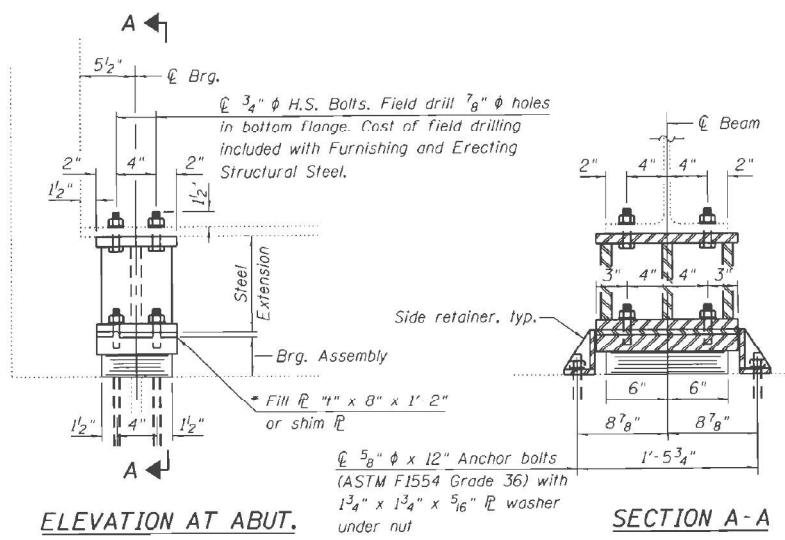
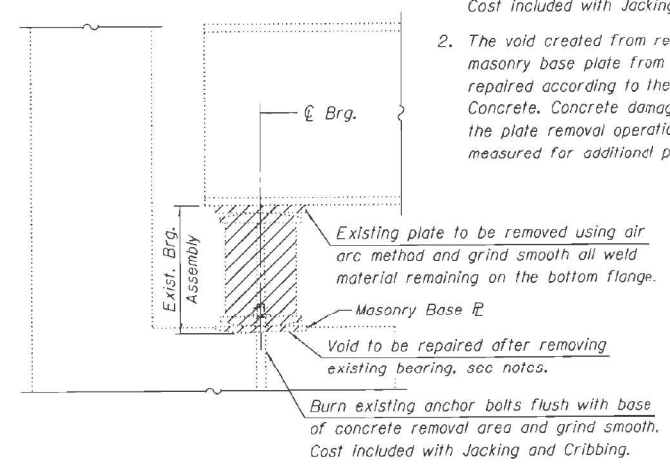


TABLE OF "I" DIMENSIONS

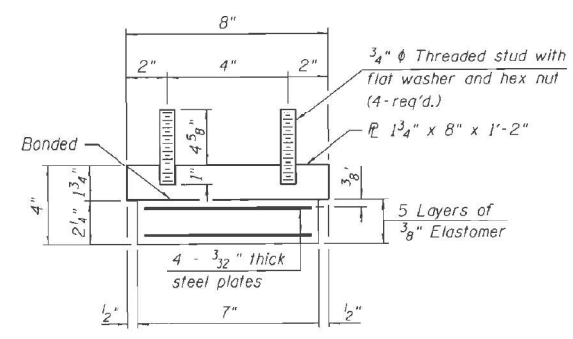
Beam No.	084-0128 (WB)	Beam No.	084-0127 (EB)
1	1 1/16"	8	1 1/16"
2	1 5/8"	9	-
3	1 3/4"	10	1 1/16"
4	1 3/16"	11	1 5/8"
5	1 5/16"	12	5/8"
6	1 9/16"	13	1 3/8"
7	1 1/16"	14	5/16"

The fill R thicknesses shown above are based on survey data to match the height of the existing bearing assemblies. Prior to ordering any material, the Contractor shall field verify all existing bearing heights and required fill plate thicknesses. The Contractor may adjust the height of the steel extensions in lieu of providing the above fill plates.

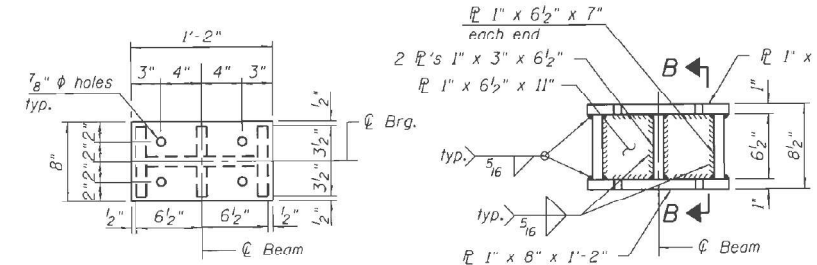


- Notes:
- Hatched area indicates removal of existing bearing assembly. Cost included with Jacking and Cribbing. See Special Provision.
  - The void created from removing the existing 1 3/8" x 9" x 1'-8 1/2" masonry base plate from the existing abutment seat shall be repaired according to the requirements for Structural Repair of Concrete. Concrete damage adjacent to the existing plate, caused by the plate removal operations and requiring repair will not be measured for additional payment.

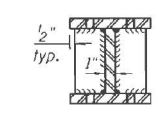
EXISTING BEARING ASSEMBLY REMOVAL DETAIL



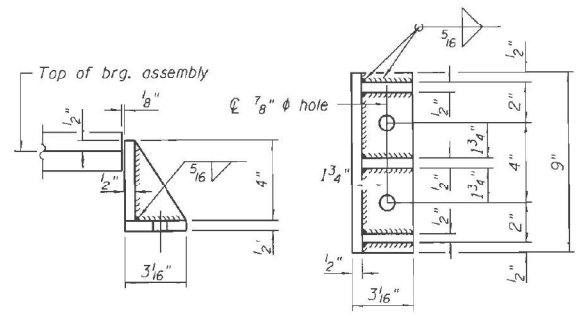
BEARING ASSEMBLY  
Shim plates shall not be placed under Bearing Assembly.



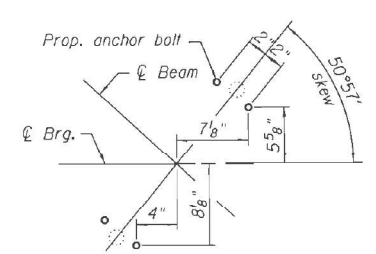
STEEL BEARING EXTENSION



SECTION B-B



SIDE RETAINER  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates. (28 required)



ANCHOR BOLT LAYOUT

(Shown for information only. It is recommended that the Contractor set the side retainers in place and use the holes in the side retainers to locate the anchor bolts on the abutment.)

JACK AND REMOVE EXISTING BEARINGS PROCEDURE

- The Contractor shall submit for approval by the Engineer, plans for jacking and removing existing bearings prior to commencing any related work. See Jacking and Cribbing Special Provisions.
- Jacking and removing existing bearings shall be done in stages after existing deck removal is completed.
- The Maximum Dead Load Reaction with deck removed (per bearing) at each abutment is 4.5 kips. Minimum jack capacity is 7 kips.
- The existing abutment seats shall be repaired, the new bearings shall be in place, and the jacks shall be lowered prior to forming and pouring the new deck.

- Notes:
- Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
  - Any damage to the concrete repair area adjacent to the proposed bearing caused from drilling anchor bolts shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost to the Department.
  - Side retainers and other steel members required for the elastomeric bearing assembly, except steel bearing extension, fill plates, and bolts in bottom flange shall be included in the cost of Elastomeric Bearing Assembly, Type I. Steel bearing extensions, fill plates, and bolts in bottom flange are included in the quantity for Furnishing and Erecting Structural Steel.
  - Two 1/8" adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Furnishing and Erecting Structural Steel.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	14
Anchor Bolts, 5/8"	Each	56
Jacking and Cribbing, Location No. 1	L.Sum	0.5
Furnishing and Erecting Structural Steel	Pound	2,410
Structural Repair of Concrete (Depth Equal to or less than 5 inches)	Sq. Ft.	18.0

Design firm: **whks**  
engineers + planners + land surveyors

1/REFR NAME : dtheberling  
MODEL : 0840127-28-72H51-024  
PLOT SCALE : 0:2.00000'"/in.  
PLOT DATE : 1/30/2020

DESIGNED - BRD  
CHECKED - SBC  
DRAWN - DLH  
CHECKED - SBC

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT BEARING DETAILS  
STRUCTURE NO. 084-0127 (EB), 084-0128 (WB)

SHEET NO. 24 OF 42 SHEETS

F.A.I. RTE. 72  
SECTION (84-9-3) I, P  
COUNTY SANGAMON  
TOTAL SHEETS 138  
SHEET NO. 120  
CONTRACT NO. 72H51

ILLINOIS FED. AID PROJECT

USER NAME = brandon.dudley  
DESIGNED -  
DRAWN -  
PLOT SCALE = 100,0000'"/in.  
PLOT DATE = 8/20/2025

DESIGNED -  
CHECKED -  
CHECKED -  
DATE -

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

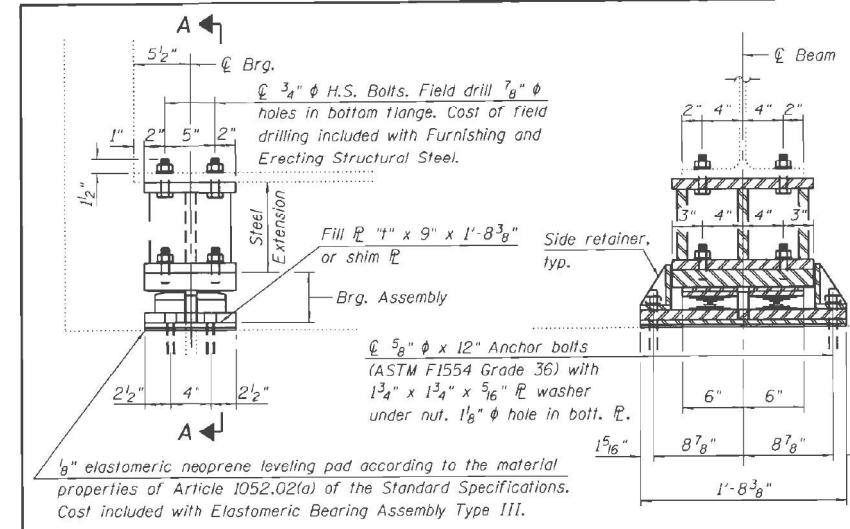
EXISTING BRIDGE PLANS, SN 084-0127 & 0128  
(FOR INFORMATION ONLY)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE. 72  
SECTION (84-9-3)BP  
COUNTY SANGAMON  
TOTAL SHEETS 7  
SHEET NO. 6  
CONTRACT NO. 72729

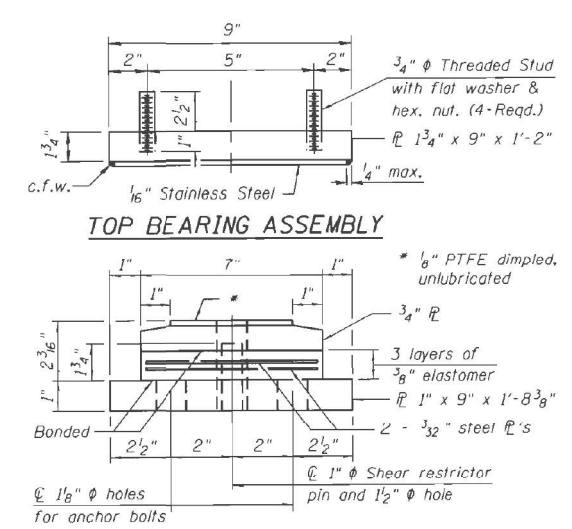
ILLINOIS FED. AID PROJECT



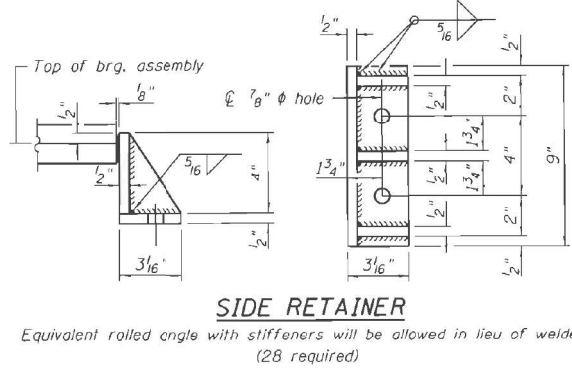


ELEVATION AT ABUT.

TYPE III ELASTOMERIC EXP. BRG.



BOTTOM BEARING ASSEMBLY

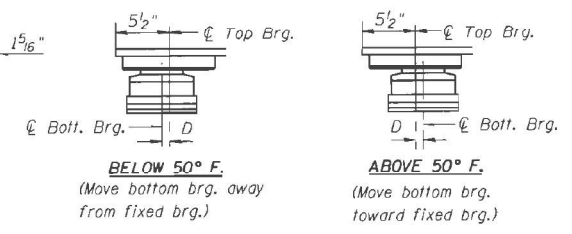


SIDE RETAINER

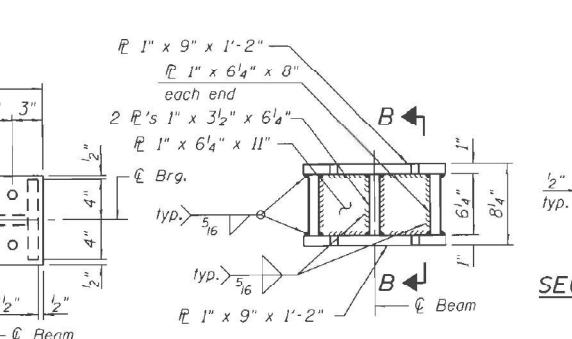
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.  
(28 required)

* TABLE OF "I" DIMENSIONS			
Beam No.	084-0128 (WB)	Beam No.	084-0127 (EB)
1	3/8"	8	3/16"
2	3/4"	9	-
3	7/8"	10	3/8"
4	5/8"	11	1/4"
5	1/4"	12	1/8"
6	1/8"	13	1/16"
7	3/8"	14	1/4"

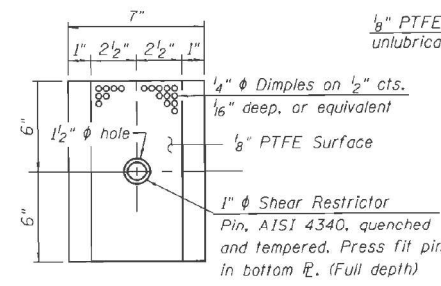
\* The fill "I" thicknesses shown above are based on survey data to match the height of the existing bearing assemblies. Prior to ordering any material, the Contractor shall field verify all existing bearing heights and required fill plate thicknesses. The Contractor may adjust the height of the steel extensions in lieu of providing the above fill plates.



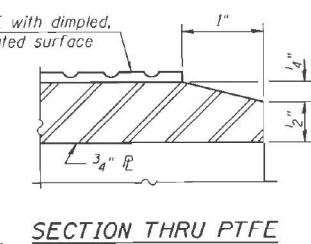
SETTING ANCHOR BOLTS AT EXP. BRG.



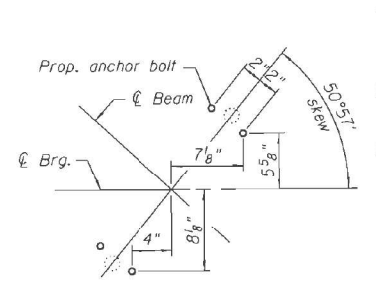
STEEL BEARING EXTENSION



PLAN-PTFE ELASTOMERIC BRG.

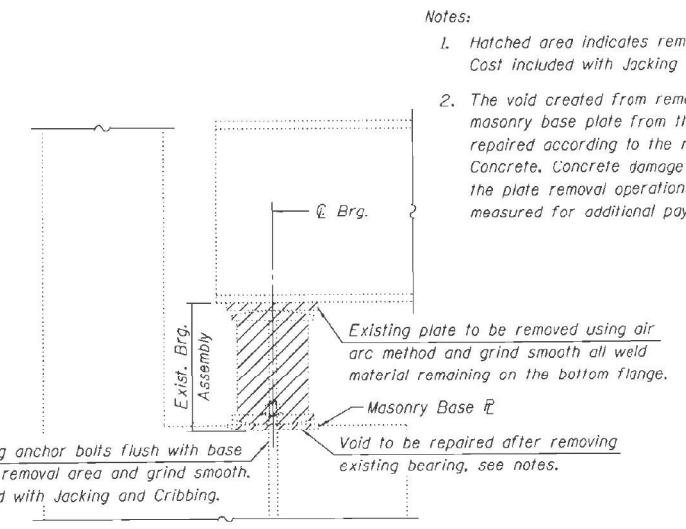


SECTION THRU PTFE



ANCHOR BOLT LAYOUT

(Shown for information only. It is recommended that the Contractor set the bottom plate in place and use the holes in the bottom plate to locate the anchor bolts on the abutment.)



EXISTING BEARING ASSEMBLY REMOVAL DETAIL

JACK AND REMOVE EXISTING BEARINGS PROCEDURE

1. The Contractor shall submit for approval by the Engineer, plans for jacking and removing existing bearings prior to commencing any related work. See Jacking and Cribbing Special Provisions.
2. Jacking and removing existing bearings shall be done in stages after existing deck removal is completed.
3. The Maximum Dead Load Reaction with deck removed (per bearing) at each abutment is 4.5 kips. Minimum jack capacity is 7 kips.
4. The existing abutment seats shall be repaired, the new bearings shall be in place, and the jacks shall be lowered prior to forming and pouring the new deck.

- Notes:
1. Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
  2. Any damage to the concrete repair area adjacent to the proposed bearing caused from drilling anchor bolts shall be repaired by the Contractor to the satisfaction of the Engineer at no additional cost to the Department.
  3. Side retainers and other steel members required for the elastomeric bearing assembly, except steel bearing extension, bottom flange bolts, and fill plates, shall be included in the cost of Elastomeric Bearing Assembly, Type III. Steel bearing extensions, bottom flange bolts, and fill plates are included in the quantity for Furnishing and Erecting Structural Steel.
  4. The 1/2" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.
  5. Bonding of 1/2" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.
  6. Two 1/2" adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details. Cost included with Furnishing and Erecting Structural Steel.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type III	Each	14
Anchor Bolts, 3/4"	Each	56
Jacking and Cribbing, Location No. 1	L. Sum	0.5
Furnishing and Erecting Structural Steel	Pound	2,390
Structural Repair of Concrete (Depth Equal to or less than 5 inches)	Sq. Ft.	18.0

Design firm  
No. 1640101036

**whks**  
engineers • planners • land surveyors

USER NAME = dtheberling  
MODEL = 0840127-28-72H51-025  
PLOT SCALE = 0.2:00000 " = 1 in.  
PLOT DATE = 1/30/2020

DESIGNED - BRD  
CHECKED - SBC  
DRAWN - DLH  
CHECKED - SBC

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT BEARING DETAILS  
STRUCTURE NO. 084-0127 (EB), 084-0128 (WB)  
SHEET NO. 25 OF 42 SHEETS

F.A.I. RTE. 72  
SECTION (84-9-3) 1, P  
COUNTY SANGAMON  
TOTAL SHEETS 138  
SHEET NO. 121  
CONTRACT NO. 72H51  
ILLINOIS FED. AID PROJECT

USER NAME = brandon.dudley  
DESIGNED -  
DRAWN -  
PLOT SCALE = 100,0000 " = 1 in.  
PLOT DATE = 8/20/2025

DESIGNED -  
CHECKED -  
DATE -

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS, SN 084-0127 & 0128  
(FOR INFORMATION ONLY)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE. 72  
SECTION (84-9-3)BP  
COUNTY SANGAMON  
TOTAL SHEETS 7  
SHEET NO. 7  
CONTRACT NO. 72729  
ILLINOIS FED. AID PROJECT