

11/07/2025 LETTING ITEM 111

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
F.A.U. ROUTE 1234 (OAK SPRING ROAD)
APPLEY AVENUE/SECOND STREET TO ST. MARY'S ROAD
PAVEMENT RESURFACING
SECTION: 23-00127-00-RS
PROJECT: JXAM(339)
VILLAGE OF LIBERTYVILLE
LAKE COUNTY
C-91-211-25

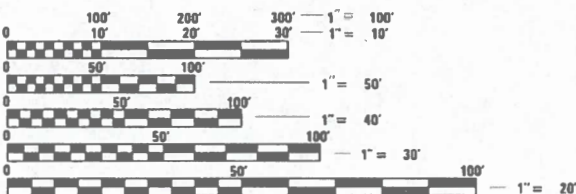
TRAFFIC DATA

OAK SPRING ROAD

DESIGN SPEED - 25 MPH (WEST OF DES PLAINES RIVER)
35 MPH (EAST OF DES PLAINES RIVER)
POSTED SPEED - 25 MPH (WEST OF DES PLAINES RIVER)
35 MPH (EAST OF DES PLAINES RIVER)

HIGHWAY CLASSIFICATION

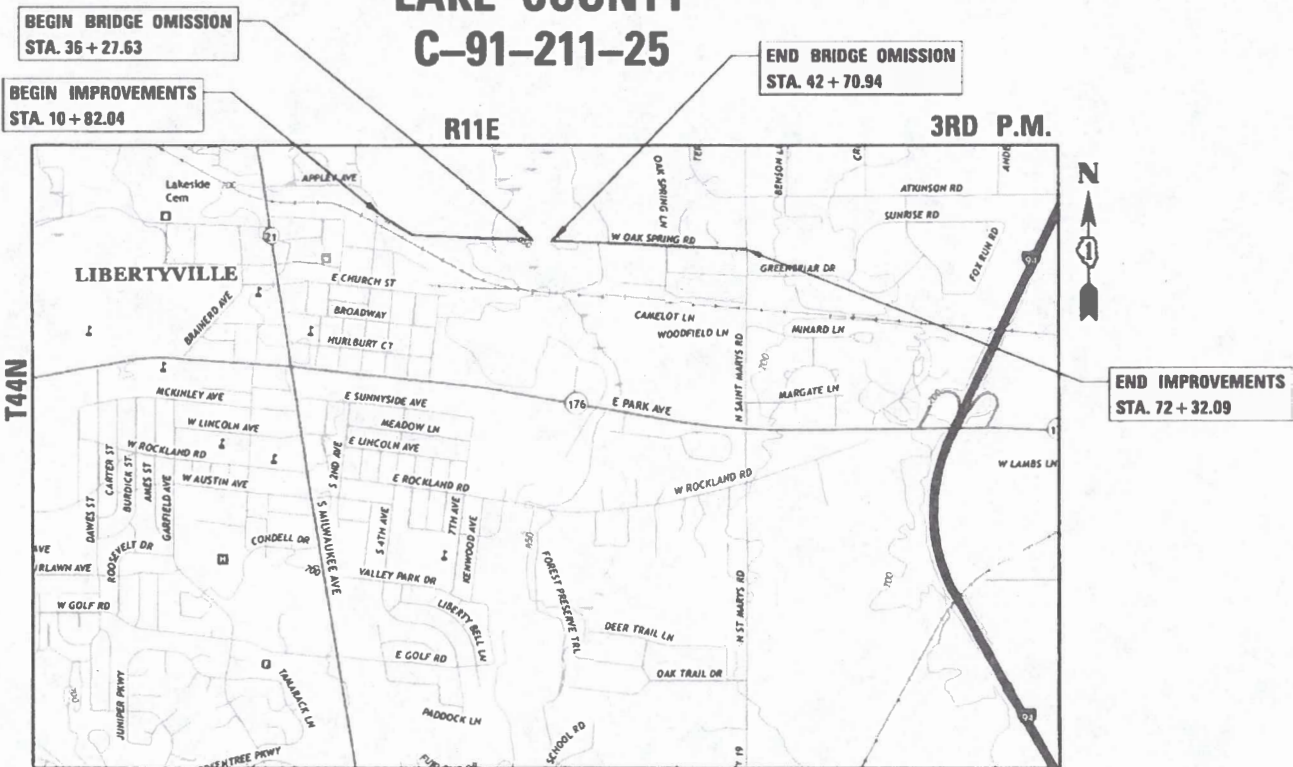
MAJOR COLLECTOR (2023 ADT=5,100)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61L83



LOCATION MAP
(NOT TO SCALE)

LIBERTYVILLE TOWNSHIP

GROSS LENGTH = 6,150.1 FT. (1.165 MILES)
NET LENGTH = 5506.8 FT. (1.043 MILES)



SIGNED *Nathan E. Murphy*
DATE 7/25/25 EXPIRES 11-30-2025



PLANS PREPARED BY:



Two Pierce Place, Suite 1400 - Itasca, Illinois 60143
Tel: 630.773.3900 - Fax: 630.773.3975
www.civiltechinc.com

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	1
		ILLINOIS	CONTRACT NO. 61L83	



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	July 29, 2025 <i>Paula</i>
VILLAGE OF LIBERTYVILLE, DIRECTOR OF PUBLIC WORKS	
APPROVED	July 23, 2025 <i>Paula</i>
LIBERTYVILLE TOWNSHIP, TOWNSHIP ENGINEER	
PASSED	August 27, 2025 <i>Chris</i>
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS	
RELEASING FOR BID BASED ON LIMITED REVIEW	Aug 27th, 2025 <i>Joe Rios</i>
REGIONAL ENGINEER	

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OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424016-06	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424026-04	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C & D PATCHES
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS

IDOT DISTRICT ONE STANDARDS

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-22	ARTERIAL ROAD INFORMATION SIGN

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

1.

ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED JANUARY 1, 2022; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2025; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" EIGHTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
2.

NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED.
3.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
4.

WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE EVERY PRECAUTION NECESSARY TO ENSURE THAT THERE WILL BE NO DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
5.

THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE PROJECT LIMITS. ALL EXCESS OR WASTE MATERIAL SHALL BE EITHER HAULED AWAY FROM THE PROJECT SITE BY THE CONTRACTOR AND DEPOSITED AT LOCATIONS PROVIDED BY HIM/HER, OR DISPOSED OF WITHIN THE RIGHT-OF-WAY IN A MANNER OTHER THAN BURNING, SUBJECT TO THE APPROVAL OF THE ENGINEER.
6.

THE ENGINEER AND ALL UTILITY COMPANIES, LOCAL POLICE, AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
7.

THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

PAVING, SHOULDERS, CURB & GUTTER AND SIDEWALK

1.

HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, AND TOPSOIL PLACEMENT HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
2.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
3.

THE EXISTING CURB AND GUTTER SHALL BE SAW CUT AT EACH LIMIT OF REMOVAL AND THE NEW CURB AND GUTTER SHALL BE TIED IN TO THE EXISTING WITH TWO 3/4 INCH STEEL DOWEL BARS (18" LONG), DRILLED INTO THE EXISTING CURB AND GUTTER END. THE ENGINEER MUST INSPECT AND APPROVE THE BASE AND FORMWORK BEFORE ANY CONCRETE IS POURED. A MINIMUM 24 HOUR NOTICE SHALL BE PROVIDED FOR FORM WORK INSPECTION.
4.

FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER, MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

UTILITIES

1.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY.
2.

COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
3.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
4.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AS COORDINATED WITH THE UTILITY OWNER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF HIS/HER CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT THE RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.

COMMITMENTS

EXTENDED RESTRICTIONS WILL APPLY FOR TREE CLEARING AND PRUNING, TREES THREE (3) INCHES IN DIAMETER OR GREATER AT BREAST HEIGHT SHALL NOT BE CLEARED OR PRUNED FROM APRIL 1ST THROUGH OCTOBER 31ST OF ANY GIVEN YEAR.

SHOULD THE PROJECT REQUIRE NEW/ADDITIONAL TEMPORARY OR PERMANENT LIGHTING, OR REPLACING EXISTING PERMANENT LIGHTS, ALL LIGHTS SHALL BE INSTALLED IN ACCORDANCE WITH IDOT BDE MANUAL - CHAPTER 56 AND WITH THE USFWS PBO FOR IBAT, NLEB AND TRICOLORED BAT. THIS INCLUDES THE FOLLOWING:

- DIRECT TEMPORARY LIGHTING AWAY FROM SUITABLE HABITAT DURING THE ACTIVE SEASON
- UTILIZING THE BUG SYSTEM, THE PROJECT SHOULD BE AS CLOSE TO 0 FOR ALL THREE RATINGS WITH PRIORITY OF "UPLIGHT" OF 0 AND "BACKLIGHT" AS LOW AS PRACTICABLE.

MODEL SHEET NAMES
FILE NAME: 032924-215-generotes.dgn

	USER NAME = nem	DESIGNED - NEM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OAK SPRING ROAD GENERAL NOTES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRS	REVISED -						1234	23-00127-00-R5	LAKE	36	2
	PLOT SCALE = 2.0000 ' / in.	CHECKED - NEM	REVISED -		CONTRACT NO. 61L83								
	PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -		ILLINOIS FED. AID PROJECT								
					SHEET 1	OF 2 SHEETS							

STORM & SANITARY AND WATER MAIN

1.

UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.
- DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE/SHE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION SEQUENCE

THIS CONSTRUCTION SEQUENCE WAS DEVELOPED TO MINIMIZE IMPACTS TO PROPERTY OWNERS AND TO PROVIDE AN ADEQUATE METHOD OF INSPECTING THE CONDITION OF THE PAVEMENT BASE AND CURB AND GUTTER. THIS CONSTRUCTION SEQUENCE SHALL BE FOLLOWED UNLESS AN ALTERNATE SEQUENCE IS APPROVED BY THE ENGINEER.

1.

SET UP APPLICABLE TRAFFIC CONTROL MEASURES USING IDOT HIGHWAY STANDARDS AND DISTRICT ONE DETAILS PROVIDED IN THE PLANS. DAILY LANE CLOSURES SHALL BE USED FOR ALL WORK DEPICTED IN THESE PLANS. PERMANENT LANE CLOSURES SHALL NOT BE ALLOWED UNLESS SHOWN ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER.
2.

REMOVE AND REPLACE CURB AND GUTTER AND ADJUST UTILITY STRUCTURES AS DETERMINED BY THE ENGINEER.
3.

REMOVE AND INSTALL SIDEWALK AND DETECTABLE WARNINGS.
4.

LANDSCAPE RESTORATION.
5.

REMOVE HMA PAVEMENT SURFACE.
6.

THE ENGINEER SHALL INSPECT THE CONDITION OF THE PAVEMENT AND MARK THE AREAS REQUIRING PAVEMENT PATCHING. UNDER NO CONDITION SHALL THE CONTRACTOR PROCEED WITH THIS WORK WITHOUT PRIOR CONSENT FROM THE ENGINEER. PERFORM PAVEMENT PATCHING.
7.

INSTALL BINDER AND HMA SURFACE.
8.

INSTALL AGGREGATE SHOULDER.
9.

INSTALL PERMANENT PAVEMENT MARKINGS AND SIGNING.
10.

REMOVE TRAFFIC CONTROL.

MAINTENANCE OF TRAFFIC GENERAL NOTES

1.

TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
2.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
3.

ALL CONSTRUCTION WARNING SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
4.

ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND, UNLESS OTHERWISE NOTED. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
5.

DRUMS WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT THE BARRICADES SHALL BE NON-METALLIC DRUMS. SPACING SHALL BE AS SHOWN ON THE HIGHWAY STANDARDS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
6.

DRUMS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 20' CENTERS ALONG TAPERS, AND 10' CENTERS IN CURVES AND RADII.
7.

TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
8.

EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED. ANY DAMAGE CAUSED BY THIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
9.

THE FIRST TWO WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
10.

EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS/HER WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
11.

W21-1(O) "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.

CONSTRUCTION REQUIREMENTS

1.

WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICTY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FEET OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE.
2.

ALL WORK SHALL BE IN ACCORDANCE WITH IDOT'S SAFETY ENGINEERING POLICY MEMORANDUM, SAFETY 4-21, INCLUDING THE REQUIREMENT FOR USE OF TEMPORARY OR MILLED SLOPE EDGES (MIN OF 1:3). THIS MAY REQUIRE ADDITIONAL PASSES OF THE MILLING MACHINE OR THE USE OF A SECONDARY, SMALLER MILLING MACHINE TO CREATE THE REQUIRED EDGE.
3.

"UNEVEN LANES" SIGNS (W8-1(O)-4848) SHALL BE PLACED AT THE INTERVALS REQUIRED BY THE ENGINEER WHEN TRAFFIC IS ADJACENT TO THE MILLED SURFACE.

CONSTRUCTION SIGNS

W21-2(O)
48" X 48"

FRESH
OIL

DATE XX-XX-XX

9" X 36"

W8-1(O)
48" X 48"

BUMP

BLACK 6" LETTERS ON ORANGE
RETROREFLECTIVE BACKGROUND

THESE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
W21-2(O) SHALL BE PLACED 48 HOURS PRIOR TO TACKING.

MODEL SHEET NAMES
FILE NAME: 032924-211-01-0000.dgn

	USER NAME = nem	DESIGNED - NEM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OAK SPRING ROAD GENERAL NOTES	SHEET 2 OF 2 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRS	REVISED -				1234	23-00127-00-RS	LAKE	36	3
	PLOT SCALE = 2.0000 ' / in.	CHECKED - NEM	REVISED -				CONTRACT NO. 61L83				
	PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -				ILLINOIS FED. AID PROJECT				

MODEL: S:\MODEL\NAMES
FILE: NAME: 032924-211-500.dgn

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODES	
					STP URBAN FUNDING	
					80% FEDERAL / 20% LOCAL	
					0005	0021
					ROADWAY RESURFACE	SAFETY
	20200100	EARTH EXCAVATION	CU YD	50	50	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	24	24	
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	11	11	
	25200110	SODDING, SALT TOLERANT	SQ YD	11	11	
	25200200	SUPPLEMENTAL WATERING	UNIT	1	1	
	28000510	INLET FILTERS	EACH	9	9	
	35101598	AGGREGATE BASE COURSE, TYPE B 3"	SQ YD	60	60	
	35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	76	76	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	10961	10961	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	5507	5507	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	80	80	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	119	119	
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2270	2270	
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1362	1362	
	42001300	PROTECTIVE COAT	SQ YD	67	67	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	546	546	

	USER NAME = nem	DESIGNED - NEM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OAK SPRING ROAD SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRS	REVISED -						1234	23-00127-00-RS	LAKE	36	4
	PLOT SCALE = 40,0000 ' / in.	CHECKED - NEM	REVISED -						CONTRACT NO. 61L83				
	PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -						ILLINOIS FED. AID PROJECT				
									SHEET 1 OF 4 SHEETS				

MODEL - 4MODELNAME
FILE NAME - 13394-215-500.dgn

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODES	
					STP URBAN FUNDING	
					80% FEDERAL / 20% LOCAL	
					0005	0021
					ROADWAY RESURFACE	SAFETY
	42400800	DETECTABLE WARNINGS	SQ FT	101	101	
	44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	16237	16237	
	44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	844	844	
	44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	1106	1106	
	44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	1120	1120	
	44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	1553	1553	
	48101498	AGGREGATE SHOULDERS, TYPE B 4"	SQ YD	1033	1033	
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6	
	67100100	MOBILIZATION	L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	150		150
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1502		1502
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	500		500
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	73		73

	USER NAME = nem	DESIGNED - NEM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OAK SPRING ROAD SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 40.0000 ' / in.	CHECKED - NEM	REVISED -						CONTRACT NO. 61L83				
	PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -						ILLINOIS FED. AID PROJECT				
					SHEET 2 OF 4 SHEETS								

MODEL \$MODELNAME\$
FILE NAME: ...0324-215-500.dgn

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODES	
					STP URBAN FUNDING	
					80% FEDERAL / 20% LOCAL	
					0005	0021
					ROADWAY RESURFACE	SAFETY
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	21518		21518
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	134		134
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	92		92
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	66		66
X	72000100	SIGN PANEL - TYPE 1	SQ FT	154		154
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	32		32
X	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	273		273
X	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146		146
X	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	21753		21753
X	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	199		199
X	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	92		92
X	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	103		103
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	256		256
	X0327036	BIKE PATH REMOVAL	SQ YD	61	61	
	X4060280	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	19	19	
	X4060995	TEMPORARY RAMP (SPECIAL)	SQ YD	220	220	

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FILE NAME: 032924-211-500.dgn

SPECIALTY ITEM	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODES	
					STP URBAN FUNDING	
					80% FEDERAL / 20% LOCAL	
					0005	0021
					ROADWAY RESURFACE	SAFETY
	X4400221	CURB REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	40	40	
	X4400223	CURB REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	70	70	
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	50	50	
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	85	85	
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5	
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	52		52
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
	Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	5	5	

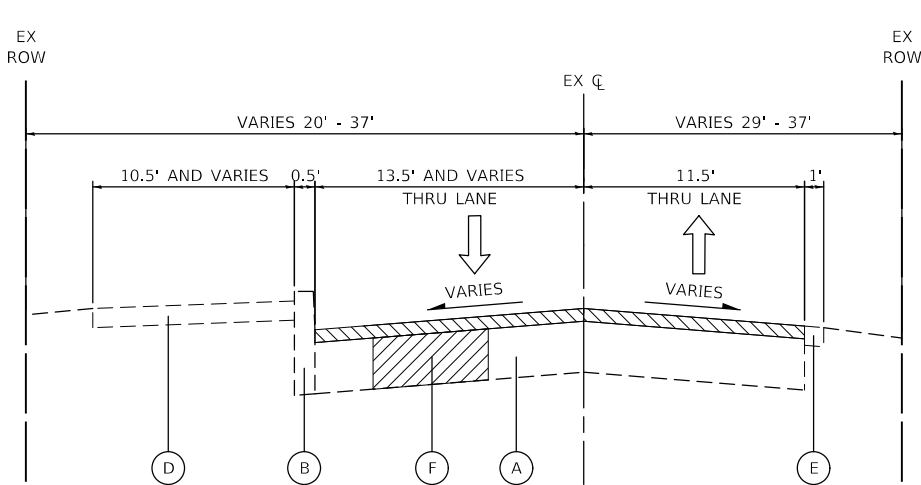
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

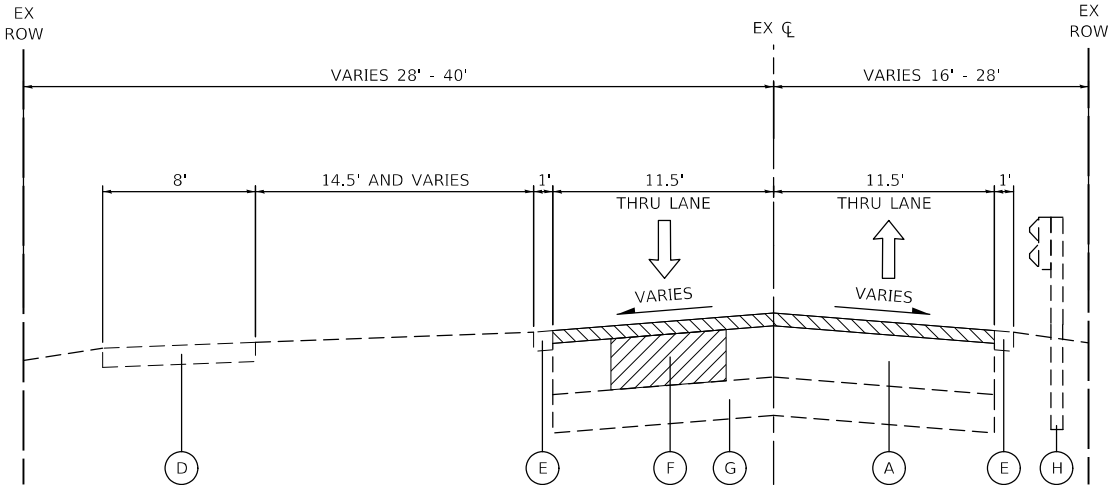
OAK SPRING ROAD
SUMMARY OF QUANTITIES

SHEET 4 OF 4 SHEETS

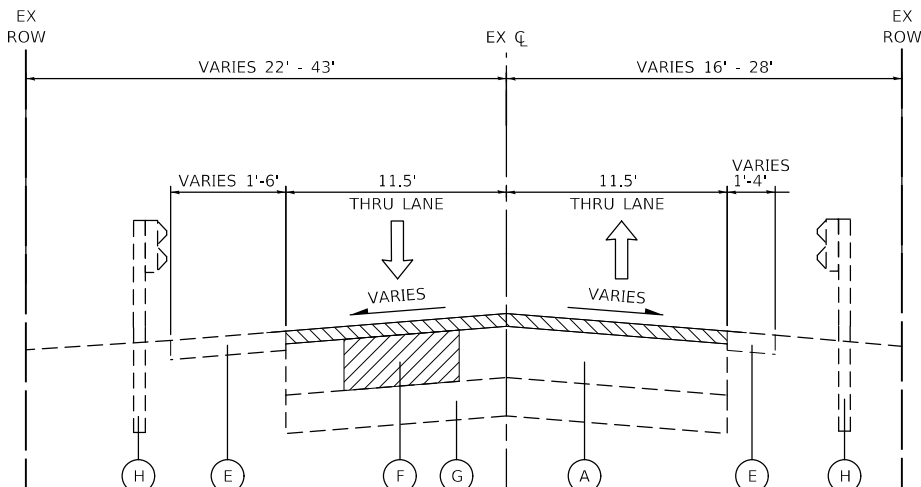
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CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



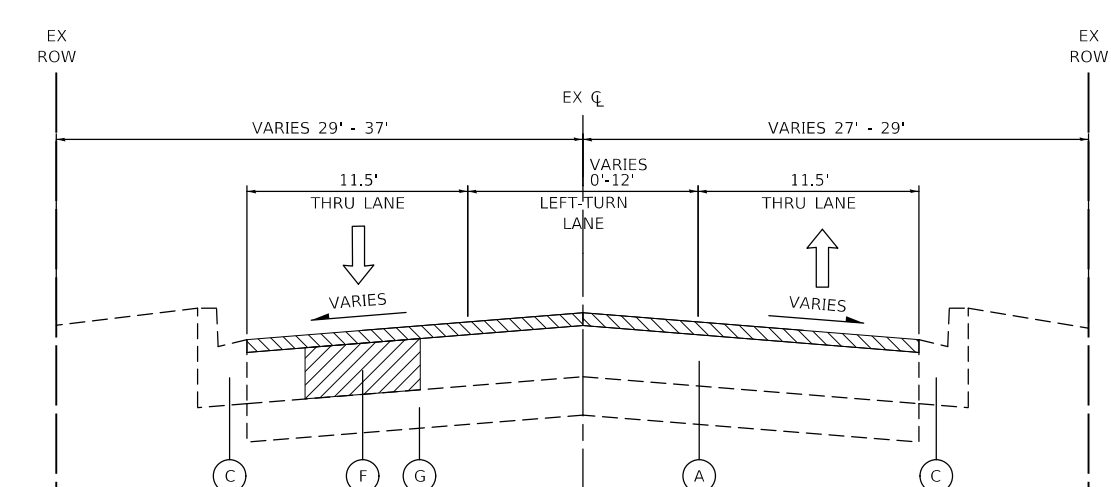
EXISTING TYPICAL SECTION
STA. 10+82 TO STA. 15+14



EXISTING TYPICAL SECTION
STA. 15+14 TO STA. 30+72



EXISTING TYPICAL SECTION
STA. 30+72 TO STA. 69+66
(BRIDGE OMISSION STA. 36+28 TO STA. 42+71)



EXISTING TYPICAL SECTION
STA. 69+66 TO STA. 72+32

LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT:
-OAK SPRING ROAD = 7" TO 15"
- (B) EXISTING CONCRETE CURB, TYPE B
- (C) EXISTING COMBINATION CONCRETE CURB AND GUTTER
TYPE B-6.18
- (D) EXISTING HOT-MIX ASPHALT PATH
- (E) EXISTING AGGREGATE SHOULDER
(REMOVAL PAID FOR AS EARTH EXCAVATION)
- (F) PROPOSED CLASS D PATCH, 8 INCH
(SEE NOTE 1)
- (G) EXISTING BASE COURSE VARIES 0"-11.5"
- (H) EXISTING STEEL PLATE BEAM GUARDRAIL

- HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- CLASS D PATCH (8")

NOTES

- THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION, WIDTH, AND LENGTH OF CLASS D PATCHES IN THE FIELD.
- THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION AND LENGTH OF THE REMOVAL AND REPLACEMENT OF COMBINATION CONCRETE CURB AND GUTTER IN THE FIELD. SEE DETAIL "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT" FOR ADDITIONAL DETAILS.
- EXISTING STEEL PLATE BEAM GUARDRAIL IS LOCATED AT STATION RANGES 30+94.59 TO 32+09.80, 42+71 TO 43+58.64 LT, AND 30+07.20 TO 30+70.96, 30+93.65 TO 32+10.11 RT.

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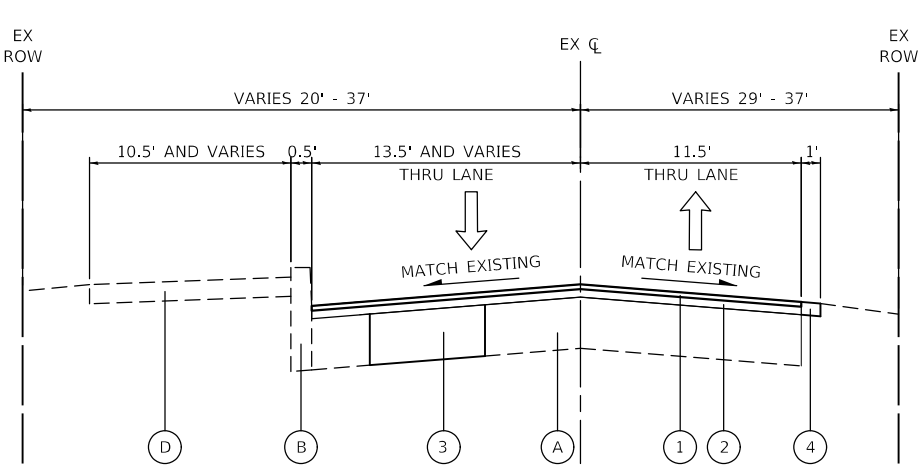
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PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**OAK SPRING ROAD
EXISTING TYPICAL SECTIONS**

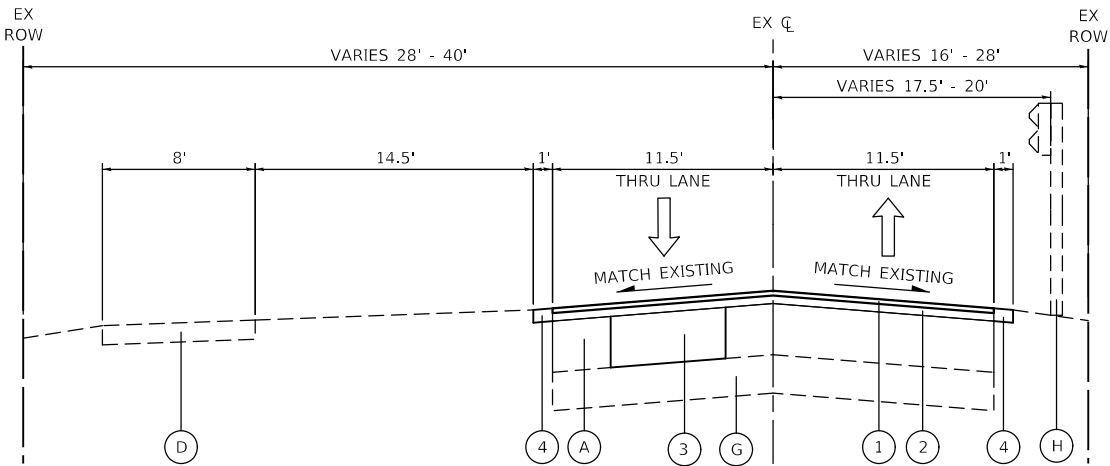
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	8
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



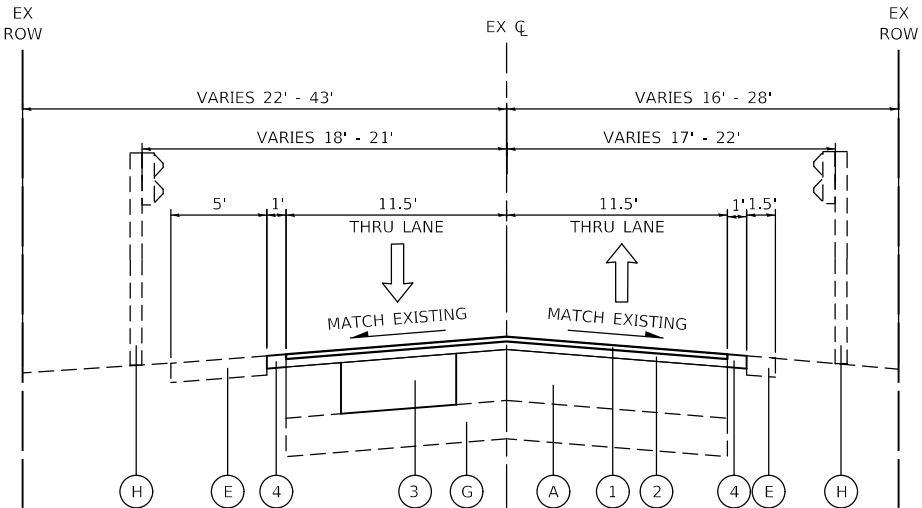
PROPOSED TYPICAL SECTION

STA. 10+82 TO STA. 15+14



PROPOSED TYPICAL SECTION

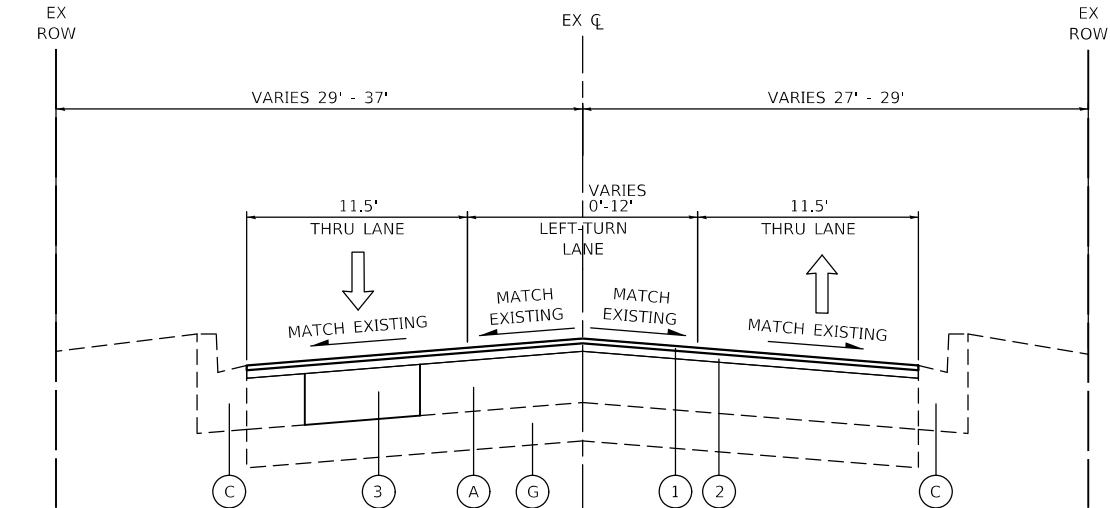
STA. 15+14 TO STA. 30+72



PROPOSED TYPICAL SECTION

STA. 30+72 TO STA. 69+66

(BRIDGE OMISSION STA. 36+28 TO STA. 42+71)



PROPOSED TYPICAL SECTION

STA. 69+66 TO STA. 72+32

LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT:
-OAK SPRING ROAD = 7" TO 15"
- (B) EXISTING CONCRETE CURB, TYPE B
- (C) EXISTING COMBINATION CONCRETE CURB AND GUTTER
TYPE B-6.18
- (D) EXISTING HOT-MIX ASPHALT PATH
- (E) EXISTING AGGREGATE SHOULDER
(REMOVAL PAID FOR AS EARTH EXCAVATION)
- (G) EXISTING BASE COURSE VARIES 0"-11.5"
- (H) EXISTING STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
- (3) CLASS D PATCHES, TYPE AS NOTED ON PLANS, 8 INCH
(SEE NOTE 1)
- (4) AGGREGATE SHOULDERS, TYPE B, 4"

NOTES

- THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION, WIDTH, AND LENGTH OF CLASS D PATCHES IN THE FIELD.
- THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF THE LOCATION AND LENGTH OF THE REMOVAL AND REPLACEMENT OF COMBINATION CONCRETE CURB AND GUTTER IN THE FIELD. SEE DETAIL "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT" FOR ADDITIONAL DETAILS.
- EXISTING STEEL PLATE BEAM GUARDRAIL IS LOCATED AT STATION RANGES 30+94.59 TO 32+09.80, 42+71 TO 43+58.64 LT, AND 30+07.20 TO 30+70.96, 30+93.65 TO 32+10.11 RT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS TABLE		
MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
PROPOSED RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1½"	4% @ 50 GYR.	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2½"	4% @ 50 GYR.	LR 1030-2
HMA DRIVEWAY PAVEMENT, 3"		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3" (IN 2 LIFTS)	4% @ 50 GYR.	LR 1030-2
TEMPORARY RAMP (SPECIAL)		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; VARIABLE DEPTH	4% @ 50 GYR.	LR 1030-2
CLASS D PATCHES		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 GYR.	LR 1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR 1030-2		

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LB/SY-IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.
- THE CONTRACTOR SHALL MILL THE PAVEMENT BEFORE PATCHING.
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HMA BINDER COURSE, IL-19.0, N50.

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PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD PROPOSED TYPICAL SECTIONS		
SCALE: N.T.S.	SHEET 2 OF 2 SHEETS	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	9
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



BENCHMARK AND CONTROL POINT INFORMATION						
BM/CP	STATION	OFFSET	NORTHING	EASTING	ELEVATION	DESCRIPTION
T.B.M. #2	30+61.37	22.4' RT	1,090,490.60	2,048,101.36	655.14	RAILROAD SPIKE (SET) IN THE FIRST TELEPHONE POLE WEST OF THE DES PLAINES RIVER TRAIL ON THE SOUTH SIDE OF OAK SPRING ROAD.
T.B.M. #27	N/A	N/A	N/A	N/A	699.66	(WEST ELLIS PROJECT) RAILROAD SPIKE (SET) IN POWER POLE WITH LIGHT IN THE SOUTHWEST QUADRANT OF THE INTERSECTION OF MILWAUKEE AVENUE & WEST ELLIS AVENUE.
T.B.M. #28	N/A	N/A	N/A	N/A	698.03	(EAST ELLIS PROJECT) RAILROAD SPIKE (SET) IN SECOND POWER SOUTH OF EAST WINCHESTER ROAD ON WEST SIDE OF EAST ELLIS AVE.

PROPOSED OAK SPRING ROAD CENTERLINE			
DESCRIPTION	STATION	NORTHING	EASTING
POT	10+00.00	1,088,518.4856	2,048,511.6110
PC	15+55.09	1,088,995.5508	2,048,227.8298
PI	16+46.38	1,089,075.6630	2,048,180.1753
PT	17+37.67	1,089,168.8033	2,048,176.4605
PC	29+99.55	1,090,429.6857	2,048,126.1735
PI	33+87.64	1,090,817.4780	2,048,110.7053
PT	37+75.81	1,091,205.5660	2,048,102.2401
PC	41+02.21	1,091,531.8864	2,048,095.1222
PI	44+62.82	1,091,892.4287	2,048,087.2579
PT	48+23.43	1,092,252.6505	2,048,070.1442
PI	50+02.53	1,092,431.5551	2,048,061.6447
PI	55+24.98	1,092,953.3945	2,048,036.4375
PI	62+04.68	1,093,632.4022	2,048,005.7103
PI	68+32.55	1,094,259.8465	2,047,982.7015
PI	71+44.71	1,094,571.7403	2,047,969.7706
POT	73+43.50	1,094,770.3224	2,047,960.6778

EX CURVE 1
PI STA. = 16+48.30
Δ = 28° 27' 44" (LT)
D = 15° 35' 20"
R = 367.54'
T = 93.21'
L = 182.58'
E = 11.64'
P.C. STA. = 15+55.09
P.T. STA. = 17+37.67

EX CURVE 2
PI STA. = 33+87.65
Δ = 1° 02' 04" (LT)
D = 0° 08' 00"
R = 43,002.36'
T = 388.18'
L = 776.34'
E = 1.75'
P.C. STA. = 29+99.47
P.T. STA. = 37+75.81

MATCH LINE STA 24+00
(SEE ABOVE)

MATCH LINE STA 24+00
(SEE BELOW)

MATCH LINE STA 38+00
(SEE PAGE \$a02)

MODEL: \$MODEL\$
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	DRAWN - JRS	REVISED -
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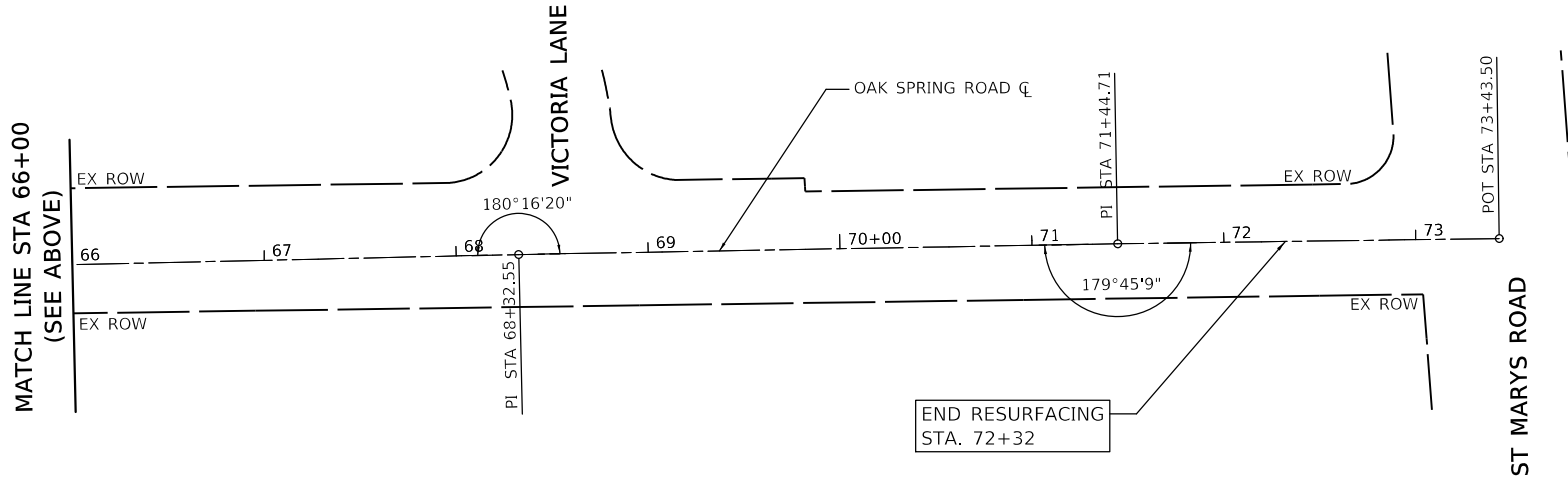
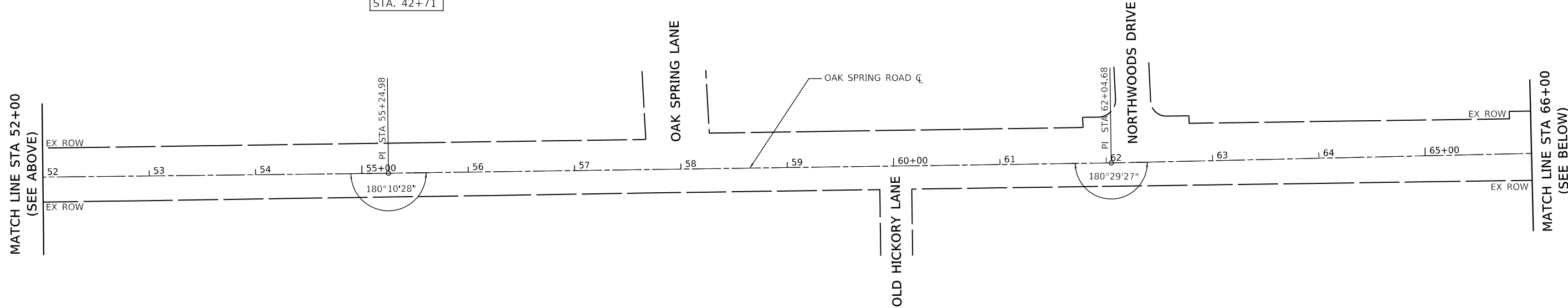
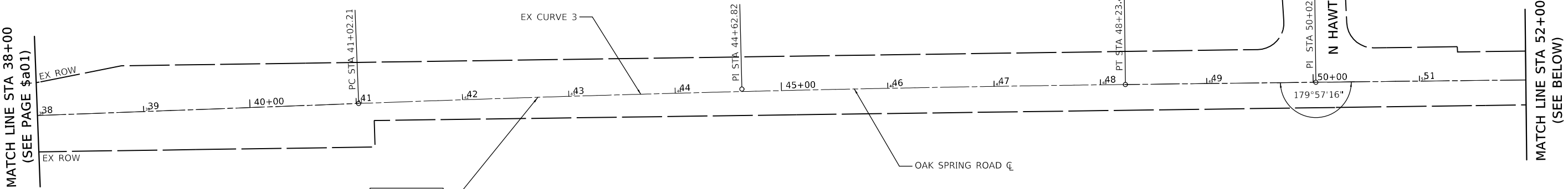
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
ALIGNMENT TIES & BENCHMARKS

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 10+00 TO STA. 38+00

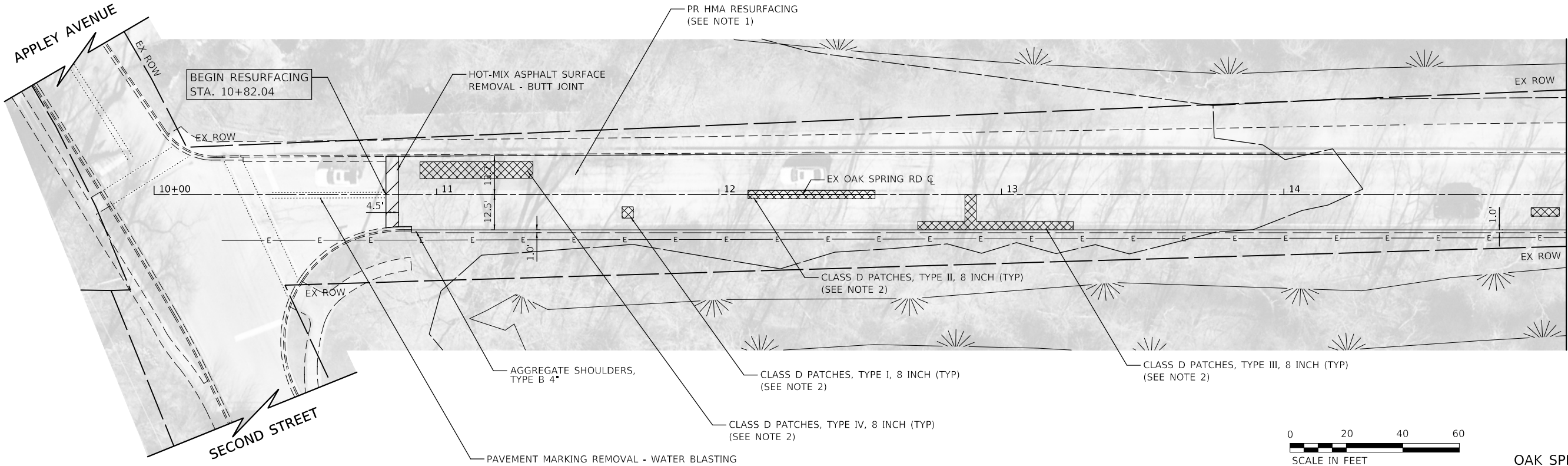
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1234	23-00127-00-RS	LAKE	36	10
CONTRACT NO. 61L83				
		ILLINOIS	FED. AID PROJECT	

EX CURVE 3
PI STA. = 44+62.84
Δ = 1° 28' 14" (RT)
D = 0° 12' 14"
R = 28,102.56'
T = 360.63'
L = 721.22'
E = 2.31'
P.C. STA. = 41+02.21
P.T. STA. = 48+23.43



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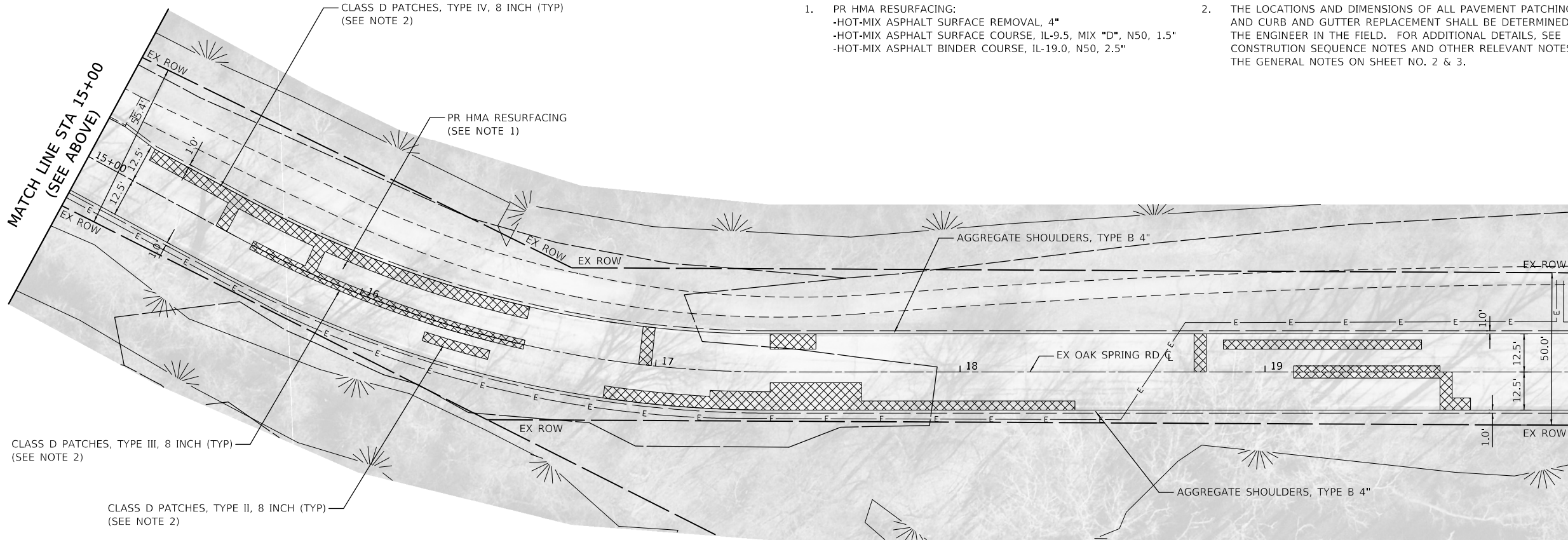


MATCH LINE STA 15+00
(SEE BELOW)

OAK SPRING ROAD - PLAN

NOTES

1. PR HMA RESURFACING:
-HOT-MIX ASPHALT SURFACE REMOVAL, 4"
-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
2. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.



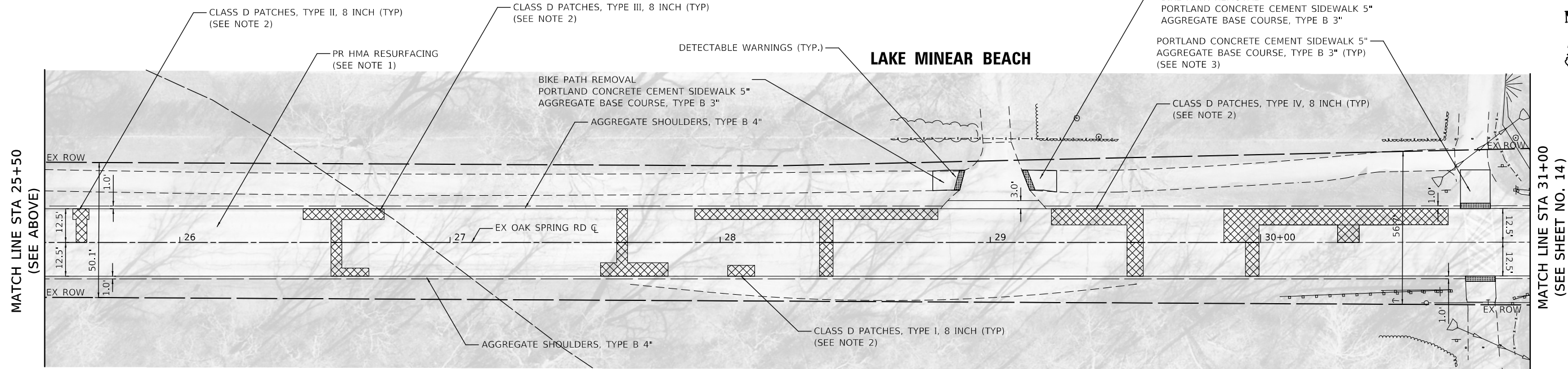
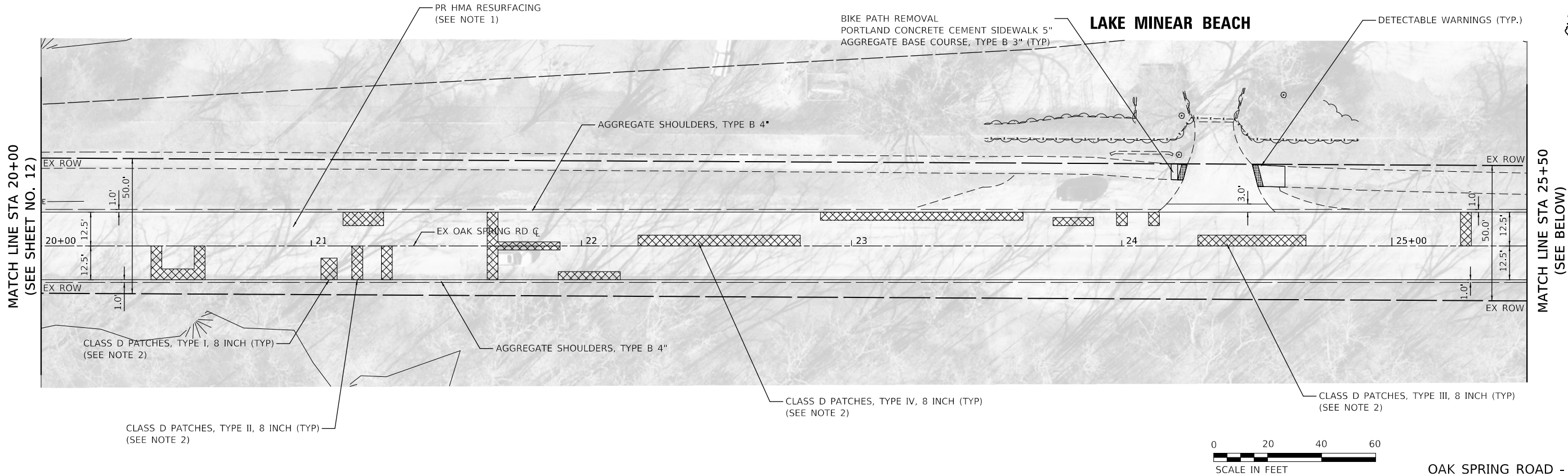
MATCH LINE STA 20+00
(SEE SHEET NO. 13)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
ROADWAY PLAN

SCALE: 1"=20' SHEET 1 OF 6 SHEETS STA. 10+82.04 TO STA. 20+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	12
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



NOTES

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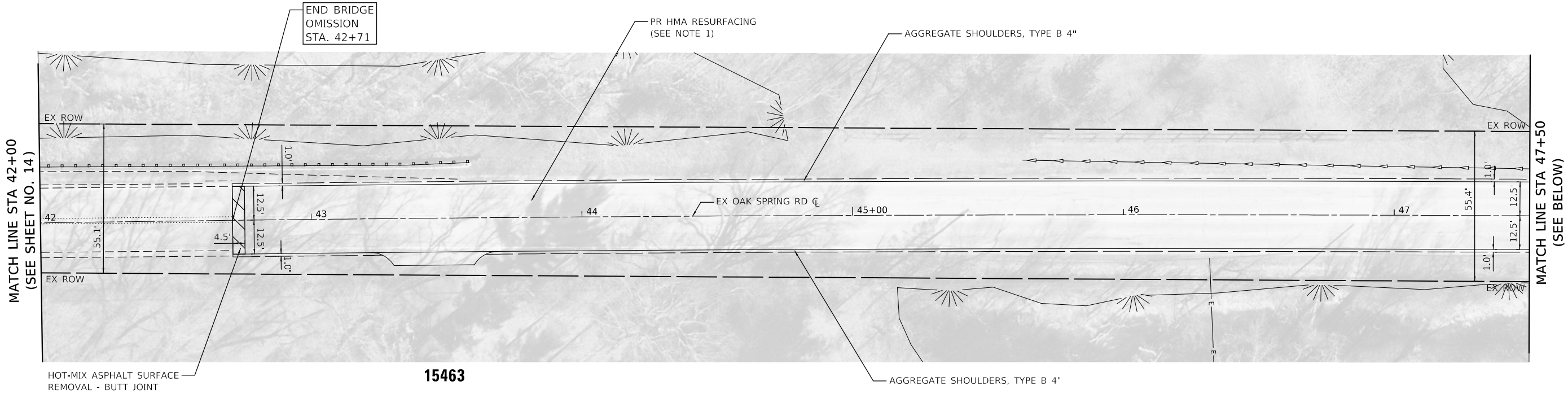
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-HOT-MIX ASPHALT SURFACE REMOVAL, 4"
-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
2.

THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.
3.

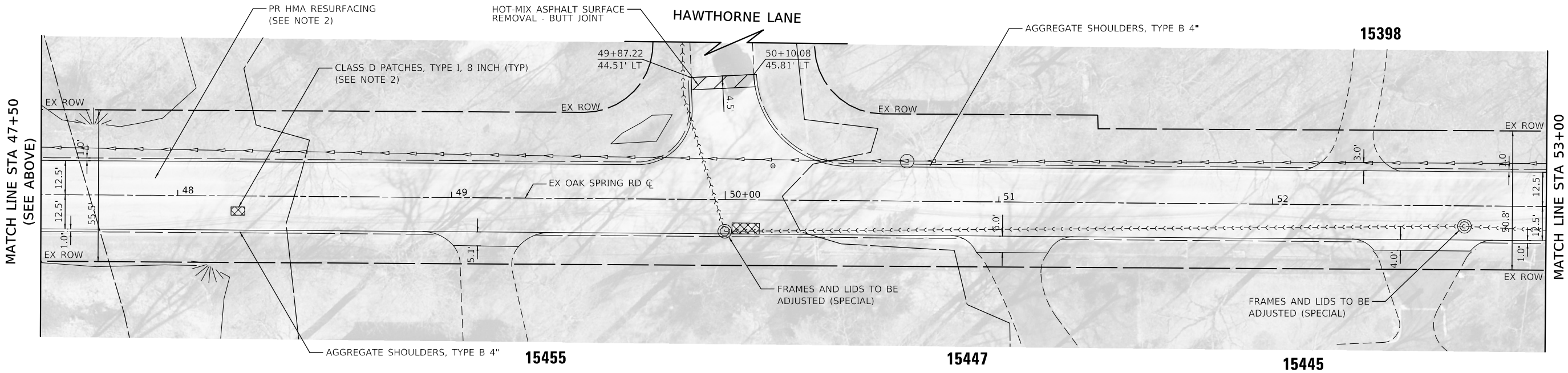
ADA RAMP SHALL BE CONSTRUCTED IN TWO STAGES TO MAINTAIN ACCESS FOR THE BIKE PATH.

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			DRAWN - JRS	REVISED -						1234	23-00127-00-R5		LAKE	36	13
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	PLOT DATE = 8/17/2025		DATE - 5/16/2025	REVISED -		SCALE: 1"=20'		SHEET 2 OF 6 SHEETS	STA. 20+00 TO STA. 31+00						
						ILLINOIS FED. AID PROJECT									



OAK SPRING ROAD - PLAN



NOTES

1. PR HMA RESURFACING:
-HOT-MIX ASPHALT SURFACE REMOVAL, 4"
-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
2. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.

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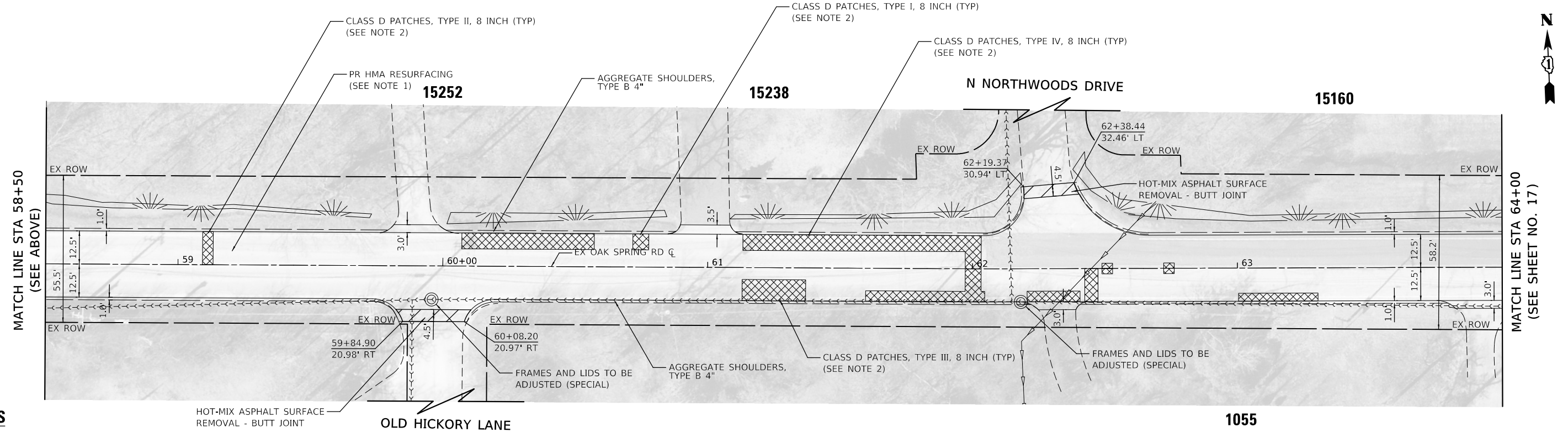
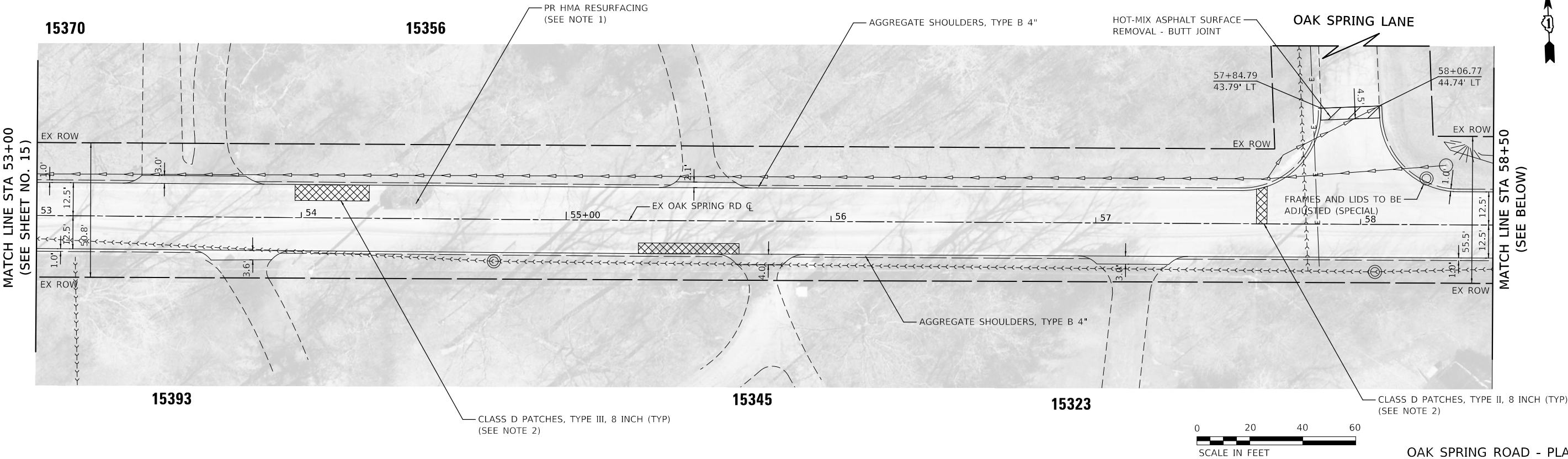
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
ROADWAY PLAN

SCALE: 1"=20' SHEET 4 OF 6 SHEETS STA. 42+00 TO STA. 53+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	15
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



NOTES

1. PR HMA RESURFACING:
-HOT-MIX ASPHALT SURFACE REMOVAL, 4"
-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
2. THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.

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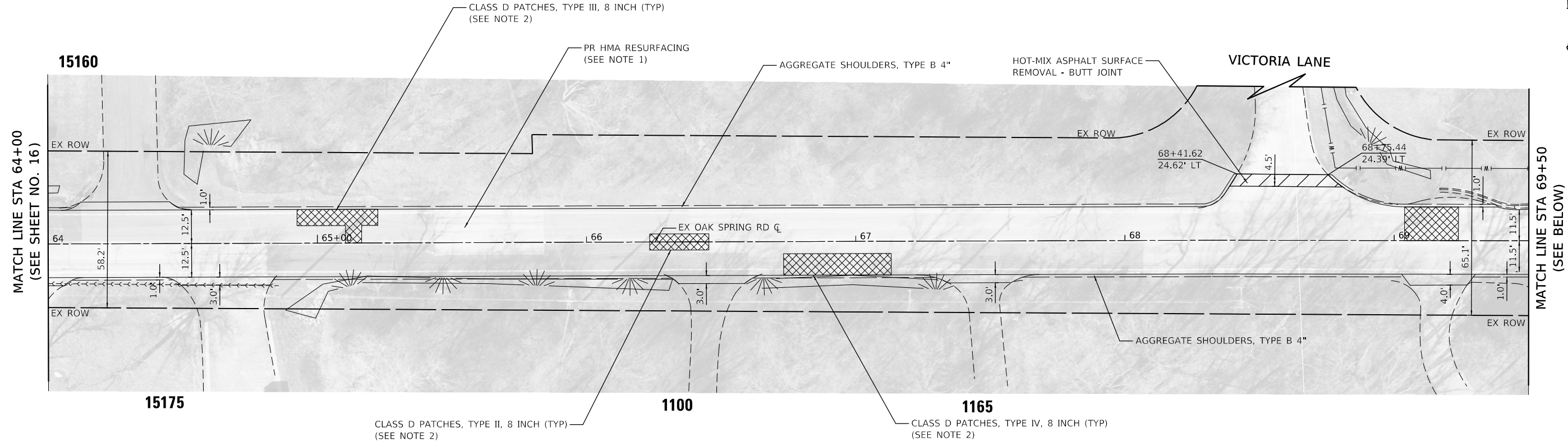
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

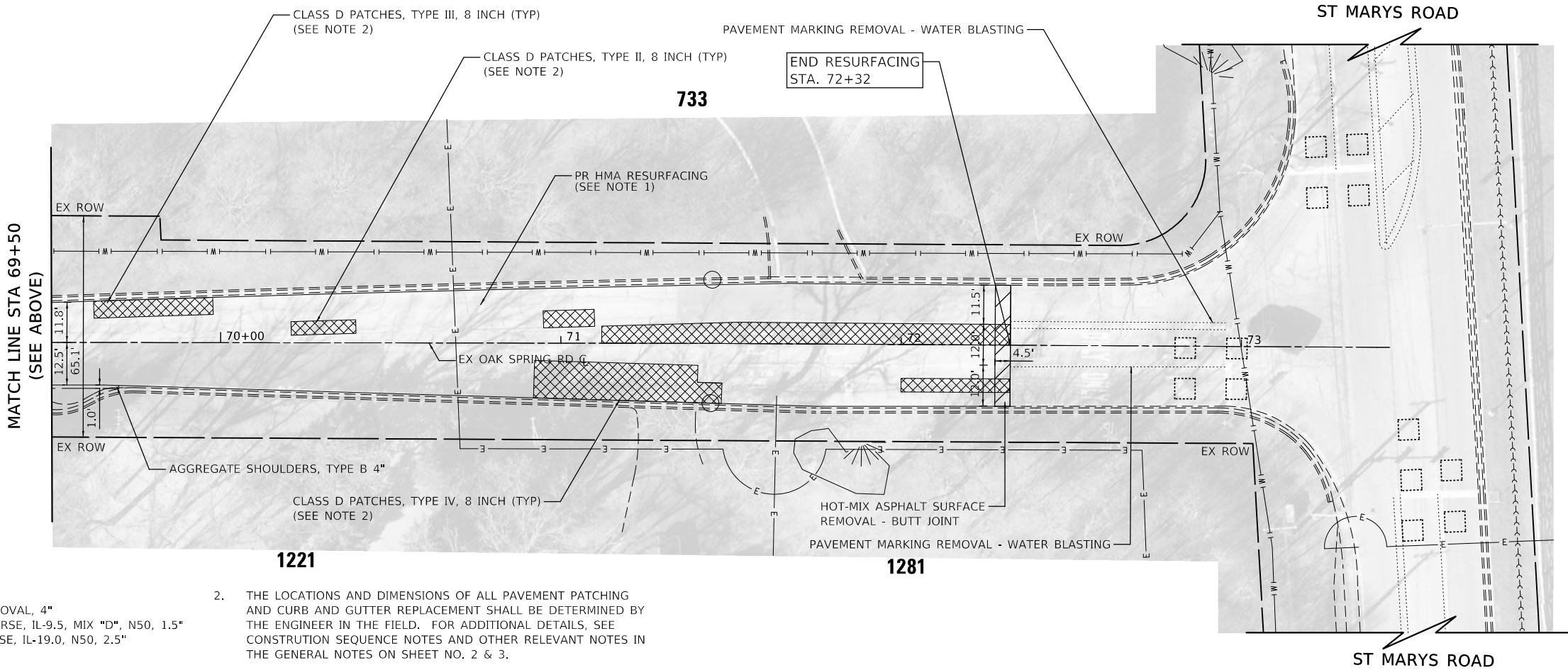
OAK SPRING ROAD
ROADWAY PLAN

SCALE: 1"=20' SHEET 5 OF 6 SHEETS STA. 53+00 TO STA. 64+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	16
CONTRACT NO. 61183				
ILLINOIS FED. AID PROJECT				



OAK SPRING ROAD - PLAN



NOTES

1.

PR HMA RESURFACING:
-HOT-MIX ASPHALT SURFACE REMOVAL, 4"
-HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1.5"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
2.

THE LOCATIONS AND DIMENSIONS OF ALL PAVEMENT PATCHING AND CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. FOR ADDITIONAL DETAILS, SEE CONSTRUCTION SEQUENCE NOTES AND OTHER RELEVANT NOTES IN THE GENERAL NOTES ON SHEET NO. 2 & 3.

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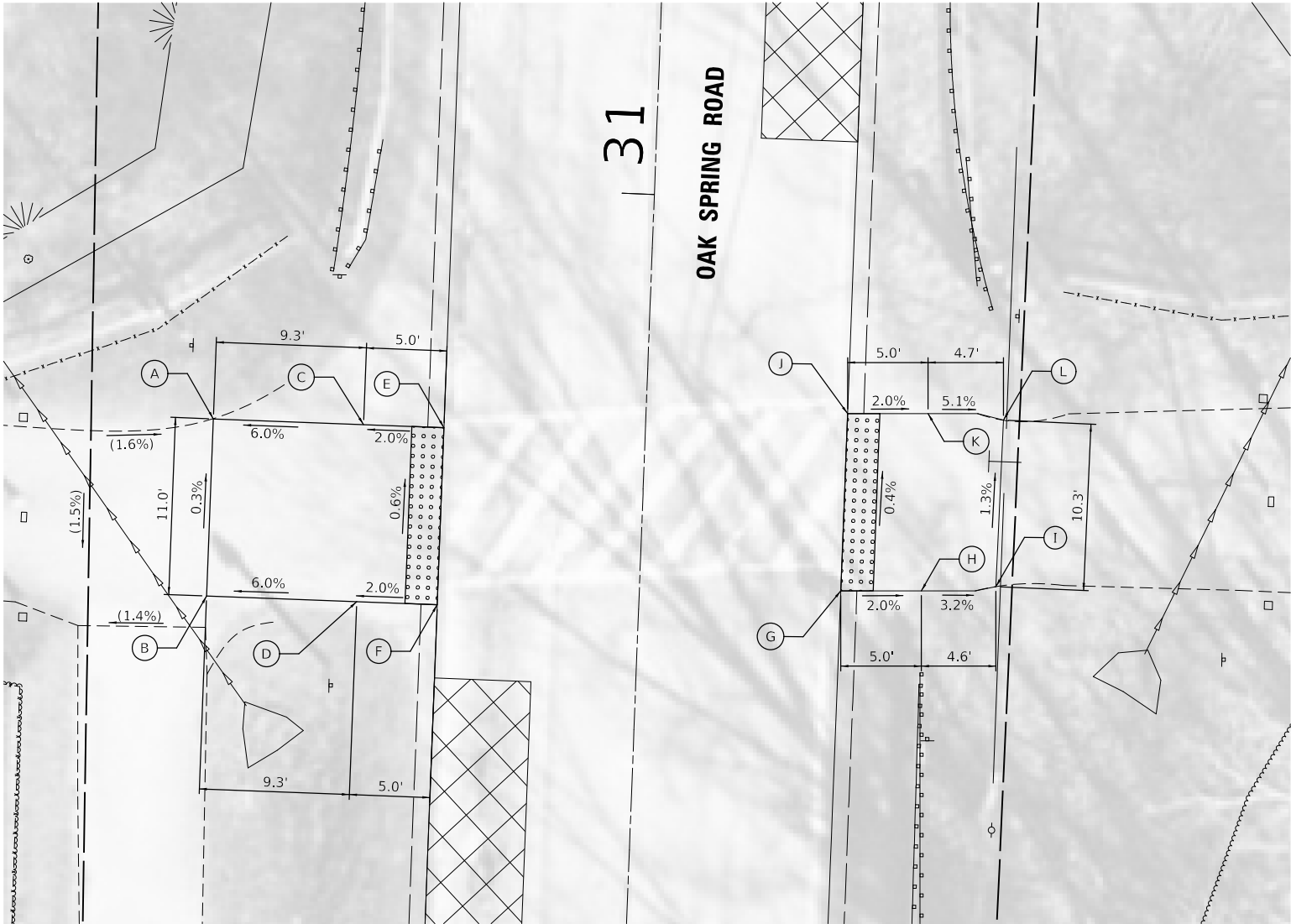
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
ROADWAY PLAN

SCALE: 1"=20' SHEET 6 OF 6 SHEETS STA. 64+00 TO STA. 72+32

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	17
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				

BIKE PATH CROSSING				
	STATION	OFFSET		ELEVATION
A	30+85.04	26.82'	LT	653.22
B	30+74.04	26.82'	LT	653.25
C	30+85.04	19.66'	LT	653.78
D	30+74.04	19.66'	LT	653.81
E	30+85.04	12.50'	LT	653.88
F	30+74.04	12.50'	LT	653.91
G	30+75.85	12.50'	RT	654.14
H	30+76.01	16.67'	RT	654.04
I	30+76.49	22.11'	RT	653.89
J	30+86.86	12.50'	RT	654.10
K	30+87.01	16.54'	RT	654.00
L	30+86.82	22.18'	RT	653.76



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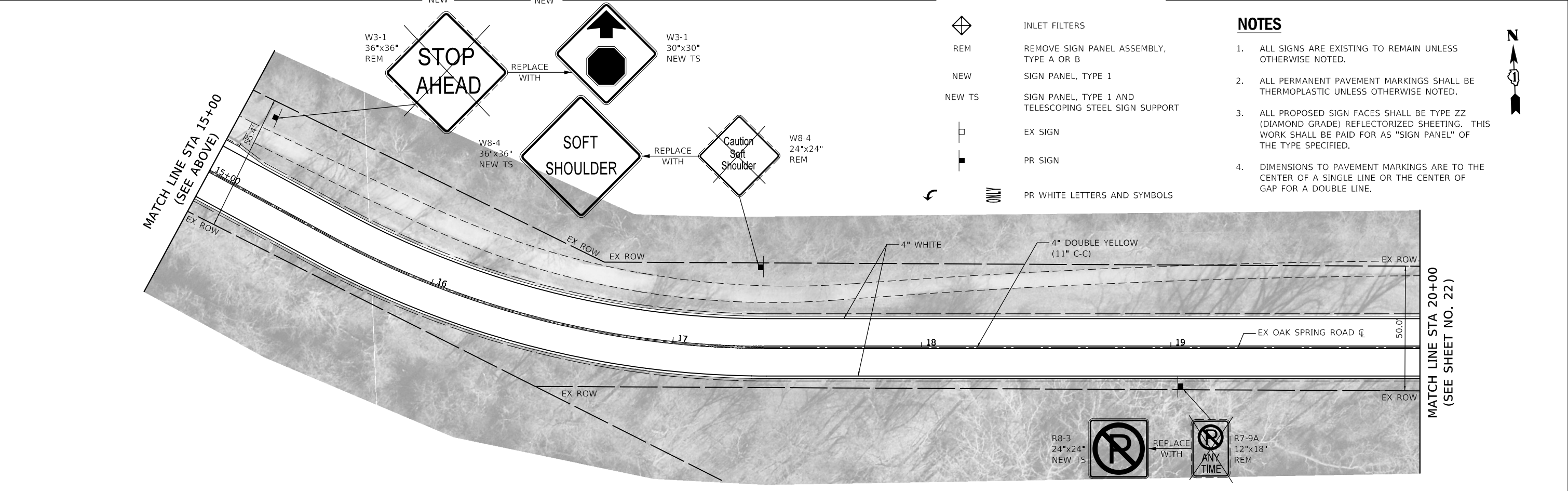
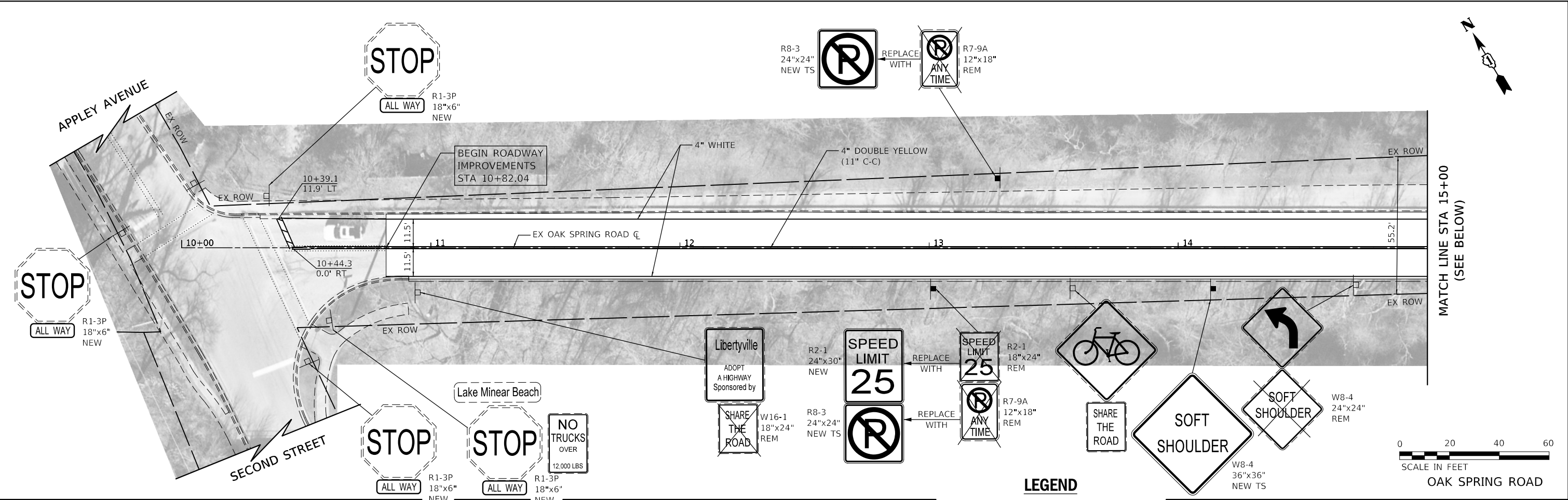
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

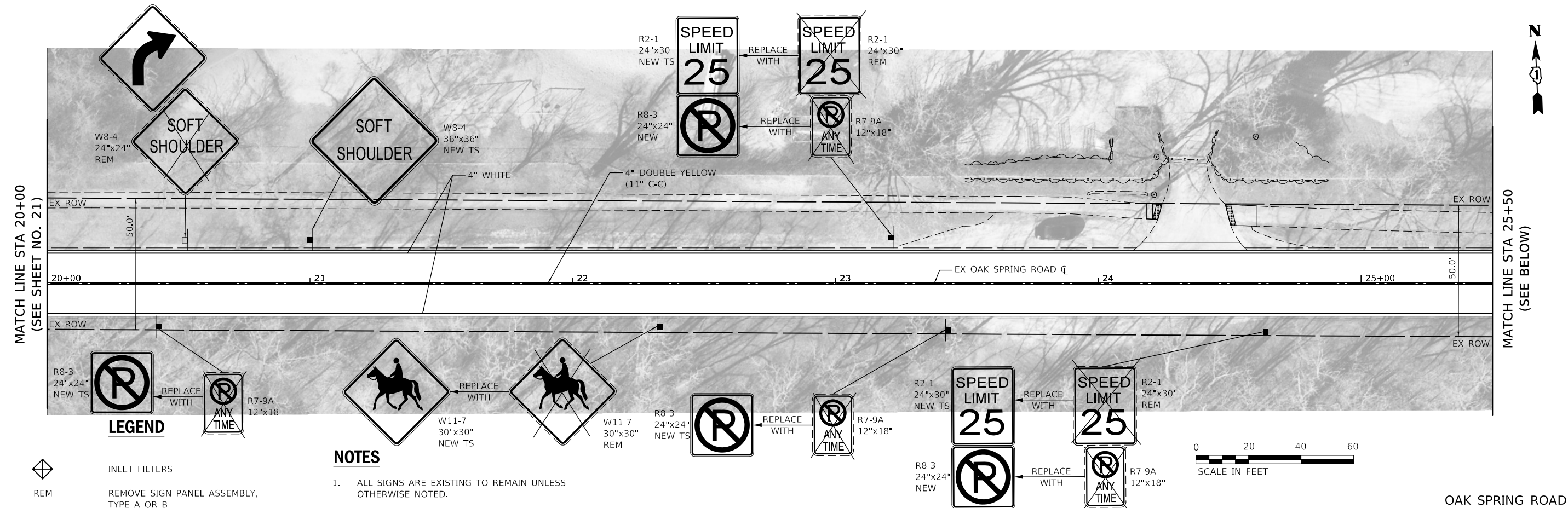
OAK SPRING ROAD
GRADING PLAN

SCALE: 1"=5' SHEET 1 OF 3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	18
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				

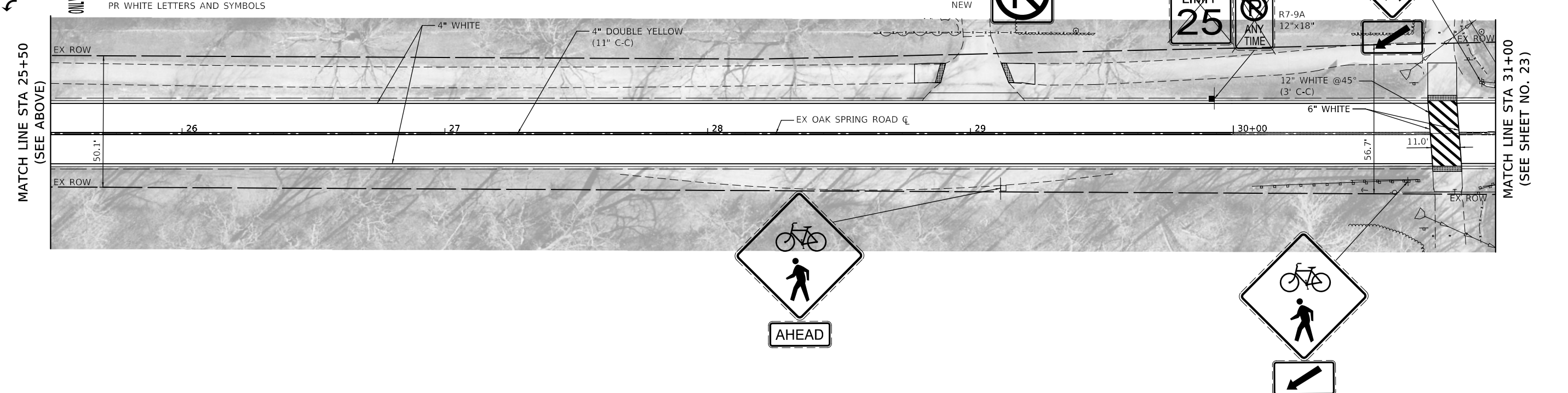


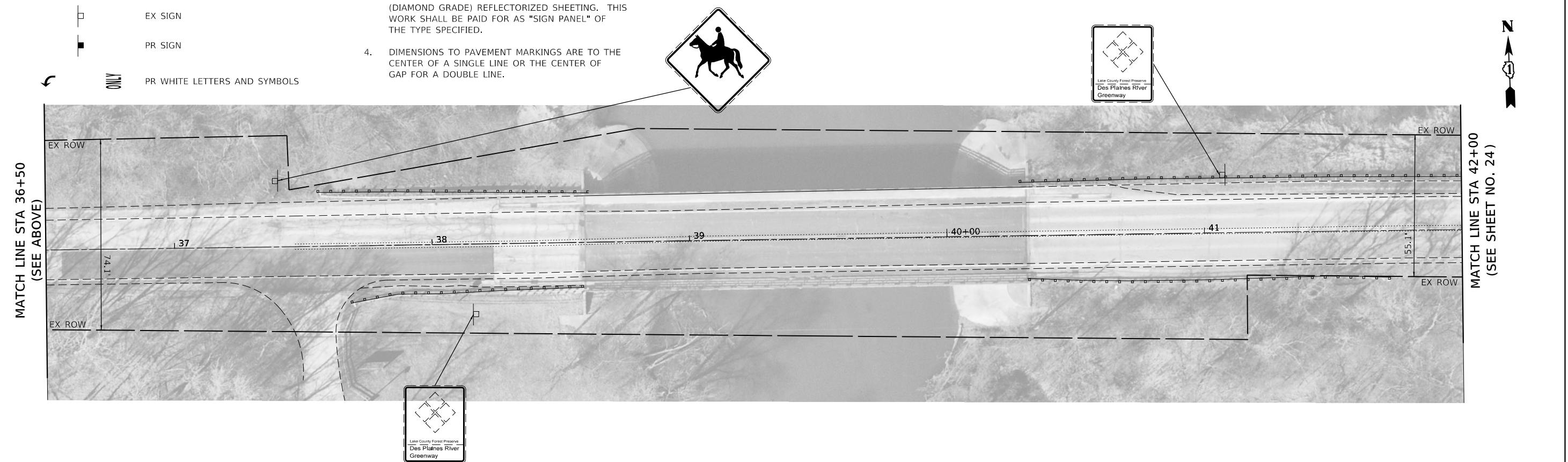
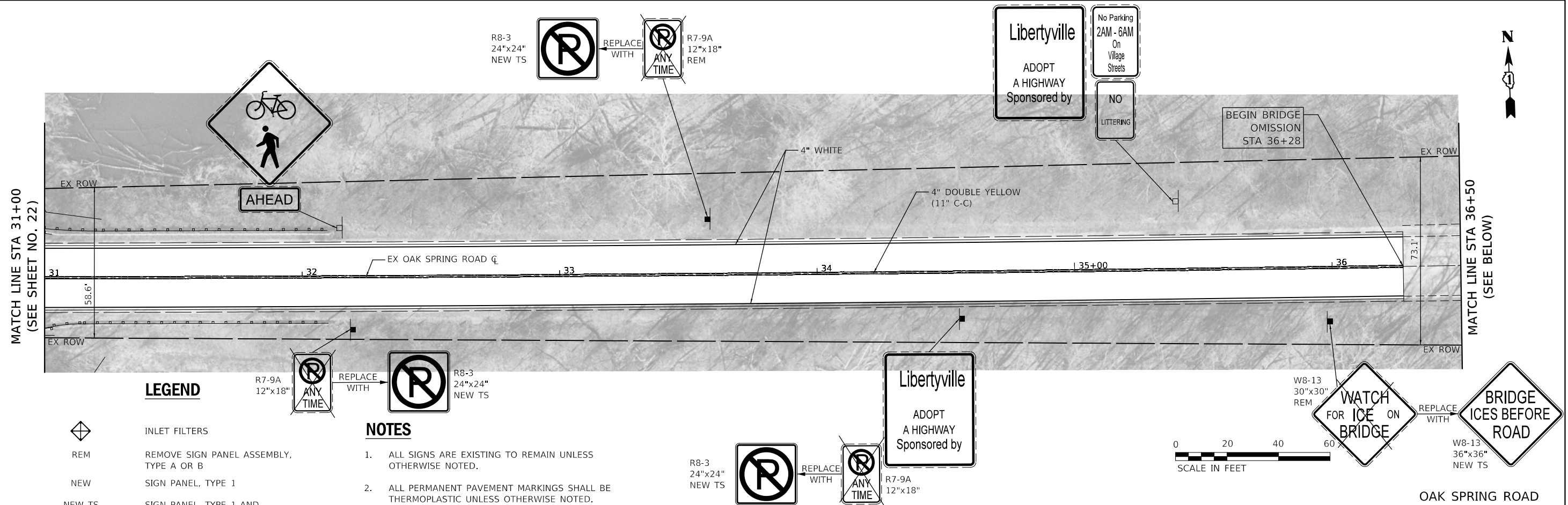
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	PLOT SCALE = 40,0000 ' / in.		DATE - 5/16/2025	REVISED -		SHEET 1 OF 6 SHEETS		CONTRACT NO. 61183				
	PLOT DATE = 8/27/2025					STA. 10+82.04 TO STA. 20+00		ILLINOIS / FED. AID PROJECT				



- LEGEND**
- INLET FILTERS
 - REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
 - NEW SIGN PANEL, TYPE 1
 - NEW TS SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
 - EX SIGN
 - PR SIGN
 - ONLY PR WHITE LETTERS AND SYMBOLS

- NOTES**
- ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
 - ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
 - ALL PROPOSED SIGN FACES SHALL BE TYPE ZZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
 - DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.





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PLOT DATE = 8/27/2025	DATE = 5/16/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

OAK SPRING ROAD
PAVEMENT MARKING, SIGNING, AND EROSION CONTROL PLAN

SCALE: 1"=20'	SHEET 3 OF 6 SHEETS	STA. 31+00 TO STA. 42+00
---------------	---------------------	--------------------------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-R5	LAKE	36	23
		CONTRACT NO. 61L83		
ILLINOIS		FED. AID PROJECT		

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MODEL: S:\MODEL\NAMES
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W8-13
30"x30"
REM

WATCH
FOR ICE
ON
BRIDGE

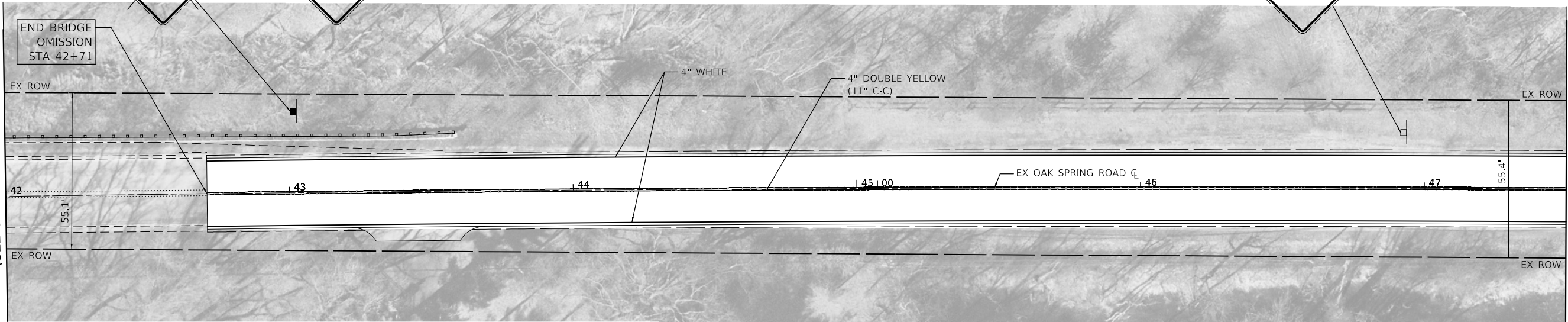
REPLACE
WITH

BRIDGE
ICES BEFORE
ROAD

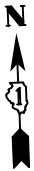
W8-13
36"x36"
NEW TS

WILDLIFE
CROSSING

MATCH LINE STA 42+00
(SEE SHEET NO. 23)



MATCH LINE STA 47+50
(SEE BELOW)



LEGEND

- INLET FILTERS
- REM REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
- NEW SIGN PANEL, TYPE 1
- NEW TS SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
- EX SIGN
- PR SIGN
- PR WHITE LETTERS AND SYMBOLS



OAK SPRING ROAD

NOTES

- ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
- ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
- ALL PROPOSED SIGN FACES SHALL BE TYPE ZZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
- DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



W14-3
48"x48"x36"
NEW TS

NO
PASSING

REPLACE
WITH

W14-3
40"x33"
REM

NO
PASSING

REPLACE
WITH

R2-1
24"x30"
NEW TS

SPEED
LIMIT
35

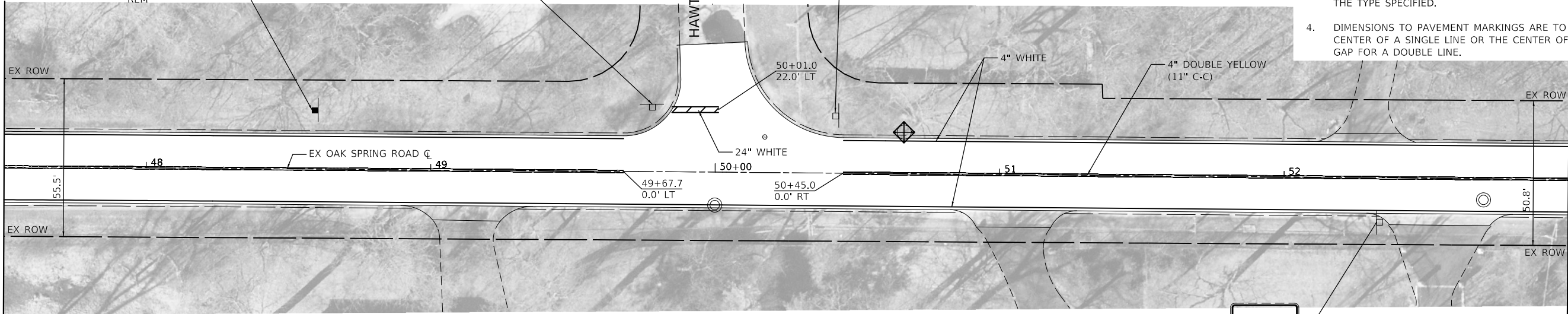
SPEED
LIMIT
35

STOP

NO OUTLET
Hawthorne LN

HAWTHORNE LANE

MATCH LINE STA 47+50
(SEE ABOVE)



MATCH LINE STA 53+00
(SEE SHEET NO. 25)

SPEED
LIMIT
35

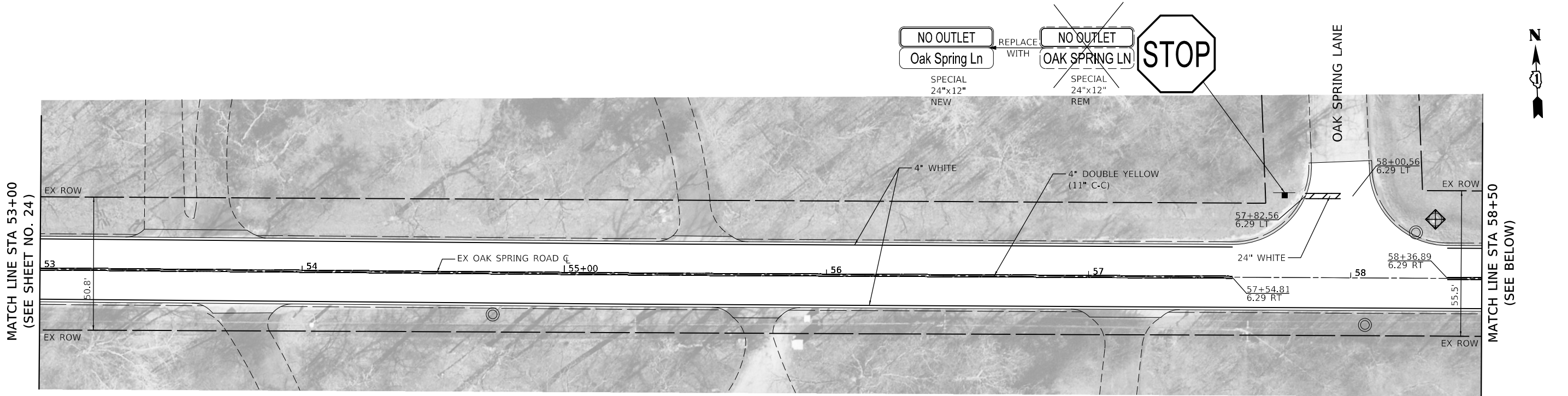
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PLOT DATE	= 8/27/2025	DATE	- 5/16/2025	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION




OAK SPRING ROAD
PAVEMENT MARKING, SIGNING, AND EROSION CONTROL PLAN

SCALE: 1"=20' SHEET 4 OF 6 SHEETS STA. 42+00 TO STA. 53+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	24
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



LEGEND

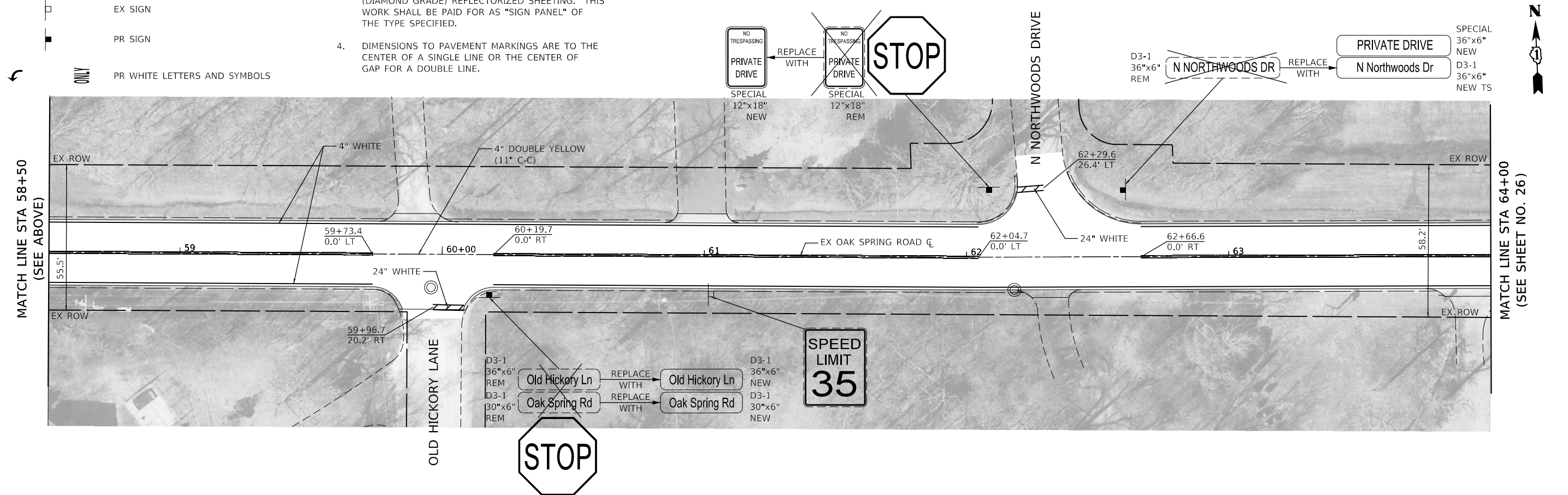
- 
REM
NEW
NEW TS
- INLET FILTERS
REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B
SIGN PANEL, TYPE 1
SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
- 

ONLY
- EX SIGN
PR SIGN
PR WHITE LETTERS AND SYMBOLS

NOTES

- ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
- ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
- ALL PROPOSED SIGN FACES SHALL BE TYPE ZZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
- DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



OAK SPRING ROAD



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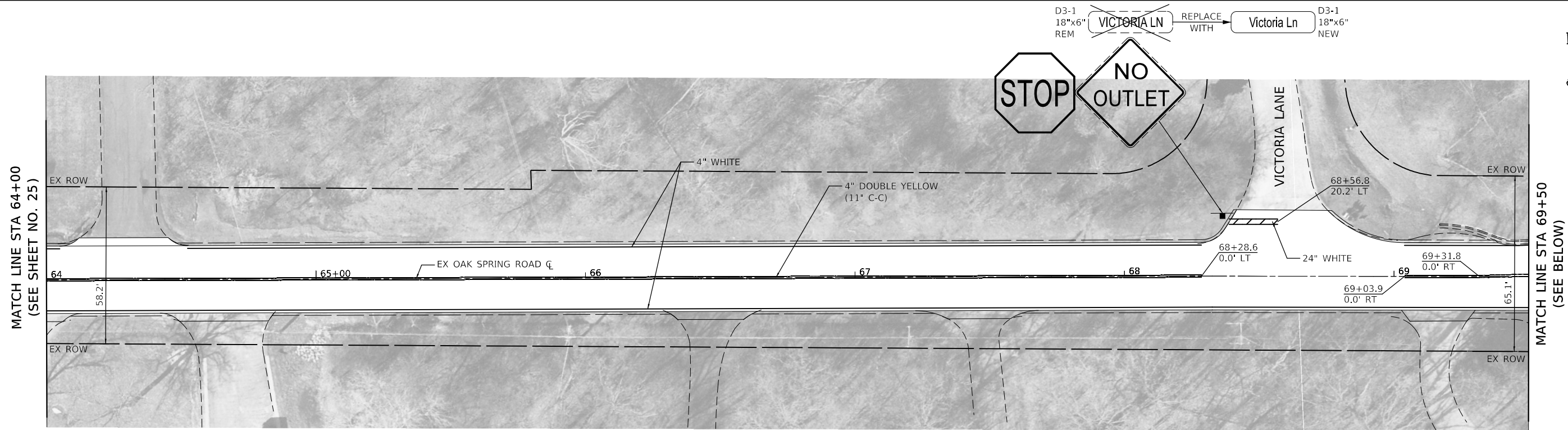
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
PAVEMENT MARKING, SIGNING, AND EROSION CONTROL PLAN

SCALE: 1"=20' SHEET 5 OF 6 SHEETS STA. 53+00 TO STA. 64+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	25
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



LEGEND

- REM

NEW

NEW TS

ONLY
- INLET FILTERS

REMOVE SIGN PANEL ASSEMBLY, TYPE A OR B

SIGN PANEL, TYPE 1

SIGN PANEL, TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT

EX SIGN

PR SIGN

PR WHITE LETTERS AND SYMBOLS

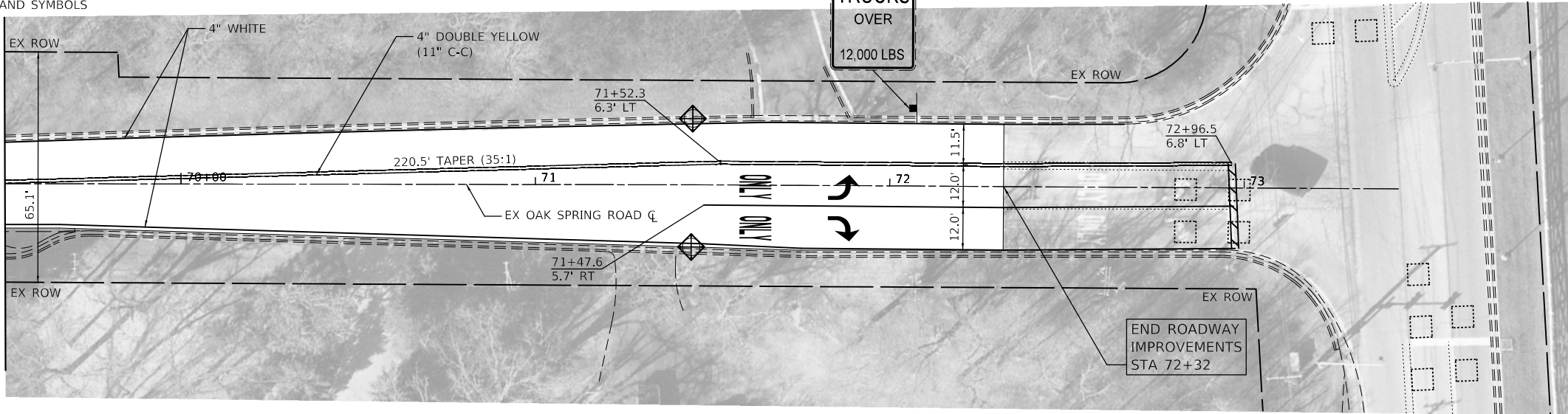
NOTES

1. ALL SIGNS ARE EXISTING TO REMAIN UNLESS OTHERWISE NOTED.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
3. ALL PROPOSED SIGN FACES SHALL BE TYPE ZZ (DIAMOND GRADE) REFLECTORIZED SHEETING. THIS WORK SHALL BE PAID FOR AS "SIGN PANEL" OF THE TYPE SPECIFIED.
4. DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.



OAK SPRING ROAD

MATCH LINE STA 69+50
(SEE ABOVE)



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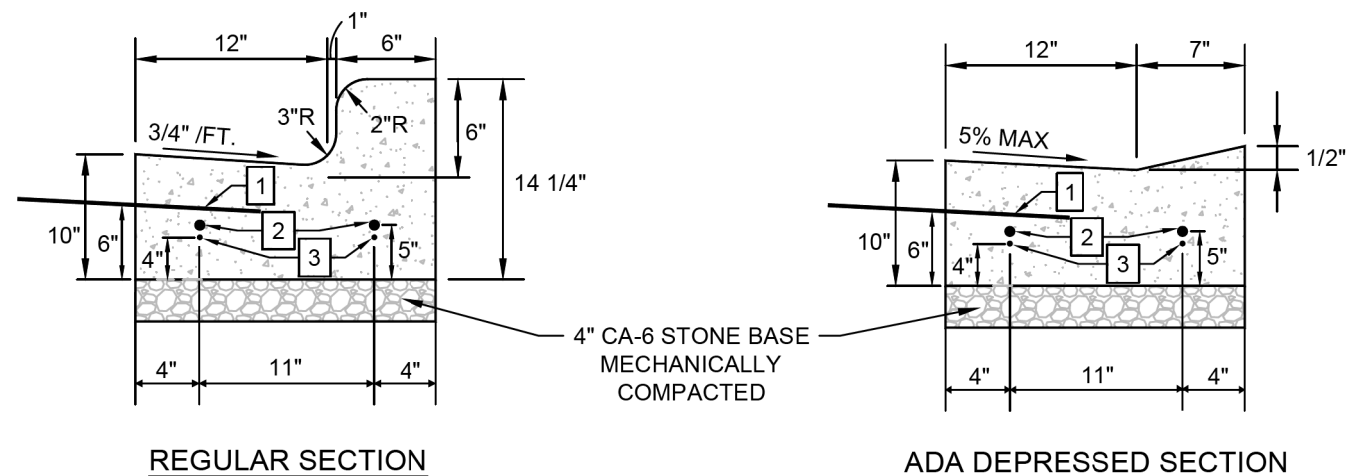
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

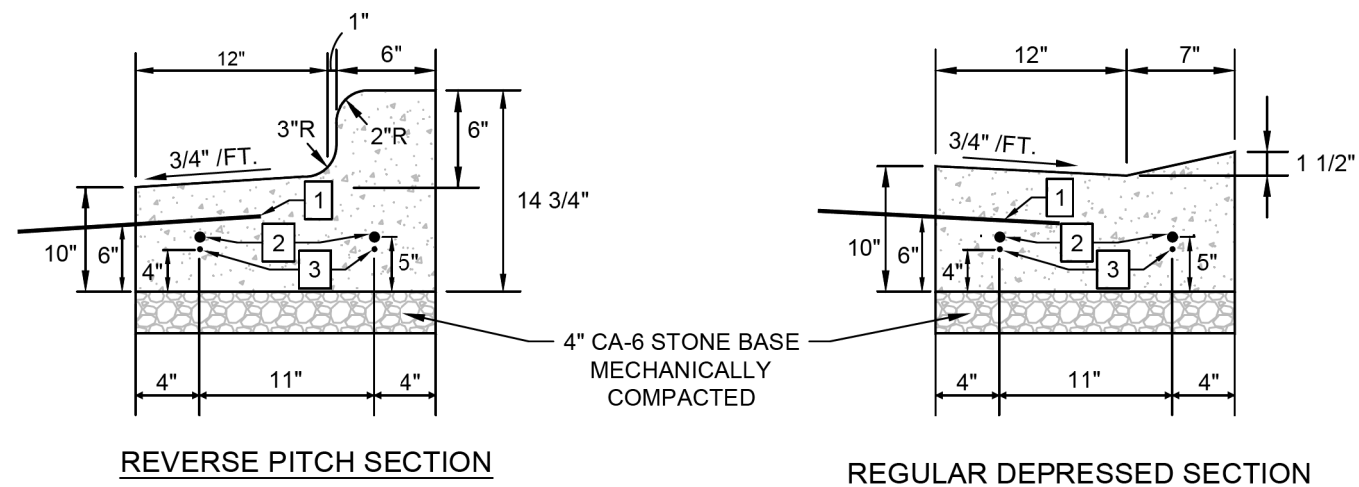
OAK SPRING ROAD
PAVEMENT MARKING, SIGNING, AND EROSION CONTROL PLAN

SCALE: 1"=20' SHEET 6 OF 6 SHEETS STA. 64+00 TO STA. 72+32

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	26
CONTRACT NO. 61183				
ILLINOIS FED. AID PROJECT				



- 1 24" LONG #6 ($\frac{3}{8}$ ") EPOXY-COATED, DEFORMED TIE BARS, GRADE 60 INSTALLED 24" ON-CENTER, EMBEDDED 8" AND EPOXIED IN PLACE (FOR P.C.C. PAVEMENT ONLY).
- 2 TWO (2) - #8 EPOXY COATED SMOOTH DOWEL BARS WITH GREASED CAPS TO BE INSTALLED AT ALL EXPANSION JOINTS
- 3 TWO (2) - #4 ($\frac{1}{2}$ ") EPOXY COATED DEFORMED CONTINUOUS STEEL REINFORCEMENT BARS ARE TO BE INSTALLED ALONG THE ENTIRE LENGTH OF THE CURB ONLY TO BE INTERRUPTED AT EXPANSION JOINTS. SUPPORT BARS WITH CHAIRS EVERY FIVE (5) FEET, MINIMUM OF TWO (2) CHAIRS.



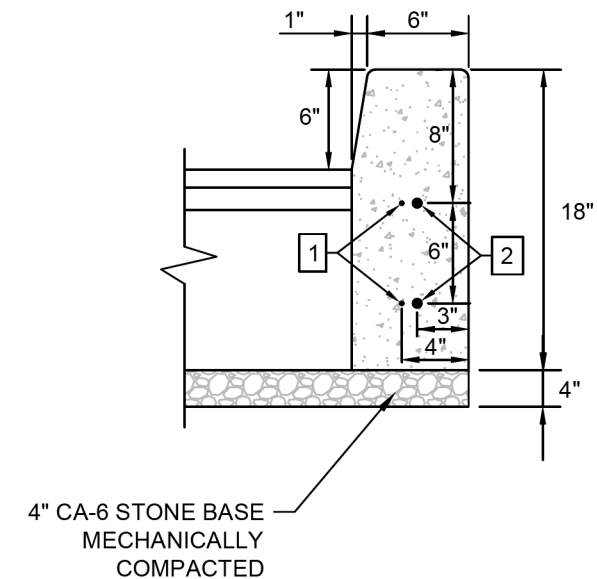
NOTES:

1. EXPANSION JOINTS TO BE SET AT ALL PC'S, PT'S, FIVE (5) FEET ON EITHER SIDE OF ANY FRAMES, AND AT 45' MAX. INTERVALS. ADDITIONAL LOCATIONS MAY BE SPECIFIED BY THE ENGINEER.
2. CONTRACTION JOINTS ARE TO BE CONSTRUCTED AT 15' MAX. INTERVALS AND AT ADDITIONAL LOCATIONS AS MAY BE SPECIFIED BY ENGINEER.
3. ANY CONTINUOUS CURB REMOVALS OF TEN (10) FEET OR GREATER IN LENGTH SHALL HAVE CONTINUOUS STEEL REINFORCEMENT BARS PER [3] ABOVE. REMOVALS LESS THAN TEN (10) FEET IN LENGTH SHALL ONLY HAVE DOWEL BARS INSTALLED AT ENDS PER [2] ABOVE.

B6.12 CURB & GUTTER

Libertyville

LAST
REVISED:
02/15/2023



- 1 TWO (2) - #4 ($\frac{1}{2}$ ") EPOXY COATED DEFORMED CONTINUOUS STEEL REINFORCEMENT BARS ARE TO BE INSTALLED ALONG THE ENTIRE LENGTH OF THE CURB ONLY TO BE INTERRUPTED AT EXPANSION JOINTS. SUPPORT BARS WITH CHAIRS EVERY FIVE (5) FEET, MINIMUM OF TWO (2) CHAIRS.
- 2 TWO (2) - #8 EPOXY COATED SMOOTH DOWEL BARS WITH GREASED CAPS TO BE INSTALLED AT ALL EXPANSION JOINTS

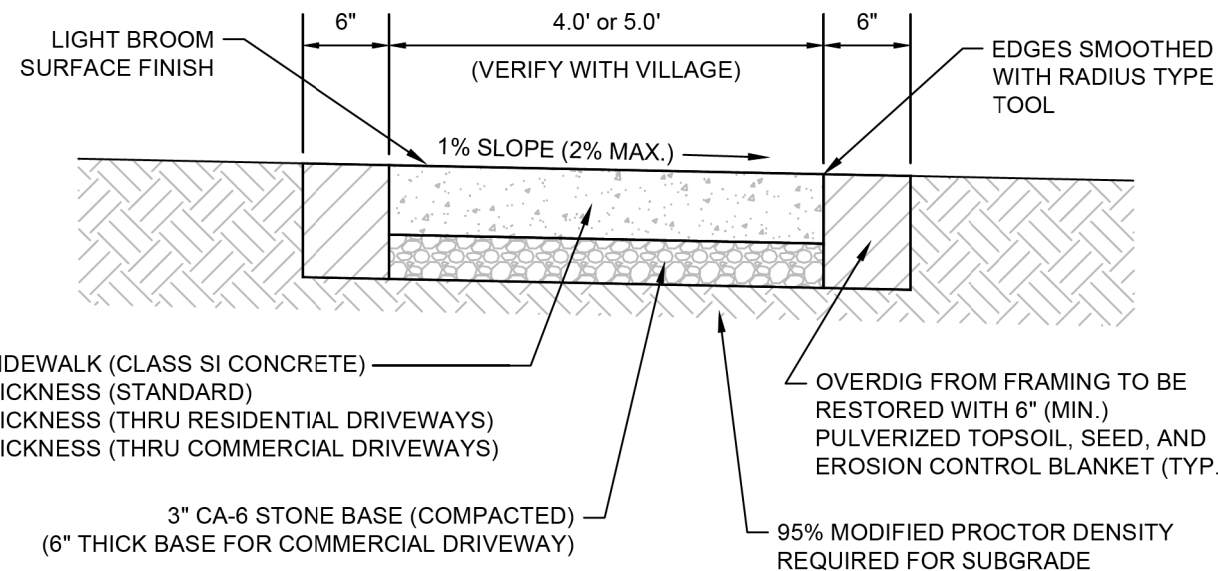
NOTES:

1. EXPANSION JOINTS TO BE SET AT ALL PC'S, PT'S, FIVE (5) FEET ON EITHER SIDE OF ANY FRAMES, AND AT 45' MAX. INTERVALS. ADDITIONAL LOCATIONS MAY BE SPECIFIED BY THE ENGINEER.
2. CONTRACTION JOINTS ARE TO BE CONSTRUCTED AT 15' MAX. INTERVALS AND AT ADDITIONAL LOCATIONS AS MAY BE SPECIFIED BY ENGINEER.
3. ANY CONTINUOUS CURB REMOVALS OF TEN (10) FEET OR GREATER IN LENGTH SHALL HAVE CONTINUOUS STEEL REINFORCEMENT BARS PER [1] ABOVE. REMOVALS LESS THAN TEN (10) FEET IN LENGTH SHALL ONLY HAVE DOWEL BARS INSTALLED AT ENDS PER [2] ABOVE.

TYPE B BARRIER CURB

Libertyville

LAST
REVISED:
02/15/2023



NOTES:

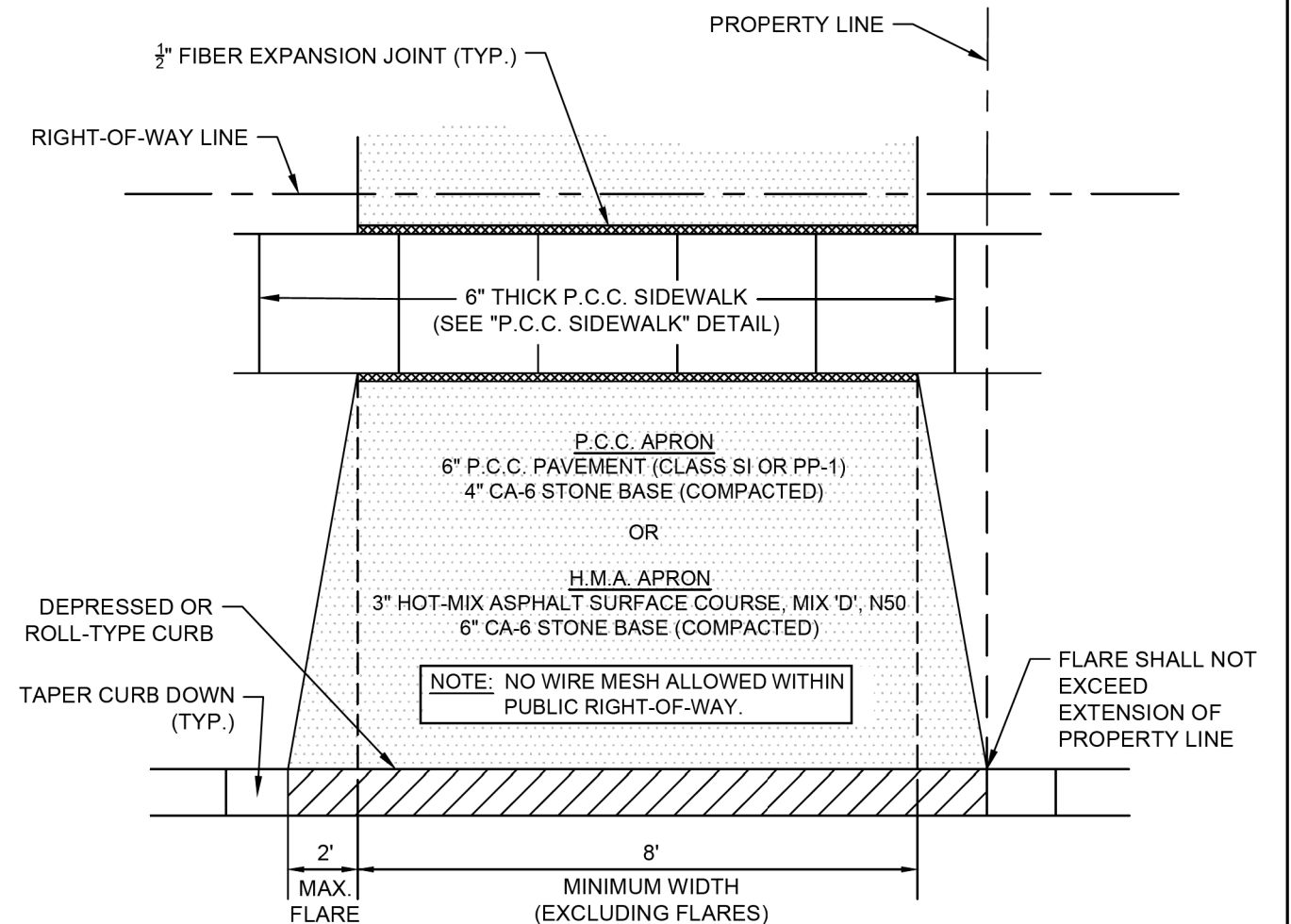
1. REFER TO THE VILLAGE "ENGINEERING DESIGN & CONSTRUCTION STANDARDS" FOR FURTHER REQUIREMENTS.
2. PROVIDE 1/2" THICK FIBER EXPANSION JOINTS AT POINTS ABUTTING CURB OR PAVEMENT AND AT 50-FOOT MAXIMUM INTERVALS.
3. CONTROL JOINTS SHALL BE 1/8" TO 1/4" WIDE AND 1/4 OF THE SIDEWALK THICKNESS DEEP. THE EDGE OF THE CONTROL JOINTS SHALL BE GIVEN A 1/4" RADIUS.
4. CONTROL JOINTS TO BE PROVIDED AT 5' ON-CENTER SPACING. SPACING CAN BE INCREASED FOR SIDEWALKS IN EXCESS OF 5-FOOT IN WIDTH AS DIRECTED BY THE ENGINEER.
5. PLACING, FINISHING, AND CURING OF P.C.C. SIDEWALK SHALL MEET THE REQUIREMENTS OF SECTION 424 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

P.C.C. SIDEWALK



LAST
REVISED:
02/15/2023

STANDARD DETAIL #
PAV - 05



NOTES:

1. ALL EXISTING ABUTTING EXPANSION JOINT MATERIAL SHALL BE REMOVED AND REPLACED.
2. WHITE CURING COMPOUND, PER IDOT SECTION 1022.01(C), SHALL BE APPLIED TO THE CONCRETE IMMEDIATELY AFTER IT HAS BEEN FINISHED OR PER MANUFACTURER'S RECOMMENDATION.
3. DRIVEWAY FLARE SHALL BE A MAXIMUM OF 2-FEET WIDE AS MEASURED FROM THE SIDEWALK TO THE BACK-OF-CURB. IF NO SIDEWALK EXISTS, THE WIDTH OF THE FLARE SHALL BE MEASURED FROM A POINT 6-FEET BEHIND THE BACK-OF-CURB TO THE BACK-OF-CURB.
4. PRE-POUR INSPECTION REQUIRED BY THE ENGINEERING DIVISION PRIOR TO PLACEMENT OF PAVEMENT AFTER FORMING IS IN PLACE. PLEASE CALL (847) 918-2020 TO SCHEDULE INSPECTIONS.

RESIDENTIAL
DRIVEWAY APRON



LAST
REVISED:
02/15/2023

STANDARD DETAIL #
PAV - 07

MODEL: STANDARD NAMES
FILE NAME: 230215-PAV-05.dgn

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	DRAWN - JRS	REVISED -
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PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

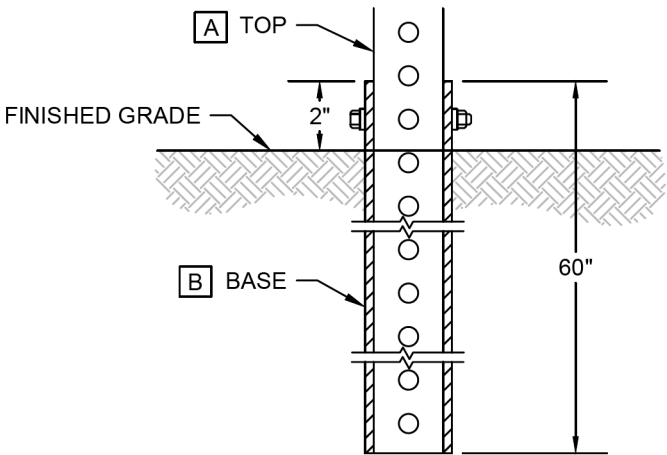
OAK SPRING ROAD
CONSTRUCTION DETAILS

SHEET 2 OF 4 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	28
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				

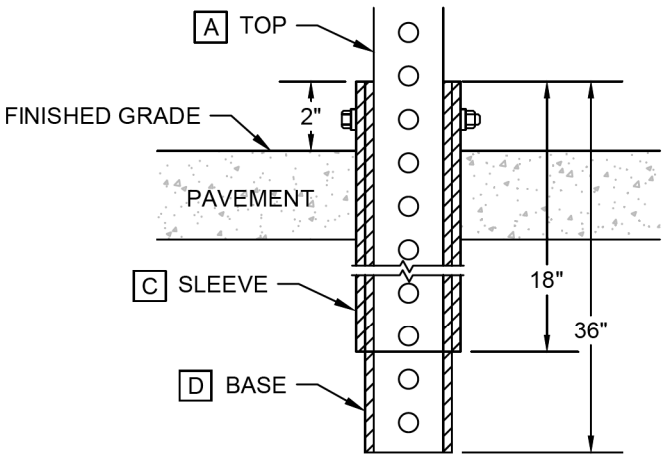
ALL SIGN POSTS TO BE GALVANIZED STEEL SQUARE POSTS, 2"x2", FULL PUNCHED.

GROUND
MOUNT
DETAIL



- [A] TOP - 2" x 2" x VARIABLE LENGTH
[B] BASE FOR GROUND MOUNT - 2" x 2" x 60"
[C] SLEEVE FOR PAVEMENT MOUNT - 2-1/2" x 2-1/2" x 18"
[D] BASE FOR PAVEMENT MOUNT - 2-1/4" x 2-1/4" x 36"

PAVEMENT
MOUNT
DETAIL



NOTES:

1. INSTALLATION HEIGHTS OF ALL SIGNAGE SHALL BE IN ACCORDANCE STATE OR OTHER APPLICABLE REGULATIONS.
2. ALL BOLTS AND OTHER ASSEMBLY MATERIAL SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
3. IF A SURFACE ANCHOR BOLT MOUNT IS REQUIRED, INSTALLATION SHALL BE PERFORMED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.

SIGN POST



LAST
REVISED:
02/15/2023

STANDARD DETAIL #
PAV - 10

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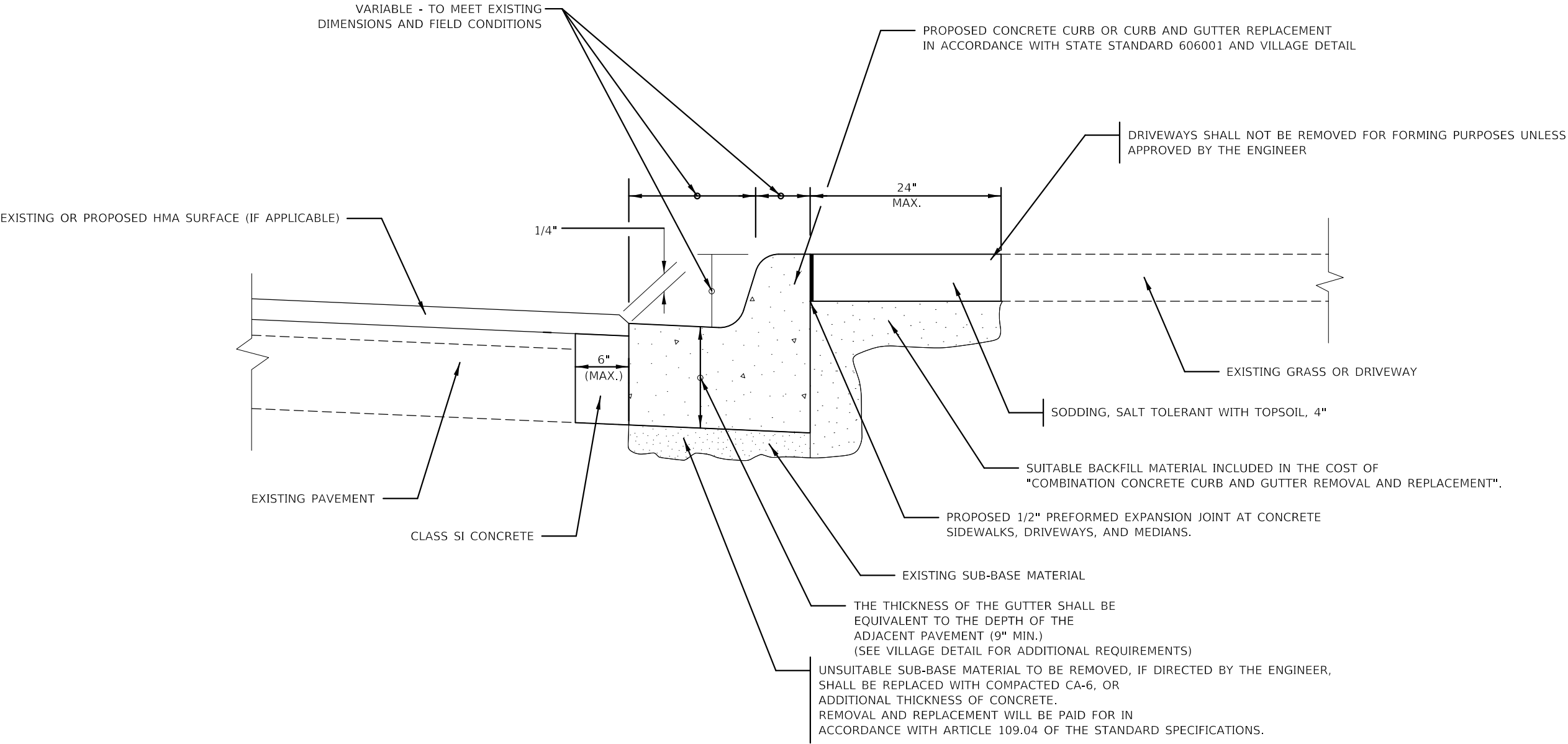
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PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

OAK SPRING ROAD
CONSTRUCTION DETAILS

SHEET 3 OF 4 SHEETS

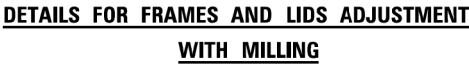
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1234	23-00127-00-RS	LAKE	36	29
CONTRACT NO. 61L83				
ILLINOIS FED. AID PROJECT				



CURB AND GUTTER REMOVAL AND REPLACEMENT DETAIL

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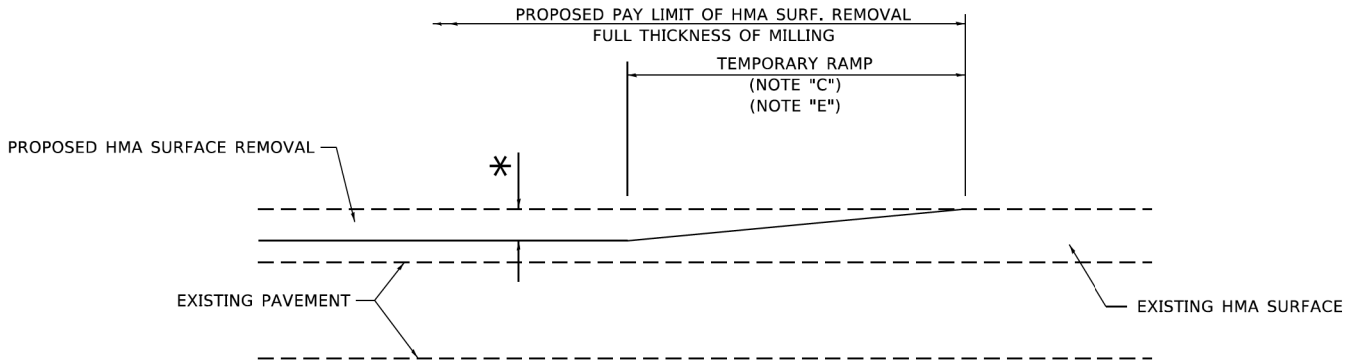
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	PLOT DATE = 8/17/2025	DATE - 5/16/2025	REVISED -					ILLINOIS FED. AID PROJECT				
						SCALE: N.T.S.	SHEET 4 OF 4 SHEETS	STA.	TO STA.			



1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

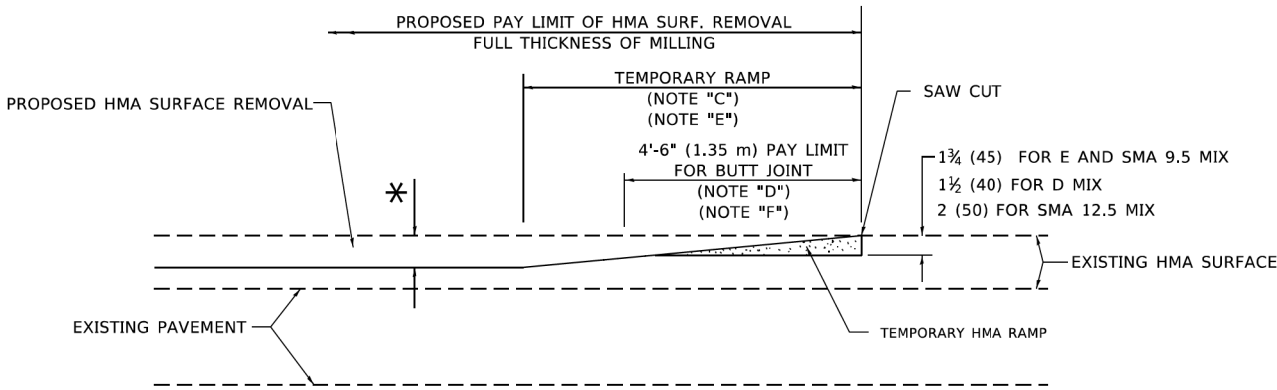
1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	31
BD600-03 (BD-08)		CONTRACT NO. 61L83		
ILLINOIS		FED. AID PROJECT		



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

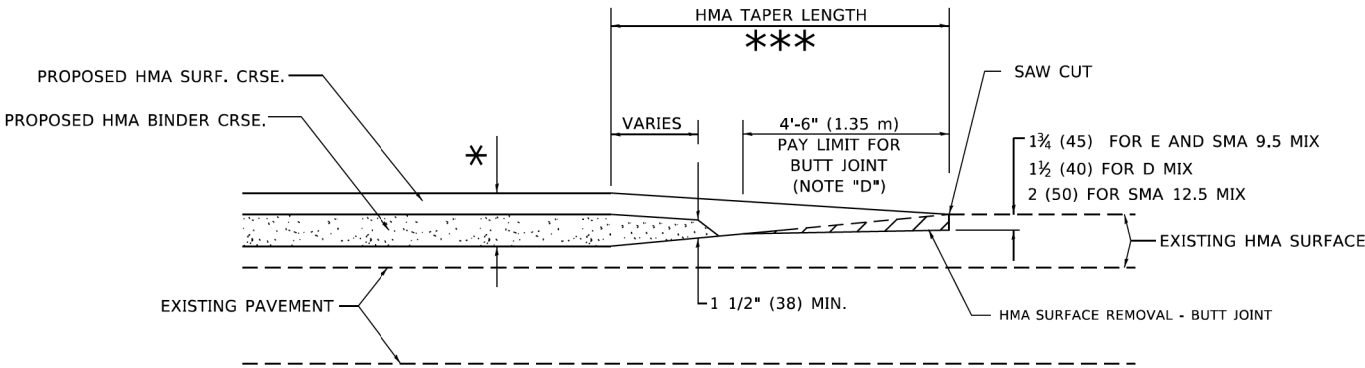
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

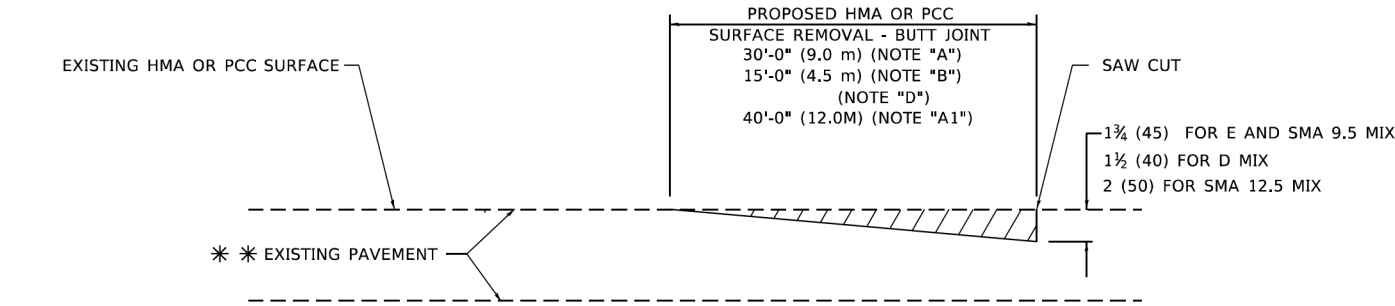
OPTION 2

TYPICAL TEMPORARY RAMP

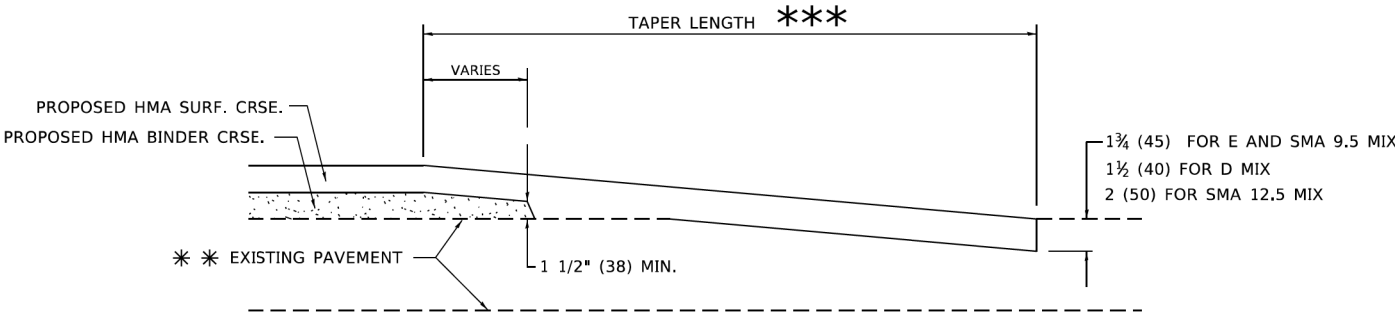


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD-32		CONTRACT NO. 61L83		
ILLINOIS		FED. AID PROJECT		

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	DRAWN	-				REVISED	-	A. SCHUETZE 07-01-13
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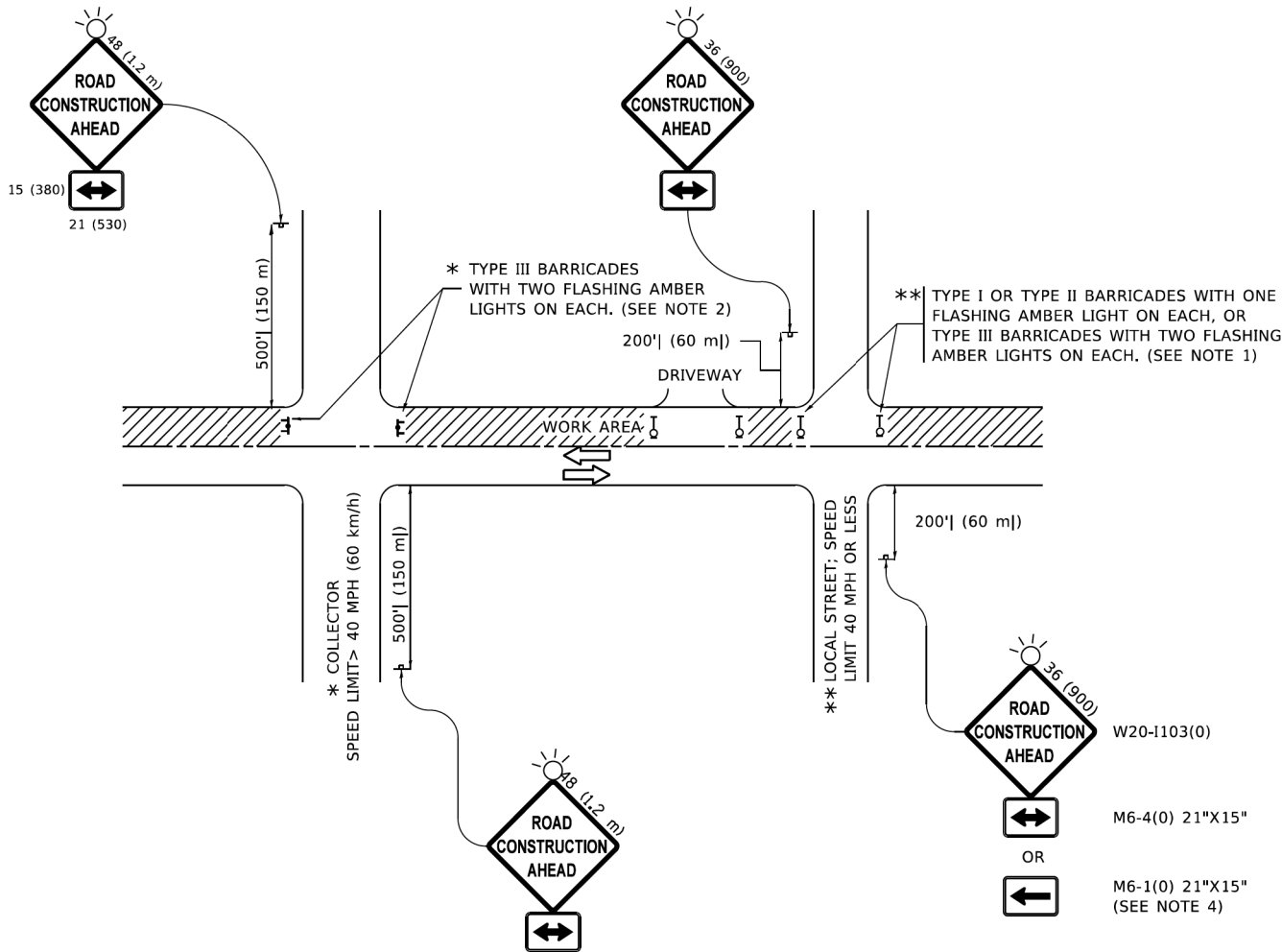
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

All dimensions are in inches (millimeters)
unless otherwise shown.

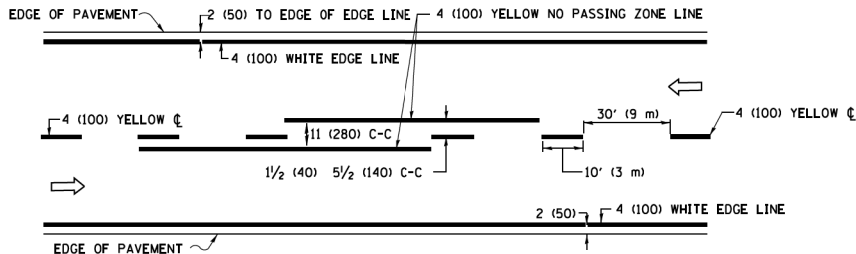
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TC-10		CONTRACT NO. 61L83		
		ILLINOIS	FED. AID PROJECT	



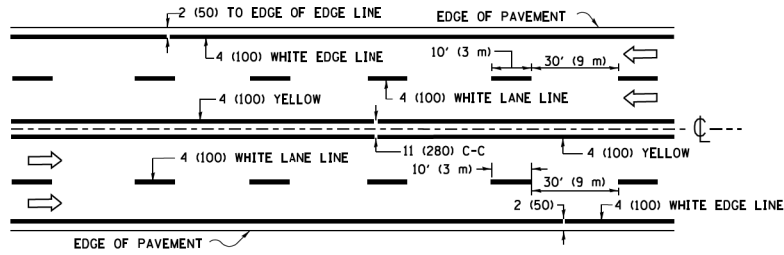
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

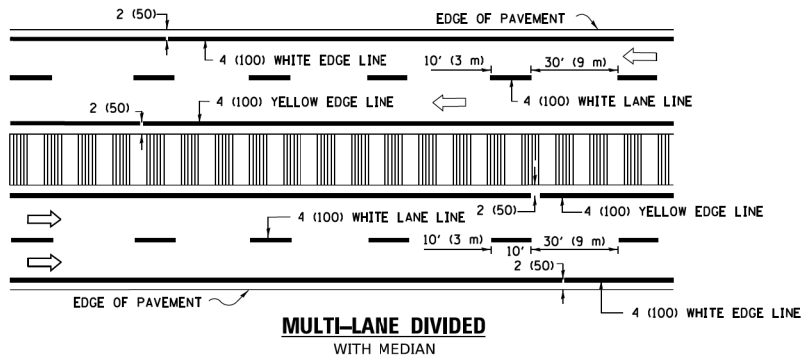
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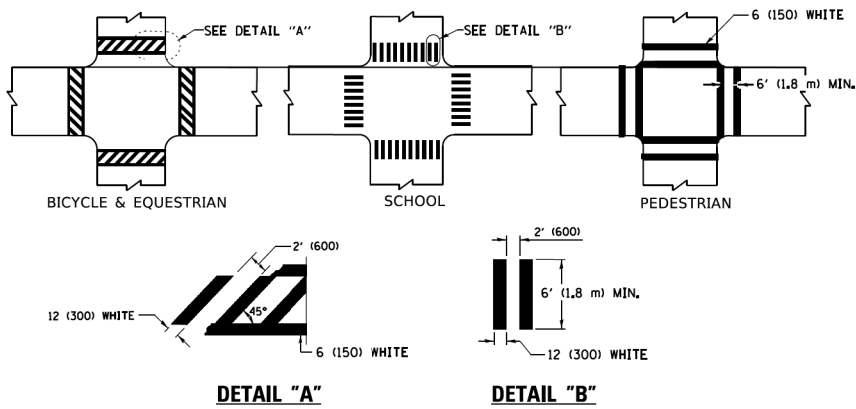
2-LANE ROADWAY



MULTI-LANE UNDIVIDED

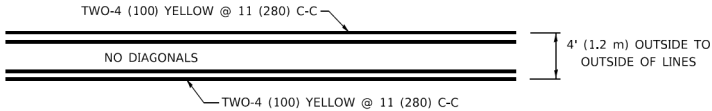


TYPICAL LANE AND EDGE LINE MARKING

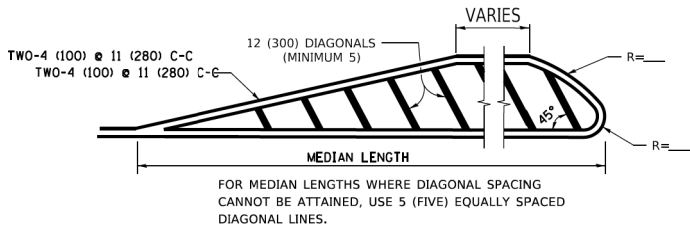


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

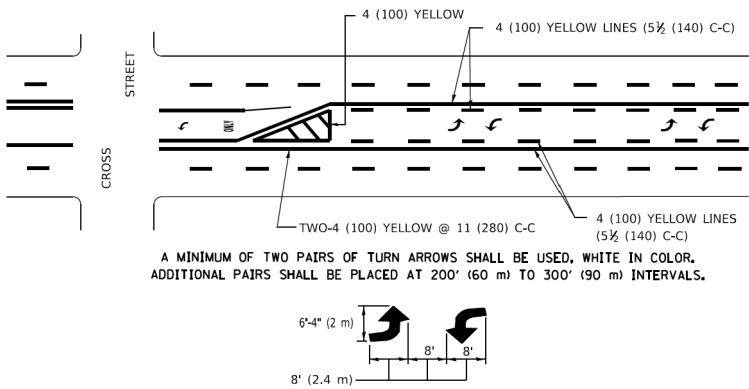


4' (1.2 m) WIDE MEDIANS ONLY



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

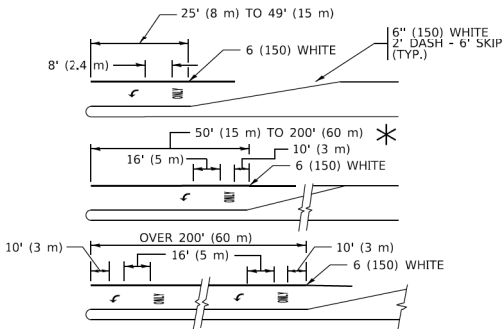
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

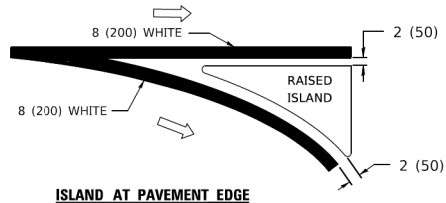
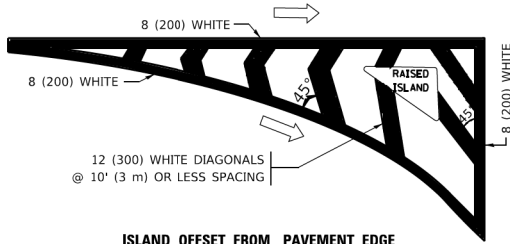


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

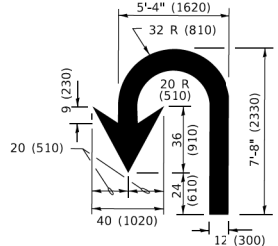
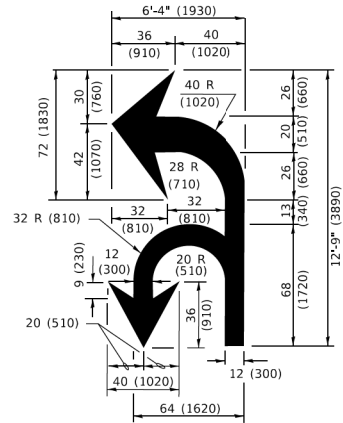
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

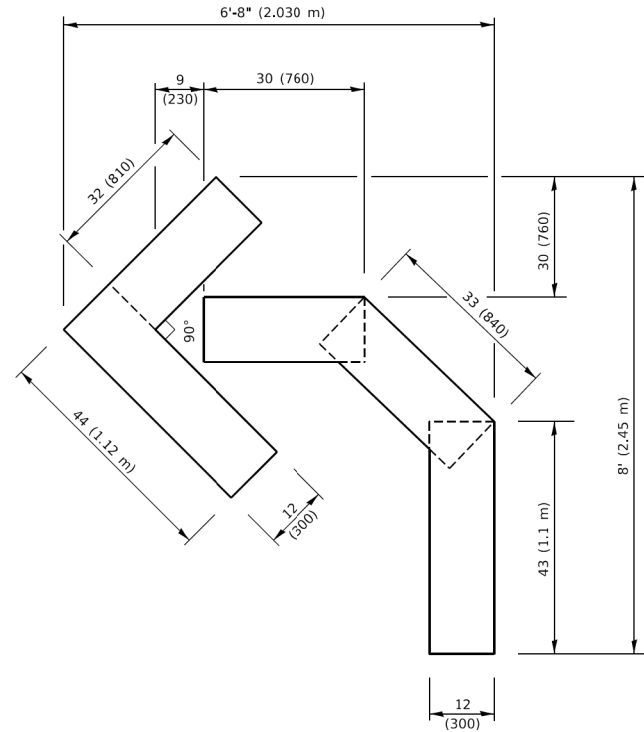
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	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

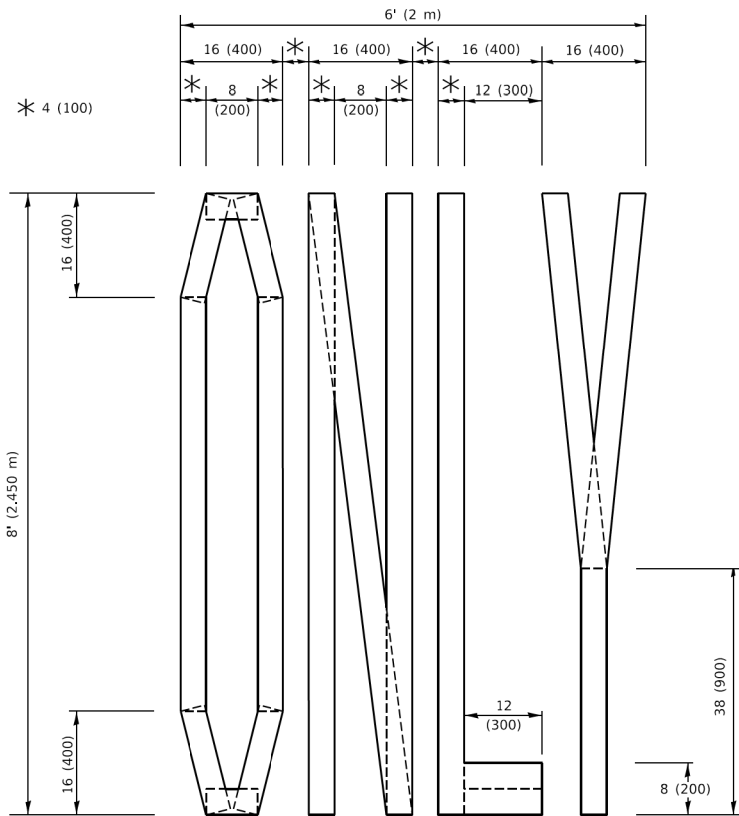
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-13		CONTRACT NO. 61L83		
		ILLINOIS FED. AID PROJECT		



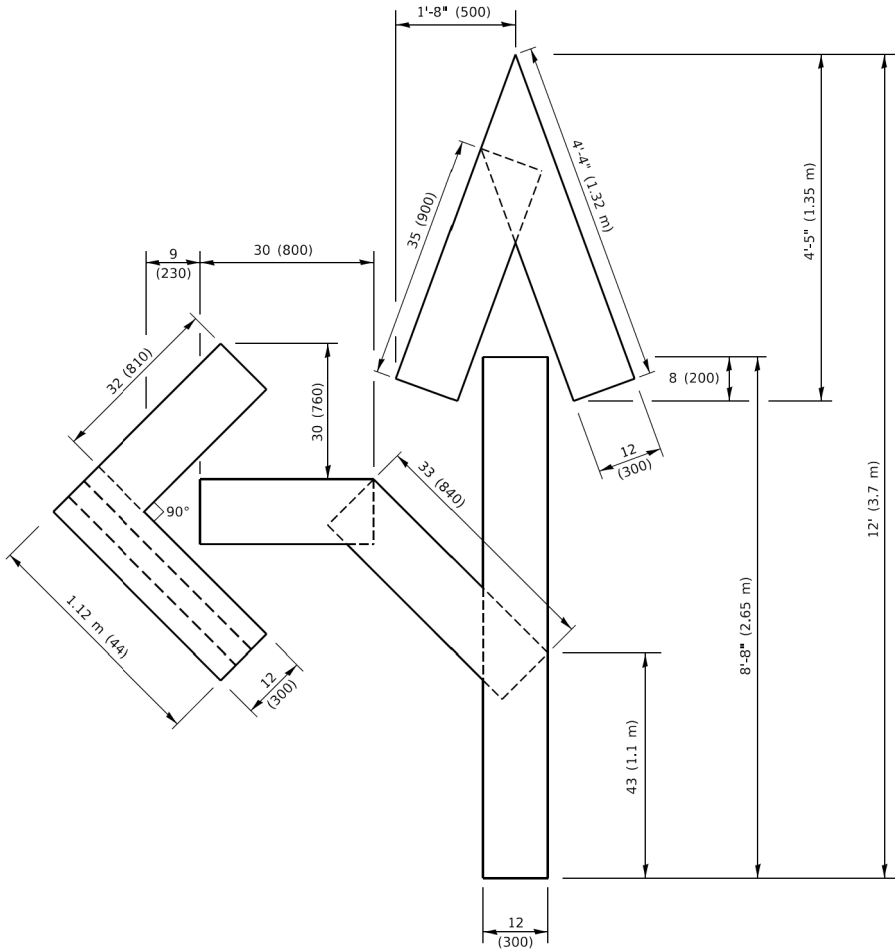
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

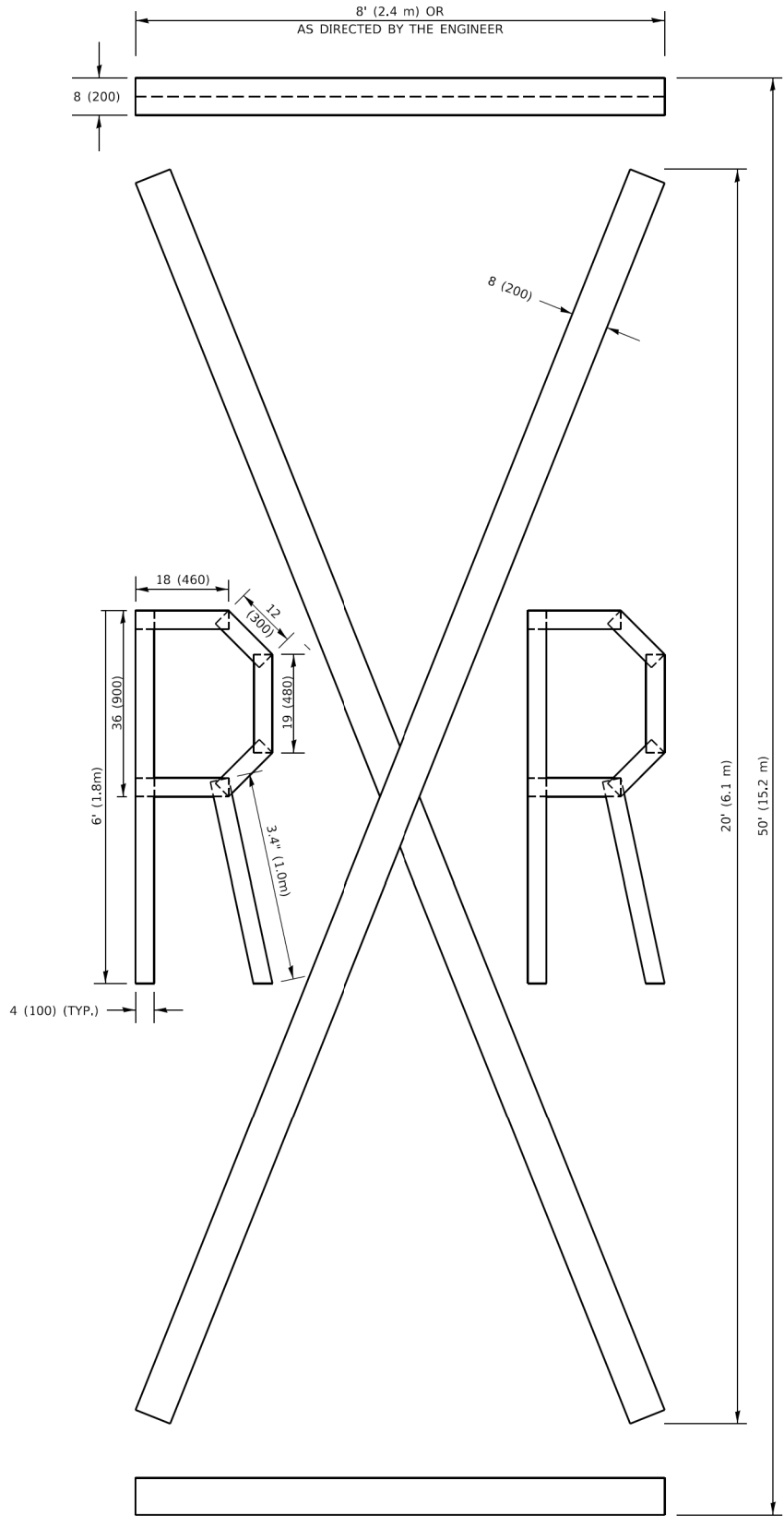


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	35
TC-16		CONTRACT NO. 61L83		
		ILLINOIS	FED. AID PROJECT	

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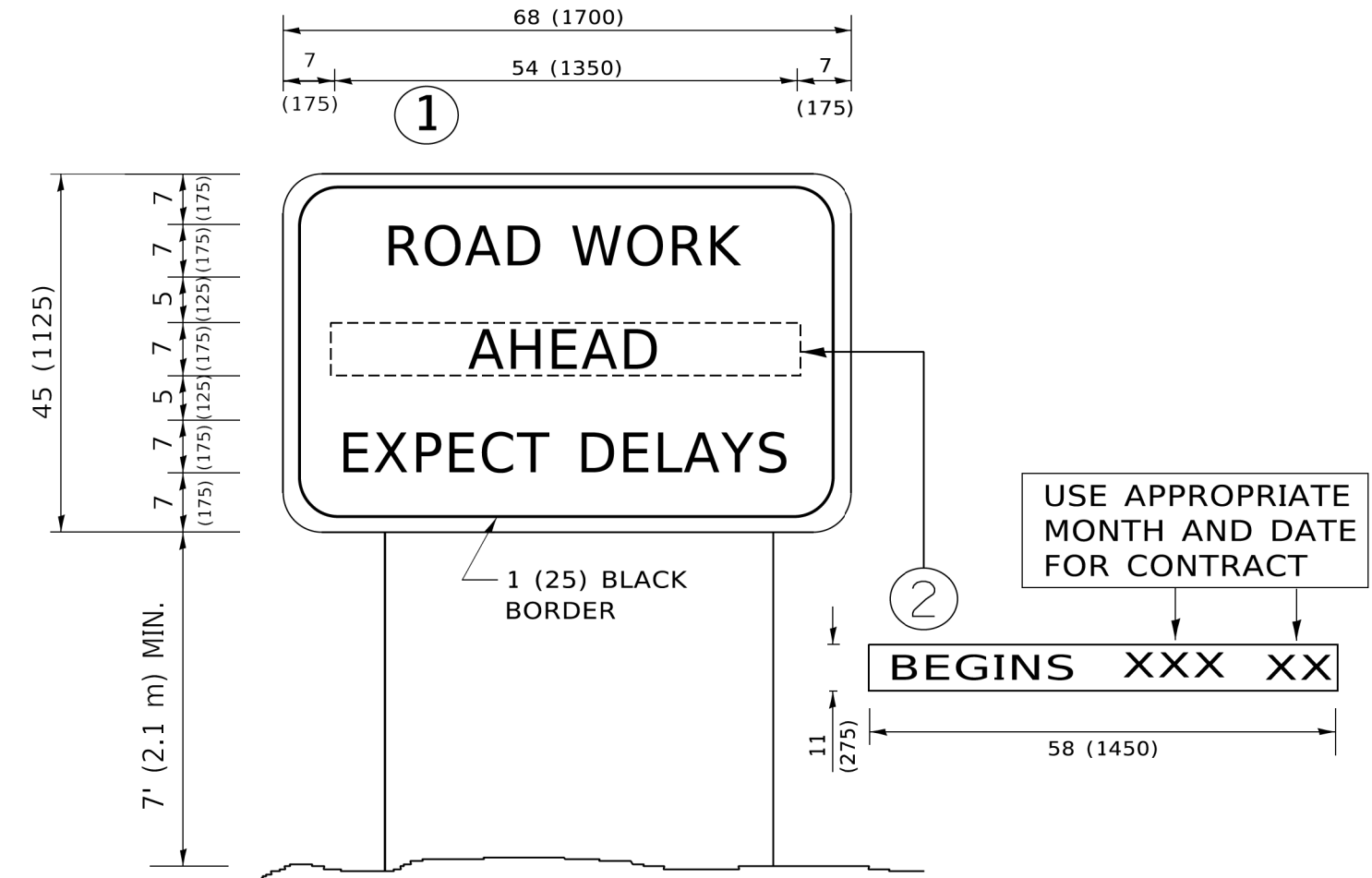
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PLOT DATE	= 3/4/2019	DATE	-	REVISED	-	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1234	23-00127-00-RS	LAKE	36	36
TC-22		CONTRACT NO. 61L83		
		ILLINOIS	FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.