

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PLANS FOR PROPOSED FEDERAL AID HIGHWAY

**FAU ROUTE 2995 (RIFORD ROAD)  
CRESCENT BOULEVARD TO ST CHARLES ROAD  
ROADWAY RESURFACING  
SECTION NO.: 24-00087-00-RS  
PROJECT NO.: FJ9N(571)  
VILLAGE OF GLEN ELLYN  
DUPAGE COUNTY**

C-91-218-25

F.A.U. RATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-RS	DUPAGE	22	1
		ILLINOIS	CONTRACT NO. 61L80	

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

**DESIGN DESIGNATION**  
RIFORD ROAD: MINOR COLLECTOR

**TRAFFIC DATA**  
AADT: 1,950 (2020)  
POSTED SPEED LIMIT: 30 MPH



**DUPAGE COUNTY STORMWATER  
CERTIFICATION NUMBER 25-17-0039**

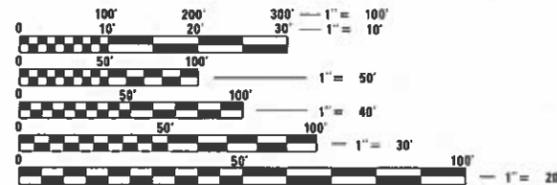
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

APPROVED 7/25/2025   
VILLAGE OF GLEN ELLYN, PROFESSIONAL ENGINEER

PASSED August 27, 2025   
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

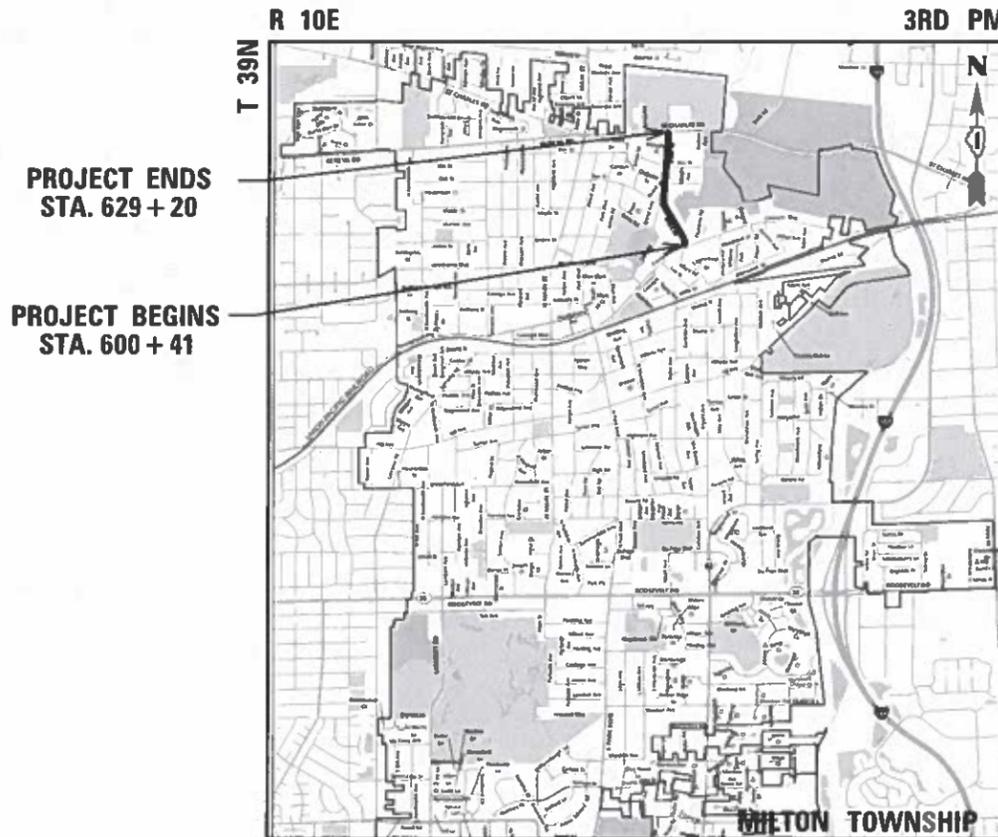
RELEASING FOR BID  
BASED ON LIMITED  
REVIEW Aug 27<sup>th</sup> 2025   
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811



PROJECT ENDS  
STA. 629 + 20

PROJECT BEGINS  
STA. 600 + 41

**LOCATION MAP**

GROSS LENGTH = 2,879 FT. = 0.545 MILE  
NET LENGTH = 2,879 FT. = 0.545 MILE



STEVEN P. FESSENBECKER  
ILLINOIS LICENSE NO. 062-051254  
DATE OF EXPIRATION 11/30/2025



650 WARRENVILLE ROAD  
SUITE 350  
LISLE, IL 60532  
TEL. 312-373-7700

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

# INDEX OF SHEETS

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# GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE COUNTY OF DUPAGE, AND THE VILLAGE OF GLEN ELLYN.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE VILLAGE OR ON COUNTY OF DUPAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE COUNTY OF DUPAGE.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING MID RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- BEFORE BEGINNING ANY WORK THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, ANY CLOSURES SHALL BE COORDINATED WITH THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA.
- WHEREVER EXCAVATION FALLS WITHIN THE DRIP-LINE OF A TREE OR AS DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL PROVIDE TREE ROOT PRUNING.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
- ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
- CONTACT THE DU PAGE COUNTY TRAFFIC SIGNAL COORDINATOR (630/407-6900) TO APPROVE LOCATIONS OF LOOPS, SIGNAL FOUNDATIONS AND SIGNAL HEADS.

### EROSION AND SEDIMENT CONTROL NOTES:

- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO START OF CONSTRUCTION.
- UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE MOST RECENT ILLINOIS URBAN MANUAL.
- THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE VILLAGE OF GLEN ELLYN.
- IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT, OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL MEASURES AND ASSURE COMPLIANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
- CONTRACTOR SHALL REFER TO THE ILLINOIS URBAN MANUAL STANDARDS FOR PERIMETER EROSION BARRIER, EROSION CONTROL BLANKET, AND INLET AND PIPE PROTECTION.

# COMMITMENTS

- ALL SOILS GENERATED FROM CURB AND SIDEWALK REPAIRS MUST REMAIN WITHIN THE RIGHT-OF-WAY.
- PROPOSED RESURFACING IMPROVEMENTS WILL MAINTAIN EXISTING PAVEMENT ELEVATIONS, SO FILL IN THE FLOODPLAIN IS NOT ANTICIPATED. HOWEVER, COMPENSATORY STORAGE CALCULATIONS USING THE CONVENTIONAL METHODS (CALCULATING VOLUME FROM CROSS SECTIONS) WILL BE REQUIRED FOR ANY IMPROVEMENT THAT ALTERS PAVEMENT OR SIDEWALK SURFACES WITHIN THE LIMITS OF THE EXISTING FLOODPLAIN AS SHOWN THE PLANS.
- SOIL EXCAVATION TO REMAIN ON SITE MAY BE PLACED AT THE LOCATIONS AS NOTED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- A DUPAGE COUNTY STORMWATER CERTIFICATION WILL BE REQUIRED AND WILL BE ISSUED BY THE VILLAGE OF GLEN ELLYN. RECORD DRAWINGS WILL BE PREPARED AS PART OF THE STORMWATER CERTIFICATION IN PHASE 3 CONSTRUCTION.

# HIGHWAY STANDARDS

000001-08 - STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07 - TEMPORARY EROSION CONTROL SYSTEMS
424001-12 - PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-06 - DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-05 - CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-06 - MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-07 - DEPRESSED CORNER FOR SIDEWALKS
442201-03 - CLASS C AND D PATCHES
604001-05 - FRAME AND LIDS TYPE 1
604006-05 - FRAME AND GRATE TYPE 3
606001-08 - CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05 - OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701011-04 - OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05 - OFF-RD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701301-04 - LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03 - LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06 - URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-10 - URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06 - SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-10 - TRAFFIC CONTROL DEVICES
814001-03 - HANDHOLES
886001-01 - DETECTOR LOOP INSTALLATIONS

# LEGEND

EXISTING	PROPOSED		EXISTING	PROPOSED	
		SANITARY MANHOLE			STRUCTURE TO BE REMOVED
		STORM MANHOLE			STRUCTURE TO BE ADJUSTED
		CATCH BASIN			DEPRESSED CURB
		INLET			COMBINATION CONCRETE CURB AND GUTTER
		VALVE VAULT			SEWER STRUCTURE
		WATER VALVE AND BOX			SEWER PIPE SEGMENT
		DOMESTIC WATER SERVICE BOX			VEGETATION LINE
		HYDRANT			WATER LINE
		HANDHOLE			PROPERTY LINE
		POWER POLE			STORM SEWER
		LIGHT POLE			SANITARY SEWER
		GUY WIRE			FENCE LINE
		SIGN			ROAD CENTER LINE
		FOUND IRON PIPE			R.O.W. LINE
		BUSH			GAS LINE
		EVERGREEN TREE WITH SIZE Ø			ELECTRIC LINE
		DECIDUOUS TREE WITH SIZE Ø			TELEPHONE LINE
					CABLE TV LINE
					FIBER OPTIC LINE
					OVERHEAD UTILITY LINE

# DISTRICT ONE DETAILS

BD-08 - DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-32 - BUTT JOINT AND HMA TAPER DETAILS
TC-10 - TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13 - TYPICAL PAVEMENT MARKINGS
TC-16 - SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22 - ARTERIAL ROAD INFORMATION SIGN
TC-26 - DRIVEWAY ENTRANCE SIGNING
TS-05 - STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

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650 WARRENVILLE ROAD  
SUITE 350  
LISLE, IL 60532  
TEL. 312-373-7700

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	DRAWN - KJB	REVISED -
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PLOT DATE = 8/27/2025	DATE - 8/27/2025	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, LEGEND,  
AND LIST OF HIGHWAY STANDARDS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	2
CONTRACT NO. 61L80			ILLINOIS FED. AID PROJECT	



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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0005	
				RESURFACING	
				60% FEDERAL / 40% LOCAL	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1	
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	681	681	
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,544	1,544	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	515	515	
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	5,137	5,137	
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	684	684	
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	630	630	
70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	64	64	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5,137	5,137	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	684	684	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	630	630	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	64	64	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0005	
				RESURFACING	
				60% FEDERAL / 40% LOCAL	
* 89502376	REBUILD EXISTING HANDHOLE	EACH	1	1	
X0323677	STREET SWEEPING	HOUR	20	20	
X2520700	SODDING (SPECIAL)	SQ YD	412	412	
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	5	5	
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1	1	
X4023000	TEMPORARY ACCESS (ROAD)	EACH	9	9	
X4060280	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	20	20	
X6026056	SANITARY MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	5	5	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	25	25	
X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	65	65	
* X8880105	DETECTOR LOOP REPLACEMENT	FOOT	92	92	
* X8950215	RELOCATE EXISTING HANDHOLE	EACH	1	1	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0019598	DUST CONTROL (SPECIAL)	GALLON	1,000	1,000	

\* INDICATES SPECIALTY PAY ITEM



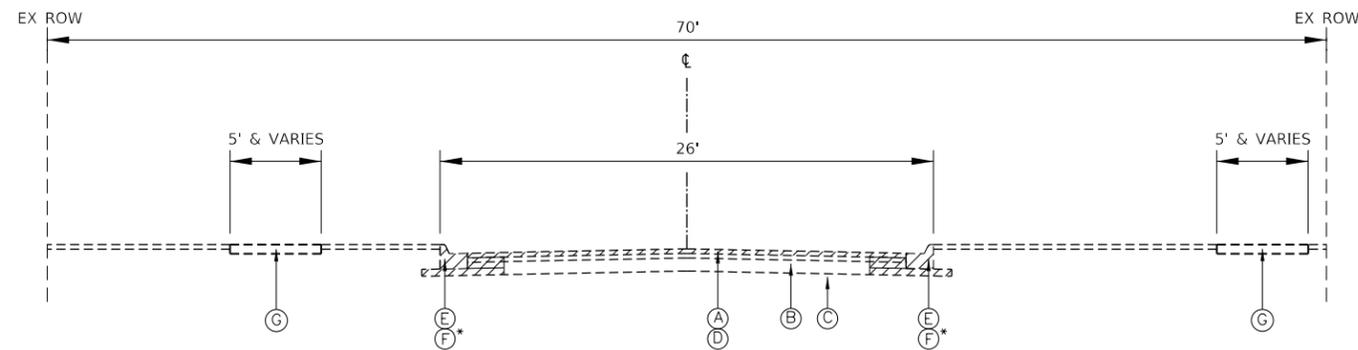
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

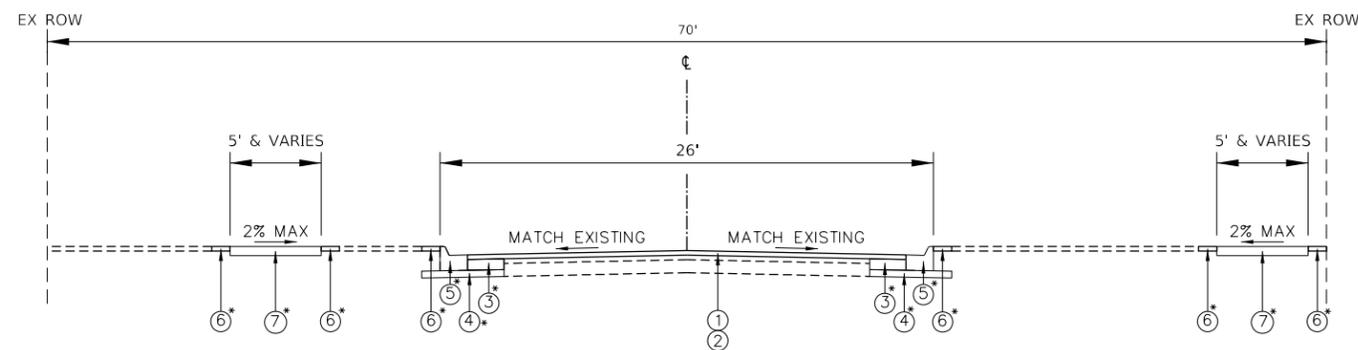
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2995	SECTION 24-00087-00-R5	COUNTY DUPAGE	TOTAL SHEETS 22	SHEET NO. 4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61L80	



**EXISTING TYPICAL SECTION**  
RIFORD ROAD  
STA 600+41.60 TO STA 629+20.20



**PROPOSED TYPICAL SECTION**  
RIFORD ROAD  
STA 600+41.60 TO STA 629+20.20

HOT MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	4% @ 50 Gyr.	LR1030-02
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	3.5% @ 50 Gyr.	LR1030-02
HMA DRIVEWAY PAVEMENT 3" (HMA SC IL-9.5 MIX D N50; 3"(IN 2 LIFTS))	4% @ 50 Gyr.	LR1030-02
CLASS D PATCHES (HMA BINDER IL-19.0)	4% @ 50 Gyr.	LR1030-02
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR1030-02		
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.		
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.		
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER COURSE, IL-4.75, N50.		
MILLING BEFORE PATCHING.		

**LEGEND**

- (A) EXISTING HMA PAVEMENT - 6"
- (B) EXISTING AGGREGATE BASE COURSE - 6" & VARIES
- (C) EXISTING SUBGRADE
- (D) HMA SURFACE REMOVAL - 3"
- (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER
- (F) COMBINATION CONCRETE CURB AND GUTTER REMOVAL
- (G) EXISTING P.C.C. SIDEWALK
- REMOVAL ITEMS

\* AS SHOWN ON THE PLANS  
OR AS DIRECTED BY ENGINEER

**LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 2"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 1"
- (3) CLASS D PATCHES, 3 INCH
- (4) AGGREGATE BASE COURSE, TYPE B 4"
- (5) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (6) SODDING (SPECIAL)
- (7) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (6" AT DRIVEWAYS)

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PLOT DATE = 8/28/2025	DATE - 8/28/2025	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

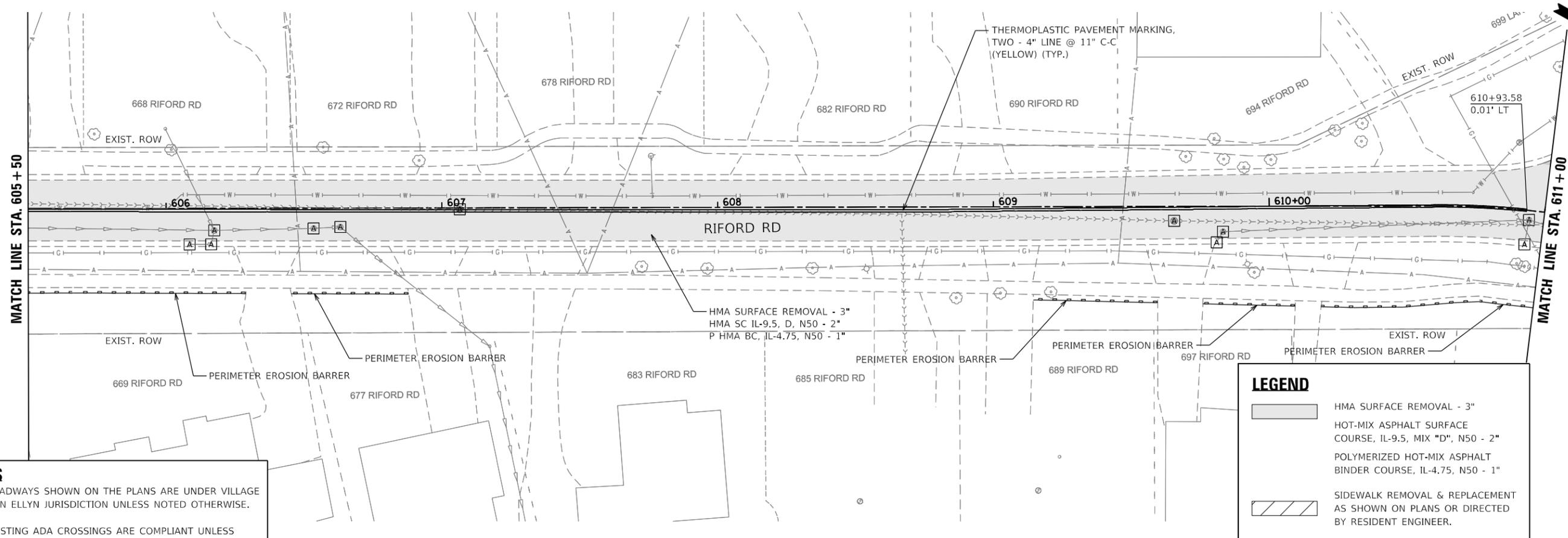
**RIFORD ROAD  
TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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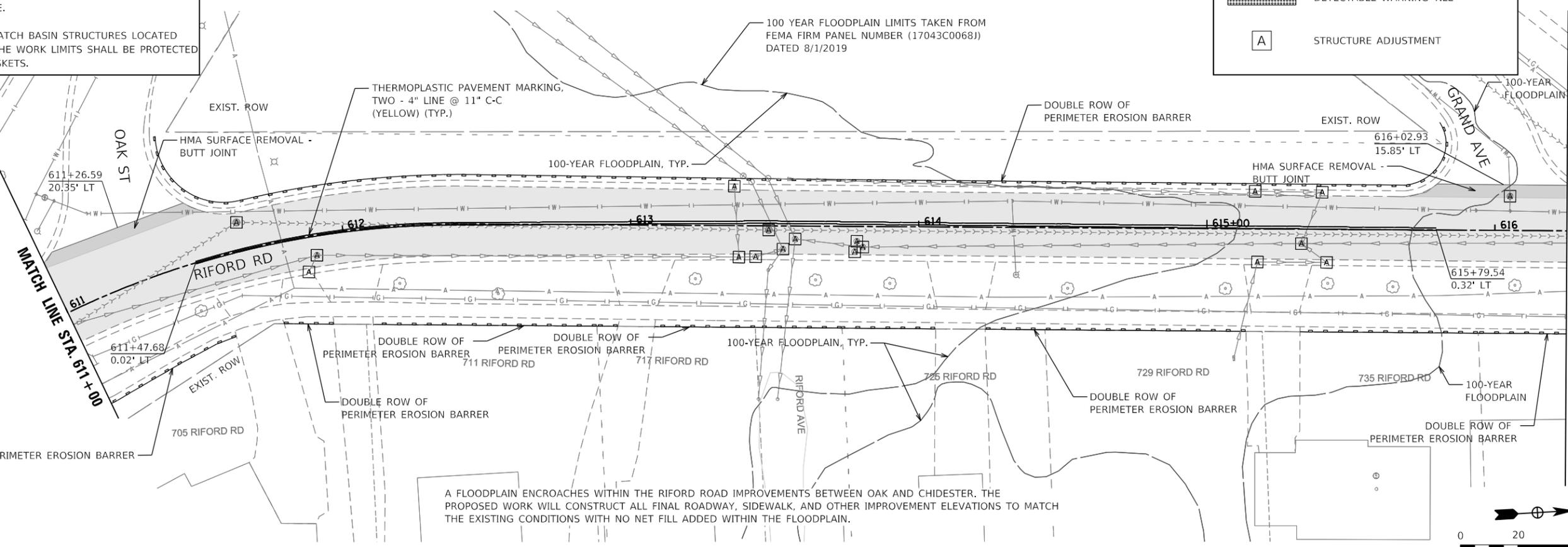


**LEGEND**

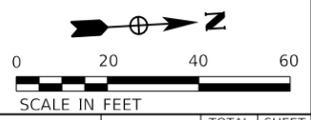
- HMA SURFACE REMOVAL - 3"
- HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 2"
- POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 1"
- SIDEWALK REMOVAL & REPLACEMENT AS SHOWN ON PLANS OR DIRECTED BY RESIDENT ENGINEER.
- DETECTABLE WARNING TILE
- STRUCTURE ADJUSTMENT

**NOTES**

1. ALL ROADWAYS SHOWN ON THE PLANS ARE UNDER VILLAGE OF GLEN ELLYN JURISDICTION UNLESS NOTED OTHERWISE.
2. ALL EXISTING ADA CROSSINGS ARE COMPLIANT UNLESS NOTED OTHERWISE. ANY RECONSTRUCTED RAMPS SHALL MEET ADA COMPLIANCE.
3. EXISTING INLET AND CATCH BASIN STRUCTURES LOCATED WITHIN 100 FEET OF THE WORK LIMITS SHALL BE PROTECTED WITH INLET FILTER BASKETS.



A FLOODPLAIN ENCLOSES WITHIN THE RIFORD ROAD IMPROVEMENTS BETWEEN OAK AND CHIDESTER. THE PROPOSED WORK WILL CONSTRUCT ALL FINAL ROADWAY, SIDEWALK, AND OTHER IMPROVEMENT ELEVATIONS TO MATCH THE EXISTING CONDITIONS WITH NO NET FILL ADDED WITHIN THE FLOODPLAIN.



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**AECOM**  
 650 WARRENVILLE ROAD  
 SUITE 350  
 LISLE, IL 60532  
 TEL. 312-373-7700

USER NAME = nolan,hicks	DESIGNED - NRH	REVISED -
PLOT SCALE = 40,0001' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 8/27/2025	CHECKED - SPF	REVISED -
	DATE - 8/27/2025	REVISED -

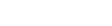
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>RIFORD ROAD RESURFACING PLAN</b>	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-RS	DUPAGE	22	8
CONTRACT NO. 61L80			ILLINOIS FED. AID PROJECT	

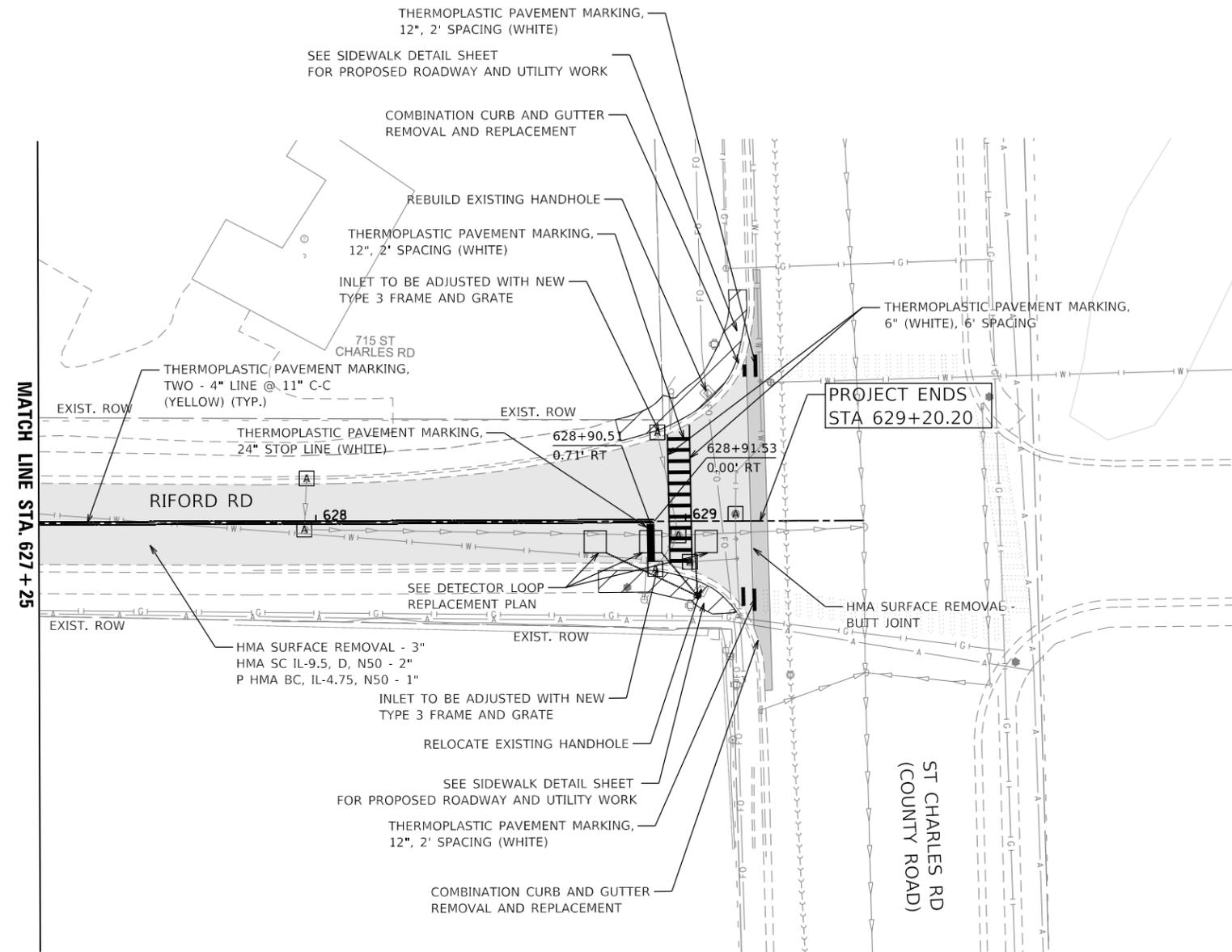


**LEGEND**

-  HMA SURFACE REMOVAL - 3"
-  HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 2"
-  POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50 - 1"
-  SIDEWALK REMOVAL & REPLACEMENT AS SHOWN ON PLANS OR DIRECTED BY RESIDENT ENGINEER.
-  DETECTABLE WARNING TILE
-  STRUCTURE ADJUSTMENT

**NOTES**

1. ALL ROADWAYS SHOWN ON THE PLANS ARE UNDER VILLAGE OF GLEN ELLYN JURISDICTION UNLESS NOTED OTHERWISE.
2. ALL EXISTING ADA CROSSINGS ARE COMPLIANT UNLESS NOTED OTHERWISE. ANY RECONSTRUCTED RAMPS SHALL MEET ADA COMPLIANCE.
3. EXISTING INLET AND CATCH BASIN STRUCTURES LOCATED WITHIN 100 FEET OF THE WORK LIMITS SHALL BE PROTECTED WITH INLET FILTER BASKETS.



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**AECOM**  
 650 WARRENVILLE ROAD  
 SUITE 350  
 LISLE, IL 60532  
 TEL. 312-373-7700

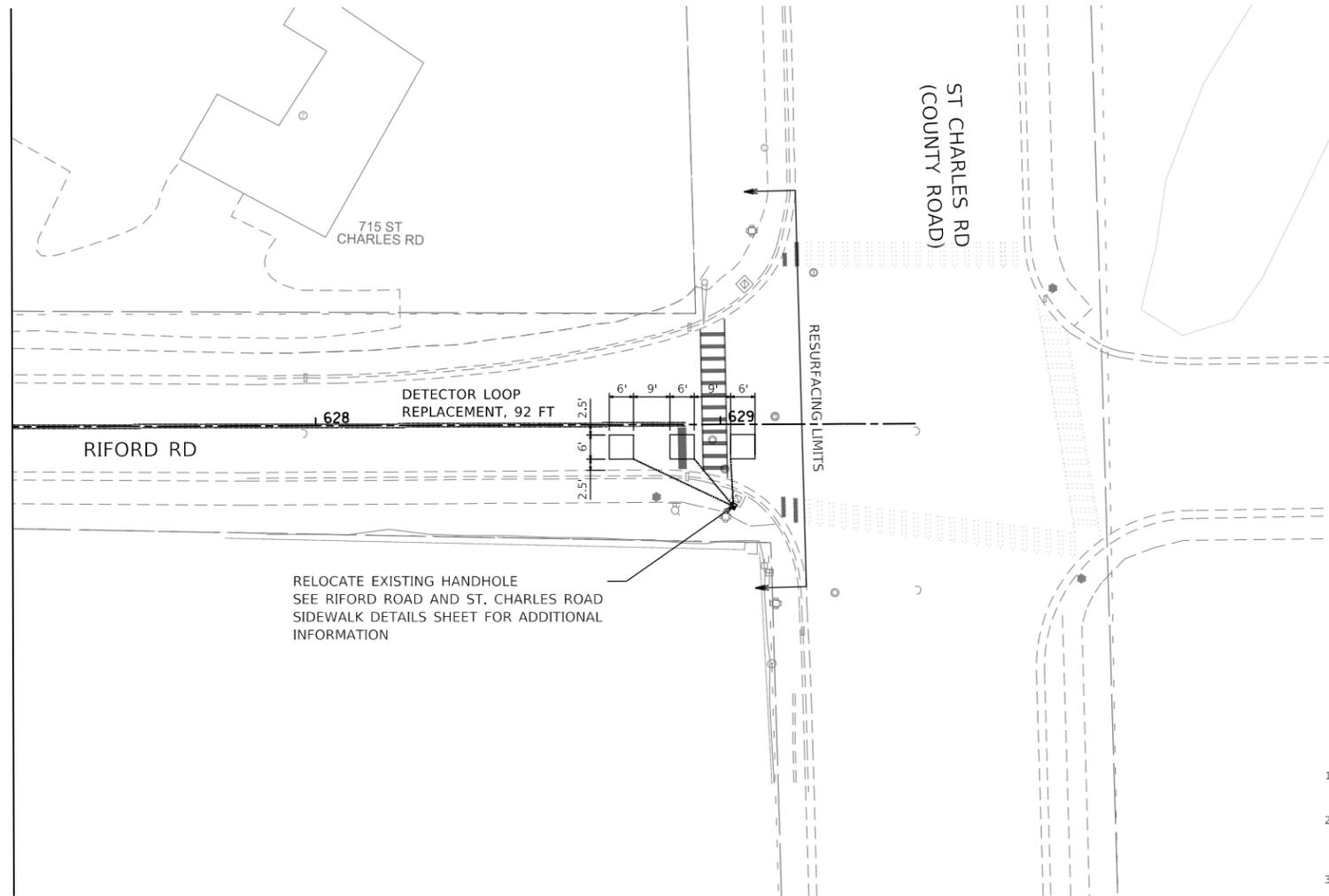
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PLOT DATE = 8/27/2025	CHECKED - SPF	REVISED -
	DATE - 8/27/2025	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RIFORD ROAD  
 RESURFACING PLAN**

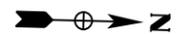
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	10
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61L80	



**NOTES**

1. DETECTOR LOOP REPLACEMENT SHALL INCLUDE ONLY LOOPS DAMAGED BY RESURFACING. OTHER EXISTING LOOPS ARE TO REMAIN.
2. THE CONTRACTOR IS NOT RESPONSIBLE FOR TAKING OVER TRAFFIC SIGNAL MAINTENANCE TO REPLACE DAMAGED LOOPS. EXISTING TRAFFIC SIGNAL OPERATIONS SHALL REMAIN IN PLACE.
3. CONTACT THE DUPAGE COUNTY TRAFFIC SIGNAL COORDINATOR (630/407-6900) TO APPROVE LOCATIONS OF LOOPS, SIGNAL FOUNDATIONS AND SIGNAL HEADS.



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**AECOM**  
 650 WARRENVILLE ROAD  
 SUITE 350  
 Lisle, IL 60532  
 TEL. 312-373-7700

USER NAME = nolan,hicks	DESIGNED - NRH	REVISED -
	DRAWN - KJB	REVISED -
PLOT SCALE = 40.0001' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 8/27/2025	DATE - 8/27/2025	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
 ST. CHARLES ROAD AND RIFORD ROAD**

SCALE: SHEET OF SHEETS STA. TO STA.

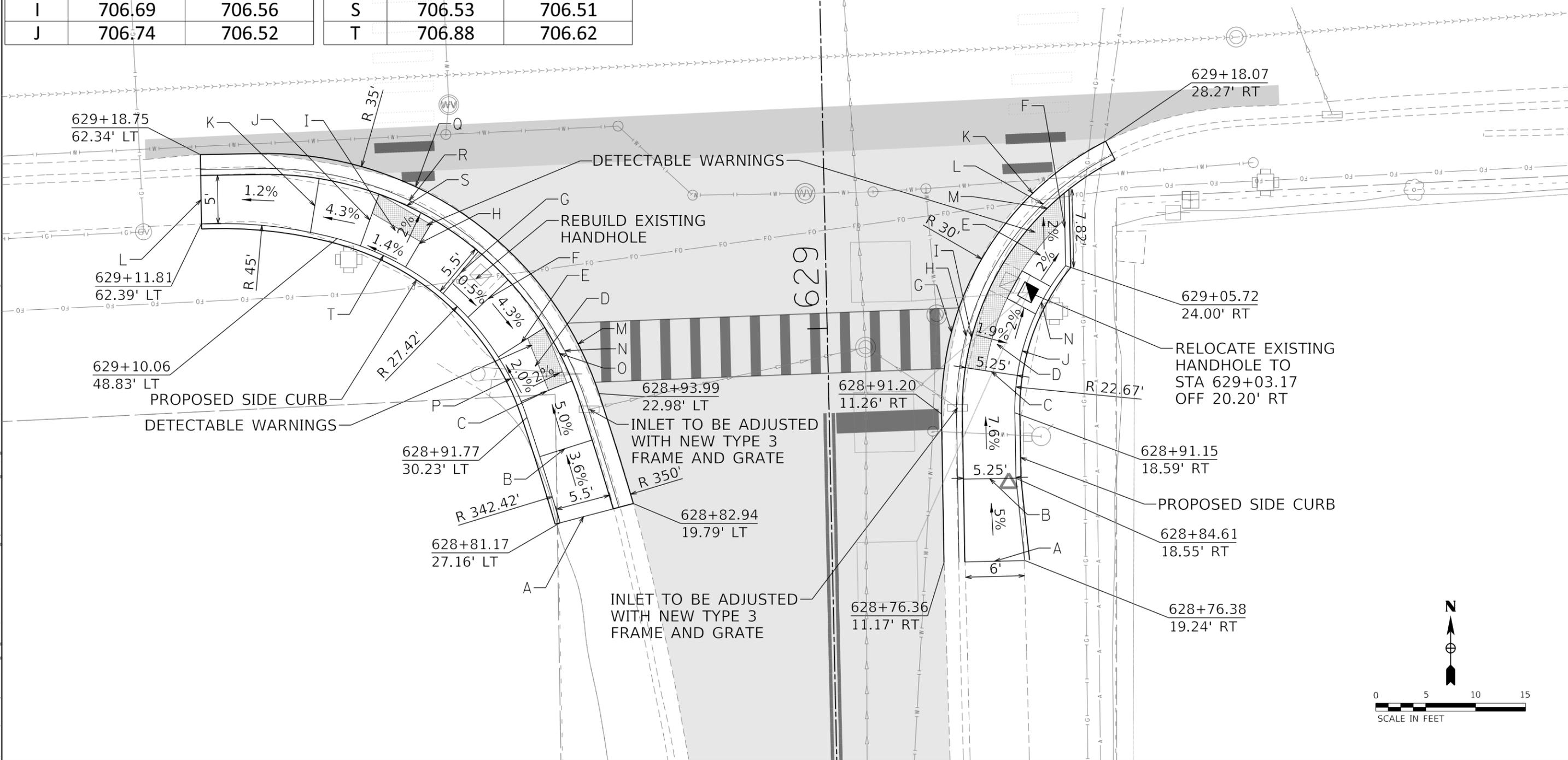
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-RS	DUPAGE	22	11
CONTRACT NO. 61L80				
ILLINOIS		FED. AID PROJECT		

SOUTHWEST QUADRANT		
POINT	EXISTING EL.	PROPOSED EL.
A	707.28	MATCH EX.
B	707.12	707.01
C	706.78	706.71
D	706.78	706.65
E	706.74	706.60
F	706.84	706.84
G	706.87	706.85
H	706.72	706.60
I	706.69	706.56
J	706.74	706.52

SOUTHWEST QUADRANT		
POINT	EXISTING EL.	PROPOSED EL.
K	706.77	706.77
L	706.91	MATCH EX.
M	706.63	MATCH EX.
N	706.56	706.56
O	706.61	706.60
P	706.85	706.71
Q	706.50	MATCH EX.
R	706.47	706.47
S	706.53	706.51
T	706.88	706.62

SOUTHWEST QUADRANT		
POINT	EXISTING EL.	PROPOSED EL.
A	707.58	MATCH EX.
B	707.28	707.16
C	706.81	706.35
D	706.70	706.29
E	706.43	706.10
F	706.30	706.02
G	706.47	706.34

SOUTHWEST QUADRANT		
POINT	EXISTING EL.	PROPOSED EL.
H	706.47	706.30
I	706.47	706.34
J	706.89	706.24
K	705.99	MATCH EX.
L	705.99	705.98
M	706.02	706.02
N	706.84	706.19



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 AECOM 650 WARRENVILLE ROAD SUITE 350 LISLE, IL 60532 TEL. 312-373-7700

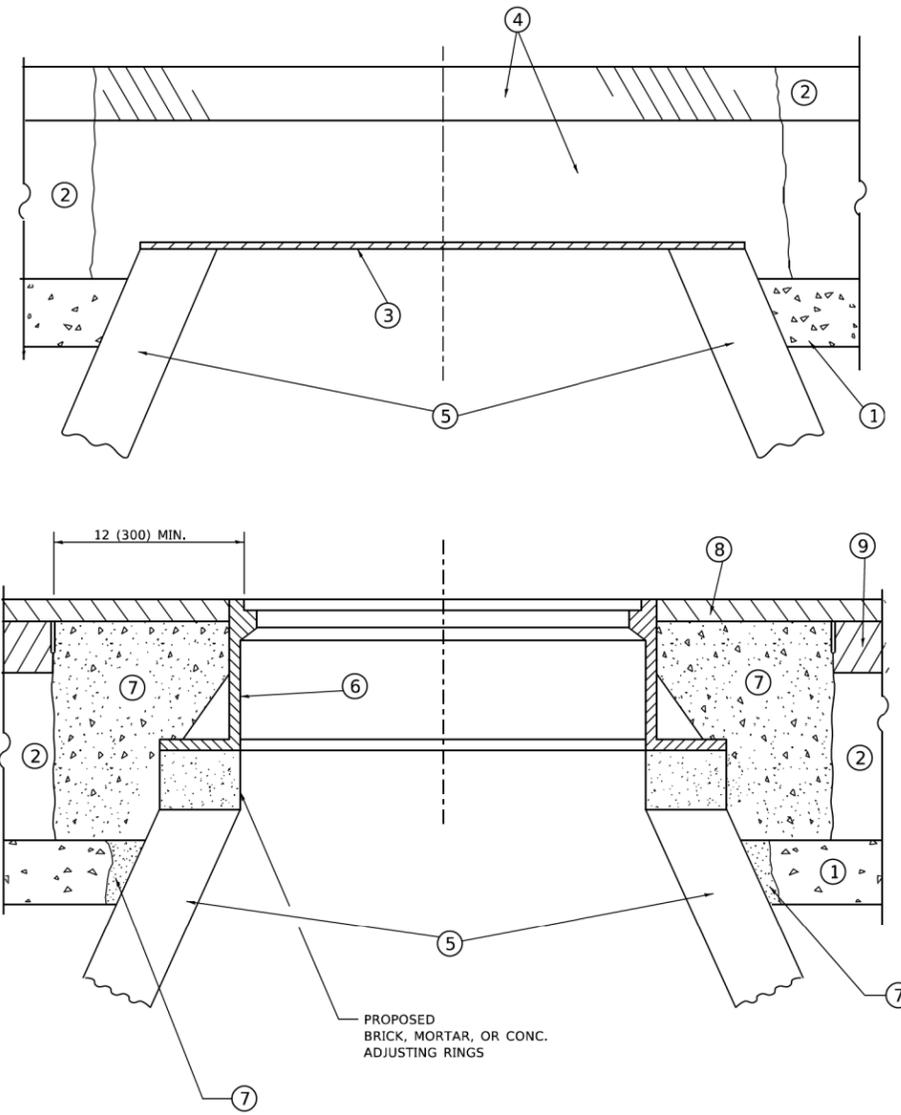
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PLOT DATE = 8/27/2025	CHECKED - SPF	REVISED -
	DATE - 8/27/2025	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**RIFORD ROAD AND ST. CHARLES ROAD**  
**SIDEWALK DETAILS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2995	SECTION 24-00087-00-R5	COUNTY DUPAGE	TOTAL SHEETS 22	SHEET NO. 12
CONTRACT NO. 61L80			ILLINOIS FED. AID PROJECT	



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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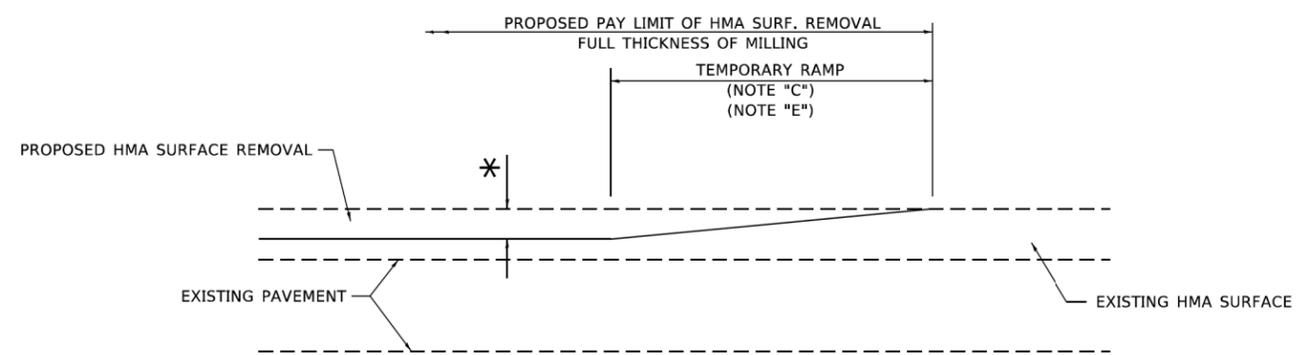
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	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

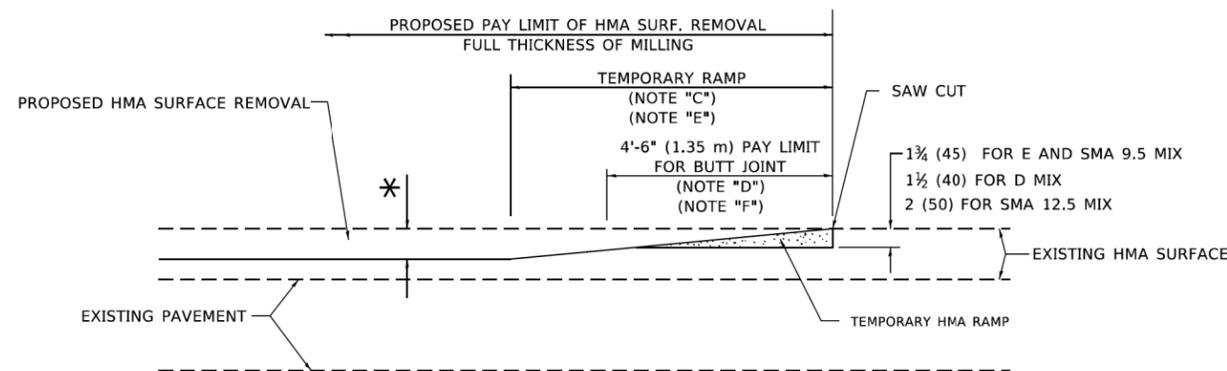
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	13
<b>BD600-03 (BD-08)</b>		CONTRACT NO. 61L80		
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

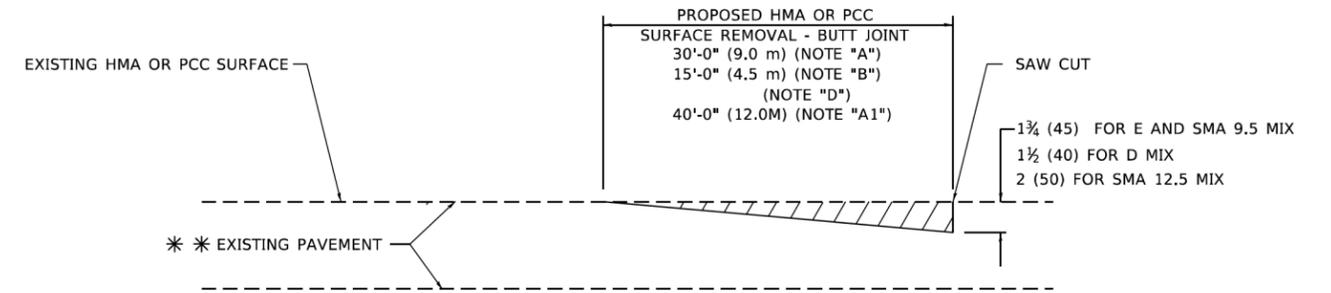
**OPTION 1**



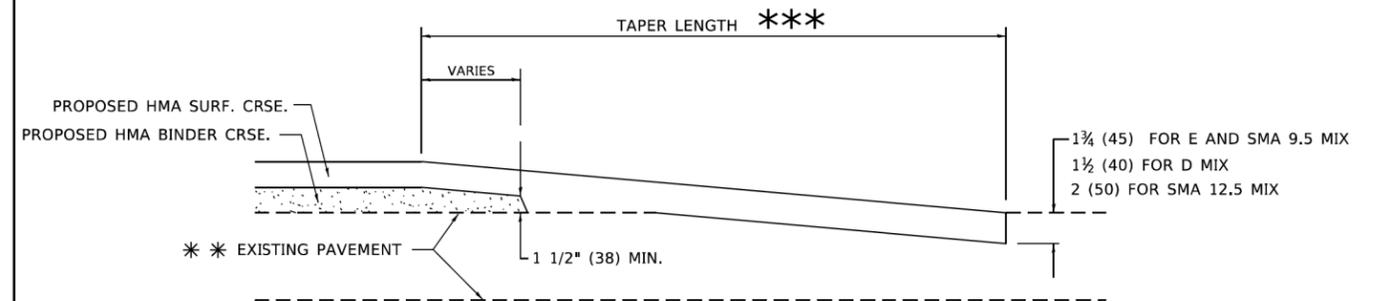
**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

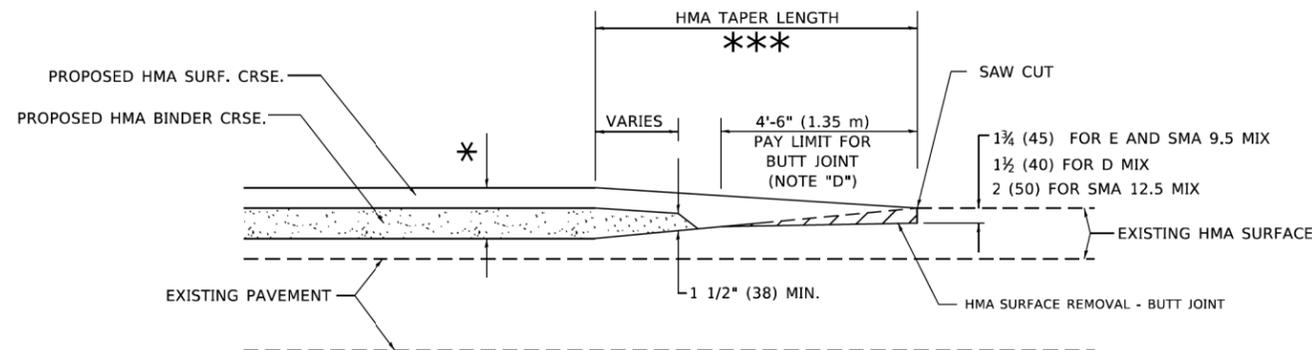
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

MODEL: Default  
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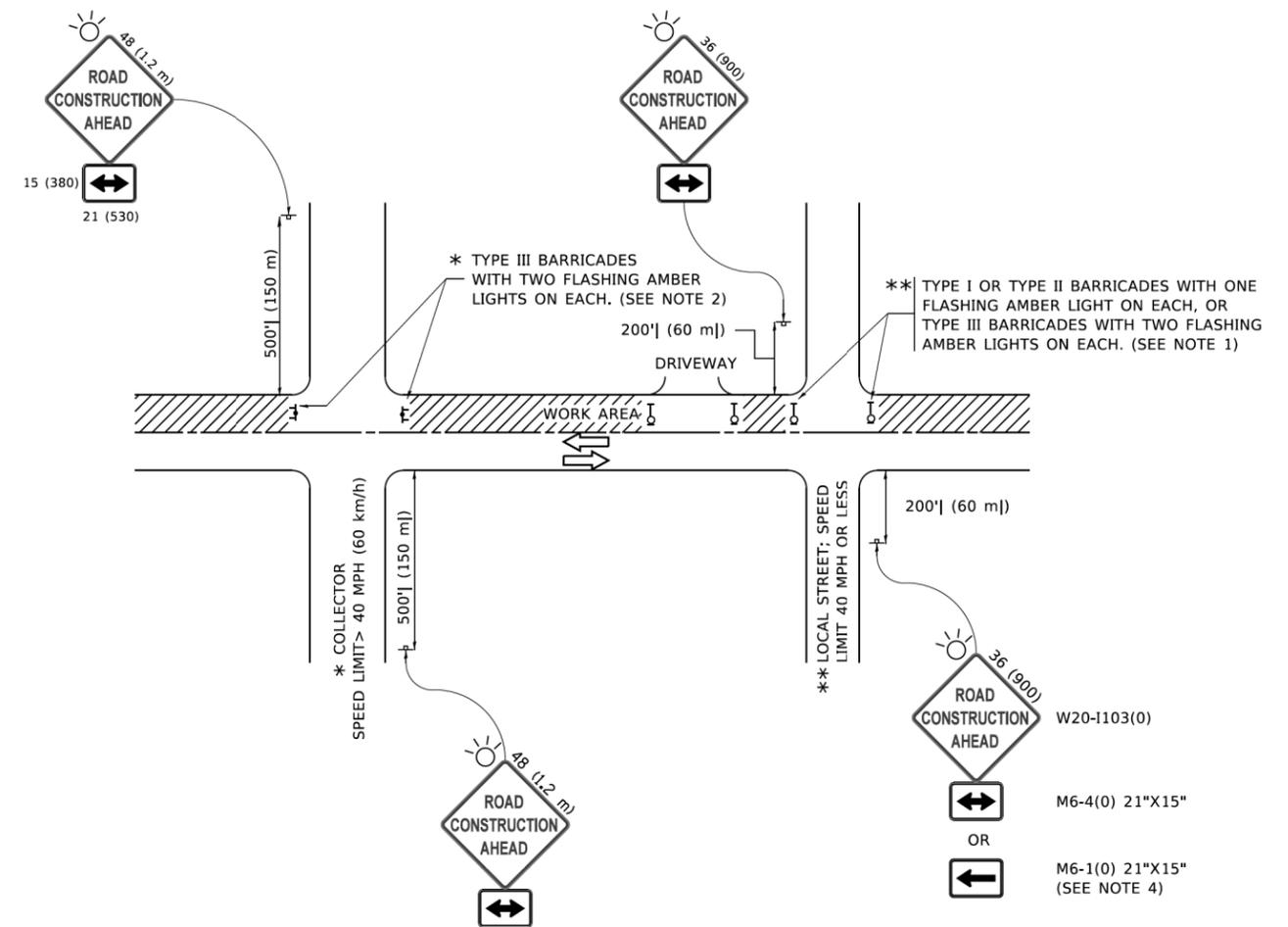
USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	14
BD400-05 BD-32		CONTRACT NO. 61L80		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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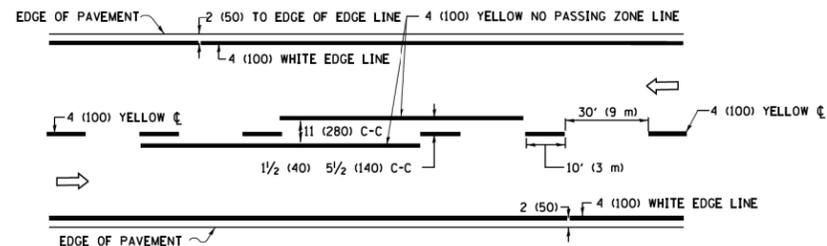
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50,0000 ' / ft.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

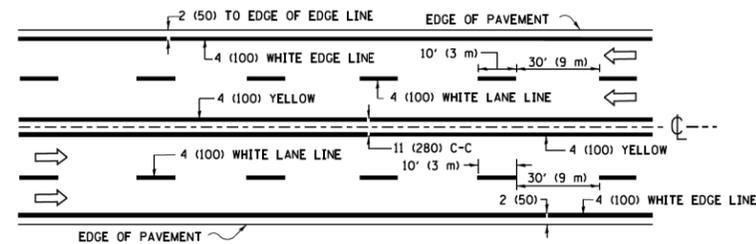
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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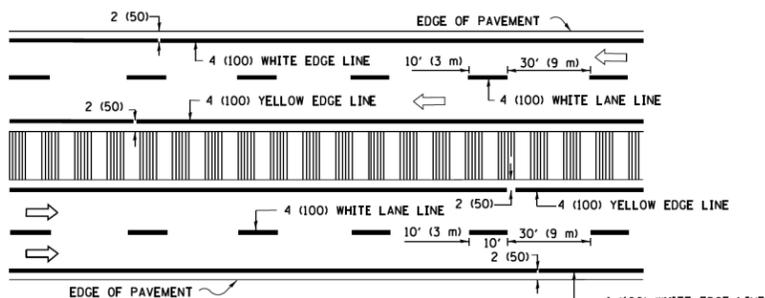
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2995	24-00087-00-RS	DUPAGE	22	15
<b>TC-10</b>			CONTRACT NO. 61L80	
ILLINOIS   FED. AID PROJECT				



**2-LANE ROADWAY**

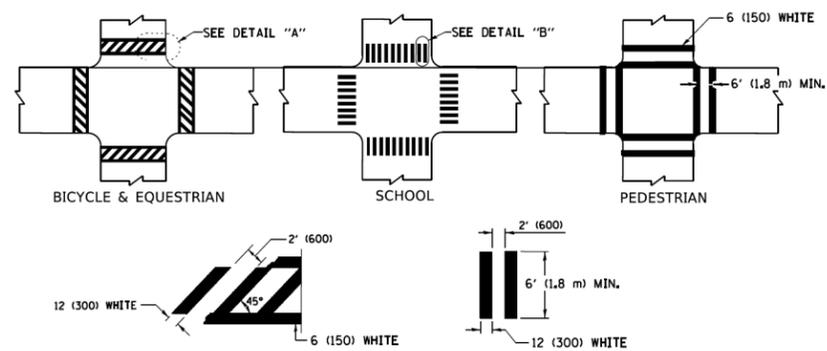


**MULTI-LANE UNDIVIDED**



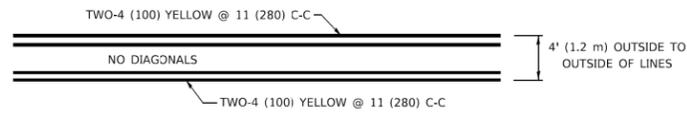
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

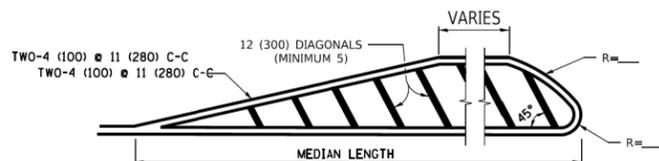


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

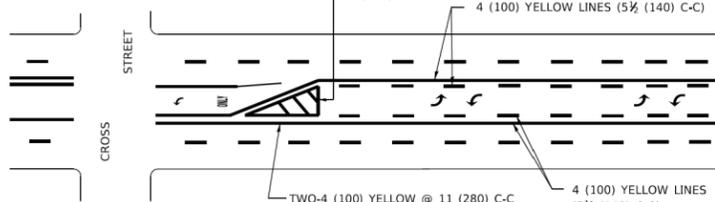


**4' (1.2 m) WIDE MEDIANS ONLY**

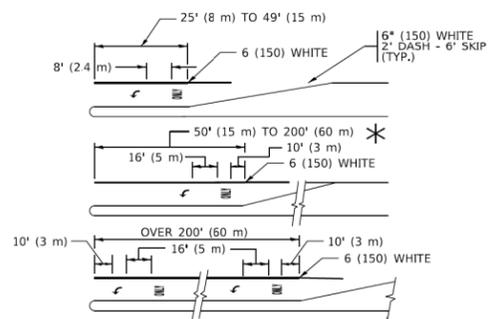
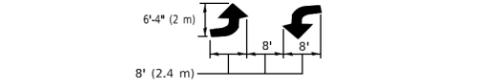


**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

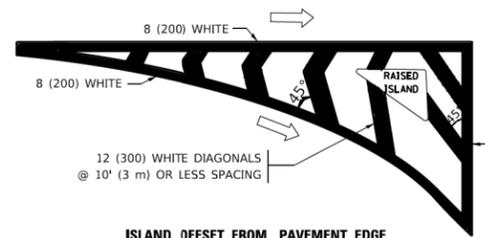


**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

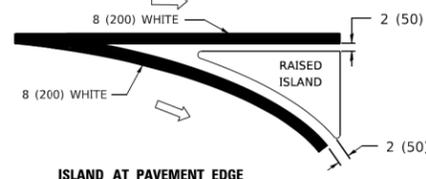


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - \*ONLY\* INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - \*ONLY\*.

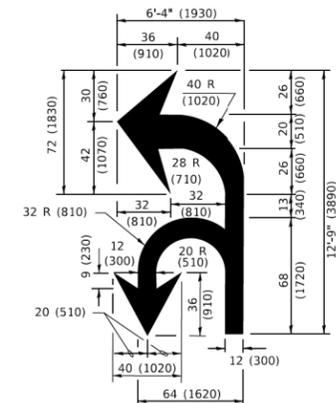
**TYPICAL LEFT (OR RIGHT) TURN LANE  
TYPICAL TURN LANE MARKING**



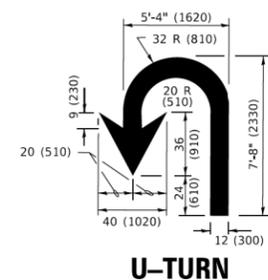
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE  
TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: *RR* IS 8' (1.8 m) LETTERS; 16 (400) LINE FOR *X*	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R* = 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH *X* = 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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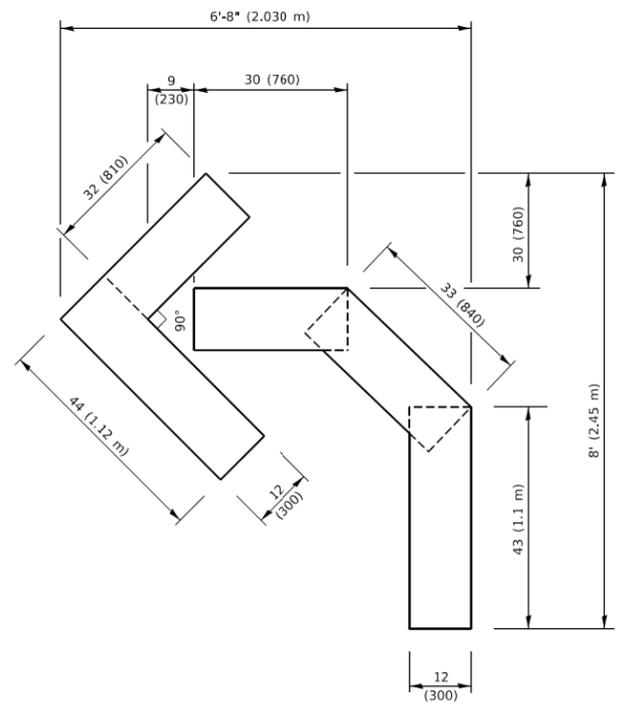
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PLOT SCALE = 50,0000 ' / in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/4/2019	CHECKED -	REVISED - C. JUCIUS 12-21-15
	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

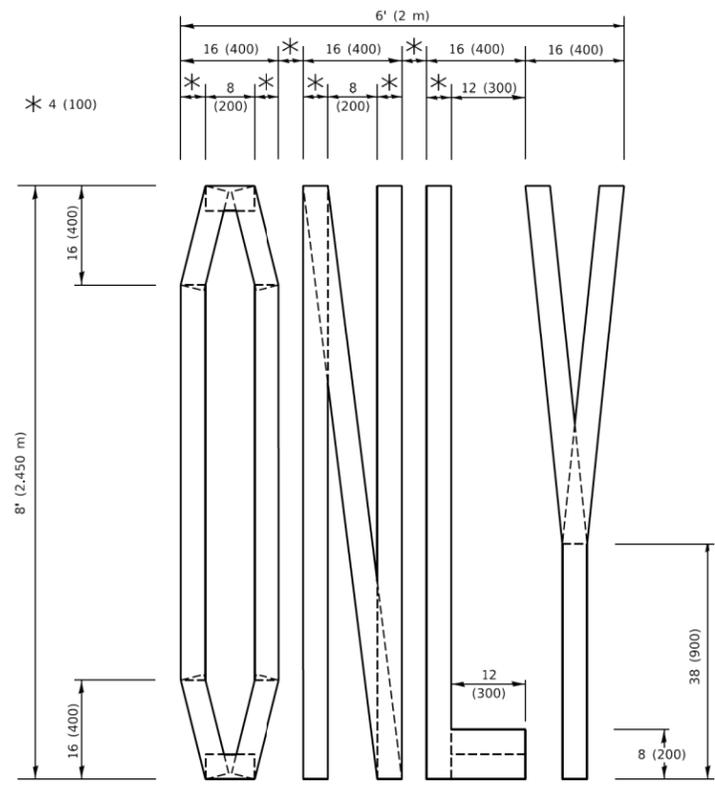
**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

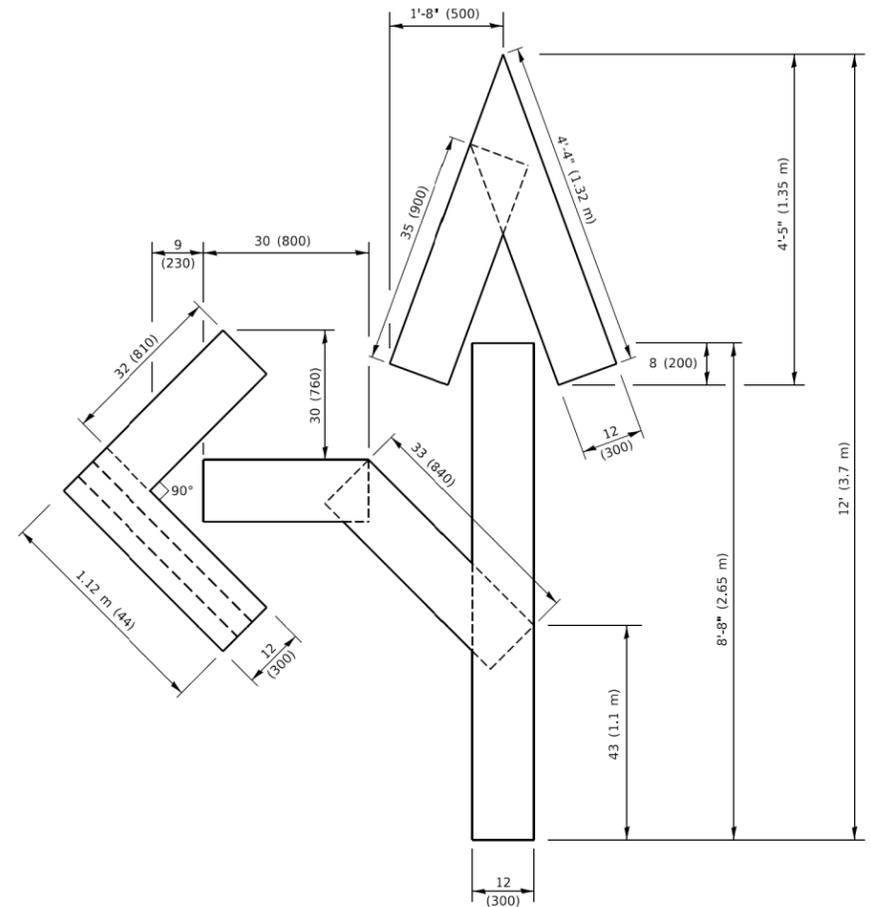
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	16
<b>TC-13</b>		CONTRACT NO. 61L80		
ILLINOIS FED. AID PROJECT				



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

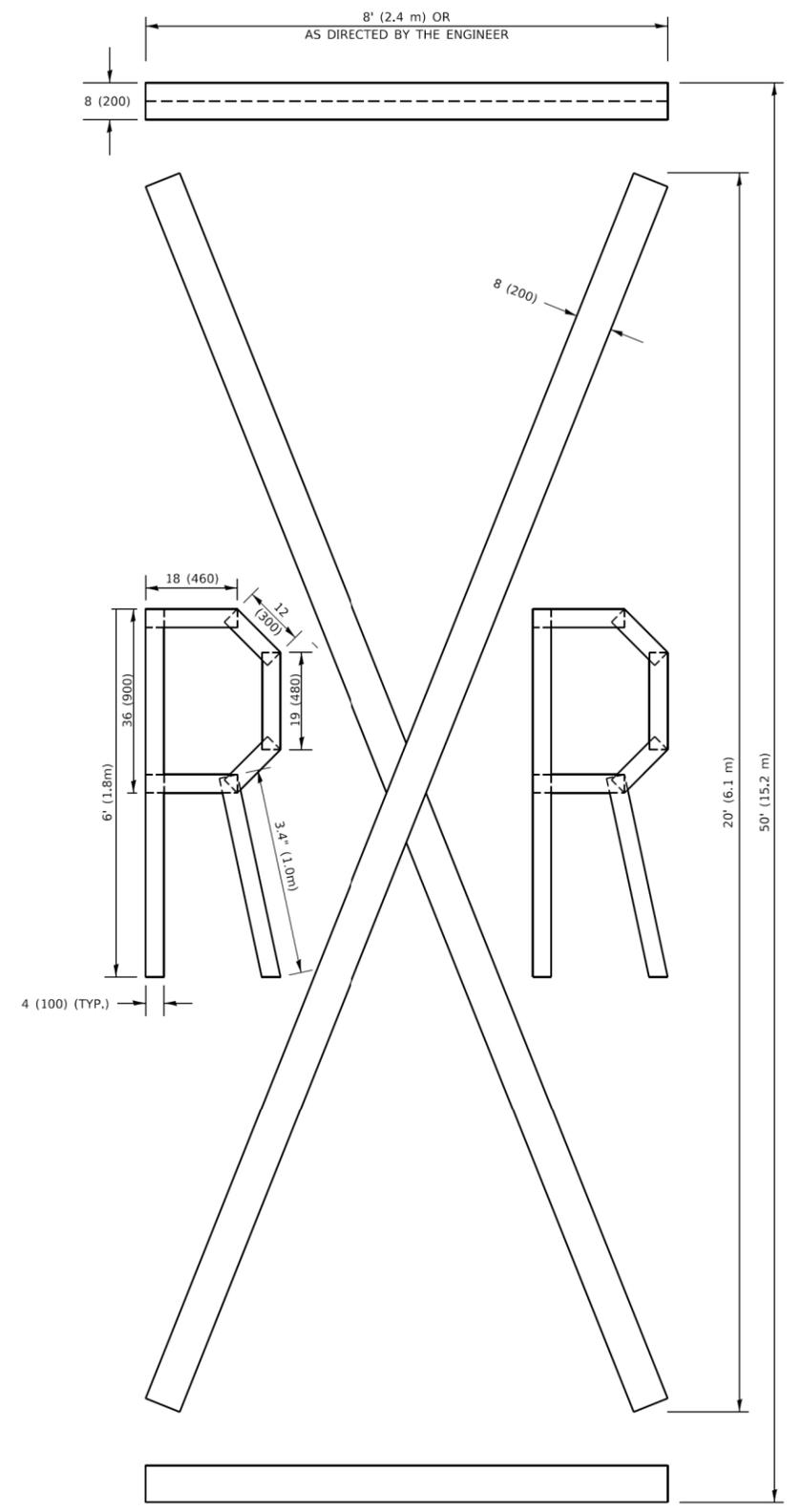


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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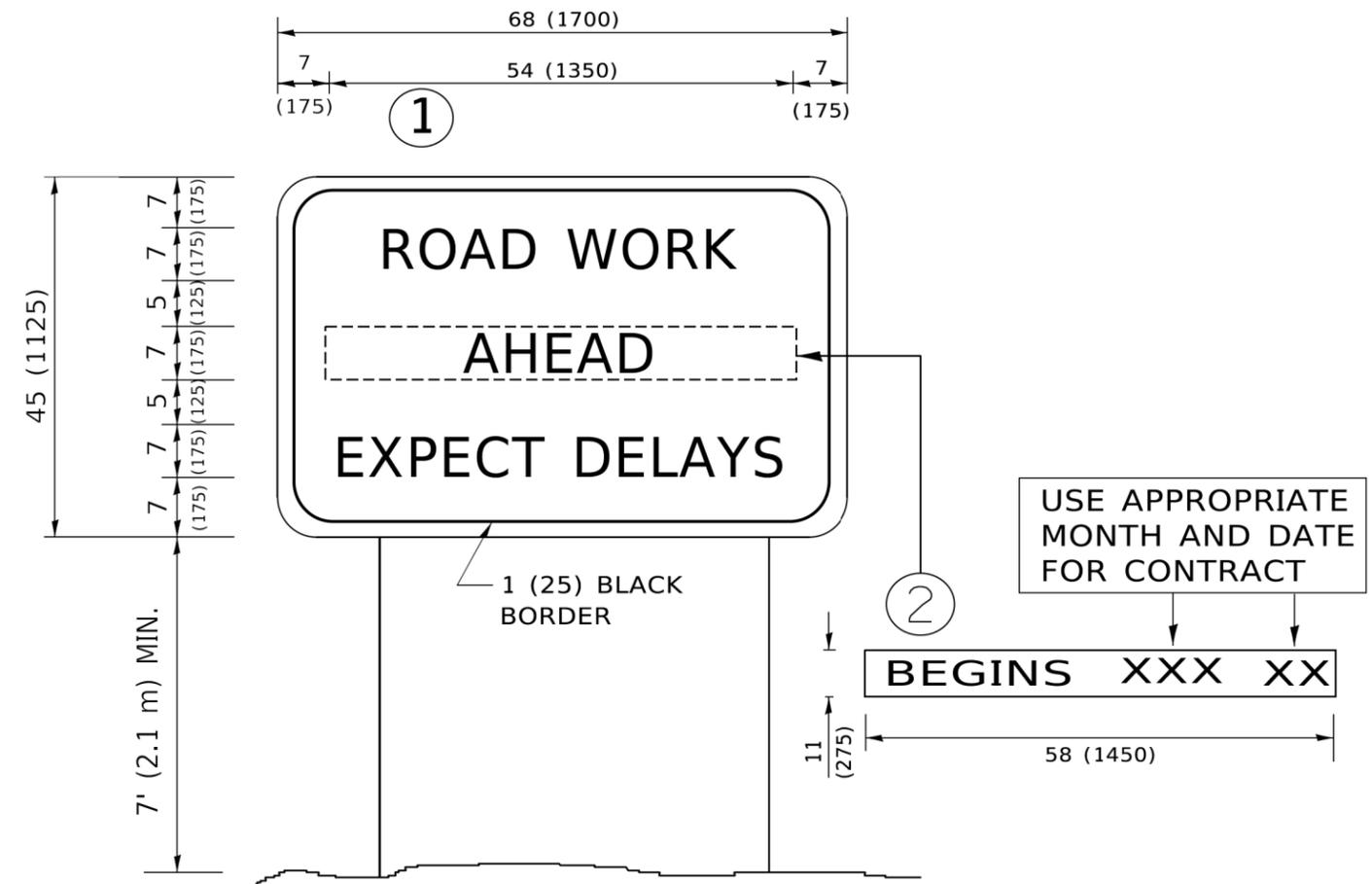
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	DRAWN -	REVISED - E. GOMEZ 08-28-00
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PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	17
<b>TC-16</b>			CONTRACT NO. 61L80	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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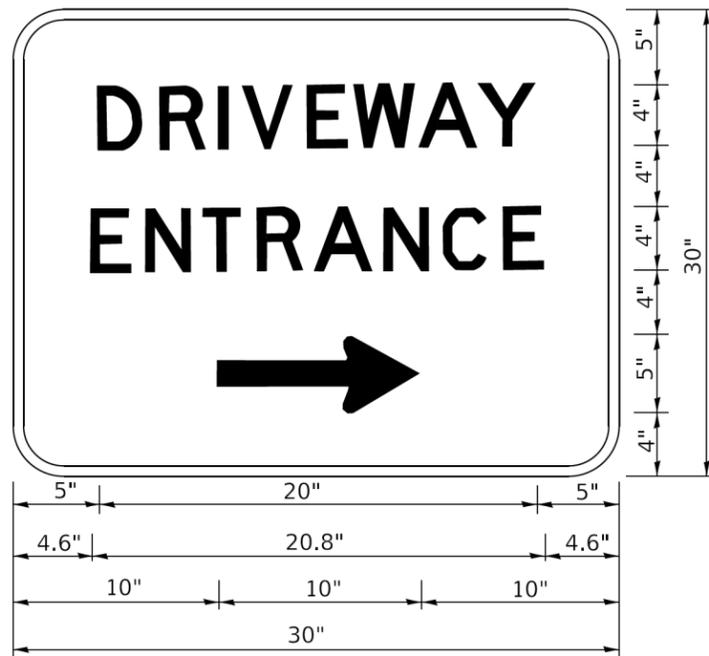
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PLOT SCALE = 50,0000 ' / ft.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-RS	DUPAGE	22	18
<b>TC-22</b>			CONTRACT NO. 61L80	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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PLOT SCALE = 50,0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 8/6/2021	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

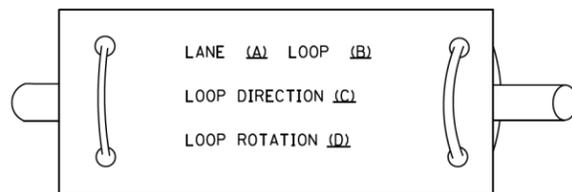
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-26			CONTRACT NO. 61L80	
ILLINOIS FED. AID PROJECT				

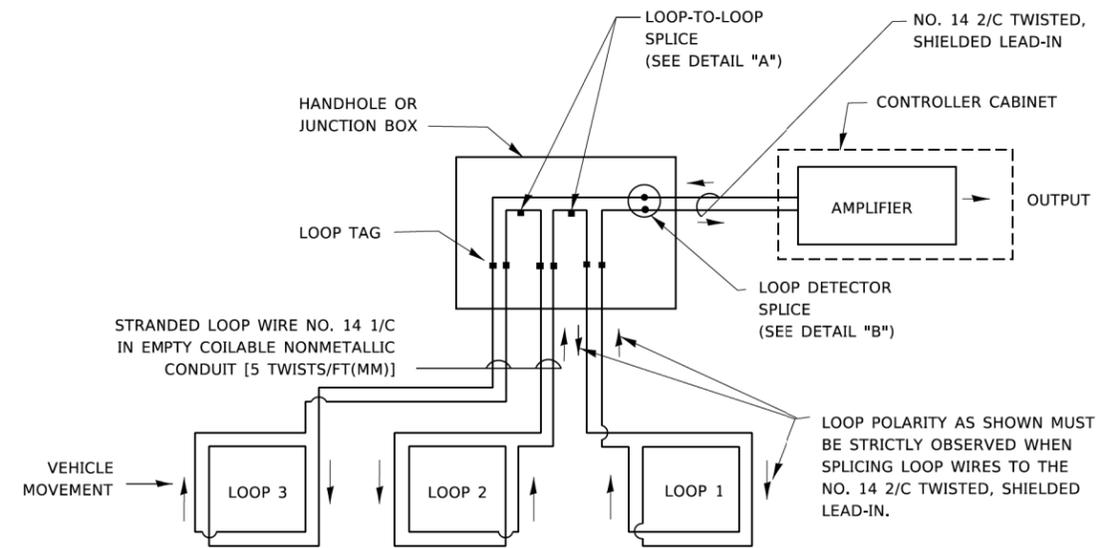
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

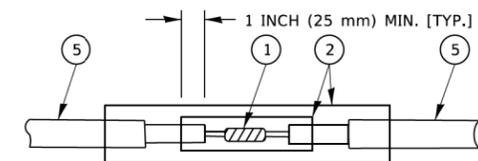


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

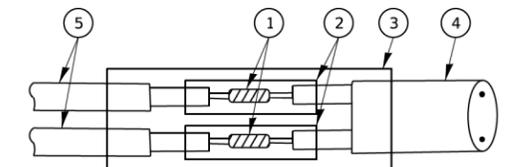


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

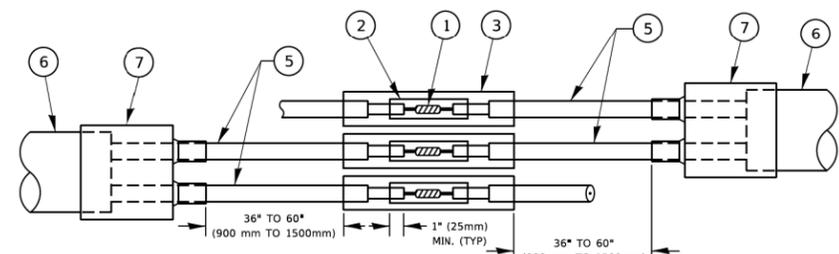


DETAIL "A"  
LOOP-TO-LOOP SPLICE

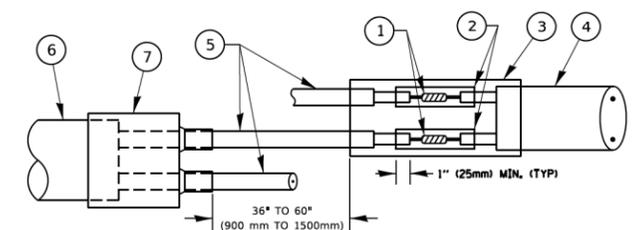


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

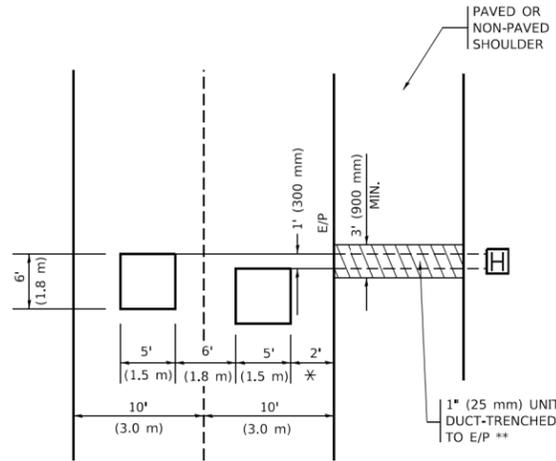
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-RS	DUPAGE	22	20
<b>TS-05</b>		CONTRACT NO. 61L80		
ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



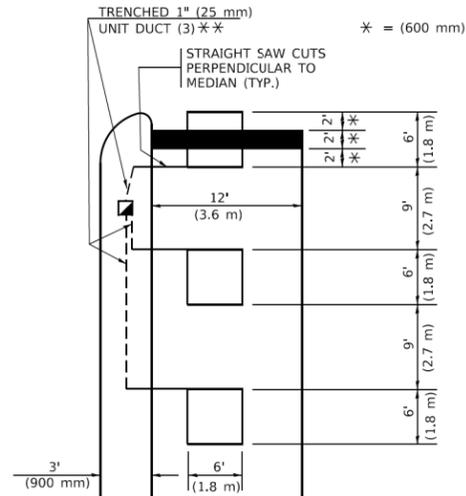
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**

**VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



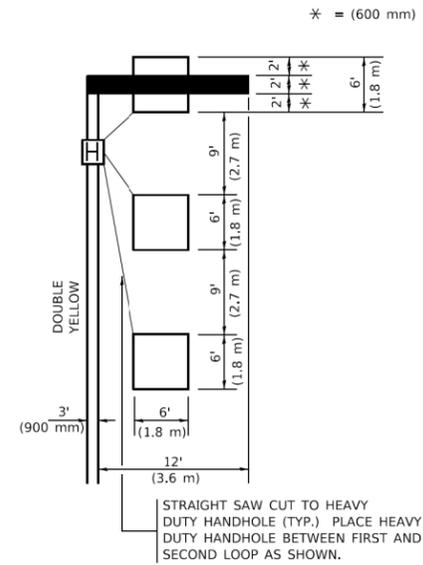
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**

**VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

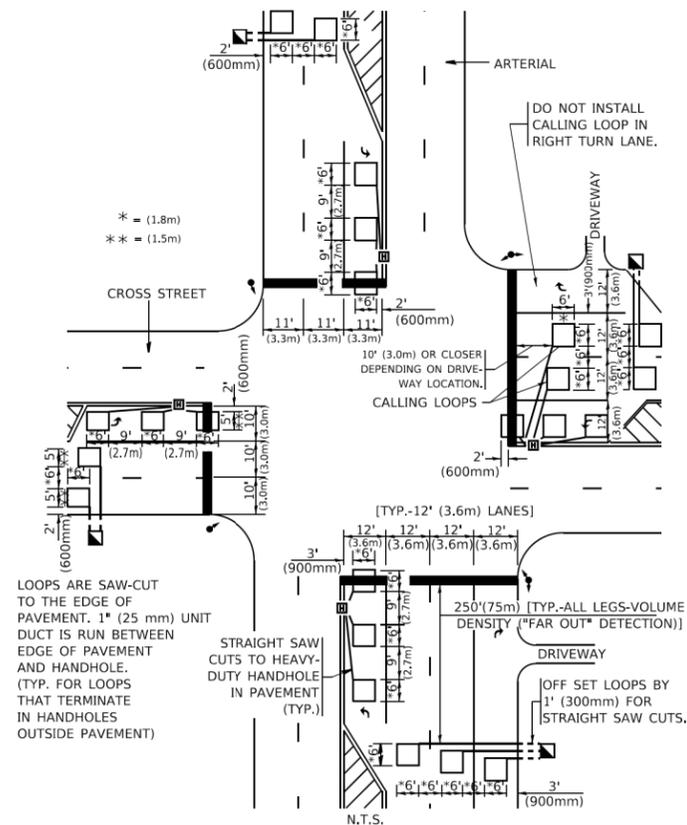
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



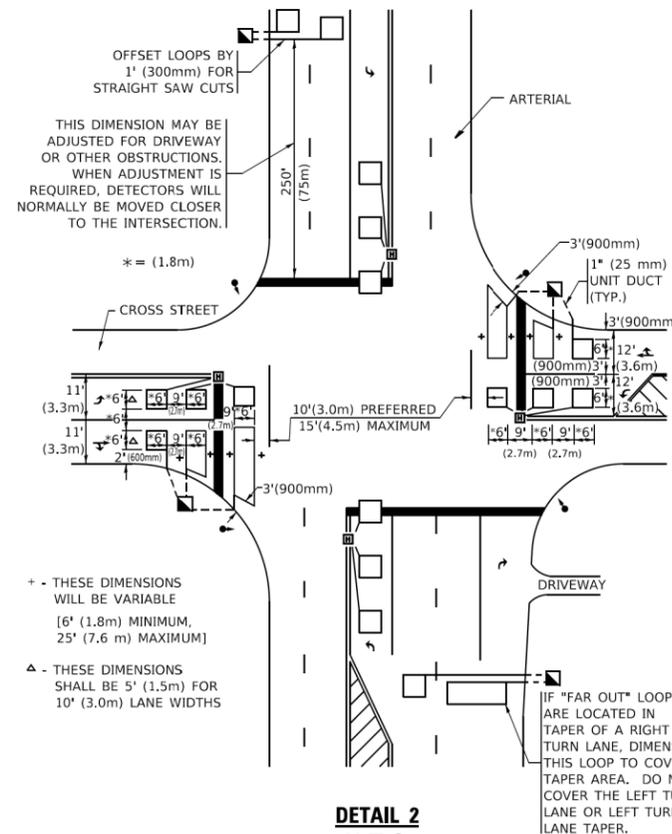
\* = (1.8m)  
\*\* = (1.5m)

LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]  
△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

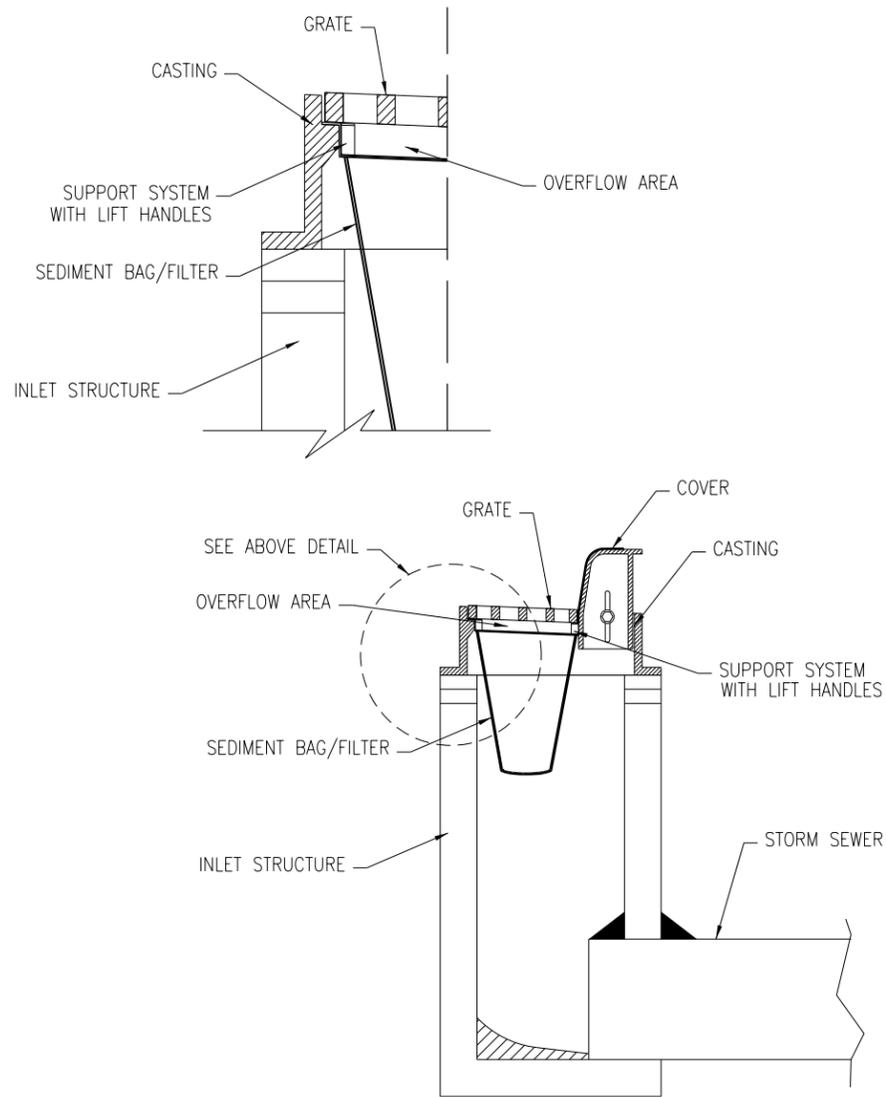
**DETAIL 2**  
N.T.S.

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

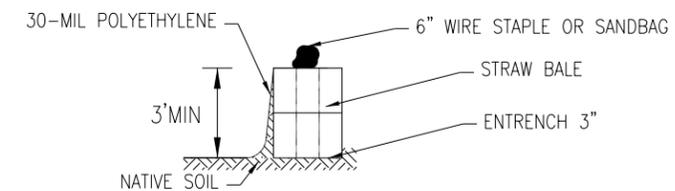
**DISTRICT 1 - DETECTOR LOOP INSTALLATION**  
**DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

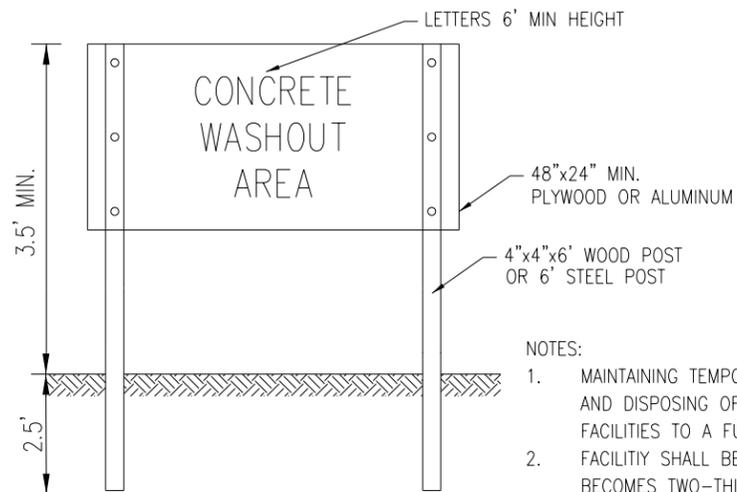
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-RS	DUPAGE	22	21
TS-07			CONTRACT NO. 61L80	
ILLINOIS FED. AID PROJECT				



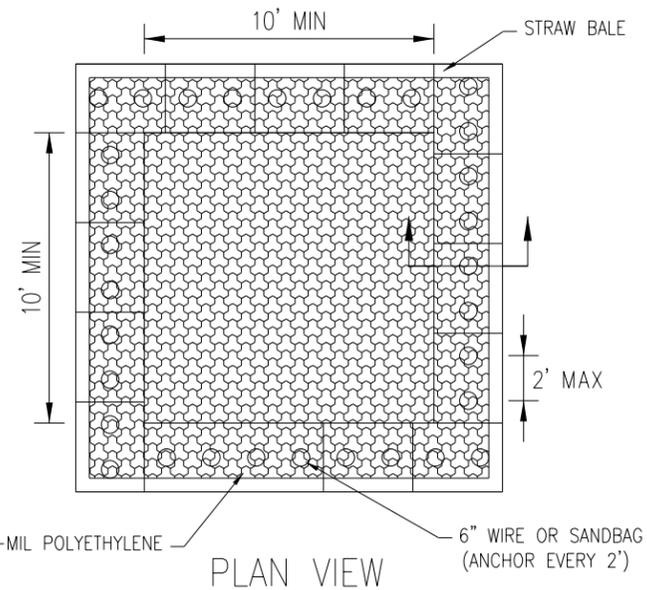
**INLET PROTECTION**  
NOT TO SCALE



**STRAW BALE ANCHOR SECTIONS**



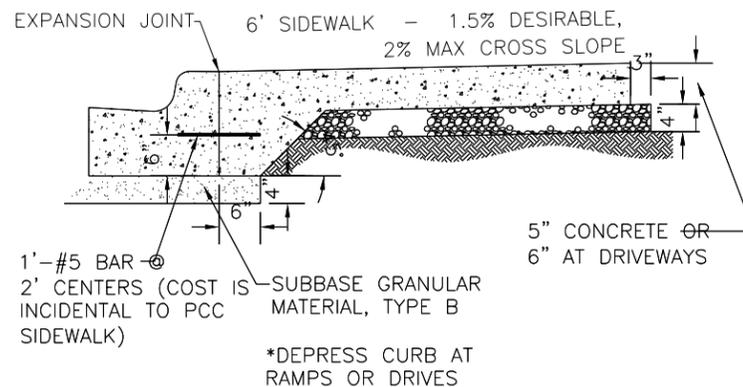
**SIGN DETAIL**



**PLAN VIEW**

- NOTES:
1. MAINTAINING TEMPORARY CONCRETE WASHOUT FACILITIES SHALL INCLUDE REMOVING AND DISPOSING OF HARDENED CONCRETE AND/OR SLURRY AND RETURNING THE FACILITIES TO A FUNCTIONAL CONDITION.
  2. FACILITY SHALL BE CLEANED OR RECONSTRUCTED IN A NEW AREA ONCE WASHOUT BECOMES TWO-THIRDS FULL.
  3. EACH STRAW BALE IS TO BE STAKED IN PLACE USING (2) 2"x2"x4" WOODEN STAKES.

**TEMPORARY CONCRETE WASHOUT FACILITY – STRAW BALE**  
NOT TO SCALE



**CONCRETE SIDEWALK ADJACENT TO CURB & GUTTER**  
NOT TO SCALE

MODEL: Default  
 FILE: \\aecom-na-pw-bentley.com\AECOM\_USA\Illinois\Documents\60699996.dwg  
 USER: nolan.hicks  
 PLOT DATE: 8/27/2025

**AECOM**  
650 WARRENVILLE ROAD  
SUITE 350  
LISLE, IL 60532  
TEL. 312-373-7700

USER NAME = nolan.hicks	DESIGNED - NRH	REVISED -
DRAWN - KJB	REVISIONS -	
CHECKED - SPF	REVISIONS -	
DATE - 8/27/2025	REVISIONS -	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2995	24-00087-00-R5	DUPAGE	22	22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61L80	