

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**THIS PROJECT IS LOCATED IN
THE VILLAGE OF WORTH
AND CHICAGO RIDGE**

IL 43 TRAFFIC DATA:

SPEED LIMIT: 35 MPH

ADT (2021): 42,500

IL 7 TRAFFIC DATA:

SPEED LIMIT: 40 MPH

ADT (2021):

WEST OF IL 43 = 20,300

EAST OF IL 43 = 24,500



John P. O'Neill

DATE 10-14-2025

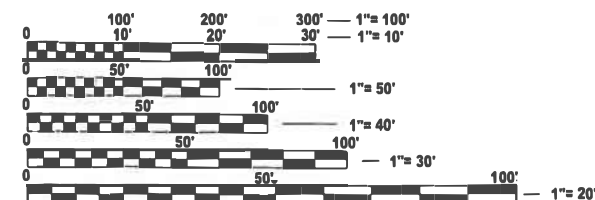
JOHN P. O'NEILL
ILLINOIS REGISTERED PROFESSIONAL ENGINEER NO. 062-048834
MY LICENSE EXPIRES ON 11-30-2025
APPLY TO SHEETS: I-117, I22-I74



Peter R. Dombrowski

DATE 10-14-2025

PETER R. DOMBROWSKI
ILLINOIS REGISTERED STRUCTURAL ENGINEER NO. 081-006519
MY LICENSE EXPIRES ON 11-30-2026
APPLY TO SHEETS: I18-I21



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER : LUKASZ POCIECHA
PROJECT MANAGER : VESELIN VELICHKOV

CONTRACT NO. 62W47

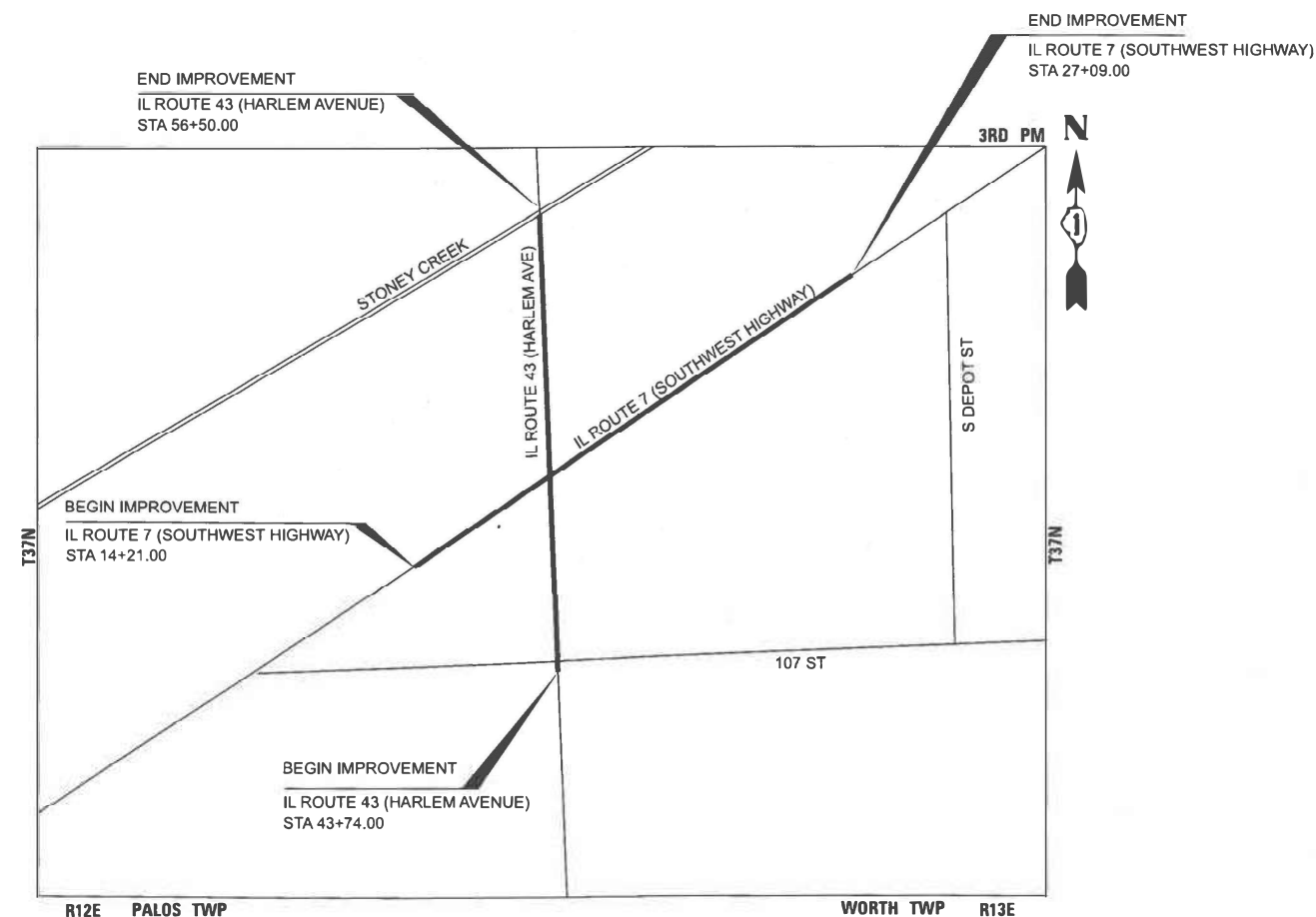
01-16-2026 LETTING ITEM 028

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PROPOSED
HIGHWAY PLANS**

**FAU ROUTE 3578: IL RTE 7 (SOUTHWEST HIGHWAY)
AT IL RTE 43 (HARLEM AVE)
SECTION: 2023-925-N-TS(L)
PROJECT: HSIP-STP-04KG(724)
INTERSECTION IMPROVEMENTS, TRAFFIC SIGNAL MODERNIZATION,
COOK COUNTY**

C-91-145-24

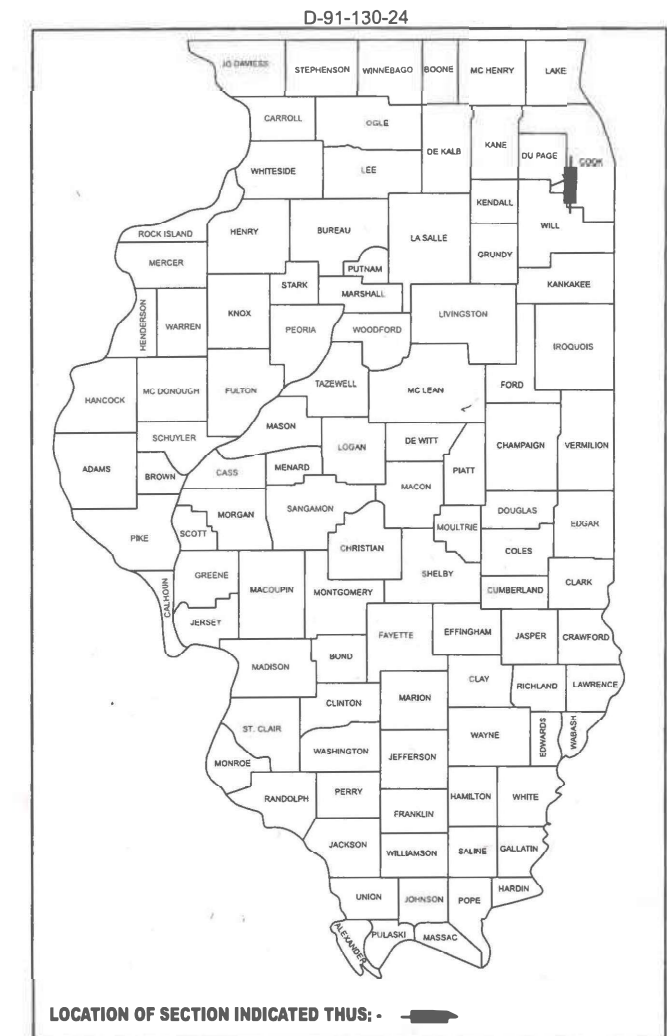


GROSS LENGTH = 2,231 FT. = 0.40 MILE
NET LENGTH = 2,231 FT. = 0.40 MILE

CONSULTING ENGINEERS
BLA, Inc.
333 PIERCE ROAD SUITE 200 ITASCA, IL 60143
P:(630) 438 6400 F:(630) 438 6444 www.bla-inc.com

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	174	1
ILLINOIS			CONTRACT NO. 62W47	

* 174 + 2 = 176 TOTAL SHEETS



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUBMITTED Oct 15th 2025
Joseph R. [Signature] REGIONAL ENGINEER

December 5th 2025
Scott A. [Signature] ENGINEER OF DESIGN AND ENVIRONMENT

December 5th 2025
Quay [Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS**

REV-SEP

MODEL: GenNotesSh01 [Sheet]
FILE NAME: W:\191~200 [DOT_208010_T08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-gennote.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
PLOT SCALE = 0.16666633" / in.	CHECKED - GJE	REVISED -
PLOT DATE = 11/25/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)					
SCALE: NTS	SHEET 1	OF 2	SHEETS	STA. N/A	TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	2
			CONTRACT NO. 62W47	
		ILLINOIS	FED. AID PROJECT	

INDEX OF SHEETS

SHEET NO.

DESCRIPTION

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DISTRICT DETAILS

BD-01	DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >15'
BD-02	DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15'
BD-08	FRAMES AND LIDS TO BE ADJUSTED WITH MILLING; AND FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
BD-33	HMA TAPER AT EDGE OF PCC PAVEMENT
TC-10	T.C & PROTECTION SIDE ROADS, INTERSECTIONS, & DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLAOW RESISTANT)
TC-13	TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-02	MAST ARM MOUNTED STREET SIGNS
TS-05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
TS-08	EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION - MAIN STREET AND CROSS STREET

HIGHWAY STANDARDS

STANDARD NO.

DESCRIPTION

000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEM
604001-05	FRAME AND LIDS TYPE 1
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM EDGE OF PAVEMENT
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701427-05	LANE CLOSURE MULTILANE, INTERMITTENT OR MOVING OPERATIONS <= 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON-TRAVES MEDIAN
701602-10	URBAN LANE CLOSURE MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE MULTILANE, SW WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-11	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-08	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-11	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS

GENERAL NOTES

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGHTS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT (CUYD) WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND THE IDOT SUBGRADE STABILITY MANUAL.

ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT CONTRACTOR'S EXPENSE.

THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.

THE AGGREGATE GRADATION FOR THE LOWER 9 INCHES OF AGGREGATE SUBGRADE IMPROVEMENT 12" SHALL BE CS 1 OR RR 1.

GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDINACE WITH ARTICLE 301.04 OF THE STANDARD SPECIFICATIONS AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

GENERAL NOTES CONTINUE ON NEXT SHEET

GENERAL NOTES CONTINUED

THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. MAJOR ROOTS OF A TREE THAT ARE TO REMAIN IN PLACE EXTENDING INTO THE EXCAVATION AREAS AT AN ELEVATION THAT WOULD INTERFERE WITH ANY PORTION OF THE PLANNED CONSTRUCTION SHALL BE SEVERED AT A POINT IMMEDIATELY OUTSIDE OF THE EXACTION AREA IN A MANNER THAT WILL CAUSE THE LEAST AMOUNT OF SYSTEMIC TO THE REMAINING TREE STRUCTURE. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.

THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" AND AROUND EXISTING WETLANDS TO ESTABLISH A "WETLAND PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLE DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE"AND "WETLAND PROTECTION ZONE." REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

THE CONTRACTOR SHALL ATTACH AN ALUMINUM SIGN WITH THE FOLLOWING TEXT: PROTECTED WETLAND - NOT INTRUSION. THE SIGN(S) SHALL BE ATTACHED TO THE STAKES BY A METHOD APPROVED BY THE ENGINEER. THE SIGN(S) WILL BE PROVIDED BY THE DEPARTMENT AND SHALL BE PICKED UP BY THE CONTRACTOR FROM THE DISTRICT ONE ROADSIDE DEVELOPMENT ARCHITECT IN SCHAUMBURG, ILLINOIS BY CONTACTING 847-705-4171. WHEN WORK HAS BEEN COMPLETED, THE SIGN SHALL BE RETURNED TO THE DISTRICT ONE ROADSIDE DEVELOPMENT UNIT. THE COST OF PICKING UP, ATTACHING THE SIGNS TO THE TEMPORARY FENCE STAKES AND RETURNING THE SIGNS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TEMPORARY FENCE.

IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK. IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITIES FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUESTS MAY BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATION OF UNDERGROUND TRAFFIC FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.

THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 4B (MODIFIED) AND 5B (MODIFIED) SHALL BE FROM NOVEMBER 15 TO MARCH 15. ALL SEEDING NOT SOWN ACCORDING TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER. FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT HIS/HER EXPENSE

EXISTING VEGETATED AREAS (TREES, SHRUBS, VEGETATIVE BUFFERS, TURF AREAS, ETC.) WHERE DISTURBANCE IS NOT OCCURRING (INCLUDING AREAS OUTSIDE THE PROJECT LIMITS) SHALL NOT BE DISTURBED TO ENSURE THAT EXISTING VEGETATION IS PRESERVED TO MINIMIZE SOIL EROSION AND TO ELIMINATE SOIL COMPACTION. NO MATERIALS ARE TO BE STORED OR VEHICLES DRIVEN OR PARKED WITHIN THESE UNDISTURBED AREAS AT ANY TIME.

IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847.705.4171 TO SCHEDULE A WALK THROUGH TO DETERMINE TREE PROTECTION, TREE REMOVAL, SELECTIVE CLEARING, AND OTHER FORESTRY WORK A MINIMUM OF 7 DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. ALL TREE PROTECTION, TREE REMOVAL, SELECTIVE CLEARING, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ROADSIDE DEVELOPMENT UNIT.

THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS, PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) and USE/WASTE REVIEW(BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND INSECTION II.G.1 AND 2 of the SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY TREE REMOVAL AND TREE PRUNING SHALL BE DONE BETWEEN OCTOBER 15 AND APRIL 15.

MODEL: GenNotesSh02 [Sheet]
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USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)

SCALE: NTS	SHEET 2	OF 2	SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	3
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

MODEL: 1
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-SQ00.dgn

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
				URBAN	URBAN	URBAN	URBAN	URBAN
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	24	24				
20100500	TREE REMOVAL, ARCES	ACRE	0.25	0.25				
20200100	EARTH EXCAVATION	CU YD	1972	1972				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	603	603				
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2592	2592				
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	347	347				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	50	50				
21101630	TOPSOIL FURNISH AND PLACE, 8"	SQ YD	810	810				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	35	35				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	35	35				
25200110	SODDING, SALT TOLERANT	SQ YD	2781	2781				
25200200	SUPPLEMENTAL WATERING	UNIT	42	42				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	52	52				
28000305	TEMPORARY DITCH CHECKS	FOOT	84	84				
28000400	PERIMETER EROSION BARRIER	FOOT	28	28				

* = SPECIALTY ITEMS



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 1 OF 14 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	4
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 2
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-shl-SQO.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
28000510	INLET FILTERS	EACH	37	37				
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	2781	2781				
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	200	200				
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2592	2592				
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	231	231				
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	2700	2700				
35300305	PORTLAND CEMENT CONCRETE BASE COURSE 8 1/4"	SQ YD	328	328				
35400325	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 8 1/4"	SQ YD	231	231				
35501317	HOT-MIX ASPHALT BASE COURSE, 8 1/4"	SQ YD	627	627				
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	2667	2667				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	11193	11193				
40600370	LONGITUDINAL JOINT SEALANT	FOOT	10008	10008				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	18	18				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	206	206				

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 2 OF 14 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	5
		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

MODEL: 3
FILE NAME: W:\191-200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\19102218-shl-SQO.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	750	750				
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	33	33				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	1722	1722				
42001300	PROTECTIVE COAT	SQ YD	3257	3257				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	396	396				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	11333	11333				
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	6001	6001				
42400800	DETECTABLE WARNINGS	SQ FT	443	443				
44000100	PAVEMENT REMOVAL	SQ YD	132	132				
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	201	201				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	10302	10302				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	889	889				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	5005	5005				
44000600	SIDEWALK REMOVAL	SQ FT	13573	13573				

* = SPECIALTY ITEMS



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USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 3 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	6
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 4
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shl-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
44003100	MEDIAN REMOVAL	SQ FT	7194	7194				
44200934	CLASS B PATCHES, TYPE II, 8 INCH	SQ YD	100	100				
44200942	CLASS B PATCHES, TYPE III, 8 INCH	SQ YD	100	100				
44201298	DOWEL BARS 1 1/4"	EACH	720	720				
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	100	100				
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	100	100				
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	100	100				
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	100	100				
44213000	PATCHING REINFORCEMENT	SQ YD	200	200				
44213200	SAW CUTS	FOOT	984	984				
44213208	TIE BARS 1 1/4"	EACH	30	30				
50102400	CONCRETE REMOVAL	CU YD	18.6					18.6
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	8	8				
55100500	STORM SEWER REMOVAL 12"	FOOT	5	5				

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS	SHEET 4	OF 14	SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	7
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 5
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-shl-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
56400400	FIRE HYDRANT TO BE RELOCATED	EACH	3	3				
60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	2	2				
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	9	9				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	44	44				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	8	8				
60500060	REMOVING INLETS	EACH	2	2				
60600605	CONCRETE CURB, TYPE B	FOOT	21	21				
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	2369	2369				
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1830	1830				
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	54	54				
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	129	129				

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 5 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	8
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 6
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shl-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	200	200				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	8	8				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	40	40				
67100100	MOBILIZATION	L SUM	1	1				
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	130	130				
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	520	520				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	13421	13421				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	5765	5765				
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	400	400				
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	12227	12227				
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	1543	1543				
70307140	TEMPORARY PAVEMENT MARKING - LINE 8" - TYPE IV TAPE	FOOT	49	49				

* = SPECIALTY ITEMS



BLA, Inc.

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	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 6 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	9
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 7
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shl-SQ00.dgn



USER NAME	= tnestor	DESIGNED	-	TN	REVISED	-	
		DRAWN	-	TN	REVISED	-	
		CHECKED	-	GJE	REVISED	-	
PLOT DATE	= 10/17/2025	DATE	-	10/15/2025	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 7 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	10
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 8
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shl-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
*								
	80400100	ELECTRIC SERVICE INSTALLATION	EACH	1			1	
*								
*								
	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	2448	1452		996	
*								
	81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	135	110		25	
*								
	81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	764	764			
*								
	81400100	HANDHOLE	EACH	6	6			
*								
	81400200	HEAVY-DUTY HANDHOLE	EACH	4	4			
*								
	81400300	DOUBLE HANDHOLE	EACH	3	3			
*	81603035	UNIT DUCT, 600V, 2-1 C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	125			125	
	81603100	UNIT DUCT, 600V, 4-1 C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	2864			2864	
*								
	81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	900			900	
*								
	81702400	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 2	FOOT	50			50	
*								
	82110008	LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H	EACH	17			17	
*								
	82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1			1	
*								
	83050810	LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM	EACH	12			12	

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 8 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	11
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 9
FILE NAME: W:\191-200\DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-shr-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
*								
	83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	99			99	
*								
	83800205	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	12			12	
*								
	84200804	REMOVAL OF POLE FOUNDATION	EACH	12			12	
*								
	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2			
*								
	87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	5579	5579			
*								
	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	4941	4941			
*								
	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2712	2712			
*								
	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2772	2772			
*								
	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1895	1895			
*								
	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2618	2618			
*								
	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	25	25			
*								
	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	2060	2060			
*								
	87501200	TRAFFIC SIGNAL POST, 16 FT.	EACH	4	4			
*								
	87702980	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EACH	2	2			

* = SPECIALTY ITEMS



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS	SHEET 9	OF 14 SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	12
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 10
FILE NAME: W:\191-200\DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
*								
	87702985	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 52 FT.	EACH	2	2			
*								
	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	20	20			
*								
	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4	4			
*								
	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30	30			
*								
	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	60	60			
*								
	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	10	10			
*								
	88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4	4			
*								
	88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4	4			
*								
	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4	4			
*								
	88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	10	10			
*								
	88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	14	14			
*								
	88500100	INDUCTIVE LOOP DETECTOR	EACH	8	8			
*								
	88600100	DETECTOR LOOP, TYPE I	FOOT	2981	2981			
*								
	88700200	LIGHT DETECTOR	EACH	2		2		

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 10 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	13
		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

MODEL: 11
FILE NAME: W\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
*	88700300	LIGHT DETECTORAMPLIFIER	EACH	1		1		
*	89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1	1			
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1	1			
*	89502380	REMOVE EXISTING HANDHOLE	EACH	17	17			
*	89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	1	1			
*	89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	13	13			
*	A2005616	TREE, OSTRYA VIRGINIANA (AMERICAN HOPHORNBEAM), 2" CALIPER, BALLED AND BURLAPPED	EACH	1	1			
*	A2007916	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	EACH	4	4			
*	A2016616	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	8	8			
*	X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	627		627		
	X0325734	SLOTTED DRAIN REMOVAL	FOOT	25	25			
	X0326806	WASHOUT BASIN	L SUM	1	1			
*	X1400150	SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1	1			
*	X1400217	TERMINATE FIBER IN CABINET	EACH	3	3			

* = SPECIALTY ITEMS



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	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 11 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	14
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 12
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-SQ00.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 12 OF 14 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	15
		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

☐ NON-PART 100% STATE

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	6		6			
X1400378	PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2		2			
X1700067	STAMPED COLORED PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	2202	2202				
X2010514	SELECTIVE CLEARING	ACRE	0.02	0.02				
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	19	19				
X4060294	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YD	383	383				
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	1995	1995				
X4403800	MEDIAN SURFACE REMOVAL	SQ FT	2282	2282				
X4406803	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1/4"	SQ YD	104	104				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	400	400				
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	8	8				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	13				
X6060052	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL)	FOOT	23	23				
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12				

* = SPECIALTY ITEMS

MODEL: 13
FILE NAME: W:\191-200\DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shl-SQ00.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1				
X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	201	201				
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	34	34				
X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	34	34				
X8000003	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	8				8	
X8250091	COMBINATION LIGHTING CONTROLLER	EACH	1				1	
X8360315	LIGHT POLE FOUNDATION, 30" DIAMETER, OFFSET	FOOT	9				9	
X8570232	FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET	EACH	1		1			
X8620200	UNINTERRUPTABLE POWER SUPPLY (SPECIAL)	EACH	1		1			
X8710024	FIBER OPTIC CABLE IN CONDUIT , NO. 62.5/125, MM12F SM24F	FOOT	5579		5579			
X8760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	10		10			
X8780012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	32		32			
X8809005	LED SIGNAL FACE, LENS COVER	EACH	52		52			
X8900104	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1			

* = SPECIALTY ITEMS



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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 13 OF 14 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	16
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 14
FILE NAME: W:\191-200\DOT_208010_IT08_IL7 at IL43 Intersection Improvement\CADD Sheets\F102218-shit-SOC.dgn

				CONSTRUCTION CODE				
CODE			TOTAL	90% FED 10% STATE ROADWAY	90% FED 10% STATE TRAFFIC SIGNAL	100% VILLAGE OF WORTH EVP	STP 80% FED 20% VILLAGE OF WORTH LIGHTING	90% FED 10% STATE STRUCTURAL
				0005	0021	0021	0021	0059
*								
X8950508	REMOVE CABLE FROM CONDUIT	FOOT	5579		5579			
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	0,4					0,4
53101408	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4"	SQ YD	156					156
Z0012800	CONCRETE PAVEMENT SCARIFICATION	SQ YD	135					135
53101002	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	156					156
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
53016000	DECK SLAB REPAIR (PARTIAL)	SQ YD	5					5
□ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	32	32				
Z0033022	CLEAN AND RELAMP EXISTING LUMINAIRE	EACH	17				17	
*								
Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1		1			
∅ Z0076600	TRAINEES	HOURL	500	500				
∅ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURL	500	500				
X4400104	SURFACE REMOVAL (SPECIAL)	SQ YD	276	276				

* = SPECIALTY ITEMS

□ NON-PART 100% STATE

∅ 0042



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

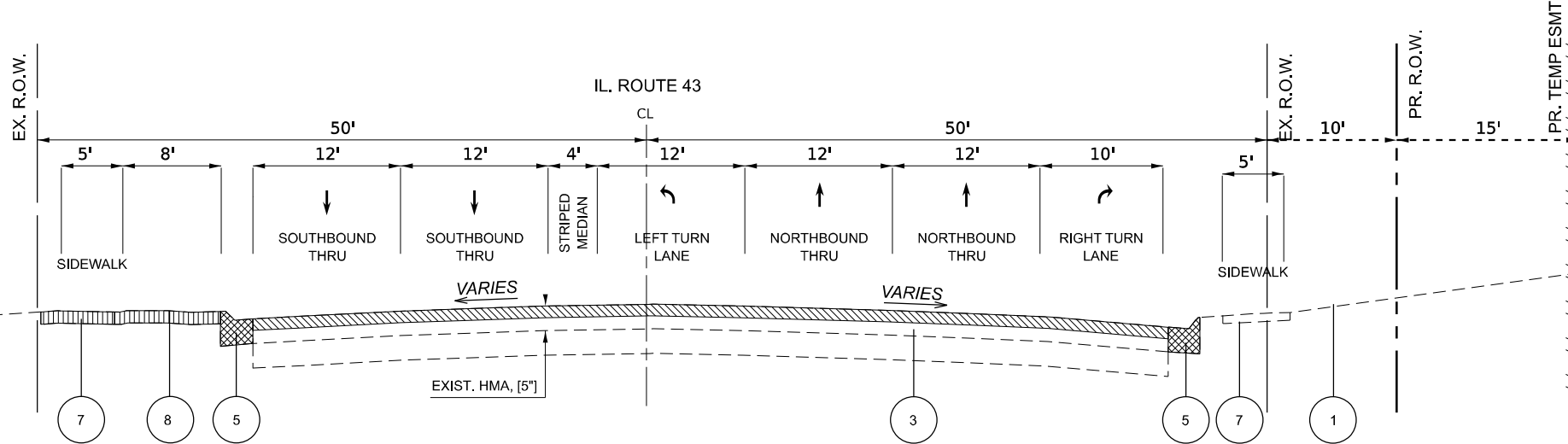
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

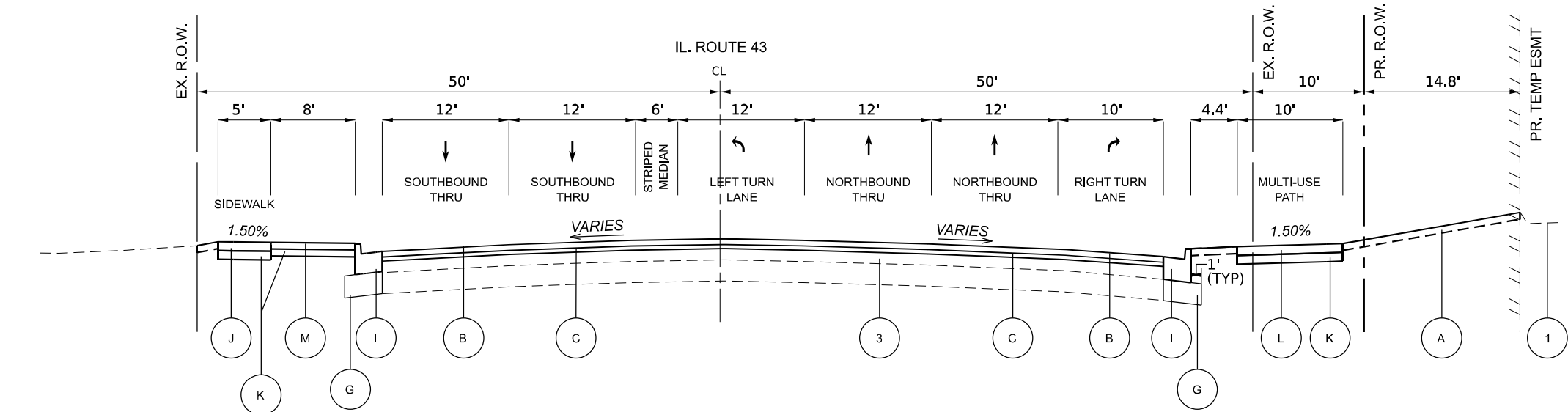
SCALE: NTS SHEET 14 OF 14 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	17
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Plan - Plan 1 [Sheet]
FILE NAME: W:\191-200 DDT_208010_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-typical.dgn



EXISTING TYPICAL SECTION
RESURFACING LIMITS: STA. 43+74.00 TO STA. 49+62.59
IL ROUTE 43 (HARLEM AVENUE) (SOUTH LEG)



PROPOSED TYPICAL SECTION
RESURFACING LIMITS: STA. 43+74.00 TO STA. 49+62.59
IL ROUTE 43 (HARLEM AVENUE) (SOUTH LEG)

LEGEND:

- | | | |
|--|--|---|
| ① EXIST. LANDSCAPE | Ⓐ PROP. LANDSCAPING | Ⓒ PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| ② EXIST. PAVEMENT (PCC), 10" | PROP. TOPSOIL, FURNISH & PLACE, 4" | Ⓓ PROP. PCC CURB & GUTTER, TYPE B6.12 |
| ③ EXIST. PAVEMENT (HMA), 5" | PROP. SODDING, SALT TOLERANT | Ⓔ PROP. PCC CURB & GUTTER, TYPE B6.24 |
| ④ EXIST. PCC CURB & GUTTER, TYPE B6.12 | Ⓑ PROP. POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4" | Ⓙ PROP. PCC SIDEWALK, 5" |
| ⑤ EXIST. PCC CURB & GUTTER, TYPE B6.24 | Ⓒ PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4" | Ⓚ PROP. AGGR. BASE COURSE, TY B, 4" |
| ⑥ EXIST. PCC OR CORRUGATED MEDIAN | Ⓓ PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1" | Ⓛ PROP. PCC SIDEWALK, 5" (MULTI-USE PATH) |
| ⑦ EXIST. PCC SIDEWALK | Ⓔ PROP. PCC BASE COURSE, 8 1/4" | Ⓜ PROP. STAMPED COLORED PCC SIDEWALK, 5" |
| ⑧ EXIST. DECORATIVE HMA PARKWAY | Ⓕ PROP. HMA BASE COURSE, 8 1/4" | |

- ▨ HMA SURFACE REMOVAL/PCC SURFACE REMOVAL (SEE PLANS FOR DEPTHS)
- ▩ COMBINATION CURB AND GUTTER REMOVAL
- ▧ SIDEWALK REMOVAL/ BIKE PATH REMOVAL
- ▦ EARTH EXCAVATION

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLY HMA BINDER COURSE IL-4.75, N50.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE		AIR VOIDS @ Ndes	
HMA PAVEMENT WIDENING			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"		3.5% AT 80 GYR.	QC/QA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"		3.5% AT 50 GYR.	QC/QA
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19.0), 8 1/4"		4% AT 90 GYR.	QC/QA
HMA RESURFACING - IL ROUTE 43 (HARLEM AVENUE)			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"		3.5% AT 80 GYR.	QC/QA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"		3.5% AT 50 GYR.	QC/QA
HMA RESURFACING - IL ROUTE 7 (SOUTHWEST HIGHWAY)			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"		3.5% AT 80 GYR.	QC/QA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"		3.5% AT 50 GYR.	QC/QA
HMA DRIVEWAY PAVEMENT 10" (CE)			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 2"		4% AT 50 GYR.	QC/QA
HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50, 8"		4% AT 50 GYR.	QC/QA
PARKING LOT HMA PATCHING			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 4" (IN 2 LIFTS)		4% AT 50 GYR.	QC/QA
HMA PATCHES			
CLASS D PATCHES (HMA BINDER IL-19.0), 8"		4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP) PAY FOR PERFORMANCE (PFP).			

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.



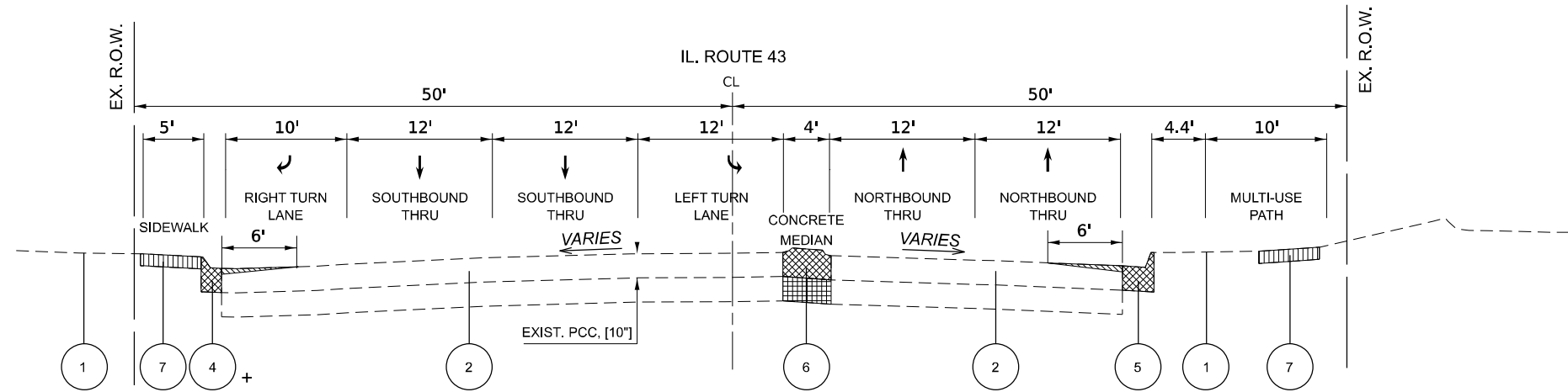
BLA, Inc.

USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/13/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: NTS	SHEET 1	OF 4 SHEETS	STA. N/A TO STA. N/A

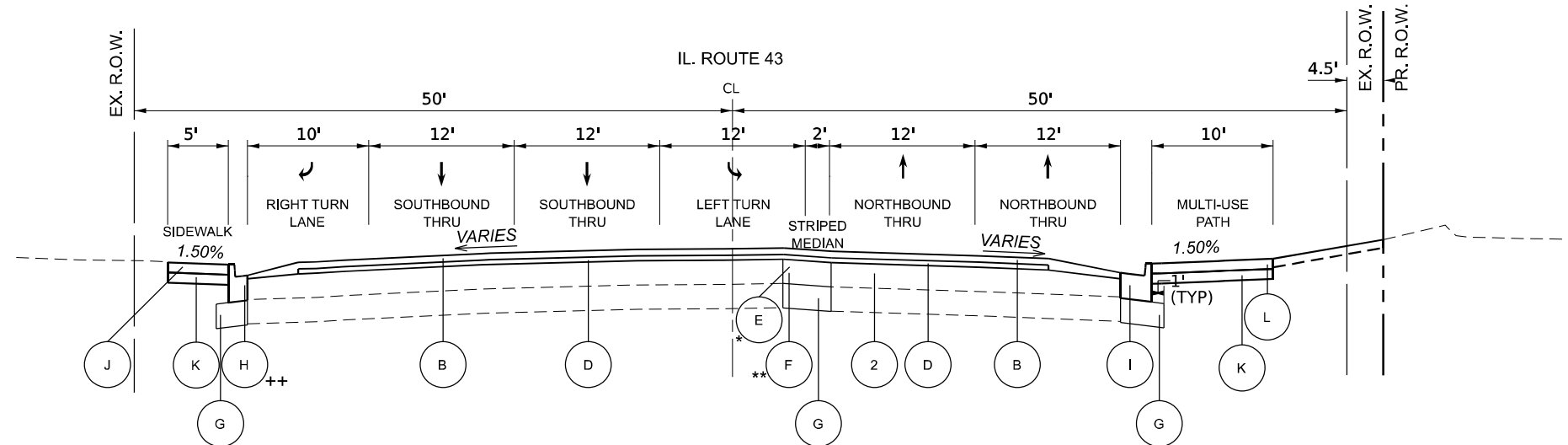
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	18
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
RESURFACING LIMITS: STA. 49+62.59 TO STA. 56+50.00
IL ROUTE 43 (HARLEM AVENUE) (NORTH LEG)

- HMA SURFACE REMOVAL/PCC SURFACE REMOVAL (SEE PLANS FOR DEPTHS)
- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL/ BIKE PATH REMOVAL
- EARTH EXCAVATION

+ EXIST. PCC CURB & GUTTER, TYPE B6.24
FROM STA. 52+57.21 TO 56+50.00



PROPOSED TYPICAL SECTION
RESURFACING LIMITS: STA. 49+62.59 TO STA. 56+50.00
IL ROUTE 43 (HARLEM AVENUE) (NORTH LEG)

++ PROP. PCC CURB & GUTTER, TYPE B6.24
FROM STA. 52+57.21 TO 56+50.00

* PCC BASE COURSE, 8 1/4 IN FROM STA. 50+70.14 TO STA. 54+83.35
** HMA BASE COURSE, 8 1/4 IN FROM STA. 54+83.35 TO STA. 55+31.00

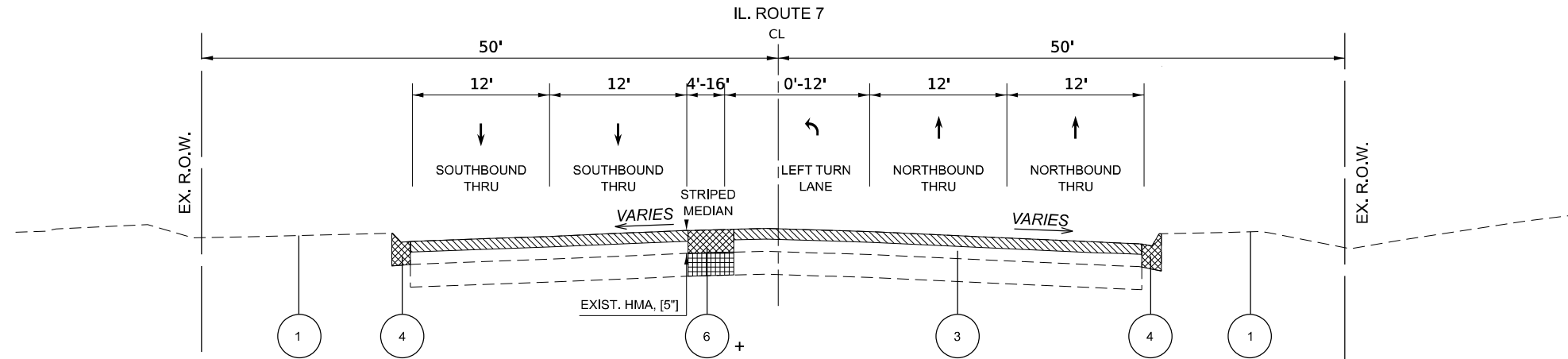
LEGEND:

- 1 EXIST. LANDSCAPE
- 2 EXIST. PAVEMENT (PCC), 10"
- 3 EXIST. PAVEMENT (HMA), 5"
- 4 EXIST. PCC CURB & GUTTER, TYPE B6.12
- 5 EXIST. PCC CURB & GUTTER, TYPE B6.24
- 6 EXIST. PCC OR CORRUGATED MEDIAN
- 7 EXIST. PCC SIDEWALK
- 8 EXIST. DECORATIVE HMA PARKWAY
- A PROP. LANDSCAPING
 - PROP. TOPSOIL, FURNISH & PLACE, 4"
 - PROP. SODDING, SALT TOLERANT
- B PROP. POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"
- C PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4"
- D PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1"
- E PROP. PCC BASE COURSE, 8 1/4"
- F PROP. HMA BASE COURSE, 8 1/4"
- G PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
- H PROP. PCC CURB & GUTTER, TYPE B6.12
- I PROP. PCC CURB & GUTTER, TYPE B6.24
- J PROP. PCC SIDEWALK, 5"
- K PROP. AGGR. BASE COURSE, TY B, 4"
- L PROP. PCC SIDEWALK, 5" (MULTI-USE PATH)
- M PROP. STAMPED COLORED PCC SIDEWALK, 5"

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLY HMA BINDER COURSE IL-4.75, N50.

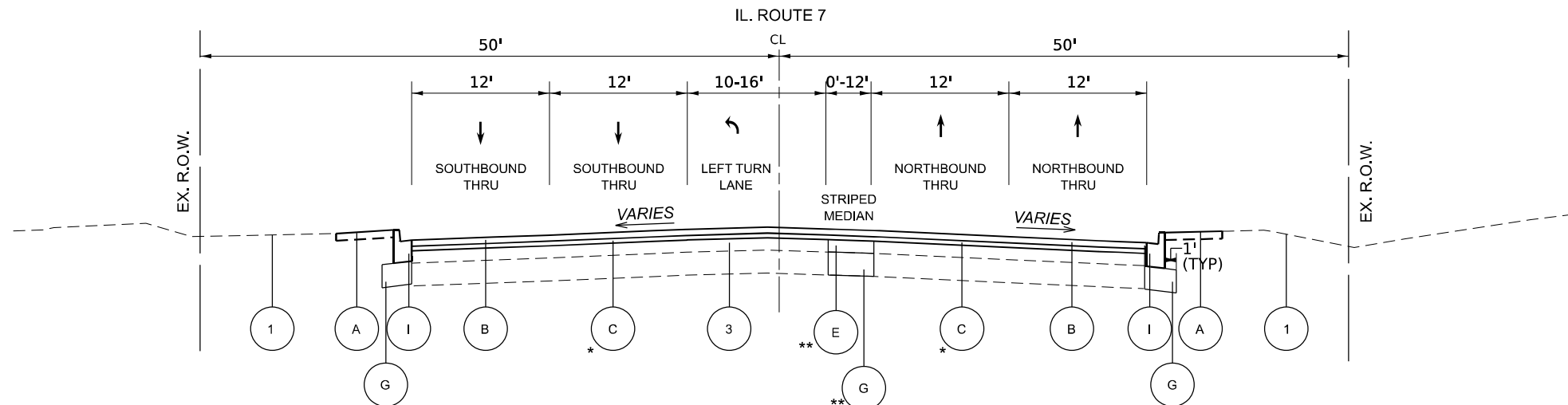
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MODEL: Unnamed Plan-1 - Plan 2 (Sheet)
FILE NAME: W:\191-200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sht-typical.dgn



EXISTING TYPICAL SECTION
RESURFACING LIMITS: STA. 14+21.00 TO STA. 20+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY) (SOUTHWEST LEG)

+ FROM STA. 18+40.36 TO 19+29.22



PROPOSED TYPICAL SECTION
RESURFACING LIMITS: STA. 14+21.00 TO STA. 20+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY) (SOUTHWEST LEG)

* PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1"
FROM STA. 18+40.36 TO STA. 20+00.00
** FROM STA. 18+40.36 TO STA. 19+29.22

- HMA SURFACE REMOVAL/PCC SURFACE REMOVAL (SEE PLANS FOR DEPTHS)
- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL/ BIKE PATH REMOVAL
- EARTH EXCAVATION

LEGEND:

- ① EXIST. LANDSCAPE
- ② EXIST. PAVEMENT (PCC), 10"
- ③ EXIST. PAVEMENT (HMA), 5"
- ④ EXIST. PCC CURB & GUTTER, TYPE B6.12
- ⑤ EXIST. PCC CURB & GUTTER, TYPE B6.24
- ⑥ EXIST. PCC OR CORRUGATED MEDIAN
- ⑦ EXIST. PCC SIDEWALK
- ⑧ EXIST. DECORATIVE HMA PARKWAY
- Ⓐ PROP. LANDSCAPING
PROP. TOPSOIL, FURNISH & PLACE, 4"
PROP. SODDING, SALT TOLERANT
- Ⓑ PROP. POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"
- Ⓒ PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4"
- Ⓓ PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1"
- Ⓔ PROP. PCC BASE COURSE, 8 1/4"
- Ⓕ PROP. HMA BASE COURSE, 8 1/4"
- Ⓖ PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"
- Ⓗ PROP. PCC CURB & GUTTER, TYPE B6.12
- Ⓘ PROP. PCC CURB & GUTTER, TYPE B6.24
- Ⓢ PROP. PCC SIDEWALK, 5"
- Ⓚ PROP. AGGR. BASE COURSE, TY B, 4"
- Ⓛ PROP. PCC SIDEWALK, 5" (MULTI-USE PATH)
- Ⓜ PROP. STAMPED COLORED PCC SIDEWALK, 5"

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLY HMA BINDER COURSE IL-4.75, N50.



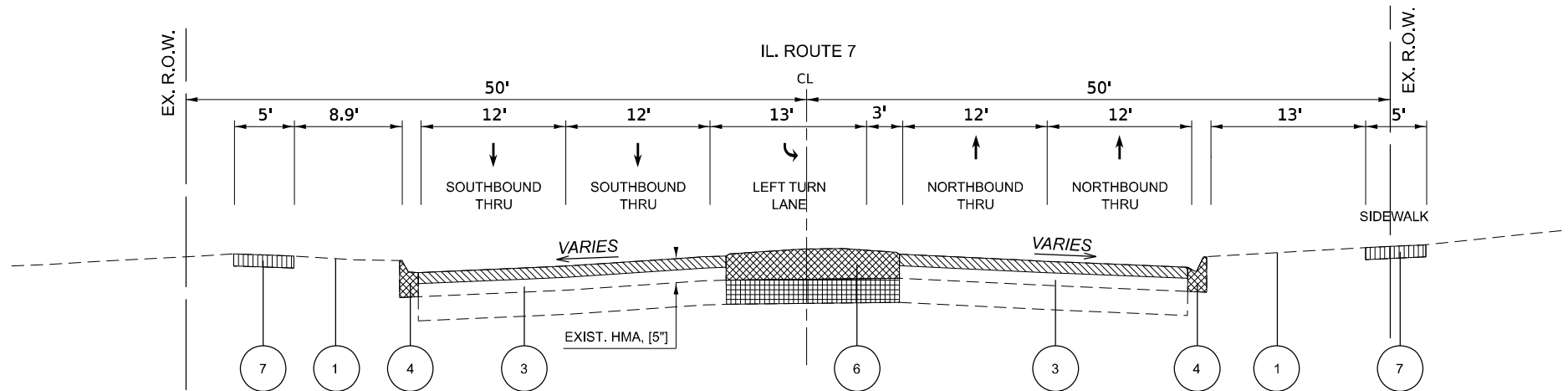
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	CHECKED -	REVISED -
PLOT DATE = 11/13/2025	DATE = 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: NTS	SHEET 3	OF 4 SHEETS	STA. N/A TO STA. N/A

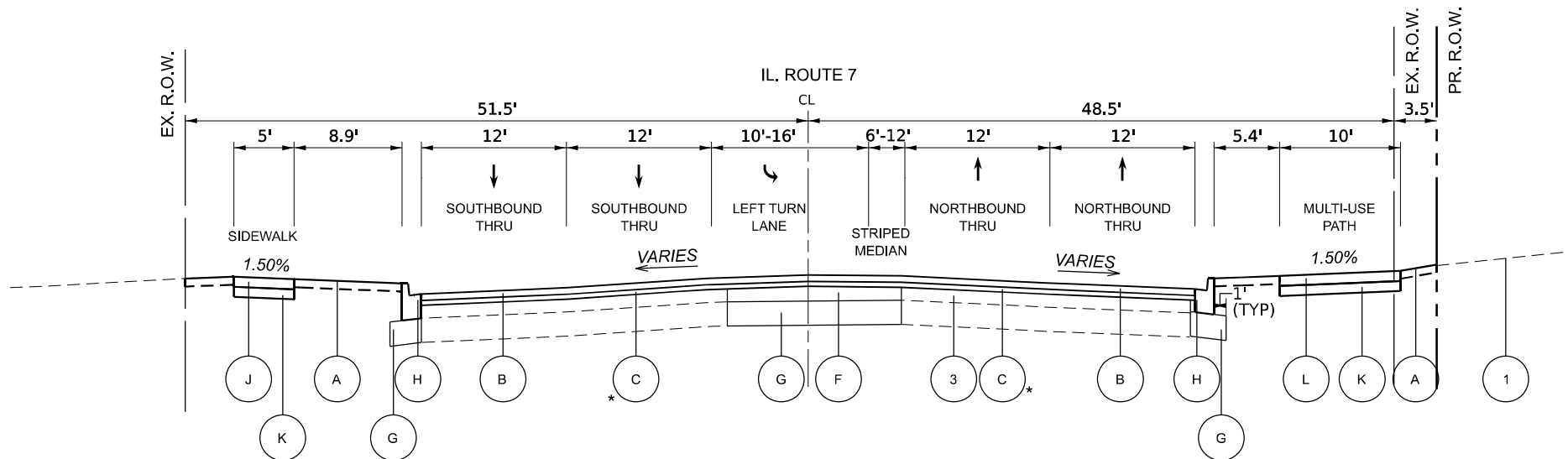
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	20
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Plan-2 - Plan 1 (Sheet)
FILE NAME: W:\191-200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-typical.dgn



- HMA SURFACE REMOVAL/PCC SURFACE REMOVAL (SEE PLANS FOR DEPTHS)
- COMBINATION CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL/ BIKE PATH REMOVAL
- EARTH EXCAVATION

EXISTING TYPICAL SECTION
RESURFACING LIMITS: STA. 20+00.00 TO STA. 25+09.00
IL ROUTE 7 (SOUTHWEST HIGHWAY) (NORTHEAST LEG)



PROPOSED TYPICAL SECTION
RESURFACING LIMITS: STA. 20+00.00 TO STA. 25+09.00
IL ROUTE 7 (SOUTHWEST HIGHWAY) (NORTHEAST LEG)

* PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1"
FROM STA. 18+40.36 TO STA. 20+00.00

NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLY HMA BINDER COURSE IL-4.75, N50.

LEGEND:

- | | | |
|--|--|---|
| 1 EXIST. LANDSCAPE | A PROP. LANDSCAPING | G PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| 2 EXIST. PAVEMENT (PCC), 10" | PROP. TOPSOIL, FURNISH & PLACE, 4" | H PROP. PCC CURB & GUTTER, TYPE B6.12 |
| 3 EXIST. PAVEMENT (HMA), 5" | PROP. SODDING, SALT TOLERANT | I PROP. PCC CURB & GUTTER, TYPE B6.24 |
| 4 EXIST. PCC CURB & GUTTER, TYPE B6.12 | B PROP. POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4" | J PROP. PCC SIDEWALK, 5" |
| 5 EXIST. PCC CURB & GUTTER, TYPE B6.24 | C PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4" | K PROP. AGGR. BASE COURSE, TY B, 4" |
| 6 EXIST. PCC OR CORRUGATED MEDIAN | D PROP. POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 1" | L PROP. PCC SIDEWALK, 5" (MULTI-USE PATH) |
| 7 EXIST. PCC SIDEWALK | E PROP. PCC BASE COURSE, 8 1/4" | M PROP. STAMPED COLORED PCC SIDEWALK, 5" |
| 8 EXIST. DECORATIVE HMA PARKWAY | F PROP. HMA BASE COURSE, 8 1/4" | |



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/13/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: NTS	SHEET 4	OF 4 SHEETS	STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	21
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 1
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-Schedules.dgn

THERMOPLASTIC PAVEMENT MARKINGS									MODIFIED URETHANE PAVEMENT MARKINGS
STATION	STATION	LETTER & SYMBO LS (SQ	4 INCH LINE (FOOT)	6 INCH LINE (FOOT)	8 INCH LINE (FOOT)	12 INCH LINE (FOOT)	24 INCH LINE (FOOT)	4 INCH LINES (FOOT)	12 INCH LINE (FOOT)
IL ROUTE 43 (HARLEM AVENUE)									
43+66.00	47+00.00		1219	222		8	47		
47+00.00	53+00.00	218	2289	1907	49	563	258		
53+00.00	55+30.84	37	1170	153		152		60	111
IL ROUTE 7 (SOUTHWEST HIGHWAY)									
14+21.50	18+50.00		1507	339		339			
21+50.00	26+95.00	36	1492	393		393			
TOTAL		291	7677	3014	49	1455	305	60	111

LOCATION STATION-STATION	AGGREGATE SUBGRADE IMPROVEMENTS (CU YD)	POLY HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80 (TON)	HMA SURFACE COURSE, IL- 9.5, MIX "D", N50 (TON)	POLY HMA BINDER COURSE, IL-4.75, N50 (TON)	PCC BASE COURSE, 8 1/4 INCH (SQ YD)	PCC BASE COURSE WIDENING, 8 1/4 INCH (SQ YD)	HMA BASE COURSE, 8 1/4 INCH (SQ YD)	AGGREGATE BASE COURSE TYPE B, 4 INCH (SQ YD)	BITUMINOUS MATERIALS (PRIME COAT) (POUND)	BITUMINOUS MATERIALS (TACK COAT) (POUND)	AGGREGATE SUBGRADE IMPROVEMENT 12" (SQ YD)
IL ROUTE 43 (HARLEM AVENUE)											
43+66.00	47+00.00	40	268	10	115			179	0	1848	151
47+00.00	53+00.00	40	673	23	313	44	155	1227	449	4322	715
53+00.00	55+30.84	40	165	0	62	284		383	811	845	541
IL ROUTE 7 (SOUTHWEST HIGHWAY)											
14+21.50	18+50.00	40	297	0	127		5	100	169	2043	267
21+50.00	26+95.00	40	319	0	133		71	811	1238	2104	918
TOTAL		200	1722	33	750	328	231	627	2700	11162	2592

STAGE	STATION	STATION	LETTER & SYMBOLS TYPE IV TAPE (SQ FT)	4 INCH LINE TYPE IV TAPE (FOOT)	6 INCH LINE TYPE IV TAPE (FOOT)	24 INCH LINE TYPE IV TAPE (FOOT)	PAVEMENT MARKING REMOVAL WATER BLASTING (SQ FT)	SHORT TERM PAVEMENT MARKING (FOOT)	SHORT TERM PAVEMENT MARKING REMOVAL (SQ FT)
STAGE 1	IL ROUTE 43 (HARLEM AVENUE)								
	43+66.00	52+00.00	36	1753	134				687
	52+00.00	55+31.00		569					190
	IL ROUTE 7 (SOUTHWEST HIGHWAY)								
	14+21.50	18+00.00		574					191
	22+00.00	26+95.00		842					281
STAGE 1A	IL ROUTE 43 (HARLEM AVENUE)								
	43+66.00	52+00.00		395					132
	52+00.00	55+31.00		71					24
	IL ROUTE 7 (SOUTHWEST HIGHWAY)								
	14+21.50	18+00.00		65					22
	22+00.00	26+95.00		22					7
STAGE 2	IL ROUTE 43 (HARLEM AVENUE)								
	43+66.00	52+00.00	73	2098	331	77			1092
	52+00.00	55+31.00		641	64				246
	IL ROUTE 7 (SOUTHWEST HIGHWAY)								
	14+21.50	18+00.00		699					233
	22+00.00	26+95.00		568					189
STAGE 2A	IL ROUTE 43 (HARLEM AVENUE)								
	43+66.00	52+00.00		404					135
	52+00.00	55+31.00							
	IL ROUTE 7 (SOUTHWEST HIGHWAY)								
	14+21.50	18+00.00		15					5
	22+00.00	26+95.00		99					33
STAGE 3	IL ROUTE 43 (HARLEM AVENUE)								
	43+66.00	52+00.00	291	1553	621	182	4334	7507	1483
	52+00.00	55+31.00		848	46		503	1360	306
	1360								
	14+21.50	18+00.00		415	171		672	1847	224
	22+00.00	26+95.00		596	176		992	2707	287
TOTAL			400	12227	1543	259	6501	13421	5765

TEMPORARY ACCESS (COMMERCIAL ENTRANCE)					
	STATION	LOCATION	STAGE 1	STAGE 2	QUANTITY EACH
	48+25	RT	1		1
	52+75	RT	1		1
	54+00	RT	1		1
	54+50	RT	1		1
	15+25	RT	1		1
	17+00	RT	1		1
	18+25	RT	1		1
	18+50	RT	1		1
	19+00	RT	1		1
	22+00	RT	1		1
	23+50	RT	1		1
	24+00	RT	1		1
	26+00	RT	1		1
	47+25	LT		1	1
	48+25	LT		1	1
	54+00	LT		1	1
	16+50	LT		1	1
	18+25	LT		1	1
	23+00	LT		1	1
SUBTOTAL:					19

INLET FILTERS		
STATION	OFFSET (LT/RT)	QUANTITY (EACH)
IL ROUTE 43 (HARLEM AVENUE)		
43+43.42	34.76' RT	1
45+38.65	33.89' LT	1
45+59.97	38.37' LT	1
45+97.21	61.99' RT	1
46+43.20	63.18' RT	1
46+75.40	33.47' LT	1
47+00.86	40.71' RT	1
47+88.66	33.47' LT	1
49+00.00	44.12' RT	1
50+76.75	28.79' RT	1
51+35.70	41.16' LT	1
51+35.77	42.94' LT	1
51+85.61	33.95' RT	1
53+13.98	38.93' LT	1
53+13.99	37.31' LT	1
53+14.09	34.05' RT	1
54+38.03	1.10' LT	1
54+40.05	34.05' RT	1
54+61.97	33.88' LT	1
54+74.07	33.89' RT	1
IL ROUTE 7 (SOUTHWEST HIGHWAY)		
13+39.47	30.57' RT	1
14+20.24	30.43' LT	1
15+12.83	32.62' RT	1
15+90.80	32.56' LT	1
16+83.81	32.62' RT	1
17+52.42	32.79' LT	1
18+40.21	32.59' RT	1
18+78.95	41.75' RT	1
18+83.96	34.05' LT	1
20+86.15	33.48' RT	1
21+57.61	34.22' LT	1
22+56.62	32.17' RT	1
22+89.02	33.07' LT	1
24+24.29	32.51' RT	1
24+24.40	33.07' LT	1
25+84.20	29.89' RT	1
25+86.33	31.14' LT	1
TOTAL		37

LOCATION STATION-STATION	PCC SIDEWALK, 5 INCH (SQ FT)	PCC SIDEWALK, 8 INCH (SQ FT)	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH (SQ YD)	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10 INCH (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
IL ROUTE 43 (HARLEM AVENUE)					
43+66.00	47+00.00	494	1357		179
47+00.00	53+00.00	5291	2738	186	1227
53+00.00	55+30.84	1536	936	96	383
IL ROUTE 7 (SOUTHWEST HIGHWAY)					
14+21.50	18+50.00			99	100
21+50.00	26+95.00	4012	970	114	106
TOTAL		11333	6001	396	2700

LOCATION STATION-STATION	PERIMETER EROSION BARRIER (FOOT)	TEMPORARY EROSION CONTROL BLANKET (SQ YD)	TEMPORARY DITCH CHECK (FOOT)
IL ROUTE 43 (HARLEM AVENUE)			
43+66.00	47+00.00	152	
47+00.00	53+00.00	795	14
53+00.00	55+30.84	28	266
IL ROUTE 7 (SOUTHWEST HIGHWAY)			
14+21.50	18+50.00	756	14
21+50.00	26+95.00	812	56
TOTAL		28	2781

LOCATION STATION-STATION	SODDING, SALT TOLERANT (SQ YD)	TOPSOIL FURNISH AND PLACE, 4" (SQ YD)	TOPSOIL FURNISH AND PLACE, 8" (SQ YD)
IL ROUTE 43 (HARLEM AVENUE)			
43+66.00	47+00.00	152	152
47+00.00	53+00.00	795	795
53+00.00	55+30.84	266	266
IL ROUTE 7 (SOUTHWEST HIGHWAY)			
14+21.50	18+50.00	756	756
21+50.00	26+95.00	812	812
TOTAL		2781	2781

LOCATION STATION-STATION	FRAMES AND LIDS TO BE ADJUSTED SPECIAL (EACH)	FRAMES AND LIDS TO BE ADJUSTED (EACH)
IL ROUTE 43 (HARLEM AVENUE)		
43+66.00	47+00.00	5
47+00.00	53+00.00	6
53+00.00	55+30.84	2
IL ROUTE 7 (SOUTHWEST HIGHWAY)		
14+21.50	18+50.00	0
21+50.00	26+95.00	0
TOTAL		13

LOCATION STATION-STATION	COMBINATION CURB AND GUTTER REMOVAL (FOOT)	HMA SURFACE REMOVAL, 1 1/2 INCH (SQ YD)	HMA SURFACE REMOVAL, 2 1/2 INCH (SQ YD)	PAVEMENT REMOVAL (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	SIDEWALK REMOVAL (SQ FT)	MEDIAN REMOVAL (SQ FT)	DECORATIVE ASPHALT PARKWAY REMOVAL (SQ YD)
IL ROUTE 43 (HARLEM AVENUE)								
43+66.00	47+00.00	394	2662			1605		171
47+00.00	53+00.00	1979	2369	121	478	6664	1864	105
53+00.00	55+30.84	824	201	11	103	2203		
IL ROUTE 7 (SOUTHWEST HIGHWAY)								
14+21.50	18+50.00	864	2943		110	77	40	
21+50.00	26+95.00	944	2098		198	3024	5290	
TOTAL		5005	201	10302	132	889	13573	276

VALVE VAULTS TO BE ADJUSTED (EACH)		
45+33.94	42.06' LT	1
46+89.95	39.06' LT	1
48+64.85	39.14' LT	1
53+71.50	47.25' RT	1
21+61.70	45.78' RT	1
21+74.30	44.01' RT	1
22+78.21	40.31' LT	1
24+18.74	44.42' LT	1
25+66.31	49.52' RT	1
TOTAL		



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/13/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: NTS	SHEET 1	OF 8 SHEETS	STA. N/A TO STA. N/A

F.A.U. RTE. 3578	SECTION 2023-925-N-TS(L)	COUNTY COOK	TOTAL SHEETS 176	SHEET NO. 22
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

MODEL: 2
FILE NAME: W\191+200 IDOT_208010_IT08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-Schedules.dgn

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITIES - STAGE 1					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0.00	28.59	0.00	0.00
43+50	0.0	0.00	50.00	0.00	0.00
44+00	0.0	0.00	50.00	0.00	0.00
44+50	0.0	0.00	50.00	0.00	0.00
45+00	0.0	0.00	50.00	0.00	0.00
45+50	0.0	0.00	50.00	0.00	0.00
46+00	0.0	0.00	50.00	0.00	0.00
46+50	0.0	12.44	50.00	622.08	23.04
47+00	24.9	25.20	33.23	837.50	31.02
47+33	25.5	24.87	16.77	417.12	15.45
47+50	24.2	23.26	50.00	1163.15	43.08
48+00	22.3	26.05	20.25	527.50	19.54
48+20	29.8	33.83	29.75	1006.47	37.28
48+50	37.9	33.30	50.00	1665.22	61.67
49+00	28.7	17.82	50.00	890.90	33.00
49+50	6.9	10.07	50.00	503.31	18.64
50+00	13.2	6.62	50.00	330.98	12.26
50+50	0.0	0.00	50.00	0.00	0.00
51+00	0.0	4.69	50.00	234.58	8.69
51+50	9.4	8.71	50.00	435.65	16.14
52+00	8.0	14.33	50.00	716.40	26.53
52+50	20.6	18.74	29.18	546.97	20.26
52+79	16.9	18.00	20.82	374.77	13.88
53+00	19.1	19.14	50.00	957.20	35.45
53+50	19.2	23.27	50.00	1163.68	43.10
54+00	27.4	23.24	50.00	1161.99	43.04
54+50	19.1	15.58	50.00	778.93	28.85
55+00	12.1	6.03	50.00	301.55	11.17
55+50	0.0				
TOTAL				542.07	

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITIES - STAGE 1					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0	28.59	0.00	0.00
43+50	0.0	0	50.00	0.00	0.00
44+00	0.0	0	50.00	0.00	0.00
44+50	0.0	0	50.00	0.00	0.00
45+00	0.0	0	50.00	0.00	0.00
45+50	0.0	0	50.00	0.00	0.00
46+00	0.0	0	50.00	0.00	0.00
46+50	0.0	0	50.00	0.00	0.00
47+00	0.0	0	33.23	0.00	0.00
47+33	0.0	0	16.77	0.00	0.00
47+50	0.0	0	50.00	0.00	0.00
48+00	0.0	0	20.25	0.00	0.00
48+20	0.0	0	29.75	0.00	0.00
48+50	0.0	0	50.00	0.00	0.00
49+00	0.0	3.59	50.00	179.50	6.65
49+50	7.2	7.62	50.00	381.00	14.11
50+00	8.1	4.03	50.00	201.50	7.46
50+50	0.0	0	50.00	0.00	0.00
51+00	0.0	0	50.00	0.00	0.00
51+50	0.0	0	50.00	0.00	0.00
52+00	0.0	0.1	50.00	5.00	0.19
52+50	0.2	0.1	29.18	2.92	0.11
52+79	0.0	0	20.82	0.00	0.00
53+00	0.0	0	50.00	0.00	0.00
53+50	0.0	0	50.00	0.00	0.00
54+00	0.0	0	50.00	0.00	0.00
54+50	0.0	0	50.00	0.00	0.00
55+00	0.0	0	50.00	0.00	0.00
55+50	0.0				
TOTAL				28.52	

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITIES - STAGE 1					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0.00	28.59	0.00	0.00
43+50	0.0	0.00	50.00	0.00	0.00
44+00	0.0	0.00	50.00	0.00	0.00
44+50	0.0	0.00	50.00	0.00	0.00
45+00	0.0	0.00	50.00	0.00	0.00
45+50	0.0	0.00	50.00	0.00	0.00
46+00	0.0	2.35	50.00	117.74	4.36
46+50	4.7	4.92	50.00	245.89	9.11
47+00	5.1	4.83	33.23	160.65	5.95
47+33	4.5	4.39	16.77	73.69	2.73
47+50	4.2	3.95	50.00	197.64	7.32
48+00	3.7	1.83	20.25	37.05	1.37
48+20	0.0	3.75	29.75	111.47	4.13
48+50	7.5	7.90	50.00	395.24	14.64
49+00	8.3	7.20	50.00	360.05	13.34
49+50	6.1	3.04	50.00	152.15	5.64
50+00	0.0	0.00	50.00	0.00	0.00
50+50	0.0	0.00	50.00	0.00	0.00
51+00	0.0	2.18	50.00	109.16	4.04
51+50	4.4	4.46	50.00	223.14	8.26
52+00	4.6	4.58	50.00	228.81	8.47
52+50	4.6	2.30	29.18	67.01	2.48
52+79	0.0	0.00	20.82	0.00	0.00
53+00	0.0	1.92	50.00	96.07	3.56
53+50	3.8	1.92	50.00	96.07	3.56
54+00	0.0	0.00	50.00	0.00	0.00
54+50	0.0	1.67	50.00	83.49	3.09
55+00	3.3	1.67	50.00	83.49	3.09
55+50	0.0				
TOTAL				105.14	



USER NAME	= tnestor
PLOT DATE	= 10/17/2025

DESIGNED	-	TN
DRAWN	-	TN
CHECKED	-	GJE
DATE	-	10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS	SHEET 2	OF 8	SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	23
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

MODEL: 3
FILE NAME: W\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-Schedules.dgn

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITES - STAGE 2					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0.00	28.59	0.00	0.00
43+50	0.0	4.30	50.00	214.97	7.96
44+00	8.6	8.47	50.00	423.50	15.69
44+50	8.3	8.24	50.00	412.10	15.26
45+00	8.1	8.14	50.00	407.15	15.08
45+50	8.1	4.07	50.00	203.57	7.54
46+00	0.0	4.57	50.00	228.57	8.47
46+50	9.1	9.14	50.00	457.14	16.93
47+00	9.1	11.14	33.23	370.28	13.71
47+33	13.1	10.83	16.77	181.62	6.73
47+50	8.5	10.83	50.00	541.50	20.06
48+00	13.1	13.14	20.25	266.14	9.86
48+20	13.1	11.14	29.75	331.50	12.28
48+50	9.1	4.57	50.00	228.57	8.47
49+00	0.0	0.00	50.00	0.00	0.00
49+50	0.0	6.55	50.00	327.49	12.13
50+00	13.1	11.35	50.00	567.73	21.03
50+50	9.6	8.04	50.00	402.15	14.89
51+00	6.5	5.85	50.00	292.57	10.84
51+50	5.2	6.14	50.00	306.97	11.37
52+00	7.1	6.14	50.00	306.97	11.37
52+50	5.2	5.85	29.18	170.75	6.32
52+79	6.5	5.85	20.82	121.83	4.51
53+00	5.2	5.23	50.00	261.33	9.68
53+50	5.2	8.18	50.00	408.82	15.14
54+00	11.1	8.18	50.00	408.82	15.14
54+50	5.2	3.45	50.00	172.33	6.38
55+00	1.7	0.83	50.00	41.66	1.54
55+50	0.0				
TOTAL				298.37	

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITES - STAGE 2					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0.00	28.59	0.00	0.00
43+50	0.0	0.00	50.00	0.00	0.00
44+00	0.0	0.00	50.00	0.00	0.00
44+50	0.0	0.00	50.00	0.00	0.00
45+00	0.0	0.00	50.00	0.00	0.00
45+50	0.0	0.00	50.00	0.00	0.00
46+00	0.0	0.22	50.00	10.83	0.40
46+50	0.4	1.03	50.00	51.68	1.91
47+00	1.6	0.82	33.23	27.15	1.01
47+33	0.0	0.00	16.77	0.00	0.00
47+50	0.0	0.00	50.00	0.00	0.00
48+00	0.0	0.00	20.25	0.00	0.00
48+20	0.0	0.00	29.75	0.00	0.00
48+50		0.00	50.00	0.00	0.00
49+00	0.0	0.00	50.00	0.00	0.00
49+50	0.0	5.61	50.00	280.53	10.39
50+00	11.2	8.91	50.00	445.53	16.50
50+50	6.6	5.11	50.00	255.50	9.46
51+00	3.6	3.34	50.00	166.78	6.18
51+50	3.1	3.42	50.00	170.80	6.33
52+00	3.8	3.37	50.00	168.60	6.24
52+50	3.0	2.23	29.18	65.20	2.41
52+79	1.5	3.78	20.82	78.61	2.91
53+00	6.0	4.87	50.00	243.50	9.02
53+50	3.7	1.85	50.00	92.38	3.42
54+00	0.0	0.00	50.00	0.00	0.00
54+50	0.0	0.00	50.00	0.00	0.00
55+00	0.0	0.00	50.00	0.00	0.00
55+50	0.0				
TOTAL				76.19	

IL ROUTE 43 (HARLEM AVE) EARTHWORK QUANTITES - STAGE 2					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0	0	28.59	0.00	0.00
43+50	0.0	0.76	50.00	37.83	1.40
44+00	1.5	1.48	50.00	73.83	2.73
44+50	1.4	1.41	50.00	70.33	2.60
45+00	1.4	1.37	50.00	68.33	2.53
45+50	1.4	0.68	50.00	34.00	1.26
46+00	0.0	0.00	50.00	0.00	0.00
46+50	0.0	0.00	50.00	0.00	0.00
47+00	0.0	0.00	33.23	0.00	0.00
47+33	0.0	0.00	16.77	0.00	0.00
47+50	0.0	0.00	50.00	0.00	0.00
48+00	0.0	0.00	20.25	0.00	0.00
48+20	0.0	0.00	29.75	0.00	0.00
48+50	0.0	0.00	50.00	0.00	0.00
49+00	0.0	0.00	50.00	0.00	0.00
49+50	0.0	0.00	50.00	0.00	0.00
50+00	0.0	0.00	50.00	0.00	0.00
50+50	0.0	0.00	50.00	0.00	0.00
51+00	0.0	0.00	50.00	0.00	0.00
51+50	0.0	0.00	50.00	0.00	0.00
52+00	0.0	0.00	50.00	0.00	0.00
52+50	0.0	0.00	29.18	0.00	0.00
52+79	0.0	0.00	20.82	0.00	0.00
53+00	0.0	0.00	50.00	0.00	0.00
53+50	0.0	0.00	50.00	0.00	0.00
54+00	0.0	1.43	50.00	71.66	2.65
54+50	2.9	2.56	50.00	127.82	4.73
55+00	2.2	1.12	50.00	56.16	2.08
55+50	0.0				
TOTAL				20.00	



USER NAME	= tnestor
PLOT DATE	= 10/17/2025

DESIGNED	-	TN
DRAWN	-	TN
CHECKED	-	GJE
DATE	-	10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS	SHEET 3	OF 8	SHEETS	STA.	TO STA.
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F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	24
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: 4
FILE NAME: W\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-Schedules.dgn

IL ROUTE 43 (HARLEMAVE) EARTHWORK QUANTITES - STAGE 3					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0				
		0.00	28.59	0.00	0.00
43+50	0.0				
		0.00	50.00	0.00	0.00
44+00	0.0				
		0.00	50.00	0.00	0.00
44+50	0.0				
		0.00	50.00	0.00	0.00
45+00	0.0				
		0.00	50.00	0.00	0.00
45+50	0.0				
		0.00	50.00	0.00	0.00
46+00	0.0				
		0.00	50.00	0.00	0.00
46+50	0.0				
		0.00	50.00	0.00	0.00
47+00	0.0				
		0.00	33.23	0.00	0.00
47+33	0.0				
		0.00	16.77	0.00	0.00
47+50	0.0				
		0.00	50.00	0.00	0.00
48+00	0.0				
		0.00	20.25	0.00	0.00
48+20	0.0				
		0.00	29.75	0.00	0.00
48+50	0.0				
		0.00	50.00	0.00	0.00
49+00	0.0				
		0.00	50.00	0.00	0.00
49+50	0.0				
		0.00	50.00	0.00	0.00
50+00	0.0				
		0.00	50.00	0.00	0.00
50+50	0.0				
		4.05	50.00	202.50	7.50
51+00	8.1				
		8.12	50.00	406.00	15.04
51+50	8.1				
		8.14	50.00	407.00	15.07
52+00	8.1				
		10.21	50.00	510.50	18.91
52+50	12.3				
		14.25	29.18	415.82	15.40
52+79	16.2				
		16.02	20.82	333.54	12.35
53+00	15.8				
		20.02	50.00	1001.00	37.07
53+50	24.2				
		19.92	50.00	995.75	36.88
54+00	15.6				
		23.55	50.00	1177.25	43.60
54+50	31.5				
		23.09	50.00	1154.25	42.75
55+00	14.7				
		7.35	50.00	367.25	13.60
55+50	0.0				
TOTAL				258.18	

IL ROUTE 43 (HARLEMAVE) EARTHWORK QUANTITES - STAGE 3					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0				
		0.00	28.59	0.00	0.00
43+50	0.0				
		0.00	50.00	0.00	0.00
44+00	0.0				
		0.00	50.00	0.00	0.00
44+50	0.0				
		0.00	50.00	0.00	0.00
45+00	0.0				
		0.00	50.00	0.00	0.00
45+50	0.0				
		0.00	50.00	0.00	0.00
46+00	0.0				
		0.00	50.00	0.00	0.00
46+50	0.0				
		0.00	50.00	0.00	0.00
47+00	0.0				
		0.00	33.23	0.00	0.00
47+33	0.0				
		0.00	16.77	0.00	0.00
47+50	0.0				
		0.00	50.00	0.00	0.00
48+00	0.0				
		0.00	20.25	0.00	0.00
48+20	0.0				
		0.00	29.75	0.00	0.00
48+50	0.0				
		0.00	50.00	0.00	0.00
49+00	0.0				
		0.00	50.00	0.00	0.00
49+50	0.0				
		0.00	50.00	0.00	0.00
50+00	0.0				
		0.00	50.00	0.00	0.00
50+50	0.0				
		0.00	50.00	0.00	0.00
51+00	0.0				
		0.00	50.00	0.00	0.00
51+50	0.0				
		0.00	50.00	0.00	0.00
52+00	0.0				
		0.00	50.00	0.00	0.00
52+50	0.0				
		0.00	29.18	0.00	0.00
52+79	0.0				
		0.00	20.82	0.00	0.00
53+00	0.0				
		0.00	50.00	0.00	0.00
53+50	0.0				
		0.00	50.00	0.00	0.00
54+00	0.0				
		0.00	50.00	0.00	0.00
54+50	0.0				
		0.00	50.00	0.00	0.00
55+00	0.0				
		0.00	50.00	0.00	0.00
55+50	0.0				
TOTAL				0.00	

IL ROUTE 43 (HARLEMAVE) EARTHWORK QUANTITES - STAGE 3					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
43+21	0.0				
		0.00	28.59	0.00	0.00
43+50	0.0				
		0.00	50.00	0.00	0.00
44+00	0.0				
		0.00	50.00	0.00	0.00
44+50	0.0				
		0.00	50.00	0.00	0.00
45+00	0.0				
		0.00	50.00	0.00	0.00
45+50	0.0				
		0.00	50.00	0.00	0.00
46+00	0.0				
		0.00	50.00	0.00	0.00
46+50	0.0				
		0.00	50.00	0.00	0.00
47+00	0.0				
		0.00	33.23	0.00	0.00
47+33	0.0				
		0.00	16.77	0.00	0.00
47+50	0.0				
		0.00	50.00	0.00	0.00
48+00	0.0				
		0.00	20.25	0.00	0.00
48+20	0.0				
		0.00	29.75	0.00	0.00
48+50	0.0				
		0.00	50.00	0.00	0.00
49+00	0.0				
		0.00	50.00	0.00	0.00
49+50	0.0				
		0.00	50.00	0.00	0.00
50+00	0.0				
		0.00	50.00	0.00	0.00
50+50	0.0				
		0.00	50.00	0.00	0.00
51+00	0.0				
		0.00	50.00	0.00	0.00
51+50	0.0				
		0.00	50.00	0.00	0.00
52+00	0.0				
		0.00	50.00	0.00	0.00
52+50	0.0				
		0.00	29.18	0.00	0.00
52+79	0.0				
		0.00	20.82	0.00	0.00
53+00	0.0				
		0.00	50.00	0.00	0.00
53+50	0.0				
		0.00	50.00	0.00	0.00
54+00	0.0				
		0.00	50.00	0.00	0.00
54+50	0.0				
		0.00	50.00	0.00	0.00
55+00	0.0				
		0.00	50.00	0.00	0.00
55+50	0.0				
TOTAL				0.00	



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 4 OF 8 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	25
CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

MODEL: 5
FILE NAME: W\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-Schedules.dgn

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITES - STAGE 1					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		1.28	50.00	64.00	2.37
14+50	2.6				
		2.56	50.00	128.00	4.74
15+00	2.6				
		2.56	50.00	128.00	4.74
15+50	2.6				
		2.56	50.00	128.00	4.74
16+00	2.6				
		2.56	50.00	128.00	4.74
16+50	2.6				
		2.56	50.00	128.00	4.74
17+00	2.6				
		2.56	50.00	128.00	4.74
17+50	2.6				
		2.56	50.00	128.00	4.74
18+00	2.6				
		9.09	18.19	165.40	6.13
18+18	15.6				
		9.09	13.42	122.03	4.52
18+32	2.6				
		3.06	18.39	56.27	2.08
18+50	3.6				
		1.78	18.61	33.13	1.23
18+69	0.0				
		0.00	31.39	0.00	0.00
19+00	0.0				
		0.00	50.00	0.00	0.00
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		3.45	50.00	172.33	6.38
20+50	6.9				
		6.39	50.00	319.65	11.84
21+00	5.9				
		5.89	50.00	294.65	10.91
21+50	5.9				
		5.89	50.00	294.65	10.91
22+00	5.9				
		5.89	50.00	294.65	10.91
22+50	5.9				
		11.90	50.00	595.06	22.04
23+00	17.9				
		11.90	50.00	595.06	22.04
23+50	5.9				
		5.89	50.00	294.65	10.91
24+00	5.9				
		5.89	50.00	294.65	10.91
24+50	5.9				
		5.89	50.00	294.65	10.91
25+00	5.9				
		5.89	50.00	294.65	10.91
25+50	5.9				
		2.95	50.00	147.33	5.46
26+00	0.0				
		0.00	50.00	0.00	0.00
26+50	0.0				
		0.00	50.00	0.00	0.00
27+00	0.0				
TOTAL				193.66	

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITES - STAGE 1					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		0.00	50.00	0.00	0.00
14+50	0.0				
		0.00	50.00	0.00	0.00
15+00	0.0				
		0.00	50.00	0.00	0.00
15+50	0.0				
		0.00	50.00	0.00	0.00
16+00	0.0				
		0.00	50.00	0.00	0.00
16+50	0.0				
		0.00	50.00	0.00	0.00
17+00	0.0				
		0.00	50.00	0.00	0.00
17+50	0.0				
		0.00	50.00	0.00	0.00
18+00	0.0				
		0.00	18.19	0.00	0.00
18+18	0.0				
		0.00	13.42	0.00	0.00
18+32	0.0				
		0.00	18.39	0.00	0.00
18+50	0.0				
		0.00	18.61	0.00	0.00
18+69	0.0				
		0.00	31.39	0.00	0.00
19+00	0.0				
		0.00	50.00	0.00	0.00
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		0.00	50.00	0.00	0.00
20+50	0.0				
		0.00	50.00	0.00	0.00
21+00	0.0				
		0.00	50.00	0.00	0.00
21+50	0.0				
		0.00	50.00	0.00	0.00
22+00	0.0				
		0.00	50.00	0.00	0.00
22+50	0.0				
		0.00	50.00	0.00	0.00
23+00	0.0				
		0.00	50.00	0.00	0.00
23+50	0.0				
		0.00	50.00	0.00	0.00
24+00	0.0				
		0.00	50.00	0.00	0.00
24+50	0.0				
		0.00	50.00	0.00	0.00
25+00	0.0				
		0.00	50.00	0.00	0.00
25+50	0.0				
		0.00	50.00	0.00	0.00
26+00	0.0				
		0.00	50.00	0.00	0.00
26+50	0.0				
		0.00	50.00	0.00	0.00
27+00	0.0				
TOTAL				0.00	

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITES - STAGE 1					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		2.94	50.00	146.82	5.44
14+50	5.9				
		5.65	50.00	282.72	10.47
15+00	5.4				
		5.36	50.00	268.06	9.93
15+50	5.3				
		5.32	50.00	266.06	9.85
16+00	5.4				
		5.30	50.00	264.81	9.81
16+50	5.2				
		4.89	50.00	244.64	9.06
17+00	4.5				
		4.48	50.00	224.23	8.30
17+50	4.4				
		4.25	50.00	212.73	7.88
18+00	4.1				
		2.04	18.19	37.19	1.38
18+18	0.0				
		1.52	13.42	20.37	0.75
18+32	3.0				
		2.30	18.39	42.32	1.57
18+50	1.6				
		1.57	18.61	29.21	1.08
18+69	1.6				
		0.79	31.39	24.68	0.91
19+00	0.0				
		0.00	50.00	0.00	0.00
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		2.45	50.00	122.67	4.54
20+50	4.9				
		5.09	50.00	254.74	9.43
21+00	5.3				
		4.98	50.00	248.98	9.22
21+50	4.7				
		5.99	50.00	299.72	11.10
22+00	7.3				
		7.33	50.00	366.38	13.57
22+50	7.3				
		3.67	50.00	183.56	6.80
23+00	0.0				
		2.45	50.00	122.40	4.53
23+50	4.9				
		4.95	50.00	247.73	9.18
24+00	5.0				
		5.08	50.00	254.14	9.41
24+50	5.2				
		5.23	50.00	261.72	9.69
25+00	5.3				
		5.43	50.00	271.39	10.05
25+50	5.5				
		2.77	50.00	138.49	5.13
26+00	0.0				
		0.00	50.00	0.00	0.00
26+50	0.0				
		0.00	50.00	0.00	0.00
27+00	0.0				
TOTAL				179.10	



BLA, Inc.

USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 5 OF 8 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	26
CONTRACT NO. 62W47				
		ILLINOIS	FED. AID PROJECT	

MODEL: 6
FILE NAME: W\191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-Schedules.dgn

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITES - STAGE 2					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0		50.00	0.00	0.00
14+50	0.0	1.28	50.00	64.00	2.37
15+00	2.6	2.56	50.00	128.00	4.74
15+50	2.6	2.56	50.00	128.00	4.74
16+00	2.6	2.56	50.00	128.00	4.74
16+50	2.6	2.56	50.00	128.00	4.74
17+00	2.6	2.56	50.00	128.00	4.74
17+50	2.6	2.56	50.00	128.00	4.74
18+00	2.6	9.70	18.19	176.39	6.53
18+18	16.8	16.84	13.42	226.03	8.37
18+32	16.9	15.08	18.39	277.27	10.27
18+50	13.3	15.09	18.61	280.89	10.40
18+69	16.9	16.91	31.39	530.79	19.66
19+00	16.9	12.14	50.00	606.94	22.48
19+50	7.3	3.67	50.00	183.58	6.80
20+00	0.0	0.00	50.00	0.00	0.00
20+50	0.0	0.00	50.00	0.00	0.00
21+00	0.0	2.61	50.00	130.66	4.84
21+50	5.2	9.57	50.00	478.40	17.72
22+00	13.9	9.07	50.00	453.40	16.79
22+50	4.2	4.23	50.00	211.33	7.83
23+00	4.2	9.89	50.00	494.40	18.31
23+50	15.5	15.27	50.00	763.56	28.28
24+00	15.0	9.61	50.00	480.48	17.80
24+50	4.2	3.39	50.00	169.66	6.28
25+00	2.6	2.56	50.00	128.00	4.74
25+50	2.6	1.28	50.00	64.00	2.37
26+00	0.0	0.00	50.00	0.00	0.00
26+50	0.0	0.00	50.00	0.00	0.00
27+00	0.0			TOTAL	240.29

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITES - STAGE 2					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0	0.00	50.00	0.00	0.00
14+50	0.0	0.00	50.00	0.00	0.00
15+00	0.0	0.00	50.00	0.00	0.00
15+50	0.0	0.00	50.00	0.00	0.00
16+00	0.0	0.00	50.00	0.00	0.00
16+50	0.0	0.00	50.00	0.00	0.00
17+00	0.0	0.00	50.00	0.00	0.00
17+50	0.0	0.00	50.00	0.00	0.00
18+00	0.0	0.00	18.19	0.00	0.00
18+18	0.0	0.00	13.42	0.00	0.00
18+32	0.0	0.00	18.39	0.00	0.00
18+50	0.0	0.00	18.61	0.00	0.00
18+69	0.0	0.00	31.39	0.00	0.00
19+00	0.0	0.00	50.00	0.00	0.00
19+50	0.0	0.00	50.00	0.00	0.00
20+00	0.0	0.00	50.00	0.00	0.00
20+50	0.0	0.00	50.00	0.00	0.00
21+00	0.0	0.00	50.00	0.00	0.00
21+50	0.0	0.00	50.00	0.00	0.00
22+00	0.0	0.00	50.00	0.00	0.00
22+50	0.0	0.00	50.00	0.00	0.00
23+00	0.0	0.00	50.00	0.00	0.00
23+50	0.0	0.00	50.00	0.00	0.00
24+00	0.0	0.00	50.00	0.00	0.00
24+50	0.0	0.00	50.00	0.00	0.00
25+00	0.0	0.00	50.00	0.00	0.00
25+50	0.0	0.00	50.00	0.00	0.00
26+00	0.0	0.00	50.00	0.00	0.00
26+50	0.0	0.00	50.00	0.00	0.00
27+00	0.0			TOTAL	0.00

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITIES - STAGE 2					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0	0.65	50.00	32.75	1.21
14+50	1.3	1.34	50.00	66.99	2.48
15+00	1.4	0.68	50.00	34.25	1.27
15+50	0.0	0.64	50.00	32.25	1.19
16+00	1.3	1.28	50.00	64.24	2.38
16+50	1.3	0.64	50.00	32.00	1.19
17+00	0.0	0.62	50.00	30.91	1.14
17+50	1.2	1.21	50.00	60.33	2.23
18+00	1.2	0.59	18.19	10.70	0.40
18+18	0.0	0.00	13.42	0.00	0.00
18+32	0.0	0.00	18.39	0.00	0.00
18+50	0.0	0.00	18.61	0.00	0.00
18+69	0.0	0.00	31.39	0.00	0.00
19+00	0.0	0.00	50.00	0.00	0.00
19+50	0.0	0.00	50.00	0.00	0.00
20+00	0.0	0.00	50.00	0.00	0.00
20+50	0.0	0.00	50.00	0.00	0.00
21+00	0.0	1.56	50.00	78.08	2.89
21+50	3.1	1.56	50.00	78.08	2.89
22+00	0.0	1.56	50.00	77.91	2.89
22+50	3.1	3.33	50.00	166.48	6.17
23+00	3.5	1.77	50.00	88.57	3.28
23+50	0.0	0.00	50.00	0.00	0.00
24+00	0.0	1.68	50.00	84.16	3.12
24+50	3.4	2.32	50.00	116.16	4.30
25+00	1.3	1.28	50.00	64.24	2.38
25+50	1.3	0.64	50.00	32.25	1.19
26+00	0.0	0.00	50.00	0.00	0.00
26+50	0.0	0.00	50.00	0.00	0.00
27+00	0.0			TOTAL	42.61



USER NAME	= tnestor
PLOT DATE	= 10/17/2025

DESIGNED	-	TN
DRAWN	-	TN
CHECKED	-	GJE
DATE	-	10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS	SHEET 6	OF 8	SHEETS	STA. N/A	TO STA. N/A
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	27
CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

MODEL: 7
FILE NAME: W\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sht-Schedules.dgn



USER NAME	= tnestor
PLOT DATE	= 10/17/2025

DESIGNED	-	TN
DRAWN	-	TN
CHECKED	-	GJE
DATE	-	10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES				
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)				
SCALE: NTS	SHEET 7	OF 8	SHEETS	STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	28
CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITIES - STAGE 3					
CUT					
STATION	CUT (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		0.00	50.00	0.00	0.00
14+50	0.0				
		0.00	50.00	0.00	0.00
15+00	0.0				
		0.00	50.00	0.00	0.00
15+50	0.0				
		0.00	50.00	0.00	0.00
16+00	0.0				
		0.00	50.00	0.00	0.00
16+50	0.0				
		0.00	50.00	0.00	0.00
17+00	0.0				
		0.00	50.00	0.00	0.00
17+50	0.0				
		0.00	50.00	0.00	0.00
18+00	0.0				
		0.00	18.19	0.00	0.00
18+18	0.0				
		0.00	13.42	0.00	0.00
18+32	0.0				
		4.18	18.39	76.87	2.85
18+50	8.4				
		8.36	18.61	155.58	5.76
18+69	8.4				
		8.36	31.39	262.42	9.72
19+00	8.4				
		4.18	50.00	209.00	7.74
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		0.00	50.00	0.00	0.00
20+50	0.0				
		4.28	50.00	214.00	7.93
21+00	8.6				
		8.69	50.00	434.50	16.09
21+50	8.8				
		8.64	50.00	432.00	16.00
22+00	8.5				
		8.47	50.00	423.50	15.69
22+50	8.5				
		10.66	50.00	533.00	19.74
23+00	12.8				
		16.00	50.00	800.00	29.63
23+50	19.2				
		22.47	50.00	1123.50	41.61
24+00	25.8				
		28.72	50.00	1436.00	53.19
24+50	31.7				
		31.48	50.00	1574.00	58.30
25+00	31.3				
		29.56	50.00	1478.00	54.74
25+50	27.8				
		25.48	50.00	1274.00	47.19
26+00	23.1				
		20.15	50.00	1007.50	37.31
26+50	17.2				
		8.58	50.00	429.00	15.89
27+00	0.0				
TOTAL				439.37	

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITIES - STAGE 3					
FILL					
STATION	FILL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		0.00	50.00	0.00	0.00
14+50	0.0				
		0.00	50.00	0.00	0.00
15+00	0.0				
		0.00	50.00	0.00	0.00
15+50	0.0				
		0.00	50.00	0.00	0.00
16+00	0.0				
		0.00	50.00	0.00	0.00
16+50	0.0				
		0.00	50.00	0.00	0.00
17+00	0.0				
		0.00	50.00	0.00	0.00
17+50	0.0				
		0.00	50.00	0.00	0.00
18+00	0.0				
		0.00	18.19	0.00	0.00
18+18	0.0				
		0.00	13.42	0.00	0.00
18+32	0.0				
		0.00	18.39	0.00	0.00
18+50	0.0				
		0.00	18.61	0.00	0.00
18+69	0.0				
		0.00	31.39	0.00	0.00
19+00	0.0				
		0.00	50.00	0.00	0.00
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		0.00	50.00	0.00	0.00
20+50	0.0				
		0.00	50.00	0.00	0.00
21+00	0.0				
		0.00	50.00	0.00	0.00
21+50	0.0				
		0.00	50.00	0.00	0.00
22+00	0.0				
		0.00	50.00	0.00	0.00
22+50	0.0				
		0.00	50.00	0.00	0.00
23+00	0.0				
		0.00	50.00	0.00	0.00
23+50	0.0				
		0.00	50.00	0.00	0.00
24+00	0.0				
		0.00	50.00	0.00	0.00
24+50	0.0				
		0.00	50.00	0.00	0.00
25+00	0.0				
		0.00	50.00	0.00	0.00
25+50	0.0				
		0.00	50.00	0.00	0.00
26+00	0.0				
		0.00	50.00	0.00	0.00
26+50	0.0				
		0.00	50.00	0.00	0.00
27+00	0.0				
TOTAL				0.00	

IL ROUTE 7 (SOUTHWEST HIGHWAY) EARTHWORK QUANTITIES - STAGE 3					
TOPSOIL STRIP					
STATION	TOPSOIL (SF)	AVERAGE	LENGTH	TOTAL	TOTAL (CY)
14+00	0.0				
		0.00	50.00	0.00	0.00
14+50	0.0				
		0.00	50.00	0.00	0.00
15+00	0.0				
		0.00	50.00	0.00	0.00
15+50	0.0				
		0.00	50.00	0.00	0.00
16+00	0.0				
		0.00	50.00	0.00	0.00
16+50	0.0				
		0.00	50.00	0.00	0.00
17+00	0.0				
		0.00	50.00	0.00	0.00
17+50	0.0				
		0.00	50.00	0.00	0.00
18+00	0.0				
		0.00	18.19	0.00	0.00
18+18	0.0				
		0.00	13.42	0.00	0.00
18+32	0.0				
		0.00	18.39	0.00	0.00
18+50	0.0				
		0.00	18.61	0.00	0.00
18+69	0.0				
		0.00	31.39	0.00	0.00
19+00	0.0				
		0.00	50.00	0.00	0.00
19+50	0.0				
		0.00	50.00	0.00	0.00
20+00	0.0				
		0.00	50.00	0.00	0.00
20+50	0.0				
		0.00	50.00	0.00	0.00
21+00	0.0				
		0.00	50.00	0.00	0.00
21+50	0.0				
		0.00	50.00	0.00	0.00
22+00	0.0				
		0.00	50.00	0.00	0.00
22+50	0.0				
		0.00	50.00	0.00	0.00
23+00	0.0				
		0.00	50.00	0.00	0.00
23+50	0.0				
		0.00	50.00	0.00	0.00
24+00	0.0				
		0.00	50.00	0.00	0.00
24+50	0.0				
		0.00	50.00	0.00	0.00
25+00	0.0				
		0.00	50.00	0.00	0.00
25+50	0.0				
		0.00	50.00	0.00	0.00
26+00	0.0				
		0.00	50.00	0.00	0.00
26+50	0.0				
		0.00	50.00	0.00	0.00
27+00	0.0				
TOTAL				0.00	

MODEL: 8
FILE NAME: W:\19+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sht-Schedules.dgn

EARTHWORK QUANTITIES SUMMARY TABLE																				
LOCATION	EARTH EXCAVATION (CU YD)			ADJUSTED EARTH EXCAVATION (15%) (CU YD)			EMBANKMENT (CU YD)			BALANCE WASTE (+) OR SHORTAGE (-)			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS (CU YD)			TOPSOIL EXCAVATION & PLACEMENT (CU YD)			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS (CU YD)	
													UNDERCUT (CU YD) (NOMINAL QTY)			TOPSOIL STRIP (CU YD)			TOPSOIL RESPREAD (CU YD)	BALANCE OF TOPSOIL (CU YD)
	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	ALL STAGES	ALL STAGES
IL ROUTE 43 (HARLEM AVE)	542	298	258	461	254	219	29	76	0	432	177	219	600			105	20	0	489	-177
IL ROUTE 7 (SOUTHWEST HIGHWAY)	194	240	439	165	204	373	0	0	0	165	204	373				179	43	0		
PROJECT TOTAL	1,972			1,676			105			1,571			600			347			489	-177

10% is assumed unusable

NOTES:

CONTRACTOR SHALL STRIP AND STOCKPILE TOPSOIL UNDER PAY ITEM, TOPSOIL EXCAVATION AND PLACEMENT

CONTRACTOR TO EXCAVATE ANY NECESSARY UNDERCUT AREAS UNDER PAY ITEM REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS.

DIFFERENCE BETWEEN TOPSOIL RESPREAD (PAID AS TOPSOIL EXCAVATION AND PLACEMENT) AND REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS TO BE HAULED OFFSITE AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS.



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 8 OF 8 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	29
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

BENCHMARK #01

ELEV. = 598.16
CHISELED SQUARE IN THE
N.E. CORNER OF CONC OF
TRAFFIC CONT BOX BASE IN
THE S.E. CORNER OF
IL 43 & IL 7

BENCHMARK #02

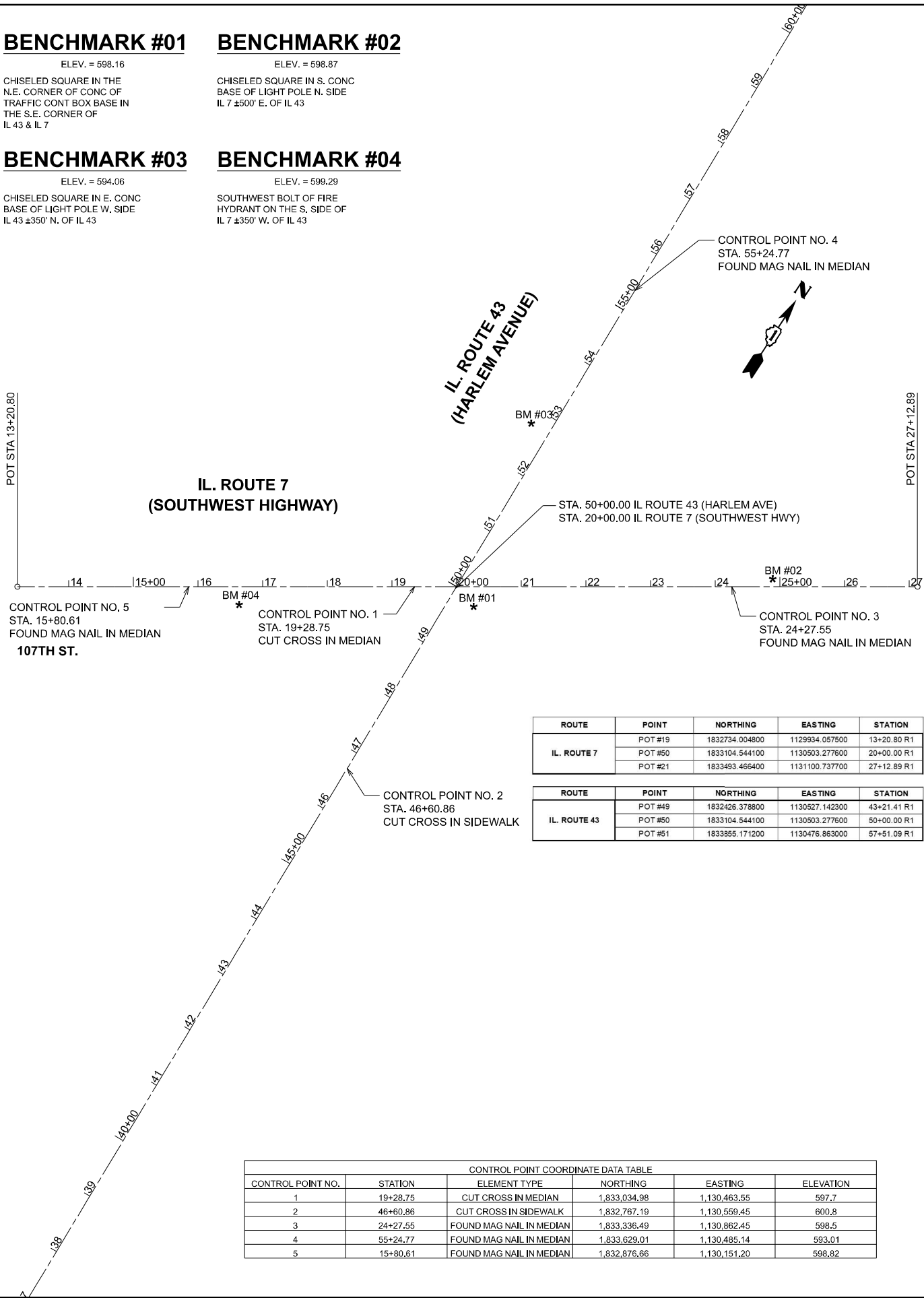
ELEV. = 598.87
CHISELED SQUARE IN S. CONC
BASE OF LIGHT POLE N. SIDE
IL 7 ±500' E. OF IL 43

BENCHMARK #03

ELEV. = 594.06
CHISELED SQUARE IN E. CONC
BASE OF LIGHT POLE W. SIDE
IL 43 ±350' N. OF IL 43

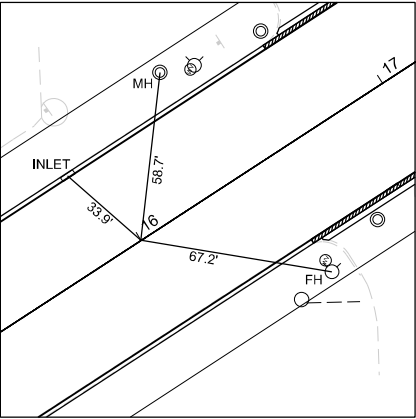
BENCHMARK #04

ELEV. = 599.29
SOUTHWEST BOLT OF FIRE
HYDRANT ON THE S. SIDE OF
IL 7 ±350' W. OF IL 43

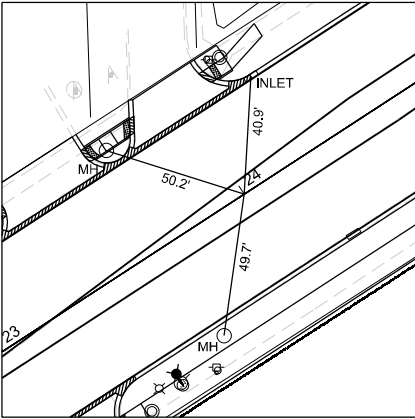


ROUTE	POINT	NORTHING	EASTING	STATION
IL ROUTE 7	POT #19	1832734.004800	1129934.057500	13+20.80 R1
	POT #50	1833104.544100	1130503.277600	20+00.00 R1
	POT #21	1833493.466400	1131100.737700	27+12.89 R1
ROUTE	POINT	NORTHING	EASTING	STATION
IL ROUTE 43	POT #49	1832426.378800	1130527.142300	43+21.41 R1
	POT #50	1833104.544100	1130503.277600	50+00.00 R1
	POT #51	1833855.171200	1130476.863000	57+51.09 R1

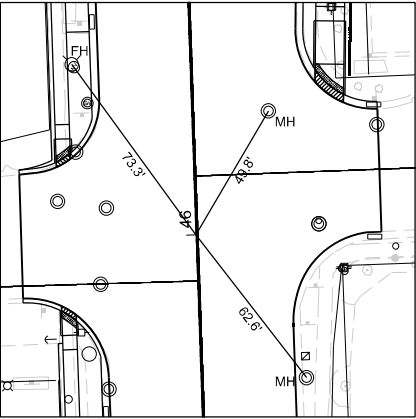
CONTROL POINT COORDINATE DATA TABLE					
CONTROL POINT NO.	STATION	ELEMENT TYPE	NORTHING	EASTING	ELEVATION
1	19+28.75	CUT CROSS IN MEDIAN	1,833,034.98	1,130,463.55	597.7
2	46+60.86	CUT CROSS IN SIDEWALK	1,832,767.19	1,130,559.45	600.8
3	24+27.55	FOUND MAG NAIL IN MEDIAN	1,833,336.49	1,130,862.45	598.5
4	55+24.77	FOUND MAG NAIL IN MEDIAN	1,833,629.01	1,130,485.14	593.01
5	15+80.61	FOUND MAG NAIL IN MEDIAN	1,832,876.66	1,130,151.20	598.82



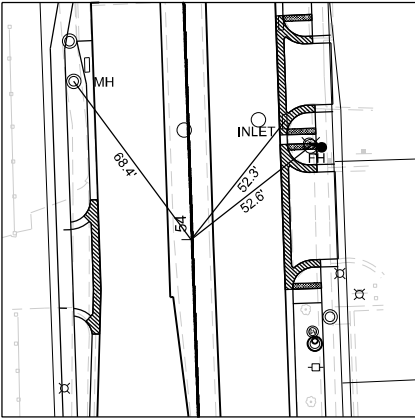
ALIGNMENT TIE (A-1)
STA: 16+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY)
N: 1,832,886.323
E: 1,130,168.048



ALIGNMENT TIE (A-2)
STA: 24+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY)
N: 1,833,322.766
E: 1,130,838.509



ALIGNMENT TIE (A-3)
STA: 46+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY)
N: 1,832,704.796
E: 1,130,517.345



ALIGNMENT TIE (A-4)
STA: 54+00.00
IL ROUTE 7 (SOUTHWEST HIGHWAY)
N: 1,833,504.302
E: 1,130,489.210

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	DRAWN - TN	REVISED -
PLOT SCALE = 0.16666633' / in.	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS
IL ROUTE 7 (SOUTHWSES HWY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: 1"=100' SHEET 1 OF 1 SHEETS STA. 12+50.00 TO STA. 42+50.00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	30
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHT 1
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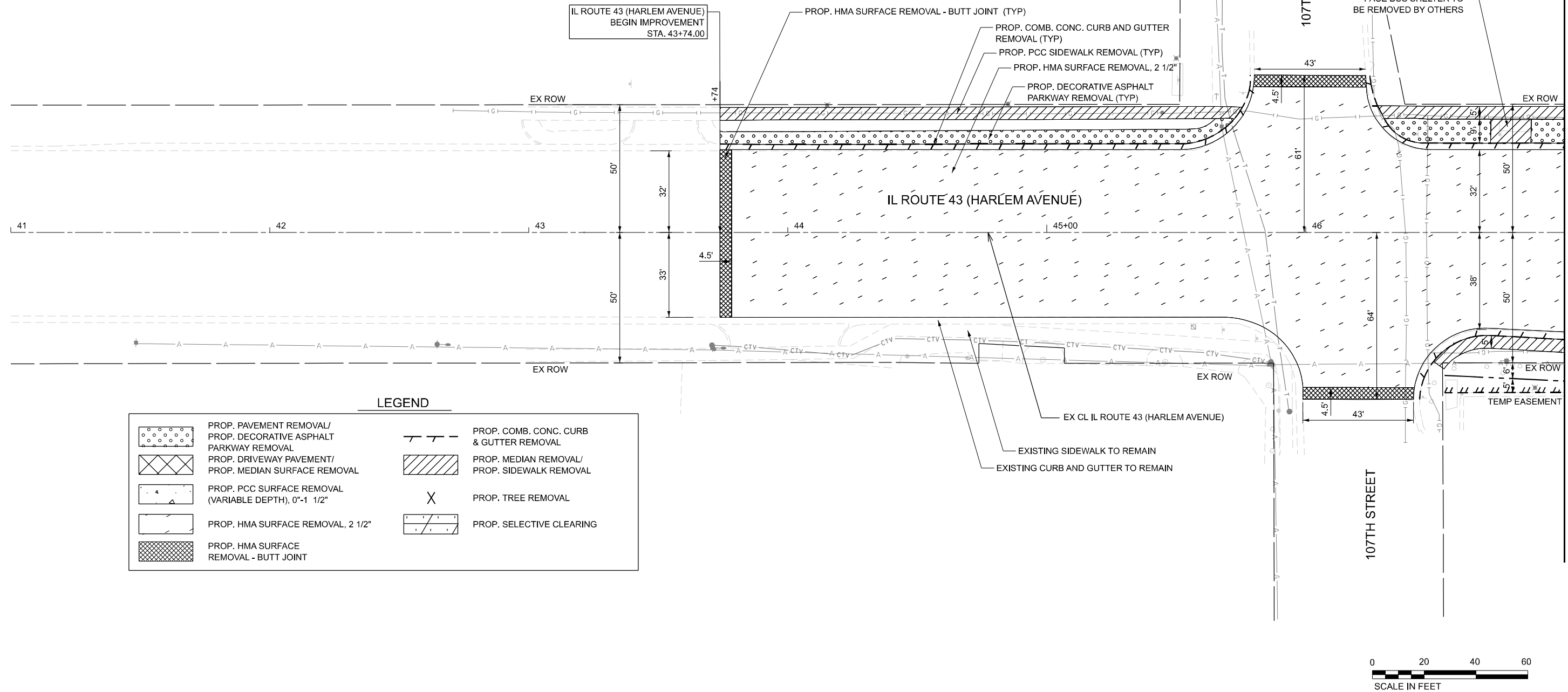
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 1 OF 5 SHEETS STA. 43+21.41 TO STA. 47+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	31
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

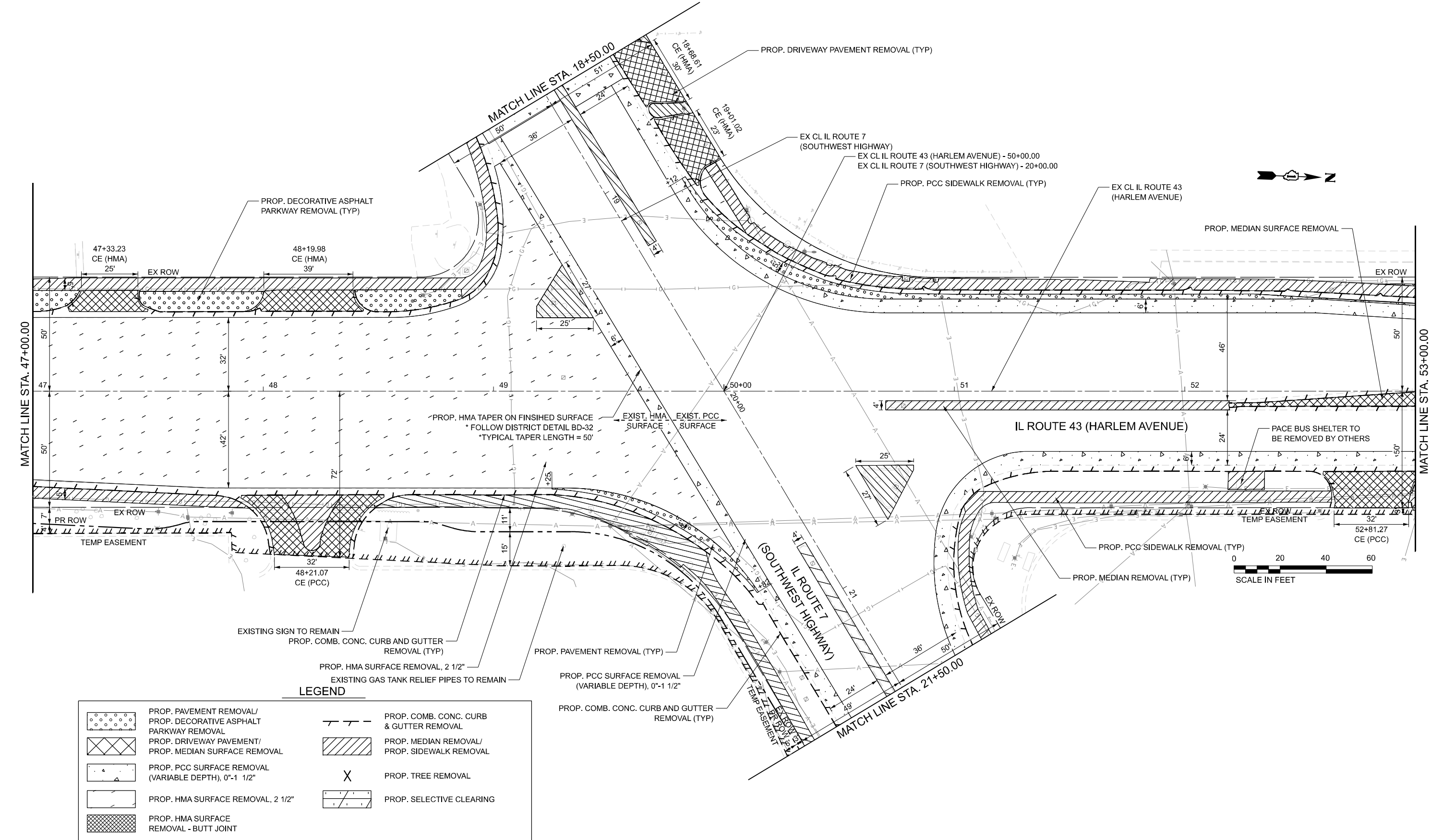


LEGEND

	PROP. PAVEMENT REMOVAL/ PROP. DECORATIVE ASPHALT PARKWAY REMOVAL		PROP. COMB. CONC. CURB & GUTTER REMOVAL
	PROP. DRIVEWAY PAVEMENT/ PROP. MEDIAN SURFACE REMOVAL		PROP. MEDIAN REMOVAL/ PROP. SIDEWALK REMOVAL
	PROP. PCC SURFACE REMOVAL (VARIABLE DEPTH), 0"-1 1/2"		PROP. TREE REMOVAL
	PROP. HMA SURFACE REMOVAL, 2 1/2"		PROP. SELECTIVE CLEARING
	PROP. HMA SURFACE REMOVAL - BUTT JOINT		



MODEL: SHT 2
FILE NAME: W:\191+200 IDOT_208010_108_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sht4-Removals.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 2 OF 5 SHEETS STA. 47+00.00 TO STA. 53+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	32
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

IL ROUTE 43 (HARLEM AVENUE)
END IMPROVEMENT
STA. 56+50.00

- EXISTING FENCE TO REMAIN (TYP

- PROP. PCC SIDEWALK REMOVAL (TYP

- PROP. DRIVEWAY PAVEMENT REMOVAL (TYP

- PROP. COMB. CONC. CURB AND GUTTER
REMOVAL (TYP)

- EX CL IL ROUTE 4
(HARLEM AVENUE

- PROP. PCC SURFACE REMOVAL-BUTT JOINT

48+19.98
CE (HMA)
35'

EX ROV

EX ROW

IL ROUTE 43 (HARLEM AVENUE)

– PROP. HMA SURFACE REMOVAL. 1 1/2

– PROPOSED HMA SURFACE REMOVAL
TO MATCH UP TO PROPOSED
DECK ELEVATIONS ONCE
MEDIAN IS REMOVED










— SEE STRUCTURAL SHEETS FOR BRIDGE REMOVAL AND WORK

- PROP. HMA SURFACE REMOVAL - BUTT JOINT (TYP

- PROP HMA SURFACE REMOVAL 1 1/2'

0 20 40 60
SCALE IN FEET

LEGEND

	PROP. PAVEMENT REMOVAL/ PROP. DECORATIVE ASPHALT PARKWAY REMOVAL		PROP. COMB. CONC. CURB & GUTTER REMOVAL
	PROP. DRIVEWAY PAVEMENT/ PROP. MEDIAN SURFACE REMOVAL		PROP. MEDIAN REMOVAL/ PROP. SIDEWALK REMOVAL
	PROP. PCC SURFACE REMOVAL (VARIABLE DEPTH), 0"-1 1/2"		PROP. TREE REMOVAL
	PROP. HMA SURFACE REMOVAL, ≥ 1 1/2"		PROP. SELECTIVE CLEARING PROP. TREE REMOVAL, ACRES
	PROP. HMA SURFACE REMOVAL - BUTT JOINT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	33
		CONTRACT NO. 62W47		

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MODEL: SHT 4
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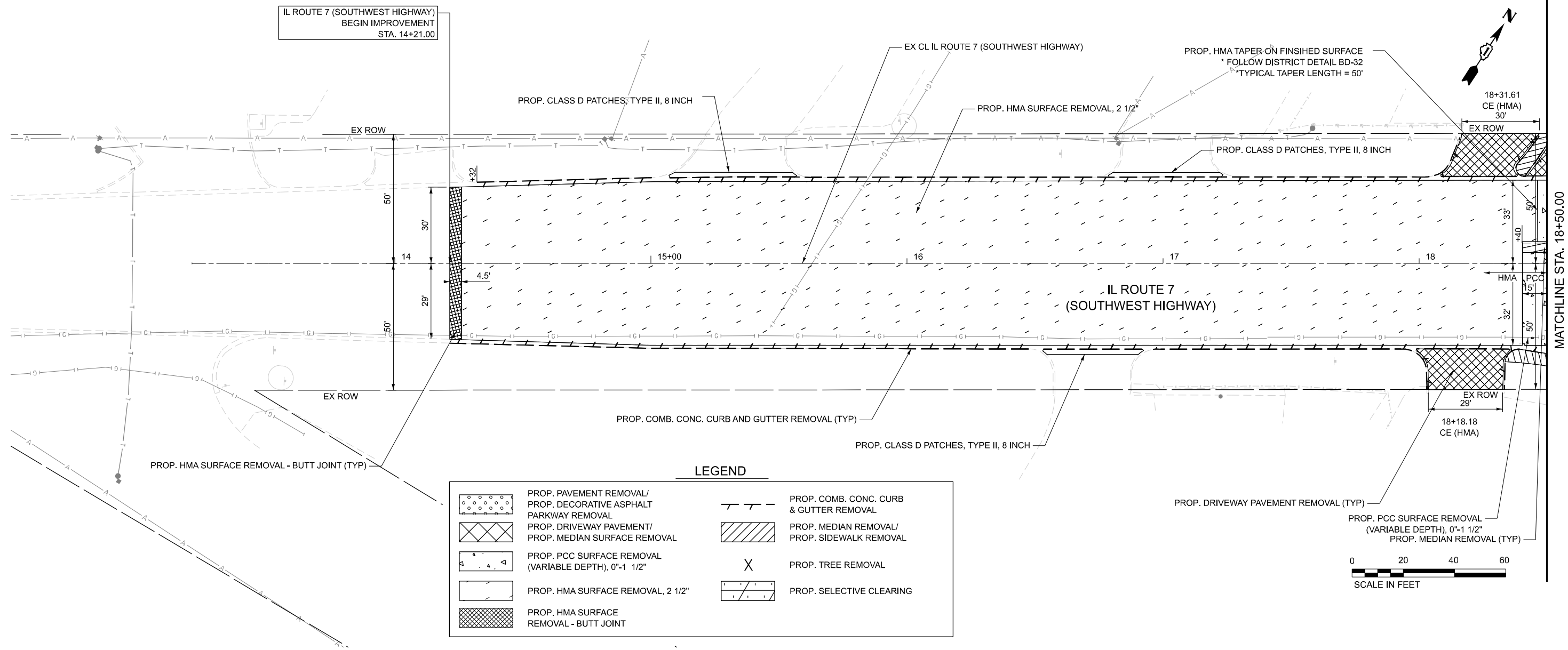


USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1"=20'	SHEET 4	OF 5 SHEETS	STA. 13+20.80 TO STA. 17+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	34
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



MODEL: SHT 5
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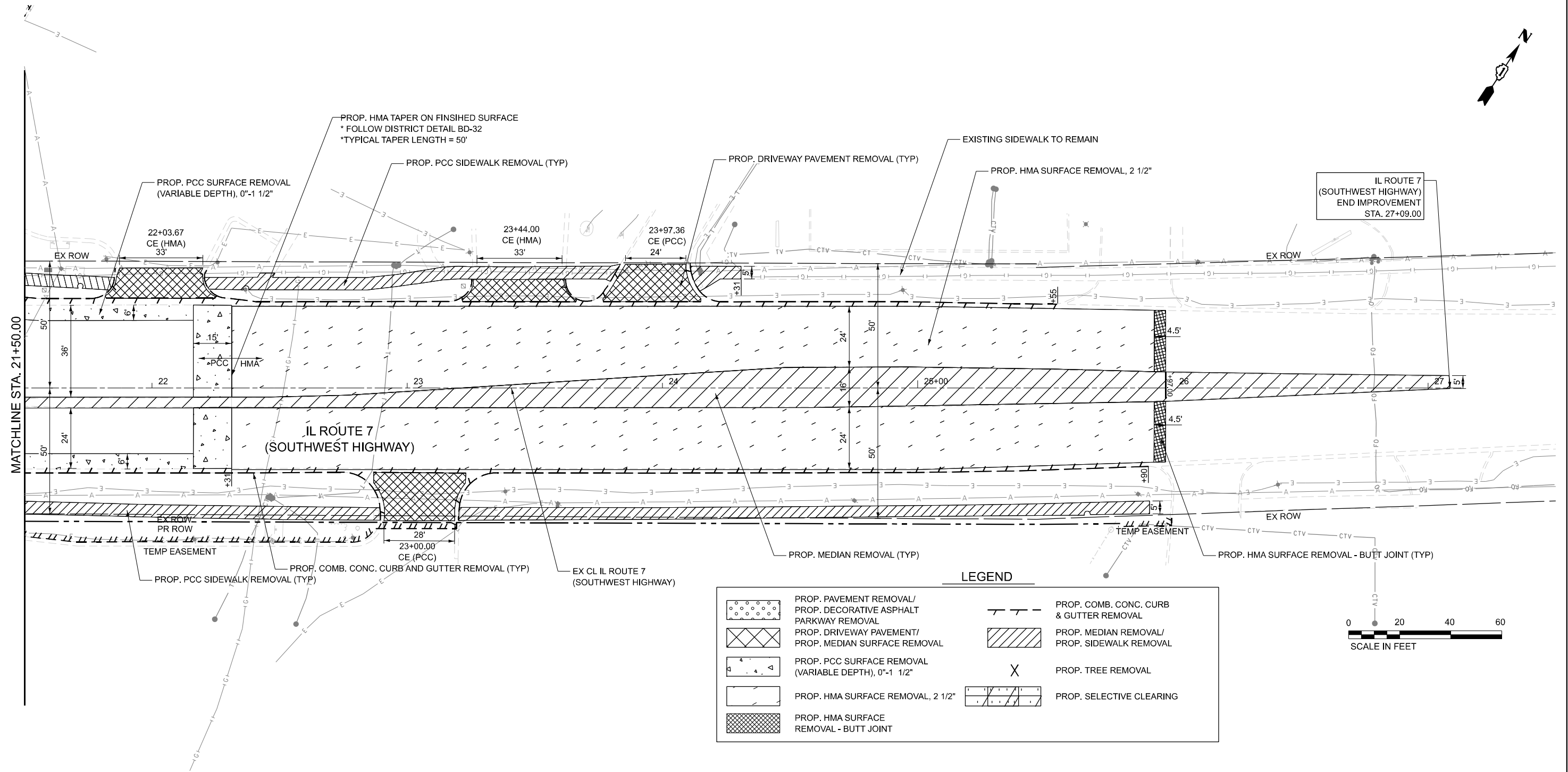
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

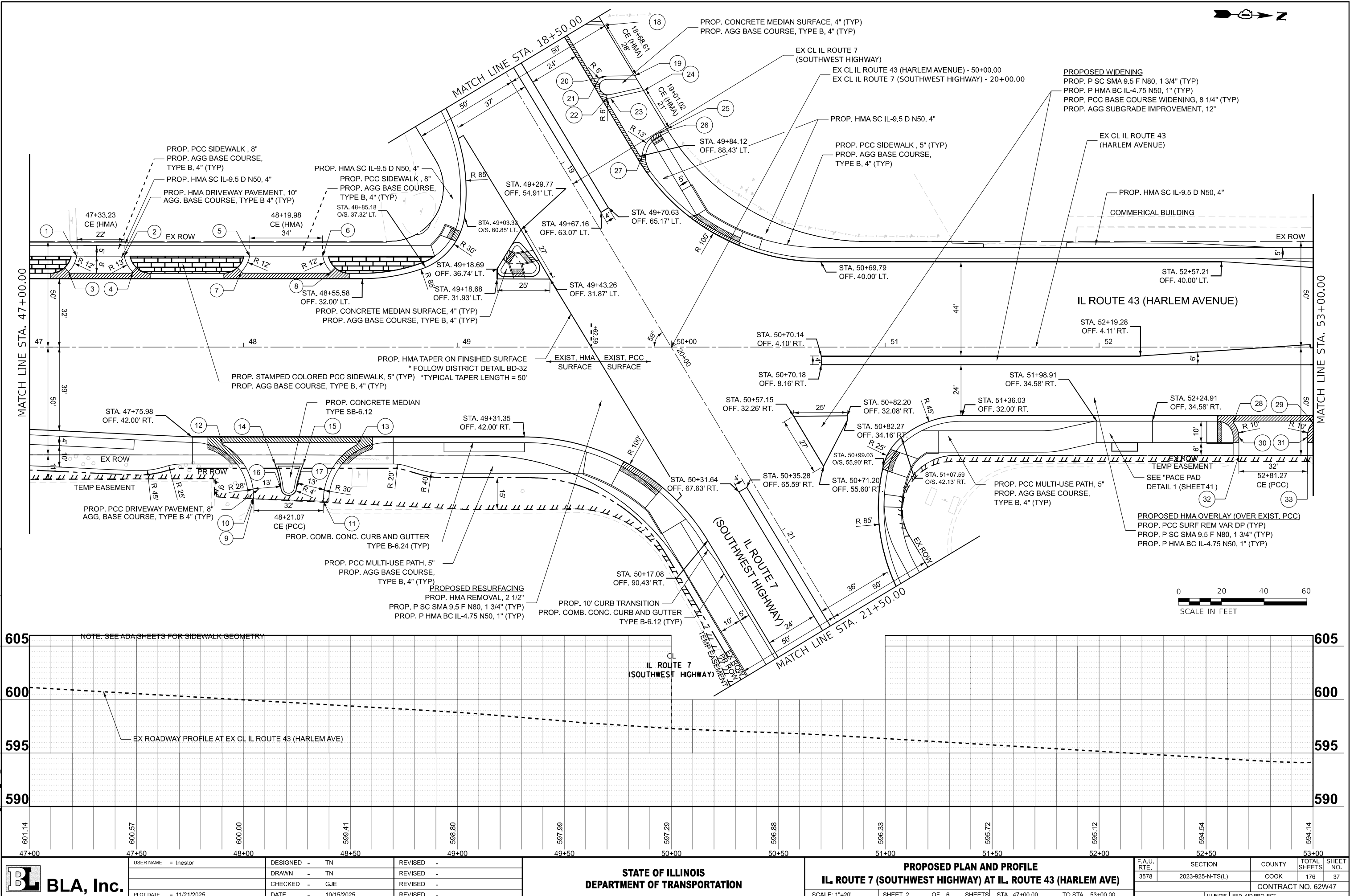
SCALE: 1"=20' SHEET 5 OF 5 SHEETS STA. 23+00.00 TO STA. 27+12.89

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	35
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				





MODEL: SHT 2
FILE NAME: W:\191+200 IDOT_208010_T08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shd-plan and profile.dgn



USER NAME = tnestor
PLOT DATE = 11/21/2025

DESIGNED - TN
DRAWN - TN
CHECKED - GJE
DATE - 10/15/2025

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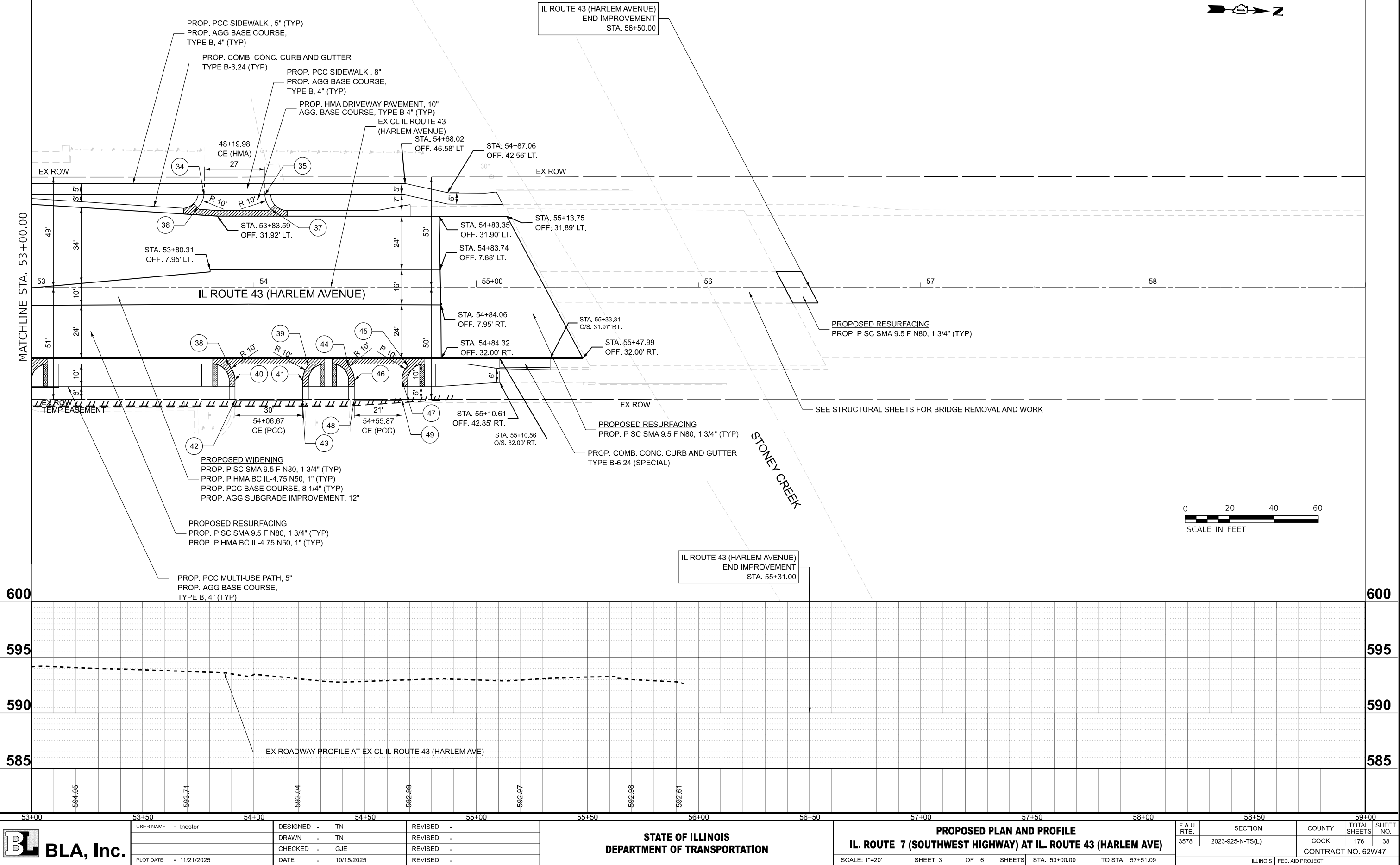
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: 1"=20'
SHEET 2 OF 6 SHEETS
STA. 47+00.00 TO STA. 53+00.00

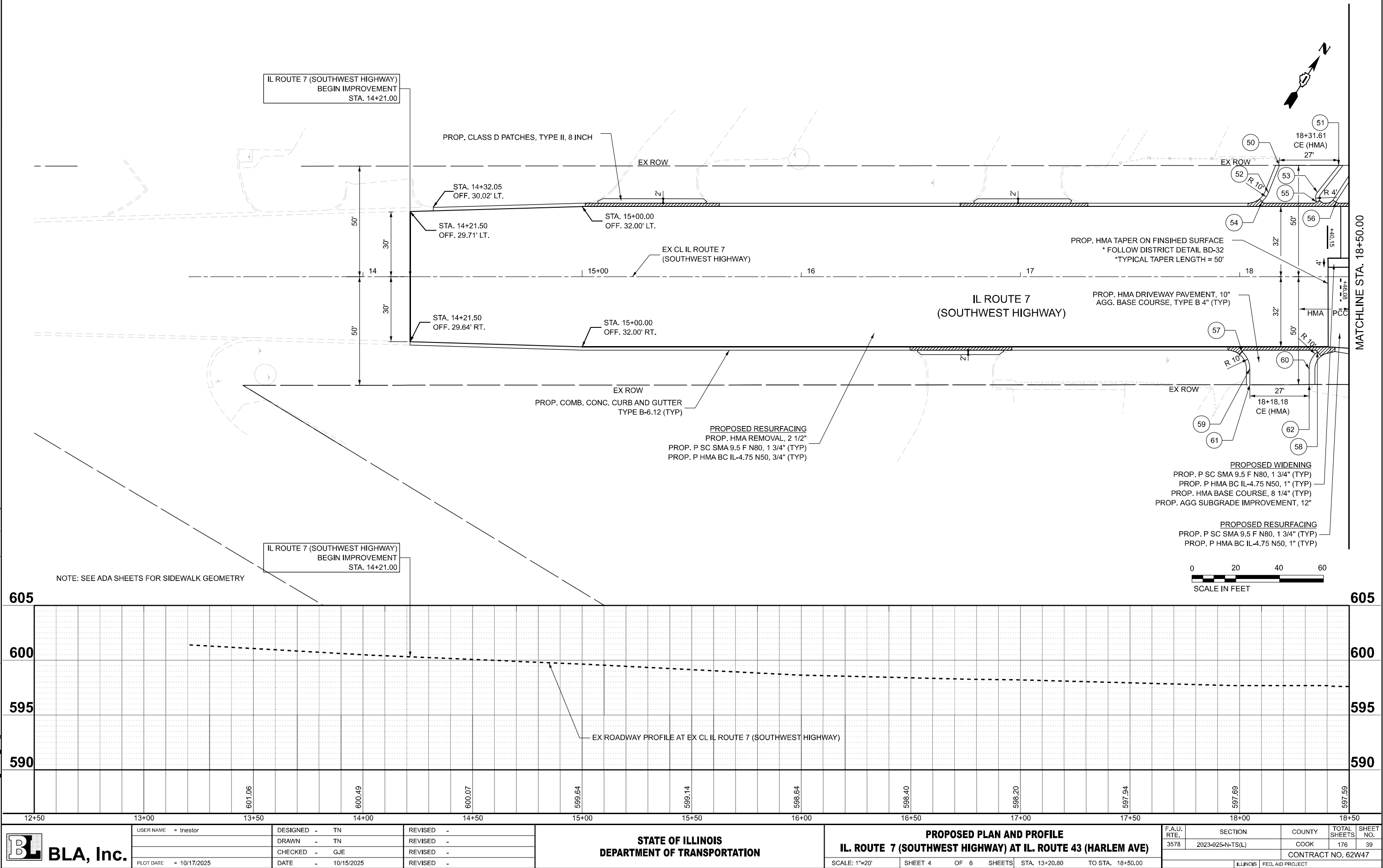
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	37

CONTRACT NO. 62W47

MODEL: SHT 3
FILE NAME: \\A191-200 IDOT_208010_108_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh-1-plan and profile.dgn



MODEL: SHT 4
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sht-plan and profile.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

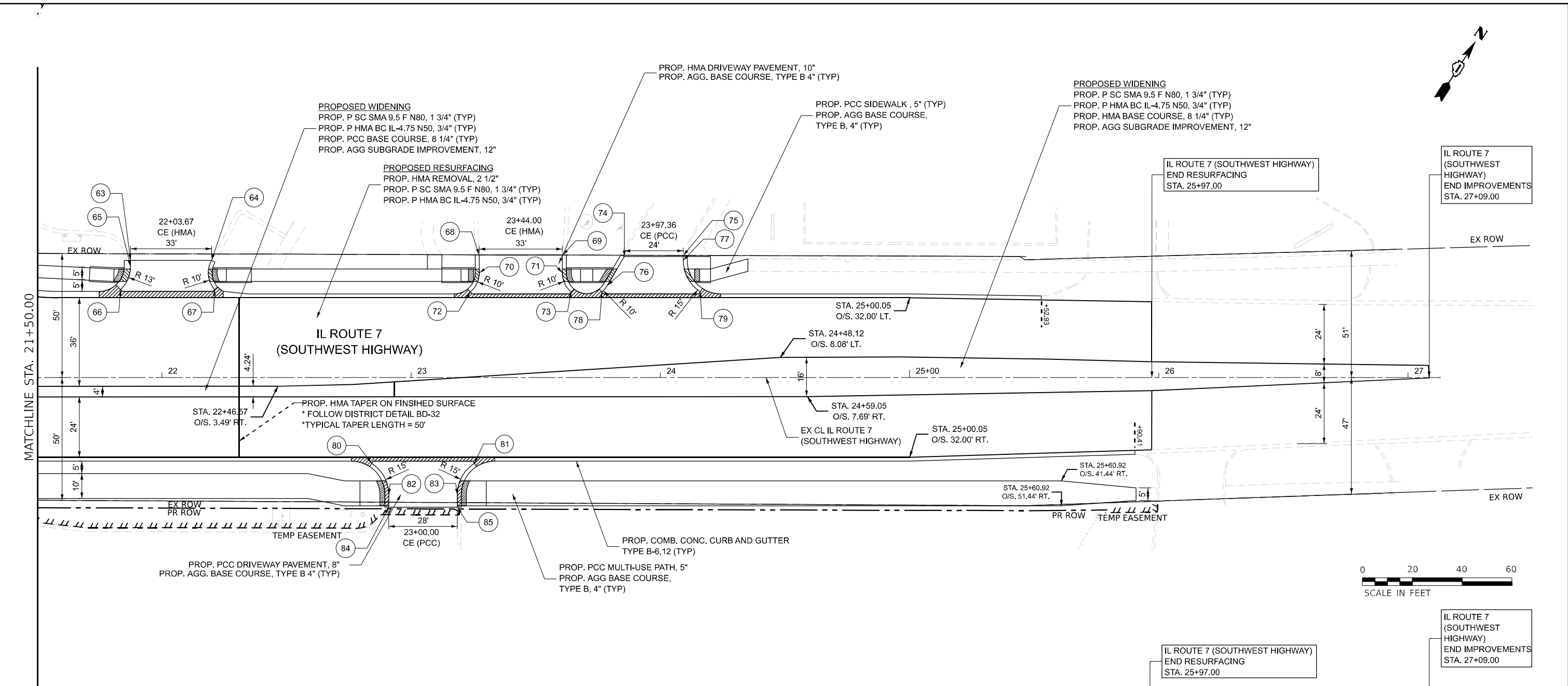
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN AND PROFILE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

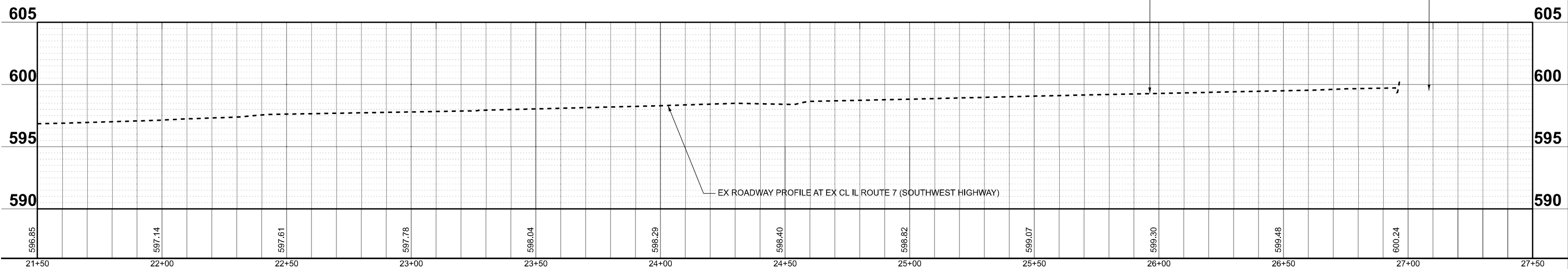
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	39
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHT 5
FILE NAME: W:\191-200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sht-plan and profile.dgn



NOTE: SEE ADA SHEETS FOR SIDEWALK GEOMETRY



<div><div></div><div>BLA, Inc.</div></div>	USER NAME = tnestor		DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN AND PROFILE IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
			DRAWN - TN	REVISED -		SCALE: 1"=20'	SHEET 5	OF 6	SHEETS	STA. 21+50.00	TO STA. 27+12.89	3578	2023-925-N-TS(L)	COOK	176	40
			CHECKED - GJE	REVISED -												
	PLOT DATE = 11/21/2025		DATE - 10/15/2025	REVISED -												
											CONTRACT NO. 62W47					
											ILLINOIS FED. AID PROJECT					

MODEL: SHT 6
FILE NAME: W:191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sht-plan and profile.dgn

DRIVEWAY TABLE		
PLAN AND PROFILE #2		
POINT NO.	STATION	OFFSET
1	47+22.08	42.58' LT
2	47+44.37	42.58' LT
3	47+18.20	35.31' LT
4	47+48.25	35.31' LT
5	48+02.84	42.58' LT
6	48+37.13	42.58' LT
7	47+98.96	35.31' LT
8	48+41.01	35.31' LT
9	48+04.78	72.01' RT
10	48+04.78	70.00' RT
11	48+37.35	72.01' RT
12	47+90.26	45.46' RT
13	48+53.35	45.47' RT
14	48+14.77	54.59' RT
15	48+28.03	54.59' RT
16	48+17.59	66.06' RT
17	48+24.83	65.74' RT
18	18+54.81	50.79' LT
19	18+82.45	50.81' LT
20	18+75.78	39.55' LT
21	18+75.98	34.15' LT
22	18+84.65	34.17' LT
23	18+85.48	35.74' LT
24	18+90.58	50.82' LT
25	19+11.47	50.84' LT
26	19+11.01	46.00' LT
27	19+16.52	34.41' LT
28	52+62.63	35.25' RT
29	52+99.87	35.25' RT
30	52+65.27	42.00' RT
31	52+97.27	42.00' RT
32	52+65.27	50.70' RT
33	52+97.27	50.72' RT

DRIVEWAY TABLE		
PLAN AND PROFILE #3		
POINT NO.	STATION	OFFSET
34	53+77.52	41.58' LT
35	54+04.99	41.58' LT
36	53+74.65	35.74' LT
37	54+07.55	35.17' LT
38	53+88.85	35.25' RT
39	54+24.49	35.25' RT
40	53+91.47	42.00' RT
41	54+21.87	42.00' RT
42	53+91.47	50.78' RT
43	54+21.87	50.79' RT
44	54+42.55	35.25' RT
45	54+69.18	35.25' RT
46	54+45.17	42.00' RT
47	54+66.56	42.00' RT
48	54+45.17	50.81' RT
49	54+66.56	50.82' RT

DRIVEWAY TABLE		
PLAN AND PROFILE #4		
POINT NO.	STATION	OFFSET
50	18+18.10	50.76' LT
51	18+45.18	50.78' LT
52	18+13.09	38.28' LT
53	18+35.51	38.47' LT
54	18+10.24	34.35' LT
55	18+35.16	34.06' LT
56	18+43.96	34.12' LT
57	18+01.08	34.35' RT
58	18+35.47	34.33' RT
59	18+04.65	42.00' RT
60	18+31.73	42.13' RT
61	18+04.65	49.31' RT
62	18+31.73	49.27' RT

DRIVEWAY TABLE		
PLAN AND PROFILE #5		
POINT NO.	STATION	OFFSET
63	21+87.40	47.00' LT
64	22+20.05	47.23' LT
65	21+87.40	44.50' LT
66	21+83.38	35.32' LT
67	22+21.21	35.25' LT
68	23+27.32	49.24' LT
69	23+60.69	49.14' LT
70	23+27.32	42.00' LT
71	23+60.69	42.00' LT
72	23+23.75	34.35' LT
73	23+64.26	34.35' LT
74	23+85.53	48.50' LT
75	24+09.19	48.50' LT
76	23+78.70	36.92' LT
77	24+09.19	42.00' LT
78	23+76.52	34.35' LT
79	24+16.02	34.42' LT
80	22+84.19	34.42' RT
81	23+25.37	34.42' RT
82	22+91.02	47.00' RT
83	23+18.54	47.00' RT
84	22+91.02	52.00' RT
85	23+18.54	52.00' RT



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PLAN AND PROFILE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 6 OF 6 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	41
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



USER NAME = Inestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/15/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PACE BUS PAD DETAILS	
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)	

SCALE: 1"=5'	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	42
CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

STAGE CONSTRUCTION GENERAL NOTES

1.

ALL OF THE TRAFFIC CONTROL DEVICES SHALL BE IN PLACE BEFORE CONSTRUCTION IS STARTED. TEMPORARY TRAFFIC SIGNALS SHALL BE CONSTRUCTED AT THE INTERSECTIONS NOTED IN THE PLANS. TEMPORARY INTERSECTION SIGNALIZATION SHALL BE ADJUSTED TO ACCOMMODATE THE VARIOUS STAGES OF CONSTRUCTION SHOWN. THE TRAFFIC CONTROL STANDARDS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT.
2.

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE INFORMATIONAL SIGNS ON TEMPORARY SUPPORTS FOR DRIVEWAYS. THESE SIGNS SHALL BE WHITE ON GREEN IN ACCORDANCE WITH THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES. ALSO, "CAUTION NEW LANES OPEN STOP HERE" SIGNS WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT THE APPROPRIATE LOCATIONS. THIS WORK WILL BE PER DISTRICT DETAIL TC-26 INCLUDED IN THE VARIOUS ITEMS FOR "TEMPORARY INFORMATION SIGNAGE".
3.

THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
4.

THE CONTRACTOR WILL GIVE THE ENGINEER AT LEAST 10 DAYS NOTICE PRIOR TO ANY TRAFFIC STAGING CHANGES.
5.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
6.

STOP SIGNS AND STOP BARS ARE TO BE MAINTAINED FOR UNSIGNALIZED SIDE STREETS AND DRIVEWAYS THROUGHALL CONSTRUCTION STAGES IN WHICH THEY ARE TO BE MAINTAINED.
7.

POSITIVE DRAINAGE WITHIN THE WORK ZONE MUST BE MAINTAINED AT ALL TIMES TO THE SATISFACTION OF THE RESIDENT ENGINEER. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY INLETS, OUTLETS, AND CONNECTIONS FOR ALL EXISTING AND PROPOSED FACILITIES INCLUDING TEMPORARY PUMPING IF NECESSARY. TEMPORARY ACCOMMODATIONS SHALL BE MAINTAINED UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE AND THE FINAL SHAPING AND GRADING OF DITCHES IS PERFORMED. THE COST OF ALL LABOR, EQUIPMENT, AND MATERIALS (TEMPORARY OR PERMANENT USED AS TEMPORARY) TO COMPLY WITH THIS REQUIREMENT WILL NOT BE PAID FOR DIRECTLY, BUT THE COST SHALL BE CONSIDERED INCLUDED IN THE PROPOSED ITEMS OF WORK IN THE CONTRACT.
8.

THE CONTRACTOR SHALL NOTE LOCATIONS OF ALL PAVEMENT MARKINGS OUTSIDE OF THE PROJECT LIMITS FOR RESTORATION PURPOSES.
9.

THE CONTRACTOR SHALL PROVIDE AT LEAST 48 HOURS PRIOR NOTICE TO THE RESIDENT ENGINEER, VILLAGE OF WORTH EMERGENCY SERVICES, SCHOOLS, AND POST OFFICE PRIOR TO IMPLEMENTING LANE CLOSURES OR MAJOR TRAFFIC CONTROL CHANGES.
10.

IF THE CONTRACTOR MUST FULLY CLOSE AN EXISTING DRIVEWAY OR SIDE STREET, THE CONTRACTOR MUST MAINTAIN A TEMPORARY ACCESS. THE CONTRACTOR SHALL GIVE AT LEAST ONE WEEK PRIOR WRITTEN NOTICE OF DRIVEWAY OR SIDE STREET CLOSURES TO THE ENGINEER, THE VILLAGE, EMERGENCY SERVICES, SCHOOLS, AND THE LOCAL POST OFFICE. DIRECTIONAL SIGNAGE SHALL BE PROVIDED TO REDIRECT DRIVERS AND PATRONS OF AFFECTED BUSINESSES TO ACCESS PROPERTIES BY ALTERNATE ROUTES. THIS WORK SHALL BE COORDINATED BY THE RESIDENT ENGINEER. ANY REDIRECTING SIGNAGE SHALL BE PAID FOR AS "TEMPORARY INFORMATION SIGNAGE" AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE. ANY ITEMS FOR CLOSURE OF THE DRIVES INCLUDING TYPE III BARRICADES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED WITH THE COST OF WORK ITEMS.
11.

THE CONTRACTOR SHALL REMOVE OR COVER ALL W21-1 (WORKERS PRESENT) AND W20-7 (FLAGGER) SIGNS, WITHIN AN HOUR, WHEN WORKERS ARE NOT PRESENT OR IF THE SIGN IS NOT APPLICABLE.
12.

TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
13.

WHEN WORKING ADJACENT TO THE ROAD AND UTILIZING DAILY LANE CLOSURES, DROP-OFFS ADJACENT TO THE TRAVEL LANES SHALL BE KEPT TO A MINIMUM. PROTECTION OF THE DROP-OFF SHALL BE ACCORDING TO THE IDOT BUREAU OF SAFETY PROGRAMS AND ENGINEERING, SAFETY ENGINEERING POLICY MEMORANDUM 4-21. DROP-OFFS GREATER THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY WILL NOT BE ALLOWED AT LOCATIONS WHERE THE DROP-OFF IS LOCATED WITHIN 8 FT OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE. THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE EXCAVATION REQUIRED FOR THE CONSTRUCTION DURING THE TIME THAT THE ADJACENT LANE IS CLOSED. AS NOTED ABOVE, PRIOR TO REOPENING THE LANE TO TRAFFIC, THE CONTRACTOR SHALL PLACE SUFFICIENT MATERIAL TO REDUCE THE DROP-OFF TO LESS THAN THE SPECIFIED MAXIMUM DROP-OFF DEPTH SHOWN IN TABLE 2, CONDITION II OF THE SAFETY 4-21 POLICY AND ENSURE THAT THE DROP-OFF AREAS MEET THE OFFSET, HEIGHT, AND DURATION REQUIREMENTS TO USE BARRICADES/DRUMS AT THE END OF EACH WORKDAY. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE THE AMOUNT OF WORK THAT CAN BE COMPLETED WITHIN THE TIME OF THE DAILY LANE CLOSURE. IF THE ABOVE REQUIREMENTS CAN'T BE MET, AND IT IS DETERMINED THAT OVERNIGHT LANE CLOSURES AND/OR TEMPORARY CONCRETE BARRIER WALL INSTALLATION WILL BE NECESSARY, THEN IDOT WRITTEN APPROVAL WILL BE REQUIRED PRIOR TO THE INSTALLATION OF THESE ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED TO COMPLY WITH THIS REQUIREMENT. WHERE POSITIVE PROTECTION (TEMPORARY CONCRETE BARRIER PER STD. 704001) IS PROVIDED, THIS REQUIREMENT IS NULLIFIED.
14.

CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL ROAD CLOSURE, TRAFFIC STAGE CHANGES, AND NEW TRAFFIC SIGNAL TURN-ON EVENTS ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGNS SHALL BE REMOVED TWO WEEKS THEREAFTER UNLESS THE SIGNS ARE NEEDED AGAIN FOR A SUBSEQUENT FUTURE EVENT THAT WILL OCCUR WITHIN 2 WEEKS ON THE SAME APPROACH OF THE EFFECTED ROADWAY. THE SIGN LOCATIONS SHALL BE (DETERMINED BY THE ENGINEER) PLACED AS DIRECTED BY THE ENGINEER.
15.

ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.

GENERAL SEQUENCE OF STAGING

STAGE I

INSTALL ALL STAGE 1 EROSION CONTROL PER STAGE 1 EROSION CONTROL PLANS. INSTALL ALL TRAFFIC CONTROL (TEMPORARY PAVEMENT MARKINGS, BARRELS, SIGNAGE, AND TEMPORARY TRAFFIC SIGNALS) FOR STAGE 1 PER STAGE 1 STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS. CLOSE WB OUTSIDE LANE ON IL ROUTE 7 (SOUTHWEST HIGHWAY) AND NB OUTSIDE LANE ON IL ROUTE 43 (HARLEM AVENUE). SAW CUT AND REMOVE EXISTING PAVEMENT AND CURB AND GUTTER REQUIRED TO CONSTRUCT THE PROPOSED NEW CURB AND GUTTER, PCC MULTI-USE PATH, STORM SEWER ADJUSTMENTS, AND FINAL LANDSCAPING.

STAGE II

REMOVE STAGE 1 TRAFFIC CONTROL. INSTALL ALL STAGE 2 EROSION CONTROL PER STAGE 2 EROSION CONTROL PLANS. INSTALL ALL TRAFFIC CONTROL (TEMPORARY PAVEMENT MARKINGS, BARRELS, SIGNAGE, AND TEMPORARY TRAFFIC SIGNALS) FOR STAGE 2 PER STAGE 2 STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS. CLOSE EB OUTSIDE LANE ON IL ROUTE 7 (SOUTHWEST HIGHWAY) AND SB OUTSIDE LANE ON IL ROUTE 43 (HARLEM AVENUE). SAW CUT AND REMOVE EXISTING PAVEMENT AND CURB AND GUTTER REQUIRED TO CONSTRUCT THE PROPOSED NEW CURB AND GUTTER, PCC MULTI-USE PATH, STORM SEWER ADJUSTMENTS, AND FINAL LANDSCAPING.

STAGE III

REMOVE STAGE 2 TRAFFIC CONTROL. INSTALL ALL STAGE 3 EROSION CONTROL PER STAGE 3 EROSION CONTROL PLANS. INSTALL ALL TRAFFIC CONTROL (TEMPORARY PAVEMENT MARKINGS, BARRELS, SIGNAGE, AND TEMPORARY TRAFFIC SIGNALS) FOR STAGE 3 PER STAGE 3 STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS. CLOSE LEFT TURN LANES ON IL ROUTE 7 (SOUTHWEST HIGHWAY) AND IL ROUTE 43 (HARLEM AVENUE). SAW CUT AND REMOVE EXISTING MEDIAN REQUIRED TO CONSTRUCT PROPOSED PAVEMENT.

STAGE IV

REMOVE STAGE 3 TRAFFIC CONTROL. UTILIZING DAYTIME CLOSURES, PREFORM VARIABLE DEPTH MILL ON PCC PAVEMENT AND MILL ON HMA PAVEMENT. PLACE BINDER COURSE AND SURFACE COURSE THROUGHOUT ENTIRE PROJECT. PLACE NEW PAVEMENT MARKINGS, LANDSCAPING AND REMOVE TEMORARY EROSION CONTROL MEASURES AND TEMPORARY TRAFFIC CONTROL DEVICES.

MODEL: Unnamed Plan-1 - Plan 2 (Sheet)
FILE NAME: W\191-200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-MOT Gen notes.dgn



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PLOT DATE = 11/25/2025		DATE - 10/15/2025	REVISED -

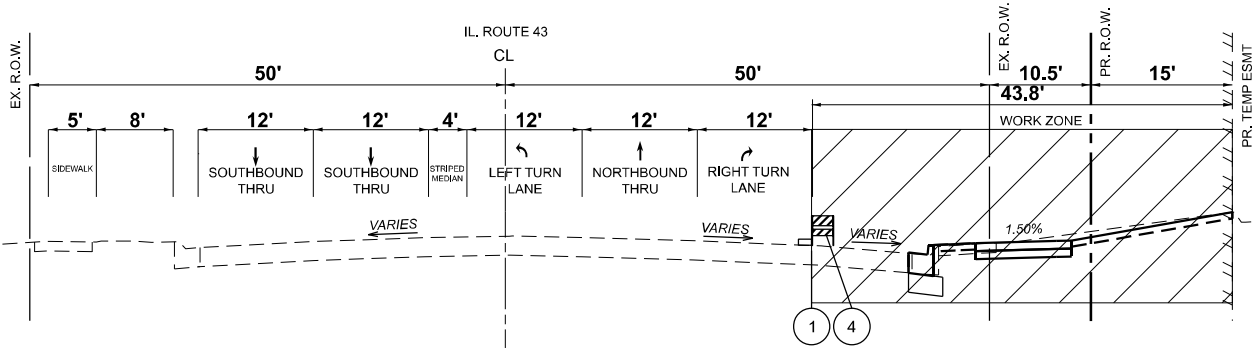
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - GENERAL NOTES
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

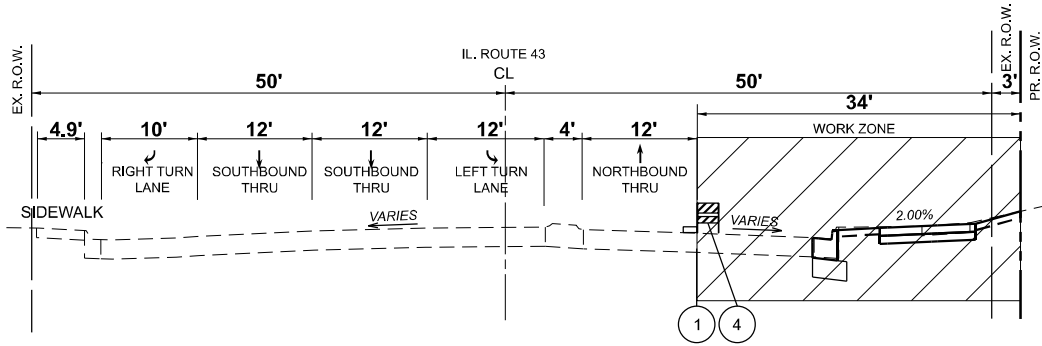
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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	43
CONTRACT NO. 62W47				
		ILLINOIS	FED. AID PROJECT	

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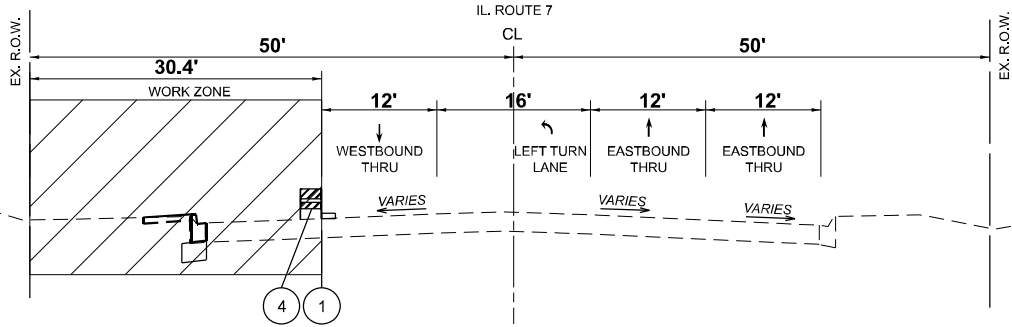
STAGE 1 - IL ROUTE 43 (HARLEM AVENUE)
STA. 43+74.00 TO STA. 50+00.00



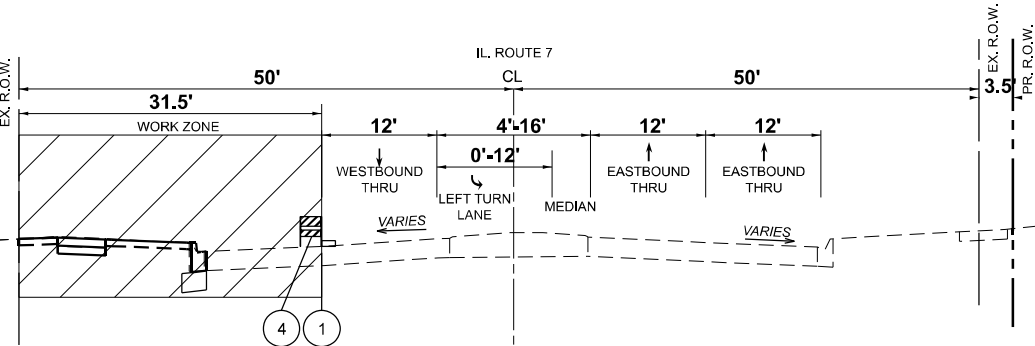
STAGE 1 - IL ROUTE 43 (HARLEM AVENUE)
STA. 50+00.00 TO STA. 56+50.00

LEGEND:

- ① PAVEMENT MARKING, 4" WHITE EDGE LINE - TYPE IV TAPE
- ② PAVEMENT MARKING, 6" WHITE LINE-TYPE IV TAPE
- ③ PAVEMENT MARKING, LINE, 4" WHITE (10' DASH - 30' SKIP) TYPE IV TAPE
- ④ TYPE II BARRICADE OR DRUM



STAGE 1 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 14+21.00 TO STA. 20+00.00



STAGE 1 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 20+00.00 TO STA. 25+09.00

- PAVEMENT MARKING
- WORK ZONE
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM



USER NAME = tnestor	DESIGNED - TN	REVISED -
DRAWN - TN	REVISED -	
PLOT SCALE = 0.16666633" / in.	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

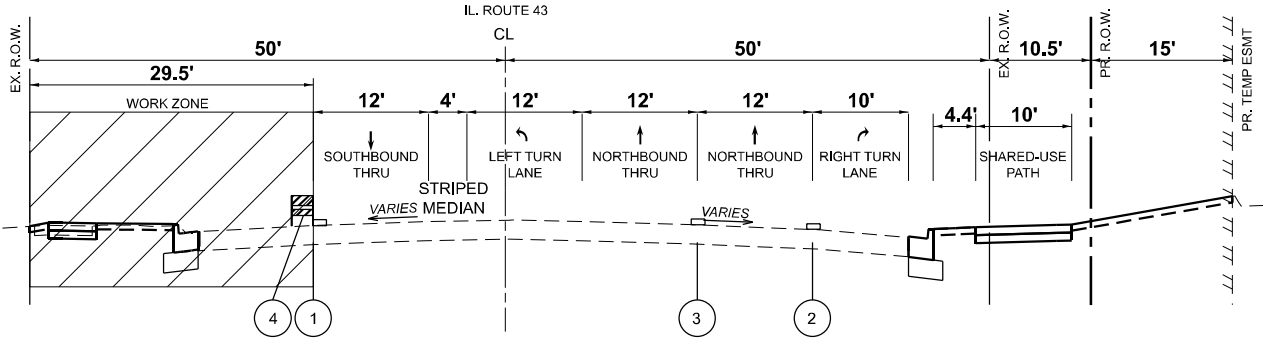
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE I TYPICAL CROSS SECTIONS
IL. ROUTE 7 (SOUTHWES HWY) AT IL. ROUTE 43 (HARLEM AVE)**

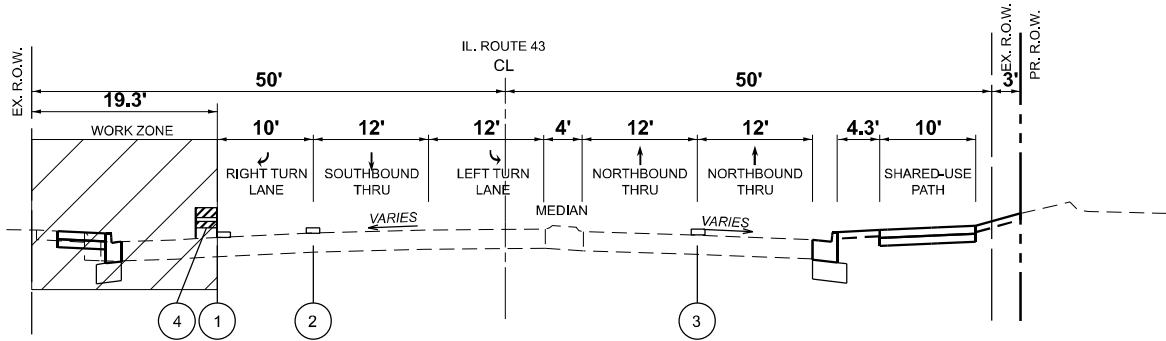
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F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	44
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: TwpStd2 [Sheet]
FILE NAME: W:\1912-200 [DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh1-typical.dgn



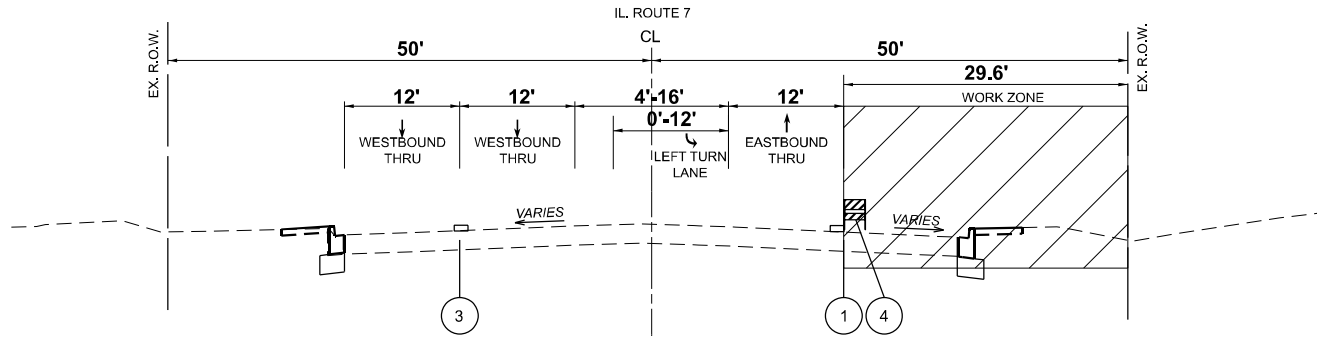
STAGE 2 - IL ROUTE 43 (HARLEM AVENUE)
STA. 43+74.00 TO STA. 50+00.00



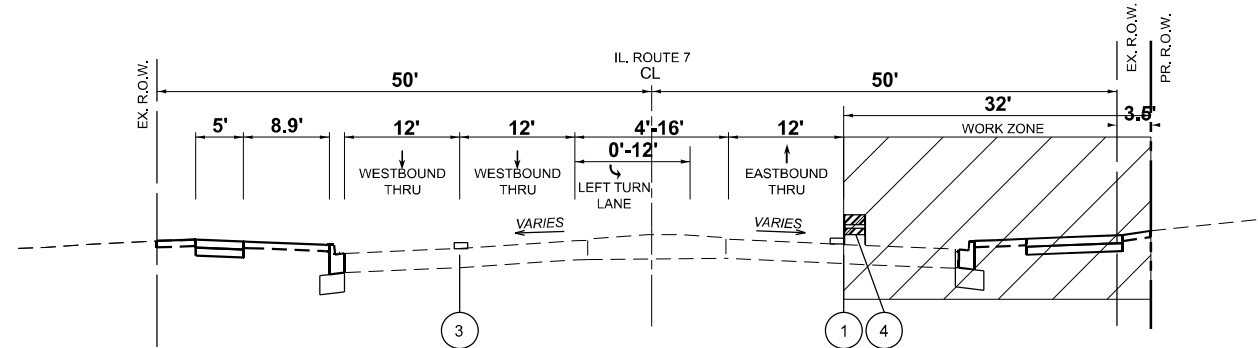
STAGE 2 - IL ROUTE 43 (HARLEM AVENUE)
STA. 50+00.00 TO STA. 56+50.00

LEGEND:

- ① PAVEMENT MARKING, 4" WHITE EDGE LINE - TYPE IV TAPE
- ② PAVEMENT MARKING, 6" WHITE LINE-TYPE IV TAPE
- ③ PAVEMENT MARKING, LINE, 4" WHITE (10' DASH - 30' SKIP) TYPE IV TAPE
- ④ TYPE II BARRICADE OR DRUM



STAGE 2 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 14+21.00 TO STA. 20+00.00



STAGE 2 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 20+00.00 TO STA. 25+09.00

- PAVEMENT MARKING
- WORK ZONE
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM



USER NAME = tnestor	DESIGNED - TN	REVISED -
DRAWN - TN	REVISED -	
PLOT SCALE = 0.16666633" / in.	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

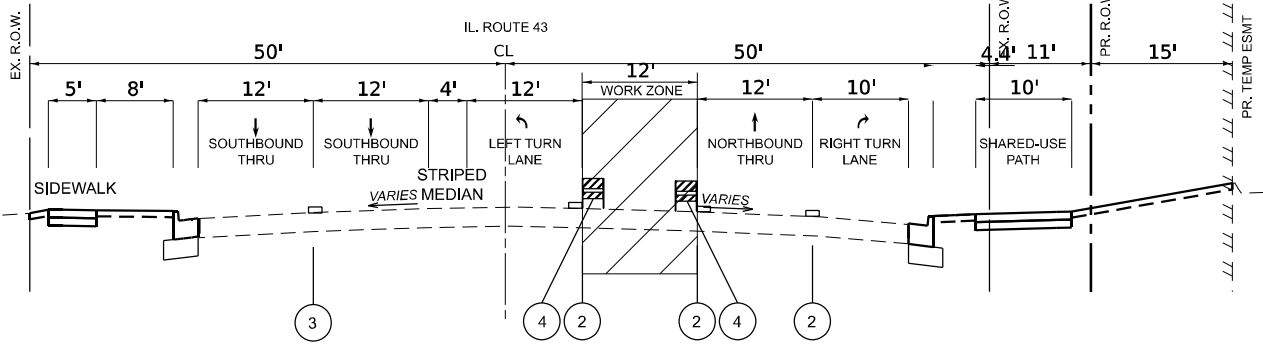
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE II TYPICAL CROSS SECTIONS
IL. ROUTE 7 (SOUTHWES HWY) AT IL. ROUTE 43 (HARLEM AVE)

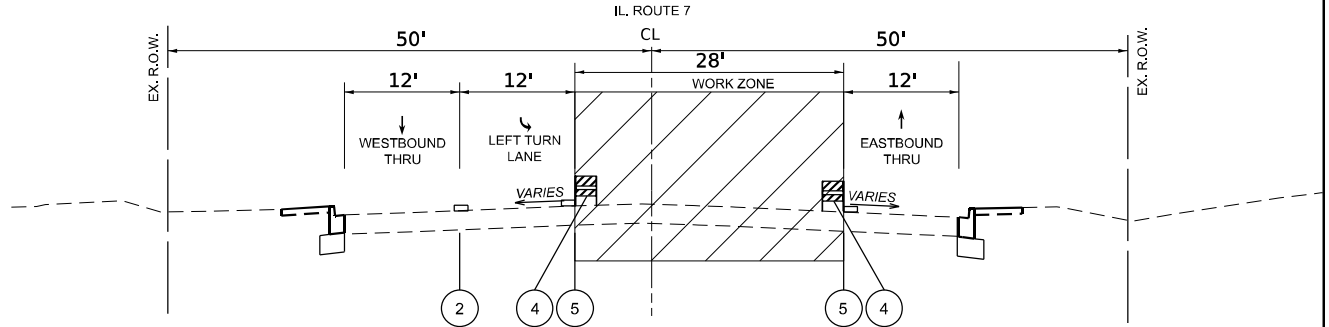
SCALE: NTS SHEET 2 OF 3 SHEETS STA. N/A TO STA. N/A

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	45
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

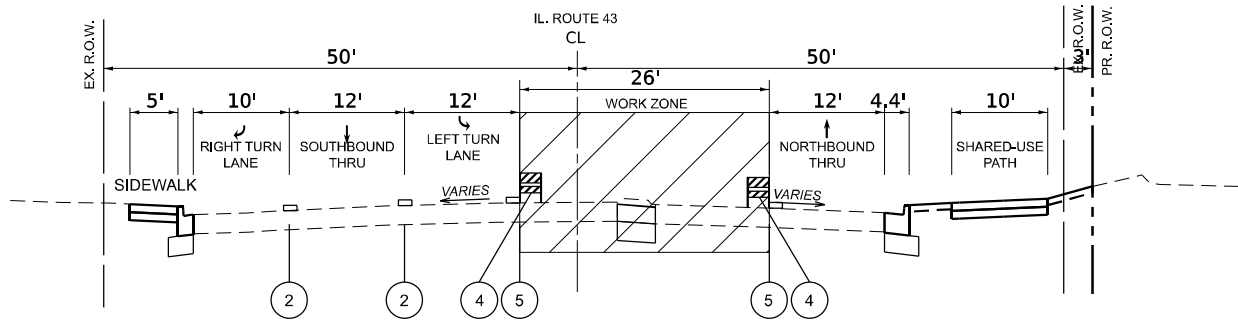
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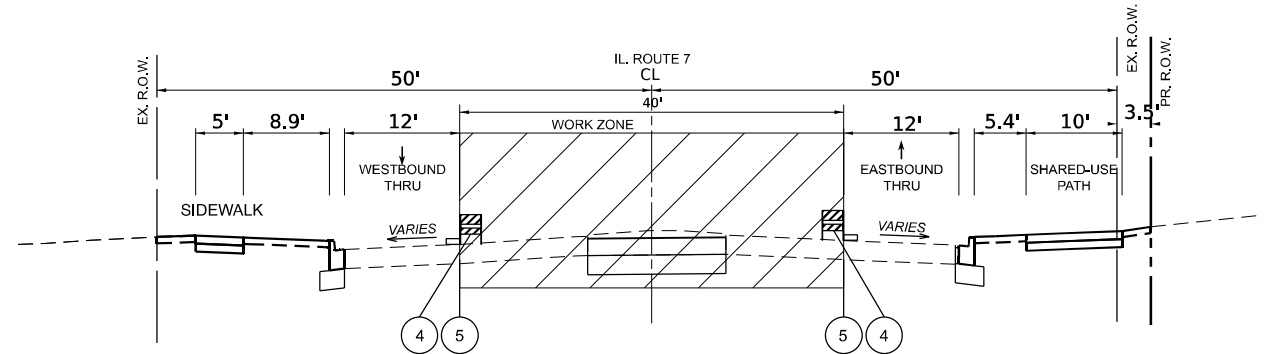
STAGE 3 - IL ROUTE 43 (HARLEM AVENUE)
STA. 43+74.00 TO STA. 50+00.00



STAGE 3 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 14+21.00 TO STA. 20+00.00



STAGE 3 - IL ROUTE 43 (HARLEM AVENUE)
STA. 50+00.00 TO STA. 56+50.00



STAGE 3 - IL ROUTE 7 (SOUTHWEST HIGHWAY)
STA. 20+00.00 TO STA. 25+09.00

LEGEND:

- ① PAVEMENT MARKING, 4" WHITE EDGE LINE - TYPE IV TAPE
- ② PAVEMENT MARKING, 6" WHITE LINE-TYPE IV TAPE
- ③ PAVEMENT MARKING, LINE, 4" WHITE (10' DASH - 30' SKIP) TYPE IV TAPE
- ④ TYPE II BARRICADE OR DRUM
- ⑤ PAVEMENT MARKING, 4" YELLOW EDGE LINE - TYPE IV TAPE

- PAVEMENT MARKING
- WORK ZONE
- DIRECTION OF TRAFFIC
- TYPE II BARRICADE OR DRUM



USER NAME = tnestor	DESIGNED - TN	REVISED -
DRAWN - TN	REVISED -	
PLOT SCALE = 0.16666633' / in.	CHECKED - GJE	REVISED -
PLOT DATE = 11/25/2025	DATE - 10/15/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE III TYPICAL CROSS SECTIONS
IL. ROUTE 7 (SOUTHWES HWY) AT IL. ROUTE 43 (HARLEM AVE)**

SCALE: NTS SHEET 3 OF 3 SHEETS STA. N/A TO STA. N/A

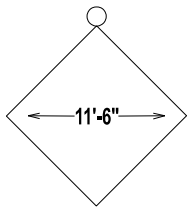
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	46
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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FILE NAME: W\191-200 IDOT_208010_T08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-st-4-MOT SIGNS.dgn

SIGN LEGEND



(A)
W20-I103 (O) - 48



(B)
W12-I102 (O) - 48



(C)*
W21-1 (O) - 48



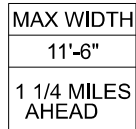
(G)
W20-I103 (O) - 48

W2-I115 (O) - 3618

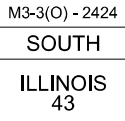
R2-1 - 3648

R10-I108P - 3618

R2-I106P - 3618



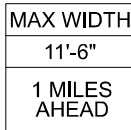
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W12-I103 (O) - 4848



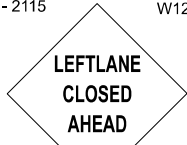
(P)
M1-50 - 2424



(H)
M6-1L (O) - 2115



(J)
W12-I103 - 4848



(Q)
W20-5L (O) - 48



(R)
M6-1 (O) - 2115



(D)
W20-I103 (O) - 48

W2-I115 (O) - 3618

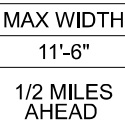
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R10-I108P - 3618

R2-I106P - 3618



(K)
M6-1R (O) - 2115



(L)
W12-I103 (O) - 4848



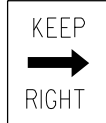
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R3-I100L - 2424

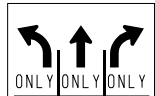
M6-2L - 2115



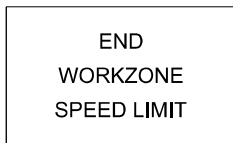
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R3-1 -2424



(U)
R4-7a -2430



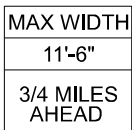
(V)
R3-8B -4860



(E)
G20-I103-6036



(F)
W20-5R (O) - 48



(M)
W12-I103 (O) - 4848

* TO BE REMOVED WHEN WORKERS ARE ABSENT FOR LONGER THEN 1 HOUR.



USER NAME	= tnestor	DESIGNED -	TN	REVISED -	
DRAWN -	TN	REVISED -			
CHECKED -	GJE	REVISED -			
PLOT DATE	= 11/25/2025	DATE -	10/15/2025	REVISED -	

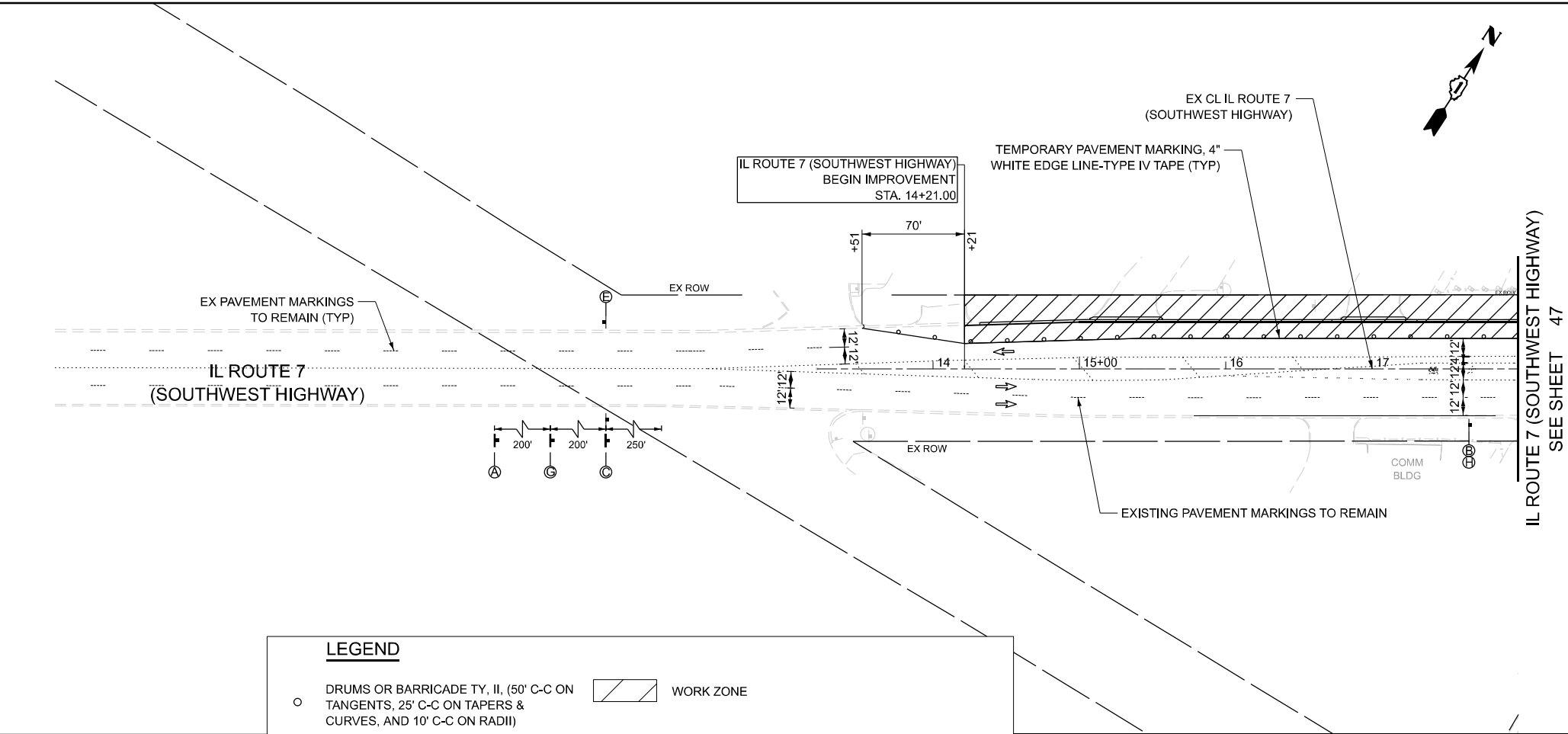
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - GENERAL NOTES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 1 OF 6 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	\$TOT	46A
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

MODEL: SHT 2
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh14-MOT STAGE 1.dgn



LEGEND

DRUMS OR BARRICADE TY, IL, (50' C-C ON TANGENTS, 25' C-C ON TAPERS & CURVES, AND 10' C-C ON RADII)

TEMPORARY TRAFFIC ADVISORY SIGN

DIRECTION OF TRAFFIC

BARRICADE TYPE III W/ FLASHING LIGHTS

WORK ZONE

FLASHING ARROWBOARD

	USER NAME = tnestor	DESIGNED - TN	REVISED -
		DRAWN - TN	REVISED -
		CHECKED - GJE	REVISED -
	PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - STAGE 1

IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

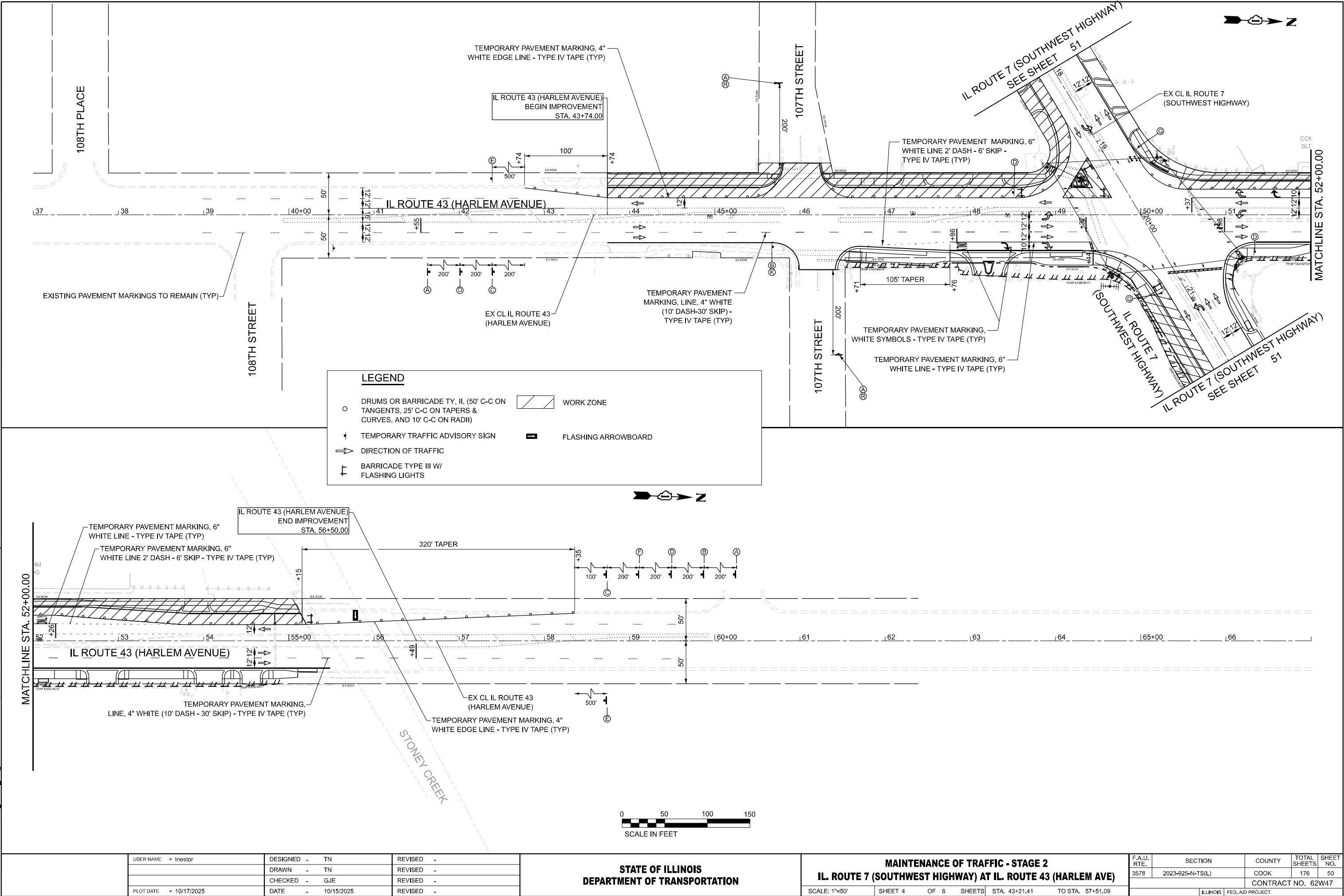
SCALE: 1"=50'

SHEET 2 OF 8 SHEETS

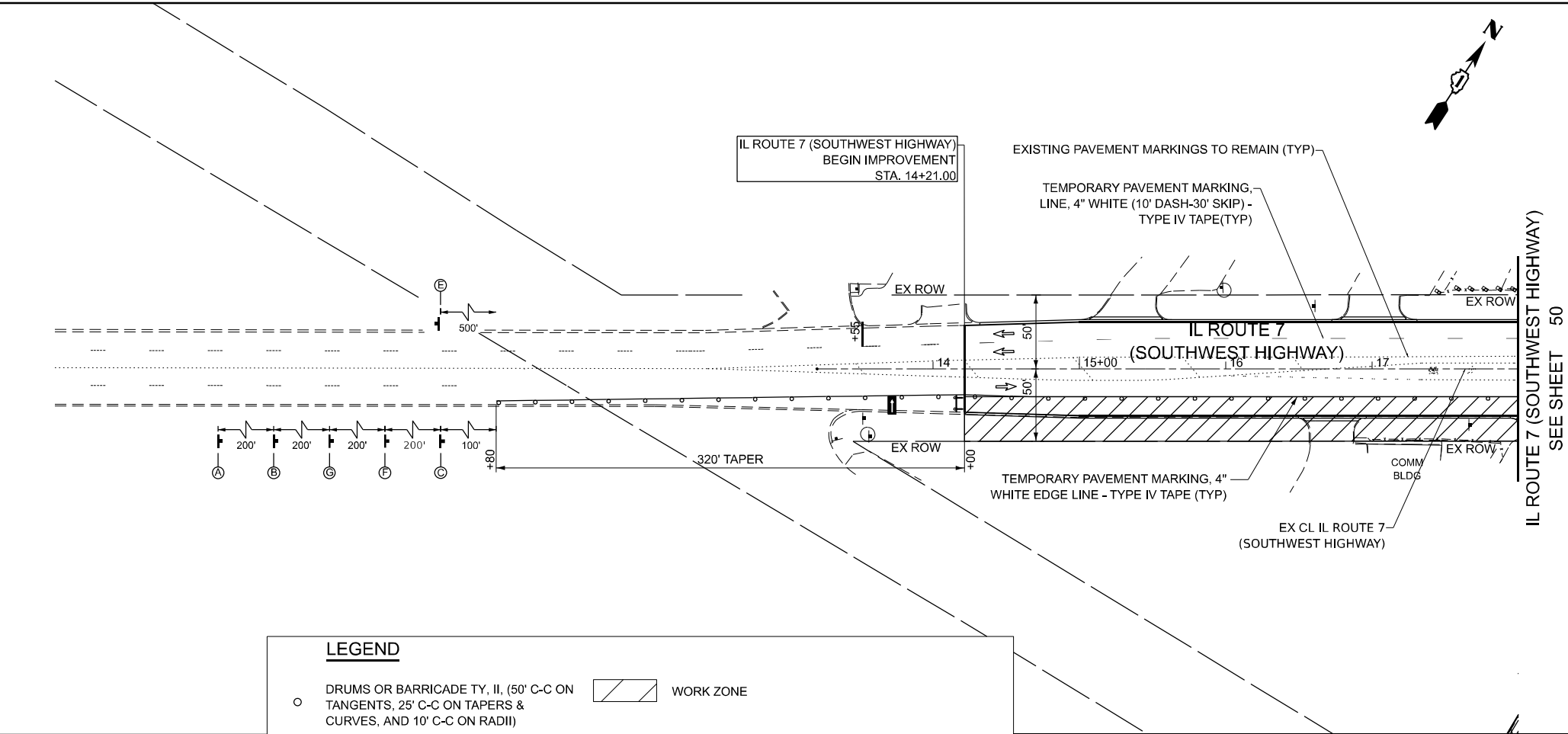
STA. 9+00.00 TO STA. 27+12.89

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	48
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHT 1
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MODEL: SHT 2
FILE NAME: W:\191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh14-MOT STAGE 2.dgn



LEGEND

DRUMS OR BARRICADE TY, IL, (50' C-C ON TANGENTS, 25' C-C ON TAPERS & CURVES, AND 10' C-C ON RADII)

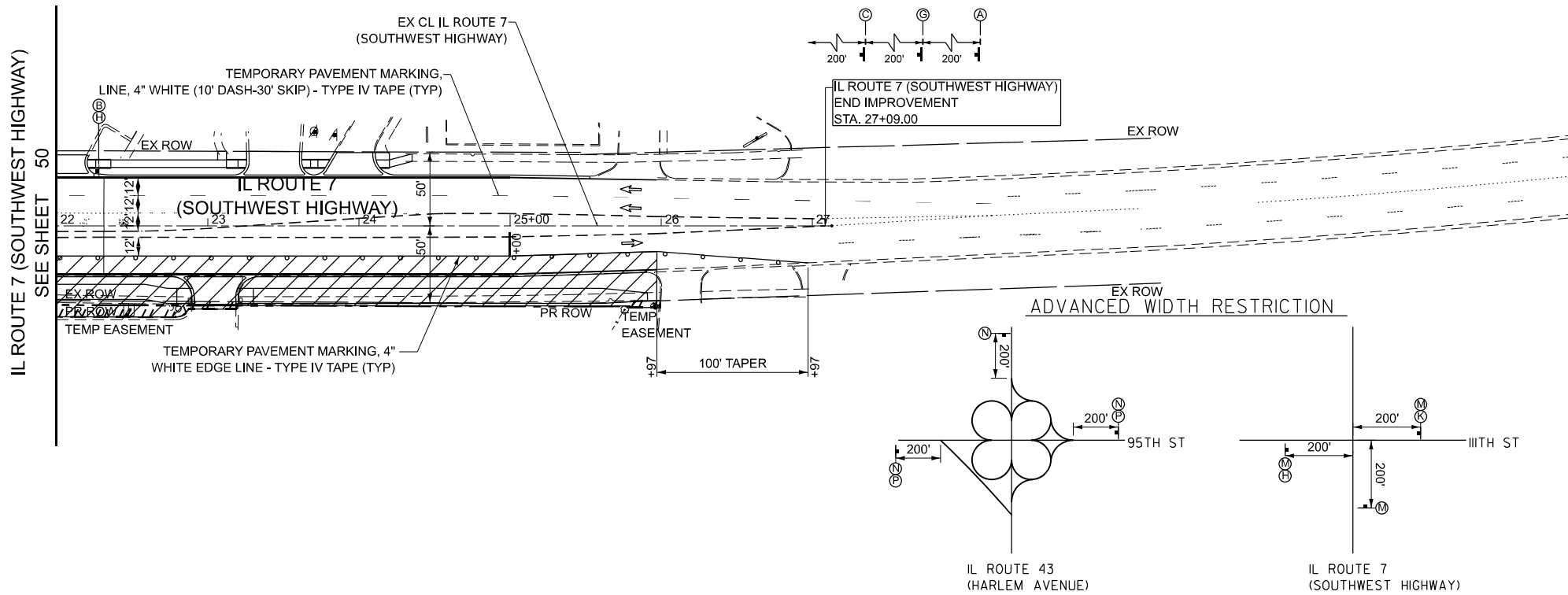
TEMPORARY TRAFFIC ADVISORY SIGN

DIRECTION OF TRAFFIC

BARRICADE TYPE III W/ FLASHING LIGHTS

WORK ZONE

FLASHING ARROWBOARD



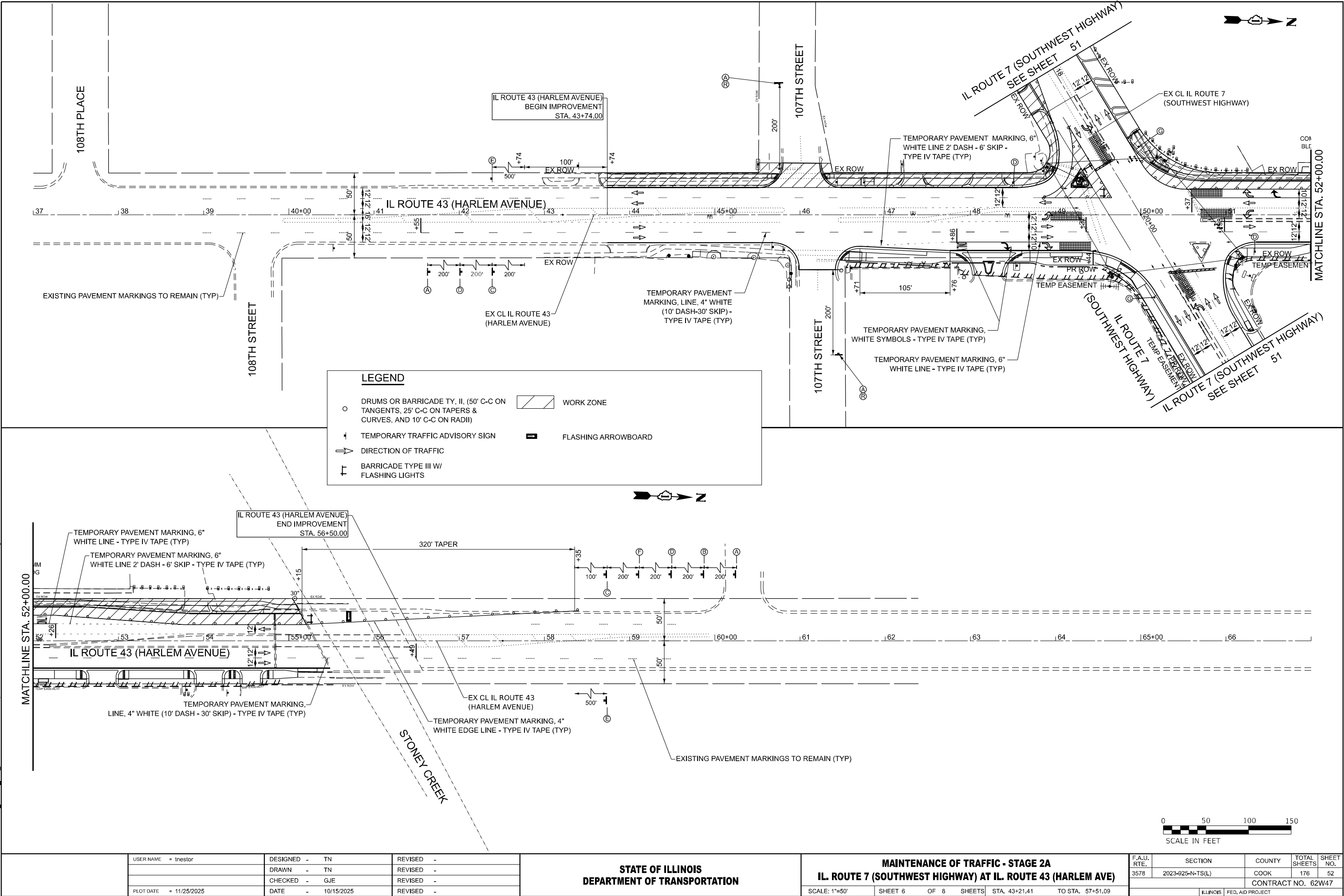
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		DRAWN - TN	REVISED -
		CHECKED - GJE	REVISED -
	DATE - 10/15/2025		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

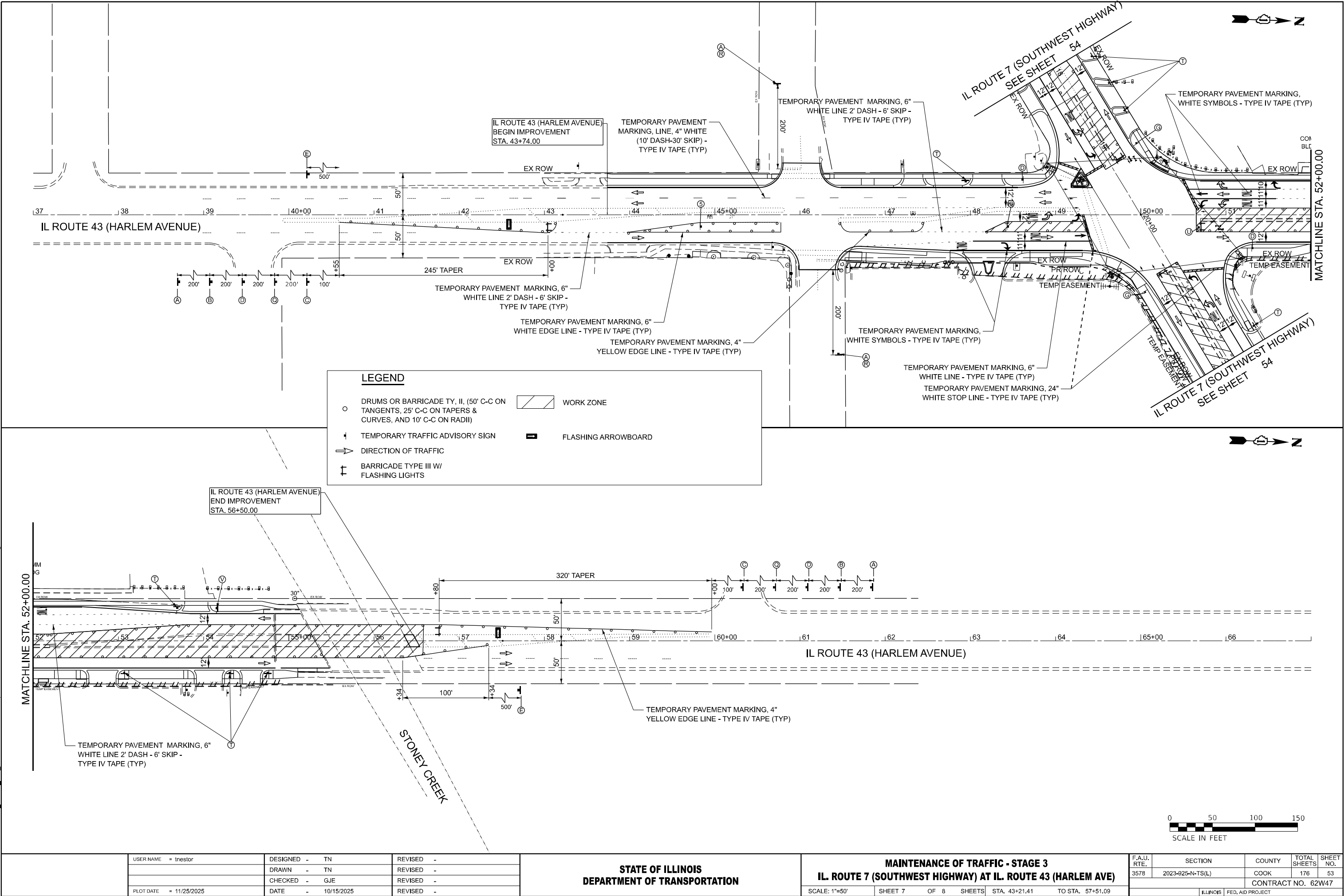
MAINTENANCE OF TRAFFIC - STAGE 2			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=50'	SHEET 5	OF 8 SHEETS	STA. 13+20.80 TO STA. 27+12.89

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	51
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

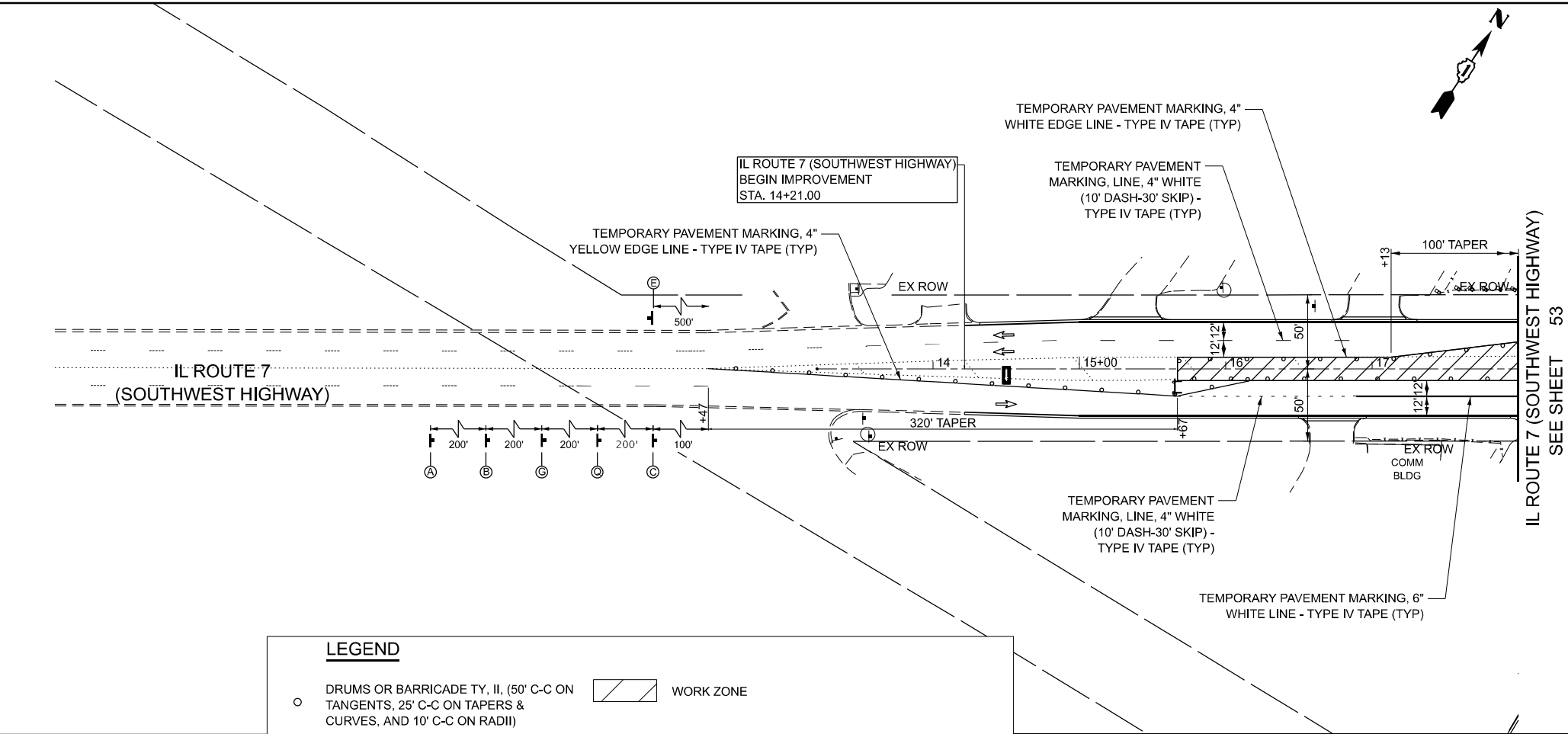
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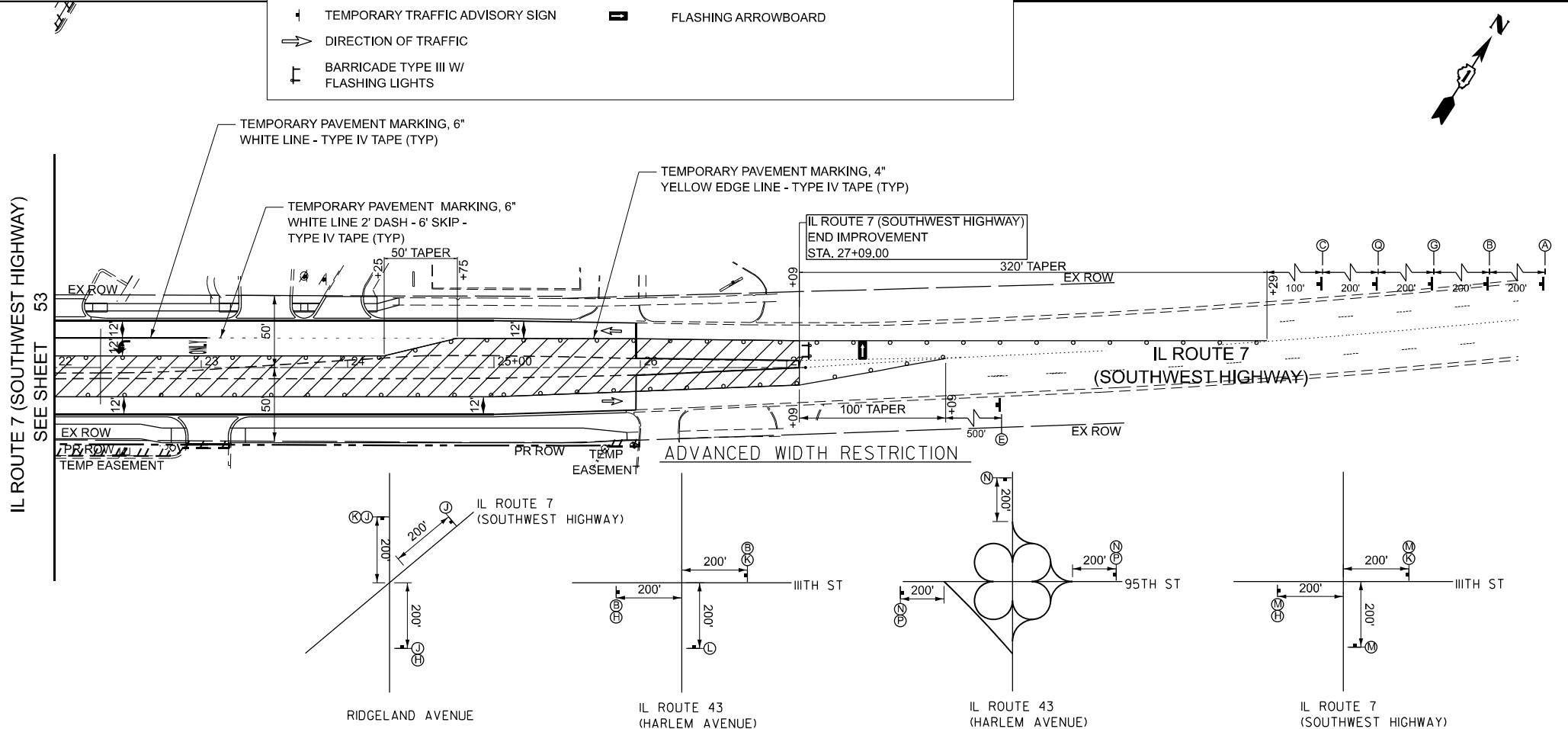


MODEL: SHT 2
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LEGEND

○	DRUMS OR BARRICADE TY, IL, (50' C-C ON TANGENTS, 25' C-C ON TAPERS & CURVES, AND 10' C-C ON RADII)		WORK ZONE
+	TEMPORARY TRAFFIC ADVISORY SIGN		FLASHING ARROWBOARD
→	DIRECTION OF TRAFFIC		
⌋	BARRICADE TYPE III W/ FLASHING LIGHTS		



PLOT DATE = 11/25/2025	USER NAME = tnestor	DESIGNED - TN	REVISED -
		DRAWN - TN	REVISED -
		CHECKED - GJE	REVISED -
	DATE - 10/15/2025		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - STAGE 3			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=50'	SHEET 8	OF 8 SHEETS	STA. 13+20.80 TO STA. 27+12.89

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	54
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL GENERAL NOTES

1. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
2. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTION (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:

A. UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.

B. AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
3. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
4. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-1 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA, PAID FOR AS STABILIZED CONSTRUCTION ENTRANCE.
5. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR DEDISTURBANCE.
6. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
7. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
8. STORM SEWERS THAT ARE/OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
9. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
10. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE IMPROVEMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER. THIS SHALL BE CONSIDERED INCLUDED IN THE COST OF DEWATERING.
11. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
12. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER THE FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
13. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURE MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

IL ROUTE 43 (HARLEM AVENUE) - GENERAL SEQUENCE OF EROSION CONTROL

STAGE 1

1. INSTALL NECESSARY TRAFFIC CONTROL DEVICES AND BEGIN STAGE 1 TRAFFIC CONFIGURATION.
2. INSTALL PERIMETER SILT FENCE AS SHOWN IN THE PLANS OR DIRECTED BY THE ENGINEER.
3. INSTALL EROSION AND SEDIMENT CONTROL DEVICES AS CALLED FOR STAGE 1.
4. PERFORM ANY TREE REMOVAL OR CLEARING NEEDED FOR PROJECT.
5. REMOVE EXISTING CURB AND GUTTER AND INSTALL PROPOSED CURB AND GUTTER AS CALLED FOR STAGE 1 TRAFFIC.
6. INSTALL INLET PROTECTION IN NEW INLETS AND CATCH BASINS. TEMPORARY SEED AS NECESSARY.
7. PROVIDE PERMANENT LANDSCAPING ITEMS.

STAGE 2

1. INSTALL NECESSARY TRAFFIC CONTROL DEVICES AND BEGIN STAGE 2 TRAFFIC CONFIGURATION.
2. RETAIN AND MAINTAIN DEVICES FROM PREVIOUS STAGES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. INSTALL PERIMETER SILT FENCE AS SHOWN IN THE PLANS OR DIRECTED BY THE ENGINEER.
4. INSTALL EROSION AND SEDIMENT CONTROL DEVICES AS CALLED FOR STAGE 2.
5. REMOVE EXISTING PAVEMENT AND INSTALL PROPOSED PAVEMENT AS CALLED FOR STAGE 2 TRAFFIC.
6. INSTALL INLET FILTERS IN NEW INLETS AND CATCH BASINS. TEMPORARY SEED AS NECESSARY.
7. PROVIDE PERMANENT LANDSCAPING ITEMS.

STAGE 3

1. INSTALL NECESSARY TRAFFIC CONTROL DEVICES AND BEGIN STAGE 2 TRAFFIC CONFIGURATION.
2. RETAIN AND MAINTAIN DEVICES FROM PREVIOUS STAGES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. INSTALL PERIMETER SILT FENCE AS SHOWN IN THE PLANS OR DIRECTED BY THE ENGINEER.
4. INSTALL EROSION AND SEDIMENT CONTROL DEVICES AS CALLED FOR STAGE 3.
5. REMOVE EXISTING PAVEMENT AND INSTALL PROPOSED PAVEMENT AS CALLED FOR STAGE 3 TRAFFIC.
6. INSTALL INLET FILTERS IN NEW INLETS AND CATCH BASINS. SEED AS NECESSARY.
7. PERFORM ONGOING MAINTENANCE ON EROSION CONTROL DEVICES.
8. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER STABILIZATION OF DISTURBED GROUND.

MODEL: Unnamed Plan-1 - Plan 2 (Sheet)
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	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL EROSION AND SEDIMENT CONTROL SEQUENCE AND GENERAL NOTES
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: NTS SHEET 1 OF 3 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	55
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

MODEL: SHT 1
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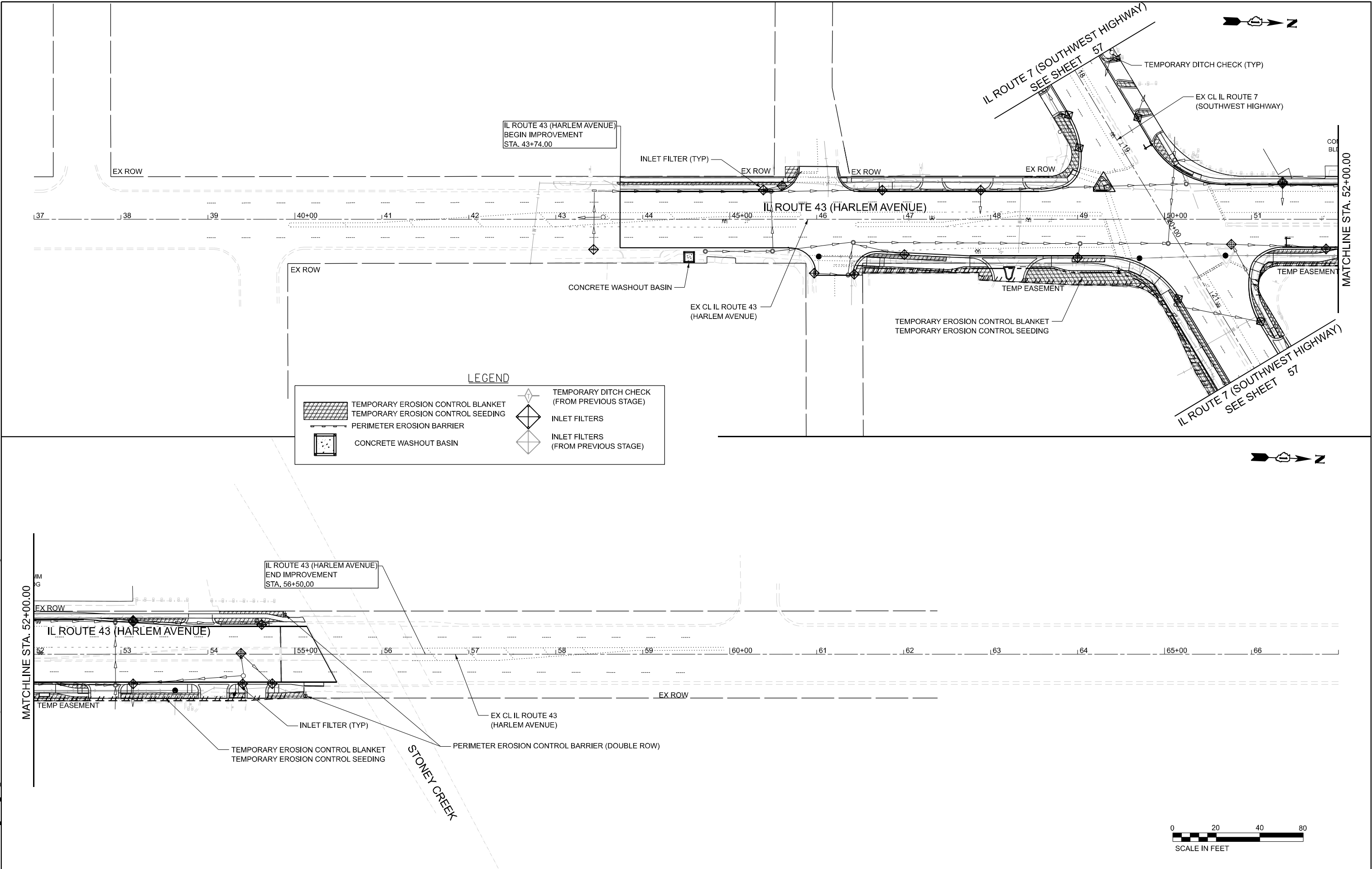


USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL EROSION AND SEDIMENT CONTROL PLAN			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1"=20'	SHEET 2	OF 3 SHEETS	STA. 43+21.41 TO STA. 57+51.09

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	56
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



MODEL: SHT 2
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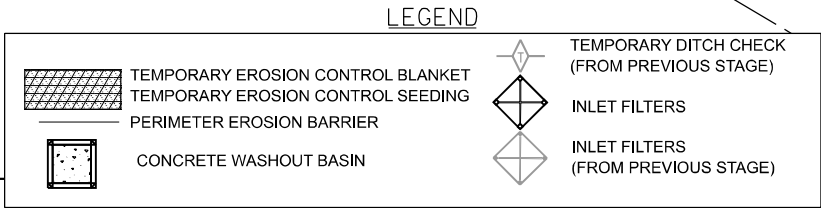
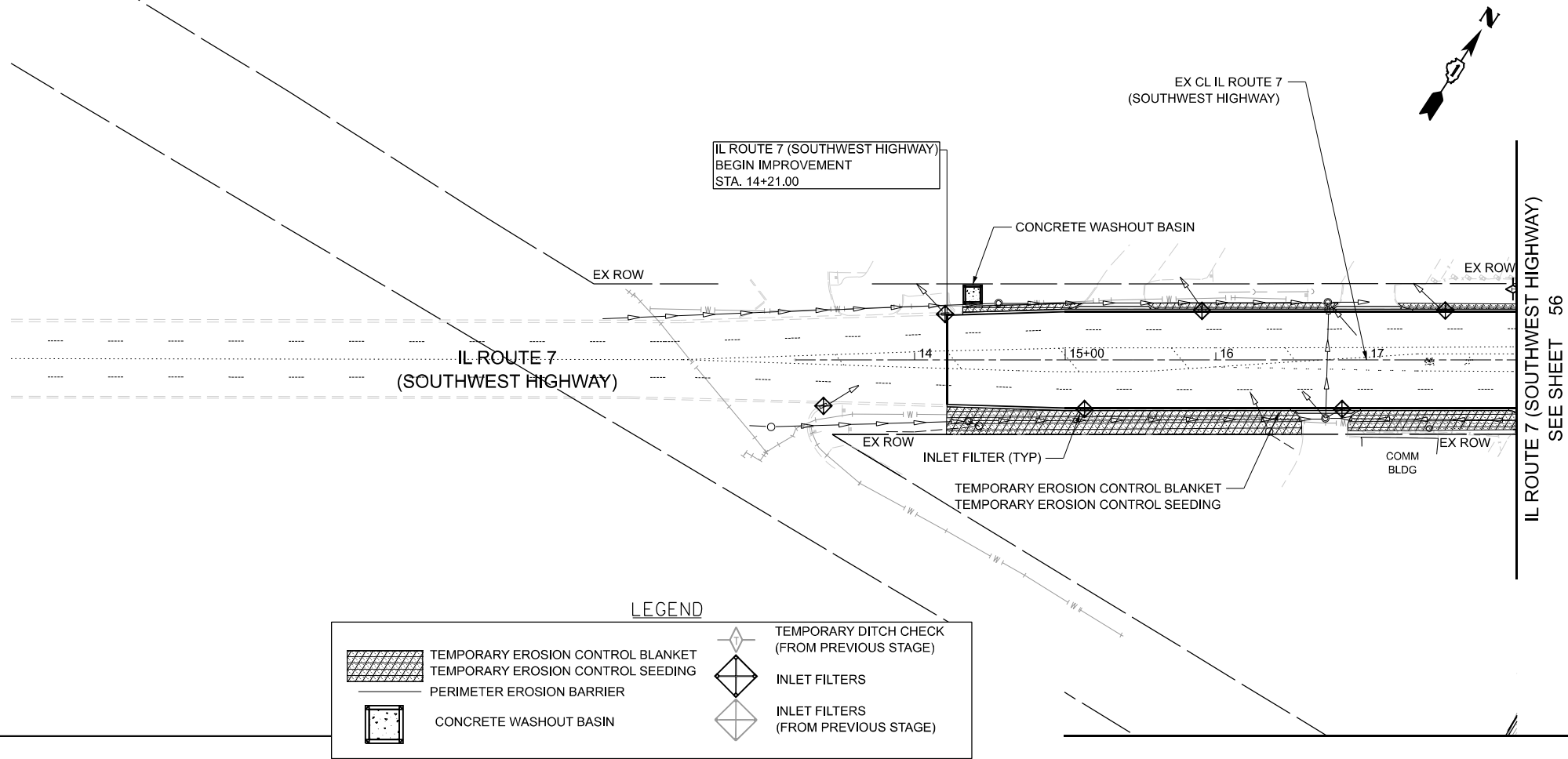
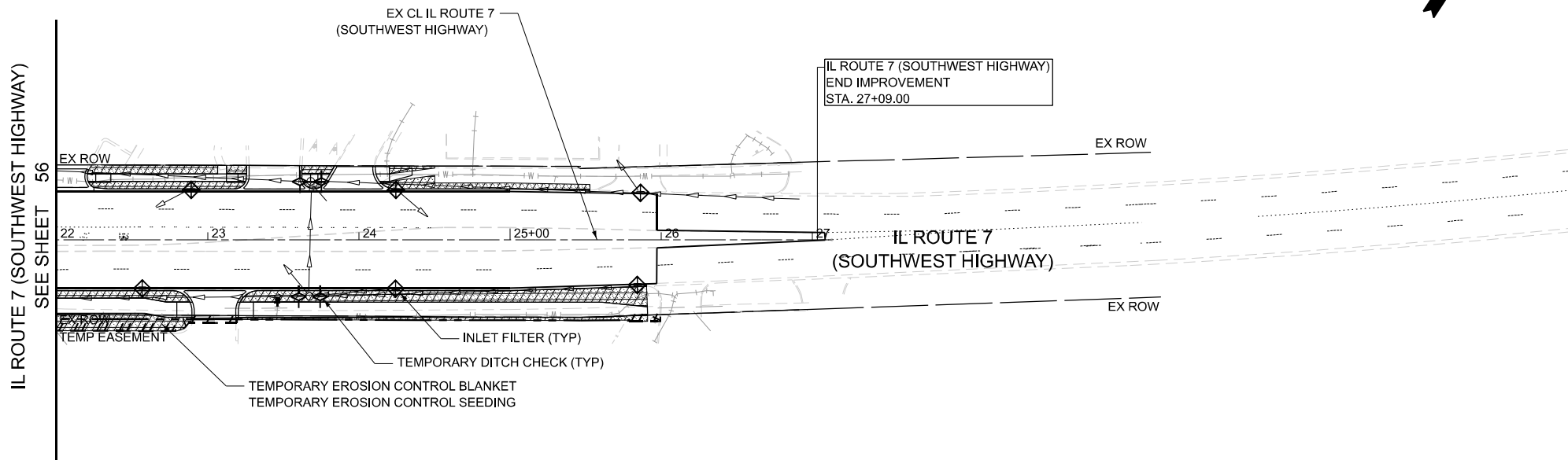


USER NAME = tnestor	DESIGNED - TN	REVISED -
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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

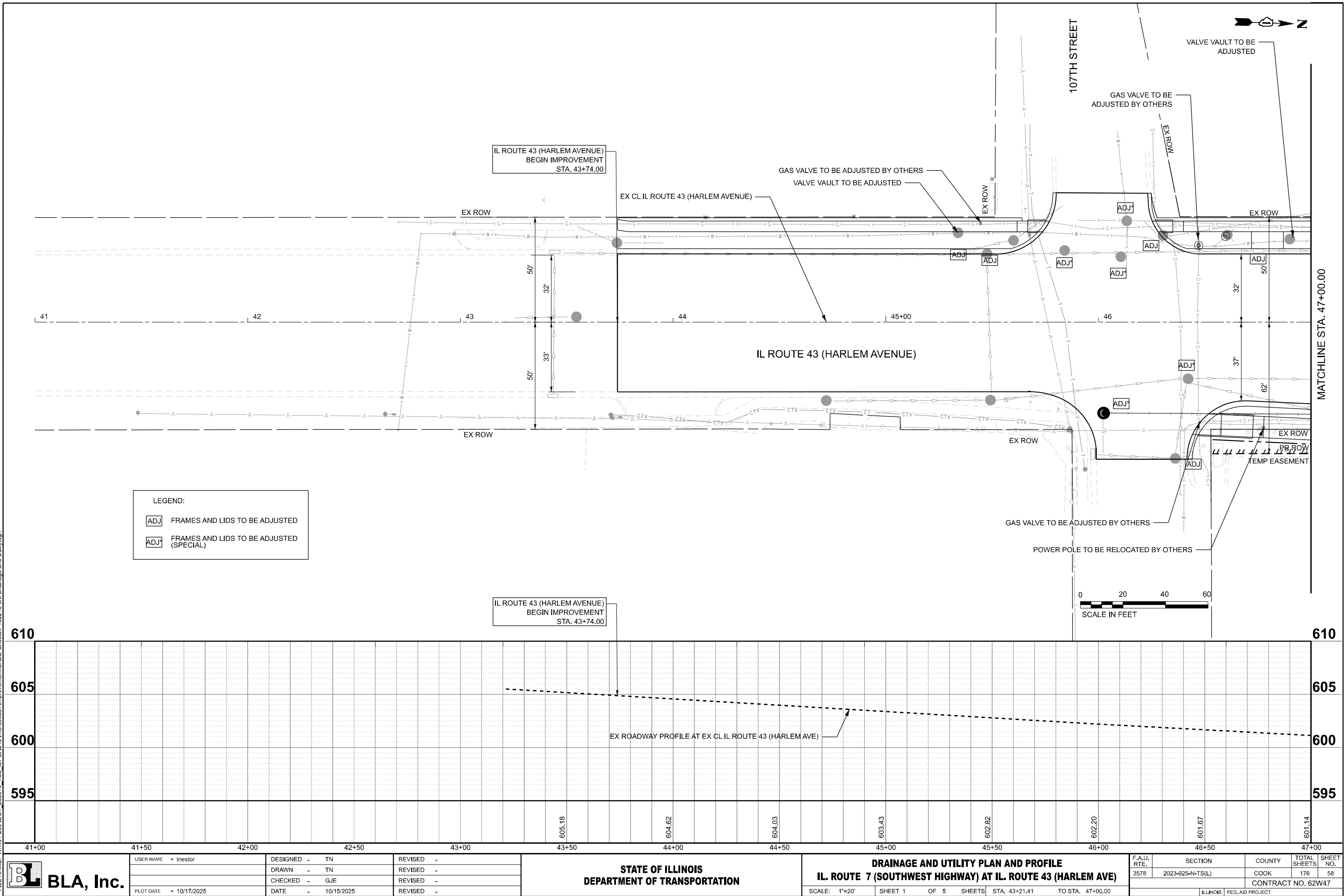
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL EROSION AND SEDIMENT CONTROL PLAN			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=20'	SHEET 3	OF 3 SHEETS	STA. 13+20.80 TO STA. 27+12.89

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	57
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

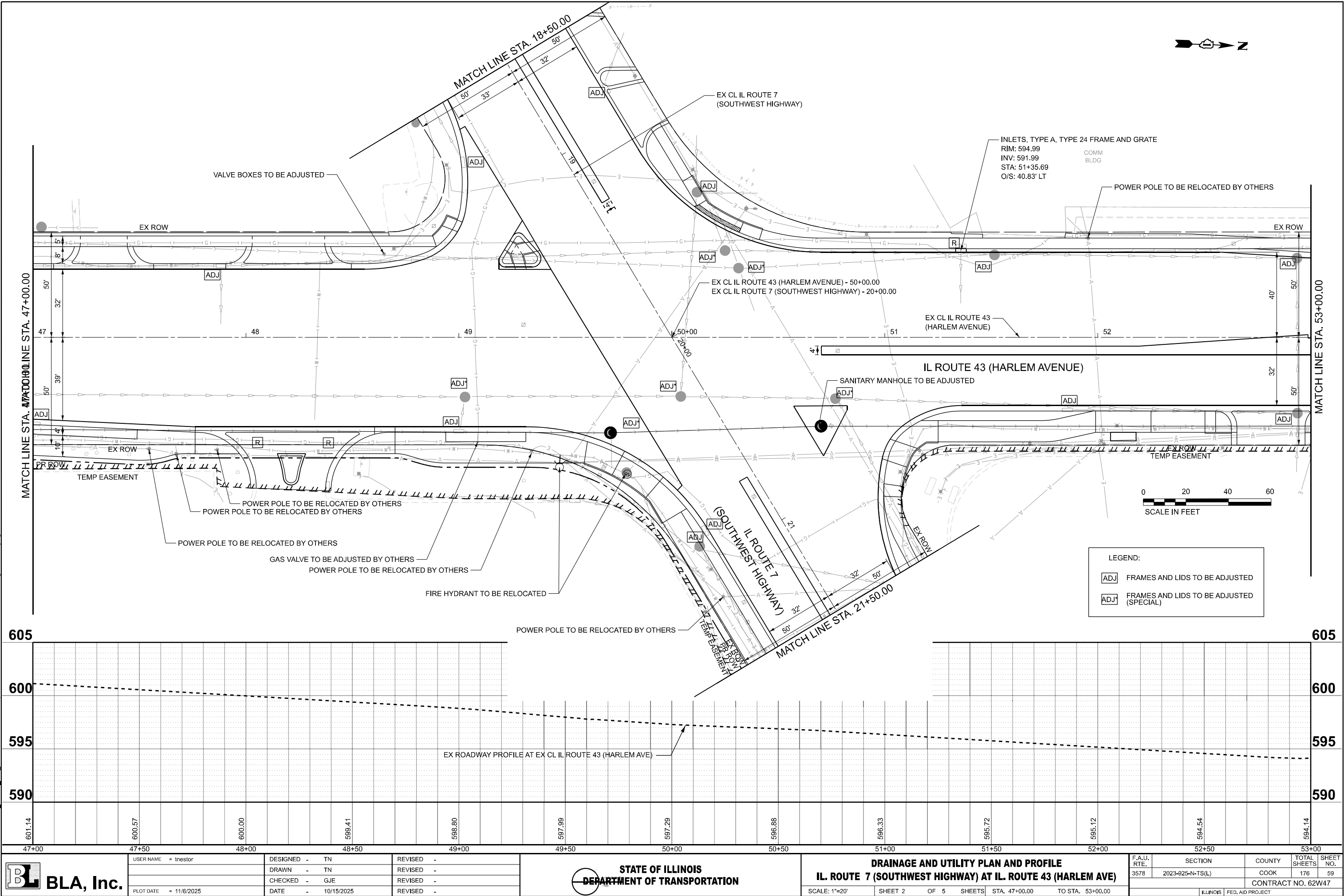


MODEL: SHT 1
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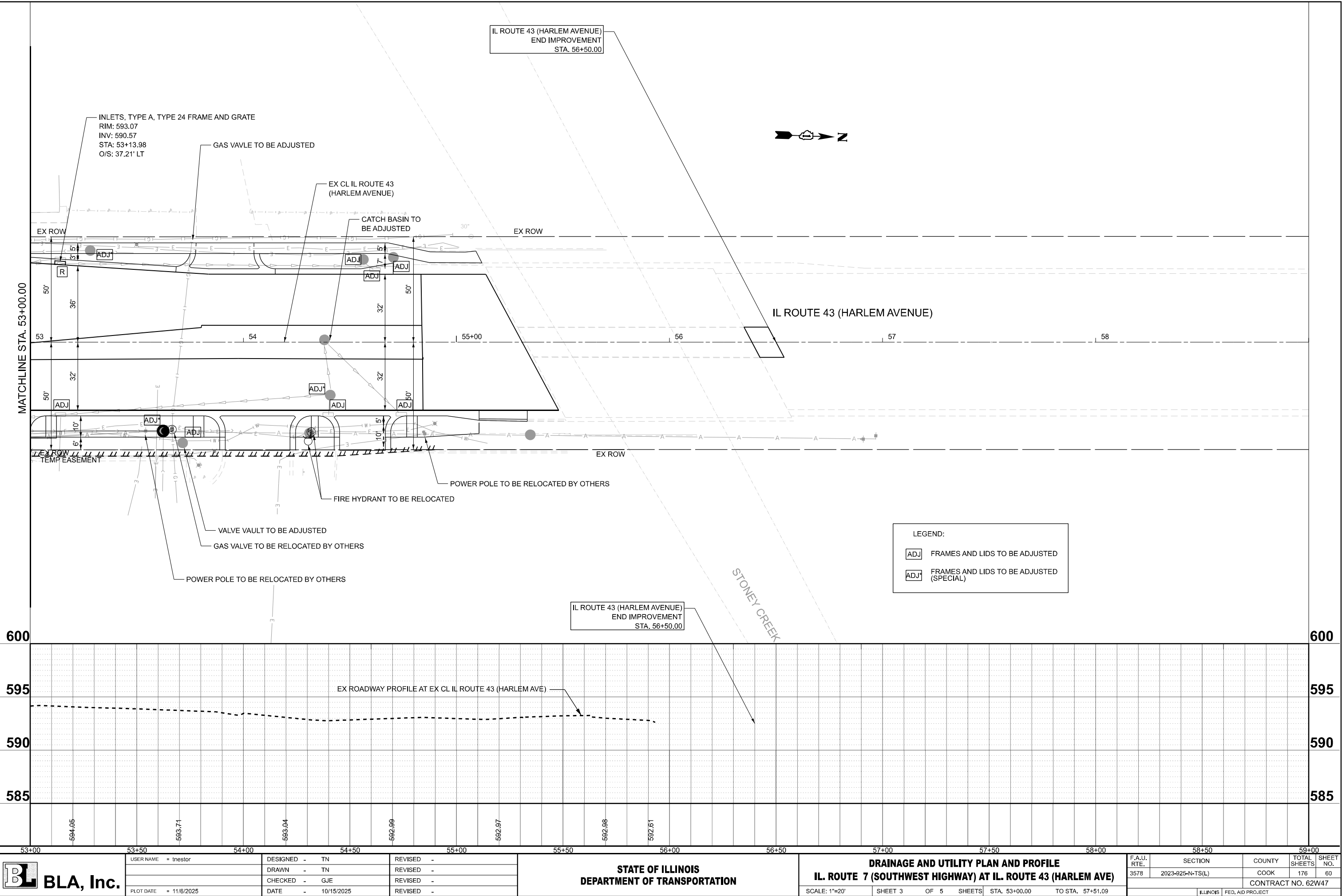
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

MODEL: SHT 2
FILE NAME: W:\19+200 IDOT_208010_108_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-shd-Drainage and Utility.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/6/2025	DATE - 10/15/2025	REVISED -

MODEL: SHT 3
FILE NAME: W:\191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh+Drainage and Utility.dgn



USER NAME = tnestor
PLOT DATE = 11/6/2025

DESIGNED - TN
DRAWN - TN
CHECKED - GJE
DATE - 10/15/2025

REVISED -
REVISED -
REVISED -
REVISED -

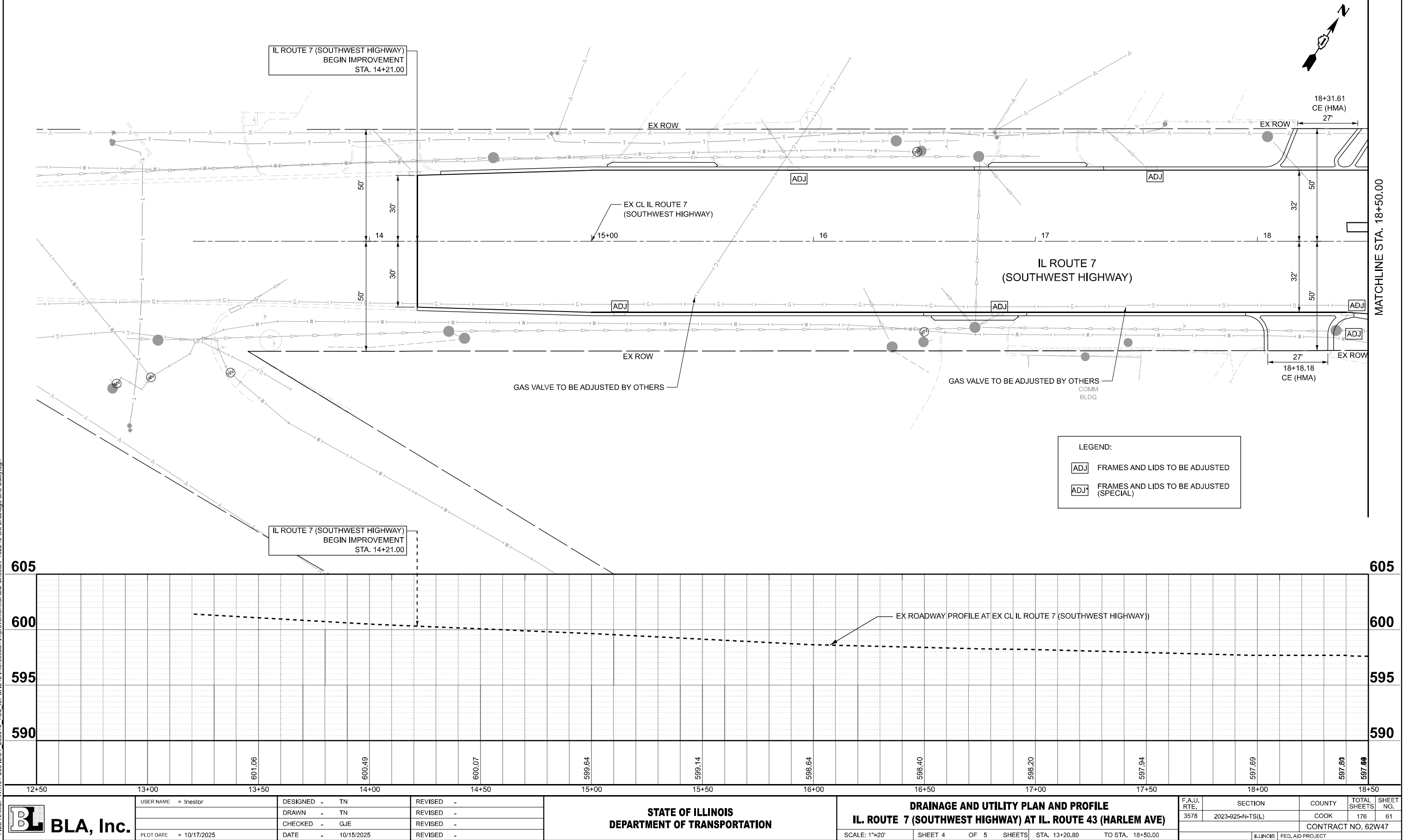
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITY PLAN AND PROFILE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

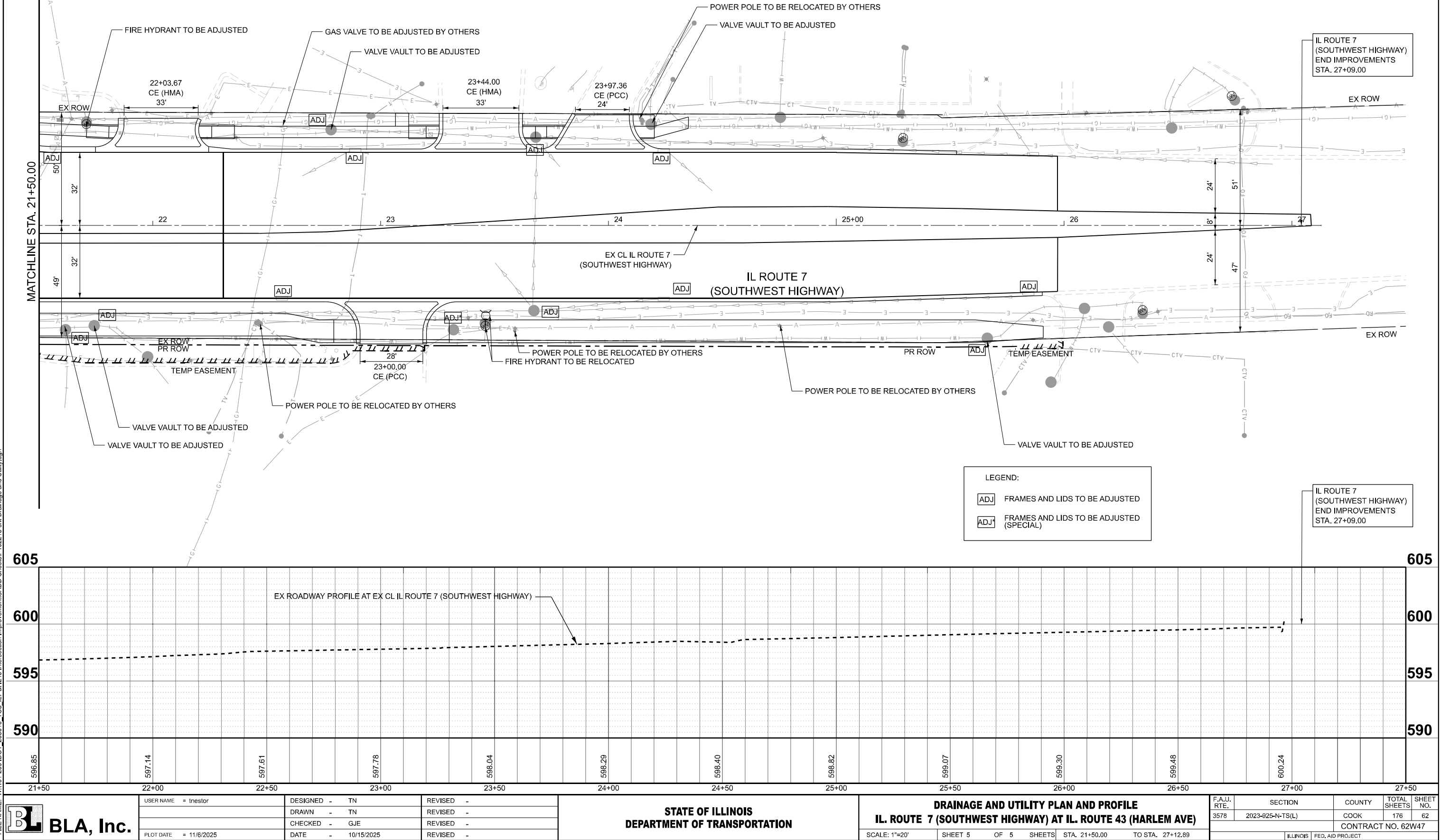
SCALE: 1"=20' SHEET 3 OF 5 SHEETS STA. 53+00.00 TO STA. 57+51.09

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	60
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHT 4
FILE NAME: W191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-sh+Drainage and Utility.dgn



MODEL: SHT 5
FILE NAME: W:\191-200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-shd-Drainage and Utility.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/6/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITY PLAN AND PROFILE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 5 OF 5 SHEETS STA. 21+50.00 TO STA. 27+12.89

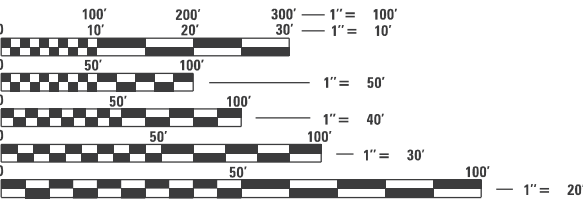
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	62
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED
FEDERAL AID HIGHWAY

ILLINOIS ROUTE 7 (SOUTHWEST HIGHWAY)
SECTION
PROJECT R-90-007-20
TYPE of IMPROVEMENT
COOK COUNTY

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
ONS0001 ONS0001TE	MAHA PROPERTIES, LLC	2	
ONS0002 ONS0002TE	LGP REALTY HOLDINGS LP, A DELAWARE LIMITED PARTNERSHIP	4	
ONS0003 ONS0003TE	A TO Z INVESTMENT ENTERPRISE, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	5	
ONS0004TE	10605 SOUTHWEST HIGHWAY CORPORATION, AS SUCCESSOR TO ORCHARD ESTATE CONDOMINIUMS UNIT 3 AS THE ASSOCIATION ON BEHALF OF THE UNIT OWNERS AS THEIR INTERESTS MAY APPEAR	5	
ONS0005TE	CHICAGO TRUST COMPANY N.A. SUCCESSOR TO SUBURBAN BANK & TRUST COMPANY SUCCESSOR TO SOUTHWEST FINANCIAL BANK & TRUST AS TRUSTEE UNDER TRUST AGREEMENT DATED AUGUST 4, 1978 KNOWN AS TRUST NO. 1-0063	6	
ONS0006TE	JOHN H. SANTOS AND MARY C. SANTOS, AS TENANTS IN COMMON	6	

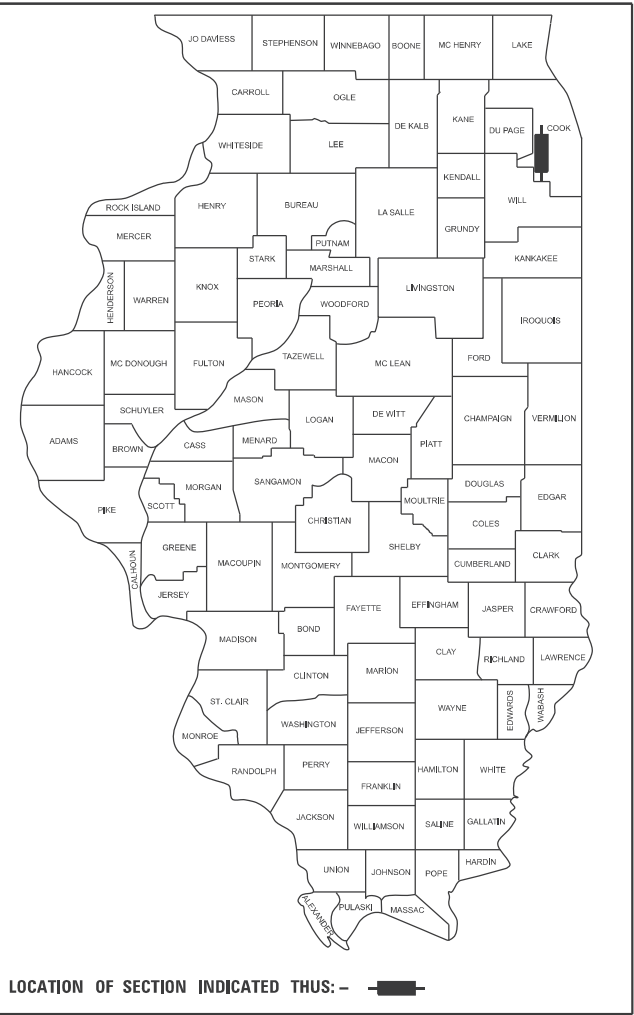
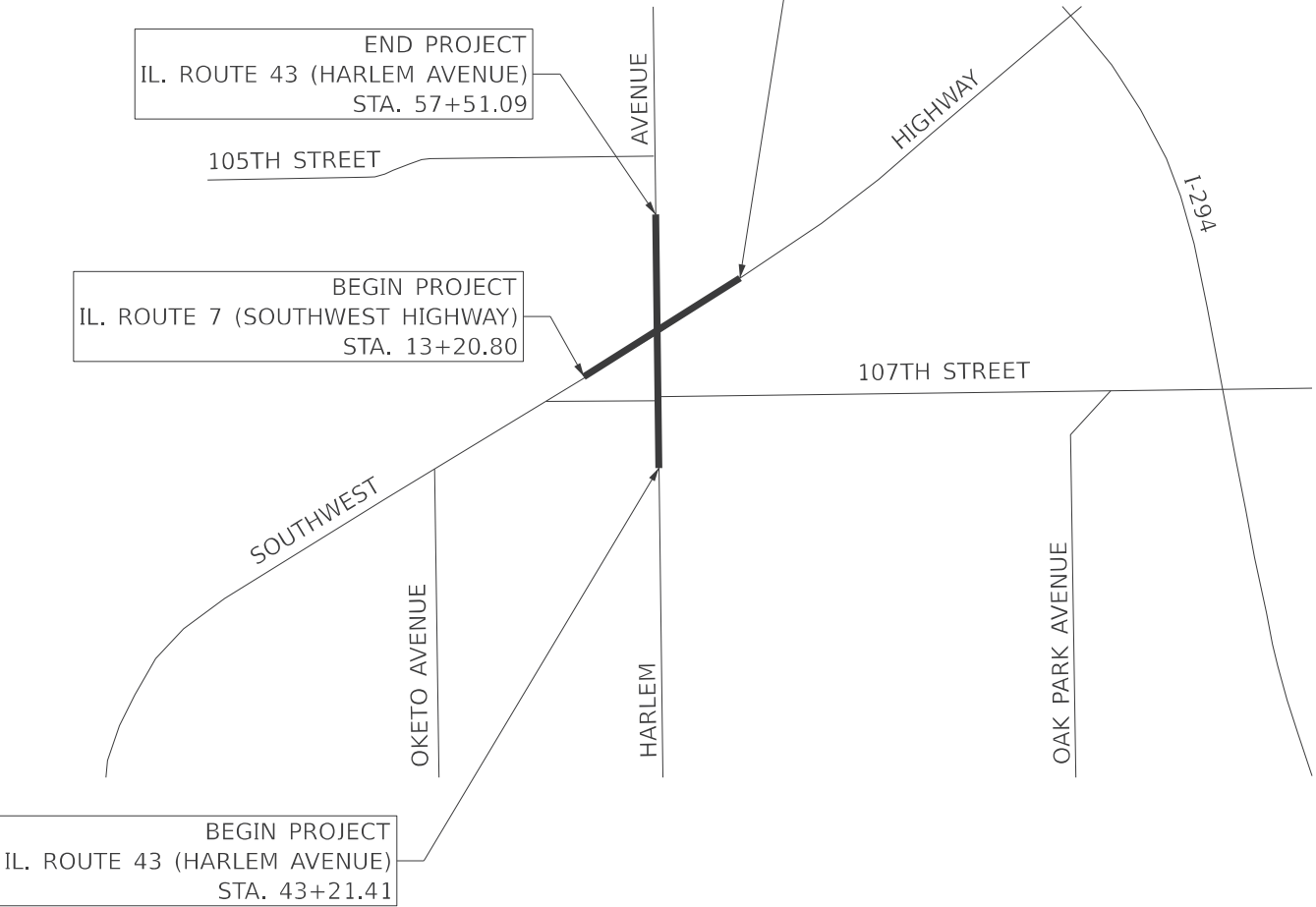


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER
PROJECT MANAGER

CONTRACT NO.



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED _____ 20 _____

REGIONAL ENGINEER

20 _____

ENGINEER OF DESIGN AND ENVIRONMENT

20 _____

DIRECTOR OF PROGRAM DEVELOPMENT

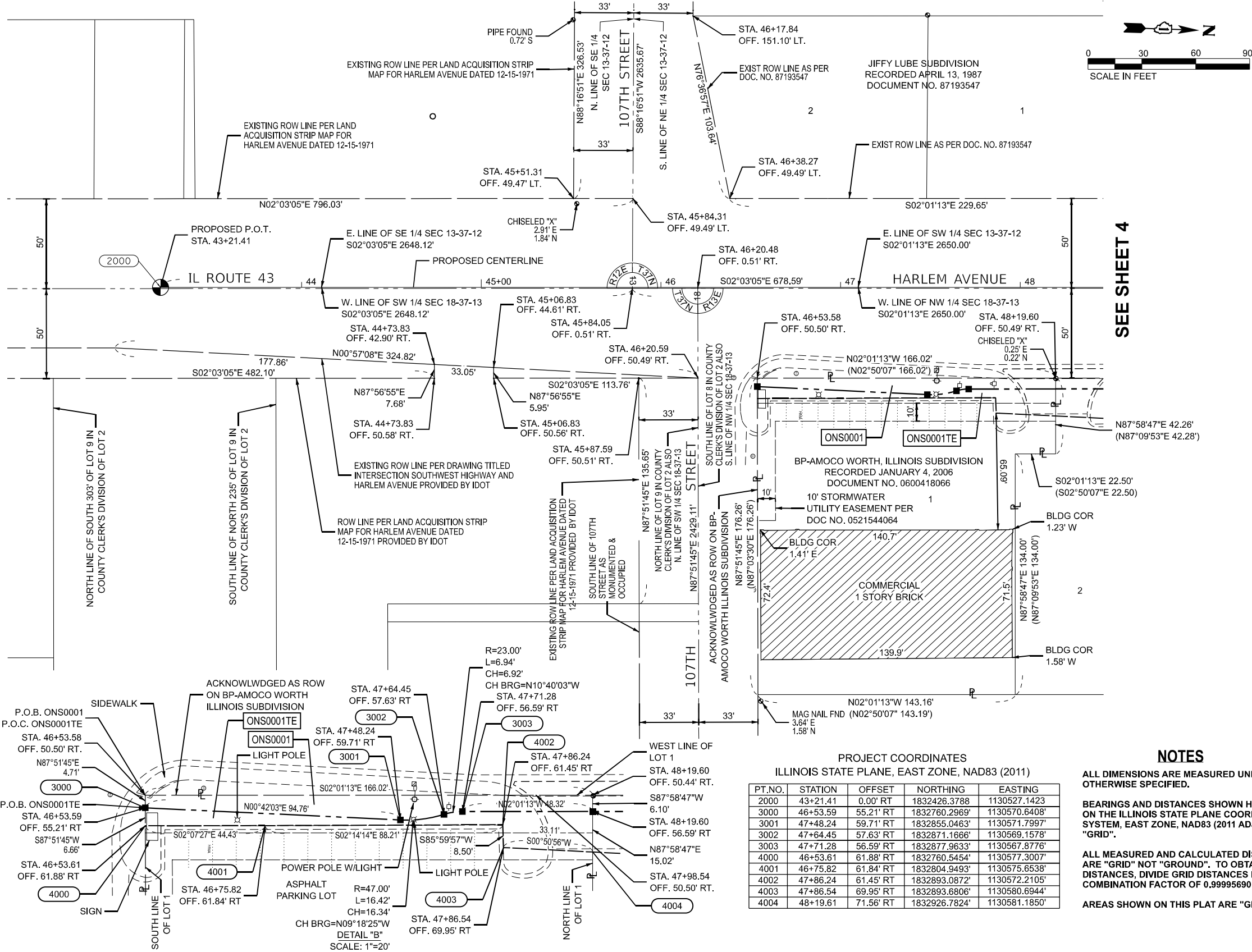
PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

APPROVED
By Doug.Horn at 12:09 pm, Feb 24, 2025

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	6	1
		ILLINOIS	CONTRACT NO.	

PART OF SECTION 13, TWP. 37 N., R. 12 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.
PART OF SECTION 18, TWP. 37 N., R. 13 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NO.	TOTAL HOLDING	FEE SIMPLE ACQUISITION						REMAINDER	EASEMENTS			PARCEL INDEX NUMBER
		GROSS		PREVIOUSLY DEDICATED		NET			PE = PERMANENT TE = TEMPORARY		EASEMENT PURPOSE	
	ACRES	ACRES	SQ FT	ACRES	SQ FT	ACRES	SQ FT	ACRES	ACRES	SQ FT		
ONS0001 ONS0001TE	0.602 1	0.026	1,138					0.576	TE=0.024	TE=1,024		24-18-101-118-0000



SEE SHEET 4

RIGHT-OF-WAY LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT-OF-WAY LINE
- FORMER RIGHT-OF-WAY LINE
- EXISTING IDOT EASEMENT LINE
- EXISTING EASEMENT LINE
- BUILDING SETBACK LINE
- EXISTING ACCESS CONTROL LINE
- EXISTING RIGHT-OF-WAY & PROPOSED ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT-OF-WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY LINE (TITLE)
- RECORDED PLAT/DEED LINE
- APPARENT PROPERTY LINE
- SAME OWNERSHIP
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS

- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS, SET 5/8 INCH METAL ROD WITH DEPARTMENT OF TRANSPORTATION SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.
- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS IN CULTIVATED AREA, A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE, SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

STATE OF ILLINOIS)
COUNTY OF COOK)

I, BRADLEY K. LUEDERS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, STATE THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED _____

BRADLEY K. LUEDERS, PLS NO. 035-003406
LICENSE EXPIRATION DATE: 11/30/2024



APPROVED
By Doug Horn at 12:09 pm, Feb 24, 2025

NOTES

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.

BEARINGS AND DISTANCES SHOWN HEREON ARE ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD83 (2011 ADJUSTMENT) "GRID".

ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES BY COMBINATION FACTOR OF 0.999956901.

AREAS SHOWN ON THIS PLAT ARE "GROUND".

PROJECT COORDINATES ILLINOIS STATE PLANE, EAST ZONE, NAD83 (2011)

PT.NO.	STATION	OFFSET	NORTHING	EASTING
2000	43+21.41	0.00' RT	1832426.3788	1130527.1423
3000	46+53.59	55.21' RT	1832760.2969	1130570.6408
3001	47+48.24	59.71' RT	1832855.0463	1130571.7997
3002	47+64.45	57.63' RT	1832871.1666	1130569.1578
3003	47+71.28	56.59' RT	1832877.9633	1130567.8776
4000	46+53.61	61.88' RT	1832760.5454	1130577.3007
4001	46+75.82	61.84' RT	1832804.9493	1130575.6538
4002	47+86.24	61.45' RT	1832893.0872	1130572.2105
4003	47+86.54	69.95' RT	1832893.6806	1130580.6944
4004	48+19.61	71.56' RT	1832926.7824	1130581.1850

ILLINOIS DEPARTMENT OF TRANSPORTATION PLAT OF HIGHWAYS

ROUTE 7

SEC

SCALE: 1"=30'

SECTION 18

T 37, R 13E OF 3RD P.M.

SHEET NO. 2 OF 6 SHEETS

COUNTY COOK

JOB# R-90-007-20

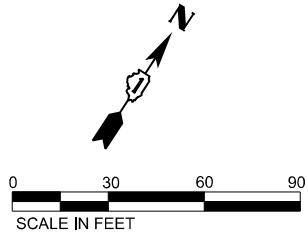
PROJECT# ILLINOIS ROUTE 7 AT ILLINOIS ROUTE 43

STA 43+21.41 TO STA 48+19.61 CONTRACT NO.



USER NAME	= kmuhr
PLOT SCALE	= 0.08333317" / in.
PLOT DATE	= 8/23/2023

DESIGNED	-	REVISED	-
DRAWN	- KWM	REVISED	-
CHECKED	- BKL	REVISED	-
DATE	-	REVISED	-



RIGHT-OF-WAY LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT-OF-WAY LINE
- FORMER RIGHT-OF-WAY LINE
- EXISTING IDOT EASEMENT LINE
- EXISTING EASEMENT LINE
- BUILDING SETBACK LINE
- EXISTING ACCESS CONTROL LINE
- EXISTING RIGHT-OF-WAY & PROPOSED ACCESS CONTROL LINE
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- PROPOSED RIGHT-OF-WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY LINE (TITLE)
- RECORDED PLAT/DEED LINE
- APPARENT PROPERTY LINE
- SAME OWNERSHIP
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS

- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DEPARTMENT OF TRANSPORTATION SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.
- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS IN CULTIVATED AREA, A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

STATE OF ILLINOIS)
COUNTY OF COOK)

I, BRADLEY K. LUEDERS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, STATE THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED _____

BRADLEY K. LUEDERS, PLS NO. 035-003406
LICENSE EXPIRATION DATE: 11/30/2024



APPROVED
By Doug Horn at 12:09 pm, Feb 24, 2025

NOTES

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.

BEARINGS AND DISTANCES SHOWN HEREON ARE ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD83 (2011 ADJUSTMENT) "GRID".

ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES BY COMBINATION FACTOR OF 0.999956901.

AREAS SHOWN ON THIS PLAT ARE "GROUND".

PROJECT COORDINATES
ILLINOIS STATE PLANE, EAST ZONE, NAD83 (2011)

PT.NO.	STATION	OFFSET	NORTHING	EASTING
2001	13+20.80	0.00' RT	1833104.5441	1130503.2776

SEE SHEET 4

MODEL: Sheet 02 (Sheet 1)
FILE NAME: N:\PROJECTS\2021\12-14-2021\DOT_WORTH_CHICAGO_FUDGE_HARLEM_AVENUE_IL_ROUTE_7\MICROSTATIONS\21-00882_06-Sheet 02.dgn



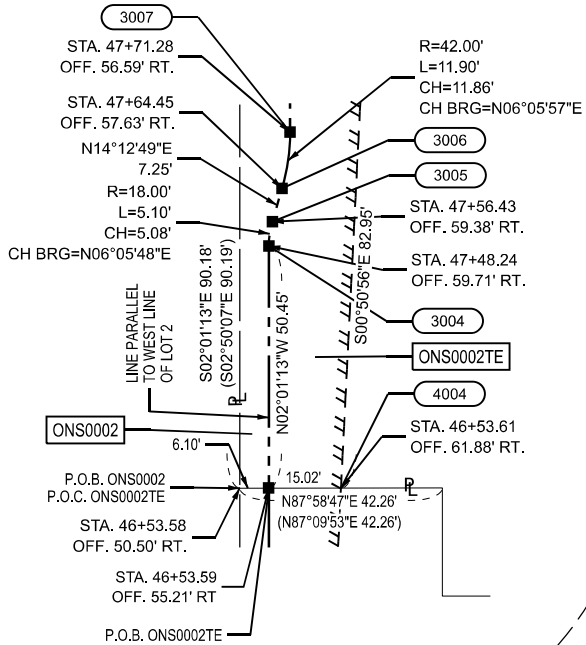
USER NAME = kmuhr	DESIGNED -	REVISED -
	DRAWN - KWM	REVISED -
PLOT SCALE = 0.08333317" / in.	CHECKED - BKL	REVISED -
PLOT DATE = 8/23/2023	DATE -	REVISED -

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

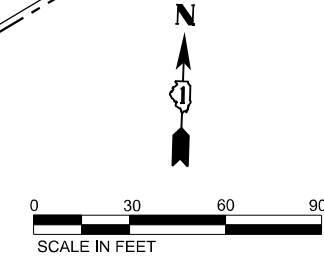
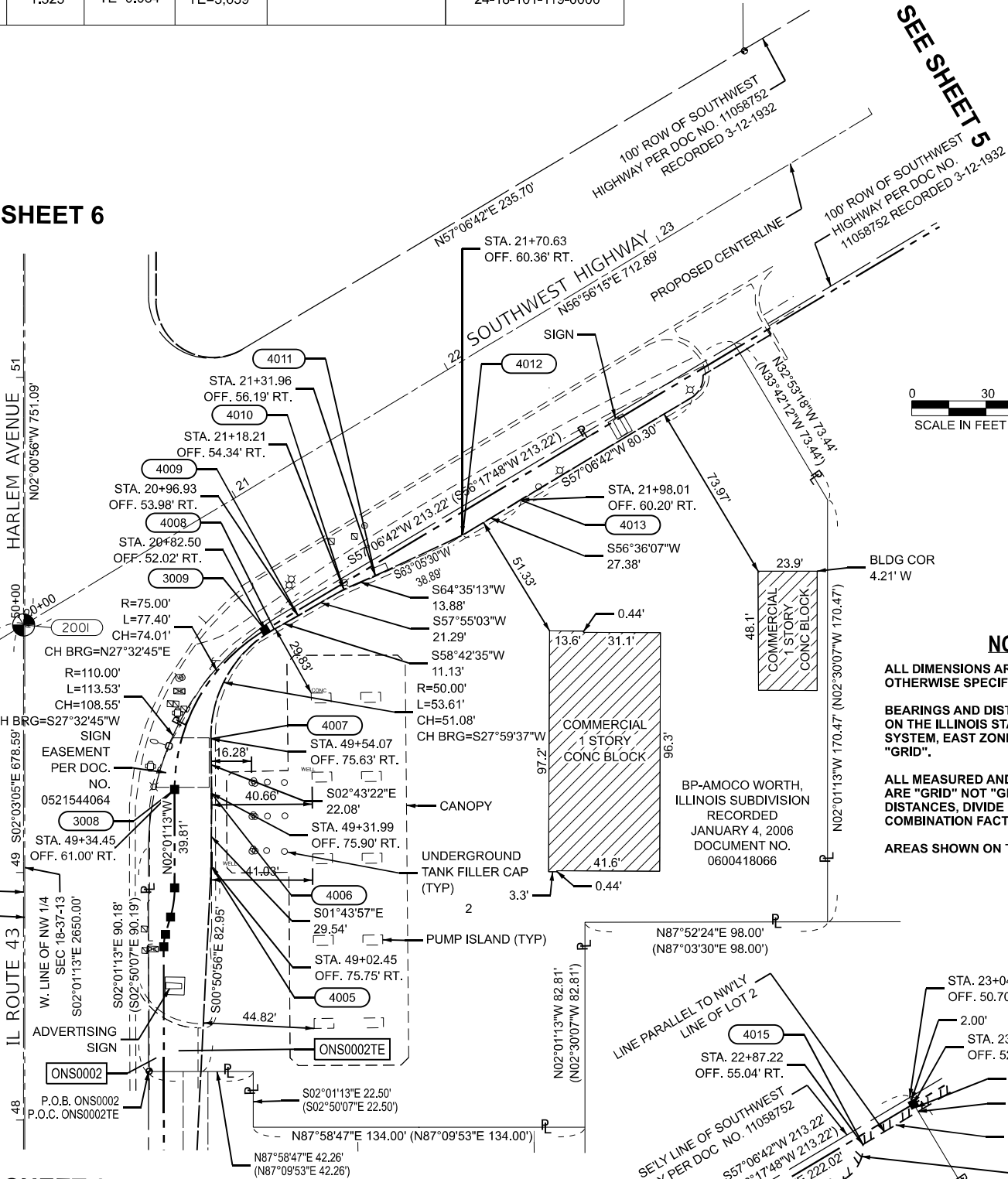
ROUTE 7	SECTION 13	COUNTY COOK	JOB# R-90-007-20
SEC	T 37, R 12E OF 3RD P.M.	PROJECT#	ILLINOIS ROUTE 7 AT ILLINOIS ROUTE 43
SCALE: 1"=30'	SHEET NO. 3 OF 6 SHEETS	STA 13+20.80	TO STA 18+00.00 CONTRACT NO.

PART OF SECTION 18, TWP. 37 N., R. 13 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NO.	TOTAL HOLDING ACRES	FEE SIMPLE ACQUISITION						REMAINDER ACRES	EASEMENTS		EASEMENT PURPOSE	PARCEL INDEX NUMBER
		GROSS ACRES	SQ FT	PREVIOUSLY DEDICATED ACRES	SQ FT	NET ACRES	SQ FT		PE = PERMANENT ACRES	TE = TEMPORARY SQ FT		
ONS0002	1.359	0.036	1,564					1.323	TE=0.084	TE=3,639		24-18-101-119-0000
ONS0002TE	1											



SEE SHEET 6



NOTES

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AREAS SHOWN ON THIS PLAT ARE "GROUND".

RIGHT-OF-WAY LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT-OF-WAY LINE
- FORMER RIGHT-OF-WAY LINE
- EXISTING IDOT EASEMENT LINE
- EXISTING EASEMENT LINE
- BUILDING SETBACK LINE
- EXISTING ACCESS CONTROL LINE
- EXISTING RIGHT-OF-WAY & PROPOSED ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT-OF-WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY LINE (TITLE)
- RECORDED PLAT/DEED LINE
- APPL APPARENT PROPERTY LINE
- SAME OWNERSHIP
- 121.45' MEASURED DIMENSION
- (121.45') RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS

■ STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DEPARTMENT OF TRANSPORTATION SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

■ B STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS IN CULTIVATED AREA, A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

STATE OF ILLINOIS)
COUNTY OF COOK) SS

I, BRADLEY K. LUEDERS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, STATE THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED _____

BRADLEY K. LUEDERS, PLS NO. 035-003406
LICENSE EXPIRATION DATE: 11/30/2024



APPROVED
By Doug Horn at 12:09 pm, Feb 24, 2025

PROJECT COORDINATES ILLINOIS STATE PLANE, EAST ZONE, NAD83 (2011)				
PT.NO.	STATION	OFFSET	NORTHING	EASTING
2001	27+12.89	0.00' RT	1833104.5441	1130503.2776
3004	47+48.24	59.71' RT	1832976.6715	1130564.3958
3005	47+56.43	59.38' RT	1832981.7258	1130564.9356
3006	47+64.45	57.63' RT	1832988.7525	1130566.7154
3007	47+71.28	56.59' RT	1833000.5422	1130567.9752
3008	49+34.45	61.00' RT	1833040.3313	1130566.5717
3009	20+82.50	52.02' RT	1833105.9550	1130600.7999
4004	46+53.61	61.88' RT	1832926.7824	1130581.1850
4005	49+02.45	75.75' RT	1833009.7247	1130582.4141
4006	49+31.99	75.90' RT	1833039.2536	1130581.5210
4007	49+54.07	75.63' RT	1833061.3052	1130580.4723
4008	20+82.50	52.02' RT	1833106.4077	1130604.4472
4009	20+96.93	53.98' RT	1833112.1874	1130613.9568
4010	21+18.21	54.34' RT	1833123.4940	1130631.9933
4011	21+31.96	56.19' RT	1833129.4493	1130644.5277
4012	21+70.63	60.32' RT	1833147.0496	1130679.2072
4013	21+98.01	60.20' RT	1833162.1228	1130702.0685
4014	22+78.32	60.44' RT	1833205.7276	1130769.5017
4015	22+87.22	55.04' RT	1833215.1097	1130774.0231

SEE SHEET 2

SEE SHEET 3

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

ROUTE 7	SECTION 18	COUNTY COOK	JOB# R-90-007-20
SEC	T 37, R 13E OF 3RD P.M.	PROJECT# ILLINOIS ROUTE 7 AT ILLINOIS ROUTE 43	
SCALE: 1"=30'	SHEET NO. 4 OF 6 SHEETS	STA 23+04.51 TO STA 25+99.43	CONTRACT NO.

MODEL: Unrecorded Plat - 4th 1 (Sheet)
FILE NAME: I:\PROJECTS\2025\18-101-119-0000\DOT_WORTH_CHICAGO_EDGE_HARLEM_AVENUE_IL_ROUTE_7_ILMCDRSTATIONING2-10-2025_06-Sheet 03.dgn



USER NAME = kmuhr	DESIGNED -	REVISED -
PLOT SCALE = 0.08333317" / in.	DRAWN - KWM	REVISED -
PLOT DATE = 8/23/2023	CHECKED - BKL	REVISED -
	DATE -	REVISED -

PART OF SECTION 18, TWP. 37 N., R. 13 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NO.	TOTAL HOLDING	FEE SIMPLE ACQUISITION						REMAINDER	EASEMENTS			PARCEL INDEX NUMBER
		GROSS		PREVIOUSLY DEDICATED		NET			PE = PERMANENT TE = TEMPORARY		EASEMENT PURPOSE	
	ACRES	ACRES	SQ FT	ACRES	SQ FT	ACRES	SQ FT	ACRES	ACRES	SQ FT		
ONS0003 ONS0003TE	1.478 1	0.013	551					1.465		TE=35		24-18-101-120-0000
ONS0004TE	0.640 1							0.640		TE=62		24-18-101-083- 1001 THROUGH 1018

RIGHT-OF-WAY LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT-OF-WAY LINE
- FORMER RIGHT-OF-WAY LINE
- EXISTING IDOT EASEMENT LINE
- EXISTING EASEMENT LINE
- BUILDING SETBACK LINE
- EXISTING ACCESS CONTROL LINE
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- 121.45' MEASURED DIMENSION
- (121.45') RECORDED DIMENSION
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- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS

NOTES

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.

BEARINGS AND DISTANCES SHOWN HEREON ARE ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD83 (2011 ADJUSTMENT) "GRID".

ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES BY COMBINATION FACTOR OF 0.999956901.

AREAS SHOWN ON THIS PLAT ARE "GROUND".

MODEL: Sheet 04 (Sheet)
FILE NAME: N:\PROJECTS\2021\12-14-2021\DOT_WORTH_CHICAGO_FIDGE_HARLEM_AVENUE_IL_ROUTE_7\MICROSTATION\21-0882_06-Sheet 04.dgn



USER NAME = kmuhr	DESIGNED -	REVISED -
PLOT SCALE = 0.08333317' / in.	DRAWN - KWM	REVISED -
PLOT DATE = 9/1/2023	CHECKED - BKL	REVISED -
	DATE -	REVISED -

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

ROUTE 7	SECTION 18	COUNTY COOK	JOB# R-90-007-20
SEC	T 37, R 13E OF 3RD P.M.	PROJECT# ILLINOIS ROUTE 7 AT ILLINOIS ROUTE 43	
SCALE: 1"=30'	SHEET NO. 5 OF 6 SHEETS	STA 23+04.51 TO STA 25+99.43	CONTRACT NO.

- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DEPARTMENT OF TRANSPORTATION SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.
- STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS IN CULTIVATED AREA, A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

STATE OF ILLINOIS)
COUNTY OF COOK) SS

I, BRADLEY K. LUEDERS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, STATE THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED

BRADLEY K. LUEDERS, PLS NO. 035-003406
LICENSE EXPIRATION DATE: 11/30/2024



APPROVED
By Doug Horn at 12:09 pm, Feb 24, 2025

PART OF SECTION 18, TWP. 37 N., R. 13 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NO.	TOTAL HOLDING	FEE SIMPLE ACQUISITION						REMAINDER	EASEMENTS			PARCEL INDEX NUMBER
		GROSS		PREVIOUSLY DEDICATED		NET			PE = PERMANENT TE = TEMPORARY		EASEMENT PURPOSE	
	ACRES	ACRES	SQ FT	ACRES	SQ FT	ACRES	SQ FT	ACRES	ACRES	SQ FT		
ONS0005TE	1.441 1							1.441	TE=0.021	TE=936		24-18-101-109-0000 THROUGH 24-18-101-112-0000
ONS0006TE	1.610 1							1.610	TE=0.003	TE=128		24-18-101-100-0000 24-18-101-098-0000

NOTES

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.

BEARINGS AND DISTANCES SHOWN HEREON ARE ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD83 (2011 ADJUSTMENT) "GRID".

ALL MEASURED AND CALCULATED DISTANCES ARE "GRID" NOT "GROUND". TO OBTAIN GROUND DISTANCES, DIVIDE GRID DISTANCES BY COMBINATION FACTOR OF 0.999956901.

AREAS SHOWN ON THIS PLAT ARE "GROUND".

RIGHT-OF-WAY LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT-OF-WAY LINE
- FORMER RIGHT-OF-WAY LINE
- EXISTING IDOT EASEMENT LINE
- EXISTING EASEMENT LINE
- BUILDING SETBACK LINE
- EXISTING ACCESS CONTROL LINE
- EXISTING RIGHT-OF-WAY & PROPOSED ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT-OF-WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY LINE (TITLE)
- RECORDED PLAT/DEED LINE
- APL APPARENT PROPERTY LINE
- SAME OWNERSHIP
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS

■ STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DEPARTMENT OF TRANSPORTATION SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

■^B STAKING OF PROPOSED RIGHT-OF-WAY AND PERMANENT EASEMENT CORNERS IN CULTIVATED AREA, A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER.

STATE OF ILLINOIS)
COUNTY OF COOK) SS

I, BRADLEY K. LUEDERS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, STATE THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

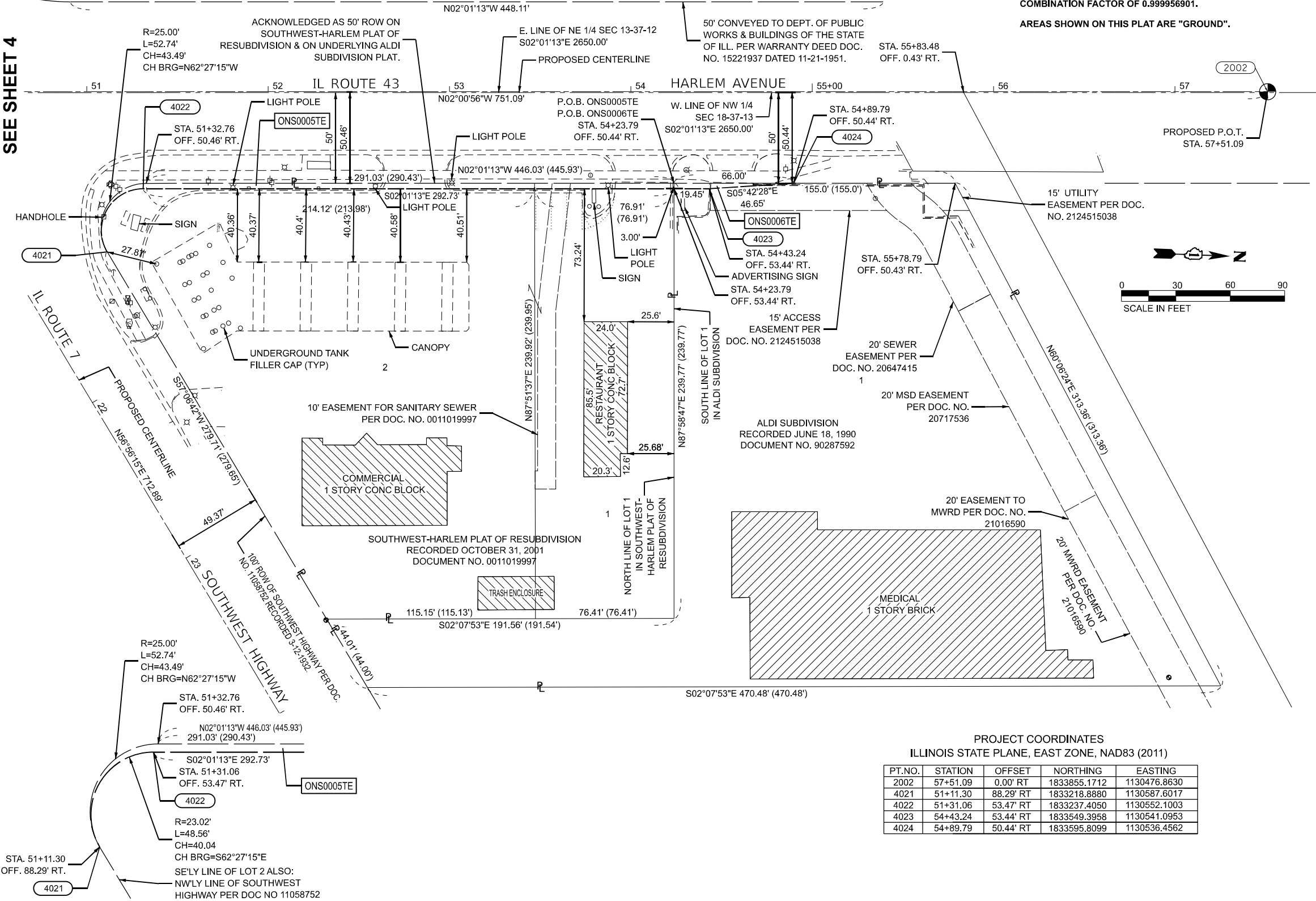
DATED _____

BRADLEY K. LUEDERS, PLS NO. 035-003406
LICENSE EXPIRATION DATE: 11/30/2024



APPROVED
By Doug Horn at 12:09 pm, Feb 24, 2025

SEE SHEET 4



PROJECT COORDINATES
ILLINOIS STATE PLANE, EAST ZONE, NAD83 (2011)

PT.NO.	STATION	OFFSET	NORTHING	EASTING
2002	57+51.09	0.00' RT	1833855.1712	1130476.8630
4021	51+11.30	88.29' RT	1833218.8880	1130587.6017
4022	51+31.06	53.47' RT	1833237.4050	1130552.1003
4023	54+43.24	53.44' RT	1833549.3958	1130541.0953
4024	54+89.79	50.44' RT	1833595.8099	1130536.4562



USER NAME = kmuhr	DESIGNED -	REVISED -
PLOT SCALE = 0.08333317' / in.	DRAWN - KWM	REVISED -
PLOT DATE = 8/23/2023	CHECKED - BKL	REVISED -
	DATE -	REVISED -

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

ROUTE 7	SECTION 18	COUNTY COOK	JOB# R-90-007-20
SEC	T 37, R 13E OF 3RD P.M.	PROJECT# ILLINOIS ROUTE 7 AT ILLINOIS ROUTE 43	
SCALE: 1"=30'	SHEET NO. 6 OF 6 SHEETS	STA 51+00.00 TO STA 57+51.09	CONTRACT NO.

MODEL: E:\R14 - Plat 1 (Sheet)
FILE NAME: N:\PROJECTS\2023\12-13-2023\DOT_WORTH_CHICAGO_EDGE_HARLEM_AVENUE_IL_ROUTE_7\MICROSTATIONS\2-10-2023_05-Sheet 05.dgn

SUE NOTES

1. HORIZONTAL CONTROL, VERTICAL CONTROL, CENTERLINE ALIGNMENT, TOPOGRAPHIC FEATURES, AND RIGHT-OF-WAY WERE SUPPLIED BY ILLINOIS DEPARTMENT OF TRANSPORTATION ON AUGUST 5TH, 2022.
2. ELECTRONIC DEPTH INFORMATION IS APPROXIMATE ONLY. DEPTHS ON THE LEVEL B LOCATES WERE CAPTURED FROM THE LOCATING EQUIPMENT AND ARE NOT LEVEL A QUALITY. ESTIMATED ELECTRONIC DEPTHS ARE MEASURED TO THE CENTER OF THE UTILITY AND MAY VARY. FOR SPECIFIC POINT DEPTHS EACH ESTIMATED ALONG A UTILITY LINE, PLEASE SEE THE POINT DATA LOCATED WITHIN THE CADD FILE OR RAW DATA FILE. SUE LEVEL A LOCATES SHOULD BE PERFORMED TO OBTAIN ACCURATE DEPTH INFORMATION.
3. UTILITY SIZES ARE INCLUDED IN SUE LABELS WHEN INFORMATION IS PROVIDED FROM RECORD/ATLAS INFORMATION. SUE LEVEL A LOCATES SHOULD BE PERFORMED TO CONFIRM UTILITY SIZES.
4. UTILITIES ARE REPRESENTED AS A COMBINATION OF THE MOST ACCURATE INFORMATION GATHERED (FIELD LOCATES, ATLASES, ETC.)
5. IN AREAS WHERE UTILITIES WERE NOT LOCATED, UTILITIES WERE DRAWN IN AS QUALITY LEVEL D FROM ATLAS INFORMATION.
6. ASE DID NOT RECEIVE IDOT UTILITY RECORDS FOR THIS SUE STUDY. ANY IDOT UTILITIES SHOWN ON SUE PLANS WERE LOCATED THROUGH FIELD INVESTIGATION AND NOT CHECKED AGAINST RECORD DRAWINGS.
7. STORM AND SANITARY LOCATES ARE NOT INCLUDED IN THIS SUE SURVEY.
8. NUMBERED AERIAL POLES AND THE CONNECTING LINEWORK IS QUALITY LEVEL C. ALL OTHER AERIAL LINEWORK IS QUALITY LEVEL D.
9. TRAFFIC SIGNAL LOCATES WERE NOT INCLUDED IN THIS SUE SURVEY.
10. AERIAL WIDE OPEN WEST AND MCI FIBER WAS FOUND ON POLES WITHIN PROJECT LIMITS. BUT ATLASES WERE NOT PROVIDED BY UTILITY COMPANIES THROUGH DESIGN JULIE. PLEASE CONTACT THESE UTILITY COMPANIES FOR RECORD INFORMATION.

STATE OF ILLINOIS)) S.S.

COUNTY OF COOK)

UTILITY(IES) LOCATIONS WERE COLLECTED AND DEPCITED AS SHOWN HEREON BY ASE IN ACCORDANCE WITH SUE INDUSTRY STANDARDS C/ASCE 38-02 FOR QUALITY LEVEL B (QLB) AND QUALITY LEVEL C (QLC). ALL OTHER INFORMATION SHOWN HAS BEEN PROVIDED BY OTHERS.

FIELD WORK WAS PERFORMED BETWEEN THE 31ST DAY OF AUGUST, 2022 AND THE 20TH DAY OF SEPTEMBER, 2022

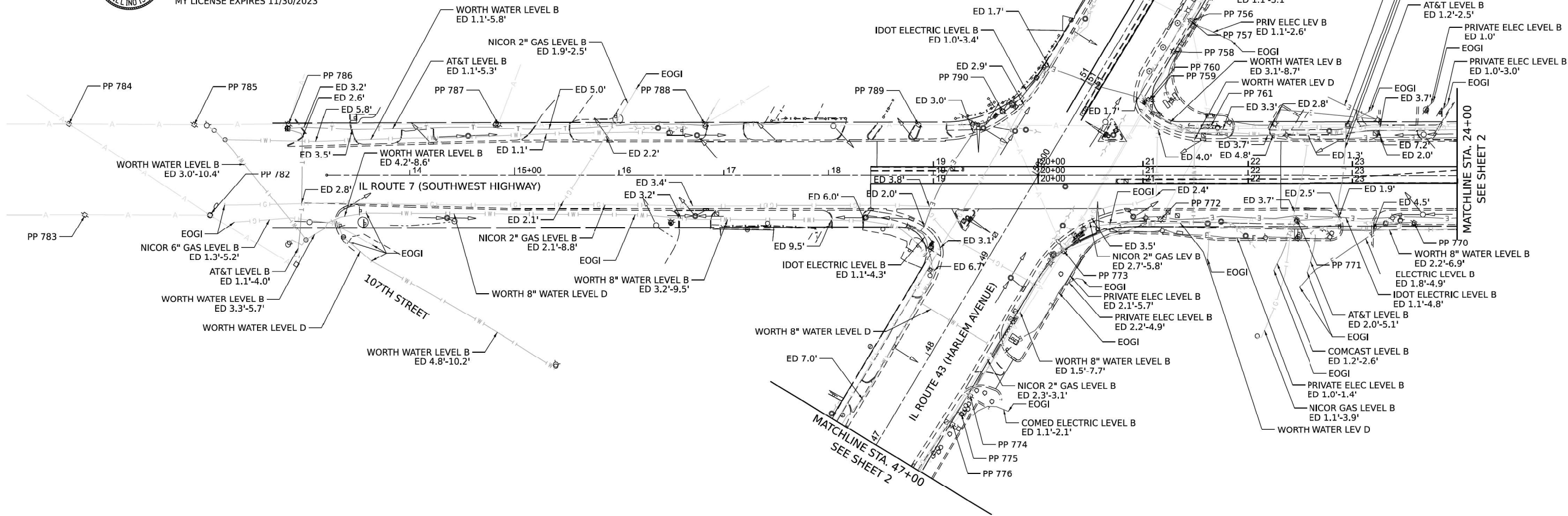
IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND AND SEAL THIS 21ST DAY OF OCTOBER A.D., 2022. CHICAGO, IL.



THOMAS A. SANDERSON - ILLINOIS PROFESSIONAL ENGINEER NUMBER 62-054022
MY LICENSE EXPIRES 11/30/2023

SEE SHEET 2 FOR POWER POLE INFORMATION TABLE

UTILITY LINE LEGEND	
	EXISTING UNDERGROUND SANITARY
	EXISTING UNDERGROUND TELEPHONE
	EXISTING UNDERGROUND WATER
	EXISTING UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND GAS
	EXISTING UNDERGROUND CABLE TV
	EXISTING UNDERGROUND FIBER OPTIC
	EXISTING UNDERGROUND OIL PIPE LINE
	EXISTING AERIAL LINE
	END OF SURFACE GEOPHYSICAL INFORMATION
EOGI	END OF SURFACE GEOPHYSICAL INFORMATION
ED	ELECTRONIC DEPTH (IN FEET)
PP	POWER POLE
VP	VENT PIPE

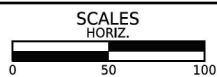


MODEL: Street
FILE NAME: Y:\0000257 - IDOT PTB198 Item 7 ASE\NO 435\CADD\Sheet\257_435_sue_01.dgn

Bravo Company
ENGINEERING

DESIGNED -	TGB	REVISED -	
DRAWN -	TGB	REVISED -	
CHECKED -	RTM	REVISED -	
DATE -	10/14/2022	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SUE STUDY PLAN			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVENUE)			
SHEET	OF	SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-025-N-TS(L)	COOK	176	69
CONTRACT NO. 62W47				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JOB NO. P-91-022-18	

SUE NOTES

- HORIZONTAL CONTROL, VERTICAL CONTROL, CENTERLINE ALIGNMENT, TOPOGRAPHIC FEATURES, AND RIGHT-OF-WAY WERE SUPPLIED BY ILLINOIS DEPARTMENT OF TRANSPORTATION ON AUGUST 5TH, 2022.
- ELECTRONIC DEPTH INFORMATION IS APPROXIMATE ONLY. DEPTHS ON THE LEVEL B LOCATES WERE CAPTURED FROM THE LOCATING EQUIPMENT AND ARE NOT LEVEL A QUALITY. ESTIMATED ELECTRONIC DEPTHS ARE MEASURED TO THE CENTER OF THE UTILITY AND MAY VARY. FOR SPECIFIC POINT DEPTHS EACH ESTIMATED ALONG A UTILITY LINE, PLEASE SEE THE POINT DATA LOCATED WITHIN THE CADD FILE OR RAW DATA FILE. SUE LEVEL A LOCATES SHOULD BE PERFORMED TO OBTAIN ACCURATE DEPTH INFORMATION.
- UTILITY SIZES ARE INCLUDED IN SUE LABELS WHEN INFORMATION IS PROVIDED FROM RECORD/ATLAS INFORMATION. SUE LEVEL A LOCATES SHOULD BE PERFORMED TO CONFIRM UTILITY SIZES.
- UTILITIES ARE REPRESENTED AS A COMBINATION OF THE MOST ACCURATE INFORMATION GATHERED (FIELD LOCATES, ATLASES, ETC.)
- IN AREAS WHERE UTILITIES WERE NOT LOCATED, UTILITIES WERE DRAWN IN AS QUALITY LEVEL D FROM ATLAS INFORMATION.
- ASE DID NOT RECEIVE IDOT UTILITY RECORDS FOR THIS SUE STUDY. ANY IDOT UTILITIES SHOWN ON SUE PLANS WERE LOCATED THROUGH FIELD INVESTIGATION AND NOT CHECKED AGAINST RECORD DRAWINGS.
- STORM AND SANITARY LOCATES ARE NOT INCLUDED IN THIS SUE SURVEY.
- NUMBERED AERIAL POLES AND THE CONNECTING LINEWORK IS QUALITY LEVEL C. ALL OTHER AERIAL LINEWORK IS QUALITY LEVEL D.
- TRAFFIC SIGNAL LOCATES WERE NOT INCLUDED IN THIS SUE SURVEY.
- AERIAL WIDE OPEN WEST AND MCI FIBER WAS FOUND ON POLES WITHIN PROJECT LIMITS, BUT ATLASES WERE NOT PROVIDED BY UTILITY COMPANIES THROUGH DESIGN JULIE. PLEASE CONTACT THESE UTILITY COMPANIES FOR RECORD INFORMATION.

PP #	Northing	Easting	Ground Elev	Utilities on Pole
752	1,833,802.45	1,130,522.69	591.53	(1) WOW, (1) Comcast, (1) AT&T Distrib., (1) ComEd, (2) Comcast Fiber
753	1,833,796.62	1,130,524.44	591.25	(2) ComEd, (1) ComEd Fiber
754	1,833,590.87	1,130,529.20	593.10	(1) WOW, (1) Comcast, (1) AT&T Distrib., (3) ComEd, (1) ComEd Fiber, (2) Comcast Fiber
755	1,833,459.90	1,130,532.47	593.91	(1) WOW, (1) Comcast, (2) AT&T Distrib., (3) ComEd, (1) ComEd Fiber, (2) Comcast Fiber
756	1,833,309.00	1,130,546.12	595.33	(2) ComEd, (1) ComEd Fiber
757	1,833,307.74	1,130,544.83	595.33	(1) WOW, (1) Comcast, (3) AT&T Distrib., (3) Comcast Fiber
758	1,833,274.24	1,130,547.23	595.79	(2) ComEd, (1) ComEd Fiber
759	1,833,224.13	1,130,554.24	596.27	(4) ComEd, (1) ComEd Fiber
760	1,833,222.62	1,130,552.25	596.27	(1) WOW, (3) Comcast Fiber
761	1,833,233.46	1,130,615.56	596.59	(2) Comcast, (2) AT&T Distrib., (4) ComEd, (1) ComEd Fiber, (1) US Signal Fiber, (1) Comcast Fiber
762	1,833,306.19	1,130,724.88	597.48	(3) Comcast, (1) AT&T Distrib., (4) ComEd, (1) ComEd Fiber, (1) US Signal Fiber, (1) Comcast Fiber
763	1,833,369.36	1,130,826.19	598.00	(2) Comcast, (1) AT&T Distrib., (5) ComEd, (1) ComEd Fiber, (1) US Signal Fiber, (1) Comcast Fiber
764	1,833,433.89	1,130,920.09	598.66	(2) Comcast, (1) AT&T Distrib., (5) ComEd, (1) ComEd Fiber, (1) US Signal Fiber, (1) Comcast Fiber, (2) AT&T Fiber
765	1,833,478.73	1,130,987.40	598.83	(2) Comcast, (1) AT&T Distrib., (4) ComEd, (1) ComEd Fiber, (1) US Signal Fiber, (1) Comcast Fiber, (2) AT&T Fiber
766	1,833,516.45	1,131,044.04	599.20	(2) Comcast, (1) AT&T Distrib., (1) US Signal Fiber, (1) Comcast Fiber, (2) AT&T Fiber
767	1,833,518.21	1,131,046.06	599.07	(5) ComEd, (1) ComEd Fiber
768	1,833,389.61	1,131,017.37	599.77	(1) AT&T Distrib., (1) ComEd, (1) AT&T Fiber
769	1,833,326.86	1,130,925.63	598.71	(1) AT&T Distrib., (2) ComEd, (1) AT&T Fiber
770	1,833,262.51	1,130,828.92	598.34	(1) AT&T Distrib., (4) ComEd, (1) AT&T Fiber
771	1,833,203.75	1,130,732.69	597.86	(1) Comcast, (2) AT&T Distrib., (3) ComEd, (1) AT&T Fiber
772	1,833,132.94	1,130,624.15	597.48	(2) Comcast, (1) AT&T Distrib., (2) ComEd
773	1,833,057.30	1,130,563.34	597.78	(1) WOW Fiber, (1) Comcast, (4) ComEd, (2) Comcast Fiber
774	1,832,893.09	1,130,563.57	599.83	(1) WOW Fiber, (1) Comcast, (3) ComEd, (3) Comcast Fiber
775	1,832,873.86	1,130,566.05	600.67	(3) ComEd
776	1,832,861.07	1,130,564.18	600.58	(1) WOW Fiber, (1) Comcast, (3) ComEd, (3) Comcast Fiber
777	1,832,784.12	1,130,564.63	601.34	(1) WOW Fiber, (1) Comcast, (2) ComEd, (3) Comcast Fiber
778	1,832,693.28	1,130,568.58	601.98	(1) WOW Fiber, (9) Comcast, (1) AT&T Distrib., (4) ComEd, (3) Comcast Fiber
779	1,832,477.50	1,130,569.91	604.92	(1) WOW Fiber, (6) Comcast, (2) ComEd
780	1,832,371.15	1,130,572.33	606.16	(1) WOW Fiber, (6) Comcast, (2) ComEd
781	1,832,670.93	1,130,477.56	601.90	(2) Comcast, (2) AT&T Distrib., (2) ComEd
782	1,832,641.55	1,129,861.76	602.37	(2) Comcast, (3) ComEd, (2) AT&T Fiber
783	1,832,576.58	1,129,761.38	603.59	(2) Comcast, (1) ComEd
784	1,832,641.06	1,129,701.27	605.09	(4) Comcast, (1) AT&T Distrib., (4) ComEd, (2) AT&T Fiber, (1) US Signal Fiber
785	1,832,705.72	1,129,801.90	600.36	(3) Comcast, (5) ComEd, (1) AT&T Fiber, (1) US Signal Fiber
786	1,832,755.47	1,129,877.10	600.81	(3) Comcast, (5) ComEd, (1) AT&T Fiber, (1) US Signal Fiber
787	1,832,864.19	1,130,044.76	597.93	(3) Comcast, (5) ComEd, (1) AT&T Fiber, (1) US Signal Fiber
788	1,832,971.88	1,130,209.88	597.66	(2) Comcast, (4) ComEd, (1) AT&T Fiber, (1) US Signal Fiber
789	1,833,082.79	1,130,381.17	597.05	(1) Comcast, (2) ComEd, (1) AT&T Fiber, (1) US Signal Fiber
790	1,833,137.30	1,130,441.57	596.48	(1) ComEd
791	1,833,298.22	1,130,449.82	595.26	(3) ComEd

STATE OF ILLINOIS)) S.S.

COUNTY OF COOK)

UTILITY(IES) LOCATIONS WERE COLLECTED AND DEPCITED AS SHOWN HEREON BY ASE IN ACCORDANCE WITH SUE INDUSTRY STANDARDS CI/ASCE 38-02 FOR QUALITY LEVEL B (QLB) AND QUALITY LEVEL C (QLC). ALL OTHER INFORMATION SHOWN HAS BEEN PROVIDED BY OTHERS.

FIELD WORK WAS PERFORMED BETWEEN THE 31ST DAY OF AUGUST, 2022 AND THE 20TH DAY OF SEPTEMBER, 2022

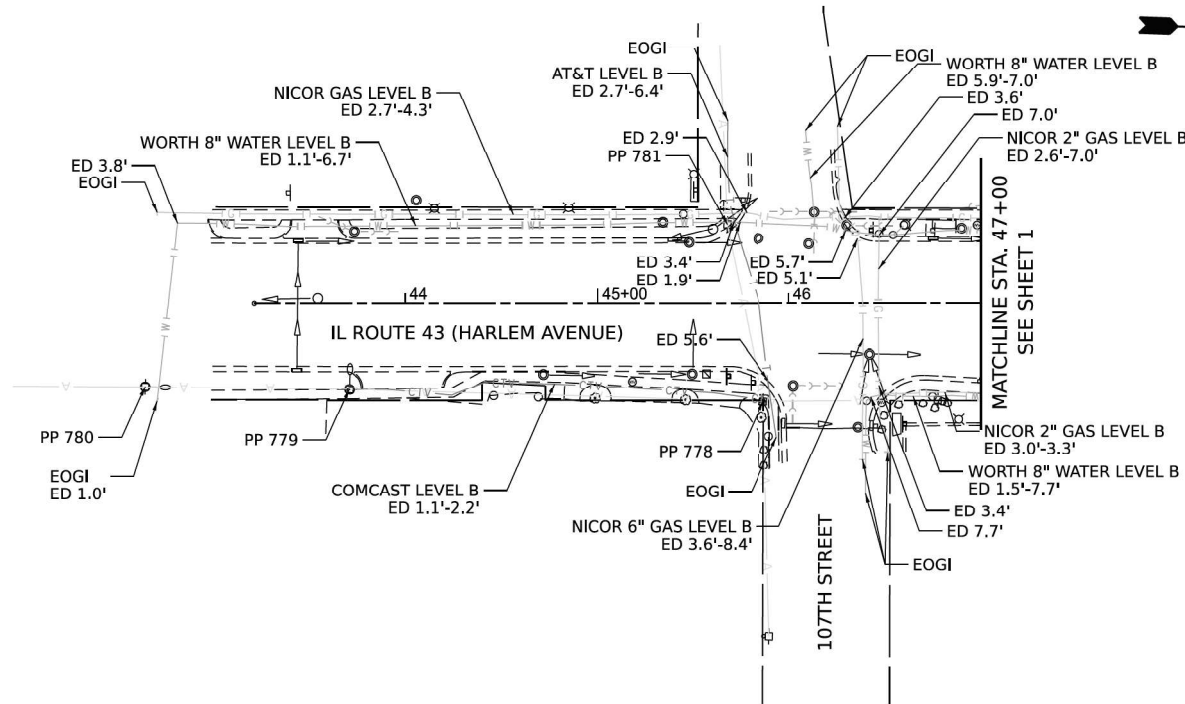
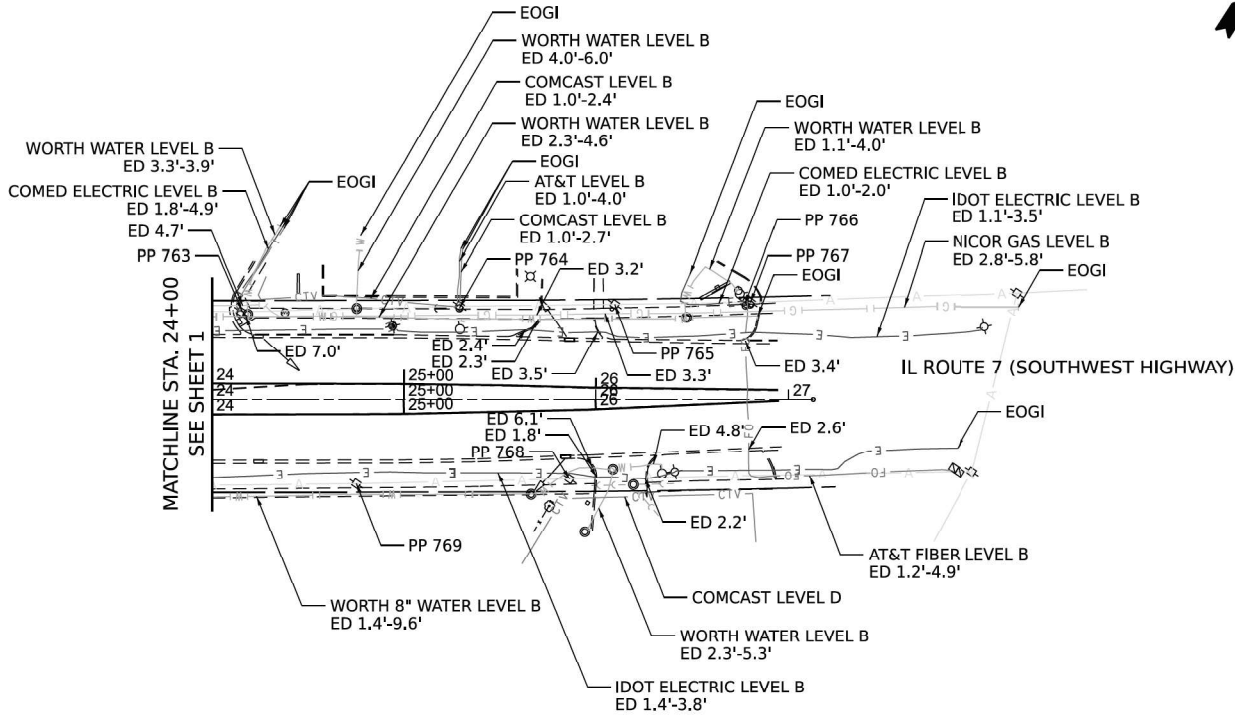
IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND AND SEAL THIS 21ST DAY OF OCTOBER A.D., 2022. CHICAGO, IL.



Thomas A. Sanderson

THOMAS A. SANDERSON - ILLINOIS PROFESSIONAL ENGINEER NUMBER 62-054022
MY LICENSE EXPIRES 11/30/2023

UTILITY LINE LEGEND	
---	EXISTING UNDERGROUND SANITARY
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING UNDERGROUND WATER
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING UNDERGROUND GAS
---	EXISTING UNDERGROUND CABLE TV
---	EXISTING UNDERGROUND FIBER OPTIC
---	EXISTING UNDERGROUND OIL PIPE LINE
---	EXISTING AERIAL LINE
---	END OF SURFACE GEOPHYSICAL INFORMATION
EOGI	END OF SURFACE GEOPHYSICAL INFORMATION
ED	ELECTRONIC DEPTH (IN FEET)
PP	POWER POLE
VP	VENT PIPE



Bravo Company
ENGINEERING

DESIGNED -	TGB	REVISED -	
DRAWN -	TGB	REVISED -	
CHECKED -	RTM	REVISED -	
DATE -	10/14/2022	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SUE STUDY PLAN
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVENUE)

SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-025-N-TS(L)	COOK	176	69A
CONTRACT NO. 62W47				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	JOB NO. P-91-022-18	

MODEL: SHT 1
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PM&L.S.dgn



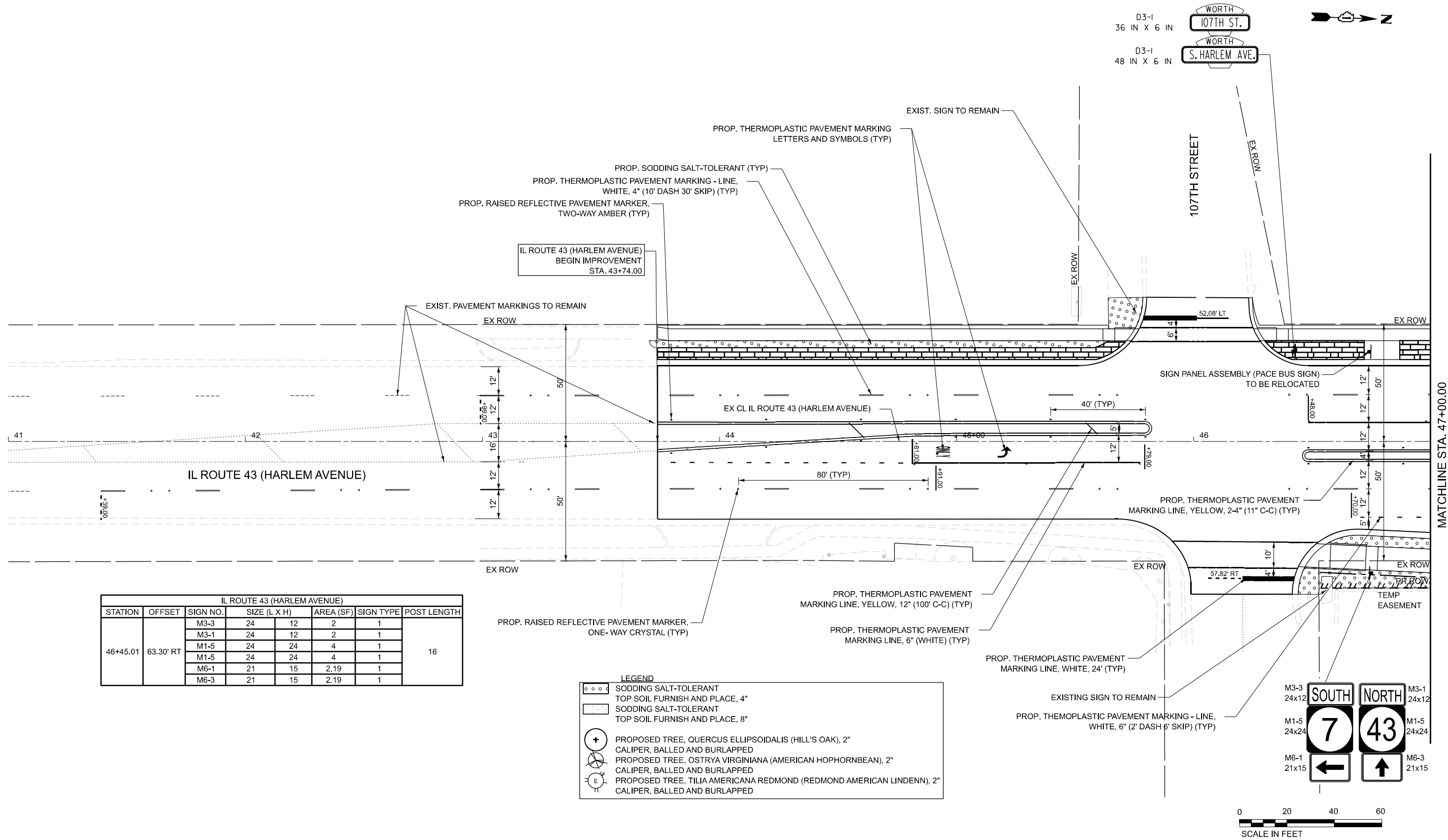
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING, LANDSCAPING AND SIGNING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

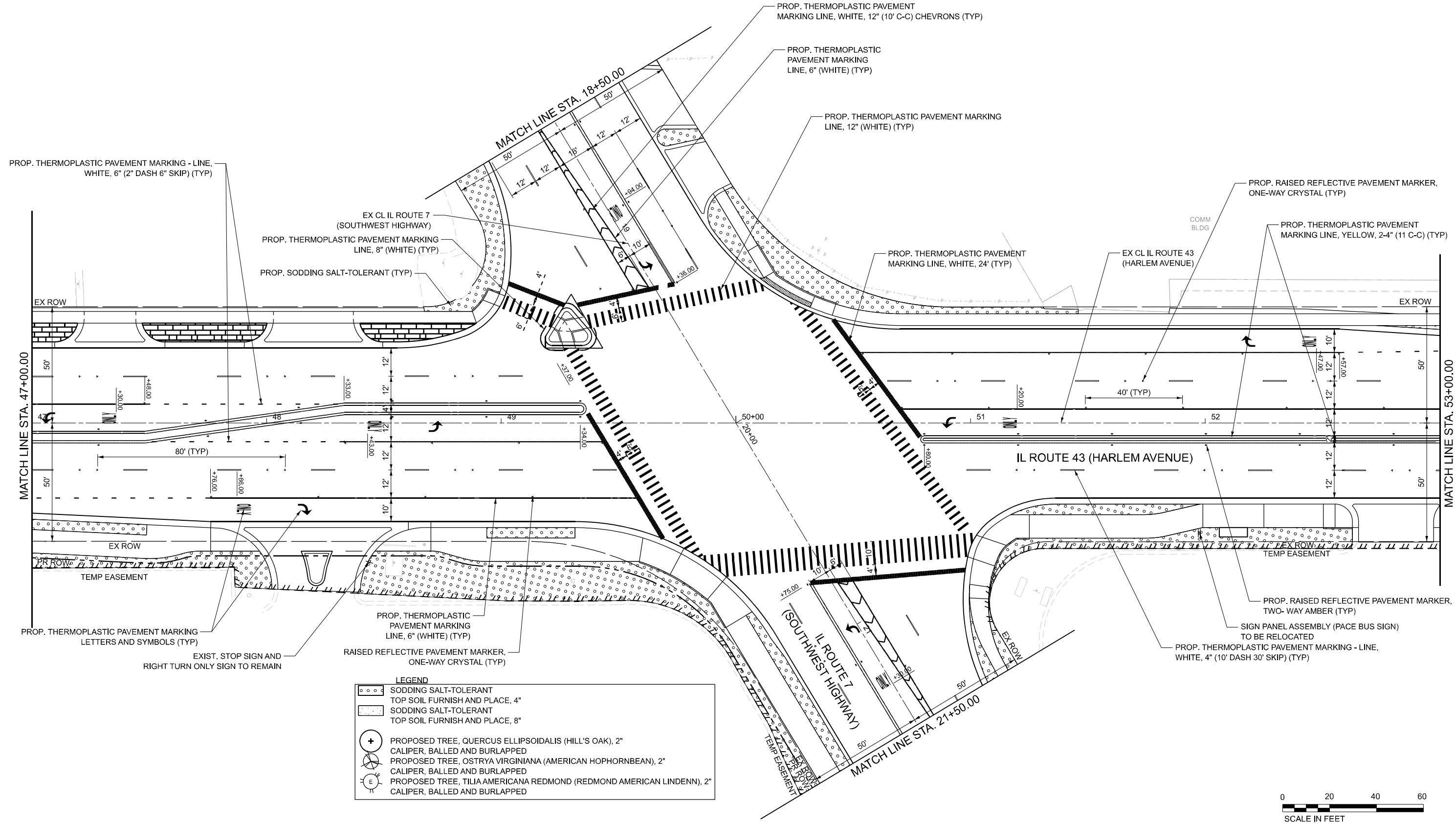
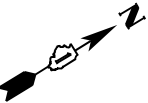
SCALE: 1"=20' SHEET 1 OF 5 SHEETS STA. 43+21.41 TO STA. 47+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	70
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



IL ROUTE 43 (HARLEM AVENUE)						
STATION	OFFSET	SIGN NO.	SIZE (L X H)	AREA (SF)	SIGN TYPE	POST LENGTH
46+45.01	63.30' RT	M3-3	24	12	2	1
		M3-1	24	12	2	1
		M1-5	24	24	4	1
		M1-5	24	24	4	1
		M6-1	21	15	2.19	1
		M6-3	21	15	2.19	1

LEGEND	
	SODDING SALT-TOLERANT TOP SOIL FURNISH AND PLACE, 4"
	SODDING SALT-TOLERANT TOP SOIL FURNISH AND PLACE, 8"
	PROPOSED TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED
	PROPOSED TREE, OSTRYA VIRGINIANA (AMERICAN HOPHORNBEAN), 2" CALIPER, BALLED AND BURLAPPED
	PROPOSED TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDENN), 2" CALIPER, BALLED AND BURLAPPED



MODEL: SHT 2
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PMK.L.S.dgn

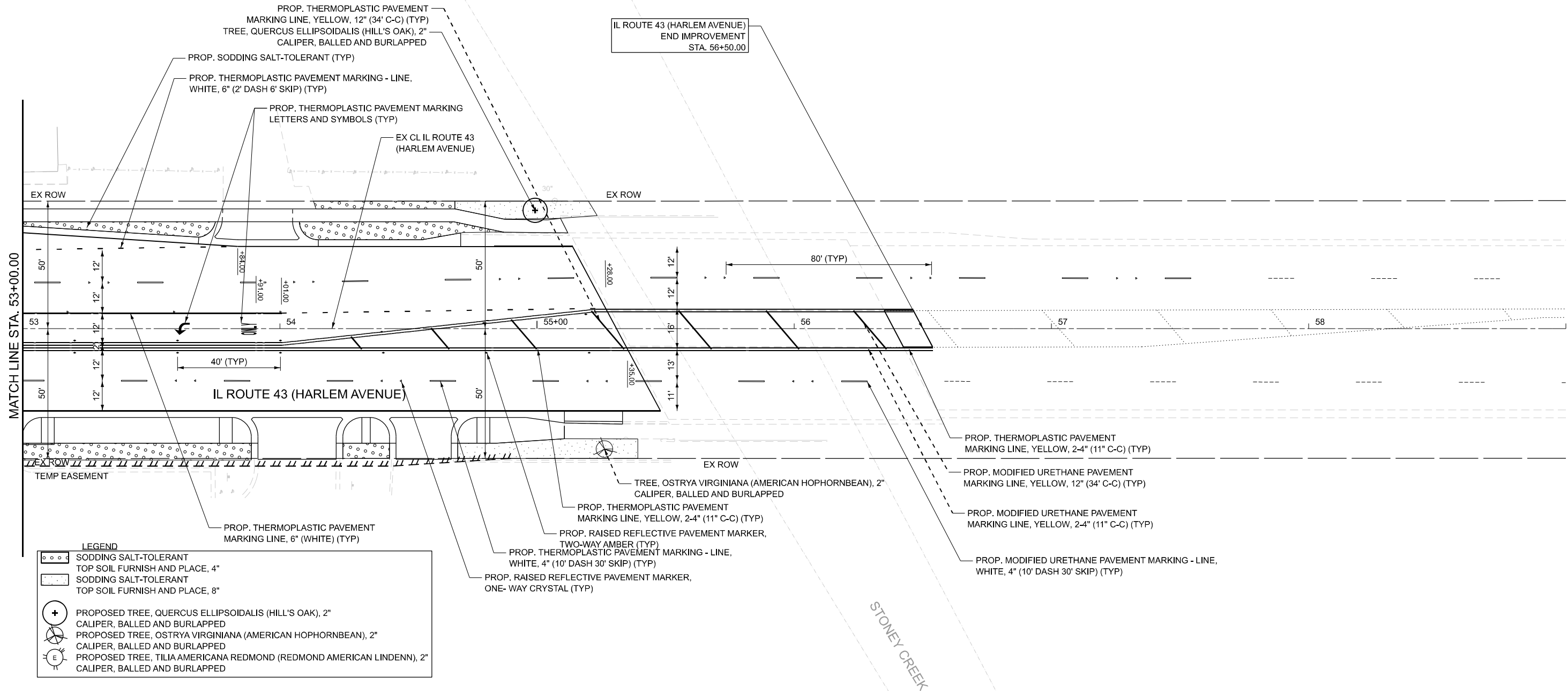
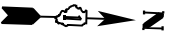


USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING, LANDSCAPING AND SIGNING PLAN			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1"=20'	SHEET 2	OF 5 SHEETS	STA. 47+00.00 TO STA. 53+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	71
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



MODEL: SHT 3
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PMK.L.S.dgn

<div><div>BL</div><div>BLA, Inc.</div></div>	USER NAME = tnestor	DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING, LANDSCAPING AND SIGNING PLAN IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - TN	REVISED -						3578	2023-925-N-TS(L)	COOK	176	72
		CHECKED - GJE	REVISED -		CONTRACT NO. 62W47								
	PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -		ILLINOIS FED. AID PROJECT								
		SCALE: 1"=20' SHEET 3 OF 5 SHEETS STA. 53+00.00 TO STA. 57+51.09											

MODEL: SHT 4
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PMK.L.S.dgn



USER NAME	= tnestor
DESIGNED	- TN
DRAWN	- TN
CHECKED	- GJE
PLOT DATE	= 10/17/2025
DATE	= 10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

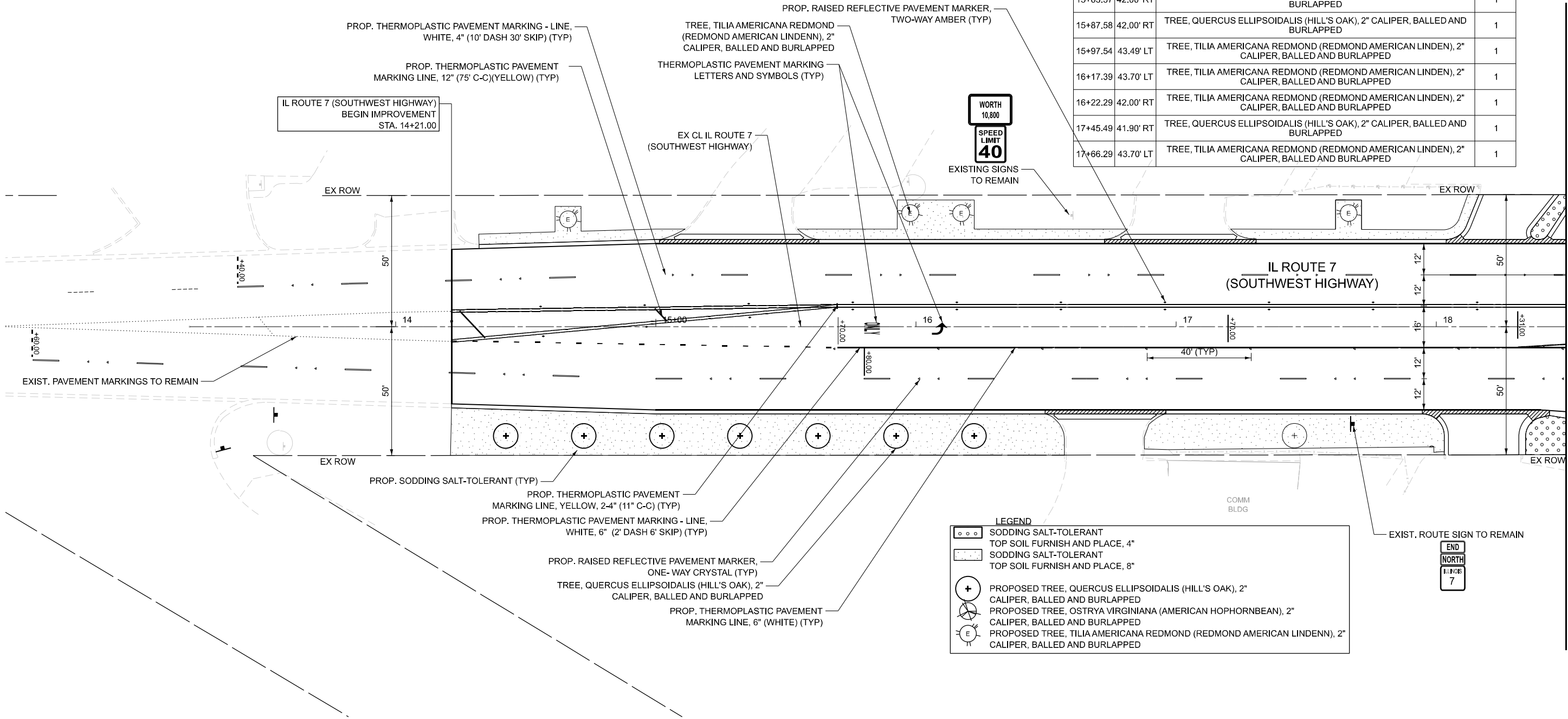
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

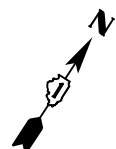
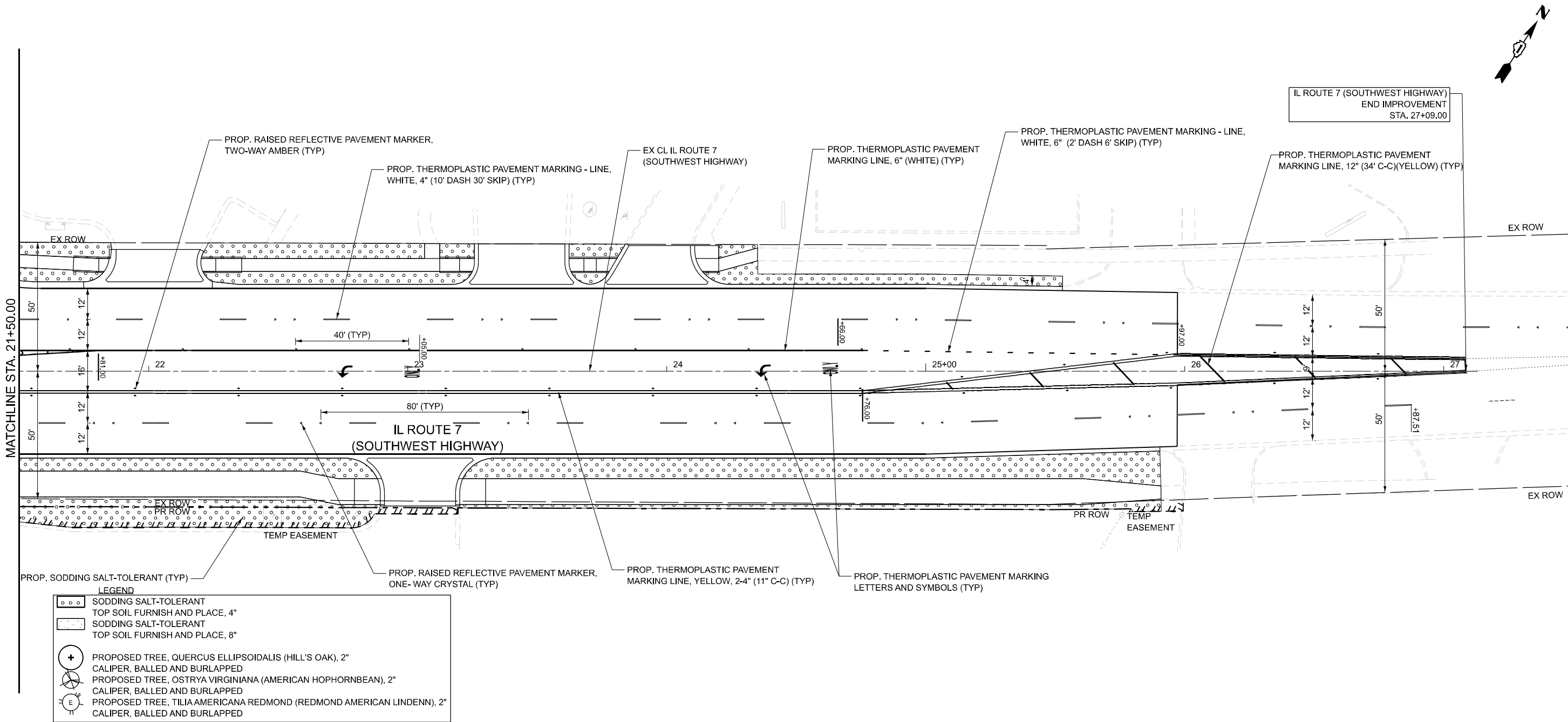
PAVEMENT MARKING, LANDSCAPING AND SIGNING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 4 OF 5 SHEETS STA. 13+20.80 TO STA. 18+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	73
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LOCATIONS OF PROPOSED TREES			
STATION	OFFSET	SPECIES	QUANTITY
54+99.21	45.96' LT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
55+26.18	46.78' RT	TREE, OSTRYA VIRGINIANA (AMERICAN HOPHORNBEAM), 2" CALIPER, BALLED AND BURLAPPED	1
14+42.29	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
14+66.32	41.39' LT	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	1
14+72.29	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
15+02.29	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
15+32.29	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
15+63.37	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
15+87.58	42.00' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
15+97.54	43.49' LT	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	1
16+17.39	43.70' LT	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	1
16+22.29	42.00' RT	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	1
17+45.49	41.90' RT	TREE, QUERCUS ELLIPSOIDALIS (HILL'S OAK), 2" CALIPER, BALLED AND BURLAPPED	1
17+66.29	43.70' LT	TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2" CALIPER, BALLED AND BURLAPPED	1

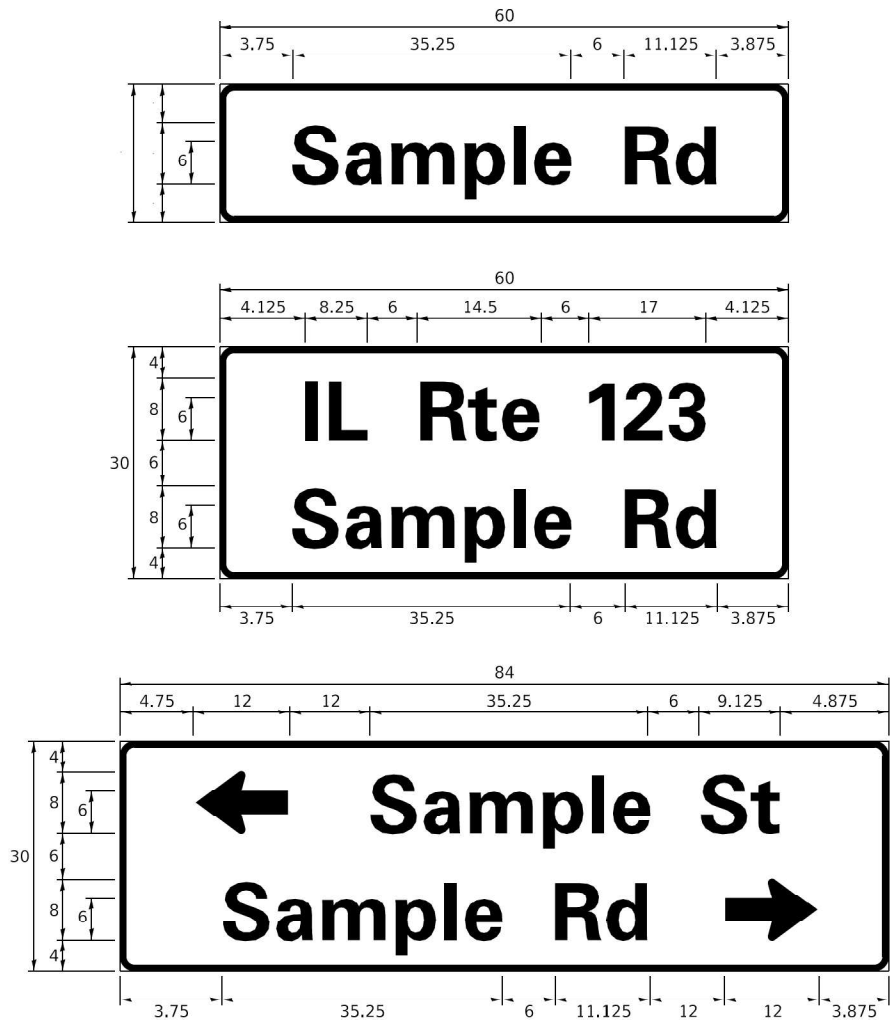




MODEL: SHT 5
FILE NAME: W:\191+200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sht4-PMK.L.S.dgn

 BLA, Inc.	USER NAME = tnestor		DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING, LANDSCAPING AND SIGNING PLAN IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - TN		REVIS	ED						3578	2023-925-N-TS(L)	COOK	176	74
	CHECKED - GJE		REVIS	ED		CONTRACT NO. 62W47								
	PLOT DATE = 10/17/2025		DATE - 10/15/2025	REVISED -		ILLINOIS FED. AID PROJECT								
	SCALE: 1"=20'		SHEET 5	OF 5							SHEETS	STA. 21+50.00	TO STA. 27+12.89	

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA

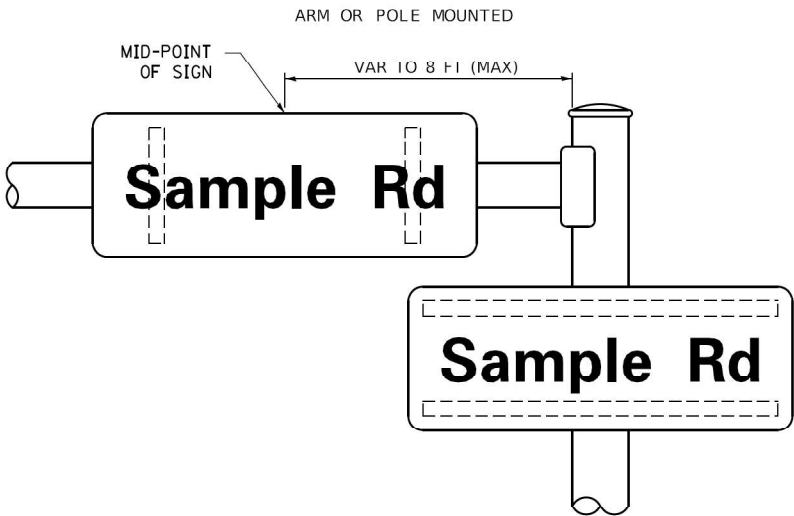
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

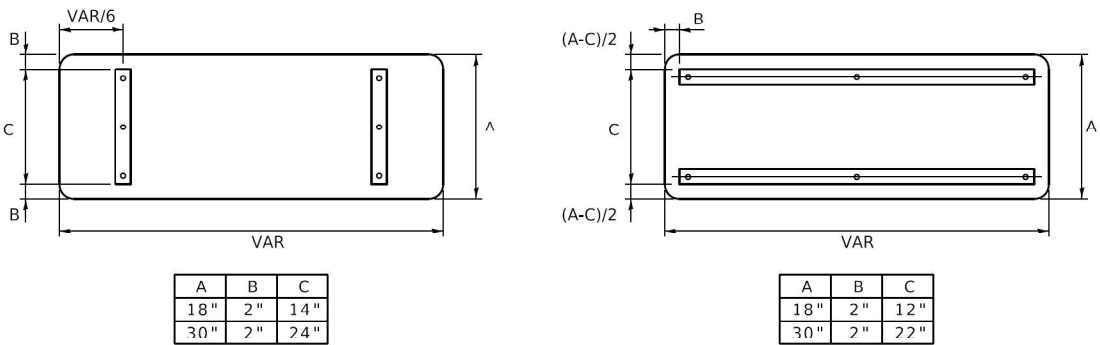
SIGN CHANNEL
SIGN SCREWS
PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.432	0.480	B	0.960	5.446	0.400
C	0.720	4.432	0.720	C	0.800	5.446	0.800
D	0.880	4.432	0.720	D	0.960	5.446	0.800
E	0.880	4.032	0.480	E	0.960	4.962	0.400
F	0.880	4.032	0.240	F	0.960	4.962	0.240
G	0.720	4.432	0.720	G	0.800	5.446	0.800
H	0.880	4.432	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.032	0.880	J	0.240	5.122	0.960
K	0.880	4.432	0.480	K	0.960	5.604	0.400
L	0.880	4.032	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.432	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.432	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.432	0.480	R	0.960	5.446	0.400
S	0.480	4.432	0.480	S	0.400	5.446	0.400
T	0.240	4.032	0.240	T	0.240	4.962	0.240
U	0.880	4.432	0.880	U	0.960	5.446	0.960
V	0.240	4.952	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.432	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.032	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.032	0.720	d	0.480	4.802	0.800
e	0.480	4.032	0.320	e	0.480	4.722	0.320
f	0.320	2.430	0.160	f	0.320	2.882	0.160
g	0.480	4.032	0.720	g	0.480	4.802	0.800
h	0.720	4.032	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.032	0.640	n	0.800	4.722	0.720
o	0.480	4.032	0.480	o	0.480	4.882	0.480
p	0.720	4.032	0.480	p	0.800	4.802	0.480
q	0.480	4.032	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.352	0.240	s	0.320	3.762	0.240
t	0.080	2.832	0.080	t	0.080	3.202	0.080
u	0.640	4.032	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.952	0.160	y	0.160	6.004	0.160
z	0.240	3.352	0.240	z	0.240	4.002	0.240
1	0.720	1.630	0.880	1	0.800	2.000	0.960
2	0.480	4.432	0.480	2	0.800	5.446	0.800
3	0.480	4.432	0.480	3	1.440	5.446	0.800
4	0.240	4.952	0.720	4	0.160	6.004	0.960
5	0.480	4.432	0.480	5	0.800	5.446	0.800
6	0.720	4.432	0.720	6	0.800	5.446	0.800
7	0.240	4.432	0.720	7	0.560	5.446	0.560
8	0.480	4.432	0.480	8	0.800	5.446	0.800
9	0.480	4.432	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

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PLOT SCALE = 50.0000' / in.	CHECKED - IP	REVISED -
PLOT DATE = 3/4/2019	DATE - 10/01/2014	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	75
TS-02		CONTRACT NO. 62W47		
ILLINOIS		FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

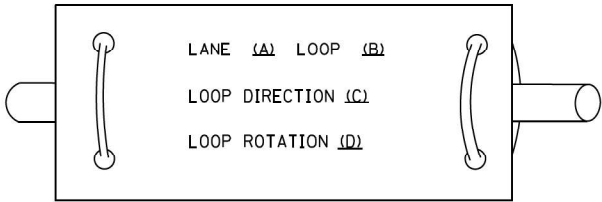
ITEM				ITEM				ITEM			
		EXISTING	PROPOSED			EXISTING	PROPOSED			EXISTING	PROPOSED
CONTROLLER CABINET				HANDHOLE				SIGNAL HEAD			
COMMUNICATION CABINET				-SQUARE				-(P) PROGRAMMABLE SIGNAL HEAD			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE							
MASTER MASTER CONTROLLER				-SQUARE							
UNINTERRUPTABLE POWER SUPPLY				-ROUND							
SERVICE INSTALLATION				DOUBLE HANDHOLE				SIGNAL HEAD WITH BACKPLATE			
-(P) POLE MOUNTED				JUNCTION BOX				-(P) PROGRAMMABLE SIGNAL HEAD			
SERVICE INSTALLATION				RAILROAD CANTILEVER MAST ARM				-(RB) RETROREFLECTIVE BACKPLATE			
-(G) GROUND MOUNTED				RAILROAD FLASHING SIGNAL							
-(GM) GROUND MOUNTED METERED				RAILROAD CROSSING GATE							
TELEPHONE CONNECTION				RAILROAD CROSSBUCK				PEDESTRIAN SIGNAL HEAD			
STEEL MAST ARM ASSEMBLY AND POLE				RAILROAD CONTROLLER CABINET				AT RAILROAD INTERSECTIONS			
ALUMINUM MAST ARM ASSEMBLY AND POLE				UNDERGROUND CONDUIT (UC), GALVANIZED STEEL				PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				ILLUMINATED SIGN			
SIGNAL POST				SYSTEM ITEM		S	SP	"NO LEFT TURN"/"NO RIGHT TURN"			
-(BM) BARREL MOUNTED - TEMPORARY				INTERSECTION ITEM		I	IP	NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED			
WOOD POLE				REMOVE ITEM			R	GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)			
GUY WIRE				RELOCATE ITEM			RL	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C			
SIGNAL HEAD				ABANDON ITEM			A	COAXIAL CABLE			
SIGNAL HEAD WITH BACKPLATE				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			RCF	VENDOR CABLE			
SIGNAL HEAD OPTICALLY PROGRAMMED				MAST ARM POLE AND FOUNDATION TO BE REMOVED			RMF	COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED			
FLASHER INSTALLATION				SIGNAL POST AND FOUNDATION TO BE REMOVED			RPF	FIBER OPTIC CABLE			
-(FS) SOLAR POWERED				DETECTOR LOOP, TYPE I				-NO. 62.5/125, MM12F			
				PREFORMED DETECTOR LOOP				-NO. 62.5/125, MM12F SM12F			
PEDESTRIAN SIGNAL HEAD				SAMPLING (SYSTEM) DETECTOR				-NO. 62.5/125, MM12F SM24F			
PEDESTRIAN PUSH BUTTON				INTERSECTION AND SAMPLING (SYSTEM) DETECTOR				GROUND ROD			
-(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON				QUEUE AND SAMPLING (SYSTEM) DETECTOR				-(C) CONTROLLER			
RADAR DETECTION SENSOR				WIRELESS DETECTOR SENSOR				-(M) MAST ARM			
VIDEO DETECTION CAMERA				WIRELESS ACCESS POINT				-(P) POST			
RADAR/VIDEO DETECTION ZONE								-(S) SERVICE			
PAN, TILT, ZOOM (PTZ) CAMERA											
EMERGENCY VEHICLE LIGHT DETECTOR											
CONFIMATION BEACON											
WIRELESS INTERCONNECT											
WIRELESS INTERCONNECT RADIO REPEATER											

MODEL Default FILE Name: p:\s\UL044EBID\NTEG\Illinois\gov\PIV\DOT\Documents\DOT_Offices\Dir\trct_1\proj\cts\DI\HS422\234\CAD\Drawal\CADsheet\ts05.dgn	USER NAME = footem]		DESIGNED - IP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - IP		REVISIED -	REVISED -		STANDARD TRAFFIC SIGNAL DESIGN DETAILS				3578	2023-925-N-TS(L)	COOK	176	76
	PLOT SCALE = 50.0000 ' / in.		CHECKED - LP	REVISED -						TS-05		CONTRACT NO.62W47		
	PLOT DATE = 3/4/2019		DATE - 9/29/2016	REVISED -		SCALE: NONE	SHEET 1	OF 7	SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT	

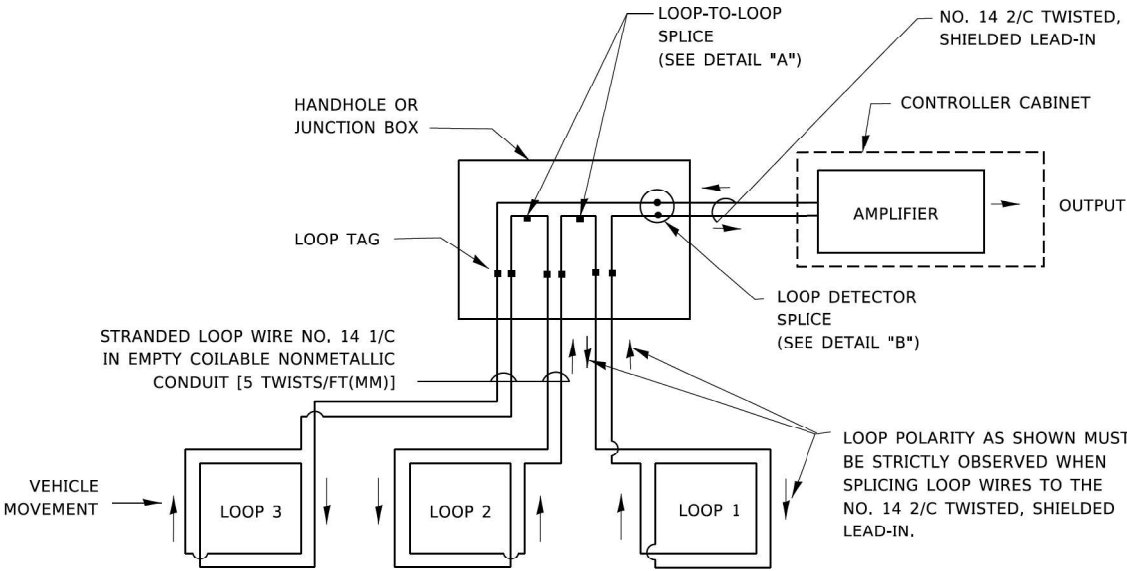
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

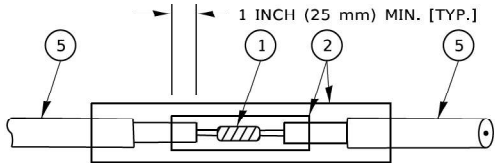


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

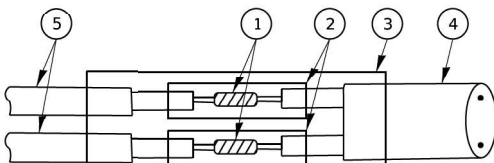


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

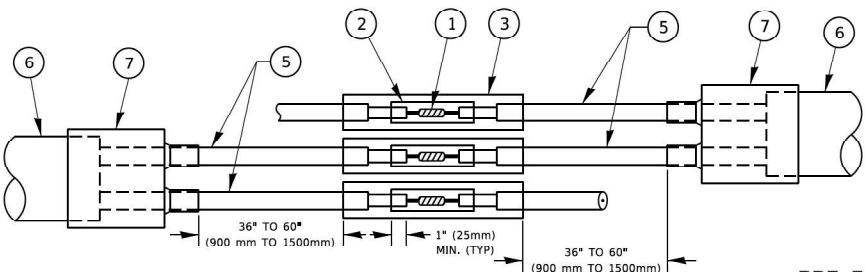


DETAIL "A"
LOOP-TO-LOOP SPLICE

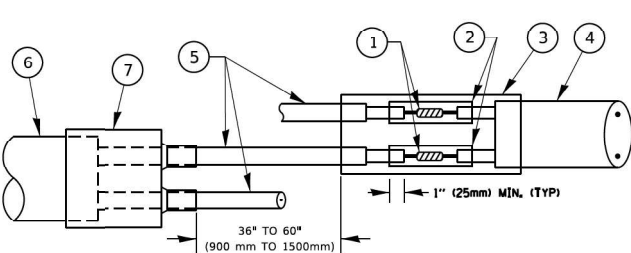


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

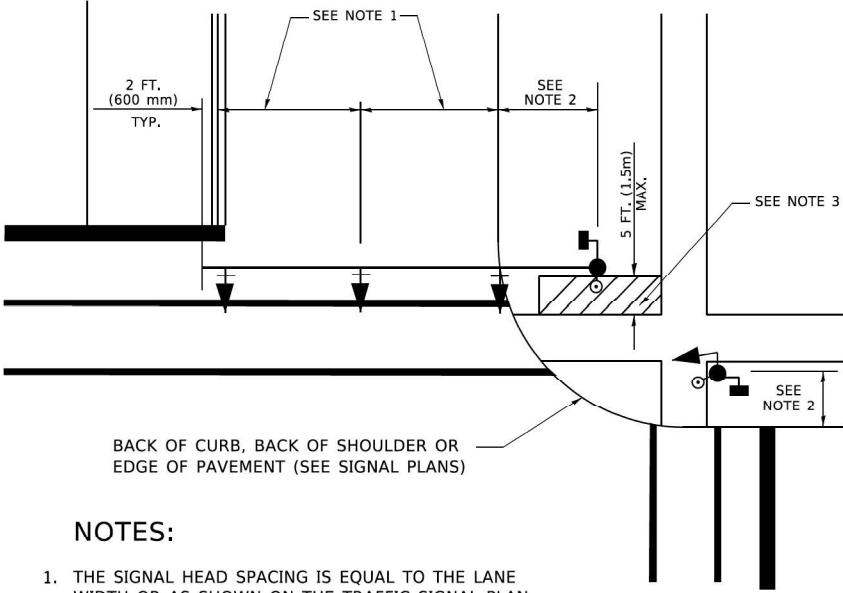
SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	77
TS-05		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

MODEL: Default
FILE NAME: p:\s\110484E\BID\NTEC\Illinois\p\WIDOT\Documents\DOT_Offices\District_1\Project\CD\HSH22\23\1\CAD\Detail\CADsheet\ts05.dgn

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

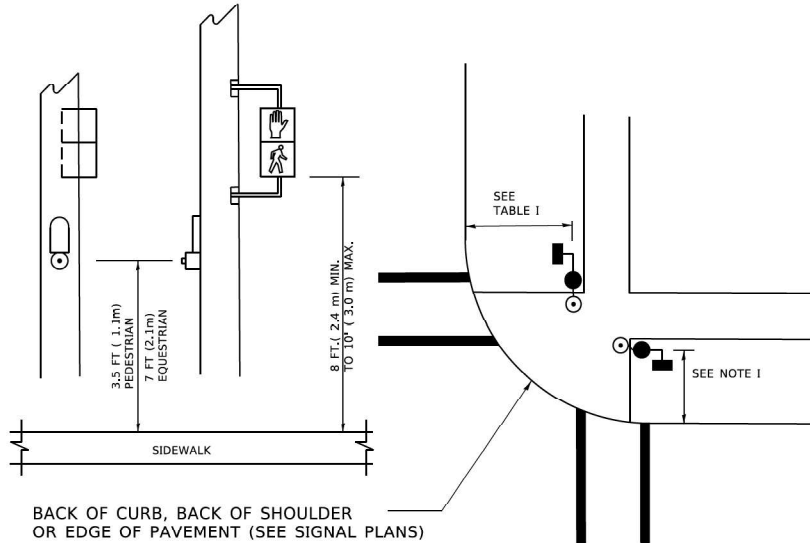
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

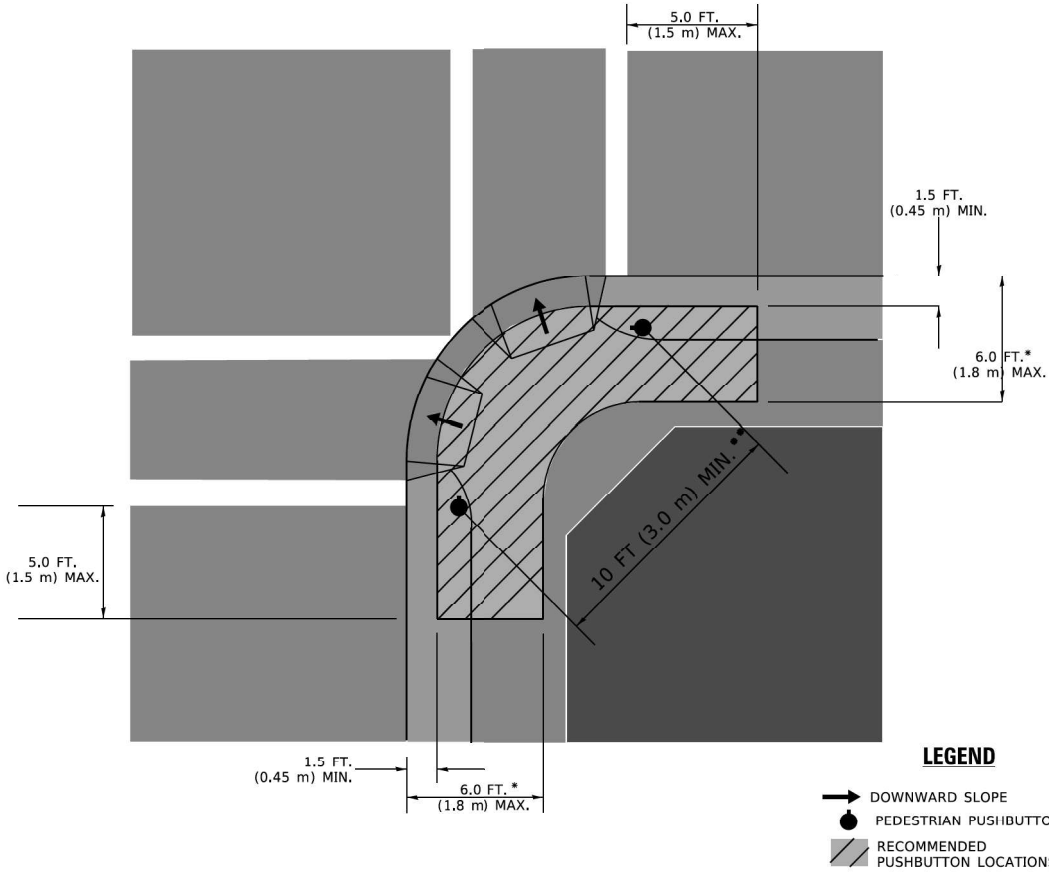
PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.5m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

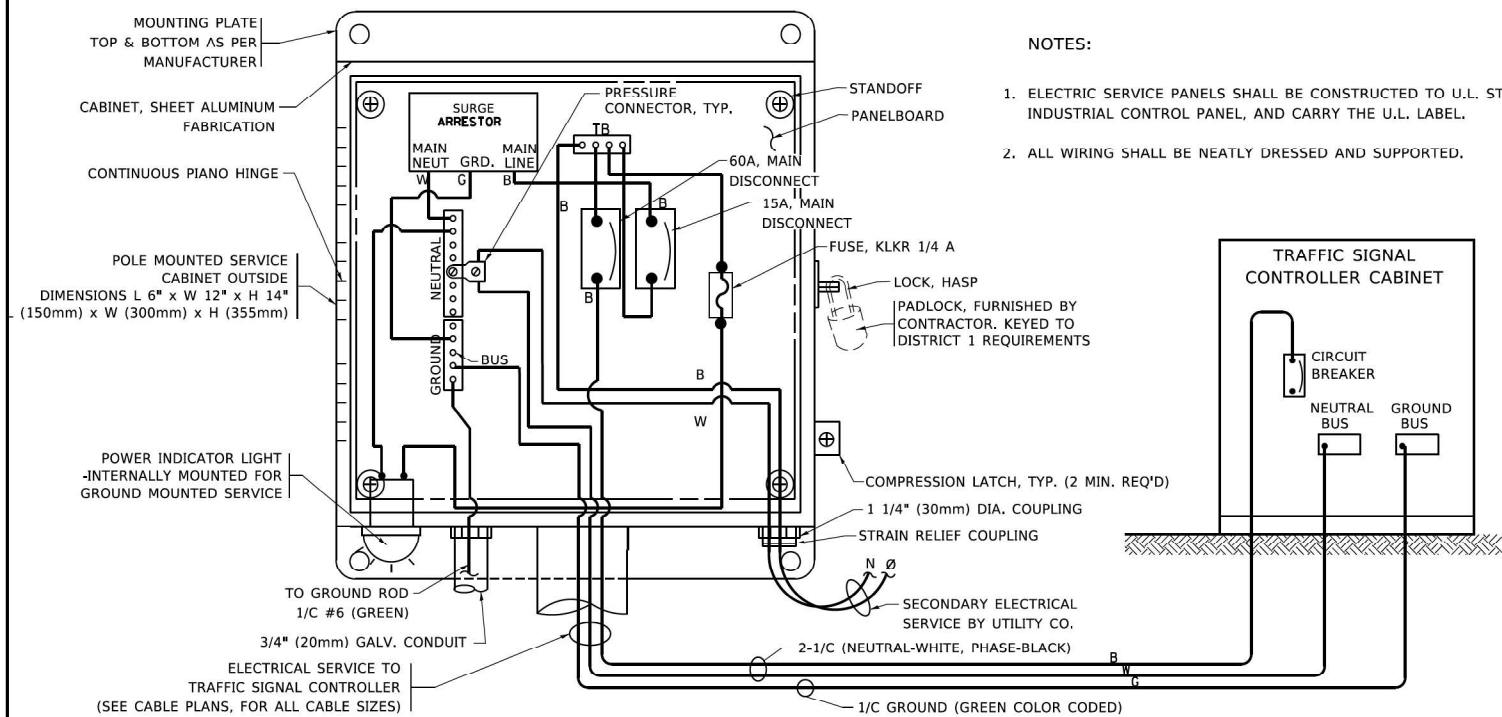
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

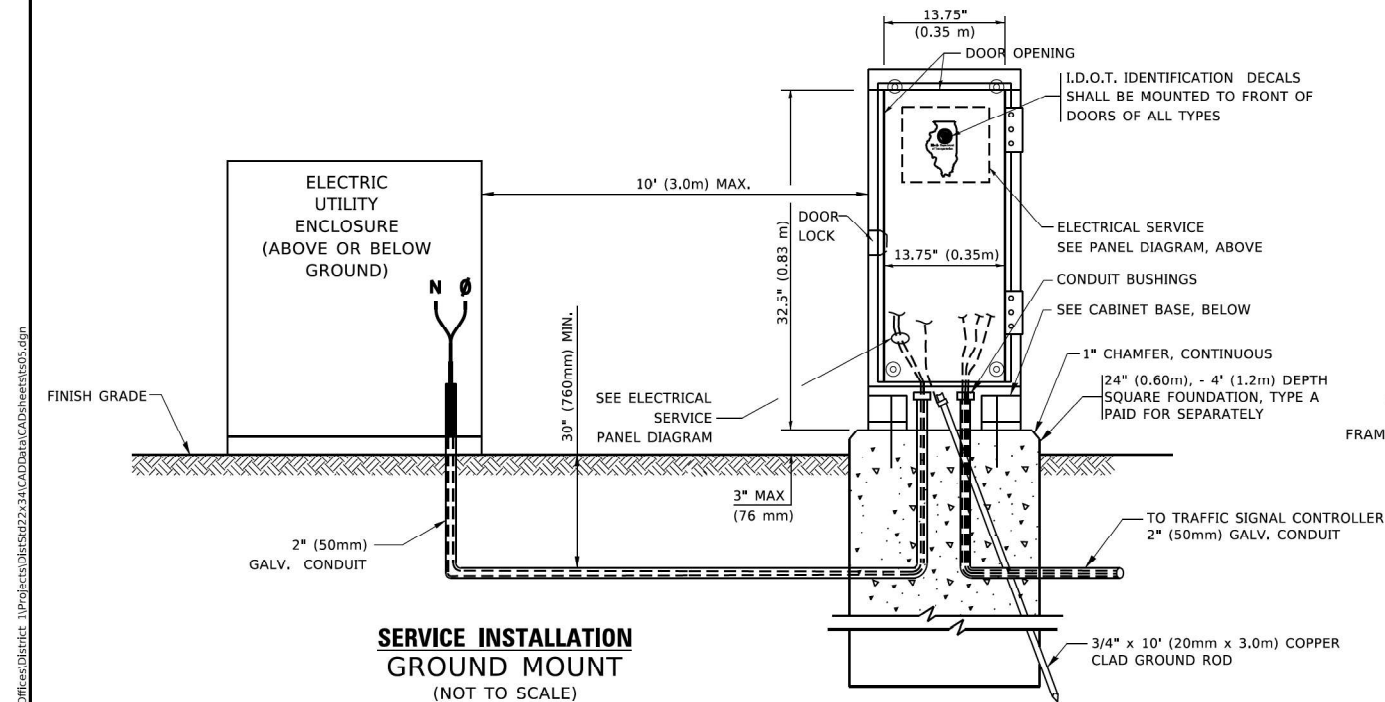
SCALE: NONE SHEET 3 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	78
TS-05		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

MODEL: Default
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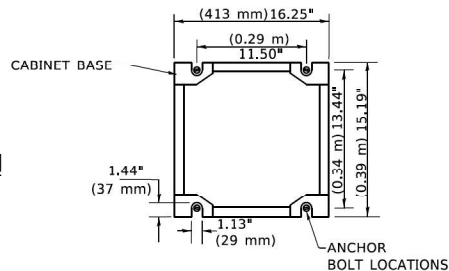


ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)



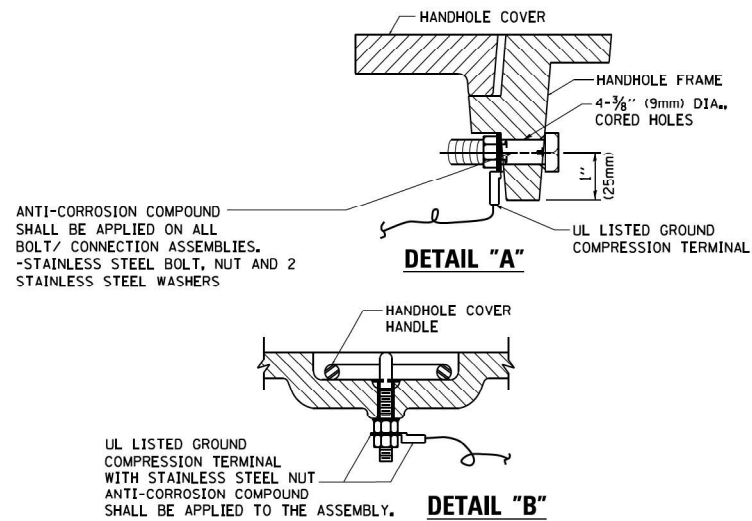
SERVICE INSTALLATION GROUND MOUNT
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN
(NOT TO SCALE)

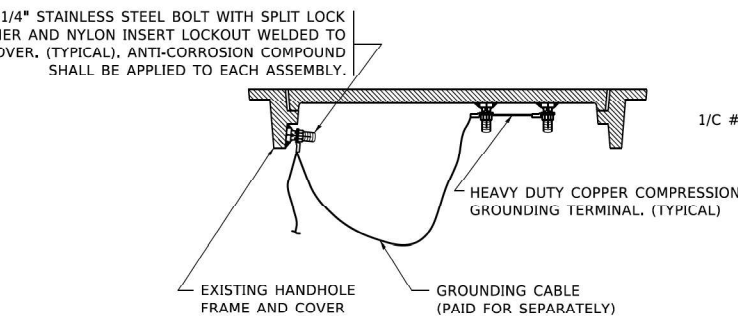


NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



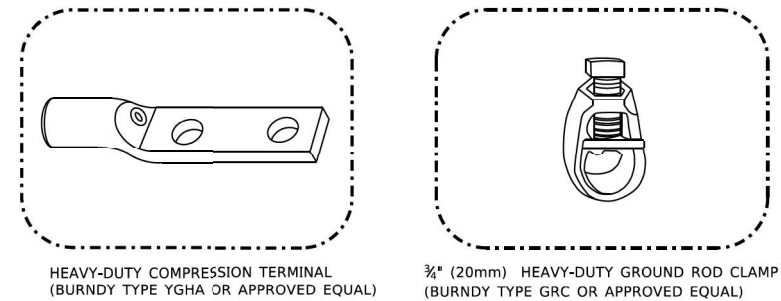
HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL
(NOT TO SCALE)

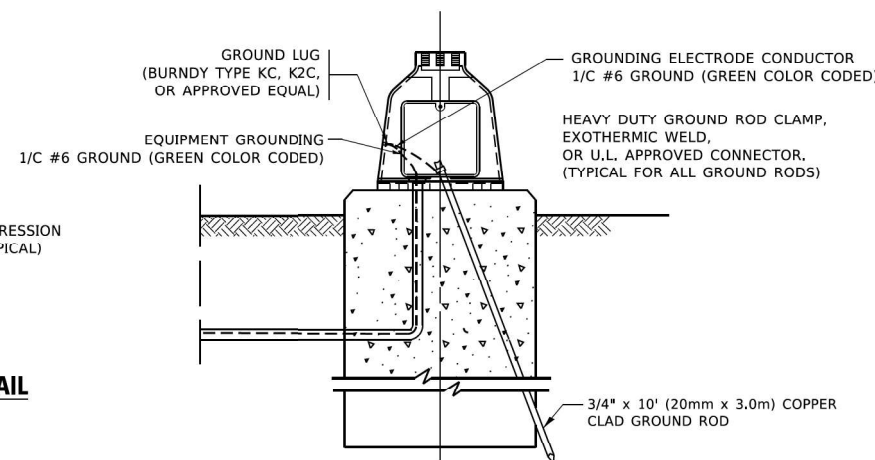
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4\"/>
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



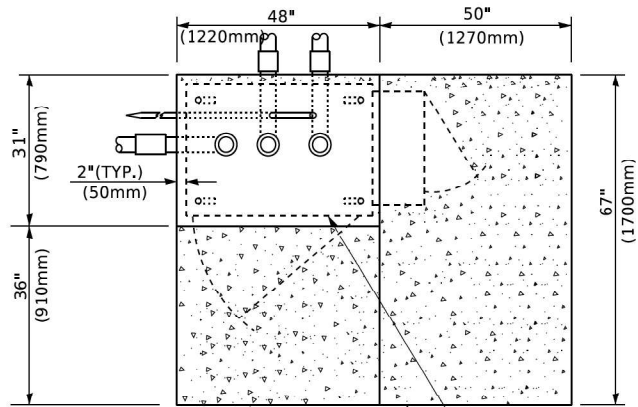
MAST ARM POLE /POST-GROUNDING DETAIL
(NOT TO SCALE)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

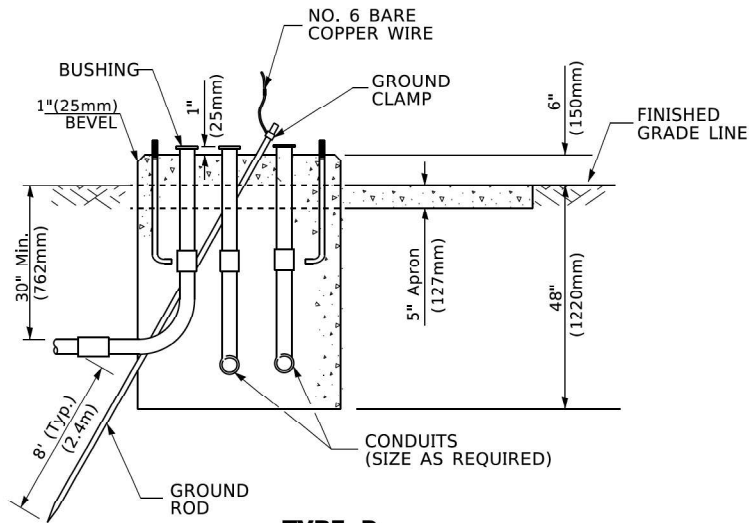
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

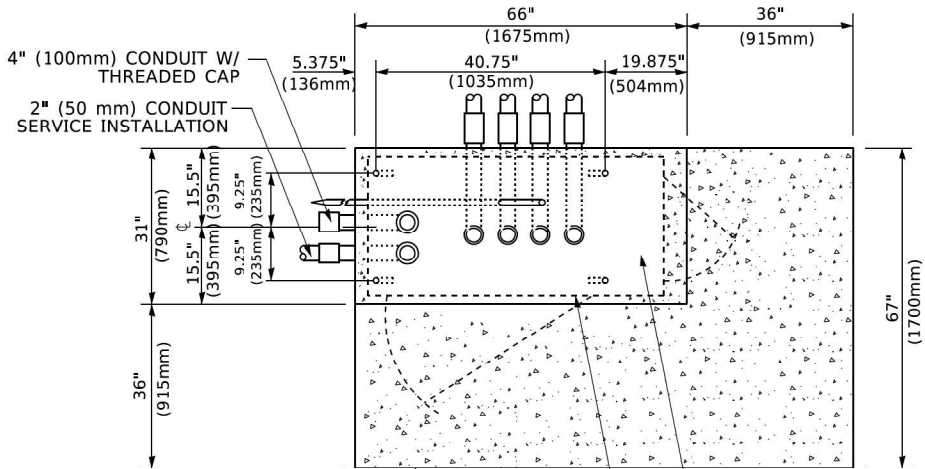
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-025-N-TS(L)	COOK	176	79
TS-05		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		



TOP VIEW



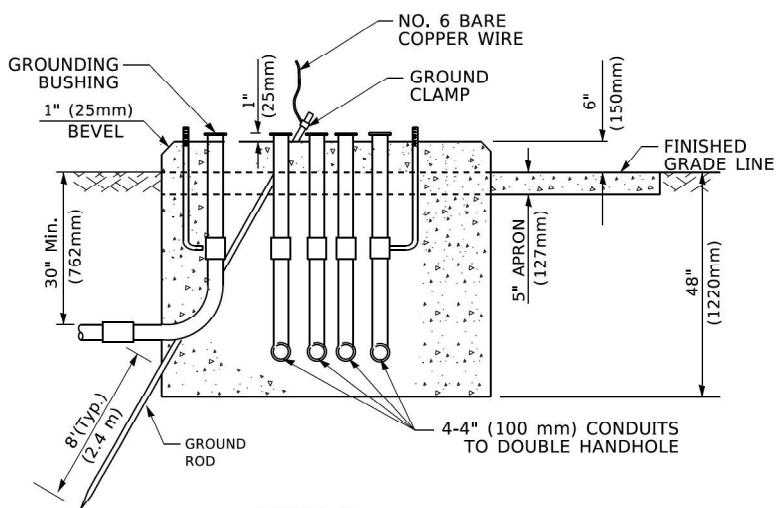
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



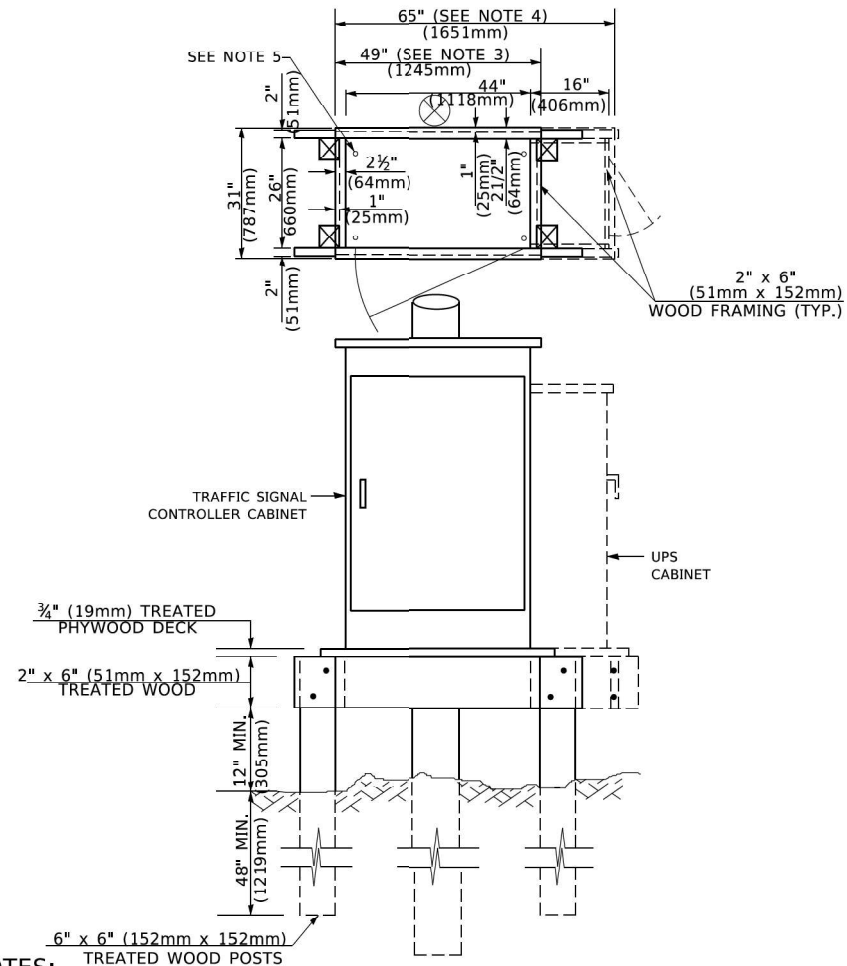
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL
BE HIGHER THAN TOP OF
DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

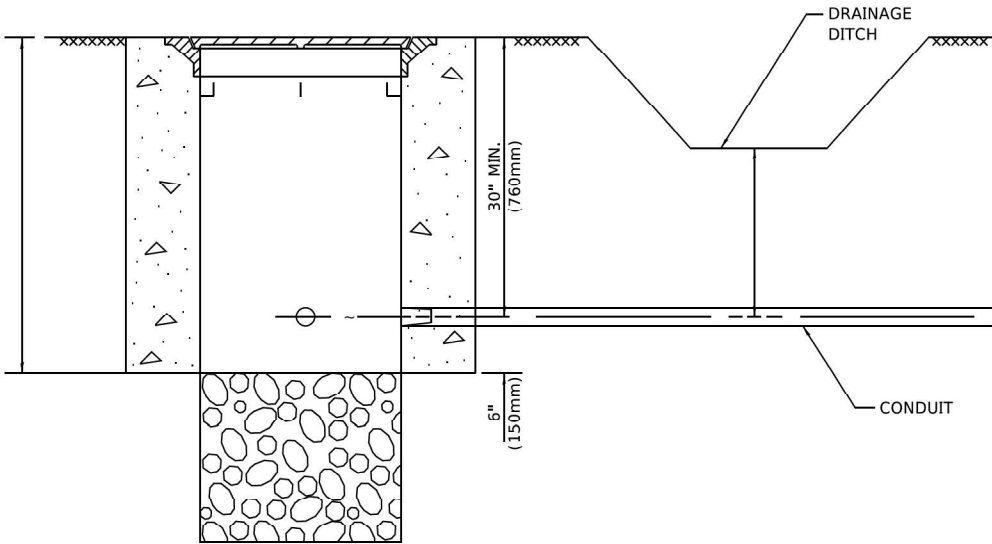
MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Q_u) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

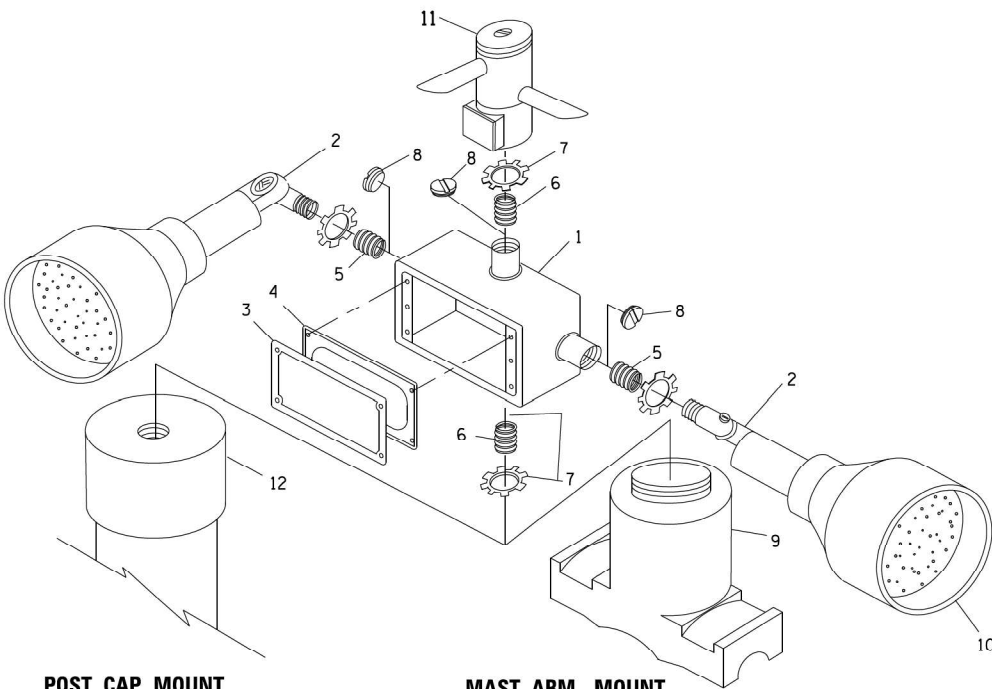
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	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -						3578	2023-025-N-TS(L)	COOK	176	80
	PLOT DATE = 3/4/2019	DATE -	REVISED -		TS-05				CONTRACT NO. 62W47				
	SCALE: NONE		SHEET 5 OF 7 SHEETS		STA.		TO STA.						



NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBIF HANDHOLES.

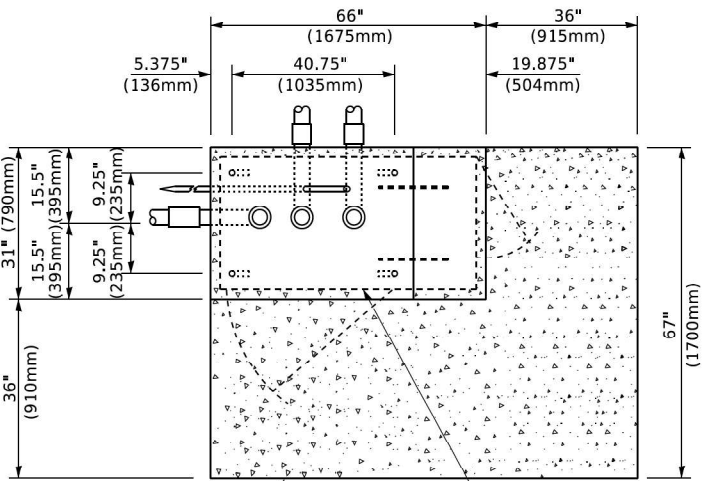
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



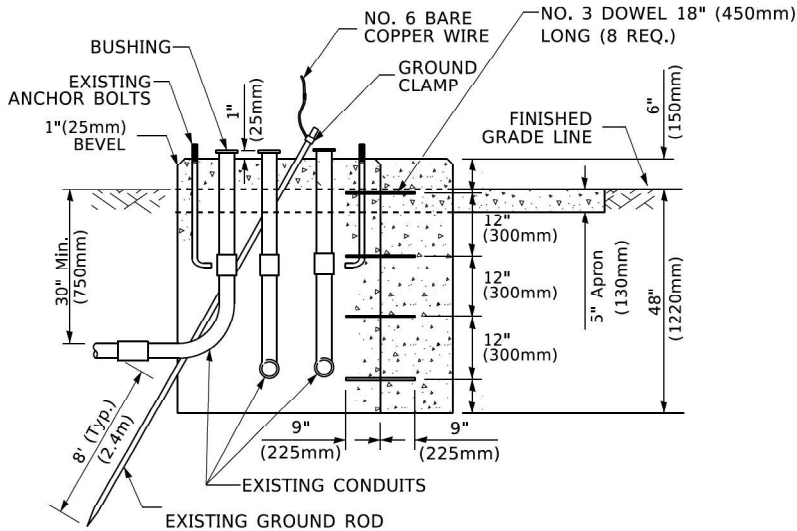
POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION
BEACON MOUNTING DETAIL



PROPOSED APRON
TOP VIEW
(NOT TO SCALE)
CONTROLLER CABINET BASE

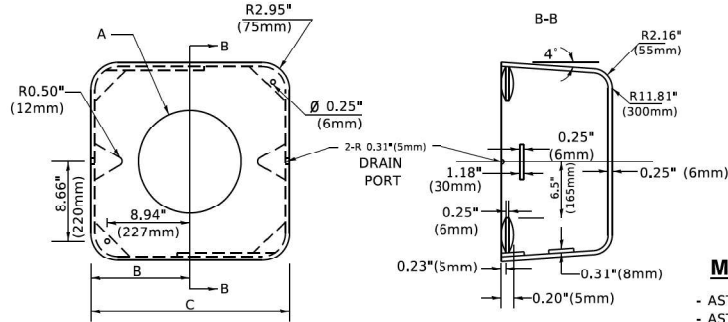


MODIFY EXISTING TYPE "D" FOUNDATION
TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 " (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



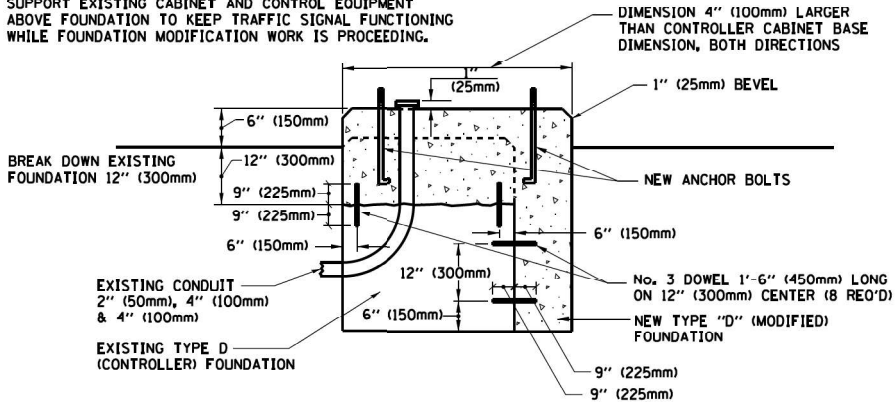
SHROUD

NOTES:

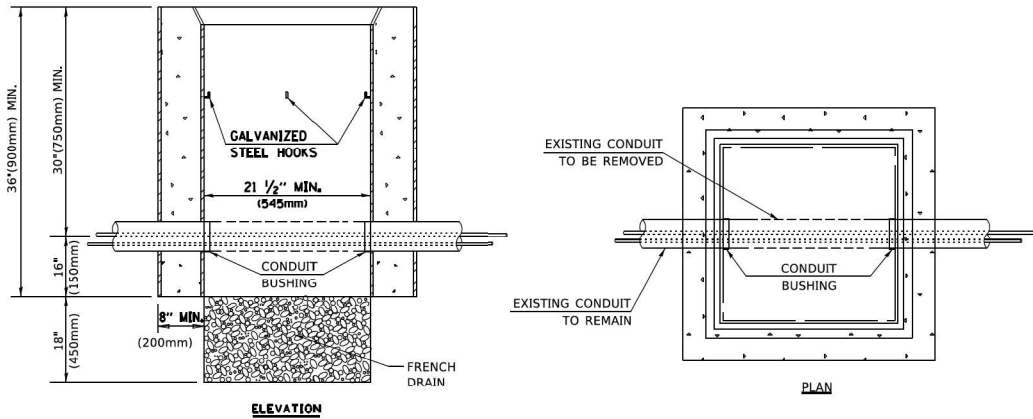
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

MODEL: Default
FILE NAME: p:\s\ul\044EB\DOT\Tech\Illinois\DOT\Office\Dr\trct_1\Proj\cts\0145422\23\CAD\Draws\CADsheet\ts05.dgn
h05.dgn 3/4/2019 11:23:51 AM User=footem

USER NAME = footem]	DESIGNED -	REVISED -
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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

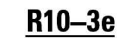
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	81
TS-05		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	



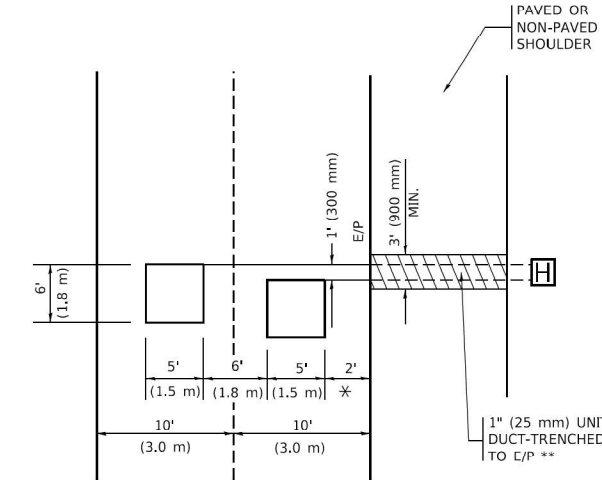
CONCRETE FOUNDATION,
TYPE A 12-INCH DIAMETER



1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

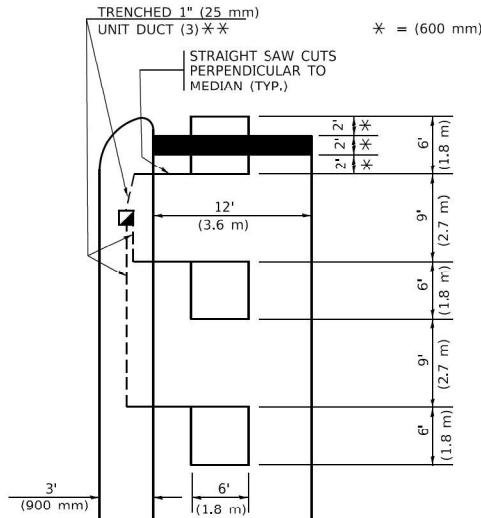


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

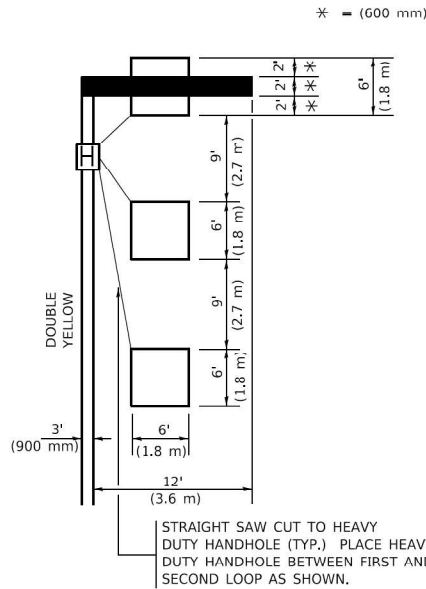
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED,
SHIELDED.

* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE
LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
PAVEMENT.

* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT
DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST
HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE
SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID
FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM
FOR DETECTOR LOOPS.

* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET
(1.8 m)

* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH
LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE
INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.

* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE
THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR
(i.e. 1-1/2, 1-3/4, 2).

* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN
INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND
INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM
DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A
SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE
AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW
CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE
TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM)
DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN
AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS
UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE
LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR
LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR
SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED
ON ALL SIGNAL LAYOUT PLAN SHEETS.

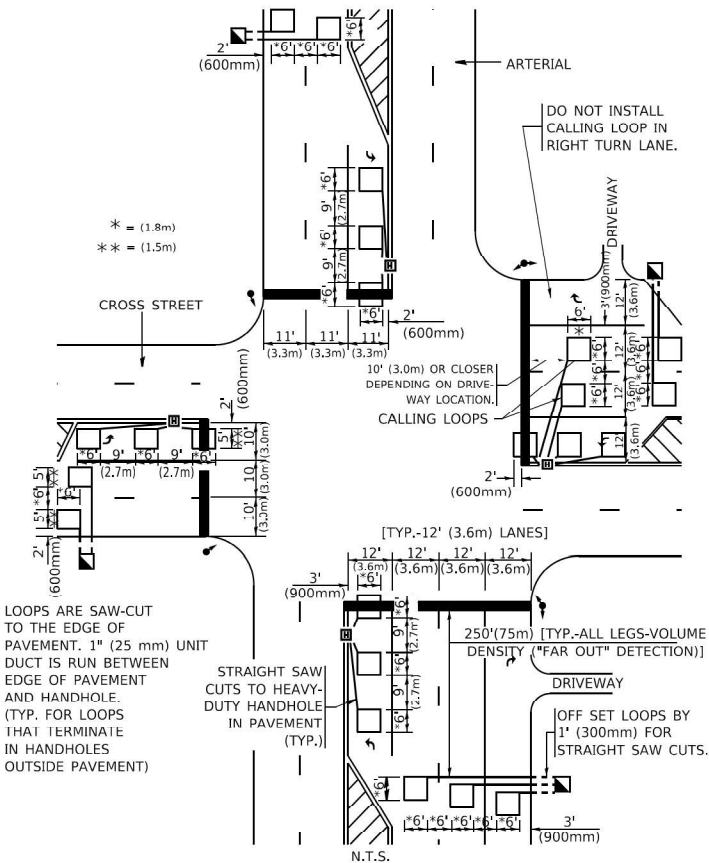
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE
DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT
TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN
ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO
NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND
10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF
PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

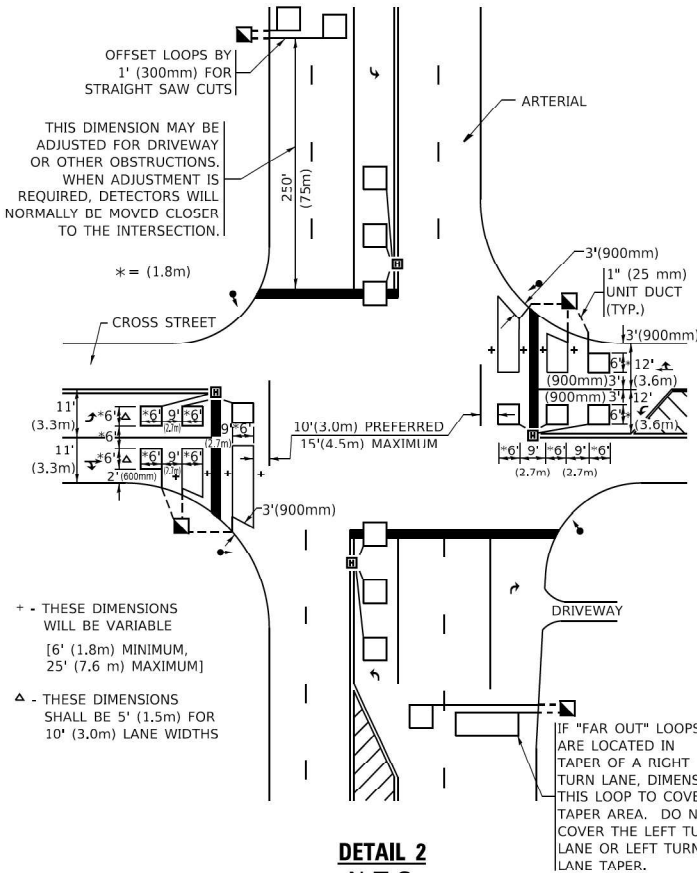
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER
FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE
DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS
FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-025-N-TS(L)	COOK	176	83
TS-07		CONTRACT NO. 62W47		
ILLINOIS		FED. AID PROJECT		

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CABINET
- 4 EACH MAST ARM ASSEMBLY AND POST
- 1 EACH PEDESTRIAN PUSH-BUTTON POST
- 5 EACH TRAFFIC SIGNAL POST
- 8 EACH 3-SECTION SIGNAL HEAD
- 8 EACH 5-SECTION SIGNAL HEAD
- 12 EACH PEDESTRIAN SIGNAL HEAD
- 6 EACH PEDESTRIAN PUSH-BUTTON
- 1 EACH SERVICE INSTALLATION
- 6 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH MAST ARM MOUNTED STREET NAME SIGNS

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR THE PICK UP OF ALL THE EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY.

VILLAGE OF WORTH

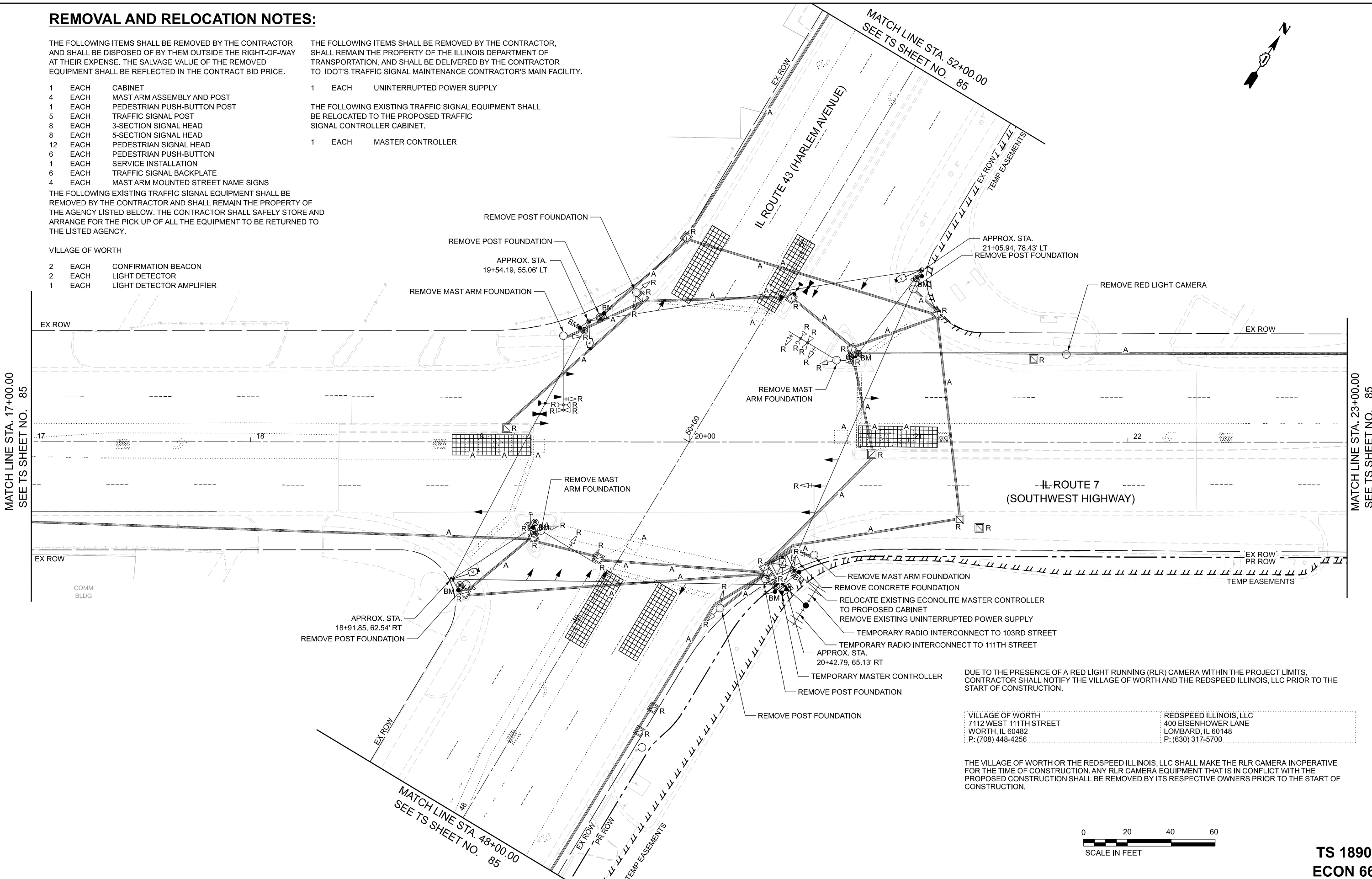
- 2 EACH CONFIRMATION BEACON
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION, AND SHALL BE DELIVERED BY THE CONTRACTOR TO IDOT'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY.

- 1 EACH UNINTERRUPTED POWER SUPPLY

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED TO THE PROPOSED TRAFFIC SIGNAL CONTROLLER CABINET.

- 1 EACH MASTER CONTROLLER



DUE TO THE PRESENCE OF A RED LIGHT RUNNING (RLR) CAMERA WITHIN THE PROJECT LIMITS, CONTRACTOR SHALL NOTIFY THE VILLAGE OF WORTH AND THE REDSPEED ILLINOIS, LLC PRIOR TO THE START OF CONSTRUCTION.

VILLAGE OF WORTH 7112 WEST 111TH STREET WORTH, IL 60482 P: (708) 448-4256	REDSPEED ILLINOIS, LLC 400 EISENHOWER LANE LOMBARD, IL 60148 P: (630) 317-5700
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THE VILLAGE OF WORTH OR THE REDSPEED ILLINOIS, LLC SHALL MAKE THE RLR CAMERA INOPERATIVE FOR THE TIME OF CONSTRUCTION. ANY RLR CAMERA EQUIPMENT THAT IS IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY ITS RESPECTIVE OWNERS PRIOR TO THE START OF CONSTRUCTION.



TS 1890
ECON 66

MODEL: SHEET 1
FILE NAME: W:\191+200 IDOT_208010_108_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-TEMP_TS.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. 17+00.00 TO STA. 23+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	84
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHEET 2
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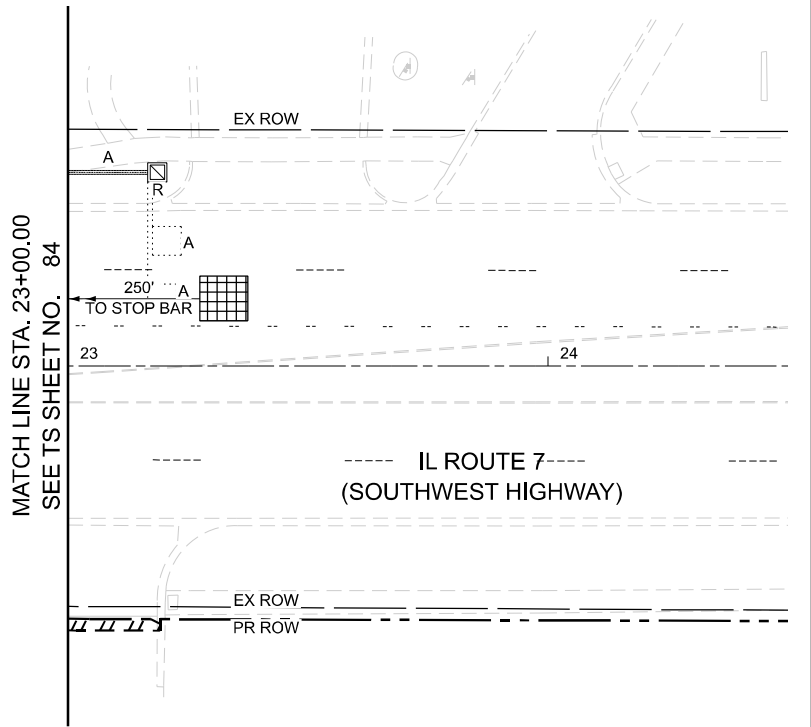
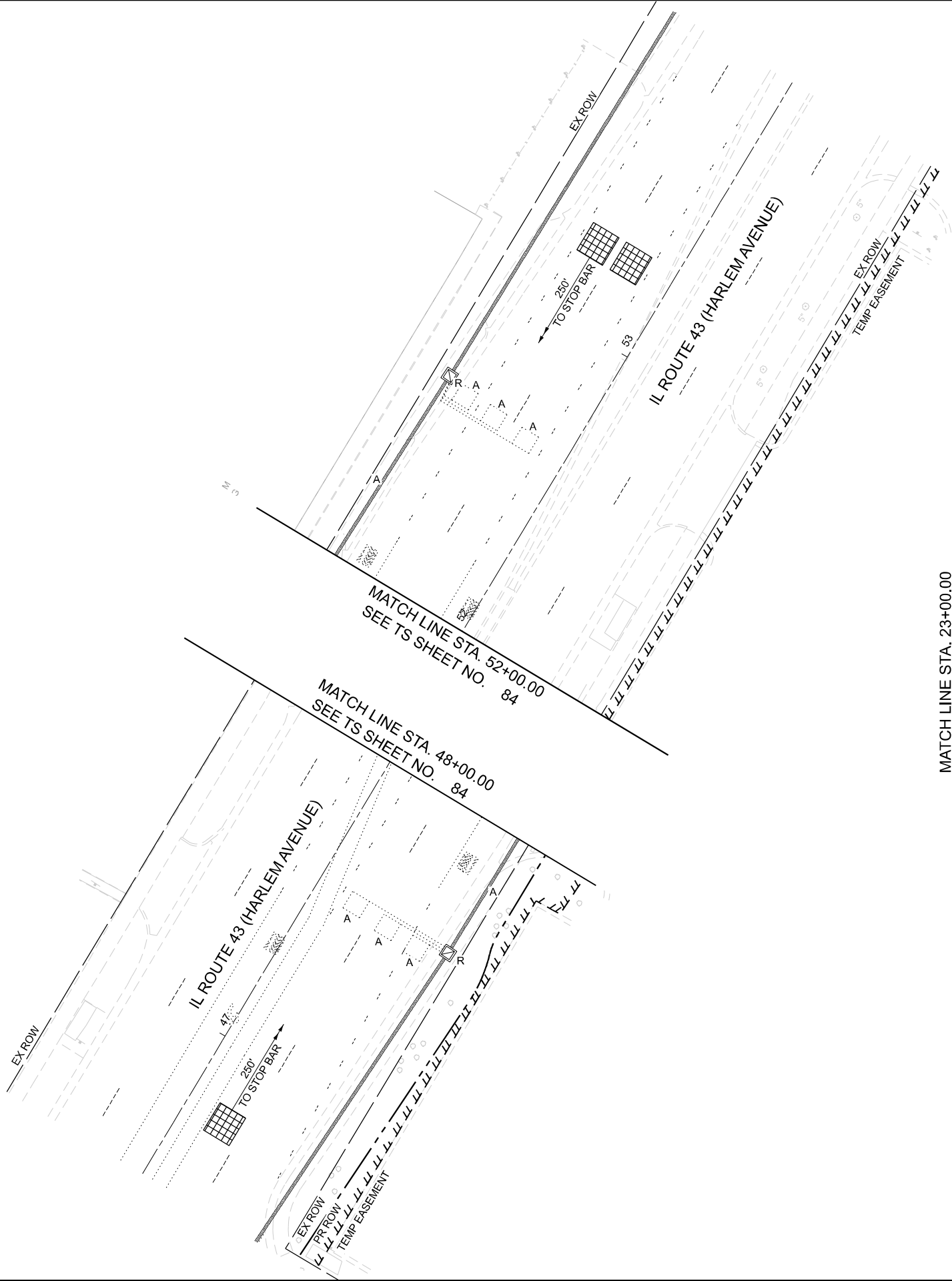
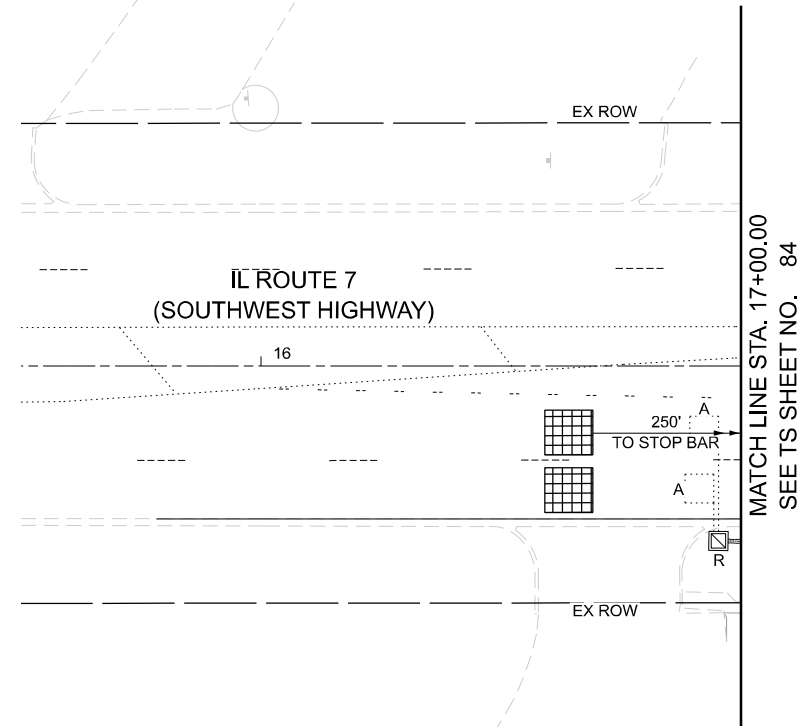
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		CHECKED -	GJE	REVISED -	
PLOT DATE	= 10/17/2025	DATE	= 10/15/2025	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1"=20' SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	85
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



TS 1890
ECON 66

MODEL: SHEET 3
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PLOT DATE	= 10/17/2025

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DRAWN	- TN
CHECKED	- GJE
DATE	= 10/15/2025

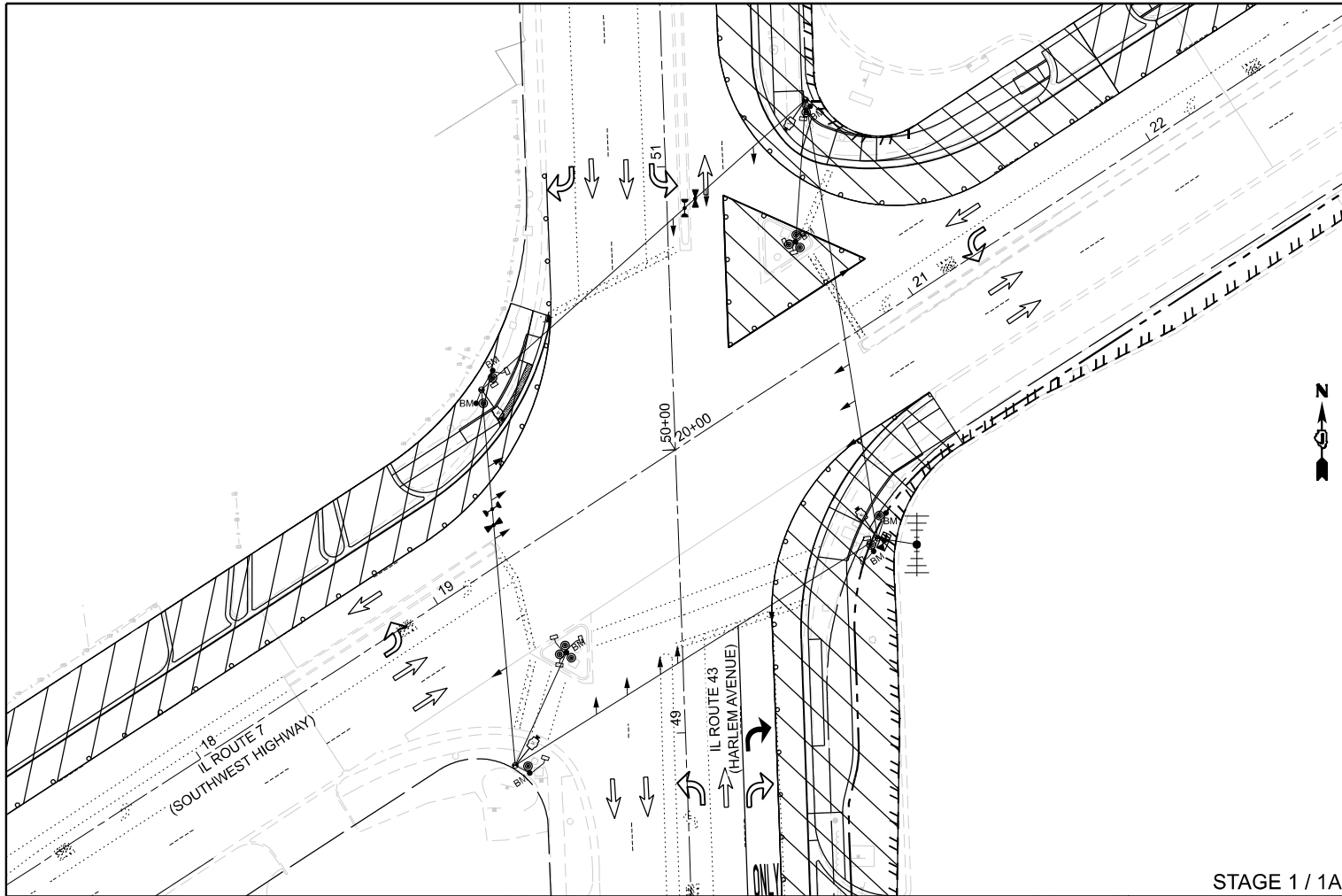
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

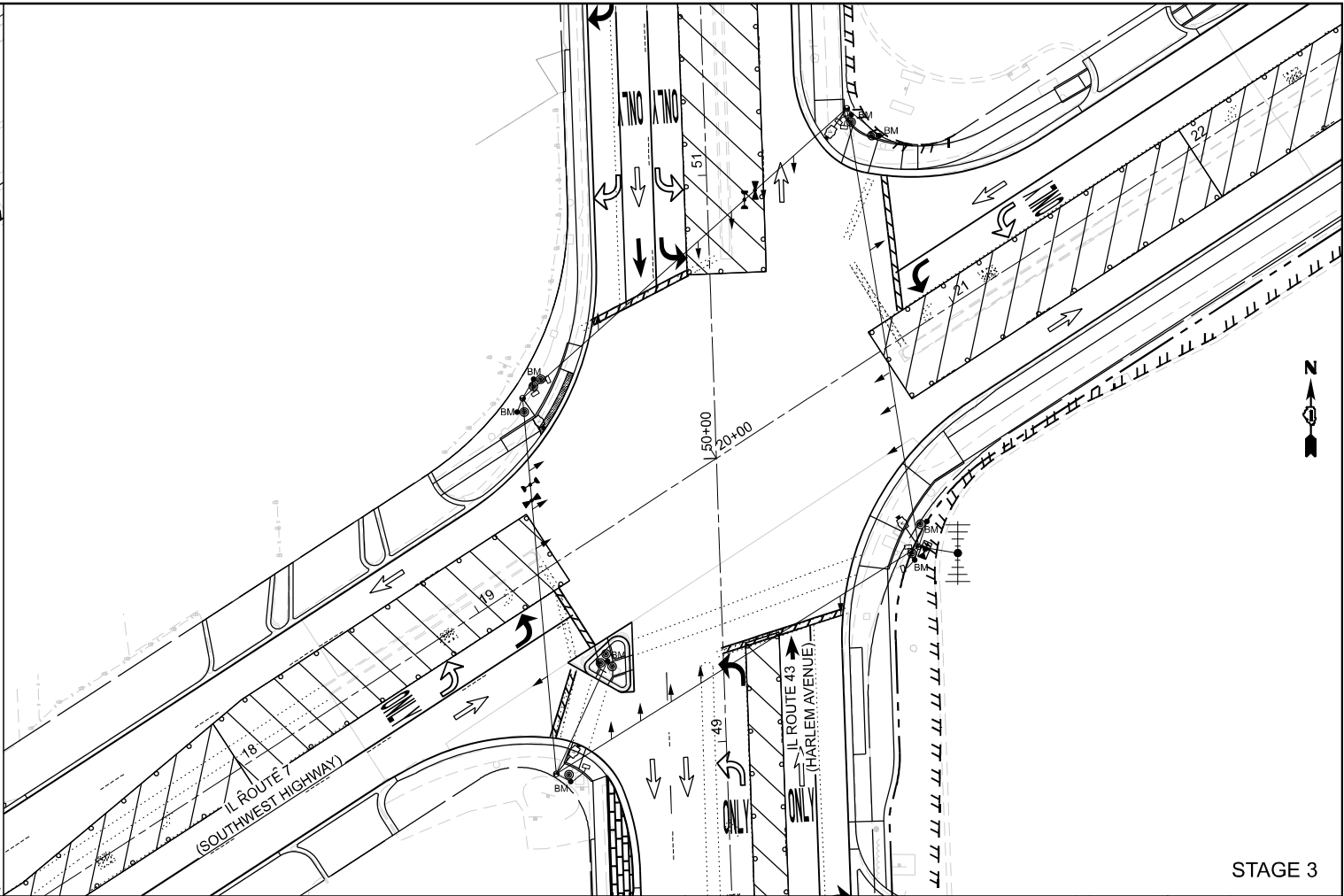
TEMPORARY TRAFFIC SIGNAL INSTALLATION - STAGE 1 TO STAGE 3
IL, ROUTE 7 (SOUTHWEST HIGHWAY) AT IL, ROUTE 43 (HARLEM AVENUE)

SCALE: 1"=30' SHEET 1 OF 1 SHEETS STA. TO STA.

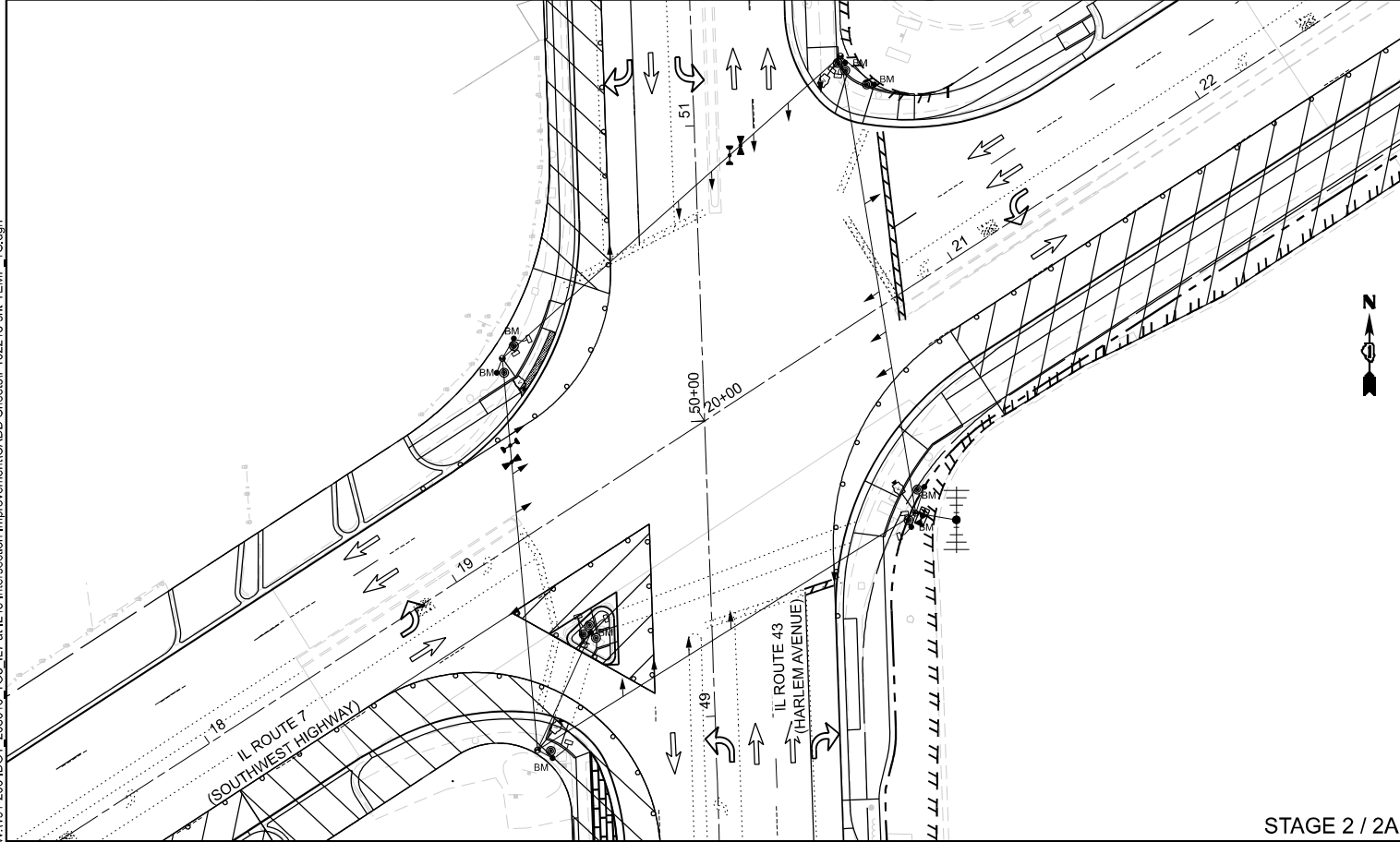
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CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



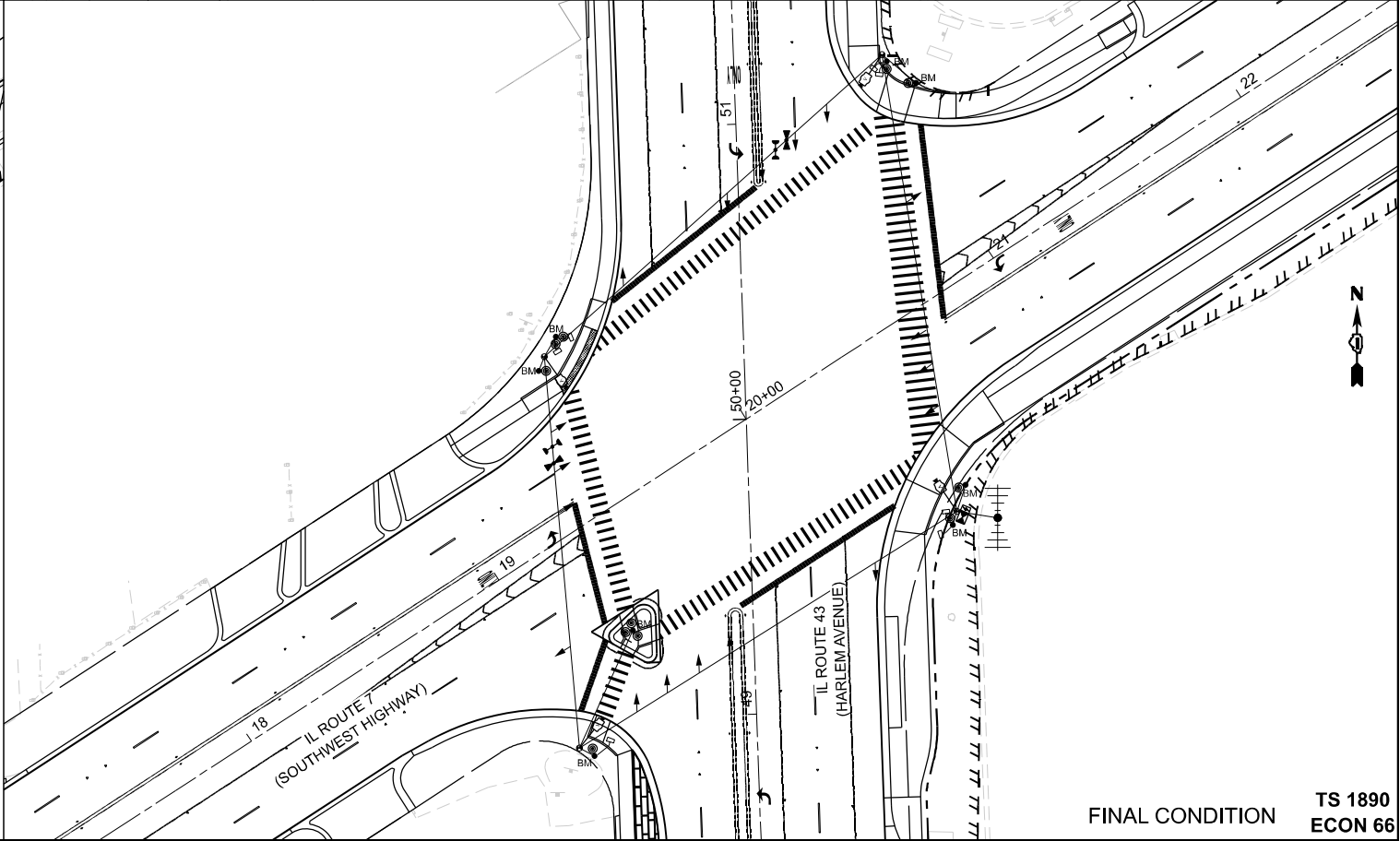
STAGE 1 / 1A



STAGE 3

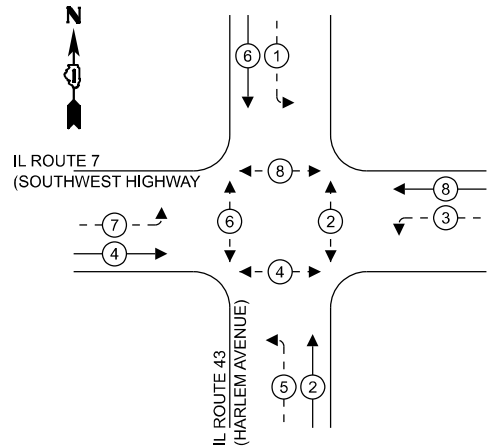


STAGE 2 / 2A



FINAL CONDITION TS 1890
ECON 66

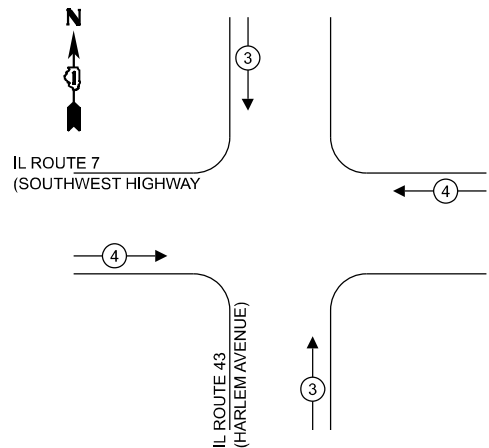
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE



TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	10	11	110
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	13	15	195
CONTROLLER	-	150	-
MASTER CONTROLLER	1	100	100
UPS	1	25	25
DETECTION RADAR OR VIDEO	4	20	80
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	1	35	35
CELLULAR MODEM	1	15	15
TOTAL UPS SIZING			664
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1,269

ENERGY COSTS TO:

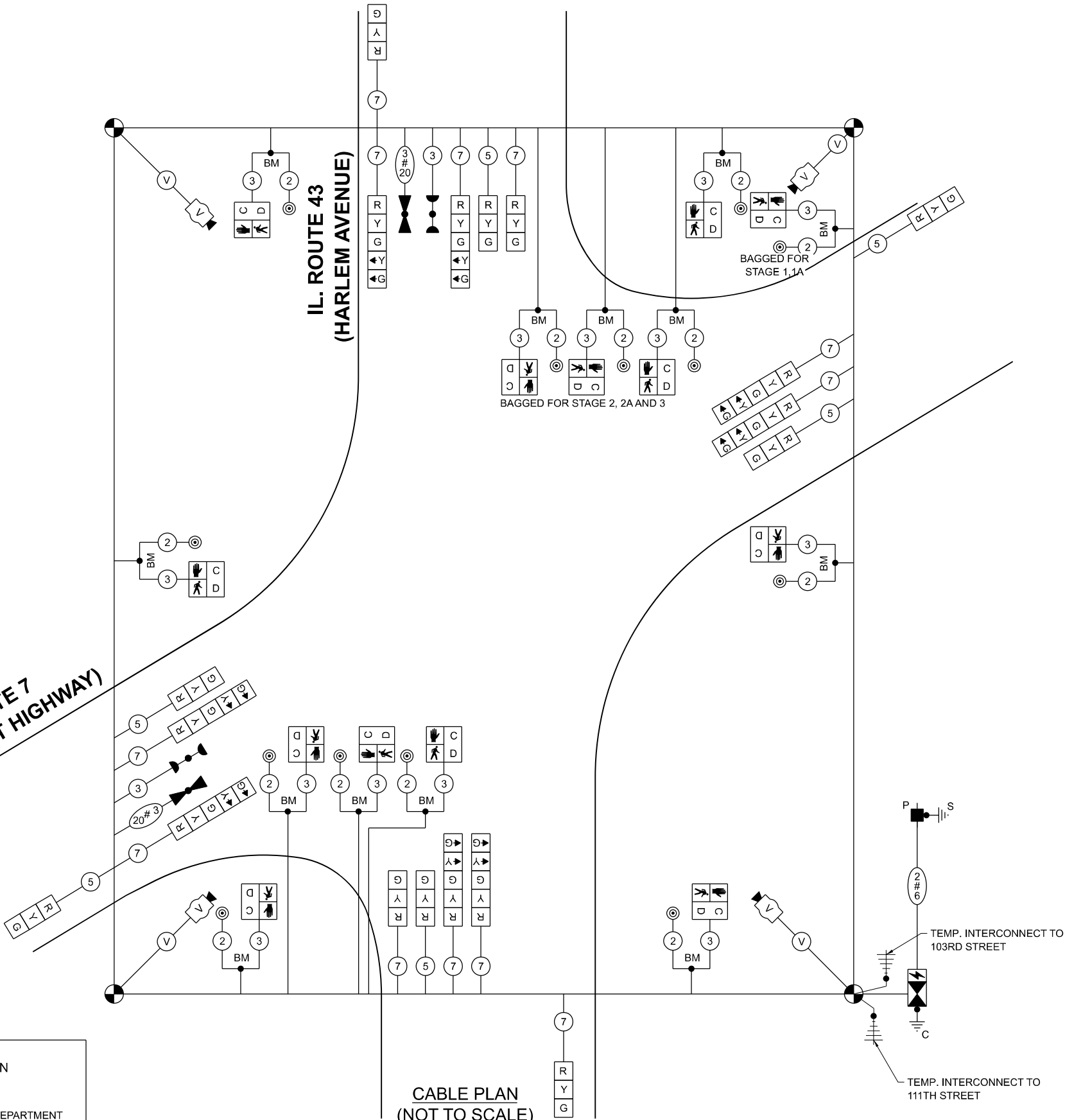
ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT
PHONE: (866) 639-3532
COMPANY: COMED
ACCOUNT NUMBER: 76630-18807
METER NUMBER: ---

IL. ROUTE 7
(SOUTHWEST HIGHWAY)

IL. ROUTE 43
(HARLEM AVENUE)

CABLE PLAN
(NOT TO SCALE)



TEMP. INTERCONNECT TO
103RD STREET

TEMP. INTERCONNECT TO
111TH STREET

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALL STAGES - CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)

SCALE: N.T.S SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	87
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: SHEET 5 Cable plan stage 1
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BLA, Inc.

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PLOT DATE = 11/13/2025

DESIGNED - TN	REVISED -
DRAWN - TN	REVISED -
CHECKED - GJE	REVISED -
DATE - 10/15/2025	REVISED -

TS 1890
ECON 66

MODEL: Unnamed Plan - Plan 1 [Sheet]
FILE NAME: W:\191-200 DOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-TEMP Interconnect.dgn



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DESIGNED	- TN
DRAWN	- TN
CHECKED	- GJE
DATE	- 10/15/2025
PLOT DATE	= 11/13/2025

DESIGNED	- TN
DRAWN	- TN
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DATE	- 10/15/2025

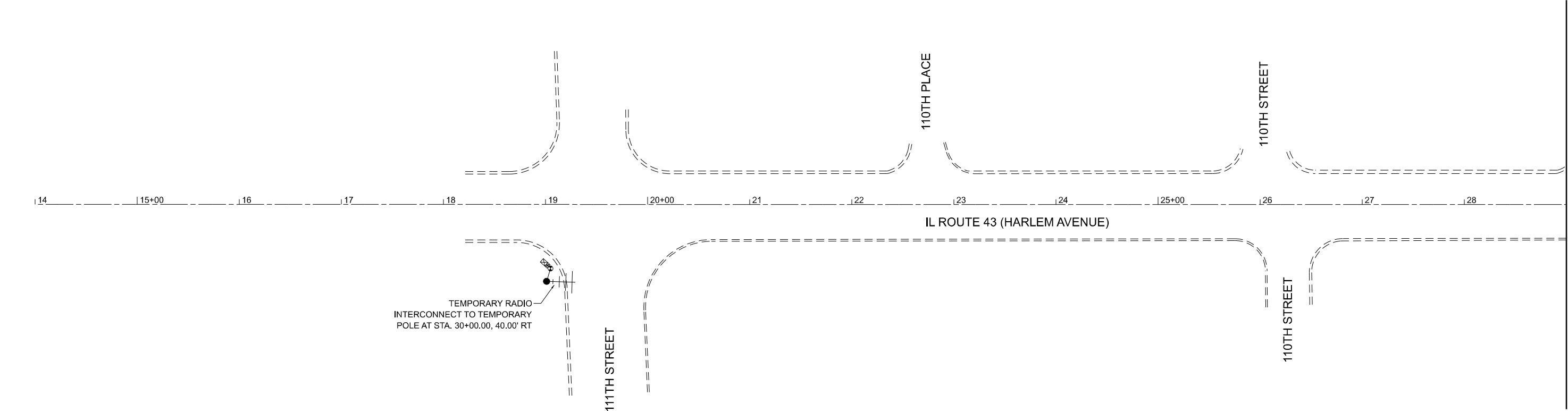
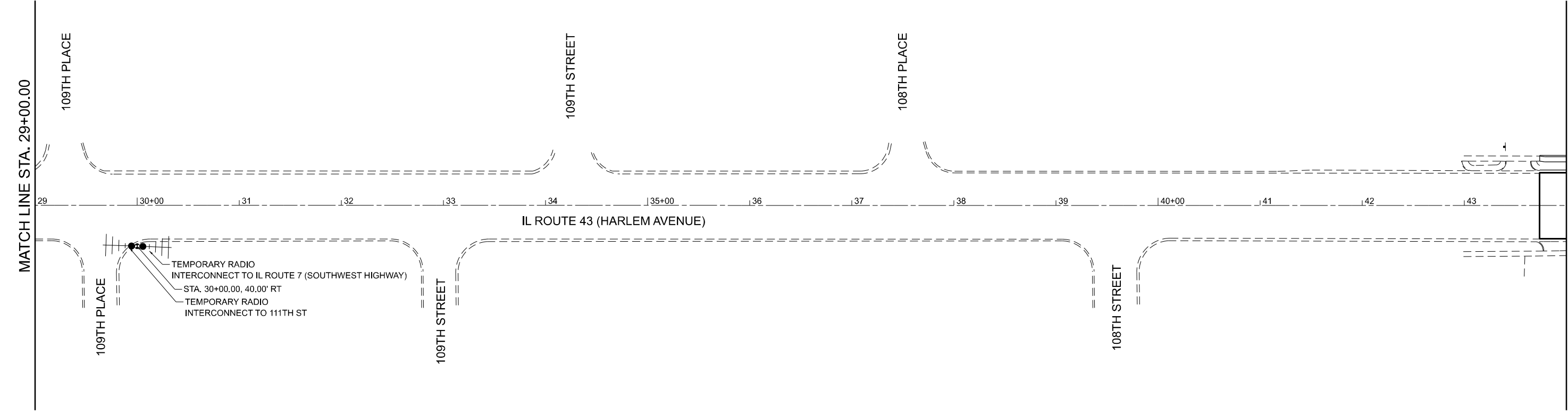
REVISED	-
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REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLANS			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=50'	SHEET 1	OF 2 SHEETS	STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	88
CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

ECON 66



MODEL: Unnamed Plan-2 - Plan 3 (Sheet)
FILE NAME: W:\191-200 IDOT_208010_IC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-TEMP Interconnect.dgn



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PLOT DATE	= 11/13/2025	DATE	-	10/15/2025	REVISED	-	

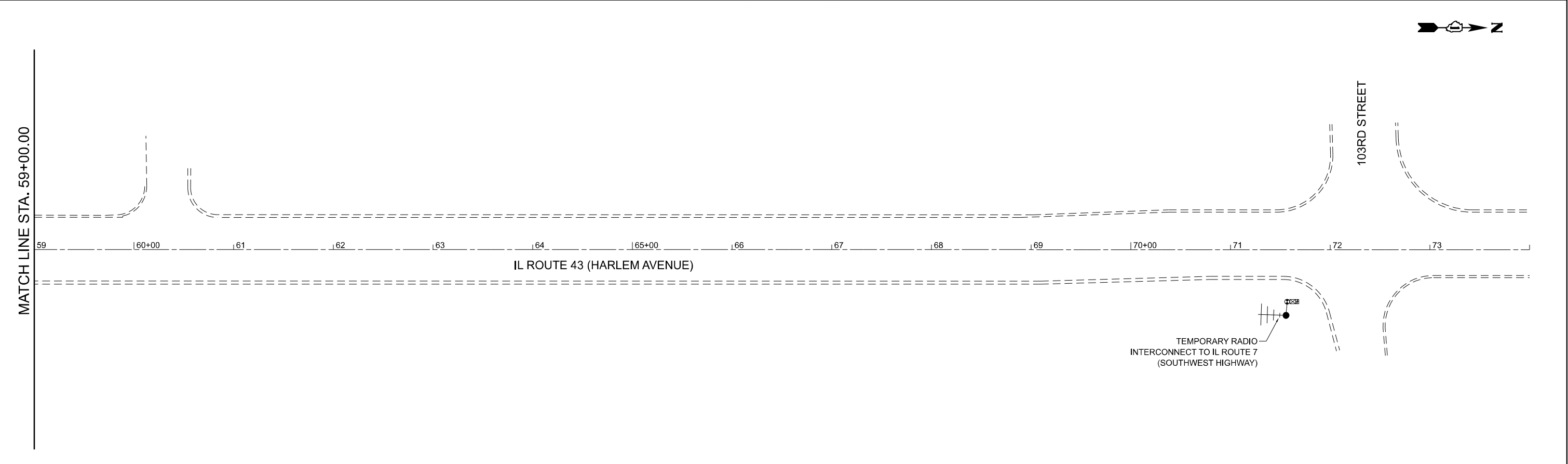
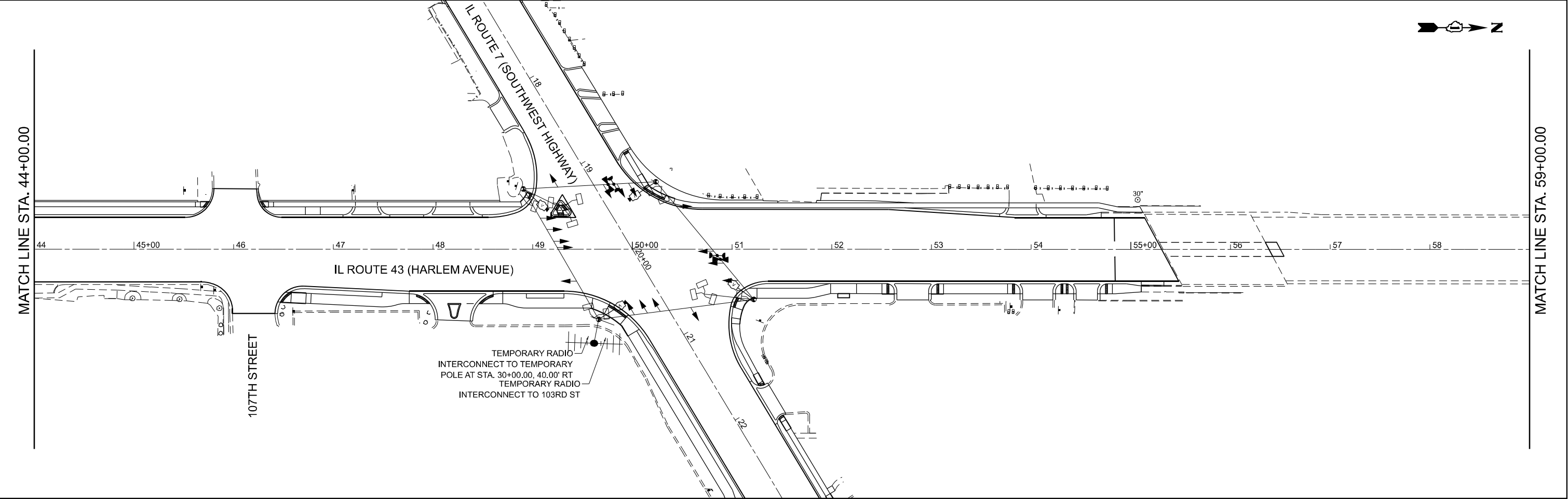
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLANS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	89
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

ECON 66



MODEL: TEMP INTERCONNECT SCHEMATIC
FILE NAME: W:\191-200 DOT_208010_1\08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh1-TEMP_TS.dgn



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USER NAME	= tnestor
DESIGNED	- TN
DRAWN	- TN
CHECKED	- GJE
PLOT DATE	= 11/13/2025

DESIGNED	- TN
DRAWN	- TN
CHECKED	- GJE
DATE	= 10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

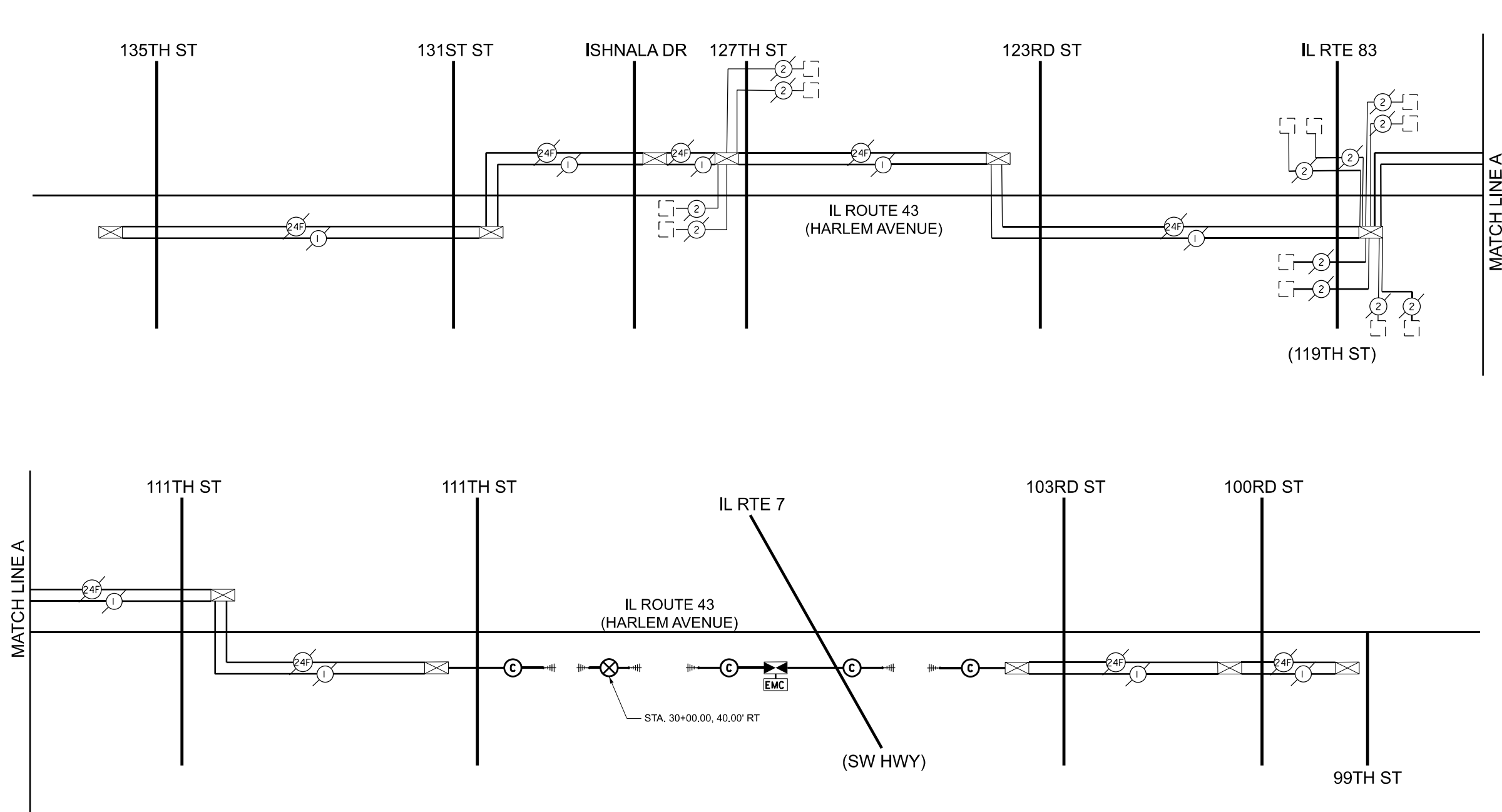
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC
IL, ROUTE 7 (SOUTHWEST HIGHWAY) AT IL, ROUTE 43 (HARLEM AVENUE)

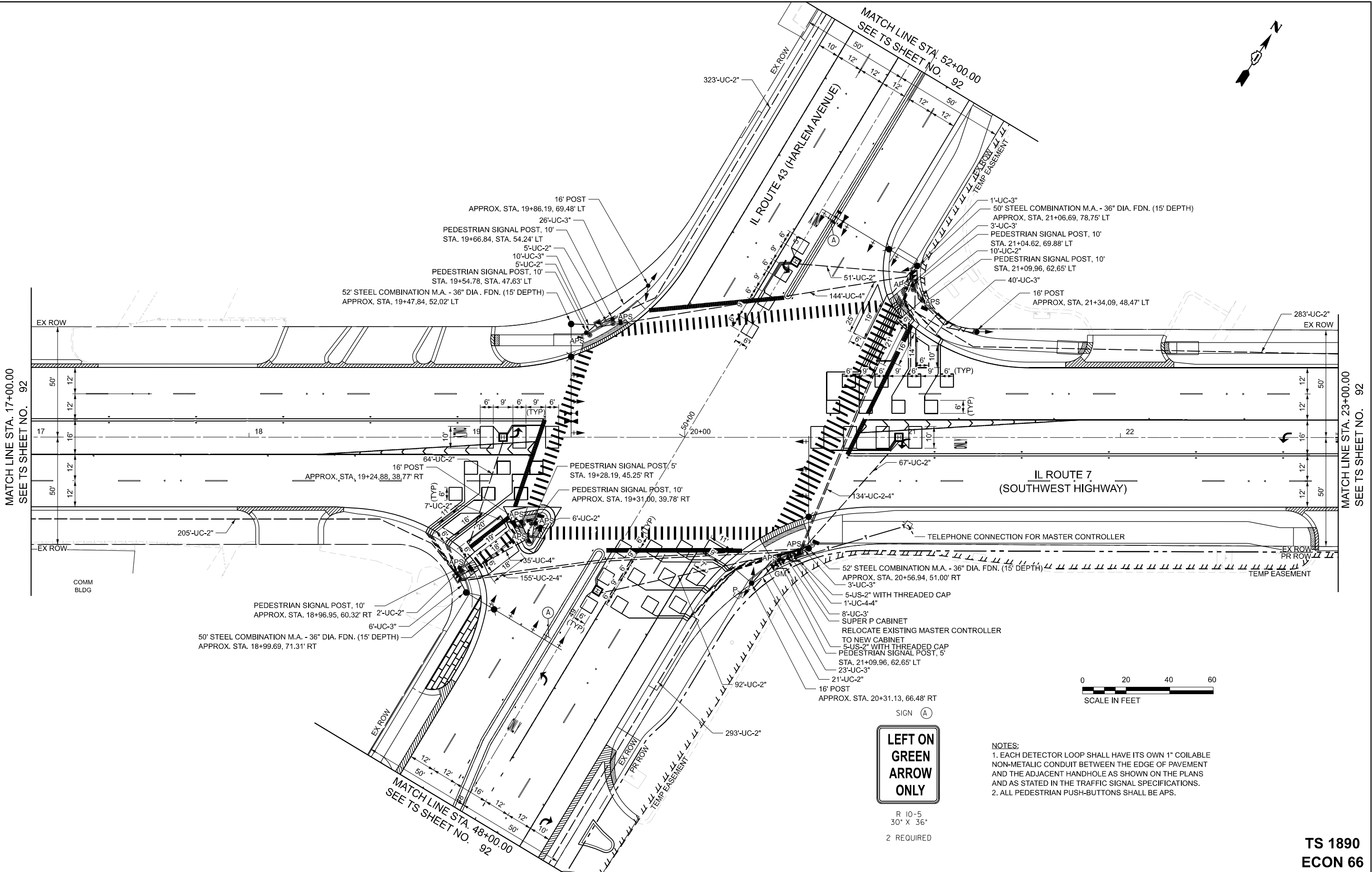
SCALE: N.T.S	SHEET 3	OF 3	SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	90
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

ECON 66

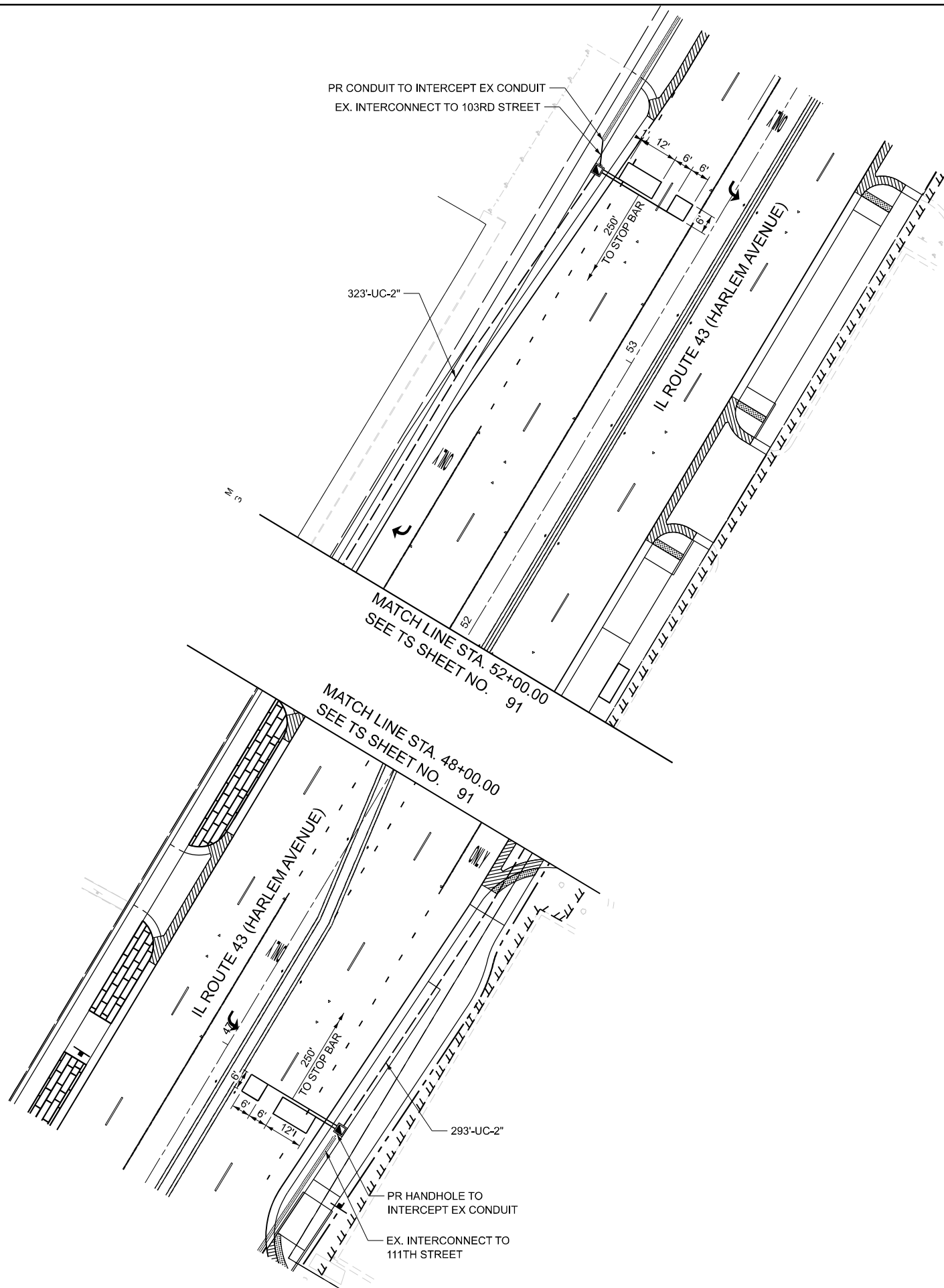


MODEL: SHEET 1
FILE NAME: W:\19+200 IDOT_208010_T08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PR_TS.dgn



 <div>BLA, Inc.</div>	USER NAME = tnestor	DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	CONTRACT NO. 62W47
		DRAWN - TN	REVISED -						3578	2023-925-N-TS(L)	COOK	176	91	
		CHECKED - GJE	REVISED -											
	PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -											
	SCALE: 1"=20'				SHEET 1	OF 2	SHEETS	STA. 17+00.00	TO STA. 23+00.00					
ILLINOIS FED. AID PROJECT														

TS 1890
ECON 66



TS 1890
ECON 66

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	92
		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		

MODEL: SHEET 2
FILE NAME: W:\191-200 IDOT 208010 T08 IL7 at IL43 Intersection Improvement\CADD Sheets\I102218-sht-PR TS.dgn



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	DRAWN = TN	REVISED =
	CHECKED = GJE	REVISED =
PLOT DATE = 10/17/2025	DATE = 10/15/2025	REVISED =

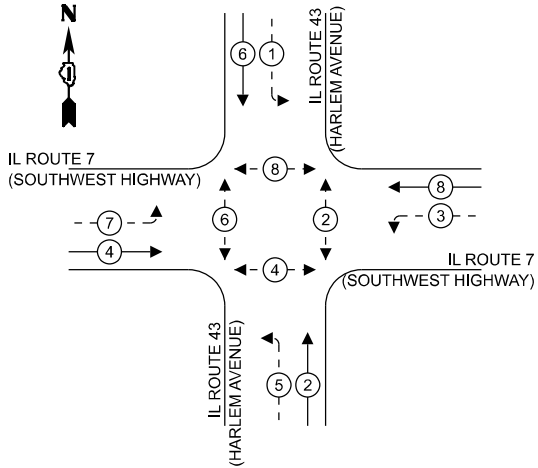
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

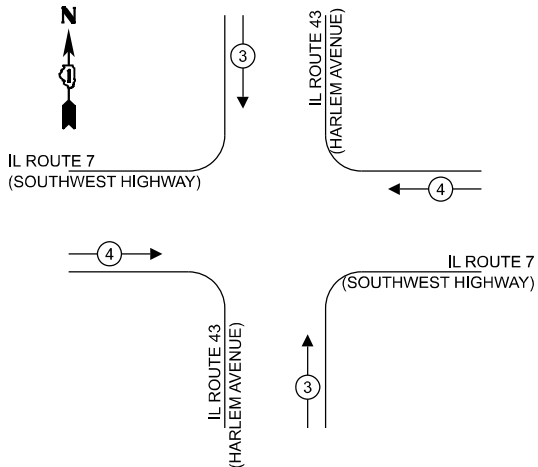
SCALE: 1"=20'	SHEET 2	OF 2	SHEETS	STA.	TO STA.
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MODEL: SHEET 3
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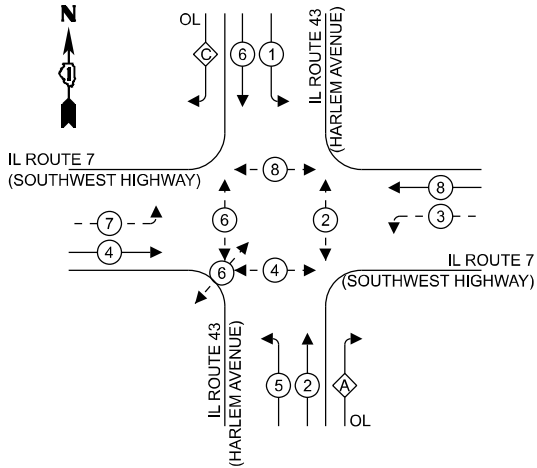
EXISTING CONTROLLER SEQUENCE



EXISTING/PROPOSED
EMERGENCY VEHICLE
PREEMPTION SEQUENCE



PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← (★) — PROTECTED PHASE
- ← - (★) - PROTECTED/PERMITTED PHASE
- ← (★) → PEDESTRIAN PHASE
- ← (★ OL) — OVERLAP

RIGHT TURN OVERLAP
PHASE DESIGNATION:

OVERLAP LETTER	=	PERMISSIVE PHASE	=	PROTECTED PHASE
A	=	2	+	3
C	=	6	+	7

TRAFFIC SIGNAL
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	14	11	154
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	10	15	150
CONTROLLER	1	150	150
MASTER CONTROLLER	1	100	100
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	1	35	35
CELLULAR MODEM	1	15	15

TOTAL UPS SIZING

733

UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-

TOTAL SERVICE WIRE SIZING

1,338

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT
PHONE: 866-639-3532
COMPANY: COMED
ACCOUNT NUMBER: 76630-18807
METER NUMBER: ---



USER NAME = tnestor
PLOT DATE = 10/17/2025

DESIGNED - TN
DRAWN - TN
CHECKED - GJE
DATE - 10/15/2025

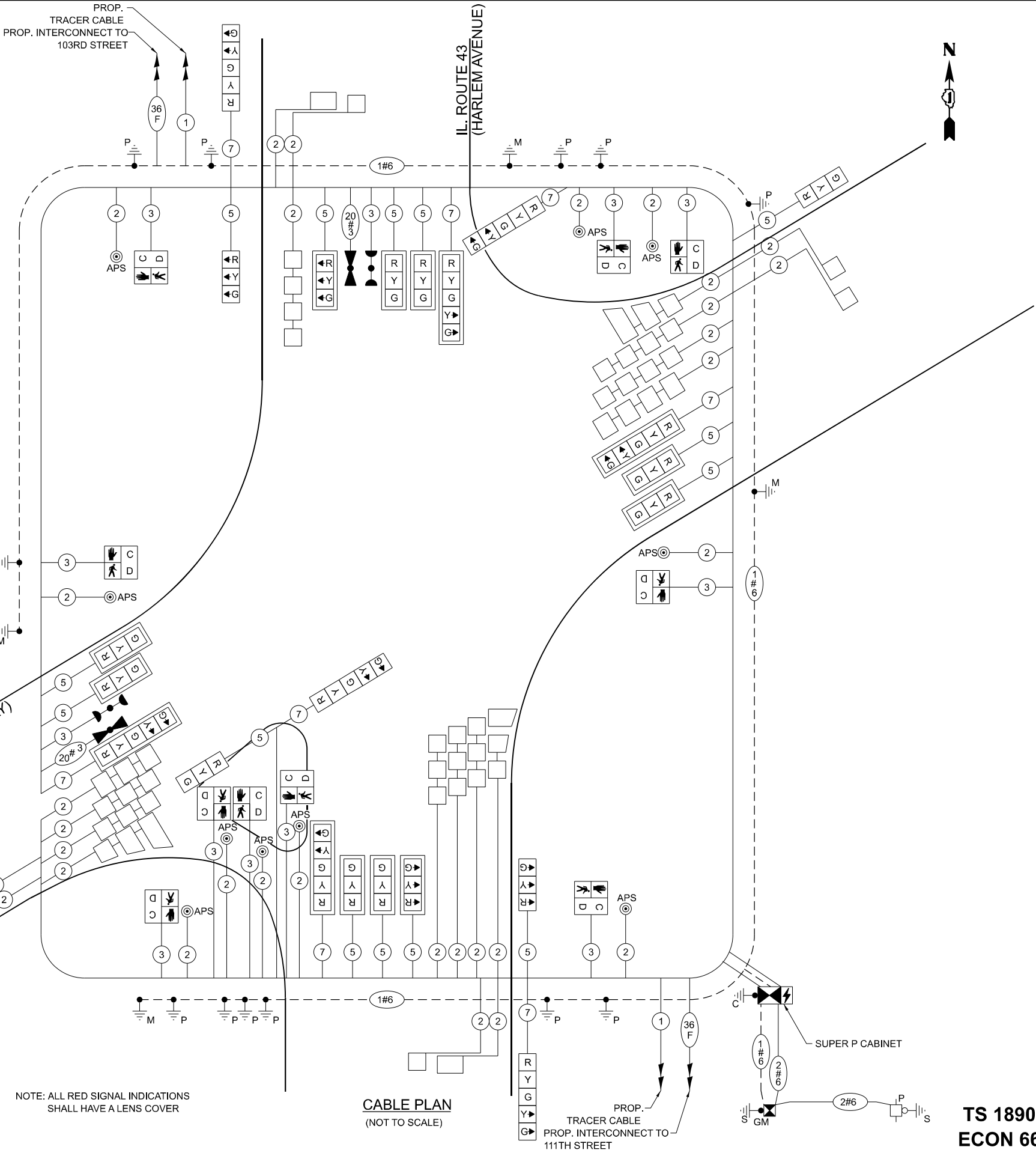
REVISED -
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REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)

SCALE: N.T.S. SHEET 3 OF 3 SHEETS STA. TO STA.

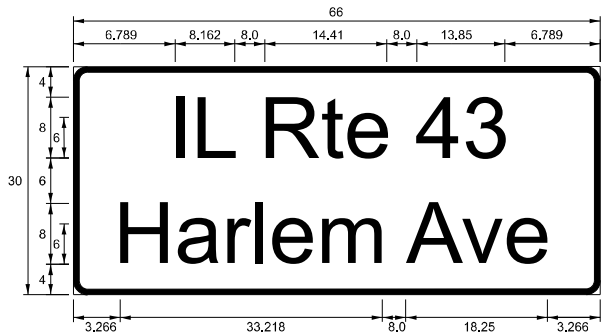
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	93
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



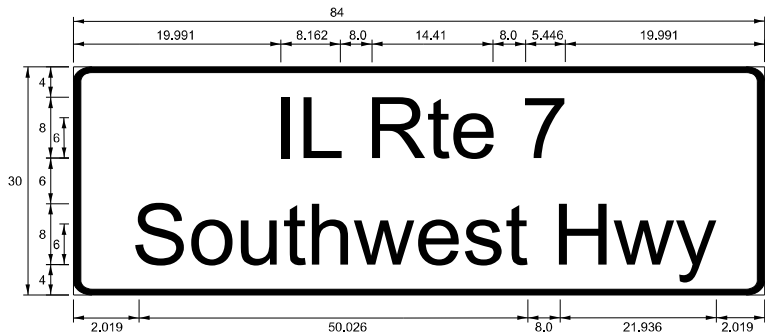
TS 1890
ECON 66

SIGN PANEL - TYPE 2

ALL DIMENSIONS ARE IN INCHES UNLESS NOTED OTHERWISE



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	13.75	2	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	17.5	2	ZZ	2

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION
PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET
NAME SIGNS DETAIL.

ITEM	UNIT	TOTAL QUANTITY
SIGN PANEL - TYPE 1	SQ FT	15
SIGN PANEL - TYPE 2	SQ FT	63
ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1444
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	108
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	764
HANDHOLE	EACH	6
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	3
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	4941
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2712
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2775
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1894
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1PAIR	FOOT	2618
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	25
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	2060
TRAFFIC SIGNAL POST, 16 FT.	EACH	4
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.	EACH	2
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 52 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	20
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	60
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	10
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	10
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	14
INDUCTIVE LOOP DETECTOR	EACH	11
DETECTOR LOOP, TYPE I	FOOT	2981
* LIGHT DETECTOR	EACH	2
* LIGHT DETECTOR AMPLIFIER	EACH	1
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	17
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	13
* EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	627
SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	6
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	2
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET	EACH	1
UNINTERRUPTABLE POWER SUPPLY (SPECIAL)	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	10
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	32
LED SIGNAL FACE, LENS COVER	EACH	52
RELOCATE EXISTING MASTER CONTROLLER	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

*100% COST TO THE CITY OF WORTH

TS 1890
ECON 66

MODEL - SHEET 4
FILE NAME: W:\191-200 IDOT_208010_IC08_IL7 at IL 43 Intersection Improvement\CADD Sheets\102218-sh14-PR_TS.dgn



USER NAME = gellwanger	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED STREET NAMES SIGNS AND SCHEDULE OF QUANTITIES			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVENUE)			
SCALE: N.T.S	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	94
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Plan - Plan 1 [Sheet]
FILE NAME: W:\191-200 DOT_208010_IT08_IL7 at IL43 Intersection Improvement\CADD Sheets\1912218-sh4-PR Interconnect.dgn



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		DRAWN	-	TN	REVISED	-	
		CHECKED	-	GJE	REVISED	-	
PLOT DATE	= 11/13/2025	DATE	-	10/15/2025	REVISED	-	

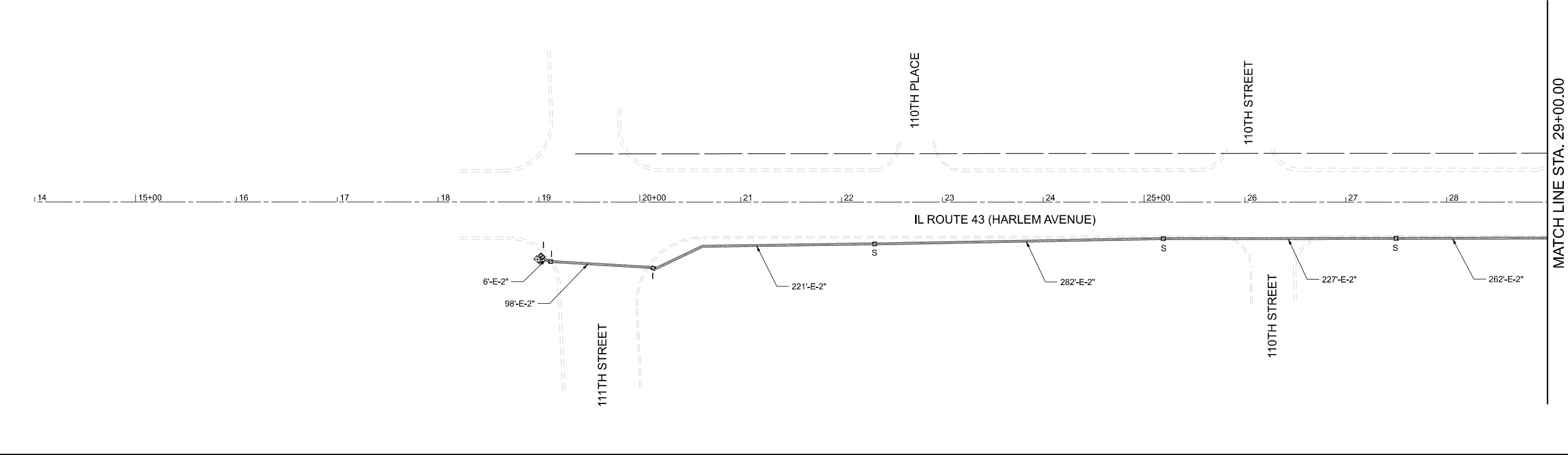
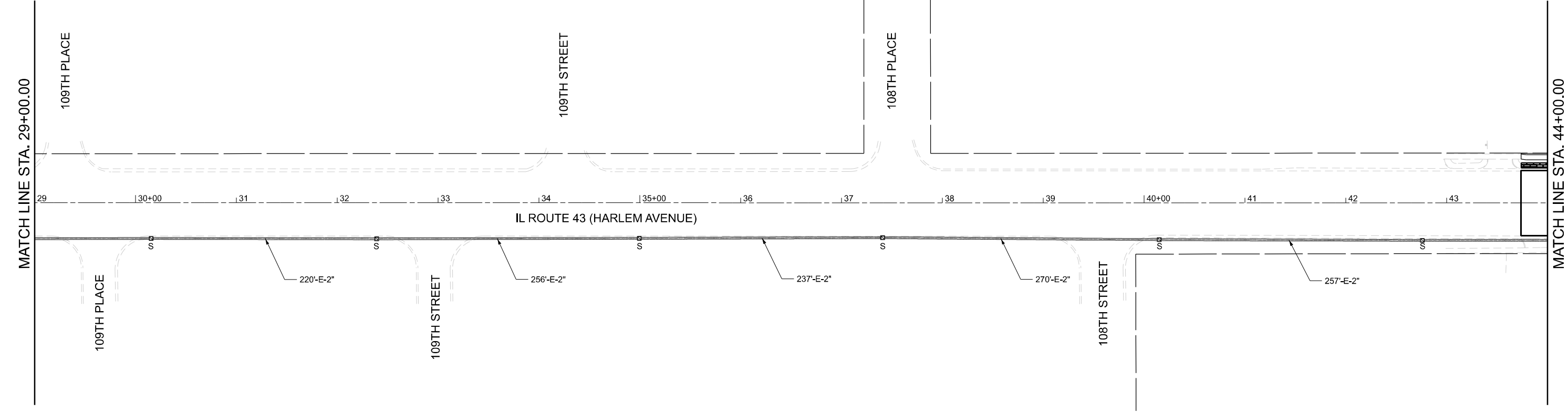
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT PLANS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

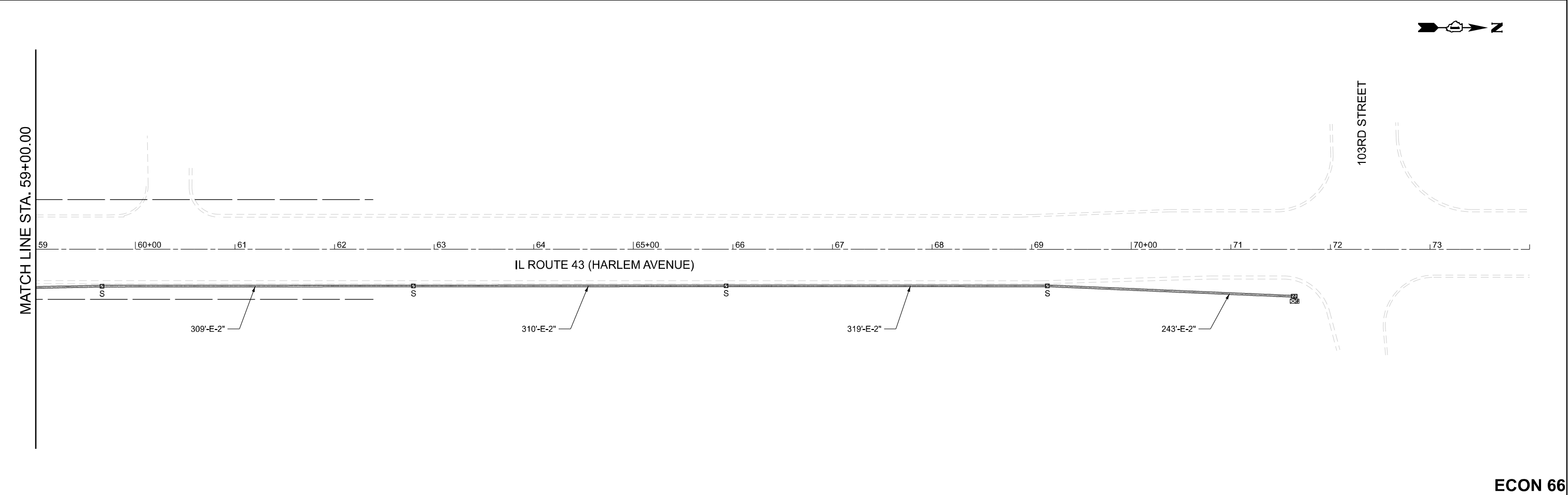
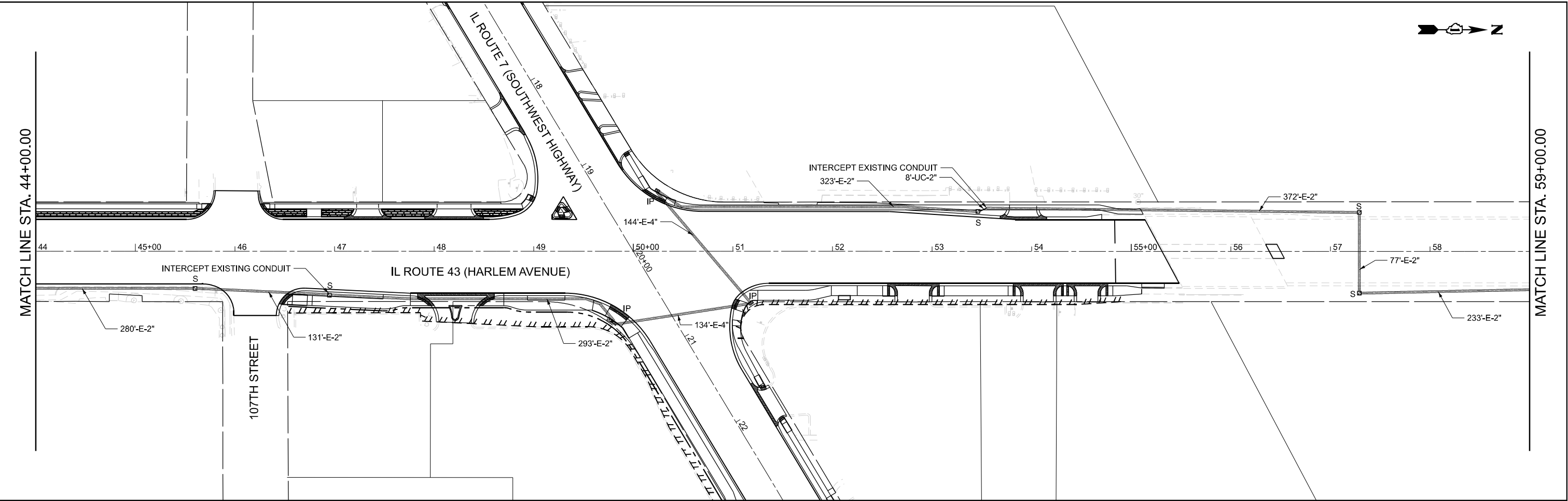
SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W47				
ILLINOIS		FED. AID PROJECT		

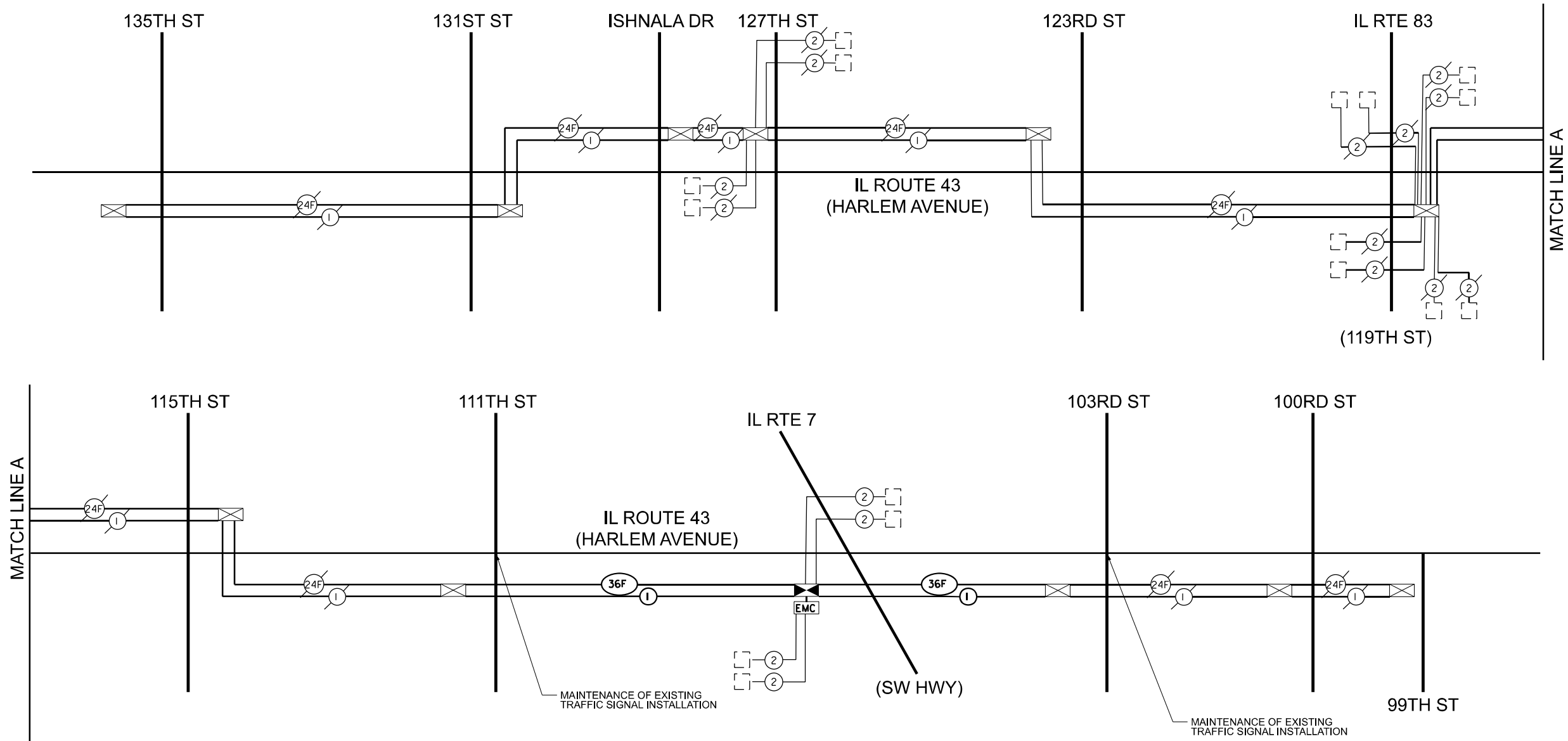
ECON 66



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<div><div></div><div>BLA, Inc.</div></div>	USER NAME = tnestor		DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED INTERCONNECT PLANS IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN - TN	REVISED -						3578	2023-925-N-TS(L)	COOK	176	96
			CHECKED - GJE	REVISED -						CONTRACT NO. 62W47				
	PLOT DATE = 11/13/2025		DATE - 10/15/2025	REVISED -		SCALE: 1"=50'	SHEET 2	OF 2	SHEETS	STA. N/A	TO STA. N/A	ILLINOIS FED. AID PROJECT		



CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	5579
X1400217	TERMINATE FIBER IN CABINET	EACH	3
X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62. 5/125, MM12F SM24F	FOOT	5579
X8950508	REMOVE CABLE FROM CONDUIT	FOOT	5579
Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

ECON 66

MODEL: Unnamed Plan-6 - Plan 2 (Sheet)
FILE NAME: W:\191-200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-PR Interconnect.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 11/13/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT SCHEMATIC			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=200'	SHEET 1	OF 1 SHEETS	STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	97
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1.

THIS PROJECT INCLUDES THE REPLACEMENT OF THE EXISTING ROADWAY LIGHTING AT THE INTERSECTION OF IL 7 AND IL 43. THE PROPOSED LIGHTING SHALL BE ENERGIZED, OWNED, AND MAINTAINED BY VILLAGE OF WORTH.
2.

THE CONTRACTOR SHALL CONTACT THE ELECTRIC UTILITY COMPANY TO COORDINATE THE ELECTRIC SERVICE WORK.
3.

THE QUANTITIES OF RACEWAY WHERE INDICATED ON THESE PLANS ARE APPROXIMATIONS ONLY. THE CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND SHALL INSTALL RACEWAYS IN COMPLETE COMPLIANCE WITH THE SPECIFIED REQUIREMENTS.
4.

THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. TO LOCATE AND MARK/STAKE ALL UNDERGROUND UTILITIES. MEADE ELECTRIC CO., DISTRICT 1 ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL MEADE ELECTRIC CO. TO TRANSFER IDOT MAINTAINED EQUIPMENT TO THE CONTRACTOR BEFORE THE START OF ANY WORK. THEIR PHONE NUMBER IS 773-287-7672.
5.

THE CONTRACTOR SHALL VERIFY LOCATIONS OF UNDERGROUND/OVERHEAD UTILITIES PRIOR TO INSTALLATION OF LIGHT POLES AND CONDUITS. IF THERE IS A CONFLICT WITH THE LIGHT POLES/CONDUITS AS SHOWN ON PLANS, THE CONTRACTOR SHALL SUGGEST ALTERNATIVE LOCATIONS AND COORDINATE WITH THE ENGINEER PRIOR TO PERFORMING ANY CONSTRUCTION WORK.
6.

TRENCHES FOR LIGHTING RACEWAYS SHALL HAVE A MINIMUM DEPTH OF 30".
7.

LIGHTING SYSTEM INSTALLATION SHALL CONFORM TO THE LATEST IDOT STANDARDS, VILLAGE OF WORTH STANDARDS, NEC AND LOCAL CODES.
8.

ALL ELECTRICAL EQUIPMENT AND PRODUCTS SHALL BE NEW, UL LISTED AND LABELED.
9.

THE CONTRACTOR SHALL TAKE CARE WHEN INSTALLING UNIT DUCT TO AVOID CONFLICTS WITH EXISTING UNDERGROUND UTILITIES AND TREE'S ROOTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE AS DETERMINED BY THE ENGINEER.
10.

THE CONTRACTOR SHALL TAKE CARE WHEN INSTALLING LIGHT POLE FOUNDATIONS TO AVOID CONFLICTS WITH UNDERGROUND UTILITIES. WHEN CONFLICTS ARE ENCOUNTERED, THE CONTRACTOR SHALL SUGGEST TO RELOCATE THE FOUNDATIONS. THE NEW LOCATION SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
11.

GROUND RODS SHALL BE INSTALLED AT EACH LIGHTING UNIT, CONTROLLER CABINET, AND SERVICE INSTALLATION. WHERE BREAKAWAY BASES ARE SPECIFIED, THE BASE AND THE POLE SHALL BE APPROPRIATELY GROUNDED.
12.

LUMINAIRE SAFETY CABLE SHALL BE INSTALLED FOR EACH PROPOSED LUMINAIRE SPECIFIED FOR INSTALLATION ON THIS PROJECT PER BE-701 OR AS DIRECTED BY ENGINEER.
13.

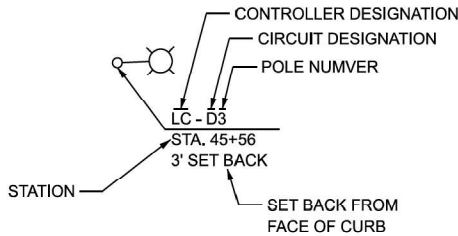
THE CONTRACTOR IS RESPONSIBLE FOR OPERATION AND MAINTENANCE OF EXISTING LIGHTING SYSTEM AND SHALL ENSURE CONTINUOUS OPERATION DURING NIGHT-TIME HOURS UNTIL PROPOSED LIGHTING IS INSTALLED AND IS OPERATIONAL.
15.

THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UNDERGROUND UTILITIES BEFORE TRENCHING AND AUGURING.
16.

GROUND RODS AND LUMINAIRE SAFETY CABLE SHALL BE INSTALLED AT EACH LIGHTING UNIT.
17.

THE CONTRACTOR SHALL COORDINATE WITH COMED REGARDING THE REMOVAL OF EXISTING MAST ARMS AND LUMINAIRES ATTACHED TO COMED POLES, WHEN PROPOSED LIGHTING IS FULLY INSTALLED AND OPERATIONAL.

CALL-OUT DESCRIPTION
(PROPOSED LIGHTING UNIT)

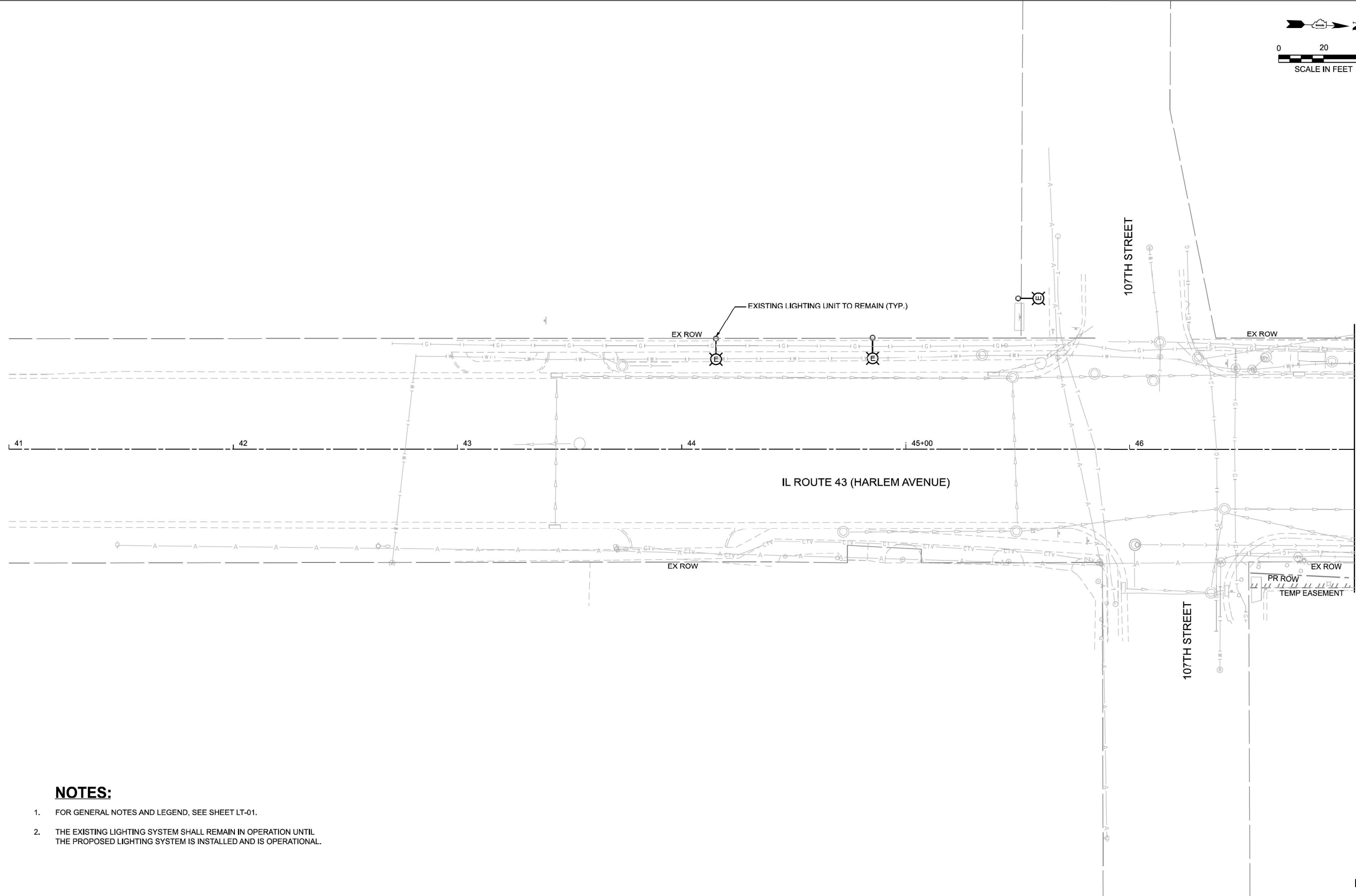
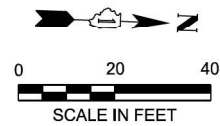


BILL OF MATERIALS

DESCRIPTION	UNIT	QUANTITIES
ELECTRIC SERVICE INSTALLATION AND CONNECTION	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	996
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	25
UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	125
UNIT DUCT, 600V, 4-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	2864
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	900
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 2	FOOT	50
LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H	EACH	17
LIGHTING CONTROLLER, BASE MOUNTED, 240 VOLT, 100 AMP	EACH	1
LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 15 FT. MAST ARM	EACH	12
LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	99
BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	12
REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	12
REMOVAL OF POLE FOUNDATION	EACH	12
MAINTENANCE OF LIGHTING SYSTEM	CAL MO	8
COMBINATION LIGHTING CONTROLLER	EACH	1
LIGHT POLE FOUNDATION, 30" DIAMETER, OFFSET	FOOT	9
LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	17

LEGEND

- EXISTING LIGHTING UNIT TO BE REMOVED AND SALVAGED
- EXISTING LIGHTING UNIT TO REMAIN
- PROPOSED LIGHTING UNIT, 47.5 FT. MH, 15 FT. MAST ARM, 240V (LINE TO LINE) LED LUMINAIRE (MIN. 30,000 LUMENS), OUTPUT DESIGNATION H WITH BREAKAWAY DEVICE ON CONCRETE FOUNDATION
- PROPOSED COMBINATION SIGNAL/LIGHT POLE, 45 FT. M.H., 15 FT. MAST ARM, 120V (LINE TO NEUTRAL) LED LUMINAIRE (MIN. 30,000 LUMENS), OUTPUT DESIGNATION H
- PROPOSED LIGHTING CONTROLLER "LC" 120/240V, SINGLE PHASE, 3 WIRE, 100AMP, BASE MOUNTED
- PROPOSED ELECTRIC SERVICE 120/240V, SINGLE PHASE, 3 WIRE
- UNIT DUCT, 600V, 4-1C NO.6 1/C NO.6 GROUND, (XLP-TYPE USE), 1¼" DIA. POLYETHYLENE
- UNDERGROUND CONDUIT GALVANIZED STEEL, 2" DIA.
- ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE), 3-1/C NO. 2, WITHIN 3" DIA. UCGS
- GROUND ROD



NOTES:

1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
2. THE EXISTING LIGHTING SYSTEM SHALL REMAIN IN OPERATION UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND IS OPERATIONAL.

MODEL: IL43_E_Plan 1
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\02218-sh-light-02.dgn

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

USER NAME	= mdeitche	DESIGNED -	MH	REVISED -	
		DRAWN -	MD	REVISED -	
PLOT SCALE	= 0.16666633' / in.	CHECKED -	BL	REVISED -	
PLOT DATE	= 6/26/2025	DATE	= 10/15/2025	REVISED -	

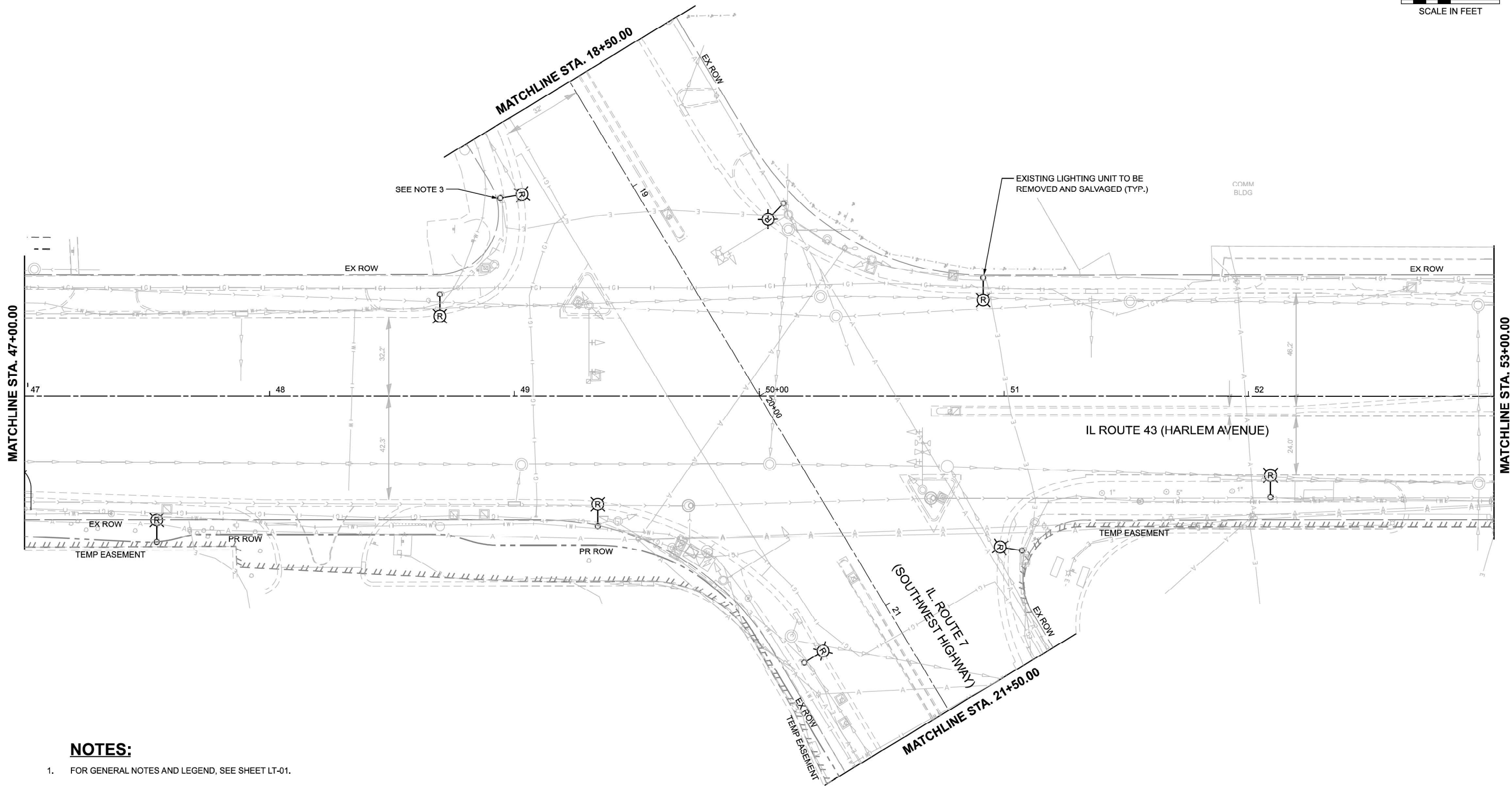
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVAL LIGHTING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)

SCALE: 1"=20' SHEET 1 OF 5 SHEETS STA. TO STA. 47+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	99
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LT-02

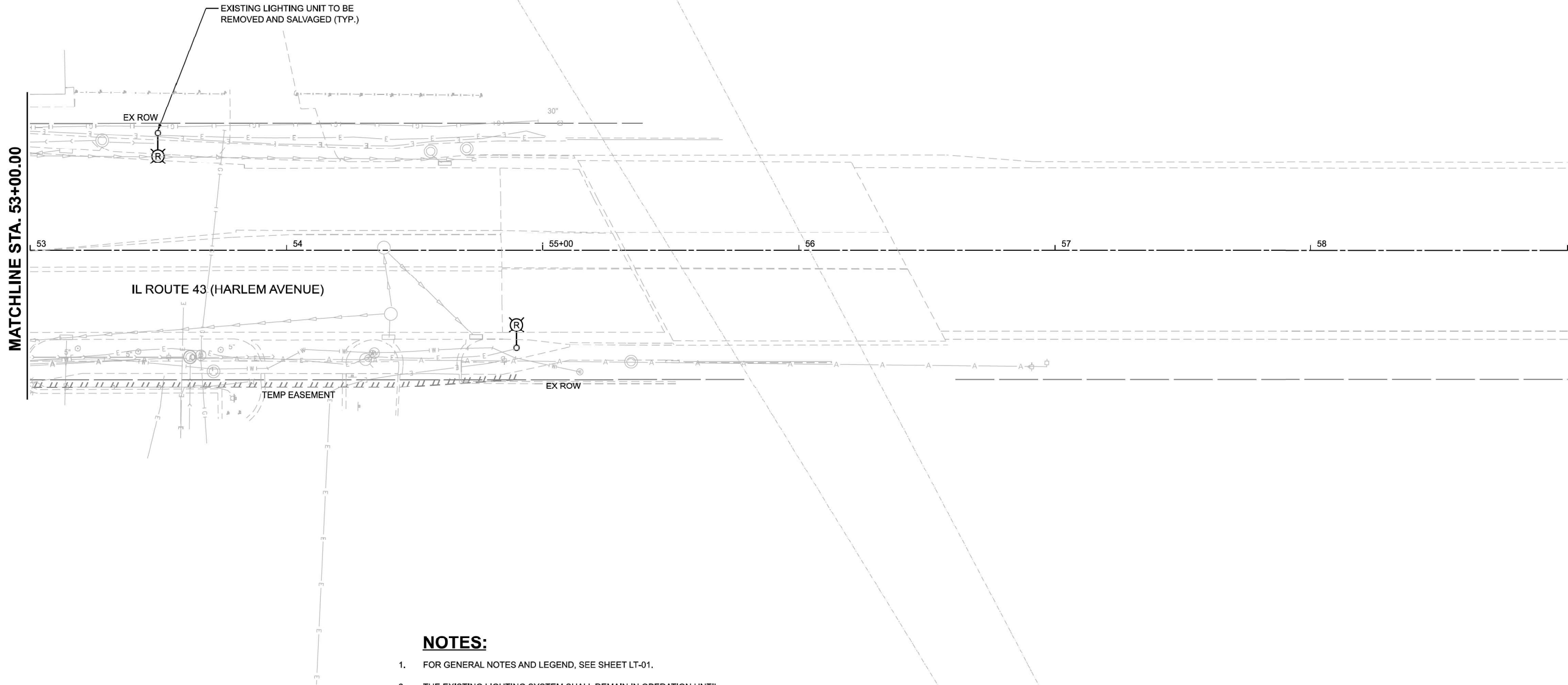
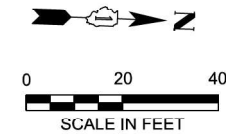


NOTES:

- 1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- 2. THE EXISTING LIGHTING SYSTEM SHALL REMAIN IN OPERATION UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND IS OPERATIONAL.
- 3. THE EXISTING STEP DOWN TRANSFORMER IN THE BASE OF THE LIGHT POLE SHALL BE REMOVED AND REINSTALLED IN THE PROPOSED LIGHTING UNIT AS INDICATED ON SHEET LT-08.

MODEL: IL43_E_Plan 2
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\102218-sh-high-03.dgn

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	PLOT SCALE = 0.16666633' / in.		DRAWN - MD	REVISED -		357B	2023-925-N-TS(L)	COOK	176	100				
	PLOT DATE = 6/26/2025		CHECKED - BL	REVISED -		CONTRACT NO. 62W47								
	DATE = 10/15/2025		REVISED -	ILLINOIS FED. AID PROJECT										
					SCALE: 1"=20'	SHEET 2	OF 5	SHEETS	STA. 47+00.00	TO STA. 53+00.00				

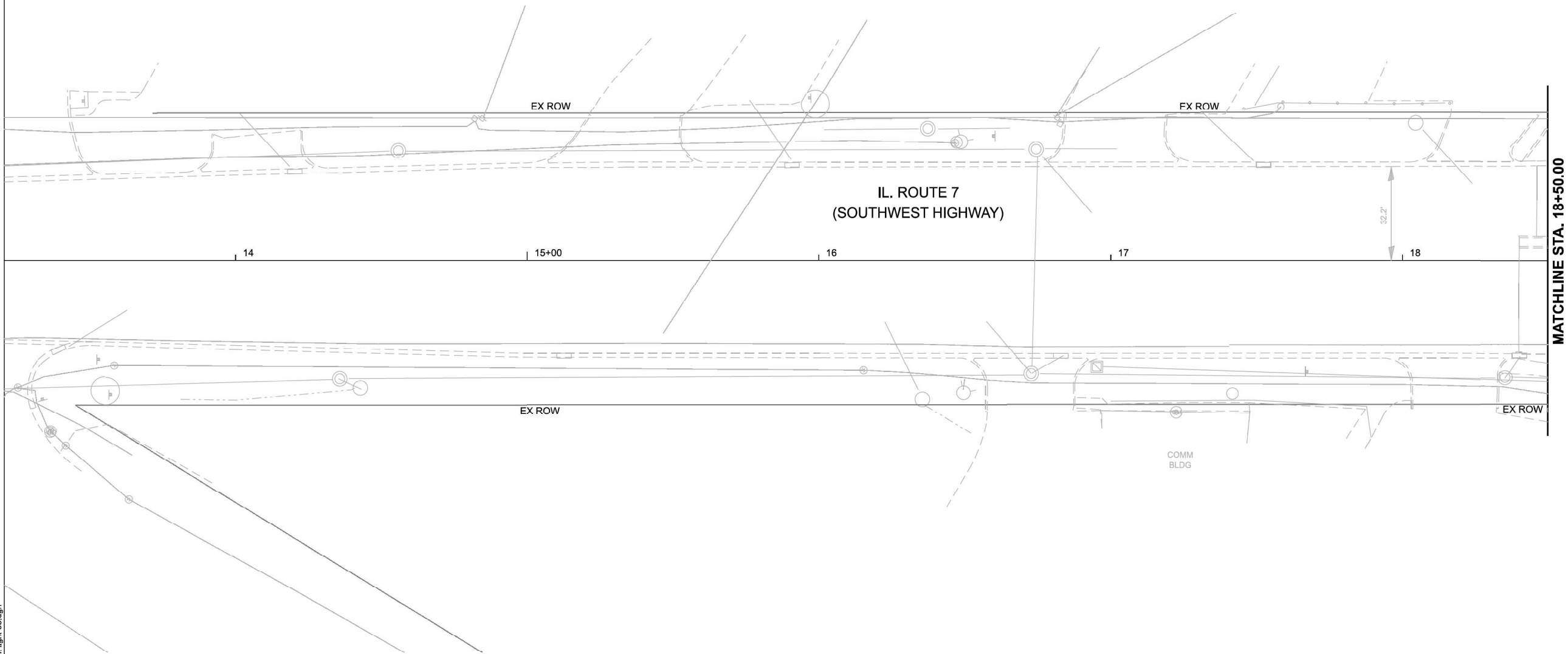
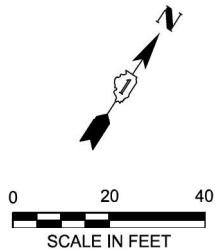


NOTES:

- 1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- 2. THE EXISTING LIGHTING SYSTEM SHALL REMAIN IN OPERATION UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND IS OPERATIONAL.

MODEL: IL43_E_Plan 3
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\02218-sh-light-04.dgn

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	PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -						357B	2023-925-N-TS(L)	COOK	176	101
	PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -		SCALE: 1"=20'				SHEET 3 OF 5 SHEETS		CONTRACT NO. 62W47		
					STA. 53+00.00 TO STA.				ILLINOIS FED. AID PROJECT				



NOTES:

- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- THE EXISTING LIGHTING SYSTEM SHALL REMAIN IN OPERATION UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND IS OPERATIONAL.

MODEL: Plan 1 - Plan 4
FILE NAME: F:\Projects\1094-Variou BLA\Design\Shp\102218-sh-high-05.dgn

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PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -
PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -

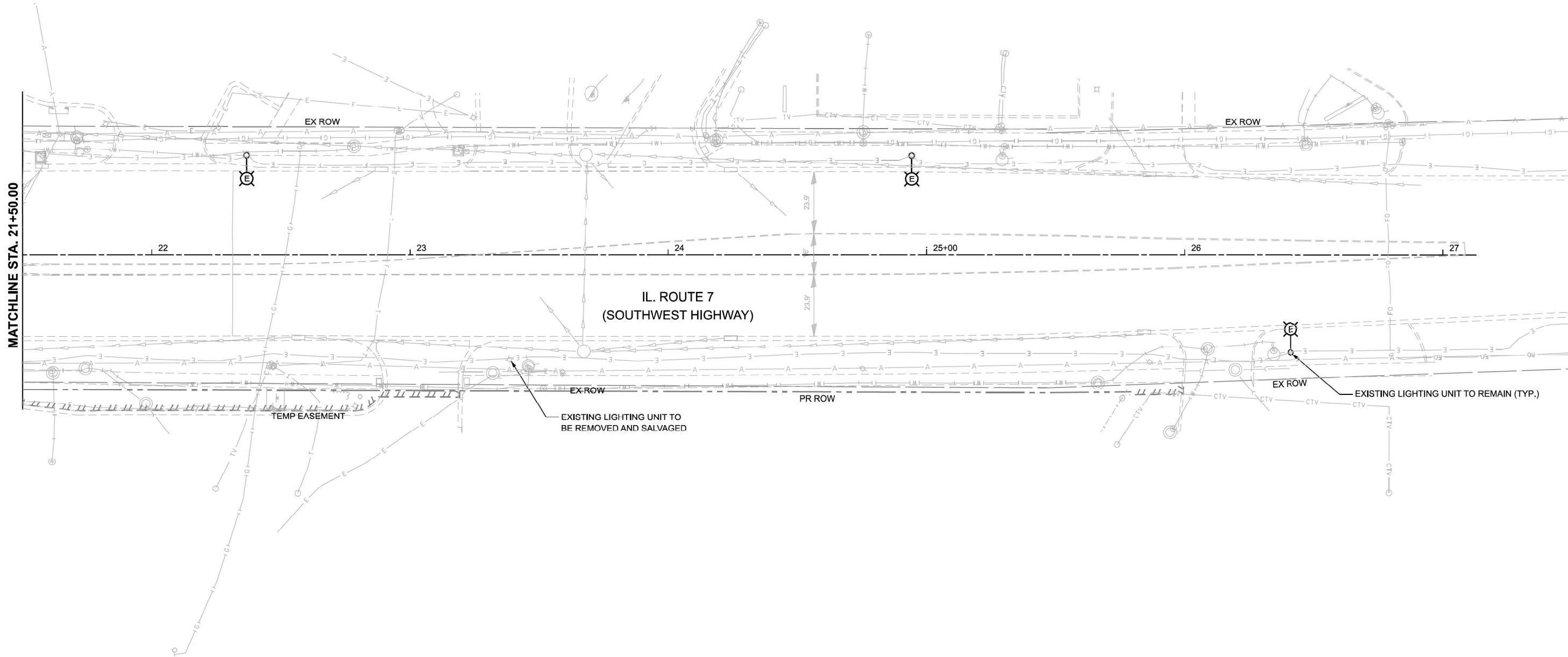
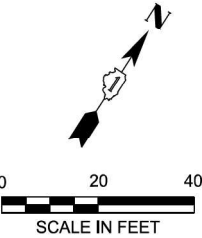
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL LIGHTING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)**

SCALE: 1"=20' SHEET 4 OF 5 SHEETS STA. TO STA. 18+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	102
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LT-05



NOTES:

- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- THE EXISTING LIGHTING SYSTEM SHALL REMAIN IN OPERATION UNTIL THE PROPOSED LIGHTING SYSTEM IS INSTALLED AND IS OPERATIONAL.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL LIGHTING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)**

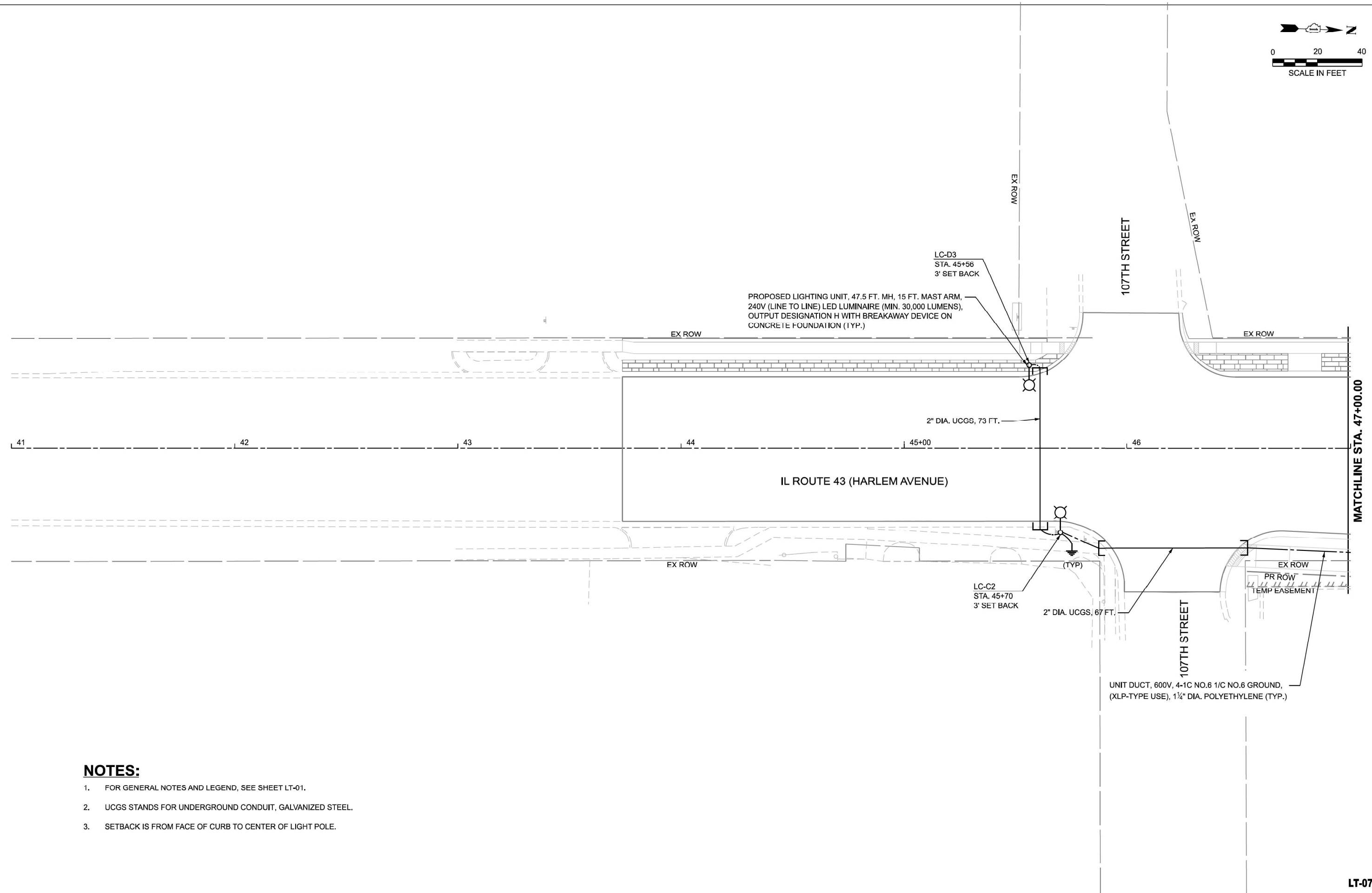
SCALE: 1"=20' SHEET 5 OF 5 SHEETS STA. 21+50.00 TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	103
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LT-06

MODEL: Unnamed Plan2 - IL Plan 3
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP 102218-sh-light-06.dgn

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	DRAWN - MD	REVISED -
PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -
PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -



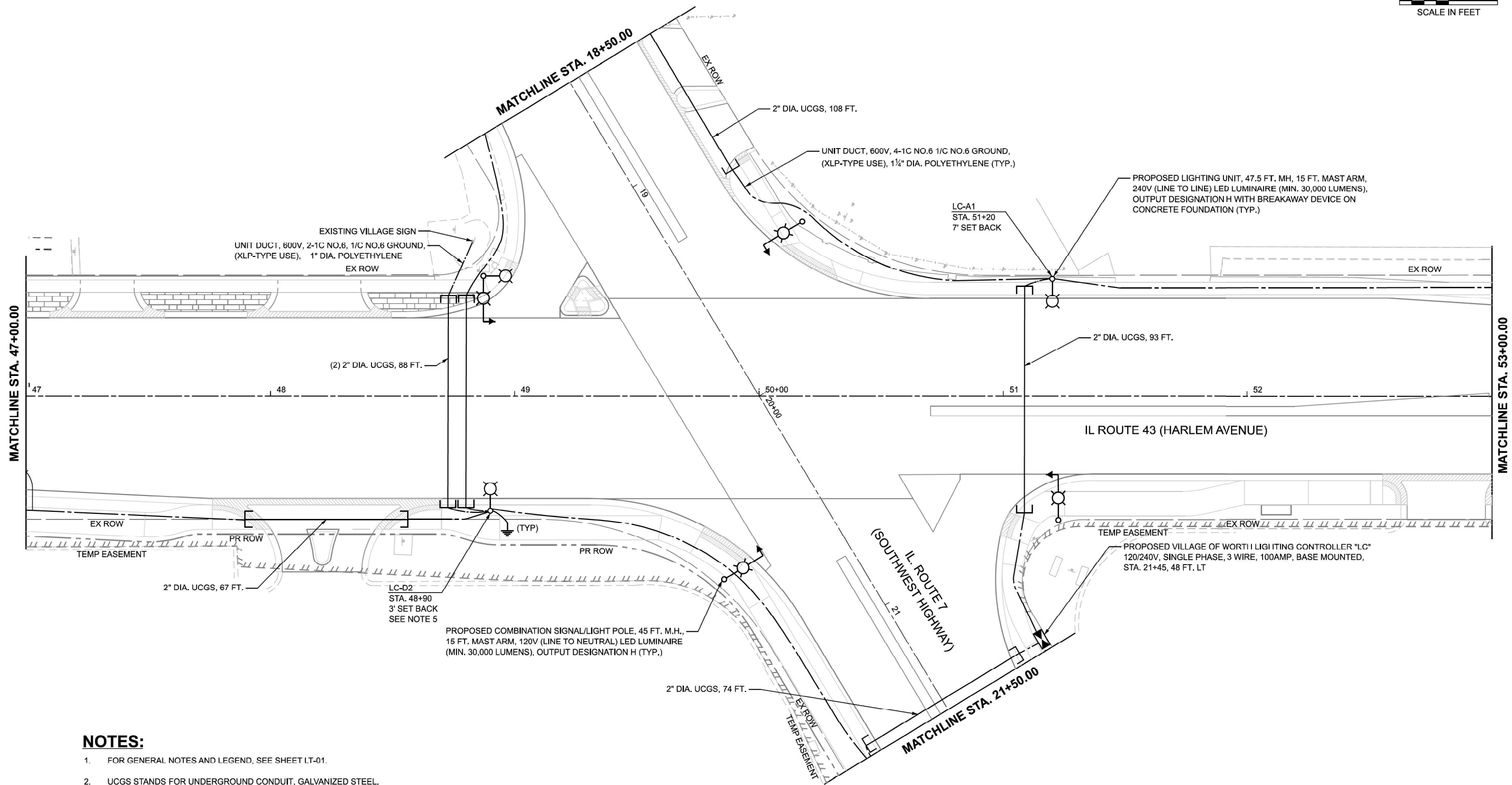
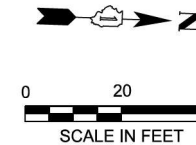
NOTES:

LT-07

PROPOSED LIGHTING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)

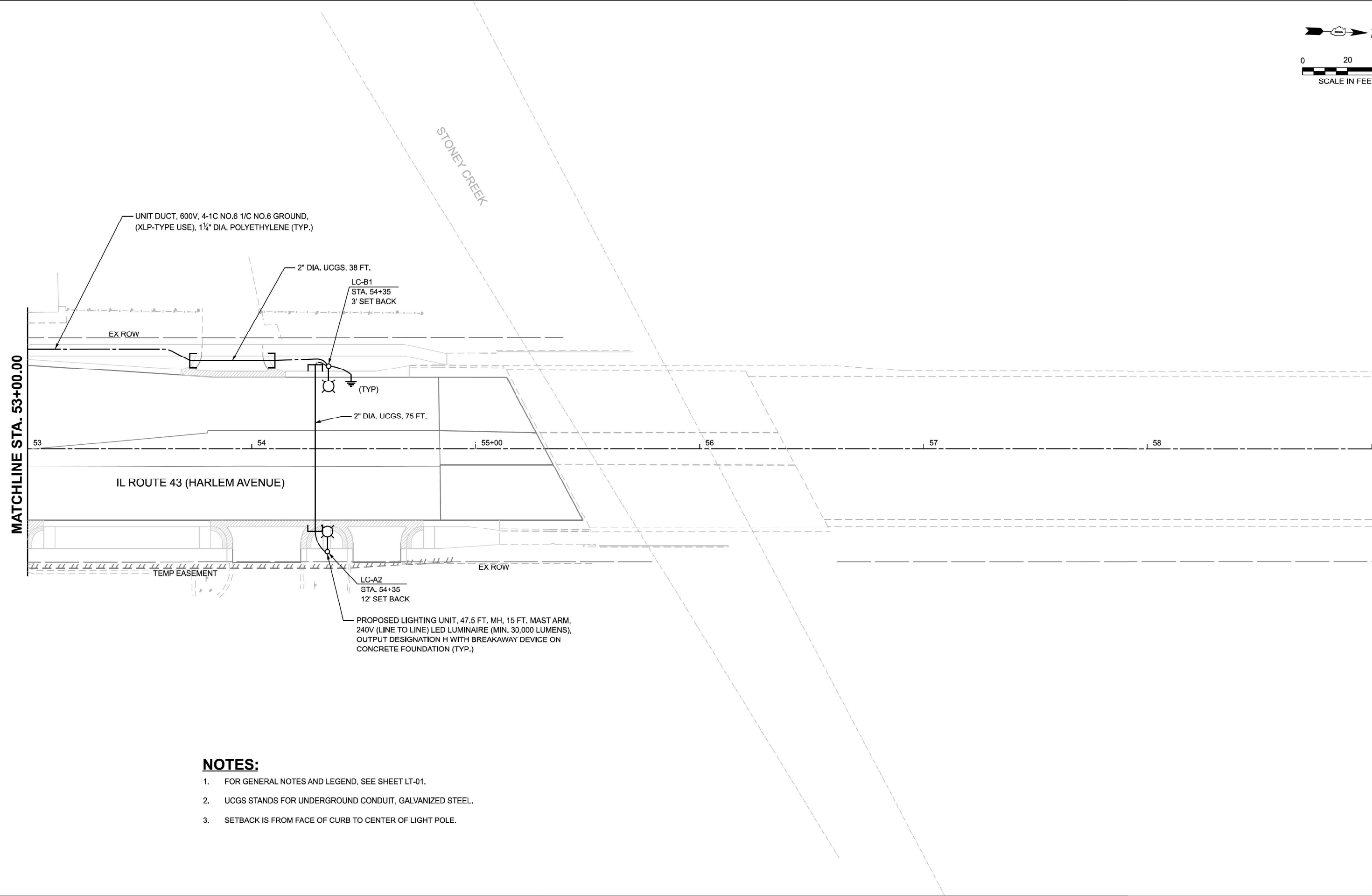
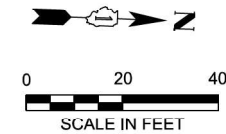
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	104
		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

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6330 Belmont Road, Suite 4B
Downers Grove, IL 60516



- NOTES:**
- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
 - UCGS STANDS FOR UNDERGROUND CONDUIT, GALVANIZED STEEL.
 - SETBACK IS FROM FACE OF CURB TO CENTER OF LIGHT POLE.
 - FOR COMBINATION LIGHTING PLAN, SEE SHEET LT-12.
 - THE EXISTING STEP DOWN TRANSFORMER SHALL BE INSTALLED TO RECONNECT SERVICE TO THE EXISTING VILLAGE SIGN.

MODEL: IL43_E_Plan 2
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\102218-sh-high-08.dgn



- NOTES:**
- 1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
 - 2. UCGS STANDS FOR UNDERGROUND CONDUIT, GALVANIZED STEEL.
 - 3. SETBACK IS FROM FACE OF CURB TO CENTER OF LIGHT POLE.

MODEL: IL43_E_Plan 3
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\02218-sh-light-09.dgn

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PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -
PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -

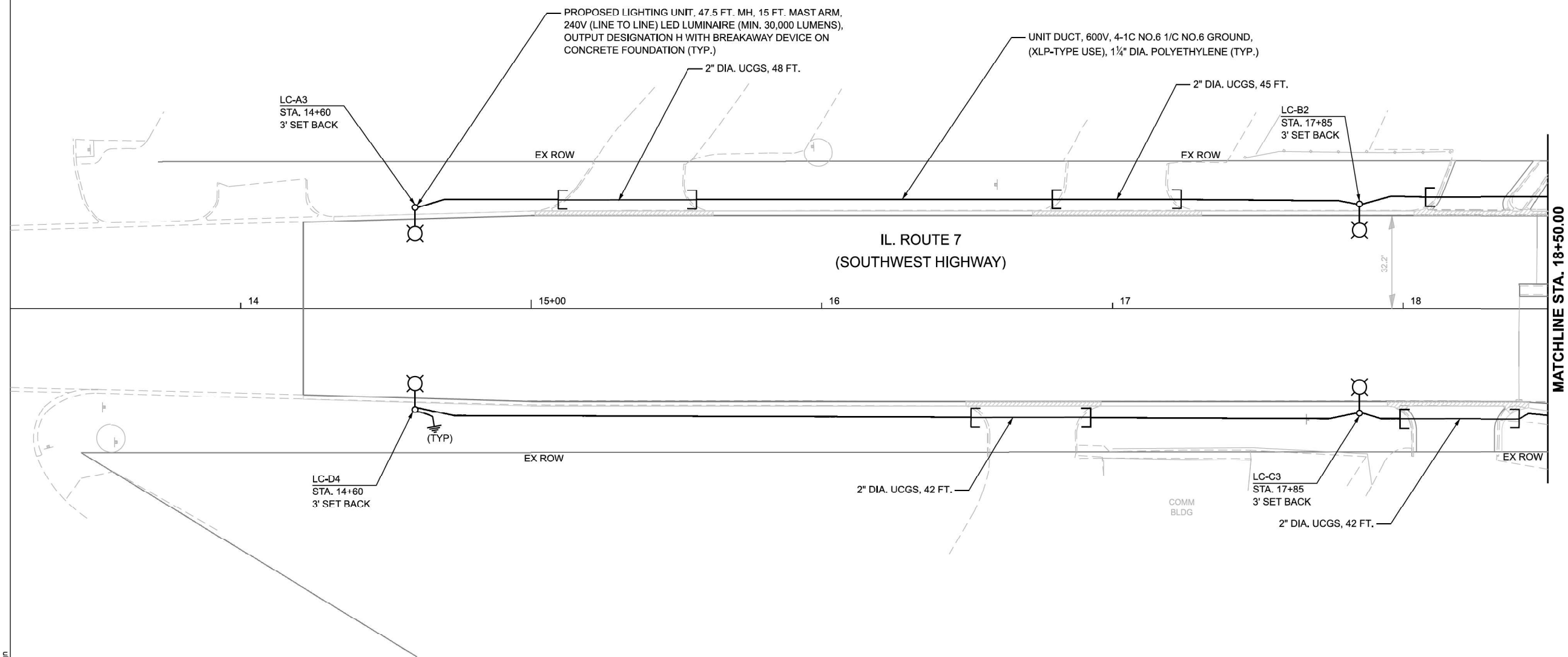
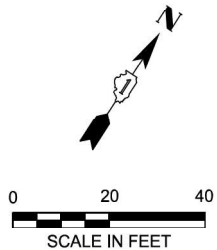
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED LIGHTING PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)

SCALE: 1"=20'	SHEET 3	OF 5	SHEETS	STA. 53+00.00	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	106
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LT-09



NOTES:

- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- UCGS STANDS FOR UNDERGROUND CONDUIT, GALVANIZED STEEL.
- SETBACK IS FROM FACE OF CURB TO CENTER OF LIGHT POLE.

MODEL: Plan 1 - Plan 4
FILE NAME: F:\Projects\1094-Variou BLA\Design\ShIP\102218-sh-light-10.dgn

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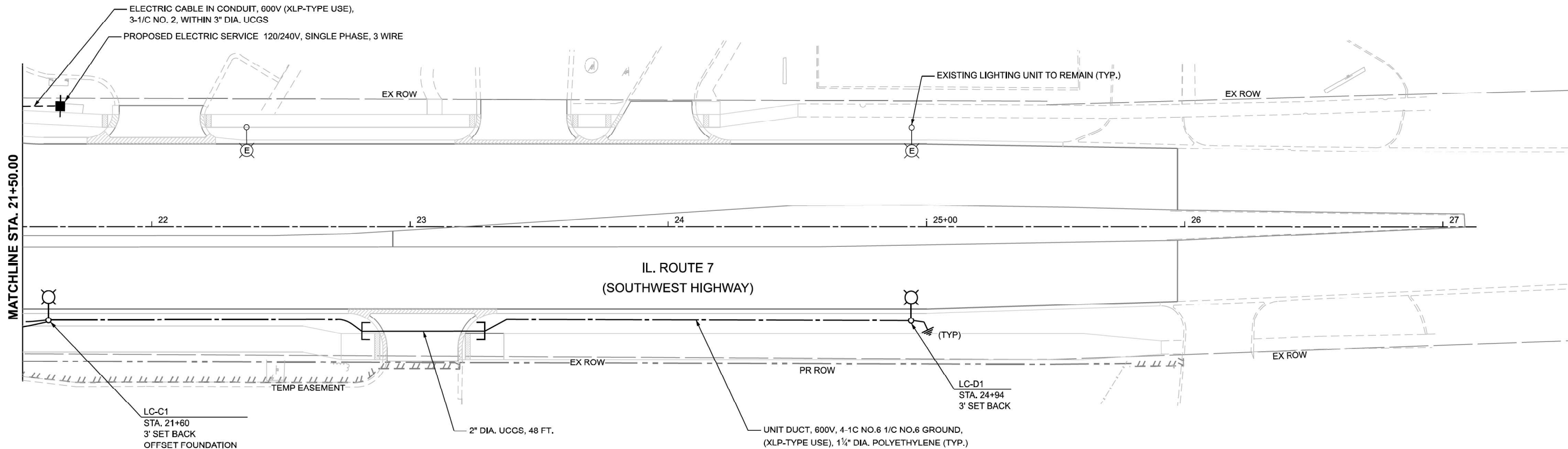
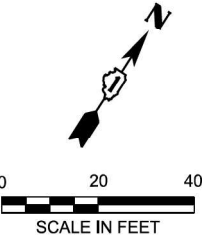
USER NAME = mdeitche	DESIGNED - MH	REVISED -
	DRAWN - MD	REVISED -
PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -
PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED LIGHTING PLAN			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)			
SCALE: 1"=20'	SHEET 4	OF 5 SHEETS	STA. TO STA. 18+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	107
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LT-10



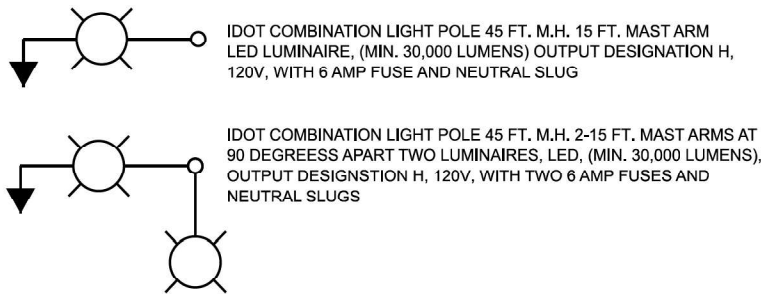
NOTES:

- 1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- 2. UCGS STANDS FOR UNDERGROUND CONDUIT, GALVANIZED STEEL.
- 3. SETBACK IS FROM FACE OF CURB TO CENTER OF LIGHT POLE.
- 4. 'OFFSET FOUNDATION' INDICATED WHERE THE LIGHTING UNIT HAS AN UNDERGROUND CONFLICT AND OFFSET FOUNDATION MAY BE REQUIRED.

MODEL: Plan4 - IL Plan 4
FILE NAME: F:\Projects\1094-Variou BLA\Design\Shp\102218-sh-light-11.dgn

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	PLOT SCALE = 0.16666633' / in.	CHECKED - BL	REVISED -		357B	2023-925-N-TS(L)	COOK	176	108				
	PLOT DATE = 6/26/2025	DATE = 10/15/2025	REVISED -		CONTRACT NO. 62W47								
					SCALE: 1"=20'	SHEET 5	OF 5	SHEETS	STA. 21+50.00	TO STA.	ILLINOIS FED. AID PROJECT		

LEGEND



IDOT COMBO LIGHTING SCHEDULE OF QUANTITIES		
DESCRIPTION	UNIT	QUANTITY
LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H	EACH	5
COMBINATION LIGHTING CONTROLLER	EACH	1
ELECTRIC CABLE IN CONDUIT, 600V, (XLP TYPE USE) 1/C NO. 10	FOOT	900
LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	5

NOTES:

- LUMINAIRES ARE POWERED FROM IDOT TRAFFIC CONTROLLER. SEE SHEET LT-16.
- THE COMBO LIGHTING CABLE AND SIGNAL CABLES SHALL BE IN SHARED CONDUIT.
- SEE TRAFFIC PLANS FOR LOCATION OF COMBO POLES, HANDHOLES AND CONDUIT.
- CIRCUIT DECALS SHALL NOT BE INSTALLED ON COMBO POLES. CIRCUITING DECALS SHOWN IN FOR GUIDANCE ONLY.
- FOR REMOVAL OF EXISTING COMBINATION LIGHTING, SEE TRAFFIC SIGNAL REMOVAL PLANS.

2 #10 ELECTRIC CABLE
IN 4" DIA. CONDUIT (TYP.)

A3
COMBO POLE

A2
COMBO POLE

A1
COMBO POLE

PROPOSED COMBINATION
LIGHTING CONTROLLER

B1 & B2
COMBO POLE

A2

A1

CIRCUIT A

CIRCUIT A

CIRCUIT B

PROPOSED COMBINATION
LIGHTING CONTROLLER

B2

B1

WIRING DIAGRAM

LT-12

MODEL: light comb (Sheet)
FILE NAME: F:\Projects\11094-Variou BLA\Design\Sheet\102218-sh-light-12.dgn

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PLOT DATE = 6/26/2025	CHECKED - BL	REVISED -
	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

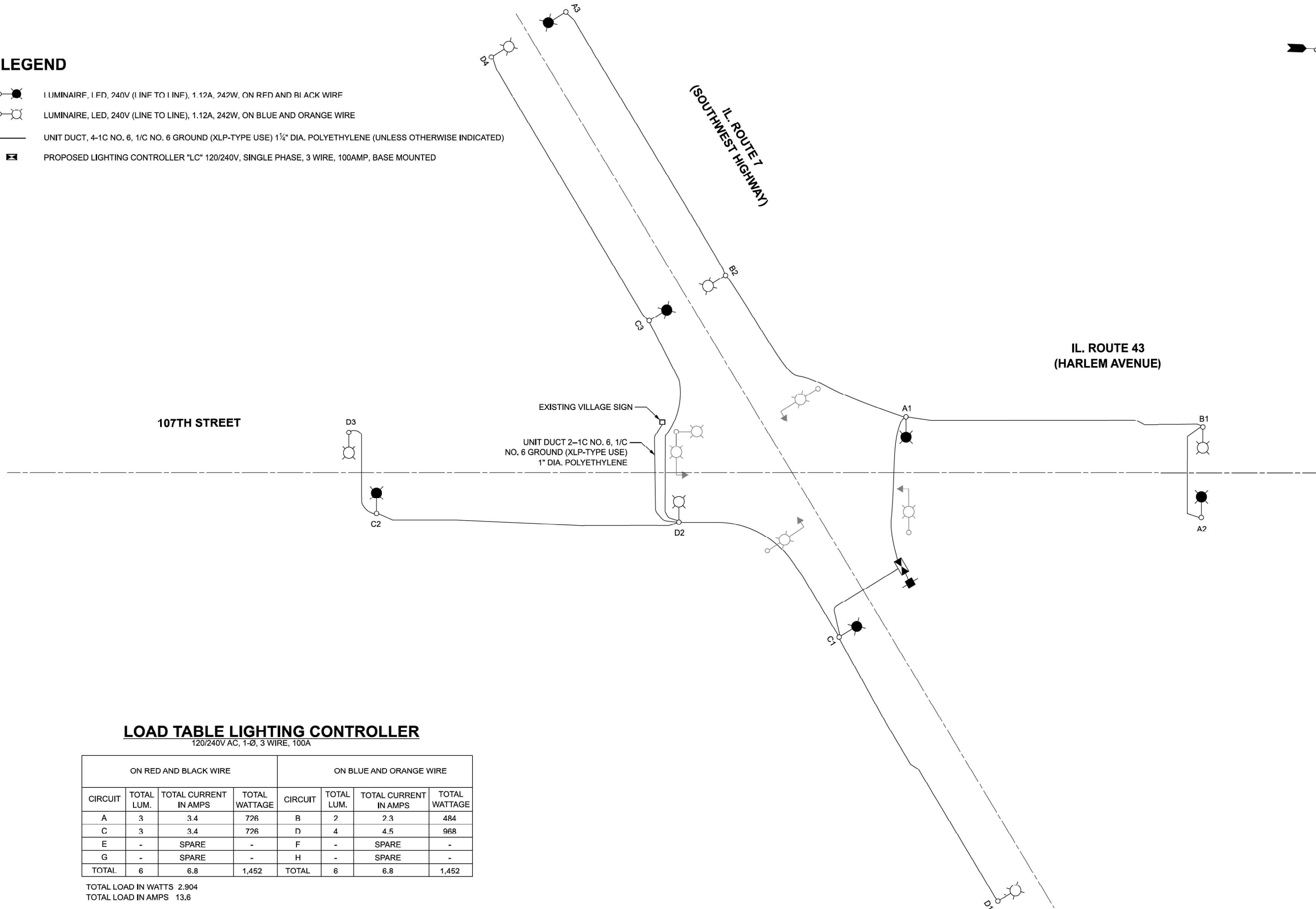
PROPOSED LIGHTING COMBINATION PLAN
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
357B	2023-925-N-TS(L)	COOK	176	109
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

LEGEND

- LUMINAIRE, LED, 240V (LINE TO LINE), 1.12A, 242W, ON RED AND BLACK WIRE
- LUMINAIRE, LED, 240V (LINE TO LINE), 1.12A, 242W, ON BLUE AND ORANGE WIRE
- UNIT DUCT, 4-1C NO. 6, 1/C NO. 6 GROUND (XLP-TYPE USE) 1¼" DIA. POLYETHYLENE (UNLESS OTHERWISE INDICATED)
- PROPOSED LIGHTING CONTROLLER "LC" 120/240V, SINGLE PHASE, 3 WIRE, 100AMP, BASE MOUNTED



LOAD TABLE LIGHTING CONTROLLER

120/240V AC, 1-Ø, 3 WIRE, 100A

ON RED AND BLACK WIRE				ON BLUE AND ORANGE WIRE			
CIRCUIT	TOTAL LUM.	TOTAL CURRENT IN AMPS	TOTAL WATTAGE	CIRCUIT	TOTAL LUM.	TOTAL CURRENT IN AMPS	TOTAL WATTAGE
A	3	3.4	726	B	2	2.3	484
C	3	3.4	726	D	4	4.5	968
E	-	SPARE	-	F	-	SPARE	-
G	-	SPARE	-	H	-	SPARE	-
TOTAL	6	6.8	1,452	TOTAL	6	6.8	1,452

TOTAL LOAD IN WATTS 2.904
TOTAL LOAD IN AMPS 13.6

MODEL: wiring [Sheet]
FILE NAME: P:\Projects\1094-Variou BLA\Design\ShIP102218-sh-light-13.dgn

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USER NAME

= mdelitche

DESIGNED

- MH

DRAWN

- MD

REVIS

-

REVIS

-

CHECKED

- BL

REVIS

-

DATE

- 10/15/2025

REVIS

-

PLOT SCALE

= 0.08333317" / in.

PLOT DATE

= 6/26/2025

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

WIRING DIAGRAM

IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)

SCALE: NONE

SHEET

OF

SHEETS

STA.

TO STA.

F.A.U. RTE.

3578

SECTION

2023-925-N-TS(L)

COUNTY

COOK

TOTAL SHEETS

176

SHEET NO.

110

CONTRACT NO.

62W47

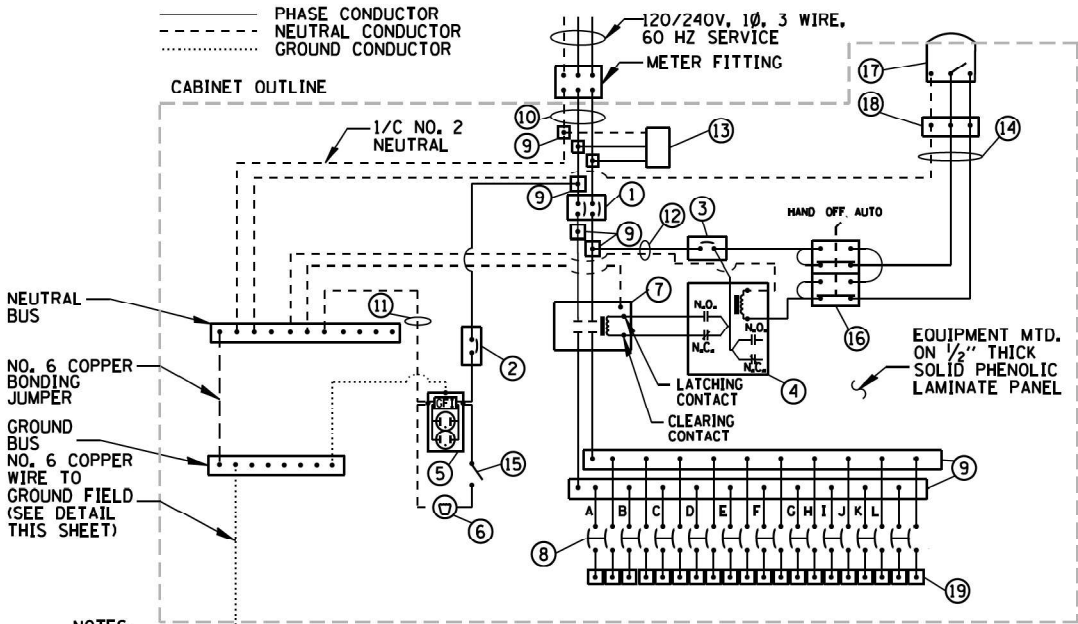
ILLINOIS

FED. AID PROJECT

ITEM	SPECIFICATION
1 MAIN CIRCUIT BREAKER	100 AMPERE, 2P, 240V SERVICE RATING, 10KAIC
2 LAMPHOLDER CIRCUIT BREAKER	20 AMPERE, 1P, 120V RATING, 10KAIC
3 PHOTOELECTRIC CONTROL CIRCUIT BREAKER	15 AMPERE, 1P, 120V RATING, 10KAIC
4 AUXILIARY RELAY	120 V OPERATED DPDT 60 HZ COIL 2 NO & 2 NC CONTACTS
5 CABINET RECEPTACLE AND BOX	COMMERCIAL GRADE GFCI 20A/120V, MOUNTED IN A WEATHERPROOF CAST ALUMINUM SINGLE GANG BOX WITH WEATHERPROOF COVER
6 CABINET LIGHT	5 WATT LED STRIP LIGHT, 60K HOUR RATING, 65K COLOR TEMPERATURE, DOOR SWITCH CONTROLLED, FASTENED TO TOP OF CABINET
7 CONTACTOR	100 AMPERE, 2P, 120V COIL, MECHANICALLY HELD
8 BRANCH LINE CIRCUIT BREAKERS	(10) 20A, 2P, 240V RATING, 10KAIC
9 POWER DISTRIBUTION BLOCK	600 VOLT, INSULATED, SIZE AS REQ'D, 10KAIC
10 SERVICE CABLES	3-600V (XLP-TYPE USE) NO. #2
11 LAMPHOLDER WIRE	2-600V XLP NO. 12
12 CONTROL WIRE	2-600V XLP NO. 12
13 SURGE ARRESTOR	10 K AMPERE RATING
14 PHOTOELECTRIC CONTROL WIRE	3-600V XLP NO. 12
15 DOOR SWITCH	20A/120V, DOOR MOUNTED SNAP ACTION TYPE PLUNGER SWITCH
16 HAND-AUTO-OFF CONTROL SWITCH	20A, 3 POS. MTD IN CAST ALUM. ENCLOSURE
17 PHOTOCELL	120V, MTD. ON CABINET, DELAY TYPE, SPST-NC
18 TERMINAL BLOCK	3 TERMINAL, SCREW TYPE, MAX. #10 WIRE SIZE
19 CIRCUIT BREAKER DIST. BLOCKS	30A, 600V, 10 KAIC - WIRE RANGE: LINE SIDE LUG (1) #2/0-#14 AWG, LOAD SIDE LUGS (2) #4-#14 AWG

NOTES:

- ALL ITEMS LISTED IN LIGHTING CONTROLLER COMPONENT SCHEDULE SHALL BE CONSIDERED INCIDENTAL TO THE PRICE BID FOR "LIGHTING CONTROLLER, BASE MOUNTED, 240 VOLT, 100 AMP" INCLUDING CABINET AND FOUNDATION.
- THE LIGHTING CONTROLLER TOGETHER WITH ALL OF ITS COMPONENTS SHALL BE UL LISTED AS AN "ENCLOSED INDUSTRIAL CONTROL PANEL" UNDER UL508A.
- CONNECTION OF SURGE ARRESTOR TO LINE SIDE OF MAIN CIRCUIT BREAKER SHALL NOT BE "DOUBLE LUGGED."
- THE MAIN CIRCUIT BREAKER SHALL BE LABELED "SERVICE DISCONNECT". LAMPHOLDER CIRCUIT BREAKER SHALL HAVE RED WARNING NAMEPLATE INDICATING "MAINTENANCE CIRCUIT IS LIVE WHEN MAIN BREAKER IS SWITCHED OFF".
- ALL SWITCHES AND CONTROLS SHALL BE IDENTIFIED USING TWO COLOR ENGRAVED NAMEPLATES.
- THE PANEL MANUFACTURER SHALL LABEL THE CABINET WITH THE APPROPRIATE ARC FLASH WARNING AND PERSONNEL PROTECTION EQUIPMENT REQUIRED FOR SERVICING.
- ALL EXPOSED BUS BARS SHALL BE INSULATED.
- ALL WIRING SHALL BE COPPER.

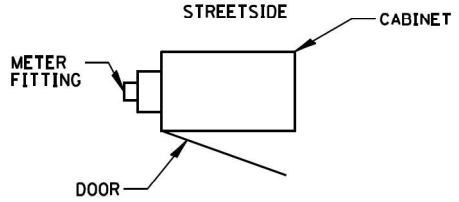


NOTES:

- ALL GROUND CONDUCTORS SHALL BE GREEN AND NEUTRAL CONDUCTORS SHALL BE WHITE. PHASE CONDUCTORS SHALL BE COLOR CODED IN ACCORDANCE WITH SECTION 1066.02 OF THE STANDARD SPECIFICATIONS. CONTRACTOR SHALL ONLY USE SOLID COLOR CODED INSULATIONS. COLOR STRIPPING OF PHASE CONDUCTORS SHALL NOT BE ALLOWED.
- IN ADDITION TO THE TERMINATIONS SHOWN, THE NEUTRAL AND GROUND BUS BARS SHALL EACH ACCOMMODATE A MINIMUM OF 8 ADDITIONAL TERMINATIONS (#2-#14).
- IN ADDITION TO THE TERMINATIONS SHOWN, THE POWER DISTRIBUTION BLOCKS ON THE LOAD SIDE OF THE MAIN BREAKER SHALL EACH ACCOMMODATE A MINIMUM OF 4 ADDITIONAL TERMINATIONS (#2-#14).

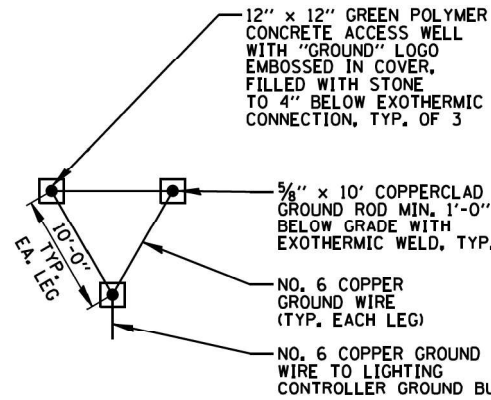
LIGHTING CONTROLLER WIRING DETAIL

N.T.S.



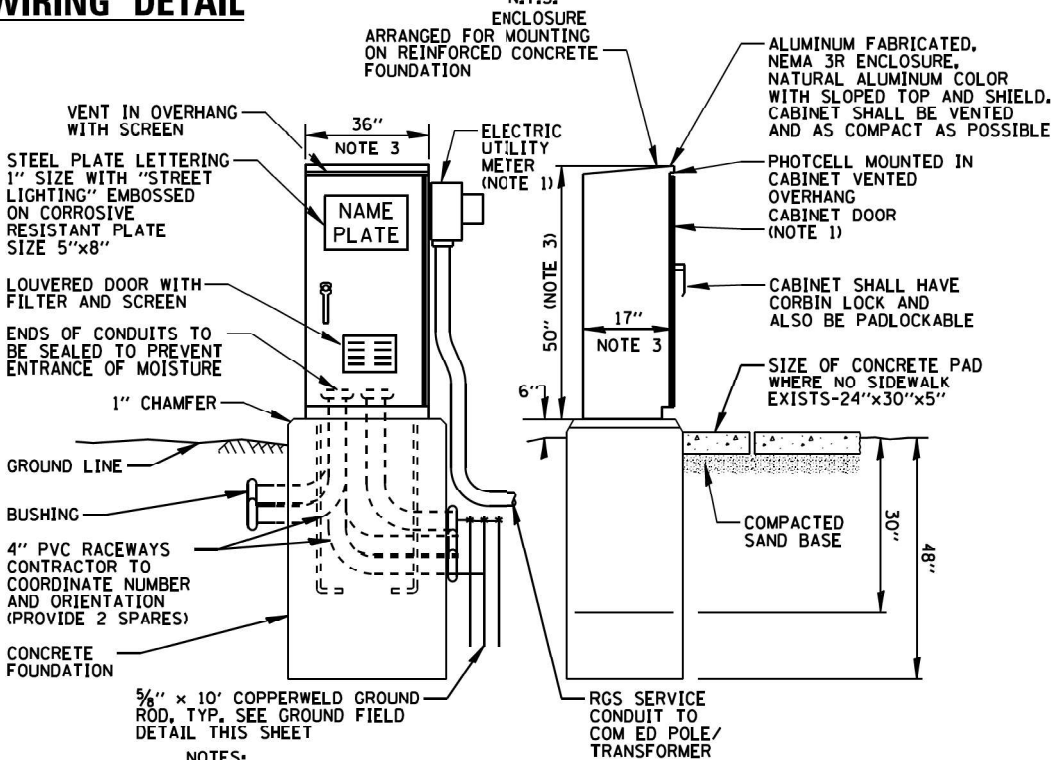
CABINET METER FITTING & DOOR ORIENTATION

N.T.S.



GROUND FIELD DETAIL (TYP.)

N.T.S.



NOTES:

- SEE CABINET METER FITTING AND DOOR ORIENTATION DETAIL FOR METER AND DOOR ORIENTATION.
- ALL ITEMS SHOWN ABOVE (INCLUDING FOUNDATION & GROUND FIELD) SHALL BE INCLUDED IN THE PRICE BID FOR "LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100 AMP", EXCEPT FOR THE SERVICE CONDUIT WHICH WILL BE PAID FOR SEPARATELY.
- CABINET DIMENSIONS SHOWN ARE APPROXIMATE, CABINET SHALL BE AS COMPACT AS POSSIBLE, CONTRACTOR TO COORDINATE.

LIGHTING CONTROLLER CABINET AND FOUNDATION

N.T.S.

LT-14

MODEL: LIGHT CONTROLLER-1 [Sheet]
FILE NAME: F:\Projects\1094-Variou BLA\Design\Shp\102218-sh-light-14.dgn

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

USER NAME = mdeitche	DESIGNED - MH	REVISED -
	DRAWN - MD	REVISED -
PLOT SCALE = 0.16666633 / in.	CHECKED - BL	REVISED -
PLOT DATE = 6/26/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHTING CONTROLLER DETAIL	
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE.)	
SCALE: 1"=20'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	111
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



1. 4 LUMINAIRES PER CIRCUIT, MAXIMUM.
2. TWO #10 (XLP-TYPE USE) CABLES TO BE USED FOR LIGHTING CIRCUITS.
3. ROUTE LIGHTING CIRCUITS IN TRAFFIC SIGNAL CONDUIT SYSTEM.
4. ALL SPLICES AND CONNECTIONS FOR ROADWAY LIGHTING SHALL BE AT POLE BASE ONLY.
5. FOR LIGHTING CIRCUITS, CONNECT TWO CIRCUIT BREAKERS TO AC SERVICE TERMINAL BLOCK.
6. ALL WIRING SHALL BE NEATLY DRESSED, IDENTIFIED BY TAGS, AND SUPPORTED.
(UNDERGROUND SPLICING OF LIGHTING CONDUCTORS IS NOT PERMITTED).
7. THE H.O.A. SWITCH SHALL BE LABELED AS "LIGHTING CONTROL" WITH THE
POSITIONS "AUTO", "OFF" AND "TEST" WITH ENGRAVED NAME PLATES.
8. LIGHTING CONNECTED TO UPS BYPASS CIRCUIT.
9. COMBINATION LIGHTING MUST BE INSTALLED PRIOR TO SIGNAL TURN ON.
10. LUMINAIRE VOLTAGE SHALL BE 120V
11. POLE WIRING & FUSE KITS ARE INCLUDED IN THE LUMINAIRE PAY ITEM.
12. THE UNDERGROUND EQUIPMENT GROUND WIRE IS SHOWN IN THE TRAFFIC SIGNAL PLANS
AND IS INCLUDED IN THE SIGNAL PLANS. IT IS SHARED GROUND BETWEEN SIGNALS AND LIGHTING.

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MODEL: Default
FILE NAME: pw

 **AMES Engineering, Inc.**
CONSULTING ENGINEERS

6330 Belmont Road, Suite 4B
Downers Grove, IL 60516



1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
3. THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
4. THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
5. THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
6. THE CONCRETE SHALL BE CLASS SL. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
7. THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
8. THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
9. ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG (MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
10. THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
11. ANCHOR RODS SHALL PROJECT 23#4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
12. THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
13. THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
14. THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0" (3.962 m)	15'-0" (4.572 m)	8-#6X12'-6" (3.810 m)	#3X122' (37.186 m)	8-#6X14'-3" (4.343 m)	#3X141' (42.977 m)
MEDIUM CLAY	9'-6" (2.896 m)	10'-9" (3.277 m)	8-#6X9'-0" (2.743 m)	#3X90' (27.432 m)	8-#6X10'-0" (3.048 m)	#3X100' (30.480 m)
STIFF CLAY	7'-0" (2.134 m)	8'-0" (2.438 m)	8-#6X6'-6" (1.981 m)	#3X66' (20.112 m)	8-#6X7'-6" (2.286 m)	#3X76' (23.165 m)
LOOSE SAND	9'-0" (2.743 m)	10'-0" (3.048 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)	8-#6X9'-6" (2.896 m)	#3X94' (28.651 m)
MEDIUM SAND	8'-3" (2.515 m)	9'-0" (2.743 m)	8-#6X8'-0" (2.438 m)	#3X78' (23.774 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
DENSE SAND	7'-9" (2.362 m)	9'-0" (2.743 m)	8-#6X7'-6" (2.286 m)	#3X73' (22.250 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
ROCK OR SOLIDIFIED SLAG	5'-0" (1.524 m)	5'-0" (1.524 m)	NONE	NONE	NONE	NONE

BILL OF MATERIAL

MARK	NO.	SIZE	LENGTH	SHAPE
a	10	6	SEE BELOW	—
s	12	4	8'-0" (2.438 m)	□
s1	3	3	7'-6" (2.286 m)	□
v1	8	6	2'-9" (0.838 m)	—
v2				

OFFSET SCHEDULE

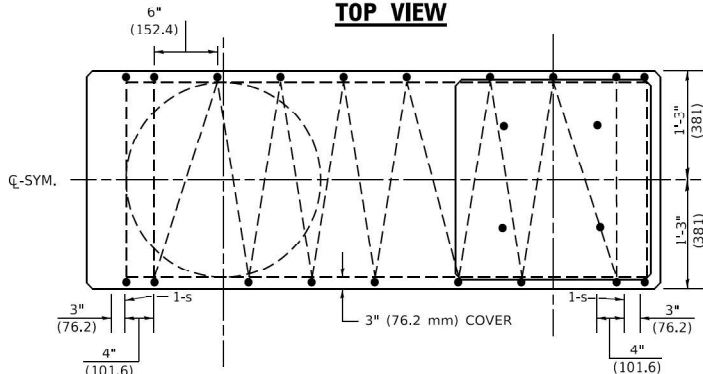
SEWER DIAM. d IN.	PILE OFFSET from C-MED'N FT.	LENGTH of BAR a FT.
UP TO 24" (609.6 mm)	3'-3" (0.991 m)	#6 x 5'-3" (1.600 m)
27" (685.8 m) TO 36" (914.4 mm)	3'-9" (1.143 m)	5'-9" (1.753 m)
42" (1066.8 mm) TO 48" (1219.2 mm)	4'-6" (1.372 m)	6'-6" (1.981 m)
54" (1371.6 mm) TO 60" (1524.0 mm)	5'-0" (1.524 m)	7'-0" (2.134 m)
66" (1676.4 mm) TO 72" (1828.8 mm)	5'-6" (1.676 m)	7'-6" (2.286 m)

NOTES

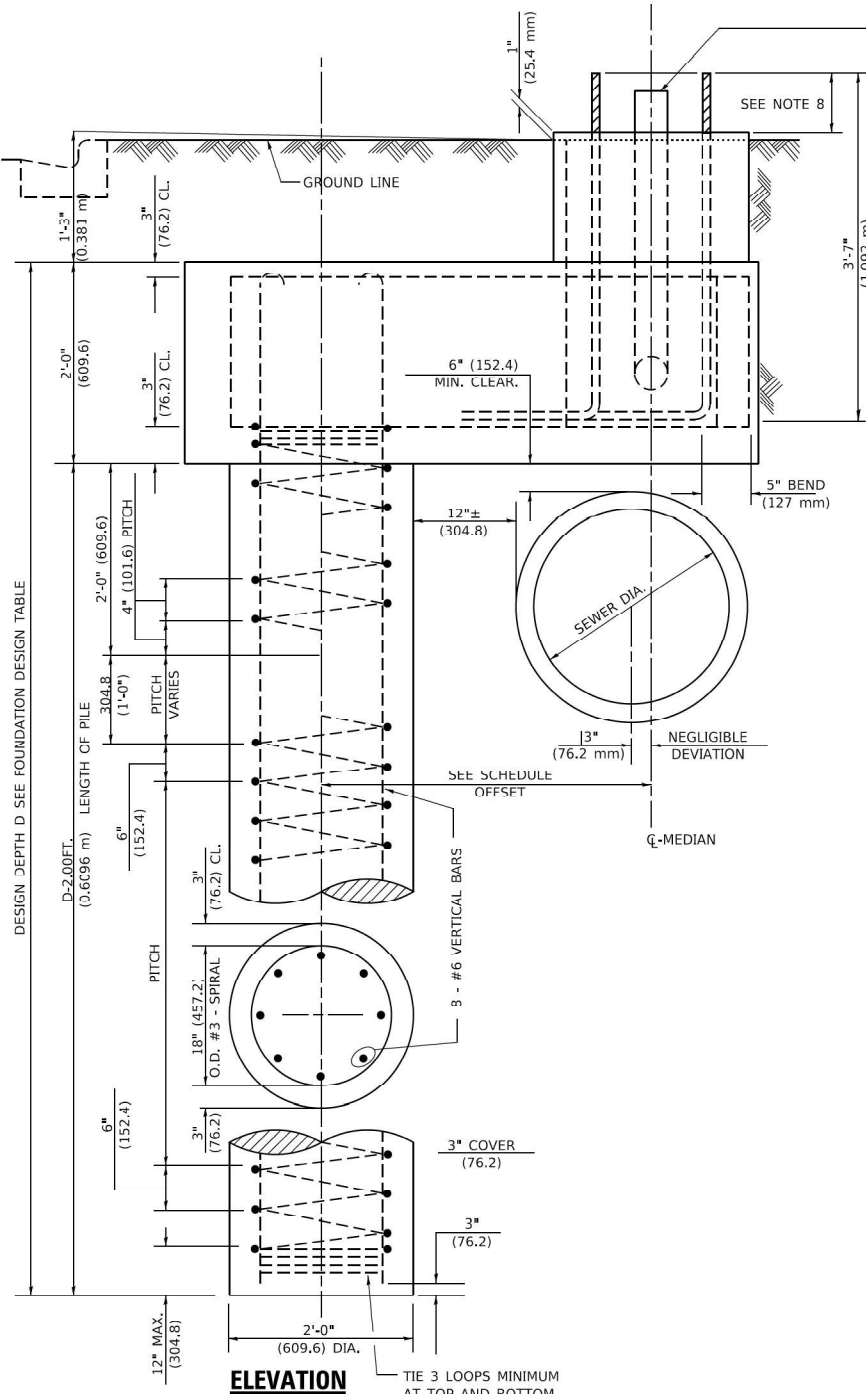
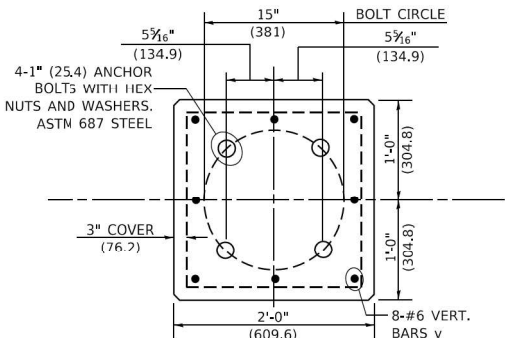
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 23#4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CARP TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.

PLAN-CAP BEAM

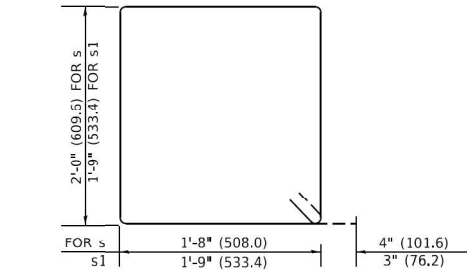
TOP VIEW



TOP VIEW



END VIEW



BARS s, s1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

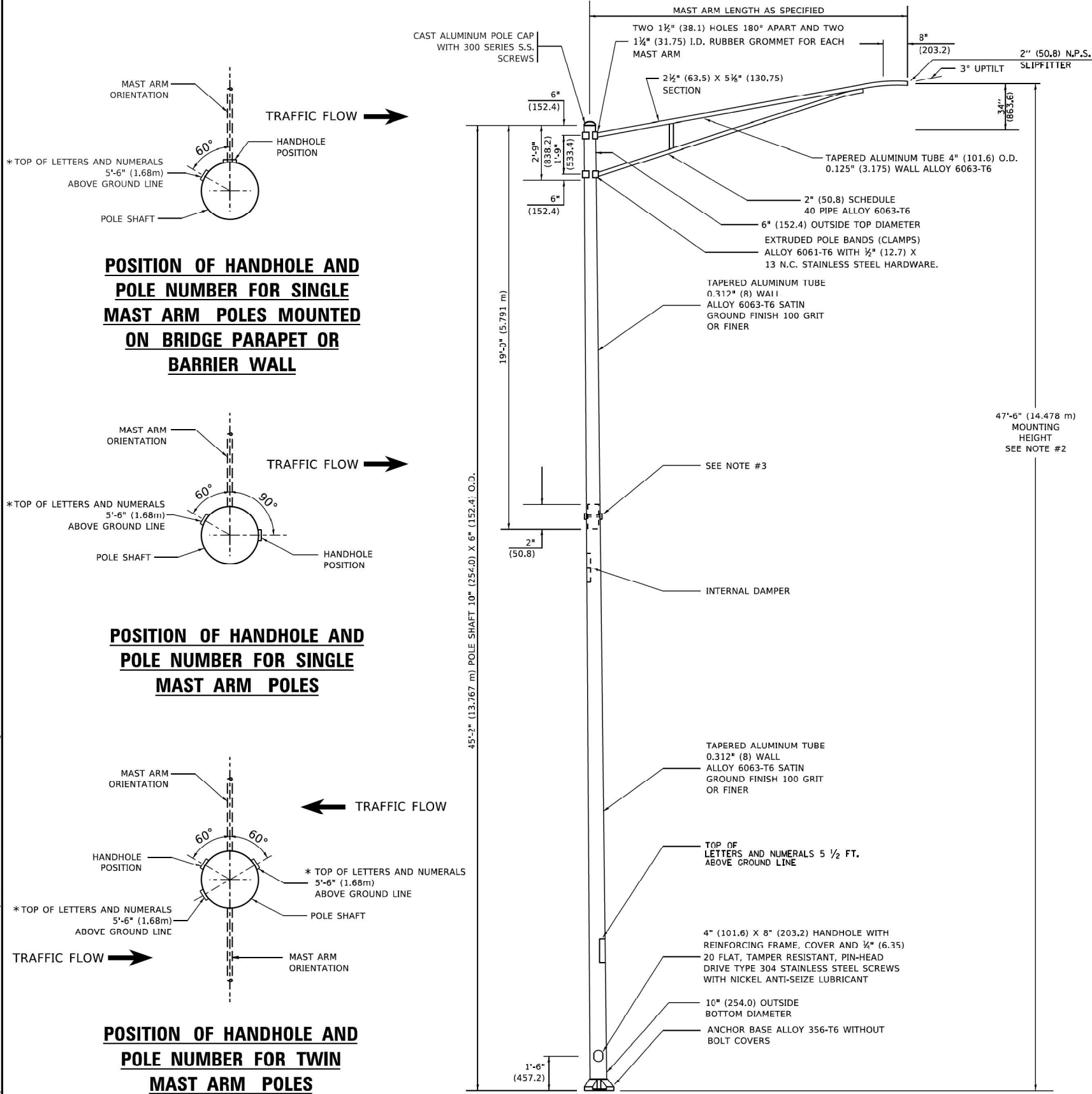
LIGHT POLE FOUNDATION OFFSET
40' (12.192m) TO 47 1/2' (14.478m) M.H., 15" (381mm) BOLT CIRCLE

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BE310		176	115
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

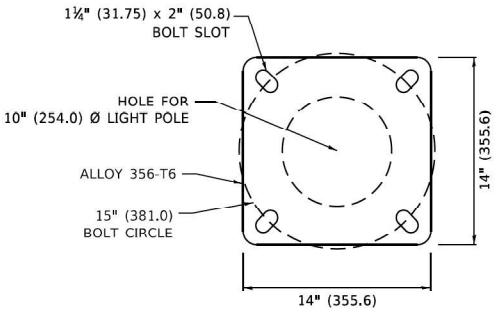
LT-18

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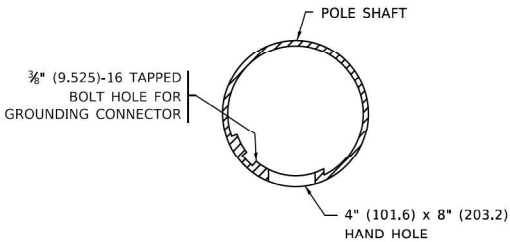
NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
- TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
- THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
- THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR. BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
- LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
- LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
- LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

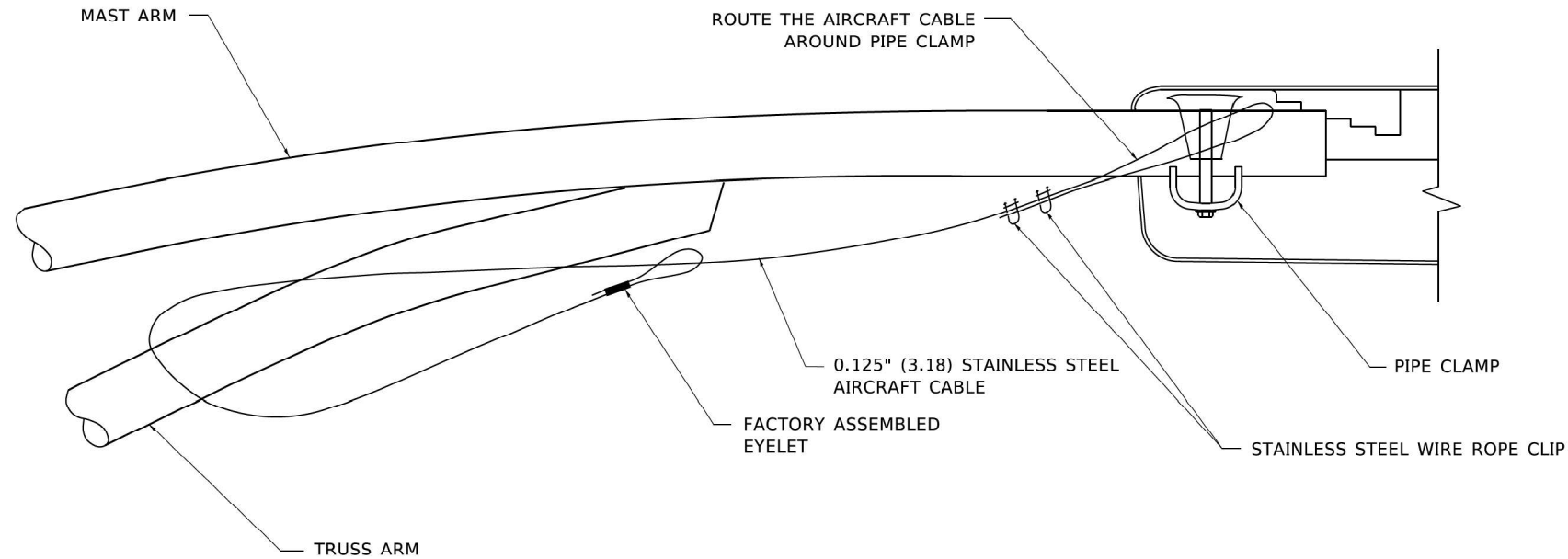


LIGHT POLE BASE PLATE DETAIL

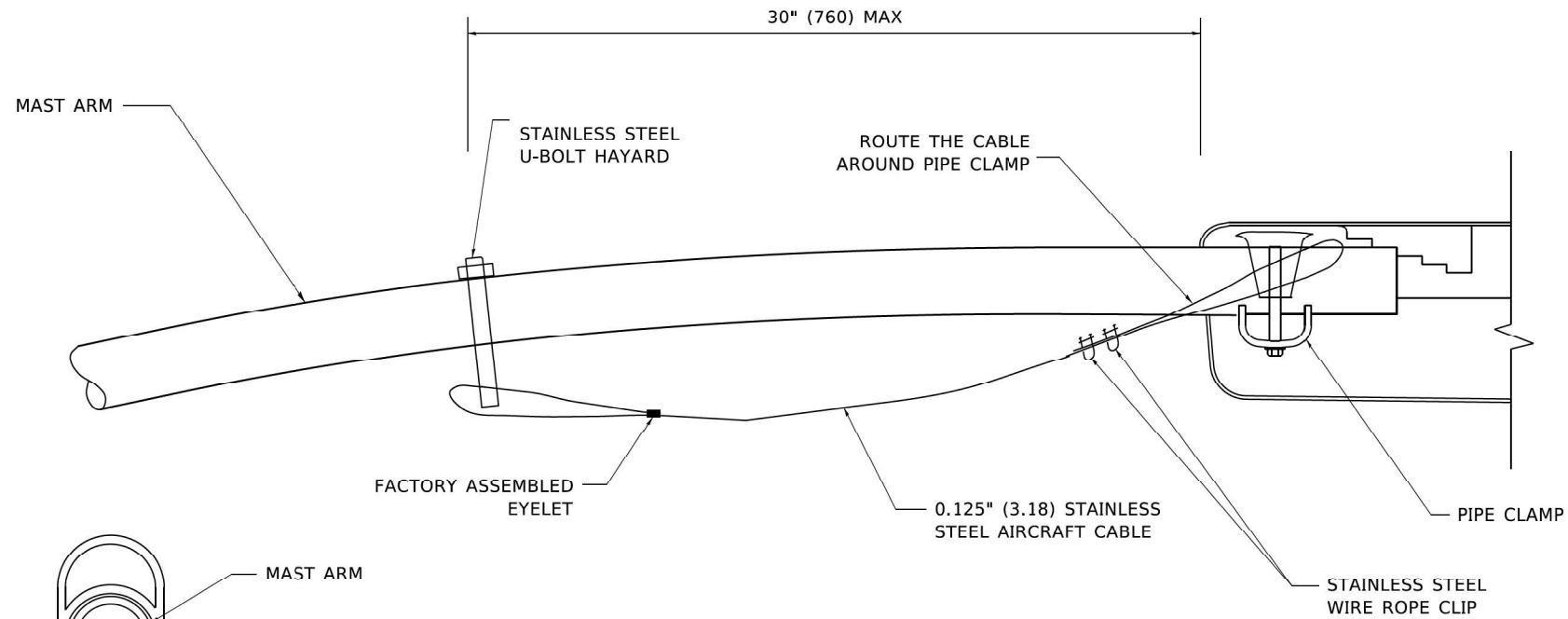
15 INCH (381.0) BOLT CIRCLE



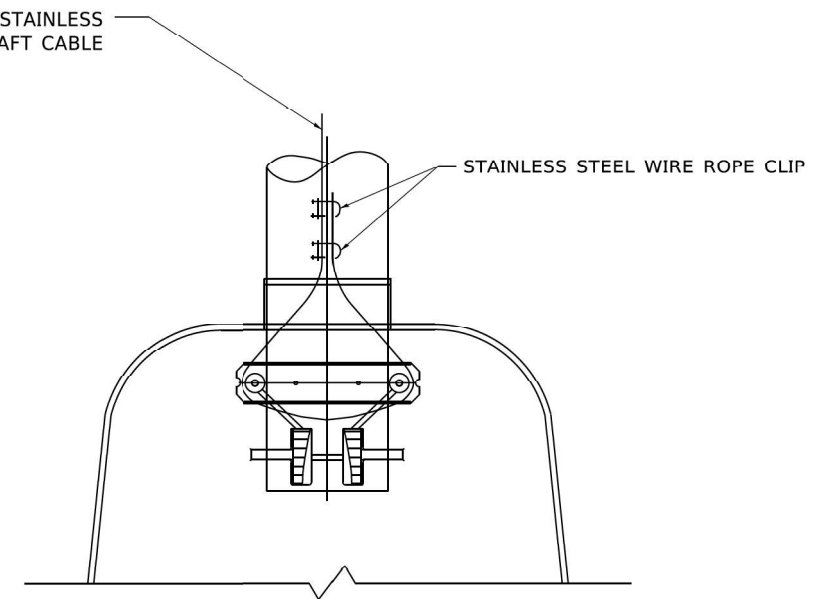
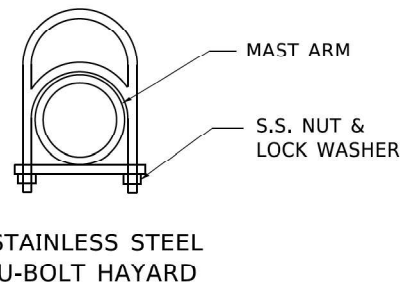
HANDHOLE DETAIL (N.T.S.)



SIDE VIEW (TRUSS ARM)
N.T.S.



SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)
N.T.S.



BOTTOM VIEW
N.T.S.

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

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AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

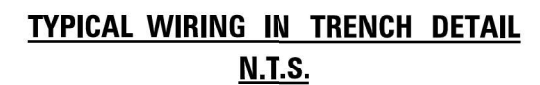
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LUMINAIRE SAFETY CABLE ASSEMBLY

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			176	117
BE-701		CONTRACT NO.		
		ILLINOIS	FED. AID PROJECT	



The proposed improvements on deck consist of removal of the existing 3½" concrete median, hydroscarification of the 14-ft median and construction of a latex concrete overlay. The proposed improvements on south approach consist of removal of the existing 3½ concrete median and ¼" scarification on existing approach pavement.

Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contactor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

1. Remove existing concrete median on deck and south approach.
2. Scarify deck and south approach below median.
3. Sound existing deck below median.
4. Perform partial deck and south approach repairs.
5. Overlay deck.

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	18.6
Approach Slab Repair (Partial Depth)	Sq Yd	0.4
Bridge Deck Latex Concrete Overlay, 2 1/4 Inches	Sq Yd	156
Bridge Deck Scarification 3/4"	Sq Yd	156
Concrete Pavement Scarification	Sq Yd	135
Deck Slab Repair (Partial)	Sq Yd	4.6

1. General Plan and Elevation
2. Bridge Deck Cross Sections
3. South Approach Cross Sections
4. Details

2024 AASHTO LRFD Bridge Design
Specifications, 10th Edition

$f'_c = 3,500 \text{ psi}$
 $f_y = 60,000 \text{ psi (Reinforcement)}$

Range 12E, 3rd P.M.

Twp. 37N

12 7

13 18

Stony Creek

IL 43

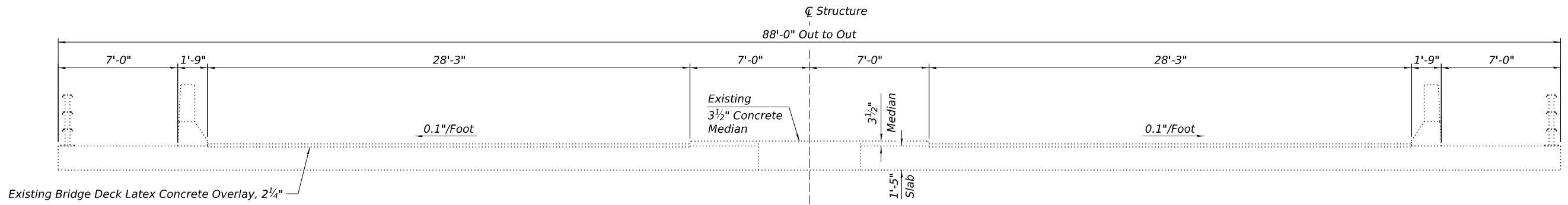
I-294

Proposed Rehabilitation Project

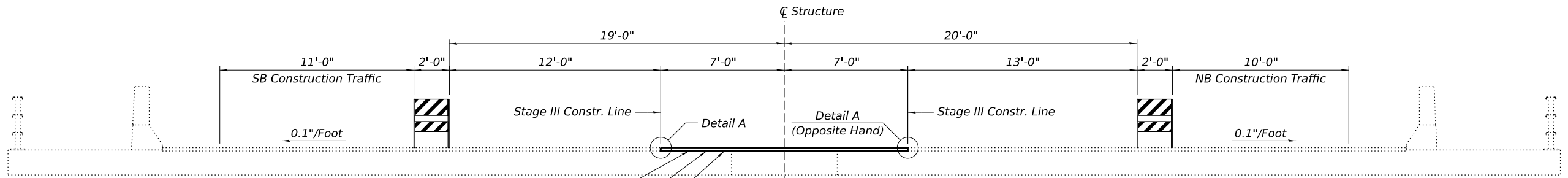
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LOCATION SKETCH

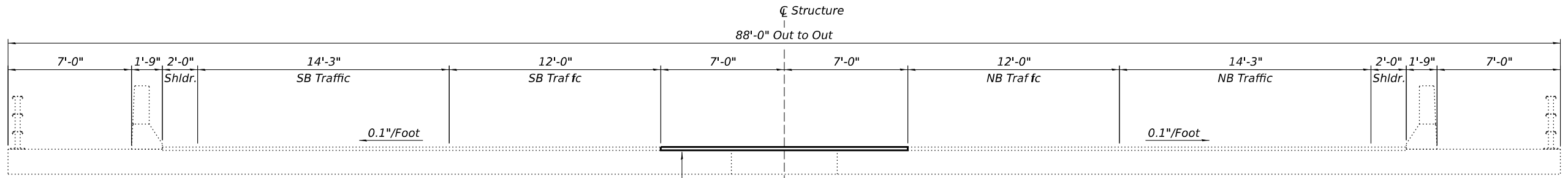
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CONTRACT NO. 62W47				
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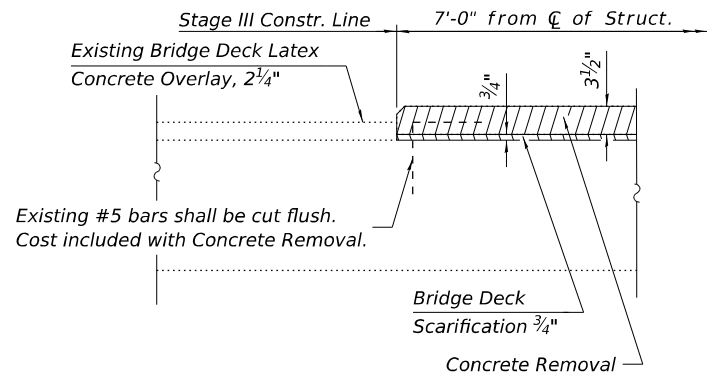
EXISTING CROSS SECTION
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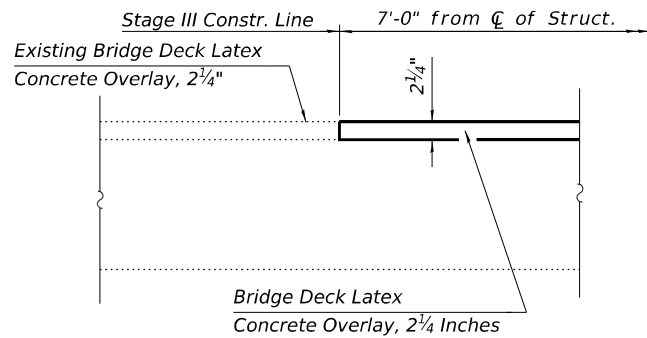
Stage III CROSS SECTION
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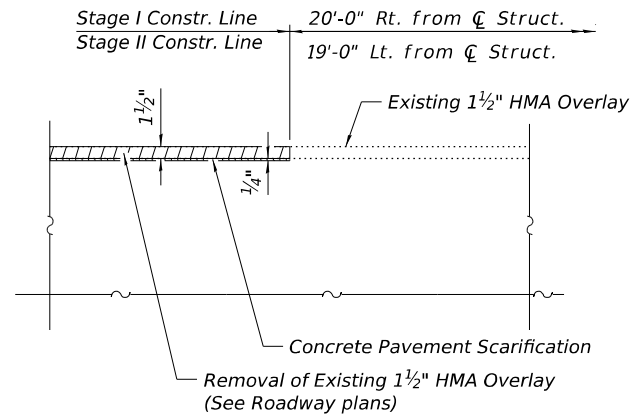
FINAL CROSS SECTION
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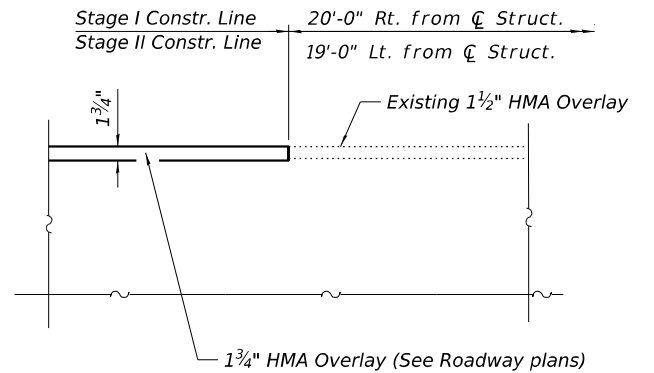
**DETAIL A
ON DECK**
(Showing Removal)



**DETAIL A
ON DECK**
(Showing Proposed)



**DETAIL B
ON SOUTH APPROACH**
(Showing Removal)



**DETAIL B
ON SOUTH APPROACH**
(Showing Proposed)

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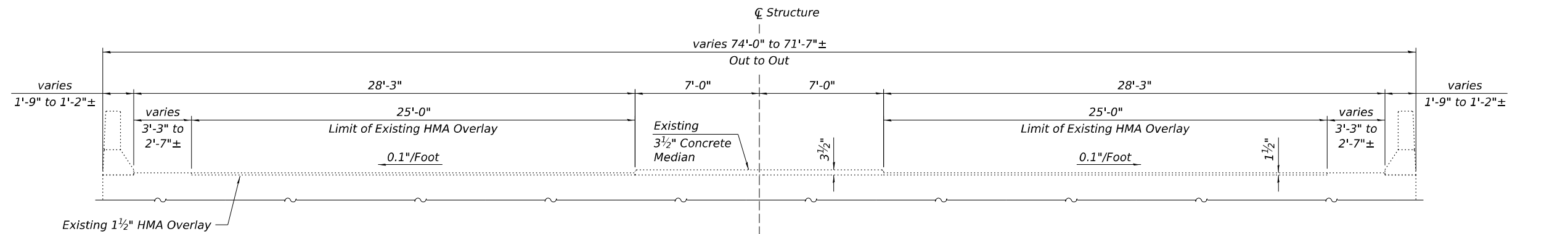
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

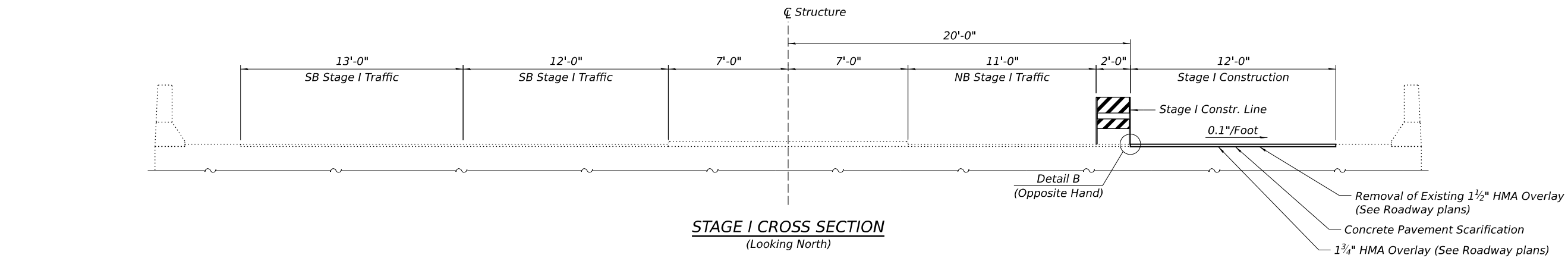
**BRIDGE DECK CROSS SECTIONS
STRUCTURE NO. 016-0322**

SHEET 2 OF 4 SHEETS

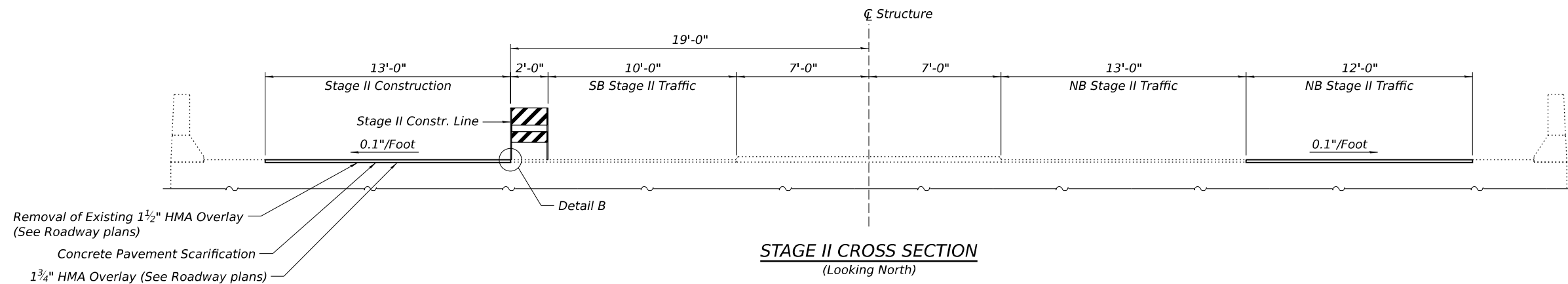
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348	2023-025-N-TS(L)	COOK	176	120
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



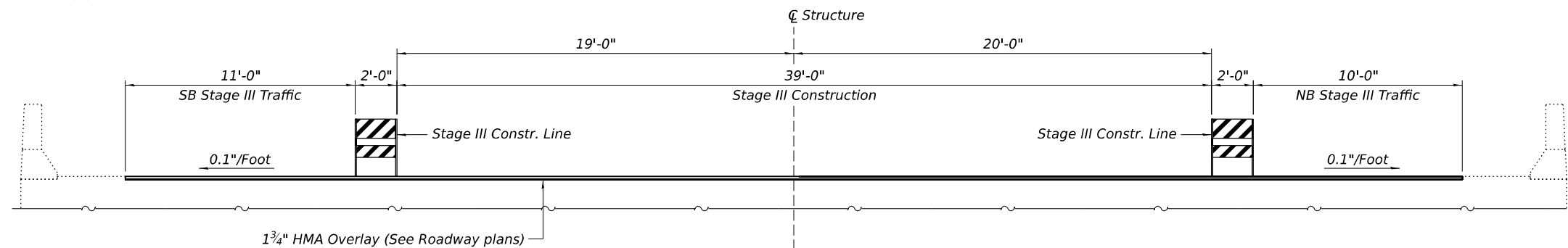
EXISTING CROSS SECTION
(Looking North)



STAGE I CROSS SECTION
(Looking North)



STAGE II CROSS SECTION
(Looking North)



STAGE III CROSS SECTION
(Looking North)

Note:
See sheet 2 of 4 for Detail B.

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	CHECKED - MJW	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH APPROACH CROSS SECTIONS
STRUCTURE NO. 016-0322

SHEET 3 OF 4 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	2023-025-N-TS(L)	COOK	176	121
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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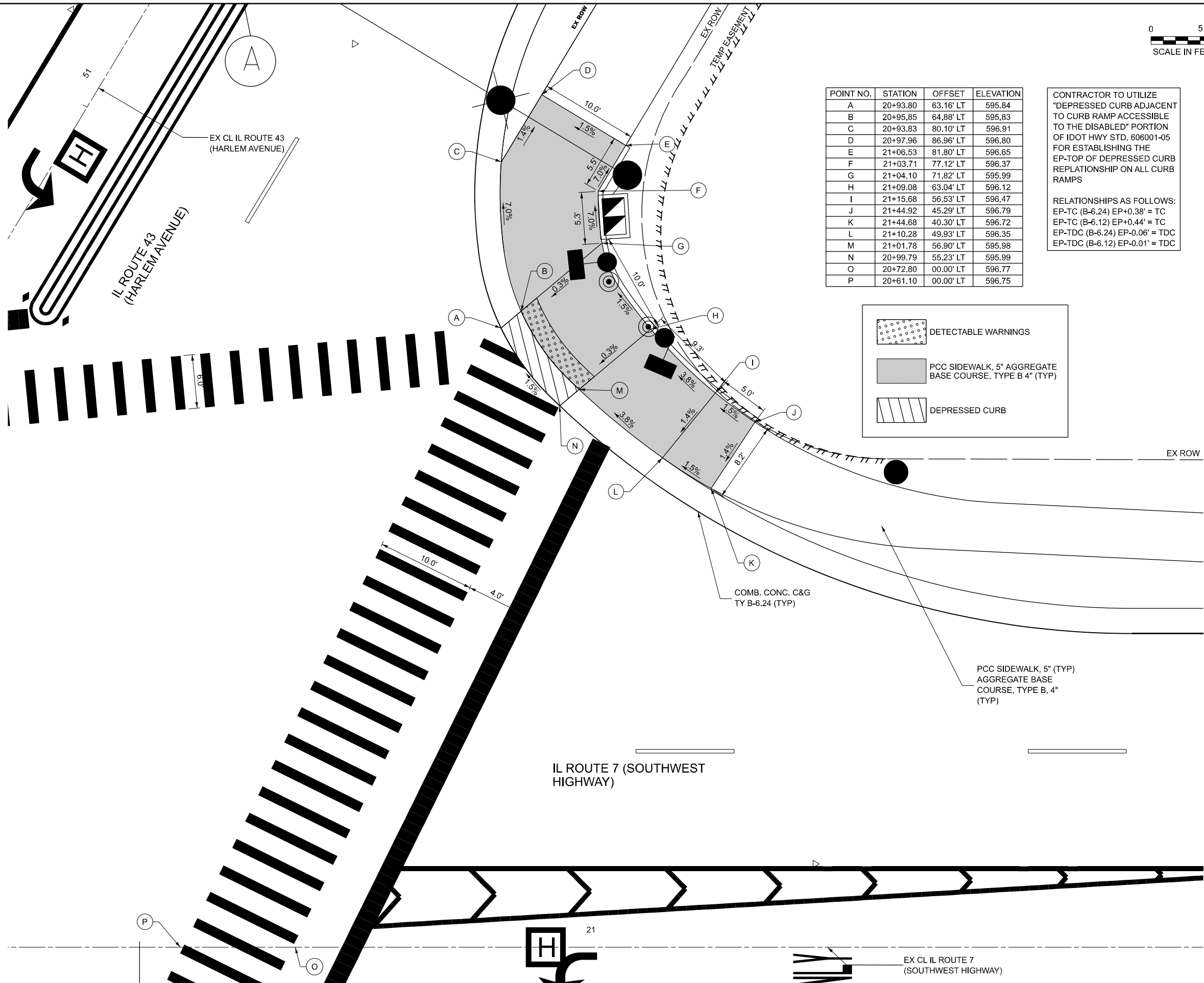
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REVISD	-
REVISD	-
REVISD	-
REVISD	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=5'	SHEET 2	OF 12 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	124
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

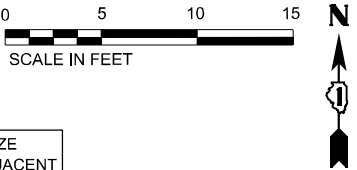


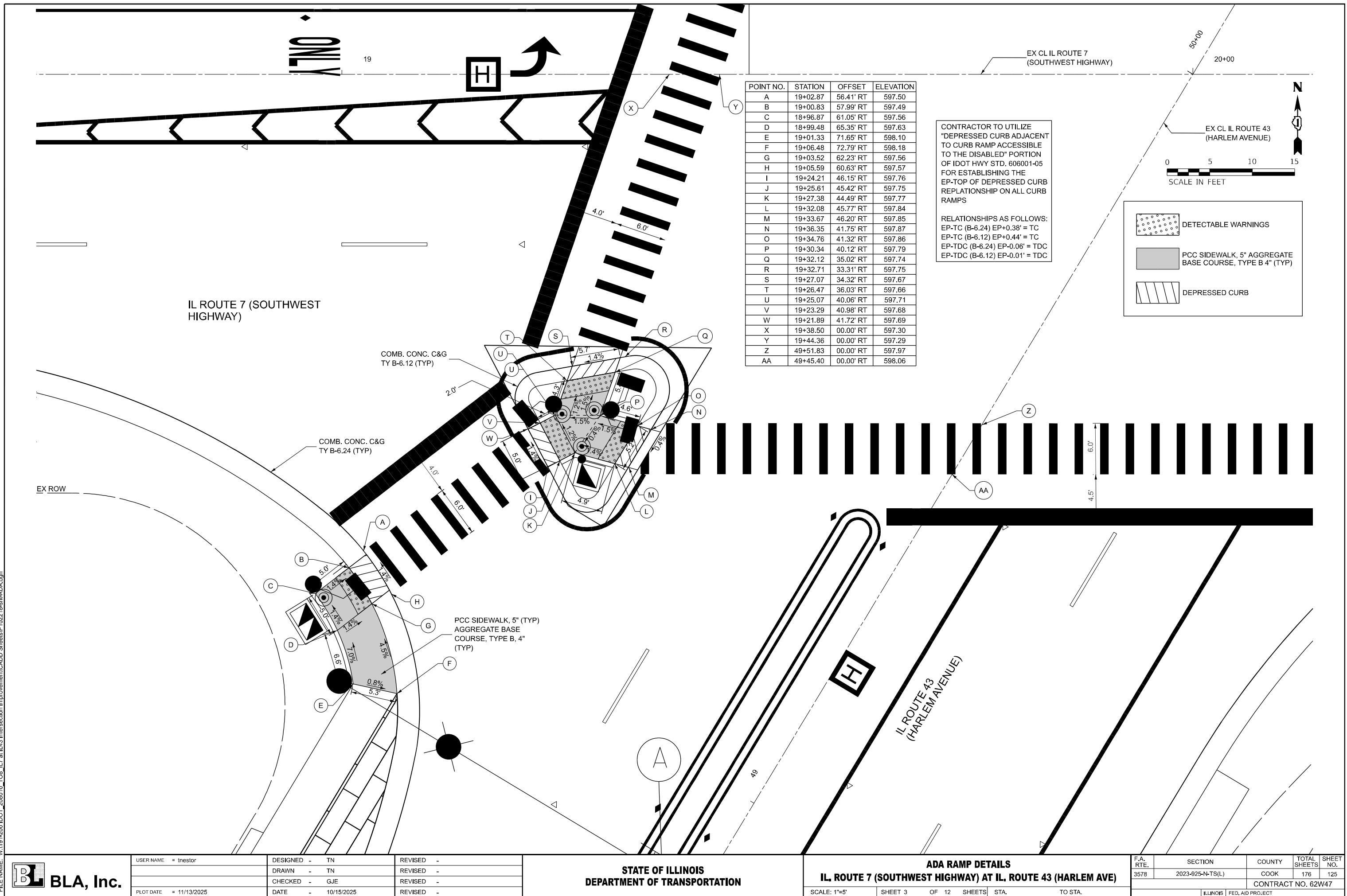
POINT NO.	STATION	OFFSET	ELEVATION
A	20+93.80	63.16' LT	595.84
B	20+95.85	64.88' LT	595.83
C	20+93.83	80.10' LT	596.91
D	20+97.96	86.96' LT	596.80
E	21+06.53	81.80' LT	596.65
F	21+03.71	77.12' LT	596.37
G	21+04.10	71.82' LT	595.99
H	21+09.08	63.04' LT	596.12
I	21+15.68	56.53' LT	596.47
J	21+44.92	45.29' LT	596.79
K	21+44.68	40.30' LT	596.72
L	21+10.28	49.93' LT	596.35
M	21+01.78	56.90' LT	595.98
N	20+99.79	55.23' LT	595.99
O	20+72.80	00.00' LT	596.77
P	20+61.10	00.00' LT	596.75

CONTRACTOR TO UTILIZE
"DEPRESSED CURB ADJACENT
TO CURB RAMP ACCESSIBLE
TO THE DISABLED" PORTION
OF IDOT HWY STD. 606001-05
FOR ESTABLISHING THE
EP-TOP OF DEPRESSED CURB
REPLATIONSHIP ON ALL CURB
RAMPS

RELATIONSHIPS AS FOLLOWS:
EP-TC (B-6.24) EP+0.38' = TC
EP-TC (B-6.12) EP+0.44' = TC
EP-TDC (B-6.24) EP-0.06' = TDC
EP-TDC (B-6.12) EP-0.01' = TDC

	DETECTABLE WARNINGS
	PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B 4" (TYP)
	DEPRESSED CURB





MODEL: 4
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PLOT DATE	= 10/17/2025

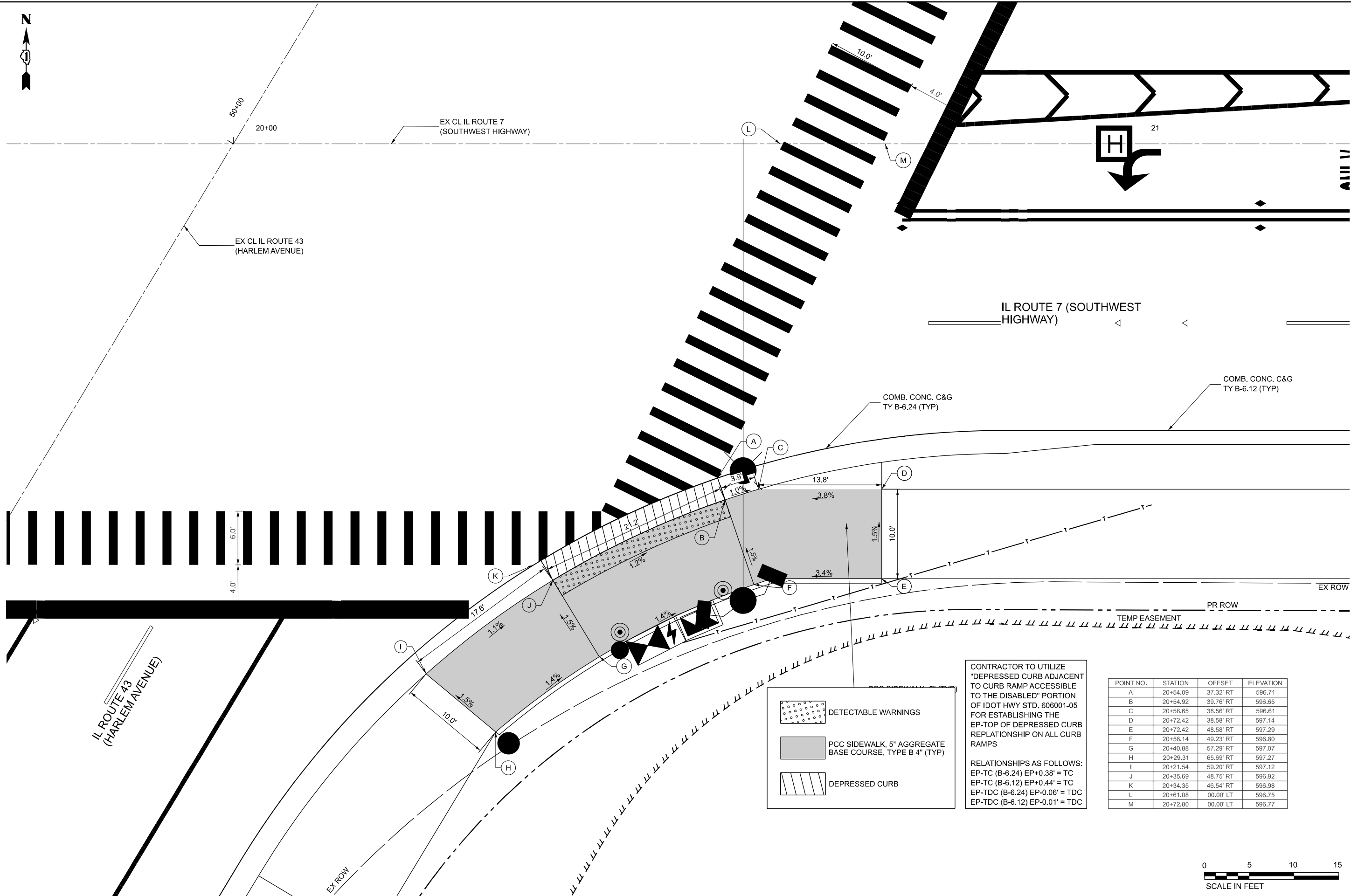
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DRAWN	-	TN
CHECKED	-	GJE
DATE	-	10/15/2025

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=5'	SHEET 4	OF 12 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	126
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



MODEL: 5
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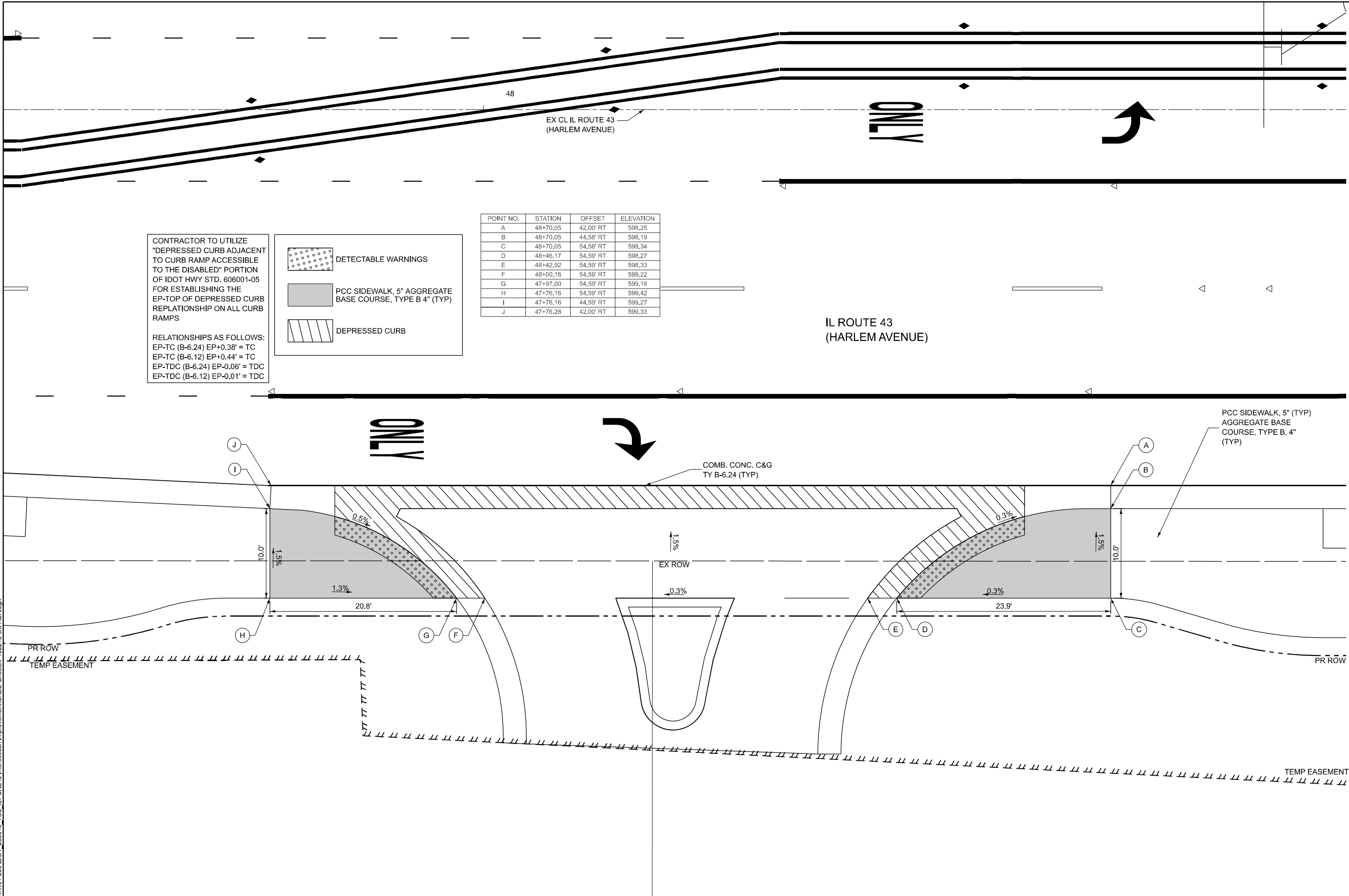
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		DRAWN -	TN	REVISED -	
		CHECKED -	GJE	REVISED -	
PLOT DATE	= 10/17/2025	DATE	= 10/15/2025	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

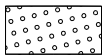
SCALE: 1"=5' SHEET 5 OF 12 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	127
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



CONTRACTOR TO UTILIZE "DEPRESSED CURB ADJACENT TO CURB RAMP ACCESSIBLE TO THE DISABLED" PORTION OF IDOT HWY STD. 606001-05 FOR ESTABLISHING THE EP-TOP OF DEPRESSED CURB REPLATIONSHIP ON ALL CURB RAMPS

RELATIONSHIPS AS FOLLOWS:
EP-TC (B-6.24) EP+0.38' = TC
EP-TC (B-6.12) EP+0.44' = TC
EP-TDC (B-6.24) EP-0.06' = TDC
EP-TDC (B-6.12) EP-0.01' = TDC



DETECTABLE WARNINGS

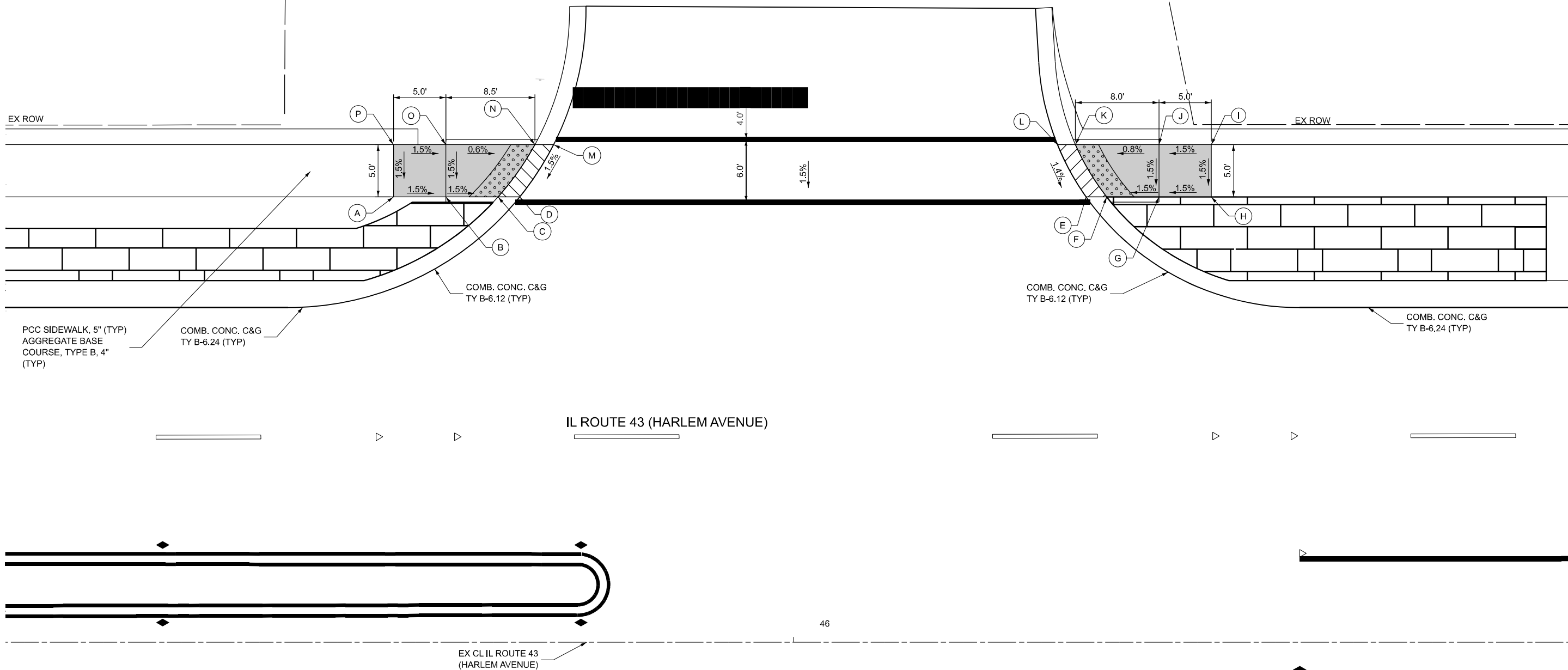


PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B 4" (TYP)



DEPRESSED CURB

POINT NO.	STATION	OFFSET	ELEVATION
A	45+61.73	42.58' LT	601.12
B	45+66.38	42.58'LT	601.05
C	45+71.73	42.58'LT	600.98
D	45+73.80	42.58'LT	600.99
E	46+27.97	42.58'LT	601.01
F	46+29.93	42.58'LT	601.00
G	46+34.93	42.58'LT	601.07
H	46+39.93	42.58'LT	601.14
I	46+39.93	47.58' LT	601.21
J	46+34.93	47.58' LT	601.14
K	46+26.95	47.58' LT	601.08
L	46+25.23	47.58' LT	601.09
M	45+77.04	47.58' LT	601.08
N	45+75.26	47.58' LT	601.07
O	45+66.73	47.58' LT	601.12
P	45+61.73	47.58' LT	601.19



MODEL: 6
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USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL, ROUTE 7 (SOUTHWEST HIGHWAY) AT IL, ROUTE 43 (HARLEM AVENUE)

SCALE: 1"=5' SHEET 6 OF 12 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	128
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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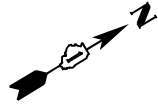


USER NAME	= tnestor	DESIGNED -	TN	REVISED -	
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		CHECKED -	GJE	REVISED -	
PLOT DATE	= 10/17/2025	DATE	= 10/15/2025	REVISED -	

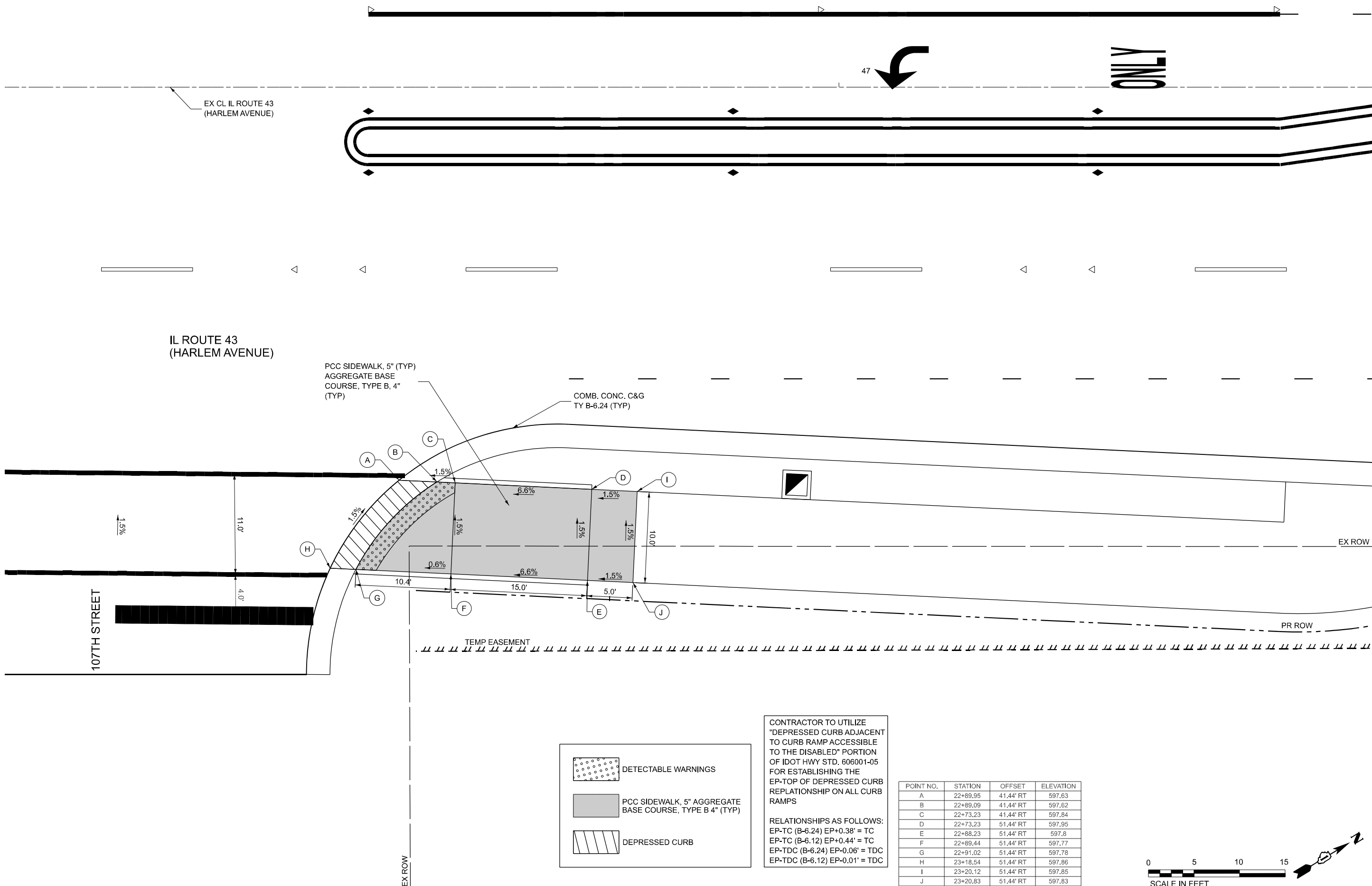
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS			
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)			
SCALE: 1"=5'	SHEET 7	OF 12 SHEETS	STA. TO STA.

POINT NO.	STATION	OFFSET	ELEVATION
A	22+89.95	41.44' RT	597.63
B	22+89.09	41.44' RT	597.62
C	22+73.23	41.44' RT	597.84
D	22+73.23	51.44' RT	597.95
E	22+88.23	51.44' RT	597.8
F	22+89.44	51.44' RT	597.77
G	22+91.02	51.44' RT	597.78
H	23+18.54	51.44' RT	597.86
I	23+20.12	51.44' RT	597.85
J	23+20.83	51.44' RT	597.83

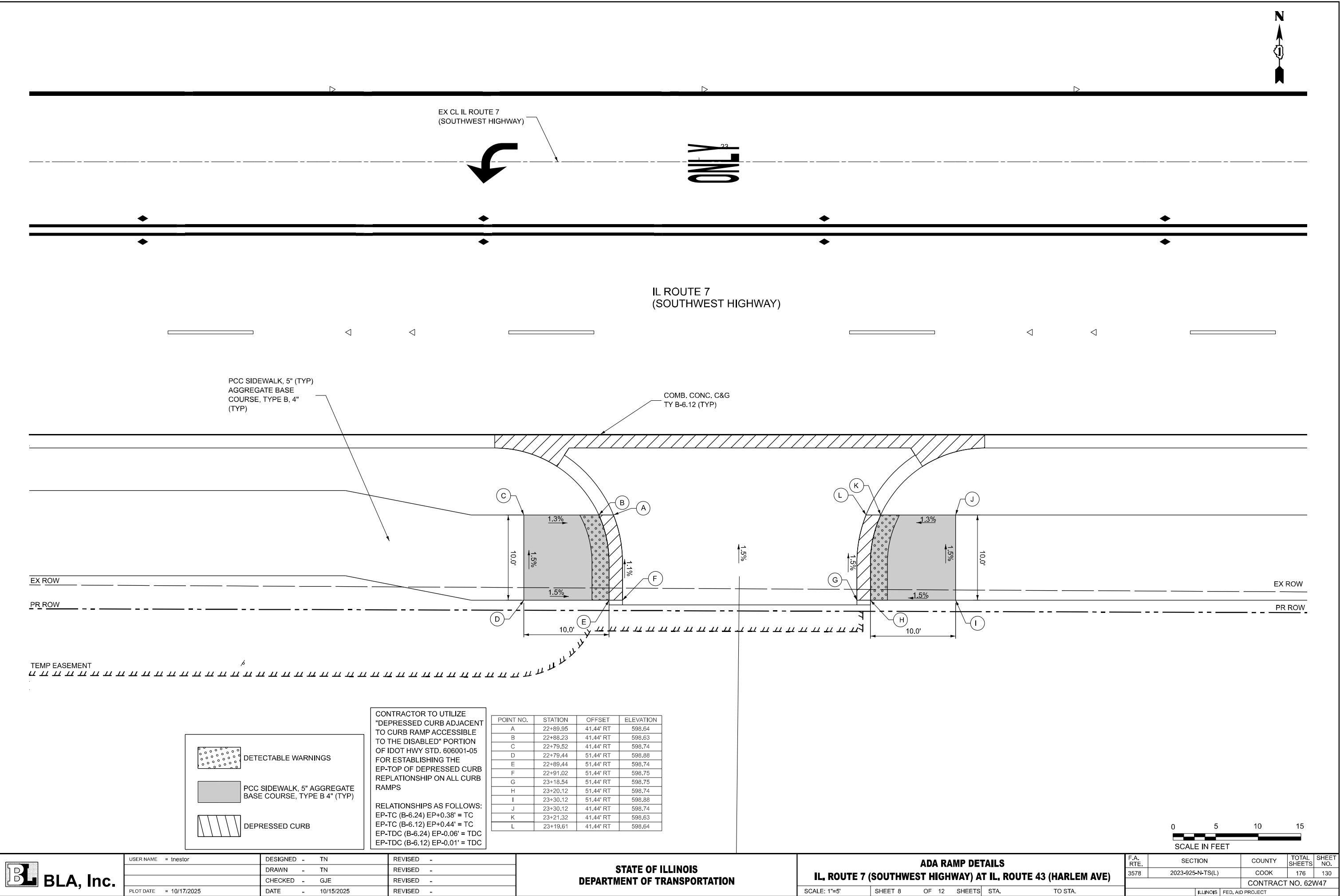


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	129
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

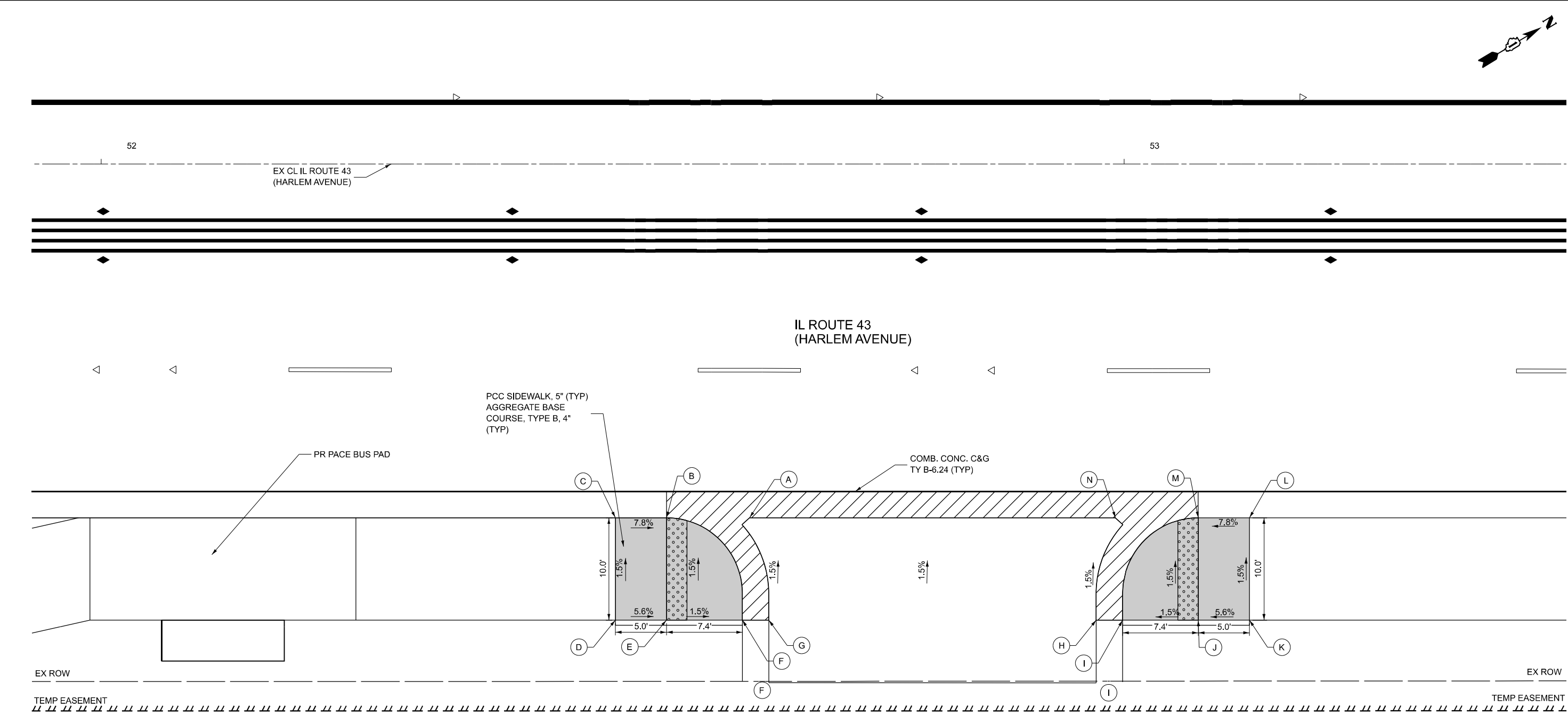



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MODEL: 9
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-sh4-4DA.dgn






DETECTABLE WARNINGS



PCC SIDEWALK, 5" AGGREGATE
BASE COURSE, TYPE B 4" (TYP)



DEPRESSED CURB

CONTRACTOR TO UTILIZE
"DEPRESSED CURB ADJACENT
TO CURB RAMP ACCESSIBLE
TO THE DISABLED" PORTION
OF IDOT HWY STD. 606001-05
FOR ESTABLISHING THE
EP-TOP OF DEPRESSED CURB
REPLATIONSHIP ON ALL CURB
RAMPS

RELATIONSHIPS AS FOLLOWS:
EP-TC (B-6.24) EP+0.38' = TC
EP-TC (B-6.12) EP+0.44' = TC
EP-TDC (B-6.24) EP-0.06' = TDC
EP-TDC (B-6.12) EP-0.01' = TDC

POINT NO.	STATION	OFFSET	ELEVATION
A	52+64.84	39.08' RT	594.50
B	52+62.09	39.08' RT	594.44
C	52+50.46	39.08' RT	594.85
D	52+50.49	49.08' RT	595.00
E	52+62.09	49.08' RT	594.59
F	52+62.69	49.08' RT	594.58
G	52+65.27	49.08' RT	594.64
H	52+97.27	49.08' RT	594.64
I	52+99.86	49.08' RT	594.58
J	53+00.46	49.08' RT	594.59
K	53+12.45	49.08' RT	595.00
L	53+12.45	39.08' RT	594.85
M	53+00.45	39.08' RT	594.44
N	52+97.71	39.08' RT	594.50





USER NAME	= tnestor	DESIGNED	- TN	REVISED	-
		DRAWN	- TN	REVISED	-
		CHECKED	- GJE	REVISED	-
PLOT DATE	= 10/17/2025	DATE	- 10/15/2025	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	131
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

DETECTABLE WARNINGS

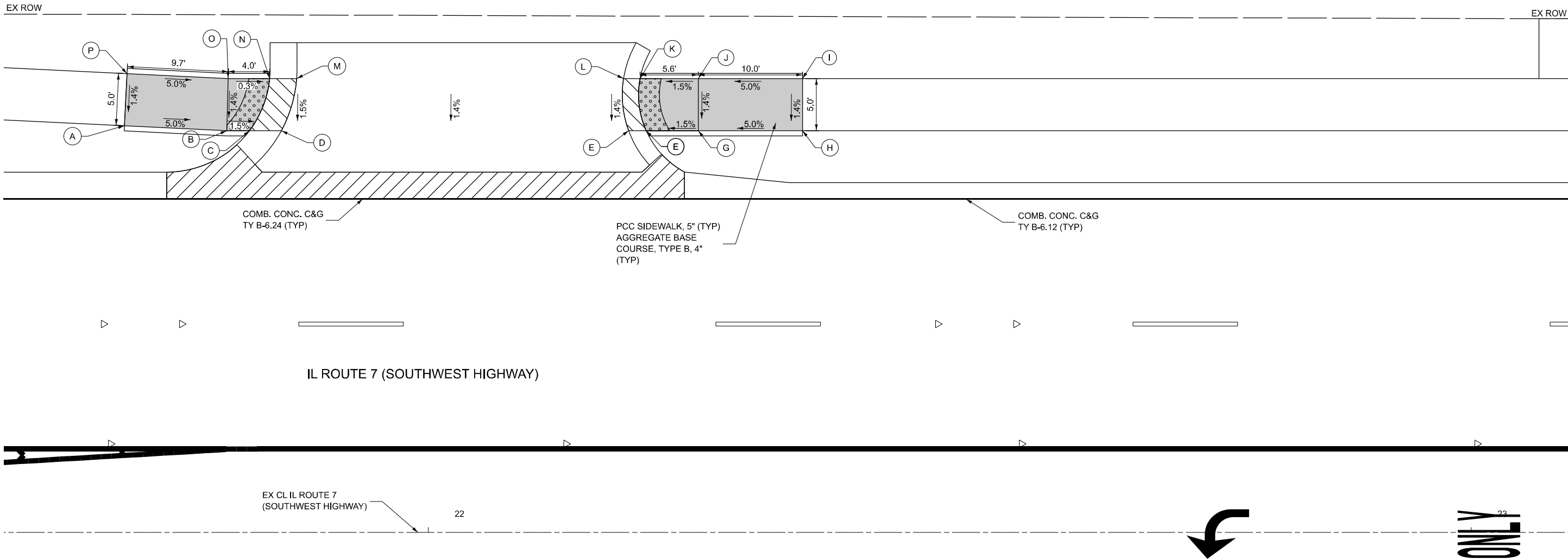
PCC SIDEWALK, 5" AGGREGATE
BASE COURSE, TYPE B 4" (TYP)

DEPRESSED CURB

CONTRACTOR TO UTILIZE
"DEPRESSED CURB ADJACENT
TO CURB RAMP ACCESSIBLE
TO THE DISABLED" PORTION
OF IDOT HWY STD. 606001-05
FOR ESTABLISHING THE
EP-TOP OF DEPRESSED CURB
REPLATIONSHIP ON ALL CURB
RAMPS

RELATIONSHIPS AS FOLLOWS:
EP-TC (B-6.24) EP+0.38' = TC
EP-TC (B-6.12) EP+0.44' = TC
EP-TDC (B-6.24) EP-0.06' = TDC
EP-TDC (B-6.12) EP-0.01' = TDC

POINT NO.	STATION	OFFSET	ELEVATION
A	21+70.86	39.03' LT	596.95
B	21+80.67	38.56' LT	596.45
C	21+82.85	38.56' LT	596.42
D	21+85.90	38.56' LT	596.48
E	22+19.20	38.56' LT	596.44
F	22+20.88	38.56' LT	596.43
G	22+25.88	38.56' LT	596.50
H	22+35.90	38.56' LT	596.75
I	22+35.48	43.56' LT	596.82
J	22+25.88	43.56' LT	596.57
K	22+20.30	43.56' LT	596.50
L	22+18.71	43.56' LT	596.51
M	21+87.37	43.56' LT	596.56
N	21+84.78	43.56' LT	596.50
O	21+80.79	43.56' LT	596.52
P	21+71.10	44.03' LT	967.02



MODEL: 10
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BLA, Inc.

USER NAME = tnestor

DESIGNED - TN

DRAWN - TN

CHECKED - GJE

PLOT DATE = 10/17/2025

REVISED -

REVISED -

REVISED -

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL, ROUTE 7 (SOUTHWEST HIGHWAY) AT IL, ROUTE 43 (HARLEM AVENUE)

SCALE: 1"=5'

SHEET 10 OF 12 SHEETS


STA. TO STA.


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	132
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				


POINT NO.	STATION	OFFSET	ELEVATION
A	23+12.33	38.56' LT	597.65
B	23+20.00	38.56' LT	597.22
C	23+25.00	38.56' LT	597.15
D	23+26.71	38.56' LT	597.16
E	23+61.30	38.56' LT	597.16
F	23+62.99	38.56' LT	597.15
G	23+68.01	38.56' LT	597.17
H	23+72.60	38.56' LT	597.19
I	23+77.83	38.56' LT	597.23
J	23+79.67	38.56' LT	597.24
K	24+11.79	38.56' LT	597.46
L	24+13.76	38.56' LT	597.45
M	24+20.14	38.56' LT	597.54
N	24+35.14	42.63' LT	598.21
O	24+35.14	47.63' LT	598.29
P	24+20.14	43.56' LT	597.61
Q	24+11.22	43.56' LT	597.53
R	24+09.59	43.56' LT	597.54
S	23+82.62	43.56' LT	597.32
T	23+80.78	43.56' LT	597.31
U	23+72.83	43.56' LT	597.27
V	23+68.01	43.56' LT	597.25
W	23+62.27	43.56' LT	597.22
X	23+60.69	43.56' LT	597.23
Y	23+27.32	43.56' LT	597.23
Z	23+25.74	43.56' LT	597.22
AA	23+20.00	43.56' LT	597.29
BB	23+12.33	43.56' LT	597.72

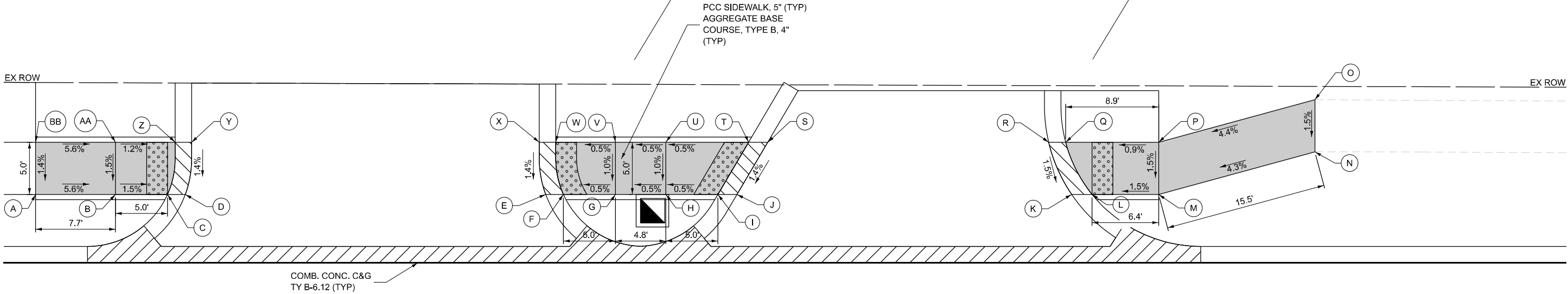
CONTRACTOR TO UTILIZE
"DEPRESSED CURB ADJACENT
TO CURB RAMP ACCESSIBLE
TO THE DISABLED" PORTION
OF IDOT HWY STD. 606001-05
FOR ESTABLISHING THE
EP-TOP OF DEPRESSED CURB
REPLATIONSHIP ON ALL CURB
RAMPS

RELATIONSHIPS AS FOLLOWS:
EP-TC (B-6.24) EP+0.38' = TC
EP-TC (B-6.12) EP+0.44' = TC
EP-TDC (B-6.24) EP-0.06' = TDC
EP-TDC (B-6.12) EP-0.01' = TDC

 DETECTABLE WARNINGS

 PCC SIDEWALK, 5" AGGREGATE
BASE COURSE, TYPE B 4" (TYP)

 DEPRESSED CURB



IL ROUTE 7 (SOUTHWEST HIGHWAY)

EX IL ROUTE 7
(SOUTHWEST HIGHWAY)



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USER NAME = gellwanger	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS
IL, ROUTE 7 (SOUTHWEST HIGHWAY) AT IL, ROUTE 43 (HARLEM AVENUE)

SCALE: 1"=5' SHEET 11 OF 12 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	133
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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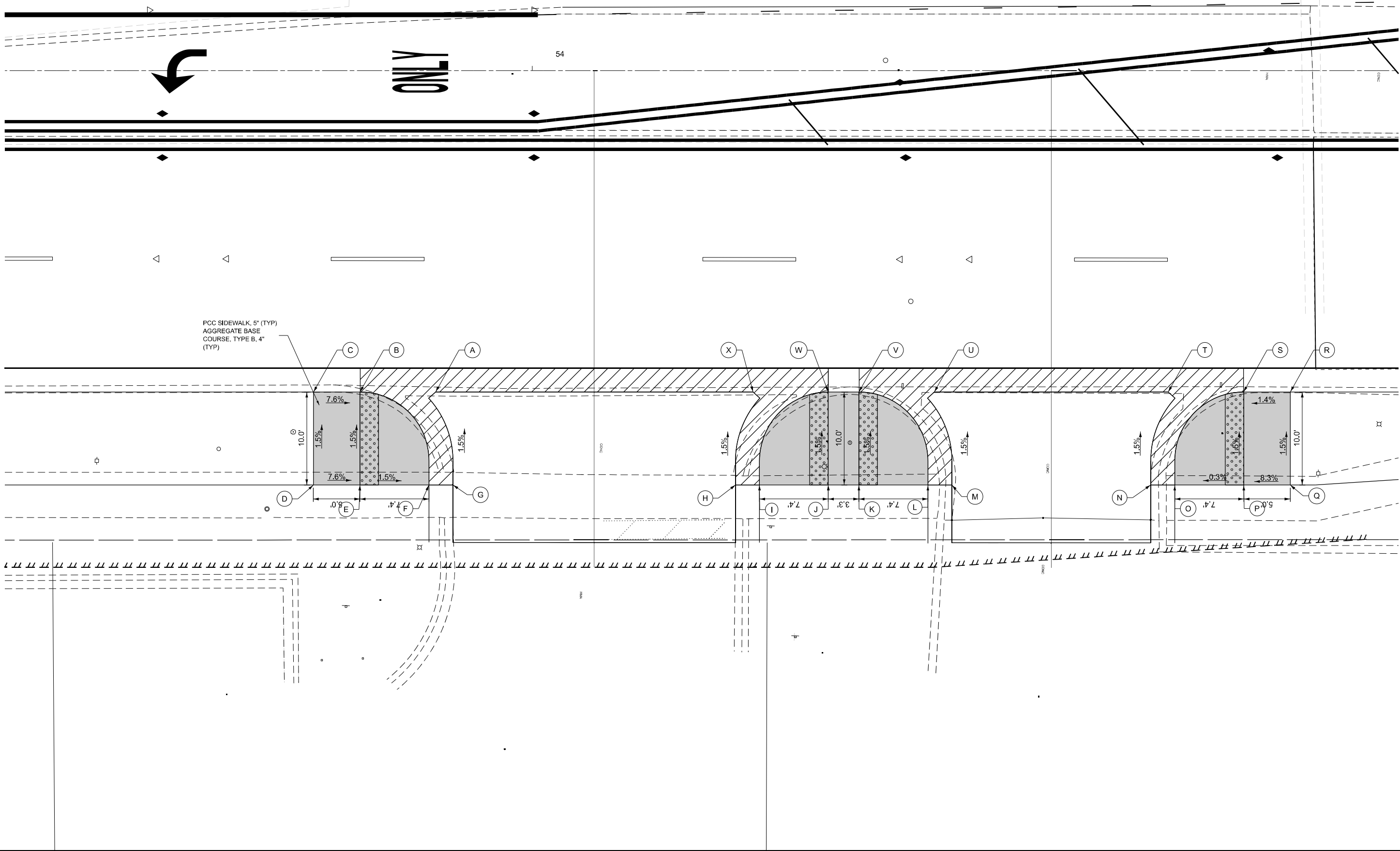
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	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

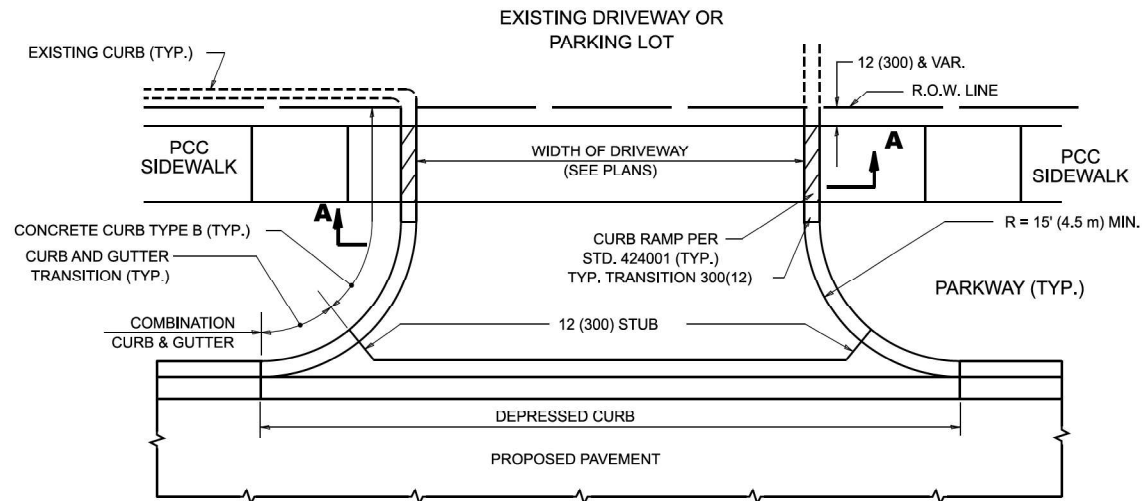
ADA RAMP DETAILS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: 1"=5' SHEET 12 OF 12 SHEETS STA. TO STA.

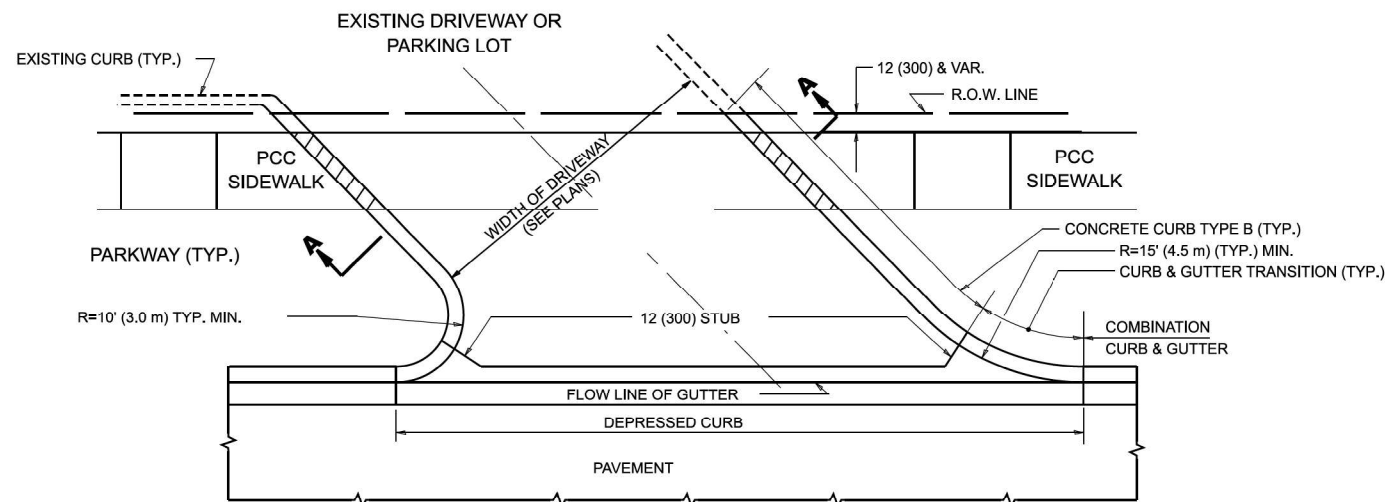
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	134
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				



MODEL: Pln Single (Sheet)
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WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B

RIGID DRIVEWAY

COMMERCIAL ENTRANCE (CE):
PCC DRIVEWAY PAVEMENT 8 (200)
MEASURED IN SQ. YD. (m²)

PRIVATE ENTRANCE (PE):
PCC DRIVEWAY PAVEMENT 6 (150)
MEASURED IN SQ. YD. (m²)

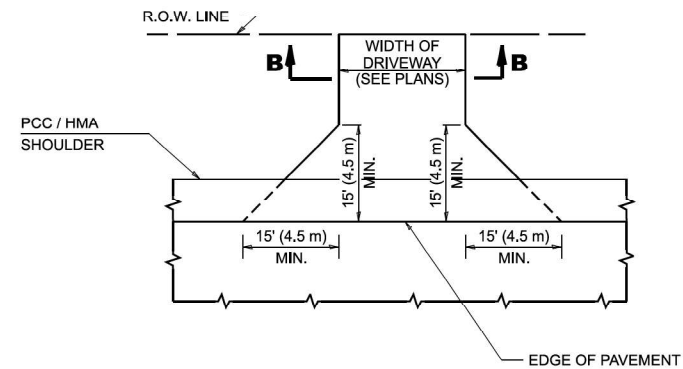
SECTION A-A

HMA DRIVEWAY

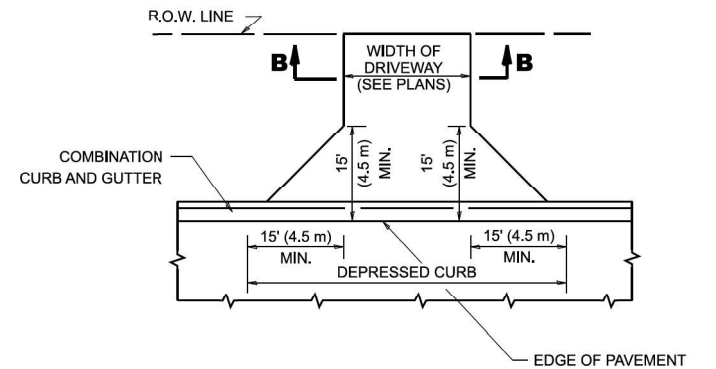
HMA SURFACE COURSE,
MIX "D", IL-9.5, N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

COMMERCIAL ENTRANCE (CE):
HMA BASE COURSE, 8 (200)
MEASURED IN SQ. YD. (m²).

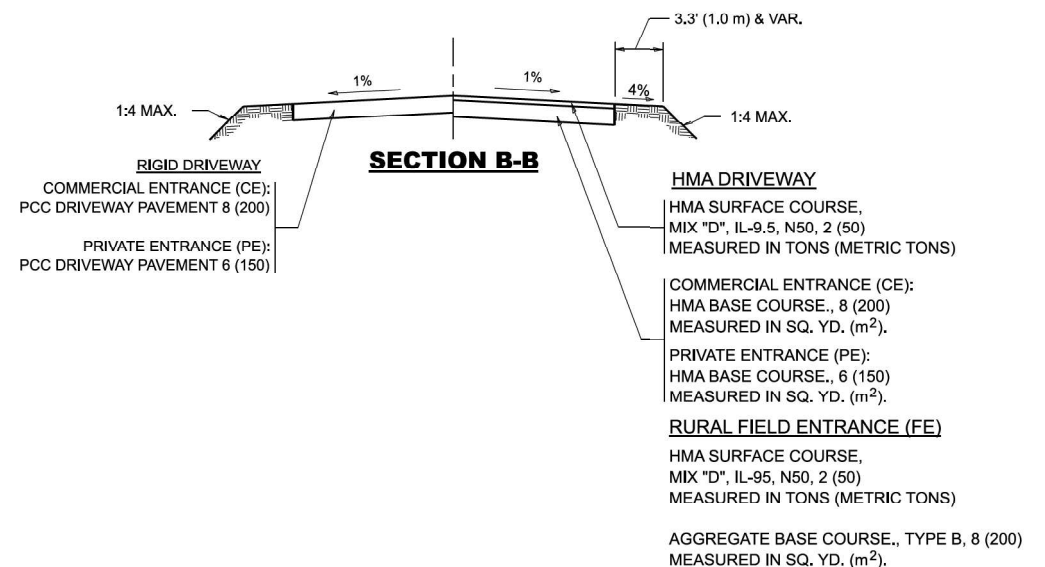
PRIVATE ENTRANCE (PE):
HMA BASE COURSE, 6 (150)
MEASURED IN SQ. YD. (m²).



ADJACENT TO PCC / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

GENERAL NOTES

- DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.
- COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE NOTED.

USER NAME = eric.l.thomas	DESIGNED - R. SHAH	REVISED - R. BORO 06-11-08
	DRAWN -	REVISED - R. BORO 09-06-11
PLOT SCALE = 0.08333317' / in.	CHECKED -	REVISED - K. SMITH 08-28-19
PLOT DATE = 8/30/2023	DATE - 11-04-95	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >15'(4.5m)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	135
BD400-01 (BD-01)		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		



1. DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.
2. WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE PCC SIDEWALK SHALL EXTEND TO THE BACK OF CURB.
3. "W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L). FROM 6' (1.8 m) TO 10' (3 m).

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.
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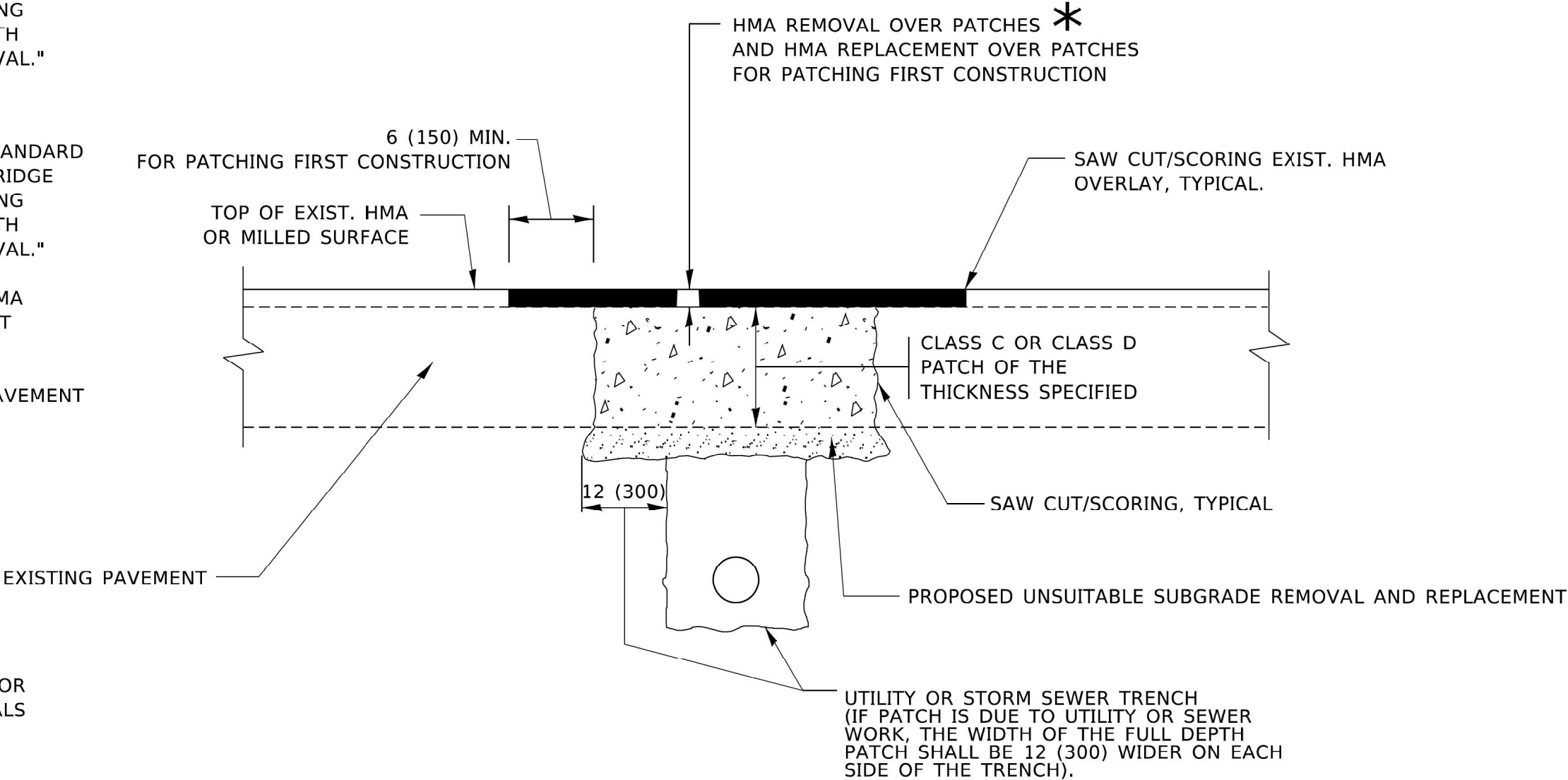
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	136
BD400-02 (BD-02)		CONTRACT NO. 62W47		
ILLINOIS		FED. AID PROJECT		

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

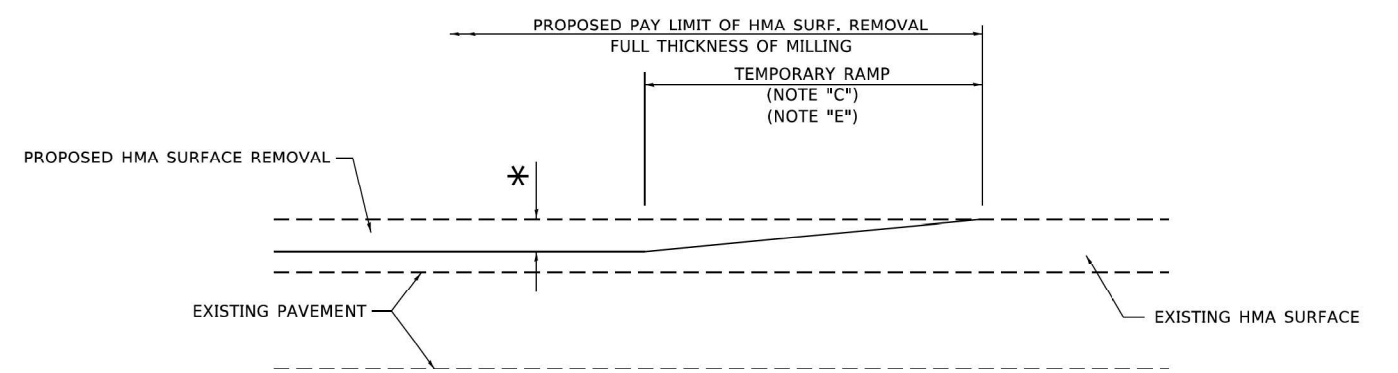
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

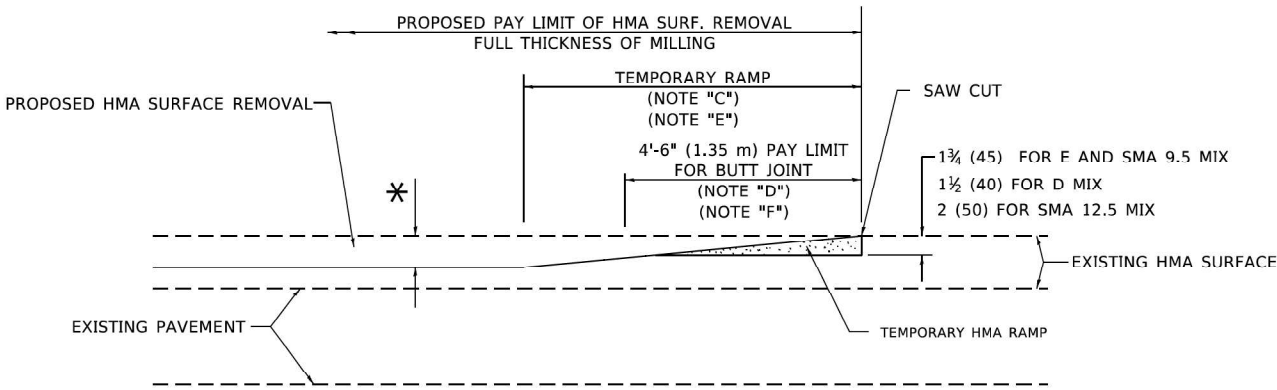
MODEL: Default
FILE: hma_patching.dwg
PLOT: 11/18/2022
PLOT SCALE: 1/8" = 1'-0"

	USER NAME	= Lawrence,DeManche	DESIGNED	-	R. SHAH	REVISED	-	R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT					F.A.U. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN	-		REVISED	-	R. BORO 09-04-07		3578	2023-025-N-TS(L)		COOK	176	137					
	PLOT SCALE	= 100.0000 ' / in.	CHECKED	-		REVISED	-	K. ENG 10-27-08		BD400-04 (BD-22)		CONTRACT NO. 62W47								
	PLOT DATE	= 11/18/2022	DATE	-	10-25-94	REVISED	-	K. SMITH 11-18-22		SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.				
															ILLINOIS		FED. AID PROJECT			



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

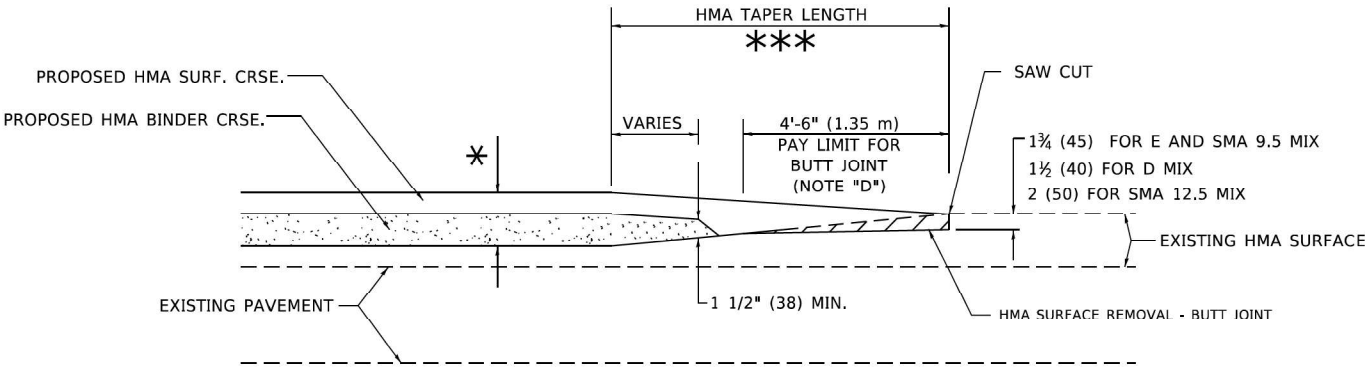
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

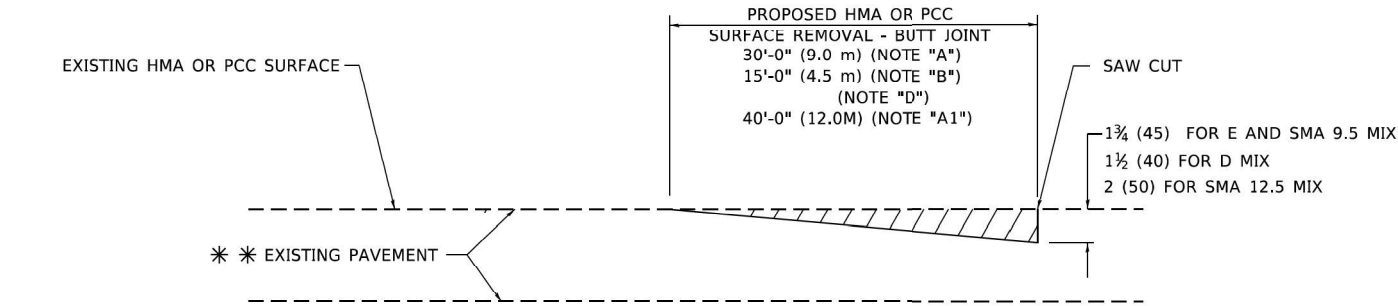
OPTION 2

TYPICAL TEMPORARY RAMP

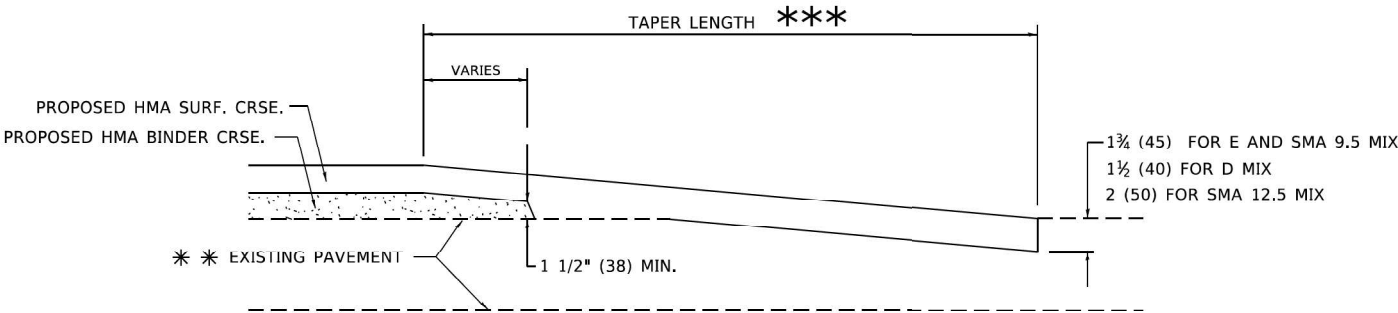


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
******* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE: hma-butt-joint-and-hma-taper-for-milling-and-resurfacing.dgn
PROJECT: 2023-025-N-TS(L)
SHEET: 176
DATE: 11/18/2022

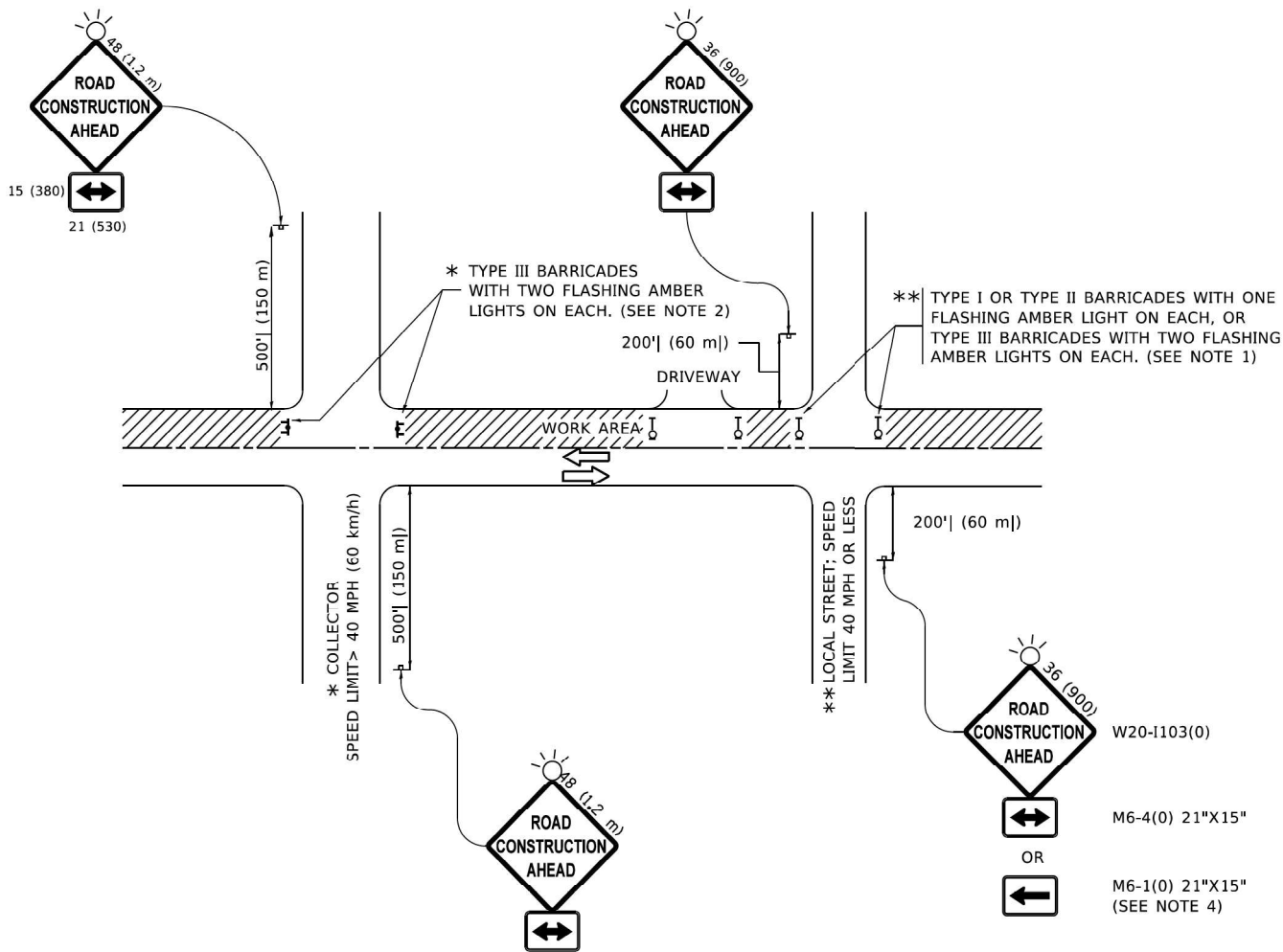
USER NAME = Lawrence,DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	138
BD400-05 BD-32		CONTRACT NO. 62W47		
ILLINOIS		FED. AID PROJECT		



NOTES:

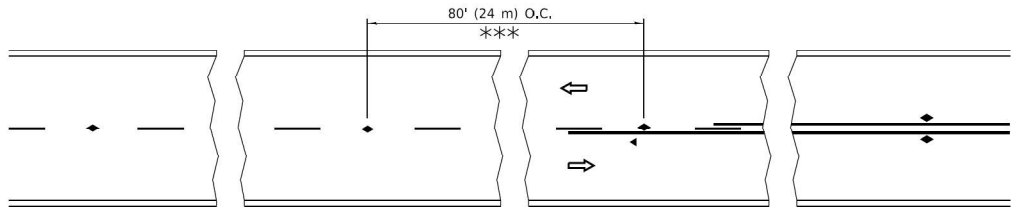
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

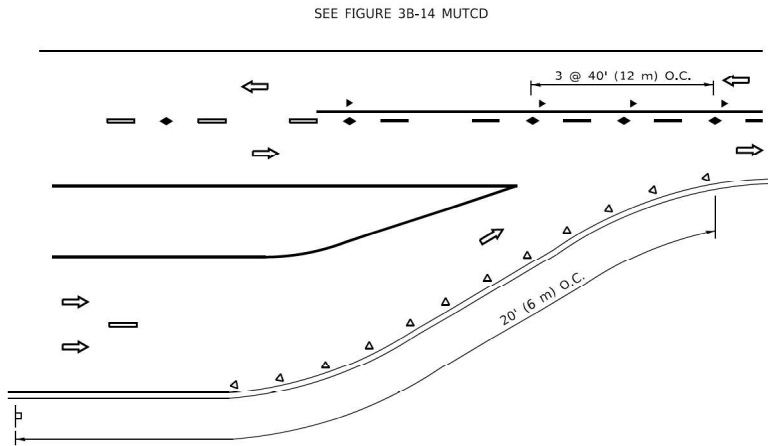
All dimensions are in inches (millimeters)
unless otherwise shown.

FILE NAME DWG	USER NAME = Lawrence.DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. SCHUETZE 07-01-13							3578	2023-925-N-TS(L)	COOK	176	139
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16		TC-10					CONTRACT NO. 62W47				
	PLOT DATE = 5/3/2024	DATE = 06-89	REVISED - D. SENDERAK 05-03-24		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT		

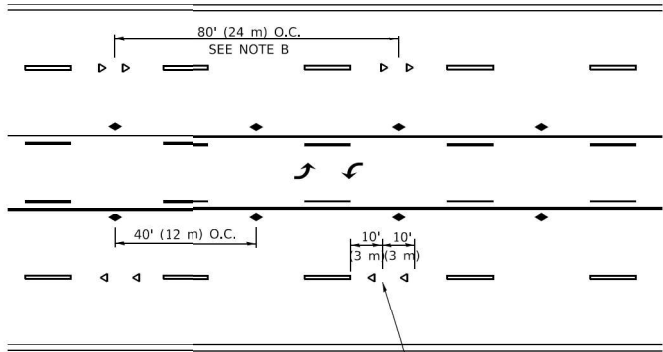


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

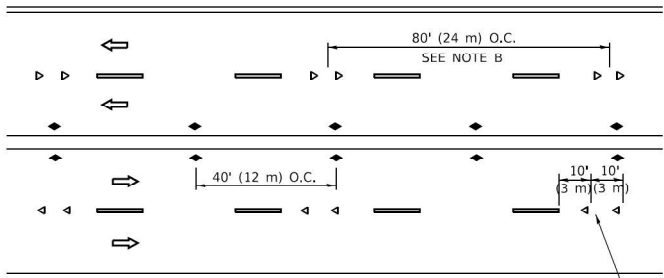
TWO-LANE/TWO-WAY



LANE REDUCTION TRANSITION

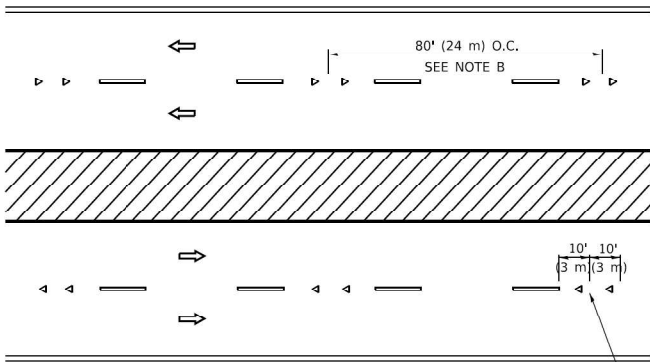


TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

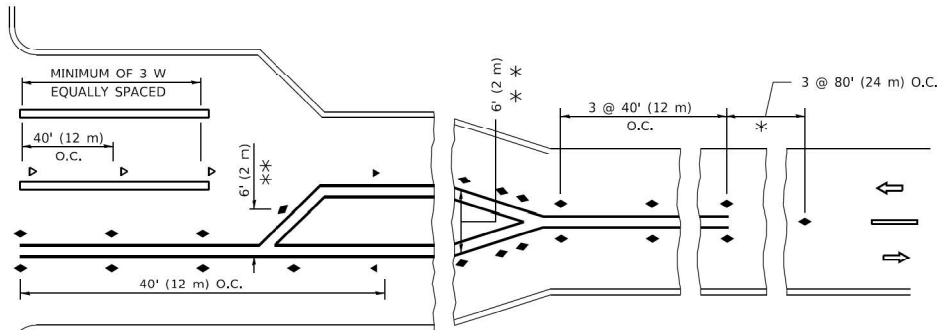
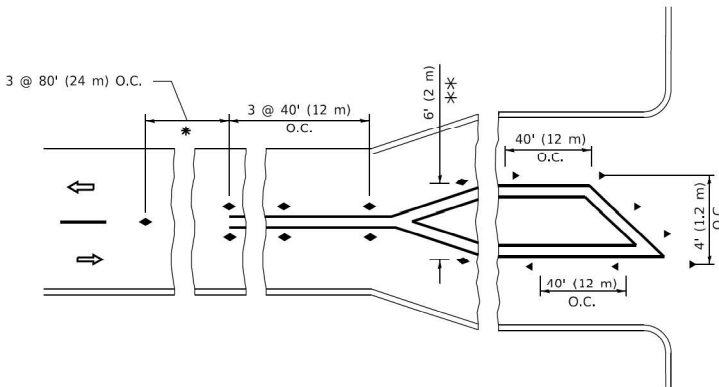
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE NAME: p:\114m 34\2019 10\29\08 AM User\footem

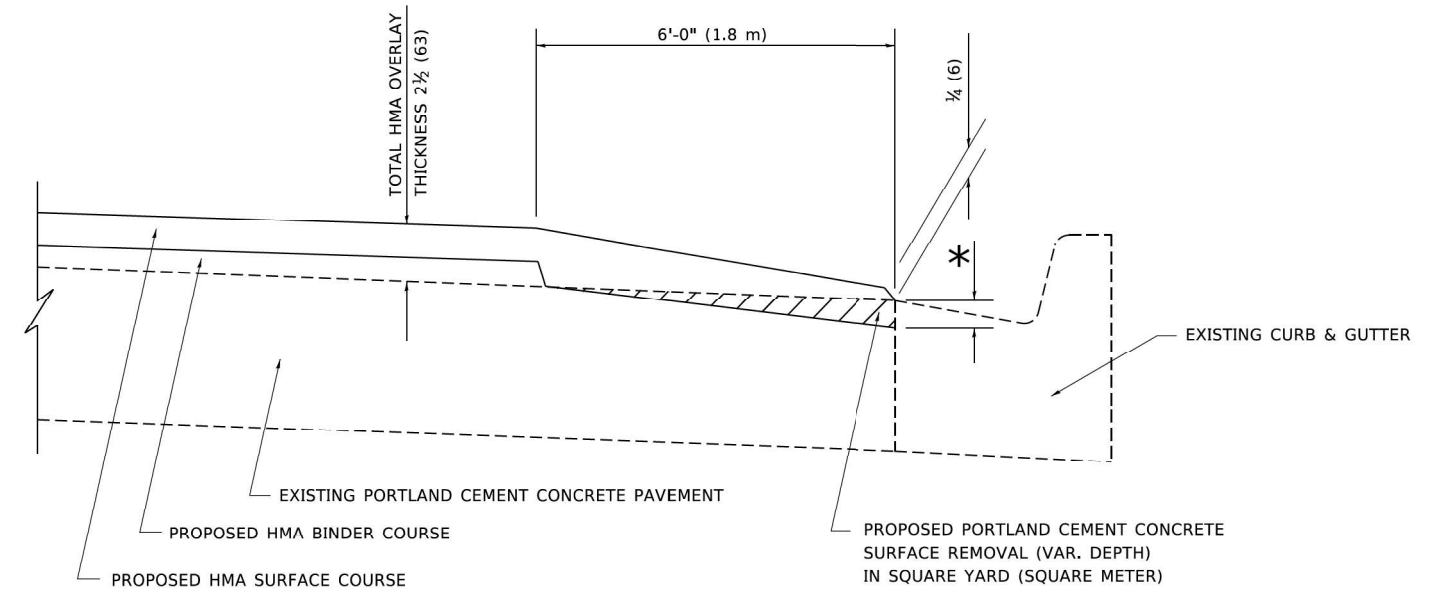
USER NAME = footem]	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	140
TC-11		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	

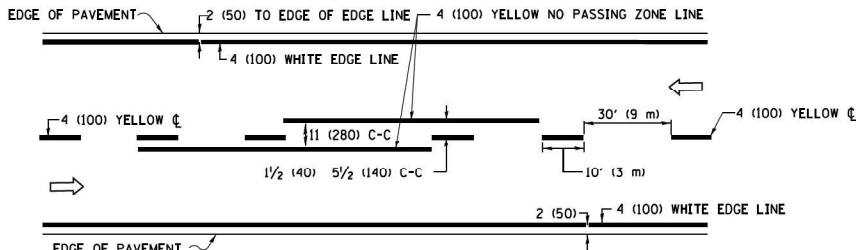


HMA TAPER AT
EDGE OF PCC PAVEMENT

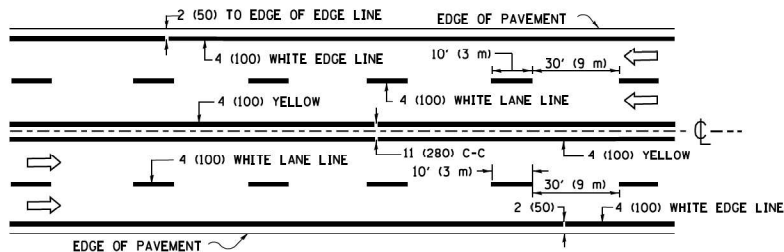
HMA SURFACE COURSE		HMA BINDER COURSE	
MIX	THICKNESS	THICKNESS	* MILLING AT GUTTER FLA
D	1½ (38)	1 (25)	1¼ (33)
E OR SMA 9.5	1¾ (44)	¾ (19)	1½ (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

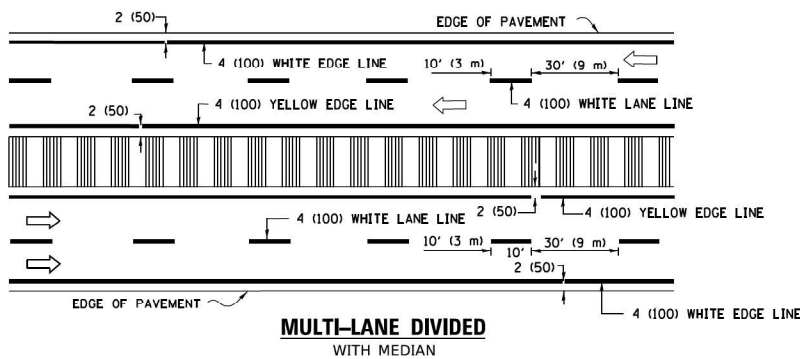
FILE NAME: D:\PROJECTS\2023\2023-925-N-TS(L)\2023-925-N-TS(L).DWG	USER NAME = Shawn.Ley	DESIGNED - R. SHAH DRAWN - JIS	REVISED - E. GOMEZ 12-21-00 REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HMA TAPER AT EDGE OF P.C.C. PAVEMENT					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / in.	CHECKED - A. ABBAS	REVISED - JP CHANG 07-08-16							3578	2023-925-N-TS(L)	COOK	176	140
	PLOT DATE = 1/23/2023	DATE - 09-10-94	REVISED - K. SMITH 11-18-22		BD400-06 BD-33 CONTRACT NO. 62W47									
										SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.



2-LANE ROADWAY

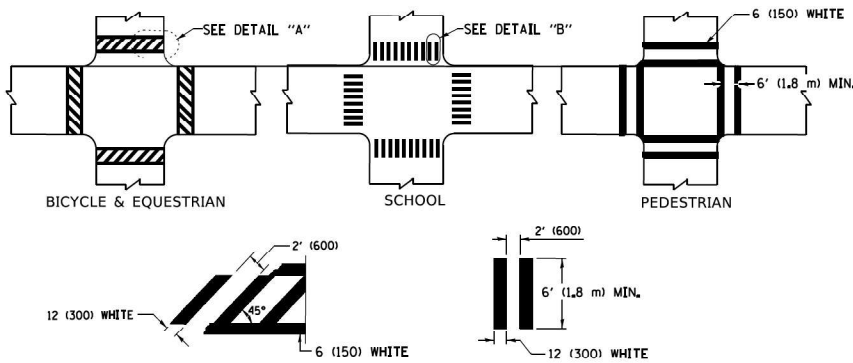


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

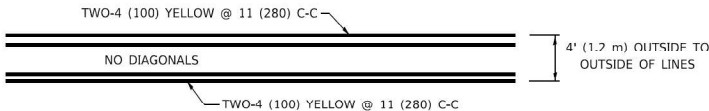


DETAIL "A"

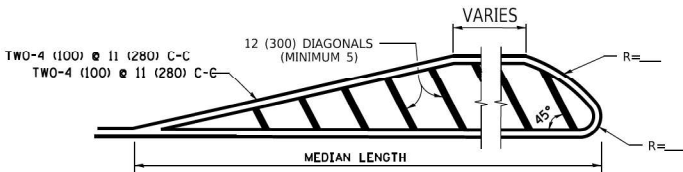
DETAIL "B"

TYPICAL CROSSWALK MARKING

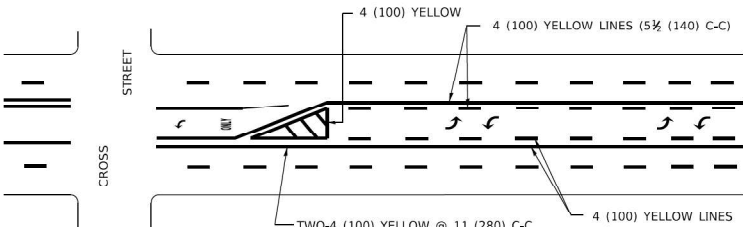
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



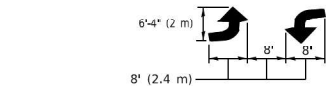
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

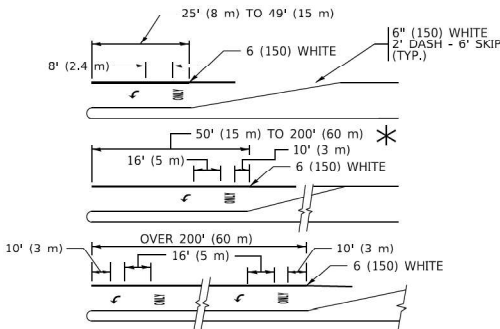


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

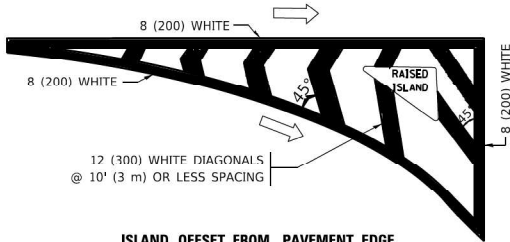


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

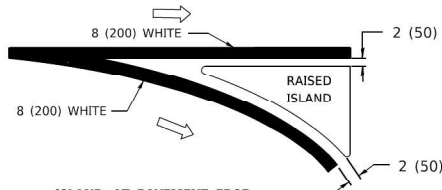
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

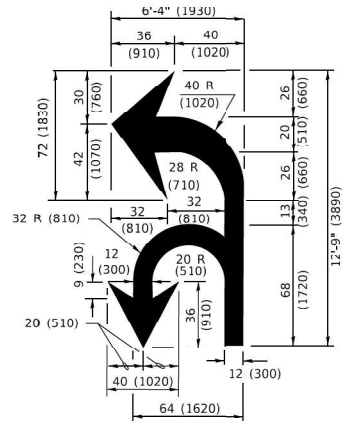


ISLAND OFFSET FROM PAVEMENT EDGE

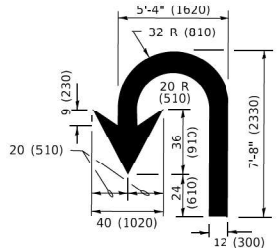


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES; FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; *RR* 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR *X*	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R* = 3.6 SQ. FT. (0.33 m ²) EACH *X* = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-025-A1-TS(L)	COOK	176	142
TC-13		CONTRACT NO. 62W47		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

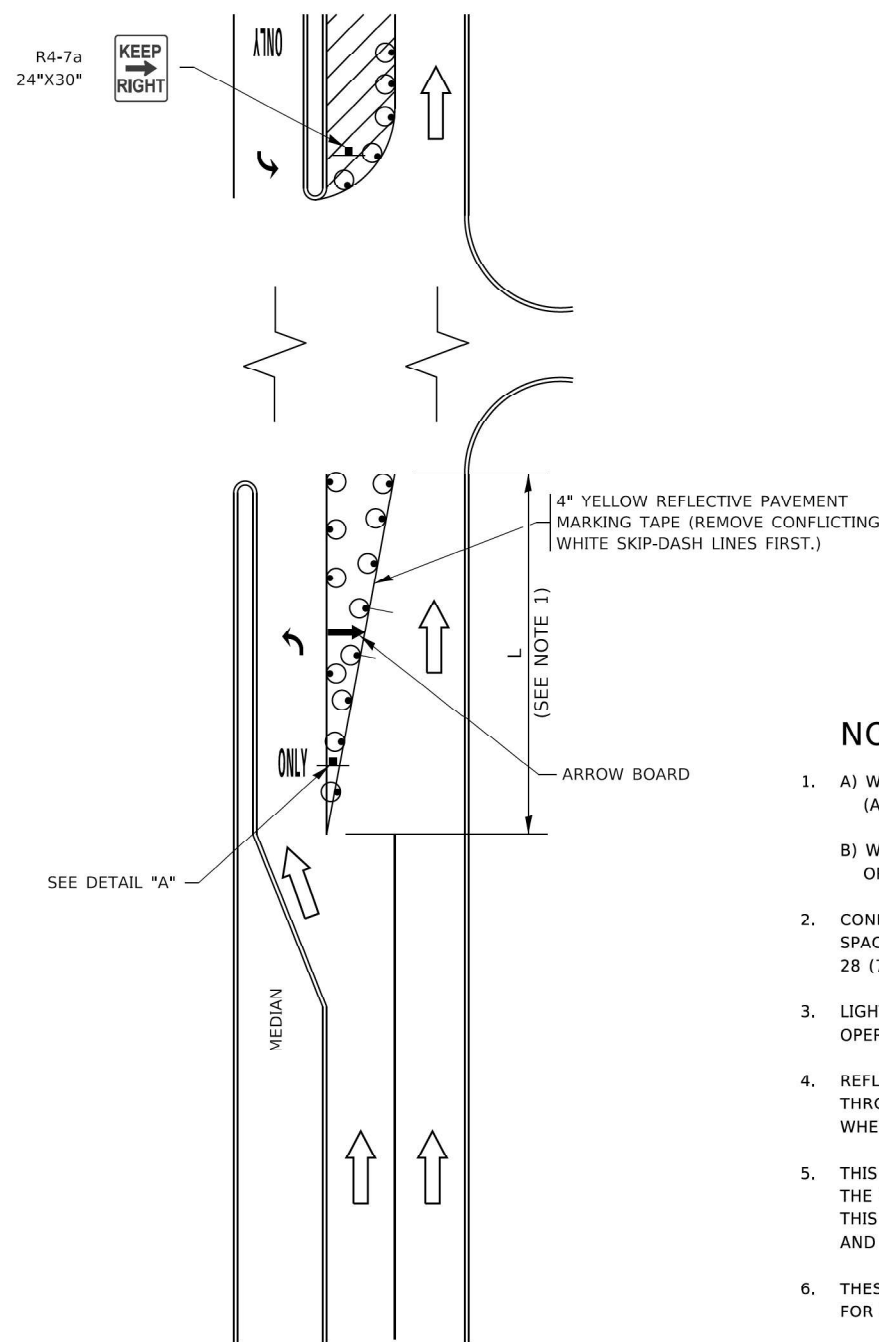


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

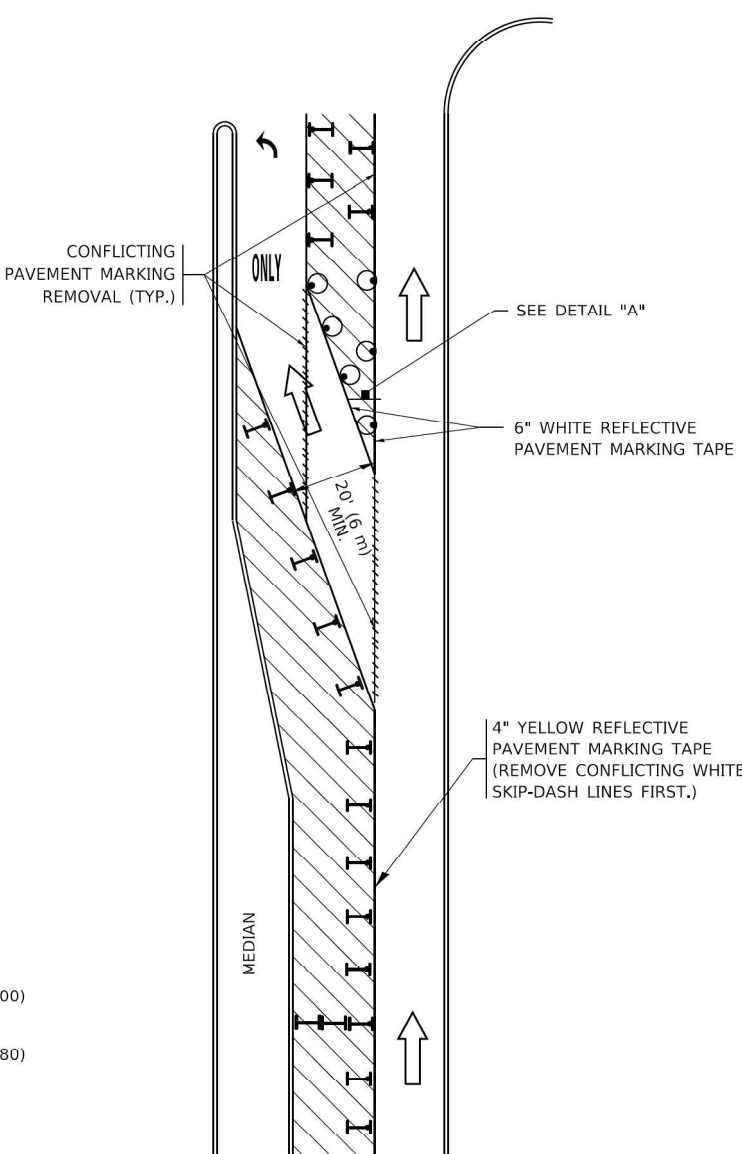
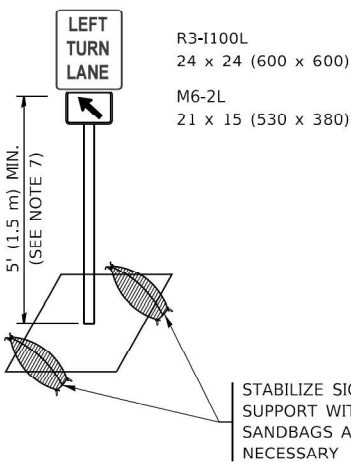


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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DESIGNED	- T. RAMMACHER 09-08-94
DRAWN	- A. HOUSEH 11-07-95
PLOT SCALE	= 50.0000 ' / in.
CHECKED	- A. HOUSEH 10-12-96
PLOT DATE	= 3/4/2019
DATE	- T. RAMMACHER 01-06-00

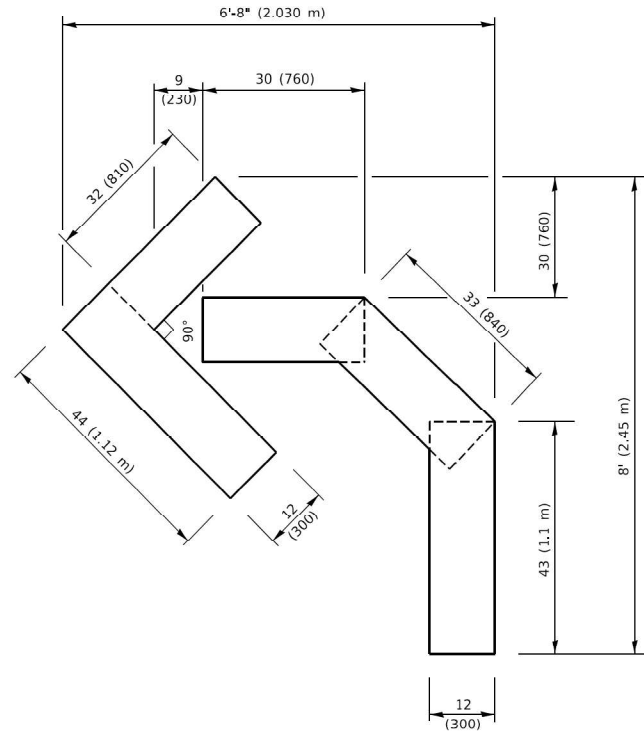
REVISED	- R. BORO 09-14-09
REVISED	- A. SCHUETZE 07-01-13
REVISED	- A. SCHUETZE 09-15-16
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

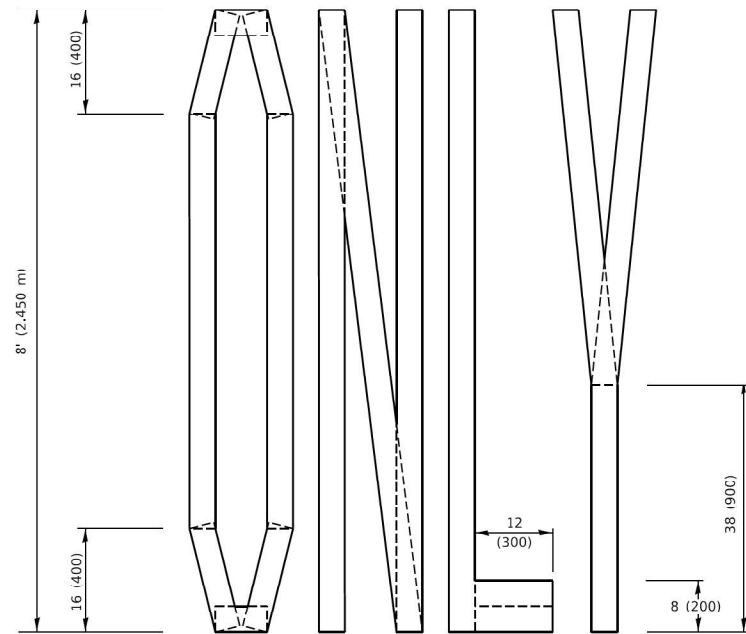
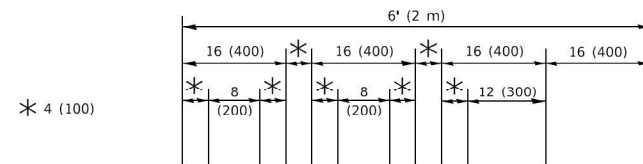
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-14		CONTRACT NO. 62W47		
		ILLINOIS FED. AID PROJECT		



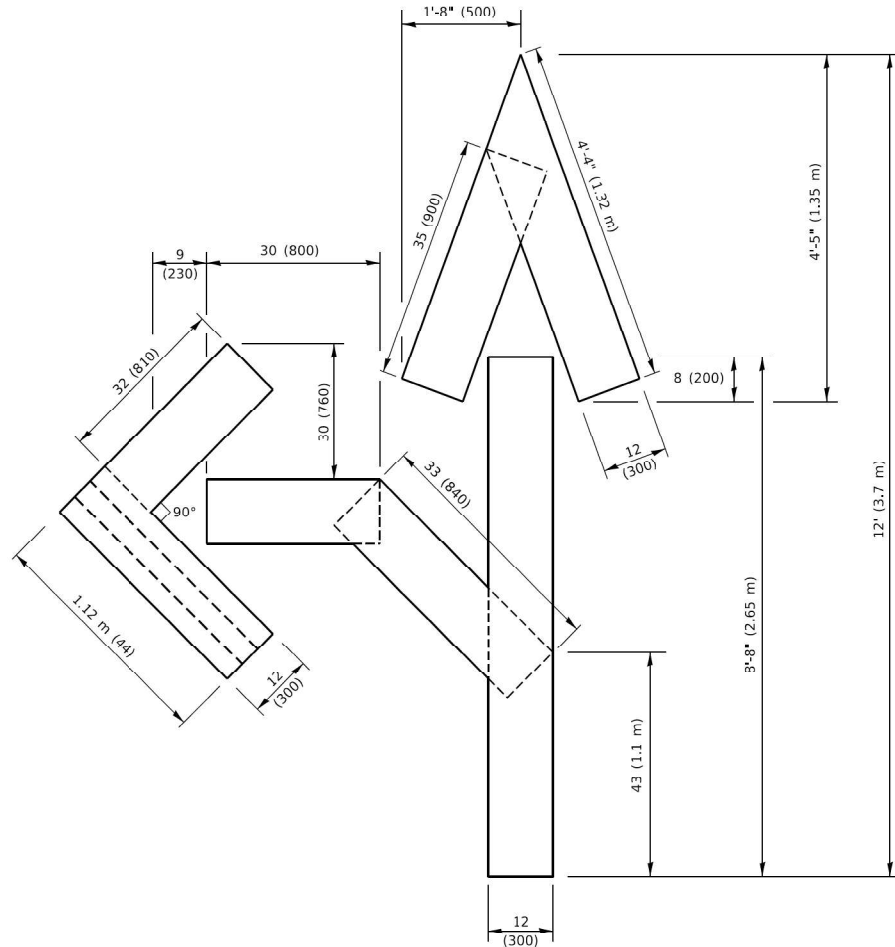
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

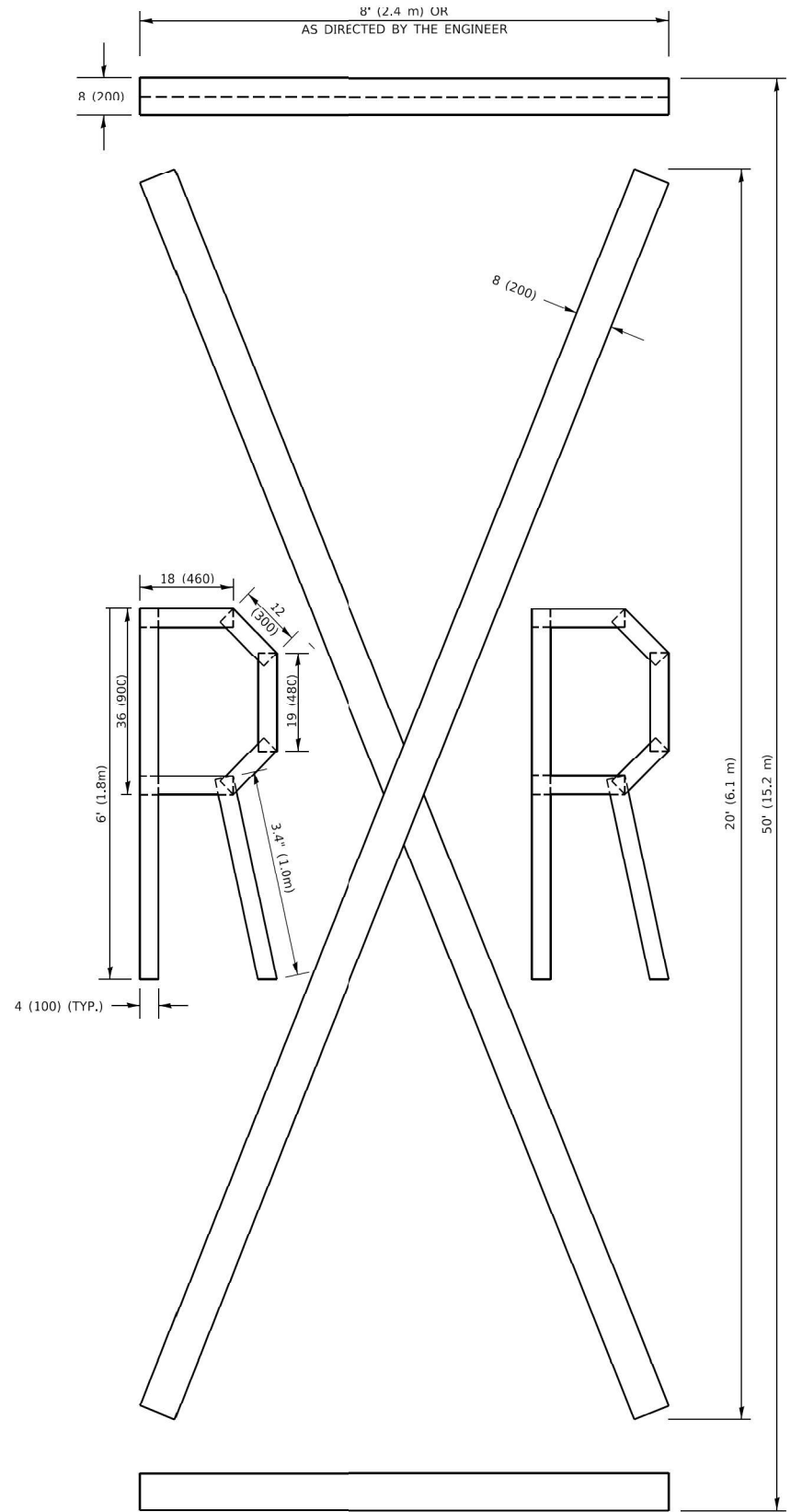


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 50.0068 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS


SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	


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FOR U.S. ROUTES
M1-40-2424




FOR ILLINOIS ROUTES
M1-50-2424




R.R., UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND




M5-1L-2115




M5-1R-2115




M6-1-2115




M6-1-2115




M6-3-2115




M3-1-2412




M3-2-2412



M3-3-2412



M3-4-2412



M4-8-2412

* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

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		DRAWN	-	REVISED	-	R. BORO 09-14-09
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PLOT DATE	= 3/4/2019	DATE	-	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1	SHEETS
STA.	TO STA.		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	144
TC-21		CONTRACT NO. 62W47		
ILLINOIS FED. AID PROJECT				

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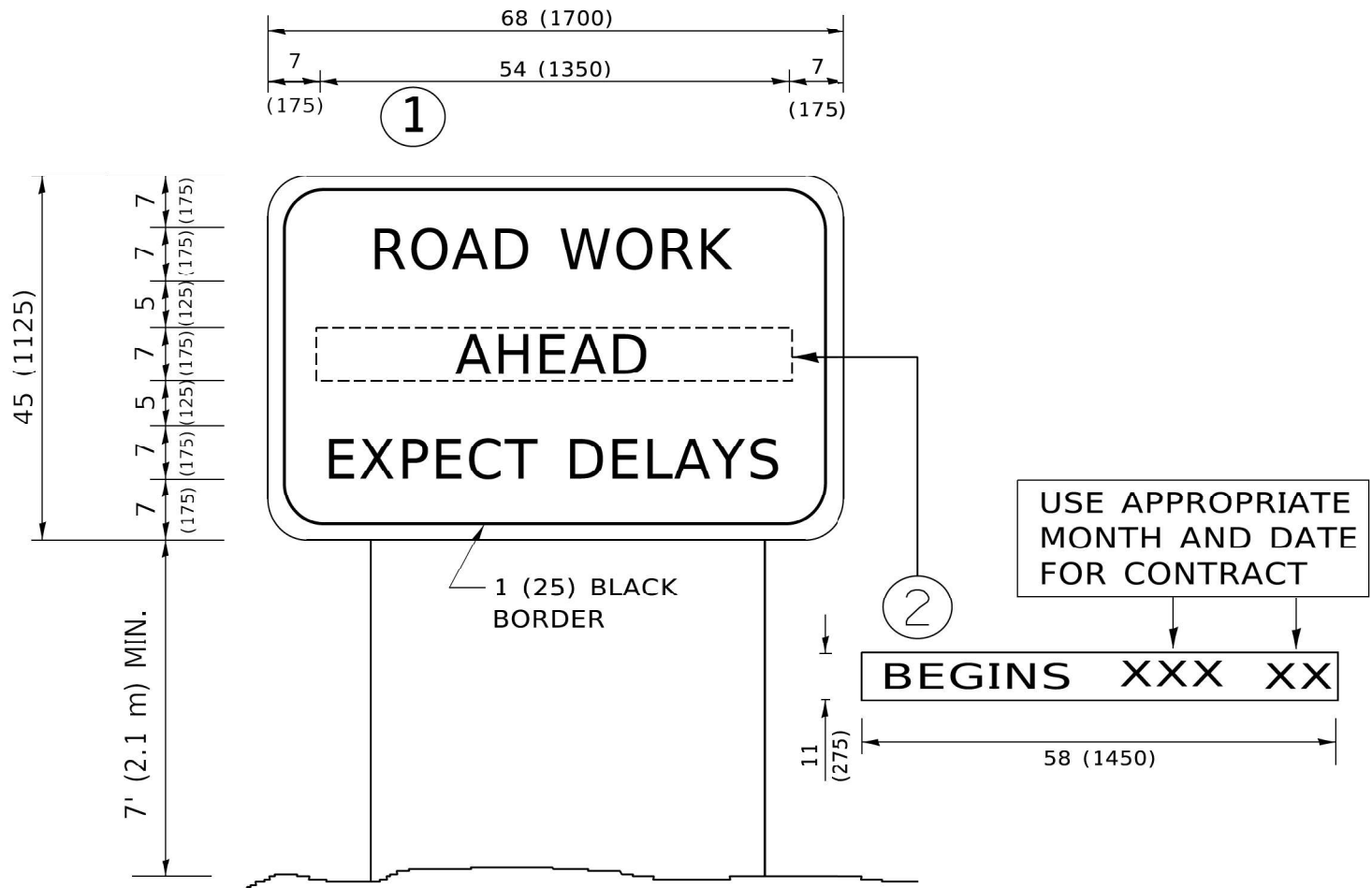
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	PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

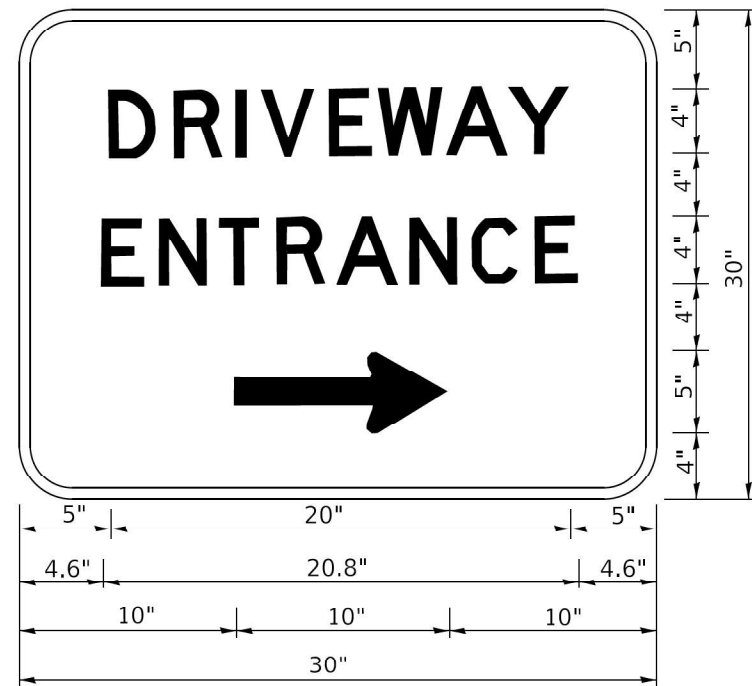
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3578	2023-925-N-TS(L)	COOK	176	145
TC-22		CONTRACT NO. 62W47		
		ILLINOIS	FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.



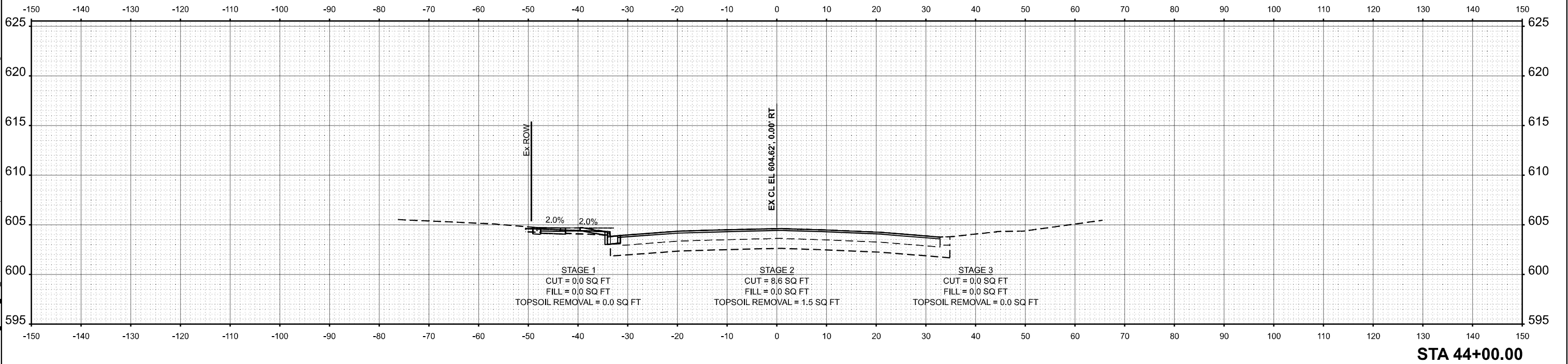
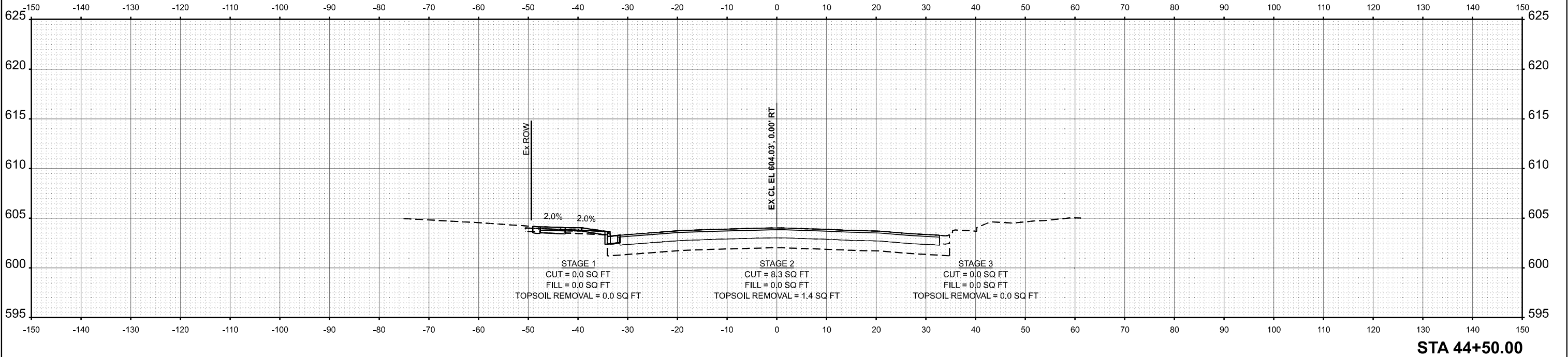
3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME: DMS	USER NAME = lleya	DESIGNED - DRAWN -	REVISED - C. JUCIUS 02-15-07 REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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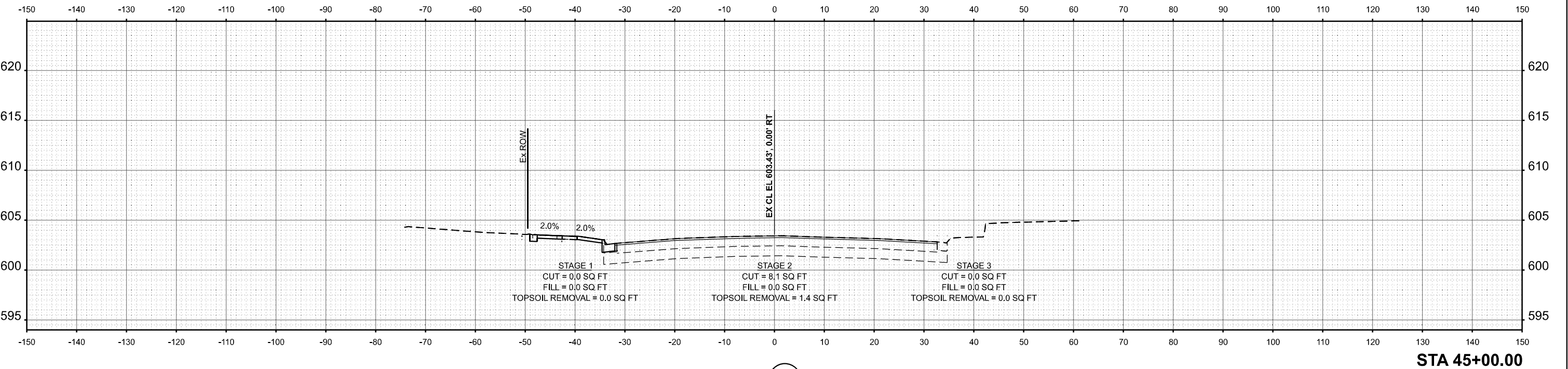
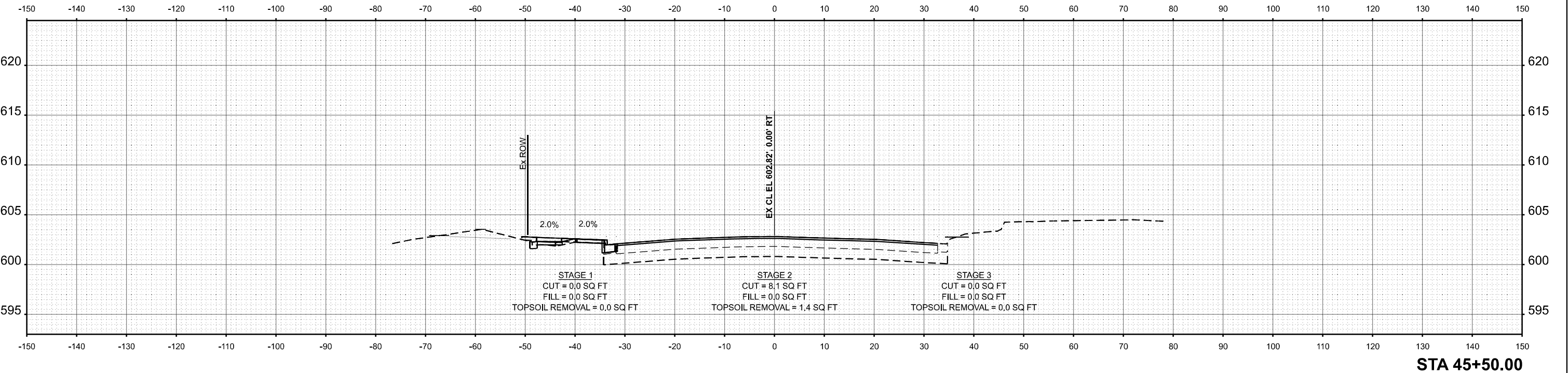
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	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 1	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	148
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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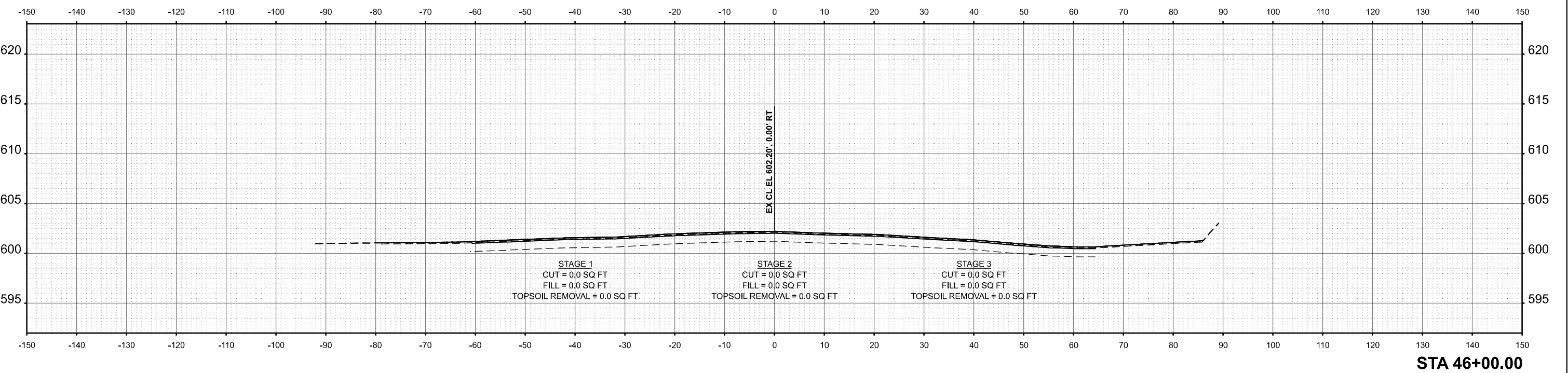
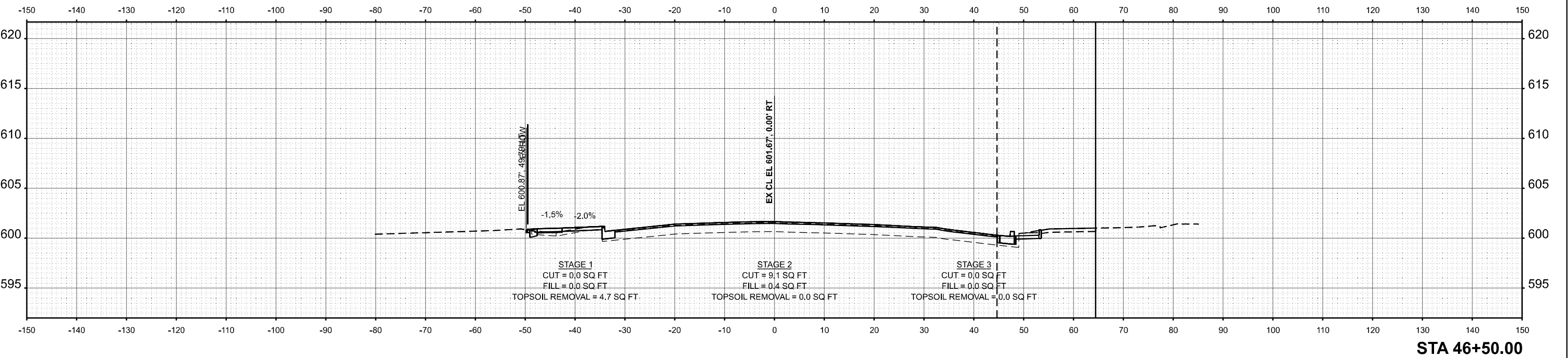
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



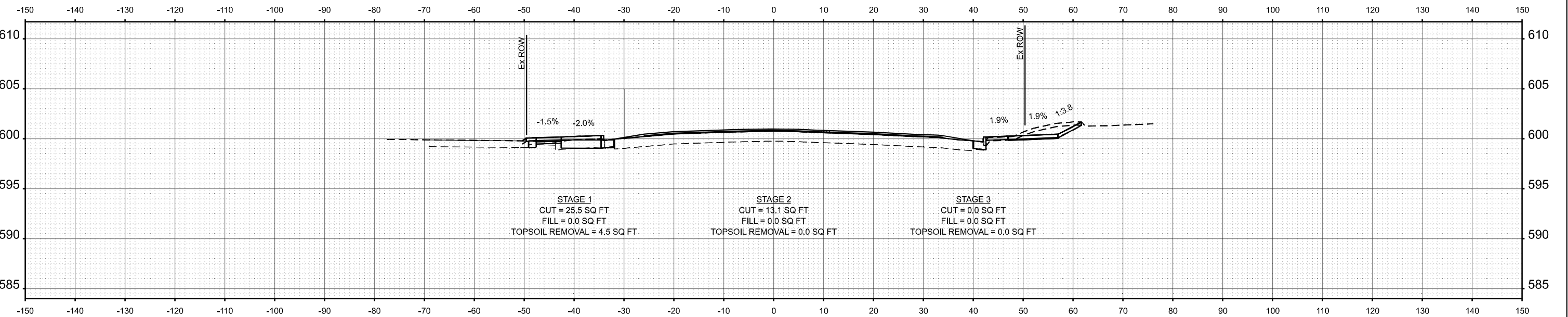
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SCALE: 1:10H 1:5V	SHEET 2	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	149
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

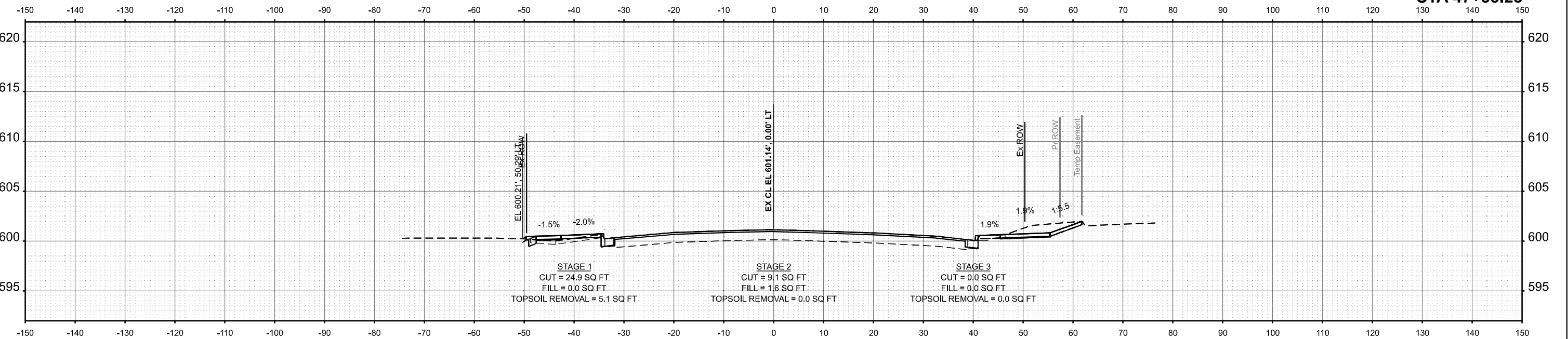
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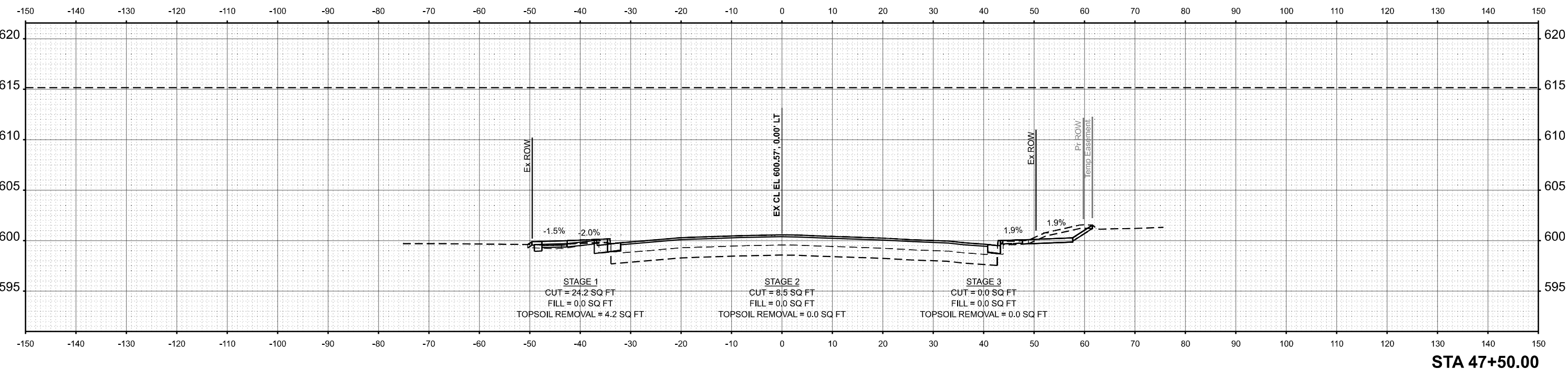
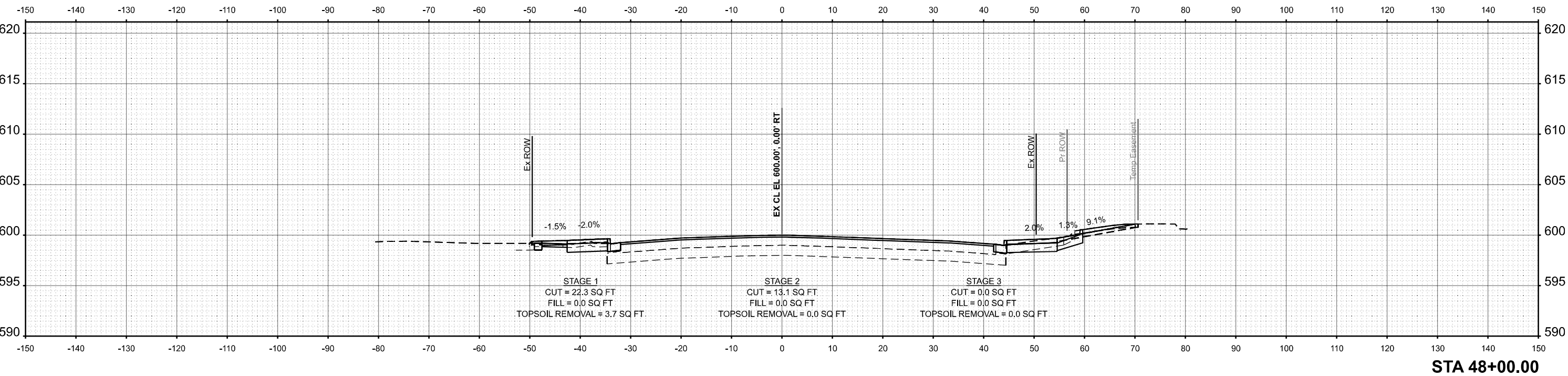
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STA 47+00.00

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		CHECKED - GJE	REVISED -					CONTRACT NO. 62W47				
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SCALE: 1:10H 1:5V		SHEET 4 OF 29 SHEETS		STA.		TO STA.						

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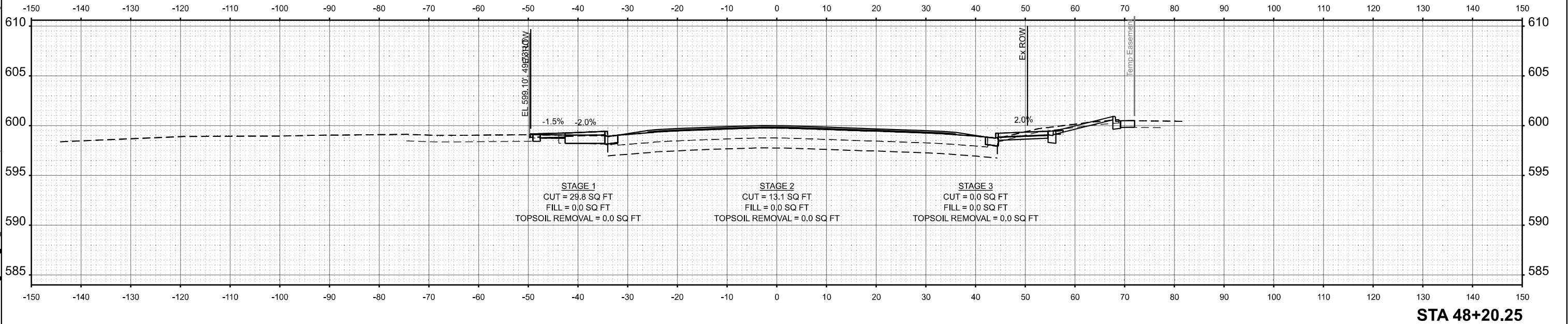
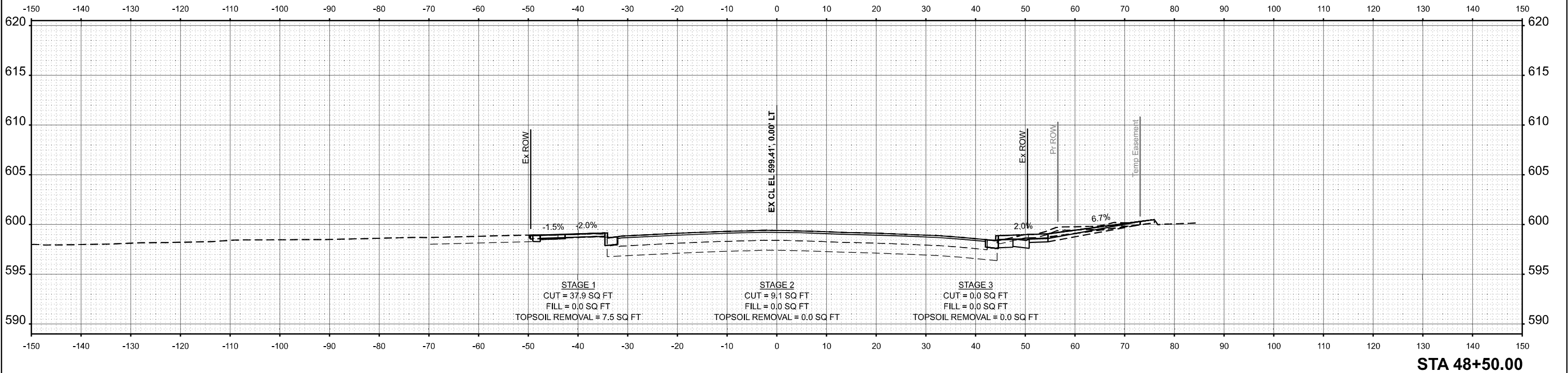
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PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)
SCALE: 1:10H 1:5V SHEET 5 OF 29 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	152
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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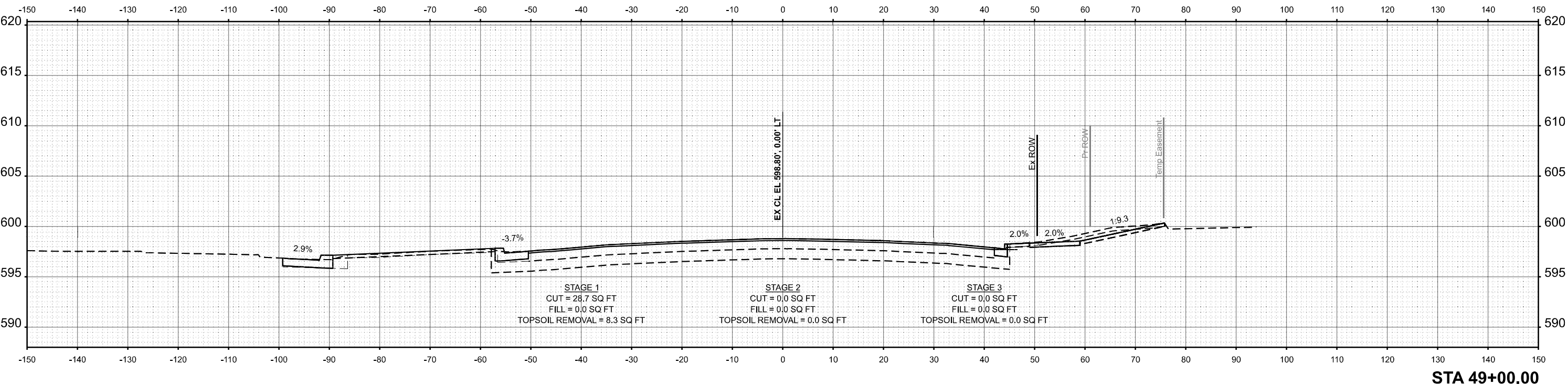
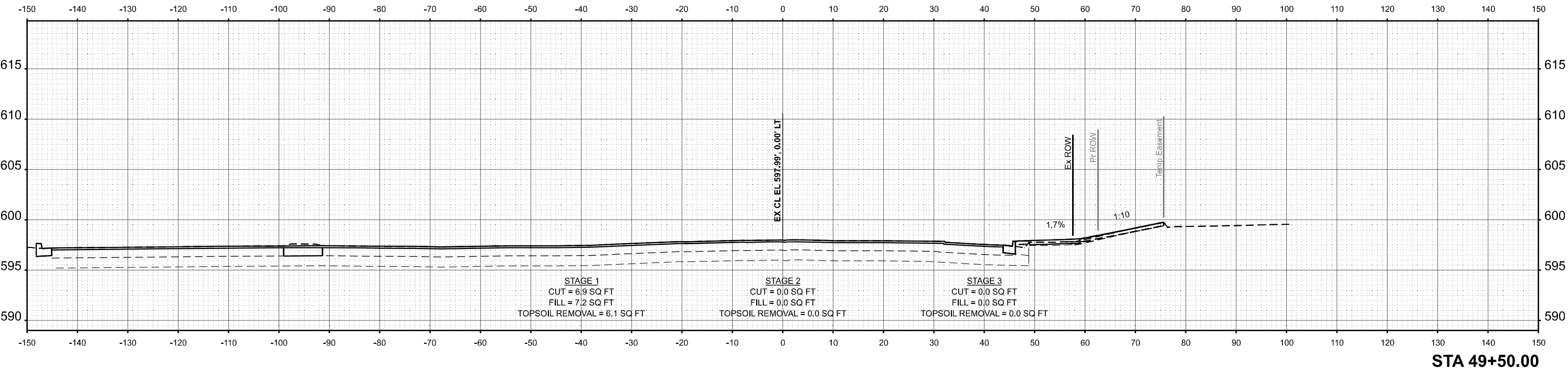
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	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)
SCALE: 1:10H 1:5V SHEET 6 OF 29 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

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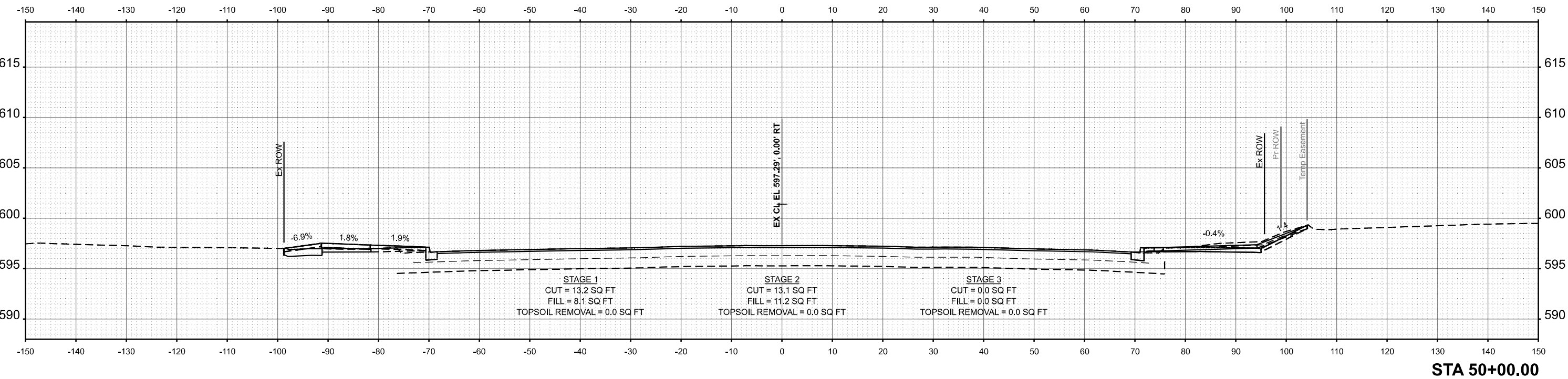
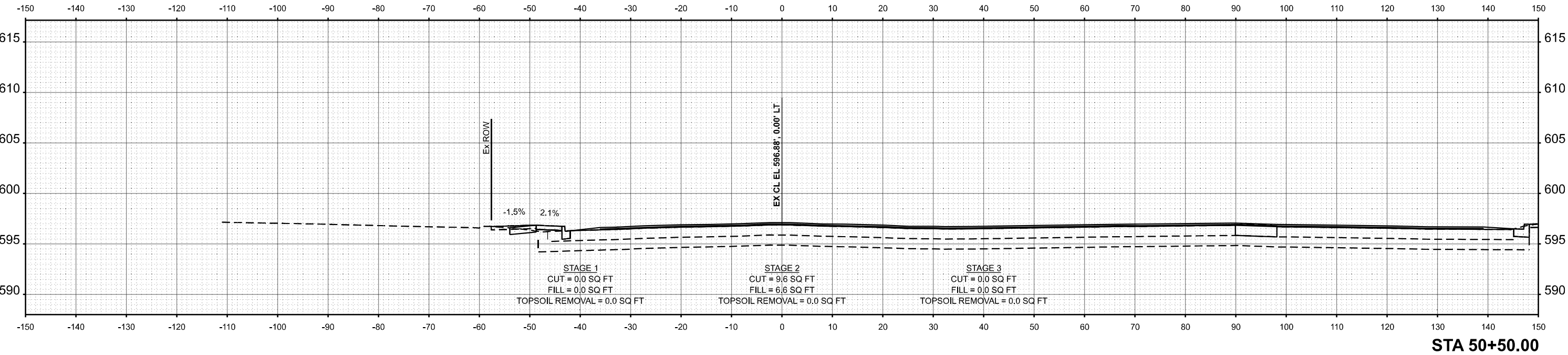
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	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 7	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				

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	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

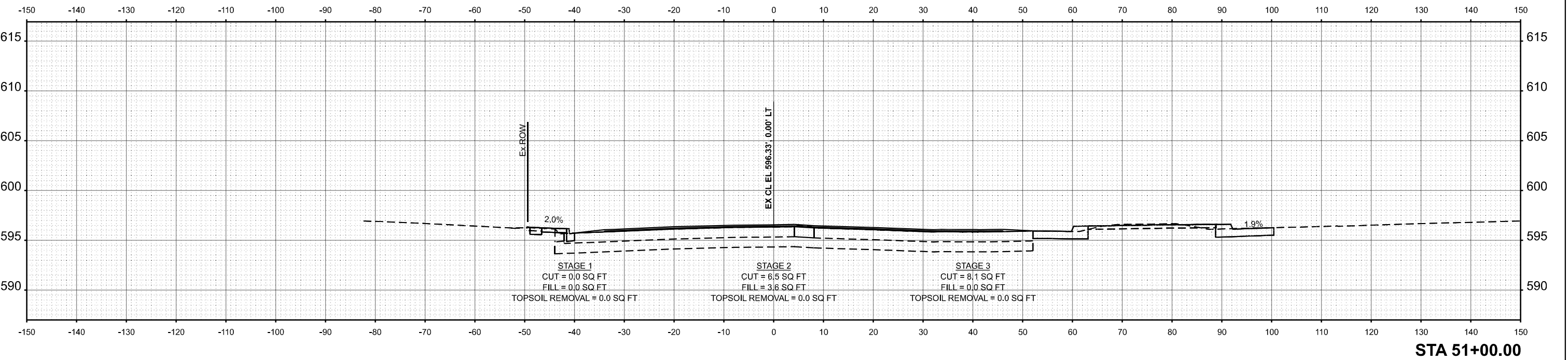
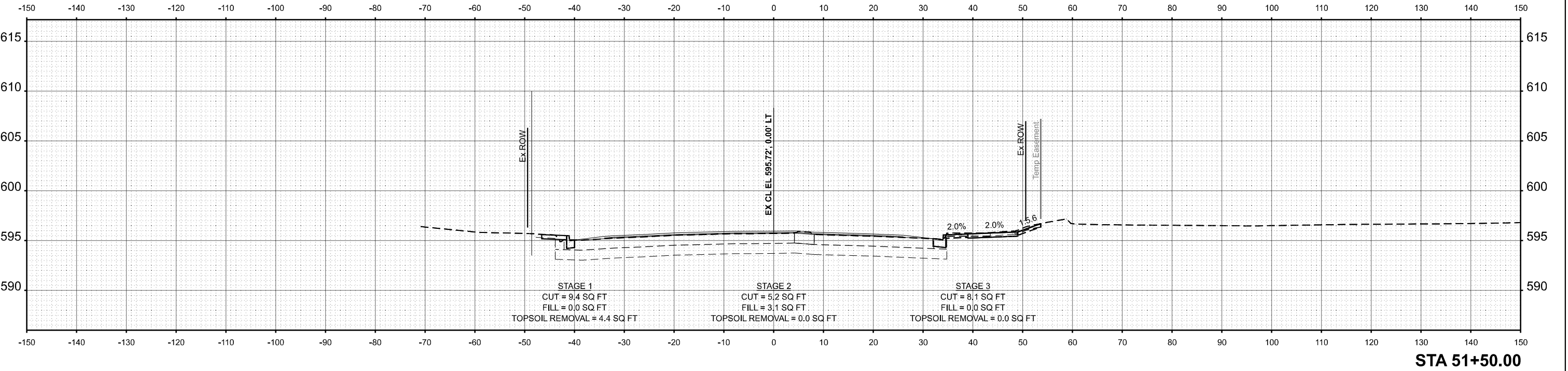
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

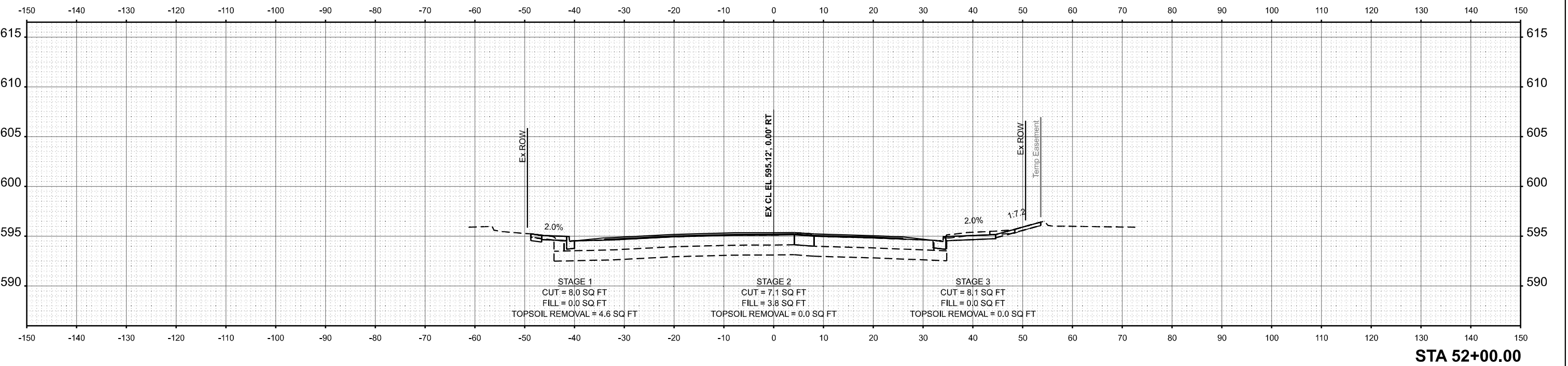
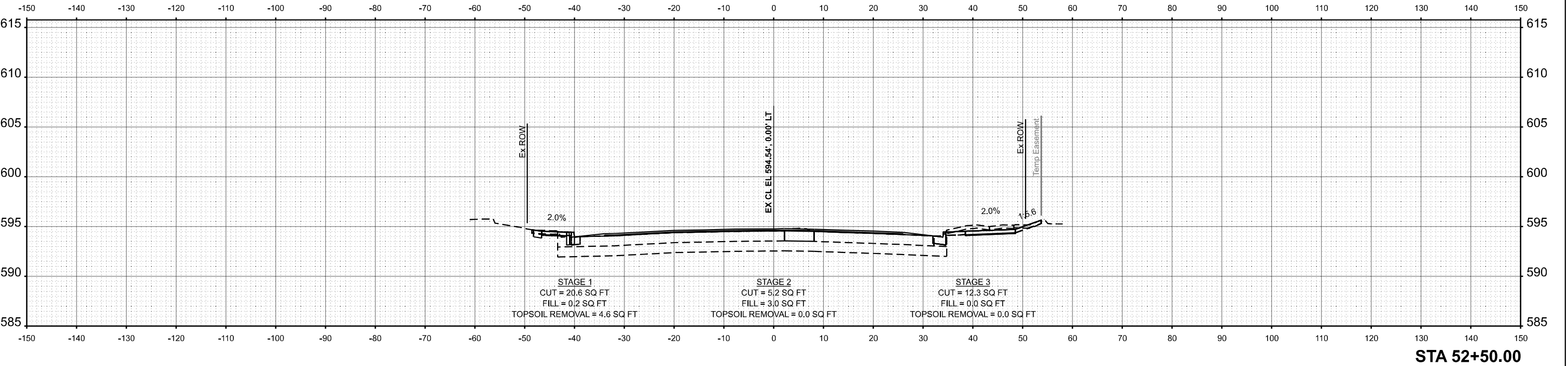
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	155
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

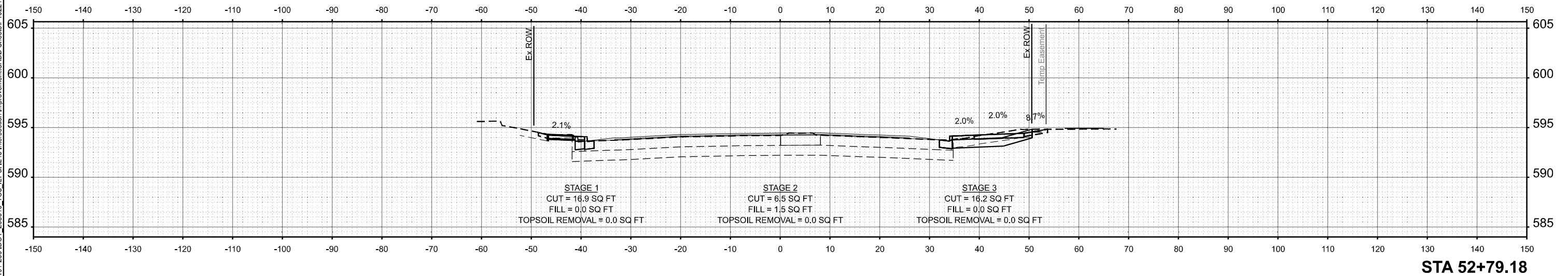
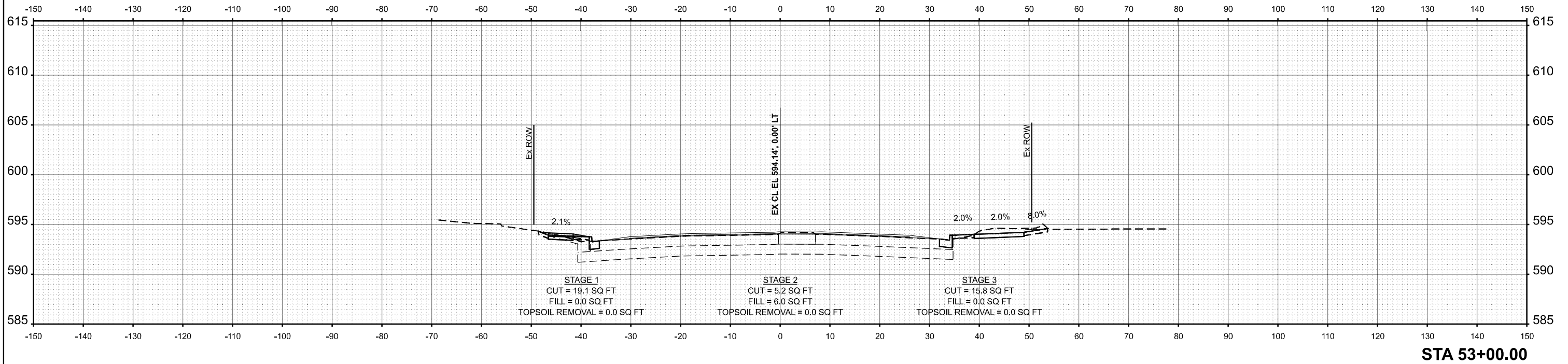
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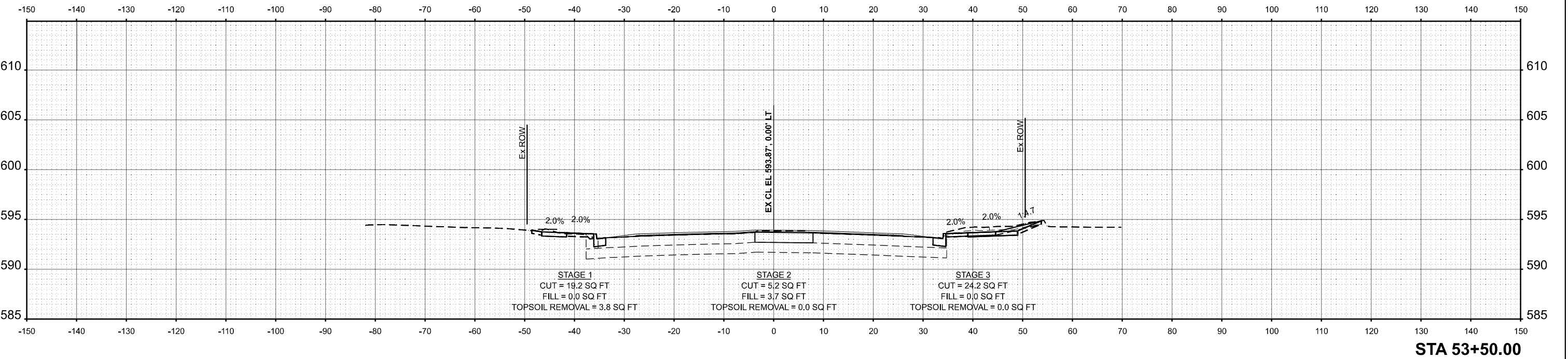
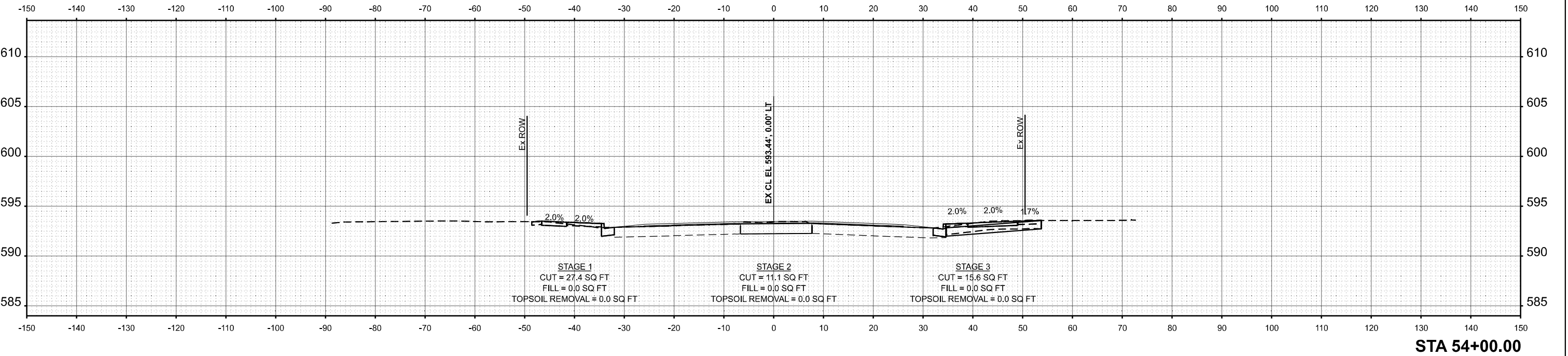
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 11	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	158
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: IL43_E-53+50.00 [Sheet]
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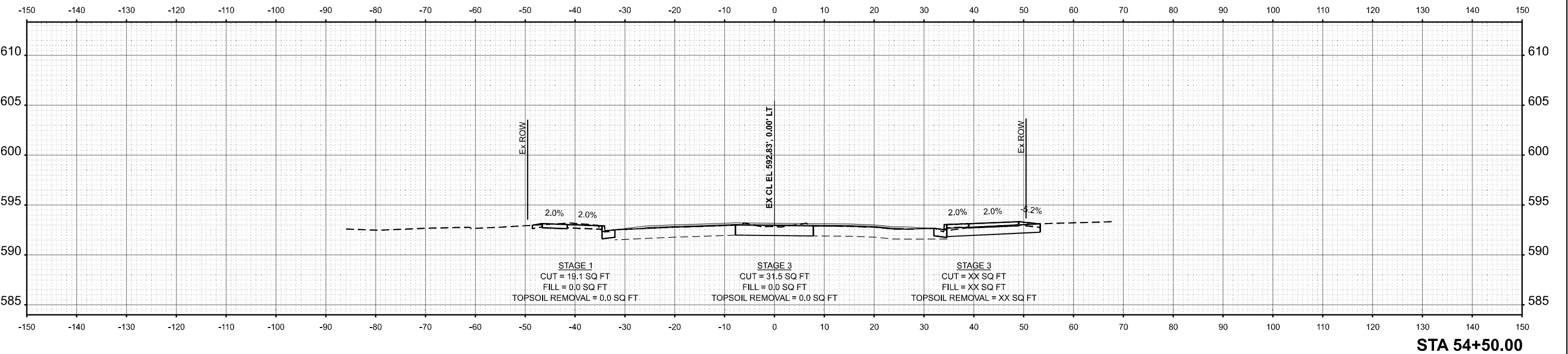
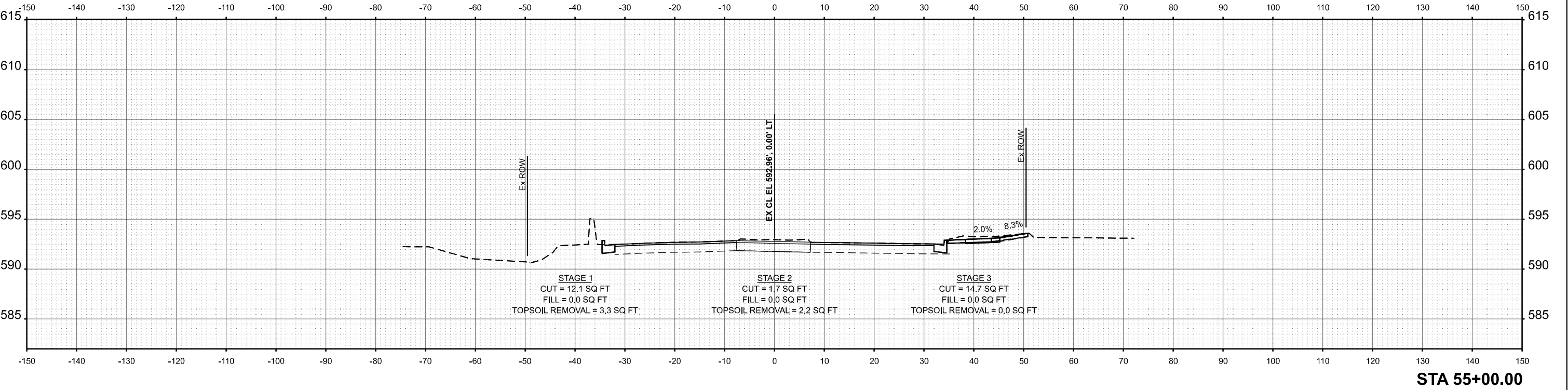
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 43 - CROSS SECTIONS IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 12	OF 29 SHEETS	STA. TO STA.

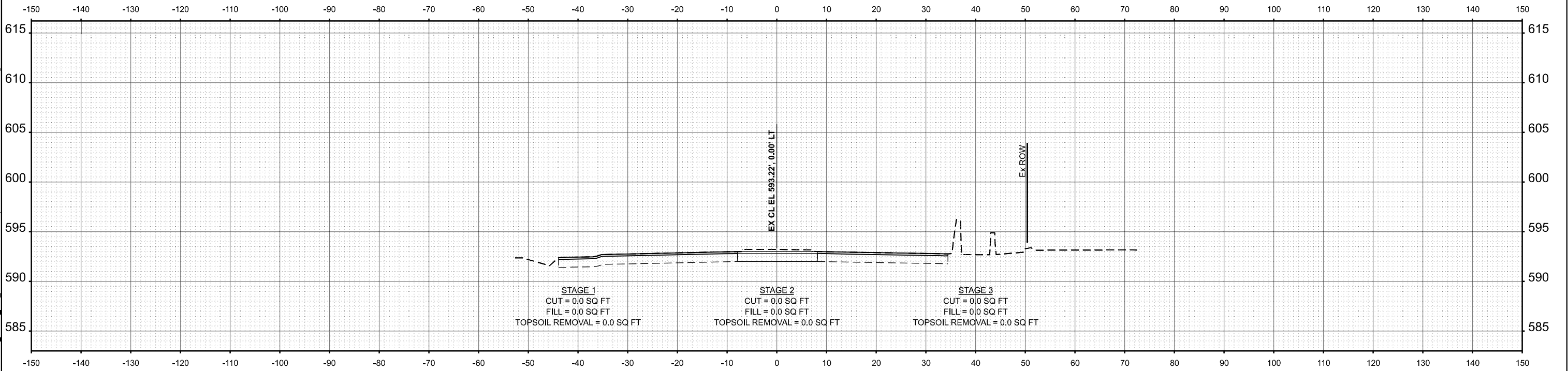
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	159
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: IL43_E-54+50.00 [Sheet]
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\P102218-ssc-IL43.dgn



<div><div></div><div>BLA, Inc.</div></div>	USER NAME = tnestor		DESIGNED - TN		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				IL. ROUTE 43 - CROSS SECTIONS					F.A.U.	SECTION		COUNTY	TOTAL	SHEET		
			DRAWN - TN		REVISED -						IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)					RTE.				SHEETS	NO.		
			CHECKED - GJE		REVISED -											3578	2023-925-N-TS(L)		COOK	176	160		
	PLOT DATE = 10/17/2025		DATE - 10/15/2025		REVISED -											CONTRACT NO. 62W47							
												SCALE: 1:10H 1:5V		SHEET 13 OF 29 SHEETS		STA.		TO STA.		ILLINOIS FED. AID PROJECT			

MODEL: IL43_E-55+00.00 [Sheet]
FILE NAME: W:\191+200 IDOT_208010_TC08_IL7 at IL43 Intersection Improvement\CADD Sheets\102218-ssc-IL43.dgn



STA 55+50.00



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

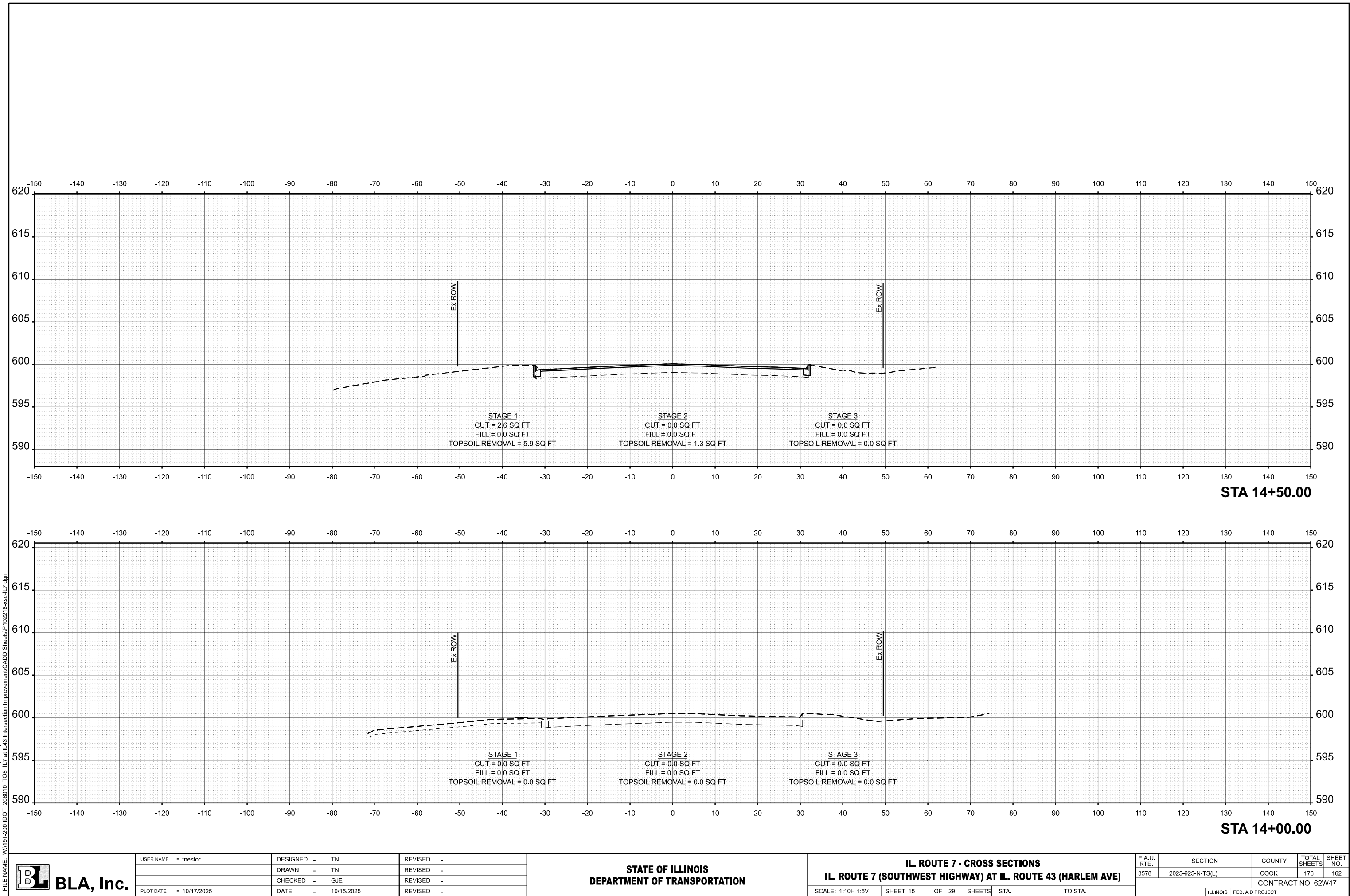
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 43 - CROSS SECTIONS
IL ROUTE 7 (SOUTHWEST HIGHWAY) AT IL ROUTE 43 (HARLEM AVE)

SCALE: 1:10H 1:5V SHEET 14 OF 29 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2023-925-N-TS(L)	COOK	176	161
CONTRACT NO. 62W47				
		ILLINOIS	FED. AID PROJECT	

MODEL: Unnamed Cross Section-2 - 14+00.00 [Sheet]
FILE NAME: W:\191-200 IDOT_208010_1038_IL7 at IL-43 Intersection Improvement\CADD Sheets\102218-ssc-IL7.dgn



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

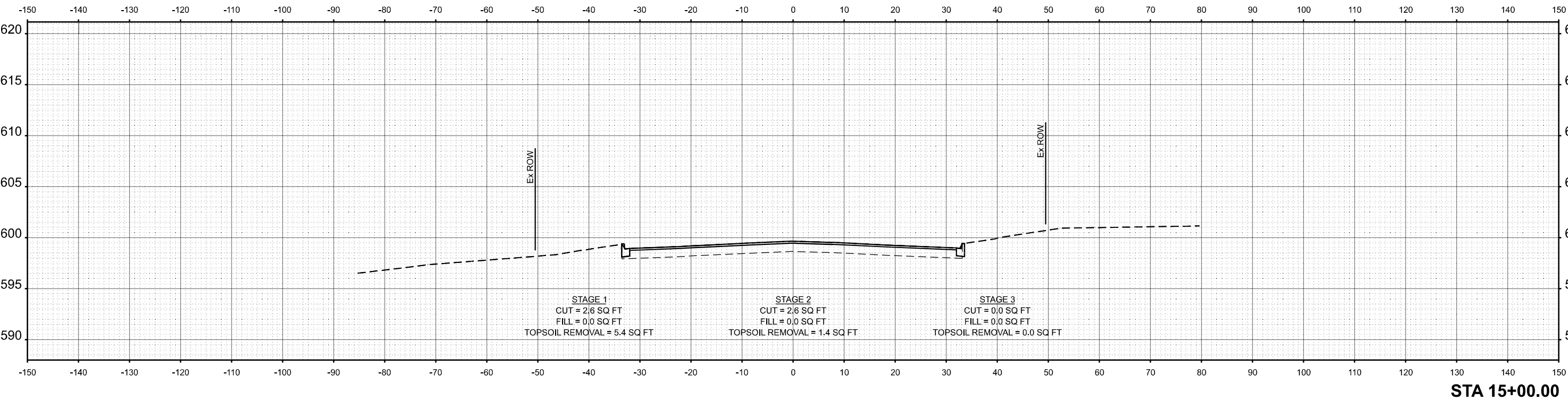
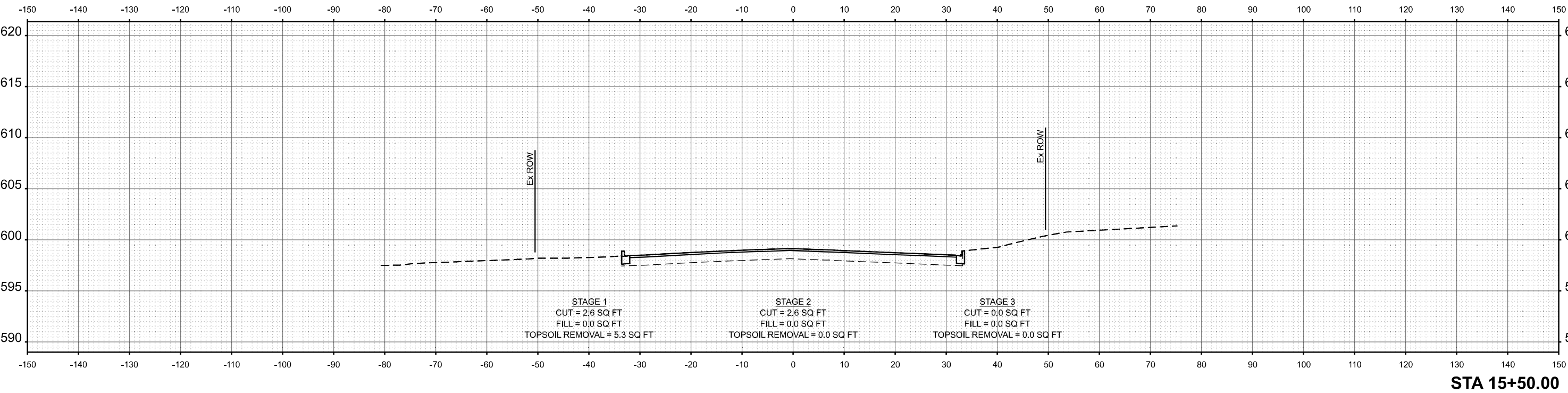
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 7 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1:10H 1:5V SHEET 15 OF 29 SHEETS STA. TO STA.

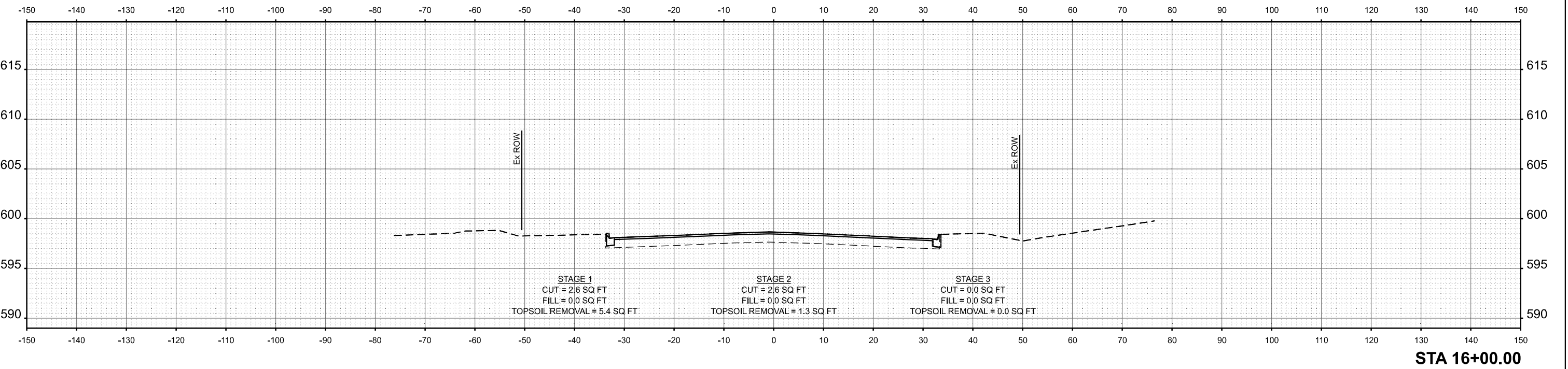
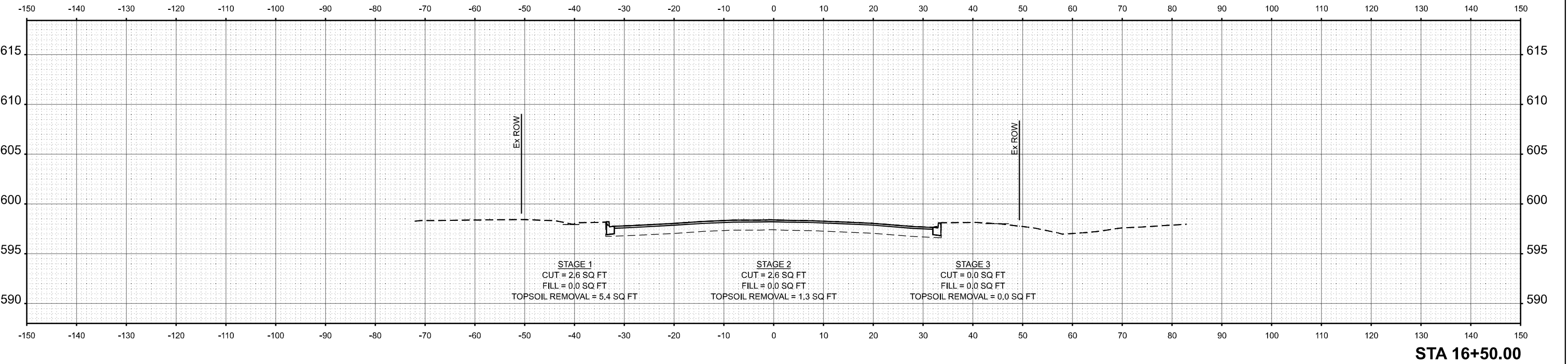
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	162
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Cross Section-2 - 15+00.00 [Sheet]
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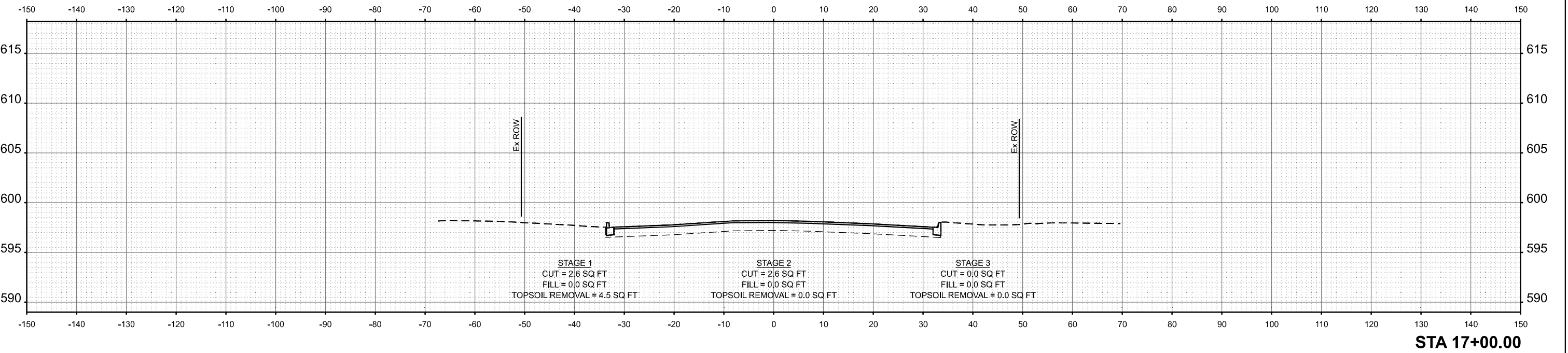
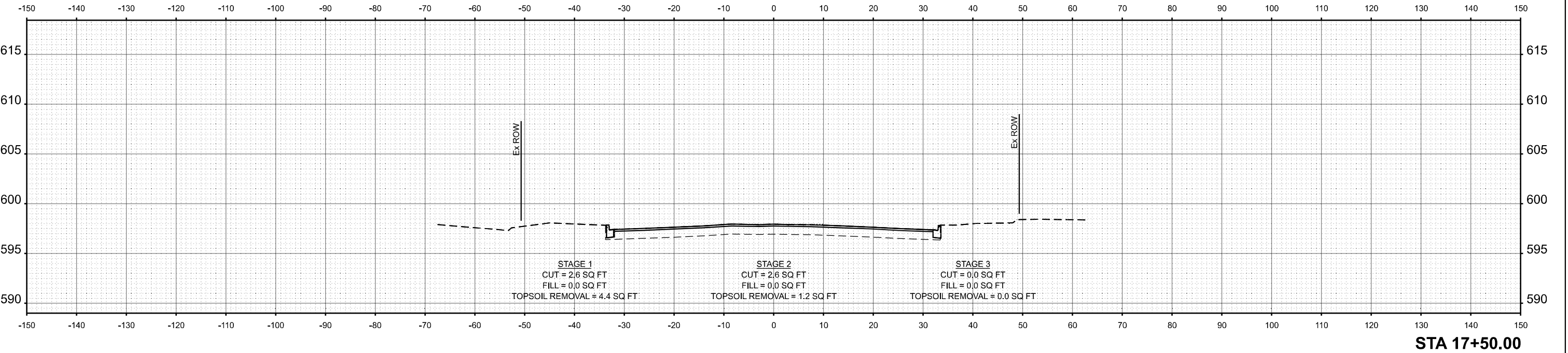


<div><div></div><div>BLA, Inc.</div></div>	USER NAME = tnestor	DESIGNED - TN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				IL. ROUTE 7 - CROSS SECTIONS IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - TN	REVISED -									3578	2025-925-N-TS(L)	COOK	176	163
		CHECKED - GJE	REVISED -									CONTRACT NO. 62W47				
	PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -									ILLINOIS FED. AID PROJECT				
				SCALE: 1:10H 1:5V		SHEET 16 OF 29 SHEETS		STA.		TO STA.						

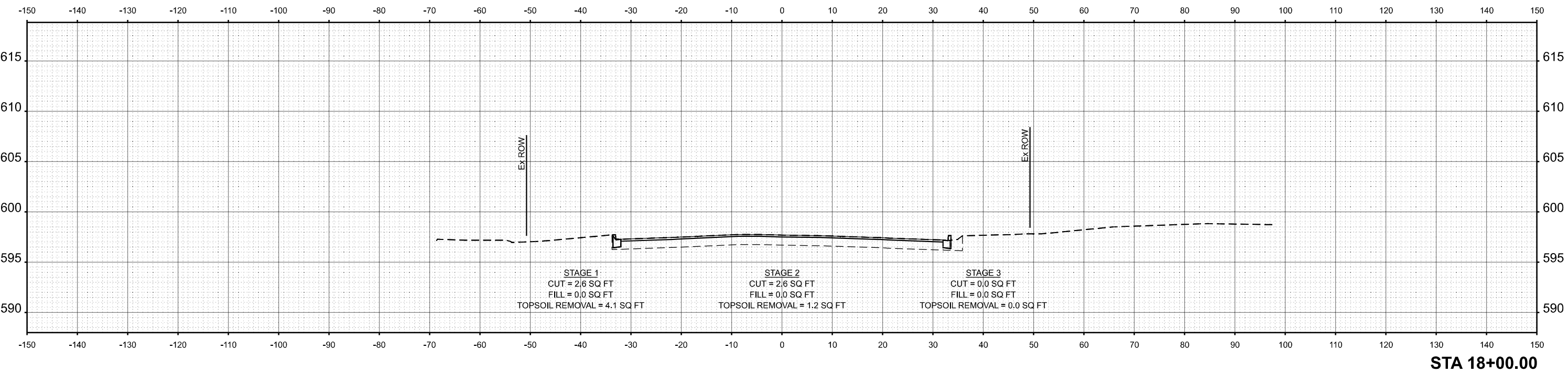
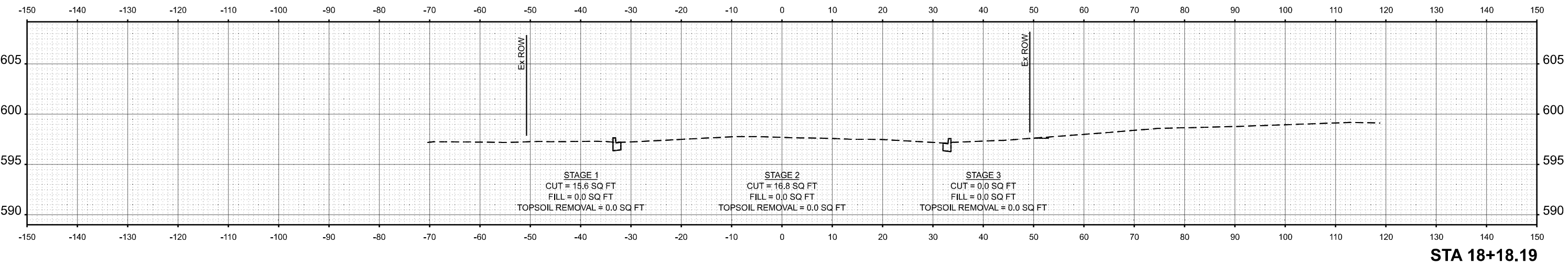
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MODEL: Unnamed Cross Section-2 - 18+00.00 [Sheet]
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USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

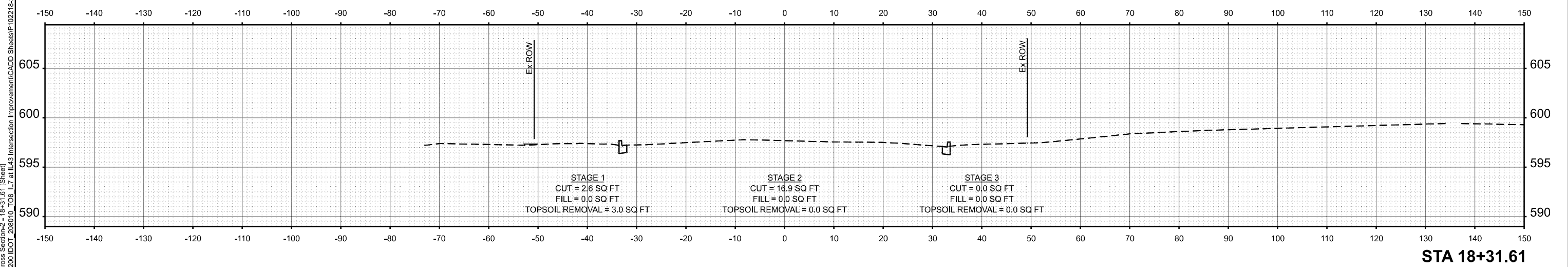
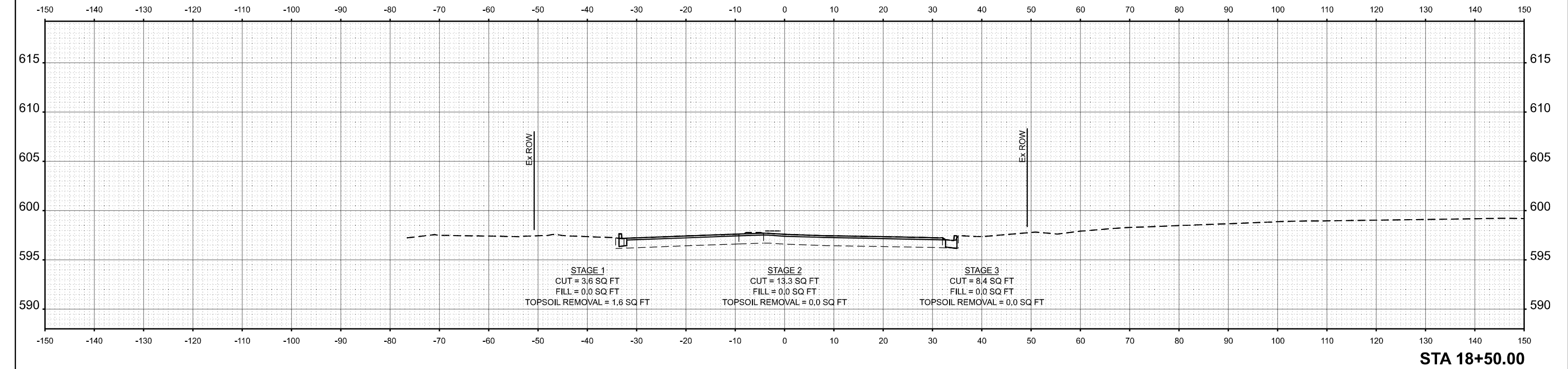
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 7 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1:10H 1:5V SHEET 19 OF 29 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	166
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Cross Section-2 - 18+31.61 [Sheet]
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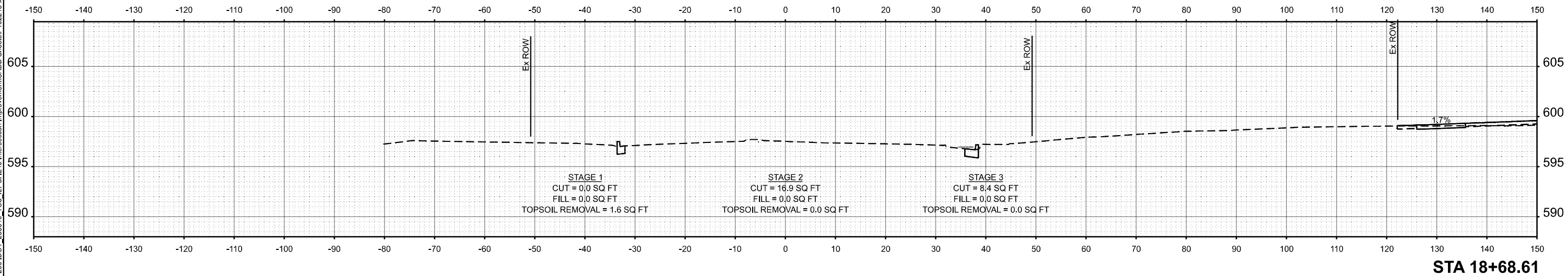
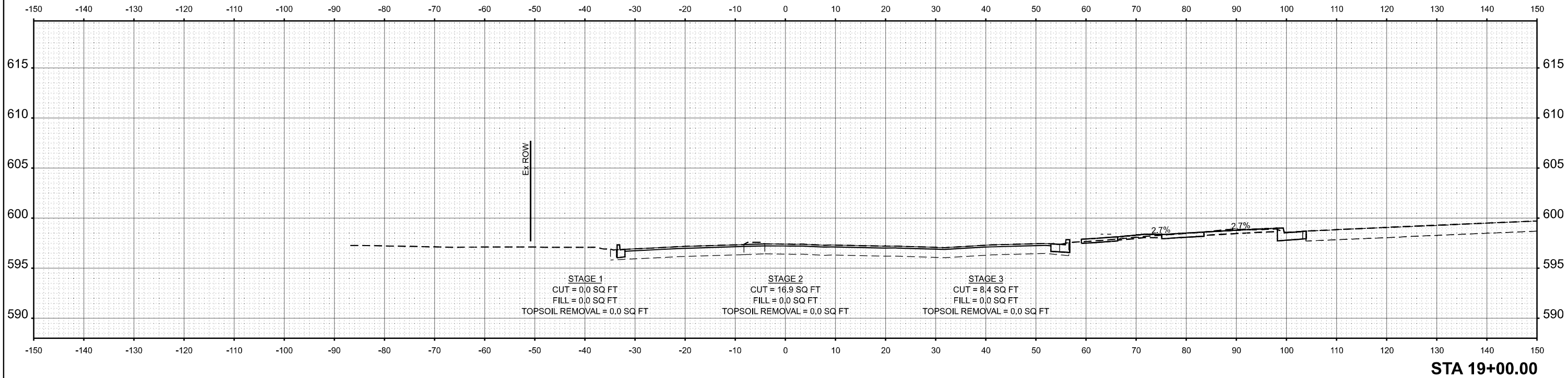
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 7 - CROSS SECTIONS			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 20	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	167
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Cross Section-2 - 18+68.61 [Sheet]
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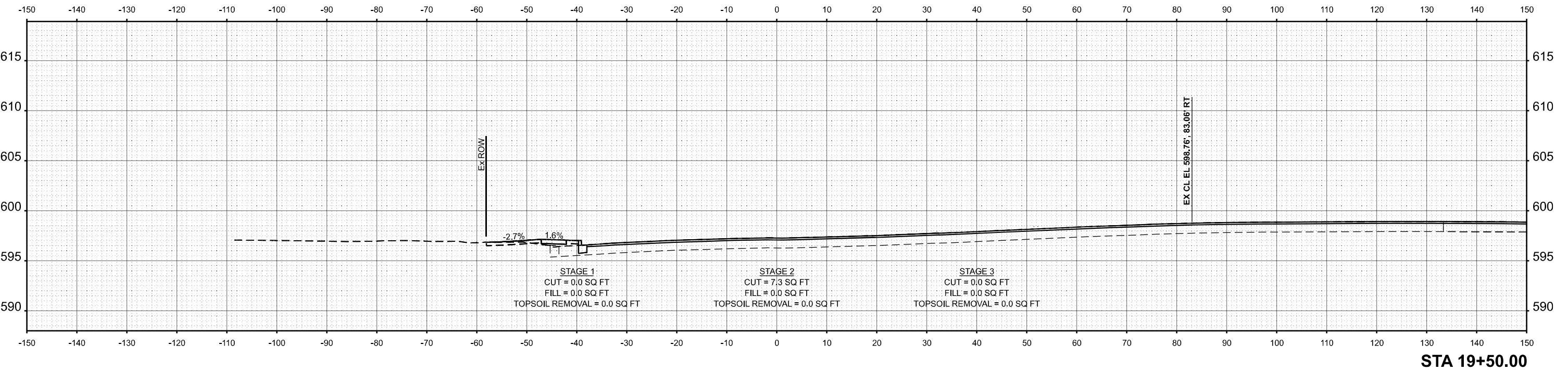
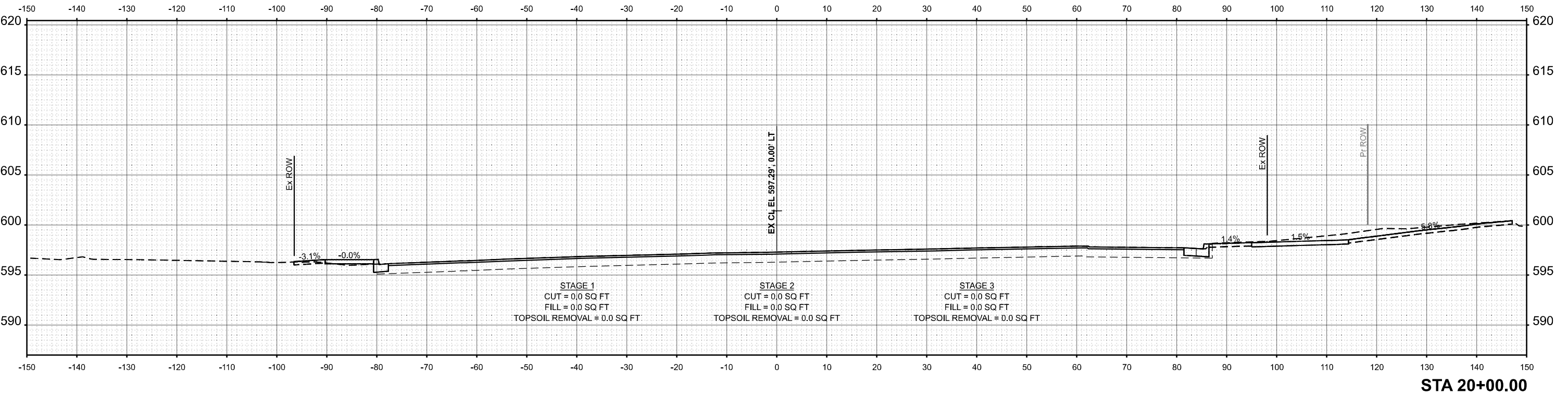
USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

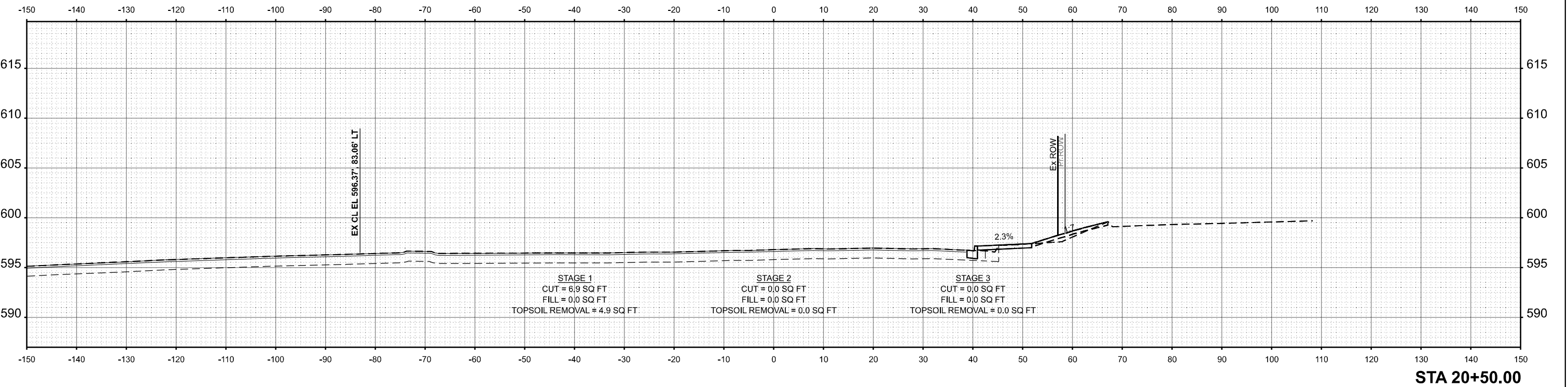
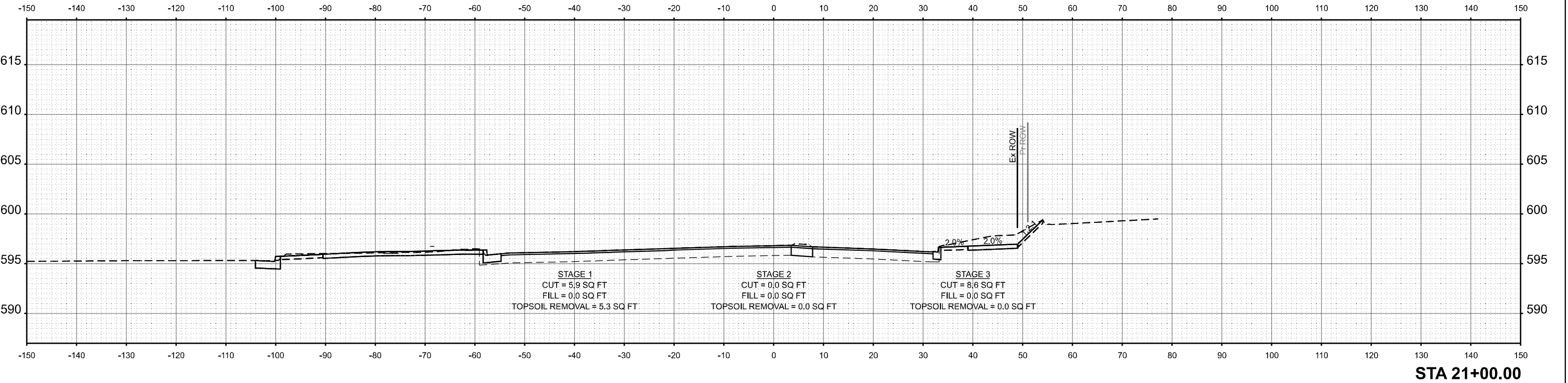
IL. ROUTE 7 - CROSS SECTIONS			
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)			
SCALE: 1:10H 1:5V	SHEET 21	OF 29 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	168
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

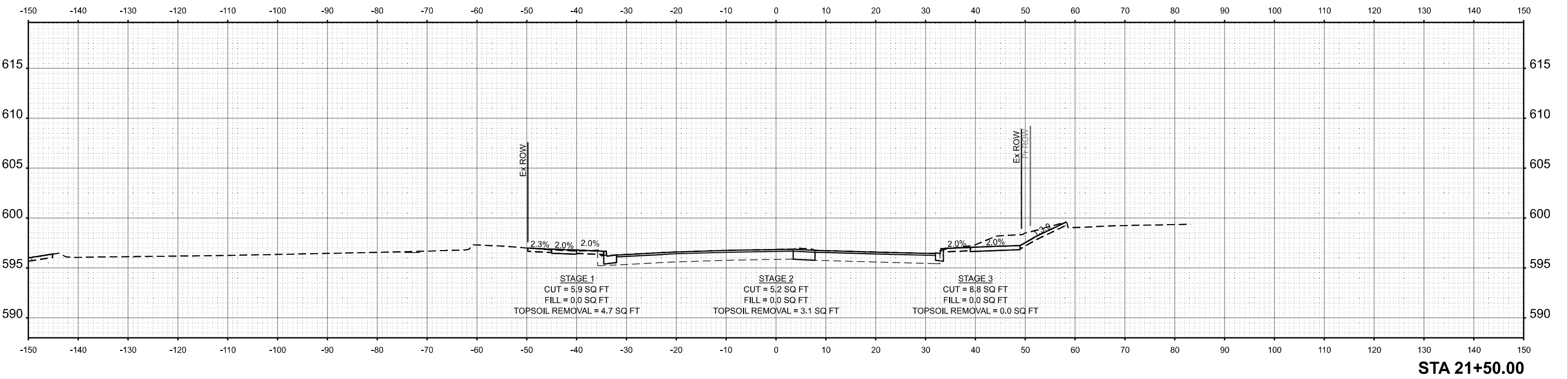
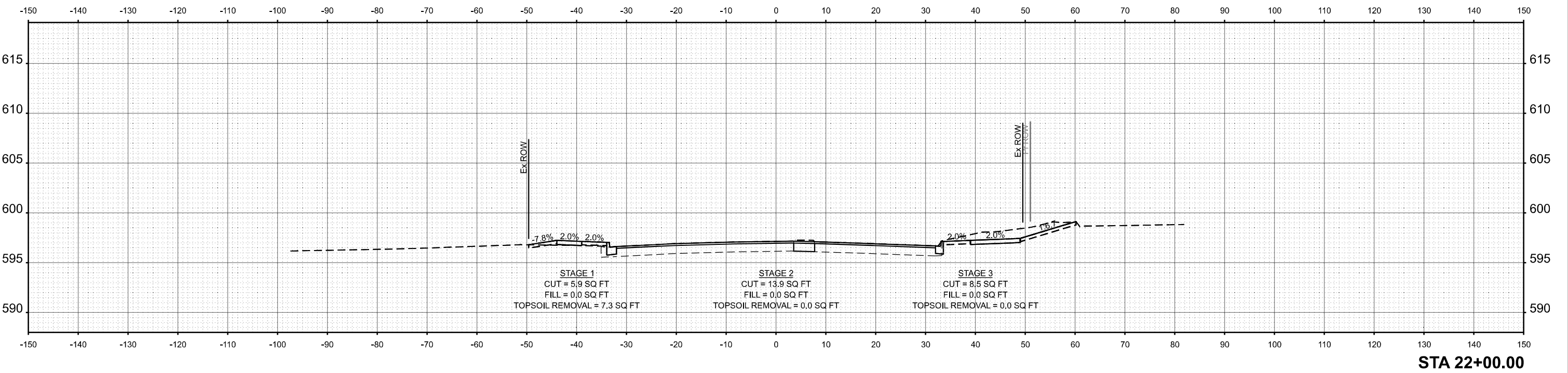
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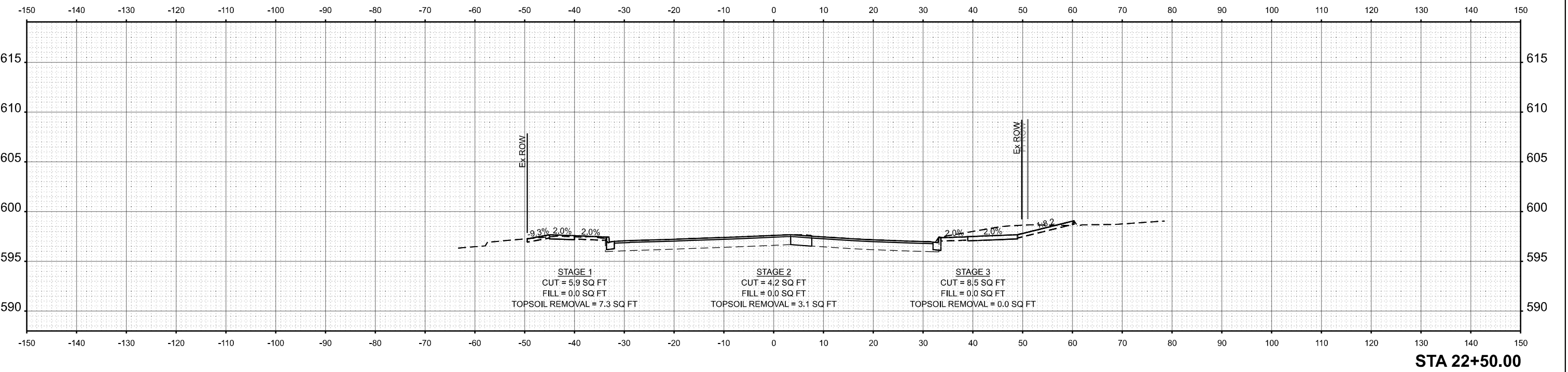
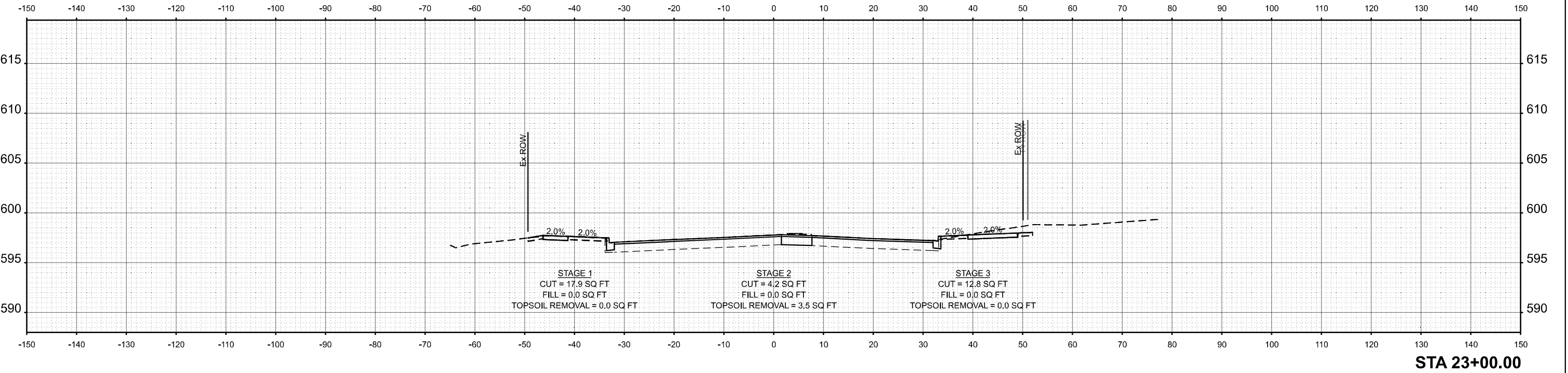
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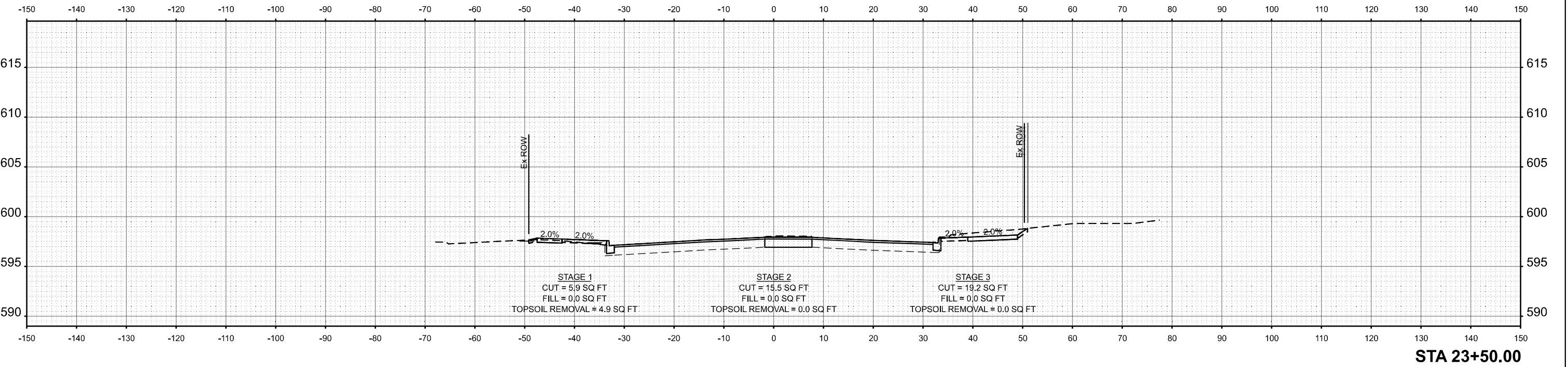
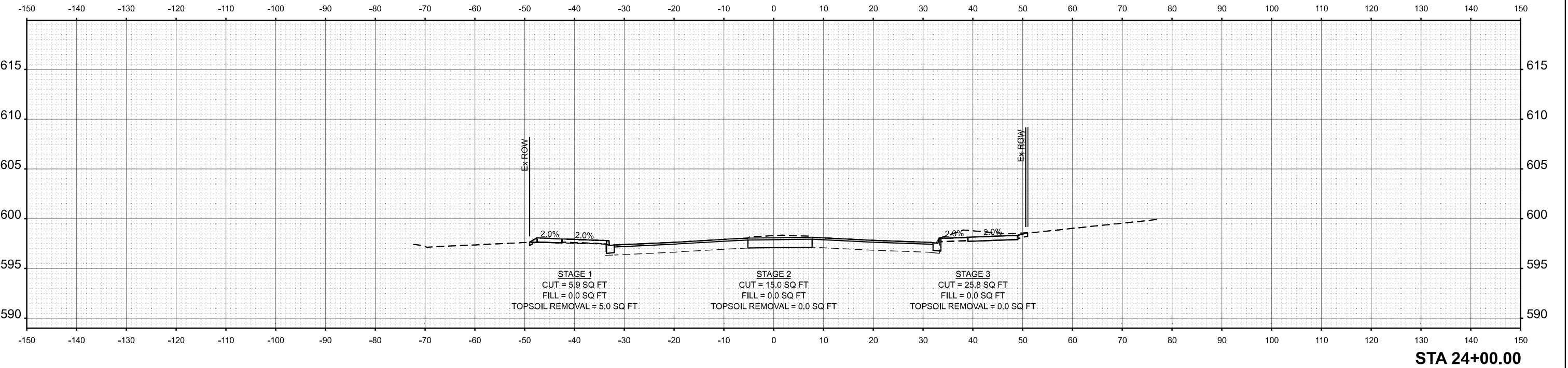
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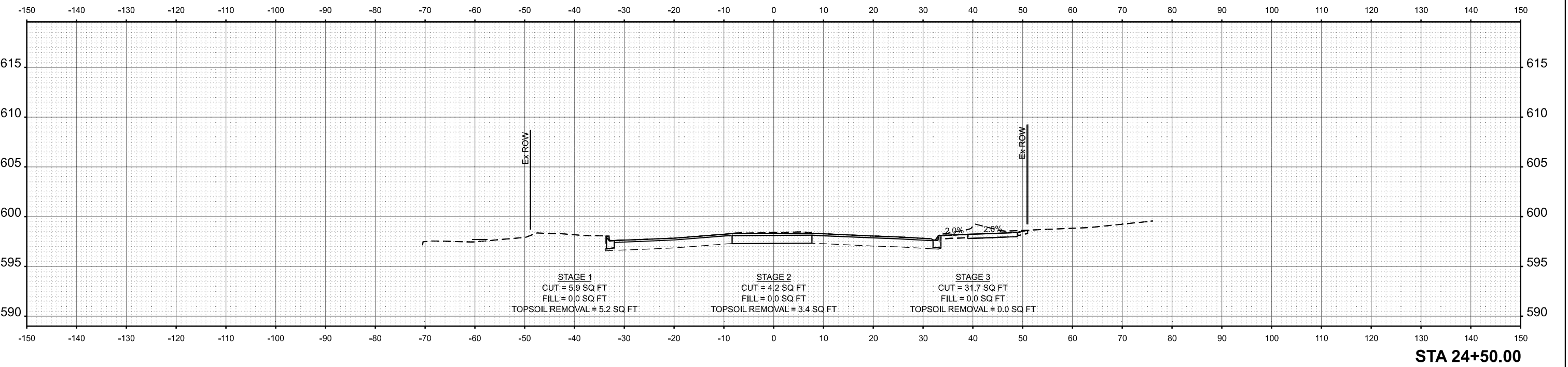
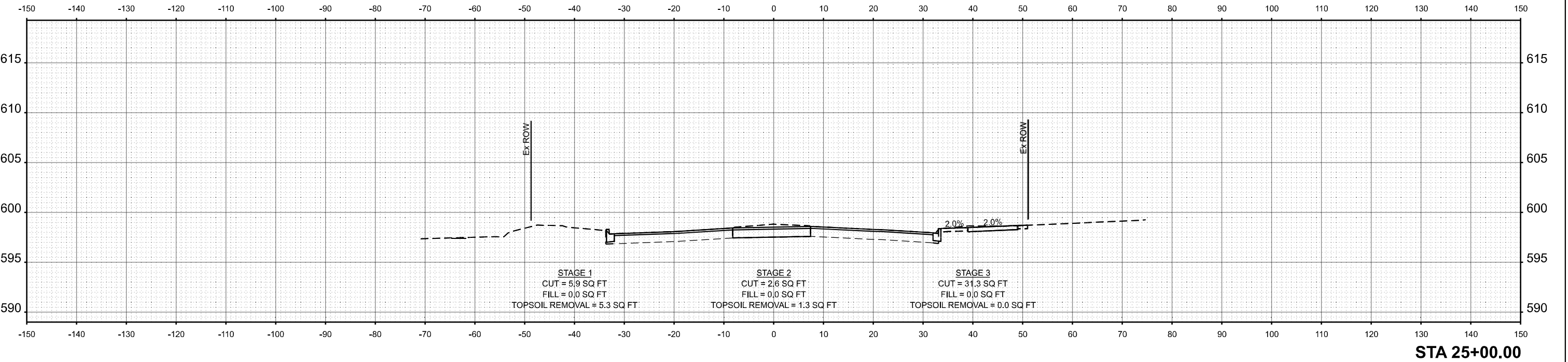
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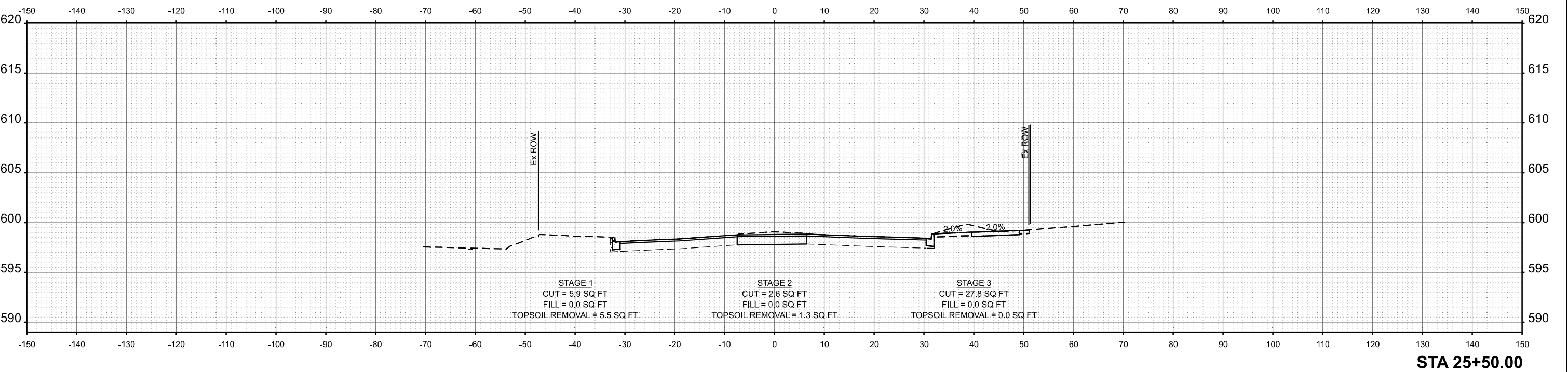
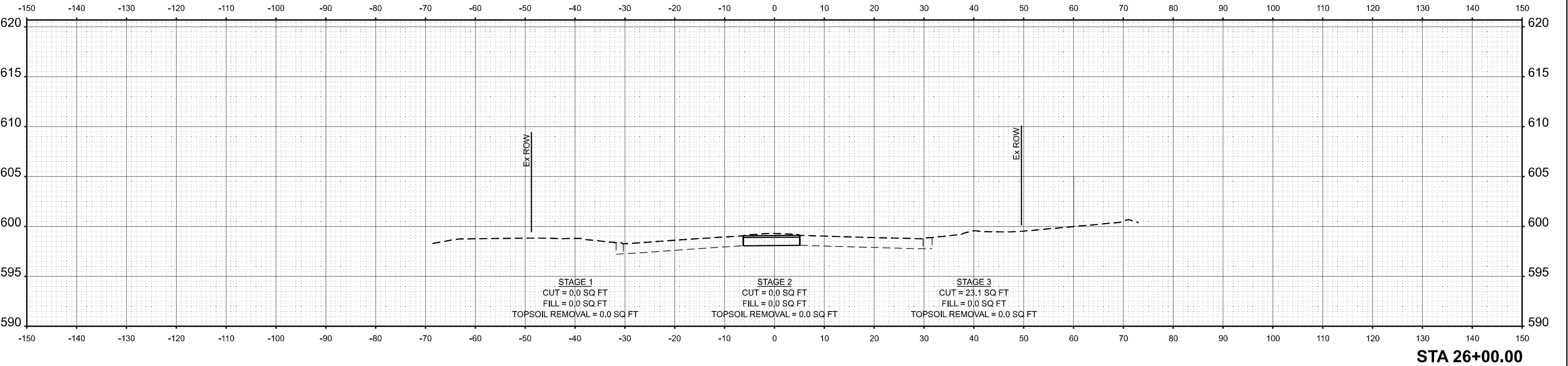
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MODEL: Unnamed Cross Section-2 - 24+50.00 [Sheet]
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MODEL: Unnamed Cross Section-2 - 25+50.00 [Sheet]
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USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

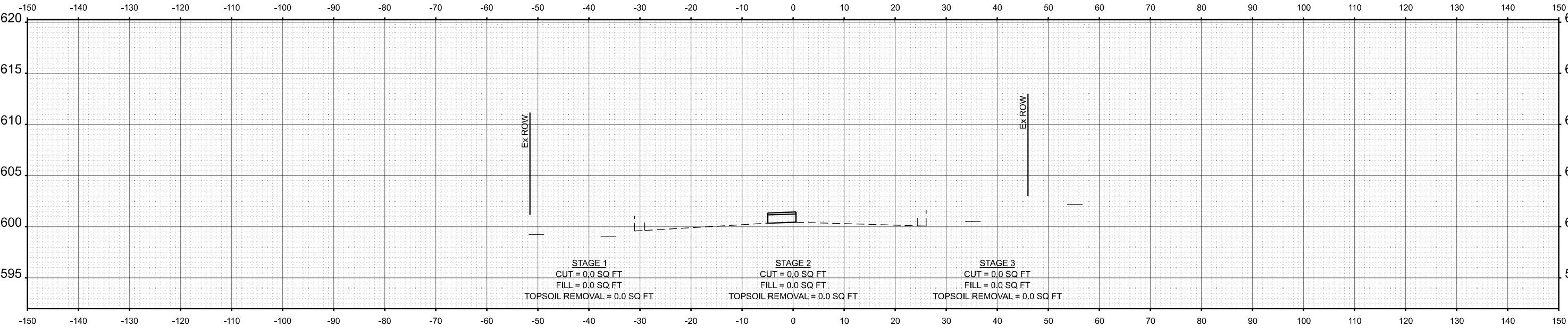
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 7 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

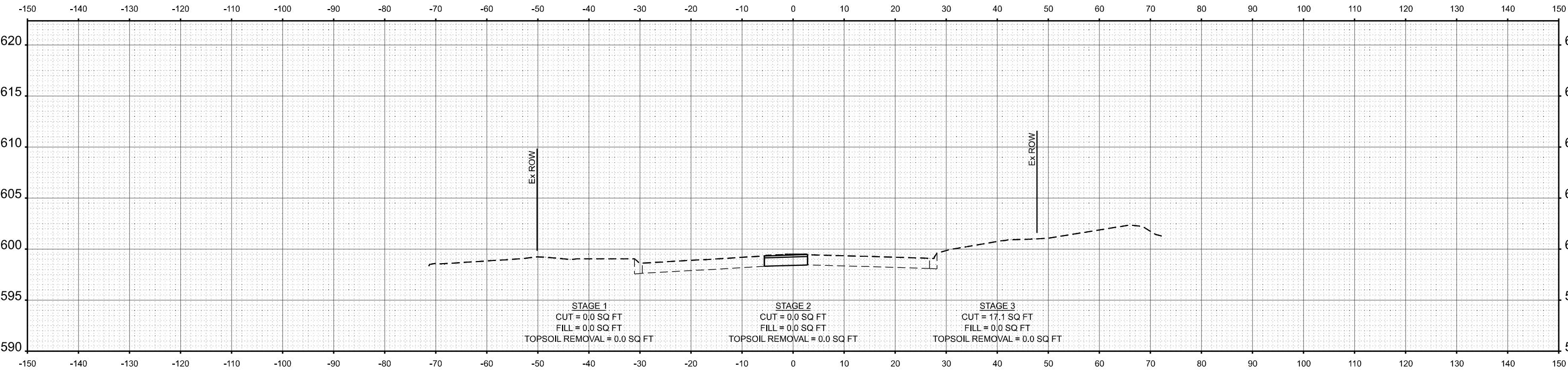
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	175
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				

MODEL: Unnamed Cross Section-2 - 26+50.00 [Sheet]
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STA 27+00.00



STA 26+50.00



USER NAME = tnestor	DESIGNED - TN	REVISED -
	DRAWN - TN	REVISED -
	CHECKED - GJE	REVISED -
PLOT DATE = 10/17/2025	DATE - 10/15/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 7 - CROSS SECTIONS
IL. ROUTE 7 (SOUTHWEST HIGHWAY) AT IL. ROUTE 43 (HARLEM AVE)

SCALE: 1:10H 1:5V SHEET 29 OF 29 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3578	2025-925-N-TS(L)	COOK	176	176
CONTRACT NO. 62W47				
ILLINOIS FED. AID PROJECT				