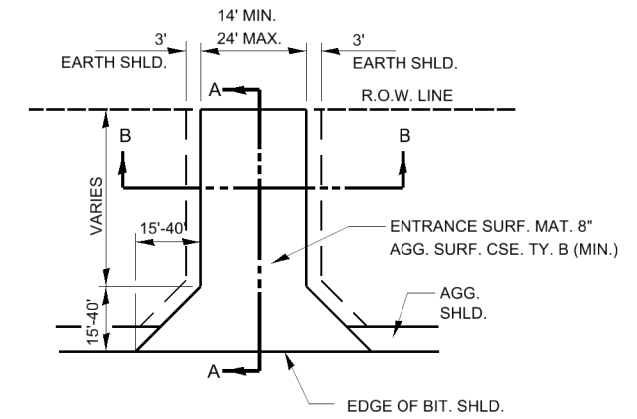
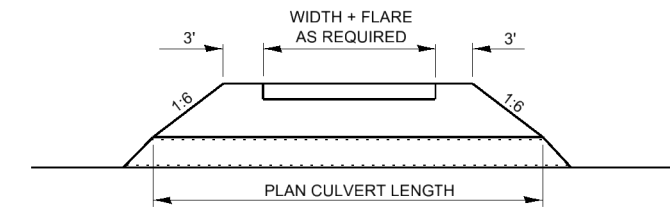


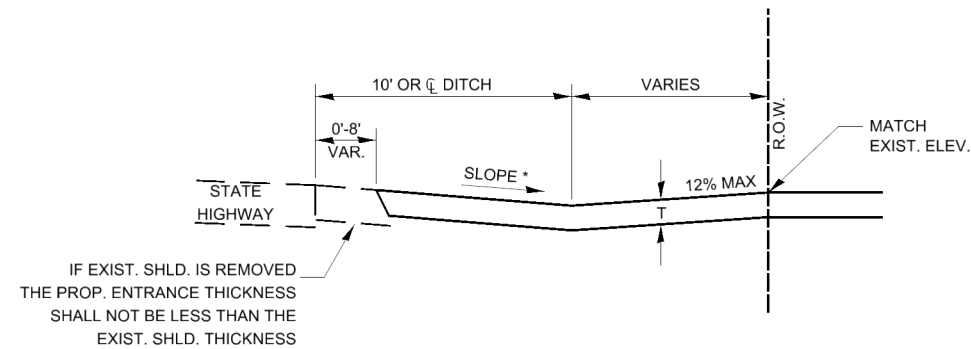
**TYPICAL NONCOMMERCIAL - RURAL
(PRIVATE ENTRANCE)**



**TYPICAL NONCOMMERCIAL - RURAL
(FIELD ENTRANCE)**



SECTION B-B



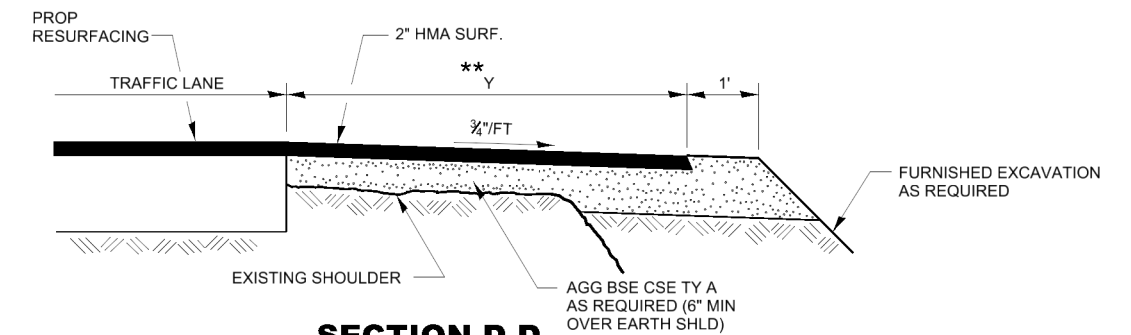
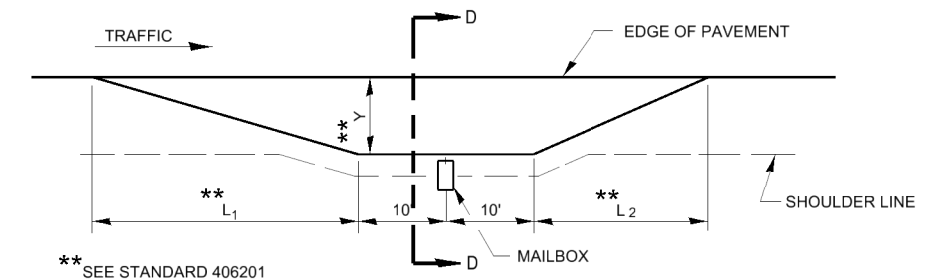
SECTION A-A

* SLOPE AWAY FROM HIGHWAY SURFACE AT 1.5% MINIMUM TO 8% MAXIMUM FOR A DISTANCE OF 10' OR TO CENTERLINE OF DITCH; 12% MAXIMUM BEYOND TO RIGHT OF WAY LINE, OR AS DETAILED IN THE PLANS.

SEE PLANS FOR PAVEMENT DESIGN (T).

DESIGNER NOTES:

1. ALL DRIVEWAYS SHALL BE SURFACED FROM THE EDGE OF THE HIGHWAY PAVEMENT TO THE RIGHT OF WAY LINE (SEE PLANS FOR T).
2. TYPICAL FOR NON-COMMERCIAL-RURAL APPLIES TO NON-COMMERCIAL-URBAN WHEN NO CURB & GUTTER IS PRESENT. EXCEPT FINAL SURFACE MUST BE HMA OR PCC PAVEMENT.



SECTION D-D

RURAL MAILBOX TURNOUT DETAILS

400-1

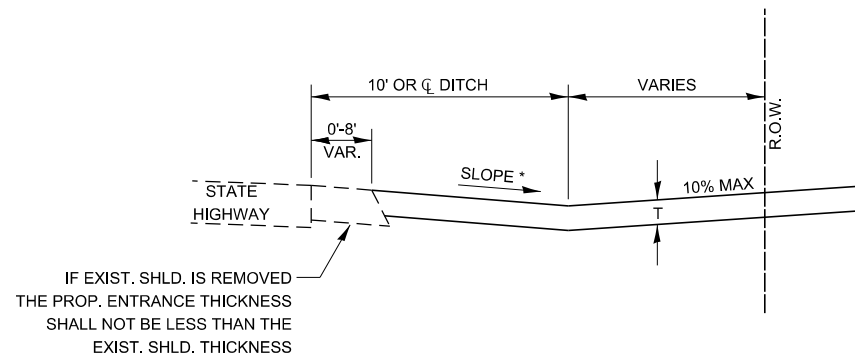
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USER NAME = Anthony.Grunsted	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000' / 1"	CHECKED -	REVISED -
PLOT DATE = 4/16/24	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

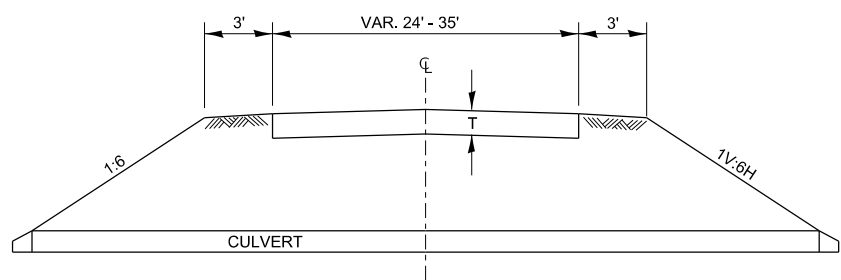
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



SECTION A-A

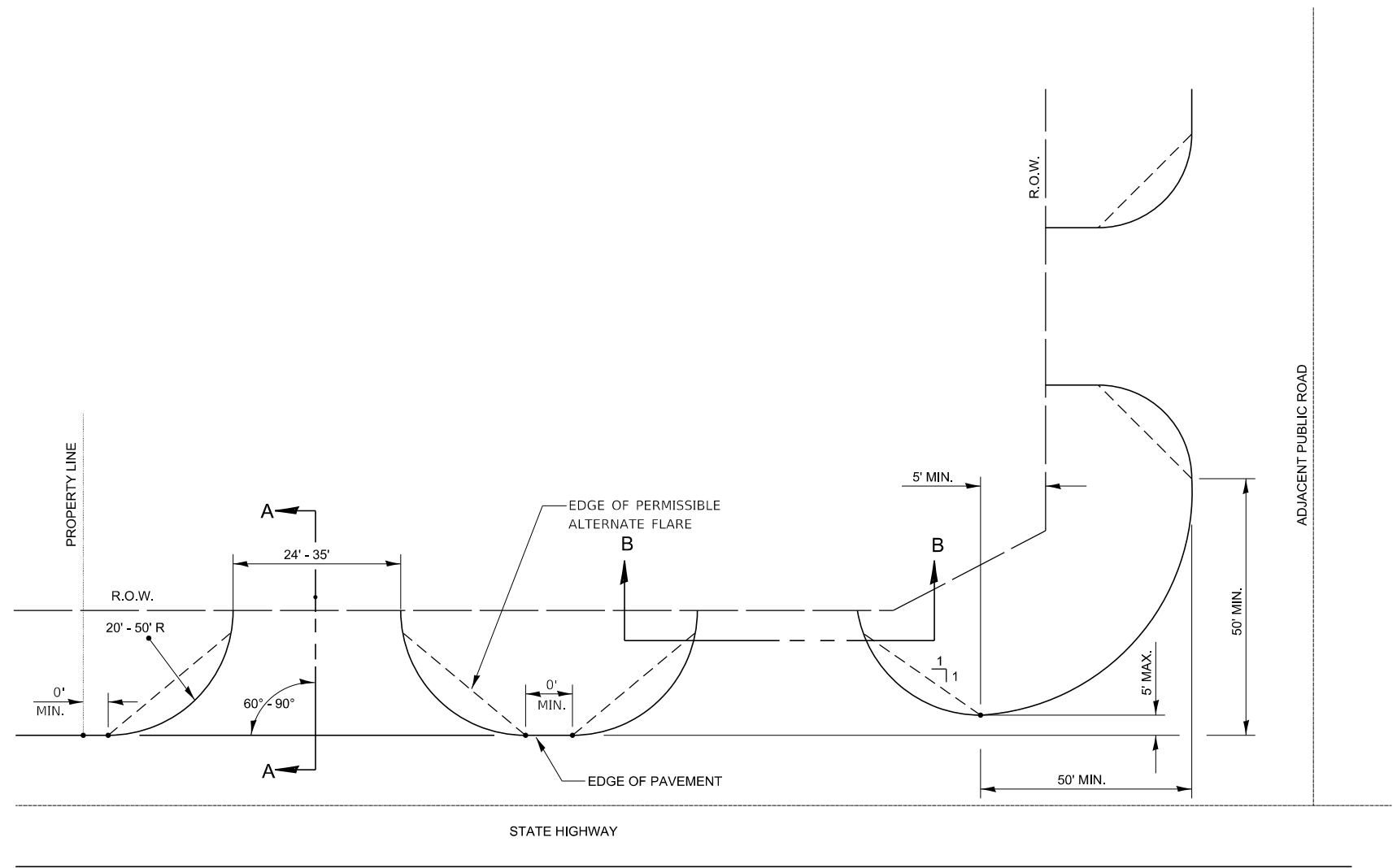
* SLOPE AWAY FROM HIGHWAY SURFACE AT 1.5% MINIMUM TO 10% MAXIMUM FOR A DISTANCE OF 10' OR TO CENTERLINE OF DITCH.



SECTION B-B

DESIGNER NOTES:

- 1.) ALL ENTRANCES SHALL BE SURFACED FROM THE EDGE OF THE HIGHWAY PAVEMENT TO THE RIGHT OF WAY LINE.
- 2.) IDENTIFY PAVEMENT THICKNESS AND MATERIALS IF NOT IDENTIFIED IN PLAN SHEETS/ SCHEDULES.



TYPICAL COMMERCIAL - RURAL

MODEL det 3 dch11b
FILE Name: p:\Illinois\Bentley.com\PI\DOT\Documents\DOT Offices\District 3\Standards - District 3\DETAILS\DISTRICT 3 STANDARD DETAILS.DGN\MO-499 SURF CSE PAVT REHAB & SHLD.dgn

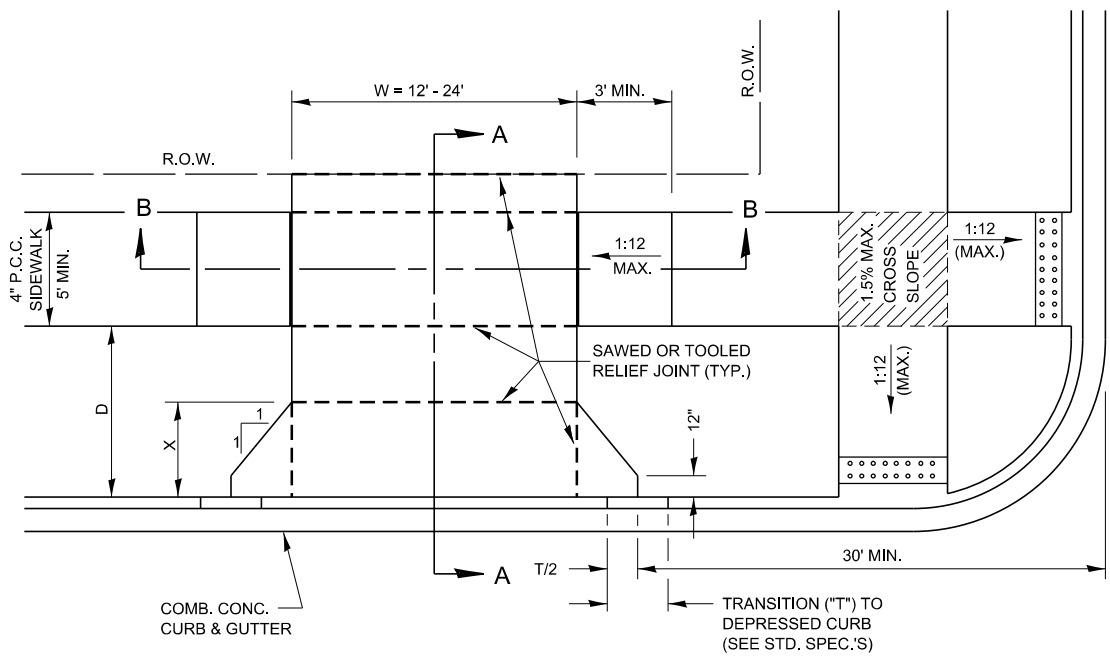
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	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

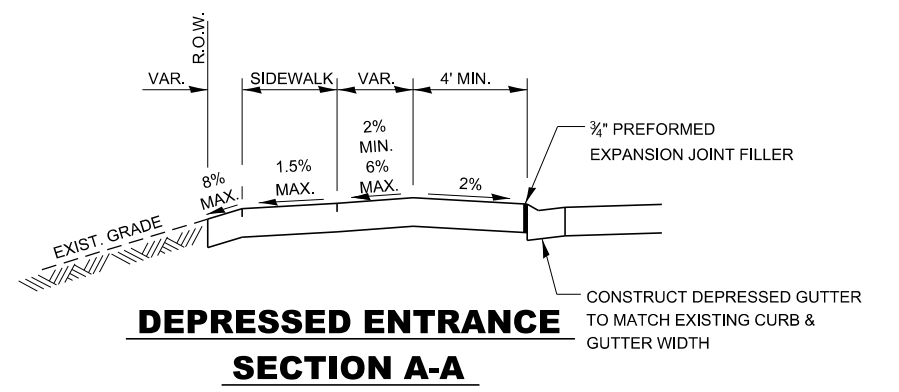
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

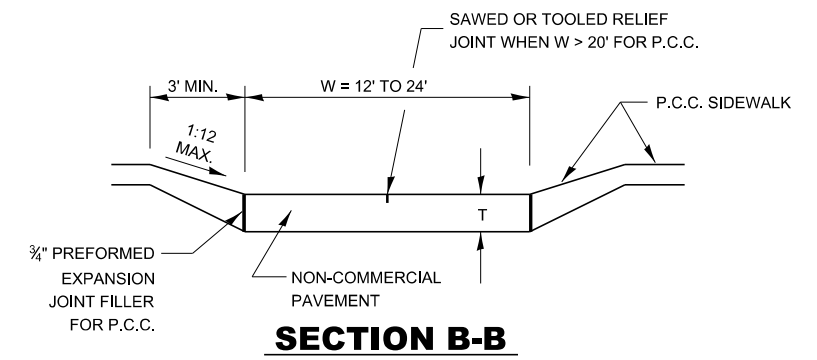
400-2



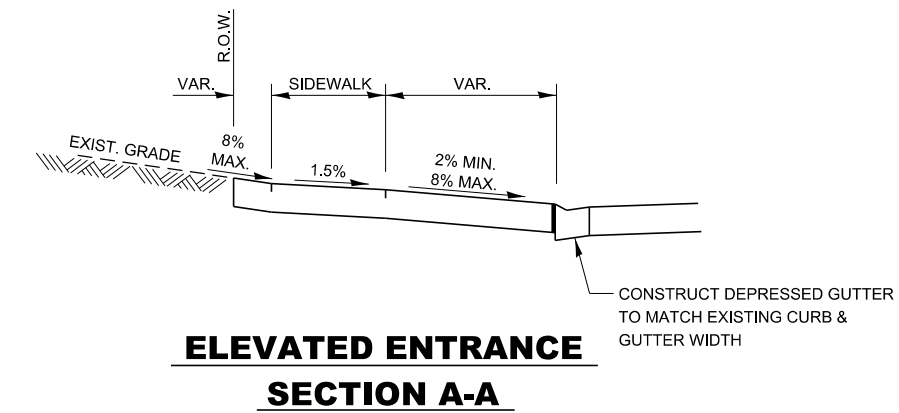
TYPICAL NON-COMMERCIAL P.C.C. ENTRANCE DETAIL - URBAN



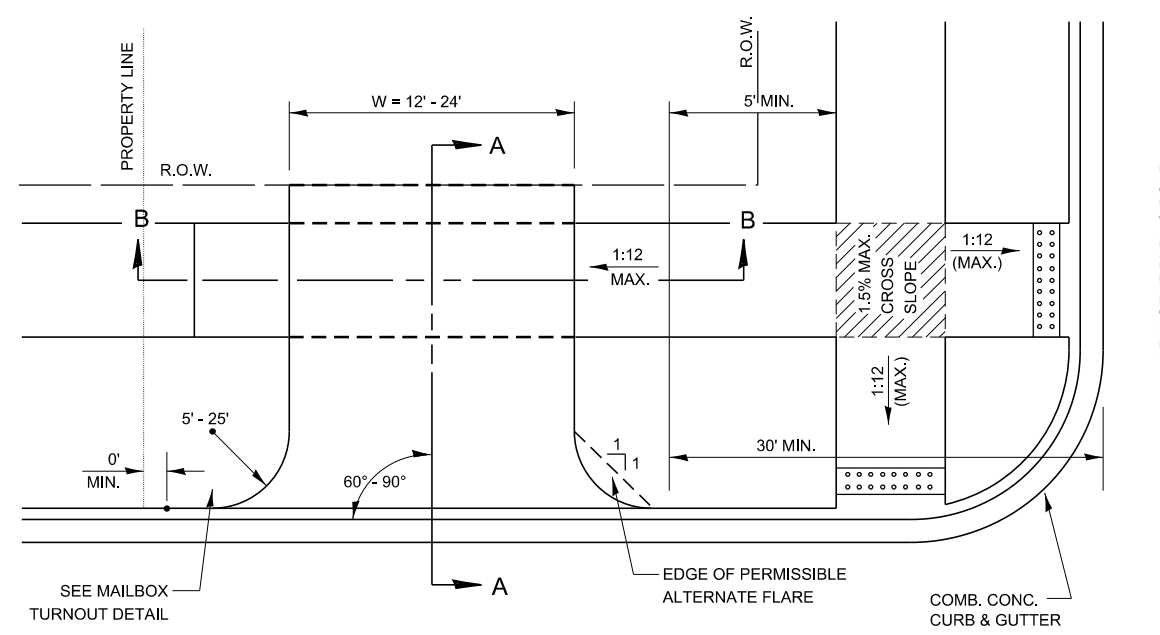
DEPRESSED ENTRANCE SECTION A-A



SECTION B-B



ELEVATED ENTRANCE SECTION A-A



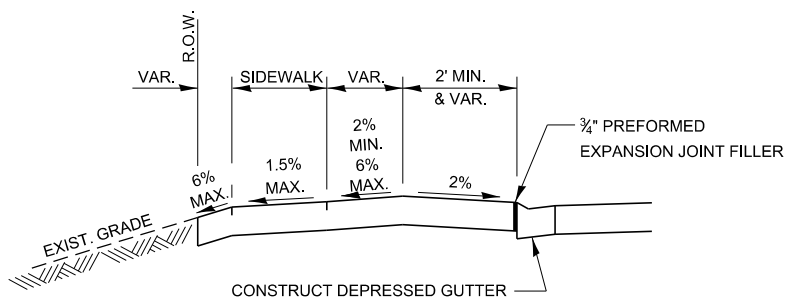
TYPICAL NON-COMMERCIAL HMA ENTRANCE DETAIL - URBAN

- NOTES:
- X = 7' (NON-COMMERCIAL) (MINIMUM)
 - INCLUDE EXPANSION JOINTS AND RELIEF JOINTS IN P.C.C. DRIVEWAY PAVEMENT TO CONTROL CRACKING. COST INCLUDED IN COST OF ENTRANCE PAVEMENT.
 - AS THE DIMENSION "D" APPROACHES A MINIMUM OF 2', THE TRANSITION TO DEPRESSED CURB SHALL BE NO STEEPER THAN 1V:12H.

- DESIGNER NOTES:
- INSTALL MAILBOX TURNOUT PER DETAIL 406-14 IF DELIVERY IS ON-ROAD BY POST MASTER.

MODEL det 3 dwt 1b
FILE: \\pawbentley.com\PI\PIDOT\Documents\DOT Office\District 3\Standards - District 3\DETAILS\DISTRICT 3 STANDARD DETAILS.DGN\400-399_SURE_CSE_PAVT_REHAB & SHLD.dgn

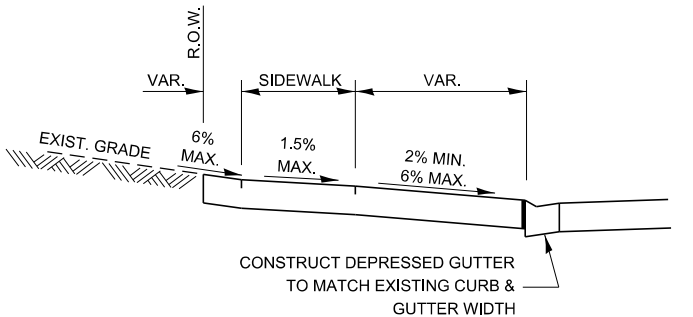
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SCALE:				SHEET OF SHEETS								STA.		TO STA.		CONTRACT NO.					
												ILLINOIS		FED. AID PROJECT							



DEPRESSED ENTRANCE - WITH DEPRESSED CURB

SECTION A-A

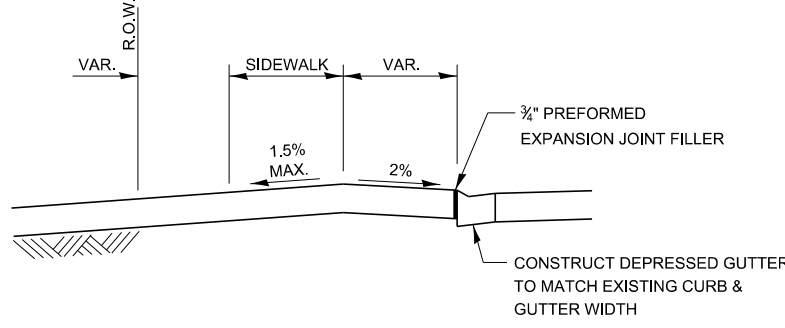
OPTION 1



ELEVATED ENTRANCE - WITH DEPRESSED CURB

SECTION A-A

OPTION 2

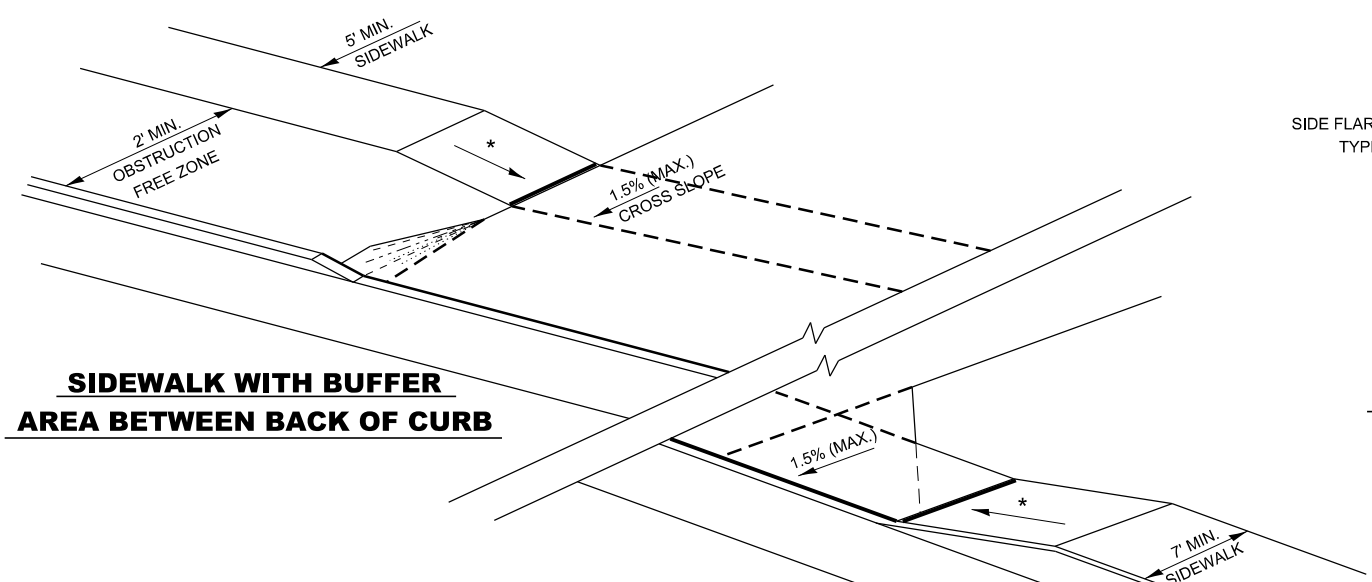


LEVEL ENTRANCE - WITH DEPRESSED CURB

SECTION A-A

OPTION 3

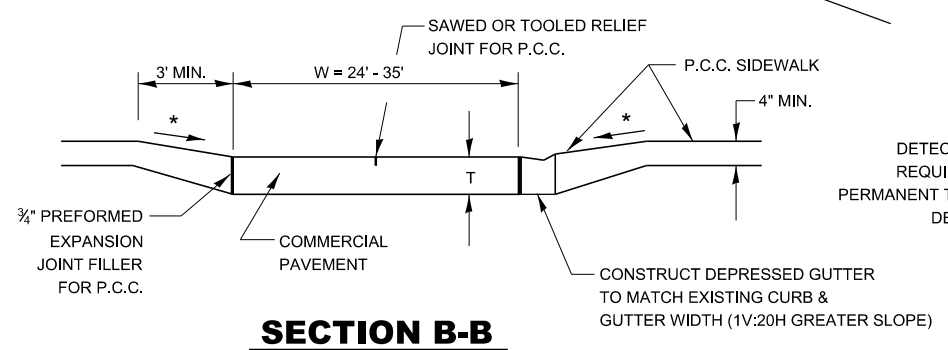
- NOTES:
1. ALL ENTRANCES SHALL BE SURFACED FROM THE EDGE OF PAVEMENT TO THE RIGHT OF WAY.
 2. COST OF EXPANSION JOINTS AND RELIEF JOINTS SHALL BE INCLUDED IN THE COST OF THE PCC DRIVEWAY PAVEMENT.



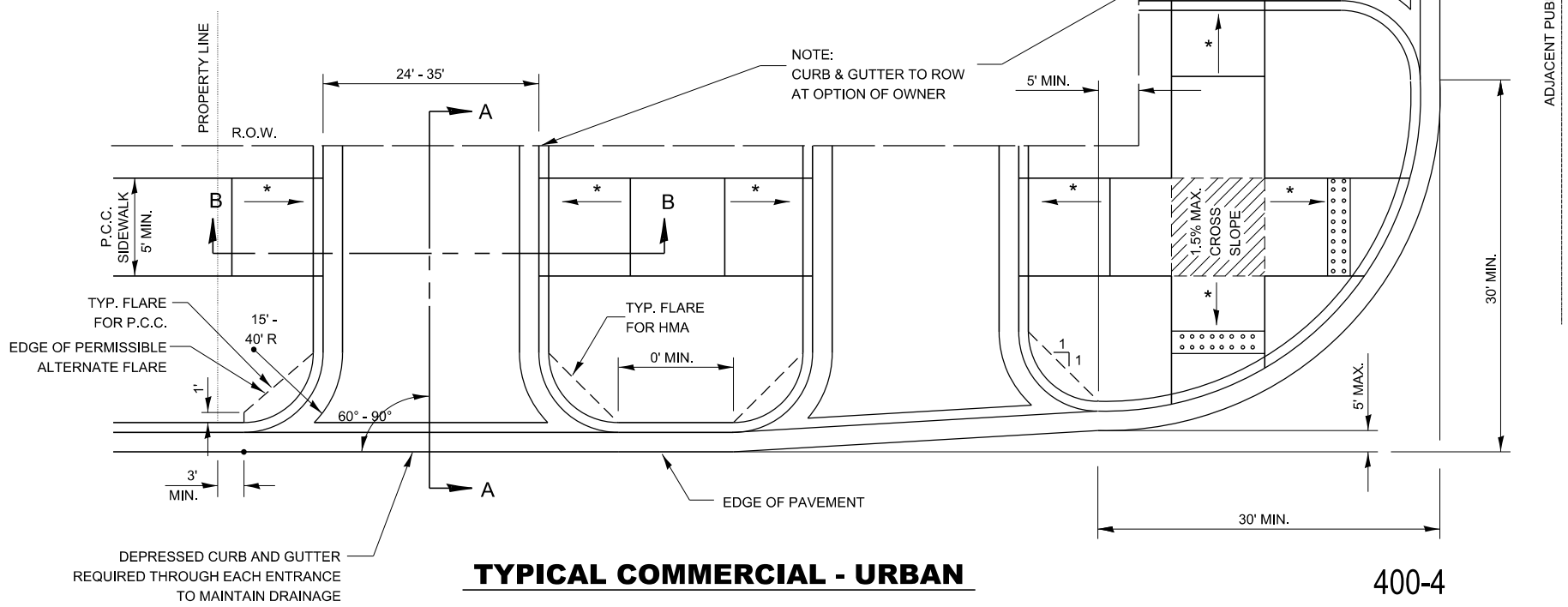
SIDEWALK WITH BUFFER AREA BETWEEN BACK OF CURB

ADA SIDEWALK DETAIL

SIDEWALK ADJACENT TO CURB & GUTTER

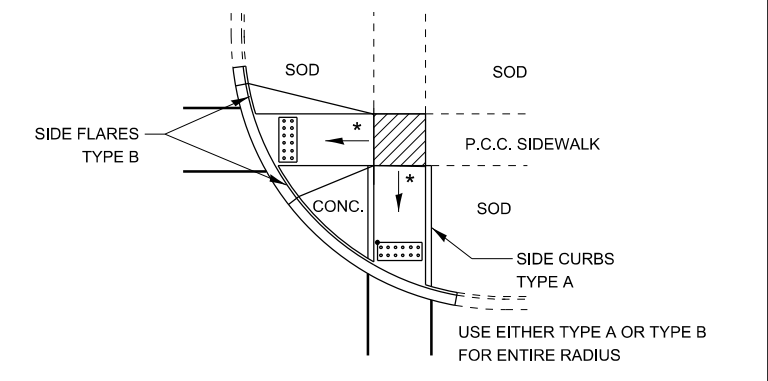


SECTION B-B



TYPICAL COMMERCIAL - URBAN

400-4



TYPICAL ADA CURB APPLICATION

LEGEND

▨ SLOPE = 1.5%

* SLOPE = 1V:12H MAX.

DETECTABLE WARNINGS PER STATE HIGHWAY STANDARD 424001 PER ADAAG - "SHALL CONTRAST VISUALLY" (TYPICALLY RED-BRICK COLOR)

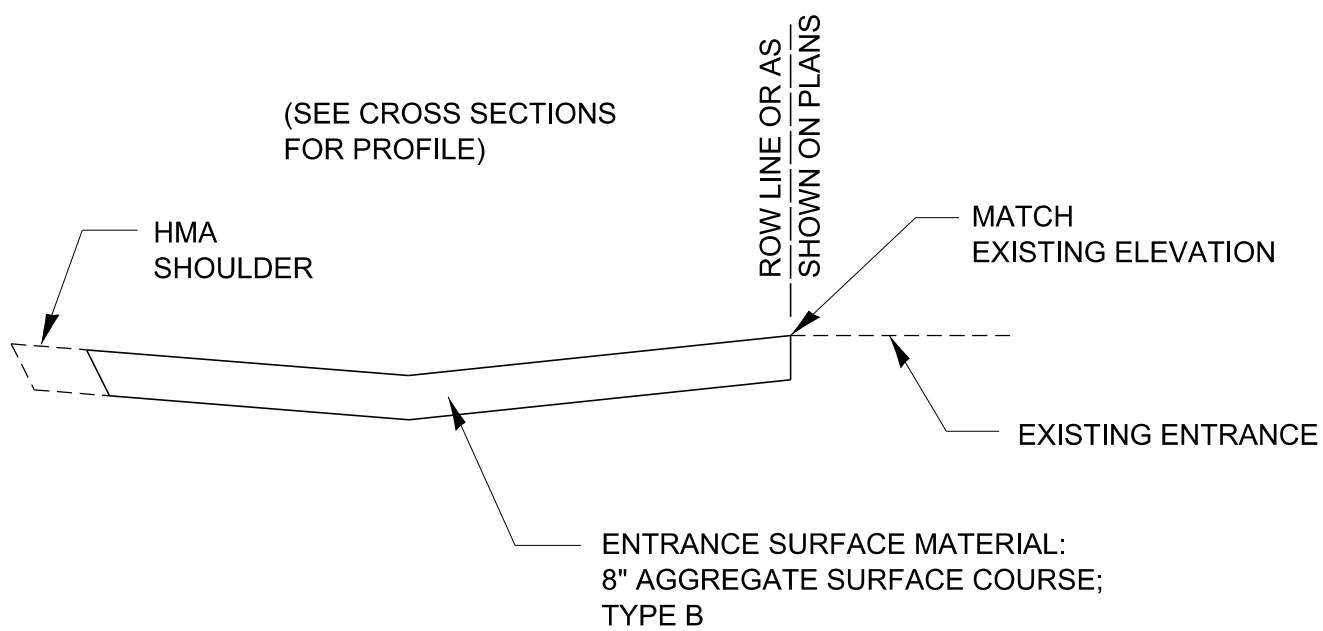
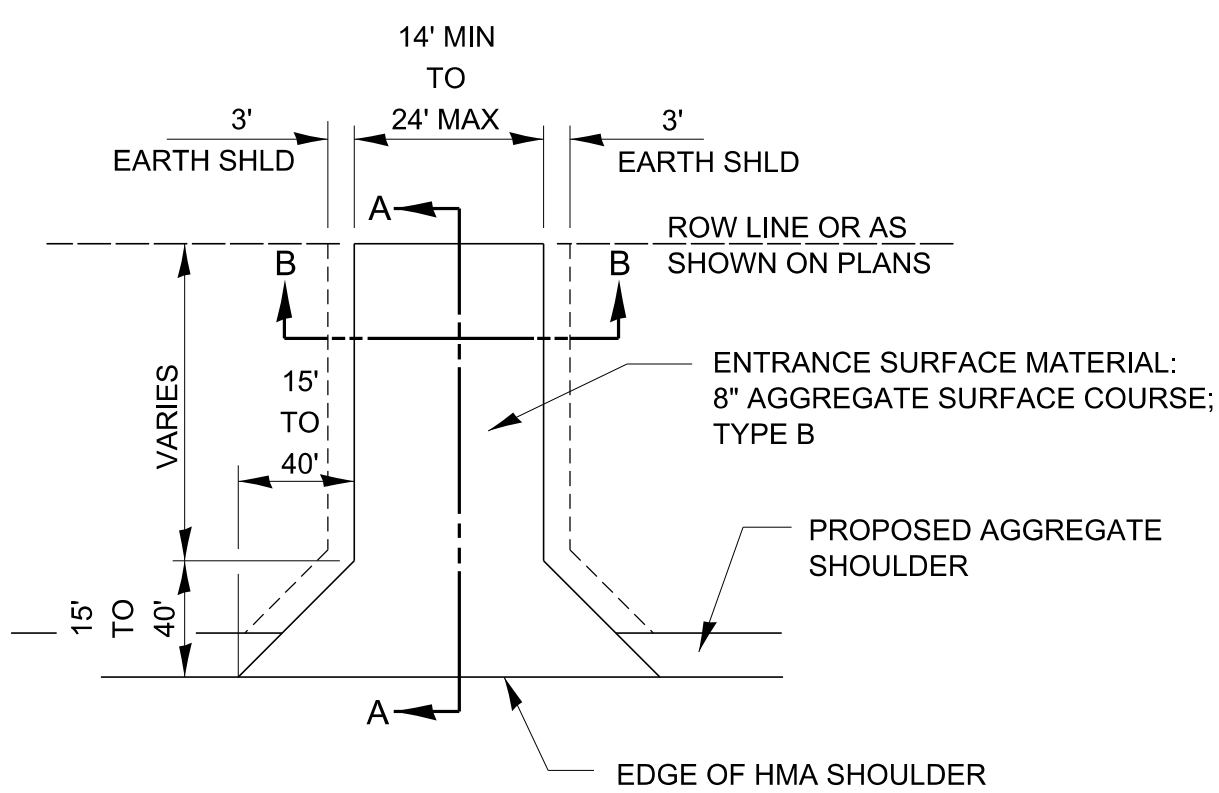
MODEL.dwg 3 14 11
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USER NAME = ronald.pohar	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/15/2024	DATE -	REVISED -

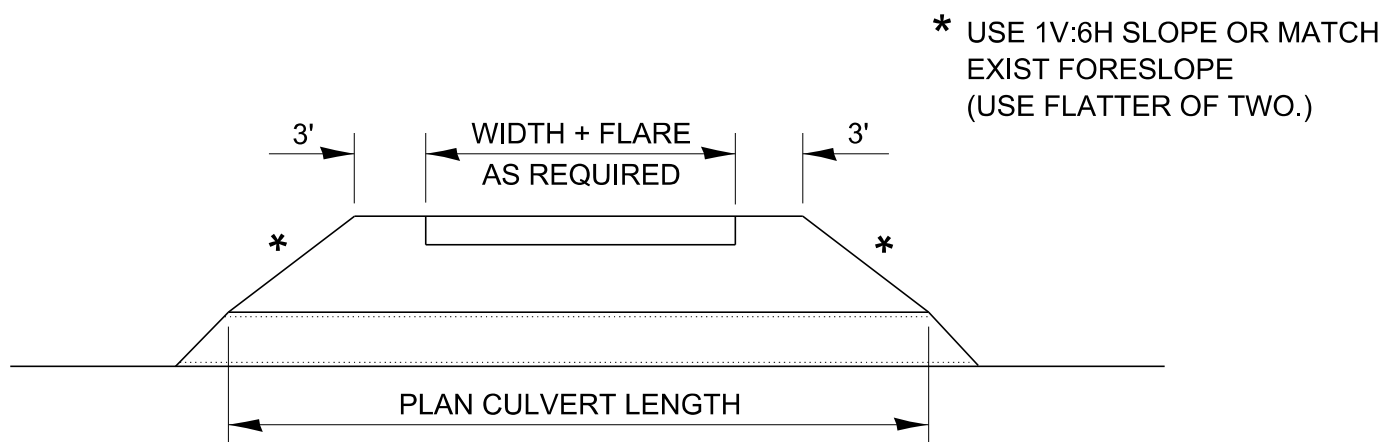
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



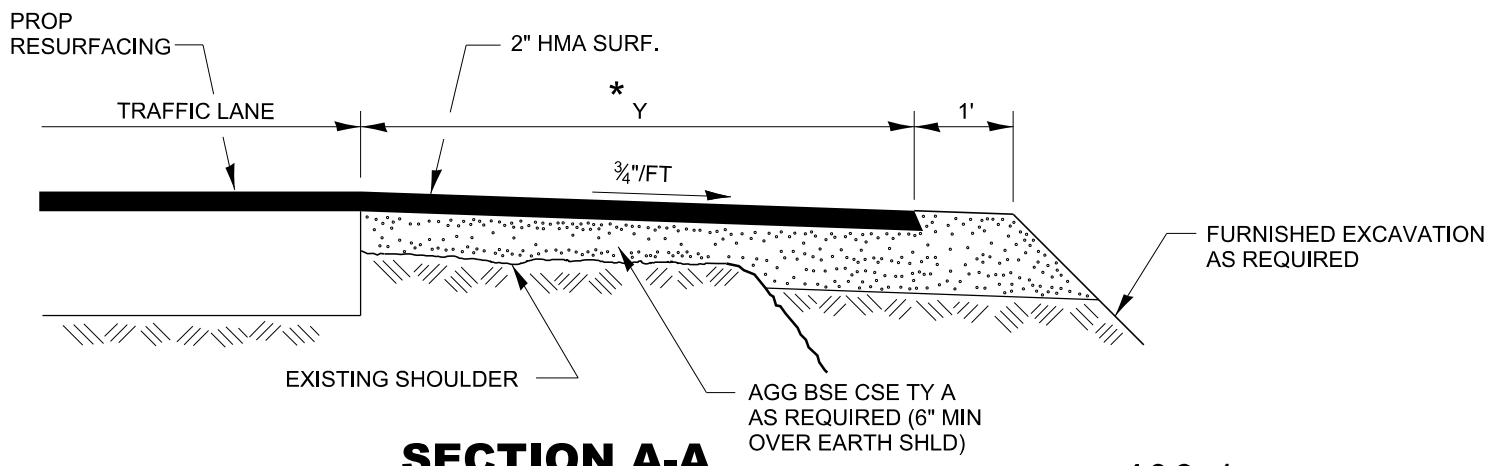
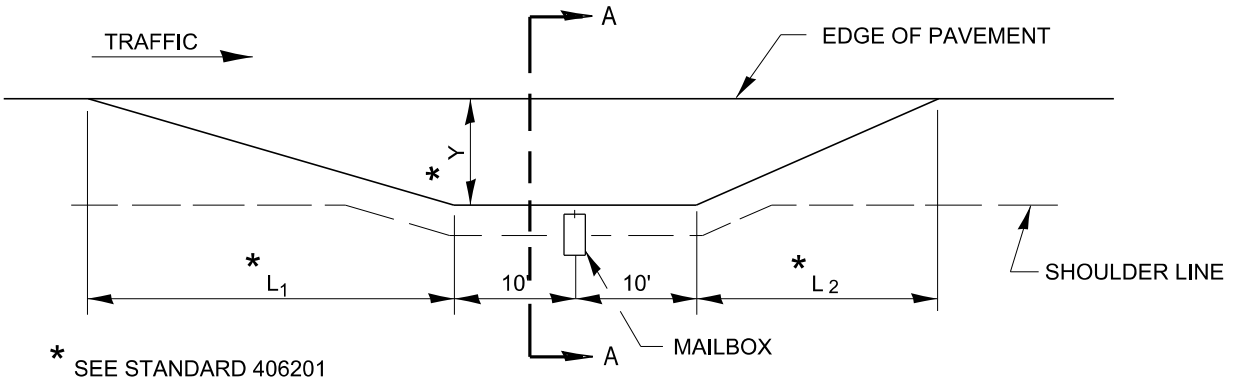
SECTION A-A



SECTION B-B

FIELD ENTRANCE DETAIL

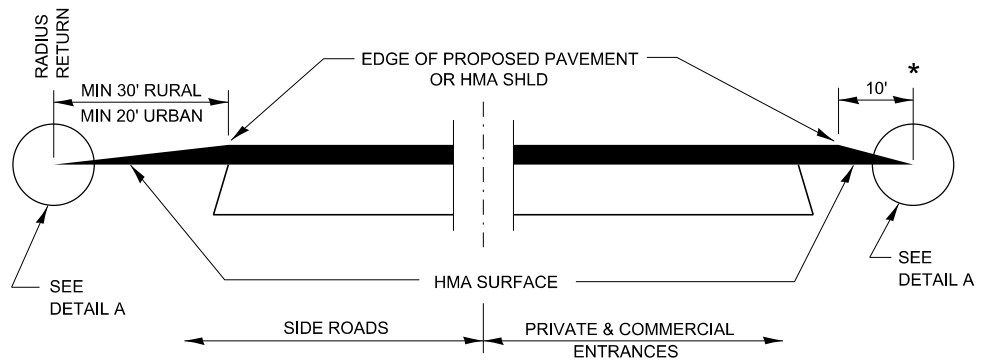
DESIGNER NOTE: TO BE USED ON 3R PROJECTS WHEN REPLACING CULVERTS AND CONSTRUCTING NEW DITCHES. WIDTH CAN BE INCREASED TO 30' FOR LARGE FARM EQUIPMENT ENTRANCES.



406-1

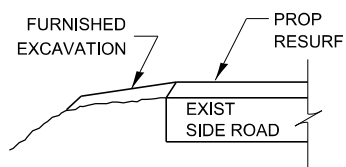
RURAL MAILBOX TURNOUT DETAILS

- DESIGNER NOTES
1. INCLUDE STD. 406201
 2. REVIEW BDE MANUAL 58-5



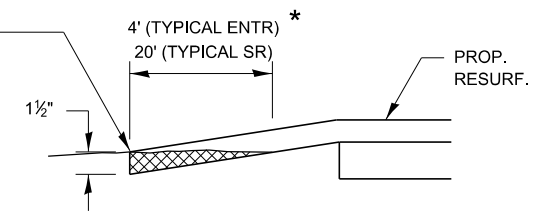
SECTION A-A

DETAILS AT ENTRANCES & SIDE ROADS

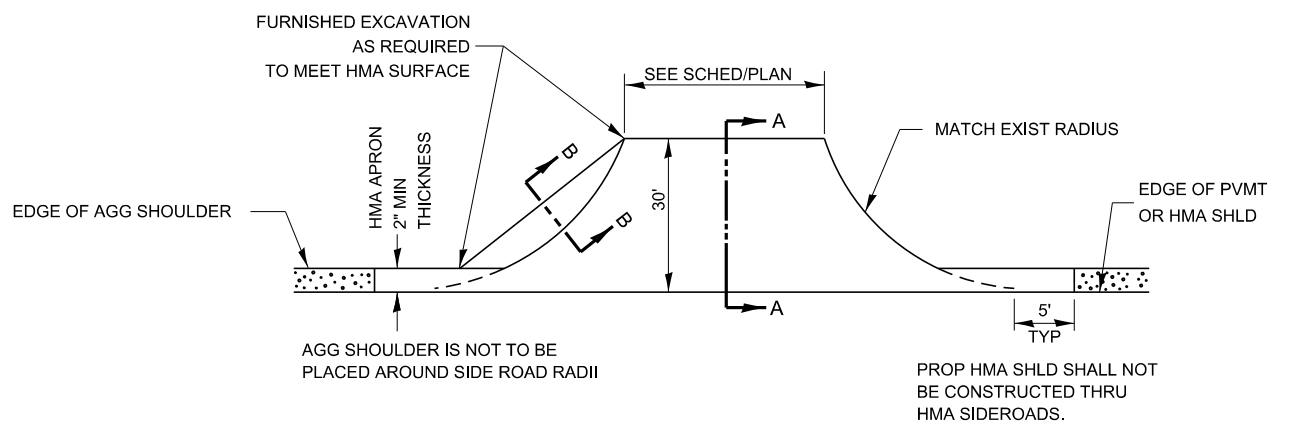


SECTION B-B

THE COST OF REMOVAL AT EXISTING HMA OR PCC LOCATIONS SHALL BE PAID FOR PER SQ YD BY THE APPROPRIATE PAY ITEM. REMOVAL AT THE EXISTING AGG LOCATIONS SHALL BE INCIDENTAL TO THE HMA. A-3 LOCATIONS SHALL BE FEATHER TAPERED.



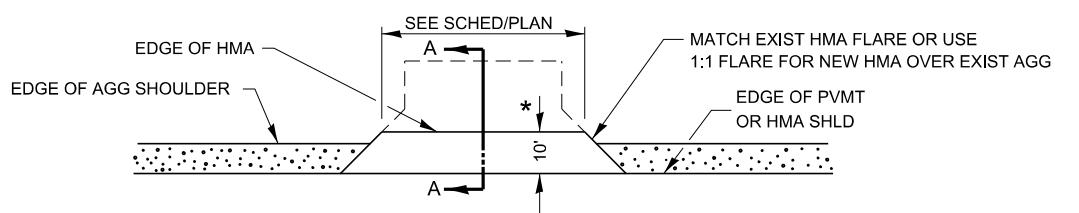
DETAIL A



PLAN AT SIDE ROADS

DESIGNER NOTES:

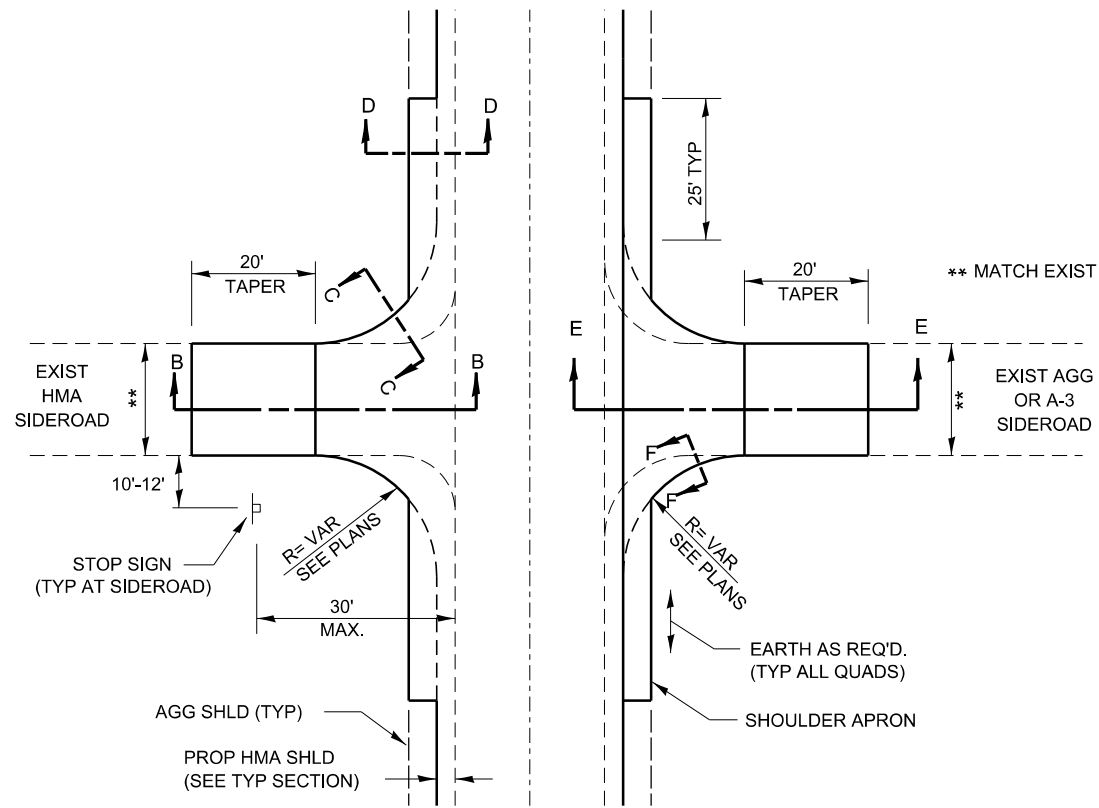
1. THIS IS FOR USE ON BASIC RURAL RS AND W&RS PROJECTS.
2. FURNISHED EXCAVATION SHALL BE INCLUDED AS A PAY ITEM.
3. ALL ENTRANCE AND SIDEROAD WIDTHS, EXISTING MATERIAL TYPE, AND QUANTITIES MUST BE SHOWN IN THE SCHEDULE OR ON THE PLAN VIEW SHEETS.



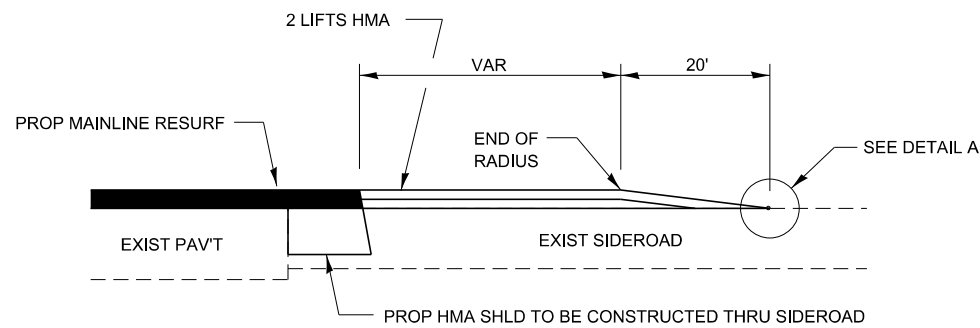
PLAN AT PRIVATE & COMMERCIAL ENTRANCES

(DO NOT RESURFACE FIELD ENTRANCES)

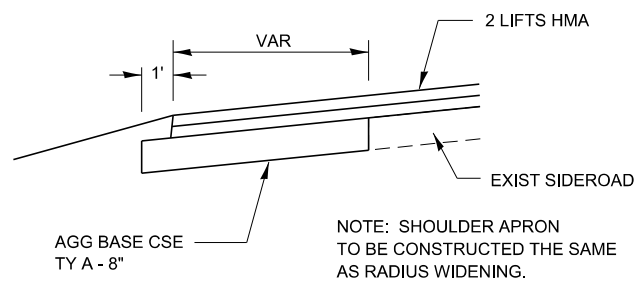
* PROPOSED HMA RESURFACING AT PUBLIC EDUCATIONAL FACILITY ENTRANCES SHALL BE EXTENDED TO THE RIGHT-OF-WAY LIMITS.



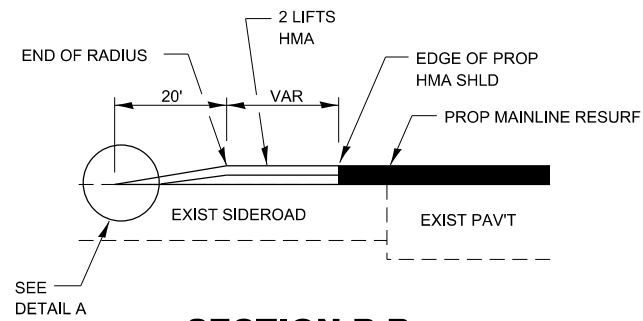
SIDEROAD DETAIL



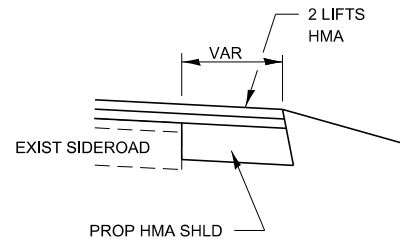
SECTION E-E



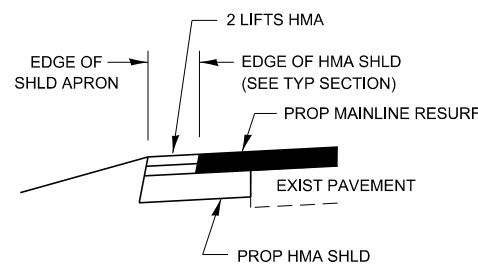
SECTION F-F



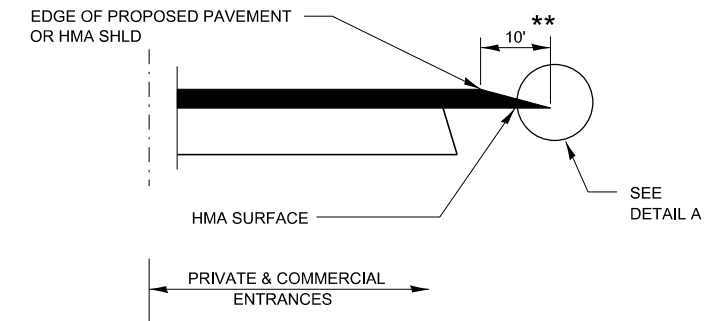
SECTION B-B



SECTION C-C

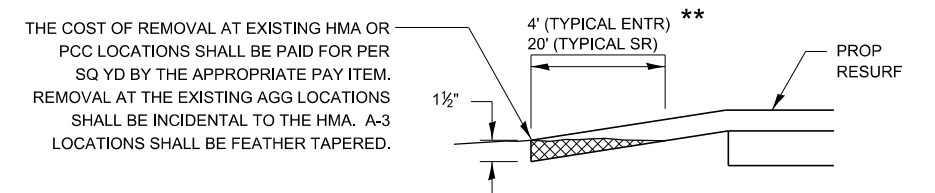


SECTION D-D



SECTION A-A

DETAILS AT ENTRANCES

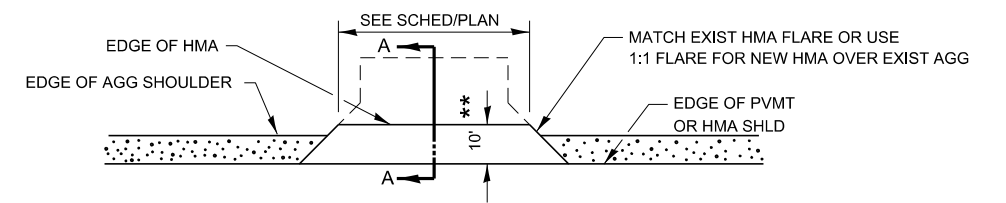


DETAIL A

** PROPOSED HMA RESURFACING AT PUBLIC EDUCATIONAL FACILITY ENTRANCES SHALL BE EXTENDED TO THE RIGHT-OF-WAY LIMITS.

DESIGNER NOTES:

1. THIS IS FOR USE ON RURAL RESURFACINGS WHEN SIDEROAD RADII ARE TO BE RECONSTRUCTED.
2. THE ADEQUACY OF SIDEROADS SHALL BE VERIFIED IN THE FIELD. SIDEROAD RECONSTRUCTION MAY BE REQUIRED.
3. INCREASING THE HMA SHOULDER THICKNESS MAY BE REQUIRED AT HIGH VOLUME SIDEROADS.
4. ALL ENTRANCE AND SIDEROAD WIDTHS, EXISTING MATERIAL TYPE, AND QUANTITIES MUST BE SHOWN IN THE SCHEDULE OR ON THE PLAN VIEW SHEETS.



PLAN AT PRIVATE & COMMERCIAL ENTRANCES

(DO NOT RESURFACE FIELD ENTRANCES)

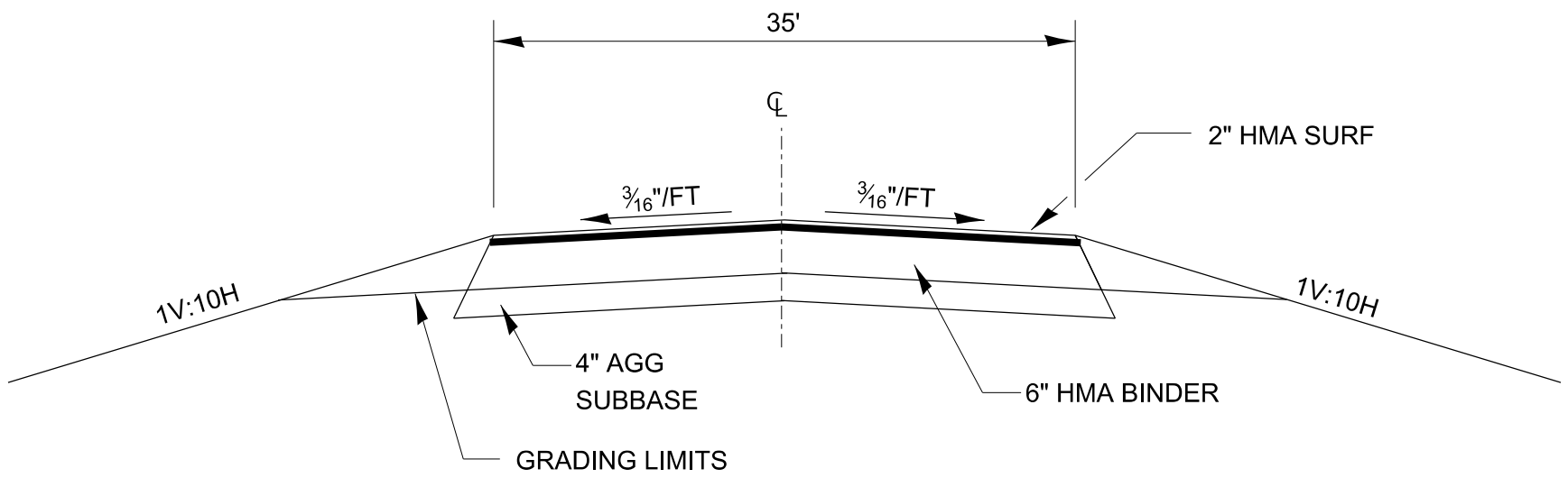
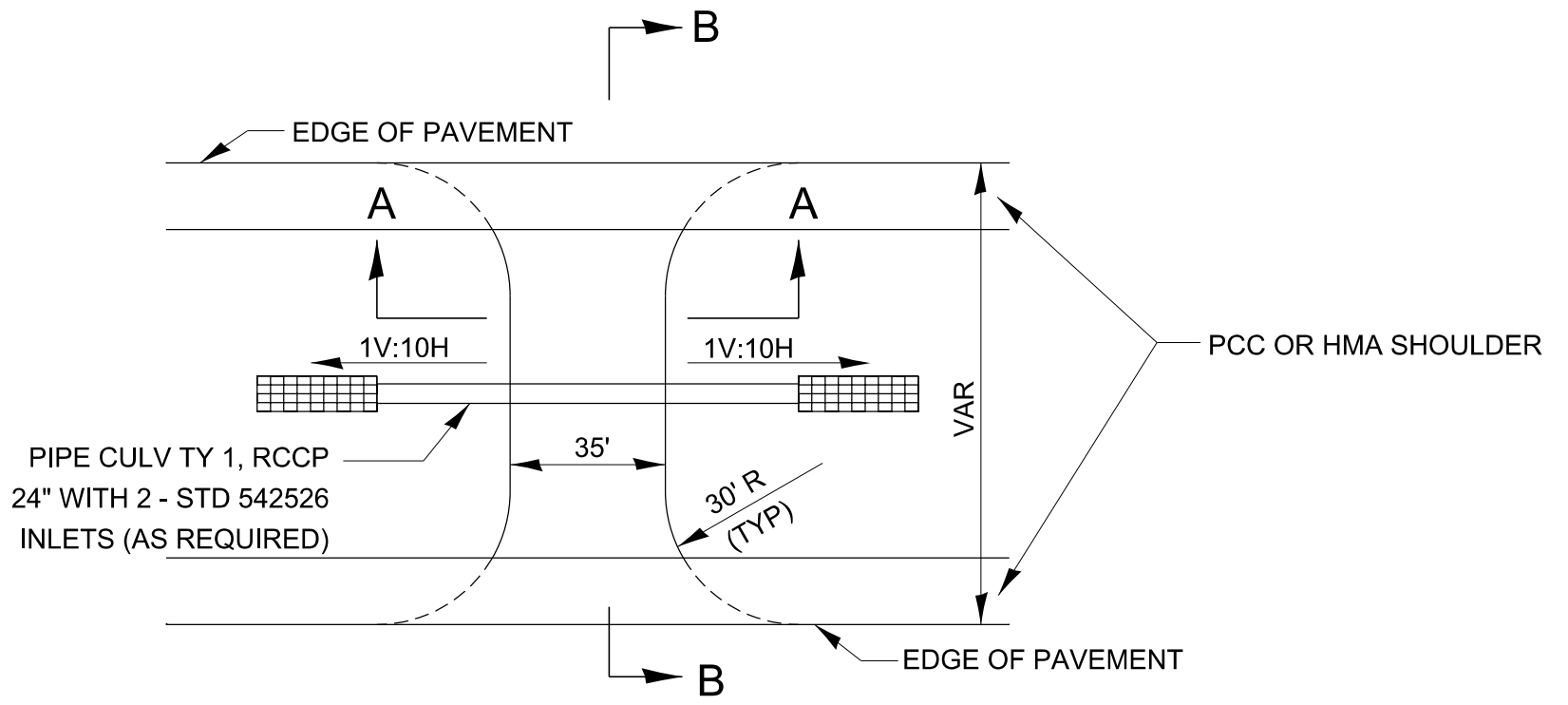
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USER NAME = ronald.pohar	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/15/2024	DATE -	REVISED -

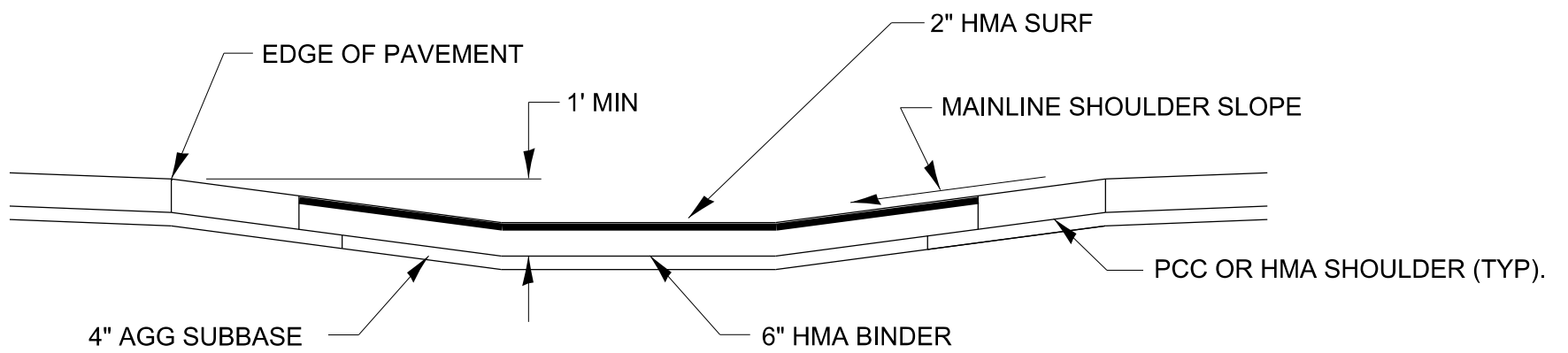
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

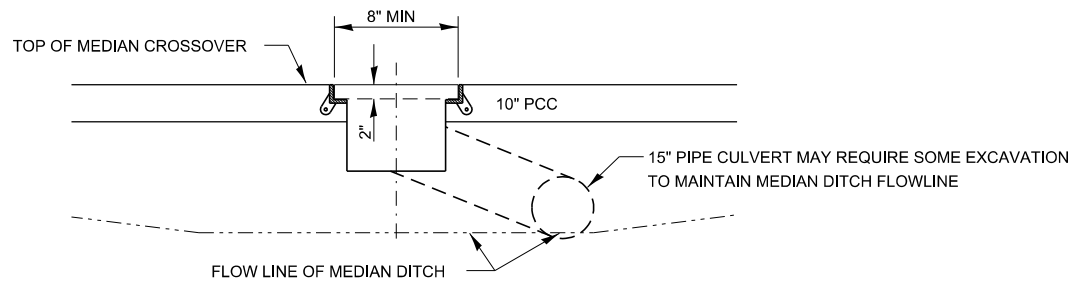


SECTION A-A

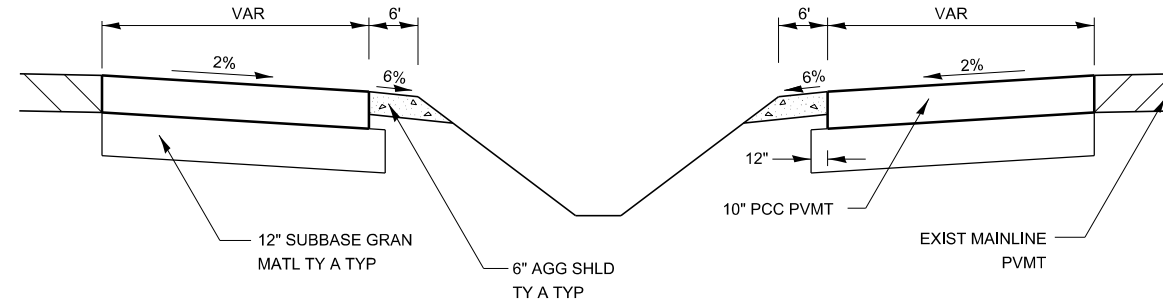
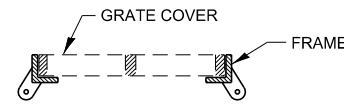


SECTION B-B

HMA MAINTENANCE CROSSOVER



SECTION A-A

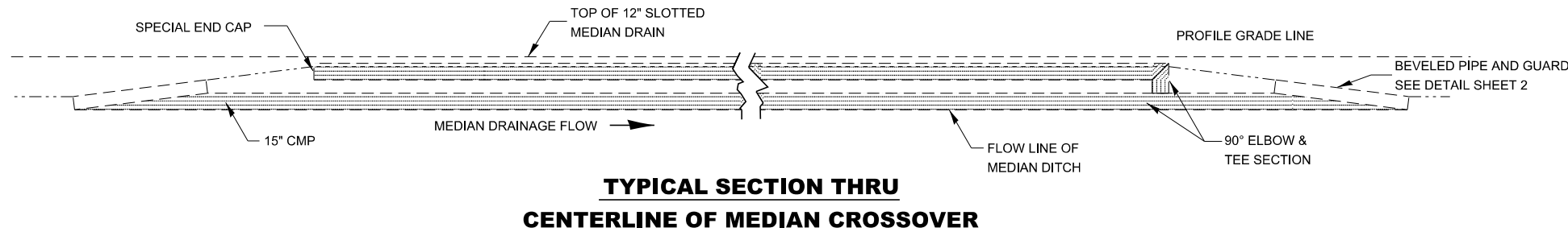


TYPICAL SECTION

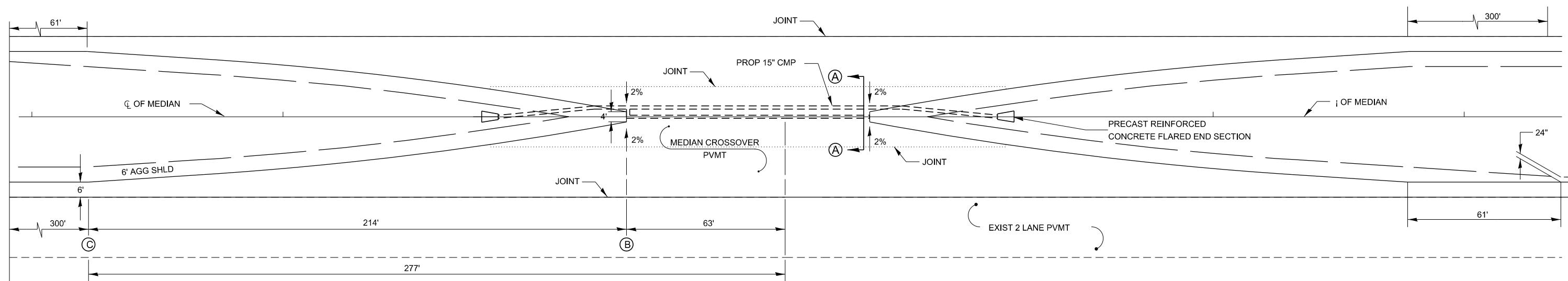
TABLE OF OFFSETS AND DROPS											
DISTANCE FROM LOCATION STATION	0	63.17'	75'	100'	125'	150'	175'	200'	225'	250'	277.49'
OFFSETS FROM INSIDE EDGE OF PAVEMENT	32'	30'	27.86'	23.67'	19.93'	16.64'	13.80'	11.40'	9.44'	7'	6'
DROP FROM INSIDE EDGE OF PAVEMENT	.64'	.60'	.56'	.47'	.40'	.33'	.28'	.23'	.19'	.16'	.24'

(B)

(C)



TYPICAL SECTION THRU CENTERLINE OF MEDIAN CROSSOVER



TYPICAL PLAN

1" = 20'

GENERAL NOTES:

UNLESS OTHERWISE SPECIFIED, WHEN THE MEDIAN CROSSOVER IS TO BE REMOVED, THE CONTRACTOR SHALL BE REQUIRED TO SAW FULL-DEPTH ALONG THE SHOULDER LINE 6' FROM THE EDGE OF PAVEMENT. THE 6' ADJACENT TO THE EDGE OF PAVEMENT SHALL REMAIN IN PLACE AND BE USED AS SHOULDERS. THE COST OF SAWING SHALL BE INCLUDED IN THE PAVEMENT REMOVAL.

JOINTS SHALL BE SAWS AT A MAXIMUM OF 14' SPACING ACROSS THE MEDIAN WIDTH. ALL JOINTS SHALL BE SEALED.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL.

MODEL det 3 details
FILE Name: p:\u003cworkspace\benet\com\p\INDOT\Documents\DOT Offices\District 3\Standards - District 3\DETAILS\SUBSTRUCT 3 STANDARD DETAILS.DGN\MO-499 SURF CSE PAVT REHAB & SHLD.dgn

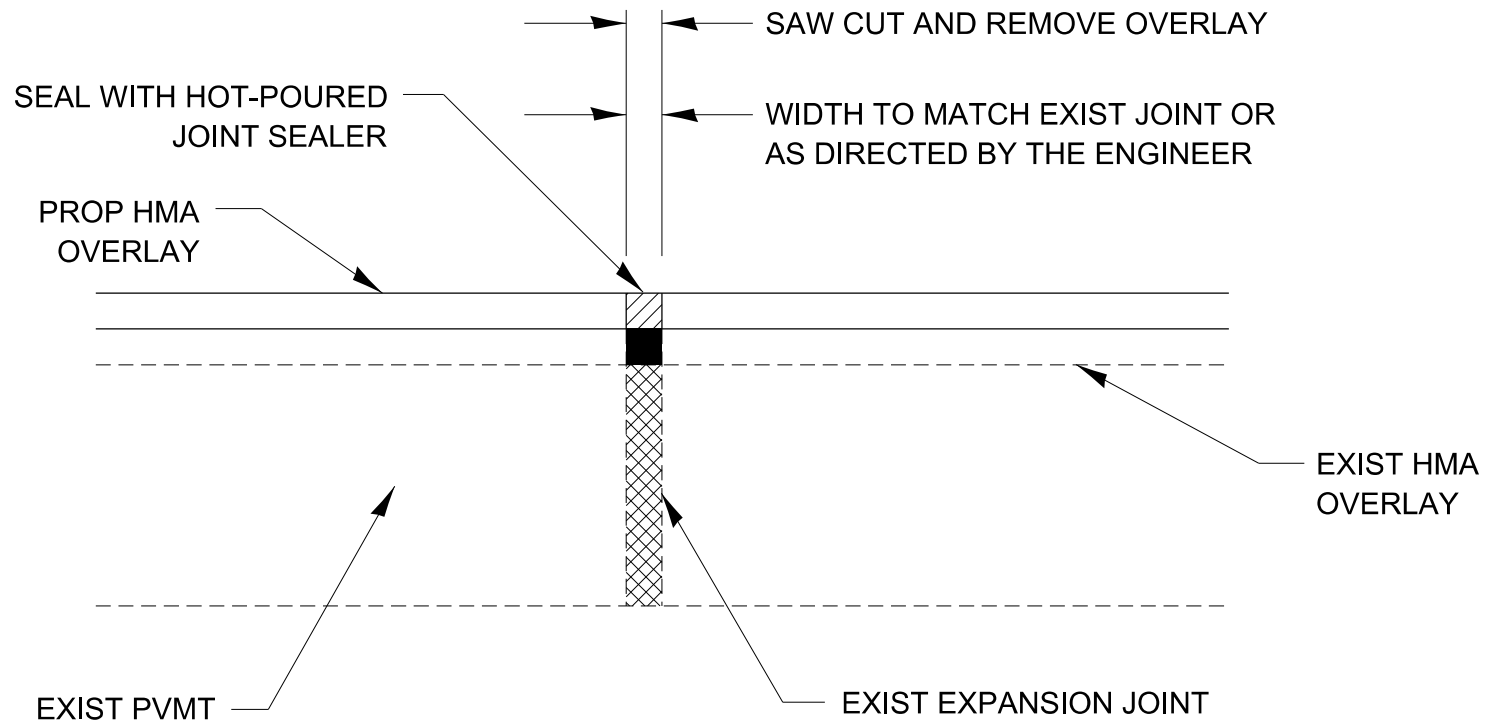
USER NAME = ronald.pohar	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

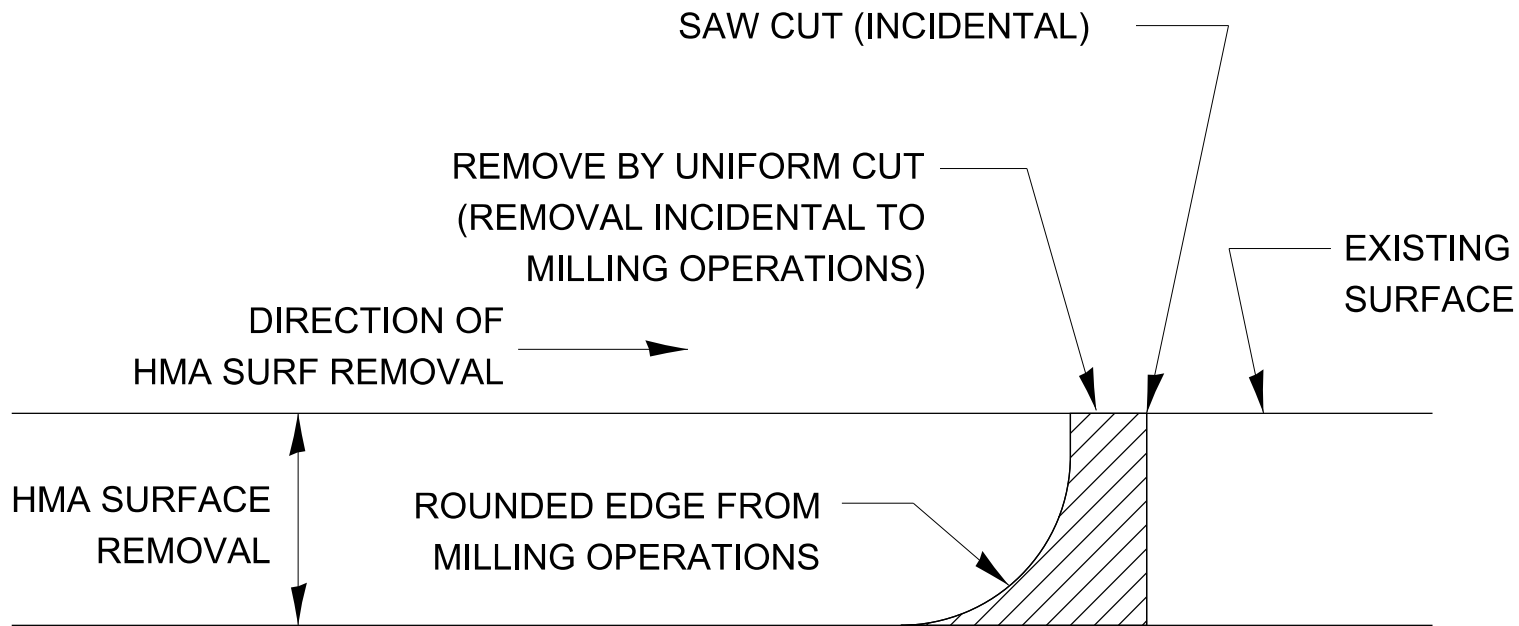
406-5



EXPANSION JOINT REHABILITATION DETAIL

GENERAL NOTES:

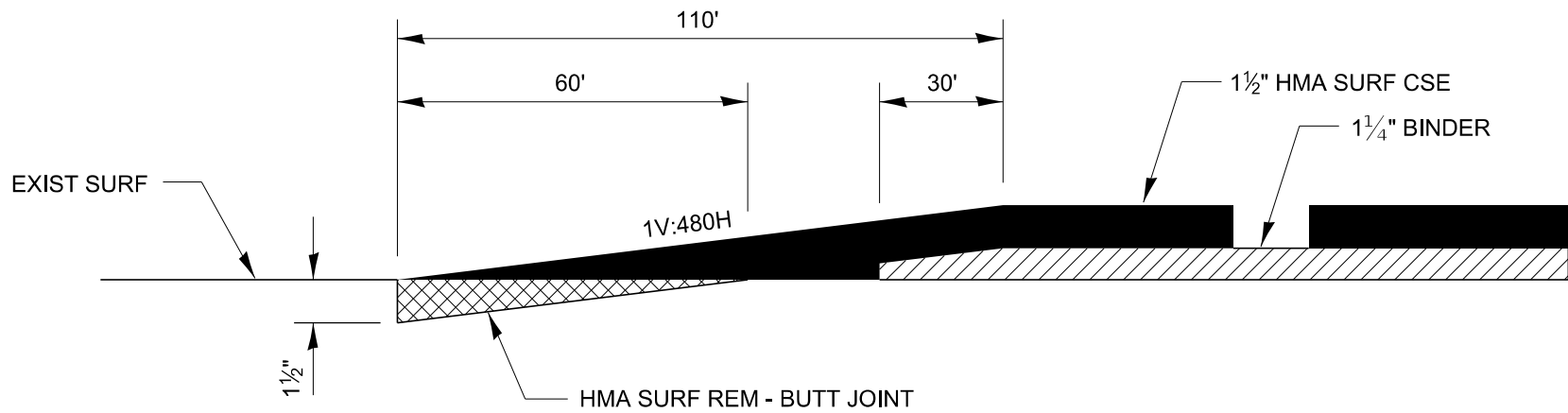
1. THE NEW HMA OVERLAY SHALL BE SAWED, REMOVED AND THE JOINT AREA CLEANED. PRIOR TO PLACING THE HOT-POURED JOINT SEALER, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR.
2. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR EXPANSION JOINT REHABILITATION.



NOTE:

WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE,
 THEN A SAW CUT SHALL BE USED TO MANUFACTURE
 A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL.
 THE ENGINEER SHALL BE THE SOLE JUDGE
 CONCERNING THE USE OF THIS DETAIL

HMA BUTT JOINT SAW CUTS

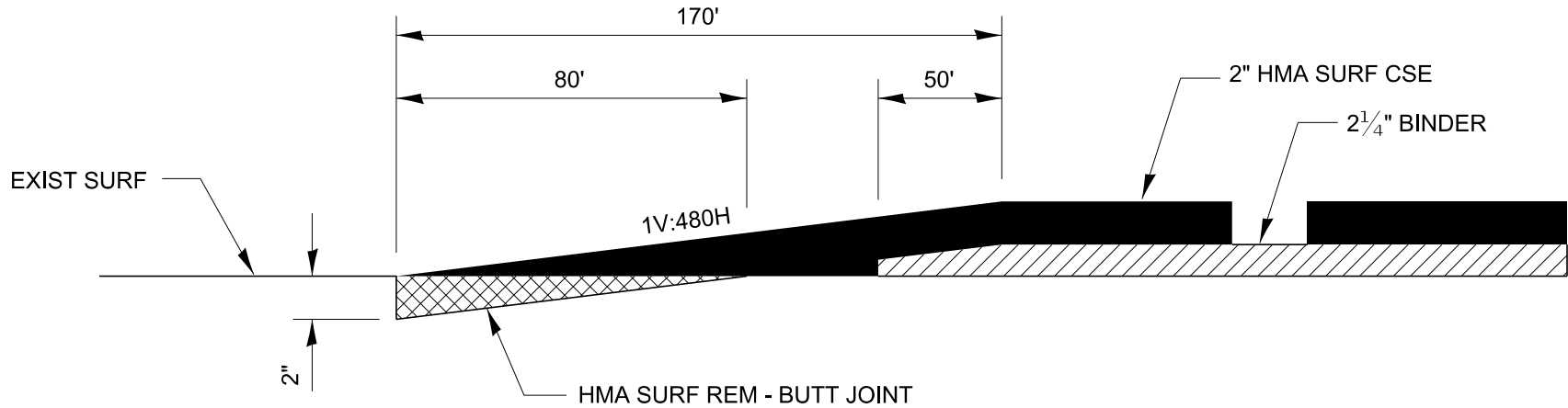


DESIGNER NOTES:

MODIFY TO MEET PROJECT SPECIFIC THICKNESS AND DIMENSIONS IF NEEDED

406-9

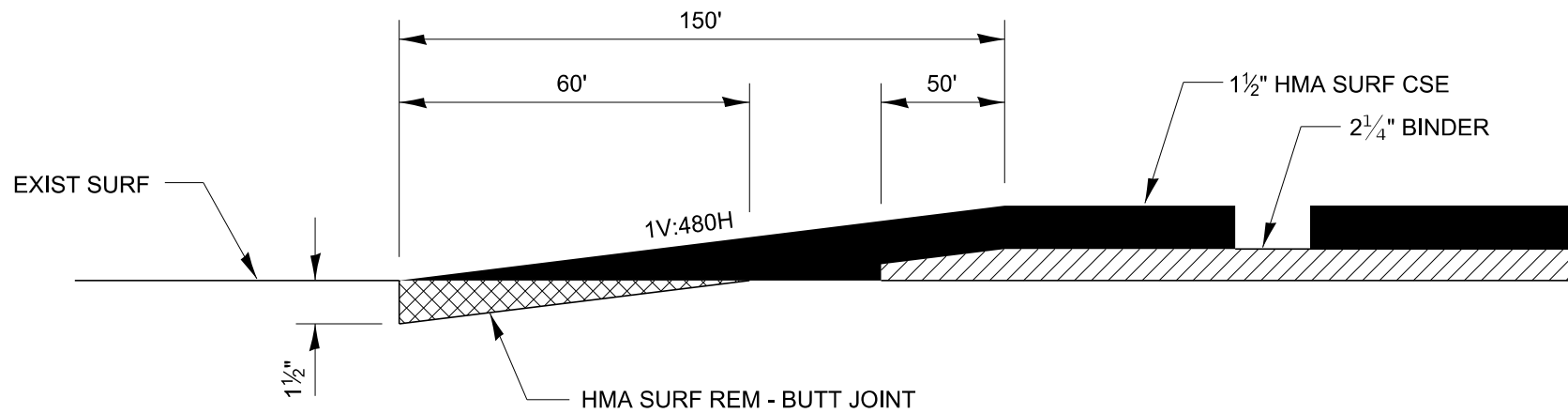
BUTT JOINT - NON INTERSTATE STANDARD OVERLAY



DESIGNER NOTE:
 MODIFY TO MEET PROJECT SPECIFIC THICKNESS AND DIMENSIONS IF NEEDED

406-10

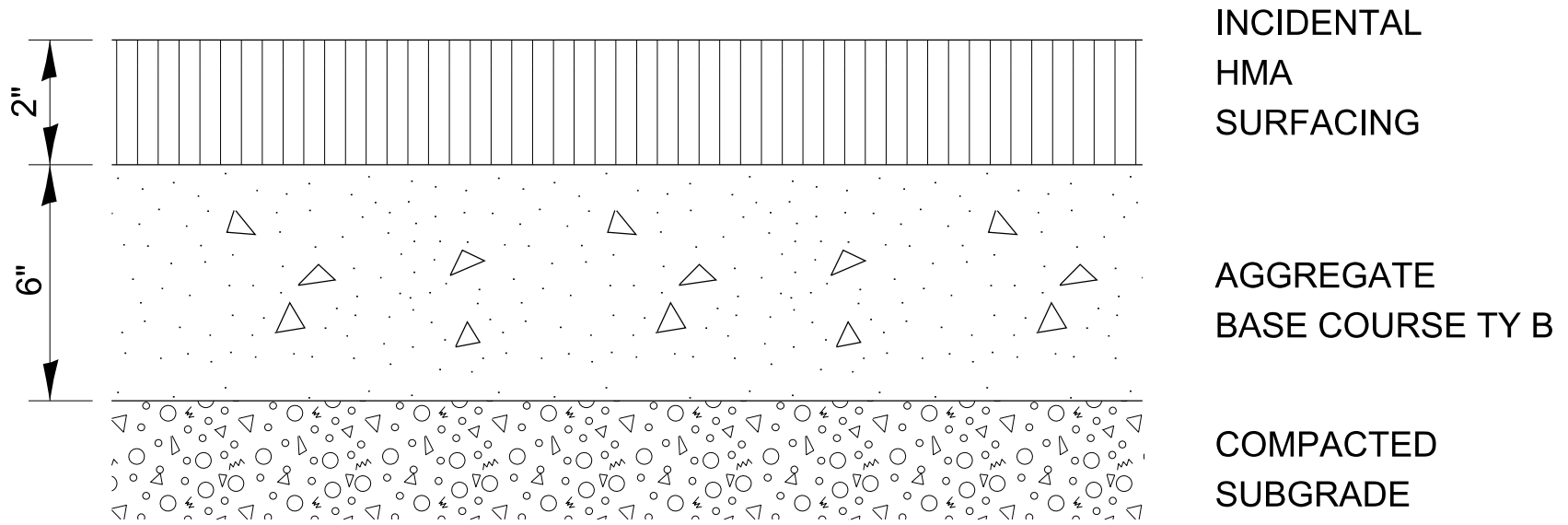
BUTT JOINT - INTERSTATE STANDARD OVERLAY



DESIGNER NOTE:
 MODIFY TO MEET PROJECT SPECIFIC THICKNESS AND DIMENSIONS IF NEEDED

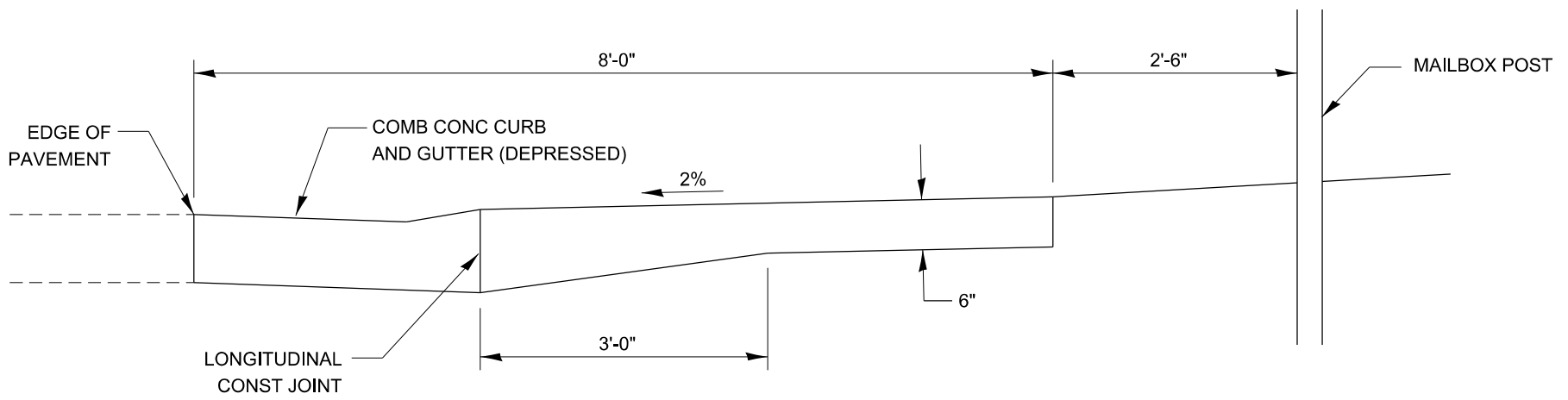
406-11

BUTT JOINT - NON INTERSTATE DESIGNED OVERLAY

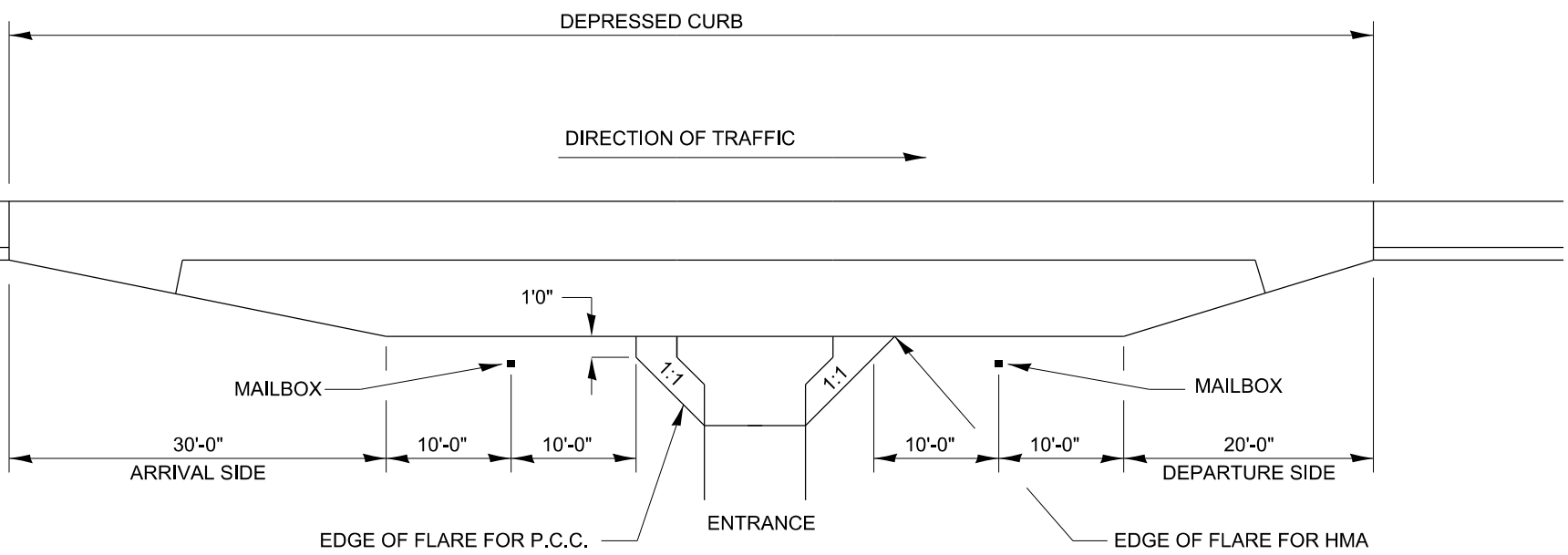
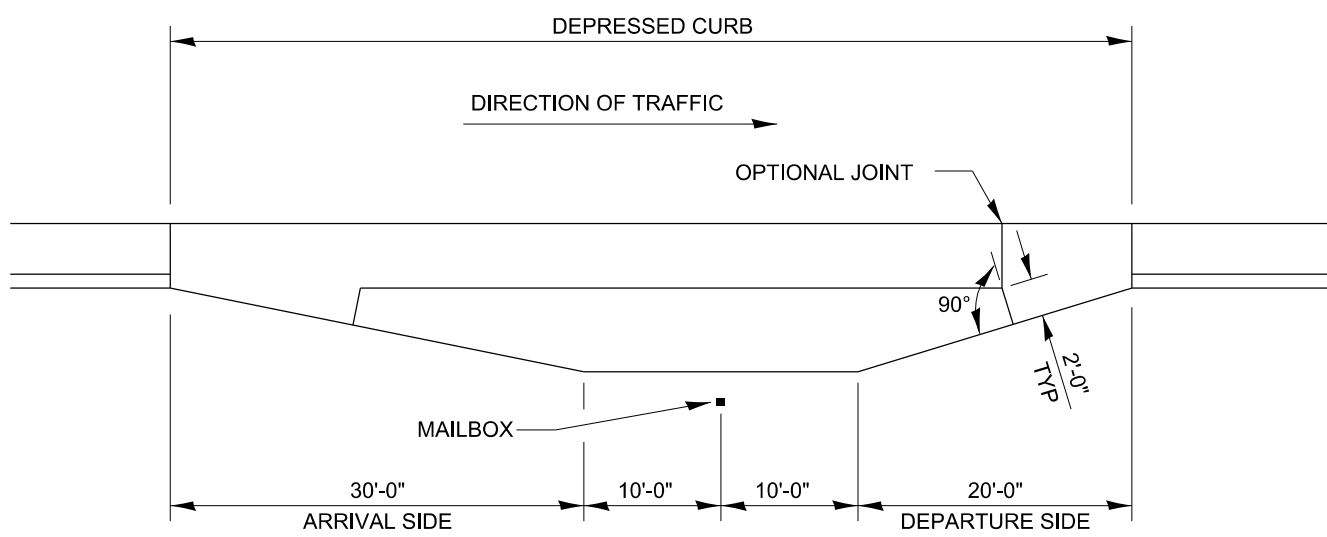


BICYCLE OR SHARED-USE PATH CROSS SECTION

406-13



TYPICAL CROSS SECTION



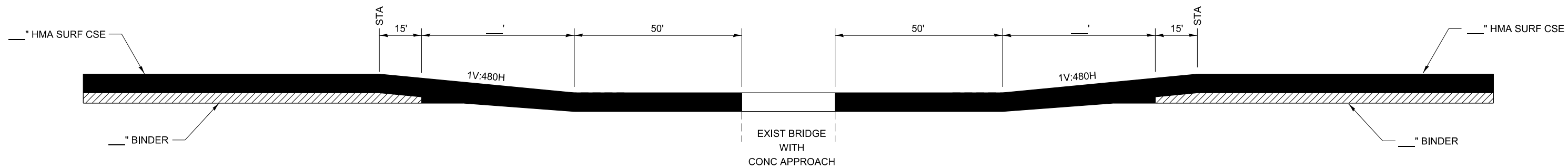
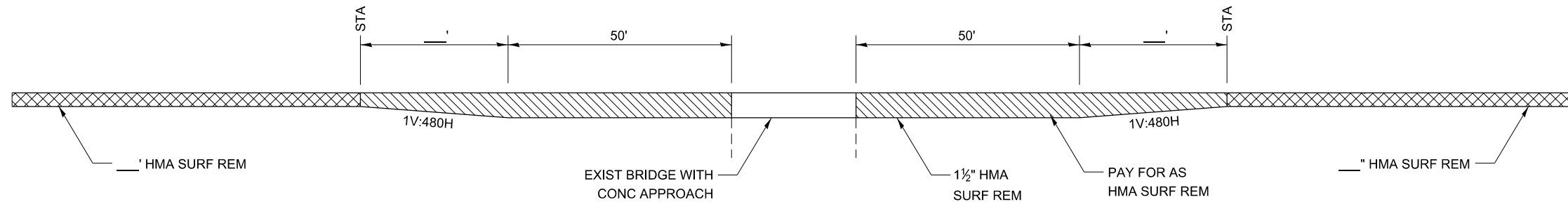
DETAIL OF MAILBOX TURNOUT IN CURB AND GUTTER SECTION
TYPICAL INSTALLATION

GENERAL NOTES

1. THE LONGITUDINAL CONSTRUCTION JOINT SHALL CONFORM TO SECTION 420.05 OF THE STANDARD SPECIFICATIONS.
2. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PC CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED ON THE PLANS WHICH PRICE SHALL INCLUDE THE LONGITUDINAL CONSTRUCTION JOINT, AND THE ADDITIONAL THICKNESS REQUIRED TO TRANSITION TO THE DEPRESSED COMBINATION CONCRETE CURB AND GUTTER.
3. MAINTAIN A MINIMUM 10' TANGENT SECTION FROM EACH SIDE OF MAILBOX.

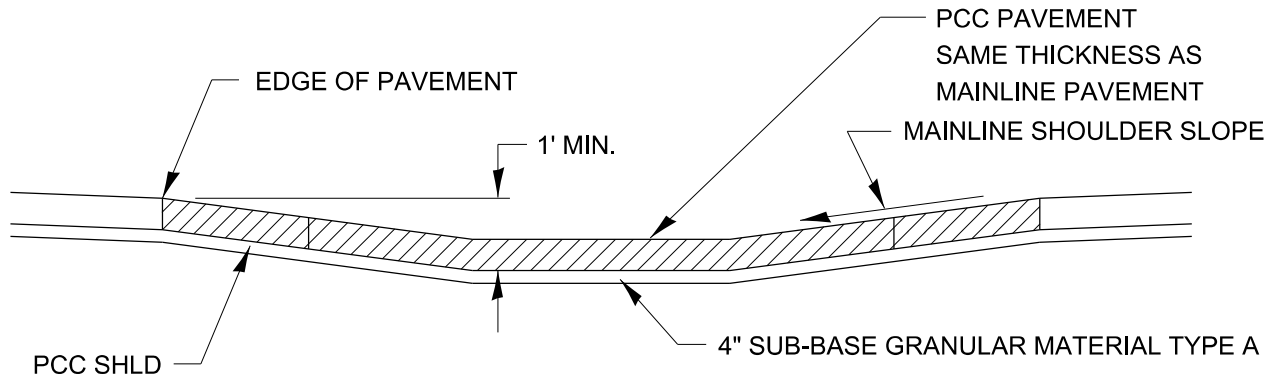
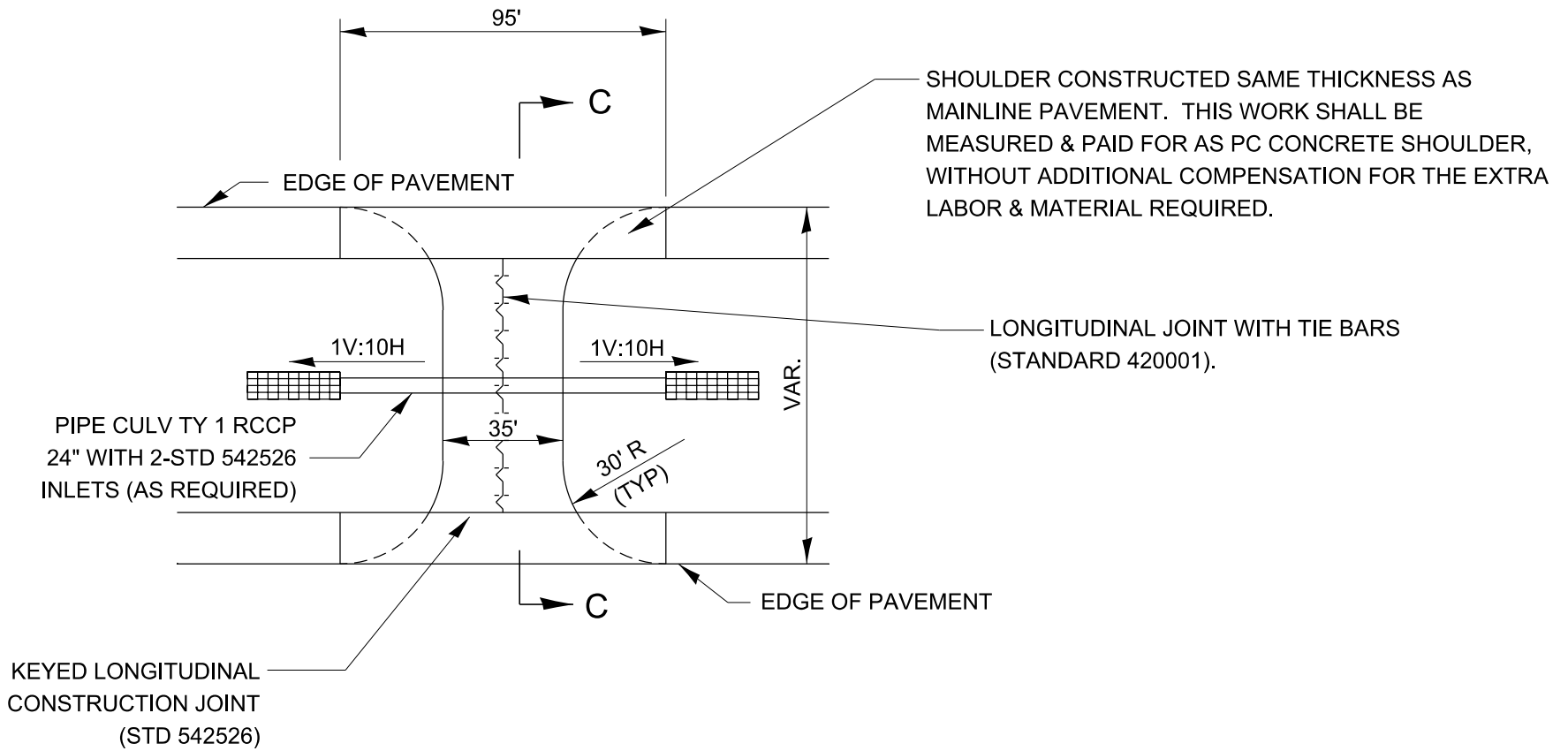
406-14

MAILBOX TURNOUT WITH
URBAN TYPICAL



DESIGNER NOTE:
 BUTT JOINT DETAIL TO BE USED TO PREVENT DYNAMIC LOADING
 ON RESURFACING PROJECTS WHEN BRIDGE IS TO BE GAPPED
 AND WHEN RESURFACING PROFILE IS HIGHER THAN STRUCTURE
 PROFILE.
 ADJUST THICKNESS AND DISTANCES TO PROJECT SPECIFIC DETAILS.

STRUCTURE BUTT JOINT
MILLING AND RESURFACING AT BRIDGE

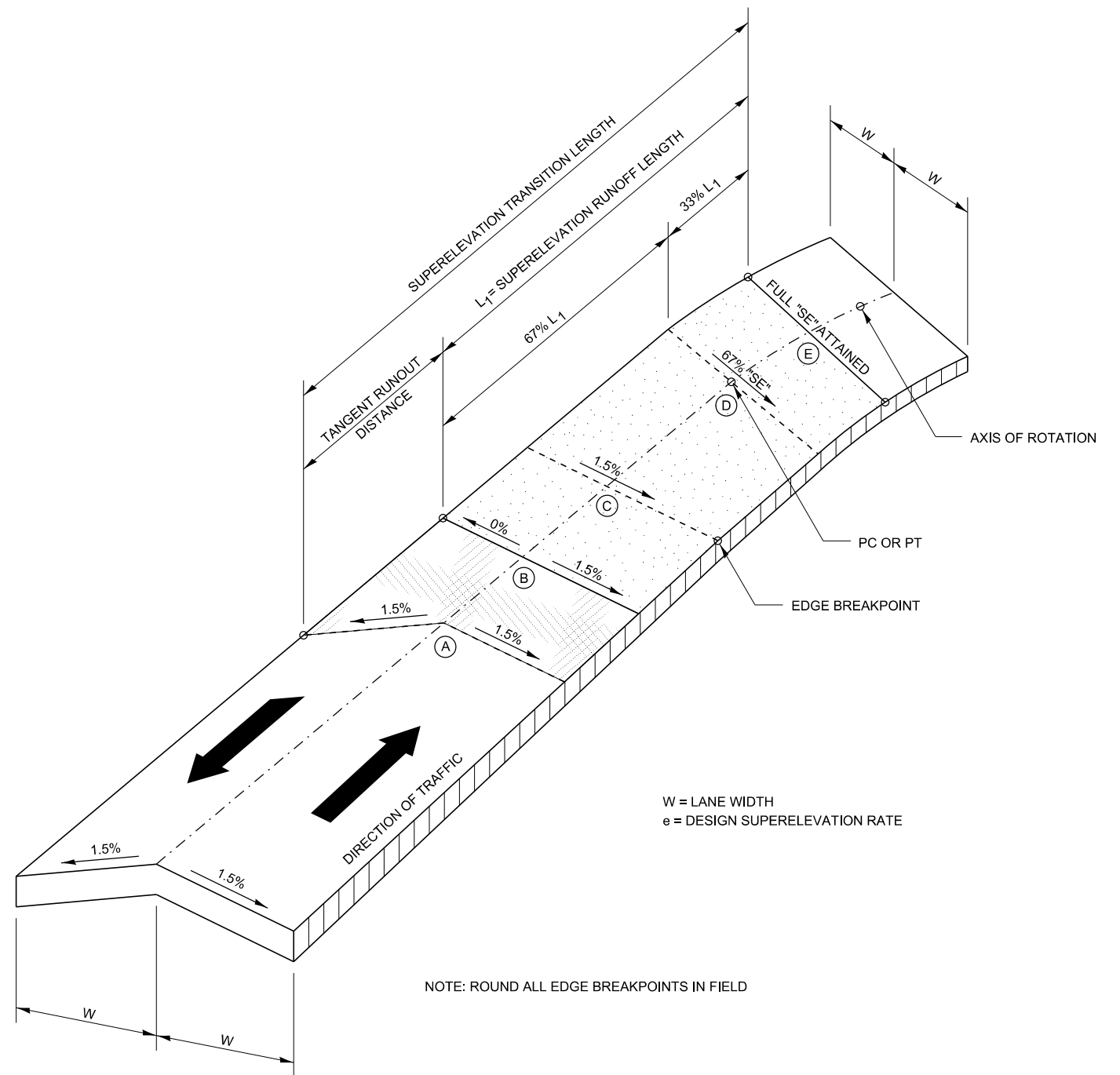
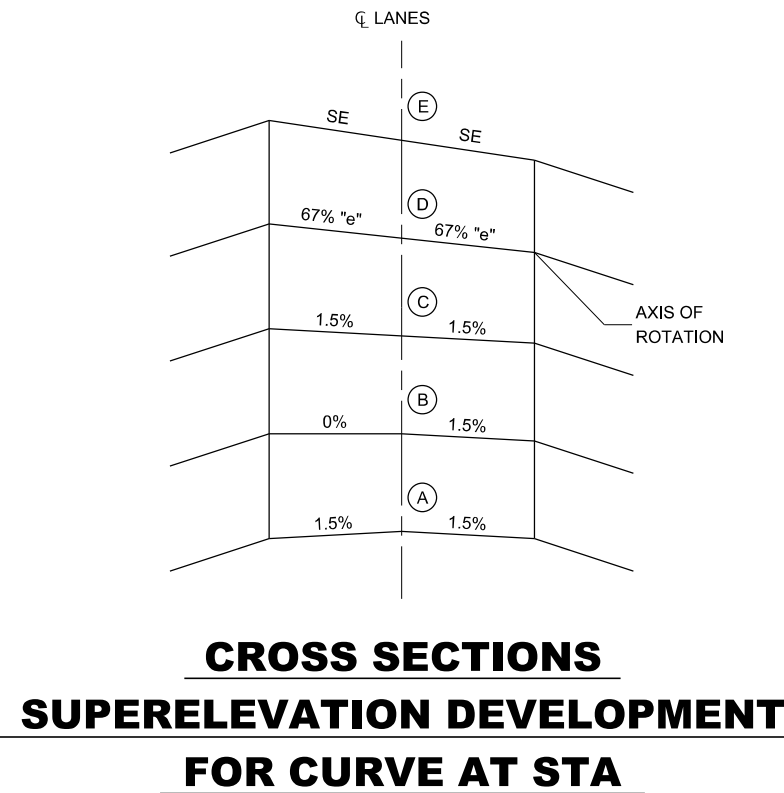


CONCRETE MAINTENANCE CROSSOVER

420-1

EX. CURVE A
P.I. STA=
 Δ =
D=
R=
T=
L=
E=
e=
T.R.=
S.E. RUN=
P.C. STA=
P.T. STA=

EX. CURVE B
P.I. STA=
 Δ =
D=
R=
T=
L=
E=
e=
T.R.=
S.E. RUN=
P.C. STA=
P.T. STA=



DESIGNER NOTE:
CURVE DATA TABLE PLACED ON ALIGNMENT SHEET IF THERE IS ONE.

W = LANE WIDTH
e = DESIGN SUPERELEVATION RATE

NOTE: ROUND ALL EDGE BREAKPOINTS IN FIELD

TRANSITION CURVE TABLE

CURVE PI STA.	W	A	B	C	D	E	SUPERELEVATION "SE"	TANGENT RUNOUT DISTANCE (TR)	SUPERELEVATION TRANSITION LENGTH (L)	SUPERELEVATION RUNOFF LENGTH (LI)
A										
B										

SUPERELEVATION TRANSITION ON TWO-LANE HIGHWAY

420-4

MODEL: det 3 det.tbl
FILE: \\hpc\paw.bentley.com\PIV\DOT\Documents\DOT Offices\District 3\Standards - District 3\BETAS\SUBDISTRICT 3 STANDARD DETAILS.DGN\MO-499_SURE_CSE_PAVT_REPAIR & SHLD.dgn

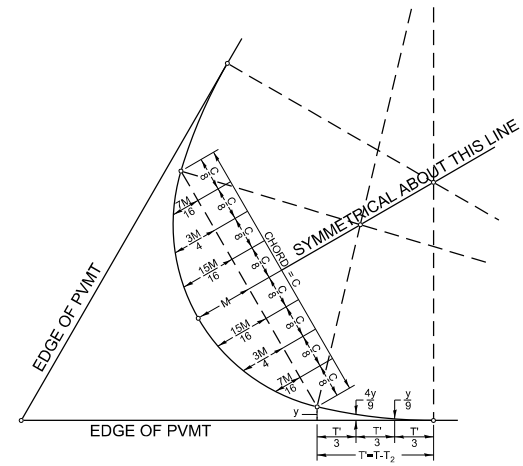
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	DRAWN -	REVISED -
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PLOT DATE = 3/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

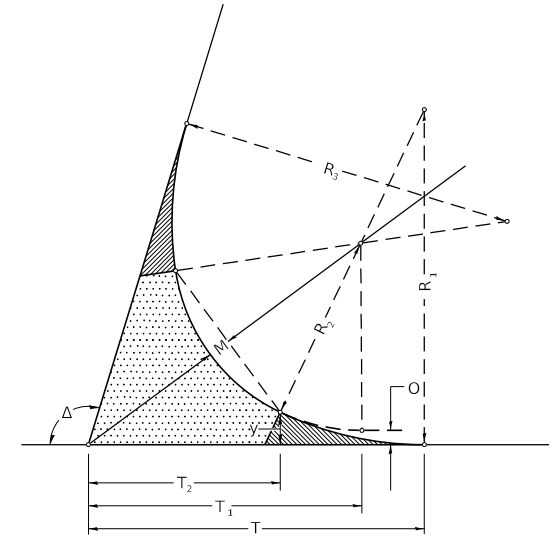
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

SYMMETRICAL CURVES

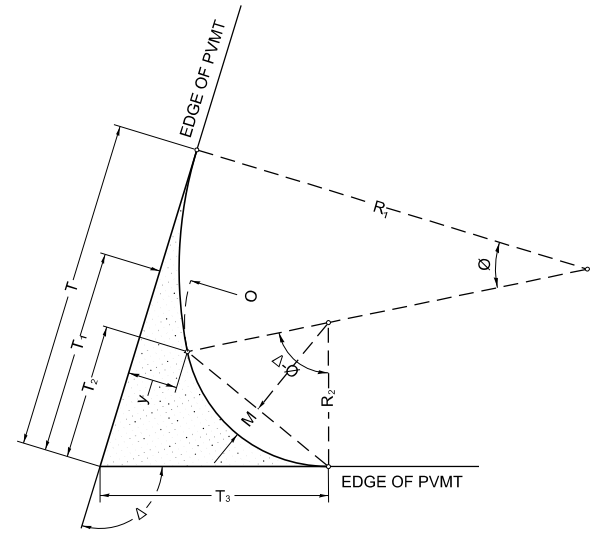


FIELD LAYOUT METHOD



FOR SYMMETRICAL CURVES

CURVE #							
R ₁							
R ₂							
R ₃							
O							
Δ							
T							
T ₁							
T ₂							
T'							
y							
$\frac{4y}{9}$							
$\frac{y}{9}$							
M							
$\frac{15M}{16}$							
$\frac{3M}{4}$							
$\frac{7M}{16}$							
C							

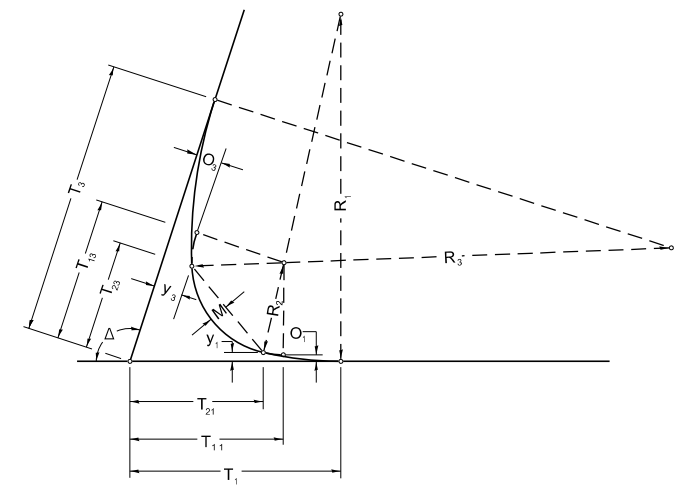


TWO CENTER CURVES

CURVE #							
R ₁							
R ₂							
O							
Δ							
T							
T ₁							
T ₂							
T ₃							
y							
$\frac{4y}{9}$							
$\frac{y}{9}$							
M							
$\frac{15M}{16}$							
$\frac{3M}{4}$							
$\frac{7M}{16}$							
C							

TWO AND THREE CENTER CURVE DATA

ASYMMETRICAL CURVES



FOR ASYMMETRICAL CURVES

CURVE #							
R ₁							
R ₂							
R ₃							
O ₁							
O ₂							
Δ							
T							
T ₁₁							
T ₁₂							
T ₁₃							
T ₂₁							
T ₂₂							
T ₂₃							
T ₃₁							
T ₃₂							
T ₃₃							
y ₁							
y ₂							
y ₃							
$\frac{4y_1}{9}$							
$\frac{y_1}{9}$							
$\frac{y_2}{9}$							
$\frac{y_3}{9}$							
M							
$\frac{15M}{16}$							
$\frac{3M}{4}$							
$\frac{7M}{16}$							
C							

ASYMMETRICAL THREE CENTER CURVES

420-5

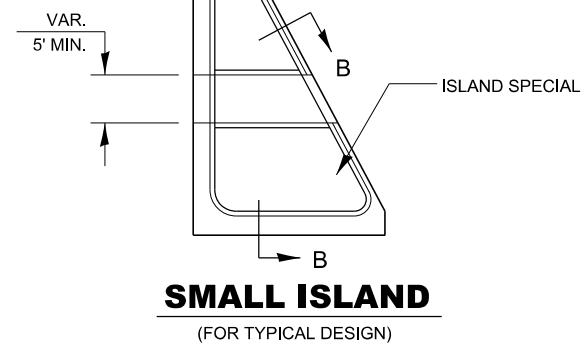
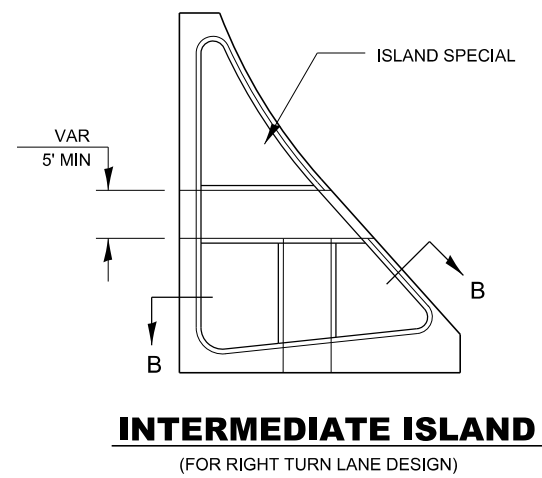
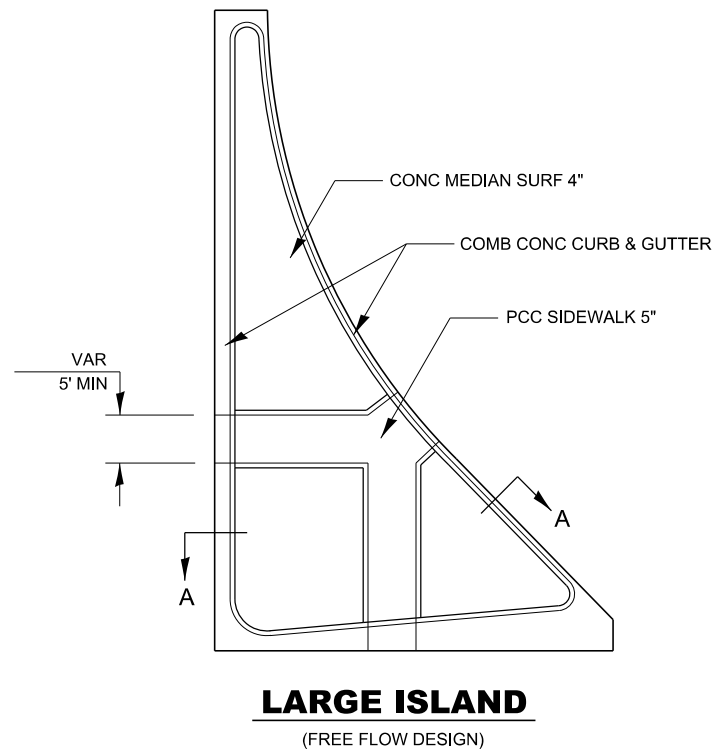
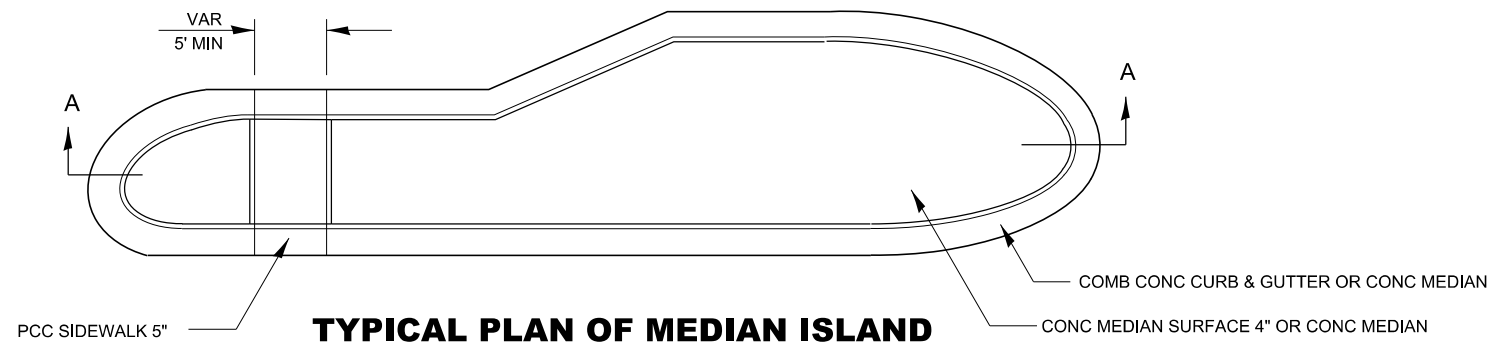
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USER NAME = ronald.pohar	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



GENERAL NOTES

SEE STANDARDS 606001, 606301, 424031, AND PLAN SHEETS FOR STATION, OFFSETS, RADII, DIMENSIONS, AND DETAILS NOT SHOWN.

THE SIDEWALK SHOULD DRAIN TO THE LOW SIDE OF THE ISLAND. IF NECESSARY THE SIDEWALK SHALL BE SLOPED TO DRAIN AT A MAXIMUM 1.5% GRADE.

SEE THE PLAN SHEETS FOR THE TYPE OF CURB & GUTTER TO BE USED ON ISLANDS.

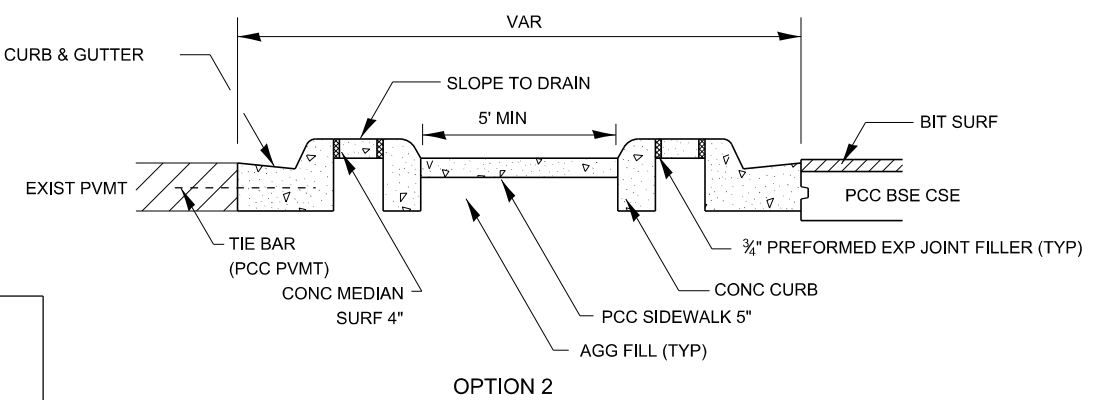
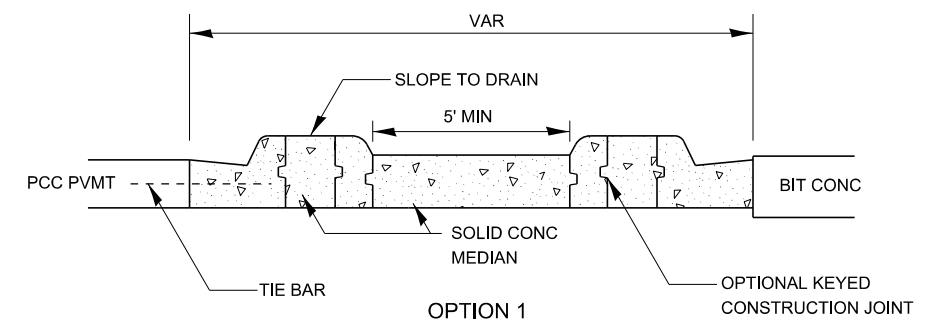
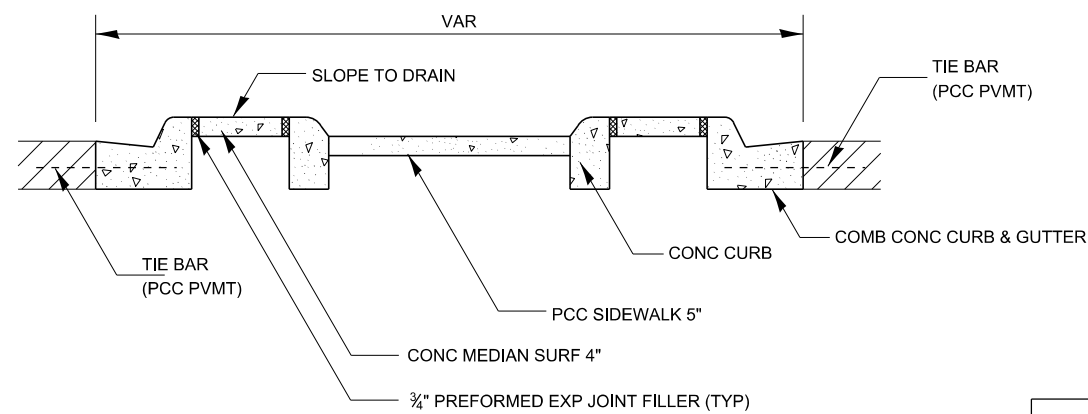
THE SIDEWALK SHOULD NOT BE CLOSER THAN 3' FROM THE CORNER OF THE ISLAND.

KEYED LONGITUDINAL CONSTRUCTION JOINTS SHALL BE CONSTRUCTED WITHOUT TIE BARS.

MEDIANS AND LARGE ISLANDS SHALL CONSIST OF PCC SIDEWALK 5", CONCRETE MEDIANS SURFACE 4", CONCRETE CURB, AND COMBINATION CONCRETE CURB & GUTTER, TYPE M OR B OR THE SIZE SPECIFIED. MEDIAN ISLAND CAN ALSO BE SOLID CONCRETE MEDIANS.

LOCATIONS, LAYOUTS, AND WIDTHS OF THE FLUSH SIDEWALK AREA, SHALL BE DETERMINED BY THE DESIGNER AND SHOWN ON THE PLANS.

THE INTERMEDIATE AND SMALL ISLANDS WILL BE MEASURED FOR PAYMENT FROM EOP TO EOP USING EITHER OPTION 1 OR OPTION 2, AS DIRECTED BY THE ENGINEER, AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ FT FOR CONCRETE MEDIAN (SPECIAL), WHICH SHALL INCLUDE THE CURB, COMBINATION CURB & GUTTER, SIDEWALK, AGGREGATE FILL, CONCRETE MEDIAN SURFACE, AND SOLID CONCRETE MEDIAN.



DESIGNER NOTE:
DESIGNER SHOULD SPECIFY ISLAND TYPE
IN THE PLANS BASED ON BDE FIG. 36-2.G

SECTION B-B

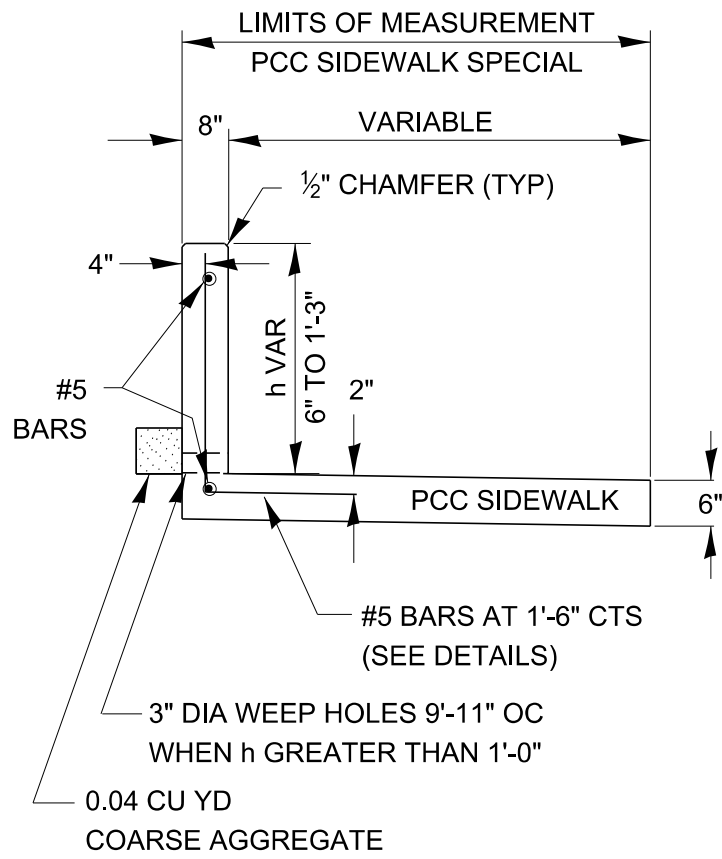
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USER NAME = ronald.pohar	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/15/2024	DATE -	REVISED -

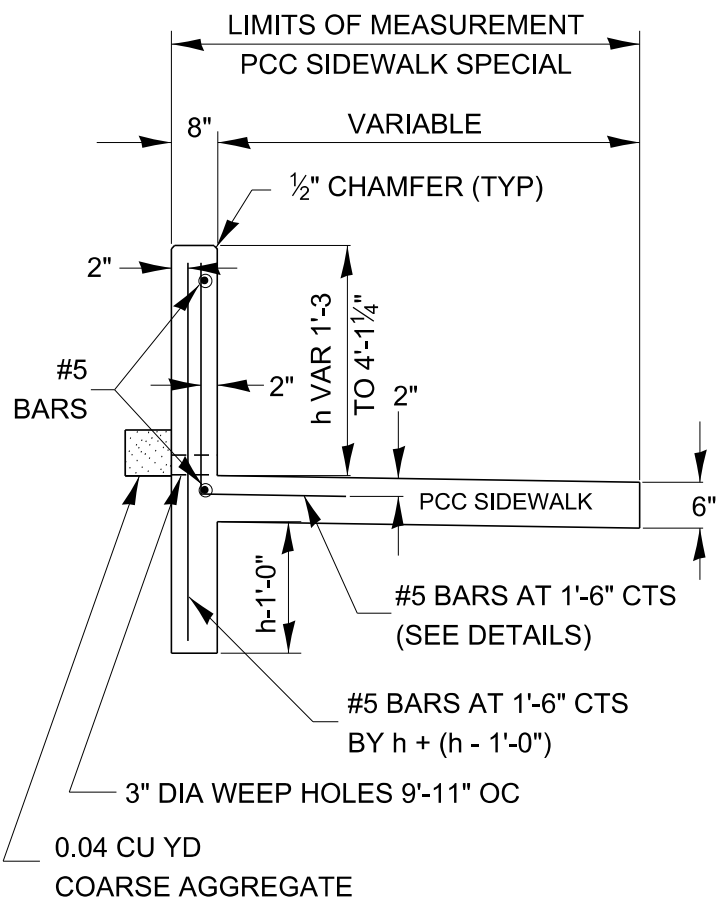
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ISLANDS AND MEDIANS WITH PEDESTRIAN ACCOMMODATIONS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

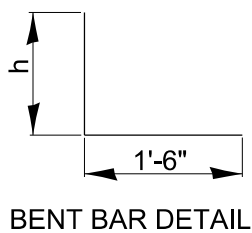
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



TYPE 1

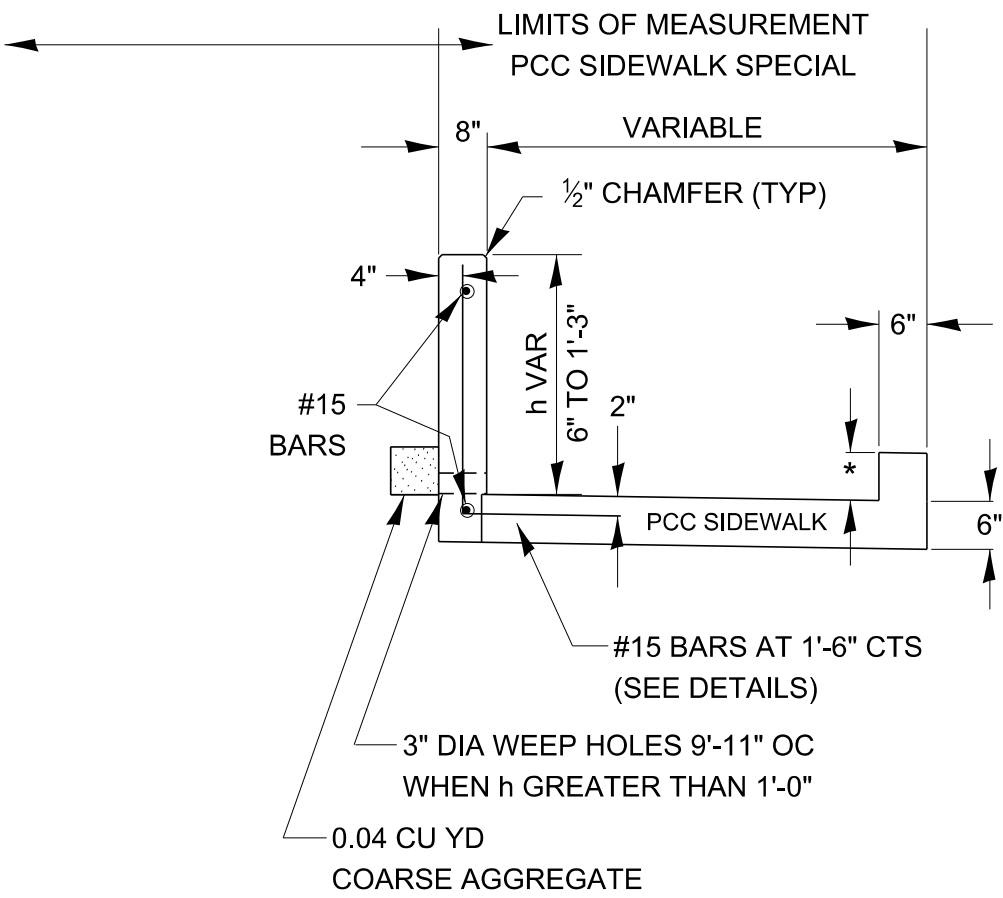


TYPE 2



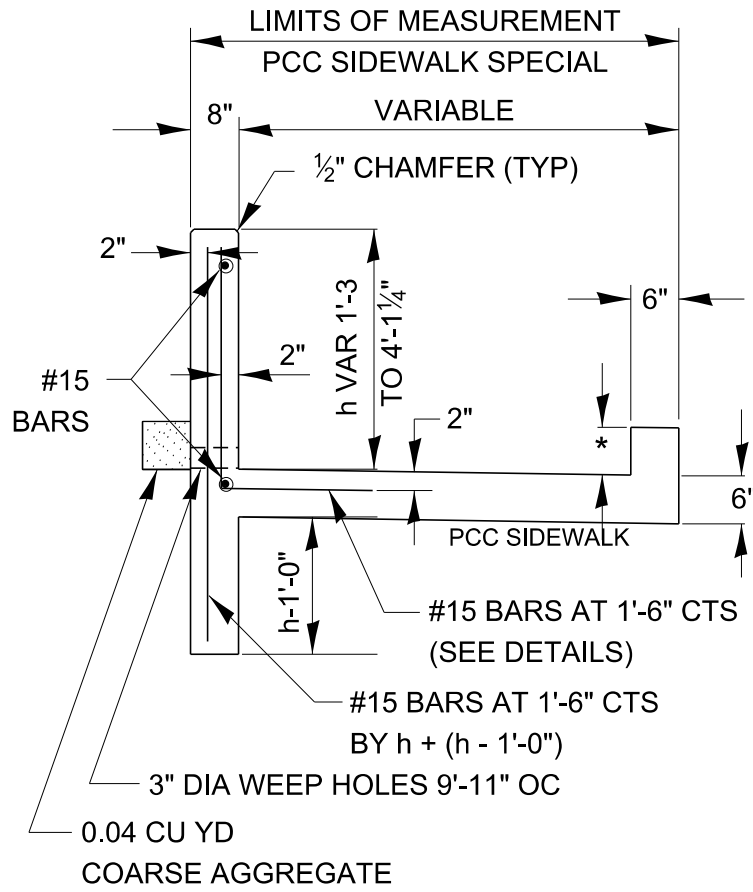
PCC SIDEWALK SPECIAL WITH RETAINING WALL

424-30



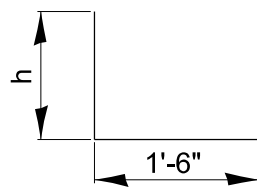
TYPE 1

APPLIED AT LOCATIONS WHERE WALL HEIGHT VARIES FROM 6" TO 1'-3"



TYPE 2

APPLIED AT LOCATIONS WHERE WALL HEIGHT VARIES FROM 1'-3" TO 4'-1 1/4"



BENT BAR DETAIL

* HEIGHT VARIES AS PER STANDARD 424001.

NOTE: PREFORMED EXPANSION JOINT FILLER SHALL BE PLACED ALONG THE ENTIRE JOINT AT LOCATIONS WHERE THE PCC SIDEWALK SPECIAL CHANGES FROM TYPE 1 TO TYPE 2. PREFORMED EXPANSION JOINT FILLER SHALL COMPLY WITH SECTION 1051 OF THE STANDARD SPECIFICATIONS. COST INCIDENTAL TO PCC SIDEWALK 6" SPECIAL.

PCC SIDEWALK SPECIAL WITH RETAINING WALL

TABLE OF TREADS & RISERS

SLOPE	TREAD	RISER
1:2	12"	6"
1:3	15"	5"
1:4	17"	4 1/4"

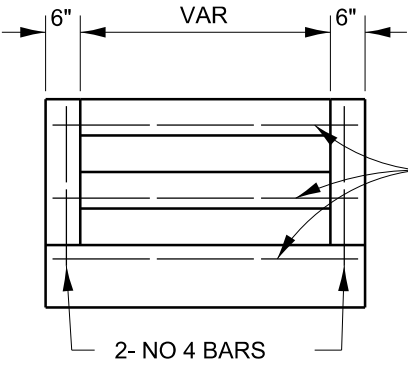
WHERE SLOPES FALL BETWEEN THOSE SHOWN IN THE TABLE ABOVE, THE STAIR RAIL SHOULD FIT THE SLOPE AND THE TREAD IN INCHES x THE RISER IN INCHES SHOULD BE BETWEEN 72 AND 78.

EXAMPLE:

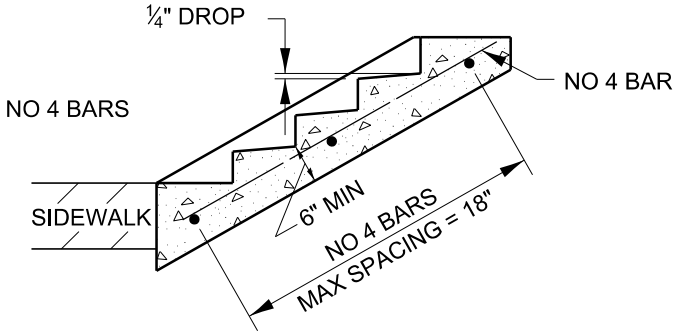
FOR A 1:4 SLOPE USE $y = \text{RISER HEIGHT}$ $4y = \sqrt{75}$ ".
 SOLVING $y^2 = \frac{75}{4}$, $y = 4.3"$ (USE 4 1/4" FOR CONVENIENCE.)
 TREAD WOULD THEN BE $4 1/4" \times 4 = 17"$

COST OF REINFORCEMENT BARS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LBS REINFORCEMENT BARS.

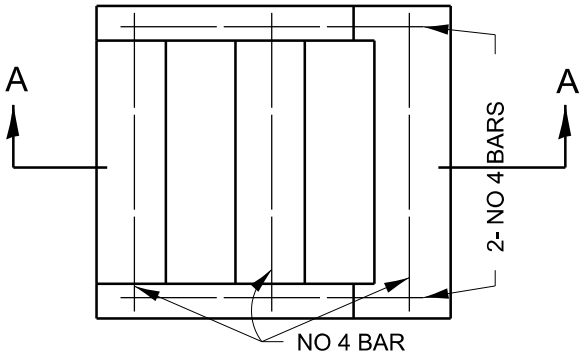
CLASS SI CONCRETE SHALL BE USED THROUGHOUT, WHICH SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS SI CONCRETE STEPS.



END ELEVATION



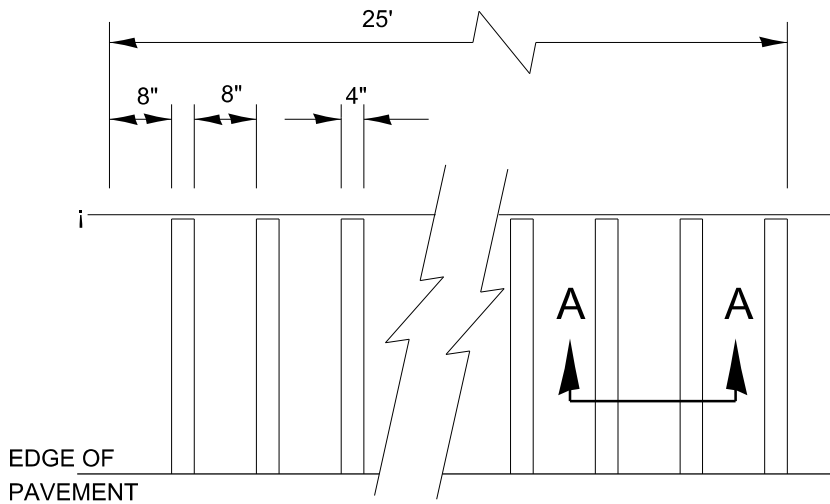
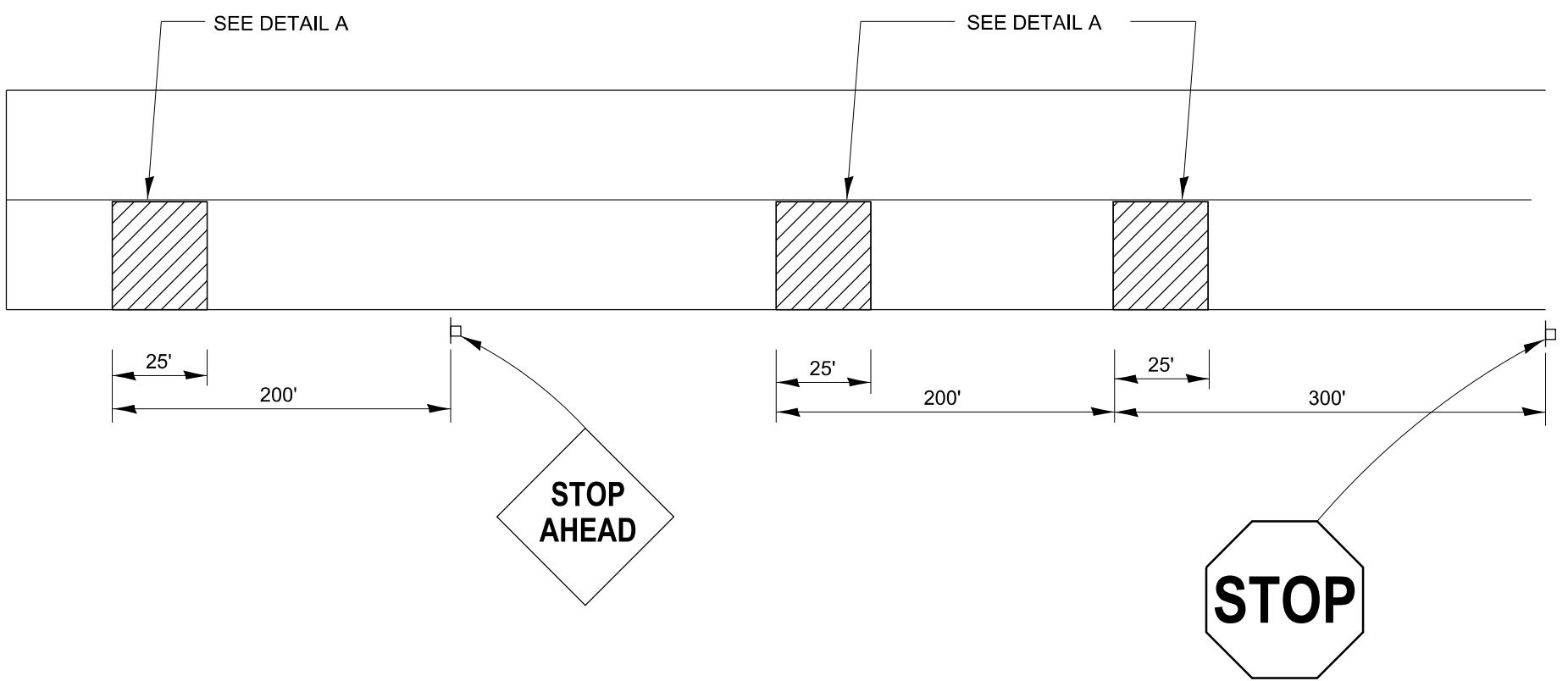
SECTION A-A



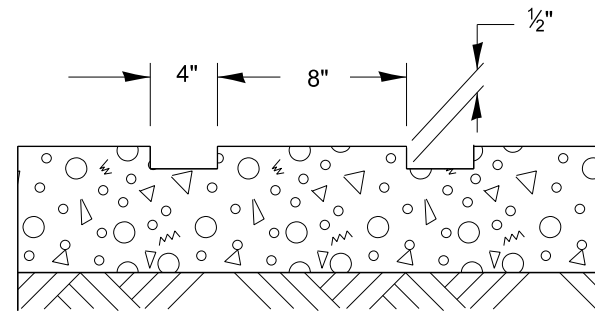
PLAN

DETAIL OF CONCRETE STEPS

424-32

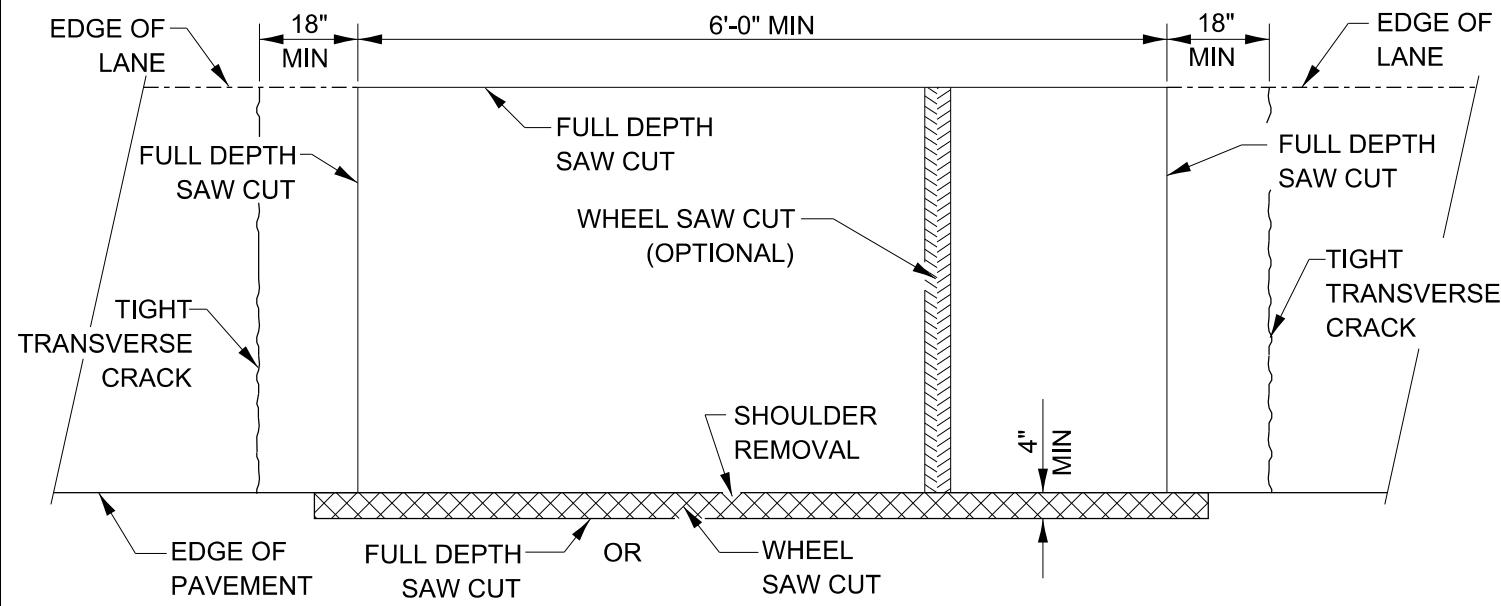


DETAIL A

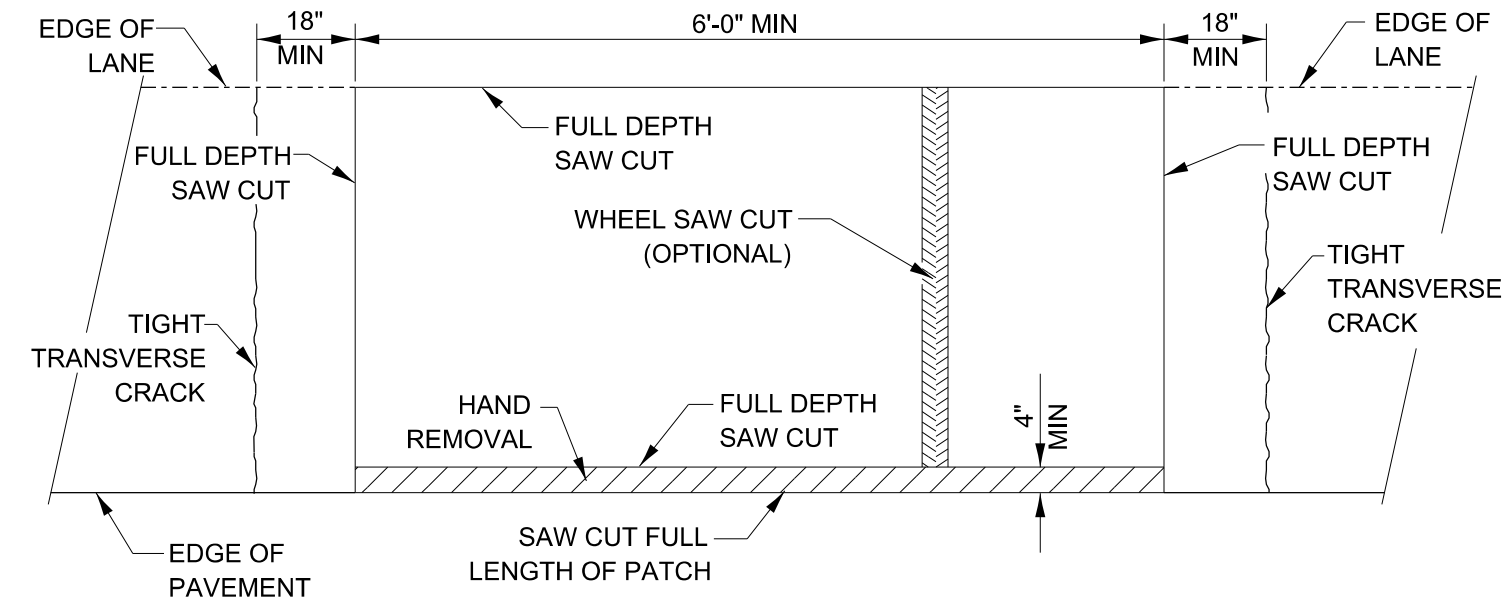


SECTION A-A

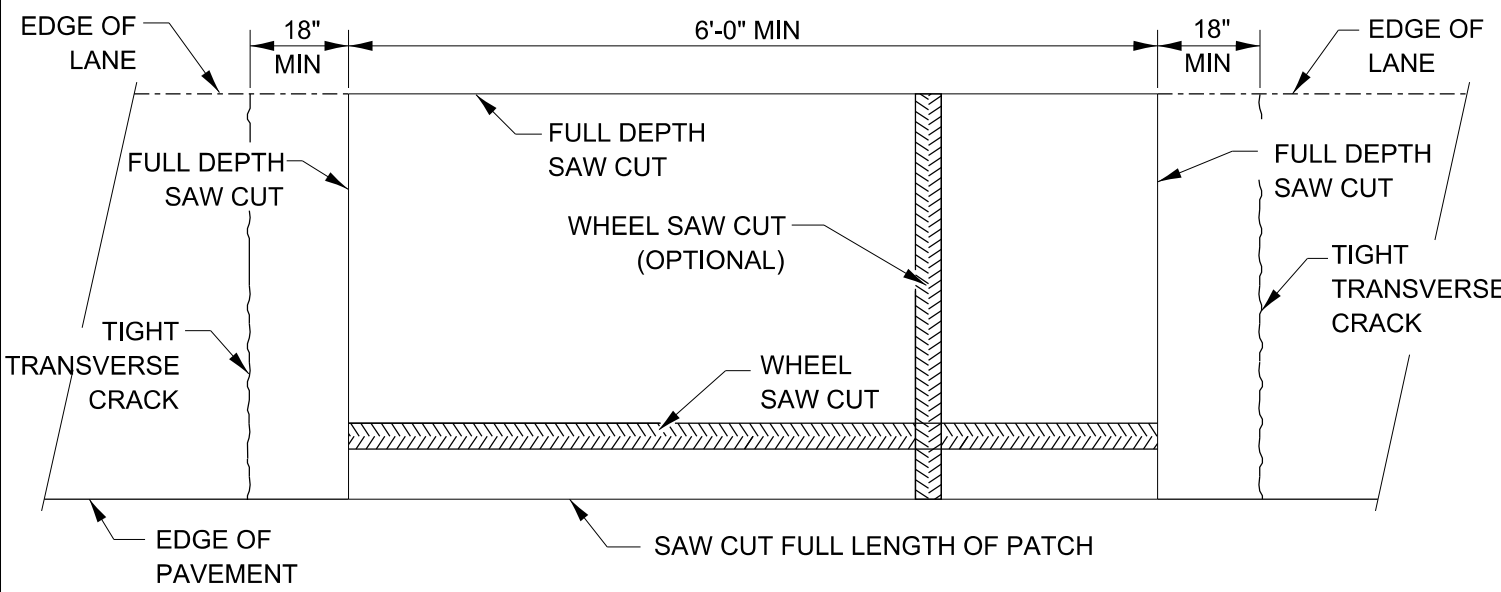
**TYPICAL GROOVED RUMBLE STRIP
APPLICATION IN ADVANCE OF AN INTERSECTION**



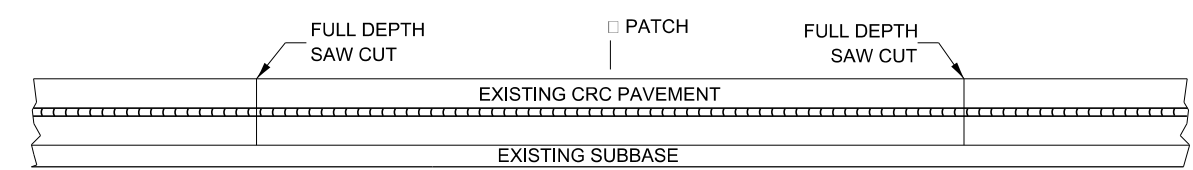
PAVEMENT SAWING DETAIL
(HMA SHOULDER)



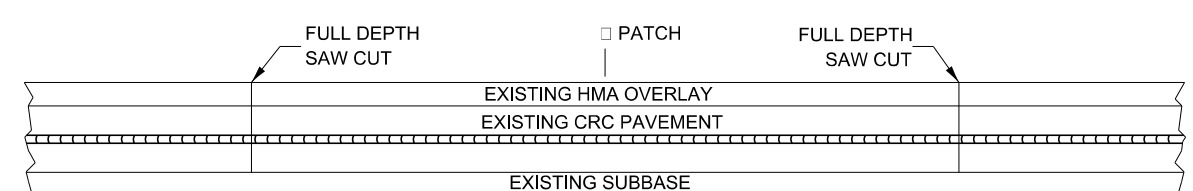
PAVEMENT SAWING DETAIL
(PCC SHOULDER)



ALTERNATE SAWING DETAIL
(PCC SHOULDER)



EXISTING PAVEMENT WITHOUT HMA SURFACE



EXISTING PAVEMENT WITH HMA SURFACE

PAVEMENT SAWING DETAIL FOR CLASS A PATCHING
(CRC PAVEMENT)

MODEL: ###
 FILE NAME: c:\pwwork\wtd\pcc\harr\dms1560400-499 SURF CSE PAVT REHAB & SHLD.dgn

USER NAME = Anthony.Grunstad	DESIGNED -	REVISED -
DRAWN -	REVISIONS -	
PLOT SCALE = 100,000 / 1 in.	CHECKED -	REVISED -
PLOT DATE = 6/26/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

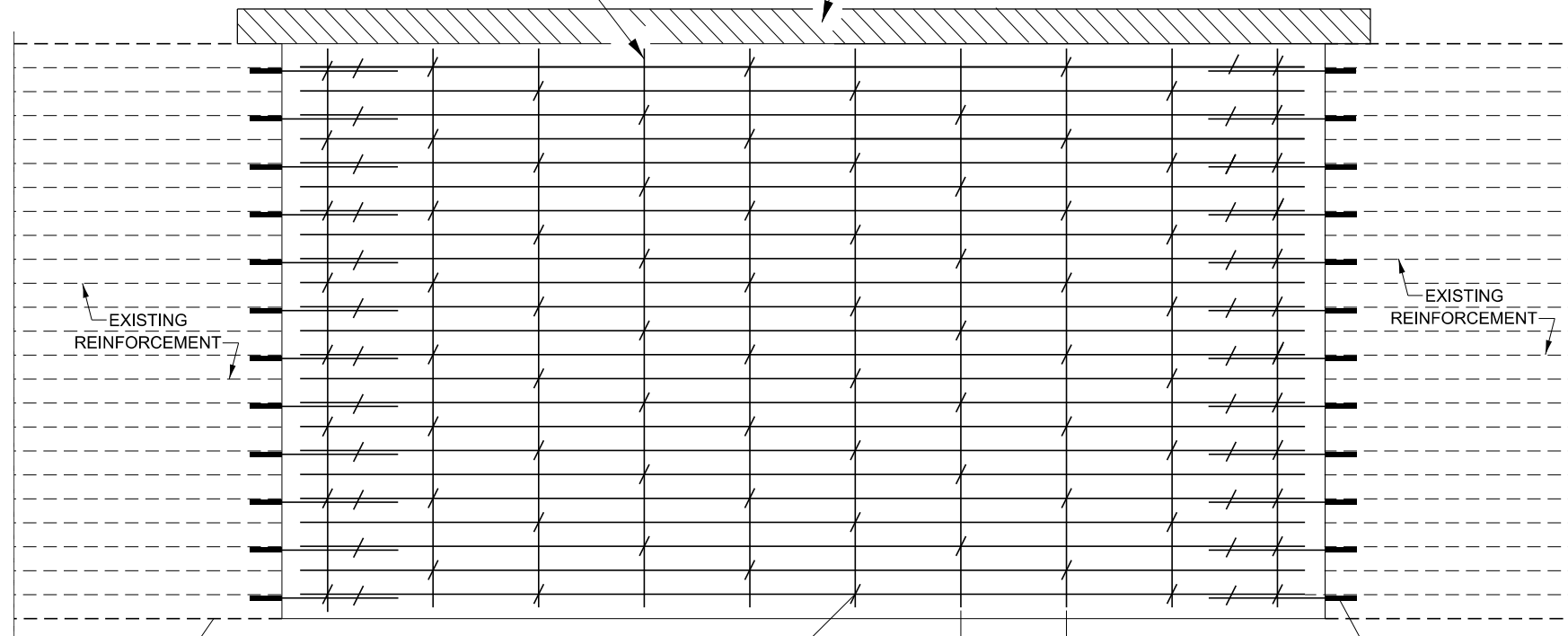
CLASS A PATCHING DETAILS

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

TRANSVERSE REBAR SHALL EXTEND TO OUTER LONGITUDINAL REBAR WHILE PROVIDING A MINIMUM 3" CLEARANCE FROM EXISTING PAVEMENT EDGE

SHOULDER REMOVAL



EDGE OF LANE

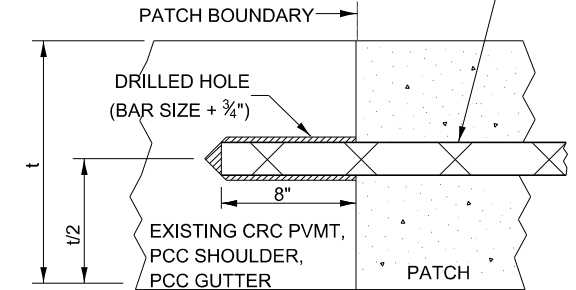
* NO. 6 TRANSVERSE BAR SHALL BE TIED TO LONGITUDINAL BAR.

4'-0" (TYP.)

#8X32" EPOXY COATED TIE BARS, DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL).

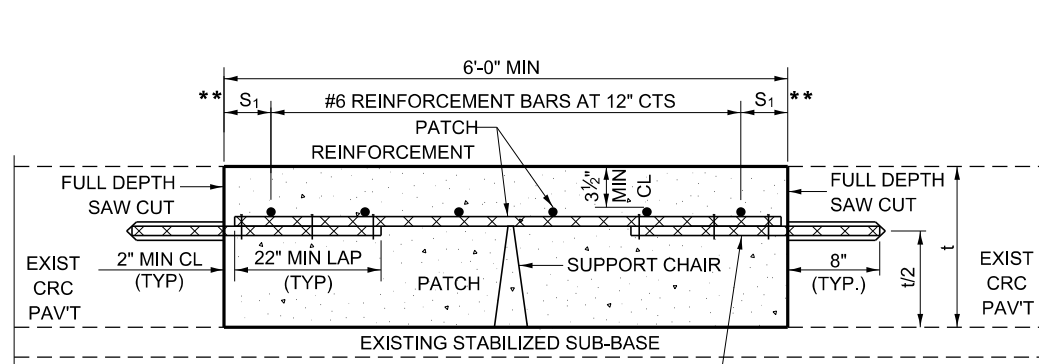
PAVEMENT REINFORCEMENT DETAIL

#6X24" EPOXY COATED TIE BARS DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL)



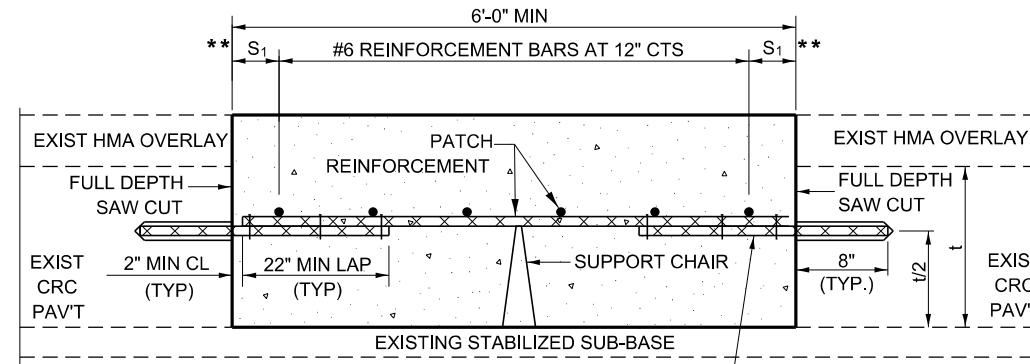
LONGITUDINAL CONSTRUCTION JOINT

PATCHES MORE THAN 20' IN LENGTH SHALL BE TIED TO ADJACENT PAVEMENT, PCC SHOULDERS OR PCC CURB AND GUTTER WITH #6X24" EPOXY COATED TIE BARS AT 36" CENTERS.



WITHOUT HMA OVERLAY

#8X32" EPOXY COATED TIE BARS DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL)



WITH HMA OVERLAY

#8X32" EPOXY COATED TIE BARS DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL)

PATCHING DETAIL FOR CLASS A PATCHING (CRC PAVEMENT)

NOTES:

EXISTING LONGITUDINAL REINFORCEMENT:
22 - #5 BARS AT 6 1/2" SPACING.

SPACING OF NEW LONGITUDINAL REINFORCEMENT SHALL MATCH THE SPACING OF EXISTING REINFORCEMENT AND SHALL BE TIED TOGETHER WITH A MINIMUM OF TWO TIES PER BAR.

TRANSVERSE CONSTRUCTION JOINTS: #8 X 32" EPOXY COATED TIE BAR SHALL BE DRILLED AND GROUTED INTO THE EXISTING PCC PAVEMENT AT A SPACING OF ± TWICE THE SPACING OF EXISTING LONGITUDINAL REINFORCEMENT BARS AND SHALL BE TIED TO PROPOSED LONGITUDINAL REINFORCEMENT WITH A MINIMUM OF TWO TIES PER BAR WHEN PRACTICAL.

* EVERY THIRD INTERSECTION MUST BE TIED. WHEN THE MINIMUM CLEARANCE CANNOT BE OBTAINED WITH THE TRANSVERSE REBAR ON TOP, THEN THE TRANSVERSE REBAR SHALL BE TIED TO THE BOTTOM OF THE LONGITUDINAL REBAR.

** VARIABLE: WHERE S₁ IS 2 1/2" MIN. AND 12" MAX.

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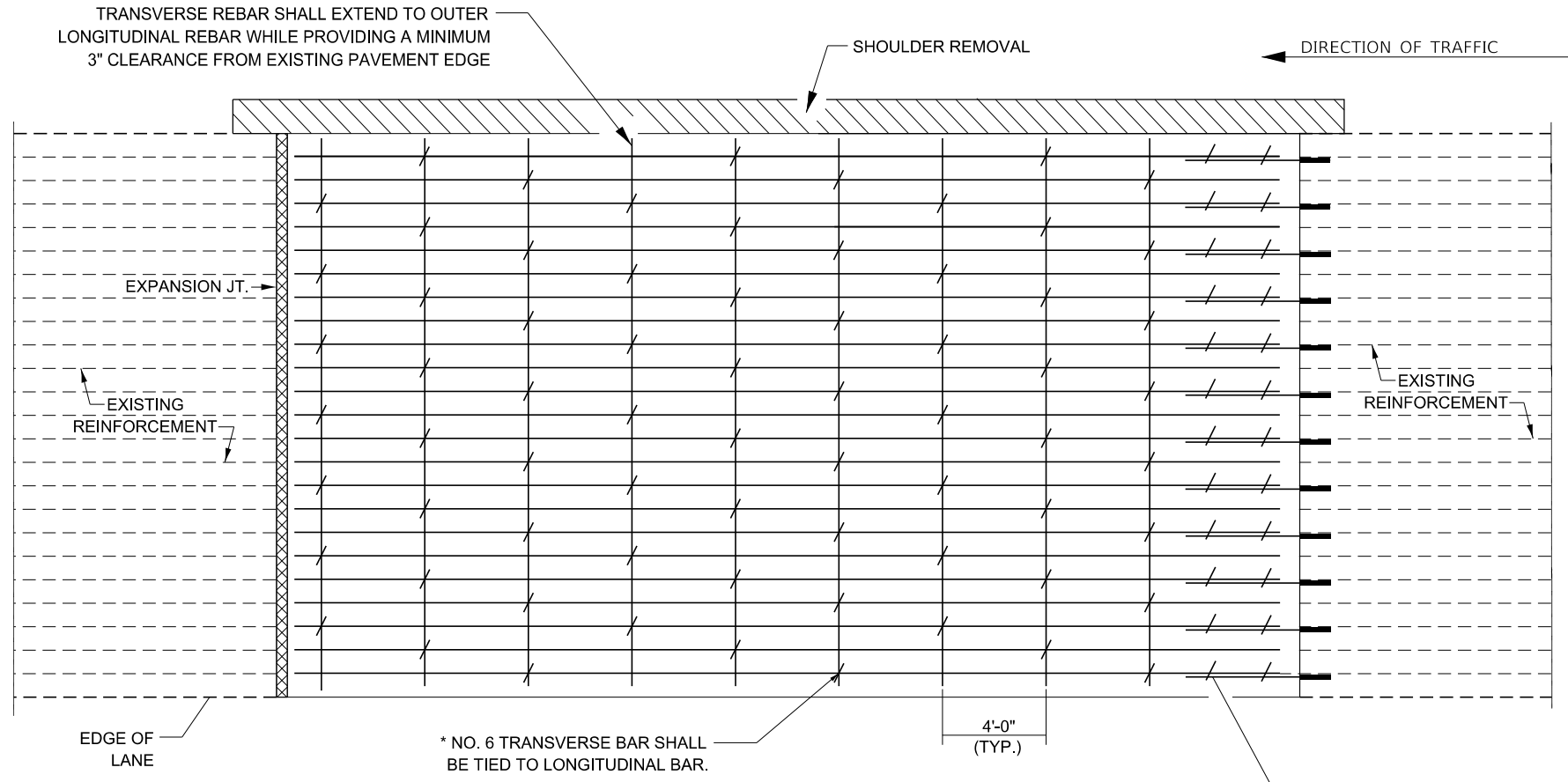
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PLOT DATE = 6/26/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

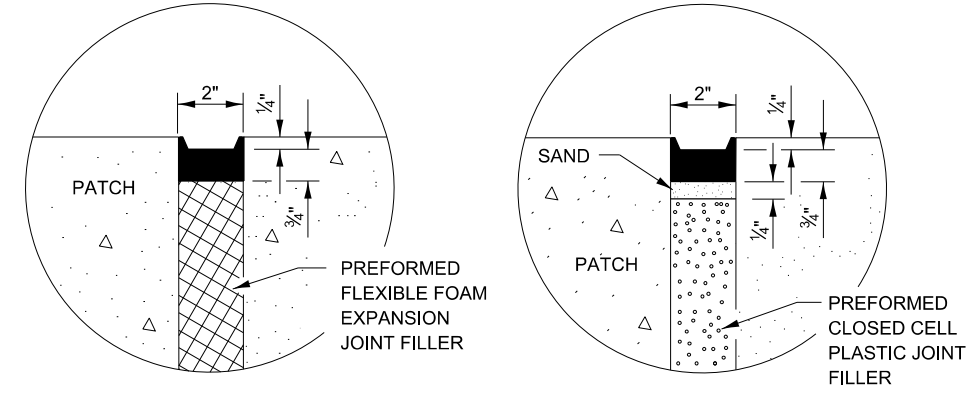
CLASS A PATCHING DETAILS

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

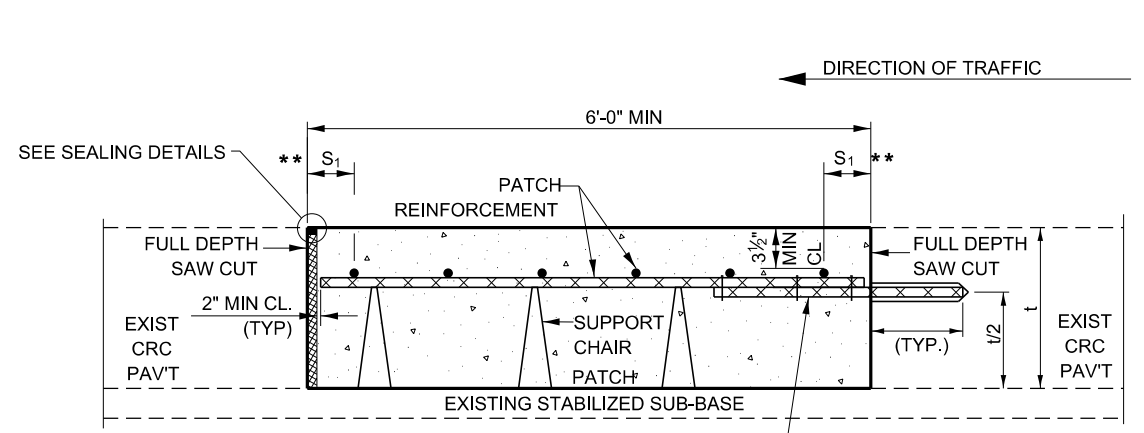
F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



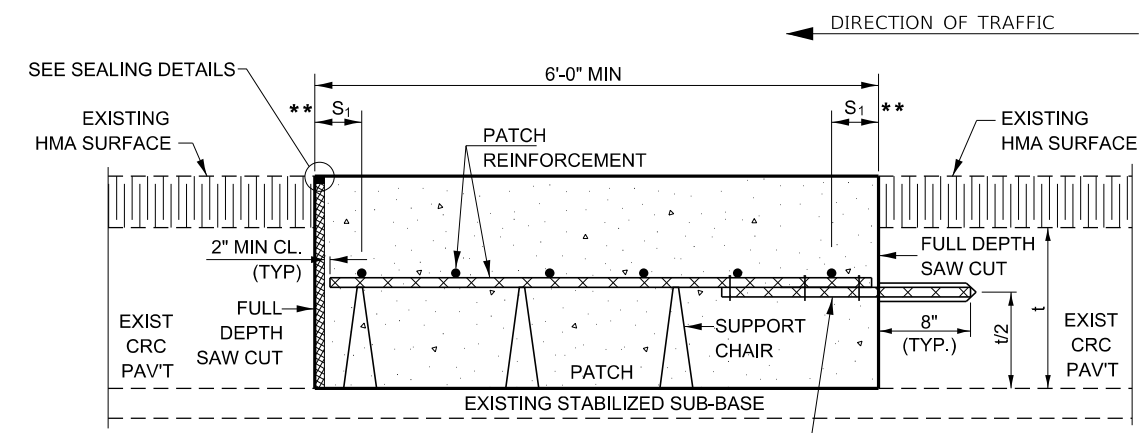
PAVEMENT REINFORCEMENT DETAIL



SEALING DETAILS



WITHOUT HMA OVERLAY



WITH HMA OVERLAY

#8X32" EPOXY COATED TIE BARS DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL). MINIMUM 22" LAP WITH PATCH REINFORCEMENT.

#8X32" EPOXY COATED TIE BARS DRILLED AND GROUTED INTO EXISTING PAVEMENT (TYPICAL). MINIMUM 22" LAP WITH PATCH REINFORCEMENT.

CLASS A PATCH WITH EXPANSION JOINT

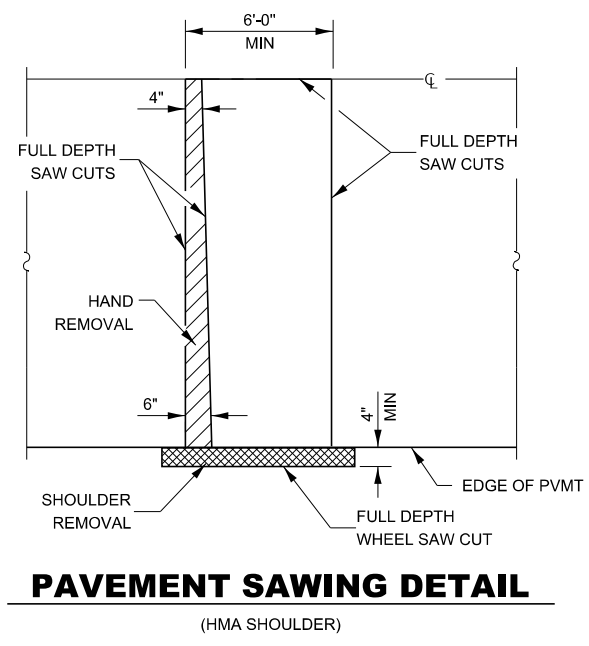
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	DRAWN -	REVISED -
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PLOT DATE = 6/26/2024	DATE -	REVISED -

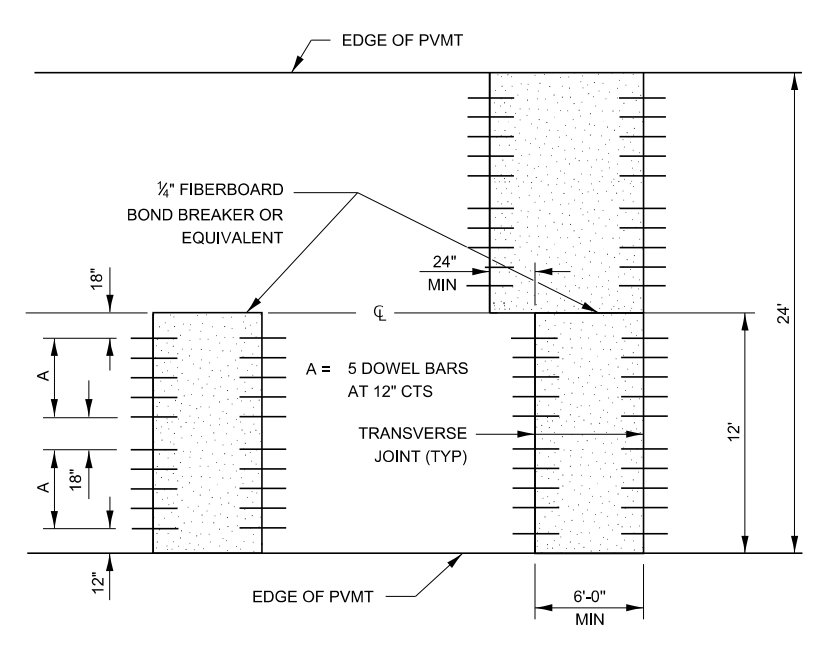
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CLASS A PATCHING DETAILS	
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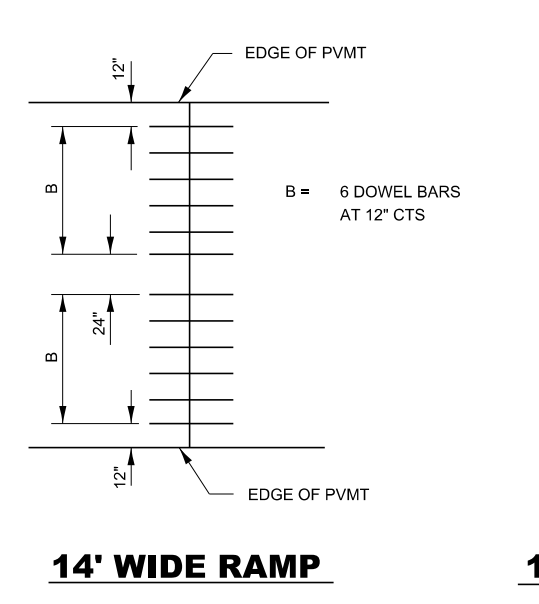
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



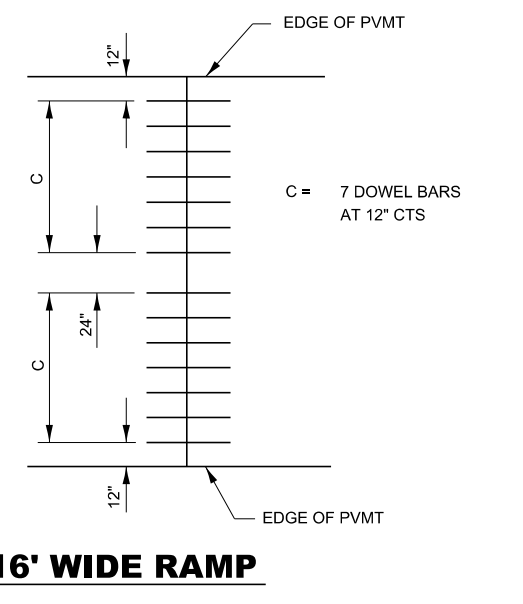
PAVEMENT SAWING DETAIL
(HMA SHOULDER)



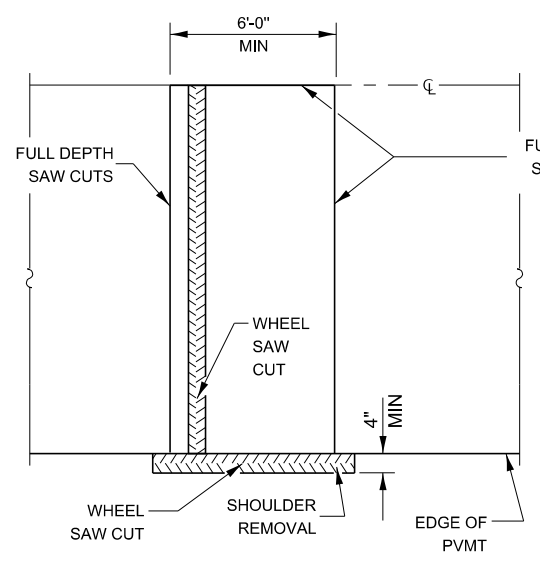
12' WIDE LANES



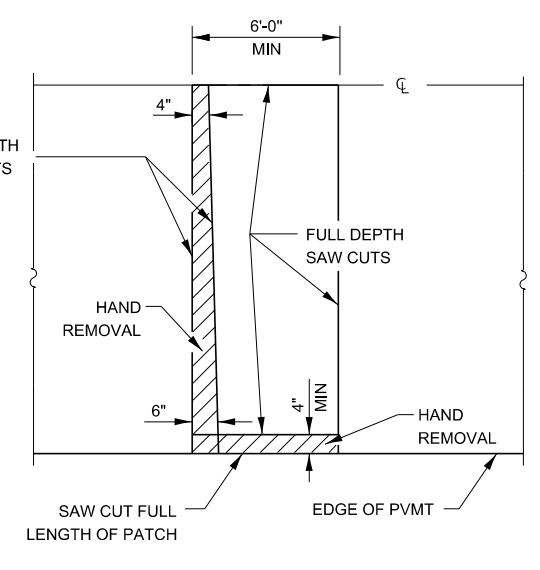
14' WIDE RAMP



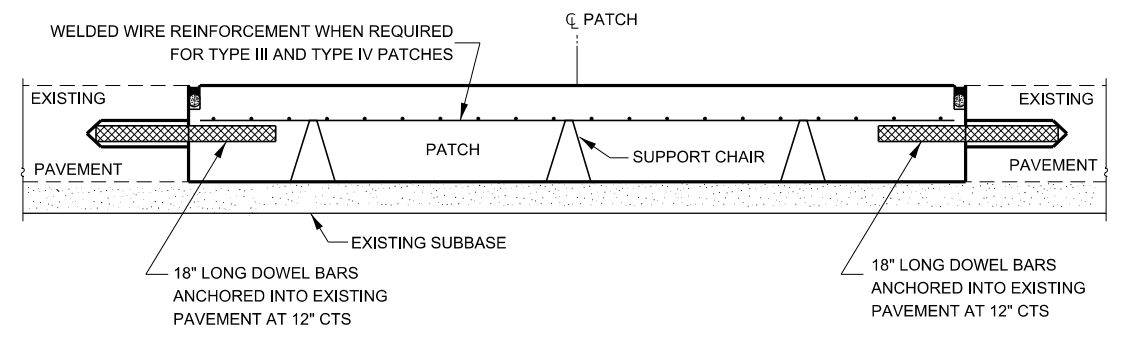
16' WIDE RAMP



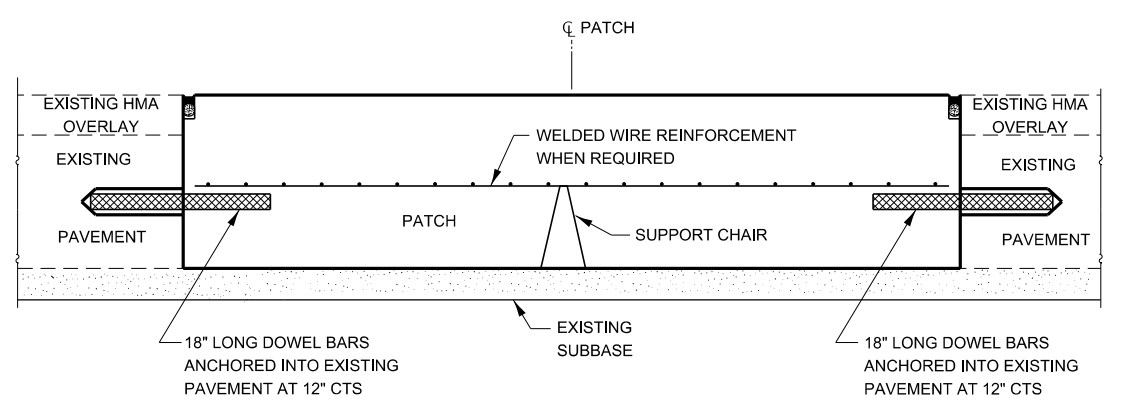
ALTERNATE SAWING DETAIL
(HMA SHOULDER)



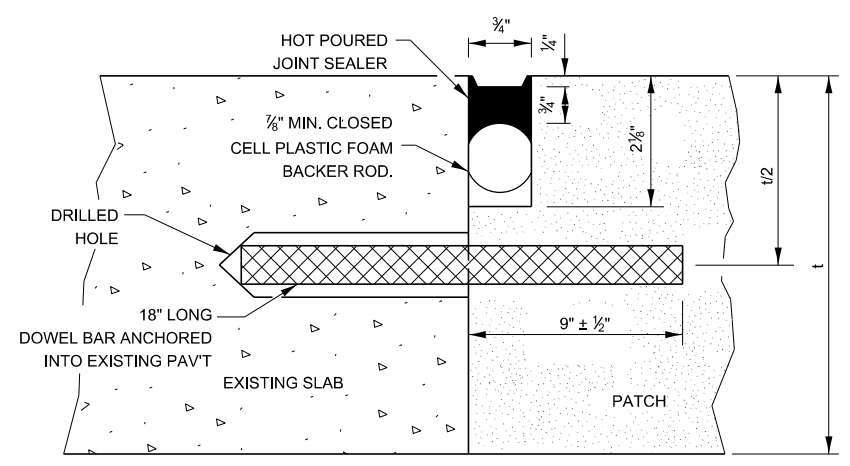
PAVEMENT SAWING DETAIL
(PCC SHOULDER)



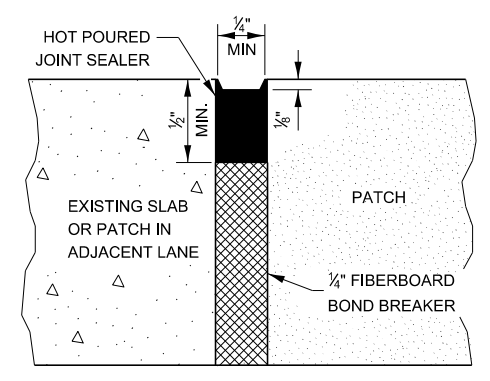
PATCHING WITHOUT HMA OVERLAY



PATCHING WITH HMA OVERLAY



TRANSVERSE JOINT



CENTERLINE JOINT

DOWEL BAR TABLE

PAVEMENT THICKNESS	DOWEL BAR DIAMETER	HOLE DIAMETER
8" OR GREATER	1 1/2"	1 3/8"
7" THRU 7.99"	1 1/4"	1 3/8"
LESS THAN 7"	1"	1 1/8"

GENERAL NOTES

THE TRANSVERSE JOINTS FOR CLASS B PATCHES SHALL ALIGN WITH JOINTS OR CRACKS IN THE ADJACENT LANE WHENEVER POSSIBLE.

SEE STANDARD 420701 FOR DETAILS OF PAVEMENT WELDED WIRE REINFORCEMENT.

442-3
SHEET 1 OF 2

MODEL: det 3 dets11b FILE: \\hp146150\share\bechtel\comp\NVDOT\Documents\DOT Offices\District 3 Standards - District 3\DETAILS\SUBDISTRICT 3 STANDARD DETAILS - DGN\400-499 - SUBR - CSE PAVT REHAB & SHLD.dgn

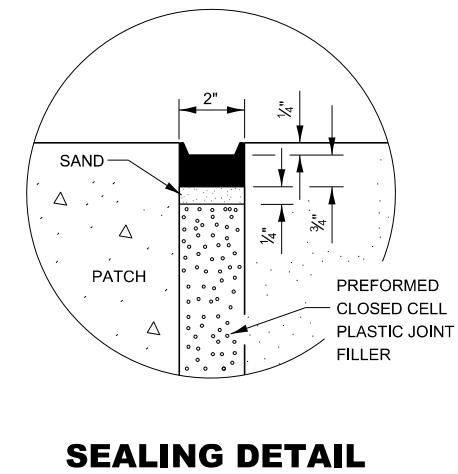
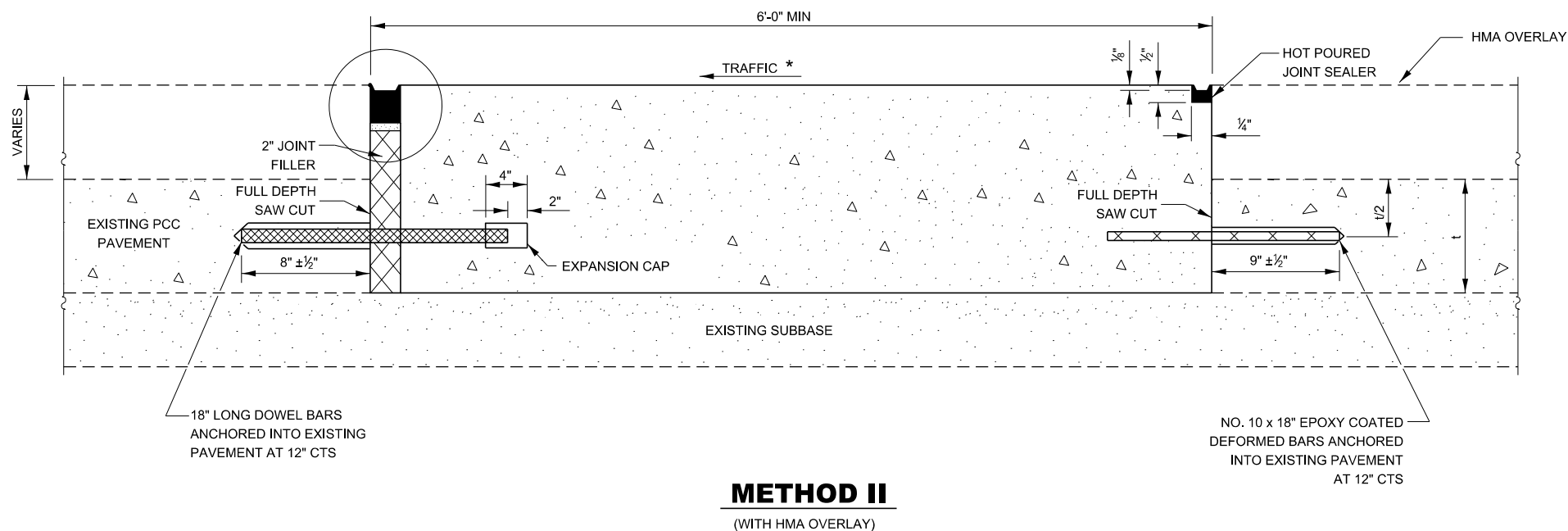
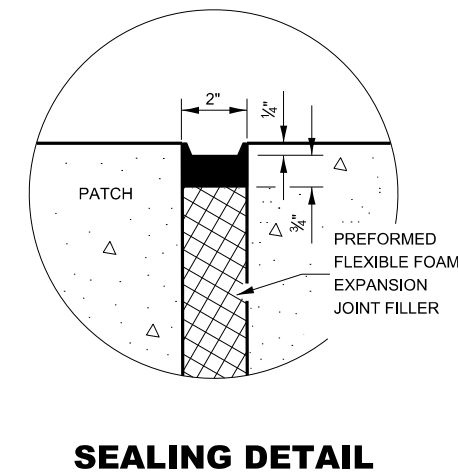
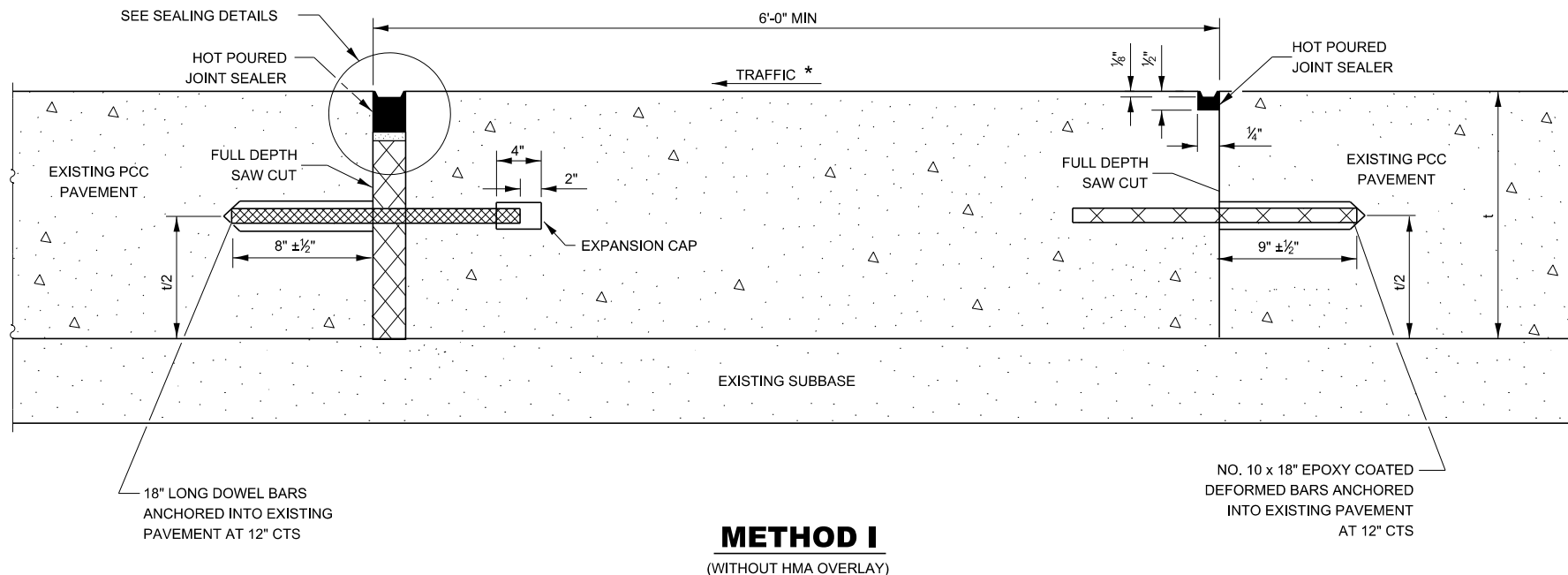
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PLOT SCALE = 100.000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 3/15/2024	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CLASS B PATCHING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
		ILLINOIS	FED. AID PROJECT	



NOTE

* WHEN RE-ESTABLISHING A TRANSVERSE EXPANSION JOINT ON A TWO-WAY ROAD, REVERSE THE ORIENTATION OF THE DOWEL BARS WITH RESPECT TO TRAFFIC FOR ONE OF THE PATCHES SUCH THAT THE JOINT WILL BE CONTINUOUS ACROSS BOTH LANES.

MODEL det 3 dwt11b
FILE Name: p:\u0000\paw_bentley.com\PI\DOT\Documents\DOT Office\District 3\Standards - District 3\DETAILS\SUBDISTRICT 3 STANDARD DETAILS.DGN\400-499_SURE_CSE_PAVT_REHAB & SHLD.dgn

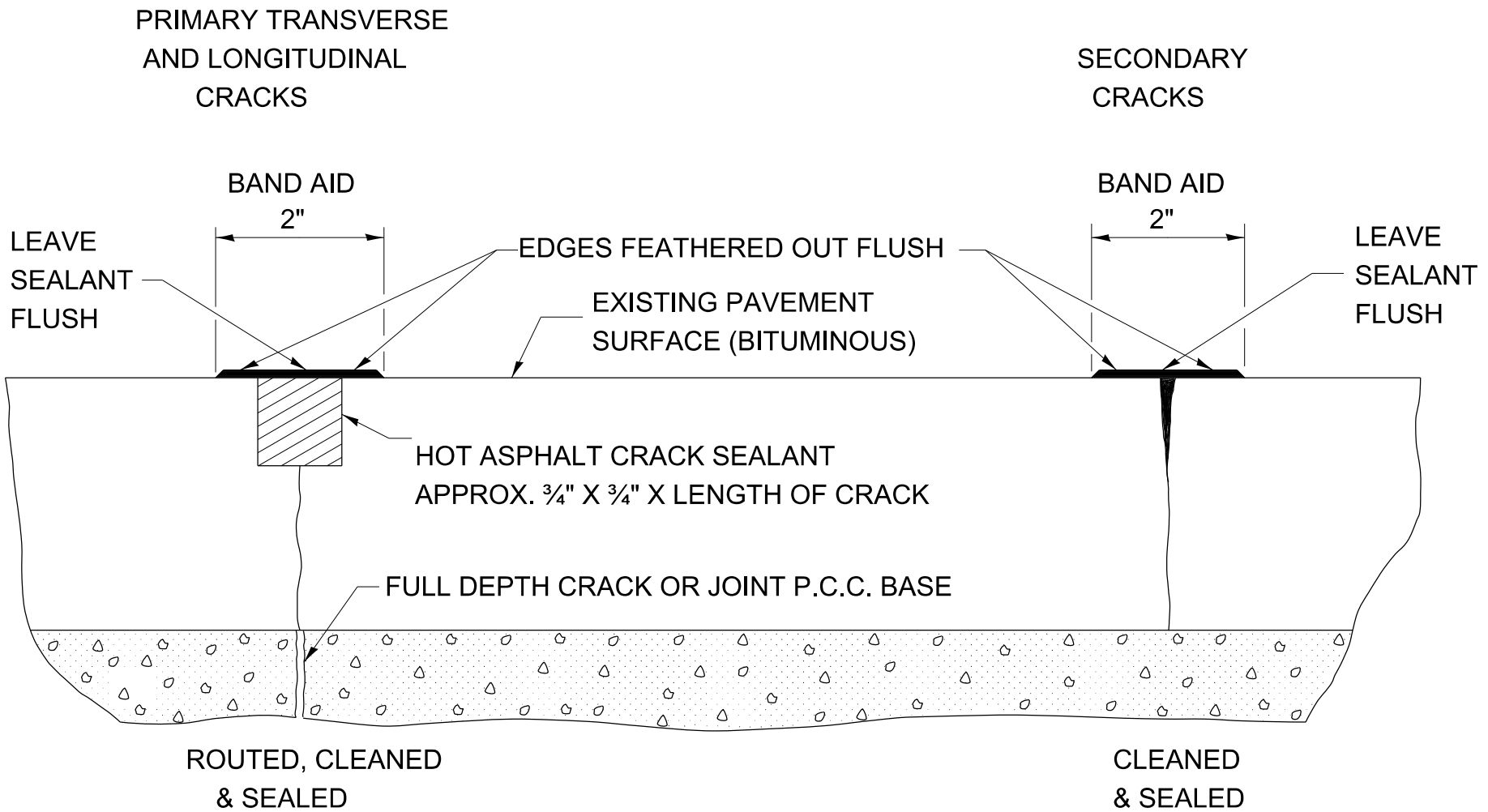
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PLOT SCALE = 100,000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 3/15/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

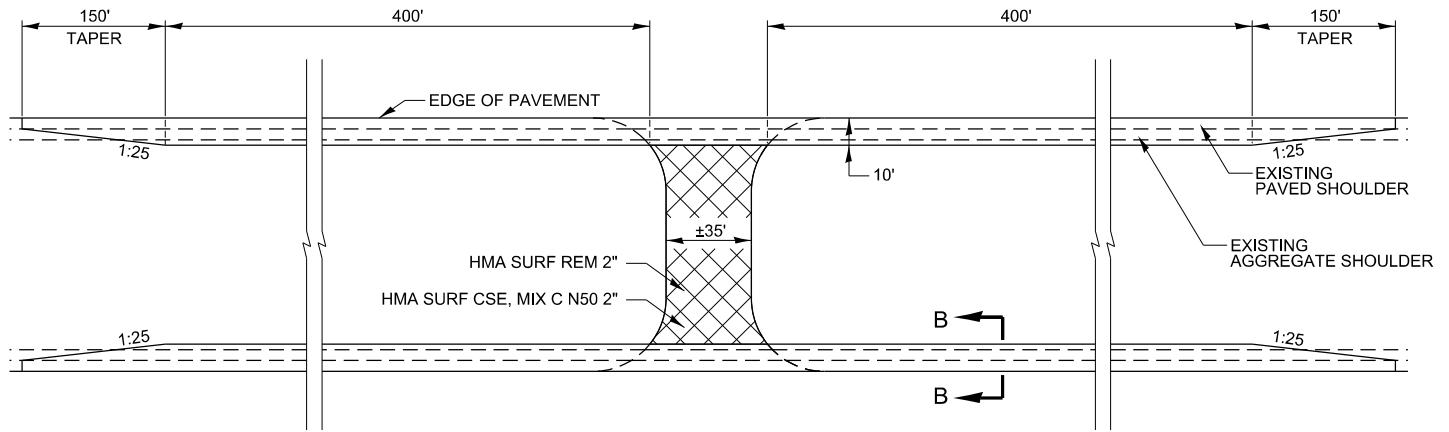
CLASS B PATCHING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

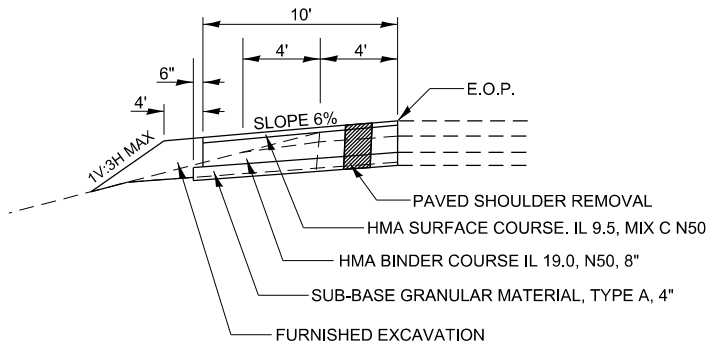
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



CRACK SEALING HMA PAVEMENT



SHOULDER WIDENING AT MAINTENANCE CROSSOVERS



NOTES

- 1.) OMIT RUMBLE STRIPS ON SHOULDERS WITHIN THESE LIMITS.
- 2.) REMOVAL OF EXISTING AGGREGATE SHOULDERS IS INCIDENTAL TO THE COST OF THE PROPOSED 8" HMA BINDER COURSE.

SECTION B-B