



I-55 at IL 59
Access Project

US 52 from River Road to Houbolt Road Public Hearing

October 10, 2024

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Illinois Department
of Transportation

The Illinois Department of Transportation, in partnership with the City of Joliet, welcomes you to the public hearing for the improvements of US 52 from River Road to Houbolt Road, in Will County. These proposed improvements are the result of the I-55 Access Study. Information shared during this presentation is available on the project website and in the exhibit hall.

Hearing Purpose

- Present the preferred alternative for US 52 from River Road to Houbolt Road
- Present Section 4(f) impacts
- Obtain public input



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The purpose of tonight's public hearing is to:

- Present the preferred alternative for US 52 from River Road to Houbolt Road,
- Present Section 4(f) impacts to Hammel Woods and the Rock Run Preserve, and
- Obtain public input

IDOT Project Phases

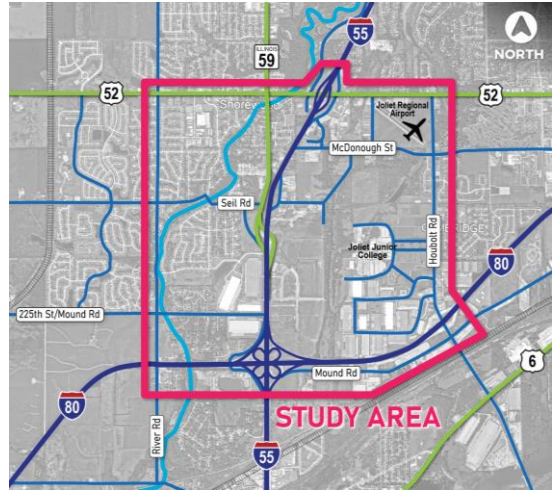


IDOT projects are typically implemented in three separate phases:

- Phase I includes preliminary engineering and environmental study. Issues are identified, current system or operational deficiencies are documented, alternative analysis studies are performed, and a preferred alternative is selected.
- Phase II includes contract plan preparation and land acquisition.
- Phase III is the actual project construction, which is most visible to the public.

I-55 Access Study

- I-55 Access Study limits
- Located within Village of Shorewood and City of Joliet



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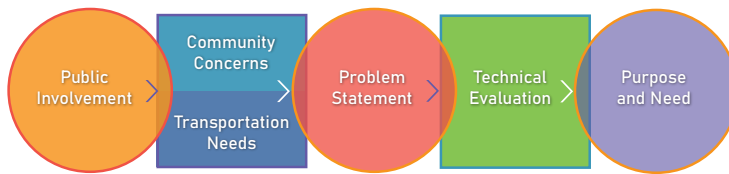
The I-55 Access Study was initiated in 2017. The study area is bordered on the south by I-80, on the east by Houbolt Road, on the north by US 52, and on the west by River Road. The study area is located in both the Village of Shorewood and the City of Joliet, in Troy Township, Will County.

I-55 Access Study

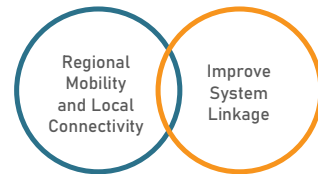
Project Purpose:

The purpose of the project is to provide an efficient transportation facility for both interstate travel, and for the regional and local roadway network accommodated by and affected by access to and from I-55 for existing and future transportation needs.

How the Purpose and Need was Developed:



Project Needs:



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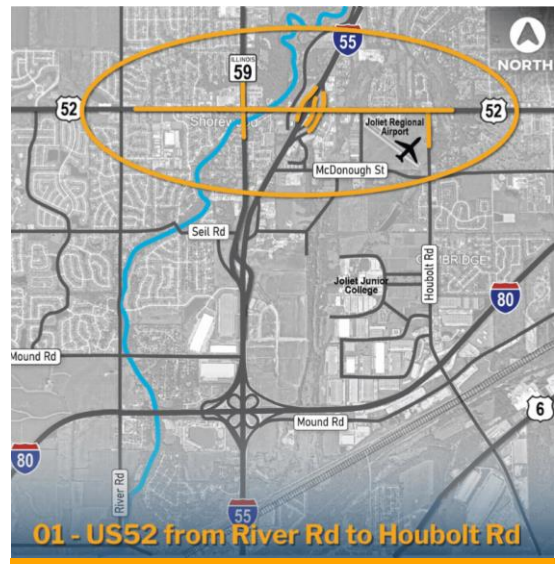
The project's Purpose and Need was developed through a rigorous public involvement process and engineering analysis.

Two distinct project needs have been identified: to improve regional mobility and local connectivity, as well as to improve system linkage.

The full Purpose and Need document is available for review in the exhibit hall and on the project website.

I-55 Access Study

- **Improvement 1**
US 52 from River Rd to Houbolt Rd
- **Improvement 2**
I-55 from I-80 to US 52
- **Improvement 3**
Olympic Blvd from I-55 E Frontage Rd to Houbolt Rd
- **Improvement 4**
Seil Rd from Raven Rd to IL 59



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The Preferred Alternative for the I-55 Access Study resulted in the identification of the following four independent improvement projects:

- Improvement 1: US 52 from River Road to Houbolt Road, which is the primary focus of today's public hearing. This improvement is under IDOT jurisdiction and highlighted in orange above.
- Improvement 2: I-55 from I-80 to US 52, which is under IDOT jurisdiction, is nearly complete and highlighted in green.
- Improvement 3: Olympic Boulevard from I-55 East Frontage Road to Houbolt Road, which is under construction, being led by the City of Joliet and is highlighted in violet.
- Improvement 4: Seil Road from Raven Road to IL 59, which is being led by the Village of Shorewood, is in Phase II and is highlighted in blue.

US 52 from River Road to Houbolt Road: Improvement Area



US 52 from River Road to Houbolt is the last of the four projects to have the Phase I Study completed and is the primary focus of tonight's public hearing. This map shows the project area for the US 52 from River Road to Houbolt Road Study.

US 52 from River Road to Houbolt Road: Proposed Improvements



- | | |
|--|---|
|  Additional Lanes and Curbed Median |  I-55 and US 52 Interchange Improvements |
|  Intersection Improvements |  Curbed Median / Turn Lanes |

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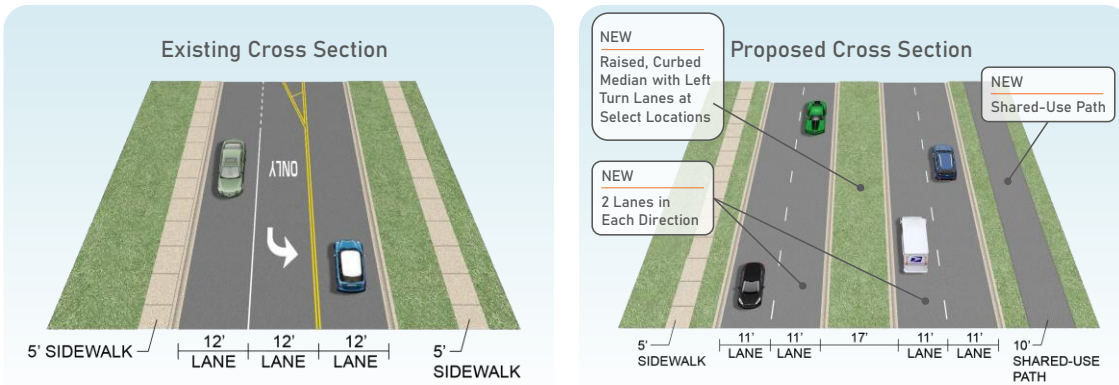
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Stakeholders have identified US 52 congestion as a major local traffic issue. The traffic and crash analysis validates these concerns. The proposed improvements address existing and future transportation mobility, operational deficiencies and improve safety along the corridor from River Road to Houbolt Road.

The preferred alternative includes the following improvements:

- Additional through lane in each direction from River Road to IL 59 to provide two travel lanes in each direction throughout the project corridor
- Raised, curbed medians throughout the corridor with left turn lanes at select locations
- Interchange improvements at I-55 and US 52
- Intersection improvements at IL 59 and Houbolt Road
- Improved connectivity for pedestrians and bicyclists throughout the corridor

US 52 from River Road to Houbolt Road: Proposed Improvements – West of IL 59

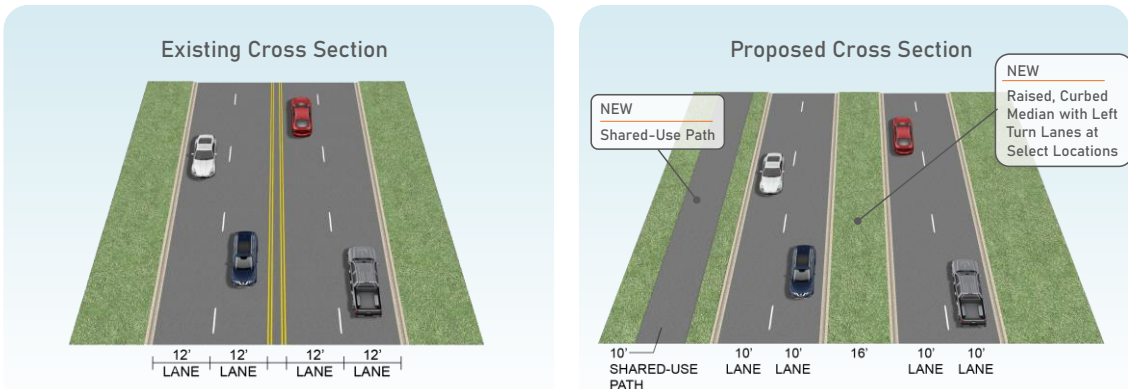


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The proposed improvements on US 52 between River Road and IL 59 include: an additional through lane in each direction and installation of a raised, curbed median with left turn lanes at select locations. A proposed off-street, two-way shared-use path will connect to the existing path near River Road with access to Shorewood residential neighborhoods, as well as the DuPage River Trail in Hammel Woods.

US 52 from River Road to Houbolt Road: Proposed Improvements – East of IL 59



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Between I-55 and Houbolt Road, the project includes the installation of a new raised, curb median with left turn lanes at select locations to improve mobility and safety, as well as a proposed off-street, two-way shared-use path that will provide connections to Joliet residential neighborhoods, Joliet Junior College, and Rock Run Preserve via the Rock Run Greenway Trail.

US 52 from River Road to Houbolt Road: Proposed Improvements - Bicycle and Pedestrian

- 2.7 miles of proposed new shared-use path
- 1.3 miles of proposed sidewalk connections
- Trail connections to DuPage River Trail, Rock Run Trail and existing trail network near River Road
- Project will create new multimodal connections over two existing barriers: DuPage River and I-55



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The Preferred Alternative includes multi-modal transportation enhancements through the proposed installation of shared-use paths and sidewalks. These support the Purpose and Need by providing local connectivity for active transportation. A total of 2.7 miles of shared-use paths and 1.3 miles of sidewalk are proposed, including a new pedestrian bridge over the DuPage River, just south of the existing US 52 roadway bridge. This new pedestrian bridge is proposed to be aesthetically similar to the bridge at the northwest corner of US 52 and River Road. These improvements will create connections between DuPage River Trail, Rock Run Trail and the existing trail network near River Rd. removing existing barriers presented by the DuPage River and I-55.

National Environmental Policy Act (NEPA) of 1969

NEPA Compliance Umbrella



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The National Environmental Policy Act (NEPA) of 1969 is a federal law that outlines policies to protect the environment. NEPA requires that federally funded projects seek to avoid, minimize and mitigate impacts to the natural and built environment. Some of the considerations include air quality, farmland, community impacts, wetlands, parklands, civil rights, endangered species and cultural resources. This study is being performed in compliance with NEPA requirements.

Section 4(f) Impacts: Hammel Woods and Rock Run Preserve



Hammel Woods

Permanent Easement - 0.03 acres

Temporary Easement - 0.07 acres

Purpose:

- Construction/maintenance of proposed retaining wall & culvert extension
- Driveway reconstruction

Rock Run Preserve

Temporary Easement - 0.073 acres

Purpose:

- Construction of shared-use path
- Rock Run Trail connection

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Section 4(f) of the U.S. Department of Transportation Act of 1966 provides for the consideration of park and recreation lands, wildlife and waterfowl refuges, and historic sites during transportation project development. There are three properties within the project limits that qualify as Section 4(f) properties. They are the Hammel Woods and Rock Run Preserve, which are owned by the Forest Preserve District of Will County, and the Joliet Regional Airport which is owned by the Joliet Park District. The project will have minimal impact (known as de minimis impact) to Hammel Woods and Rock Run Preserve. The project will not impact the Joliet Regional Airport.

Additional information regarding impacts to Forest preserve properties is available on the project website and in the exhibit hall.

Traffic Noise Study

- Two walls found to be feasible and reasonable
- Potential wall near Brookshore Drive is 670 feet long
- Potential wall along IL 59 is 770 feet long
- Wall heights range from 8 to 12 feet
- Wall implementation is dependent upon voting by benefitted receptors (5+ dB(A) traffic noise reduction)

Example Noise Abatement Wall

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A noise study was performed to evaluate the proposed improvements' traffic noise impacts. Two potential noise abatement walls were found to be feasible and reasonable. The first wall would be located on the north side of US 52 near Brookshore Drive and be 670 feet long. The second wall would be located on the east side of IL 59 north of US 52 and be 770 feet long. Wall heights would range between 8 and 12 feet.

Property owners and tenants that would achieve a benefit will have an opportunity to vote on whether the noise walls should be installed. The viewpoint solicitation was recently mailed to those benefitted by the walls.

Additional information regarding the noise study is available on the project website and in the exhibit hall.

IDOT Land Acquisition Process

- If you have any questions about land acquisition, please speak with an IDOT representative during tonight's hearing

OWNERSHIP DETERMINATION

APPRAISAL

NEGOTIATION

COURT PROCEEDINGS (IF NECESSARY)

RELOCATION

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IDOT's process for land acquisition complies with federal Uniform Relocation and Assistance and Real Property Acquisition Policies Act (URA). The following activities would not begin until the Phase I Study is complete.

- Step 1: Determine ownership and prepare plat of survey
- Step 2: Prepare independent appraisal
- Step 3: Negotiations with property owners
- Step 4: Court proceedings: This is only necessary if an agreement on acquisition price cannot be reached or if there are title issues or liens on properties.
- Step 5: Relocation

The land acquisition process typically requires between 1 and 3 years. If you have any questions about land acquisition, please speak with an IDOT representative during tonight's hearing.

Funding and Project Phasing

- Phase I approval anticipated by end of 2024 or early 2025
- Estimated construction cost: \$54 million
- Included in later years of IDOT's FY 2025-2030 Proposed Highway Improvement Program

Phase I for US 52 improvements from River Road to Houbolt Road is anticipated to be completed and receive design approval by the end of 2024 or early 2025. The estimated construction cost for this roadway improvement project is \$54 million.

This project is currently included in IDOT's Fiscal Years 2025-2030 Proposed Highway Improvement Program.

Next Steps

- Evaluate public comments
- Comment period ends November 12, 2024
- Refine design
- Environmental documentation
- Complete Phase I study



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Your interest and input are critical to the success of this project! All comments received will be considered in the study. Comments received by November 12, 2024, will be included as part of the hearing's official public record.

After the hearing, IDOT will refine the proposed improvement designs and complete environmental documentation. The Phase I study will then conclude, and the project will move into Phases II and III. Phases II and III are expected to take approximately 2-3 years each.

US 52 from River Road to Houbolt Road

Public Forum
6:00 p.m.

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Tonight's public hearing includes a Public Forum, which will begin at 6:00 p.m.

Public Forum Rules

Forum Rules:

- All speakers **MUST** complete a Public Forum sign-up form
- Comments are limited to two minutes maximum

 **US 52 from River Road to Houbolt Road Public Hearing – October 10, 2024**
Public Forum Sign-up

*All speakers will have a maximum of two minutes each to present their comments to a court reporter.

A moderator will announce the speaker's name in the order the forms are received. The Illinois Department of Transportation welcomes your comments and requests that you present your remarks in a respectful manner, within the established two-minute time limit.

Name: _____

Address: _____

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The Public Forum is an opportunity to make comments about the proposed improvements. The Forum will be held in the adjacent room. Individuals wanting to speak must complete a Public Forum sign-up form and give it to a project team member. Forms are available at the registration table. Speakers will have a maximum of two minutes, and comments will be recorded by a court reporter.

Please Provide Your Comments!

- Project website
- Email
- Mail
- Comments period ends on November 12, 2024



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You may also provide your comments to the project team using a comment form or through the project website, by dropping your comment in the comment box provided in the next room, email or mail. Contact information and comment forms are available in the hearing brochure and at the registration table. Comments received by November 12, 2024, will be included as part of the hearing's official public record.



**I-55 at IL 59
Access Project**

US 52 from River Road to Houbolt Road

Thank you for attending!

This presentation will restart momentarily.

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Thank you for attending tonight's public hearing. If you have not already, please review the exhibits and discuss them with the project study team in the adjacent room. All material from tonight's hearing is available on the project's website. This presentation will restart from the beginning momentarily.