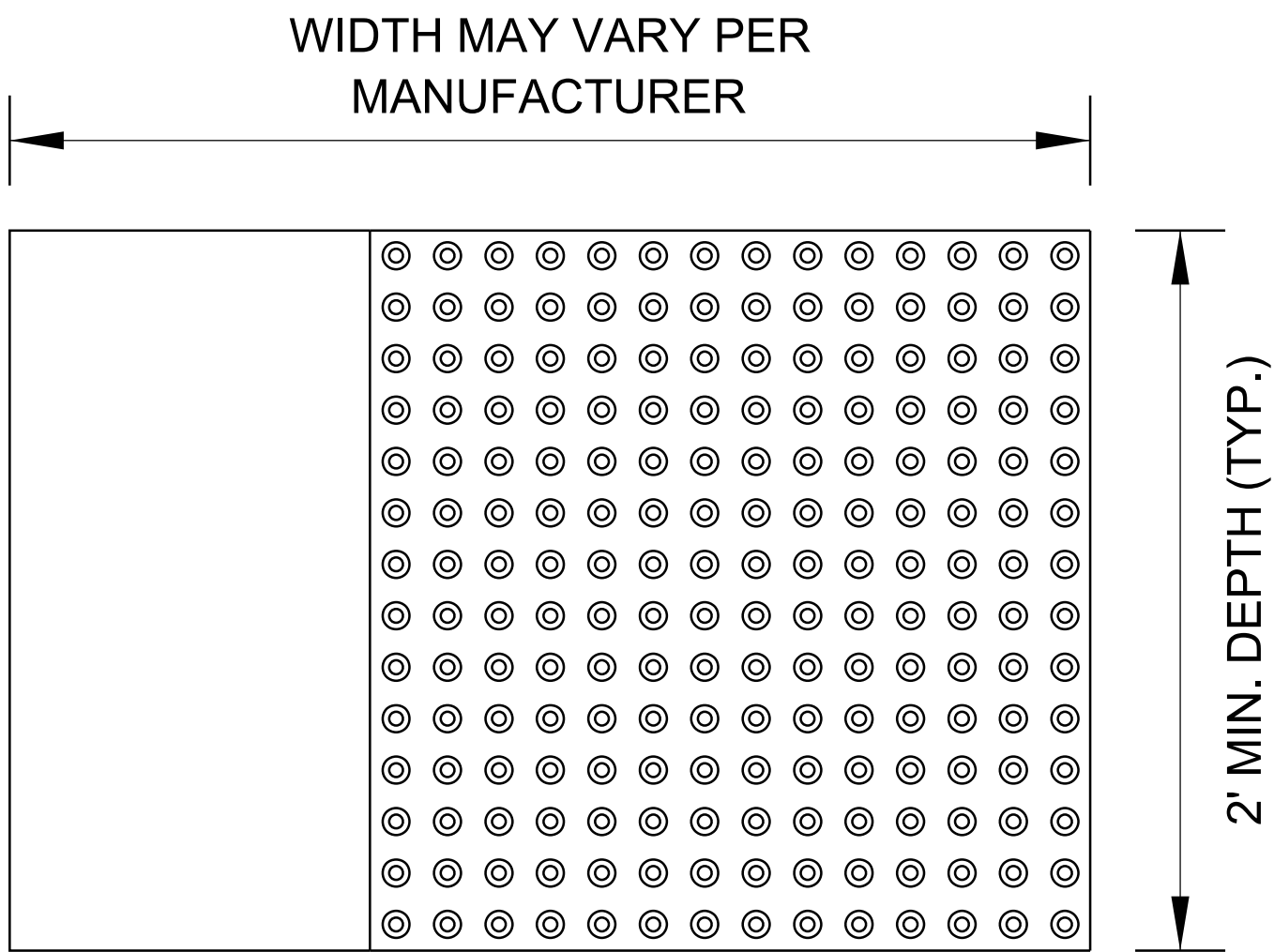
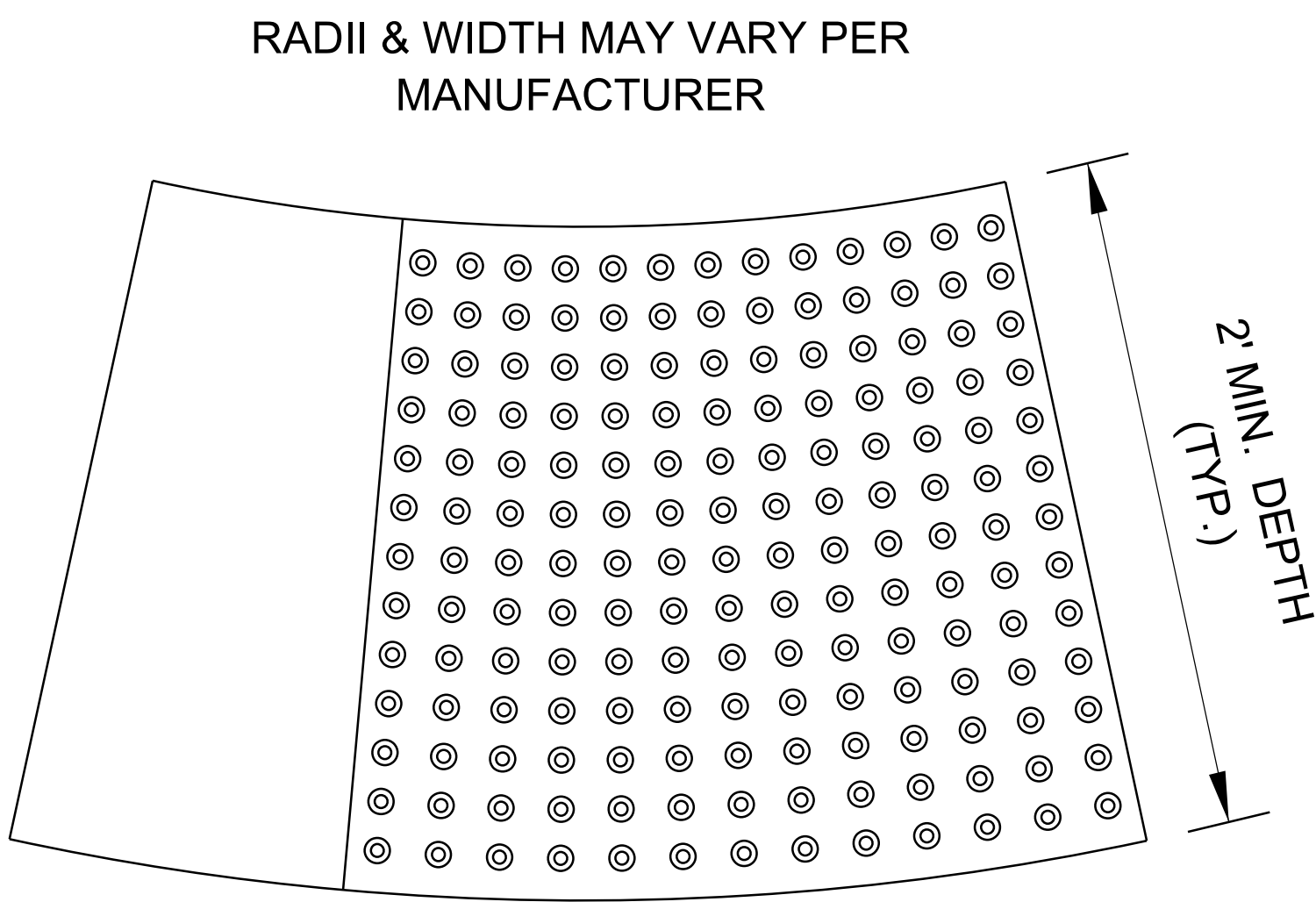


STRAIGHT DETECTABLE WARNING UNITS



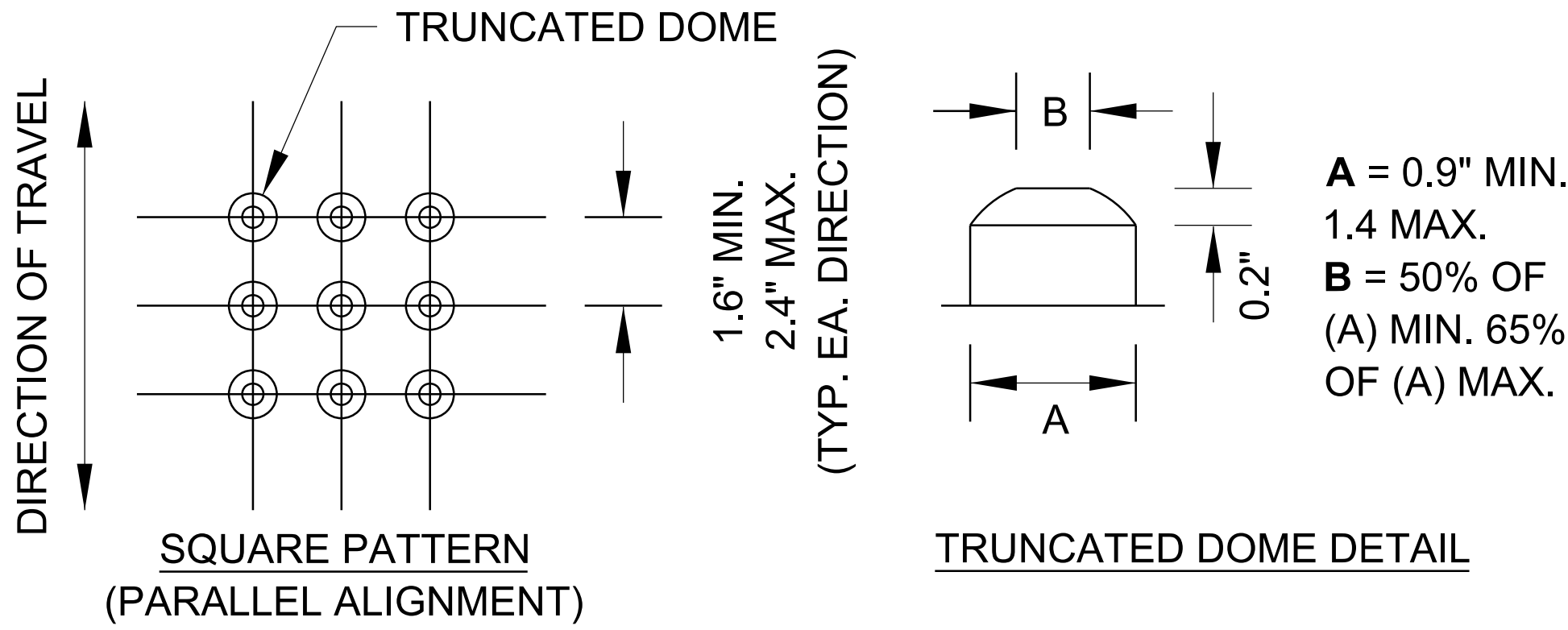
RADIAL DETECTABLE WARNING UNITS



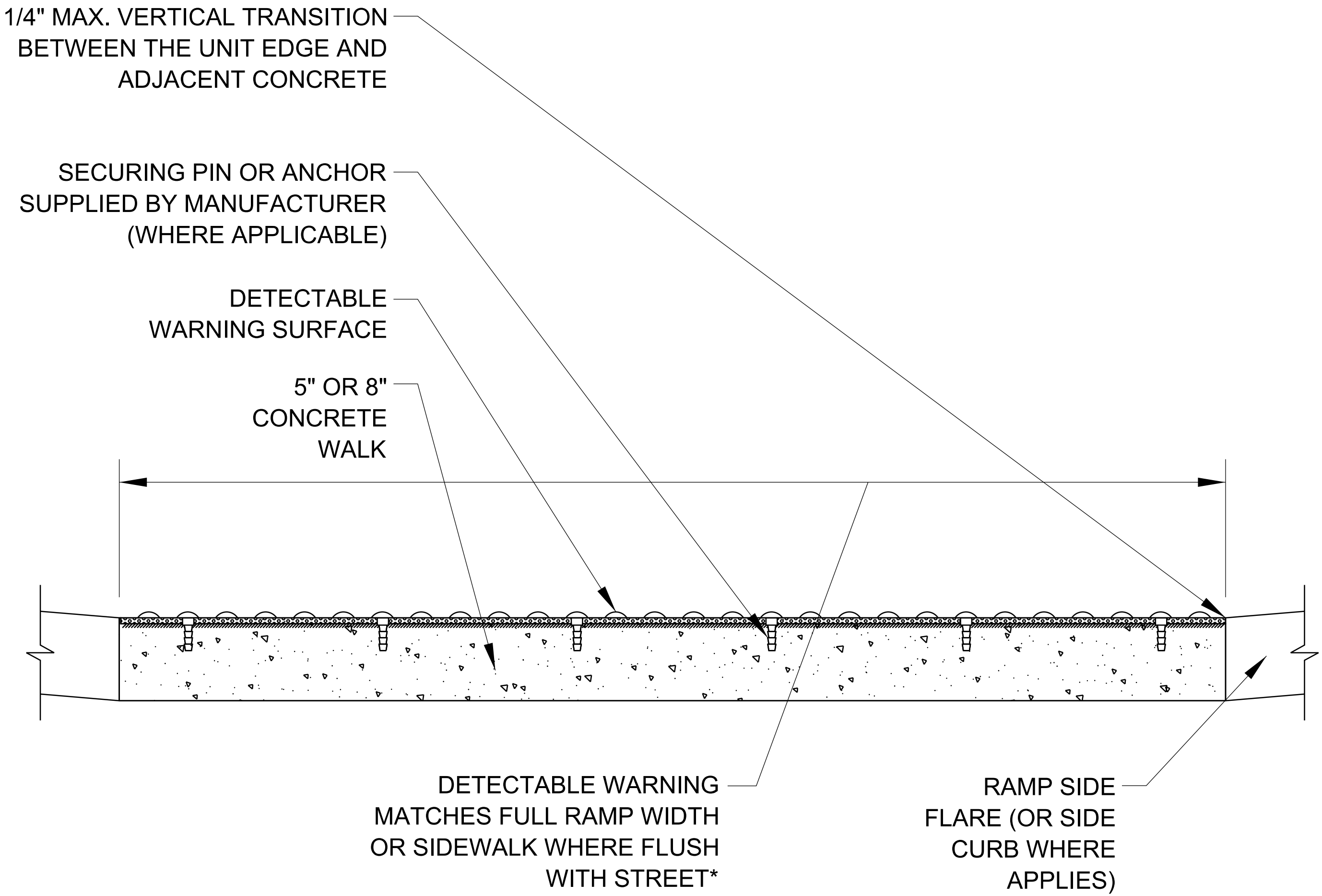
DETECTABLE WARNING UNIT SIZES

- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:
THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

MODEL: Default
FILE NAME: p:\planroom.dot\Illinois.gov-P\WIDOT\Documents\DOT Offices\District 1\Projects\Bldg5422x31\CADData\CADsheets\bd58.dgn
8/5/2019 10:08:20 AM User:footemj

	USER NAME = footemj		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CITY OF CHICAGO DETECTABLE WARNINGS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -		REVIS	REVISED -									
	PLOT SCALE = 50.0000 ' / in.		CHECKED -	REVISED -					BD 58		CONTRACT NO.		
	PLOT DATE = 10/8/2019		DATE - 06-20-2017	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		