

FOR INDEX OF SHEETS, SEE SHEET NO. 2

2022 ADT = 24,700

TRAFFIC DATA

MU% =

SU% =

MU% =

PV% =

SU% =

MU% =

US_45_UNDER 1-74_

2022 ADT = 17,300

FOR SUMMARY OF QUANTITIES, SEE SHEETS NO. 3-7

EAST OF LINCOLN AVE. TO WEST OF US 45

25,300

84.4%

2.6%

13.0%

1-74 (EB)

82.6%

2.4%

15.0%

AT US 45 OVERHEAD (BETWEEN RAMPS) **J**-74

83.5%

2.5%

14.0%

94.8%

3.5%

1.7%

FUNCTIONAL CLASSIFICATION

US 45 - OTHER PRINCIPAL ARTERIAL

I-74 - INTERSTATE

0

0

12-13-2024 SPECIAL LETTING ITEM 005 STATE OF ILLINOIS

(14HB)BR.(14B)BR-1 CHAMPAIGN 90 1 CONTRACT NO. 70D76

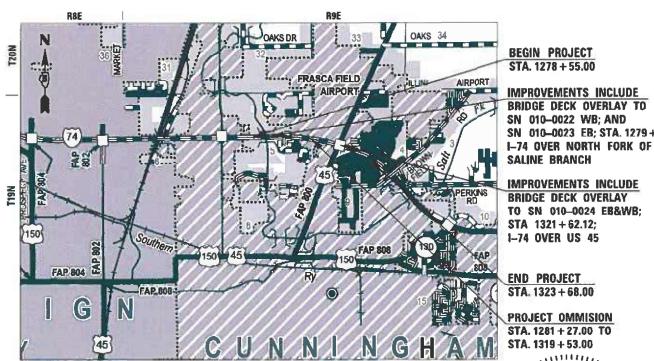
DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

FAI ROUTE 74 (I–74) SECTION (14HB)BR,(14B)BR-1 PROJECT NHPP-N0L7(973) BRIDGE DECK OVERLAY BRIDGE DECK REPAIR CHAMPAIGN COUNTY

C-95-069-22

AT SALINE BRANCH 0.8 MILES W OF US 45 & AT US 45 N OF URBANA



END PROJECT STA. 1323 + 68.00

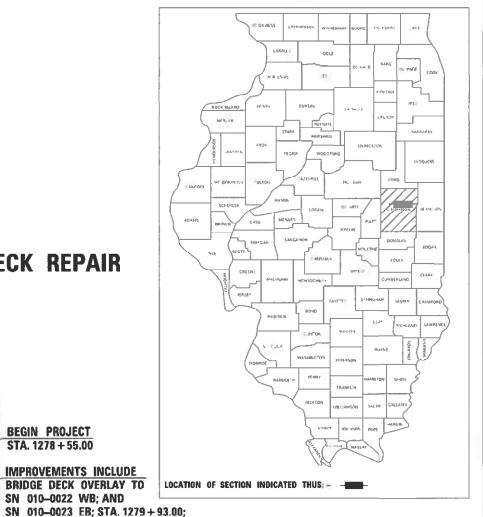
BEGIN PROJECT STA. 1278 + 55.00

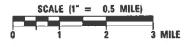
IMPROVEMENTS INCLUDE

IMPROVEMENTS INCLUDE BRIDGE DECK OVERLAY TO SN 010-0024 EB&WB;

PROJECT OMMISION STA. 1281 + 27.00 TO STA. 1319 + 53.00

P-95-028-22





PROJECT LENGTH = 287.00 FT. = 0.054 MILES TOTAL PROJECT LENGTH = 287.00 FT. = 0.054 MILES

LOCATION MAP

ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123 OR 811 TOWNSHIP(S): CUNNINGHAM

PROJECT ENGINEER: RYAN CARROLL, PE. SENIOR SQUAD LEADER: CLARE DIETZ, PE. (217) 465-4181

CONTRACT NO. 70D76

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET #	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS AND GENERAL NOTES
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8-17	TYPICAL SECTIONS
18-23	SCHEDULE OF QUANTITIES
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25-31	PLAN SHEETS
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48-52	PAVEMENT MARKING PLANS
53	ALTERNATE ROUTE SIGNING PLAN
54-67	STRUCTURAL DETAILS (SN 010-0022, 0023; I-74 OVER SALINE BRANCH)
68-84	STRUCTURAL DETAILS (SN 010-0024, I-74 OVER US 45)
85-86	DETAILS
87	D5 DETAIL NO. 7800BBBB-6IN, PAVEMENT MARKING
88	D5 DETAIL NO. X032AAAA, REAL-TIME TRAFFIC CONTROL SYSTEM
89	D5 DETAIL TEMPORARY RUMBLE STRIPS (SPECIAL)
90	D5 DETAIL NO. X7200201, WIDTH RESTRICTION SIGNING

LIST OF HIGHWAY STANDARDS

DESCRIPTION
STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
AREAS OF REINFORCEMENT BARS
DECIMAL OF AN INCH AND OF A FOOT
NAME PLATES FOR STRUCTURES
SHOULDER RUMBLE STRIPS, 16 IN.
OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
LANE CLOSURE, FREEWAY/ EXPRESSWAY
LANE CLOSURE, FREEWAY/ EXPRESSWAY, WITH BARRIER
LANE CLOSURE, MULTILANE, AT ENTERANCE OF EXIT RAMP, FOR SPEEDS > 45 MPH
LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS > 45 MPH
TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
TWO LANE CLOSURE FREEWAY/EXPRESSWAY
URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
TRAFFIC CONTROL DEVICES
TEMPORARY CONCRETE BARRIER
TYPICAL PAVEMENT MARKINGS
TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

G.N.-100B

MICROSTATION AND GEOPAK FILES OF THIS PROJECT WILL BE MADE AVAILABLE TO THE CONTRACTOR AFTER CONTRACT AWARD. IF THERE IS A CONFLICT BETWEEN THE ELECTRONIC FILES AND THE PRINTED CONTRACT PLANS AND DOCUMENTS, THE PRINTED CONTRACT PLANS AND DOCUMENTS SHALL TAKE PRECEDENCE OVER THE ELECTRONIC FILES. THE CONTRACTOR SHALL ACCEPT ALL RISK ASSOCIATED WITH USING THE ELECTRONIC FILES AND SHALL HOLD THE DEPARTMENT HARMLESS FOR ANY ERRORS OR OMISSIONS IN THE ELECTRONIC FILES AND THE DATA CONTAINED THEREIN. ERRORS OR DELAYS RESULTING FROM THE USE OF THE ELECTRONIC FILES BY THE CONTRACTOR SHALL NOT RESULT IN AN EXTENSION OF TIME FOR ANY INTERIM OR FINAL COMPLETION DATE OR SHALL NOT BE CONSIDERED CAUSE FOR ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL NOT USE, SHARE, OR DISTRIBUTE THESE ELECTRONIC FILES EXCEPT FOR THE PURPOSE OF CONSTRUCTING THIS CONTRACT. ANY CLAIMS BY THIRD PARTIES DUE TO USE OR ERRORS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL INCLUDE THIS DISCLAIMER WITH THE TRANSFER OF THESE ELECTRONIC FILES TO ANY OTHER PARTIES AND SHALL INCLUDE APPROPRIATE LANGUAGE BINDING THEM TO SIMILAR RESPONSIBILITIES.

G.N.-105.09A

ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).

G.N.-406H

MIXTURE REQUIREMENTS: FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT

LOCATION:	I - 74				
MIXTURE USE(S):	MAINLINE 11/2" SURFACE & STAGED SHOULDER TOP 2"				
AG/PG	SBS PG 70-22				
DESIGN AIR VOIDS:	4.0% @ NDES = 90				
MIXTURE COMPOSITION:	IL 9.5				
FRICTION AGGREGATE:	MIX E				
MIXTURE WEIGHT:	112 LBS/SQ YD/INCH				
QUALITY MANAGEMENT PROGRAM:	QC/QA				
SUBLOT SIZE:	3000				
MATERIAL TRANSFER DEVICE (REQUIRED)	NO				

G.N.-501A

THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHOULD FOLLOW THE GUIDE BRIDGE SPECIAL PROVISIONS FOR CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.

G.N.-703A

SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (TACK COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES, SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G.N.-781

THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS.

COMMITMENTS:

NO COMMITMENTS

APPLICATION RATES:

THE FOLLOWING APPLICATION RATES HAVE BEEN ASSUMED IN CALCULCATING PLAN QUANTITIES.

HOT-MIX ASPHALT = 112 LBS/SQ YD/IN BITUMINOUS MATERIALS (TACK COAT) = 0.05 LBS/SQ FT

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS
AND GENERAL NOTES

SHEET 1 OF 1 SHEETS STA. TO STA.

 F.A.I. RTE.
 SECTION
 COUNTY SHEETS NO.
 TOTAL SHEETS NO.
 SHEETS NO.

 74
 (14HB)BR,(14B)BR-1
 CHAMPAIGN CONTRACT
 9
 2

 CONTRACT NO. 70D76

 IILLINOIS FED. AID PROJECT

CODE NO. 28100105 STO	SUMMARY OF QUANTITIES ITEM	UNIT	TOTAL	FAI ROUTE 74 (I-74) INTERSTATE CHAMPAIGN COUNTY 90% FEDERAL 10% STATE	FAI ROUTE 74 (I-74) INTERSTATE CHAMPAIGN COUNTY 90% FEDERAL 10% STATE
NO.		UNIT	TOTAL	CHAMPAIGN COUNTY 90% FEDERAL 10% STATE	CHAMPAIGN COUNTY 90% FEDERAL
NO.		UNIT	TOTAL	COUNTY 90% FEDERAL 10% STATE	COUNTY 90% FEDERAL
NO.		UNIT	TOTAL	90% FEDERAL 10% STATE	90% FEDERAL
NO.		UNIT	TOTAL	10% STATE	
NO.		UNIT			
		UNIT		SN 010-0022 &	SN 010-0024
		UNIT		SN 010-0023	J
		01111	QUANTITY	0059	0059
28100105 STC			QO/MITTI		0000
	ONE RIPRAP, CLASS A3	SQ YD	3		3
28200200 FILT	TER FABRIC	SQ YD	3		3
40600290 BITI	TUMINOUS MATERIALS (TACK COAT)	POUND	7752	4092	3660
40600982 HOT	T-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	2747	1534	1213
40604174 POL	LYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N90	TON	1841	973	868
44000157 HOT	T-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	14072	7469	6603
48102100 AGC	GREGATE WEDGE SHOULDER, TYPE B	TON	159	92	67
10.00100				1	
50102400 CON	NCRETE REMOVAL	CU YD	55.5	33.2	22.3
50157300 PRO	OTECTIVE SHIELD	SQ YD	1047		1047
50300255 CON	NCRETE SUPERSTRUCTURE	CU YD	64.7	42.4	22.3
					2011
50300300 PRO	OTECTIVE COAT	SQ YD	4387	1773	2614
50500405 FUR	RNISHING AND ERECTING STRUCTURAL STEEL	POUND	27000	+	27000
101		. 50115	2.000	1	2.555
50800205 REII	INFORCEMENT BARS, EPOXY COATED	POUND	5480	2440	3040
50800515 BAF * DENOTES SPEC	R SPLICERS	EACH	124	24	100

	USER NAME = sskinner	DESIGNED -	REVISED -
BACON FARMER WORKMAN		DRAWN	REVISED
ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED
	PLOT DATE = 8/15/2024	DATE	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

		SUMI	ИАР	Y OF O	UANTITIE	S	F.A.I. RTE.	SECTION		COUNTY	TOTAL	SHEET NO.
							74	(14HB)BR,(14B)BR-	1	CHAMPAIGN	90	3
I–74								CONTRACT	NO. 7	0D76		
SHEET	1	OF	5	SHEETS	STA.	TO STA.		ILLINOIS	FED. A	D PROJECT		

	SUMMARY OF QUANTITIES	FAI ROUTE 74 (I-74)	FAI ROUTE 74 (I-74)		
				INTERSTATE	INTERSTATE
				CHAMPAIGN	CHAMPAIGN
				COUNTY	COUNTY
				90% FEDERAL	90% FEDERAL
CODE			TOTAL	10% STATE	10% STATE
				SN 010-0022 &	SN 010-0024
				SN 010-0023	
NO.	ITEM	UNIT	QUANTITY	0059	0059
52000110	PREFORMED JOINT STRIP SEAL	FOOT	188		188
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	32		32
32100020	ELASTOMENIO BEANNO ASSEMBET, TITE II	LACIT	- J2		32
52100520	ANCHOR BOLTS, 1"	EACH	64		64
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	14	7	7
00300303	TRAINES AND EIDS TO BE ADSSOTED	LAGIT	1.7	,	
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	12624	6800	5824
67000500	ENGINEER'S FILED OFFICE, TYPE B	CAL MO	12	6	6
0,00000	ENGINEERO NEED OFFICE, THE B	37 LE 1110			
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70107005	PAVEMENT MARKING BLACKOUT TAPE, 5"	FOOT	19397	11047	8350
70107007	PAVEMENT MARKING BLACKOUT TAPE, 7"	FOOT	1190	1190	
70107016	PAVEMENT MARKING BLACKOUT TAPE, 16"	FOOT	50	50	
70407007	CUANOFARI E MECCACE CION	041.53			
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	7	21
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	0.5	0.5
70300100	SHORT TERM PAVEMENT MARKING	FOOT	461	252	209
		''	701		
	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	26605	15318	11287
* DENOTES S	SPECIALTY ITEM				

	US
BACON FARMER WORKMAN	
ENGINEERING & TESTING, INC.	PL
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	USER NAME ■ sskinner	DESIGNED -	REVISED -
MAN		DRAWN -	REVISED -
INC.	PLOT SCALE ■ 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE ■ 8/15/2024	DATE -	REVISED -

SCALE:

	SUMMARY OF QUANTITIES				F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS NO. N 90 4 CT NO. 70D76			
I–74						74	(14HB)BR,(14B)BR•1	CHAMPAIGN	90	4	
<u> </u>						CONTRACT	NO. 70	D76			
SHEET	2	OF	5	SHEETS	STA.	TO STA.	ILLINOIS FED. A		AID PROJECT		

	SUMMARY OF QUANTITIES			FAI ROUTE 74 (I-74)	FAI ROUTE 74 (I-74)
1				INTERSTATE	INTERSTATE
				CHAMPAIGN	CHAMPAIGN
1				COUNTY	COUNTY
				90% FEDERAL	90% FEDERAL
CODE			TOTAL	10% STATE	10% STATE
			a colored service year.	SN 010-0022 &	SN 010-0024
ĺ				SN 010-0023	
NO.	ITEM	UNIT	QUANTITY	0059	0059
70300221	TEMPORARY PAVEMENT MARKING - LINE 4" - PAINT	FOOT	3365	1328	2037
70200244	TEMPODADY DAVEMENT MADIZING, LINE CIL DAINT	ГООТ	240	240	
70300241	TEMPORARY PAVEMENT MARKING - LINE 6" - PAINT	FOOT	210	210	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4"- TYPE IV TAPE	FOOT	51915	29150	22765
				20.00	
70307130	TEMPORARY PAVEMENT MARKING - LINE 6"- TYPE IV TAPE	FOOT	840	540	300
70400100	TEMPORARY CONCRETE BARRIER	FOOT	2600	1150	1450
anne na seuro e recognistrativo		2000	659657700955550	B 300000000	200000000
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2325	1150	1175
70000000	IMPACT ATTENUATORS TEMPORARY (NON REDIRECTIVE) TEST LEVEL 2	FACIL		2	2
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2
1					
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2
	INIT ACT ATTENDATORS, REESCATE (NON-REDIRECTIVE), TEST EEVEE S	LAGIT	7	<u>, </u>	-
1					
78008330	POLYUREA PAVEMENT MARKING TYPE II - LINE 6"	FOOT	16489	8386	8103
78008340	POLYUREA PAVEMENT MARKING TYPE II - LINE 8"	FOOT	290	290	
<u> </u>					
78008350	POLYUREA PAVEMENT MARKING TYPE II - LINE 12"	FOOT	50	50	
70044025	CDOOMING FOR RECESSED DAYEMENT MARKING 7"	FOOT	44400	7050	6040
78011035	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	14468	7658	6810
1					
78011045	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	290	290	
	THE PROPERTY OF THE PARTY OF TH	1001			
1					
78011065	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	50	50	

BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

	SUMMARY OF QUANTITIES	FAI ROUTE 74 (I-74)	FAI ROUTE 74 (I-74)		
				INTERSTATE	INTERSTATE
				CHAMPAIGN	CHAMPAIGN
				COUNTY	COUNTY
CODE			TOTAL	90% FEDERAL 10% STATE	90% FEDERAL 10% STATE
				SN 010-0022 & SN 010-0023	SN 010-0024
NO.	ITEM	UNIT	QUANTITY	0059	0059
78100300	REPLACEMENT REFLECTOR	EACH	579	326	253
78200011	PARRIER WALL REFLECTORS TYRE C	EACH	628	317	311
78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	628	317	311
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	2694	1163	1531
X6020050	INLET BOXES TO BE ADJUSTED (SPECIAL)	EACH	4	2	2
A6020050	INLET BOXES TO BE ADJUSTED (SPECIAL)	EACH	4	2	2
X6050222	FILLING INLETS, TEMPORARY	EACH	4	2	2
X7010118	TEMPORARY RUMBLE STRIPS (SPECIAL)	EACH	8	4	4
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	0.5	0.5
X7011854	REAL-TIME TRAFFIC CONTROL CENTRAL BASE UNIT	CAL MO	9	4.5	4.5
X7011860	REAL-TIME TRAFFIC CONTROL WARNING SIGN	CAL MO	72	36	36
X7011862	REAL-TIME TRAFFIC MONITORING SENSOR UNIT	CAL MO	144	72	72
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1		1
7/200201	WIDTH RESTRICTION SIGNING	L 30W	I		1
X7200202	ALTERNATE ROUTE SIGNING	L SUM	1	0.5	0.5
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	32		32
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	17100		17100

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į	BACON FARMER WORKMAN	
	ENGINEERING & TESTING, INC.	Р
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

			SUMI	VIAF	Y OF C	UANTITIE	S	F.A.I. SECTION			COUNTY	TOTAL SHEETS	SHEET NO.
	I–74								(14HB)BR,(14B)BR-1		CHAMPAIGN	90	6
											CONTRACT	NO. 70	DD76
	SHEET	4	OF	5	SHEETS	STA.	TO STA.		ILLINOIS	FED. AI	D PROJECT		

	SUMMARY OF QUANTITIES		FAI ROUTE 74 (I-74)	FAI ROUTE 74 (I-74)	
				INTERSTATE	INTERSTATE
				CHAMPAIGN	CHAMPAIGN
				COUNTY	COUNTY
			I	90% FEDERAL	90% FEDERAL
CODE			TOTAL	10% STATE	10% STATE
JOBE			I	SN 010-0022 &	SN 010-0024
				SN 010-0022 Q	314 0 10-0024
NO.	ITEM	UNIT	QUANTITY	0059	0059
NO.	I I EIVI	UNIT	QUANTITY	0009	0059
Z0006014	BRIDGE DECK LATEX CONCRETE OVELAY, 2 1/2 INCHES	SQ YD	4368	1773	2595
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	4368	1773	2595
70040754	OTPHOTUDAL PEDAID OF CONODETE (PEDTU FOUND TO OD LEGO THAN 5 WIGHTS)	00.57			
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	32		32
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	66	17	49
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	4368	1773	2595
	, ,				
Z0076600	TRAINEES	HOUR	1,500	1,500	
20070000		HOOK	1,500	1,300	
70076604	TRAINIFEC TRAINING PROCRAM CRADULATE	HOUR	1 500	1.500	
Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	1,500	1,500	
* DEMOTES	CDECIAL TV ITEM				
* DENOTES	SPECIALTY ITEM				

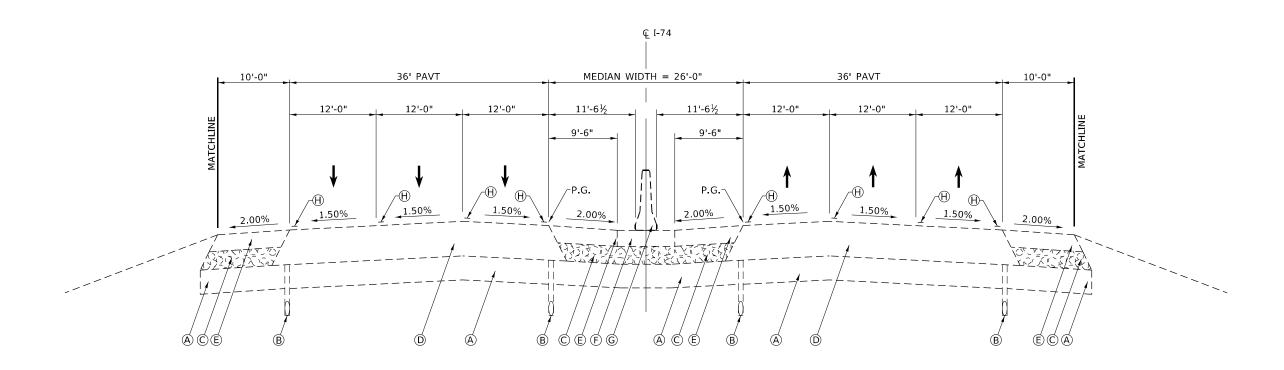
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1		USER NAME = sskinner	DESIGNED -	REVISED -
Ĺ	BACON FARMER WORKMAN ENGINEERING & TESTING, INC.		DRAWN -	REVISED -
1	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
		PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:

		SUMI	VIAF	Y OF C	UANTITIE	F.A.I. SECTION			COUNTY	TOTAL SHEETS	SHEET NO.	
I–74								(14HB)BR,(14B)BR	-1	CHAMPAIGN	90	7
				1-/-						CONTRACT	NO. 70	D76
SHEET	5	OF	5	SHEETS	STA.	TO STA.		ILLINOIS	FED. A	D PROJECT		



EXISTING TYPICAL SECTION (A) I-74, SIX LANE

WB LANES

LT STA 1271+60 TO LT STA 1278+55 LT STA 1279+15 TO LT STA 1280+60 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK LT STA 1281+20 TO LT STA 1288+25

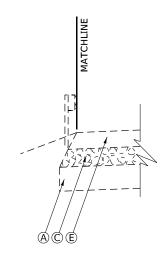
EB LANES

RT STA 1271+60 TO RT STA 1279+21 RT STA 1279+21 TO RT STA 1280+67 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK RT STA 1281+27 TO RT STA 1288+25

EXISTING LEGEND

- (A) LIME MODIFIED SOILS, 12"
- B PIPE UNDERDRAIN, 4" (TYP)
 C SUB-BASE GRANULAR MATERIAL, TYPE C
- $\widecheck{\mathbb{D}}$ HMA PAVEMENT, 17.25"
- E HMA SHOULDERS, 8.25"
- (F) PCC BARRIER BASE, 8"
- © CONCRETE MEDIAN BARRIER

 © EXISITING PAVEMENT MARKING LINE



EX GUARDRAIL SECTION

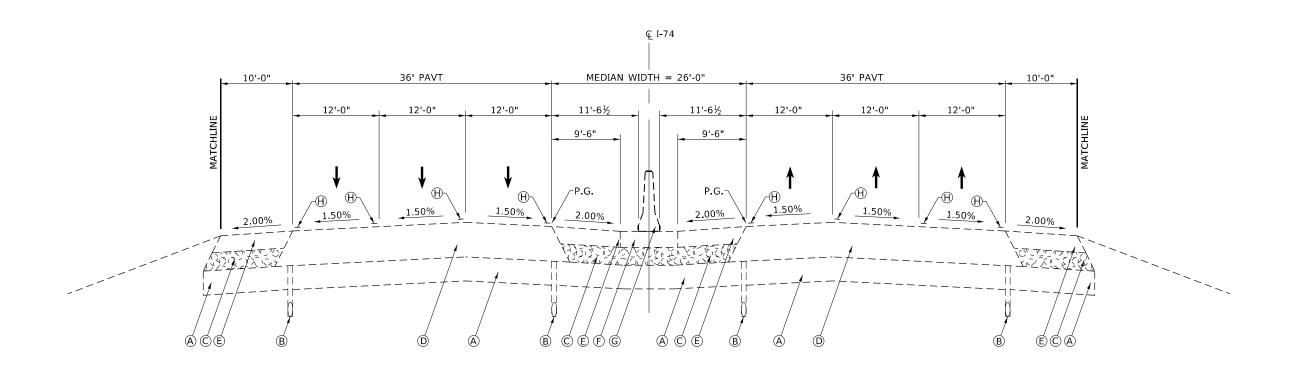
:			USER NA
	DEM	BACON FARMER WORKMAN	
	DL AA	ENGINEERING & TESTING, INC.	PLOT SCA
1			PLOT DA

	USER NAME = sclyne	DESIGNED -	REVISED -
V		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SCALE: N.T.S.

				I-74			F.A.I. RTE	SECTION		COUNTY	TOTA
EXISTING TYPICAL SECTION							74	(14HB)BR,(14B)B	R-1	CHAMPAIGN	90
		.AJO I J	NU	I II JUA	L SECTION				CONTRACT	NO.	
CHEET	1	O.E.	5	SHEETS	CTA	TO STA		TURNOT	- cco 4	D DDOLEGE	



EXISTING TYPICAL SECTION (B) I-74, SIX LANE

WB LANES

LT STA 1278+55 TO LT STA 1279+15 LT STA 1279+15 TO LT STA 1280+60 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK LT STA 1280+60 TO LT STA 1281+20

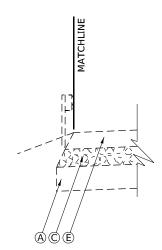
EB LANES

RT STA 1278+61 TO RT STA 1279+21 RT STA 1279+21 TO RT STA 1280+67 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK RT STA 1280+67 TO RT STA 1281+27

EXISTING LEGEND

- (A) LIME MODIFIED SOILS, 12"
- B PIPE UNDERDRAIN, 4" (TYP)
- SUB-BASE GRANULAR MATERIAL, TYPE C
- $\widecheck{\mathbb{D}}$ HMA PAVEMENT, 17.25"
- E HMA SHOULDERS, 8.25"
- (F) PCC BARRIER BASE, 8"
- © CONCRETE MEDIAN BARRIER

 EXISTING PAVEMENT MARKING LINE



EX GUARDRAIL SECTION

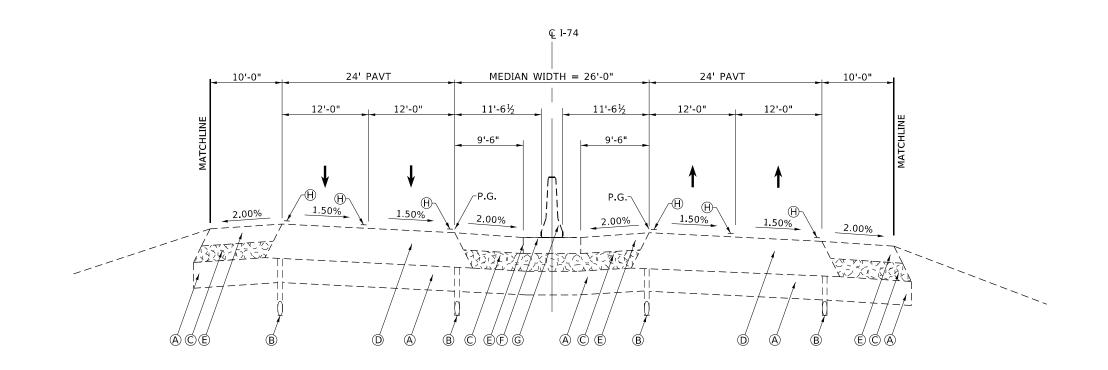
5			USER 1
	DEM	BACON FARMER WORKMAN	
	DL AA	ENGINEERING & TESTING, INC.	PLOT S
1			PLOT D

	USER NAME = sclyne	DESIGNED -	REVISED -
١N		DRAWN -	REVISED -
×	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SCALE: N.T.S.

				I-74			F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EXISTING TYPICAL SECTION							74	(14HB)BR,(14B)BR-1	CHAMPAIGN	90	9
EXISTING TITIONE SECTION									CONTRACT	NO. 70	D76
SHEET	2	OF	5	SHEETS	STA	TO STA.		TILLINOIS EED	AID PROJECT		



EXISTING TYPICAL SECTION (C) I-74, FOUR LANE

WB LANES

LT STA 1312+50 TO LT STA 1315+50 LT STA 1327+65 TO LT STA 1330+65 **EB LANES**

RT STA 1312+50 TO RT STA 1315+50 RT STA 1327+65 TO RT STA 1330+65

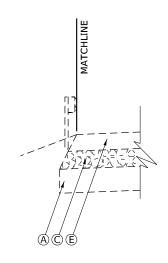
EXISTING LEGEND

- (A) LIME MODIFIED SOILS, 12"
- B PIPE UNDERDRAIN, 4" (TYP)
 C SUB-BASE GRANULAR MATERIAL, TYPE C

SCALE: N.T.S.

- D HMA PAVEMENT, 17.25"
- E HMA SHOULDERS, 8.25"
 F PCC BARRIER BASE, 8"
- © CONCRETE MEDIAN BARRIER

 EXISTING PAVEMENT MARKING LINE



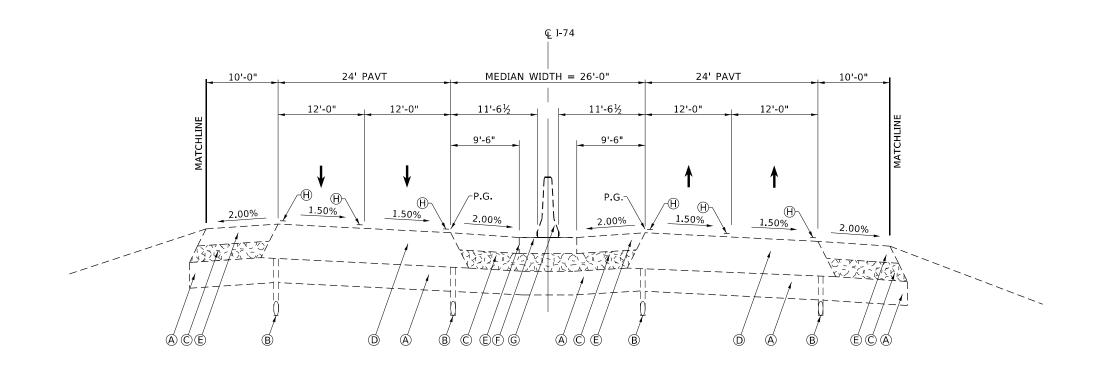
EX GUARDRAIL SECTION

		USER NAME	= sc
DEVA	BACON FARMER WORKMAN		
DL AA	ENGINEERING & TESTING, INC.	PLOT SCALE	= 0.
		PLOT DATE	= 8/

	USER NAME = sclyne	DESIGNED -	REVISED -
7		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
ı	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

	I–74							F.A.I. RTE.			TOTAL SHEETS	SHEET NO.
EXISTING TYPICAL SECTION						I SECTIO	in [74	(14HB)BR,(14B)BR-1	CHAMPAIGN	90	10
	LAISTING TIFICAL SECTION						14			CONTRACT	NO. 70)D76
	SHEET	3	OF	5	SHEETS	STA	TO STA.		TILLINOIS FED A	ID DROIFCT		



EXISTING TYPICAL SECTION (D) I-74, FOUR LANE

WB LANES

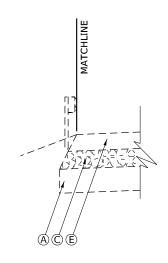
LT STA 1315+50 TO LT STA 1318+62 LT STA 1323+68 TO LT STA 1330+65 **EB LANES**

RT STA 1315+50 TO RT STA 1319+53 RT STA 1323+62 TO RT STA 1330+65

EXISTING LEGEND

- (A) LIME MODIFIED SOILS, 12"
- B PIPE UNDERDRAIN, 4" (TYP)
 C SUB-BASE GRANULAR MATERIAL, TYPE C
- $\widecheck{\mathbb{D}}$ HMA PAVEMENT, 17.25"
- E HMA SHOULDERS, 8.25"
 F PCC BARRIER BASE, 8"
- © CONCRETE MEDIAN BARRIER

 EXISTING PAVEMENT MARKING LINE



EX GUARDRAIL SECTION

CHAMPAIGN 90 11

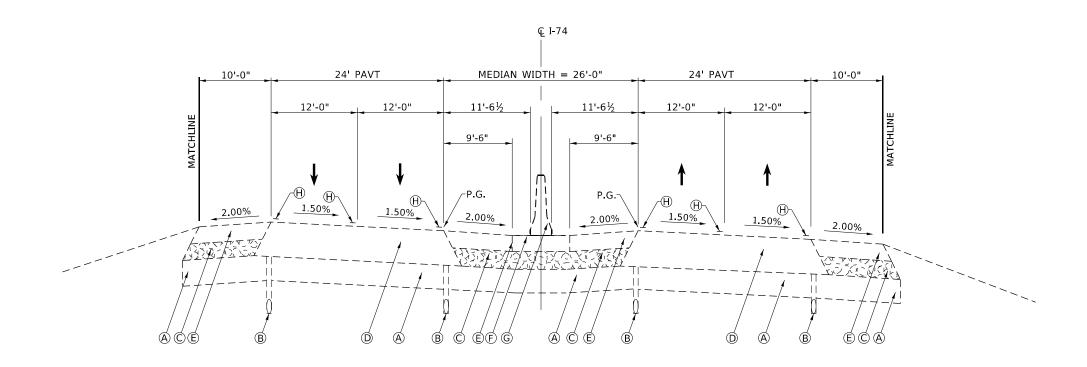
CONTRACT NO. 70D76

	USER NAME
BACON FARMER WORKMAN	
ENGINEERING & TESTING, INC.	PLOT SCALE
	PLOT DATE

	USER NAME = sclyne	DESIGNED -	REVISED -
١N		DRAWN -	REVISED -
×	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

					I-74			F.A.I. RTE	SECT	'ION	
		E	леті	NG	TVDICA	L SECTIO	nni	74	(14HB)BR,	(14B)BR	-1
		L	(1011	IVU	111107	L SLUIJO	/14				
SCALE: N.T.S.	SHEET	4	OF	5	SHEETS	STA.	TO STA.			ILLINOIS	FED. A



EXISTING TYPICAL SECTION (E) I-74, FOUR LANE

WB LANES

LT STA 1319+62 TO LT STA 1320+22 LT STA 1320+22 TO LT STA 1323+08 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK LT STA 1323+08 TO LT STA 1323+68

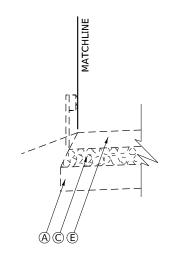
EB LANES

RT STA 1318+53 TO RT STA 1320+13 RT STA 1320+13 TO RT STA 1323+02 - EX BRIDGE APPROACH PAVEMENTS AND BRIDGE DECK RT STA 1323+02 TO RT STA 1323+62

EXISTING LEGEND

- (A) LIME MODIFIED SOILS, 12"
- B PIPE UNDERDRAIN, 4" (TYP)
- SUB-BASE GRANULAR MATERIAL, TYPE C
- $\widecheck{\mathbb{D}}$ HMA PAVEMENT, 17.25"
- E HMA SHOULDERS, 8.25"
- (F) PCC BARRIER BASE, 8"
- © CONCRETE MEDIAN BARRIER

 EXISTING PAVEMENT MARKING LINE



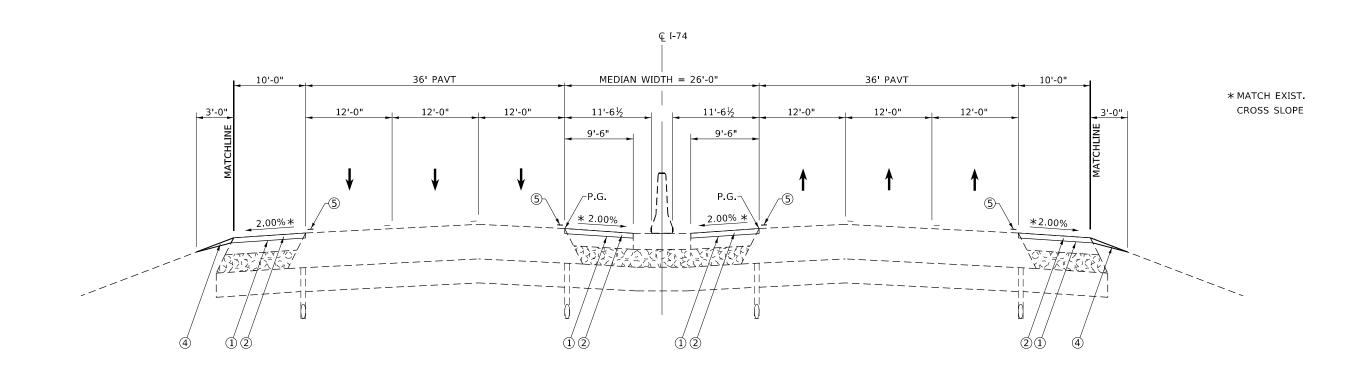
EX GUARDRAIL SECTION

	USER NAME = sclvne	DESIGNED -	REVISED -
	OSER NAME = Sciyile	DESIGNED -	KEAISED -
BACON FARMER WORKMAN		DRAWN -	REVISED -
ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SCALE: N.T.S.

	I–74					F.A.I. RTE			TOTAL SHEETS	SHEET NO.			
EXISTING TYPICAL SECTION						74	(14HB)BR,(14B)BR	-1	CHAMPAIGN	90	12	
EVISITING LILLICHT SECTION										CONTRACT	NO. 70	D76	
HEET	5	OF	5	SHEETS	STA	TO STA			IL LINIOIC	LEED AT	D DDOLECT		



PROPOSED TYPICAL SECTION (1) I-74, SIX LANE

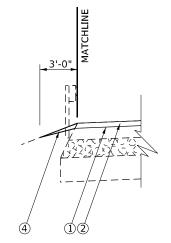
WB LANES

LT STA 1271+60 TO LT STA 1278+55 LT STA 1281+20 TO LT STA 1288+25 **EB LANES**

RT STA 1271+60 TO RT STA 1278+61 RT STA 1281+27 TO RT STA 1288+25

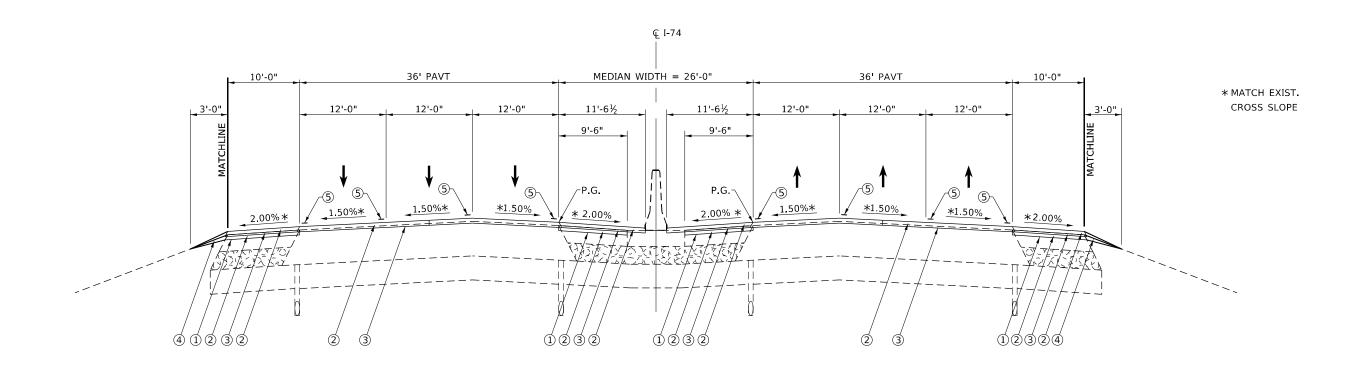
PROPOSED LEGEND

- 1 HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 2 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX"E", N90
 3 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT
 4 AGGREGATE WEDGE SHOULDERS, TYPE B
 5 POLYUREA PAVEMENT MARKING LINE, 6"



EX GUARDRAIL SECTION

pw:		USER NAME = sclyne	DESIGNED -	REVISED -			I–74	F.A.I.	SECTION	COUNTY TO	JTAL SHE	ET
AME.	BACON FARMER WORKMAN		DRAWN - REVISED - STATE OF ILLINOIS				(14HB)BR,(14B)BR-1	CHAMPAIGN :	90 1	3		
E N	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		PROPOSED TYPICAL SECTION			CONTRACT NO	O 70D7	٦
: Ē		PLOT DATE = 8/15/2024	DATE -	REVISED -		SCALE: N.T.S.	SHEET 1 OF 5 SHEETS STA. TO STA.		ILLINOIS FED. AII	PROJECT		\neg



PROPOSED TYPICAL SECTION (2) I-74, SIX LANE

WB LANES

LT STA 1278+55 TO LT STA 1279+15 - HMA BUTT JOINT

LT STA 1279+15 TO LT STA 1280+60 - 3/4" SCARIFICATION; 21/2" LATEX CONCRETE OVERLAY;

1/4" DIAMOND GRINDING (SEE BRIDGE REPAIR DETAILS)

LT STA 1280+60 TO LT STA 1281+20 - HMA BUTT JOINT

EB LANES

RT STA 1278+61 TO RT STA 1279+21 - HMA BUTT JOINT

RT STA 1278+21 TO RT STA 1280+67 - 3/4" SCARIFICATION; 21/2" LATEX CONCRETE OVERLAY;

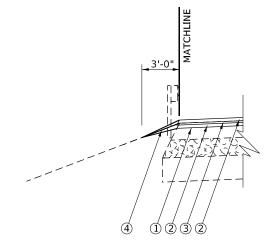
1/4" DIAMOND GRINDING (SEE BRIDGE REPAIR DETAILS)

RT STA 1280+67 TO RT STA 1281+27 - HMA BUTT JOINT

SCALE: N.T.S.

PROPOSED LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX"E", N90
- 3 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT 4 AGGREGATE WEDGE SHOULDERS, TYPE B
- 5 POLYUREA PAVEMENT MARKING LINE, 6"



EX GUARDRAIL SECTION

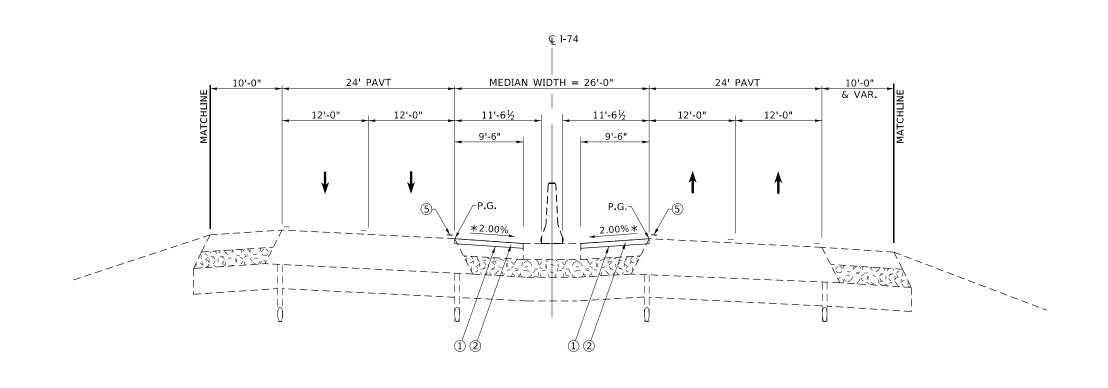
		USER NA
	BACON FARMER WORKMAN	
	ENGINEERING & TESTING, INC.	PLOT SCA
١		PLOT DAT

	USER NAME = sclyne	DESIGNED -	REVISED -
N		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -
			•

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

					I–74			F.A.I. RTE.	SECTION
PROPOSED TYPICAL SECTION									(14HB)BR,(14B)BR-
			01 0	LD	111107	AL SEUI	UIV		
	SHEET	2	OF	5	SHEETS	STA	TO STA		TUTNOTC

CHAMPAIGN 90 14 CONTRACT NO. 70D76



PROPOSED TYPICAL SECTION (3) I-74, FOUR LANE

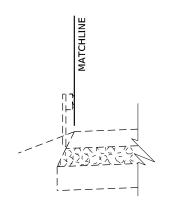
WB LANES

LT STA 1312+50 TO LT STA 1315+50 LT STA 1327+65 TO LT STA 1330+65 **EB LANES**

RT STA 1312+50 TO RT STA 1315+50 RT STA 1321+65 TO RT STA 1330+65

PROPOSED LEGEND

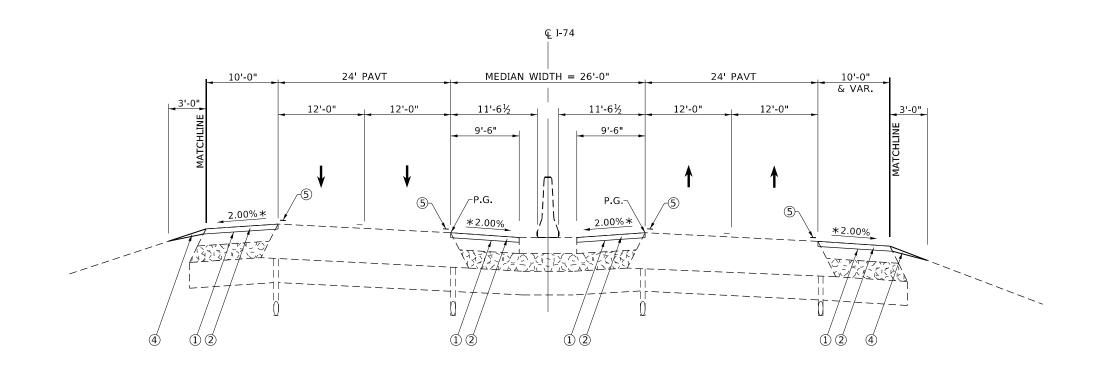
- 1 HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 2 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX"E", N90
 3 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT
 4 AGGREGATE WEDGE SHOULDERS, TYPE B
 5 POLYUREA PAVEMENT MARKING LINE, 6"



* MATCH EXIST. CROSS SLOPE

EX GUARDRAIL SECTION

fault pw:		USER NAME = sclyne	DESIGNED -	REVISED -		I–74	F.A.I. SECTION	COUNTY TOTAL SHEET
B NAME OF	BACON FARMER WORKMAN		DRAWN -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		74 (14HB)BR.(14B)BR-1	CHAMPAIGN 90 15
	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -		PROPOSED TYPICAL SECTION	, , , ,	CONTRACT NO. 70D76
ΣĒ		PLOT DATE = 8/15/2024	DATE -	REVISED -		SCALE: N.T.S. SHEET 3 OF 5 SHEETS STA. TO STA.	ILLINOIS FED. A	ID PROJECT



PROPOSED TYPICAL SECTION (4) I-74, FOUR LANE

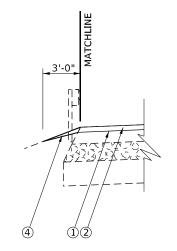
WB LANES

LT STA 1315+50 TO LT STA 1319+62 LT STA 1323+68 TO LT STA 1330+65 **EB LANES**

RT STA 1315+50 TO RT STA 1319+53 RT STA 1323+62 TO RT STA 1330+65

PROPOSED LEGEND

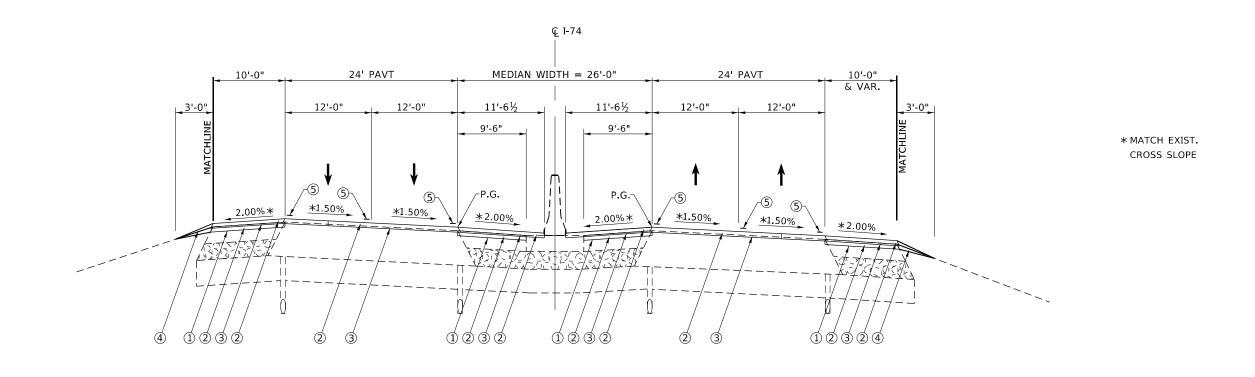
- 1 HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 2 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX"E", N90
 3 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT
 4 AGGREGATE WEDGE SHOULDERS, TYPE B
 5 POLYUREA PAVEMENT MARKING LINE, 6"



* MATCH EXIST. CROSS SLOPE

EX GUARDRAIL SECTION

		USER NAME = sclyne	DESIGNED -	REVISED -			I_7 <i>A</i>	F.A.I.	SECTION	COUNTY	SHI
DEVA	BACON FARMER WORKMAN		DRAWN -	REVISED -	STATE OF ILLINOIS			74	(14HB)BR (14B)BR-1	CHAMPAIGN	3110
DL AA	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 ' / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION SCA		PROPOSED TYPICAL SECTION		(CONTRACT	· NO
		PLOT DATE = 8/15/2024	DATE -	REVISED -		SCALE: N.T.S.	SHEET 4 OF 5 SHEETS STA. TO STA.	ILLINOIS FE		ID PROJECT	
	BFW	BACON FARMER WORKMAN ENGINEERING & TESTING, INC.	USER NAME = sclyne USER NAME = sclyne USER NAME = sclyne PLOT SCALE = 0.1667 ' / in. PLOT DATE = 8/15/2024	USER NAME Solyne	FINGINEERING & TESTING INC. THOSE COLUMN TO SECULE TO SECURE TO SECULE TO SECULE TO SECULE TO SECULE TO SECURE TO SE	BACON FARMER WORKMAN DRAWN - REVISED - STATE OF ILLINOIS	BACON FARMER WORKMAN DRAWN - REVISED - STATE OF ILLINOIS	BACON FARMER WORKMAN DRAWN - REVISED - STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DEPARTMENT OF TRANSPORTATION	BACON FARMER WORKMAN ENGINEERING & TESTING, INC. DRAWN - REVISED - STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PLOT SCALE = 0.1667 '/ in. CHECKED - REVISED - DEPARTMENT OF TRANSPORTATION COALS N.T.S. SHEET 4. OF S. SHEET STA. TO STA	BACON FARMER WORKMAN DRAWN - REVISED - STATE OF ILLINOIS PROPOSED TYPICAL SECTION REVISED - REVISED - TO THAN SPORTATION REVISED - REVI	BACON FARMER WORKMAN DRAWN - REVISED - STATE OF ILLINOIS PROPOSED TYPICAL SECTION PLOT SCALE = 0.1667 '/ in. CHECKED - REVISED - REVISED - TO STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION CONTRACT CONTRACT



PROPOSED TYPICAL SECTION (5) I-74, FOUR LANE

WB LANES

LT STA 1319+62 TO LT STA 1320+22 - HMA BUTT JOINT

LT STA 1320+22 TO LT STA 1323+08 - 3/4" SCARIFICATION; 21/2" LATEX CONCRETE OVERLAY;

1/4" DIAMOND GRINDING (SEE BRIDGE REPAIR DETAILS)

LT STA 1323+08 TO LT STA 1323+68 - HMA BUTT JOINT

EB LANES

RT STA 1319+53 TO RT STA 1320+13 - HMA BUTT JOINT

RT STA 1320+13 TO RT STA 1323+02 - 3/4" SCARIFICATION; 21/2" LATEX CONCRETE OVERLAY;

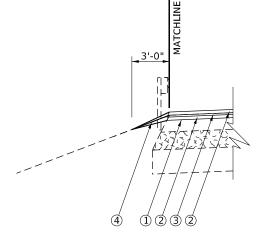
1/4" DIAMOND GRINDING (SEE BRIDGE REPAIR DETAILS)

RT STA 1323+02 TO RT STA 1323+62 - HMA BUTT JOINT

SCALE: N.T.S.

PROPOSED LEGEND

- ① HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (2) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX"E", N90
- 3 HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT
 4 AGGREGATE WEDGE SHOULDERS, TYPE B
- 5 POLYUREA PAVEMENT MARKING LINE, 6"



EX GUARDRAIL SECTION

2			USER N
	DEIM	BACON FARMER WORKMAN	
7	DL AA	ENGINEERING & TESTING, INC.	PLOT S
			PLOT D

	USER NAME = sclyne	DESIGNED -	REVISED -
Ν		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

					I-74			F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PROPOSED TYPICAL SECTION						I SECTION		74	(14HB)BR,(14B)BR-1	CHAMPAIGN	90	17
	FROTOSED TIFICAL SECTION									CONTRACT	NO. 70	D76
	SHEET	5	OF	5	SHEETS	STA	TO STA.		TILLINOIS FED A	ID PROJECT		

				<u> </u>	1	SHOULDER PA	AVING SCHEDU	ILE FOR STAGE	CONSTRUCTION	ON			
LOCATION				LENGTH	PAVEMENT WIDTH	SHOULDER WIDTH	COMMENTS	HMA SURFACE REMOVAL, 2"	BIT MATLS (TACK COAT)	POLYMER HMA SURFACE COURSE, IL- 9.5, MIX "E", N90 (2.0" THICK)	AGGREGATE WEDGE SHOULDER, TYPE B (VAR. DEPTH)	FRAMES AND LIDS TO BE ADJUSTED	SHOULDER RUMBLE STRIPS, 16 INCH (CONSTRUCTEL AFTER STAGE CONSTRUCTION IS COMPLETED)
PAY C	ODE NO.							44000157	40600290	40604174	48102100	60300305	64200116
	STATION		STATION	(FOOT)	(FOOT)	(FOOT)		(SQ YD)	(POUND)	(TON)	(TON)	(EACH)	(FOOT)
	IPAIGN COUN												
	BL - OUTSIDE												
RT	1270+80.00		1279+34.00	854.00	36.0	10.0		948.9	427.0	106.3			854.0
RT RT	1280+60.00 1314+70.00	_	1289+05.00 1320+36.00	845.00 566.00	36.0	10.0 10.0		938.9	422.5	105.2	21.1		845.0 566.0
RT	1314+70.00		1328+45.00	579.00	24.0 24.0	10.0		628.9 643.3	283.0 289.5	70.4 72.1	14.2 14.5		500.0
KI	1322+00.00		1320+43.00	579.00	24.0	10.0		043.3	209.5	72.1	14.5		579.0
I-72 EI	BL - MEDIAN S	HOL	LDER										
RT	1270+80.00		1279+28.00	848.00	36.0	9.5		895.1	402.8	100.3		2.0	848.0
RT	1280+53.00		1289+05.00	852.00	36.0	9.5		899.3	404.7	100.7		3.0	852.0
RT	1311+70.00		1320+54.00	884.00	24.0	9.5		933.1	419.9	104.5			884.0
RT	1322+69.00		1331+45.00	876.00	24.0	9.5		924.7	416.1	103.6		3.0	876.0
1.72 \	I BL - MEDIAN :	SHO	II DED										
LT	1270+80.00		1279+28.00	848.00	36.0	9.5		895.1	402.8	100.3		2.0	848.0
LT	1280+53.00		1289+05.00	852.00	36.0	9.5		899.3	404.7	100.3		2.0	852.0
LT	1311+70.00		1320+54.00	884.00	24.0	9.5		933.1	419.9	104.5		4.0	884.0
LT	1322+69.00		1331+45.00	876.00	24.0	9.5		924.7	416.1	103.6			876.0
I-72 W	BL - OUTSIDE	SHC	DULDER										
<u>LT</u>	1264+60.00		1266+90.00	230.00	24.0	4.0	· ·	102.2	46.0	11.4			
LT	1270+80.00		1279+22.00	842.00	36.0	10.0		935.6		104.8			842.0
LT	1280+46.00		1289+05.00	859.00	36.0	10.0		954.4	429.5	106.9	21.5		859.0
LT	1314+70.00		1320+60.00	590.00	24.0	15.0		983.3	442.5	110.1	14.8		590.0
LT	1322+76.00		1328+45.00	569.00	24.0	10.0		632.2	284.5	70.8	14.2		569.0
SUBT	OTAL		<u> </u>					14072.2	6332.5	1576.1	142.6	14.0	12624.0
								2.2	5532.0		. 12.0	11.0	.2021.0
TOTA	Ľ		•					14072	6333	1576	143	14	12624

VOI.		
w M		Į
AME:	BACON FARMER WORKMAN	
ž H	ENGINEERING & TESTING, INC.	F
Ĭ		F

USER NAME ■ sskinner	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE • 0.1667 / in	CHECKED -	REVISED -
PLOT DATE ■ 8/15/2024	DATE -	REVISED -

I–74	F.A.I. RTE.	SECTION	ON		COUNTY	TOTAL SHEETS	SHEET NO.
SCHEDULES OF QUANTITIES	74	(14HB)BR,(14B)BR•1			CHAMPAIGN	90	18
		•			CONTRACT	NO. 70	D76
SCALE: N.T.S. SHEET 1 OF 6 SHEETS STA. TO STA.		II II	LLINOIS	FED. AI	D PROJECT		

	USER N
N FARMER WORKMAN	
NEERING & TESTING, INC.	PLOT 5
	PLOT D

						F	PAVING SCHE	DULE - PAVEM	IENT AREAS				
LOCA	ΓΙΟΝ			LENGTH	PAVEMENT	OUTSIDE	MEDIAN		HMA SURF	BIT MATLS	POLYMER	AGGREGATE	COMMENTS
					WIDTH	SHOULDER	SHOULDER		REM - BUTT	(TACK	HMA	WEDGE	
						WIDTH	WIDTH		JOINT	COAT) -	SURFACE	SHOULDER,	
							and a good a good a good a good a good a good and a good a go		(INCLUDES	BETWEEN	COURSE, IL-	TYPE B	
									PCC AT	ALL LIFTS	9.5, MIX "E",	(VAR.	
									MEDIAN	(0.05 LB/SF	N90 (1.5"		
									BARRIER)	ON MILLED	THICK)	:,	
									Drawa Lity	SURF.; 0.025	I I II OI ()		
										LB/SF			
										BETWEEN			
										LIFTS)			
DAVC	ODE NO.								40600982	40600290	40604174	48102100	
	STATION	ТО	STATION	(FOOT)	(FOOT)	(FOOT)	(FOOT)		(SQ YD)	(POUND)	(TON)	(TON)	
	PAIGN COUN		CIATION	(1001)	(1 001)	(1 551)	(1.501)		(00 10)	(1 00140)	(1011)	(1011)	
1-72 EE		. ·											
RT	1278+61.0		1279+21.0	60.0	36.0	10.0	11.5		383.3	172.5	32.2	1.5	PAVEMENT & SHOULDER
RT	1279+21.0		1279+28.0	7.0			11.5		1	4.0			MEDIAN SHOULDER
RT	1279+21.0		1279+33.0	12.0	36.0	10.0				6.0	1.1	0.3	OUTSIDE SHOULDER
RT	1280+55.0		1280+67.0	12.0	36.0		11.5			6.9	1.3		MEDIAN SHOULDER
RT	1280+60.0		1280+67.0	7.0	36.0					3.5	0.7		OUTSIDE SHOULDER
RT	1280+67.0		1281+27.0	60.0	36.0	10.0	11.5		383.3	172.5	32.2	1.5	PAVEMENT & SHOULDER
RT	1319+53.0		1320+13.0	60.0	24.0		11.5		303.3		25.5		PAVEMENT & SHOULDER
RT	1320+13.0		1320+35.0	22.0	24.0					11.0		0.6	OUTSIDE SHOULDER
RT	1320+13.0		1320+50.0	37.0	24.0		11.5			21.3	4.0		MEDIAN SHOULDER
RT	1322+67.0		1323+02.0	35.0	24.0	10.0				17.5	3.3	0.9	OUTSIDE SHOULDER
RT	1322+70.0		1323+02.0	32.0	24.0		11.5		-	18.4	3.4		MEDIAN SHOULDER
RT	1323+02.0		1323+62.0	60.0	24.0		0.0.0150		303.3		25.5		PAVEMENT & SHOULDER
IXI	1323102.0		1020102.0	00.0	24.0	10.0	11.5		303.3	130.5	20.0	1.5	FAVEWENT & SHOOLDER
I-72 W	L												
LT	1278+55.0		1279+15.0	60.0	36.0	10.0	11.5		383.3	172.5	32.2	1.5	PAVEMENT & SHOULDER
LT	1279+15.0		1279+22.0	7.0	36.0	10.0				3.5	0.7	0.2	OUTSIDE SHOULDER
LT	1279+15.0		1279+27.0	12.0	36.0		11.5			6.9	1.3		MEDIAN SHOULDER
LT	1280+48.0		1280+60.0	12.0	36.0					6.0			OUTSIDE SHOULDER
LT	1280+53.0		1280+60.0	7.0			11.5			4.0	3.00.000		MEDIAN SHOULDER
LT	1280+60.0		1281+20.0	60.0	36.0	10.0	11.5		383.3	172.5	32.2	1.5	PAVEMENT & SHOULDER
1.7	1319+62.0		1320+22.0	60.0	24.0	10.0	44 5		303.3	136.5	25.5	1.5	PAVEMENT & SHOULDER
LT LT	1319+62.0		1320+22.0	32.0	24.0	5000 Dati	11.5 11.5		303.3	18.4	3.4		MEDIAN SHOULDER
LT	1320+22.0	 	1320+54.0	37.0	24.0		11.5		 	18.5	3.4		OUTSIDE SHOULDER
-	1020122.0		1020103.0	37.0	27.0	10.0				10.0	5.5	0.9	3016IDE SHOOLDER
LT	1322+71.0		1323+08.0	37.0	24.0		11.5			21.3	4.0		MEDIAN SHOULDER
LT	1322+76.0		1323+08.0	32.0	24.0					16.0			OUTSIDE SHOULDER
LT	1323+08.0		1323+68.0	60.0	24.0		11.5		303.3		25.5	1.5	PAVEMENT & SHOULDER
SUB-T	OTAL =								2746.7	1419.2	264.9	16.1	
TOTAL	=								2747	1419	265	16	

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AIVIE:	BACON FARMER WORKMAN	
Ž L	ENGINEERING & TESTING, INC.	-
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	USER NAME = sskinner	DESIGNED -	REVISED -
7		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

_						I–74			F.A.I. RTE	SEC	TION		COUNTY	TOTAL SHEETS	
		CHEC	1111	es ue c	74	(14HB)BR	(14B)BR	-1	CHAMPAIGN	90	19				
	SCHEDULES OF QUANTITIES												CONTRACT	NO. 70	D76
SCALE: N.T.S. SHEET 2 OF 6 SHEETS STA. TO STA.							TO STA.			ILLINOIS	FED. A	ID PROJECT			

											PAVEN	IENT MARKIN	NG QUANTITI	ES									
LOCATION			LENGTH	PAVEMENT MARKING	PAVEMENT MARKING		SHORT TERM	SHORT TERM		POLYUREA PAVEMENT		POLYUREA PAVEMENT		GROOVING FOR	GROOVING FOR	GROOVING FOR	REPLACE MENT	TEMPORARY PAVEMENT	TEMPORARY PAVEMENT	TEMPORARY PAVEMENT	TEMPORARY PAVEMENT	TEMPORARY PAVEMENT	TEMPORARY PAVEMENT
					BLACKOUT			PAVEMENT	MARKING	MARKING	MARKING	MARKING	MARKING	RECESSED	RECESSED	RECESSED	REFLECTOR	MARKING -	MARKING -	MARKING -	MARKING -	MARKING -	MARKING -
				TAPE, 5"	TAPE, 7"	TAPE, 16"	MARKING	MARKING	TYPE II-	TYPE II -	TYPE II -	TYPE II -	TYPE II-	PAVEMENT	PAVEMENT	PAVEMENT	(TO BE	LINE 4" -		LINE 6" -TYPE		LINE 4" -	LINE 6" -
								REMOVAL	LINE 6" (SKIP	LINE 6" (SOLID,	LINE 6" (SOLID,	LINE 8" (WHITE)	LINE 12" (WHITE)	MARKING, 7"	MARKING, 9"	MARKING, 13	REPLACED AT THE END	TYPE IV TAPE (WHITE)	IV TAPE (YELLOW)	IV TAPE (WHITE)	PAINT (WHITE)	PAINT (YELLOW)	PAINT (WHITE)
									DASH,	WHITE)	YELLOW)	(VVIII1L)	(**************************************				OF THE	(**************************************	(TEELOW)	(WIIII L)	WINTER	WINTER	WINTER
									WHITE)								PROJECT)				SHUTDOWN	SHUTDOWN	SHUTDOWN
																							1
																							1
DAY CODE NUMBER				7040700E	70107007	70407046	70200400	70200450	70000000	70000000	70000000	70000240	70000250	79011025	79011045	79011065	79400200	70207120	70207120	70207120	70200221	70200224	70200244
PAY CODE NUMBER LT/RT STATION T	тоТ	STATION I	(FOOT)	70107005 (FOOT)	70107007 (FOOT)	70107016 (FOOT)	70300100 (FOOT)	70300150 (SQ FT)	78008330 (SQ FT)	78008330 (FOOT)	78008330 (FOOT)	78008340 (FOOT)	78008350 (FOOT)	78011035 (FOOT)	78011045 (FOOT)	78011065 (FOOT)	78100300 (EACH)	70307120 (FOOT)	70307120 (FOOT)	70307130 (FOOT)	70300221 (FOOT)	70300221 (FOOT)	70300241 (FOOT)
		.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	(1.001)	1. 55.7	(, 55.7	(1.00.7)	(1.00.7	(04) 17	(52.1.7	(1.001)	(1.55.7	(1.001)	(1.001)	(/	(/	(1.00.7	(=, (0.1.)	(/	(/	(: 00.7	(/	(1.001)	(. 55.7
FINAL MARKING																							 '
I-74 EBL 1264+60.00	1	266+60.00	200.0				0	0	0	0	0			0	0)		-		0	0	
RT 1266+60.00		268+60.00	200.0				0	0	0	0	0			0	0	`)				0	0	0
RT 1268+60.00 RT 1270+80.00		270+80.00 278+61.00	220.0 781.0				31	·	0	781	781			0 1562	0	')				0	0	0
RT 1278+61.00	_	279+21.00	60.0				14							1502	0	')				0	0	0
RT 1279+21.00		280+67.00	146.0				35							0	0	<u> </u>					0	0	0
RT 1280+67.00 RT 1281+27.00		281+27.00 289+05.00	60.0 778.0				14 31		30	60 778	60 778			150 1556	0) ()				0	0	0
RT 1289+05.00		307+70.00	1865.0				0		0	0	0			0	0)		_		0	Ö	0
RT 1307+70.00	_	311+70.00	400.0				0	0	0		0			0	0						0	0	0
RT 1311+70.00 RT 1315+50.00		315+50.00 318+70.00	380.0 320.0				13	3	0	380 320	380 320			760 640	0	<u> </u>) [-	_		0	0	0
RT 1318+70.00		319+53.00	83.0				3	1	0	83	83			166	0						0	Ö	0
RT 1319+53.00		320+13.00	60.0				8	3	15		60			135	0				_		0	0	0
RT 1320+13.00 RT 1323+02.00		323+02.00 323+62.00	289.0 60.0				40 8		72 15		289 60			135	0	<u> </u>			_		0	0	0
RT 1323+62.00	1	327+65.00	403.0				16		0	403	403			806	0						0	0	0
RT 1327+65.00	1	331+45.00	380.0				8	3	0	380	380			760	0	0)				0	0	0
I-74 WBL	-																		_				 '
LT 1264+60.00		266+70.00	210.0				0	0	0	0	0	210	50	0	210	50					0	0	0
LT 1266+70.00 LT 1270+80.00		270+80.00 278+55.00	410.0 775.0				0		0	410	410 775			820 1550	0						0	0	0
LT 1278+55.00		279+15.00	60.0				31 14		30	775 60	60			150	0)				0	0	0
LT 1279+15.00	12	280+60.00	145.0				35	12		145	145			0	0) ()				0	Ō	0
LT 1280+60.00 LT 1281+20.00		281+20.00 289+05.00	60.0 785.0				14 31		30	785	60 785			150 1570	0	4					0	0	0
LT 1289+05.00		306+05.00	1700.0				0		0	0	703			1370	0)				0	0	0
LT 1306+05.00		307+30.00	125.0				0	-	0	0	0			0	0	`					0	0	0
LT 1307+30.00 LT 1311+70.00		311+70.00 315+50.00	440.0 380.0				8	3	0	380	380			760	0)				0	0	0
LT 1315+50.00		318+70.00	320.0				13	4	0	320	320			640	0				_		0	Ö	0
LT 1318+70.00		319+62.00	92.0				4	1	0	92	92			184	0						0	0	0
LT 1319+62.00 LT 1320+22.00		320+22.00 323+08.00	60.0 286.0				40	13	72	60 286	60 286			135	0)				0	0	0
LT 1323+08.00	13	323+68.00	60.0				8		15					135	0					4	0	Ö	0
LT 1323+68.00 LT 1327+65.00		327+65.00 331+45.00	397.0 380.0				16 8	5	0	397 380	397 380			794 760	0						0	0	0
1327+03.00	_ '	331143.00	360.0				•			360	360			760									
I-74 EB ENTRANCE RAN				ERCHANGE))																		
LT & RT 263+60.00 I-74 WB EXIT RAMP 183				NGF)																			
LT & RT 264+60.00		268+10.00	350.0									80			80								
I-74 EB EXIT RAMP 184				-																			
RT 1307+70.00 LT & RT 307+70.00		316+10.00																1					
I-74 WB ENTRANCE RA	MP 18	84 (US 45 IN	NTERCHAN																				
LT & RT 306+05.00																							
I-74 EB ENTRANCE RAN LT & RT 330+10.00		335+70.00																					
LT & RT 335+70.00		343+25.00	755.0																				
RT 1330+00.00 I-74 WB EXIT RAMP 184		335+70.00	570.0															-	_				
LT & RT 330+90.00		334+90.00	400.0																				
LT 334+90.00		33772.0	282.0																				
SUB-TOTAL =				0	0	0	461	154	469	8010	8010	290	50	14468	290	50		1 0		n	<u> </u>	n	
							401	134	709	3010	0010	230	30	17700	230	. 30			<u> </u>				
TOTAL =				19397	1190	50	461	26605	469	10 to 80 to	8010	290	50	14468	290	50	579	5 3000000000000000000000000000000000000		840			210
										16489			l		l		<u> </u>	51	915	l	J 33	865	

BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC.

	USER NAME = sskinner	DESIGNED -	REVISED -
Z		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATI	E 01	F ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

			I-74			F.A.I. RTE	SEC ⁻	ПОИ		COUNTY	TOTAL SHEETS	SHEET NO.
•	CHEC	NIII F	S OF O	HANTI	TIFC	74	(14HB)BR,	(14B)BR	-1	CHAMPAIGN	90	23
	OIILL	OLL	.5 01 0	UANI	IILO					CONTRACT	NO. 70	D76
6	OF	6	SHEETS	STA	TO STA			TURNOTC	EED A	D. DROJECT		

PAY CODE STATION T 1-74 STAGE 1 EASTBOUND 1271+80.0 1275+50.0 1275+81.0	1275+50.0 1275+80.0 1278+30.0 1281+55.0	TEMPORARY CONCRETE BARRIER 70400100.0 (FOOT)	RELOCATE TEMPORARY CONCRETE BARRIER 70400200.0 (FOOT)	ETE BARRIER SC IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3 70600250 (EACH)	RELOCATE IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3 70600350 (EACH)	BARRIER WALL REFLECTORS, TYPE C (PLACED ON TEMPORARY CONCRETE BARRIER) 78200011 (EACH)	BARRIER WALL REFLECTORS, TYPE C (PLACED ON PERMANENT MEDIAN BARRIER, PARAPET, GUARDRAIL)
STATION T -74 STAGE 1 EASTBOUND 1271+80.0 1275+50.0	1275+50.0 1275+80.0 1278+30.0	MESSEC MESSECONS SERVICESCONS	PRODUCT PROGRAMMENT SERVICES	70600250	70600350	84RRIER) 78200011	BARRIER, PARAPET, GUARDRAIL) 78200011
STATION T -74 STAGE 1 EASTBOUND 1271+80.0 1275+50.0	1275+50.0 1275+80.0 1278+30.0	MESSEC MESSECONS SERVICESCONS	PRODUCT PROGRAMMENT SERVICES		***************************************		
I-74 STAGE 1 EASTBOUND 1271+80.0 1275+50.0	1275+50.0 1275+80.0 1278+30.0	(FOOT)	(FOOT)	(EACH)	(EACH)	(EACH)	(EACH)
STAGE 1 EASTBOUND 1271+80.0 1275+50.0	1275+80.0 1278+30.0						\
STAGE 1 EASTBOUND 1271+80.0 1275+50.0	1275+80.0 1278+30.0						
1271+80.0 1275+50.0	1275+80.0 1278+30.0			1			
1275+50.0	1275+80.0 1278+30.0						
	1278+30.0						16
		250.0		1		11	1
1278+30.0		250.0 325.0				11 13	10 13
1281+55.0	1288+05.0	320.0				10	26
1312+70.0	1317+76.0			,			21
1317+76.0	1318+06.0	110 F		1		6	1 5
1318+07.5 1319+20.0	1319+20.0 1323+95.0	112.5 475.0				19	19
1323+95.0	1330+45.0	475.0				13	26
WESTBOUND							
1271+80.0	1278+30.0	205.0				10	26
1278+30.0 1281+55.0	1281+55.0 1284+04.0	325.0 250.0				13 11	13 10
1284+05.0	1284+35.0	230.0		1			1
1284+35.0	1288+05.0						16
1312+70.0	1319+20.0	475.0				10	26
1319+20.0 1323+95.0	1323+95.0 1325+07.5	475.0 112.5				19 6	19 5
1325+09.0	1325+39.0	112.0		1		Ü	1
1325+39.0	1330+45.0						21
OTA OF A OUR		2005.0					
STAGE 1 SUB-	TOTAL =	2325.0	0	4	0	98	276
STAGE 2							
EASTBOUND							
1275+50.0 1275+81.0	1275+80.0 1278+30.0		250.0		1	44	
1278+30.0	1281+55.0		325.0			11 13	11 8
12.0.00.0	120110010		525.5				
1317+76.0	1318+06.0				1		2
1316+70.0	1319+20.0	137.5	112.5			11	10
1319+20.0	1323+95.0		475.0			19	19
WESTBOUND	-						
1267+00.0	1278+30.0						45
1278+30.0	1281+55.0		325.0			13	13
1281+55.0	1284+04.0		250.0		4	11	10
1284+05.0	1284+35.0				1		2
1319+20.0	1323+95.0		475.0			19	19
1323+95.0	1326+45.0	137.5	112.5			11	7
1326+46.0	1326+76.0				1		
STAGE 2 SUB-	TOTAL =	275.0	2325.0	0	4	108	146
TOTAL =		2600.0	2325.0	4	4	206	422

:wd		
FILE NAME:	BFW	BACON FARMER WORKMAN ENGINEERING & TESTING, INC.

	USER NAME = sskinner	DESIGNED -	REVISED -
7		DRAWN -	REVISED -
	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

SCALE: N.T.S.

				I-74			F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	c	CHED	III F	S OF C	HARITI	rife	74	(14HB)BR,(14B)BR-1	CHAMPAIGN	90	20
	- 0	UIILL	OLL	.5 01 0	LUAIVII	I JLJ			CONTRACT	NO. 70	D76
SHEET	3	OF	6	SHEETS	STA.	TO STA.		ILLINOIS FED.	AID PROJECT		

									PAVEN	MENT MARKIN	NG QUANTITI	ES									
LOCATION	LENGTH	PAVEMENT MARKING		PAVEMENT MARKING	SHORT TERM	SHORT TERM		POLYUREA		POLYUREA			GROOVING FOR	GROOVING FOR	REPLACE	TEMPORARY PAVEMENT		TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY
		BLACKOUT	MARKING BLACKOUT		PAVEMENT	PAVEMENT	PAVEMENT MARKING	PAVEMENT MARKING	PAVEMENT MARKING	MARKING	PAVEMENT MARKING	FOR RECESSED	RECESSED	RECESSED	MENT REFLECTOR	MARKING -	PAVEMENT MARKING -	PAVEMENT MARKING -	PAVEMENT MARKING -	PAVEMENT MARKING -	PAVEMENT MARKING -
		TAPE, 5"	TAPE, 7"	TAPE, 16"	MARKING	MARKING	TYPE II-	TYPE II -	TYPE II -	TYPE II -	TYPE II-	PAVEMENT	PAVEMENT	PAVEMENT	(TO BE	LINE 4" -		LINE 6" -TYPE		LINE 4" -	LINE 6" -
						REMOVAL	LINE 6"	LINE 6"	LINE 6"	LINE 8"	LINE 12"	MARKING, 7"	MARKING, 9"	MARKING, 13"	REPLACED	TYPE IV TAPE	IV TAPE	IV TAPE	PAINT	PAINT	PAINT
							(SKIP DASH,	(SOLID, WHITE)	(SOLID, YELLOW)	(WHITE)	(WHITE)				AT THE END OF THE	(WHITE)	(YELLOW)	(WHITE)	(WHITE) WINTER	(YELLOW) WINTER	(WHITE) WINTER
							WHITE)	vv⊓!!⊑ <i>)</i>	TELLOW)						PROJECT)				SHUTDOWN	SHUTDOWN	SHUTDOWN
															,						
								2													
PAY CODE NUMBER		70107005	70107007	70107016	70300100	70300150	78008330	78008330	78008330	78008340	78008350	78011035	78011045	78011065	78100300	70307120	70307120	70307130	70300221	70300221	70300241
LT/RT STATION TO STATION CHAMPAIGN COUNTY	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(SQ FT)	(SQ FT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(EACH)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)
FOR STAGE I TRAFFIC	 																-				
I-74 EBL																					
RT 1266+60.00 1271+80.00 RT 1271+80.00 1278+61.00	+				0		0	0	0	0	0	0	0	0	13				0	0	9
RT 1271+80.00 1278+61.00 RT 1278+61.00 1279+21.00		_			0		0	0	0	J	0		0		17				75		1 6
RT 1279+21.00 1280+67.00	146.0				0		0	0	0	0	0	0	0	0	4	292	146		183	0	Č
RT 1280+67.00 1281+27.00					0	91	0	0	0	0	0	0	0	0	2	120			75		C
RT 1281+27.00 1288+05.00 RT 1288+05.00 1308+20.00		1017			0		0	0	0		0		0	0	17				0		
RT 1308+20.00 1312+70.00		0		0	0		0	0	0	0	0	0	0	0	0				0	0	, ,
RT 1312+70.00 1318+70.00		950			0	996	0	0	0	0	0	0	0	0	15				0	0	(
RT 1318+70.00 1319+53.00 RT 1319+53.00 1320+13.00					0	126 91	0	0	0	ŭ	0	0	0	0	2	166 120			75	0	1
RT 1320+13.00 1323+02.00	289.0	361	0		ő	440	0	0	ő	0	Ö		0	Ö	7	578	289		361		
RT 1323+02.00 1323+62.00		75		- U	0	91	0	0	0	0	0	0	0	0	2	128			75	0	C
RT 1323+62.00 1324+45.00 RT 1324+45.00 1330+45.00		0 104 0 750		0	0	126 913	0	0	0	0	0	0	0	0	15	166 1200			0	0	
(1 1024 - 40.00 1000 - 40.00	000.	700		Ü		310			Ŭ	J	U	Ŭ	Ü		13	1200	000				
-74 WBL											_										
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I	. INTERCH	ANGE)	1												+		 				
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74 EB EXIT RAMP 184 (US 45 INTERC	HANGE)														1						
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74 WB ENTRANCE RAMP 184 (US 45																					
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: N.T.S. SHEET 4 OF 6 SHEETS STA.

TO STA.

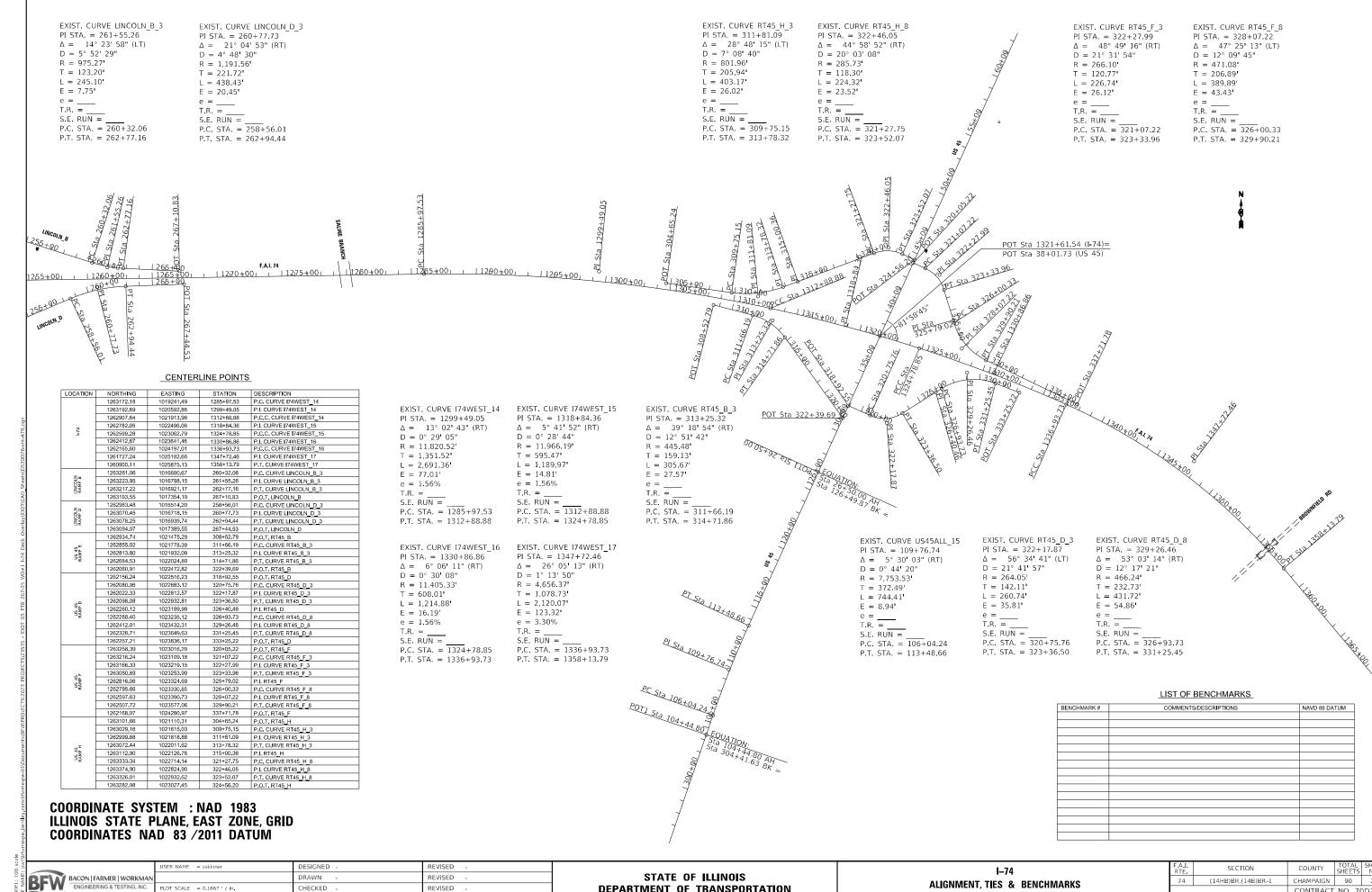
LOCATION		IDAY:=::-:	·I.B.A.V.=-:	DAY == :	01:05=	0	DOI\#!==:	Inoly#i==		MENT MARKIN			00000000	00000000	I per :	TEMPOS : =:	I TEMPOS :	TELIBOR: -	TEL 15.55	TEL 15.55	TEN/202 : -:
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		BLACKOUT		BLACKOUT			MARKING	MARKING	MARKING	MARKING	MARKING	RECESSED	RECESSED	RECESSED	REFLECTOR	MARKING -	MARKING -	MARKING -	MARKING -	MARKING -	MARKING -
		TAPE, 5"	TAPE, 7"	TAPE, 16"	MARKING	MARKING	TYPE II-	TYPE II -	TYPE II -	TYPE II -	TYPE II-	PAVEMENT	PAVEMENT	PAVEMENT	(TO BE	LINE 4" -	LINE 4" -TYPE	LINE 6" -TYPE		LINE 4" -	LINE 6" -
						REMOVAL	LINE 6"	LINE 6"	LINE 6"	LINE 8"	LINE 12"	MARKING, 7"	MARKING, 9"	MARKING, 13"	REPLACED	TYPE IV TAPE	IV TAPE	IV TAPE	PAINT	PAINT	PAINT
							(SKIP DASH,	(SOLID, WHITE)	(SOLID, YELLOW)	(WHITE)	(WHITE)				AT THE END OF THE	(WHITE)	(YELLOW)	(WHITE)	(WHITE) WINTER	(YELLOW) WINTER	(WHITE) WINTER
							WHITE)	\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\	TELEGOV)						PROJECT)				SHUTDOWN	SHUTDOWN	SHUTDOWN
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LT/RT STATION TO STATION CHAMPAIGN COUNTY	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(SQ FT)	(SQ FT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(EACH)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)
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RT 1278+61.00 1279+21.00	60.0	75			0	01	0	0	0	0	0	0	0	0	2	120			15		
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-74 WBL	210.0	52	3 210	50	_	439	0			0		0			_	220	240	170			21/
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-74 EB ENTRANCE RAMP 183 (LINCOL	N AVE. IN	TERCHANGE)																		
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RT 264+60.00 268+10.00	350.0		100		0		0	0	0	0	0	0	0	0	3	0	0	170	0	0	(
-74 EB EXIT RAMP 184 (US 45 INTERCI																					
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USER NAME = ssk	nner	DI	ESIGNED -		REVISED	-				TE OF 1111					I-7	74		F.A. RTE	I. SECTION	он с	OUNTY TOTA

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: N.T.S. SHEET 5 OF 6 SHEETS STA.

TO STA.

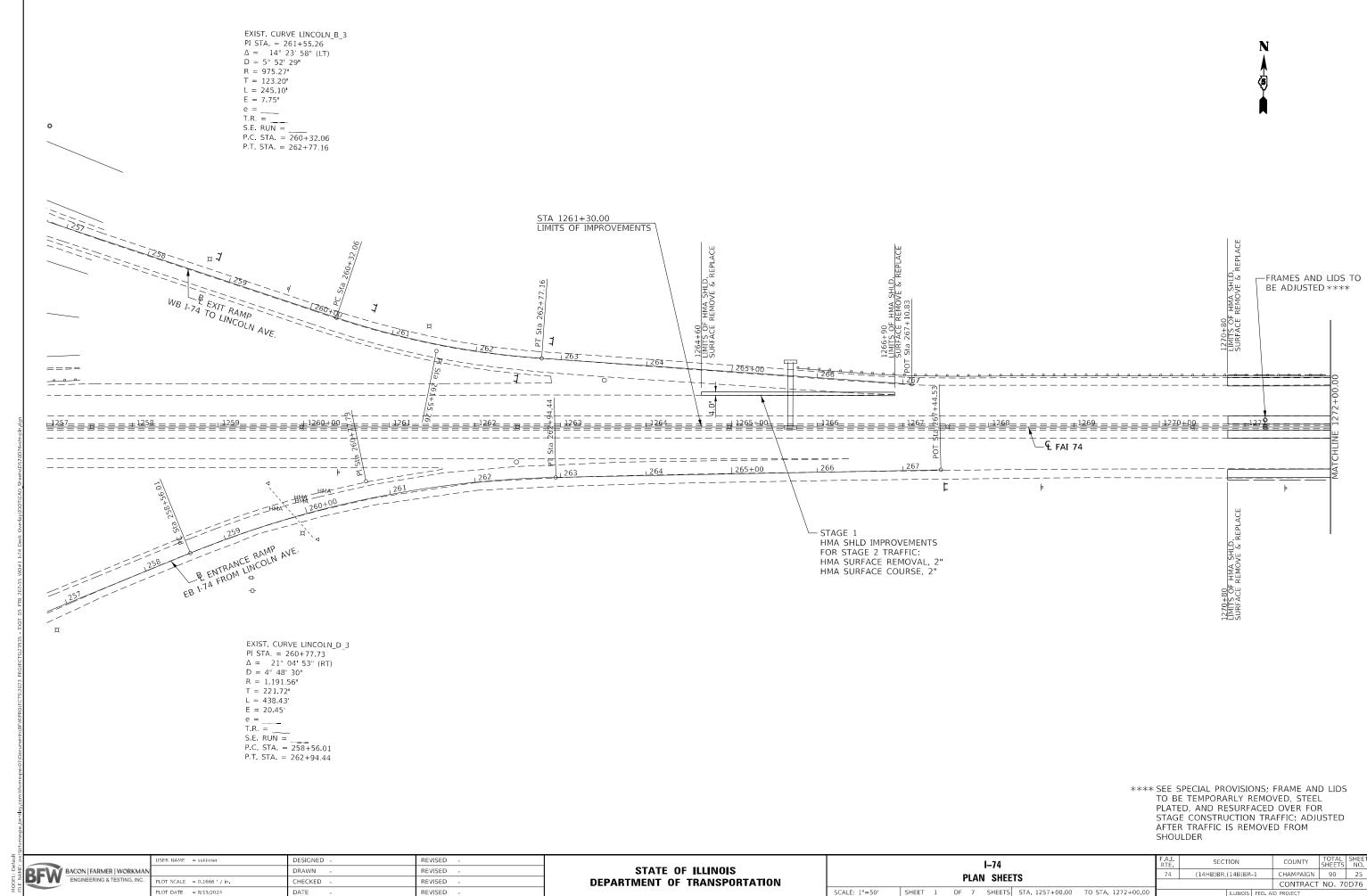


HECKED REVISED PLOT DATE = 8/15/2024 DATE REVISED

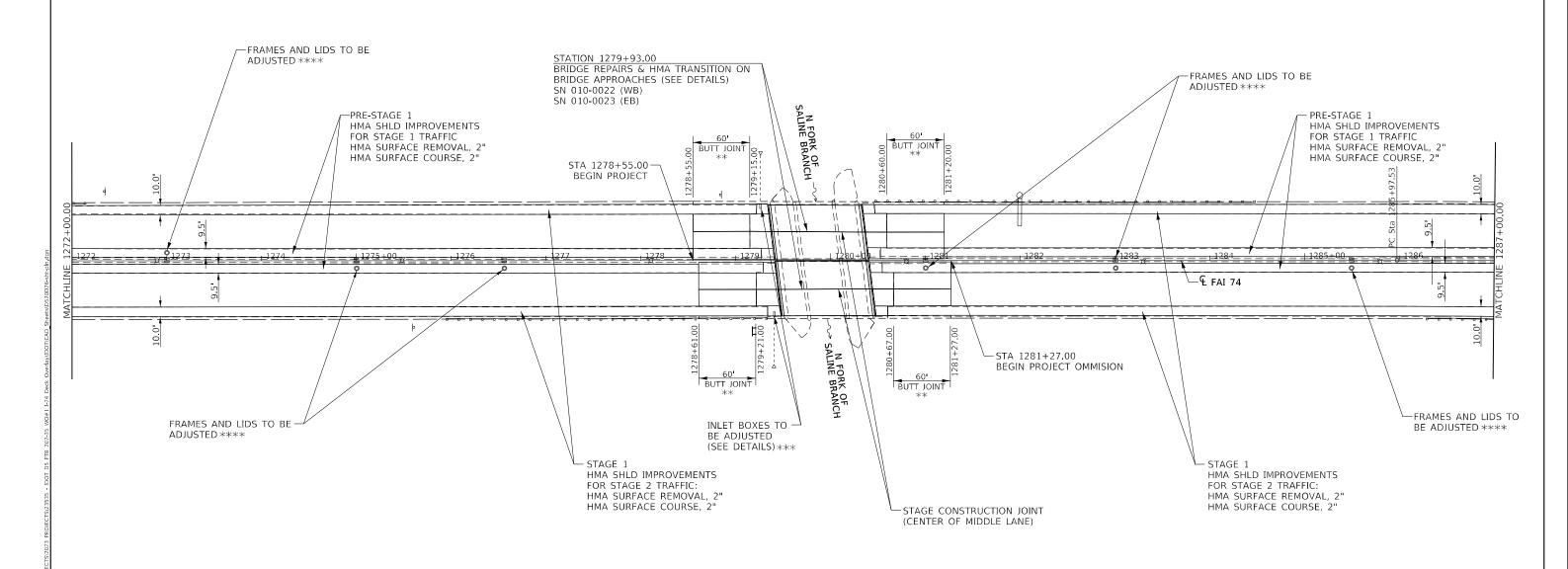
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES & BENCHMARKS SCALE: N.T.S SHEET 1 OF 1 SHEETS STA. 1255+00 TO STA. 1365+00

(14HB)BR,(14B)BR-1 90 24 CONTRACT NO. 70D76







** SEE DETAILS ON SHEET 86

*** INLETS TO BE TEMPORARILY FILLED PRIOR TO STAGE CONSTRUCTION TRAFFIC (SEE SPECIAL PROVISIONS)

**** SEE SPECIAL PROVISIONS; FRAME AND LIDS TO BE TEMPORARLY REMOVED, STEEL PLATED, AND RESURFACED OVER FOR STAGE CONSTRUCTION TRAFFIC; ADJUSTED AFTER TRAFFIC IS REMOVED FROM SHOULDER

Ó.		US
NAME:	BACON FARMER WORKMAN	
_	ENGINEERING & TESTING, INC.	PL
FILE		PLO

DESIGNED JSER NAME = sskinner REVISED DRAWN REVISED LOT SCALE = 0.1666 / in HECKED REVISED LOT DATE = 8/15/2024 DATE

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION I-74 (14HB)BR,(14B)BR-1 PLAN SHEETS SCALE: 1"=50' SHEET 2 OF 7 SHEETS STA. 1272+00.00 TO STA. 1287+00.00

90 26 CHAMPAIGN CONTRACT NO. 70D76

EXIST. CURVE 174WEST_14
PI STA. = 1299+49.05
Δ = 13° 02' 43" (RT)
D = 0° 29' 05"
R = 11,820.52'
T = 1,351.52'
L = 2,691.36'
E = 77.01' L = 2,691.36' E = 77.01' e = 1.56% T.R. = S.E. RUN = P.C. STA. = 1285+97.53 P.T. STA. = 1312+88.88 FRAMES AND LIDS TO BE ADJUSTED **** **** SEE SPECIAL PROVISIONS; FRAME AND LIDS TO BE TEMPORARLY REMOVED, STEEL PLATED, AND RESURFACED OVER FOR STAGE CONSTRUCTION TRAFFIC; ADJUSTED AFTER TRAFFIC IS REMOVED FROM SHOULDER

BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC.

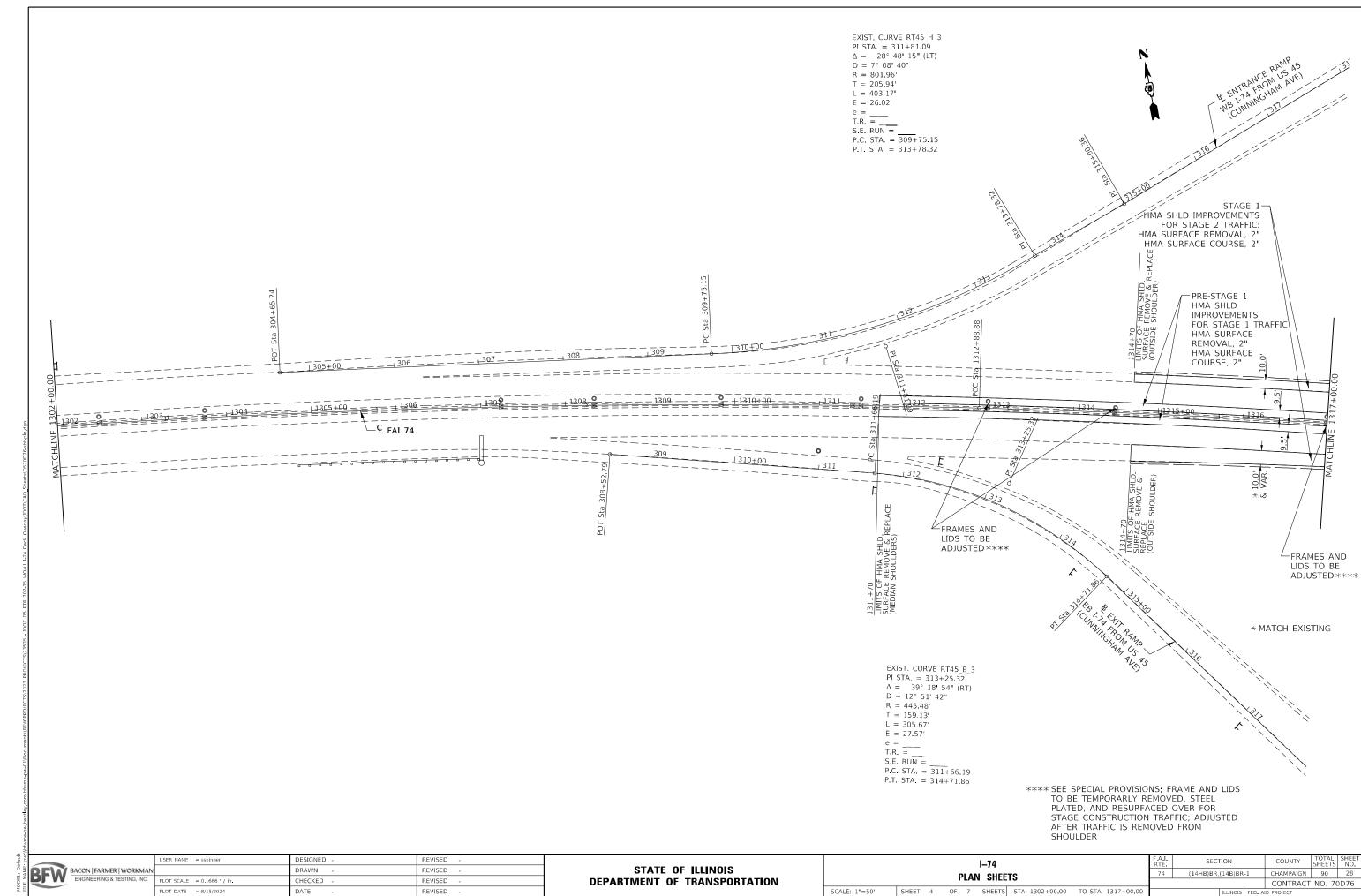
 USER NAME
 = sskinner
 DESIGNED REVISED

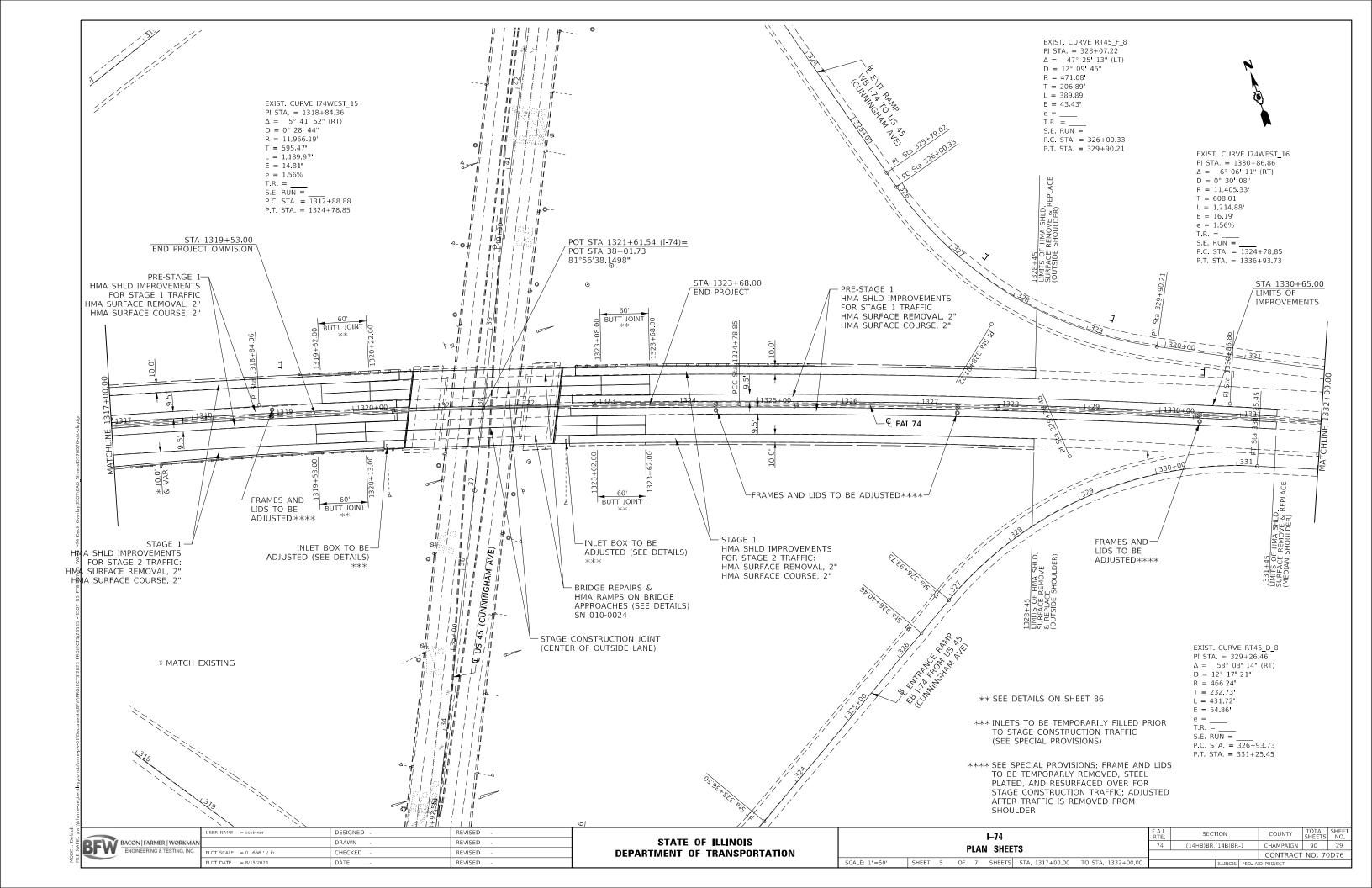
 N
 DRAWN REVISED

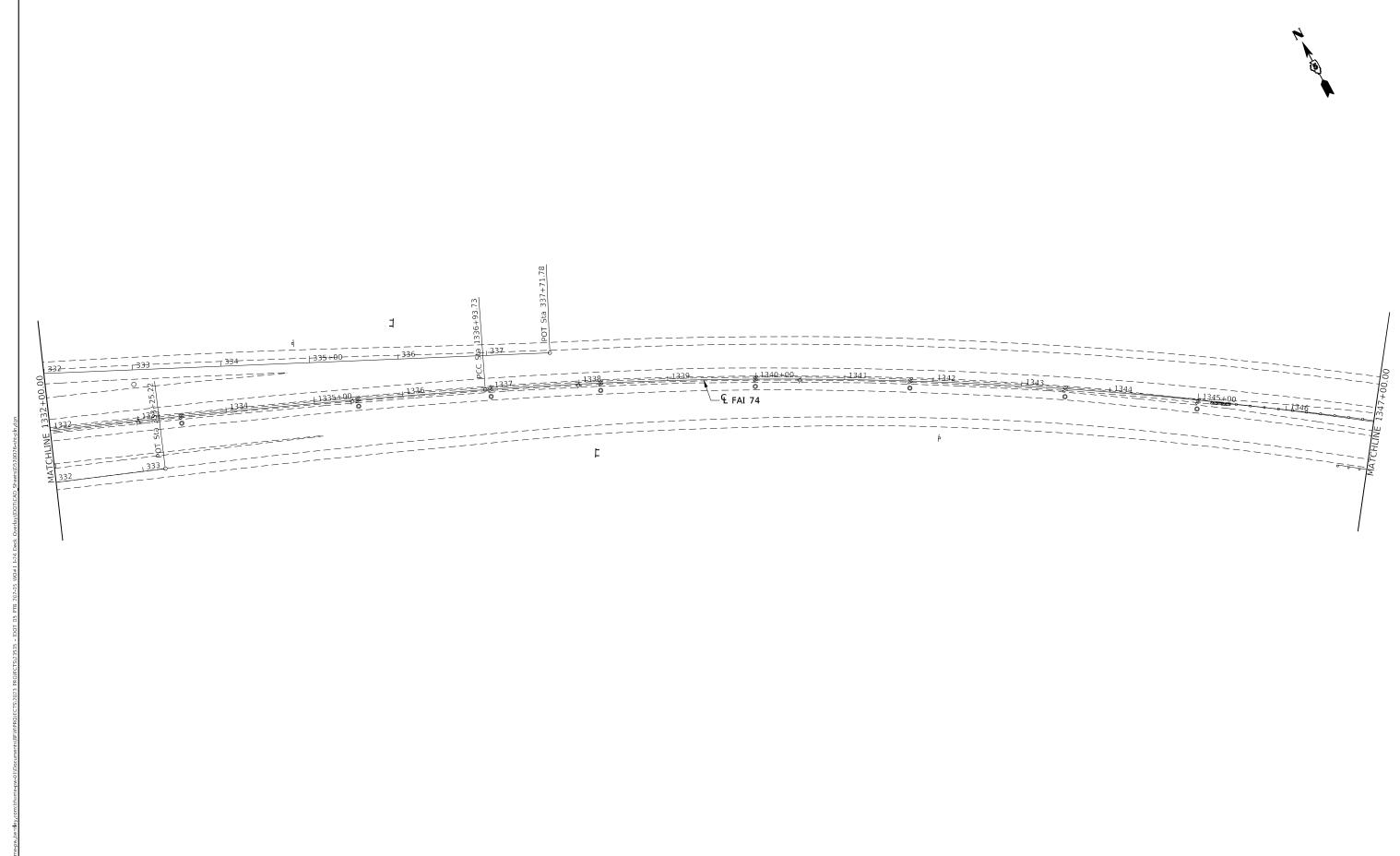
 PLOT SCALE
 = 0.1666 ' / in.
 CHECKED REVISED

 PLOT DATE
 = 8/15/2024
 DATE REVISED

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION







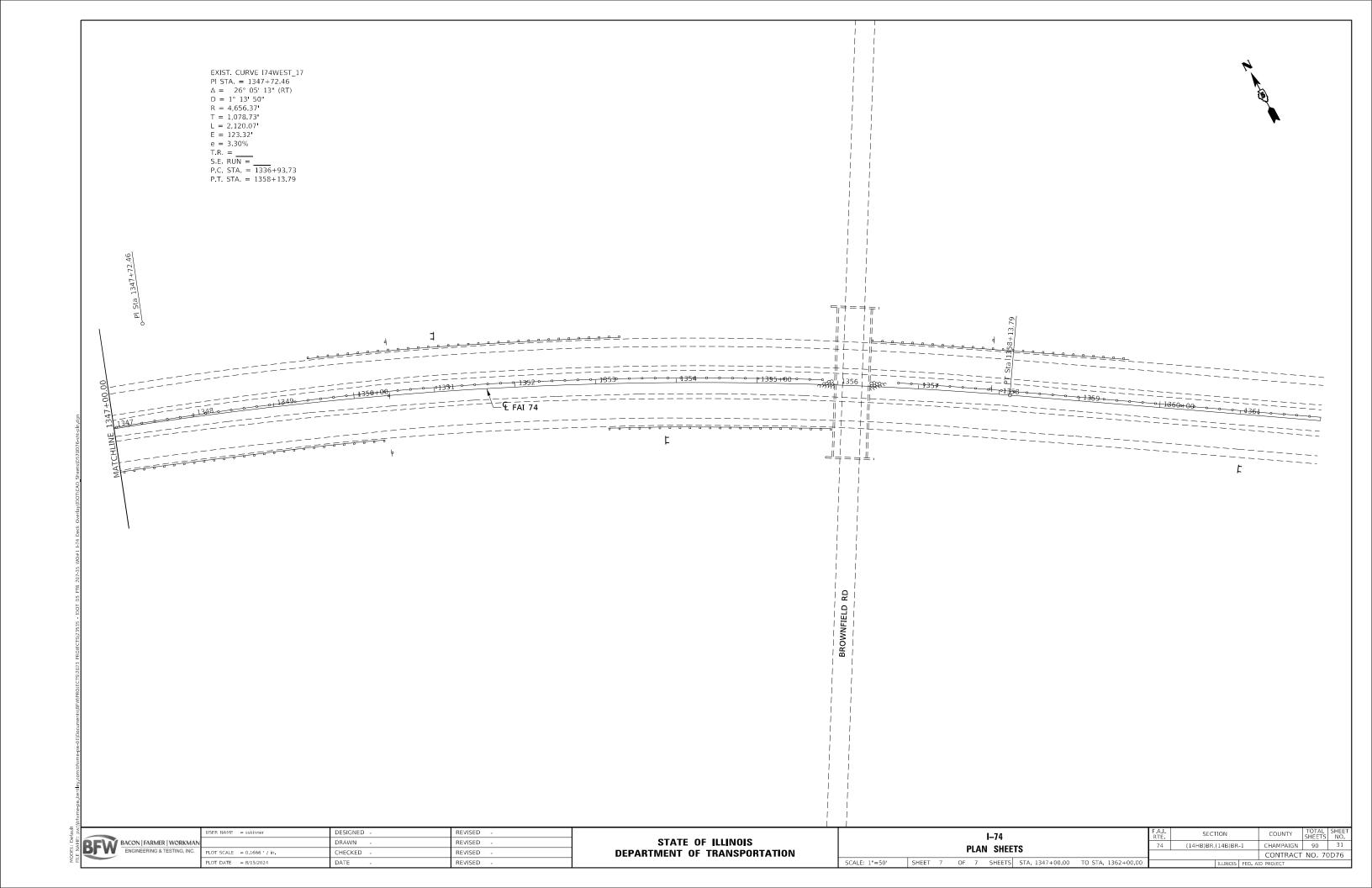
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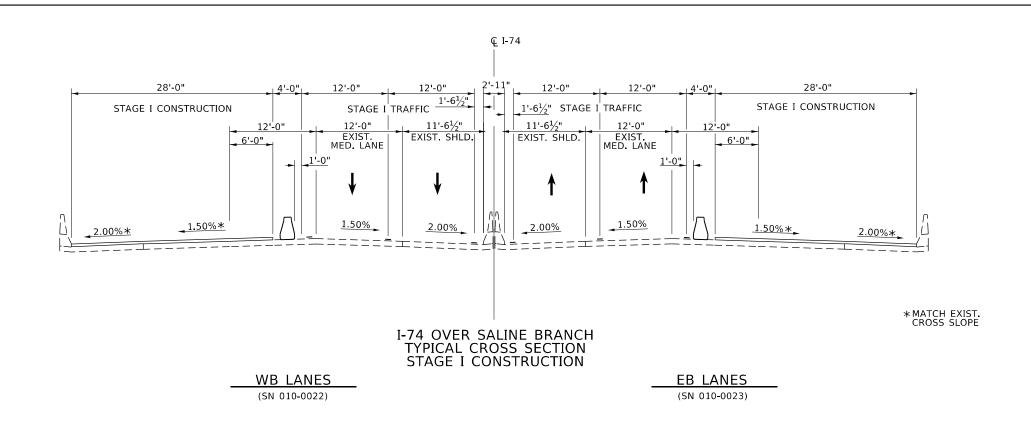
BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC.

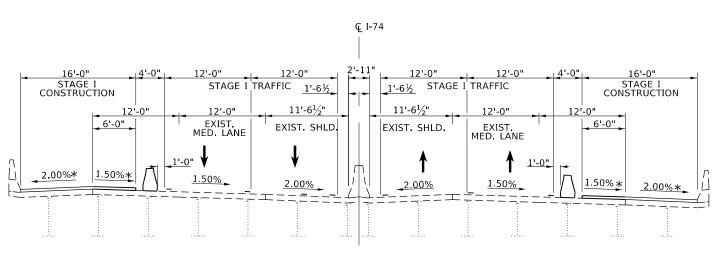
DESIGNED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| -74 | F.A.I. RTE. | F.A.I. R







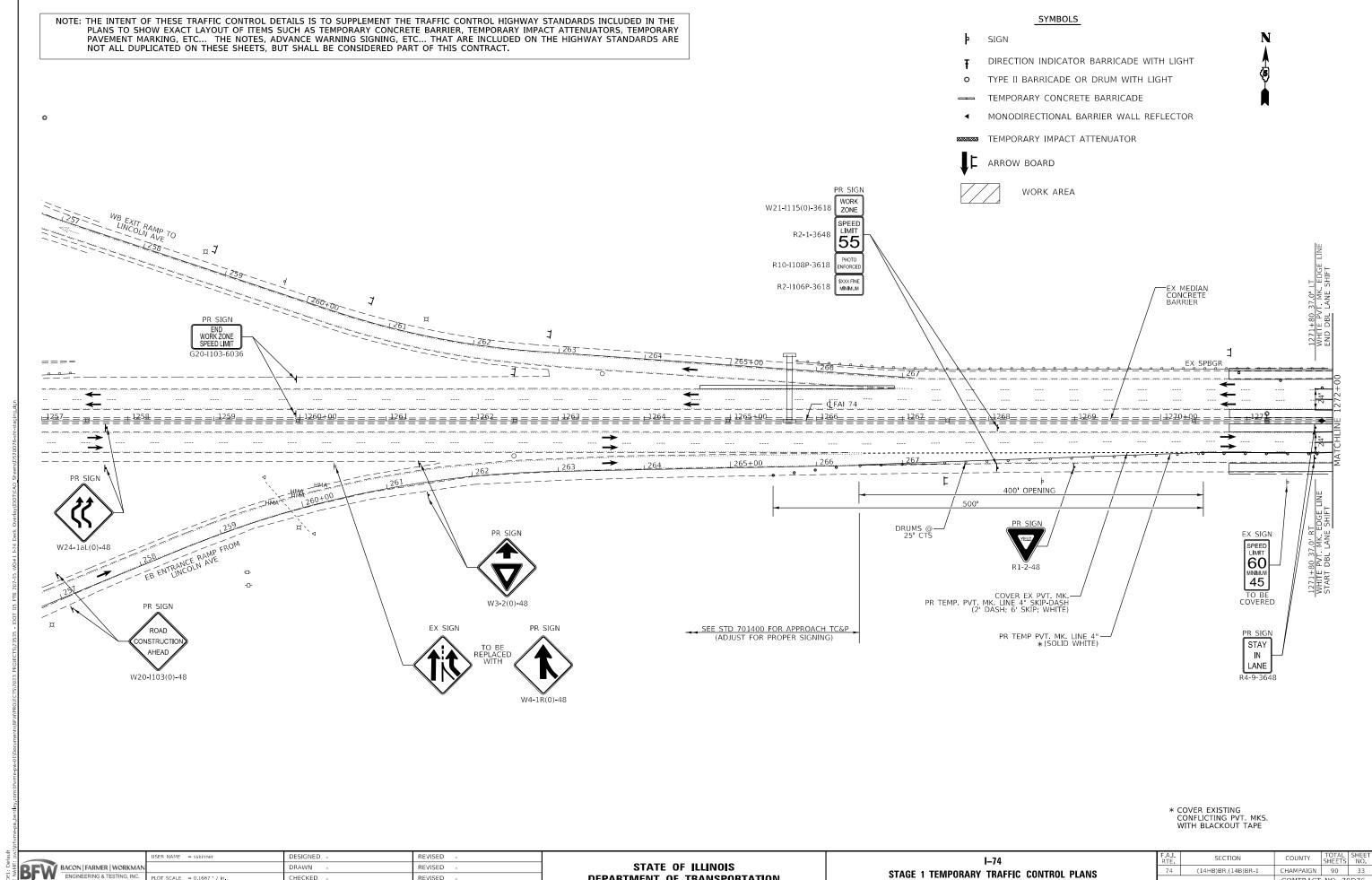
I-74 OVER US 45 TYPICAL CROSS SECTION STAGE I CONSTRUCTION (SN 010-0024)

*MATCH EXIST. CROSS SLOPE

WB LANES

EB LANES

∯P£													
B NAME: pw:		USER NAME = sskinner	DESIGNED -	REVISED -	STATE OF ILLINOIS	I-74 STAGE I			F.A.I.	SECTION	COUNTY	TOTAL S	HEET
	BACON FARMER WORKMAN		DRAWN -	REVISED -					74	(14HB)BR.(14B)BR-1	CHAMPAIGN	90	32
	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		TEMPORARY TRAFFIC CONTROL TYPICAL SECTIONS				CONTRACT	CT NO. 70D	576
Ē		PLOT DATE = 8/15/2024	DATE -	REVISED -		SCALE: N.T.S.	SHEET 1 OF 1 SHEETS	STA. TO STA.		ILLINOIS FED.	AID PROJECT		



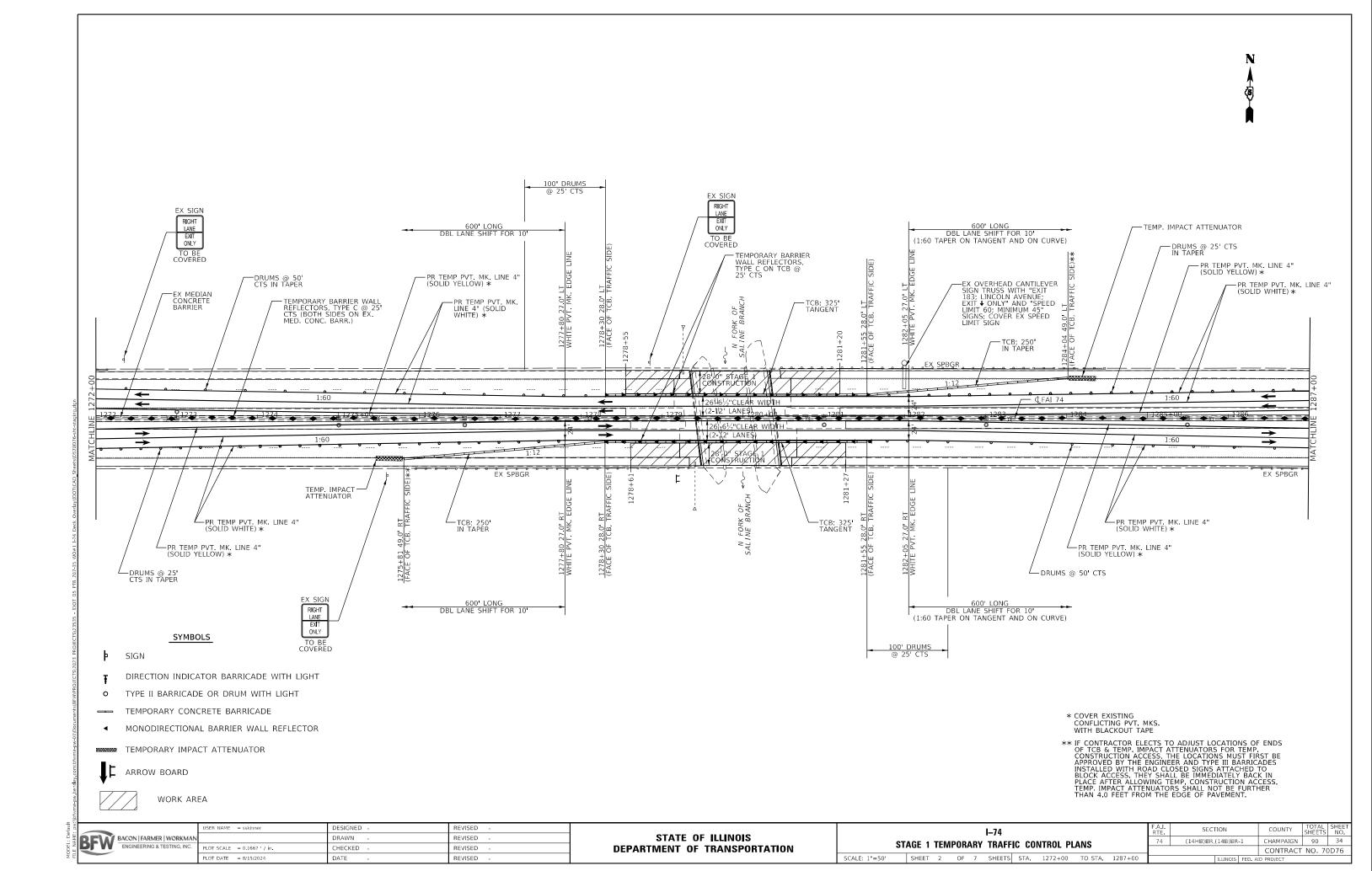
DEPARTMENT OF TRANSPORTATION

HECKED

REVISED

STAGE 1 TEMPORARY TRAFFIC CONTROL PLANS SHEET 1 OF 7 SHEETS STA. 1257+00 TO STA. 1272+00

(14HB)BR,(14B)BR-1 CHAMPAIGN 90 33 CONTRACT NO. 70D76



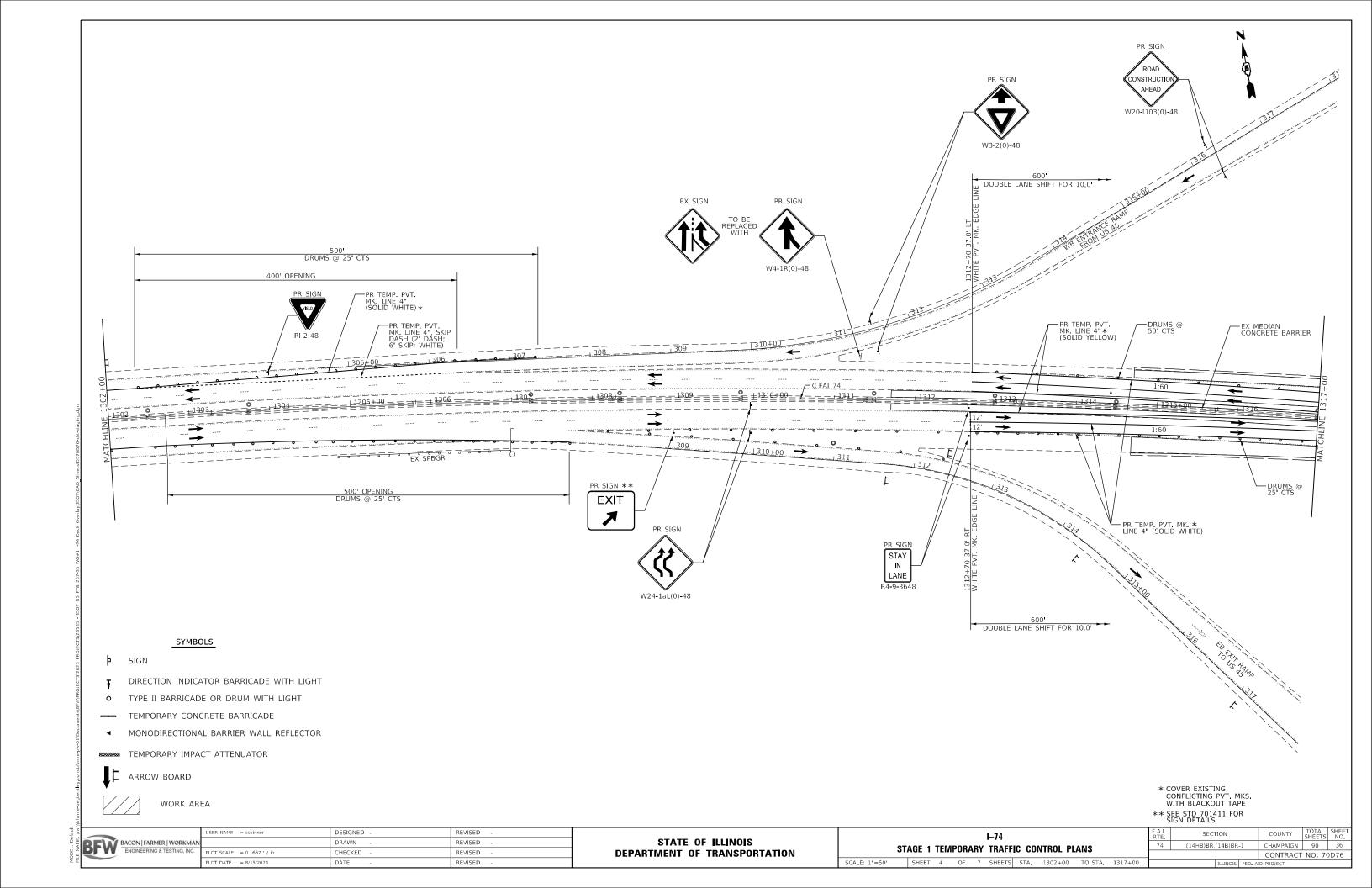
PR SIGN W21-I115(0)-3618 WORK ZONE PR SIGN STAY IN LANE R4-9-3648 R2-1-3648 R10-I108P-3618 288+05 37 0' LT VHITE PVT MK EDGE LINE —EX MEDIAN CONCRETE BARRIER R2-I106P-3618 PR TEMP. PVT. MK. LINE 4" (SOLID WHITE) DRUMS @ 50' CTS DRUMS @ 25' CTS 1288+05 37.0' RT WHITE PVT. MK. EDGE - DRUMS @ 50' CTS WORK ZONE W21-I115(0)-3618 PR SIGN **

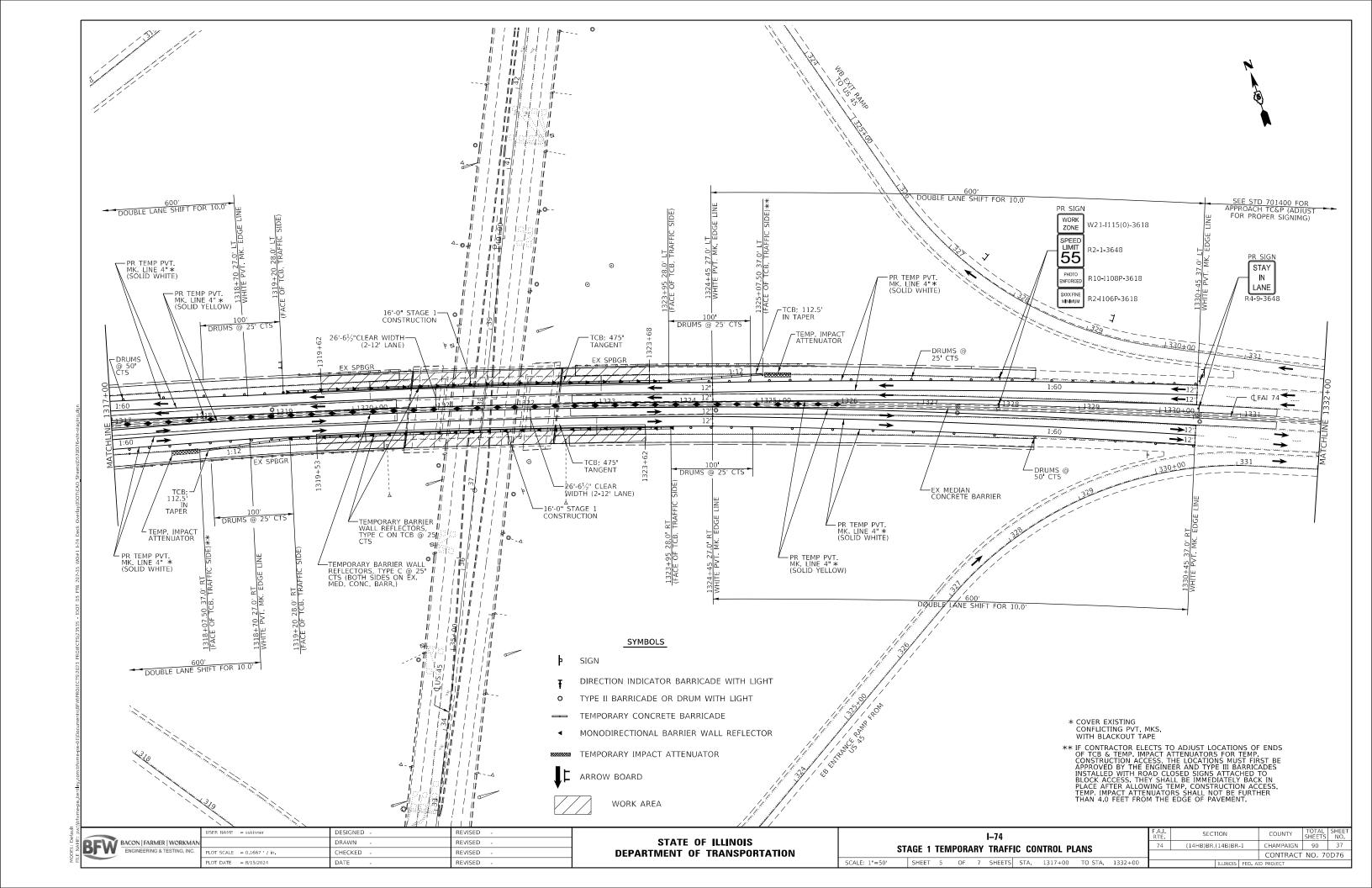
EXIT

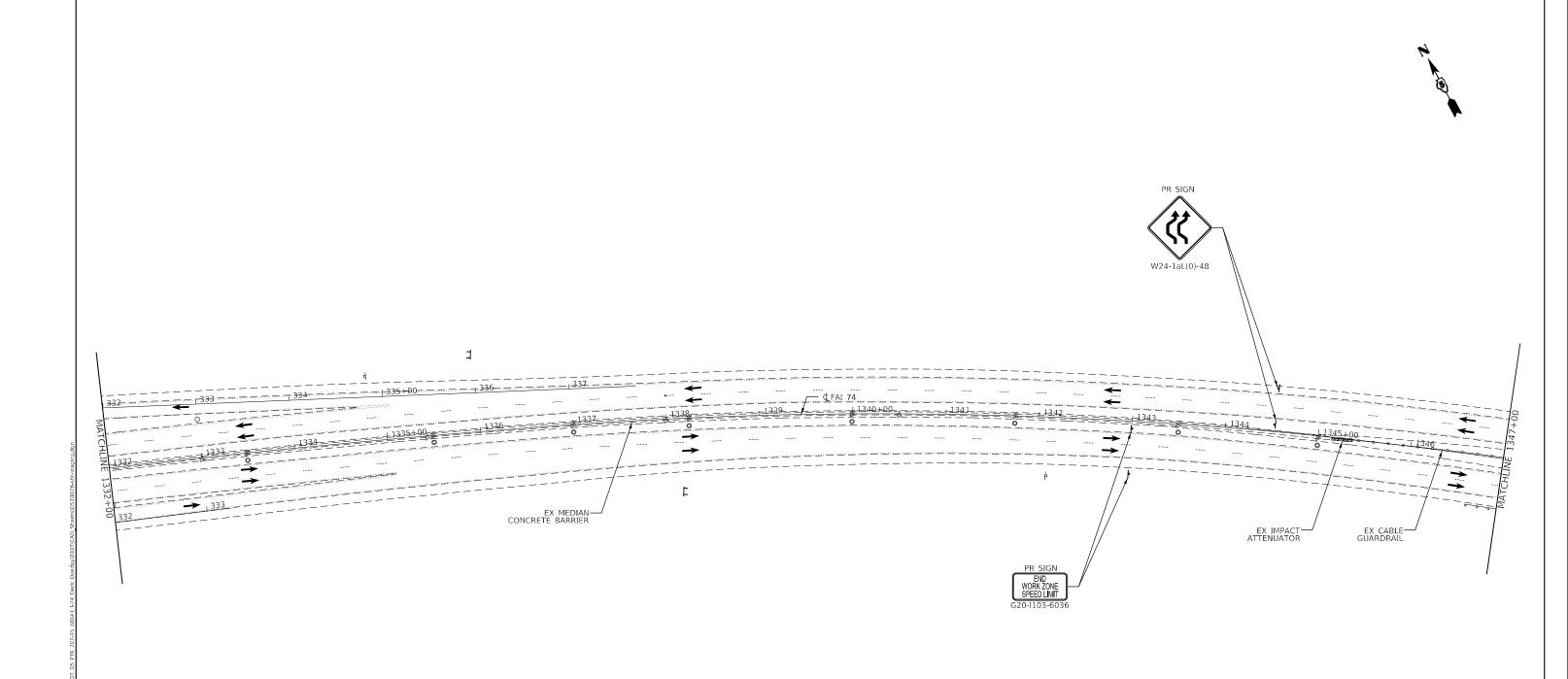
OPEN

AHEAD SPEED LIMIT 55 R2-1-3648 R10-I108P-3618 R2**-**I106P**-**3618 SYMBOLS SIGN DIRECTION INDICATOR BARRICADE WITH LIGHT TYPE II BARRICADE OR DRUM WITH LIGHT TEMPORARY CONCRETE BARRICADE MONODIRECTIONAL BARRIER WALL REFLECTOR TEMPORARY IMPACT ATTENUATOR ARROW BOARD * COVER EXISTING CONFLICTING PVT, MKS, WITH BLACKOUT TAPE WORK AREA ** SEE STD 701411 FOR SIGN DETAILS DESIGNED REVISED SECTION I-74 BFW BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC. STATE OF ILLINOIS DRAWN REVISED (14HB)BR,(14B)BR-1 STAGE 1 TEMPORARY TRAFFIC CONTROL PLANS LOT SCALE = 0.1667 / in. CHECKED REVISED **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. 70D76

SCALE: 1"=50' SHEET 3 OF 7 SHEETS STA. 1287+00 TO STA. 1302+00







SYMBOLS

- SIGN
- DIRECTION INDICATOR BARRICADE WITH LIGHT
- TYPE II BARRICADE OR DRUM WITH LIGHT
- TEMPORARY CONCRETE BARRICADE
- MONODIRECTIONAL BARRIER WALL REFLECTOR

******* TEMPORARY IMPACT ATTENUATOR

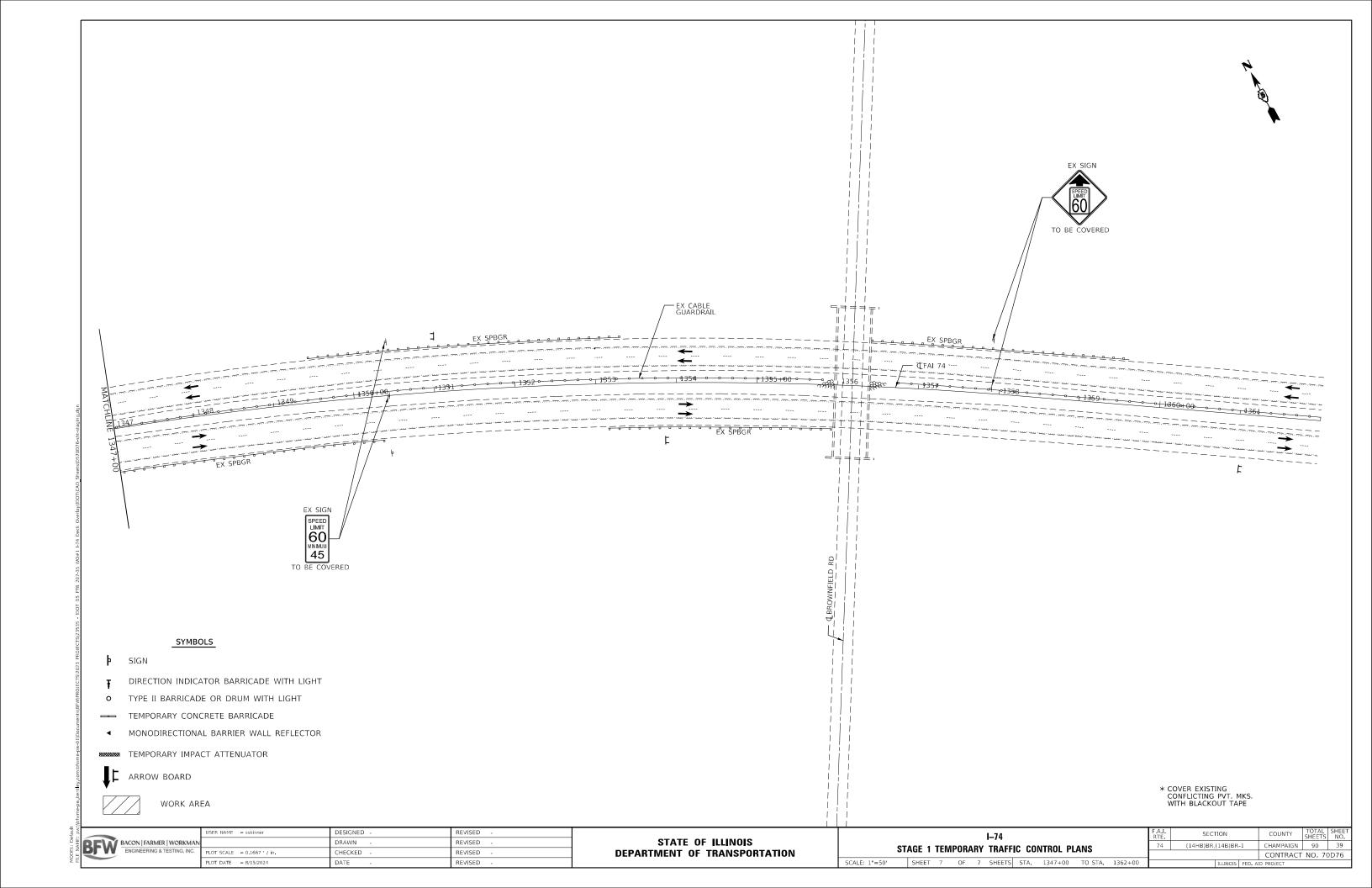
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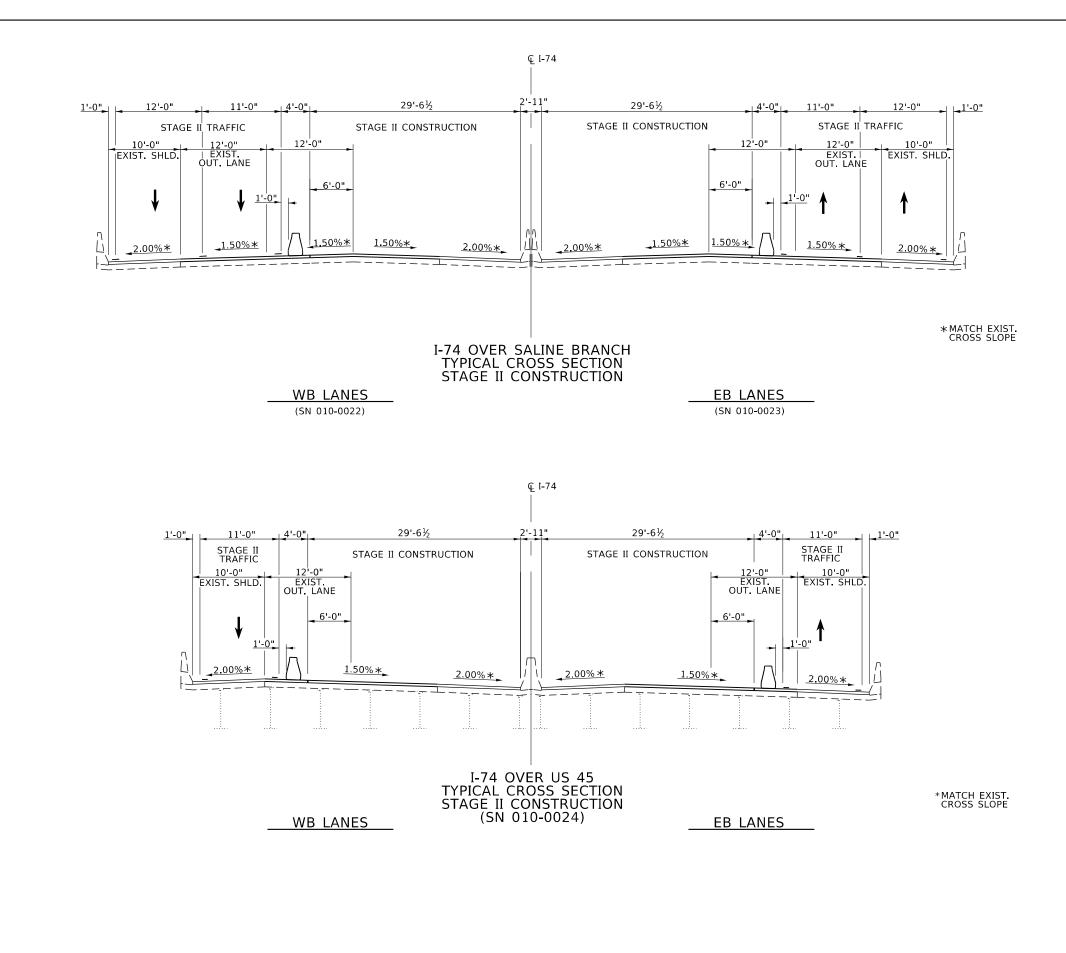
ARROW BOARD

WORK AREA

* COVER EXISTING CONFLICTING PVT. MKS. WITH BLACKOUT TAPE

: bwc		USER NAME = sskinner	DESIGNED -	REVISED -		I–74	F.A.I. RTE	SECTION	COUNTY TOTAL SHEET SHEETS NO.
BF	BACON FARMER WORKMAN		DRAWN -	REVISED -	STATE OF ILLINOIS	STAGE 1 TEMPORARY TRAFFIC CONTROL PLANS	74	(14HB)BR,(14B)BR-1	CHAMPAIGN 90 38
	ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	STAGE I TEMPUKAKI TKAFFIC CUNTKUL PLANS			CONTRACT NO. 70D76
Ĺ		PLOT DATE = 8/15/2024	DATE -	REVISED -		SCALE: 1"=50' SHEET 6 OF 7 SHEETS STA. 1332+00 TO STA. 1347+00		ILLINOIS FED. A	ID PROJECT





l			USER
١	- 1 A S S S S S S S S S S S S S S S S S S	ACON FARMER WORKMAN	
١		ENGINEERING & TESTING, INC.	PLOT
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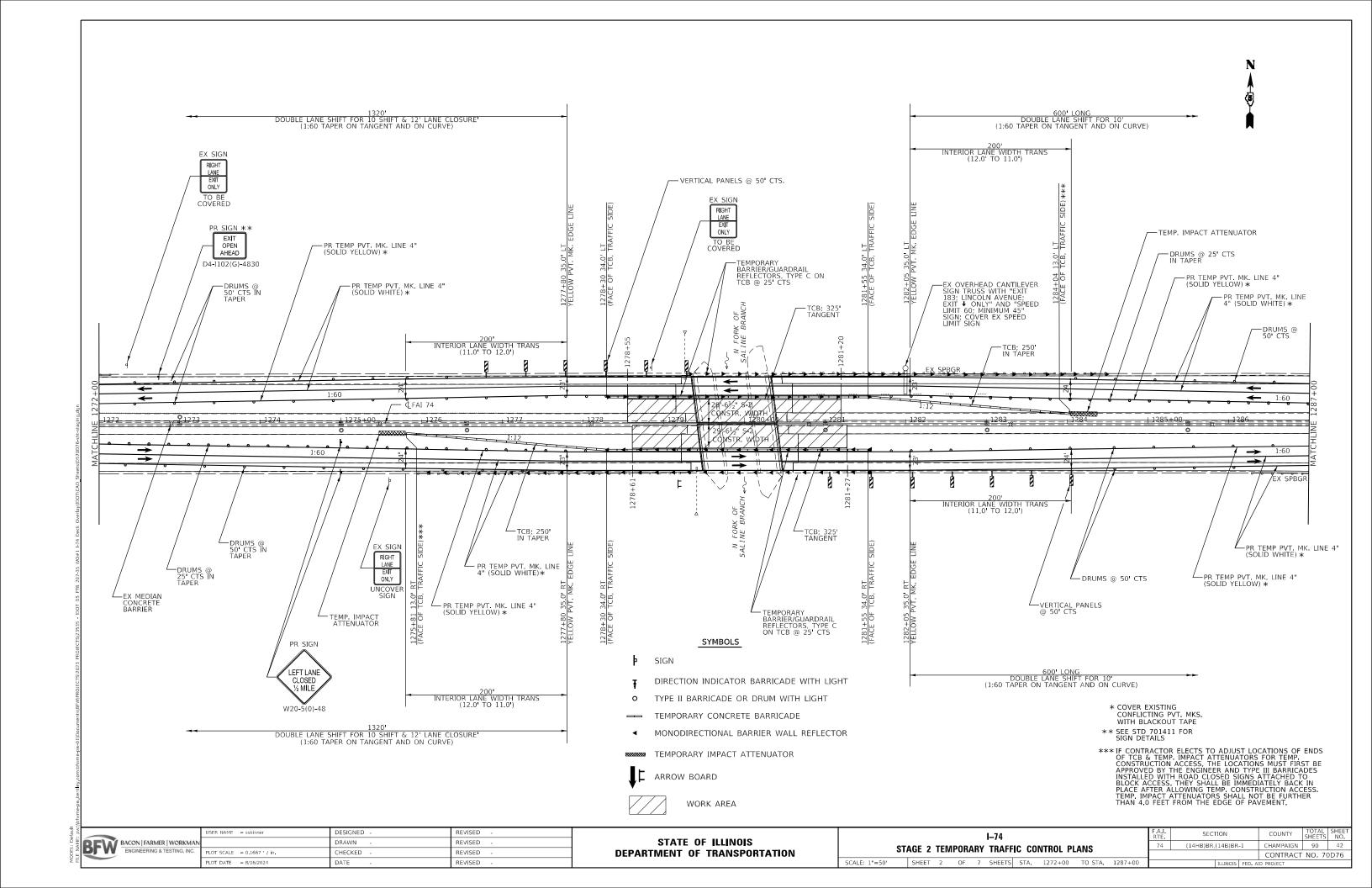
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~	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

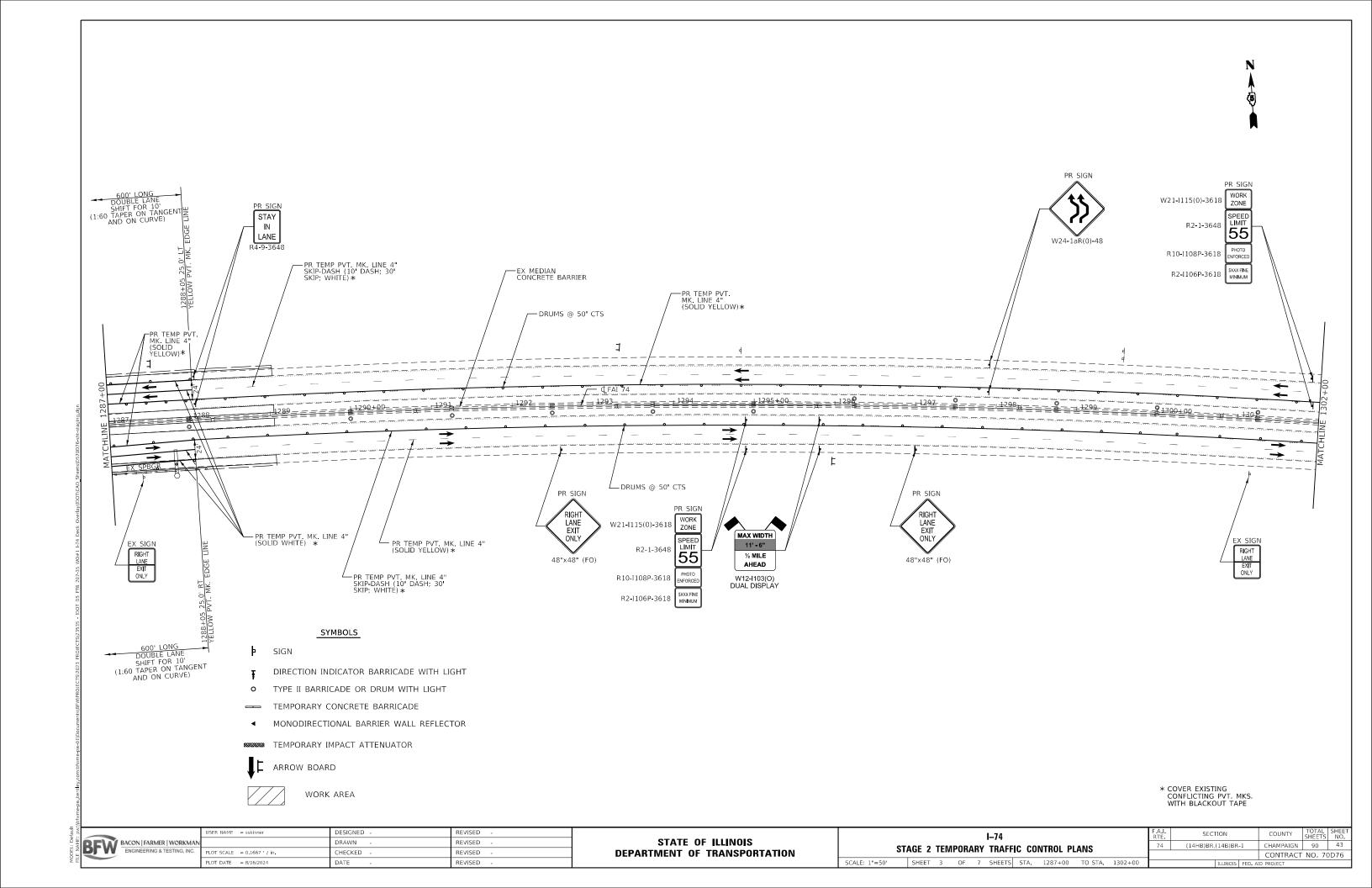
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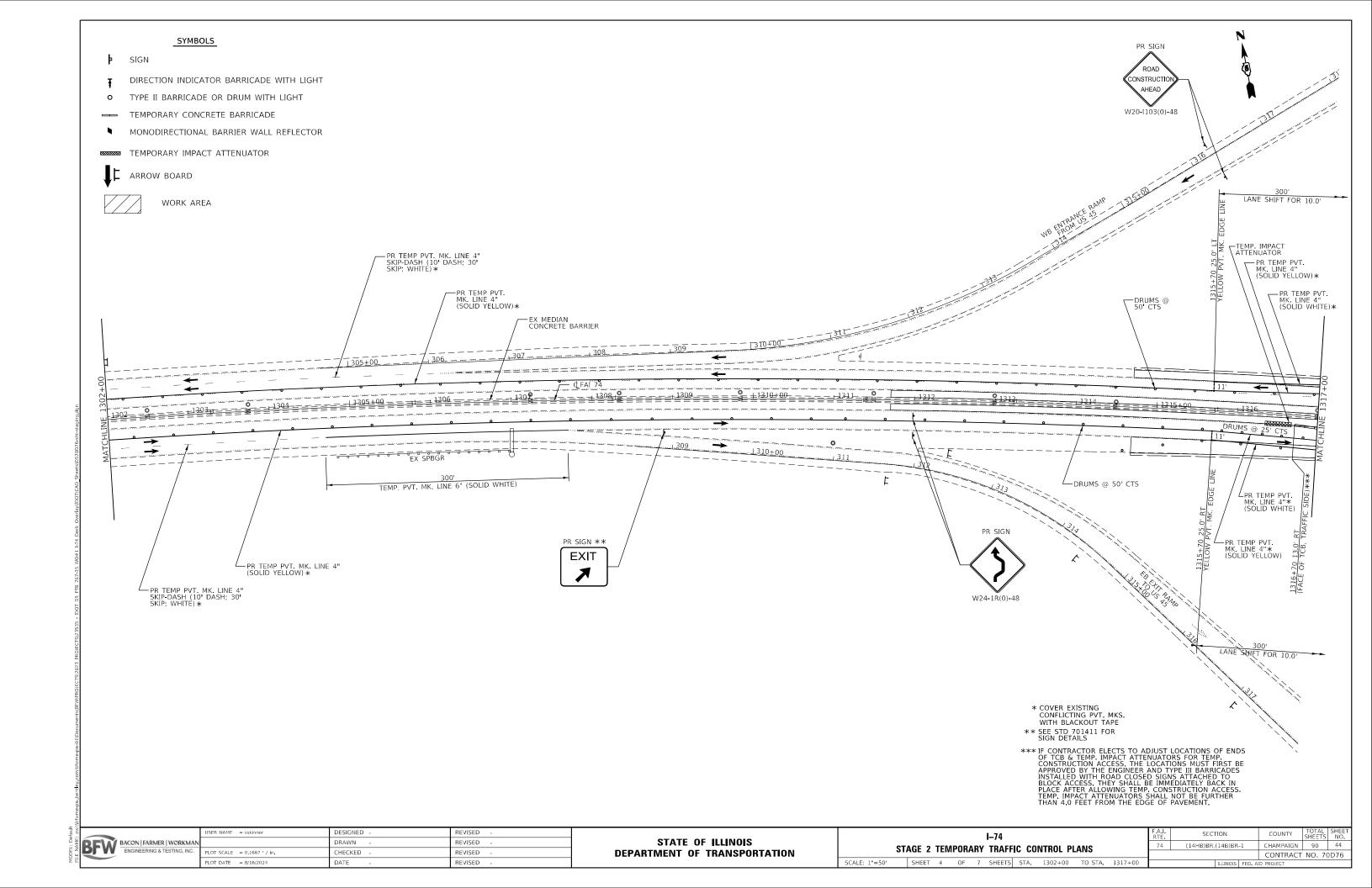
	F.A.I. RTE	SECTION	
TEMPORARY TRAFFIC CONTROL TYPICAL SECTIONS	74	(14HB)BR,(14B)BR-1	CH
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SHEET 1 OF 1 SHEETS STA. TO STA.		ILLINOIS FED.	AID PF

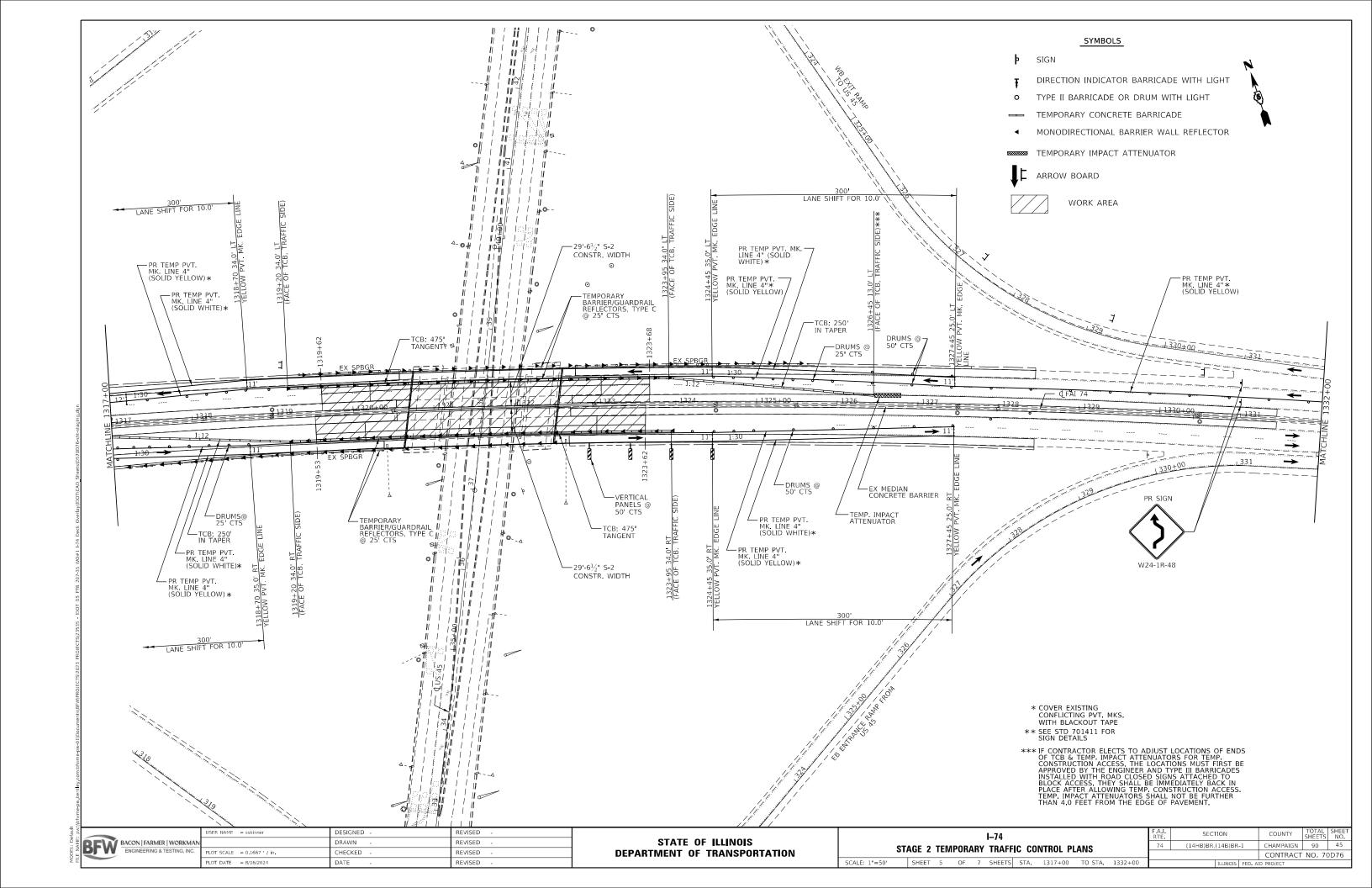
CHAMPAIGN 90 40 CONTRACT NO. 70D76 SCALE: N.T.S.

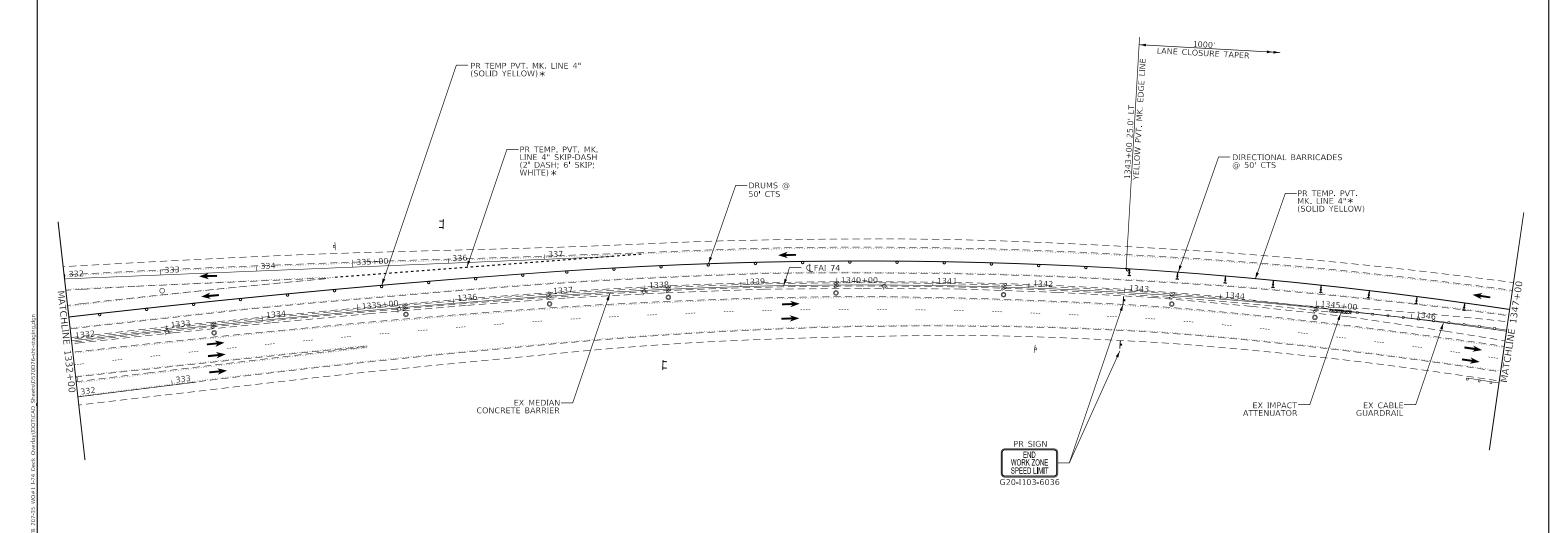
NOTE: THE INTENT OF THESE TRAFFIC CONTROL DETAILS IS TO SUPPLEMENT THE TRAFFIC CONTROL HIGHWAY STANDARDS INCLUDED IN THE PLANS TO SHOW EXACT LAYOUT OF ITEMS SUCH AS TEMPORARY CONCRETE BARRIER, TEMPORARY IMPACT ATTENUATORS, TEMPORARY PAVEMENT MARKING, ETC... THE NOTES, ADVANCE WARNING SIGNING, ETC... THAT ARE INCLUDED ON THE HIGHWAY STANDARDS ARE NOT ALL DUPLICATED ON THESE SHEETS, BUT SHALL BE CONSIDERED PART OF THIS CONTRACT. 1320' DOUBLE LANE SHIFT FOR 10 SHIFT & 12' LANE CLOSURE (1:60 TAPER ON TANGENT AND ON CURVE) PR SIGN ** 1264+60 13.0' LT /ELLOW PVT. MK. EDGE **EXIT** -PR TEMP PVT. MK. LINE 4" (SOLID YELLOW)∗ -PR TEMP PVT. MK. LINE 4" (SOLID WHITE)∗ EX MEDIAN CONCRETE BARRIER PR TEMP PVT. MK.-LINE 4" SKIP-DASH (2' DASH; 6' SKIP; WHITE) EX MEDIAN CONCRETE BARRIER DRUMS @ -50' CTS DRUMS @ 50' CTS END WORK ZONE SPEED LIMIT 180' OPENING /─ **(**FAI 74 --*-*---------PR SIGN 300' OPENING 500' PR TEMP PVT. MK.— LINE 4" SKIP-DASH (10' DASH; 30' SKIP; WHITE)* STAY LANE WORK W21-I115(0)-3618 SPEED LIMIT W3-2(0)-48 60 R2-1-3648 PR TEMP PVT. — MK. LINE 4"* (SOLID WHITE) 45 ROAD R10-I108P-3618 TO BE COVERED 1264+60 13.0' RT YELLOW PVT MK ONSTRUCTION AHFAD SYMBOLS W20-I103(0)-48 PR TEMP PVT. MK. LINE — 4" SKIP-DASH (2' DASH; 6' SKIP; WHITE)* PR TEMP PVT.— MK. LINE 4" (SOLID YELLOW)* DIRECTION INDICATOR BARRICADE WITH LIGHT TYPE II BARRICADE OR DRUM WITH LIGHT SEE STD 701400 FOR APPROACH TC&P (ADJUST FOR PROPER SIGNING) DOUBLE LANE SHIFT FOR 10 SHIFT & 12' LANE CLOSURE' (1:60 TAPER ON TANGENT AND ON CURVE) TEMPORARY CONCRETE BARRICADE MONODIRECTIONAL BARRIER WALL REFLECTOR TEMPORARY IMPACT ATTENUATOR ARROW BOARD * COVER EXISTING CONFLICTING PVT. MKS. WITH BLACKOUT TAPE WORK AREA ** SEE STD 701411 FOR SIGN DETAILS DESIGNED REVISED SECTION I-74 STATE OF ILLINOIS BACON FARMER WORKMA
ENGINEERING & TESTING, INC. DRAWN REVISED (14HB)BR,(14B)BR-1 CHAMPAIGN 90 41 STAGE 2 TEMPORARY TRAFFIC CONTROL PLANS LOT SCALE = 0.1667 / in. HECKED REVISED **DEPARTMENT OF TRANSPORTATION** CONTRACT NO. 70D76 SHEET 1 OF 7 SHEETS STA. 1257+00 TO STA. 1272+00











SYMBOLS

- SIGN
- DIRECTION INDICATOR BARRICADE WITH LIGHT
- TYPE II BARRICADE OR DRUM WITH LIGHT
- TEMPORARY CONCRETE BARRICADE
- MONODIRECTIONAL BARRIER WALL REFLECTOR

******** TEMPORARY IMPACT ATTENUATOR

ARROW BOARD

WORK AREA

* COVER EXISTING CONFLICTING PVT. MKS. WITH BLACKOUT TAPE

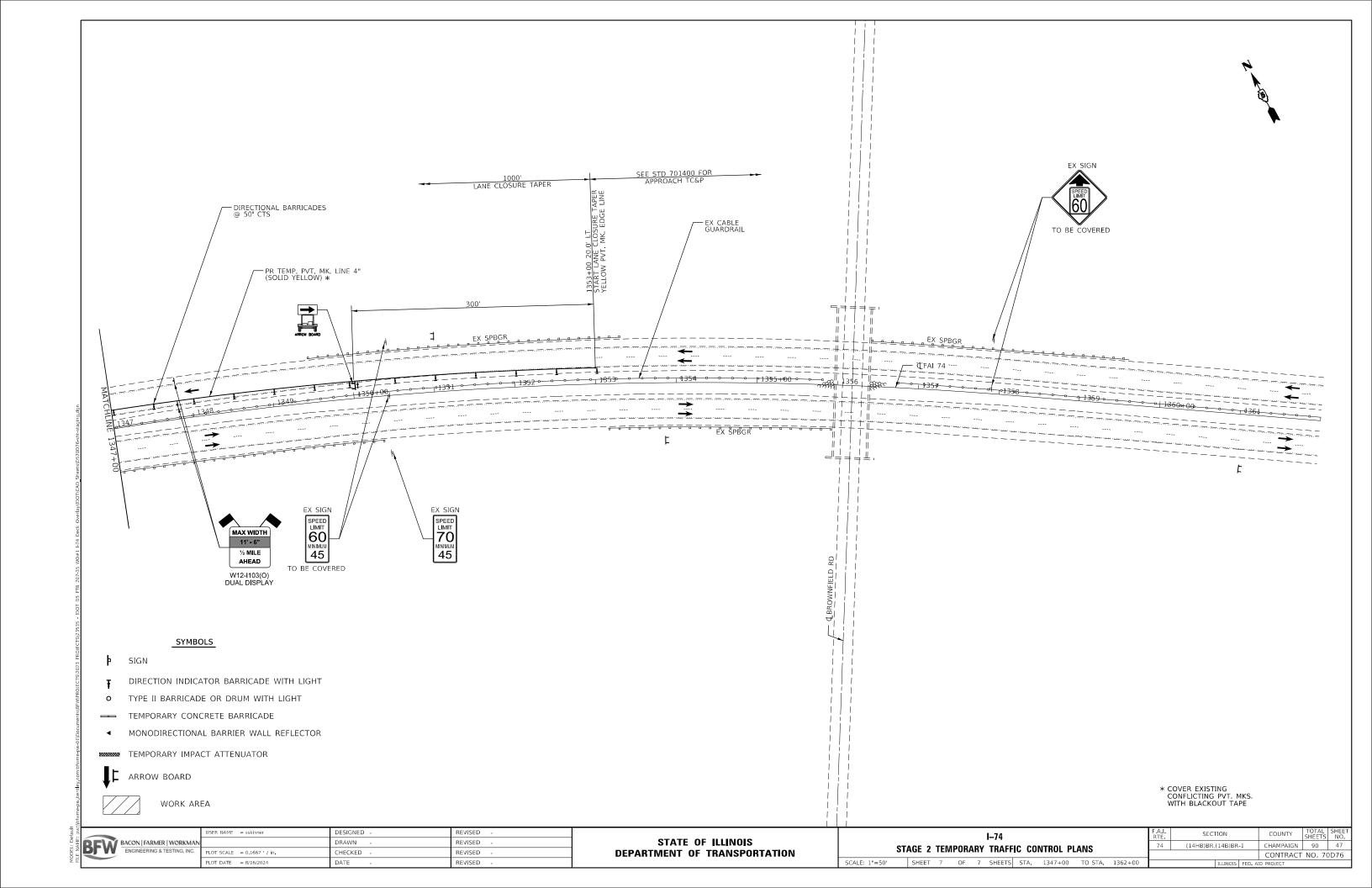
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NAME: pw	BFW	BACON FARMER WORKM ENGINEERING & TESTING, IN
FILE		

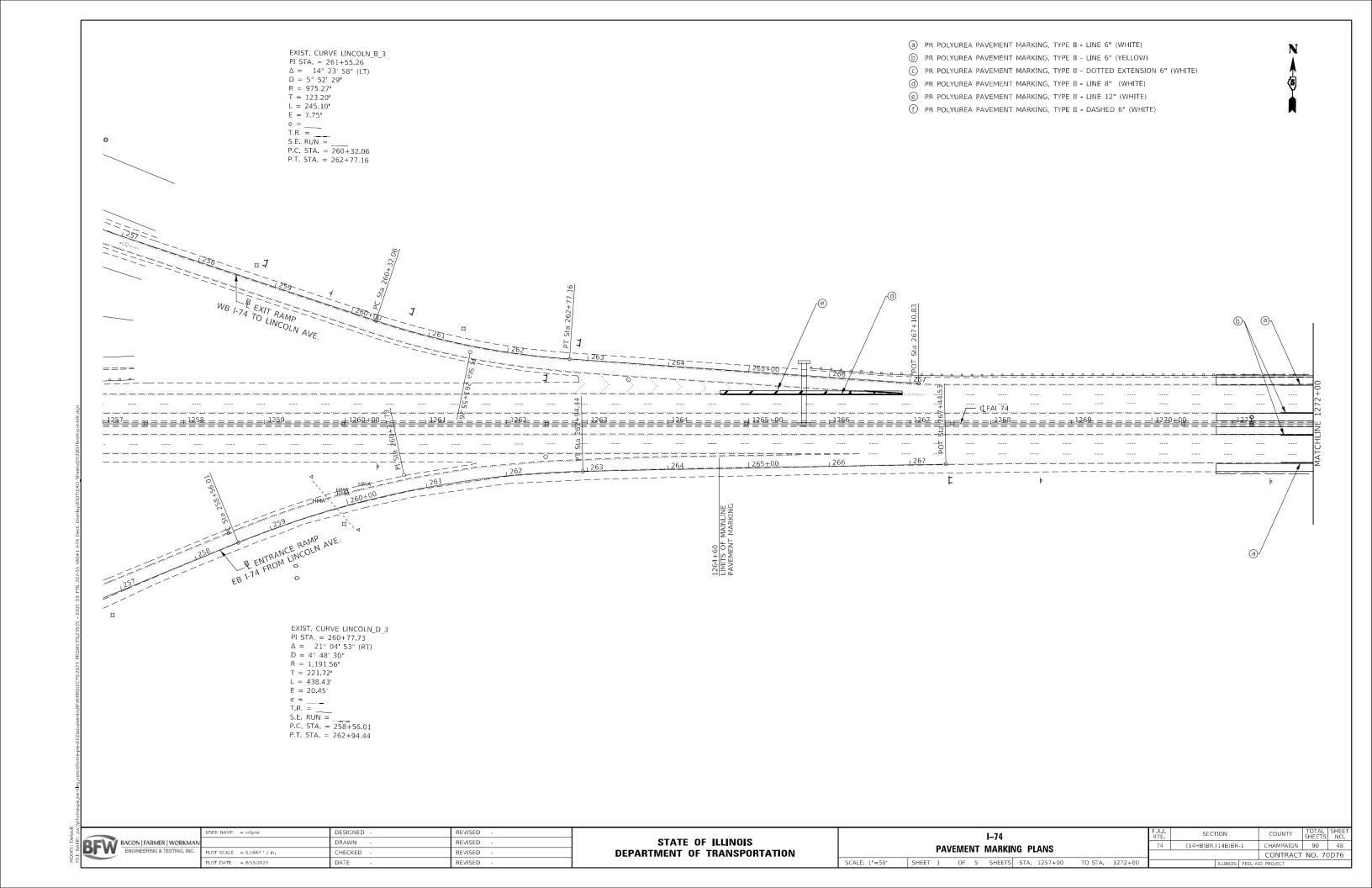
	USER NAME = sskinner	DESIGNED -	REVISED -
BACON FARMER WORKMAN		DRAWN -	REVISED -
ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/16/2024	DATE -	REVISED -

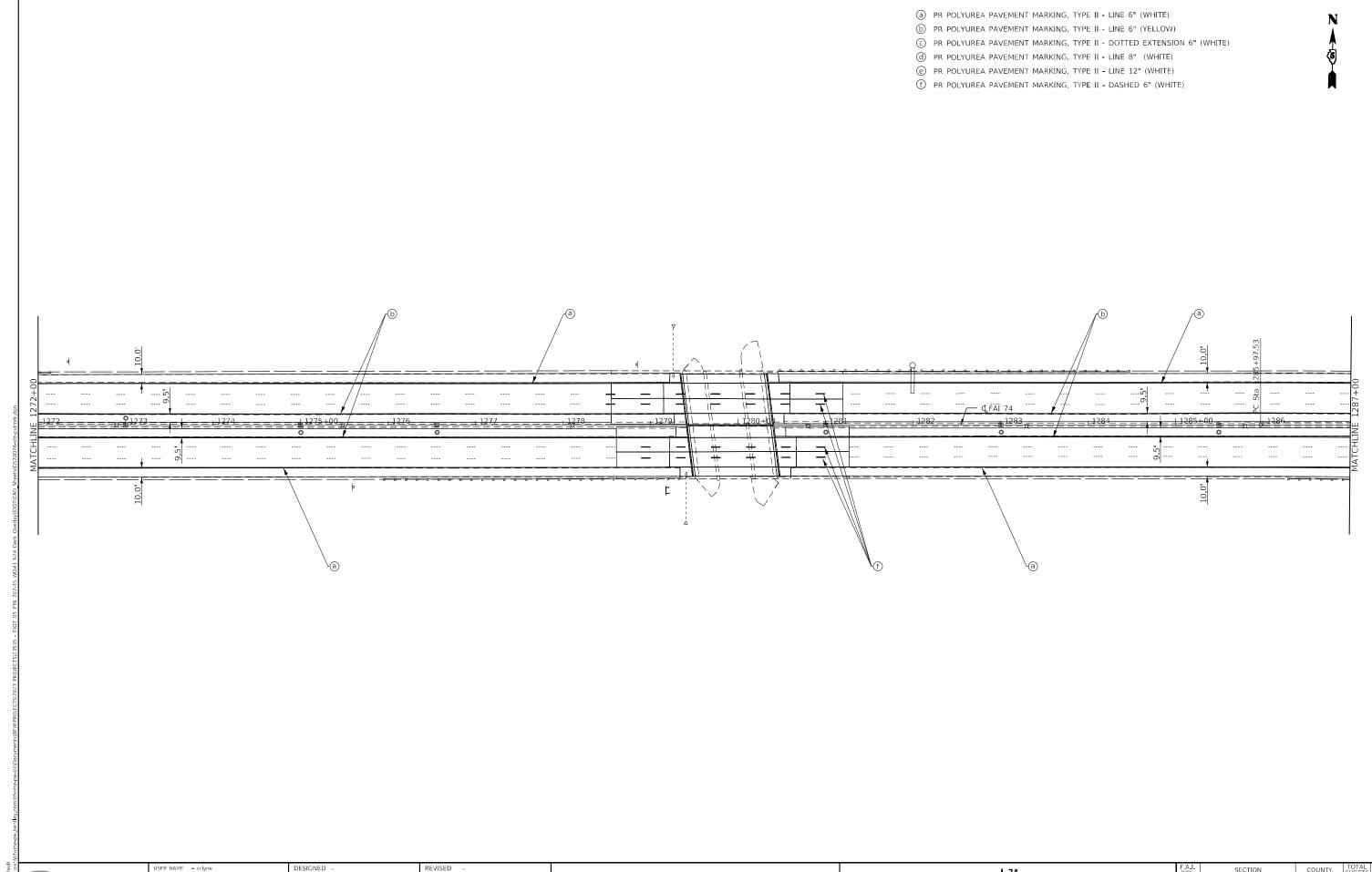
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50'		SHEET	6	OF	7	SHEETS	STA.	1332+00	TO STA.	1347+00		

RTE	SEC	HON		COUNTY	SHEETS	NO.
74	(14HB)BR,	(14B)BR	-1	CHAMPAIGN	90	46
				CONTRACT	NO. 70	D76
		ILLINOIS	FED. A	ID PROJECT		







BACON | FARMER | WORKMAN ENGINEERING & TESTING, INC.

DRAWN CHECKED PLOT DATE = 8/15/2024 DATE

REVISED

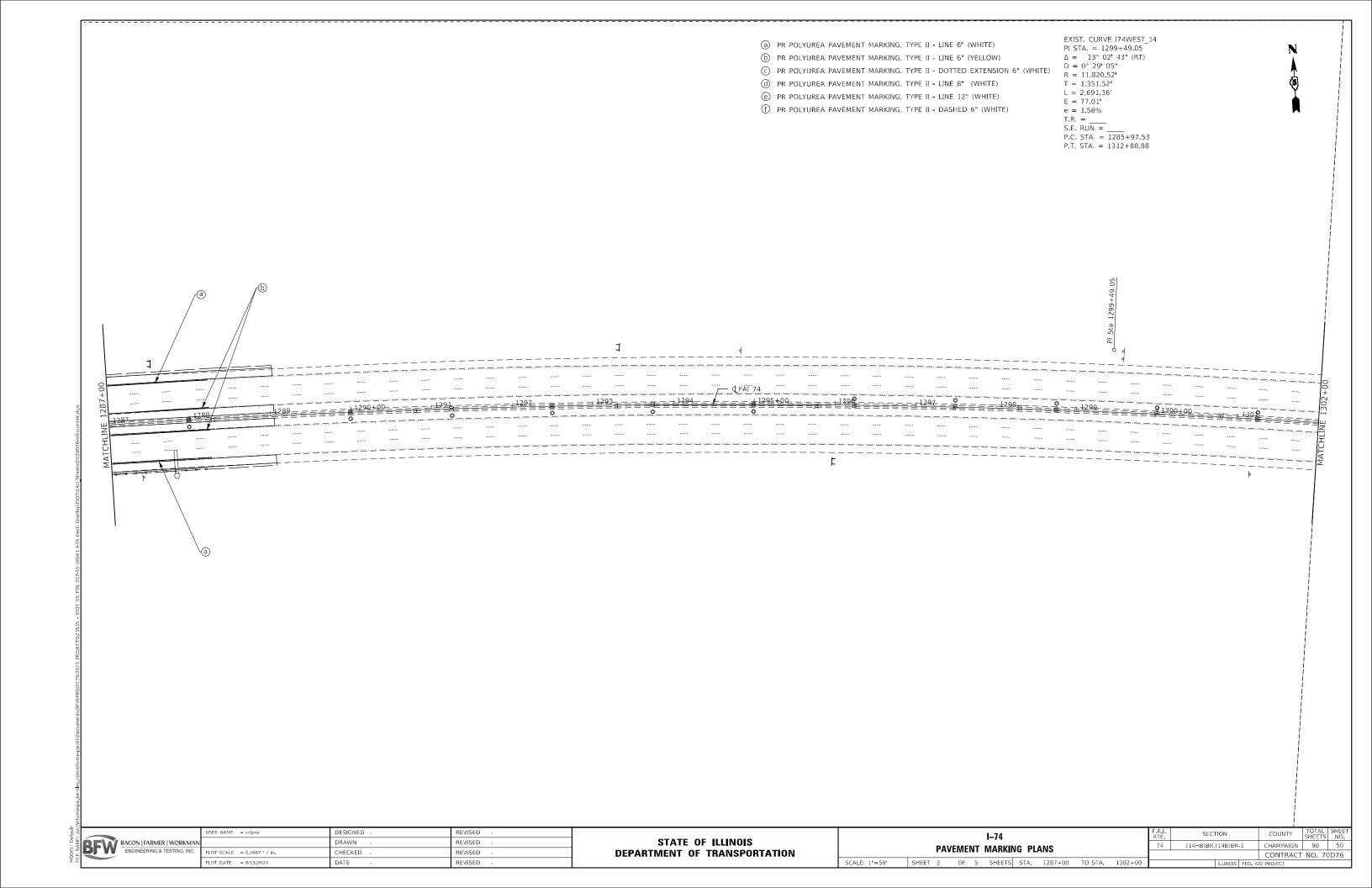
REVISED

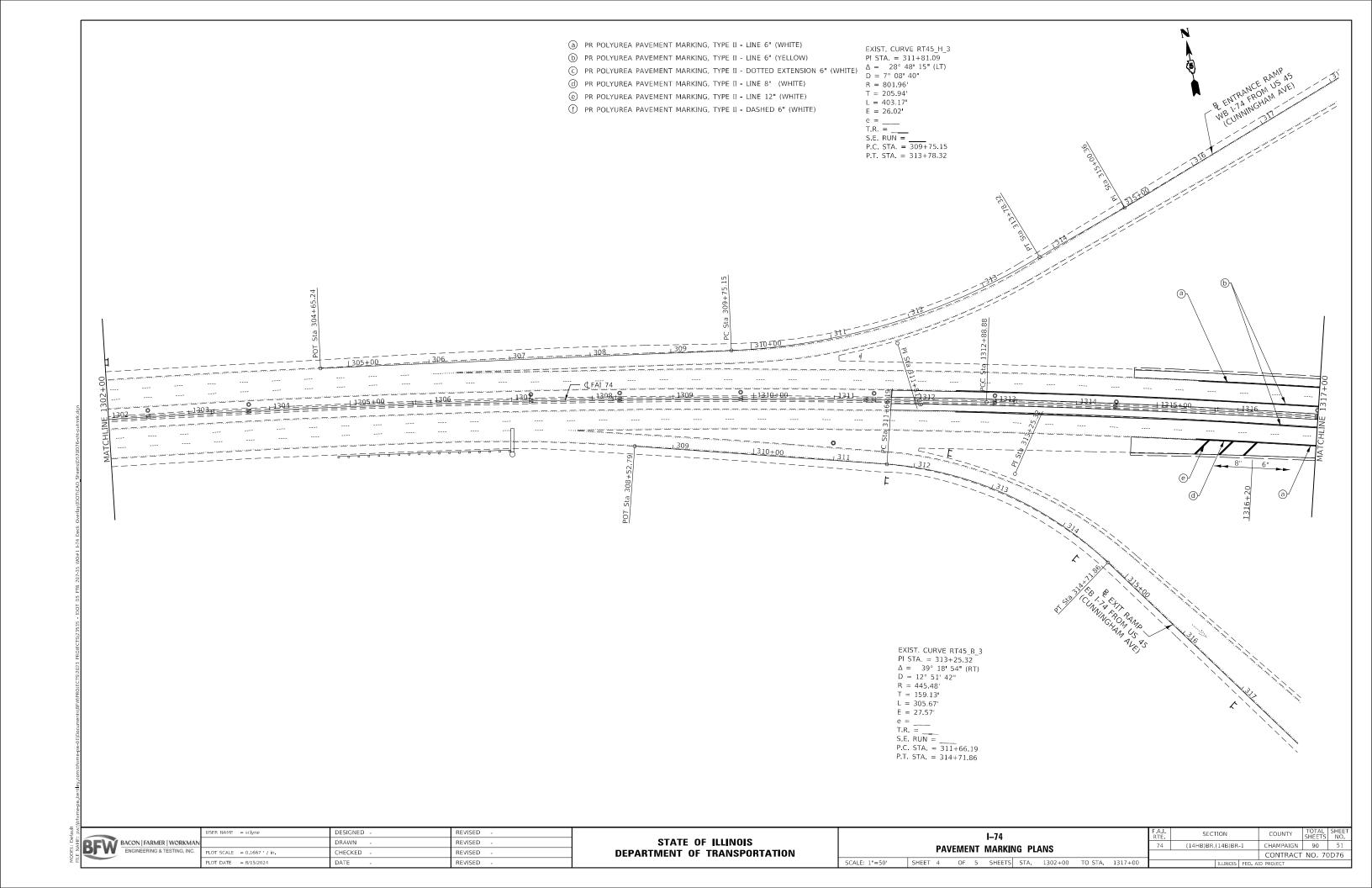
DEPARTMENT OF TRANSPORTATION

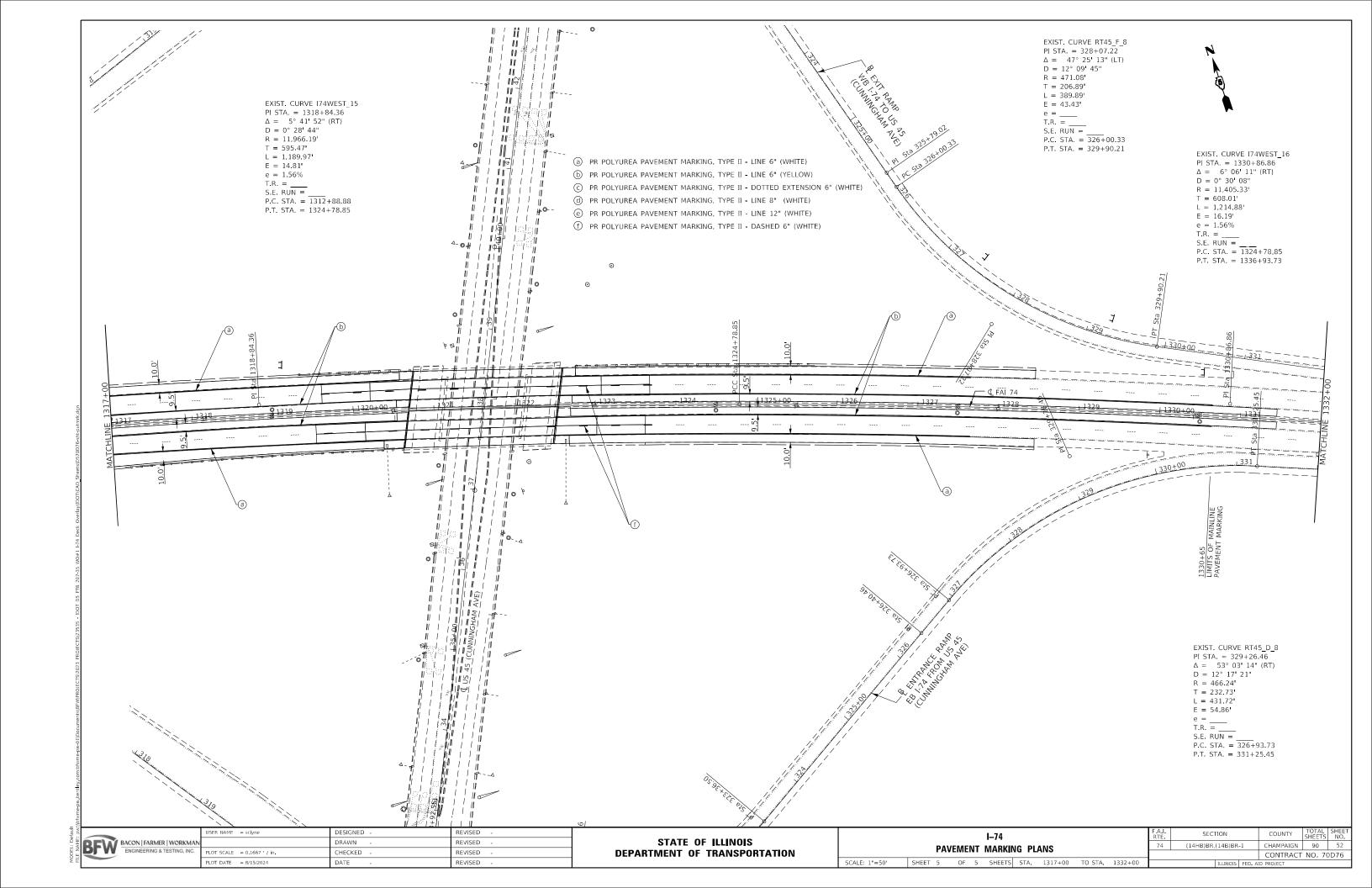
I-74 PAVEMENT MARKING PLANS SCALE: 1"=50' SHEET 2 OF 5 SHEETS STA. 1272+00 TO STA. 1287+00

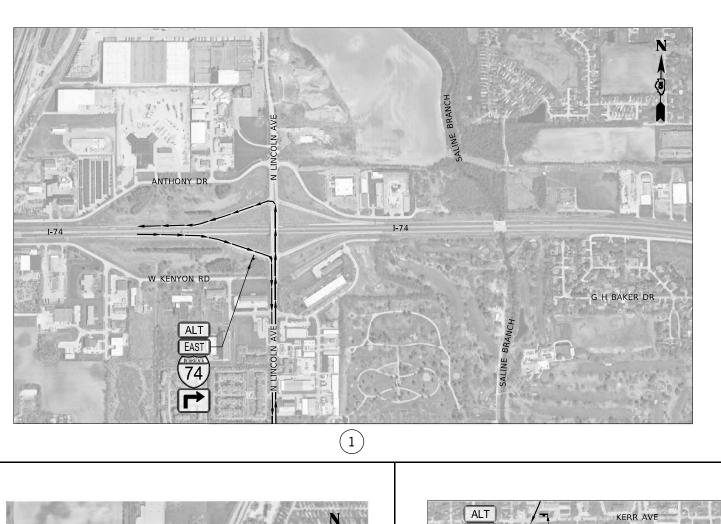
SECTION (14HB)BR,(14B)BR-1 CHAMPAIGN 90 49 CONTRACT NO. 70D76

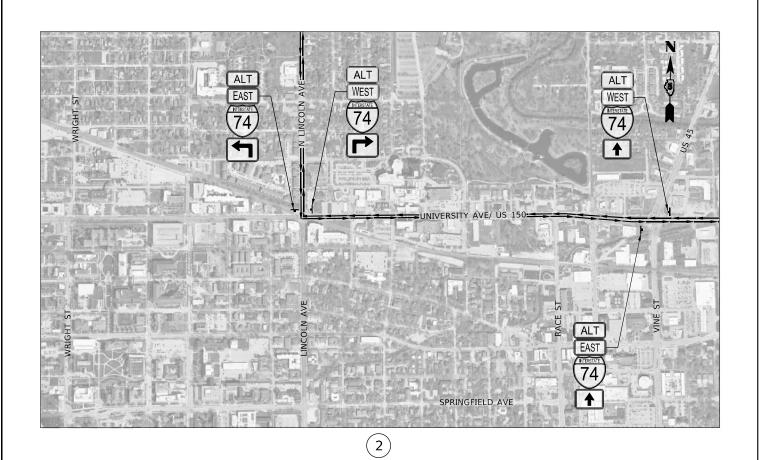
STATE OF ILLINOIS



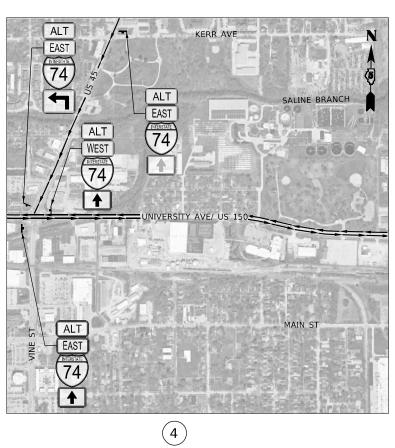














BACON | FARMER | WORKMA ENGINEERING & TESTING, INC.

DESIGNED REVISED DRAWN REVISED LOT SCALE = 100.0000 / in. CHECKED REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-74 ALTERNATE ROUTE SIGNING PLAN SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA.

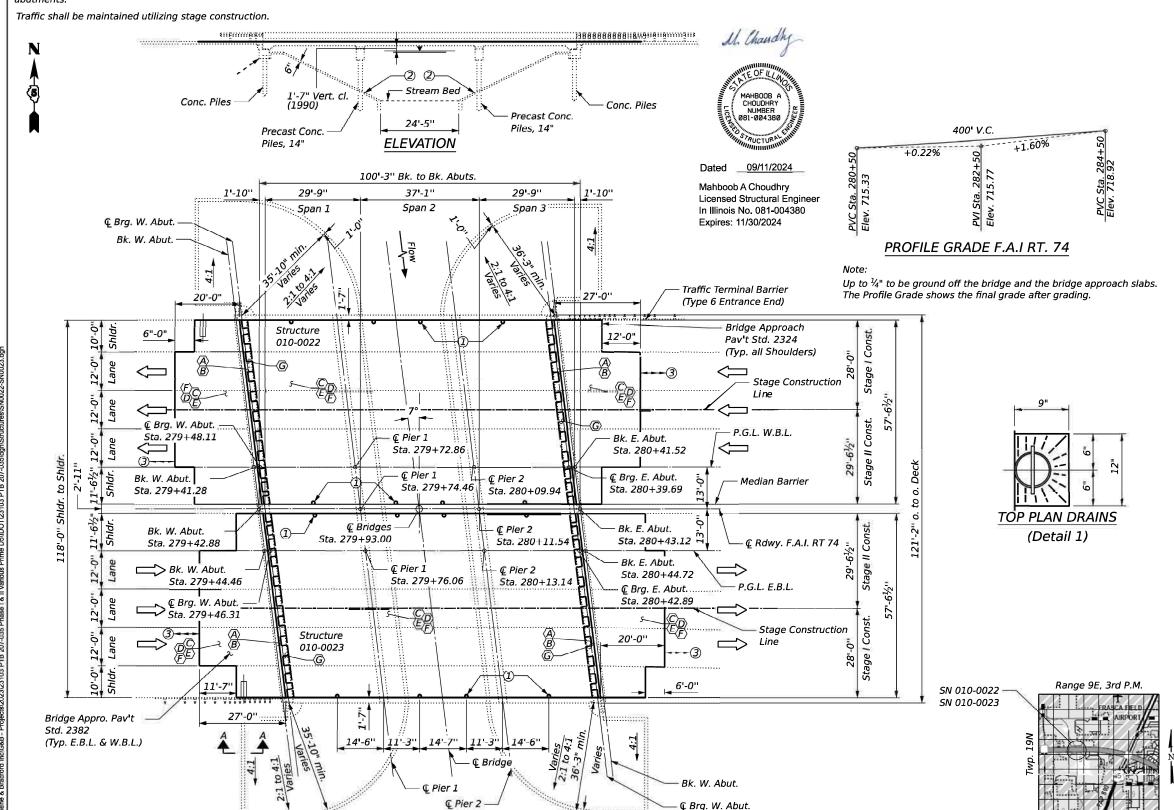
CHAMPAIGN 90 54

CONTRACT NO SECTION (14HB)BR,(14B)BR-1 CONTRACT NO. 70D76

Bench Mark: B.M. # 48 Chiseled SW Corner concrete base of light standard # 13. 60' Rt. Station 273+00.00. Elev. 711.06. The Benchmark is from 1991 plans. Verify with latest survey. Information in the plan, elevations and profile views are also from 1991 plans.

Existing Structure: SN 010-0022 and SN 010-0023 at Sta. 279+93.00 were built in 1956 by the State of Illinois under F.A. Rt. 39, Section 14-B. Original twin structures consisted of dual three-span continuous reinforced concrete slabs over the Saline West Branch. Structures were 100'-3"back-to-back abutments with 36'-4"out-to-out wide deck. Each deck accommodated two 12-ft lanes. In 1979, The decks were waterproofed. In 1991, the bridges underwent major changes. Piers and slope walls were rehabilitated. Additional piles were installed. The decks were removed and replaced with 60'-7"wide decks that allowed three lanes in each direction with median barriers in between. There is an inch gap between the two structures. Original roller bearings on abutments were replaced with expansion elastomeric bearings.

Proposed Work: The current work includes, along with other rehabilitation items, the conversion of stub abutments into integral abutments. Existing elastomeric bearings will remain in place and will be incorporated into the new integral



LEGEND

- 1 6" Floor Drains to remain 8 drains per structure. 16 drains total (See Detail 1)
- (2) 2:1 Slope at Rt. Ls to Abuts.
- 3 HMA RAMP. See Roadway drawings for Details

SCOPE OF WORK:

- Removal of deck ends and hatch blocks.
- Sandblasting and rust removal in bearings at abutments.
- Complete bridge scarification $\frac{3}{4}$ ".
- Place bridge deck latex concrete overlay $2\frac{1}{2}$ ".
- Complete diamond grinding.
- Long. Bridge deck grooving.
- Place concrete superstructure

DESIGN STRESSES

FIELD UNITS

NEW CONSTRUCTION

f'c = 4000 psi (Superstructure)

fy = 60,000 psi (Reinforcement)

fy = 36,000 psi (M270 Grade 36)

EXISTING CONSTRUCTION (1991)

AASHTO 1983 Standard Specifications for Highway Bridges

LOADING: HS 20-44 & Alt. Milt. 25 psf for future surface

 $f'c = 3500 \ psi \ (Superstructure)$

fc = 1400 psi (Substructure)

fy = 60,000 psi (Reinforcement superstructure)

fs = 24,000 psi (Reinforcement substructure)

Structural Steel

fy = 36,000 psi (M183)

EXISTING CONSTRUCTION (1956)

Loading H20-S16-44 (Allowable Stress Design)

fc = 1400 psi (Superstructure)

fc = 800 psi (Substructure)

fs = 20,000 psi (Reinforcement)

fs = 18,000 psi (Structural Steel)

GENERAL PLAN & ELEVATION F.A.I. RT. 74 OVER N. FORK SALINE BRANCH DRAINAGE DITCH SECTION (14HB)BR, (14B)BR-1 STA. 279+93.00 S.N. 010-0022 (W.B.) S.N. 010-0023 (E.B.)

90 54

USER NAME BenjaminBell DESIGNED -MAC REVISED GREENE & BRADFORD, INC. **STATE OF ILLINOIS** CHECKED -KAS REVISED DRAWN REVISED **DEPARTMENT OF TRANSPORTATION** PLOT SCALE ... SKC PLOT DATE 9/11/2024 CHECKED REVISED

PLAN

10

12" x 3'-0"

Toe Wall (Typ.)

COUNTY **GENERAL PLAN AND ELEVATION** (14HB)BR,(14B)BR-1 CHAMPAIGN S. N. 010-0022, S. N. 010-0023 CONTRACT NO. 70D76 SHEET 1 OF 14 SHEETS

LOCATION MAP

INDEX OF SHEETS

Sheet No.	<u>Description</u>
1.	General Plan and Elevation
2.	General Data
3.	Bridge Deck Patching S.N. 010-0022
4.	Bridge Deck Patching S.N. 010-0023
<i>5-6</i> .	Existing and Proposed Typical Cross Sections
7-8.	Stage Construction Details
9.	Temporary Concrete Barrier
10-11.	Removal Details at Abutments
12-13.	Repair Details at Abutments
14.	Bar Splicer Assembly and Mechanical Splicer Detail

GENERAL NOTES

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contactor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Existing bearings shall be sand blasted and cleaned of all rust and existing caps shall be cleaned of all debries prior to the placement of new concrete.

See Special Provision "Deck Slab Repair" for additional requirements pertaining to deck slab repair and hma surface removal (deck).

Longitudinal removal for deck slab repair (full-depth, type II) must be done in alternating sections of no more than 10 feet. Adjacent sections must not be removed until both of the following requirements are met:at least 72 hours shall have elapsed from the end of the previous pour, and the concrete shall have attained a minimum modulus of rupture of 650 psi or a minimum compressive strength of 3500 psi.

Protective Coat shall be applied to all new concrete superstructure and all new concrete overlays as specified in Article 503.19 of the Standard Specifications.

Deck Survey performed on July 23, 2023. Locations and sizes in the plan are approximate.

Synthetic Fibres shall be included in the bridge deck concrete overlay specified. See Special Provisions.

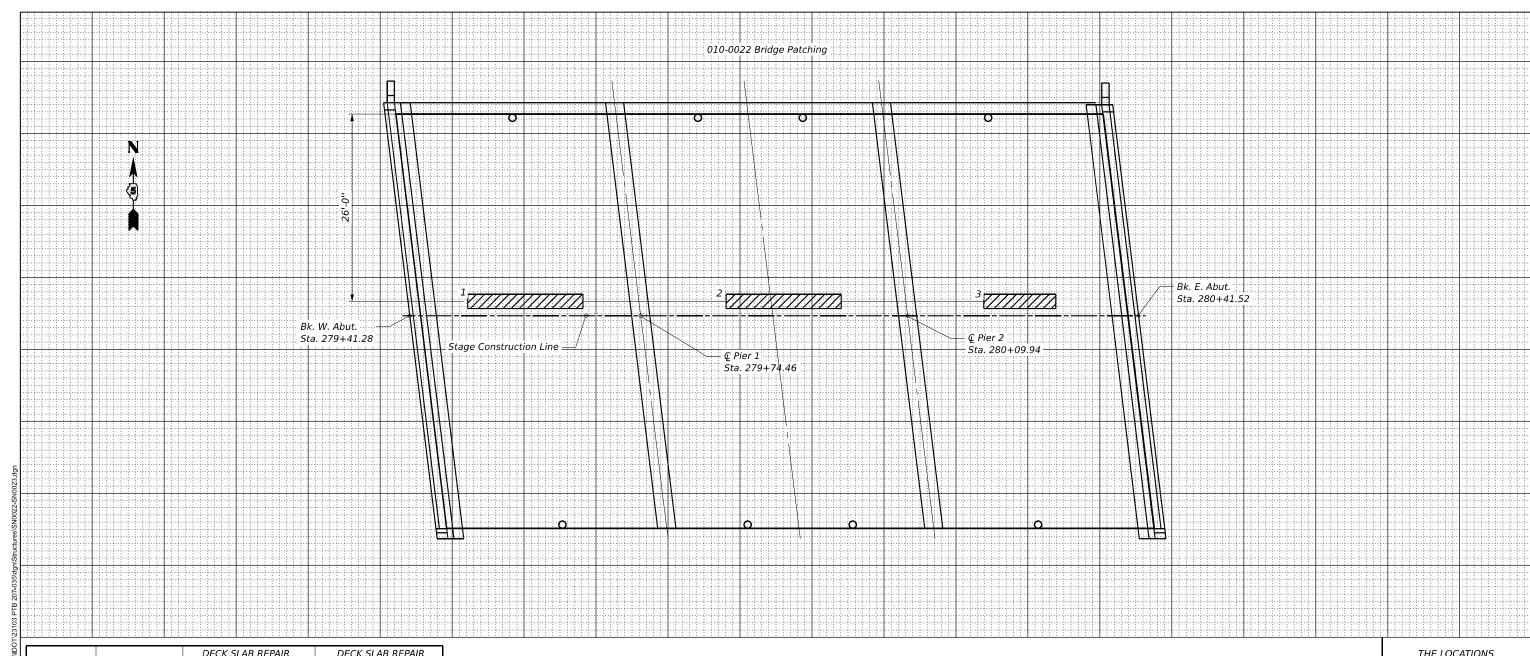
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	33.2	-	33.2
Concrete Superstructure	Cu. Yd.	42.4	-	42.4
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1163	-	1163
Protective Coat	Sq. Yd.	1773	-	1773
Reinforcement Bars, Epoxy Coated	Pound	2440	-	2440
Bar Splicers	Each	24	-	24
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq. Yd.	1773	-	1773
Bridge Deck Scarification 3/4"	Sq. Yd.	1773	-	1773
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	17	-	17
Diamond Grinding (Bridge Section)	Sq. Yd.	1773	-	1773

† Special Provisions

DESIGN SPECIFICATION

2002 AASTHO Standard Specifications for Highway Bridges (17th Edition)



PATCH	SIZE (Ft.)		DECK SLAB REPAIR (FULL DEPTH TYPE I)	DECK SLAB REPAIR (FULL DEPTH TYPE II)
No.	LENGTH WIDTH		SIZE (Sq.Ft.)	SIZE (Sq.Ft.)
1	16	2		32
2	16 2			32
3	10	2		20
TOTAL ROUNDS TO:				84

THE LOCATIONS SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE

W DECK S

DATE OF SURVEY: 7/27/2023 SURVEY BY: METHOD OF SURVEY: VISUAL

> ESTIMATED PAY QUANTITIES

DECK SLAB REPAIR (FULL DEPTH TYPE II) 10 Sq. Yd.

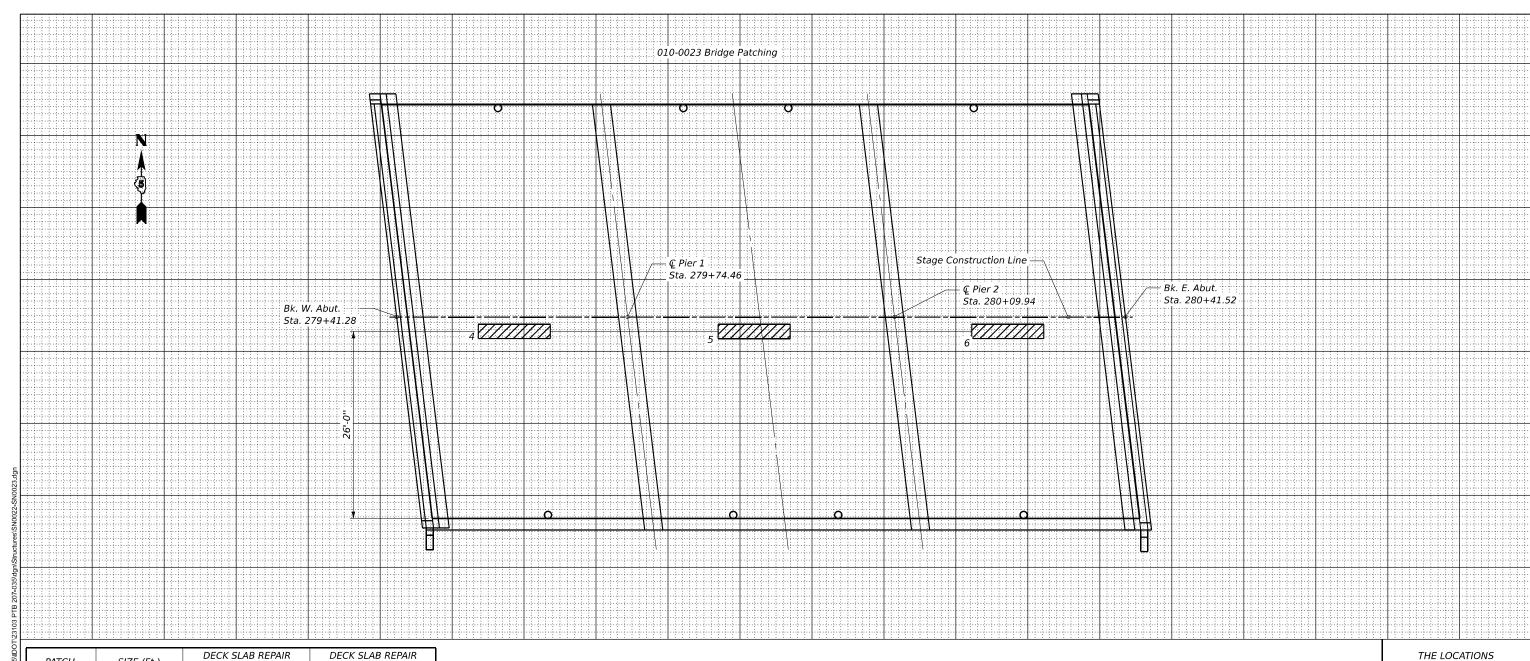
Note:

Areas of Deck repairs shown are estimated. The Engineer Shall show actual locations and size of deck repairs on As-built Plans.

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	SEEME & BRADE	GREENE & BRADFORD, I
	ON THE SENSE OF TH	CONSULTING ENGINEERS 3991 CONSTITUTION DEFICE SPRINGFELD ILLINGS 6271 PROFESSIONAL DESIGN FFM NO. 184-001175 PROFESSIONAL LAND SURVEYING FRM NO. 044-0 (217) 792-8844, 793-8227 (F)

ORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -
ELD		CHECKED - KAS	REVISED -
RIVE 62711 O. 184-001179	PLOT SCALE =	DRAWN - SKC	REVISED -
RM NO. 048-000098	PLOT DATE = 9/8/2024	CHECKED - KAS	REVISED -

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74	(14HB)BR,(14B)BR-1		CHAMPAIGN	90	5	
				CONTRA	CT NO. 7	'0D
	ILLINOIS FED. AID PROJECT					



PATCH	SIZE (Ft.)		DECK SLAB REPAIR (FULL DEPTH TYPE I)	DECK SLAB REPAIR (FULL DEPTH TYPE II)
No.	LENGTH WIDTH		SIZE (Sq.Ft.)	SIZE (Sq.Ft.)
4	10 2			20
5	10 2			20
6 10 2			20	
TOTAL ROUNDS TO:				60

SHOWN GRAPHICALLY **ABOVE** ARE APPROXIMATE

DATE OF SURVEY: 7/27/2023 SURVEY BY: METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES

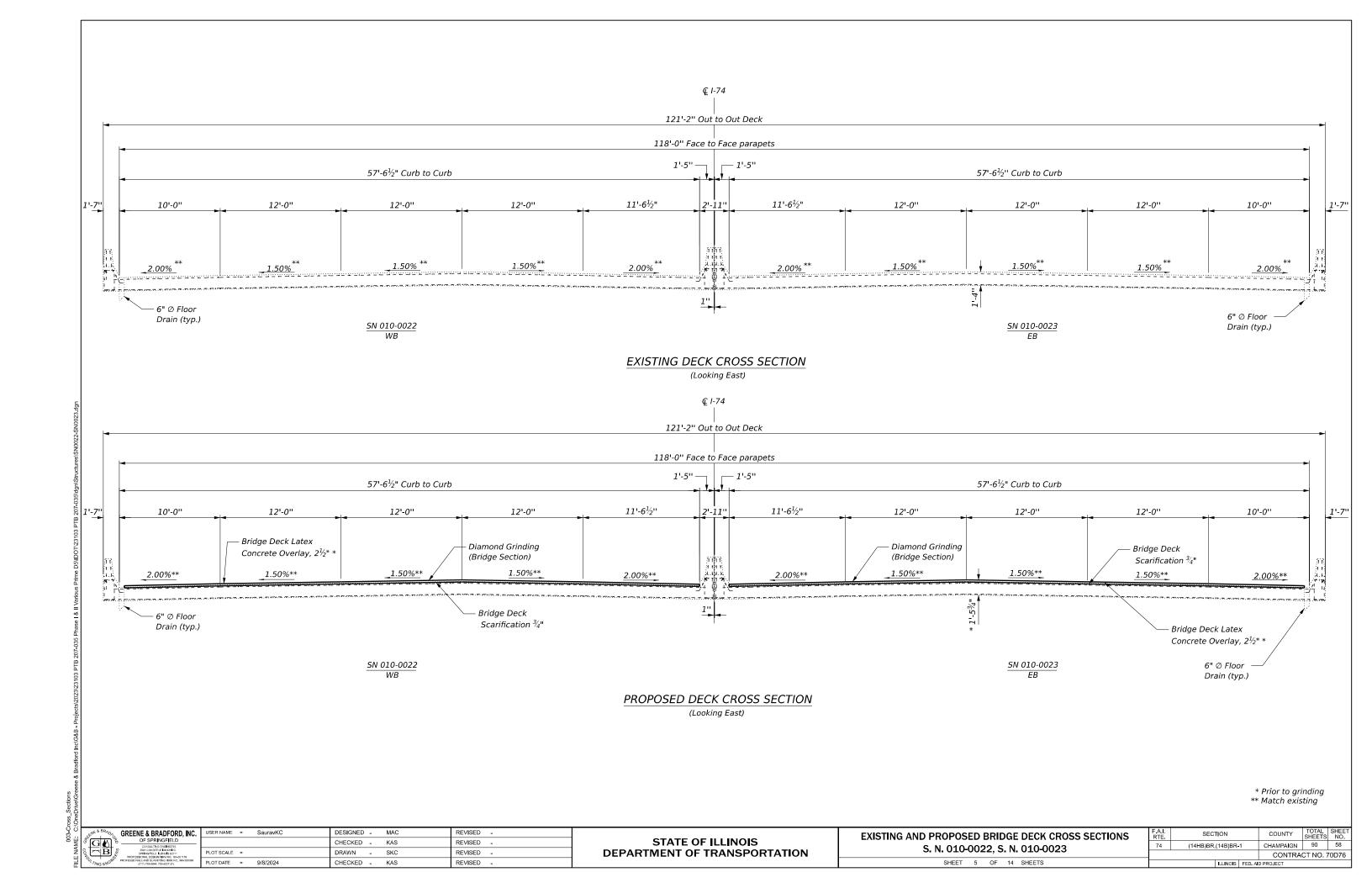
DECK SLAB REPAIR (FULL DEPTH TYPE II) 7 Sq. Yd.

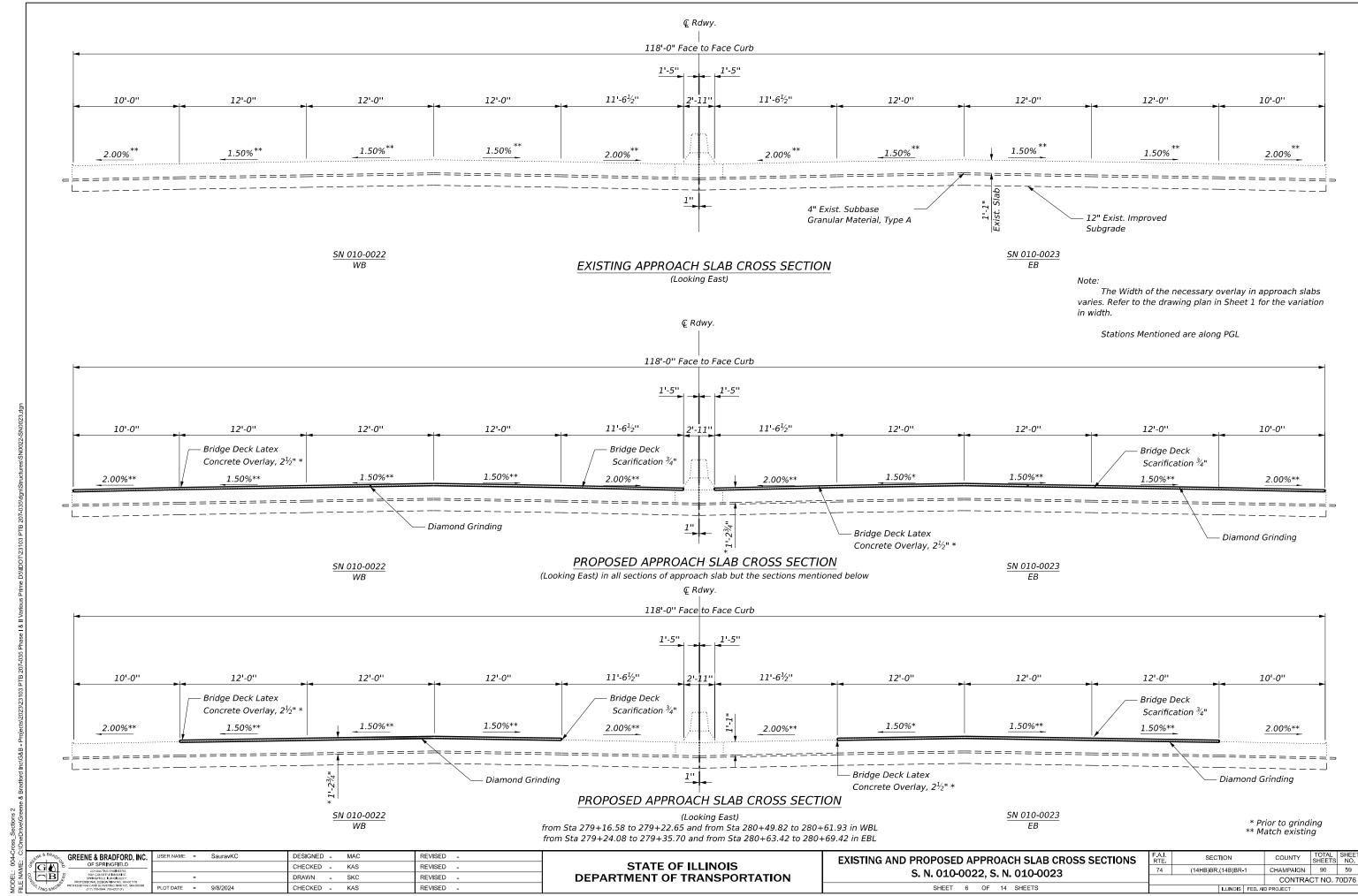
Areas of Deck repairs shown are estimated. The Engineer Shall show actual locations and size of deck repairs on As-built Plans.

GREENE & BRADFORD,
OF SPRINGFIELD

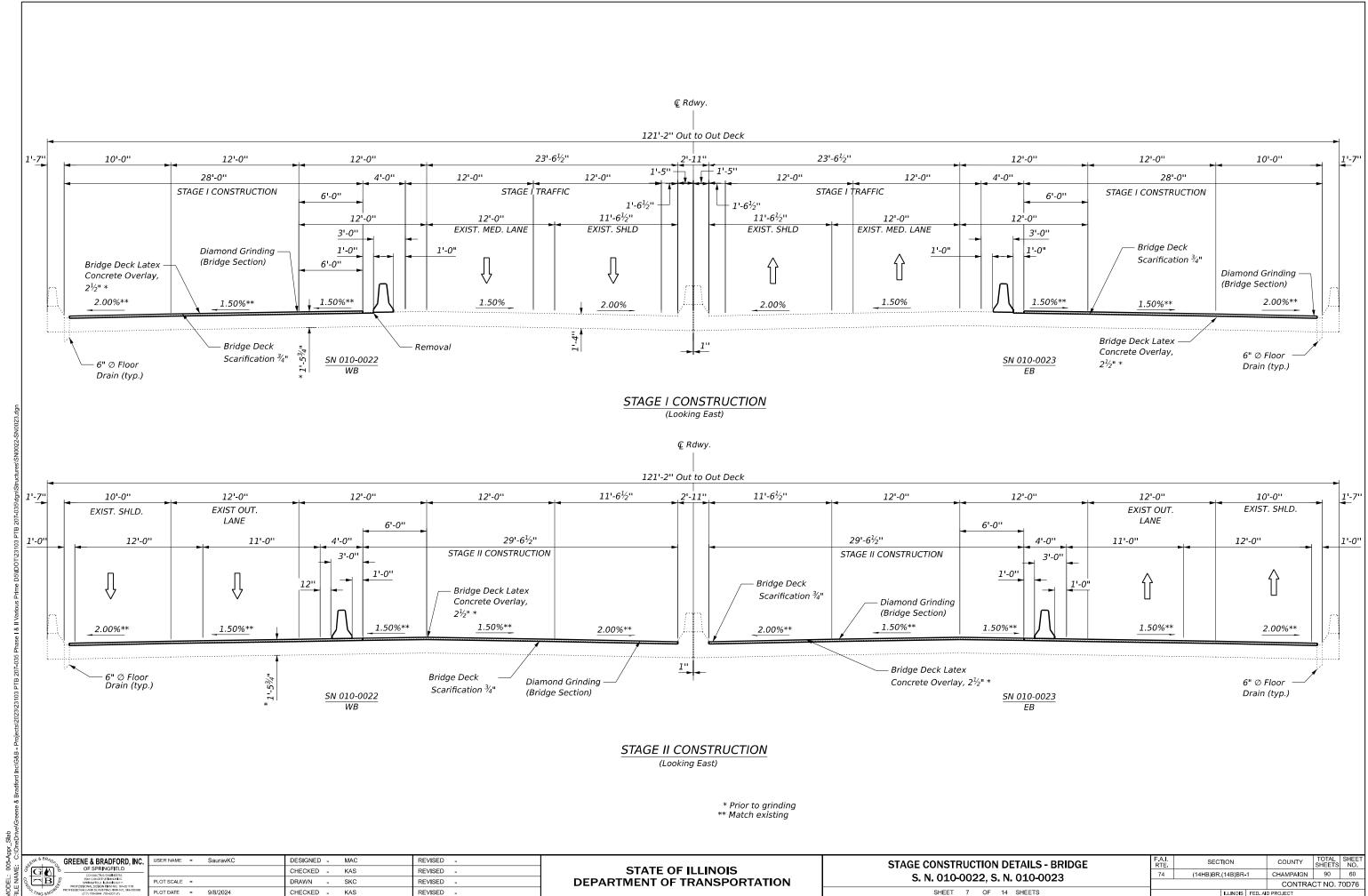
RD, INC.	USER NAME	=	SauravKC	DESIGNED	-	MAC	REVISED	-
				CHECKED	-	KAS	REVISED	-
-001179	PLOT SCALE	-		DRAWN	-	SKC	REVISED	-
. 048-000098	PLOT DATE	=	9/8/2024	CHECKED	-	KAS	REVISED	-

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				CONTRA	CT NO. 7	70D7
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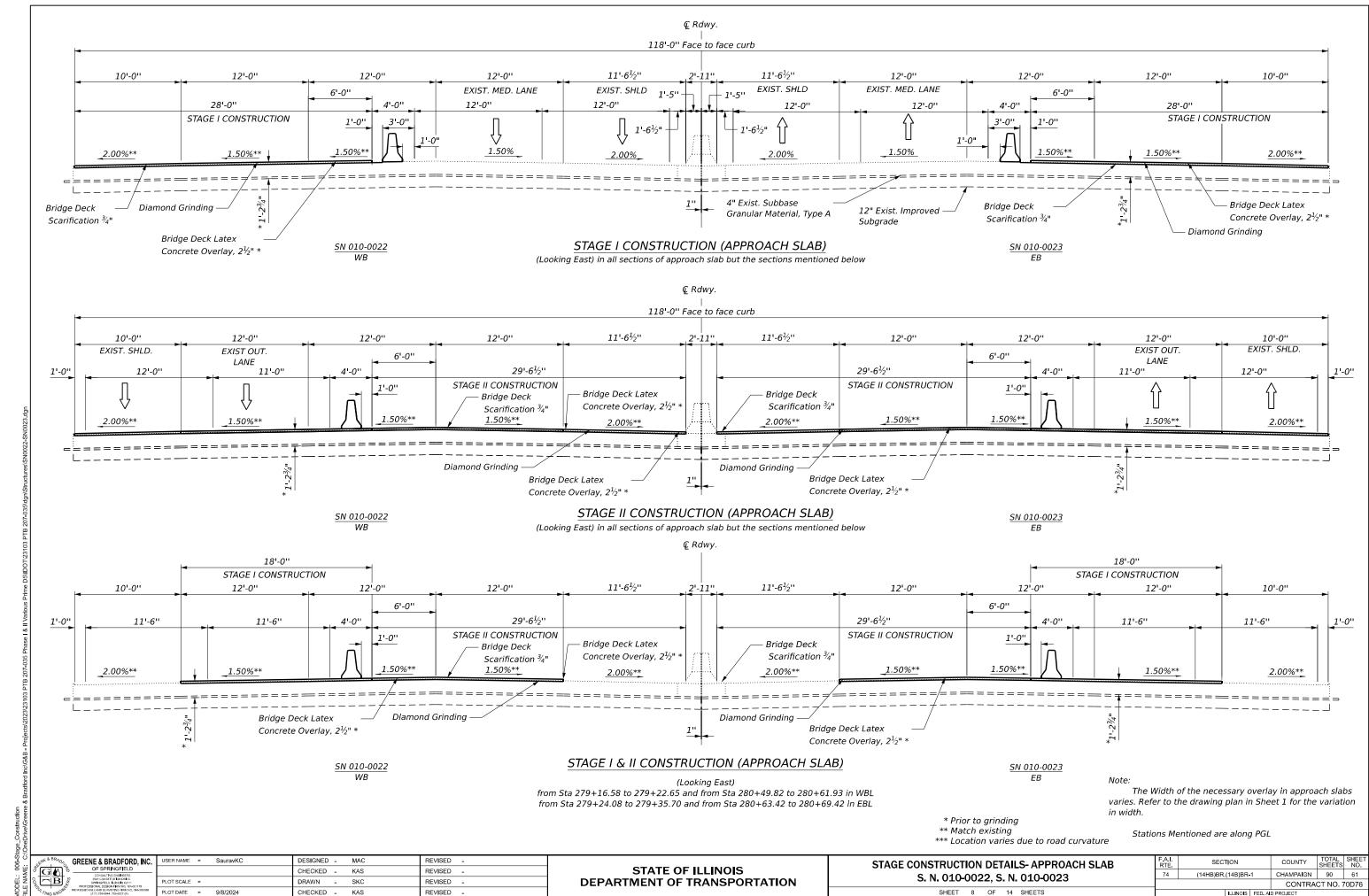




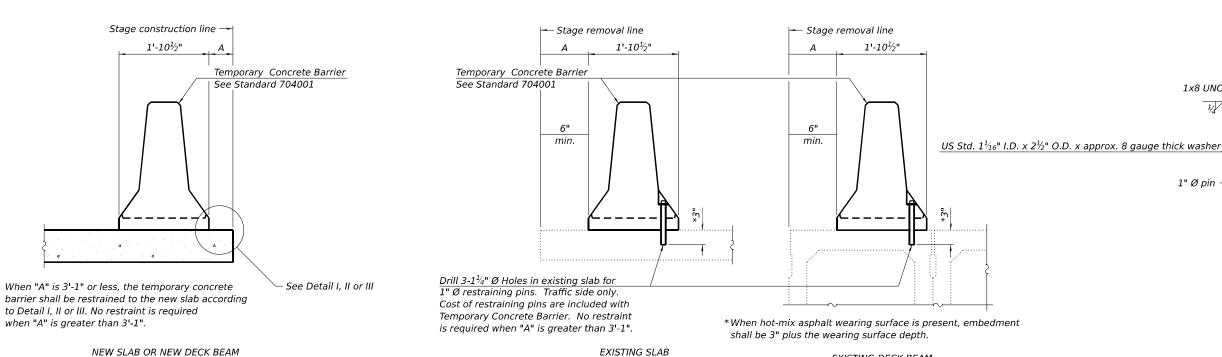
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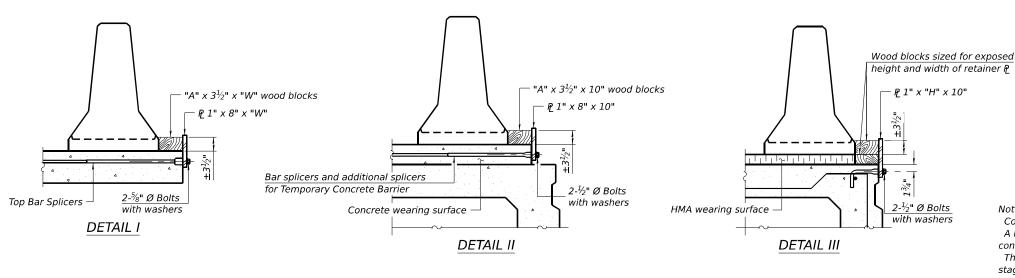


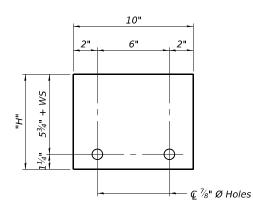
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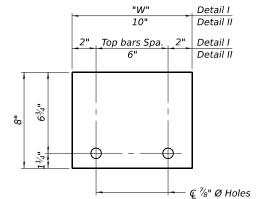


EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM







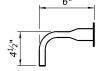
RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 5-15-2023

STEEL RETAINER P 1" x 8" x "W" (Detail I and II)

STEEL RETAINER P 1" x "H" x 10" (Detail III)



BAR SPLICER FOR #4 BAR - DETAIL III

RESTRAINING PIN

₹16" Ø hole

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate © of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

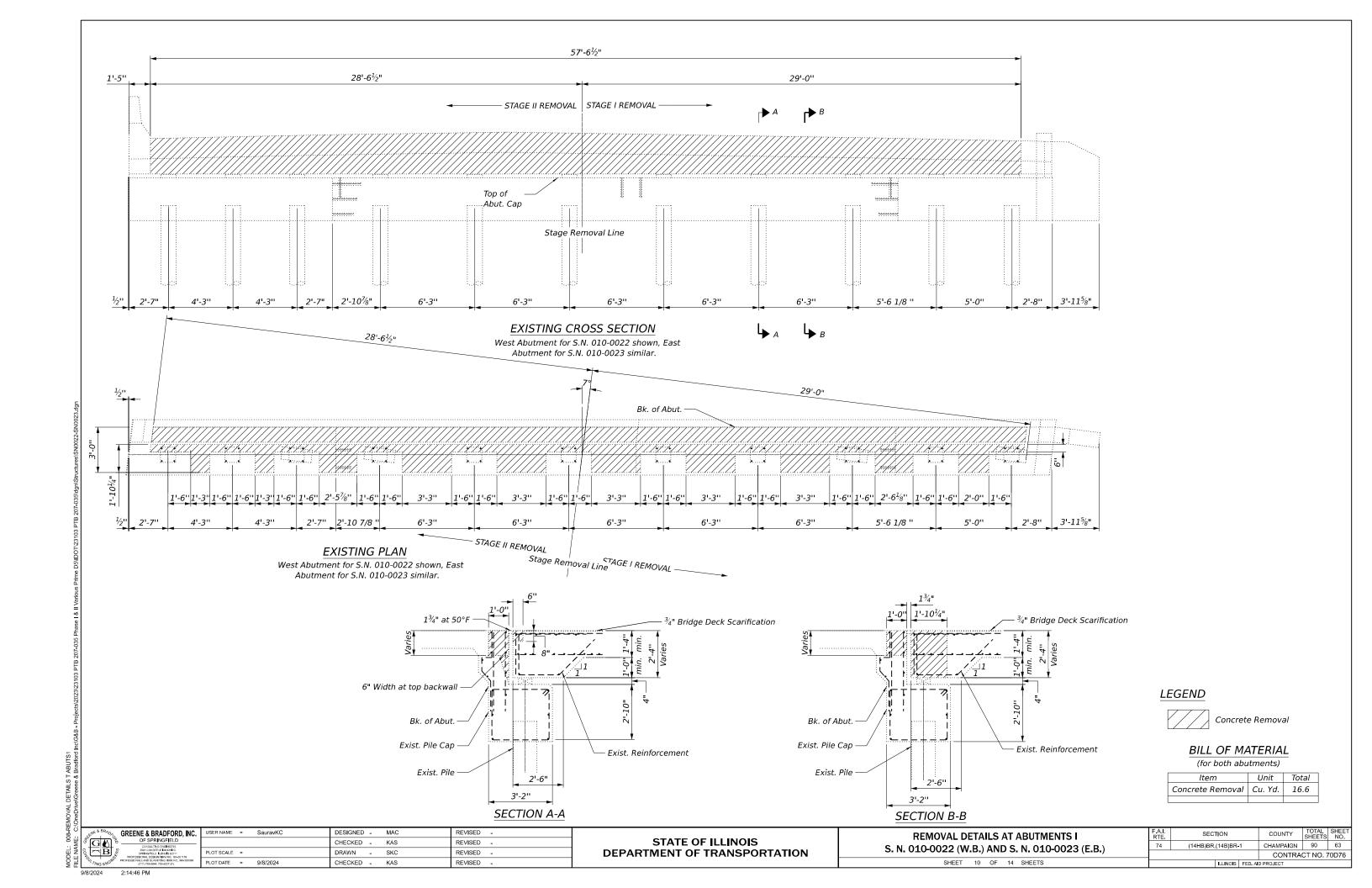
When the 'A' dimension is less than $1\frac{1}{2}$ ", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

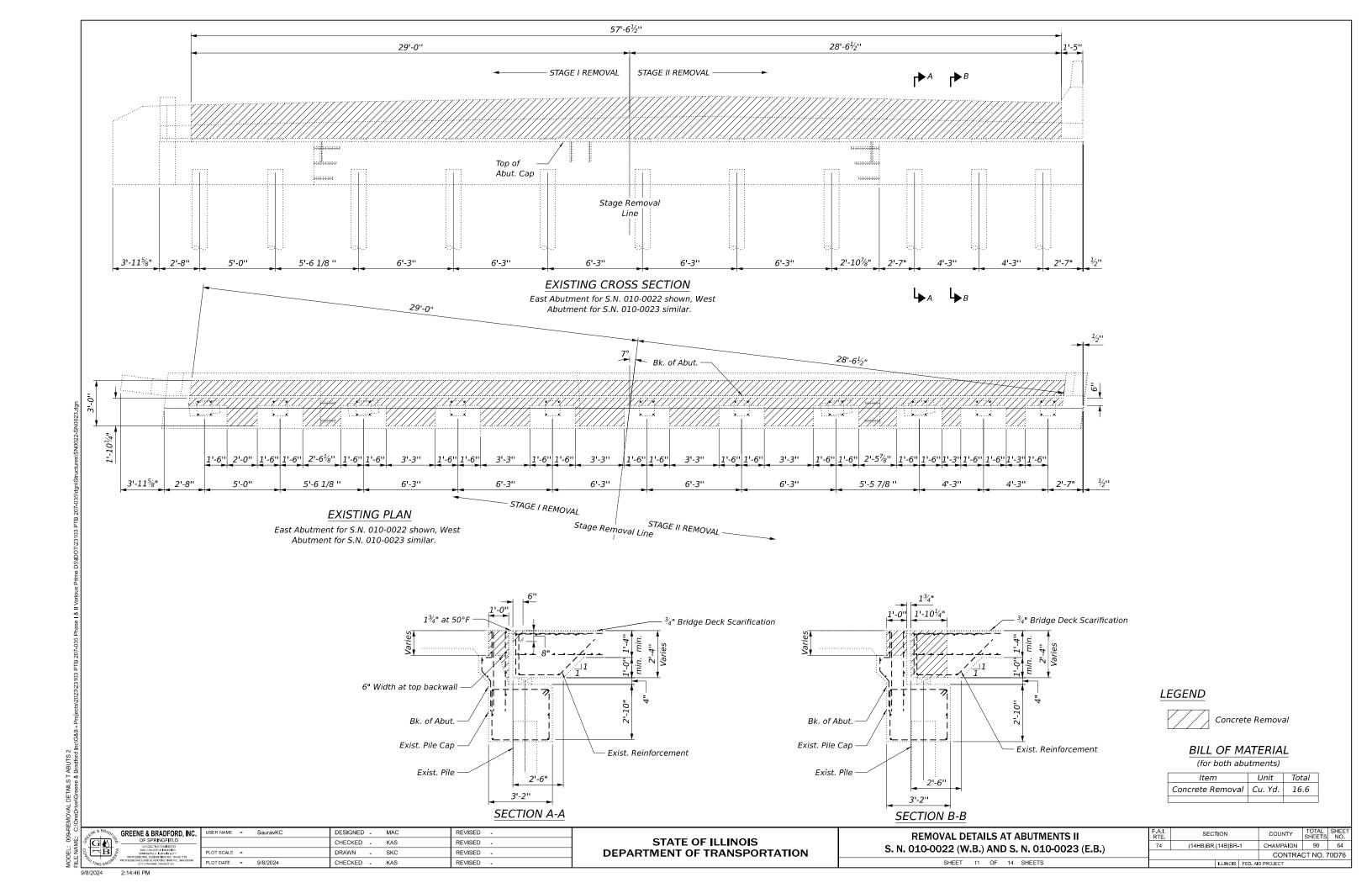
Detail I - Installation for a new bridge deck or bridge slab.

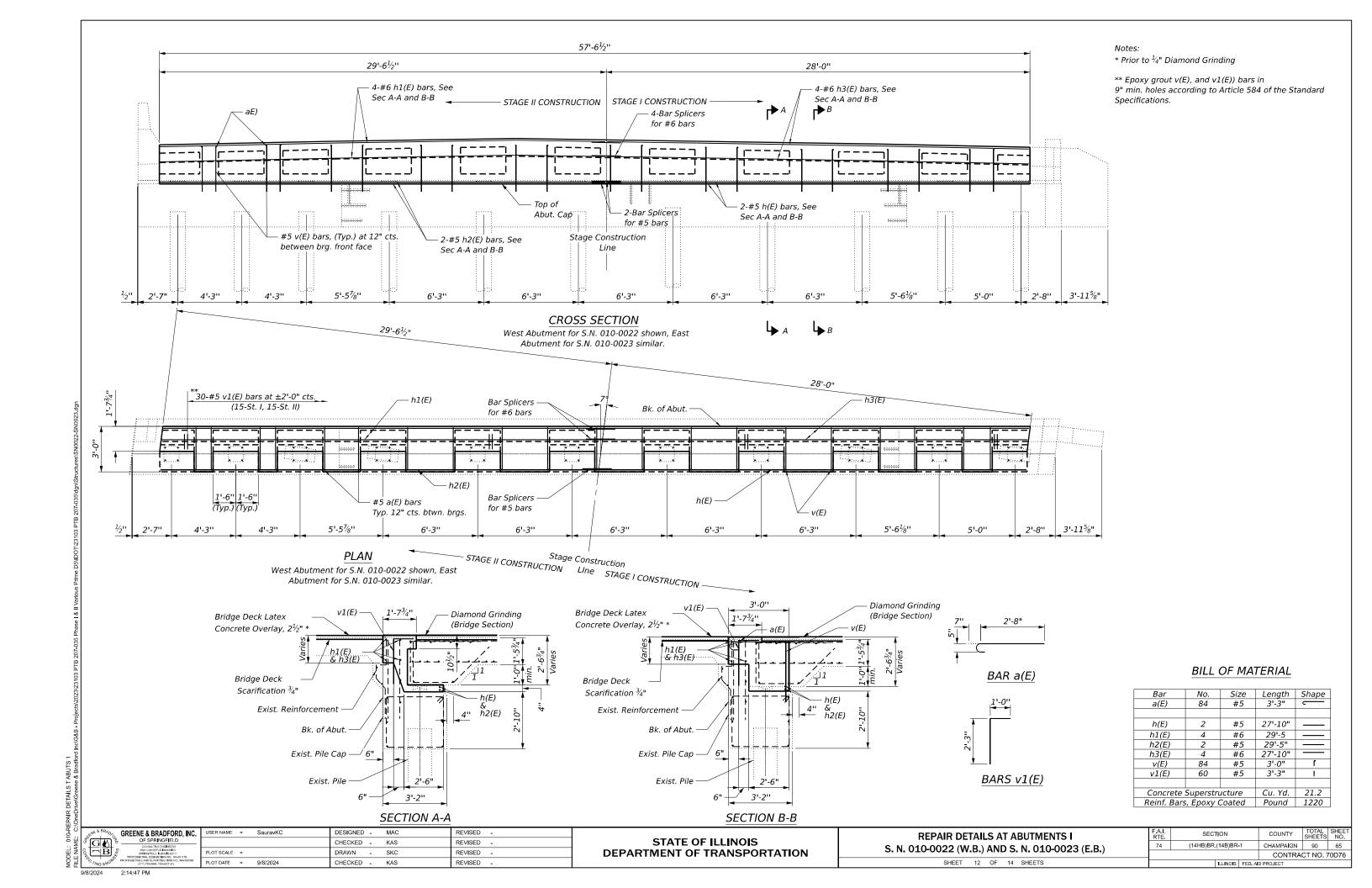
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

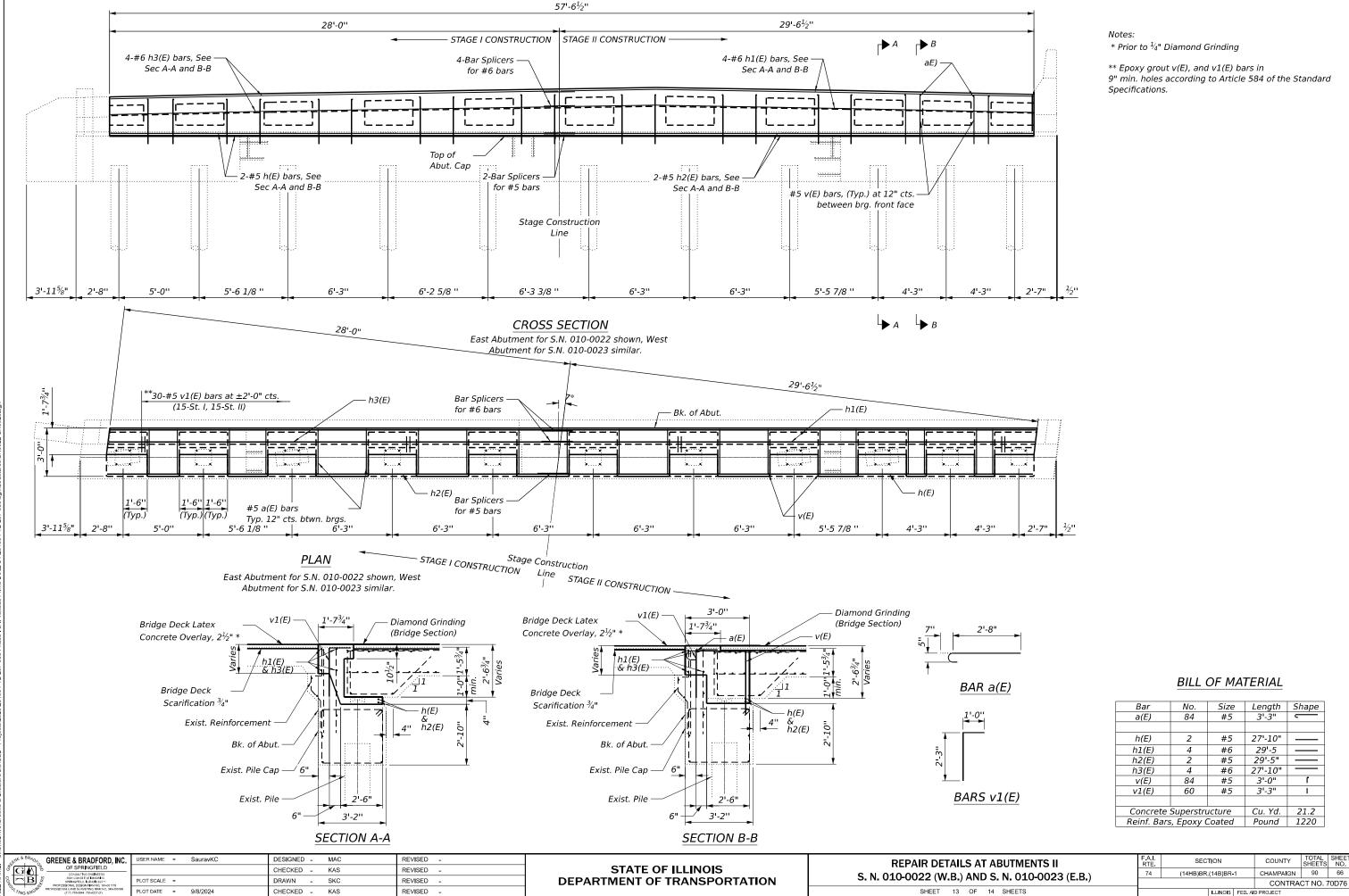
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -		TEMPORARY CONCRETE BARRIER	F.A.I. SECTION	COUNTY TOTAL SHEET
OF SPRINGFIELD CONSULTING ENGINEERS		CHECKED - KAS	REVISED -	STATE OF ILLINOIS		74 (14HB)BR,(14B)BR-1	CHAMPAIGN 90 62
9901 CONSTITUTION DRIVE	PLOT SCALE =	DRAWN - SKC	REVISED -	DEPARTMENT OF TRANSPORTATION	S. N. 010-0022, S. N. 010-0023		CONTRACT NO. 70D76
7/NG ENGIN PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098 (217) 793-8844, 793-8227 (F)	PLOT DATE = 9/8/2024	CHECKED - KAS	REVISED -		SHEET 9 OF 14 SHEETS	ILLINOIS FED.	AID PROJECT









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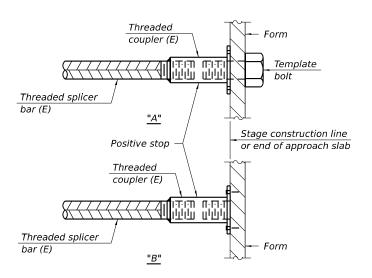
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + $1\frac{1}{2}$ " + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar	No. assemblies	Minimum
Location	size	required	lap length
Hatch Block	#6	16	4'-0"
Slab	#5	8	3'-6"

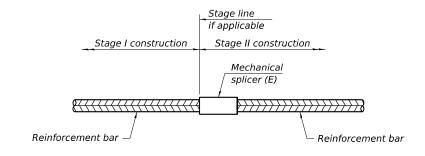


INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
"B": Set bar splicer assembly by nailing to wood forms or

(E) : Indicates epoxy coating.

cementing to steel forms.



STANDARD MECHANICAL SPLICER

Location	Bar	No. assemblies
20cation	size	required

Notes

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

GREENE & BRADFORD, INC.

OF SPRINGFIELD

OF SP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
S. N. 010-0022, S. N. 010-0023

SHEET 14 OF 14 SHEETS

AI. SECTION COUNTY TOTAL SHEETS NO.

74 (14HB)BR,(14B)BR-1 CHAMPAIGN 90 67

CONTRACT NO. 70D76

B.M. #58 Chiseled in W. Abutment I320+60-401 Rt. Elev. = 743.81. The Benchmark is from the 1992 plans. Verify with the latest survey. Information in the plan, elevation and profile views are also from the 1992 plans. INDEX OF SHEETS Sheet No. Description The bridge was originally built in 1956 under F.A. RT. 39, Section 14-HB-F-4 at Sta. 321 + 64.64 with four continuous spans, 41'-9", 49'-9", 49'-9", and 41'-9". The superstructure comprised 30" wide flange steel beams and a 1 General Plan and Elevation reinforced concrete deck. Originally, the bridge carried two 12-foot lanes in each direction separated by a four-foot wide median. In addition, there were acceleration and deceleration lanes on each side. The structure has pile-bent 2. General Data abutments and triple hammerhead piers with footings on piles. The abutments' back-to-back length of the bridge is 186'-9". Original out-out deck width was 88'-0". In 1992, major rehabilitation/reconstruction was carried out. All Deck Patching Sheet 3. abutments, piers, and slopewalls were repaired or reconstructed. To provide 14.5 ft clearance for Rt. 45 traffic below, the concrete deck was removed and rebuilt at a higher level. The reconstructed deck provides two 12-ft lanes in Structural Repair of Concrete each direction separated by a 26-ft wide paved median with a concrete barrier in the middle. Ten-foot-wide outer shoulders are also provided on the exterior sides. The total out-to-out deck was thus increased to 97'-2". There was no 5-6. Existing and Proposed Cross Sections change in number of spans. In 1979, a waterproofing system and hma overlay was applied to the deck. In 2004, six beams damaged by impact were partially replaced. In 2012, three different impact-damaged beams were 7-8. Stage Construction Details Temporary Concrete Barrier 9. 10. Framing Plan Work shall be completed with stage construction. Diapharm Details 11. Beam Repair Details 12. 13. Abutment Bearing Replacement Details 14-15. Abutment Joint Replacement Details Preformed Joint Strip Seal Details 16. Bar Splicer Assembly and Mechanical Splicer Details 17. € US 45 **Q** US 45 SCOPE OF WORK: M. Chaudhy Complete bridge scarification $\frac{3}{4}$ ". Exist Complete deck patching, removal of deck ends and hatch blocks. Concrete or Conc. Piles Creosoted CHOUDHR Complete steel beam repairs and end diaphragm replacements. Southbound Northbound Shldr. Median Piles (typ.) 4" Conc. Replace bearings at abutments. Slopewall (Typ.) **ELEVATION** Œ. Cast deck ends and hatch blocks. 09/11/2024 Dated (LOOKING NORTH) Place preformed joint strip seal. Mahboob A Choudhry Licensed Structural Engineer Place bridge deck latex concrete overlay $2\frac{1}{2}$ ". In Illinois No. 081-004380 186'-9"Bk. to Bk. of Abut. Expires: 11/30/2024 Complete diamond grinding and bridge deck grooving. 41'-9" 49'-9" 49'-9" 41'-9" Span No. 2 Span No. 3 Span No. 4 Span No. 1 Stone Riprap, Class A3 and Filter Fabric: Southwest Abutment Corner 1 at top of slopewall near weep hole drain. - End of Deck End of Deck-1'-101/2" 1'-101/2' Traffic Barrier Terminal SN 010-0024 Type 5(Exit End)(Typ.) Traffic Barrier Terminal Range 9E, 3rd P.M. 54**'**-0" 48'-0" Bridge Approach Shldr. Type 6(Entrance End)(Typ.) Pavt. Std. 2324-6 Ç E. Abut. (Typ. all shidrs.) 17'-7 € Brg. P-3 16'-0" Bk. W. Abut. € Brg. P-2 € Brg. P-1 Sta. 322+13.66 Sta. 321+63.96 Sta. 320+70.69 Sta. 321+14.26 € P-3 ÆH) C Brg. W. Abut. Sta. 322+11.87 Sta. 320+72.56 C P-1 Bk. E. Abut. HMA RAMP Sta. 321+12.37 4 - Stage Construction Sta. 322+57.23 Line (typ.) LOCATION SKETCH € P-2 € Brg. E. Abut. P.G.L. Median Sta. 321+62.12 Sta. 322+55.36 W.B.L Parapet **TÖP PLAN DRAINS** TABLE OF DIMENSIONS (Detail 1) L Rdwy. F.A.I. Rt. 74 Stage € F.A.I. Rt. 74 Sta. C Brg. W. Abut. € Brg. E. Abut. D Construction 321+62.12 Sta. 320+70.62 Sta. 322+53.62 Bk. OF W.A. 10'-21/4" 11'-41/4" 11'-8¾" 9'-9¾" Line = C U.S. 45 Sta. P.G.L. € P-2 9'-9¾' 11'-8¾" 11'-4¹/₄" 10'-21/4" & Brg. W. Abut. HMA RAMP (typ.) 38+01.80 E.B.L Sta. 320+68.68 Bk. OF E.A. 10'-21/4" 11 -4¼" 11'-8¾" 9'-9¾" C Brg. P-3 € Brg. E. Abut. € Brg. P-1 Sta. 322+10.08 Sta. 321+10.47 Sta. 322+51.88 Bk. W. Abut. € Brg. P-2 Sta. 320+66.80 Bk. E. Abut. Bridge Approach Pavt. Std. Sta. 321+60.28 Sta. 322+53.75 2360-4 (Typ. E.B.I. & W.B.L.) GENERAL PLAN & ELEVATION HMA RAMP (typ.) HMA RAMP (typ.) F.A.I. RTE. 74 OVER U.S. RTE. 45 16'-0" 4" SECTION (14HB)BR Traffic Barrier Terminal 48**'**-0" 15'-0" |13'-6" 54'-0" **LEGEND** STA. 321+62.12 Type 6(Entrance End)(Typ.) Bk. W. Abut. 1 6" Ø Floor Drains (Typ. at each parapet, S.N. 010-0024 (I)-Bk. E. Abut. spans 1 and 4 only, See Detail 1) PLAN € E. Abut. (2) See Roadway drawings for HMA Ramp Details DESIGNED -REVISED USER NAME BenjaminBell MAC GREENE & BRADFORD, INC. SECTION COUNTY **GENERAL PLAN AND ELEVATION STATE OF ILLINOIS** CHECKED -KAS REVISED (14HB) BR, (14B) BR-1 CHAMPAIGN 90 68 **STRUCTURE NO. 010-0024** DRAWN SKC REVISED **DEPARTMENT OF TRANSPORTATION** PLOT SCALE ... CONTRACT NO. 70D76 SHEET 1 OF 17 SHEETS

PLOT DATE

9/11/2024

CHECKED

KAS

REVISED

Fasteners shall be ASTM F 3125 Grade A325 Type 1, Fasteners shall be hot dip galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel." Bolts 3/4"ø, holes 13/16"ø, unless otherwise noted.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars designated (E) shall be epoxy coated

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC - SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding $\frac{1}{4}$ in. deep shall be identified and reported to the Bureau of Bridges & Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contactor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Protective Coat shall be applied to all new concrete superstructure and all new concrete overlays as specified in Article 503.19 of the Standard Specifications.

All new structural steel and bearing assemblies shall be hot-dipped galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".

All structural steel shall conform to AASHTO Classification M-270 Grade 36, unless otherwise noted

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Structural Steel Repair.

Synthetic Fibers shall be included in the bridge deck concrete overlay specified. See special provisions.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specification when the deck is poured at an ambient temperature other than 50°F.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures", and the Standard Specifications. The color of the final finish coat shall be Interstate Green, Munsell No. 7.5G 4/8. Cost included with Structural Steel Repair.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Remove and reinstall guardrail connections, if necessary to complete overlay.

Deck Survey performed on July 23, 2023. Locations and sizes in the plan are approximate.

Contractor shall protect/preserve existing utilities.

LIST OF ABBREVIATION

Typ. Typical Exist. Existing

F.A. Federal Ald

Sta. Station

RT. Route

ft Feet

yd. Yard

Elev. Elevation

B.M. Bench Mark P.G.L.Profile Ground Level

Brg. Bearing

E.B.L. East Bound Lane

West Bound Lane Center Line

Plate

Abut. Abutment

E. East W. West

Bk. Back

F.A.I. Federal Aid Ilinois

Pvm'tPavement FI. Flevation

Dia Diameter

Sec Section

Rdwy. Roadway

Conc. Concrete

I.D. Inside Diameter O.D. Outside Diameter

e.a. Each

Alt. Alternate

Αt

PTFE Polytetrafluoroethylene

Hex. Hexagonal

Reg'dReguired

Min. Minimum Vert. Vertical

Reinf.Reinforcement

Long, Longitudinal

cts. Centers

Specs.Specification

PVC Point of Vertical Curve Point of Vertical Intersection

PVI I.F. Inside Face

DESIGN STRESSES

FIELD UNITS

LOADING HL-93

NEW CONSTRUCTION

f'c = 4,000 psi (Superstructure)

fy = 60,000 psi (Reinforcement)

 $f_V = 36,000 \text{ psi (Structural Steel M270)}$

O.F. Outside Face

N.S. Near Side

F.S. Far Side

CURVE DATA (C 1-74) P.I Sta. = 318 + 84.89

△ = 5°40'55"

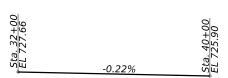
T = 595.49R = 12,000.00

L = 1,190.00

 $D = 0^{\circ}28'39''$

E = 14.77

SE = 0.0156 ft/ft



PROFILE GRADE U.S. 45

Loading H20-S16-44

fc = 800 psi (Substructure)

AASHTO 1983 Standard Specifications for Highway Bridges LOADING: HS 20-44 & Alternate

25 psf for future surface

SECTION A-A

(through sidewall)

f'c = 3500 psi (Superstructure)

fc = 1400 psi (Substructure)

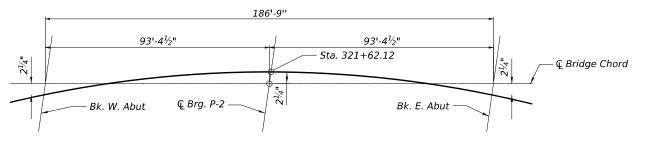
fy = 60,000 psi (Reinforcement)

fs= 18,000 psi (Existing Steel)

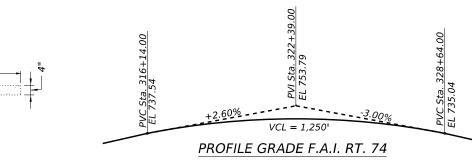
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	22.3	-	22.3
Concrete Superstructure	Cu. Yd.	22.3	-	22.3
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	1531	-	1531
Protective Coat	Sq. Yd.	2614	-	2614
Protective Shield	Sq. Yd.	1047	-	1047
Reinforcement Bars, Epoxy Coated	Pound	3040	-	3040
Bar Splicers	Each	100	-	100
Preformed Joint Strip Seal	Foot	188	-	188
Elastomeric Bearing Assembly, Type II	Each	32	-	32
Jack and Remove Existing Bearings	Each	32	-	32
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq. Yd.	2595	-	2595
Bridge Deck Scarification 3/4"	Sq. Yd.	2595	-	2595
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	49	-	49
Diamond Grinding (Bridge Section)	Sq. Yd.	2595	-	2595
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	32	-	32
Stone Riprap, Class A3	Sq. Yd.	3	-	3
Filter Fabric	Sq. Yd.	3	-	3
Furnishing and Erecting Structural Steel	Pound	27000	-	27000
Structural Steel Removal	Pound	17100	-	17100
Anchor Bolts, 1"	Each	64	-	64

[†] Special Provisions



LAYOUT PLAN



Note: Up to $\frac{1}{4}$ " may be ground off the bridge deck and the bridge approach slabs.

DESIGN SPECIFICATION

2002 AASTHO Standard Specifications for Highway Bridges (17th Edition)

EXISTING CONSTRUCTION (1956) EXISTING CONSTRUCTION (1992)

fc = 1400 psi (Superstructure)

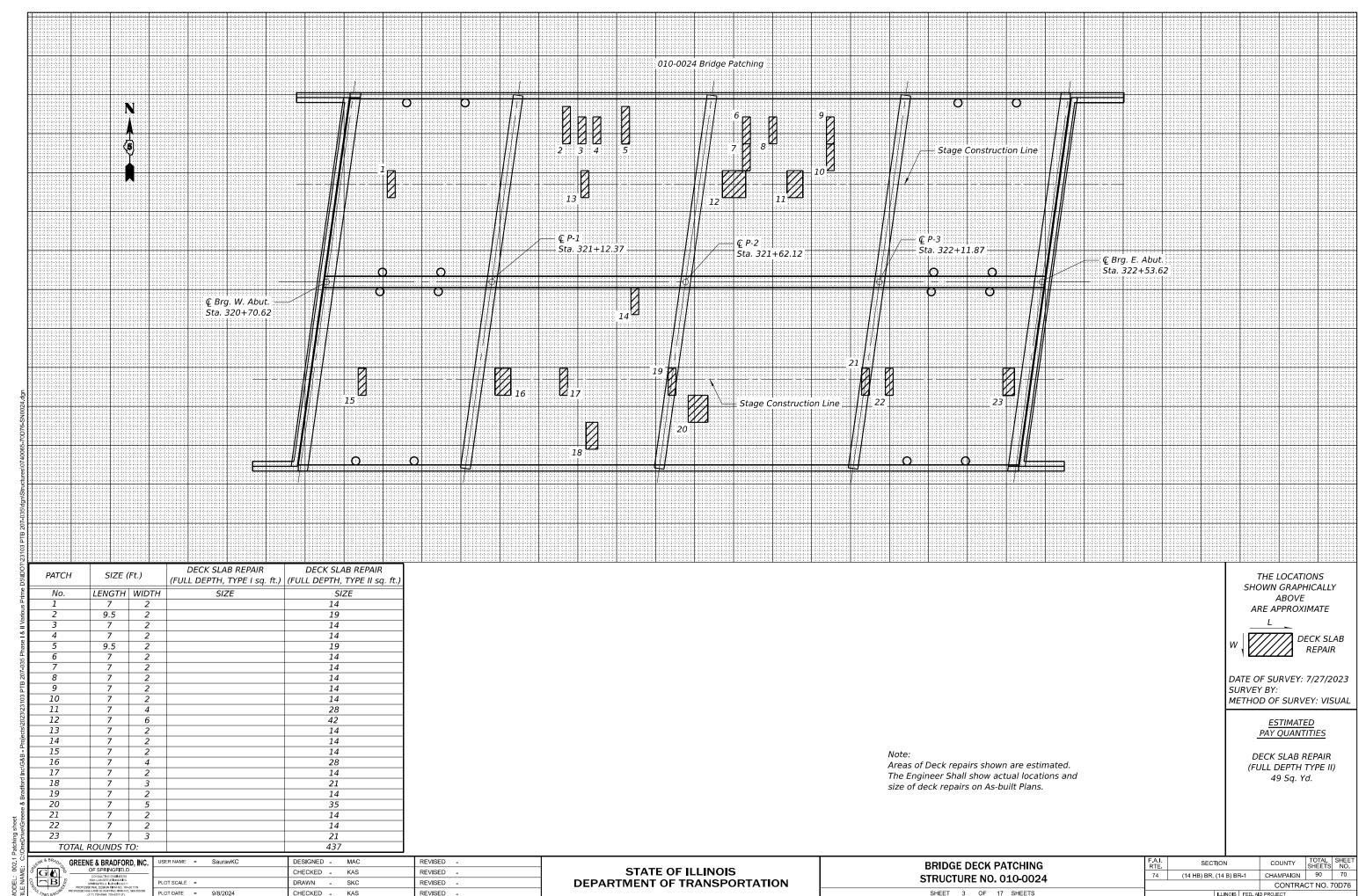
fs = 20,000 psi (Reinforcement)fs = 18,000 psi (Structural Steel)

> Structural Steel fs= 20,000 psi (New Steel)

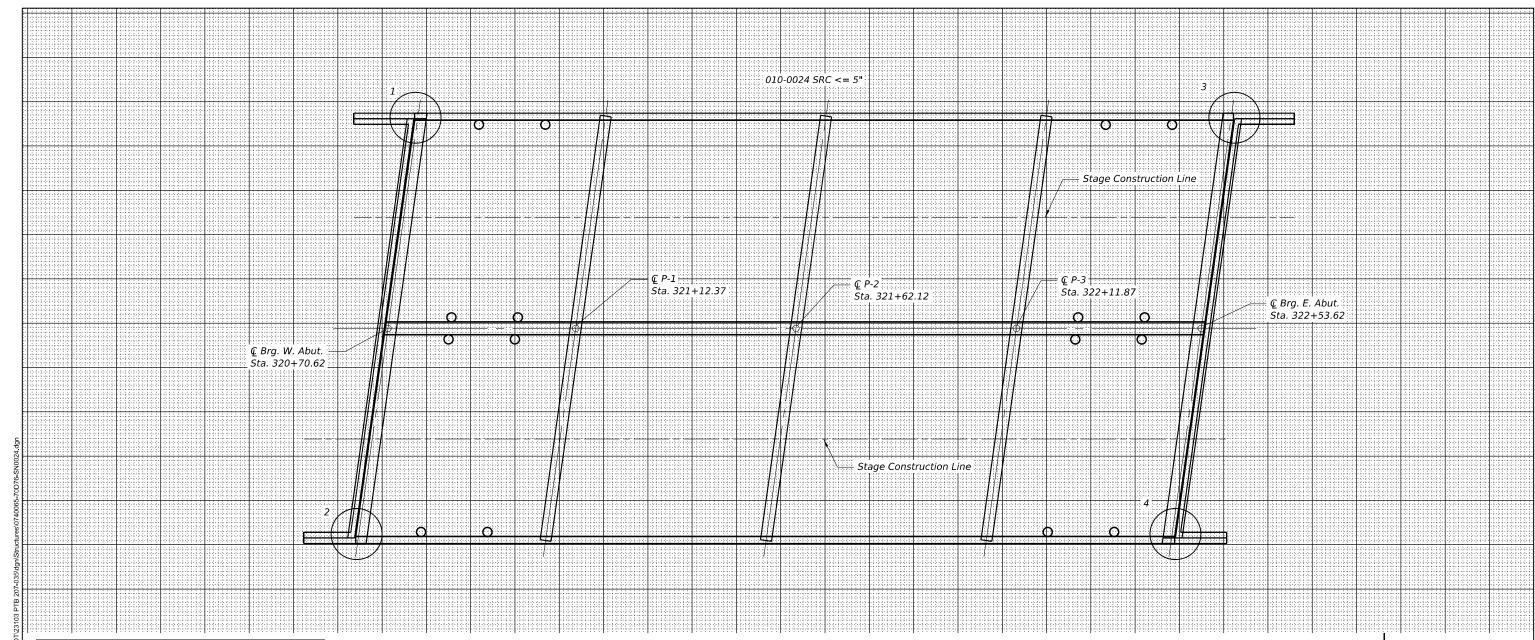
DESIGNED - MAC REVISED GREENE & BRADFORD, INC. JSER NAME = BenjaminBell CHECKED - KAS REVISED REVISED PLOT DATE = 9/10/2024 CHECKED -REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION COUNTY **GENERAL DATA** (14 HB) BR, (14 B) BR-1 CHAMPAIGN 90 69 **STRUCTURE NO. 010-0024** CONTRACT NO. 70D76 SHEET 2 OF 17 SHEETS



9/8/2024 2:11:33 P



PATCH	* SIZE (Ft.)		Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	
No.	WIDTH	HEIGHT	SIZE (Sq. Ft.)	
1 (Wingwall)	2	2	4	
2 (Wingwall)	2	2	4	
3 (Wingwall)	2	2	4	
4 (Wingwall)	2	2	4	
1 (Backwall)	2	2	4	
2 (Backwall)	2	2	4	
3 (Backwall)	2	2	4	
4 (Backwall)	2	2	4	
TOTAL ROUNDS TO (Sq. Ft.) :			32	
TOTAL RO	UNDS TO (32		

SHOWN GRAPHICALLY **ABOVE** ARE APPROXIMATE

THE LOCATIONS

DECK SLAB REPAIR

DATE OF SURVEY: 7/13/2023 SURVEY BY: METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES

Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

32 Sq. Ft.

The Engineer shall show actual locations of concrete repair on the as-built plans.

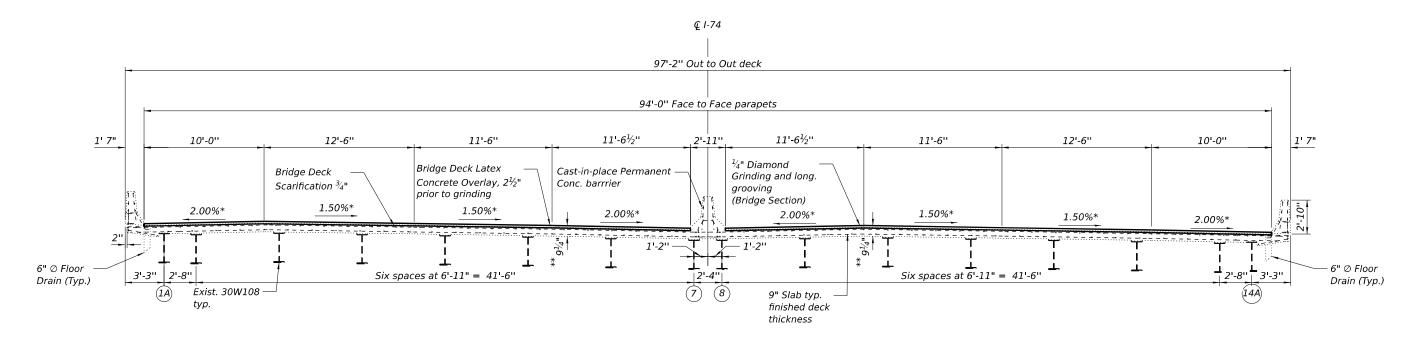
* The 2' wide region along wingwall and backwall from beam seat to the bottom hatch block.

	GREENE & BRADFORD, INC.	Ī
	S OF SPRINGFIELD	I
	CONSULTING ENGINEERS 3901 CONSTITUTION DRIVE	ŀ
	SPRINGFELD ALINOIS 62711	ı
П	PROFESSIONAL DESIGN FIRM NO. 184-001179 PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098	ł
٠	7NG ENG (217) 793-8844, 793-8227 (F)	ı

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3 ENGINEERS			
UTION DRIVE ILLINOIS 62711 I FIRM NO. 184-001179	PLOT SCALE =	DRAWN - SKC	REVISED -
YING FIRM NO. 048-000098 793-8227 (F)	PLOT DATE = 9/8/2024	CHECKED - KAS	REVISED -

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74	(14HB) BR, (14B) HB-1	CHAMPAIGN	90	7		
		CONTRACT NO. 70D7				

$\frac{\textit{EXISTING DECK CROSS SECTION}}{\textit{(Looking East)}}$

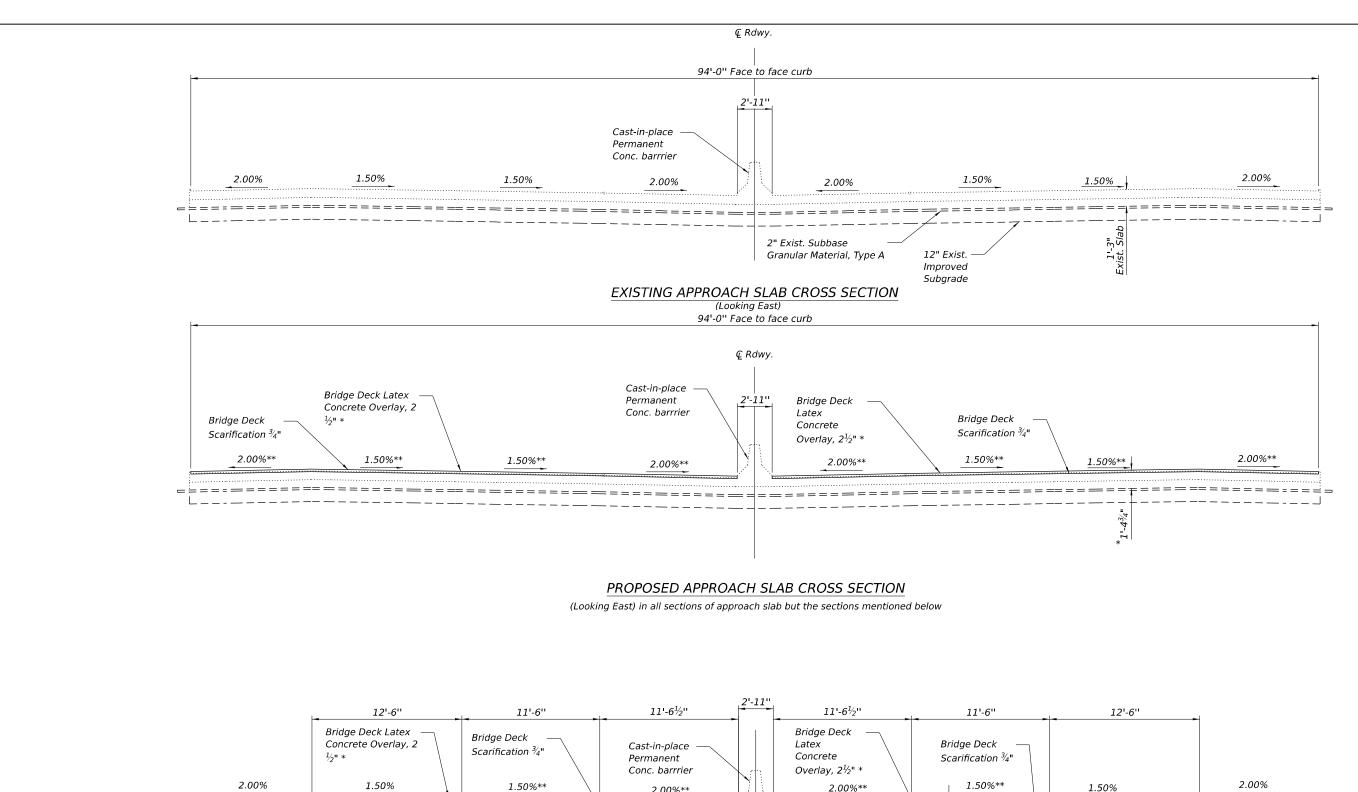


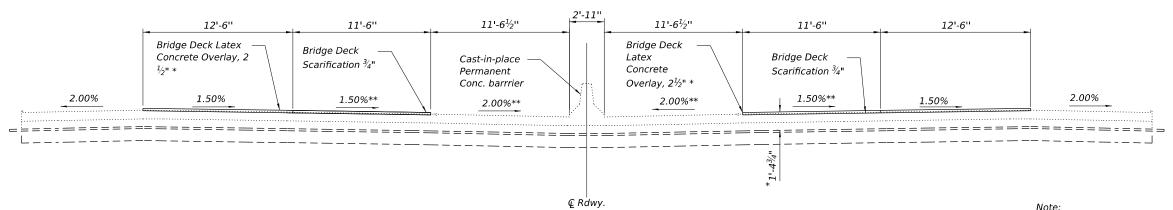
PROPOSED DECK CROSS SECTION

(Looking East)

** Prior to grinding * Match Existing

유 : [GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -		EXISTING AND PROPOSED DECK CROSS SECTIONS		COUNTY TOTAL SHEET
9 1	OF SPRINGFIELD CONSULTING ENGINEERS		CHECKED - KAS	REVISED -	STATE OF ILLINOIS	STRUCTURE NO. 010-0024	74 (14HB) BR, (14B) HB-1	CHAMPAIGN 90 72
Ž	B 9501 CONSTITUTION DRIVE SPROFESSIONAL DESIGNATION NO. 184-001178	PLOT SCALE = DRAWN - SKC REVISED - DEPART!	DEPARTMENT OF TRANSPORTATION	51RUCTURE NO. 010-0024		CONTRACT NO. 70D76		
II OU TING	TING ENGIN PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098 (217) 793-8844, 793-8227 (F)	PLOT DATE = 10/2/2024	10/2/2024 CHECKED - KAS REVISED -		SHEET 5 OF 17 SHEETS	ILLINOIS FE	D. AID PROJECT	





PROPOSED APPROACH SLAB CROSS SECTION

(Looking East) from Sta 320+22.58 to 320+58.99 in WBL from Sta 322+76.58 to 323+08.58 in WBL from Sta 320+13.58 to 320+34.41 in EBL from Sta 322+68.58 to 323+02.58 in EBL

The Width of the necessary overlay in approach slabs varies. Refer to the drawing plan in Sheet 1 for the variation

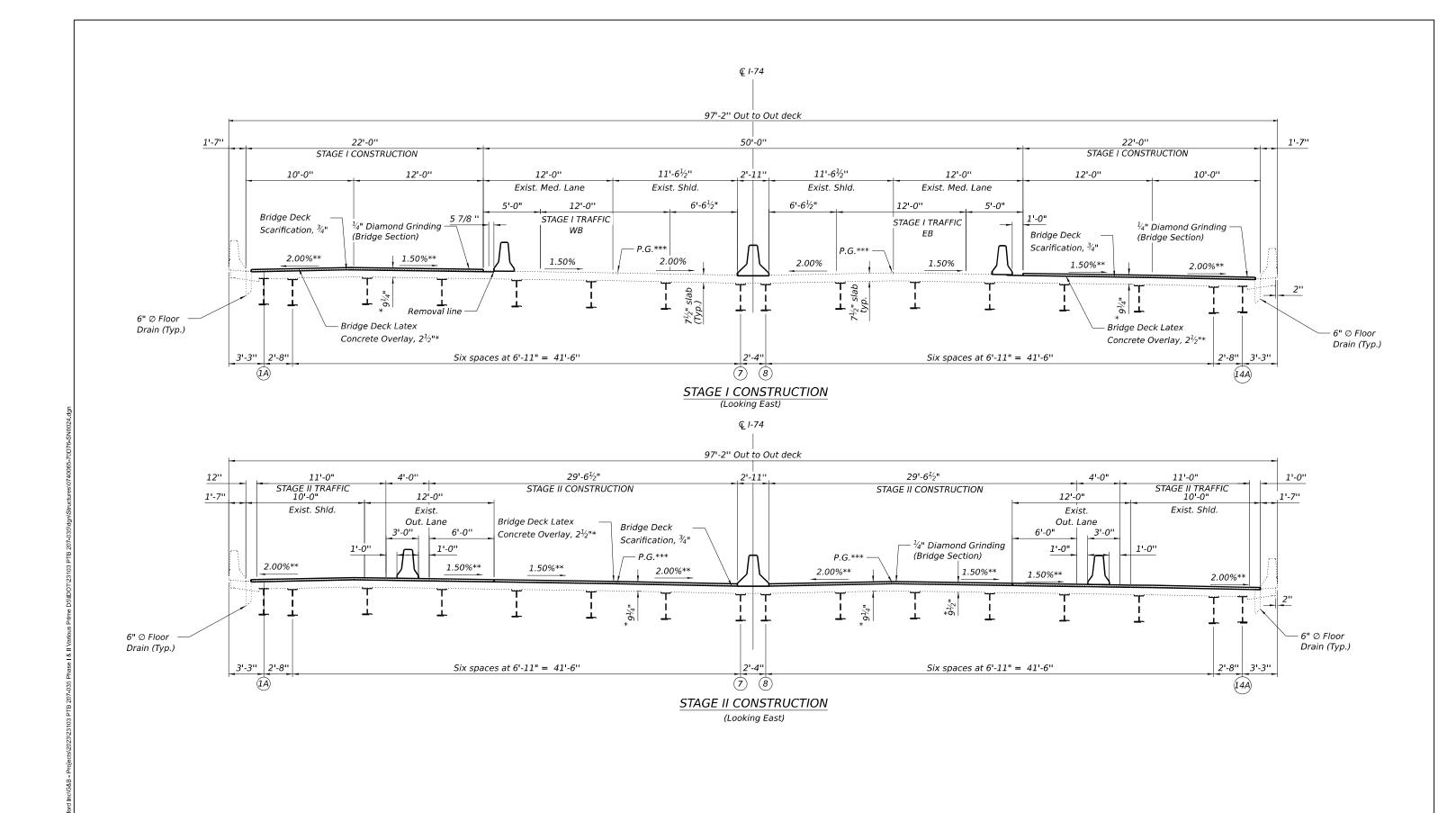
Stations Mentioned are along PGL

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١	GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -	Г
N	OF SPRINGFIELD CONSULTING ENGINEERS		CHECKED - KAS	REVISED -	ı
ž	3991 CONSTITUTION DRIVE 3991 CONSTITUTION DRIVE PROFESSIONAL DESIGN FEW NO. 184-601178	=	DRAWN - SKC	REVISED -	ı
	TING ENGIN PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098 (217) 793-5844, 793-6227 (F)	PLOT DATE = 9/8/2024	CHECKED - KAS	REVISED -	L

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** **EXISTING AND PROPOSED APROACH SLAB CROSS SECTIONS** (14HB) BR, (14B) HB-1 **STRUCTURE NO. 010-0024** SHEET 6 OF 17 SHEETS

90 73 CHAMPAIGN CONTRACT NO. 70D76

* Prior to grinding ** Match Existing



- * Prior to grinding
- ** Match existing
- *** Location varies due to road curvature

GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -
OF SPRINGFIELD		CHECKED - KAS	REVISED -
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

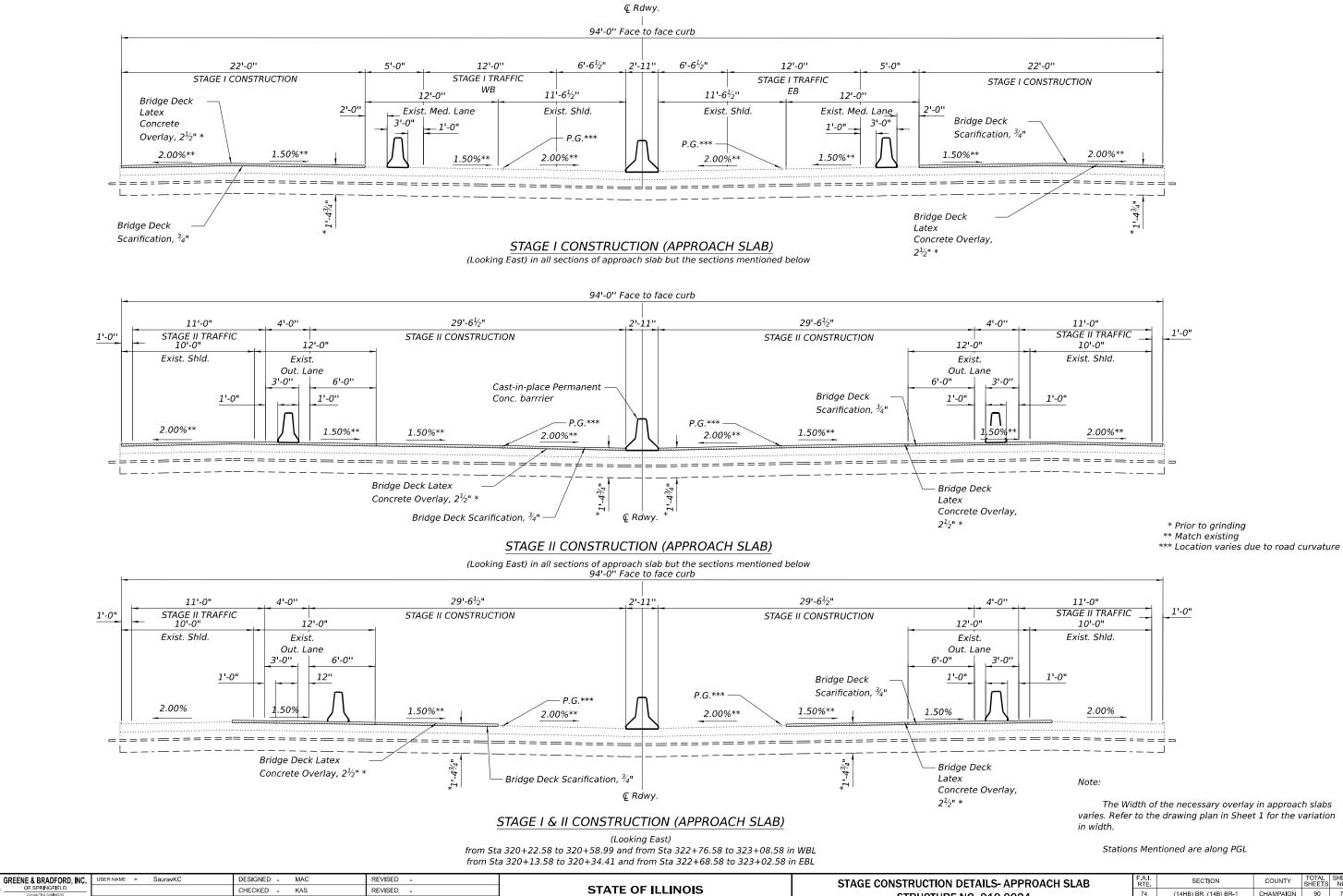
STAGE CONSTRUCTION DETAILS - BRIDGE
STRUCTURE NO. 010-0024

SHEET 7 OF 17 SHEETS

 F.A.I. RTE.
 SECTION
 COUNTY
 TOTAL SHEETS
 SHEET NO.

 74
 (14HB) BR, (14B) BR-1
 CHAMPAIGN
 90
 74

 CONTRACT NO. 70D76



PLOT DATE = 9/8/2024

DEPARTMENT OF TRANSPORTATION

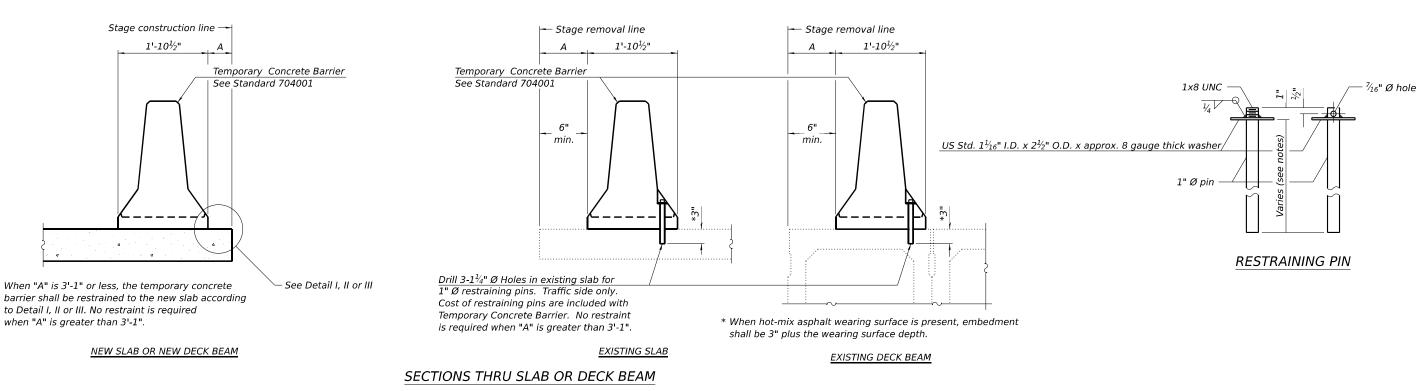
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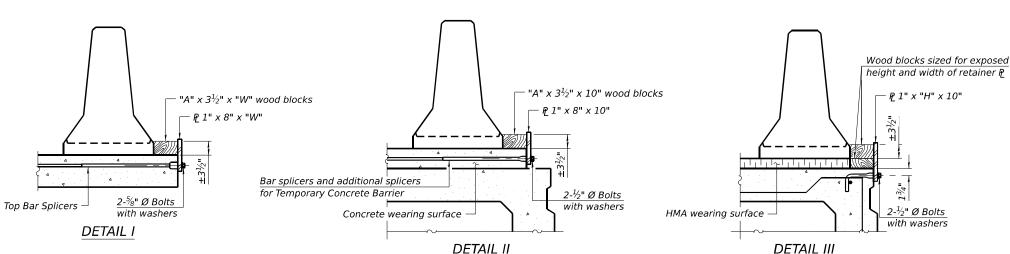
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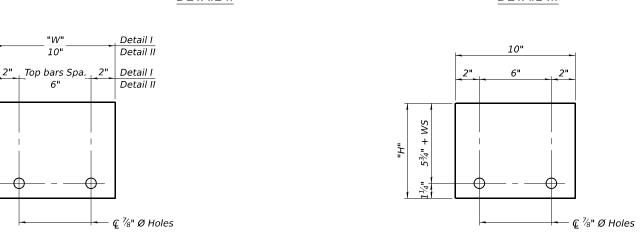
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STRUCTURE NO. 010-0024 SHEET 8 OF 17 SHEETS

(14HB) BR, (14B) BR-1 CHAMPAIGN 90 75 CONTRACT NO. 70D76







RAILING CRITERIA

NCHRP 350 Test Level 3
Railing Weight (plf) 440

R-27 5-15-2023

STEEL RETAINER P 1" x 8" x "W"

(Detail I and II)

STEEL RETAINER & 1" x "H" x 10" (Detail III)

= 27,4

BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate ${\mathfrak C}$ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

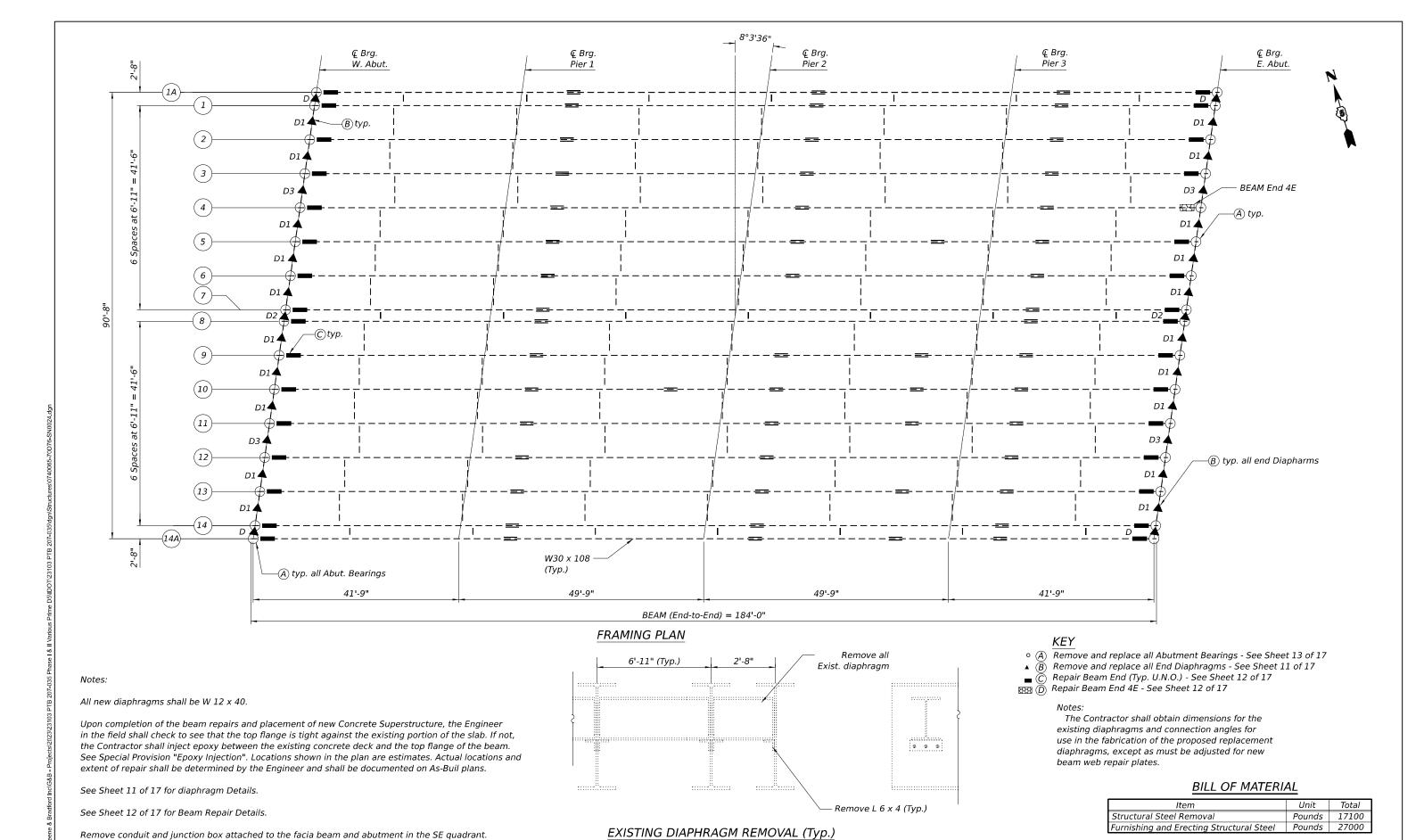
When the 'A' dimension is less than $1\frac{1}{2}$ ", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

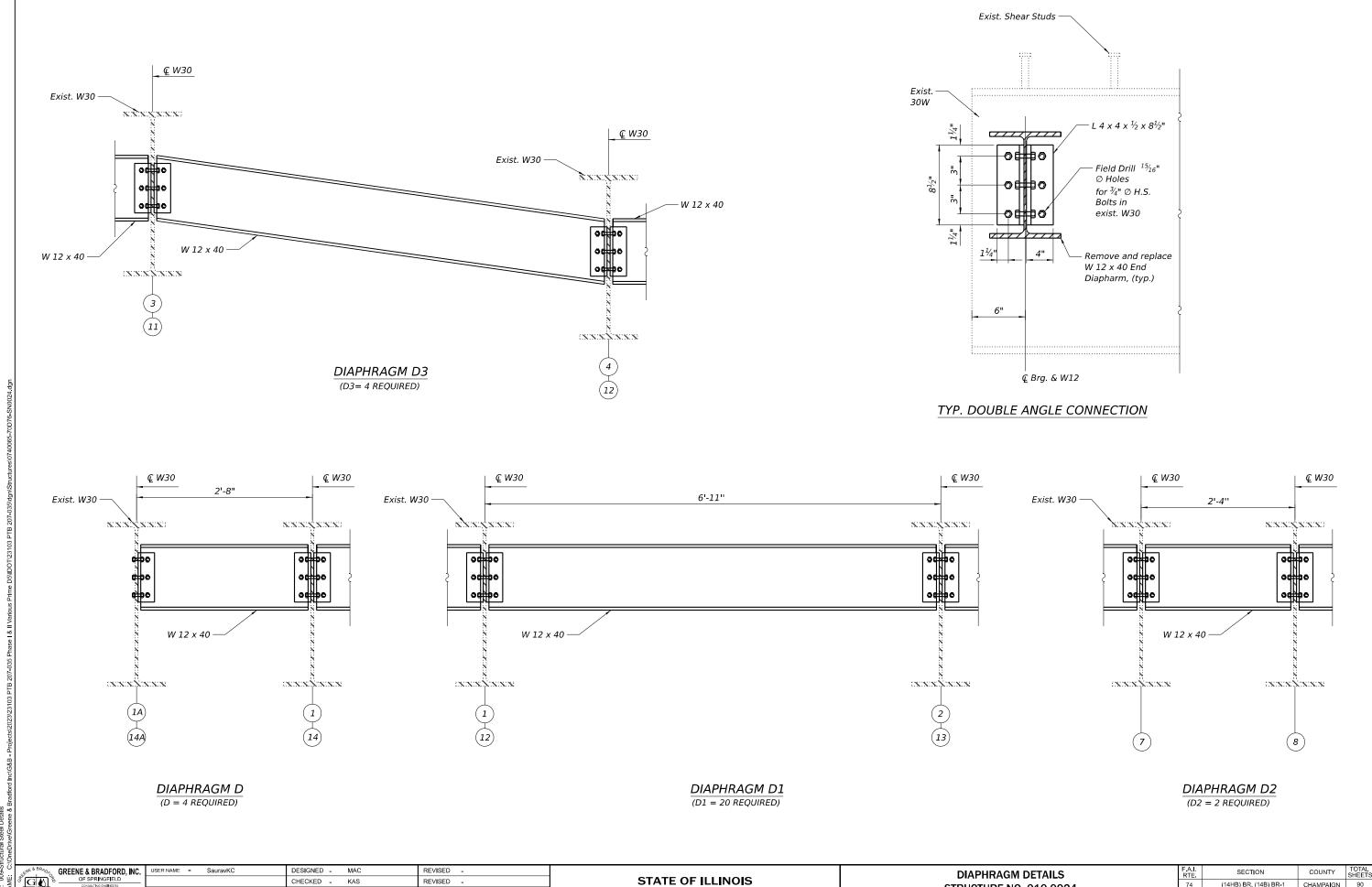
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

SENE & BRAO	GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED -	MAC	REVISED -		TEMPORARY CONCRETE BARRIER	F.A.I. RTE	SECTION	COUNTY TOT	TAL SHEET EETS NO.
S G N	S OF SPRINGFIELD CONSULTING ENGINEERS		CHECKED -	KAS	REVISED -	STATE OF ILLINOIS		74	(14HB) BR. (14B) BR-1	CHAMPAIGN 9	JO 76
% □ B	9 3901 CONSTITUTION DRIVE SPRINGHELD, ILLINOIS 62711	PLOT SCALE =	DRAWN -	SKC	REVISED -	DEPARTMENT OF TRANSPORTATION	STRUCTURE NO. 010-0024		(,, (,	CONTRACT N	NO. 70D76
CY TING ENG	PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098 (217) 793-8844, 793-8227 (F)	PLOT DATE = 9/8/2024	CHECKED -	KAS	REVISED -		SHEET 9 OF 17 SHEETS		ILLINOIS FED. A	AID PROJECT	



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Cost included with Structural Steel Repair



PLOT DATE = 9/8/2024

DEPARTMENT OF TRANSPORTATION

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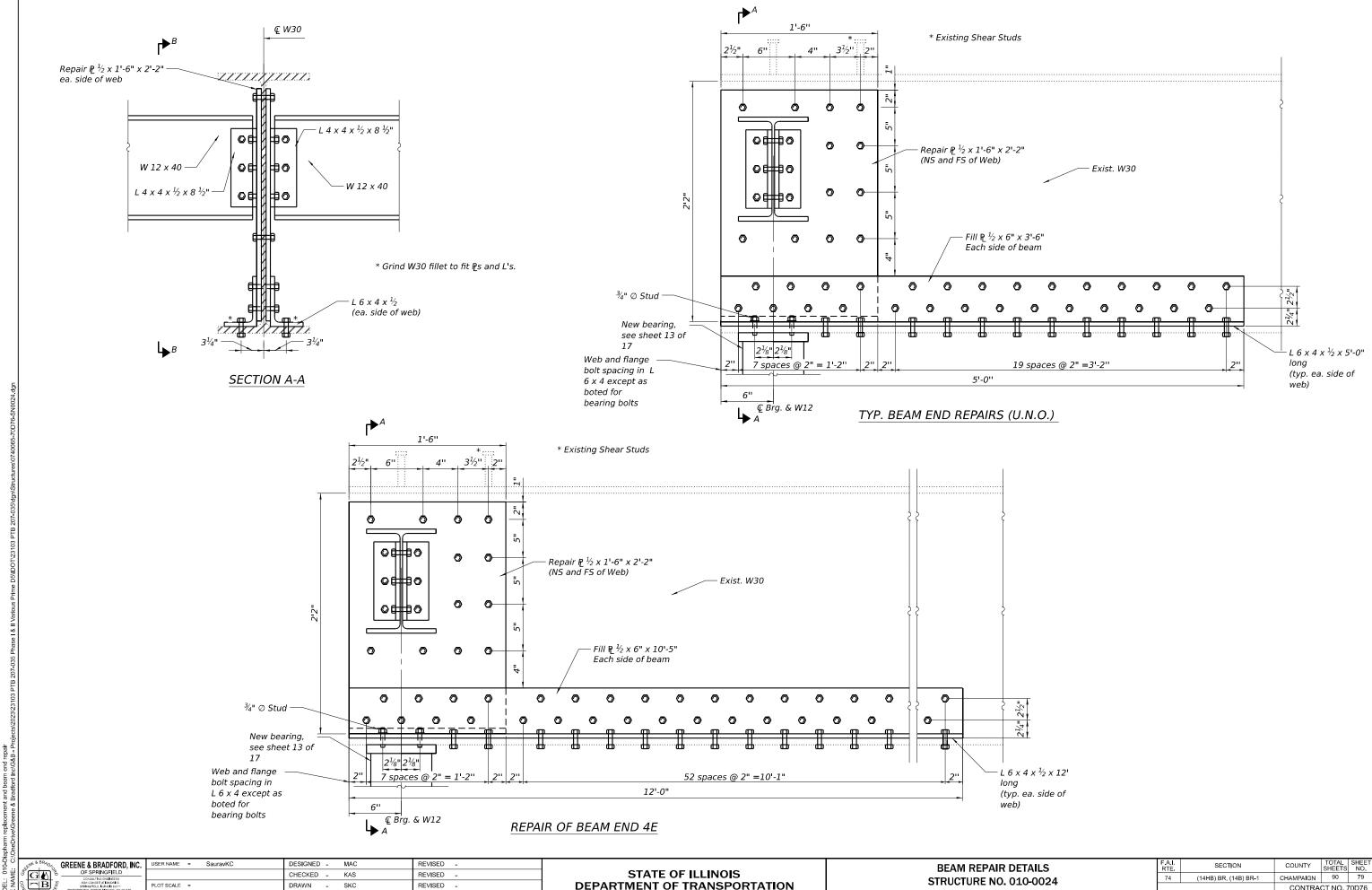
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CHECKED - KAS

STRUCTURE NO. 010-0024 SHEET 11 OF 17 SHEETS

 SECTION
 COUNTY
 TOTAL SHEETS NO.

 (14HB) BR, (14B) BR-1
 CHAMPAIGN 90 78
 CONTRACT NO. 70D76



PLOT DATE = 9/8/2024

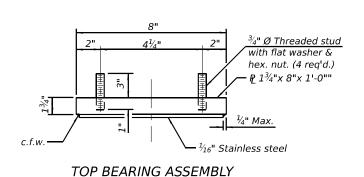
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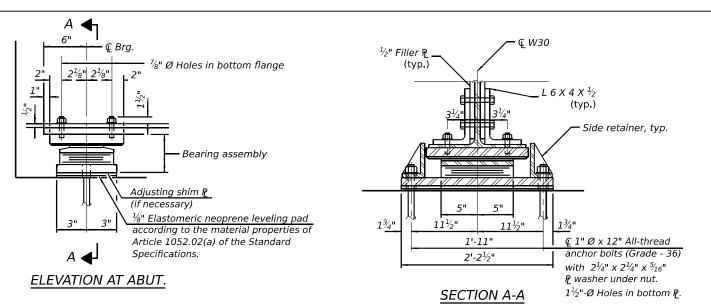
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DEPARTMENT OF TRANSPORTATION

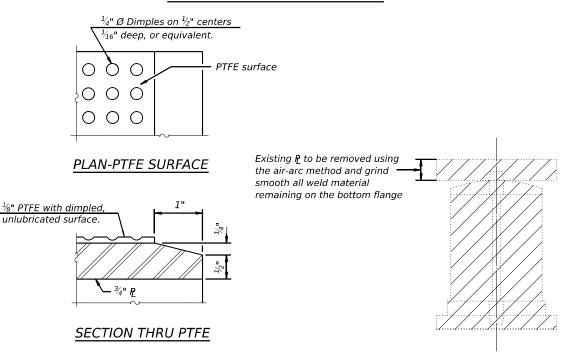
STRUCTURE NO. 010-0024 SHEET 12 OF 17 SHEETS

CONTRACT NO. 70D76





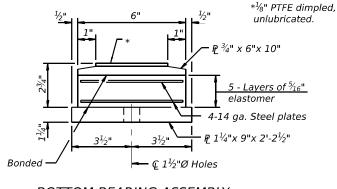
TYPE II ELASTOMERIC EXP. BRG.



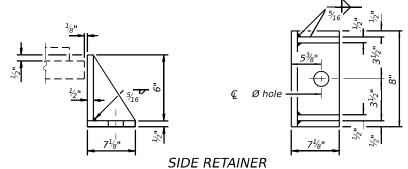
© Top Brg.

EXISTING BEARING REMOVAL DETAILS

Cost included with Jack and Remove Existing Bearings



BOTTOM BEARING ASSEMBLY



Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

EXPANSION BEARING ORIENTATION

€ Bott. Brg.

The above diagrams are for informational purposes only to show the amount of expected offset "D" for the current temperature in the field.

 $D=\frac{1}{8}$ " per each 100' of expansion for every 15° temp.

change from the normal temp. of 50°F.

	W. Abut.	Pier 1	D: 2
	or	or	Pier 2
	E. Abut.	Pier 3	
R_{DL} (K)	16.40	52.95	51.28
R_{LL} (K)	38.20	46.33	46.35
IMP. (K)	11.20	13.57	13.25
R_{TOTAL} (K)	65.80	112.85	110.88

REACTION TABLE - EXISTING INTERIOR BEAM

REACTION TABLE - NEW BEAM 1A - 14A

		W. Abut. or E. Abut.	Pier 1 or Pier 3	Pier 2
R _{DL}	(K)	18.93	61.11	59.18
R LL	(K)	33.18	41.30	41.29
IMP.	(K)	9.72	12.10	11.81
R _{TOTAL}	(K)	61.83	114.51	112.28

Reaction Tables taken from 1989 drawing set Sheet 10 of 25 Design Specification: AASTHO 1983 Standard Specification for Highway Bridges,1984 thru 1988 Interims

Notes:

Side retainers and leveling pad required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.

The $\frac{1}{8}$ " PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of \(^1\)/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.

Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Adjustment must account for deck heave due to pack rust (if present).

Min. jack capacity = 26 Tons Min.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s)

All new steel extensions, bearing plates (Type I only), side retainers, anchor bolts, nuts, and washers shall be hot-dipped galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel". Mask surface of plate to be bonded to elastomer and paint masked surface.

Two $\frac{1}{8}$ " adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing	Fach	32
Assembly, Type II	Lacri	52
Anchor Bolts 1"	Each	64
Jack and Remove Existing Bearings	Each	32
· · · · · · · · · · · · · · · · · · ·		•

5/15/2023

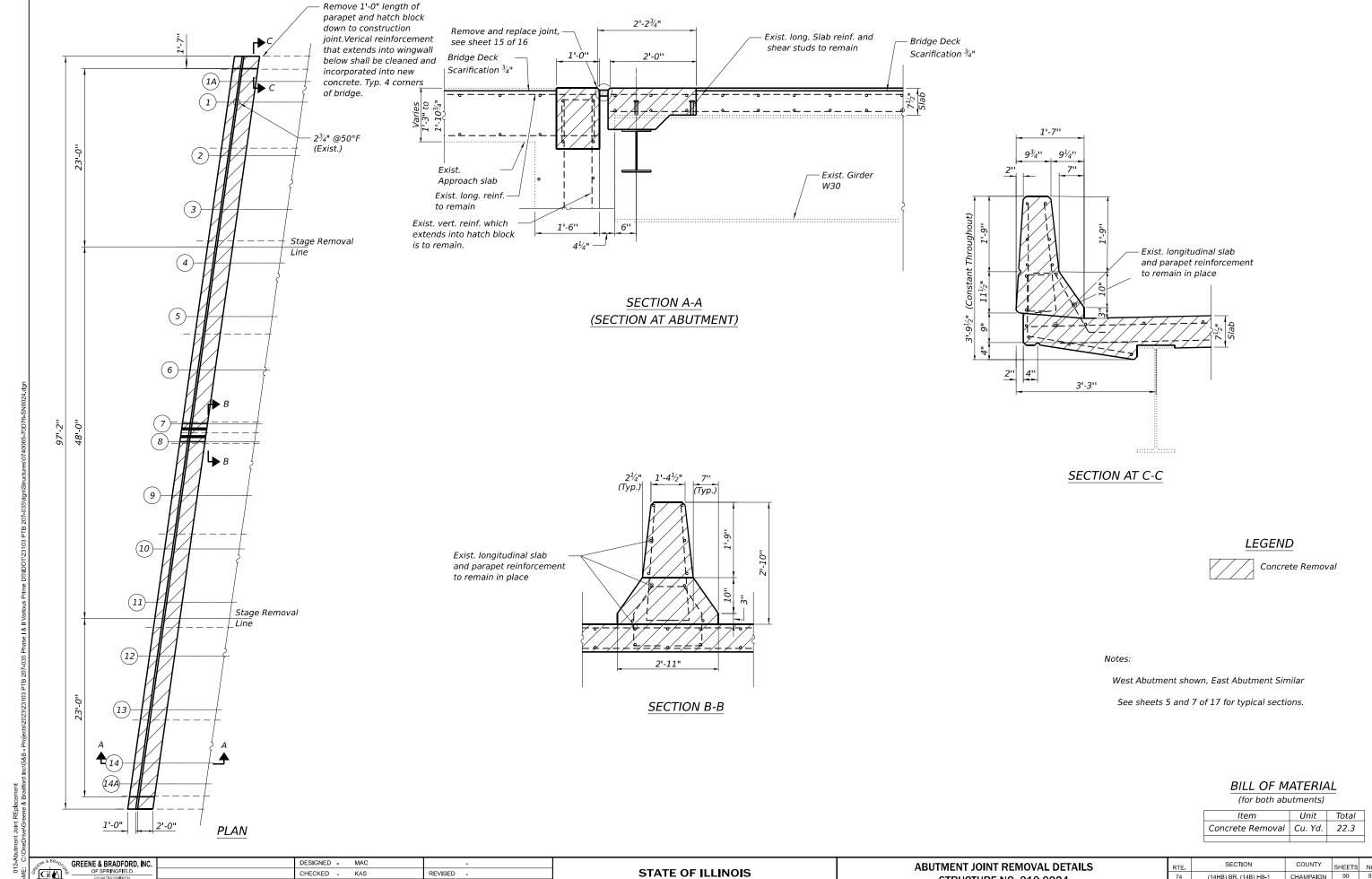
€ Bott. Brg. —

CENE & BRYON	GREENE & BRADFORD, INC.	USER NAME = SauravKC	DESIGNED - MAC	REVISED -
FGV F	OF SPRINGFIELD CONSULTING ENGINEERS		CHECKED - KAS	REVISED -
% [□B] å	9 3901 CONSTITUTION DRIVE SPRINGFIELD ILLINOIS 62711 PROFESSIONAL DESIGN FROM NO. 184-501178	PLOT SCALE =	DRAWN - SKC	REVISED -
OE MACHEN	PROFESSIONAL LAND SURVEYING FIRM NO. 048-000098 (217) 793-8844, 793-4227 (F)	PLOT DATE = 9/8/2024	CHECKED - KAS	REVISED -

 				ACEMENT DETAIL 10-0024
SHEET	13	OF	17	SHEETS

F.A.I. RTE.	SECT	TION		COUNTY	TOTAL SHEETS	SHEET NO.
74	'4 (14HB) BR, (14B) BR-1		CHAMPAIGN	90	80	
				CONTRA	CT NO. 7	70D76
		ILLINOIS	FED. A	D PROJECT		

I-2E-2

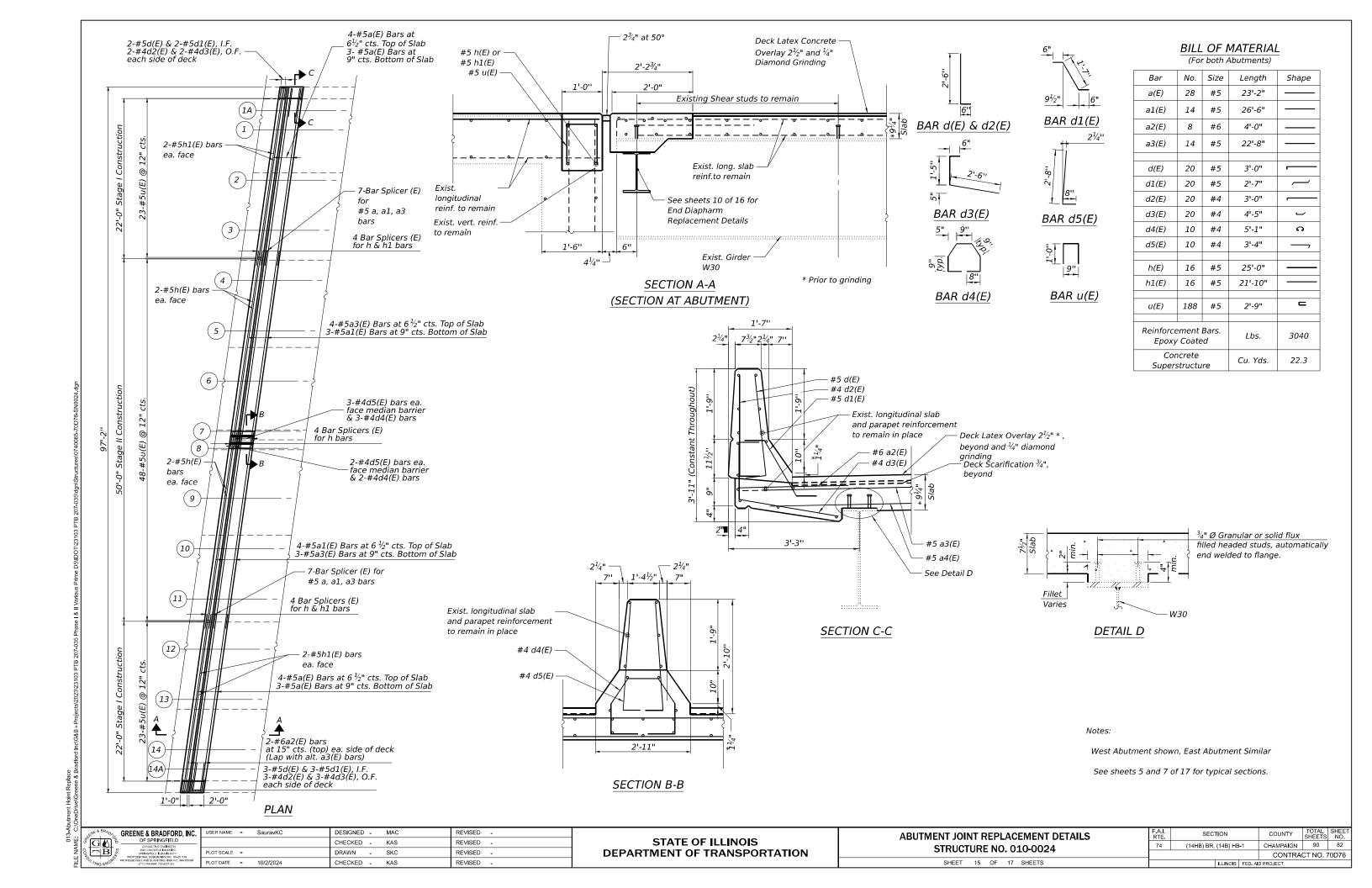


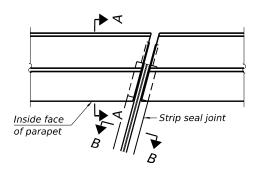
CHECKED - KAS REVISED -PLOT DATE = 10/2/2024 CHECKED - KAS REVISED -

DEPARTMENT OF TRANSPORTATION

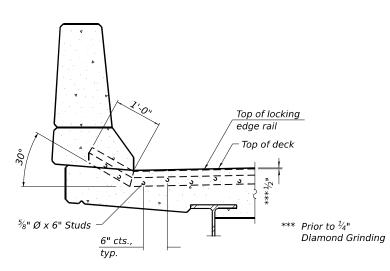
STRUCTURE NO. 010-0024 SHEET 14 OF 17 SHEETS

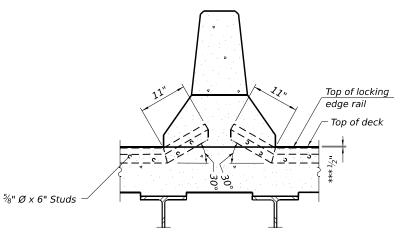
COUNTY SHEETS NO.
CHAMPAIGN 90 81 (14HB) BR, (14B) HB-1 CONTRACT NO. 70D76





PLAN FOR SKEWS ≤ 30°





SECTION THRU MEDIAN

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

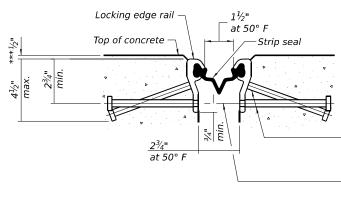
The Maximum space between locking edge rail segments shall be $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

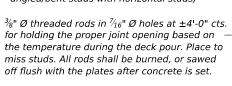
The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

SECTION AT A-A



SHOWING ROLLED RAIL JOINT

* $\frac{5}{8}$ " Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)



SECTION B-B * Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

Locking edge rail at 50° F Top of concrete Strip seal at 50° F

SHOWING WELDED RAIL JOINT

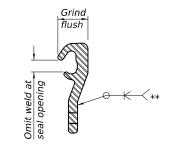
LOCKING EDGE RAILS

WELDED RAIL

** Back gouge not required if complete joint penetration is verified by mock-up.

ROLLED

(EXTRUDED) RAIL



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	188

GREENE & BRADFORD, INC.

DESIGNED - MAC REVISED -JSER NAME = SauravKC CHECKED - KAS REVISED -REVISED PLOT DATE = 9/8/2024 CHECKED -REVISED . KAS

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PREFORMED JOINT STRIP SEAL DETAILS **STRUCTURE NO. 010-0024** SHEET 16 OF 17 SHEETS

SECTION COUNTY 90 83 (14HB) BR, (14B)BR-1 CHAMPAIGN CONTRACT NO. 70D76

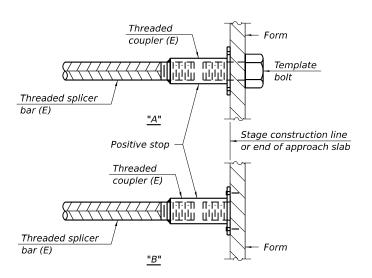
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + $1\frac{1}{2}$ " + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Hatch Block	#5	24	3'-6"
Slab	#5	76	3'-6"

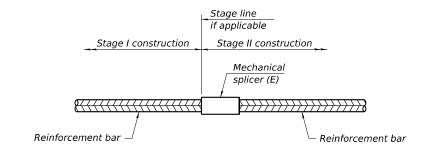


INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
"B": Set bar splicer assembly by nailing to wood forms or

(E) : Indicates epoxy coating.

cementing to steel forms.



STANDARD MECHANICAL SPLICER

Location	Bar	No. assemblies
LOCATION	size	required

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

GREENE & BRADFORD, INC.

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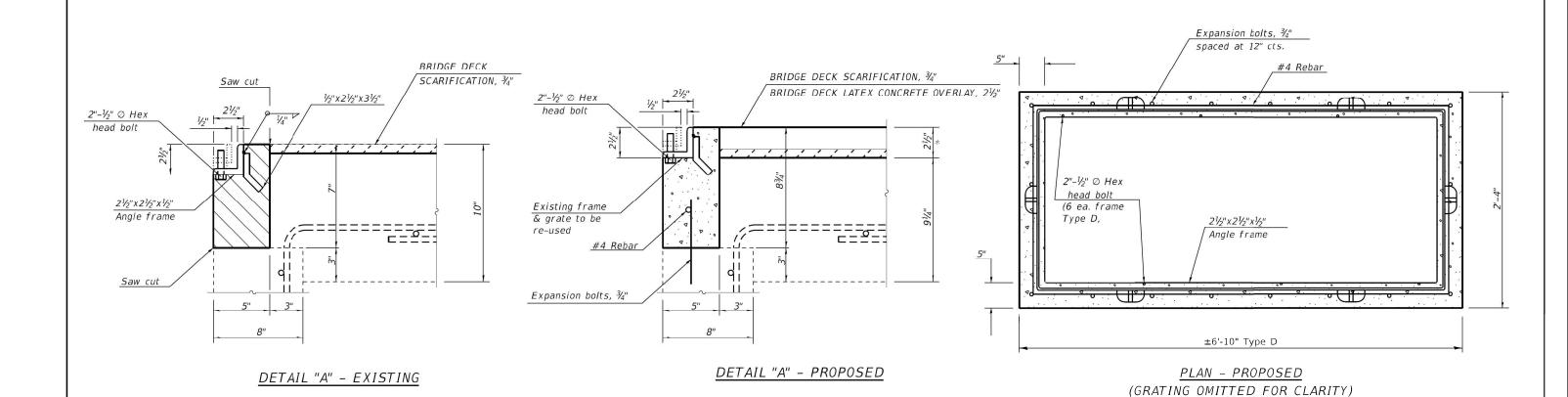
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

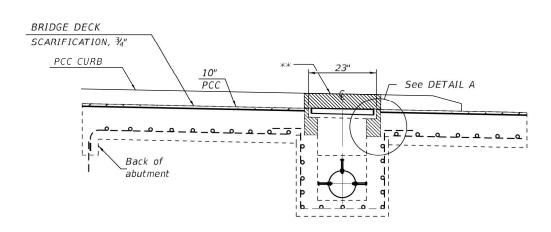
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 010-0024

SHEET 17 OF 17 SHEETS

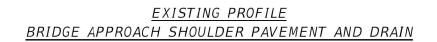


Class S.I. Concrete

* PRIOR TO GRINDING



Concrete Removal



BRIDGE DECK SCARIFICATION, ¾"

BRIDGE DECK LATEX CONCRETE OVERLAY, 2½" *

**

See DETAIL A

Back of abutment

** REMOVE & REPLACE
PCC CURB IF REQUIRED
TO ADJUST GRATE.

<u>PROPOSED PROFILE</u>
BRIDGE APPROACH SHOULDER PAVEMENT AND DRAIN

<u>NOTES:</u> TYPE D FRAMES (3 GRATES) OCCUR AT 10' - WIDE SHOULDERS

EXPANSION BOLTS SHALL EXTEND A MINIMUM OF 4" INTO THE PROPOSED CONCRETE.

ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH APPLICABLE PORTIONS OF SECTION 602 & 610.

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "INLET BOXES TO BE ADJUSTED (SPECIAL)" WHICH SHALL INCLUDE ALL LABOR, EQIUPMENT AND MATERIAL NECESSARY TO COMPLETE THE WORK

ESTIMATED QUANTITIES

TYPE D

ITEM	UNIT	TOTAL
CONCRETE REMOVAL	CU YD	0.12
REINFORCEMENT BARS (EPOXY)	POUND	11
CLASS S.I. CONCRETE	CU YD	0.18
EXPANSION BOLTS, ¾"	EACH	18

BILL OF MATERIAL

ITEM	UNIT	TOTAL
INLET BOXES TO BE ADJUSTED (SPECIAL)	EACH	4.0

* QUANTITY INCLUDES 4 TYPE D

	USER NAME = sclyne	DESIGNED -	REVISED - 11/06
BACON FARMER WORKMAN		DRAWN -	REVISED - 12/20
ENGINEERING & TESTING, INC.	PLOT SCALE = 0.1667 / in	CHECKED -	REVISED -
	PLOT DATE = 8/15/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INLET BOX TO BE ADJUSTED SN 010-0022 WB; 010-0023 EB; & 010-0024								
	SHEET	1	OF	1	SHEETS	STA.	TO STA.	

F.A.I. SECTION COUNTY TOTAL SHEET'S WO.

74 (14HB)BR,(14B)BR-1 CHAMPAIGN 90 85

CONTRACT NO. 70D76

NOTE: COST OF SAW CUT SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL - BUTT JOINT

HMA SURFACE REMOVAL - BUTT JOINT

HMA BUTT JOINT

I-74 EB:

RT STA 1278+61 TO RT STA 1279+21 RT STA 1280+67 TO RT STA 1281+27 RT STA 1319+53 TO RT STA 1320+13 RT STA 1323+02 TO RT STA 1323+62

I-74 WB:

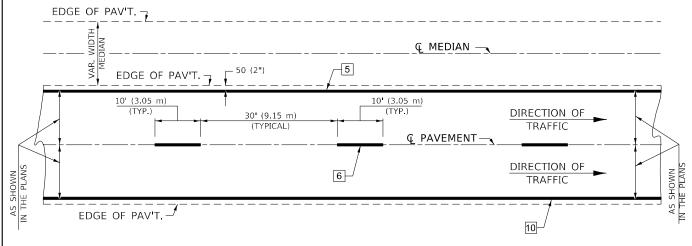
LT STA 1278+55 TO LT STA 1279+15 LT STA 1280+60 TO LT STA 1281+20 LT STA 1319+62 TO LT STA 1320+22 LT STA 1323+08 TO LT STA 1323+68

NOTE: ON MEDIAN AND OUTSIDE SHOULDERS HMA SURFACE COURSE WILL EXTEND PAST THE HMA RAMP TO THE EXISTING CONC. BRIDGE APPR. SHLD PAVT THAT WILL HAVE THE SAME PROPOSED TREATMENT AS THE BRIDGE APPROACH PAVEMENT AND BRIDGE DECKS.

BFW	BACON FARMER WORK ENGINEERING & TESTING,

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTERLINE INTERSTATE OR MULTI-LANE TWO WAY DIVIDED HIGHWAY

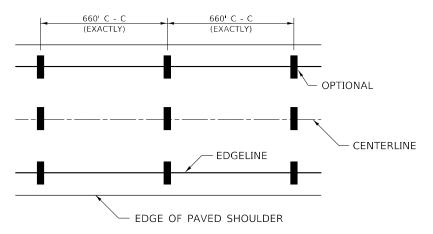


NOTE: PAVEMENT MARKINGS ARE
TO BE EXTENDED THROUGH
OMISSIONS WHEN APPLICABLE.

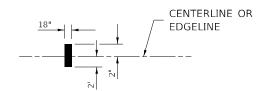
NOTE: SEE ARTICLES 780.04 & 781.03 FOR LOCATION OF STRIPES AND MARKERS RELATIVE TO EDGES OR JOINTS.

FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO STANDARD 781001.

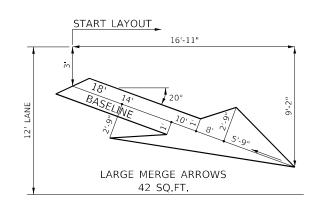
TYPICAL PAVEMENT MARKING LEGEND



IT WILL BE NECESSARY TO HAVE A REPRESENTATIVE OF THE STATE POLICE PRESENT SO THAT THE ACCURACY OF MEASUREMENT CAN BE ATTESTED TO IN COURT.



AERIAL SPEED CHECK ZONES



1 4" (100) SKIP-DASH (YELLOW)

2 4" (100) SOLID (YELLOW)

3 12" (300) DIAGONAL (YELLOW)

4" (100) DOUBLE YELLOW (NARROW)

5 6" (150) SOLID (YELLOW)

6 6" (150) SKIP-DASH (WHITE)

7 4" (100) SKIP-DASH (WHITE)

8 4" (100) SOLID (WHITE)

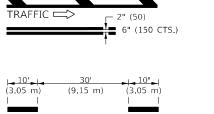
9 12" (300) DIAGONAL (WHITE)

10 6" (150) SOLID (WHITE)

11 24" (600) STOP BAR (WHITE)

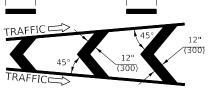
12 8" (200) SOLID (WHITE)

13 6" (150) LANE LINE EXTENSIONS (WHITE)



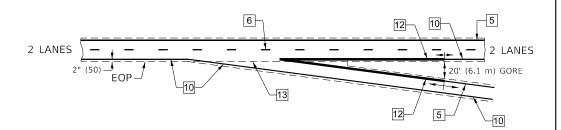
(9.15 m)

(3.05 m)

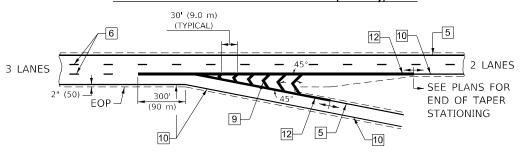




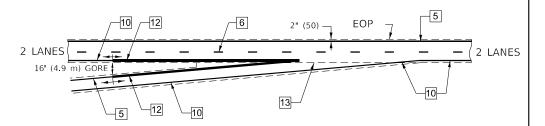
TYPICAL EXIT RAMP TERMINAL



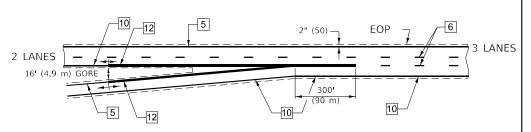
EXIT RAMP TERMINAL with EXCLUSIVE (auxiliary) LANE



TYPICAL ENTRANCE RAMP TERMINAL



ENTRANCE RAMP TERMINAL with EXCLUSIVE LANE



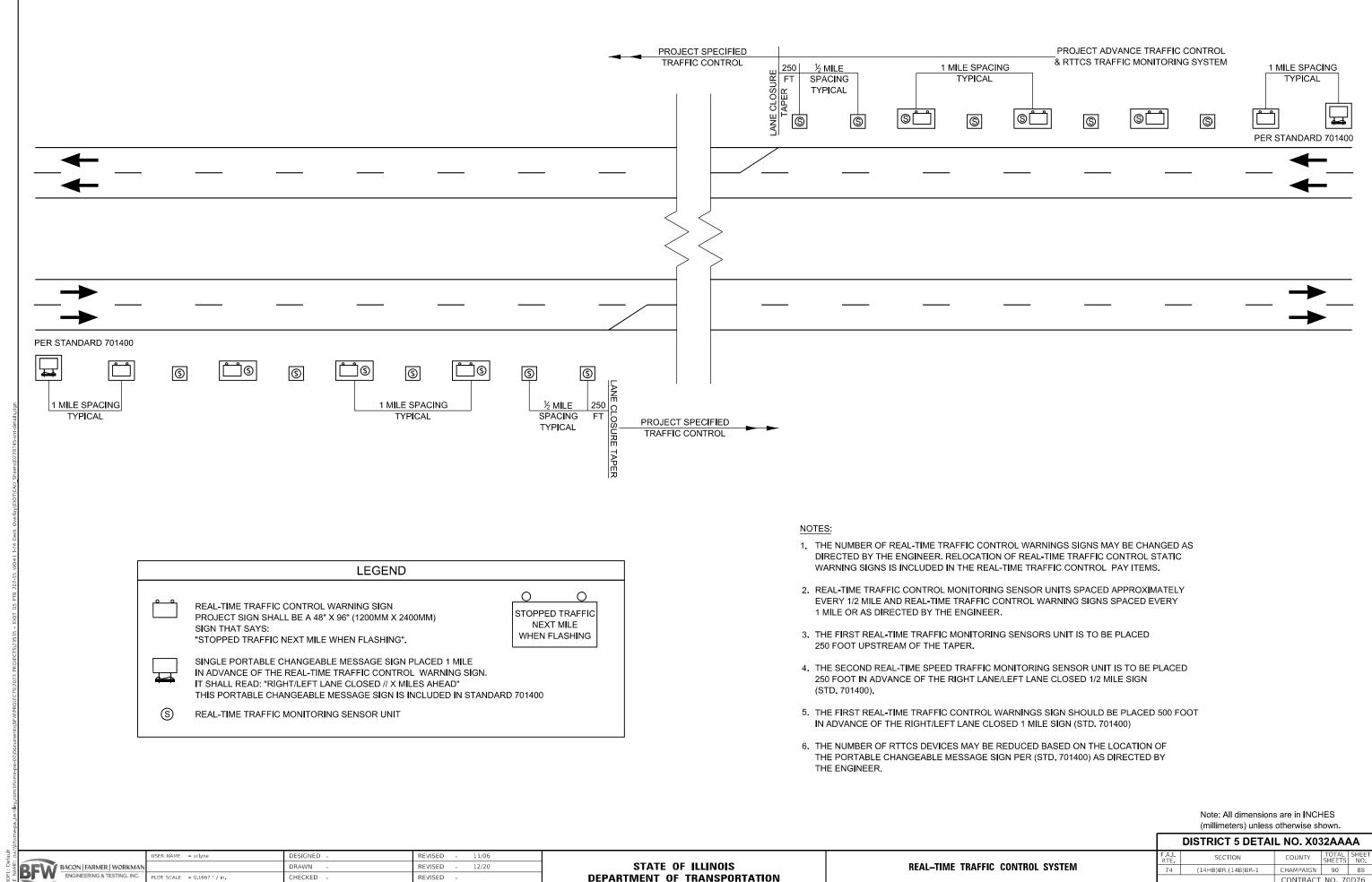
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT	5	DFTAIL	NO.	7800BBBB

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING (INTERSTATE & MULTI-LANE APPLICATIONS)

ALE: SHEET 1 OF 1 SHEETS STA. TO STA.



PLOT DATE = 8/15/2024

DATE

REVISED

DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 1 SHEETS STA.

(14HB)BR,(14B)BR-1 CONTRACT NO. 70D76

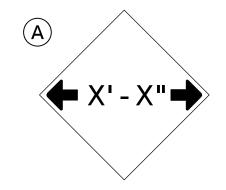
DETAIL FOR TEMPORARY RUMBLE STRIPS (SPECIAL) CHANGEABLE MESSAGE SIGN OR RTTCS STATIC WARNING SIGN 3 TO 5 MILES FROM TAPER ± RTTCS STATIC WARNING SIGN 1½ MILES FROM TAPER ± \triangleleft $\langle \Box$ WHITE BACKGROUND MEDIAN SEE DETAIL A $\qquad \qquad \Box >$ SEE STANDARD 701400 SEE APPROPRIATE FOR DETAILS OF APPROACH STANDARD FOR DETAILS TO LANE CLOSURE OF LANE CLOSURE 250' 250' 500' 500' (150 m) (75 m) (150 m) (75 m) SEE STANDARD 701400 1100' FOR DETAILS OF APPROACH (330 m) (330 m) TO LANE CLOSURE ROAD W20-I103(0)-48 48x48 (0) **X MILES** W16-3a (0)-3612 **DETAIL A SYMBOLS GENERAL NOTES** - 20' (TYP.) REMOVE THE TEMPORARY RUMBLE STRIPS (SPECIAL) SIGN PRIOR TO THE REMOVAL OF THE ADVANCED WARNING SIGNS. TEMPORARY RUMBLE STRIPS (SPECIAL) ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN. TRAILER MOUNTED SIGN 11' (TYP.) RTTCS STATIC WARNING SIGN INSTALLATION DESIGNED -REVISED 11/06 SECTION DETAIL FOR TEMPORARY BFW BACON FARMER WORKMAN ENGINEERING & TESTING, INC. STATE OF ILLINOIS DRAWN REVISED 12/20 (14HB)BR,(14B)BR-1 **RUMBLE STRIPS (SPECIAL)**

CHECKED REVISED PLOT DATE = 8/15/2024 DATE

DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 1 SHEETS STA. TO STA.

CHAMPAIGN 90 89 CONTRACT NO. 70D76

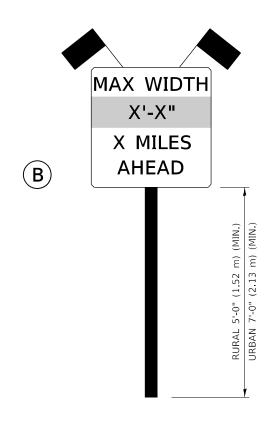


W12-2(0)-48"x48"(1200x1200)

SIGN (A) 2 SIGNS - W12-2(O)-48"x48"(1200x1200) ARE TO BE PLACED AS SHOWN

SIGN (B) 2 SIGNS - (SIGN PANEL, TYPE II) AS SHOWN ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

IN THE PLANS OR AS DIRECTED BY THE ENGINEER.



SIGN PANEL, TYPE II

BLACK LETTERS MAX WIDTH (ORANGE) X'-X" (B) X MILES WHITE BACKGROUND **AHEAD**

W12-I103(O)-48"x48"(1200x1200) "D" LETTERS/NUMBERS

GENERAL NOTES

- 1. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED AND MAINTAINED BY THE CONTRACTOR.
- 2. ALL (B) SIGNS SHALL HAVE FLAGS INSTALLED UNLESS OTHERWISE DIRECTED.
- 3. LOCATIONS OF TRAFFIC CONTROL DEVICES MAY BE ADJUSTED BY THE ENGINEER.
- 4. ALL TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR WIDTH RESTRICTION
- 5. ALL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE DIRECTED.
- 6. ALL SIGNS SHOWN ORANGE (O) SHALL BE FLUORESCENT ORANGE.
- 7. ALL SIGNS SHOWN SHALL CONSIST OF THE CURRENT RETROREFLECTIVE SHEETING REQUIREMENTS AS OUTLINED IN SECTION 1106.01 OF THE STANDARD SPECIFICATIONS BOOK.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT	5	DETAIL	NΩ	¥720020
DISTRICT	J	DEIAIL	NU.	A/ZUUZU

BFW BACON | FARMER | WORKMAN ENGINEERING & TESTING. INC.

SER NAME = sclyne DESIGNED -REVISED -11/06 DRAWN REVISED -12/20 CHECKED REVISED PLOT DATE = 8/15/2024 REVISED DATE

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** WIDTH RESTRICTION SIGNING

SECTION (14HB)BR,(14B)BR-1 CHAMPAIGN 90 90 CONTRACT NO. 70D76

SHEET 1 OF 1 SHEETS STA. TO STA.