

**IDOT PROJECT LABOR AGREEMENT DETERMINATION**

To: Erica J Borggren, Acting Secretary  
From: Omer Osman, Director  
Date: May 21, 2014  
Re: IL 16 Turnlanes & Resurfacing, Contract Number 74423, Coles County

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In accordance with the Project Labor Agreement Act (30 ILCS 571), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency)
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project

IL 10 - Runways & Resurfacing  
Contract Number 74423  
Coles County  
Item

8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project

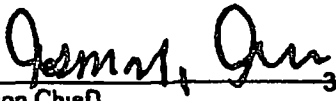
10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).

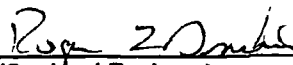
12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:


\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed:  8/4/14  
{Division Chief} AAW (Date)

Agreed:  7/21/2014  
{Bureau of Design & Environment} (Date)

Agreed  6/23/14  
{Regional Engineer} (Date)

Approved:  8/7/14  
Erica J. Borggren, Acting Secretary (Date)

FHWA concurrence in the PLA for the above mentioned contract

|   |           |
|---|-----------|
| Thomas P. Holian  | 6/18/2014 |
| FHWA Acting Chief Counsel (see 6/18 approval memo re 74423) |           |

**Attachment A:**

Justification for the use of Project Labor Agreement on Contract # 74423, Coles County

The use of a Project Labor Agreement on this project is consistent with all State and Local statutory and regulatory requirements

Item 2 This project is federally funded with a state match.

Item 3: The Estimated Cost of this Project is \$3,000,000.00

The work on this project is located at the intersection of IL 16 and Harrison Ave east of Charleston. The average daily traffic at this location is 4,100 with 6.8% (280) trucks. In order to complete the mainline IL route 16 turn lanes and resurfacing project without any adverse traffic delays for the traveling public or delays in commerce, IDOT feels a Project Labor Agreement is necessary for this project.

The work on this project consists of clearing and grubbing, tree removal, earth excavation, furnished excavation, pavement patching, milling, placement of hot-mix asphalt binder and surface course, aggregate and hot-mix asphalt shoulders, hot-mix asphalt base course, pavement removal, seeding, pavement marking, and any other work necessary to complete this section. With the large number of different construction trades that will be involved in the rehabilitation of this section of roadway, IDOT feels the use of a Project Labor Agreement is necessary to avoid any labor related delays.

The turn lane & resurfacing work involves various construction sequences creating numerous traffic control challenges. The contractor will need a reliable work force to complete the work in a timely manner which will maintain safe travel for the traveling public during all stages of traffic control. It is IDOT's findings that the large skilled workforce needed to meet all the challenges with this project can be provided by the union trades involved and is a necessary requirement for a Project Labor Agreement.

With no construction in the project limits the speed limit is 55 MPH. Once the project limits are under construction the speed limit will be 45 MPH. Based on user cost of \$10.00/hour for passenger vehicles and \$20.00/hour for trucks, a user cost of \$377/day will incur due to any work stoppages. Based on the adverse travel due to construction zones, IDOT feels the use of a Project Labor Agreement is necessary to avoid any labor related delays.

Item 5. There are 150 working days to complete this project. This job is on an August letting which is a late letting so it is possible for the project to be extended into 2 possible 3 construction seasons, so any labor issues could extend the project even longer. A Project Labor Agreement would ensure the stability of the work force allowing the work to be completed in the time allotted.

Item 6: This project could extend beyond the expiration date of several collective bargaining agreements listed with the following locals

Operators Local 956 Ex. 4-30-2015

Iron Workers Local 46 Ex. 5-1-2016

Concrete Finishers Local 143 Ex. 3-31-2016

Carpenters Local 347 Ex. 7-31-2016

| <b>Work Zone User cost calculations</b>           |                    |             |
|---|--------------------|-------------|
| <b>Project Number:</b> 74423 IL 16 Turnlane&Resur |                    | 5/21/2014   |
| <b>CRS:</b> 4.6                                   |                    |             |
| <b>Detour cost calculation procedure</b>          |                    |             |
|   | Passenger Car      | B/C Truck   |
| <b>Cost per hour:</b>                             | \$10.00            | \$20.00     |
| <b>Length of work zone in miles:</b>              | 2                  | 2           |
| <b>Length of Detour in miles:</b>                 | 2                  | 2           |
| <b>Free flow speed (normal 85% speed) in mph:</b> | 55                 | 55          |
| <b>Detour zone speed (85%) in mph:</b>            | 45                 | 45          |
| <b>Average AADT of full section:</b>              | 4100               | 280         |
| <b>Duration of Closure in days</b>                | 150                | 150         |
| <b>Calculated values:</b>                         |                    |             |
| <b>Travel time in free flow (secs):</b>           | 130.9090909        | 130.9090909 |
| <b>Travel Time in detour (secs):</b>              | 160                | 160         |
| <b>Delay (secs):</b>                              | 29                 | 29          |
| <b>Delay (hours):</b>                             | 0.008080808        | 0.008080808 |
| <b>Cost per Vehicle:</b>                          | \$0.08             | \$0.16      |
| <b>Cost per day per closure:</b>                  | \$331.31           | \$45.25     |
| <b>Total Cost for closure duration:</b>           | \$49,696.97        | \$6,787.88  |
| <b>Total Cost for all vehicles:</b>               | <b>\$56,484.85</b> |             |
| <b>Average cost per day</b>                       | <b>\$376.57</b>    |             |

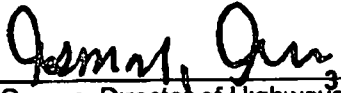
The Average cost per day is the MAXIMUM that may be used as incentive / disincentive

Spreadsheet protection password CONSTRUCTION

See Contact Reynaldo Stargell in the Office of Traffic Engineering for the latest adjustment factor based on the CPI [ftp://ftp.bls.gov/pub/special requests/cpi/cpia1.txt](ftp://ftp.bls.gov/pub/special%20requests/cpi/cpia1.txt)


Execution Page

Illinois Department of Transportation

  
\_\_\_\_\_  
Omer Osman, Director of Highways AAW

  
\_\_\_\_\_  
Tony Small, Director of Finance & Administration

  
\_\_\_\_\_  
Michael A Forti, Chief Counsel

  
\_\_\_\_\_  
Erica J Borggren, Acting Secretary

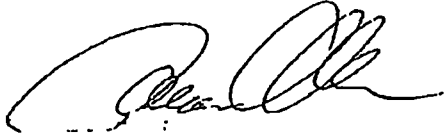
8/7/14  
\_\_\_\_\_  
(Date)

Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the local unions listed below:

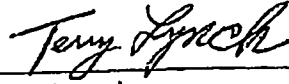
  
\_\_\_\_\_  
Michael T. Parrigan

7-16-14  
\_\_\_\_\_  
(Date)

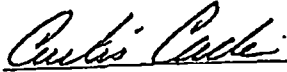
List Union Locals:



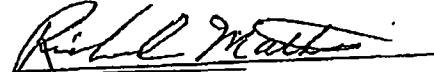
Jim Allen  
Bricklayers



Terry Lynch  
Heat & Frost Insulators & Allied  
Workers



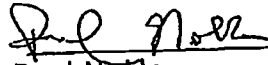
Curtis Cade  
United Association



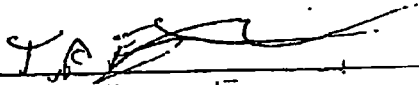
Richard Mathis  
Roofers

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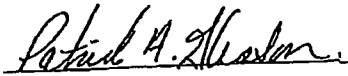
Ed Christensen, Elevator  
Constructors



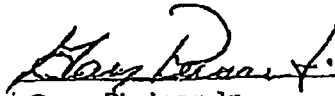
Paul Noble  
IBEW



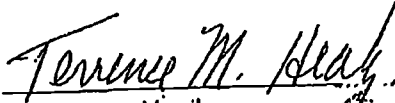
Terry Fitzmaurice  
Painters



Pat Gleason  
Teamsters



Gary Perinar Jr.  
Carpenters



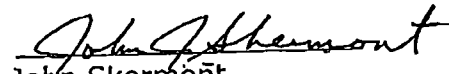
Terrence Healy  
LIUNA



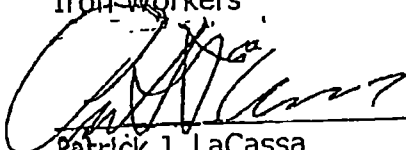
Brian Mulheran  
Sheet Metal Workers



David Beard  
Iron Workers



John Skermont  
Boilermakers



Patrick J. LaCassa  
OPCMIA

\*only If Elevator Constructors master agreement  
language is attached to PLA