

ITEM 08A
 LETTING DATE: NOVEMBER 17, 2023

TOTAL SHEETS: 14

CONTRACT NO. PE003

CONSTRUCTION PLANS FOR GENERAL WAYNE A. DOWNING PEORIA INTERNATIONAL AIRPORT PEORIA, IL

REHABILITATE HANNA CITY T-HANGAR TAXIWAY PAVEMENTS - PHASE 2

IL. PROJ. NO: PIA-4989

AIP PROJ. NO: N/A

100%

SEPTEMBER 22, 2023

Sheet List Table	
Sheet Number	Sheet Title
GI001	COVER SHEET
GI101	AIRPORT SITE PLAN
GC001	CONSTRUCTION ACTIVITY NOTES 1
GC002	CONSTRUCTION ACTIVITY NOTES 2
GC003	CONSTRUCTION ACTIVITY DETAILS
GC101	CONSTRUCTION ACCESS
GC102	CONSTRUCTION ACTIVITY PLAN
CD101	EXISTING CONDITIONS & REMOVALS
CP101	HORIZONTAL CONTROL PLAN
CP102	PROPOSED IMPROVEMENTS
CP301	TYPICAL SECTIONS
CP501	PAVING & MISC DETAILS
CS101	STAKING PLAN
CM101	MARKING PLAN

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR150520	MOBILIZATION	L SUM	1
AR152515	SUBGRADE UNDERCUT	CU YD	350
AR201660	BITUMINOUS CRACK REPAIR	FOOT	1000
AR201670	CRACK CONTROL FABRIC	SQ YD	710
AR401610	BITUMINOUS SURFACE COURSE	TON	1550
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	12600
AR603510	BITUMINOUS TACK COAT	GALLON	2000
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	4550
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	305

811 Know what's below.
 Call before you dig.
 COMMON GROUND ALLIANCE
 www.call811.com or
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

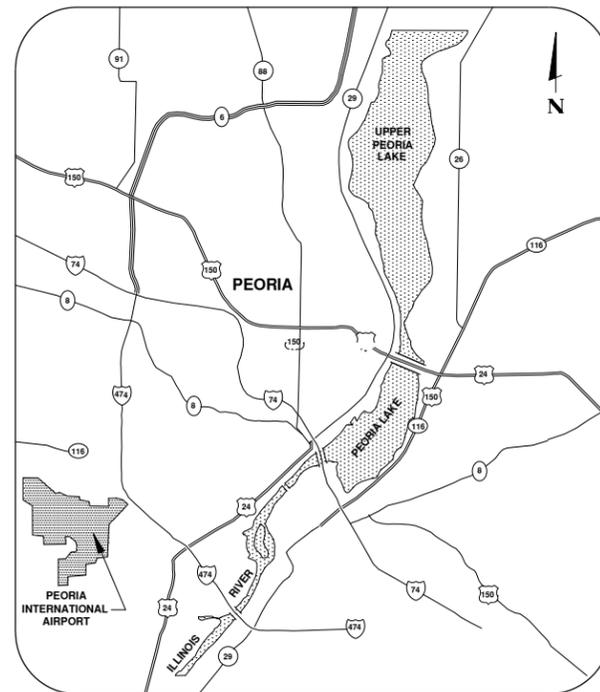
GEOMETRIC CRITERIA

AIRPLANE DESIGN GROUP I
 TAXIWAY DESIGN GROUP IA
 RWY 13/31 RUNWAY SAFETY AREA (RSA) = 500'
 RWY 4/22 RUNWAY SAFETY AREA (RSA) = 500'
 TXY SAFETY AREA (TSA) = 171'
 TXY OBJECT FREE AREA (TOFA) = 243'

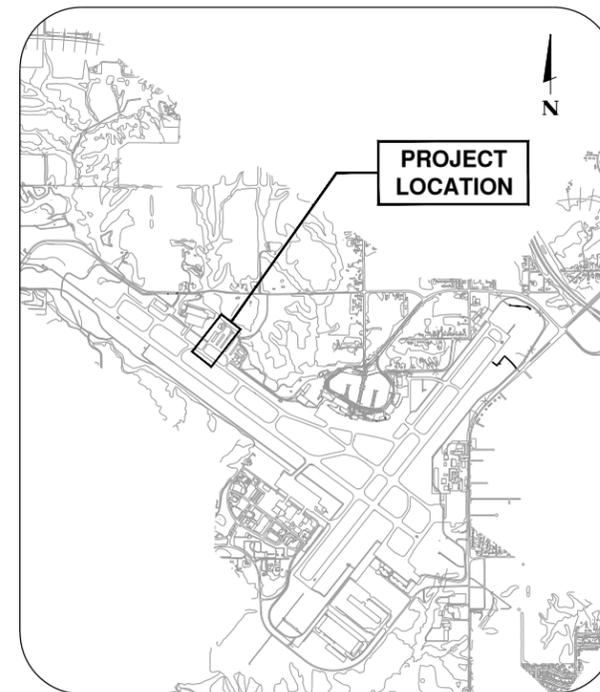
**MAXIMUM EQUIPMENT HEIGHT = 25'
 GROUND FREQUENCY 121.85**

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT

TOWNSHIP: 8 NORTH
 RANGE: 7 EAST OF THE 4TH P.M.
 SECTION: 22
 COUNTY: PEORIA
 CIVIL TOWNSHIP: LIMESTONE



LOCATION MAP



SITE PLAN

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**



APPROVED *[Signature]*
 DATE OCTOBER 13, 2023



License No. 184-000613 © Copyright CMT, Inc.

SUBMITTED BY *[Signature]*
 DATE OCTOBER 13, 2023

CMT JOB NUMBER: 21001963

K:\Peoria\PIA\21001963-00_RehabHannaCity\Draw\Sheets
 FILE: 21001963-GI001.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 10/12/2023 10:24 AM

Path: K:\Peoria\A\21001963-00_RehabHannaCity\Draw\Sheets\21001963_GC000.dwg
Date: Thursday, October 12, 2023 10:24:26 AM

0. GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2 (LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.
- WORK SHALL BE COMPLETED IN 51 CALENDAR DAYS (BASE BID)
- CONSTRUCTION WILL NOT BEGIN UNTIL APRIL 1ST 2024 OR LATER AT AN AGREED UPON DATE WITH THE AIRPORT & CONTRACTOR.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

3. CONSTRUCTION ACTIVITY AREAS

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS ALLOWED IN THE PLANS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN IN THE PLANS.

4. PROTECTION OF NAVIGATIONAL AIDS

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAID FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN IN THE PLANS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED & LOCKED DURING WORK HOURS OR THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO STRICTLY CONTROL ACCESS THROUGH THE GATE(S) IN ACCORDANCE WITH THE AIRPORT'S SECURITY PROCEDURES. THE CONTRACTOR SHALL IMMEDIATELY REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.

5. CONTRACTOR ACCESS (CONTINUED)

- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T. THE CONTRACTOR SHALL ENTER A ROAD USE AGREEMENT WITH THE ROAD DISTRICT OF PEORIA COUNTY, ILLINOIS.
- CONTRACTOR EMPLOYEES WILL BE REQUIRED TO OBTAIN AN AIRPORT SECURITY IDENTIFICATION BADGE AS REQUIRED BY THE AIRPORT. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH OBTAINING AN IDENTIFICATION BADGE.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. THE CONTRACTOR WILL NEED AN APPROPRIATE NUMBER OF EMPLOYEES BADGED WITH AIRFIELD DRIVING PRIVILEGES. THIS WILL BE NEEDED TO CLOSE AIRFIELD PAVEMENT AND BE ABLE TO ACCESS ACTIVE AIRFIELD MOVEMENT AREAS. AIRFIELD DRIVING PRIVILEGES WILL TAKE APPROXIMATELY 30 HOURS TO OBTAIN. THE CONTRACTOR WILL ALSO NEED AN APPROPRIATE NUMBER OF EMPLOYEES BADGED WITH DRIVING PRIVILEGES IN CLOSED CONSTRUCTION AREAS WITHIN THE AIRFIELD FENCE. THE CONTRACTOR SHALL MAINTAIN A SUFFICIENT NUMBER OF BADGED EMPLOYEES IN EACH WORK AREA TO ACT AS AN ESCORT WITH DIRECT OVERSIGHT OF UNBADGED WORKERS. A BADGED EMPLOYEE ON FOOT SHALL NOT ESCORT AN UNBADGED EMPLOYEE IN A VEHICLE. CONSTRUCTION AREA DRIVING PRIVILEGES WILL TAKE APPROXIMATELY 8 HOURS TO OBTAIN. ONLY THOSE INDIVIDUALS WHO RECEIVE THESE DESIGNATIONS WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT.
- CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER THE PLAN DETAILS AND SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) WILL NOT NEED TO OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. THE GATE GUARD MUST CHECK TO ENSURE THAT ALL DRIVERS ENTERING THE JOB SITE ARE ON THIS LIST EVERY TIME A VEHICLE ATTEMPTS TO ENTER THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE UNDER THE CONTROL OF AND SUPERVISED BY THE CONTRACTOR.
- CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH AIR TRAFFIC AT ALL TIMES WHEN WITHIN THE MOVEMENT AREA. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY PASSED THE APPROVED AIRPORT TESTS MAY OPERATE THESE RADIOS.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH AIR TRAFFIC. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE RETURNED TO & STORED AT THE STAGING AREA.
- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT IF ANY WILDLIFE IS SEEN ON OR ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED & LOCKED WHEN THE CONTRACTOR IS NOT WORKING. ANY JOBSITE FENCING IS TO BE INSPECTED & MAINTAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
- THE CONTRACTOR SHALL CONTROL GRASS HEIGHTS THROUGH MOWING UNTIL ACCEPTANCE OF PROJECT TO ASSIST WITH WILDLIFE CONTROL.
- THE CONTRACTOR SHALL TAKE NECESSARY METHODS TO PREVENT & REMOVE THE ACCUMULATION OF STANDING WATER ON THE JOBSITE. STANDING WATER SHALL NOT EXIST FOR MORE THAN 48 HOURS.

7. FOREIGN OBJECT DEBRIS MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN INSIDE THE AIRFIELD FENCE.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF SAFETY DATA SHEETS (SDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE AIRPORT PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911, THEN CALL THE AIRPORT OPERATIONS AT 309-303-1001.
- CONTRACTOR SHALL NOTIFY THE ARFF PERSONNEL THROUGH THE AIRPORT DIRECTOR OF OPERATIONS WHEN THE CONTRACTORS EQUIPMENT AND PERSONNEL WILL CAUSE POTENTIAL REROUTING, BLOCKING, AND RESTORATION OF EMERGENCY ACCESS ROUTES. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW:

<u>DIRECTOR OF OPERATIONS</u> RANDY HURST	309-303-0994
<u>AIRPORT MAINTENANCE</u> GREG HUSER	309-303-1005
<u>AIRPORT OPERATIONS</u> OPS CELL	309-303-1001
<u>ENGINEER</u> LUKE WOJCIK/ CMT - RESIDENT ENGINEER TBD	314-393-1709

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 (LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST AN OPERATIONAL INSPECTION WITH THE AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED FOR AIRCRAFT USE. AIRPORT OPERATIONS SHALL MAKE THE FINAL DETERMINATION OF THE ACCEPTANCE TO REOPEN THIS WORK AREA. THIS WILL NOT BE CONSIDERED AS FINAL OR PARTIAL ACCEPTANCE OF THE WORK IN THIS PHASE.

5. CONTRACTOR ACCESS (CONTINUED)

- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR AND OBTAIN AN IDENTIFICATION BADGE.
- CONTRACTOR SHALL NOT PARK EQUIPMENT WITHIN 10' OF THE PERIMETER FENCE.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT IF ANY WILDLIFE IS SEEN ON OR ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED & LOCKED WHEN THE CONTRACTOR IS NOT WORKING. ANY JOBSITE FENCING IS TO BE INSPECTED & MAINTAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.
- THE CONTRACTOR SHALL CONTROL GRASS HEIGHTS THROUGH MOWING UNTIL ACCEPTANCE OF PROJECT TO ASSIST WITH WILDLIFE CONTROL.
- THE CONTRACTOR SHALL TAKE NECESSARY METHODS TO PREVENT & REMOVE THE ACCUMULATION OF STANDING WATER ON THE JOBSITE. STANDING WATER SHALL NOT EXIST FOR MORE THAN 48 HOURS.

7. FOREIGN OBJECT DEBRIS MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN INSIDE THE AIRFIELD FENCE.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF SAFETY DATA SHEETS (SDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE AIRPORT PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911, THEN CALL THE AIRPORT OPERATIONS AT 309-303-1001.
- CONTRACTOR SHALL NOTIFY THE ARFF PERSONNEL THROUGH THE AIRPORT DIRECTOR OF OPERATIONS WHEN THE CONTRACTORS EQUIPMENT AND PERSONNEL WILL CAUSE POTENTIAL REROUTING, BLOCKING, AND RESTORATION OF EMERGENCY ACCESS ROUTES. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW:

<u>DIRECTOR OF OPERATIONS</u> RANDY HURST	309-303-0994
<u>AIRPORT MAINTENANCE</u> GREG HUSER	309-303-1005
<u>AIRPORT OPERATIONS</u> OPS CELL	309-303-1001
<u>ENGINEER</u> LUKE WOJCIK/ CMT - RESIDENT ENGINEER TBD	314-393-1709

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 (LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL REQUEST AN OPERATIONAL INSPECTION WITH THE AIRPORT OPERATIONS THROUGH THE RESIDENT ENGINEER OF EACH PHASE WORK AREA PRIOR TO THE AREA BEING REOPENED FOR AIRCRAFT USE. AIRPORT OPERATIONS SHALL MAKE THE FINAL DETERMINATION OF THE ACCEPTANCE TO REOPEN THIS WORK AREA. THIS WILL NOT BE CONSIDERED AS FINAL OR PARTIAL ACCEPTANCE OF THE WORK IN THIS PHASE.

10. INSPECTION REQUIREMENTS (CONT.)

- THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A "SUBSTANTIALLY COMPLETE" LETTER WHEN HE FEELS THE PROJECT IS COMPLETE AND AT THE POINT OF HAVING FINAL ACCEPTANCE. UPON THE RESIDENT ENGINEERS RECOMMENDATION TO IDA, A FINAL ACCEPTANCE OF THE PROJECT WILL THEN BE SCHEDULED.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. THE AIRPORT, AND CONTACT THE LOCAL FAA OFFICE (309-697-1363) TO ARRANGE FOR UTILITY LOCATES.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP, THE CONTRACTOR'S APPROVED SPCD OR THE SECURITY PLAN MAY RESULT IN FINES AS ALLOWED BY LAW.
- FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY & TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE PLANS.
- NO RUNWAY CLOSURES WILL BE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF THE PLANS & FAA AC 150/5370-2 (LATEST VERSION.)
- IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE, THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED RUNWAYS AND TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVER OR DE-ENERGIZE AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

15. MARKINGS AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED IN THE PLANS.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D (OR LATEST) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE AIRPORT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

(NOTES CONTINUE ON SHEET GC002)



License No. 184-000613

CONSULTANTS

100%
SEPTEMBER 22, 2023

**REHABILITATE HANNA CITY
T-HANGAR TAXIWAY
PAVEMENTS - PHASE 2**

OWNER

**GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT**

**GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, IL**

MARK	DATE	DESCRIPTION

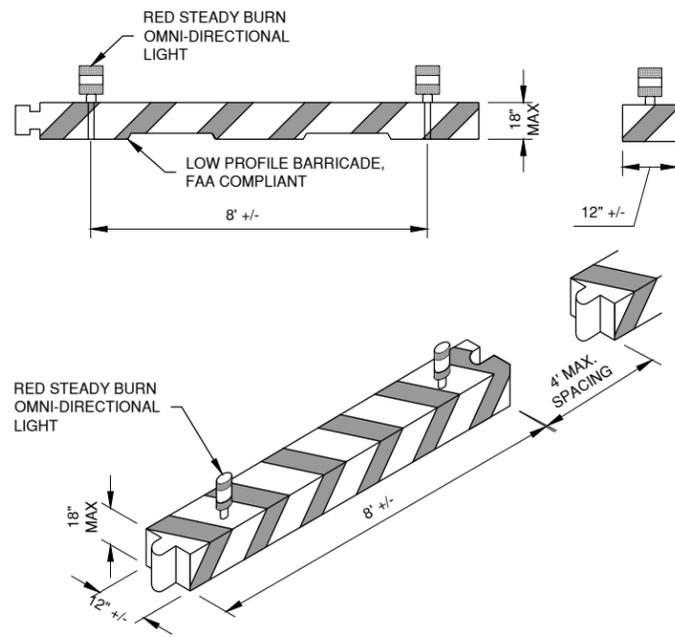
AIP PROJ. NO: N/A
IL PROJ. NO: PIA-4989
CMT PROJECT NO: 210019-63
CAD DWG FILE: 21001963-GC000.DWG
DESIGNED BY: MRK
DRAWN BY: MRK
CHECKED BY: LEW
APPROVED BY: CBG
COPYRIGHT:

SHEET TITLE

**CONSTRUCTION
ACTIVITY NOTES 1**

GC001

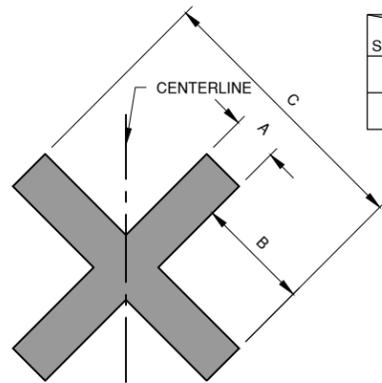
SHEET **3** OF **14**



1 BEAM BARRICADE DETAIL
N.T.S.

BEAM BARRICADE NOTES

- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
- BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
- BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



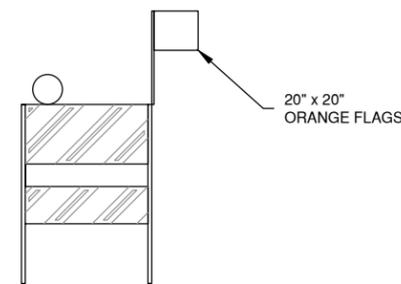
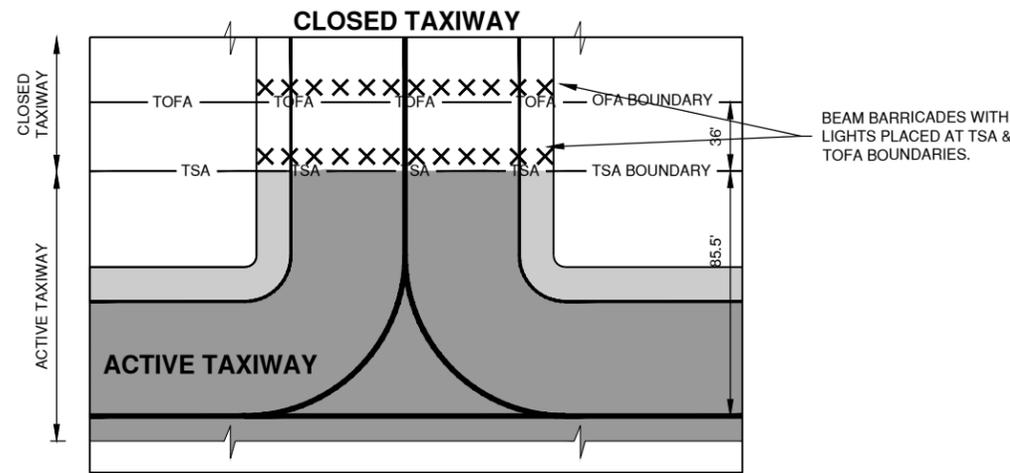
4 NON-LIGHTED CLOSURE MARKER
N.T.S.

NOTES

- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
- MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.

DIMENSION	A	B	C
CLOSED TAXIWAY	5'-0"	12'-6"	30'-0"
CLOSED RUNWAY	10'-0"	25'-0"	60'-0"

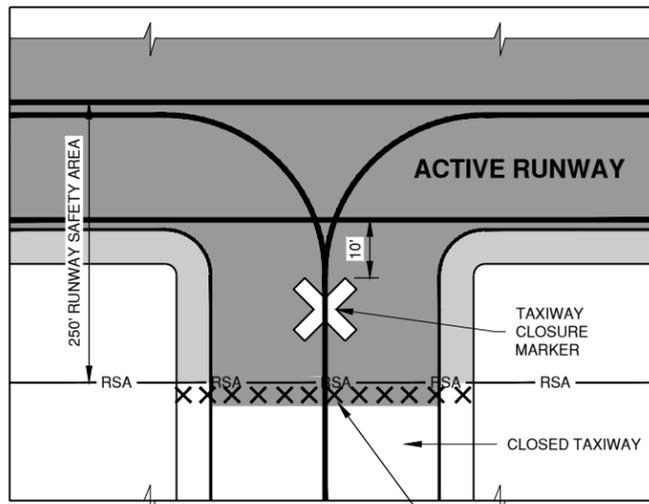
2 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.



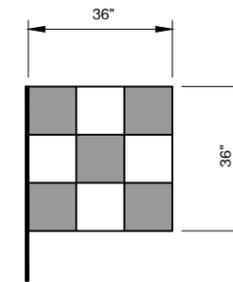
5 FLASHER BARRICADE DETAIL - IDOT TYPE 1
N.T.S.

FLASHER BARRICADE NOTES

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT 20' INTERVALS.



6 CLOSED TAXIWAY/ACTIVE RUNWAY BARRICADE DETAIL
N.T.S.



3 EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

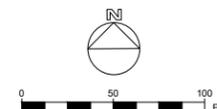
- ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG OR A ROTATING/FLASHING AMBER BEACON.
- WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
- CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

- THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
- THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
- IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT IS THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
- WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
- DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL PERIMETER ROADS INSIDE THE FENCE SHALL REMAIN ACCESSIBLE TO THE AIRPORT.
- THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENTANT AND CONTRACTOR OPERATIONS.
- CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE AIRPORT SITE PLAN.
- THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATE WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
- CONTRACTOR SHALL SUPPLY RAMPS (RUBBER OR OTHER SUITABLE MATERIAL) TO BE USED IF AN AIRCRAFT NEEDS TO BE MOVED OVER MILLED SURFACE. THE RAMP SHOULD BE 2" ON ONE SIDE AND 0" ON THE OTHER.

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	N/A
IL PROJ. NO:	PIA-4989
CMT PROJECT NO:	210019-63
CAD DWG FILE:	21001963-GC000.DWG
DESIGNED BY:	MRK
DRAWN BY:	MRK
CHECKED BY:	LEW
APPROVED BY:	CBG
COPYRIGHT:	



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100%
SEPTEMBER 22, 2023

REHABILITATE HANNA CITY
T-HANGAR TAXIWAY
PAVEMENTS - PHASE 2

OWNER



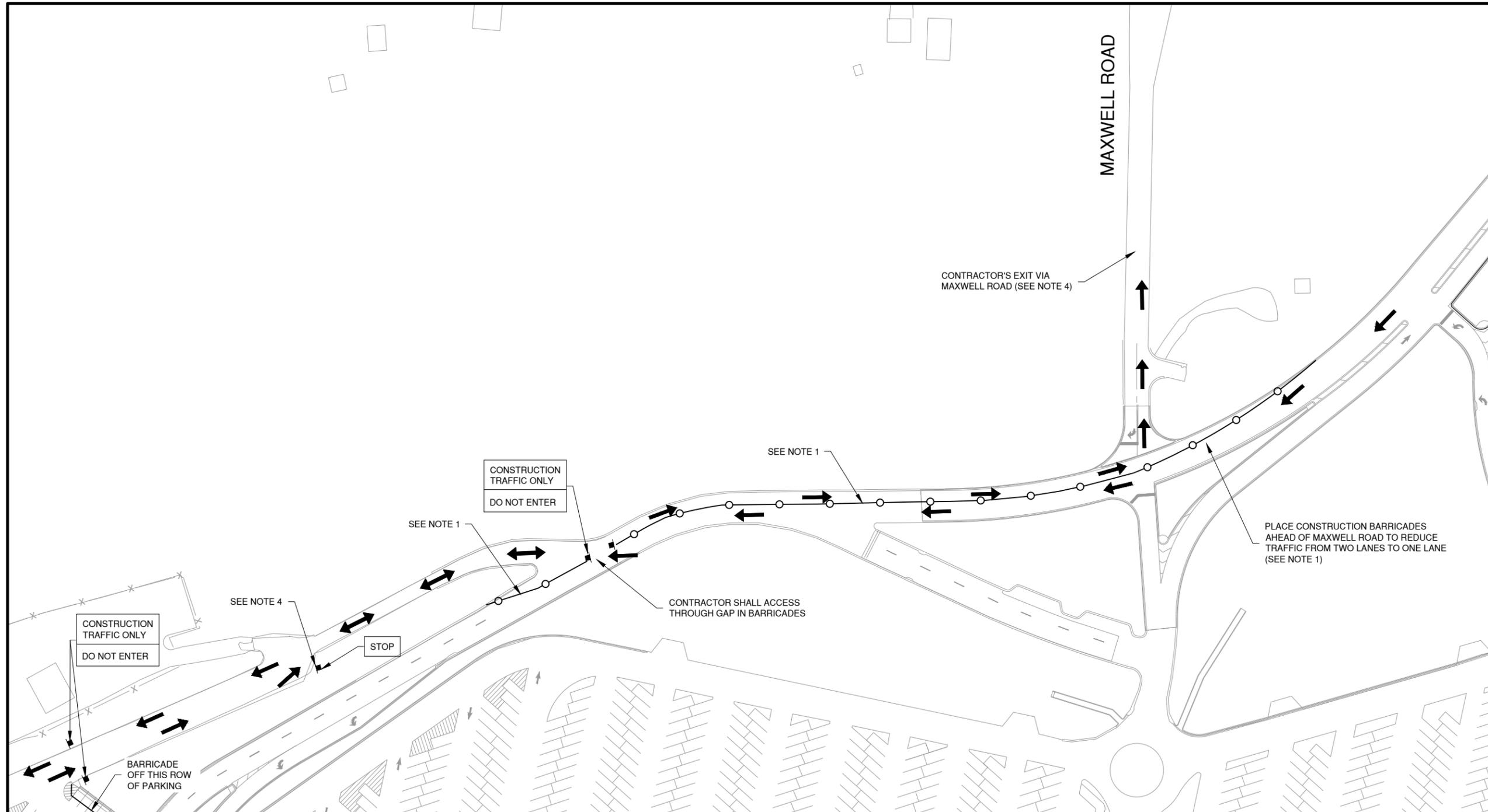
GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	N/A
IL PROJ. NO:	PIA-4989
CMT PROJECT NO:	210019-63
CAD DWG FILE:	21001963-GC100.DWG
DESIGNED BY:	HWI
DRAWN BY:	DPA
CHECKED BY:	LEW
APPROVED BY:	CBG
COPYRIGHT:	

SHEET TITLE
**CONSTRUCTION
ACCESS**

GC101
SHEET 6 OF 14



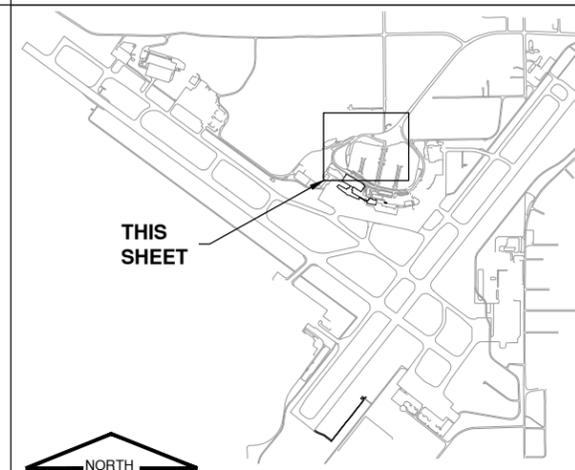
NOTES

1. PLACEMENT OF BARRICADES AND TRAFFIC SIGNAGE NECESSARY FOR THE REDUCTION OF TRAFFIC FROM TWO LANES TO ONE LANE SHALL MEET IDOT STANDARD REQUIREMENTS FOR LONG TERM LANE CLOSURES.
2. CONTRACTOR SHALL USE EXISTING TEMPORARY ACCESS ROAD IN PLACE. CONTRACTOR SHALL MAINTAIN THIS ROAD THROUGHOUT THE DURATION OF THIS PROJECT. THIS ROAD IS TO BE USED BY PUBLIC/NON-CONSTRUCTION TRAFFIC ONLY.
3. THE SIGNAGE SHOWN ON THIS SHEET SHALL BE IN ADDITION TO ALL TEMPORARY CONSTRUCTION TRAFFIC SIGNAGE REQUIRED BY IDOT STANDARDS FOR LANE CLOSURES WHICH ARE NOT SHOWN.
4. CONTRACTOR SHALL PLACE A TEMPORARY STOP SIGN DIRECTING ALL CONSTRUCTION TRAFFIC EXITING THE SITE TO STOP AND ALLOW ALL ONCOMING CONSTRUCTION TRAFFIC TO PROCEED PRIOR TO PROCEED THROUGH THE 2-WAY 1-LANE ZONE. THE CONTRACTOR SHALL USE EXTREME CAUTION IN THIS AREA AND SHALL REQUIRE COORDINATED COMMUNICATION WITH ALL INDIVIDUALS USING THIS ACCESS.
5. CONTRACTOR SHALL PROVIDE SIGNAGE TO TEMPORARILY CLOSE AIRPORT ACCESS FOR THE GENERAL PUBLIC FROM MAXWELL ROAD. MAXWELL ROAD SHALL BE CLOSED TO THRU TRAFFIC IN ACCORDANCE WITH IDOT STANDARD 701901.
6. MAINTENANCE OF TRAFFIC CONTROL ITEMS AND TEMPORARY ROAD MILLINGS SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT THE PROJECT DURATION AND SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.

LEGEND

- TEMPORARY PUBLIC PARKING ACCESS
- CONTRACTOR'S ACCESS
- CONSTRUCTION BARRICADES
- TEMPORARY SIGNAGE

KEY MAP

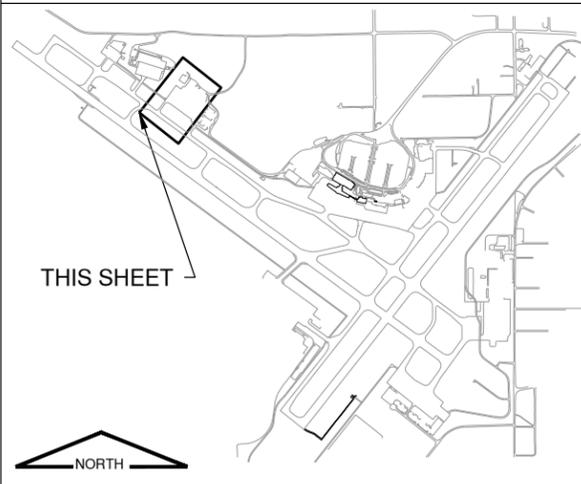


Path: K:\Peoria\AIP\21001963-00_RehabHannaCity\Draw\Sheets\21001963_GC100.dwg
Date: Thursday, October 12, 2023 10:24:52 AM

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	CONTROL LINE	STA. 10+00.00	N:1458625.9872 E:2424941.7209	STA. 19+00.00	N: 1458094.3855 E: 2425667.9437

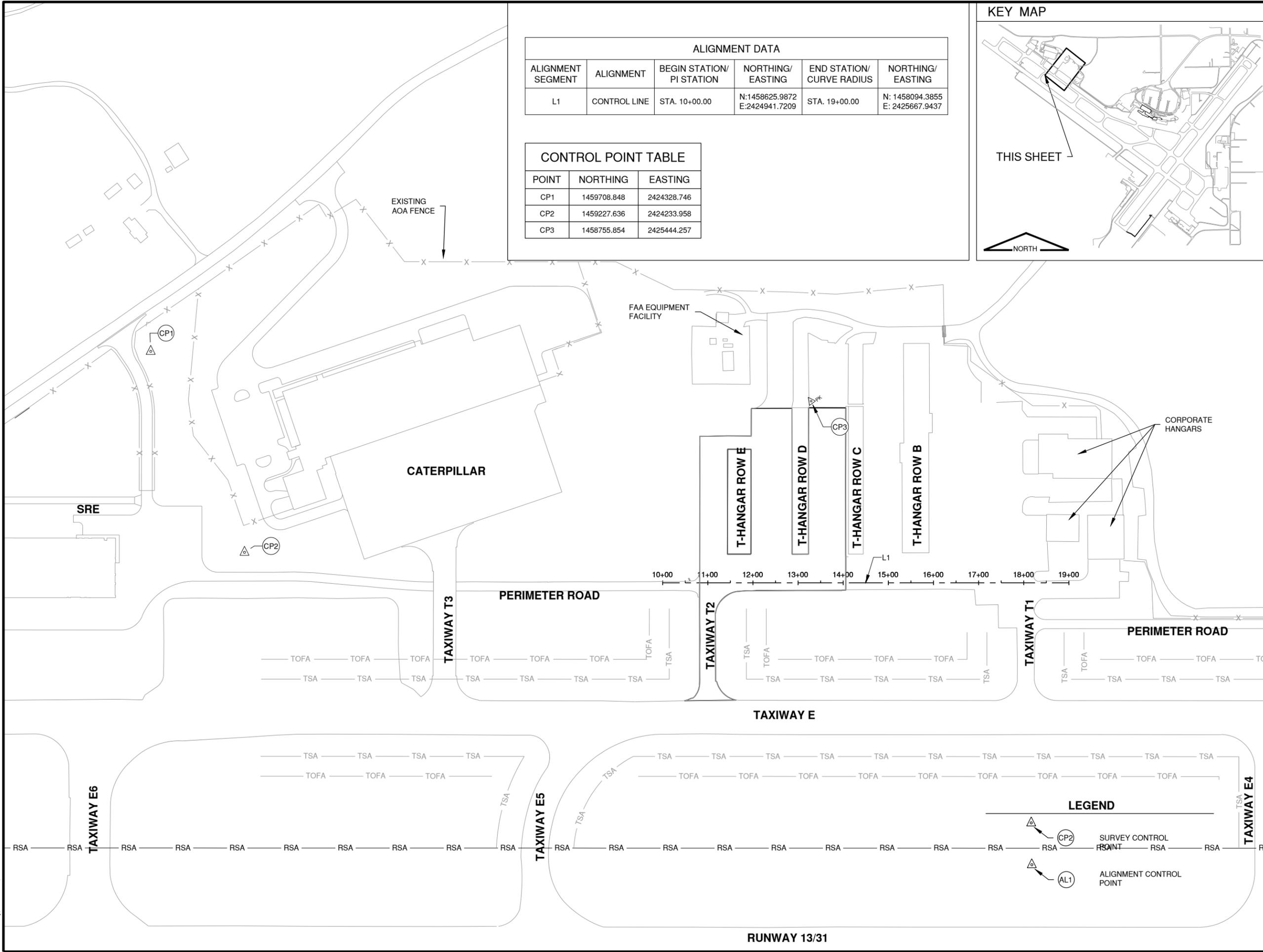
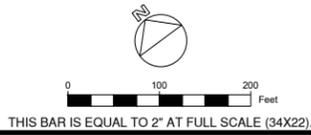
CONTROL POINT TABLE		
POINT	NORTHING	EASTING
CP1	1459708.848	2424328.746
CP2	1459227.636	2424233.958
CP3	1458755.854	2425444.257

KEY MAP



License No. 184-000613

CONSULTANTS



100%
SEPTEMBER 22, 2023

REHABILITATE HANNA CITY
T-HANGAR TAXIWAY
PAVEMENTS - PHASE 2

OWNER



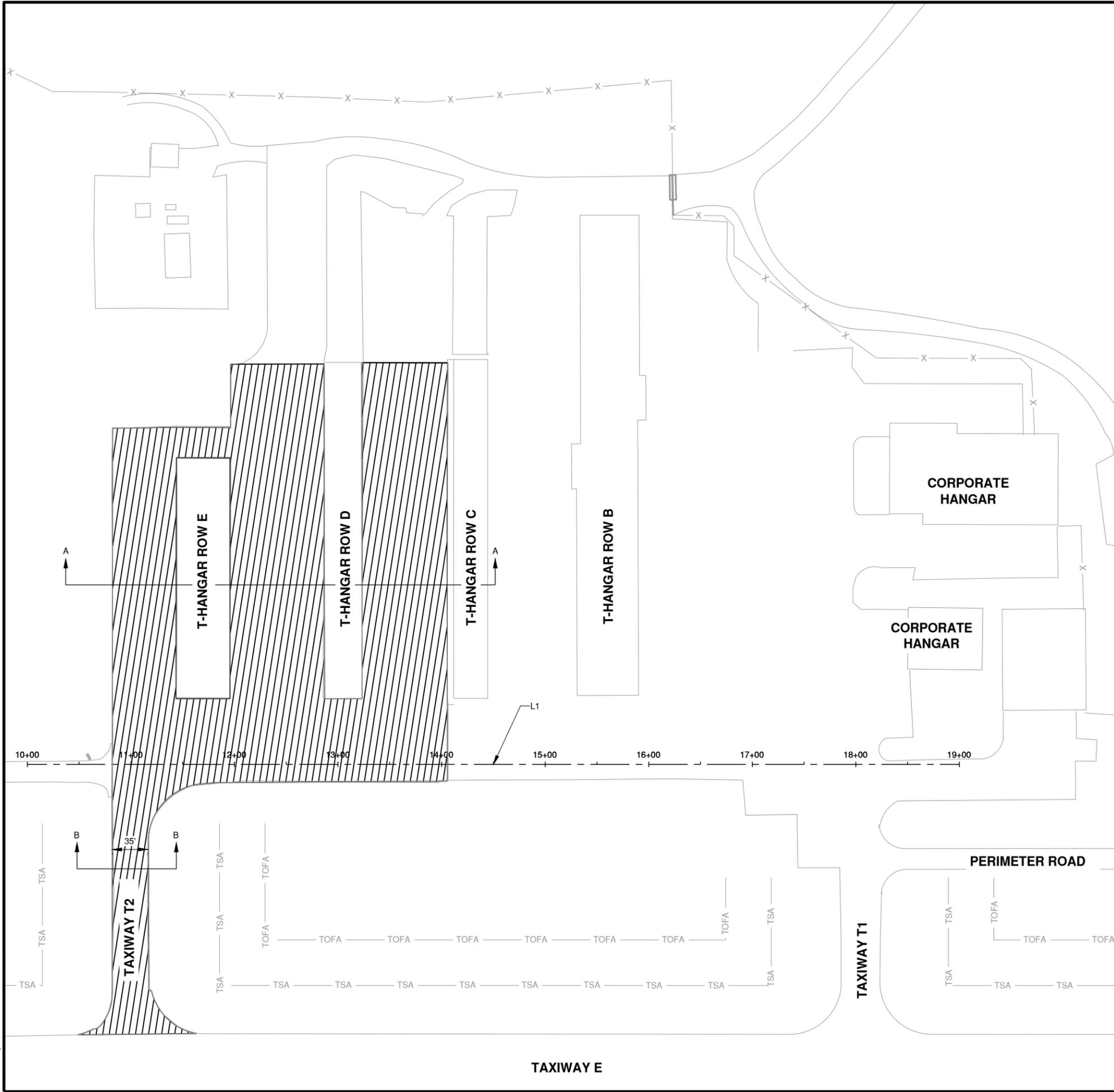
GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, IL

MARK	DATE	DESCRIPTION

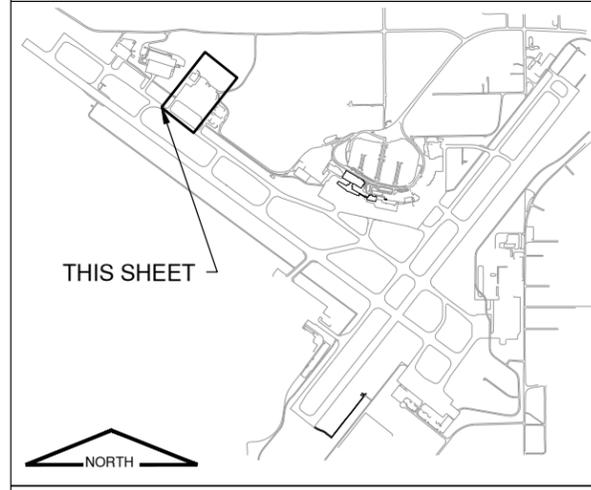
AIP PROJ. NO:	N/A
IL PROJ. NO:	PIA-4989
CMT PROJECT NO:	210019-63
CAD DWG FILE:	21001963-CP100.DWG
DESIGNED BY:	MRK
DRAWN BY:	MRK
CHECKED BY:	LEW
APPROVED BY:	CBG
COPYRIGHT:	

SHEET TITLE
**HORIZONTAL
CONTROL PLAN**

CP101
SHEET 9 OF 14



KEY MAP



LEGEND

 MILL 2" BITUMINOUS SURFACE
 NEW 2" BITUMINOUS SURFACE

NOTES

- 1. REFER TO SHEET CP301 FOR TYPICAL SECTIONS

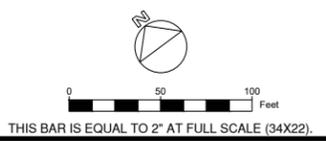
UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



License No. 184-000613
 CONSULTANTS



100%
 SEPTEMBER 22, 2023

REHABILITATE HANNA CITY
 T-HANGAR TAXIWAY
 PAVEMENTS - PHASE 2



GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL
 AIRPORT
 PEORIA, IL

MARK	DATE	DESCRIPTION

PROPOSED IMPROVEMENTS

Path: K:\Peoria\A\21001963-00_RehabHannaCity\Ph2\Draw\Sheets\21001963-CP100.dwg
 Date: Thursday, October 12, 2023 10:25:24 AM

100%
SEPTEMBER 22, 2023

REHABILITATE HANNA CITY
T-HANGAR TAXIWAY
PAVEMENTS - PHASE 2

OWNER



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: N/A

IL PROJ. NO: PIA-4989

CMT PROJECT NO: 210019-63

CAD DWG FILE: 21001963-CP300.DWG

DESIGNED BY: MRK

DRAWN BY: MRK

CHECKED BY: LEW

APPROVED BY: CBG

COPYRIGHT:

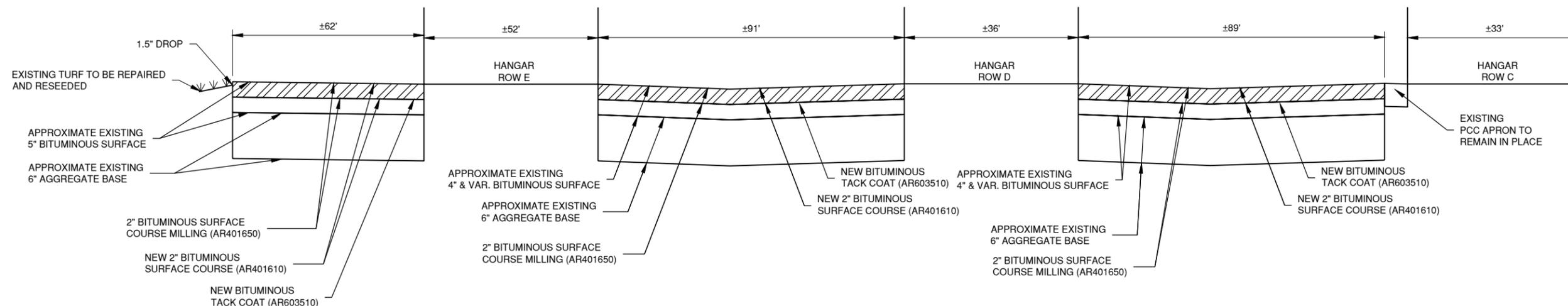
SHEET TITLE

TYPICAL SECTIONS

CP301
SHEET 11 OF 14

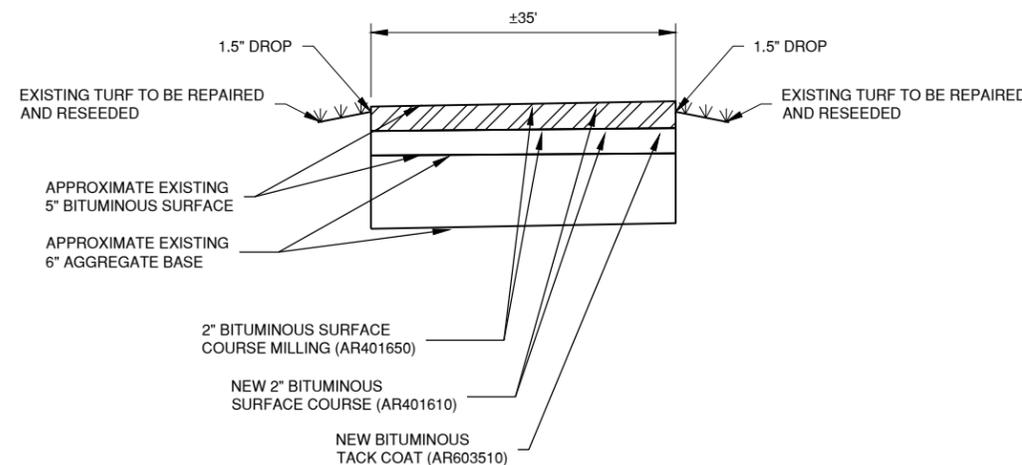
SECTION A-A

N.T.S.



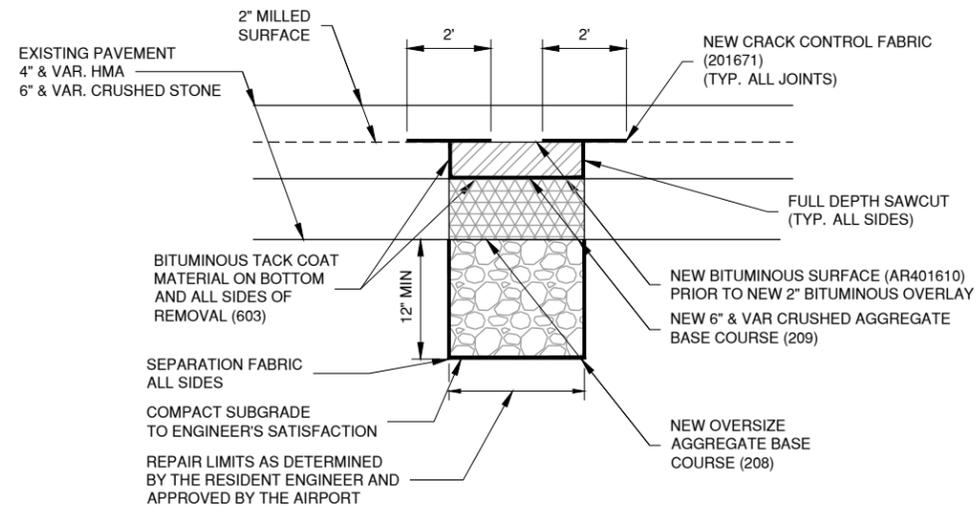
SECTION B-B

N.T.S.



NOTES:

BITUMINOUS TACK COAT SHALL BE USED BETWEEN ALL LIFTS OF BITUMINOUS SURFACE COURSE AND ON ALL VERTICAL ABUTTING SURFACES

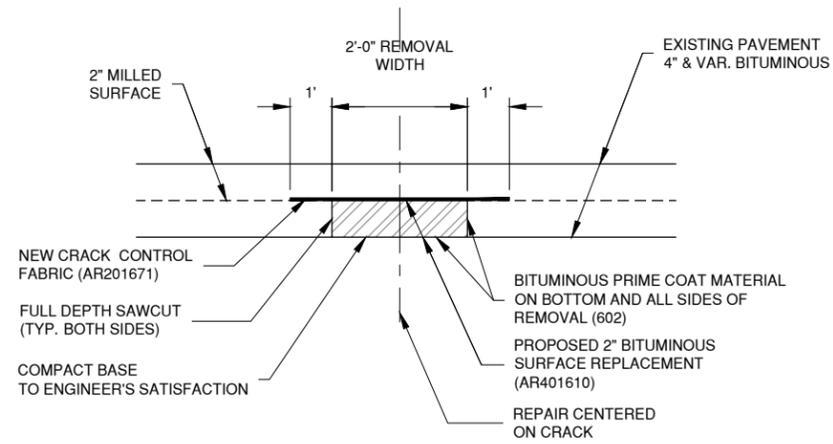


SUBGRADE UNDERCUT (AR152515 - C.Y.)

N.T.S

SUBGRADE UNDERCUT NOTES:

1. THE REMOVAL AREA SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION BY THE RESIDENT ENGINEER.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - 2.1. SAWCUT AND REMOVE THE HMA PAVEMENT REMAINING AFTER MILLING.
 - 2.2. REMOVE BITUMINOUS PAVEMENT, AGGREGATE BASE AND SOIL SUBGRADE TO A MINIMUM DEPTH OF 12" BELOW BOTTOM OF AGGREGATE BASE OR AS DIRECTED BY THE RESIDENT ENGINEER.
 - 2.3. COMPACT THE SUBGRADE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
 - 2.4. PLACE SEPARATION FABRIC ITEM ON TOP OF THE COMPACTED SUBGRADE AND UP THE SIDES OF THE EXCAVATION.
 - 2.5. PLACE AND CONSOLIDATE OVERSIZED AGGREGATE ITEM 208 TO THE BOTTOM OF THE EXISTING AGGREGATE BASE COURSE.
 - 2.6. PLACE AND COMPACT CRUSHED AGGREGATE ITEM 209 TO THE BOTTOM OF THE EXISTING BITUMINOUS PAVEMENT.
 - 2.7. APPLY TACK COAT ITEM 603510 TO THE BOTTOM AND SIDES OF THE EXCAVATED AREA.
 - 2.8. PLACE NEW BITUMINOUS PAVEMENT ITEM 401610 TO THE MILLED SURFACE OF THE REMAINING PAVEMENT.
 - 2.9. PLACE CRACK CONTROL FABRIC ITEM 201671 AS SHOWN ON THE DETAIL.
3. OVERSIZE AGGREGATE SHALL BE IDOT GRADATION CA-01 OR OTHER SUITABLE MATERIAL APPROVED BY THE RESIDENT ENGINEER.
4. THE FOLLOWING SHALL BE INCIDENTAL TO THE SUBGRADE UNDERCUT PAY ITEM (AR152515):
 - 4.1. FULL DEPTH SAWCUT
 - 4.2. UNCLASSIFIED EXCAVATION
 - 4.3. SUBGRADE PREPARATION
 - 4.4. SEPARATION FABRIC
 - 4.5. OVERSIZE AGGREGATE
 - 4.6. AGGREGATE BASE COURSE
 - 4.7. BITUMINOUS PRIME COAT
 - 4.8. BITUMINOUS TACK COAT
5. BITUMINOUS SURFACE REPLACEMENT AND OVERLAY AND CRACK CONTROL FABRIC WILL BE PAID SEPARATELY AT THE AS-BID UNIT PRICES.



BITUMINOUS CRACK REPAIR (AR201660 - L.F.)

N.T.S

BITUMINOUS CRACK REPAIR NOTES:

1. THE REMOVAL WIDTH SHALL BE 2'-0" WIDE OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE CRACK:
 - 2.1. SAWCUT AND REMOVE THE FULL DEPTH OF BITUMINOUS PAVEMENT TO A MINIMUM WIDTH OF 24" CENTERED ON THE CRACK.
 - 2.2. COMPACT THE AGGREGATE BASE COURSE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
 - 2.3. APPLY PRIME COAT ITEM 602510 TO THE AGGREGATE BASE COURSE AND THE SIDES OF THE AREA TO BE PATCHED.
 - 2.4. PLACE BITUMINOUS PAVEMENT ITEM 401610 TO THE TOP OF THE MILLED SURFACE.
 - 2.5. PLACE CRACK CONTROL FABRIC ITEM 201671 AS SHOWN IN THE PLANS PRIOR TO 2" BITUM. OVERLAY.
3. THE FOLLOWING SHALL BE INCIDENTAL TO THE BITUMINOUS CRACK REPAIR PAY ITEM (AR201660):
 - 3.1. FULL DEPTH SAWCUT
 - 3.2. PAVEMENT REMOVAL
 - 3.3. BASE PREPARATION
 - 3.4. BITUMINOUS PRIME COAT
4. BITUMINOUS SURFACE REPLACEMENT AND OVERLAY AND CRACK CONTROL FABRIC WILL BE PAID SEPARATELY AT THE AS-BID UNIT PRICES.



License No. 184-000613

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100%
SEPTEMBER 22, 2023

REHABILITATE HANNA CITY
T-HANGAR TAXIWAY
PAVEMENTS - PHASE 2

OWNER



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL
AIRPORT
PEORIA, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO: N/A

IL PROJ. NO: PIA-4989

CMT PROJECT NO: 210019-63

CAD DWG FILE: 21001963-CP500.DWG

DESIGNED BY: MRK

DRAWN BY: MRK

CHECKED BY: LEW

APPROVED BY: CBG

COPYRIGHT:

SHEET TITLE

PAVING & MISC
DETAILS

CP501
SHEET 12 OF 14

