

CITY OF AURORA AURORA, ILLINOIS

CONSTRUCTION PLANS FOR

AURORA MUNICIPAL AIRPORT

OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

ILLINOIS PROJECT: ARR-4565



D. Kyle Peabody

SEPTEMBER 22, 2023

LICENSE EXPIRATION
DATE: 11/30/2023
DATE SIGNED: 09/29/2023

BASE BID				
SUMMARY OF QUANTITIES				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	170	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	50	
AR156520	INLET PROTECTION	EACH	4	
AR201661	CLEAN AND SEAL BITUMINOUS CRACKS	FOOT	1,240	
AR201663	SAND MIX CRACK REPAIR	FOOT	940	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	20	
AR208604	4" AGGREGATE BASE COURSE	SQ YD	1,190	
AR208608	8" AGGREGATE BASE COURSE	SQ YD	50	
AR401610	BITUMINOUS SURFACE COURSE	TON	525	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	4,650	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	200	
AR602510	BITUMINOUS PRIME COAT	GALLON	60	
AR603510	BITUMINOUS TACK COAT	GALLON	685	
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	140	
AR910200	ROADWAY SIGN	EACH	1	

ADDITIVE ALTERNATE NO. 1				
SUMMARY OF QUANTITIES				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS152410	UNCLASSIFIED EXCAVATION	CU YD	110	
AS152540	SOIL STABILIZATION FABRIC	SQ YD	150	
AS156520	INLET PROTECTION	EACH	2	
AS208515	POROUS GRANULAR EMBANKMENT	CU YD	50	
AS208604	4" AGGREGATE BASE COURSE	SQ YD	15	
AS208608	8" AGGREGATE BASE COURSE	SQ YD	150	
AS401610	BITUMINOUS SURFACE COURSE	TON	18	
AS401650	BITUMINOUS PAVEMENT MILLING	SQ YD	90	
AS401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	100	
AS403610	BITUMINOUS BASE COURSE	TON	16	
AS602510	BITUMINOUS PRIME COAT	GALLON	45	
AS603510	BITUMINOUS TACK COAT	GALLON	25	
AS701515	15" RCP, CLASS IV	FOOT	16	
AS752415	PRECAST REINFORCED CONC. FES 15"	EACH	1	
AS752900	REMOVE END SECTION	EACH	1	

INDEX TO SHEETS

1. COVER SHEET
2. SITE PLAN
3. STORM WATER POLLUTION PREVENTION NOTES AND DETAILS
4. SEQUENCE OF CONSTRUCTION
5. SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
6. EXISTING CONDITIONS AND PROPOSED REMOVALS
7. TYPICAL SECTIONS
8. GEOMETRIC LAYOUT, EROSION CONTROL AND PAVEMENT MARKING PLAN



CITY OF AURORA, ILLINOIS

APPROVED BY *[Signature]* MAYOR
RICHARD C. IRVIN

DATE 09/27 2023

APPROVED BY *[Signature]* AIRPORT MANAGER
STEPHEN K. ANDRAS, P.E.

DATE *SEPT 27* 2023

CMT 21002029.00
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

SUBMITTED BY *D. Kyle Peabody*
D. KYLE PEABODY, P.E.

DATE 09/29/2023

811 Know what's below. Call before you dig.

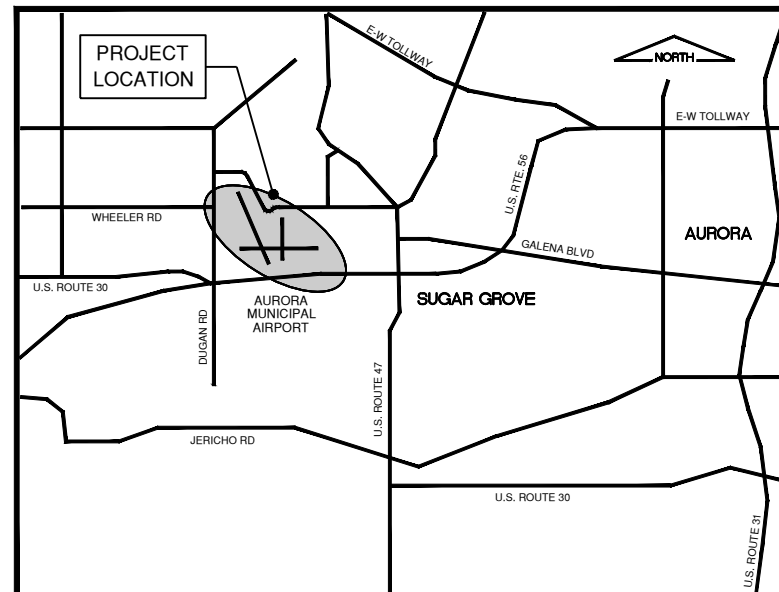
J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

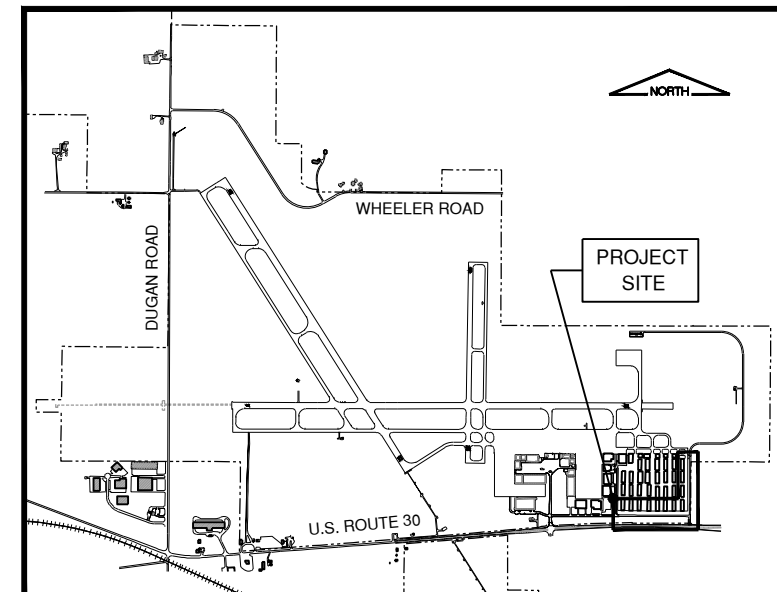
CALL J.U.L.I.E BEFORE EXCAVATING AT 811

AURORA MUNICIPAL AIRPORT

SECTION: 17 AND 18 COUNTY: KANE
RANGE: 7 EAST U.S. ROUTE 30
TOWNSHIP: 38 NORTH SUGAR GROVE TOWNSHIP



LOCATION MAP



SITE PLAN

NOTES

1. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO THE CONTRACT.
4. THE CONTRACTOR WILL BE REQUIRED TO FURNISH AND PLACE CONSTRUCTION LAYOUT STAKES FOR THIS PROJECT. CONSTRUCTION LAYOUT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.
5. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND/OR TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP OR I.D.O.T. COST INCIDENTAL TO THE CONTRACT.
6. CONTRACTOR SHALL BE REQUIRED TO OBTAIN AN **IDOT HIGHWAY PERMIT** FOR WORK WITHIN THE RIGHT-OF-WAY OF U.S. ROUTE 30. ALL FEES AND CHARGES INCURRED BY THE CONTRACTOR TO OBTAIN THE NECESSARY PERMITS/PERMISSIONS TO INSTALL AND PERFORM THE WORK SHALL BE NOT MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE **INCIDENTAL TO CONTRACT (ADDITIVE ALTERNATE NO. 1 WORK)**.
7. SCOPE OF WORK FOR ADDITIVE ALTERNATE NO. 1 INCLUDES INTERSECTION WIDENING AND REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B WITHIN THE U.S. 30 RIGHT OF WAY. THIS INCLUDES ALL PLACEMENT, REMOVAL AND FOR ALL LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THIS SCOPE OF WORK.

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
BM-1	N.W. CORNER FLAG POLE FOUNDATION	703.61

VERTICAL CONTROL COORDINATES EXPRESSED IN NAD88

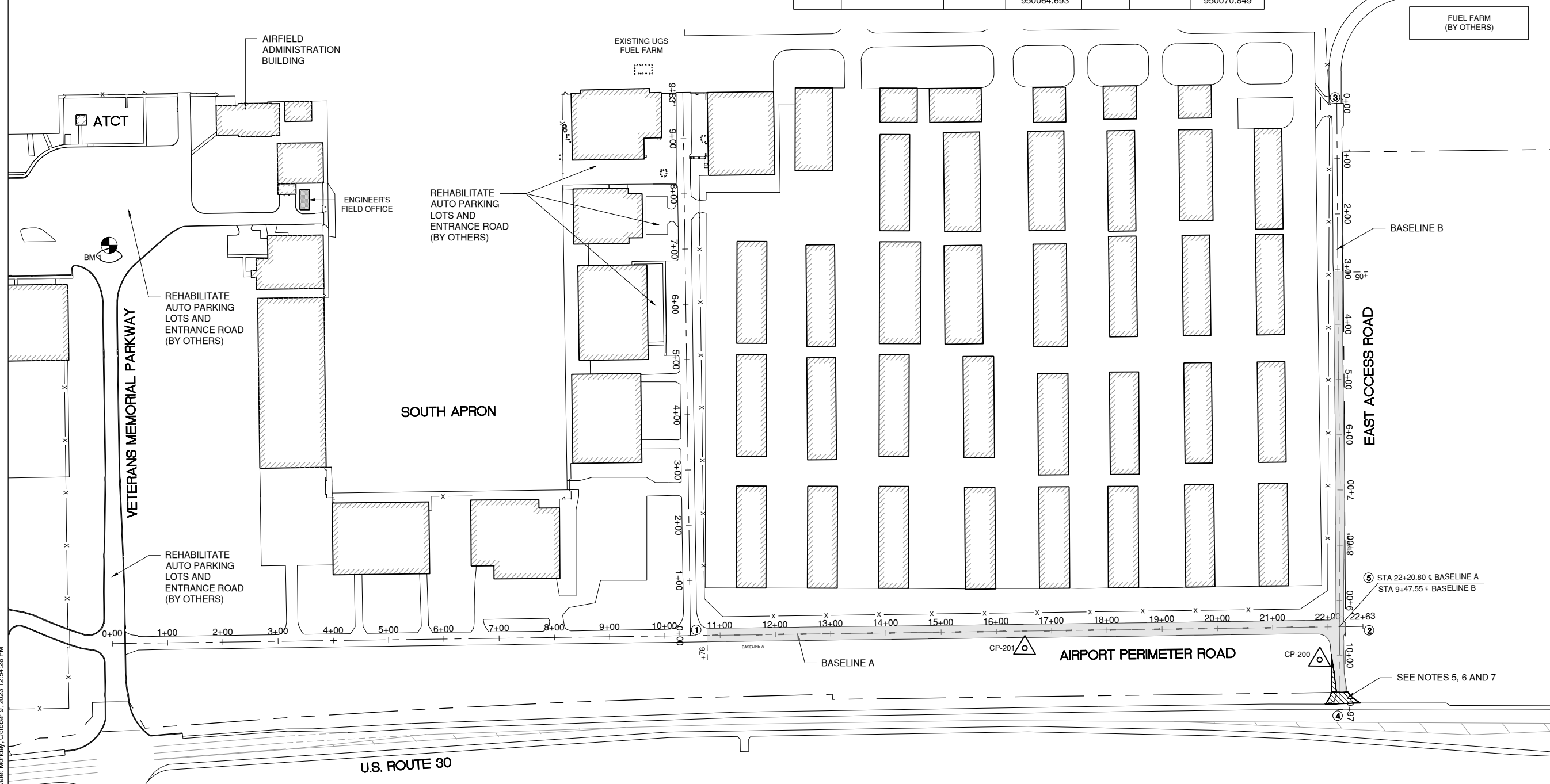
HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
CP-200	IRON ROD	1857161.440	950033.670
CP-201	IRON ROD	1857182.655	949499.807

HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAD83

ALIGNMENT DATA						
POINT	ALIGNMENT	STATION	NORTHING/ EASTING	POINT	STATION	NORTHING/ EASTING
1	BASELINE A	0+00.00	1857205.164 948894.960	2	22+63.32	1857221.875 950111.867
3	BASELINE B	0+00.00	1858168.801 950064.693	4	10+97.28	1857071.598 950070.849



- LEGEND**
- PROPOSED IMPROVEMENTS (BASE BID)
 - PROPOSED IMPROVEMENTS (ADDITIVE ALTERNATE NO. 1)
 - EXISTING BUILDING
 - EXISTING FENCE
 - AIRPORT PROPERTY LINE
 - BENCHMARK (BM-1)
 - CONTROL POINT (CP-200)



CONSULTANTS

FINAL

**OVERLAY SOUTHEAST
QUADRANT AIRPORT
PERIMETER ROADWAYS
- PHASE 2**

SEPTEMBER 22, 2023



MARK	DATE	DESCRIPTION

IL PROJECT NO: ARR-4565
 CMT PROJECT NO: 21002029.00
 CAD DWG FILE:
 DESIGNED BY: STL
 DRAWN BY: JRO
 CHECKED BY: STL
 APPROVED BY: DKP
 COPYRIGHT:

SITE PLAN

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STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING AN OVERLAY, PAVEMENT PATCHING AND PAVEMENT MILLING AT AURORA MUNICIPAL AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

- UNCLASSIFIED EXCAVATION (PGE AND AGGREGATE BASE COURSE SHALL BE CONSTRUCTED THE SAME DAY AS THE EXCAVATION).
- 4" AGGREGATE BASE FOR SHOULDERS.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE WELCH CREEK.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

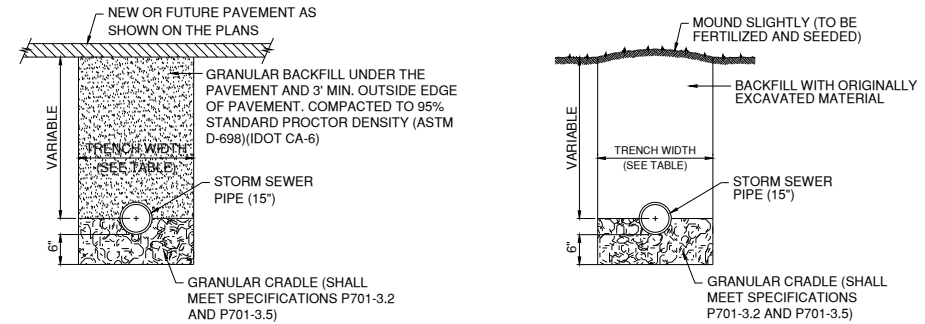
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

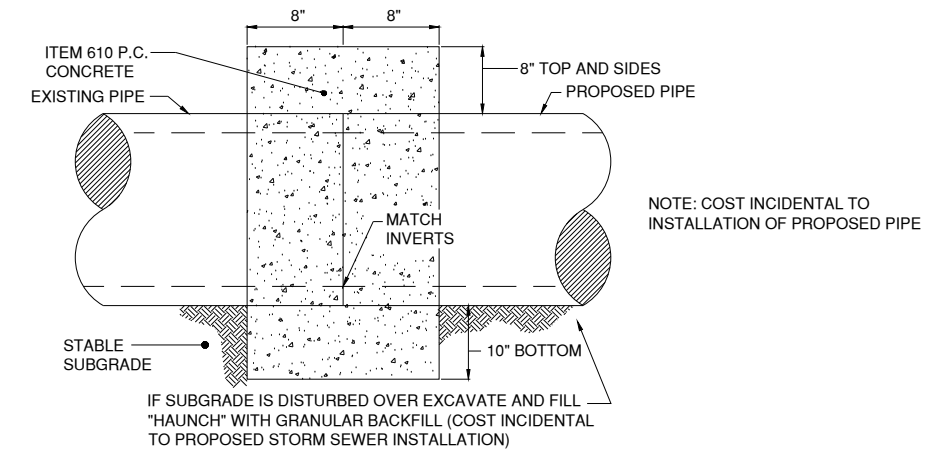
MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

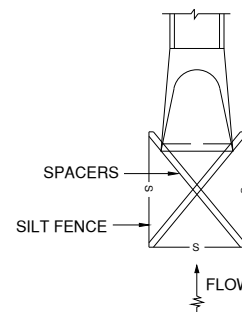
INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"
66	9'-5"
72	10'-0"
78	10'-7"
84	11'-2"
90	11'-9"
96	12'-4"
102	12'-11"
108	13'-6"



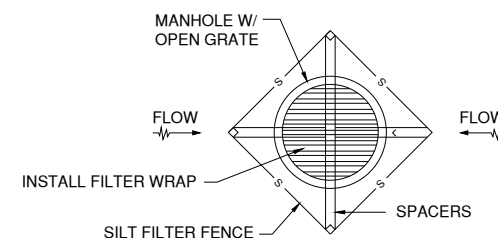
TRENCH DETAILS - STORM SEWER
NOT TO SCALE



CONCRETE COLLAR - STORM SEWER
NOT TO SCALE



INLET PROTECTION (END SECTION)
NOT TO SCALE
IDOT STANDARD 280001-07



INLET PROTECTION (INLET/MANHOLES - IN TURF)
NOT TO SCALE
IDOT STANDARD 280001-07

NOTES FOR INLET PROTECTION DETAILS

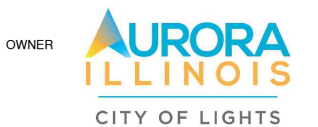
- FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2022.
- FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
- COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CONSULTANTS

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OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

SEPTEMBER 22, 2023

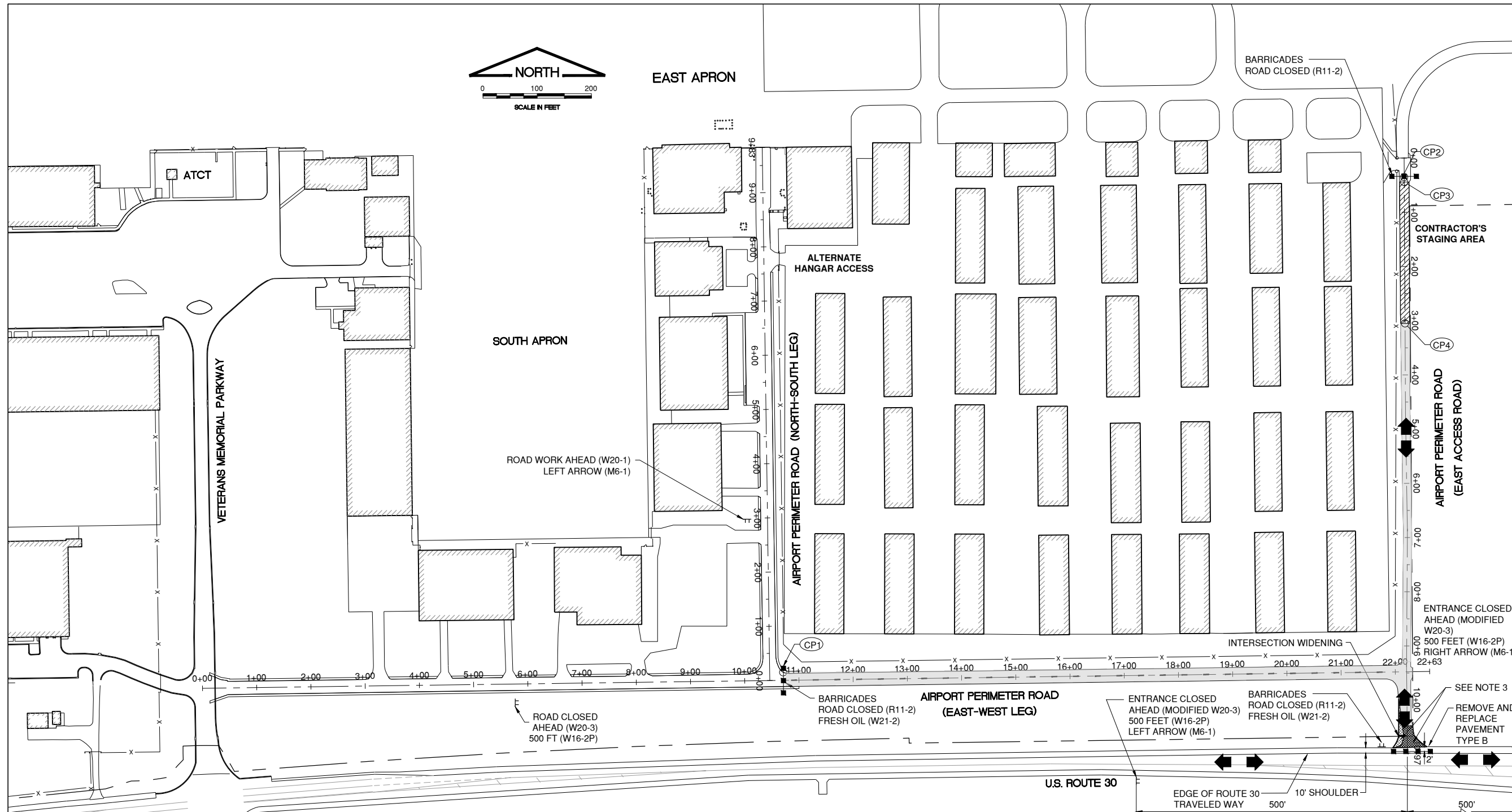


MARK | DATE | DESCRIPTION

IL PROJECT NO:	ARR-4565
CMT PROJECT NO:	21002029.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DLP
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SHEET TITLE

STORM WATER POLLUTION PREVENTION NOTES AND DETAILS



LEGEND

- PROPOSED IMPROVEMENTS
- CONTRACTOR'S EMPLOYEE PARKING, EQUIPMENT AND MATERIAL STORAGE LOCATION
- CONTRACTOR ACCESS ROUTE
- CRITICAL POINT AND NUMBER
- TEMPORARY TYPE III BARRICADES WITH FLASHING LIGHT
- TEMPORARY TRAFFIC CONTROL SIGN
- AIRPORT PROPERTY LINE

SUGGESTED SEQUENCE OF CONSTRUCTION

- PLACE TYPE III BARRICADE AND APPLICABLE TEMPORARY TRAFFIC CONTROL SIGNS FOR ROAD CLOSURES.
- PERFORM UNCLASSIFIED EXCAVATION, CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE WITHIN THE SAME DAY. CONSTRUCT 4" BITUMINOUS BASE COURSE.
- AS DIRECTED BY THE RESIDENT ENGINEER, REMOVE AND REPLACE FULL DEPTH OF BITUMINOUS PAVEMENT WITH BITUMINOUS BASE COURSE. IF REQUIRED, REMOVE AGGREGATE BASE AND UNCLASSIFIED EXCAVATION. CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE.
- BITUMINOUS PAVEMENT MILLING AND PLACE TACK COAT.
- CONSTRUCT 2" BITUMINOUS SURFACE COURSE.
- CONSTRUCT REMAINING IMPROVEMENTS.
- CLEAN PAVEMENTS, REMOVE BARRICADES AND SIGNS AND OPEN ALL TRAFFIC LANES.

GENERAL NOTES

1. COORDINATE ALL ROAD CLOSURES WITH THE RESIDENT ENGINEER AND AIRPORT MANAGER 14 DAYS PRIOR TO THE START OF CLOSURES AT EACH PHASE.
2. AT THE CONTRACTOR'S OPTION, THE PAVEMENT MARKING AND AGGREGATE SHOULDERS CAN BE CONSTRUCTED AT THE END OF THE PROJECT WHILE MAINTAINING TRAFFIC CONTROL WITH NO ROAD CLOSURES.
3. CONTRACTOR SHALL PERFORM UNCLASSIFIED EXCAVATION, CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE WITHIN THE SAME DAY. DROP OFF GREATER THAN 6-INCH IS NOT ALLOWED WITHIN THE IDOT RIGHT OF WAY.
4. TEMPORARY TRAFFIC CONTROL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".

MARK	DATE	DESCRIPTION

IL PROJECT NO:	ARR-4565
CMT PROJECT NO:	21002029.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DLP
COPYRIGHT:	

SEQUENCE OF CONSTRUCTION

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 Date: Monday, October 9, 2023 12:54:37 PM



CONSULTANTS

FINAL

OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

SEPTEMBER 22, 2023



MARK	DATE	DESCRIPTION

SHEET TITLE

SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS

GENERAL NOTES

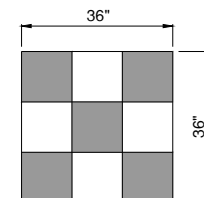
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICLE ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY IDOT AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2 (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED.
- THE AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN AND VEHICULAR SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE TENANT PARKING LOTS AND DRIVEWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL HAVE FLASHING LIGHT(S) AND CONFORM TO IDOT STANDARD 701901-07 AND TYPE III SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- AURORA MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT. THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DETAILS HOW THE CONTRACTOR WILL COMPLY WITH THE CSPP. ALSO, IT WILL NOT BE POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS (FOR EXAMPLE SPECIFIC HAZARD EQUIPMENT AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHTS) DURING THE DEVELOPMENT OF THE CSPP. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AND SPCD THAT IN CONFORMANCE WITH FAA AC 150/5370-2 (LATEST EDITION) PRIOR TO ISSUANCE OF A NOTICE-TO-PROCEED. THE SPCD IS A SUBSET OF THE CSPP, SIMILAR TO HOW A SHOP DRAWING REVIEW IS A SUBSET TO THE TECHNICAL SPECIFICATIONS.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS. THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE SITE DUE TO ADJACENT CONSTRUCTION.

- REHABILITATE AUTO PARKING LOTS AND ENTRANCE ROAD (IL. PROJECT NO. 4544)
- REVV AVIATION FUEL FARM PROJECT.

POINT	APPROXIMATE ELEVATION OF GROUND (NAV 83)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (NAV 83)	LATITUDE (NAD 83)	LONGITUDE (NAD 83)
CP1	703.6	SEMI/DUMP TRUCK - 25'	728.6	41° 45' 56.34"	88° 27' 46.23"
CP2	702.5	SEMI/DUMP TRUCK - 25'	727.5	41° 46' 05.38"	88° 27' 31.13"
CP3	702.5	SEMI/DUMP TRUCK - 25'	727.5	41° 46' 05.29"	88° 27' 31.13"
CP4	703.2	SEMI/DUMP TRUCK - 25'	728.2	41° 46' 02.71"	88° 27' 31.10"

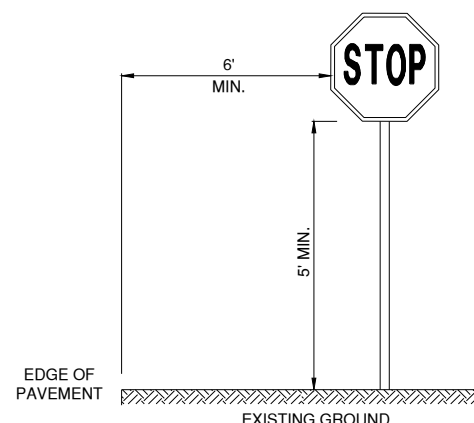


CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



SIGN DETAIL
NOT TO SCALE

ALL SIGNS ARE STANDARD MATERIALS, COLORS AND LETTER STYLE AND SIZE AS SHOWN IN THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION.



POST MOUNTED SIGN LOCATION DETAIL
NOT TO SCALE

OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

SEPTEMBER 22, 2023

OWNER **AURORA ILLINOIS** CITY OF LIGHTS

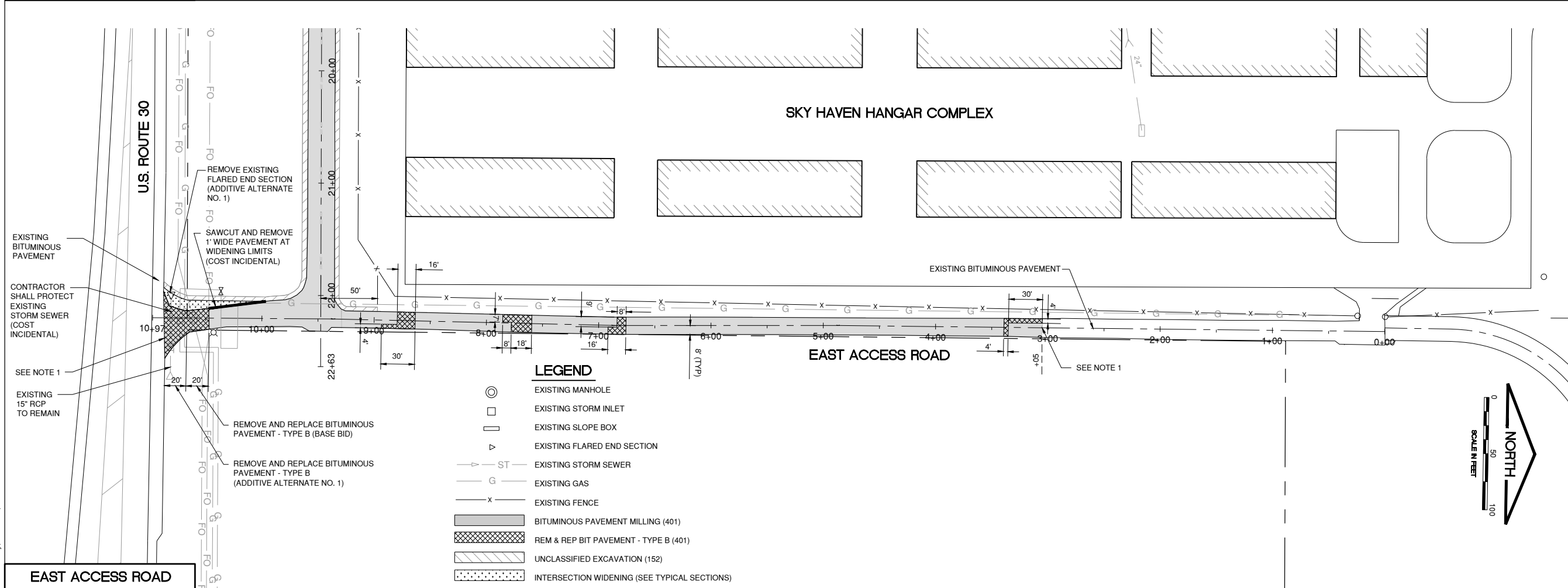
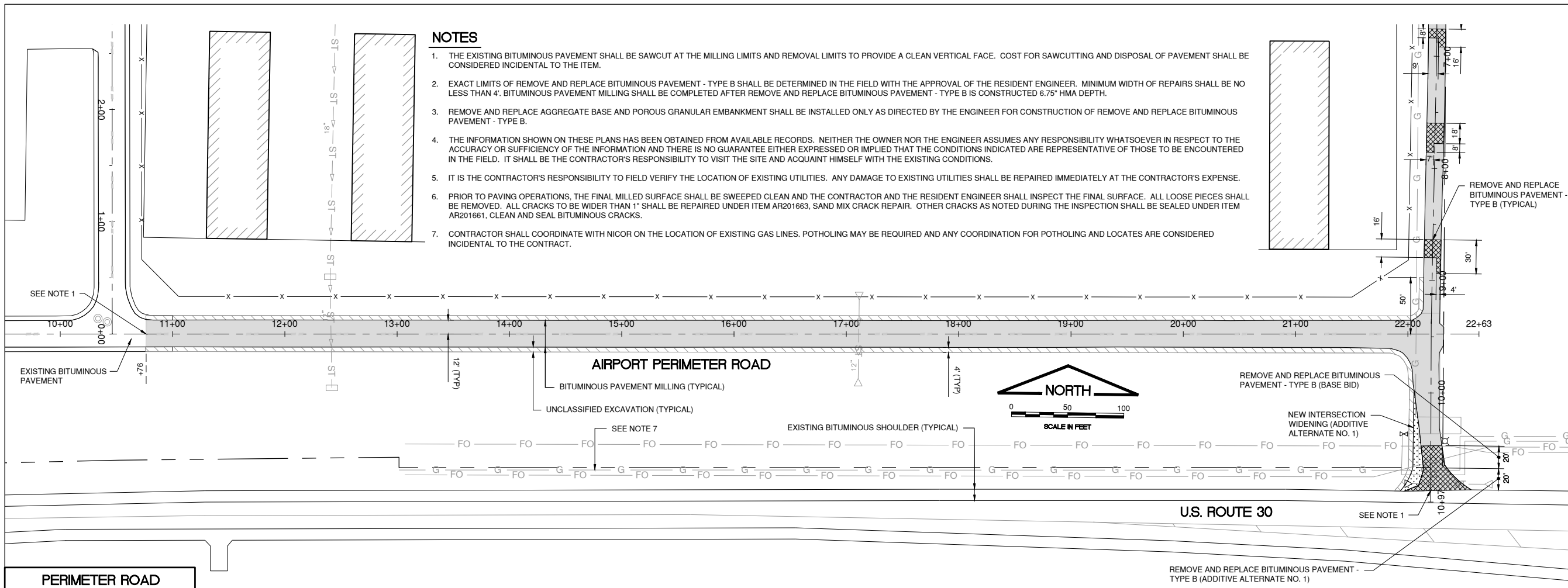


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CMT PROJECT NO:	21002029.00	
CAD DWG FILE:		
DESIGNED BY:	STL	
DRAWN BY:	JRO	
CHECKED BY:	STL	
APPROVED BY:	DLP	
COPYRIGHT:		

SHEET TITLE **EXISTING CONDITIONS AND PROPOSED REMOVALS**

NOTES

1. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT AT THE MILLING LIMITS AND REMOVAL LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. EXACT LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B SHALL BE DETERMINED IN THE FIELD WITH THE APPROVAL OF THE RESIDENT ENGINEER. MINIMUM WIDTH OF REPAIRS SHALL BE NO LESS THAN 4'. BITUMINOUS PAVEMENT MILLING SHALL BE COMPLETED AFTER REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS CONSTRUCTED 6.75" HMA DEPTH.
3. REMOVE AND REPLACE AGGREGATE BASE AND POROUS GRANULAR EMBANKMENT SHALL BE INSTALLED ONLY AS DIRECTED BY THE ENGINEER FOR CONSTRUCTION OF REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B.
4. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
6. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR. OTHER CRACKS AS NOTED DURING THE INSPECTION SHALL BE SEALED UNDER ITEM AR201661, CLEAN AND SEAL BITUMINOUS CRACKS.
7. CONTRACTOR SHALL COORDINATE WITH NICOR ON THE LOCATION OF EXISTING GAS LINES. POTHOLING MAY BE REQUIRED AND ANY COORDINATION FOR POTHOLING AND LOCATES ARE CONSIDERED INCIDENTAL TO THE CONTRACT.



LEGEND

- EXISTING MANHOLE
- EXISTING STORM INLET
- EXISTING SLOPE BOX
- EXISTING FLARED END SECTION
- EXISTING STORM SEWER
- EXISTING GAS
- EXISTING FENCE
- BITUMINOUS PAVEMENT MILLING (401)
- REM & REP BIT PAVEMENT - TYPE B (401)
- UNCLASSIFIED EXCAVATION (152)
- INTERSECTION WIDENING (SEE TYPICAL SECTIONS)

CONSULTANTS

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OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

SEPTEMBER 22, 2023



OWNER



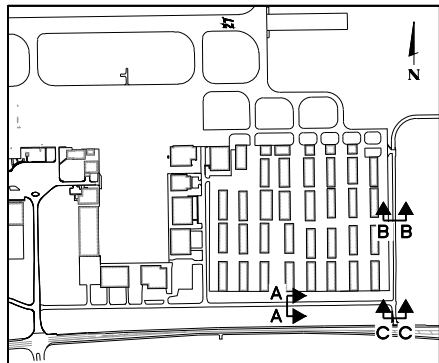
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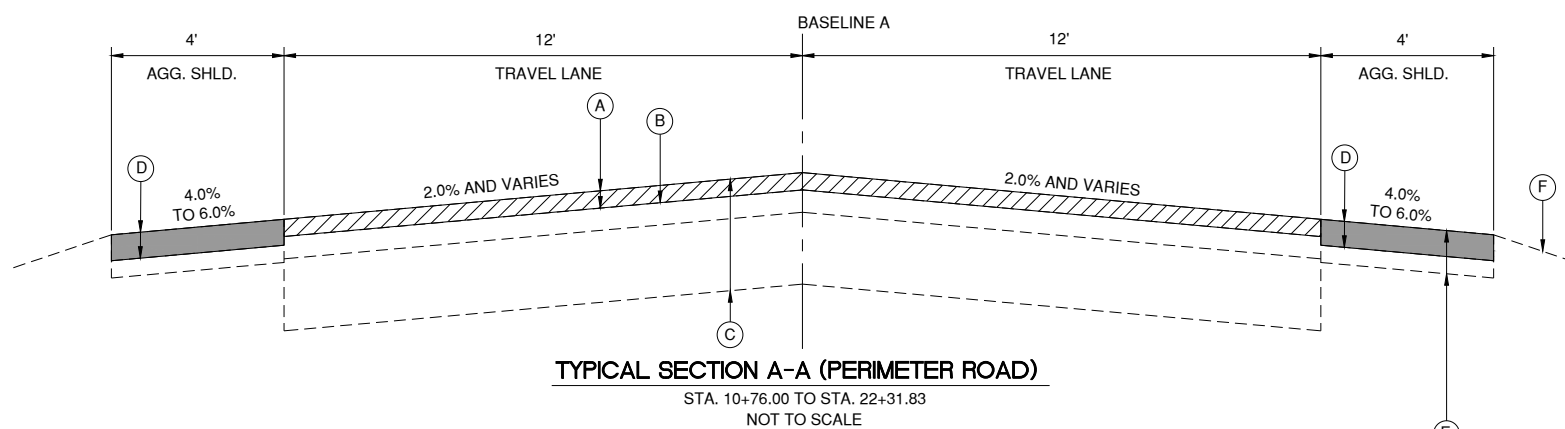
TYPICAL SECTIONS

LEGEND

- (A) NEW 2" BITUMINOUS SURFACE MILLING (SEE NOTE 1)(401)
NEW 2" BITUMINOUS SURFACE COURSE (401)
- (B) NEW BITUMINOUS TACK COAT BETWEEN LIFTS (603)
- (C) EXISTING APPROX. 5-1/2" BITUMINOUS PAVEMENT
EXISTING APPROX. 10" CRUSHED AGGREGATE BASE COURSE
- (D) NEW UNCLASSIFIED EXCAVATION (152)
NEW 4" AGGREGATE SHOULDER (208)
- (E) EXISTING 6" AGGREGATE SHOULDER
- (F) EXISTING GROUND LINE
- (G) EXISTING APPROX. 6-3/4" BITUMINOUS PAVEMENT
EXISTING APPROX. 8" CRUSHED AGGREGATE BASE COURSE
- (H) NEW UNCLASSIFIED EXCAVATION (152)
NEW 2" BITUMINOUS SURFACE COURSE (401)
NEW 4" BITUMINOUS BASE COURSE (403)
NEW 8" AGGREGATE BASE COURSE (208)
NEW 12" POROUS GRANULAR EMBANKMENT (208)
NEW SOIL STABILIZATION FABRIC (152)

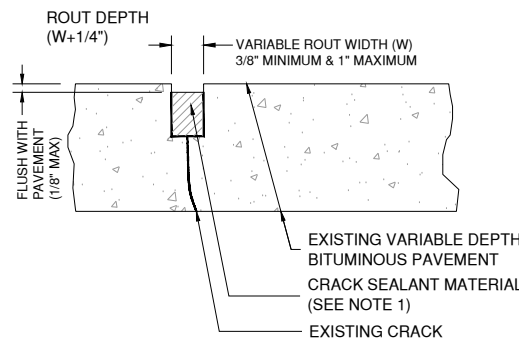


KEY MAP



TYPICAL SECTION A-A (PERIMETER ROAD)

STA. 10+76.00 TO STA. 22+31.83
NOT TO SCALE

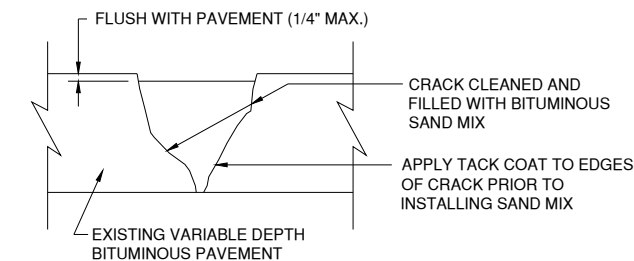


CLEAN AND SEAL BITUMINOUS CRACKS

NOT TO SCALE

CRACK SEALING NOTES:

- THE CRACK ROUTING DIMENSIONS WILL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL.
- CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.

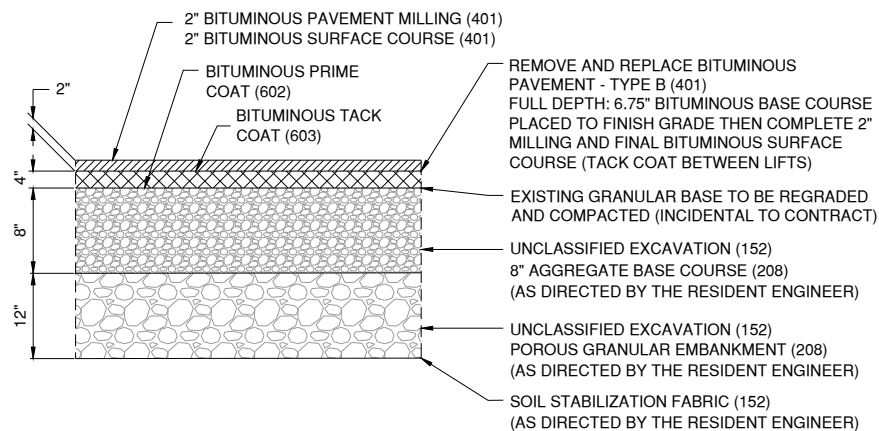


HMA SAND MIX CRACK REPAIR DETAIL

NOT TO SCALE

CRACK REPAIR NOTES:

- CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
- SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL ROUTE, CRACK PLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.

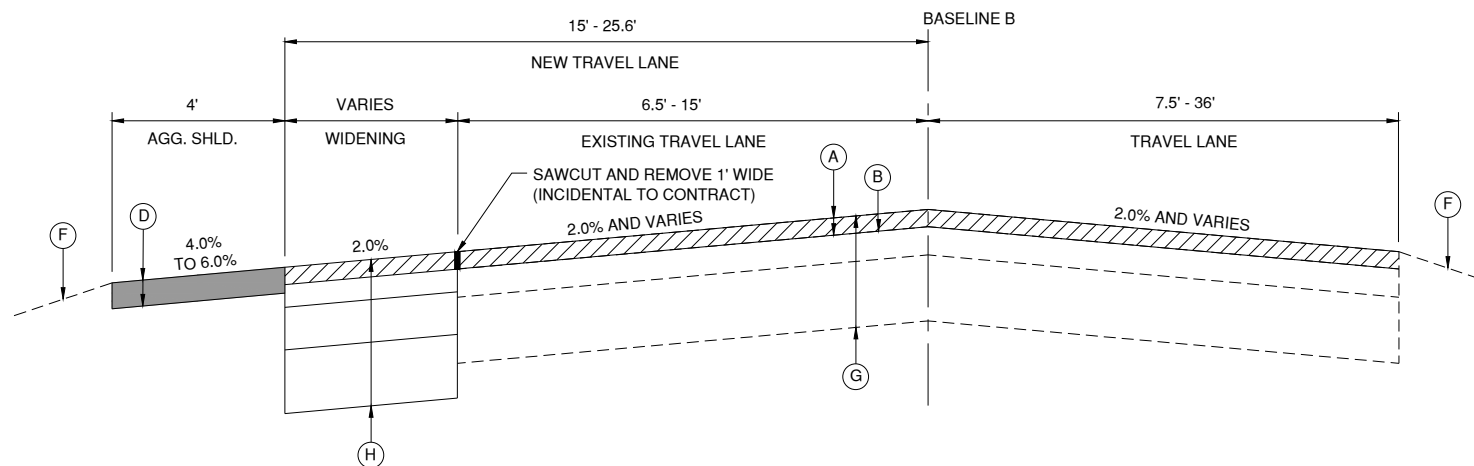


REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B DETAIL

NOT TO SCALE

NOTES

- DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
- PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
- AS FIELD CONDITIONS WARRANT AT THE TIME IF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.



TYPICAL SECTION C-C (EAST ACCESS ROAD INTERSECTION WIDENING)

ADDITIVE ALTERNATE NO. 1
STA. 9+97.00 TO STA. 10+47.00
NOT TO SCALE



CONSULTANTS

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OVERLAY SOUTHEAST QUADRANT AIRPORT PERIMETER ROADWAYS - PHASE 2

SEPTEMBER 22, 2023



NOTES

BASE BID

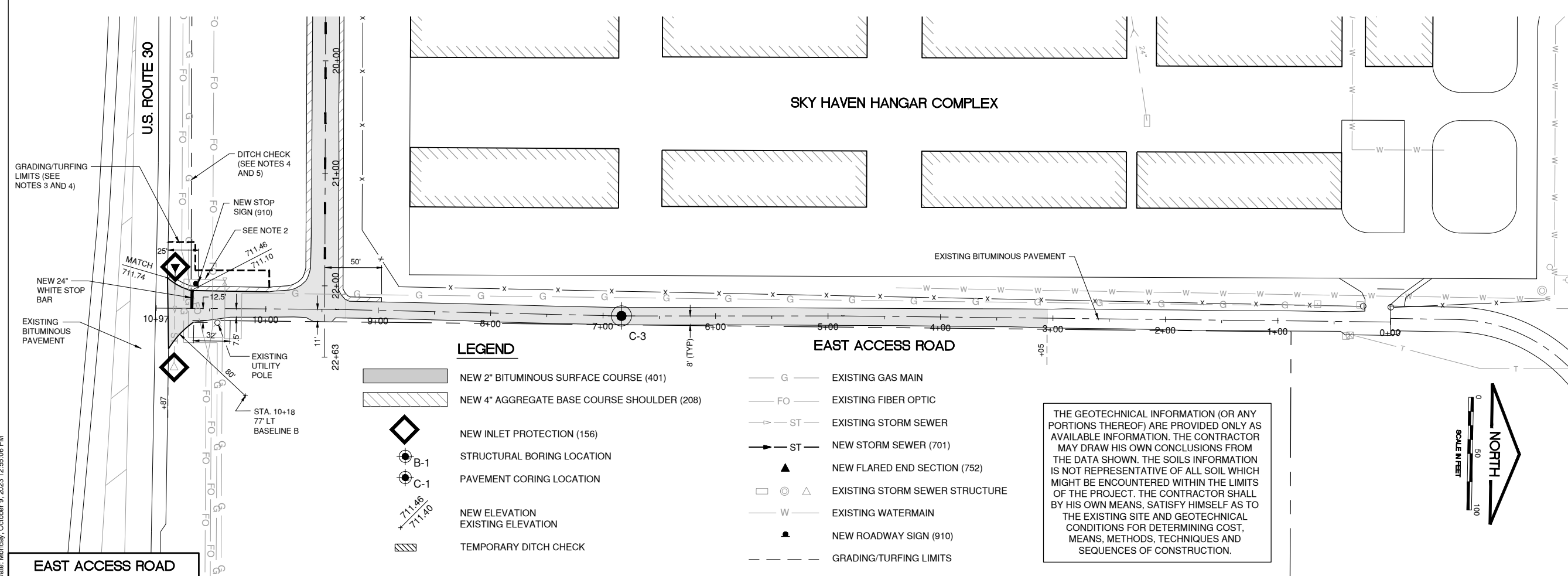
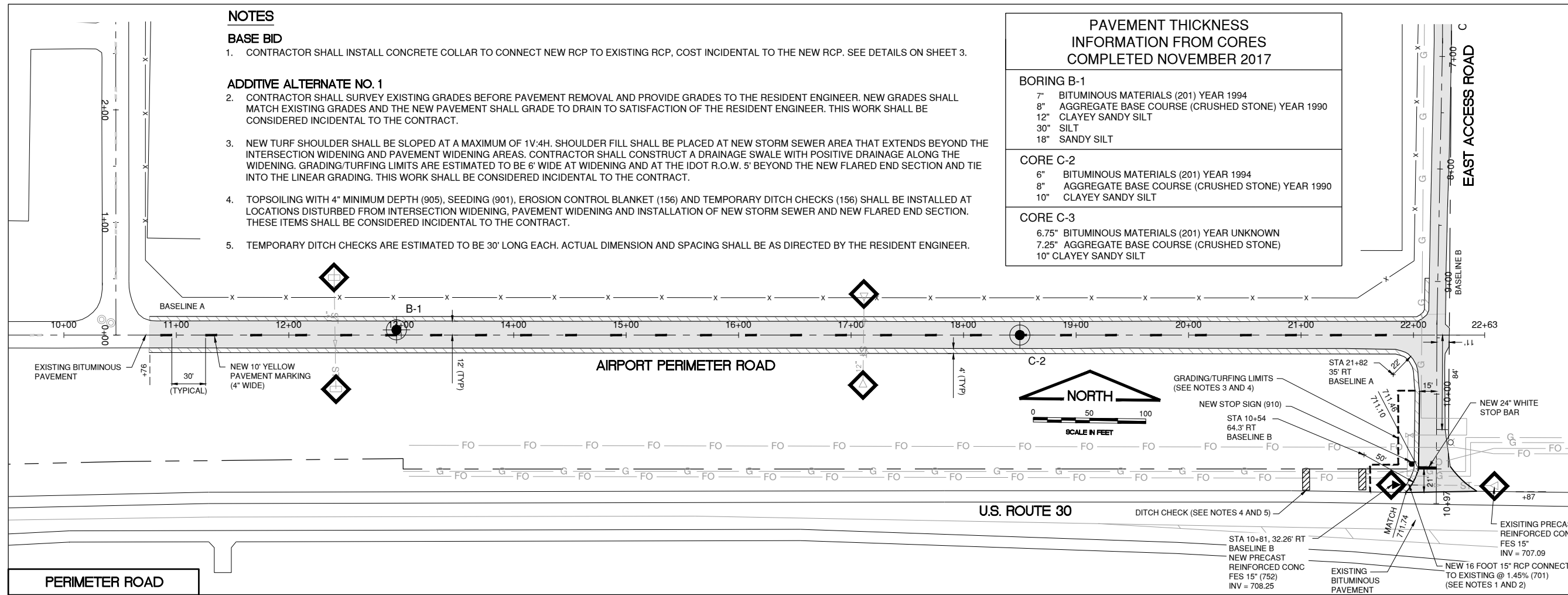
1. CONTRACTOR SHALL INSTALL CONCRETE COLLAR TO CONNECT NEW RCP TO EXISTING RCP, COST INCIDENTAL TO THE NEW RCP. SEE DETAILS ON SHEET 3.

ADDITIVE ALTERNATE NO. 1

- CONTRACTOR SHALL SURVEY EXISTING GRADES BEFORE PAVEMENT REMOVAL AND PROVIDE GRADES TO THE RESIDENT ENGINEER. NEW GRADES SHALL MATCH EXISTING GRADES AND THE NEW PAVEMENT SHALL GRADE TO SATISFACTION OF THE RESIDENT ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- NEW TURF SHOULDER SHALL BE SLOPED AT A MAXIMUM OF 1V:4H. SHOULDER FILL SHALL BE PLACED AT NEW STORM SEWER AREA THAT EXTENDS BEYOND THE INTERSECTION WIDENING AND PAVEMENT WIDENING AREAS. CONTRACTOR SHALL CONSTRUCT A DRAINAGE SWALE WITH POSITIVE DRAINAGE ALONG THE WIDENING. GRADING/TURFING LIMITS ARE ESTIMATED TO BE 6' WIDE AT WIDENING AND AT THE IDOT R.O.W. 5' BEYOND THE NEW FLARED END SECTION AND TIE INTO THE LINEAR GRADING. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- TOPSOILING WITH 4" MINIMUM DEPTH (905), SEEDING (901), EROSION CONTROL BLANKET (156) AND TEMPORARY DITCH CHECKS (156) SHALL BE INSTALLED AT LOCATIONS DISTURBED FROM INTERSECTION WIDENING, PAVEMENT WIDENING AND INSTALLATION OF NEW STORM SEWER AND NEW FLARED END SECTION. THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- TEMPORARY DITCH CHECKS ARE ESTIMATED TO BE 30' LONG EACH. ACTUAL DIMENSION AND SPACING SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.

PAVEMENT THICKNESS INFORMATION FROM CORES COMPLETED NOVEMBER 2017

BORING B-1	
7"	BITUMINOUS MATERIALS (201) YEAR 1994
8"	AGGREGATE BASE COURSE (CRUSHED STONE) YEAR 1990
12"	CLAYEY SANDY SILT
30"	SILT
18"	SANDY SILT
CORE C-2	
6"	BITUMINOUS MATERIALS (201) YEAR 1994
8"	AGGREGATE BASE COURSE (CRUSHED STONE) YEAR 1990
10"	CLAYEY SANDY SILT
CORE C-3	
6.75"	BITUMINOUS MATERIALS (201) YEAR UNKNOWN
7.25"	AGGREGATE BASE COURSE (CRUSHED STONE)
10"	CLAYEY SANDY SILT



LEGEND

	NEW 2" BITUMINOUS SURFACE COURSE (401)		EXISTING GAS MAIN
	NEW 4" AGGREGATE BASE COURSE SHOULDER (208)		EXISTING FIBER OPTIC
	NEW INLET PROTECTION (156)		EXISTING STORM SEWER
	STRUCTURAL BORING LOCATION		NEW STORM SEWER (701)
	PAVEMENT CORING LOCATION		NEW FLARED END SECTION (752)
	NEW ELEVATION		EXISTING STORM SEWER STRUCTURE
	EXISTING ELEVATION		EXISTING WATERMAIN
	TEMPORARY DITCH CHECK		NEW ROADWAY SIGN (910)
			GRADING/TURFING LIMITS

THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.

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DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DLP
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SHEET TITLE	
GEOMETRIC LAYOUT, EROSION CONTROL AND PAVEMENT MARKING PLAN	
SHEET	8 OF 8