

**IDOT PROJECT LABOR AGREEMENT DETERMINATION**

To: Omer Osman, Secretary  
From: Kirk H. Brown, Region 5 Engineer  
Date: 8/21/2023  
Re: IL 3 Resurfacing and Safety Shoulders  
{November 17, 2023 letting}

In accordance with the Project Labor Agreement Act (30 ILCS 571), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances.
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time.
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern.

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project.

10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).

12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed: DocuSigned by:  11/7/2023 | 3:23 PM CST  
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{Division Chief} (Date)

Agreed: DocuSigned by:  11/7/2023 | 1:14 PM CST  
A3AD6055B4AA490...  
{Bureau of Design & Environment} (Date)

Agreed:  8/23/23  
{Kirk H. Brown, Region 5 Engineer} (Date)

Approved: DocuSigned by:  11/8/2023 | 9:33 AM CST  
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Omer Osman, Secretary (Date)

FHWA concurrence in the PLA for the above mentioned contract

**MICHAEL W SMART** Digitally signed by MICHAEL W SMART  
Date: 2023.10.16 09:04:16 -05'00'

\_\_\_\_\_  
FHWA (Date)

**ATTACHMENT A:**

The project involves milling 1-1/2" from Howardton Rd to the Jackson/Union county line with a 1-1/4" binder course and a 1-1/2 surface course and adding 4' safety shoulders, and milling 1/2" from the Jackson/Union county line to just North of the Cape T intersection with a 2-1/4" binder and a 1-1/2" surface course. This project is 25.43 miles.

Program amount is \$13,000,000

#4 – Project has 180 working days

#8 – In order to maintain traffic during construction, lane closures, lane shifts, narrow lanes, and uneven pavement will all be required. Due to the high traffic volumes these temporary conditions, while necessary during construction, pose a safety hazard to the traveling public. This hazard can be mitigated by minimizing the time that these temporary conditions are in place.

Execution Page

**Illinois Department of Transportation**

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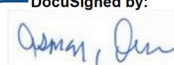
Director of Highways Project Implementation

DocuSigned by:  
  
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Director of Finance & Administration

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Michael S. Prater, Acting Chief Counsel

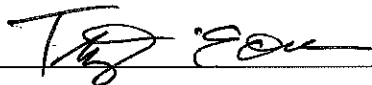
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11/8/2023 | 9:33 AM CST

Omer Osman, Secretary

(Date)

**Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the Unions listed below:**

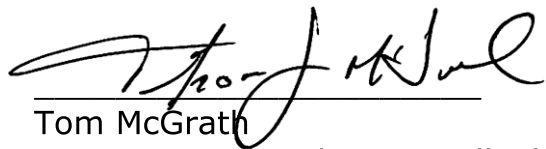


11/02/2023  
(Date)


List Unions:



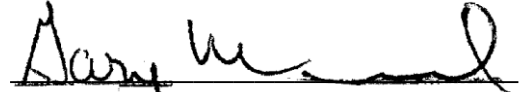
Mike Volpentesta  
Bricklayers



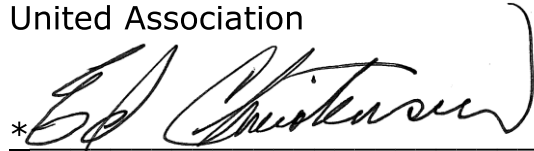
Tom McGrath  
Heat & Frost Insulators & Allied  
Workers




Evan Wooding  
United Association



Gary Menzel  
Roofers & Waterproofers



\*Ed Christensen  
Elevator Constructors



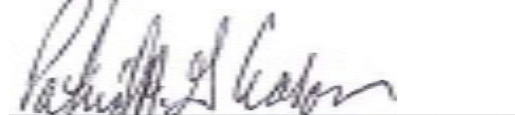
Shad Etchason  
IBEW



Ryan Anderson  
IUPAT



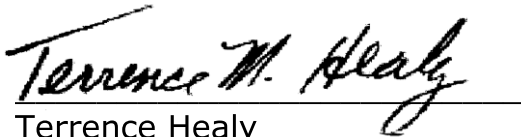
Michael R Kresge  
IUOE



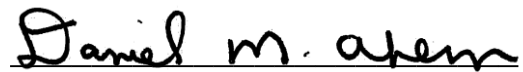
Pat Gleason  
Teamsters



Ron Culbertson  
Carpenters



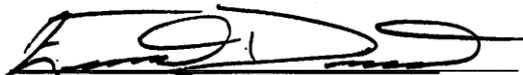
Terrence Healy  
LIUNA



Daniel M. Ahern  
Sheet Metal Workers



David Beard  
Iron Workers



Eric S. Davis  
Boilermakers



Kevin J. Farley  
OPCMIA

\*Elevator Constructors master agreement language  
must be attached to PLA