

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

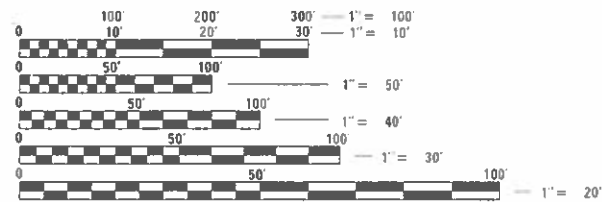
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-RS	DUPAGE	40	1
		ILLINOIS	CONTRACT NO. 61J85	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LISTING OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 1379 (FULLERTON AVENUE)
SCHMALE ROAD TO BLOOMINGDALE ROAD
RESURFACING
SECTION 21-00073-00-RS
PROJECT NO. 1QRB(658)
VILLAGE OF GLENDALE HEIGHTS
DUPAGE COUNTY
JOB NO. C-91-009-22

FULLERTON AVENUE
FUNCTIONAL CLASSIFICATION
MINOR ARTERIAL
POSTED SPEED
25 M.P.H.
TRAFFIC DATA
ADT (YEAR) = 5900 (2020)

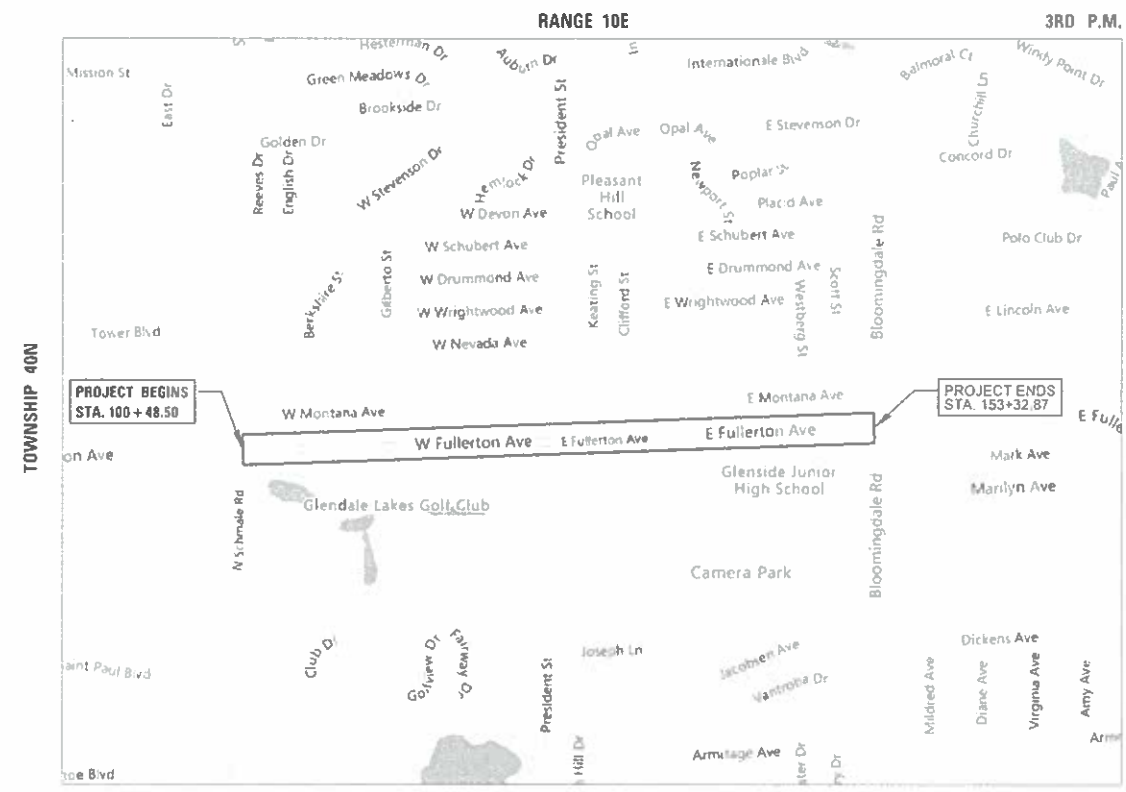


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-00175
EXPIRATION DATE: APRIL 30, 2025
CONTRACT NO. 61J85



LOCATION MAP
NOT TO SCALE
GROSS & NET LENGTH = 5,284 FT. = 1.00 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED *Chok S. Kholshar*
MAYOR, VILLAGE OF GLENDALE HEIGHTS

PASSED *SEPT. 6, 2023*
Chok S. Kholshar
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW
SEPTEMBER 6, 2023
Jose Luis Lopez
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CARMEN E. RAMOS, P.E. (847) 705-4021 SCHAUMBURG, IL

BRIAN LIBUNAO
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
DATE: 9/1/2023
BRIAN LIBUNAO
ILLINOIS REGISTRATION No. 062-071040
EXPIRATION DATE: 11/30/2023

INDEX OF SHEETS

DESCRIPTION

SHEET NO.

HIGHWAY STANDARDS

TITLE SHEET	1	000001-08 - STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
INDEX OF SHEETS AND HIGHWAY STANDARDS, D1 STANDARDS, & COMMITMENTS	2	424001-11 - PERPENDICULAR CURB RAMPS FOR SIDEWALKS
GENERAL NOTES	3	424006-05 - DIAGONAL CURB RAMPS FOR SIDEWALKS
SUMMARY OF QUANTITIES	4-7	424011-04 - CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
TYPICAL SECTIONS	8	424021-06 - DEPRESSED CORNER FOR SIDEWALKS
CONSTRUCTION DETAILS	9-10	442201-03 - CLASS C AND D PATCHES
ALIGNMENT, TIES AND BENCHMARKS	11-12	602301-04 - INLET – TYPE A
PROPOSED PLANS	13-17	604001-05 - FRAMES AND LIDS TYPE 1
PAVEMENT MARKING PLANS	18-22	606001-08 - CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
ADA CURB RAMP DETAILS	23-26	701006-05 – OFF RD OPERATIONS, 2L, 2W, 15’ (4.5 M) TO 24” (600 mm) FROM PAVEMENT EDGE
PEDESTRIAN REFUGEE ISLAND CONSTRUCTION DETAIL	27	701301-04 - LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
EXISTING TRAFFIC SIGNAL PLANS – SCHMALE ROAD	28-29	701311-03 – LANE CLOSURE 2L, 2W MOVING OPERATIONS-DAY ONLY
EXISTING TRAFFIC SIGNAL PLANS – BLOOMINGDALE ROAD	30-31	701501-06 - URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)	32	701502-09 – URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	33	701701-10 - URBAN LANE CLOSURE, MULTILANE INTERSECTION
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	34	701801-06 - SIDEWALK, CORNER OR CROSSWALK CLOSURE
BUTT JOINT AND HMA TAPER DETAILS (BD-32)	35	701901-08 - TRAFFIC CONTROL DEVICES
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	36	780001-05 – TYPICAL PAVEMENT MARKINGS
DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	37	
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)	38	
DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)	39	
ARTERIAL ROAD INFORMATION SIGN (TC-22)	40	

DISTRICT 1 STANDARD DETAILS

- BD-8 – DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-22 – PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- BD-24 – CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
- BD-32 – BUTT JOINT AND HMA TAPER DETAILS
- TC-10 – TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-13 – DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- TC-16 - SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
- TC-22 – ARTERIAL ROAD INFORMATION SIGN
- TS-07 – DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

COMMITMENTS

NO PARKING WILL BE ALLOWED ON W. FULLERTON AVENUE WITHIN THE PROJECT LIMITS THROUGHOUT CONSTRUCTION.

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2023; REVISION 3, NOVEMBER 2021 OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD) AND "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" EIGHTH EDITION; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
2. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.
3. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.
5. THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE AND ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
7. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATIONS IS REQUIRED).
8. THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

STORM SEWER AND SANITARY SEWER

9. WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE SHALL BE COMPLETED PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS.
10. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE/SHE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLAN, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE.

11. ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.
12. STRUCTURE ADJUSTMENTS: PRECAST CONCRETE ADJUSTMENT RINGS ARE NOT TO EXCEED 12 INCHES IN OVERALL HEIGHT AND SHALL BE USED IF AN ADJUSTMENT TO THE FINISHED GRADE ESTABLISHED BY THE PROJECT ENGINEER IS NECESSARY. MAXIMUM TWO RINGS.
13. THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATIONS OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK MAY LED TO THE REJECTION OF THE STRUCTURE IN THE FIELD.
14. THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE VILLAGE WATER DEPARTMENT. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

15. ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION.
16. DIMENSIONS: THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
17. LIMITS OF PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
18. THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THEY ARE TO BE PLACED.
19. DEPRESSED CURB: RAMPS AND DEPRESSED CURBS ACCESSIBLE TO THE HANDICAPPED SHALL BE PROVIDED AT ALL CROSSWALKS. THE TRANSITION FROM DEPRESSED TO FULL HEIGHT SHALL BE 6' LONG WHERE THERE IS SIDEWALK ADJACENT TO BACK OF CURB.
20. DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS SHOWN ON THE PLANS.
21. ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.
22. FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS.
23. NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.
24. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION/DIRECTION AND MEANS/METHODS OF CONSTRUCTION.

25. BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ENGINEER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.
26. SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SOD APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.
27. ALL SAW CUTTING SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.
28. THE CONTRACTOR SHALL KEEP ONE LANE IN EACH DIRECTION OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL COMPLETE ALL MILLING OPERATIONS BEFORE INSTALLING ANY HOT-MIX ASPHALT PAVEMENT.
29. ALL TRAFFIC SIGNAL MODIFICATIONS/DETECTOR LOOP REPLACEMENT MUST BE PERFORMED BY AN IDOT PRE-QUALIFIED ELECTRICAL CONTRACTOR.
30. THE TRAFFIC SIGNAL DETECTOR LOOPS ON FULLERTON AVENUE AT SCHMALE ROAD (EAST LEG) AND FULLERTON AVENUE AT THE BLOOMINGDALE ROAD INTERSECTION (WEST LEG) MUST BE INSTALLED IN THE HMA BINDER COURSE BY AN IDOT DISTRICT 1 PRE-QUALIFIED ELECTRICAL CONTRACTOR.
31. THE ELECTRICAL CONTRACTOR MUST SUBMIT A CATALOG CUT OF THE TRAFFIC SIGNAL DETECTOR WIRE AND SEALANT TO THE COUNTY FOR REVIEW/APPROVAL PRIOR TO THE INSTALLATIONS VIA EMAIL: MARYANNE.SIOSON@DUPAGECO.ORG.
32. THE CONTRACTOR MUST PROVIDE AND MAINTAIN SUFFICIENT WASHROOMS FOR THE PROJECT.
33. ADVANCE WARNING SIGNS NEED TO BE ERECTED AT LEAST 7 DAYS PRIOR TO START OF CONSTRUCTION.
34. PRIOR TO INSTALLING SURFACE COURSE AND AT THE ENGINEER'S DISCRETION, THE CONTRACTOR SHALL SAWCUT THE EXISTING PAVEMENT TO PROVIDE A NEW BUTT JOINT TO PAVE AGAINST.
35. ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR. THE VILLAGE WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.
36. THE CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
37. THE CONTRACTOR SHALL CONTACT KALPANA HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72-HOURS IN ADVANCE OF BEGINNING WORK.
38. PLEASE CONTACT MS. MARYANNE SIOSON, DUPAGE COUNTY'S TRAFFIC SIGNAL ENGINEER AT (630) 407-6908 A MINIMUM OF 48 HOURS IN ADVANCE TO INSPECT THE TRAFFIC SIGNAL DETECTOR LOOP LAYOUT PRIOR TO INSTALLATIONS.
39. A PRE-CONSTRUCTION VIDEO WILL BE PERFORMED PRIOR TO THE START OF CONSTRUCTION BY THE ENGINEER TO DOCUMENT THE EXISTING CONDITIONS.

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FULLERTON AVENUE RESURFACING GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\GLENDALE\IGHTS\040051D208\Civil\NOT	D208.SHT	DRAWN - BRL	REVISED -			1379	21-00073-00-R5	DUPAGE	40	3	
	PLOT SCALE = NOT TO SCALE	CHECKED - AMP	REVISED -			CONTRACT NO. 61J85					
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -			SCALE: N.T.S.	SHEET NO. 3 OF 40 SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES

FUNDING SOURCE					STP 60% FED 40% LOCAL		
CONSTRUCTION TYPE CODE					0005	0042	0021
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAINEES	NON-PARTICIPATING
~	20101100	TREE TRUNK PROTECTION	EACH	35	35		
~	20101200	TREE ROOT PRUNING	EACH	31	31		
~	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	6	6		
~	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	12	12		
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	425	425		
	20800150	TRENCH BACKFILL	CU YD	160	160		
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2,340	2,340		
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	915	915		
	21101630	TOPSOIL FURNISH AND PLACE, 8"	SQ YD	75	75		
	25200110	SODDING, SALT TOLERANT	SQ YD	920	920		
	25200200	SUPPLEMENTAL WATERING	UNIT	50	50		
	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	25	25		
	28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	75	75		
	28000510	INLET FILTERS	EACH	71	71		
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	390	390		
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15,845	15,845		
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	50	50		
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	205	205		
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	3,290	3,290		
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1,975	1,975		

SUMMARY OF QUANTITIES

FUNDING SOURCE					STP 60% FED 40% LOCAL		
CONSTRUCTION TYPE CODE					0005	0042	0021
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAINEES	NON-PARTICIPATING	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	29,590	8,455		21,135	
44000100	PAVEMENT REMOVAL	SQ YD	70	70			
42400800	DETECTABLE WARNINGS	SQ FT	170	170			
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	23,470	23,470			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	130	130			
44000600	SIDEWALK REMOVAL	SQ FT	30,295	9,160		21,135	
44003100	MEDIAN REMOVAL	SQ FT	310	310			
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	585	585			
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	585	585			
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	585	585			
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	585	585			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	245	245			
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	35	35			
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	5	5			
55100500	STORM SEWER REMOVAL 12"	FOOT	245	245			
55100700	STORM SEWER REMOVAL 15"	FOOT	35	35			
55100900	STORM SEWER REMOVAL 18"	FOOT	5	5			
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	155	155			
60600605	CONCRETE CURB, TYPE B	FOOT	155	155			
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	310	310			

SUMMARY OF QUANTITIES

FUNDING SOURCE					STP 60% FED 40% LOCAL		
CONSTRUCTION TYPE CODE					0005	0042	0021
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAINEES	NON-PARTICIPATING
~	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	25	25		
~	66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1		
~	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1		
~	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1		
~	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	30	30		
	67100100	MOBILIZATION	L SUM	1	1		
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1		
	70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1		
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1		
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,475	2475		
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	300	300		
~	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	415	415		
~	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,780	9,780		
~	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,925	1,925		
~	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,305	1,305		
~	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	285	285		
~	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2		
	X0323677	STREET SWEEPING	HOUR	8	8		
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	101	101		

SUMMARY OF QUANTITIES

FUNDING SOURCE					STP 60% FED 40% LOCAL		
CONSTRUCTION TYPE CODE					0005	0042	0021
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	TRAINEES	NON-PARTICIPATING	
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	8	8			
X4023000	TEMPORARY ACCESS (ROAD)	EACH	8	8			
X4400501	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	215	215			
X4400503	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	980	980			
X6020710	CATCH BASINS TO BE ADJUSTED WITH SPECIAL FRAME AND GRATE	EACH	66	66			
X6023508	INLETS, TYPE A, WITH SPECIAL FRAME AND GRATE	EACH	1	1			
~ X8860105	DETECTOR LOOP REPLACEMENT	FOOT	1,185	1185			
XX002260	STRUCTURES TO BE REMOVED	EACH	1	1			
XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	565	565			
XX006429	SIDEWALK, SPECIAL	SQ FT	500	500			
XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	250	250			
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1			
Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	9	9			
Z0017500	DRAINAGE & UTILITY STRUCTURE ADJUSTMENT (SPECIAL)	EACH	18	18			
Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	2	2			
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	104	104			
Z0076600	TRAINEES	HOURL	500		500		
Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURL	500		500		

~ INDICATES SPECIALTY ITEM

FILE NAME: N:\S\ENDALEY\21075\2107510209\CONTRACT NO. 61J85	USER NAME: bjhunee	DESIGNED - BRL	REVISIONS:	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.U. RTE. 1379	SECTION 21-00073-00-R5	COUNTY	TOTAL SHEETS 40	SHEET NO. 7
PLO SCALE: NOT TO SCALE	PLO DATE: 9/13/2023	DRAWN - BRL CHECKED - AMP DATE - 9/13/2023	REVISIONS:	SCALE: N.T.S.	SHEET NO. 7 OF 40 SHEETS	STA. TO STA.	DUPAGE	CONTRACT NO. 61J85	ILLINOIS FED. AID PROJECT	

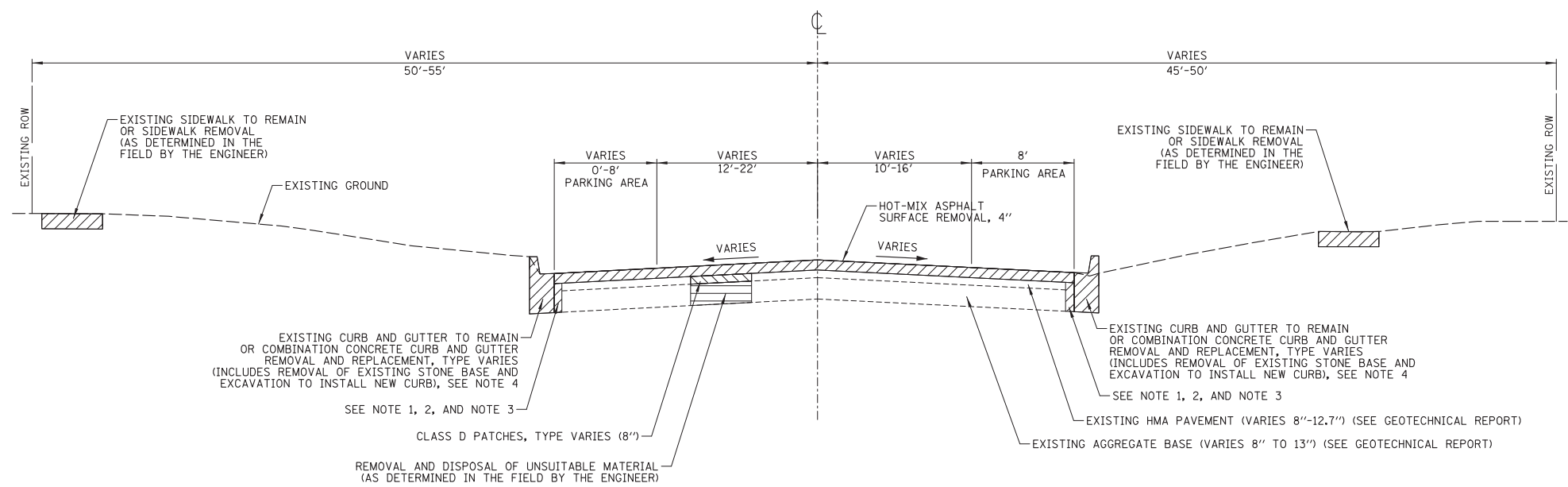
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE ITEM	AIR VOIDS @ Ndes	OMP DESIGNATION
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 1 1/2"	4% @ 50 GYR	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"	4% @ 50 GYR	LR 1030-2
HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT		
RESIDENTIAL HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; 3" (IN 2 LIFTS)	4% @ 50 GYR	LR 1030-2
COMMERCIAL HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50; 2"	4% @ 50 GYR	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 4"	4% @ 50 GYR	LR 1030-2
PATCHING		
CLASS D PATCHES, TYPE (I, II, III, IV) 8 INCHES (HMA BINDER IL-19)	4% @ 70 GYR	LR 1030-2
OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSUARNC (QA/QC) PER LR 1030-2		

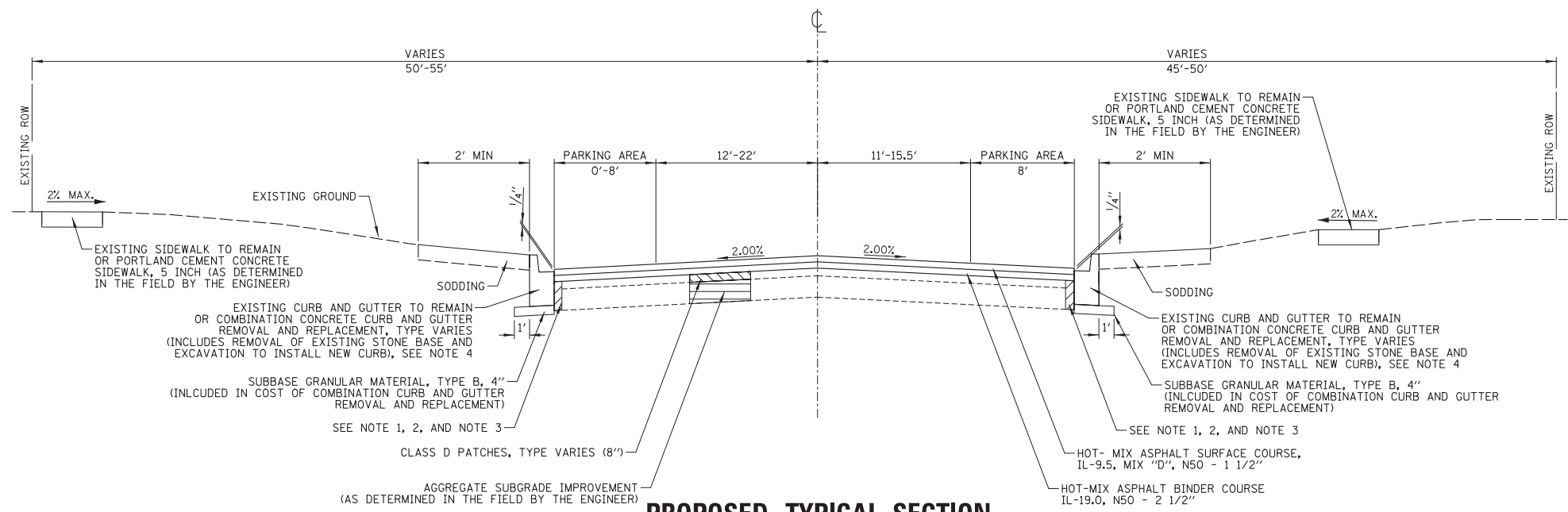
- NOTES:
- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 - THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

GENERAL NOTES:

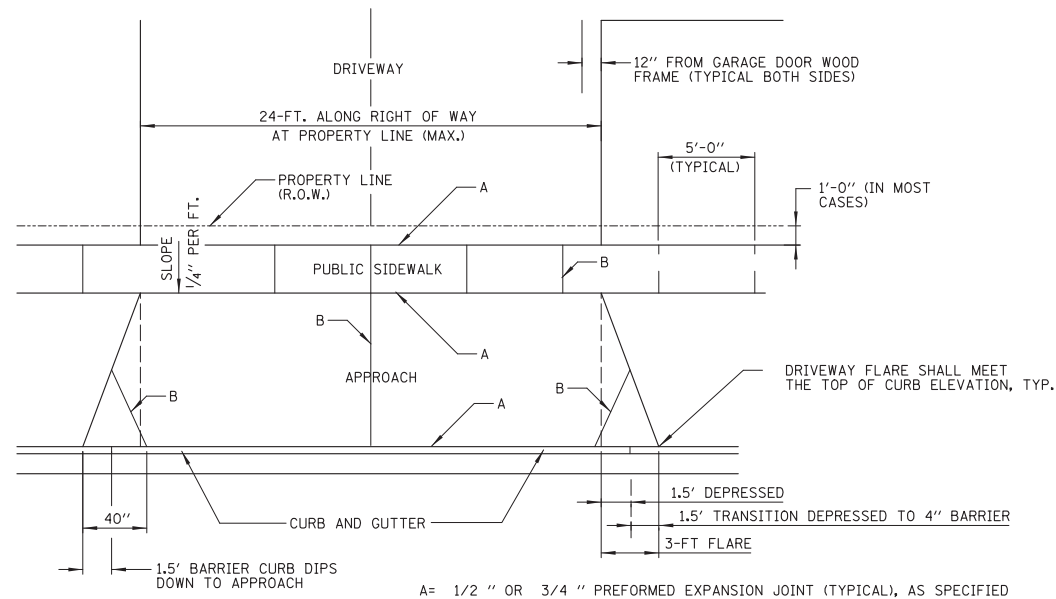
- PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
- LIMITS OF CURB AND GUTTER REPAIR WORK TO BE MARKED IN FIELD BY ENGINEER PRIOR TO CONSTRUCTION.
- ALL WORK INCLUDING SOD MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
- FOR ROADWAYS WITH DIFFERING ELEVATIONS AT THE EDGE OF PAVEMENT, CONTRACTOR SHALL PAVE THE SIDE WITH THE HIGHER ELEVATION FIRST. THE CONTRACTOR SHALL PROVIDE GRADE STAKES SHOWING FINISHED PAVEMENT GRADES AT 2% (MIN) OFF HIGH SIDE OF CENTERLINE ELEVATION.
- CONTRACTOR SHALL VERIFY CROSS SLOPE OF ROADWAY MEETS 2% (MIN). ANY AREAS THAT DO NOT MEET THIS REQUIREMENT SHALL BE CORRECTED BY THE CONTRACTOR PRIOR TO PAYMENT.
- CONTRACTOR SHALL MILL THE EXISTING PAVEMENT PRIOR TO INSTALLATION OF PATCHING.



EXISTING TYPICAL SECTION
STA. 100+48.50 TO STA. 153+32.87



PROPOSED TYPICAL SECTION
STA. 100+48.50 TO STA. 153+32.87

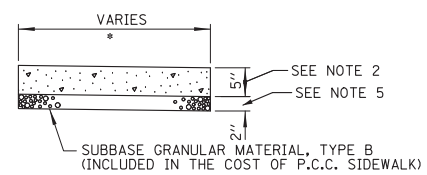


DRIVEWAY WITH CURB AND GUTTER

GENERAL NOTES:

- DRIVEWAY SHALL NOT EXCEED 24 FT. IN WIDTH MEASURED AT THE RIGHT OF WAY
- DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF PREFERABLY 8%.
- APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 10%.
- ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED. (95% PROCTOR)
- PUBLIC SIDEWALK SHALL BE 6" THICK P.C. CONCRETE AT DRIVEWAY. (NO WIRE MESH) (PAID FOR AS P.C.C. SIDEWALK, 5 INCH)
- MINIMUM THICKNESS FOR APPROACH. (NO WIRE MESH) INCLUDED IN COST FOR DRIVEWAY REPLACEMENT.
 - 6" P.C. CONCRETE (HIGH EARLY STRENGTH) ON 4" COMPACTED AGGREGATE BASE COURSE, TYPE B
 - 8" P.C. CONCRETE (HIGH EARLY STRENGTH) ON 6" COMPACTED AGGREGATE BASE COURSE, TYPE B (COMMERCIAL DRIVEWAYS)
- 3" HOT-MIX ASPHALT SURFACE ON 6" COMPACTED AGGREGATE BASE COURSE, TYPE B
2" HOT-MIX ASPHALT SURFACE AND 4" HOT-MIX ASPHALT BINDER ON 8" AGGREGATE BASE COURSE, TYPE B (COMMERCIAL DRIVEWAYS)

A= 1/2 " OR 3/4 " PREFORMED EXPANSION JOINT (TYPICAL), AS SPECIFIED
B= TOOLED OR SAWED CONTRACTION JOINTS

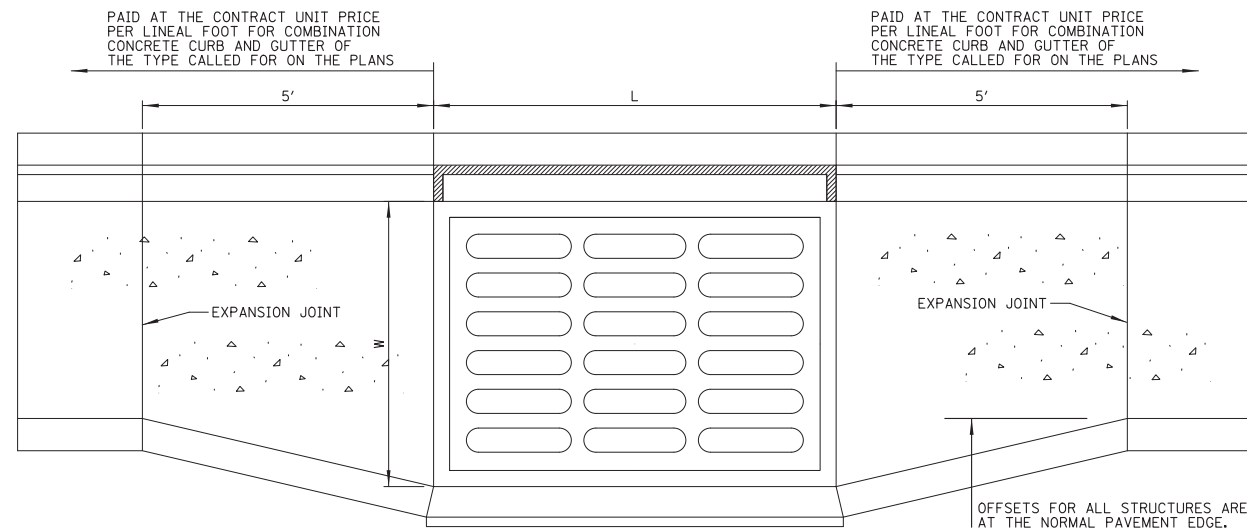


NOTES:

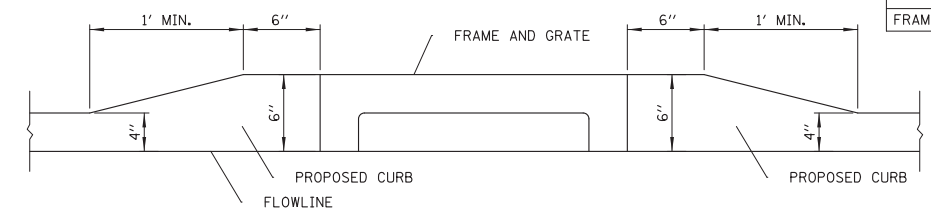
- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 5 INCH. THIS INCLUDES SIDEWALK THAT MAY BE RELOCATED.
- THICKNESS SHALL BE INCREASED TO 6" WHERE SIDEWALK IS ADJACENT TO A DRIVEWAY AND 5" WHERE SIDEWALK IS LOCATED WITHIN DUPAGE COUNTY RIGHT-OF-WAY. NO WIRE MESH. (INCLUDED IN THE COST OF P.C.C. SIDEWALK).
- IN LOCATIONS WHERE SIDEWALK IS REMOVED AND REPLACED THROUGH DRIVEWAYS, DRIVEWAYS SHALL BE SAWCUT AND PATCHED A MINIMUM OF 1' ON EITHER SIDE OF THE WALK. THIS WORK SHALL BE PAID FOR PER SQUARE YARD AT THE CONTRACT UNIT PRICE FOR CONCRETE DRIVEWAY REPLACEMENT OR HOT-MIX ASPHALT DRIVEWAY REPLACEMENT.
- WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
- SIDEWALKS SHALL BE PLACED ON 2" OF SUBBASE GRANULAR MATERIAL WHEN REPLACING EXISTING SIDEWALK, AND 4" OF SUBBASE GRANULAR MATERIAL WHEN CONSTRUCTING NEW SIDEWALK. (INCLUDED IN THE COST OF SIDEWALK)

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH DETAIL

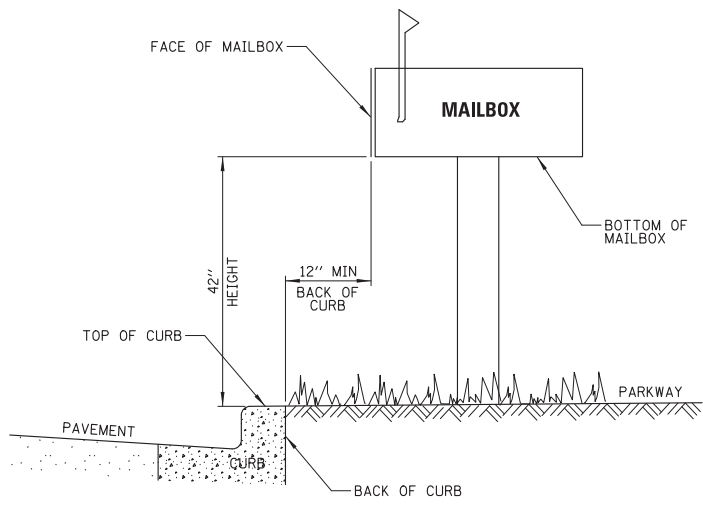
* CROSS SLOPE 0.5% (MIN.) TO 1.9% (MAX.) OR AS SHOWN ON ADA CURB RAMP DETAILS



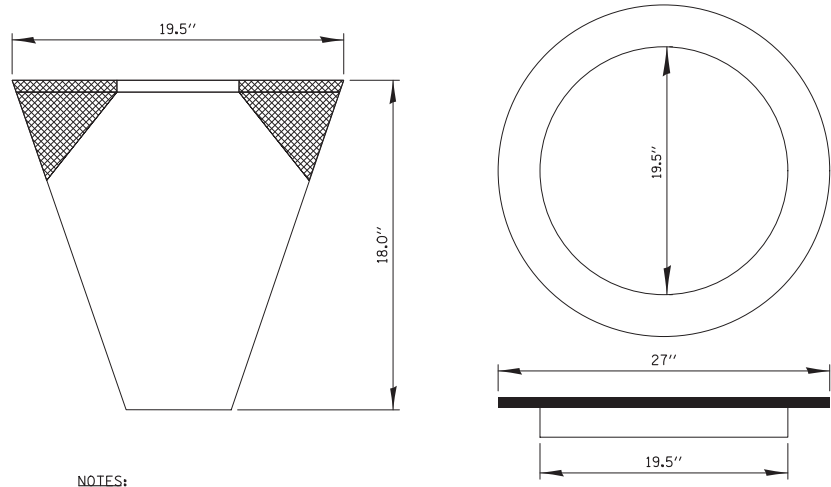
GUTTER WIDENING AT DRAINAGE STRUCTURES



STRUCTURE TO BE ADJUSTED WITH SPECIAL FRAME AND GRATE



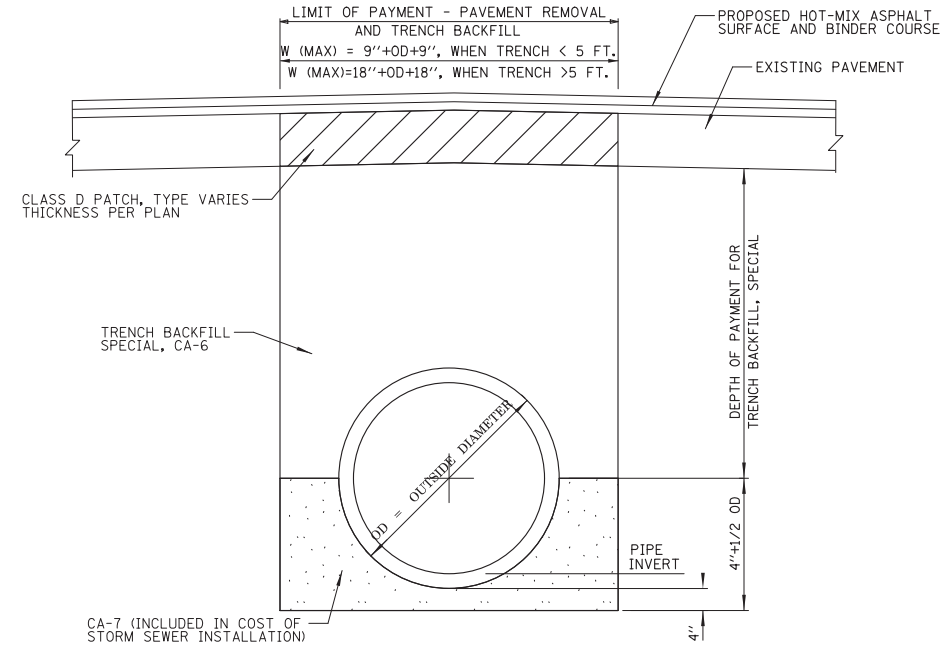
LOCATION OF MAILBOX IN PARKWAY AREAS



NOTES:

- FRAME: FLANGE SHALL BE FABRICATED FROM 1/8" FLAT STACK. BASE RING SHALL BE FABRICATED FROM 1 1/2" x 1 1/2" x 1/8" CHANNEL. DOMESTIC STEEL CONFORMING TO ASTM-A36.
- SEDIMENT BAG: SHALL BE FABRICATED FROM 4 OZ/SQ YD NON-WAVEN POLYPROPYLENE GEOTEXTILE AND SHALL BE REINFORCED WITH POLYESTER MESH. THE BAG SHALL BE SECURED TO THE BASE RING WITH STAINLESS STEEL STRAP AND LOCK.

CATCH BASIN AND INLET FILTER
N.T.S.



NOMINAL PIPE DIAMETER INCHES	PIPE OD INCHES	W (TRENCH WIDTH PAY LIMITS)	
		INCHES < 5' DEPTH	INCHES > 5' DEPTH
12"	16"	34"	52"
15"	19.5"	37.5"	55.5"
18"	23"	41"	59"

TRENCH DETAIL FOR STORM SEWER

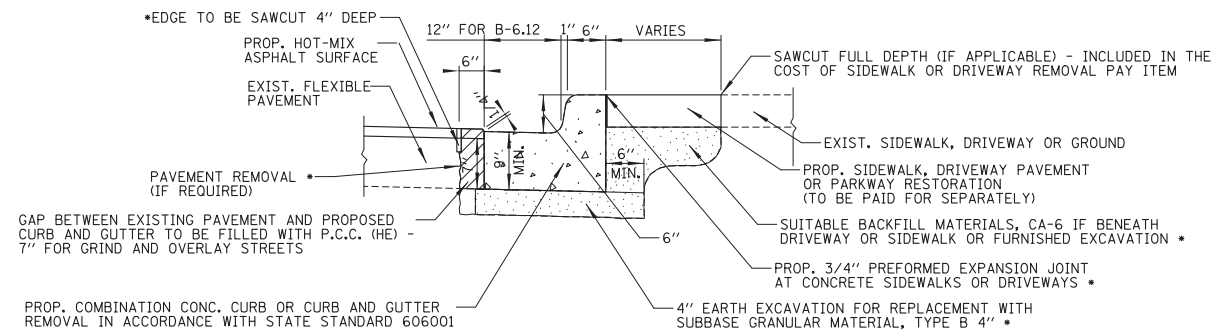
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	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FULLERTON AVENUE RESURFACING
CONSTRUCTION DETAILS

SCALE: N.T.S. SHEET NO. 9 OF 40 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	9
			CONTRACT NO. 61J85	
ILLINOIS FED. AID PROJECT				

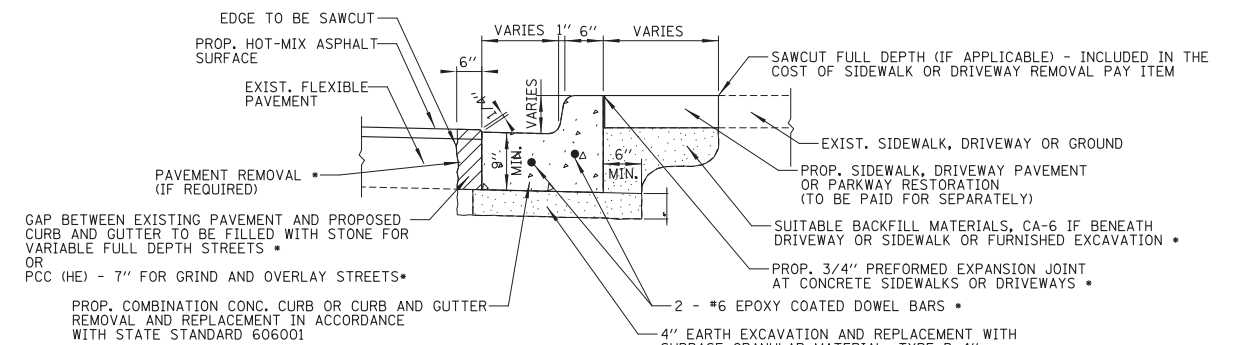


- INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

NOTES:

1. LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF COMBINATION CURB GUTTER REMOVAL.
2. WORK TO INSTALL CURB AND GUTTER INCLUDES ALL NECESSARY PAVEMENT REMOVAL, EXCAVATION, EMBANKMENT AND SUBBASE GRANULAR MATERIAL. EXCAVATED MATERIAL WILL NOT BE PERMITTED TO BE STOCKPILED BEHIND THE CURB.
3. FOR GRIND AND OVERLAY STREETS, ANY EXISTING PAVEMENT REMOVAL ADJACENT TO THE NEW CURB AND GUTTER SHALL BE REPLACED WITH CLASS S1 CONCRETE.
4. EXPANSION JOINTS PLACED AT A MAXIMUM 60 FEET SPACING. CONTRACTION JOINTS PLACED AT A MAXIMUM 20 FEET SPACING.
5. ALL FRAMING SHALL BE SET TO FINAL GRADE OF THE POUR. NO ANGLE IRONS WILL BE ALLOWED. NO WATERING CANS SHALL BE ALLOWED ON SITE.

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

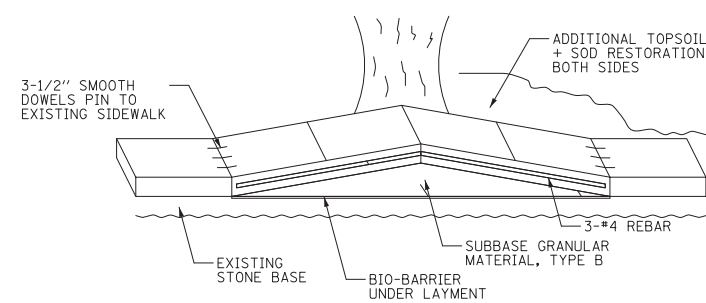


- INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

NOTES:

1. LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (SPECIAL).
2. WORK TO REMOVE AND REPLACE CURB AND GUTTER INCLUDES ALL NECESSARY PAVEMENT REMOVAL, EXCAVATION, EMBANKMENT AND SUBBASE GRANULAR MATERIAL. EXCAVATED MATERIAL WILL NOT BE PERMITTED TO BE STOCKPILED BEHIND THE CURB.
3. FOR GRIND AND OVERLAY STREETS, ANY EXISTING PAVEMENT REMOVAL ADJACENT TO THE NEW CURB AND GUTTER SHALL BE REPLACED WITH CLASS S1 CONCRETE. FOR STREETS WITH FULL DEPTH REPLACEMENT, ANY EXISTING PAVEMENT REMOVED ADJACENT TO THE NEW CURB AND GUTTER SHALL BE REPLACED WITH SUITABLE STONE BACKFILL.
4. EXPANSION JOINTS PLACED AT A MAXIMUM 60 FEET SPACING. CONTRACTION JOINTS PLACED AT A MAXIMUM 20 FEET SPACING.
5. ALL FRAMING SHALL BE SET TO FINAL GRADE OF THE POUR. NO ANGLE IRONS WILL BE ALLOWED. NO WATERING CANS SHALL BE ALLOWED ON SITE.

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



SIDEWALK SPECIAL (ROOT PRESERVATION)

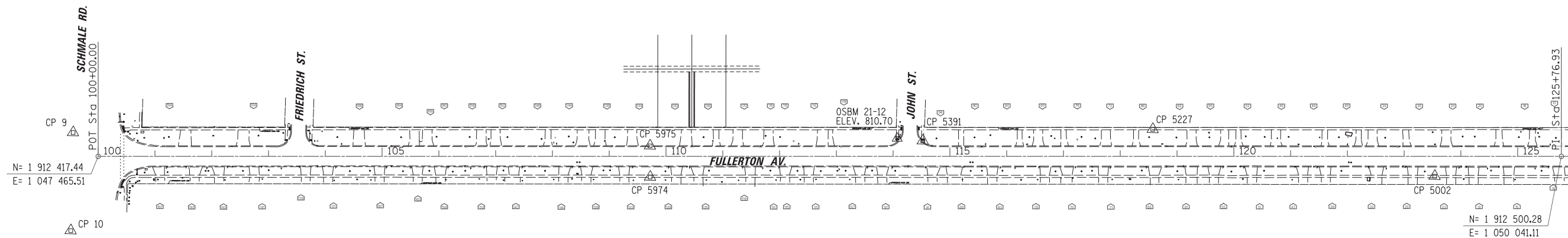
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	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FULLERTON AVENUE RESURFACING
CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET NO. 10 OF 40 SHEETS STA. TO STA.

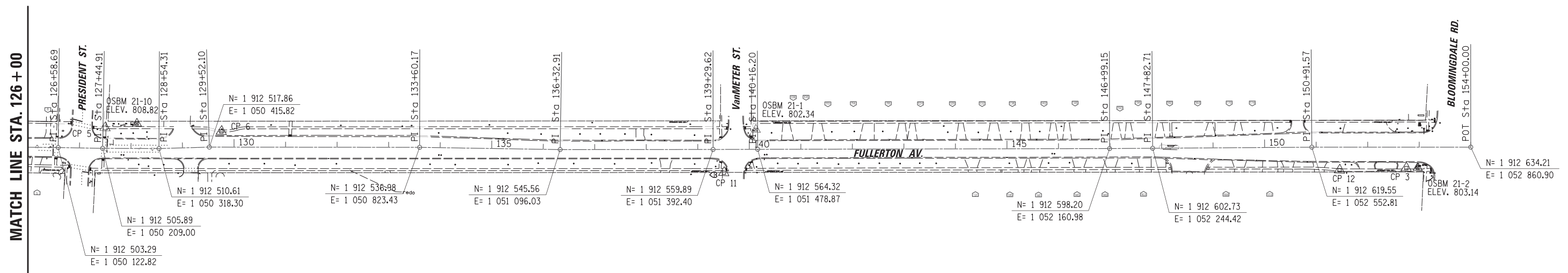
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	10
			CONTRACT NO. 61J85	
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 126 + 00

HORIZONTAL CONTROL POINTS					
CP NO.	NORTHING	EASTING	STATION	OFFSET	DESCRIPTION
9	1 912 457.98	1 047 420.02	OFFSITE	OFFSITE	CP-XCUT
10	1 912 285.43	1 047 421.43	OFFSITE	OFFSITE	CP-XCUT
5975	1 912 467.20	1 048 436.30	109+71.88	18.53' LT	CP-XCUT
5974	1 912 412.41	1 048 438.38	109+72.20	36.30' RT	CP-XCUT
5391	1 912 491.51	1 048 916.19	114+52.31	27.40' LT	CP-XCUT
5227	1 912 524.72	1 049 320.31	118+57.28	47.60' LT	CP-XCUT
5002	1 912 458.30	1 049 819.26	123+53.84	34.83' RT	CP-XCUT

ELEVATION BENCHMARKS DATUM: NAVD '88 (GPS OBSERVED)		
NO.	DESCRIPTION	ELEV.
OSBM 21-12	MINI RAILROAD SPIKE SET IN SOUTH FACE OF UTILITY POLE AT NORTHWEST CORNER OF FULLERTON AV. & JOHN ST.	810.70
OSBM 21-10	SQUARE CUT ON SOUTHWEST CORNER OF CONC. BASE OF TEL BOX AT NORTHEAST CORNER OF PRESIDENT ST. & FULLERTON AV.	808.82



MATCH LINE STA. 126 + 00

HORIZONTAL CONTROL POINTS					
CP NO.	NORTHING	EASTING	STATION	OFFSET	DESCRIPTION
5	1 912 545.83	1 050 212.05	127+49.67	39.77' LT	CP-XCUT
6	1 912 549.38	1 050 437.87	129+75.60	30.25' LT	CP-XCUT
11	1 912 516.01	1 051 415.69	139+50.63	45.02' RT	CP-PK
12	1 912 577.62	1 052 608.78	151+45.46	44.47' RT	CP-XCUT
3	1 912 587.68	1 052 737.93	152+74.95	40.59' RT	CP-XCUT

ELEVATION BENCHMARKS DATUM: NAVD '88 (GPS OBSERVED)		
NO.	DESCRIPTION	ELEV.
OSBM 21-1	SOUTHWEST BOLT OF HYDRANT AT THE NE CORNER OF FULLERTON AV. & VAN METER ST.	802.34
OSBM 21-2	X-CUT ON SW CORNER OF DOUBLE TRAFFIC LIGHT HANDHOLE AT SW CORNER OF FULLERTON AV. & BLOOMINGDALE RD.	803.14

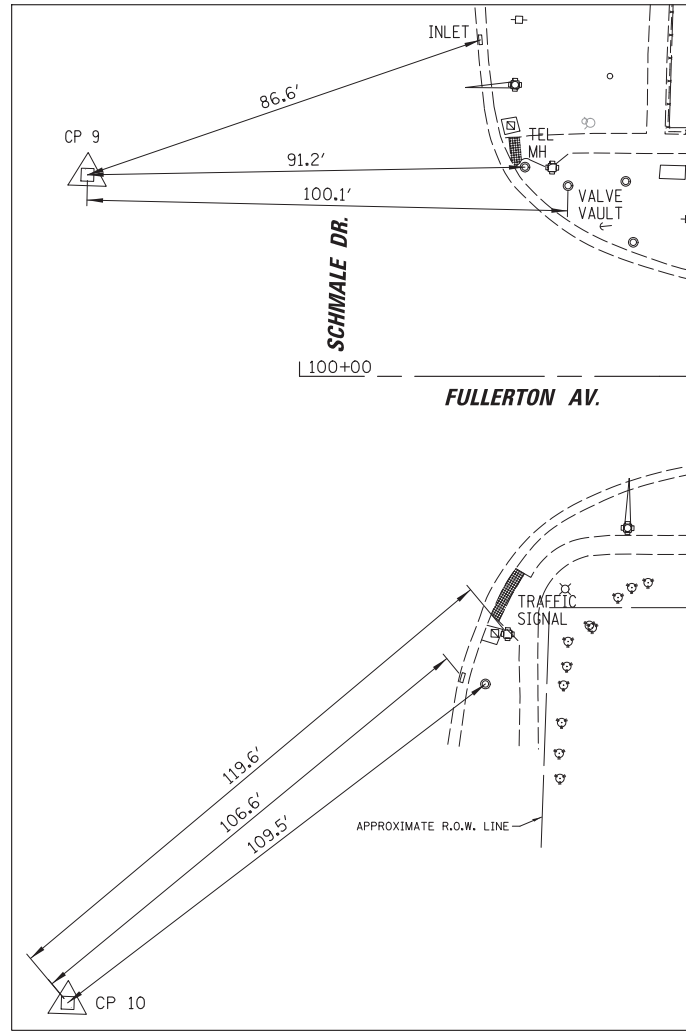
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

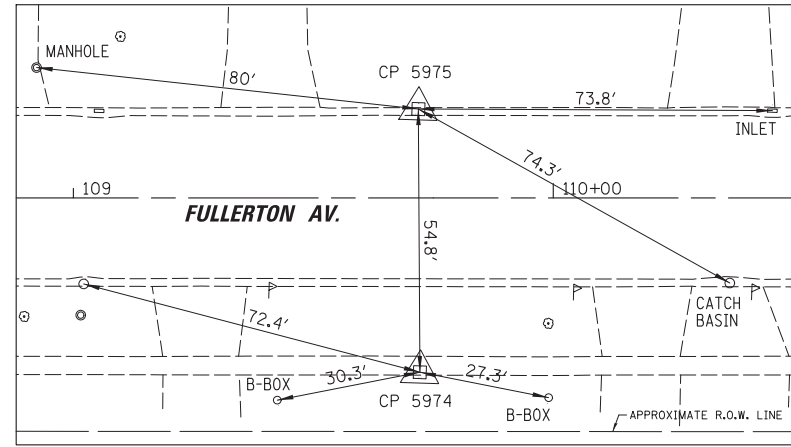
**FULLERTON AVENUE
ALIGNMENT, TIES AND BENCHMARKS**

SCALE: 100' SHEET NO. 11 OF 40 SHEETS STA. 100+00.00 TO STA. 154+00.00

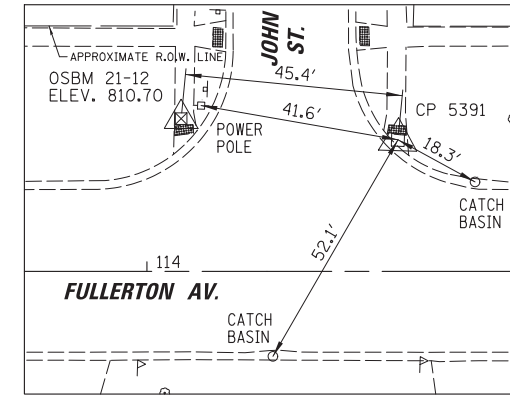
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ILLINOIS FED. AID PROJECT				CONTRACT NO. 61J85



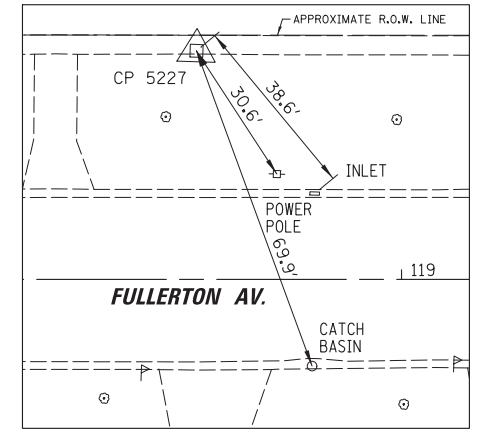
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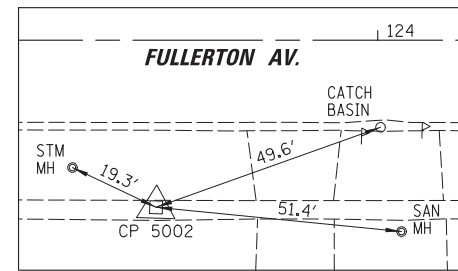
CP 5974, CP 5975



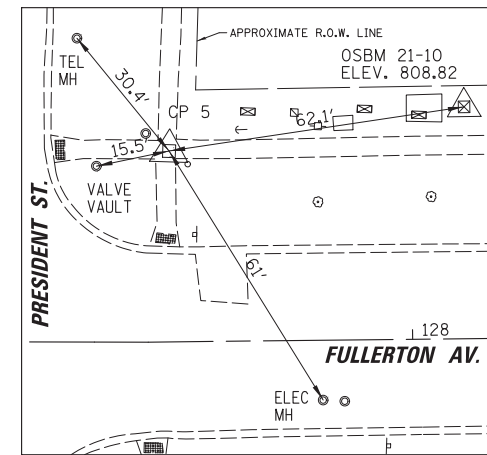
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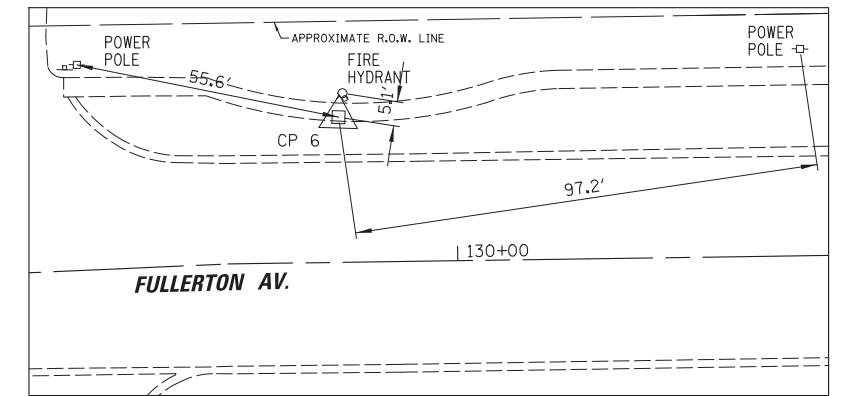
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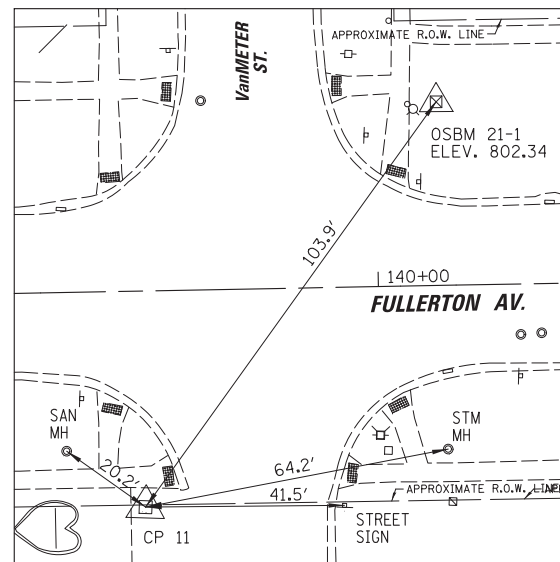
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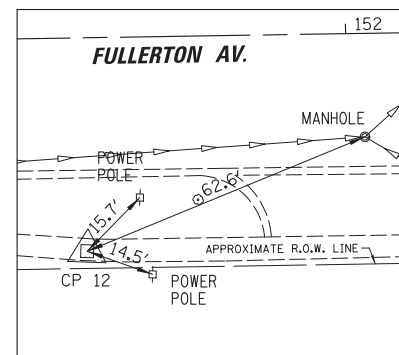
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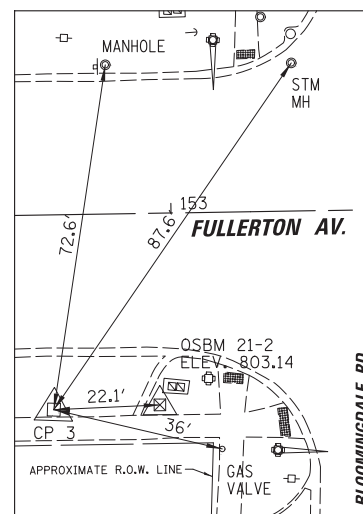
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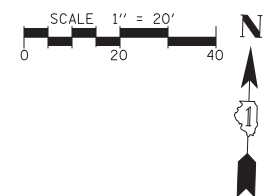
CP 11



CP 12



CP 3



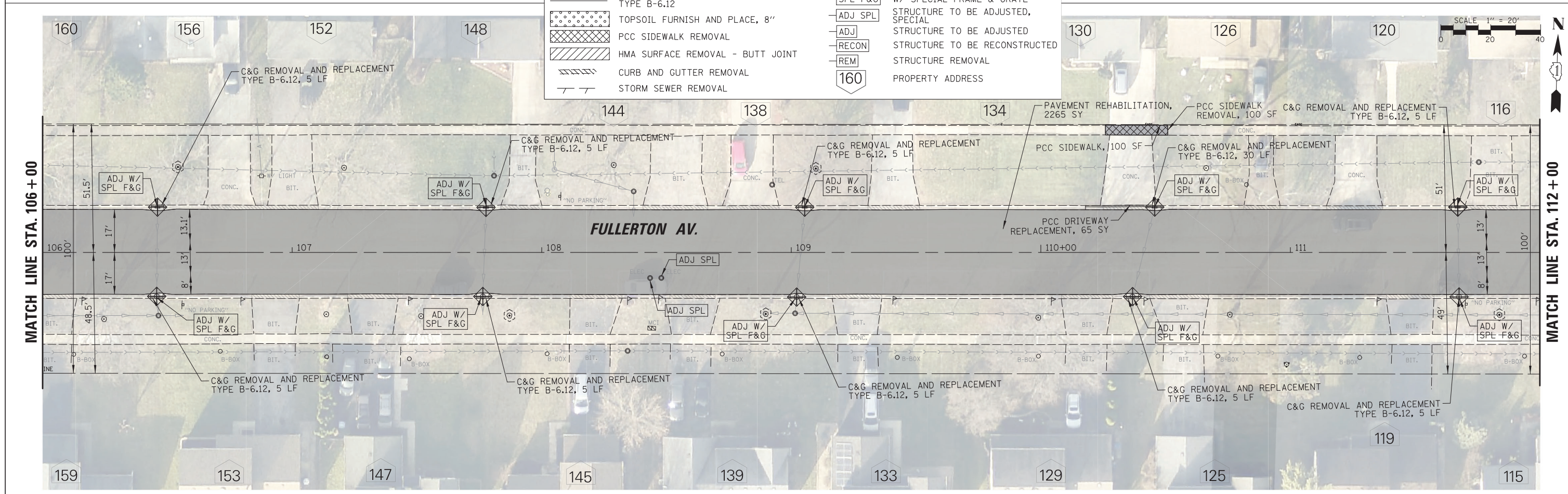
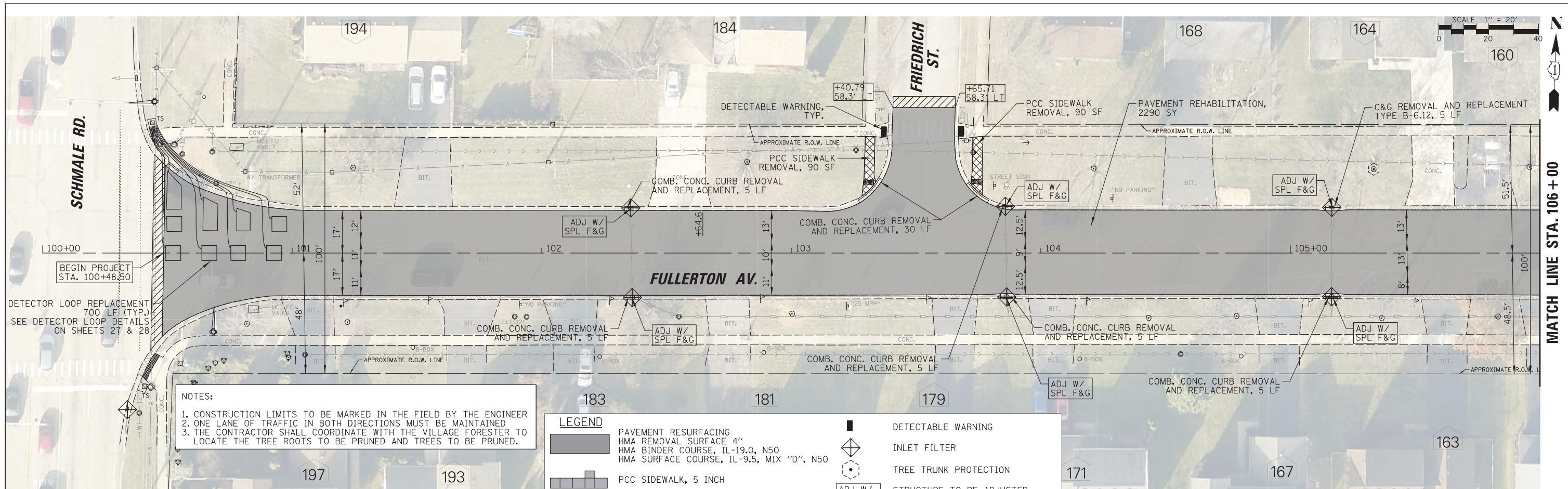
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		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FULLERTON AVENUE
ALIGNMENT, TIES AND BENCHMARKS**

SCALE: 20' SHEET NO. 12 OF 40 SHEETS STA. 100+00.00 TO STA. 154+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	12
				CONTRACT NO. 61J85
ILLINOIS FED. AID PROJECT				



NOTES:

1. CONSTRUCTION LIMITS TO BE MARKED IN THE FIELD BY THE ENGINEER
2. ONE LANE OF TRAFFIC IN BOTH DIRECTIONS MUST BE MAINTAINED
3. THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE FORESTER TO LOCATE THE TREE ROOTS TO BE PRUNED AND TREES TO BE PRUNED.

LEGEND	
	PAVEMENT RESURFACING HMA REMOVAL SURFACE 4" HMA BINDER COURSE, IL-19.0, N50 HMA SURFACE COURSE, IL-9.5, MIX "D", N50
	DETECTABLE WARNING
	PCC SIDEWALK, 5 INCH
	PROPOSED CURB AND GUTTER TYPE B-6.12
	TOPSOIL FURNISH AND PLACE, 8"
	PCC SIDEWALK REMOVAL
	HMA SURFACE REMOVAL - BUTT JOINT
	CURB AND GUTTER REMOVAL
	STORM SEWER REMOVAL
	INLET FILTER
	TREE TRUNK PROTECTION
	STRUCTURE TO BE ADJUSTED W/ SPECIAL FRAME & GRATE
	STRUCTURE TO BE ADJUSTED, SPECIAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE RECONSTRUCTED
	STRUCTURE REMOVAL
	PROPERTY ADDRESS

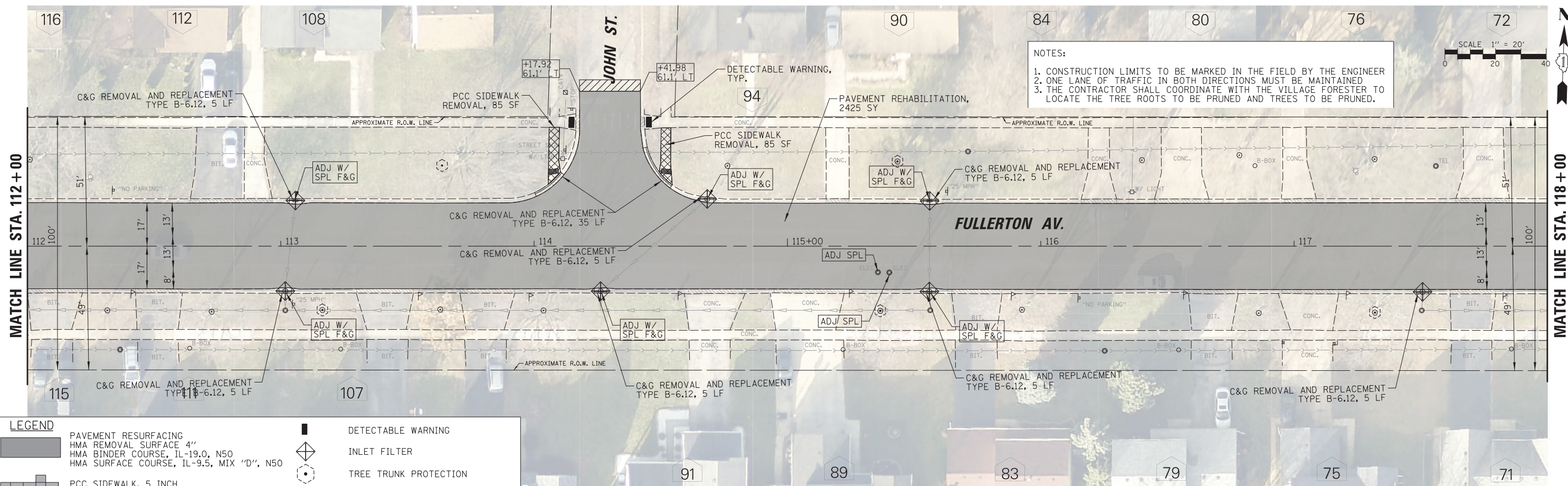
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		CHECKED - AMP	REVISED -
		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

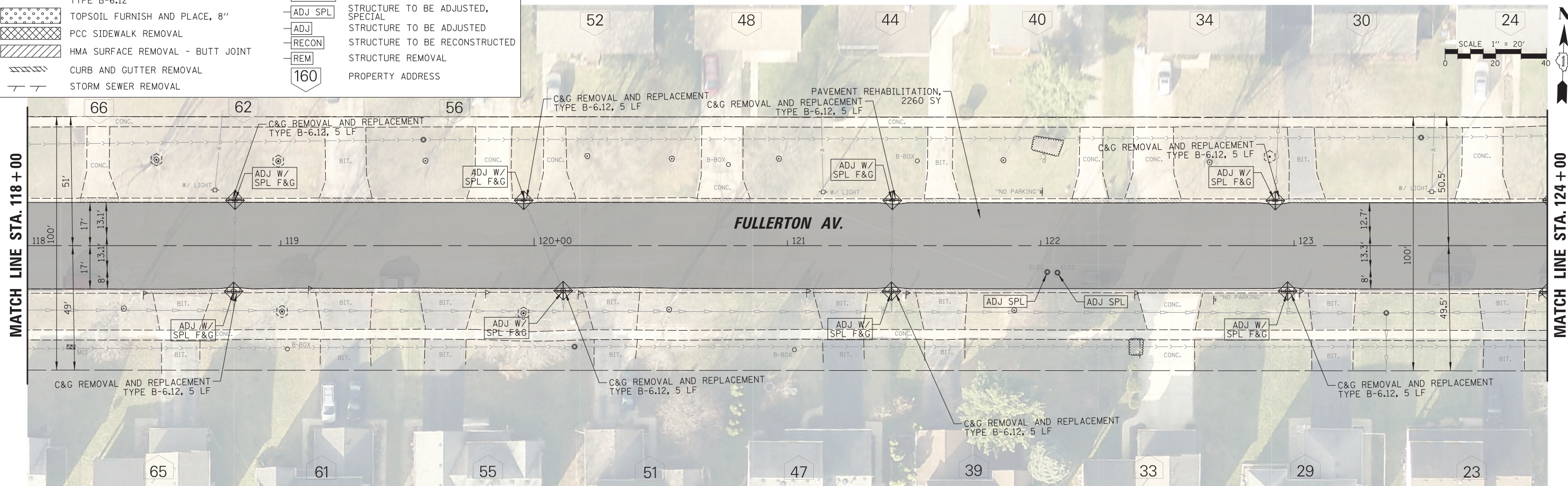
**FULLERTON AVENUE RESURFACING
PROPOSED PLANS**

SCALE: 20' SHEET NO. 13 OF 40 SHEETS STA. 100+48.50 TO STA. 112+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	13
			CONTRACT NO. 61J85	
ILLINOIS FED. AID PROJECT				



LEGEND	
	PAVEMENT RESURFACING HMA REMOVAL SURFACE 4" HMA BINDER COURSE, IL-19.0, N50 HMA SURFACE COURSE, IL-9.5, MIX "D", N50
	PCC SIDEWALK, 5 INCH
	PROPOSED CURB AND GUTTER TYPE B-6.12
	TOPSOIL FURNISH AND PLACE, 8"
	PCC SIDEWALK REMOVAL
	HMA SURFACE REMOVAL - BUTT JOINT
	CURB AND GUTTER REMOVAL
	STORM SEWER REMOVAL
	DETECTABLE WARNING
	INLET FILTER
	TREE TRUNK PROTECTION
	STRUCTURE TO BE ADJUSTED W/ SPECIAL FRAME & GRATE
	STRUCTURE TO BE ADJUSTED, SPECIAL
	STRUCTURE TO BE ADJUSTED
	STRUCTURE TO BE RECONSTRUCTED
	STRUCTURE REMOVAL
	PROPERTY ADDRESS

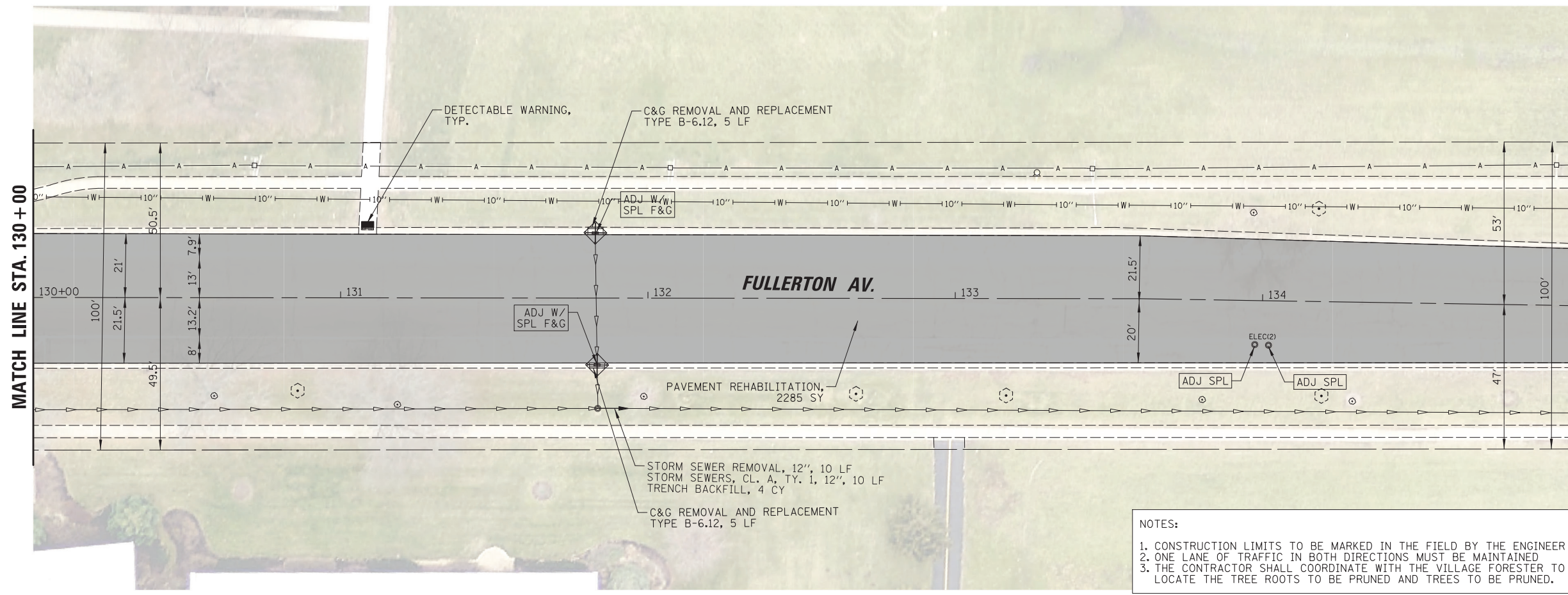
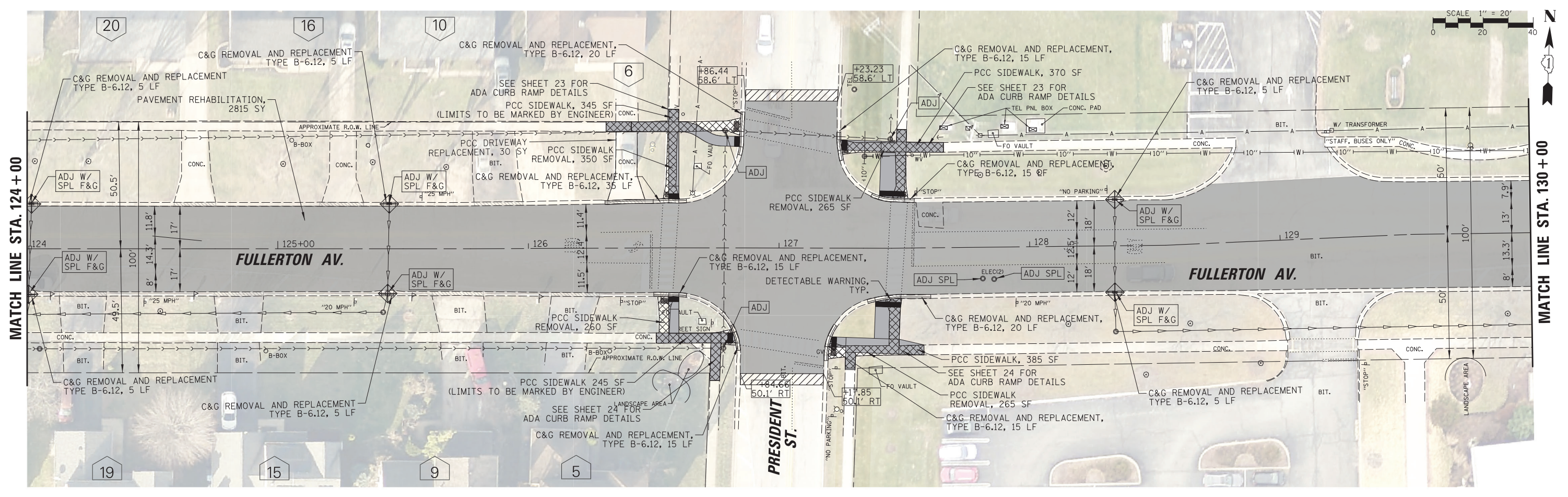


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	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PROPOSED PLANS		
SCALE: 28'	SHEET NO. 14 OF 40 SHEETS	STA. 112+00.00 TO STA. 124+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	14
			CONTRACT NO.	61J85
ILLINOIS FED. AID PROJECT				



LEGEND

- PAVEMENT RESURFACING
- HMA REMOVAL SURFACE 4"
- HMA BINDER COURSE, IL-19.0, N50
- HMA SURFACE COURSE, IL-9.5, MIX "D", N50
- PCC SIDEWALK, 5 INCH
- PROPOSED CURB AND GUTTER TYPE B-6.12
- TOPSOIL FURNISH AND PLACE, 4"
- PCC SIDEWALK REMOVAL
- HMA SURFACE REMOVAL - BUTT JOINT
- CURB AND GUTTER REMOVAL
- STORM SEWER REMOVAL
- DETECTABLE WARNING
- INLET FILTER
- TREE TRUNK PROTECTION
- ADJ W/SPL F&G STRUCTURE TO BE ADJUSTED W/ SPECIAL FRAME & GRATE
- ADJ SPL STRUCTURE TO BE ADJUSTED, SPECIAL
- RECON STRUCTURE TO BE RECONSTRUCTED
- REM STRUCTURE TO BE REMOVED
- PROPERTY ADDRESS

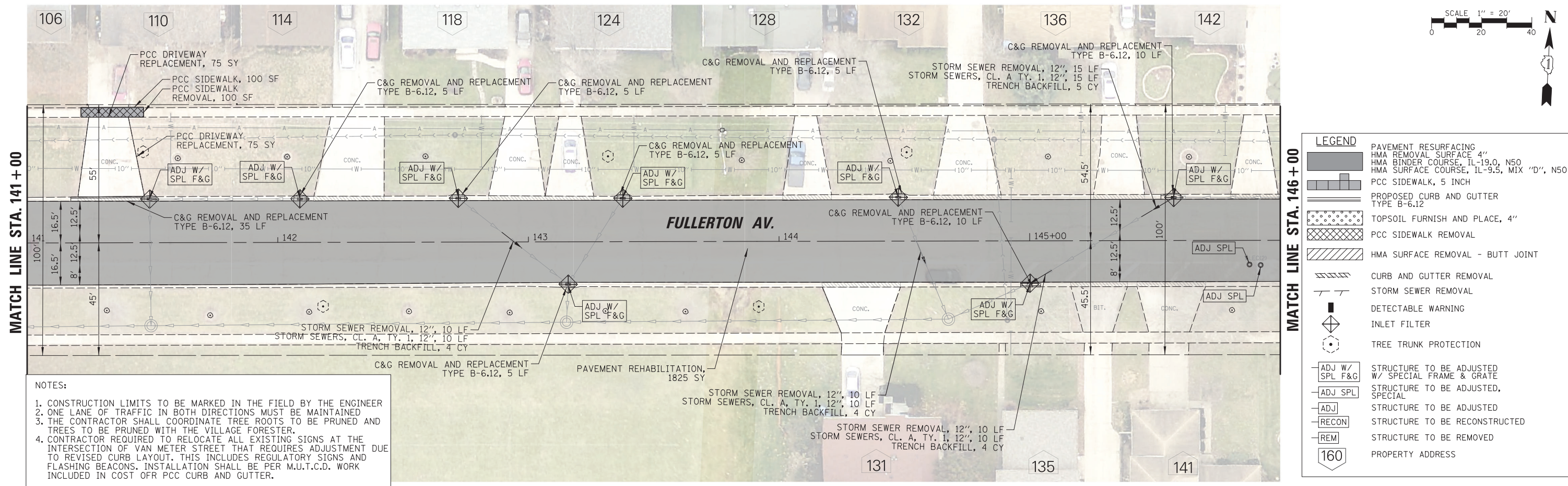
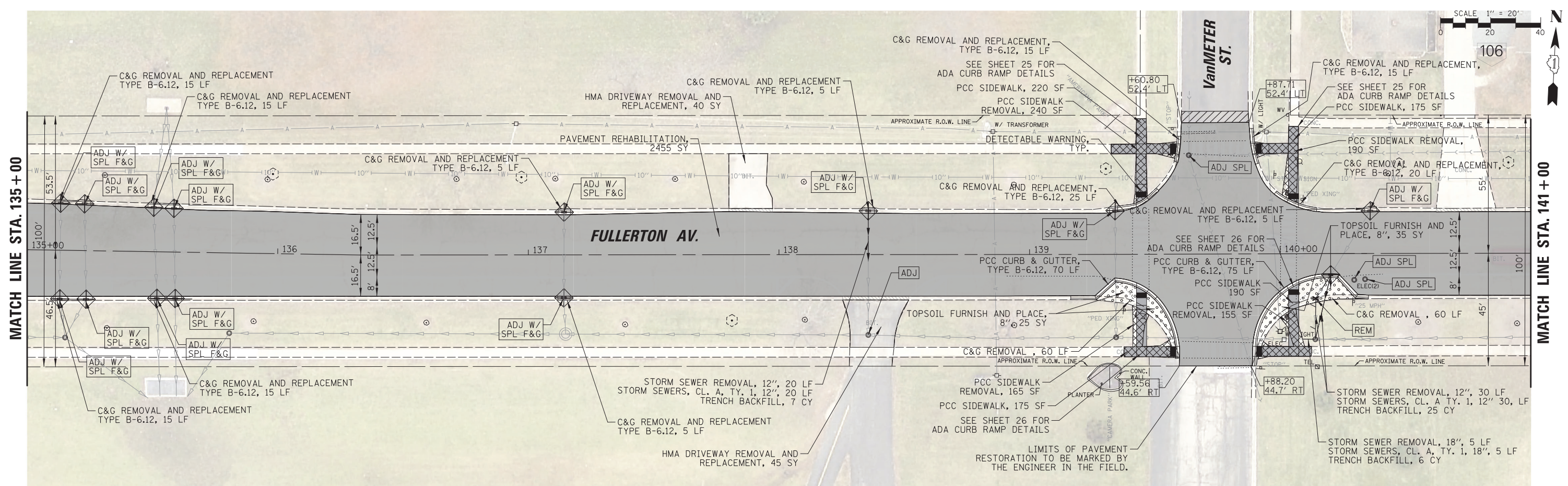
NOTES:

- CONSTRUCTION LIMITS TO BE MARKED IN THE FIELD BY THE ENGINEER
- ONE LANE OF TRAFFIC IN BOTH DIRECTIONS MUST BE MAINTAINED
- THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE FORESTER TO LOCATE THE TREE ROOTS TO BE PRUNED AND TREES TO BE PRUNED.

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
N:\GLENDALE\GHTS\240051D208\Civil\p1n\03_d208.sht		DRAWN - KK	REVISED -
		CHECKED - AMP	REVISED -
		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PROPOSED PLANS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: 28'	SHEET NO. 15 OF 40 SHEETS	1379	21-00073-00-R5	DUPAGE	40	15
STA. 124+00.00 TO STA. 135+00.00		CONTRACT NO. 61J85				
ILLINOIS FED. AID PROJECT						



LEGEND

[Pattern]	PAVEMENT RESURFACING
[Pattern]	HMA REMOVAL SURFACE 4"
[Pattern]	HMA BINDER COURSE, IL-19.0, N50
[Pattern]	HMA SURFACE COURSE, IL-9.5, MIX "D", N50
[Pattern]	PCC SIDEWALK, 5 INCH
[Pattern]	PROPOSED CURB AND GUTTER TYPE B-6.12
[Pattern]	TOPSOIL FURNISH AND PLACE, 4"
[Pattern]	PCC SIDEWALK REMOVAL
[Pattern]	HMA SURFACE REMOVAL - BUTT JOINT
[Pattern]	CURB AND GUTTER REMOVAL
[Pattern]	STORM SEWER REMOVAL
[Symbol]	DETECTABLE WARNING
[Symbol]	INLET FILTER
[Symbol]	TREE TRUNK PROTECTION
[Symbol]	ADJ W/ SPL F&G: STRUCTURE TO BE ADJUSTED W/ SPECIAL FRAME & GRATE
[Symbol]	ADJ SPL: STRUCTURE TO BE ADJUSTED, SPECIAL
[Symbol]	ADJ: STRUCTURE TO BE ADJUSTED
[Symbol]	RECON: STRUCTURE TO BE RECONSTRUCTED
[Symbol]	REM: STRUCTURE TO BE REMOVED
[Symbol]	160: PROPERTY ADDRESS

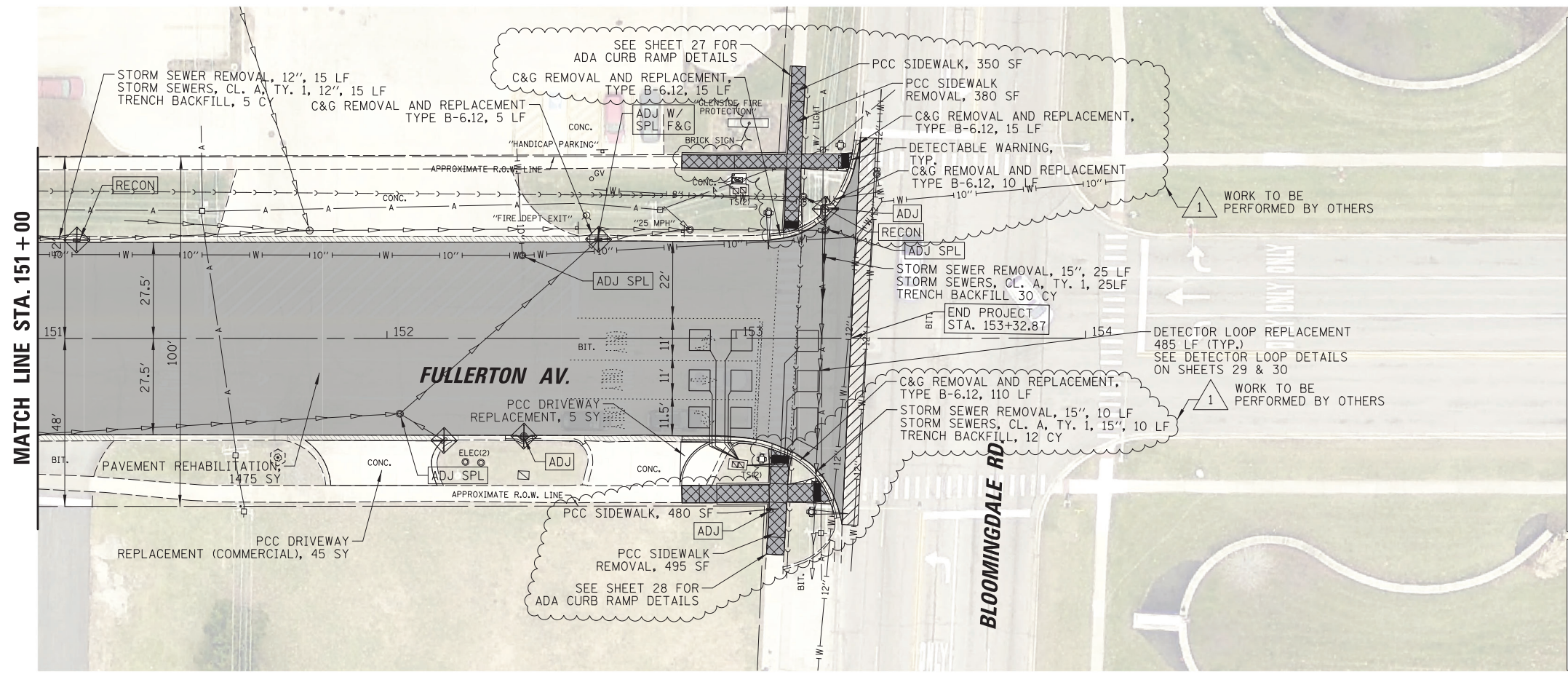
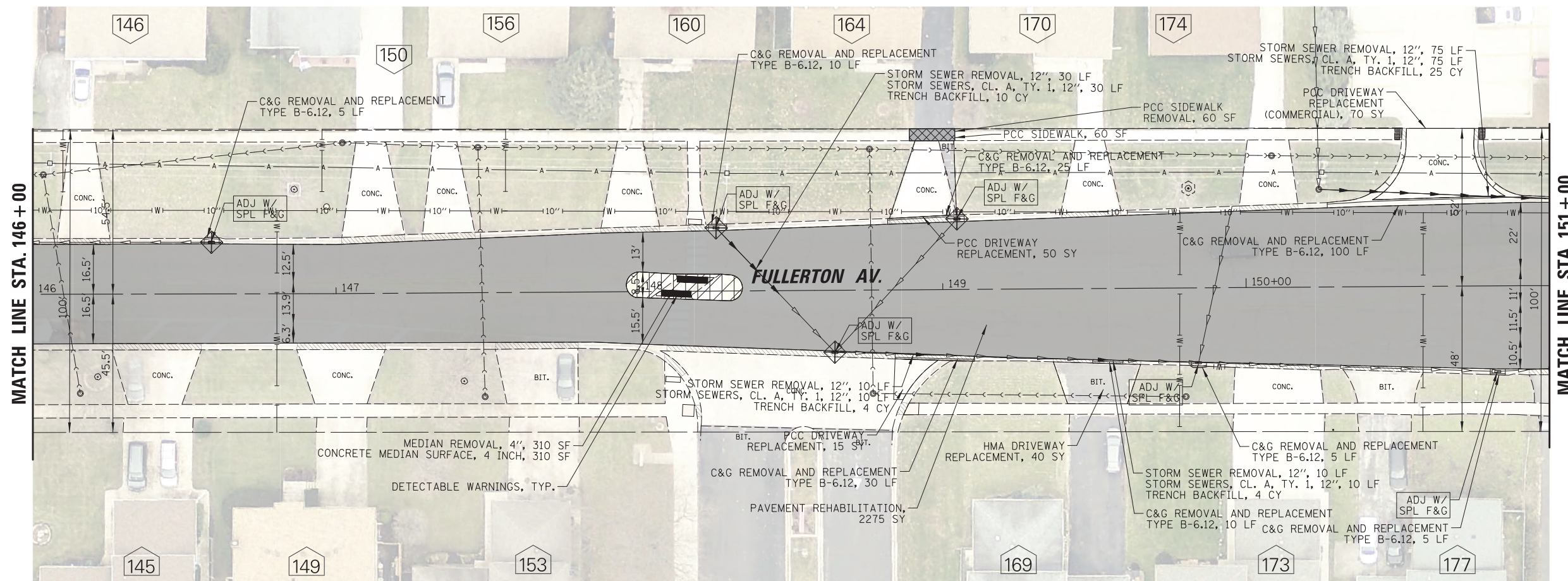
- NOTES:**
- CONSTRUCTION LIMITS TO BE MARKED IN THE FIELD BY THE ENGINEER
 - ONE LANE OF TRAFFIC IN BOTH DIRECTIONS MUST BE MAINTAINED
 - THE CONTRACTOR SHALL COORDINATE TREE ROOTS TO BE PRUNED AND TREES TO BE PRUNED WITH THE VILLAGE FORESTER.
 - CONTRACTOR REQUIRED TO RELOCATE ALL EXISTING SIGNS AT THE INTERSECTION OF VAN METER STREET THAT REQUIRES ADJUSTMENT DUE TO REVISED CURB LAYOUT. THIS INCLUDES REGULATORY SIGNS AND FLASHING BEACONS. INSTALLATION SHALL BE PER M.U.T.C.D. WORK INCLUDED IN COST OF PCC CURB AND GUTTER.

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
N:\GLD\LALEIGH\T\240051D208\Civil\p\In\24_d208.sht		DRAWN - KK	REVISED -
		CHECKED - AMP	REVISED -
		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PROPOSED PLANS	
SCALE: 20'	SHEET NO. 16 OF 40 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	16
			CONTRACT NO.	61J85
ILLINOIS FED. AID PROJECT				



LEGEND

	PAVEMENT RESURFACING
	HMA REMOVAL SURFACE 4"
	HMA BINDER COURSE, IL-19.0, N50
	HMA SURFACE COURSE, IL-9.5, MIX "D", N50
	PCC SIDEWALK, 5 INCH
	PROPOSED CURB AND GUTTER TYPE B-6.12
	TOPSOIL FURNISH AND PLACE, 4"
	PCC SIDEWALK REMOVAL
	HMA SURFACE REMOVAL - BUTT JOINT
	CURB AND GUTTER REMOVAL
	STORM SEWER REMOVAL
	DETECTABLE WARNING
	INLET FILTER
	TREE TRUNK PROTECTION
	ADJ W/ SPL F&G: STRUCTURE TO BE ADJUSTED W/ SPECIAL FRAME & GRATE
	ADJ SPL: STRUCTURE TO BE ADJUSTED, SPECIAL
	ADJ: STRUCTURE TO BE ADJUSTED
	RECON: STRUCTURE TO BE RECONSTRUCTED
	REM: STRUCTURE TO BE REMOVED
	160: PROPERTY ADDRESS

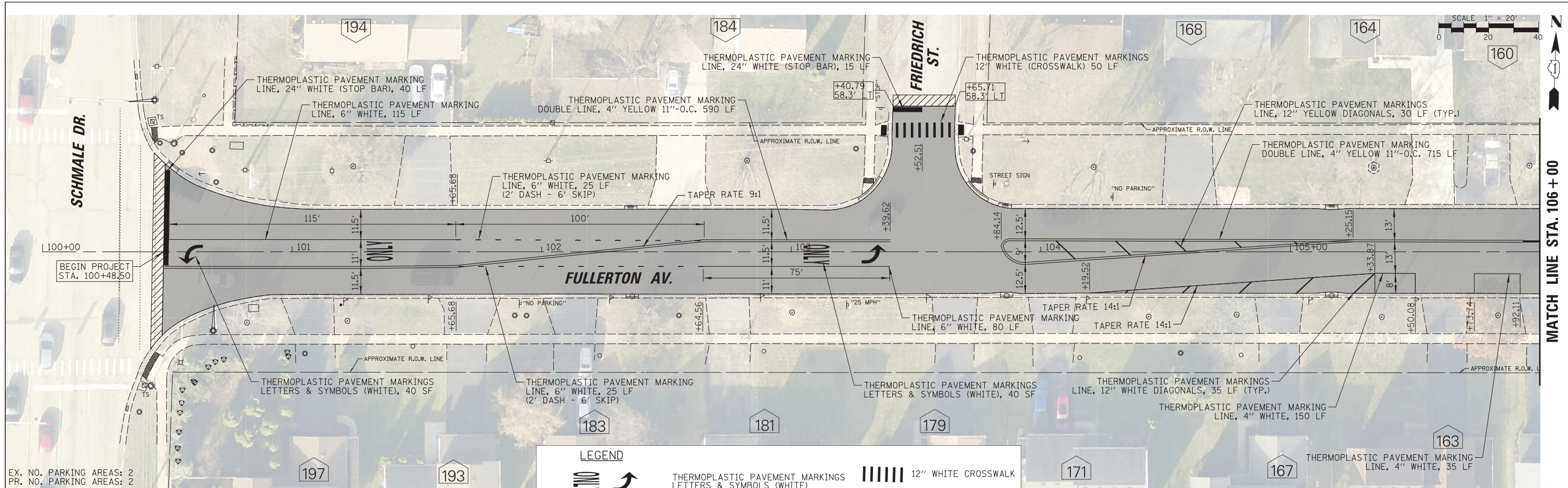
- NOTES:**
- CONSTRUCTION LIMITS TO BE MARKED IN THE FIELD BY THE ENGINEER
 - ONE LANE OF TRAFFIC IN BOTH DIRECTIONS MUST BE MAINTAINED
 - THE PEDESTRIAN PUSH BUTTON EXTENSION BRACKETS SHOULD BE NO LONGER THAN 12", AND THE FACE OF THE PED BUTTON SHOULD BE NO CLOSER THAN 8" TO 10" FROM THE NEAR EDGE OF THE SIDEWALK.
 - THE CONTRACTOR SHALL COORDINATE TREE ROOTS TO BE PRUNED AND TREES TO BE PRUNED WITH THE VILLAGE FORESTER.

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
N:\GLENDALE\IGHTS\040051D208\Civil\p1n05_d2080.sht		DRAWN - KK	REVISED -
		CHECKED - AMP	REVISED -
		DATE - 9/1/2023	REVISED -

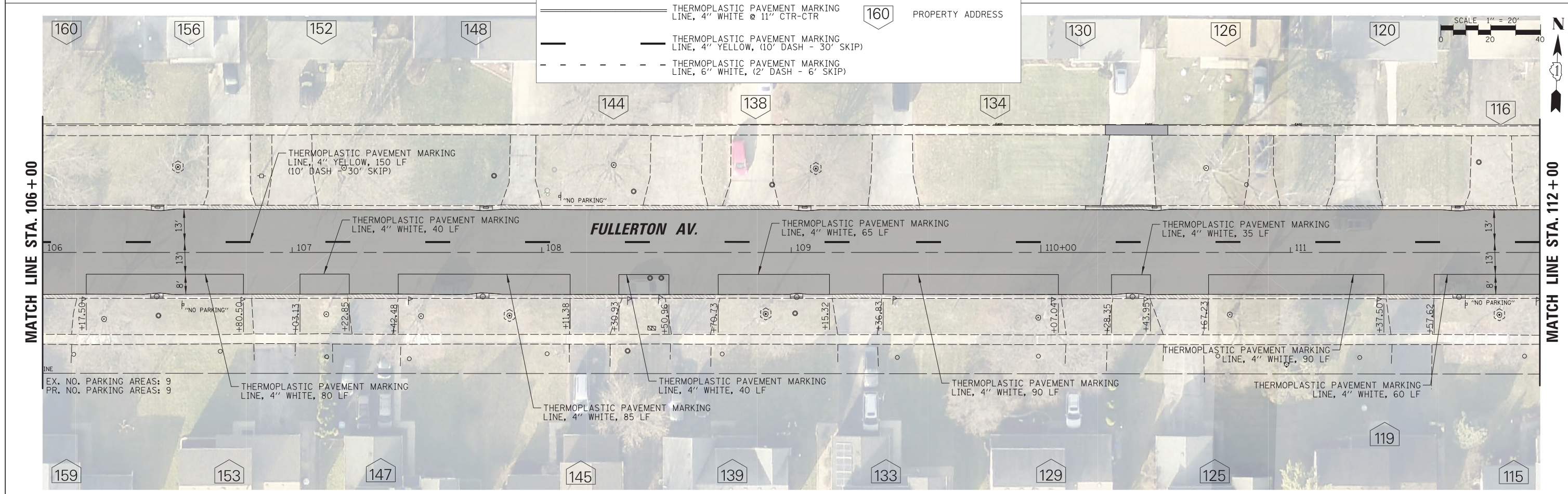
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PROPOSED PLANS			
SCALE: 20'	SHEET NO. 17 OF 40 SHEETS	STA. 146+00.00 TO STA. 153+32.87	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	17
			CONTRACT NO. 61J85	
ILLINOIS FED. AID PROJECT				

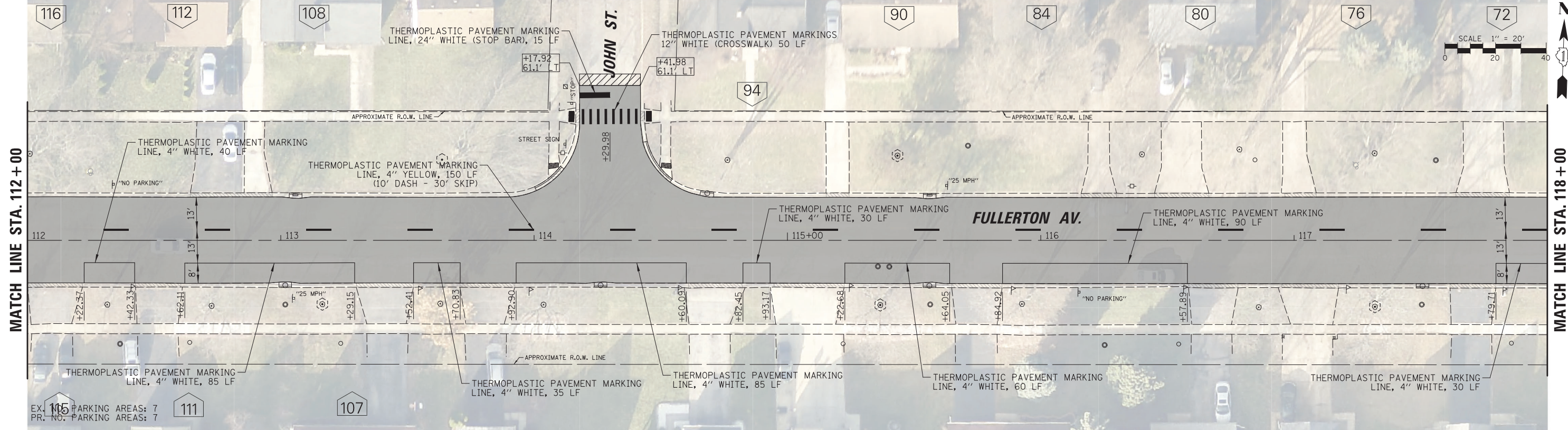


EX. NO. PARKING AREAS: 2
PR. NO. PARKING AREAS: 2



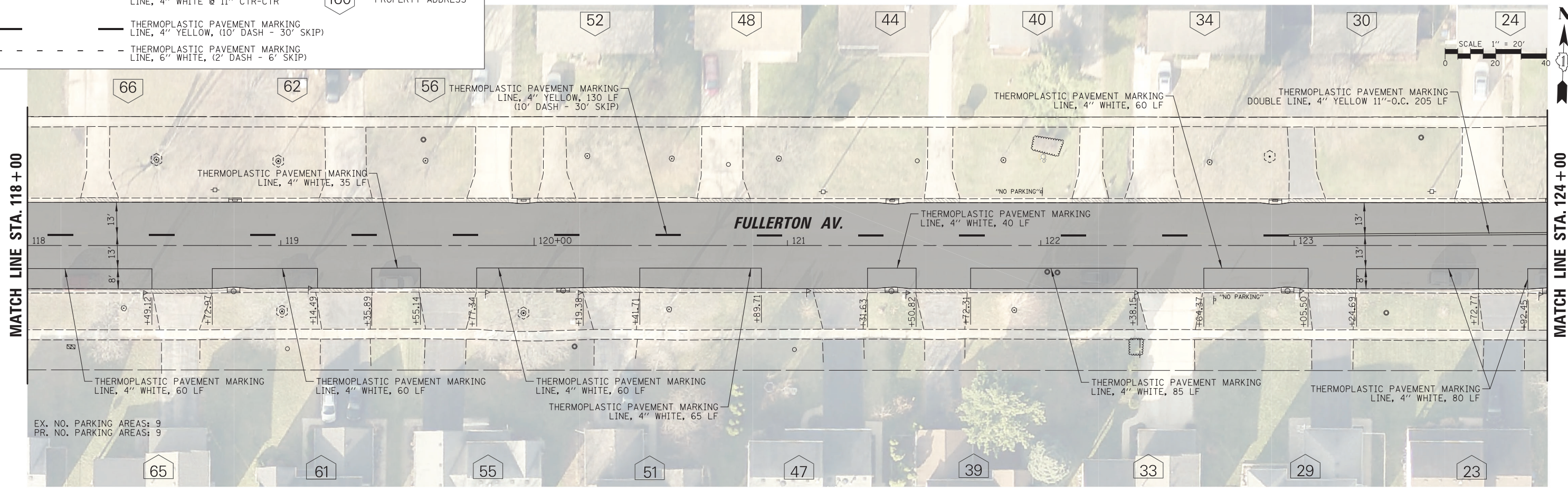
EX. NO. PARKING AREAS: 9
PR. NO. PARKING AREAS: 9

FILE NAME = N:\GLENDALE\HIGHWAYS\040051D200\Civil\pmk_01_d2008.sht	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FULLERTON AVENUE RESURFACING PAVEMENT MARKING PLANS	F.A.U. RTE. 1379	SECTION 21-00073-00-R5	COUNTY DUPAGE	TOTAL SHEETS 40	SHEET NO. 18	
PLOT SCALE = 28'	CHECKED - AMP	REVISIED -	REVISIED -			SCALE: 28'	SHEET NO. 18 OF 40 SHEETS	STA. 100+48.50 TO STA. 112+00.00	CONTRACT NO. 61J85		
PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISIED -	REVISIED -			ILLINOIS FED. AID PROJECT					



LEGEND

	THERMOPLASTIC PAVEMENT MARKINGS LETTERS & SYMBOLS (WHITE)		12" WHITE CROSSWALK
	24" WHITE STOP BAR		PROPERTY ADDRESS
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" WHITE @ 11" CTR-CTR		
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" YELLOW, (10' DASH - 30' SKIP)		
	THERMOPLASTIC PAVEMENT MARKING LINE, 6" WHITE, (2' DASH - 6' SKIP)		

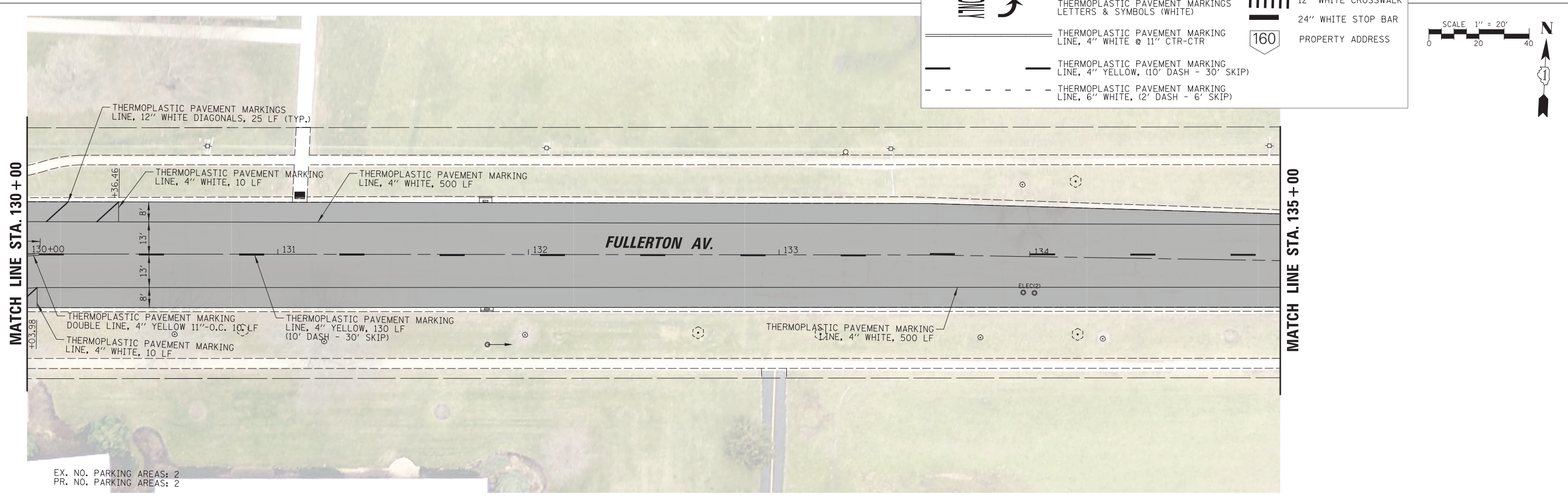
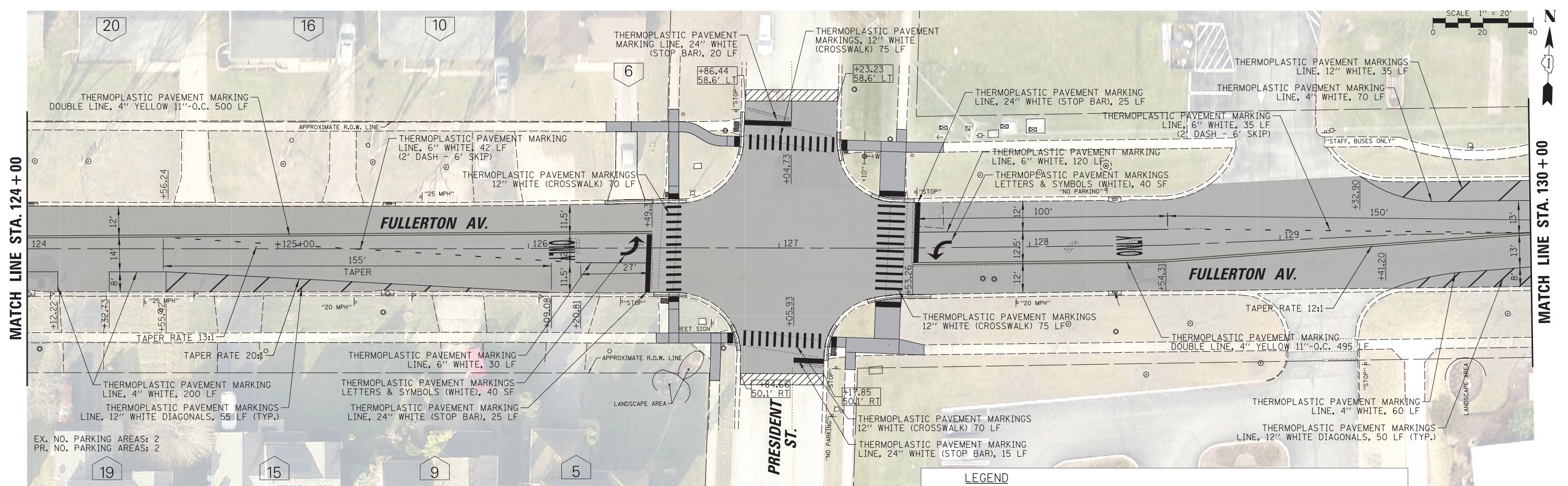


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02_d208.sht		DRAWN - KK	REVISED -
PLOT SCALE = 20'		CHECKED - AMP	REVISED -
PLOT DATE = 9/1/2023		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PAVEMENT MARKING PLANS			
SCALE: 20'	SHEET NO. 19 OF 40 SHEETS	STA. 112+00.00 TO STA. 124+00.00	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	19
				CONTRACT NO. 61J85
ILLINOIS FED. AID PROJECT				



LEGEND

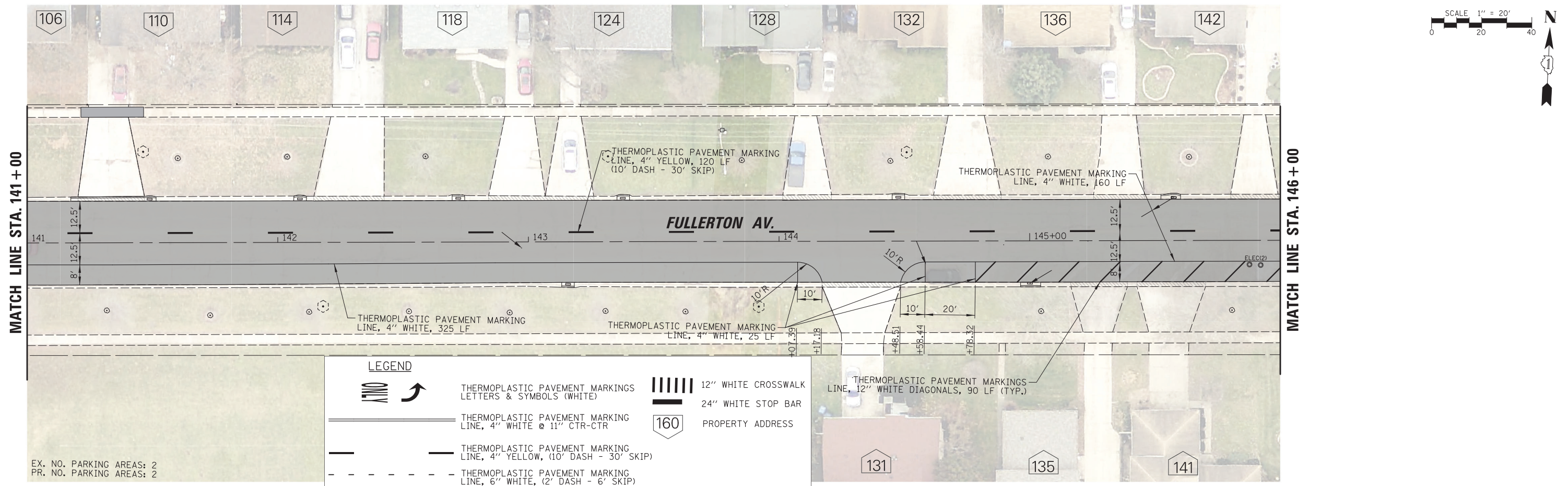
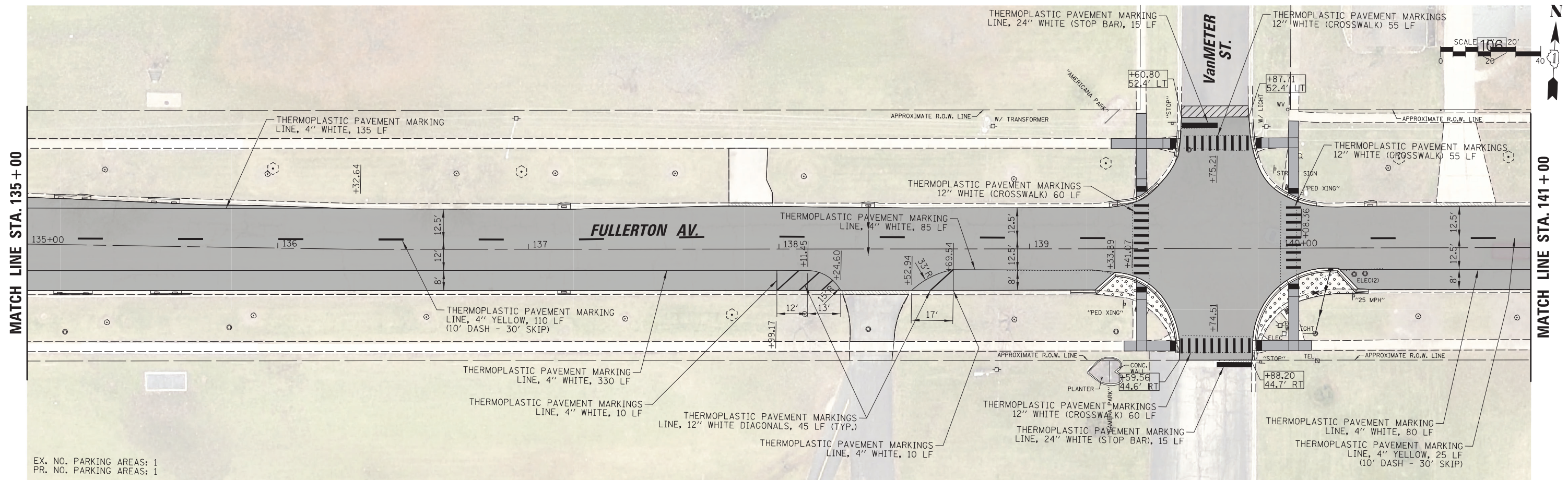
	THERMOPLASTIC PAVEMENT MARKINGS LETTERS & SYMBOLS (WHITE)		12" WHITE CROSSWALK
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" WHITE @ 11" CTR-CTR		24" WHITE STOP BAR
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" WHITE (10' DASH - 30' SKIP)		PROPERTY ADDRESS
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" YELLOW (10' DASH - 30' SKIP)		
	THERMOPLASTIC PAVEMENT MARKING LINE, 6" WHITE (2' DASH - 6' SKIP)		

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
N:\GLENDALE\IGHTS\2040051D208\Cv11\pmk03.d208.sht		DRAWN - KK	REVISED -
	PLOT SCALE = 28'	CHECKED - AMP	REVISED -
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

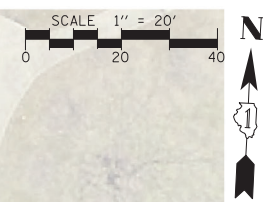
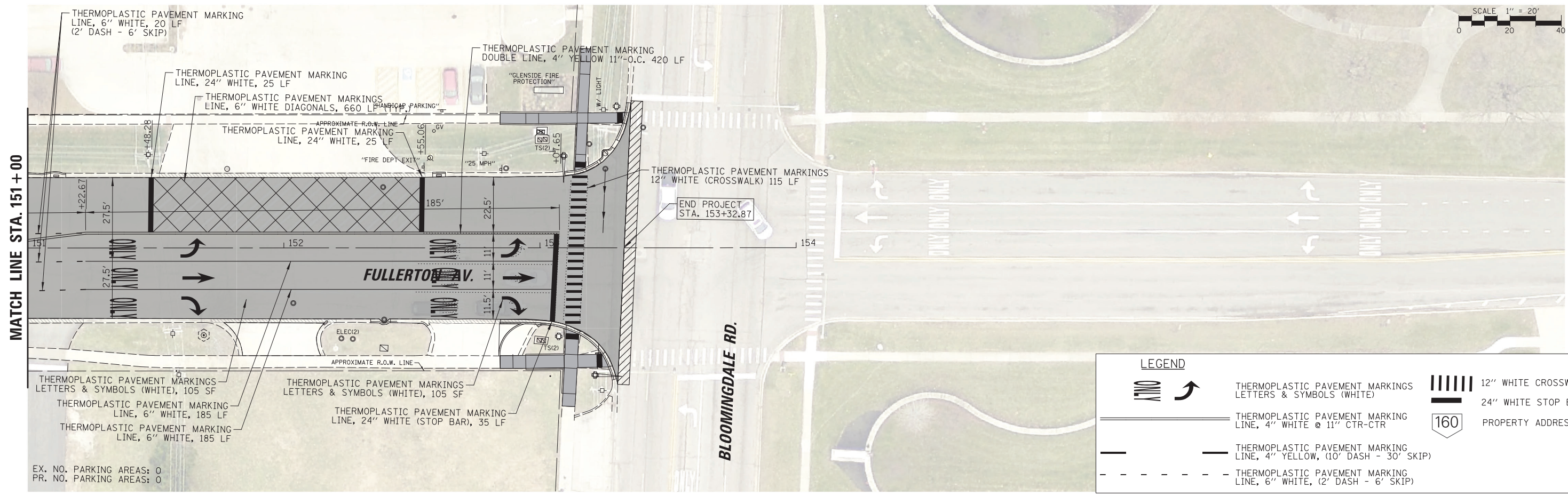
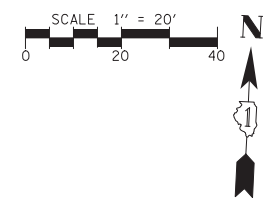
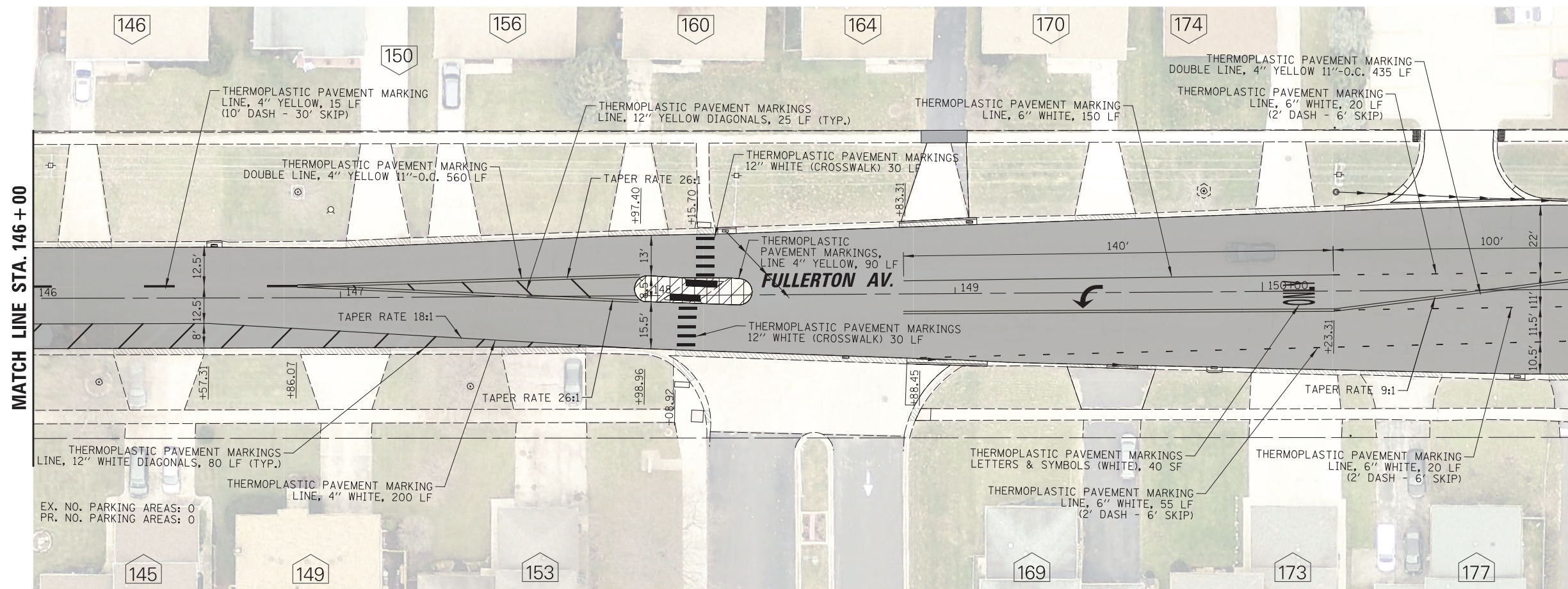
FULLERTON AVENUE RESURFACING PAVEMENT MARKING PLANS	
SCALE: 28'	SHEET NO. 20 OF 40 SHEETS
STA. 124+00.00 TO STA. 135+00.00	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	20
CONTRACT NO. 61J85			ILLINOIS FED. AID PROJECT	



LEGEND

	12" WHITE CROSSWALK
	24" WHITE STOP BAR
	160 PROPERTY ADDRESS
	THERMOPLASTIC PAVEMENT MARKINGS LETTERS & SYMBOLS (WHITE)
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" WHITE @ 11" CTR-CTR
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" YELLOW, (10' DASH - 30' SKIP)
	THERMOPLASTIC PAVEMENT MARKING LINE, 6" WHITE, (2' DASH - 6' SKIP)



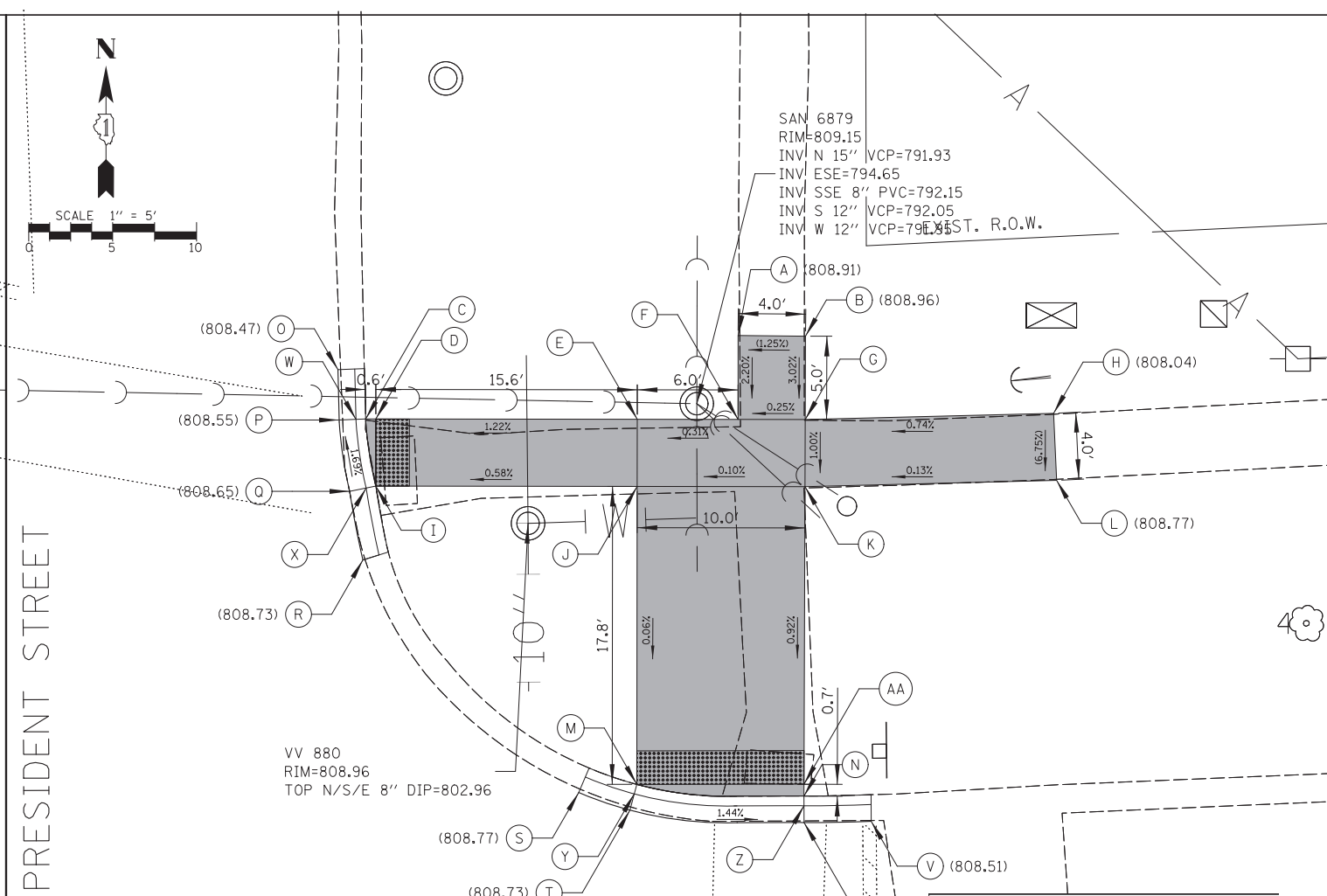
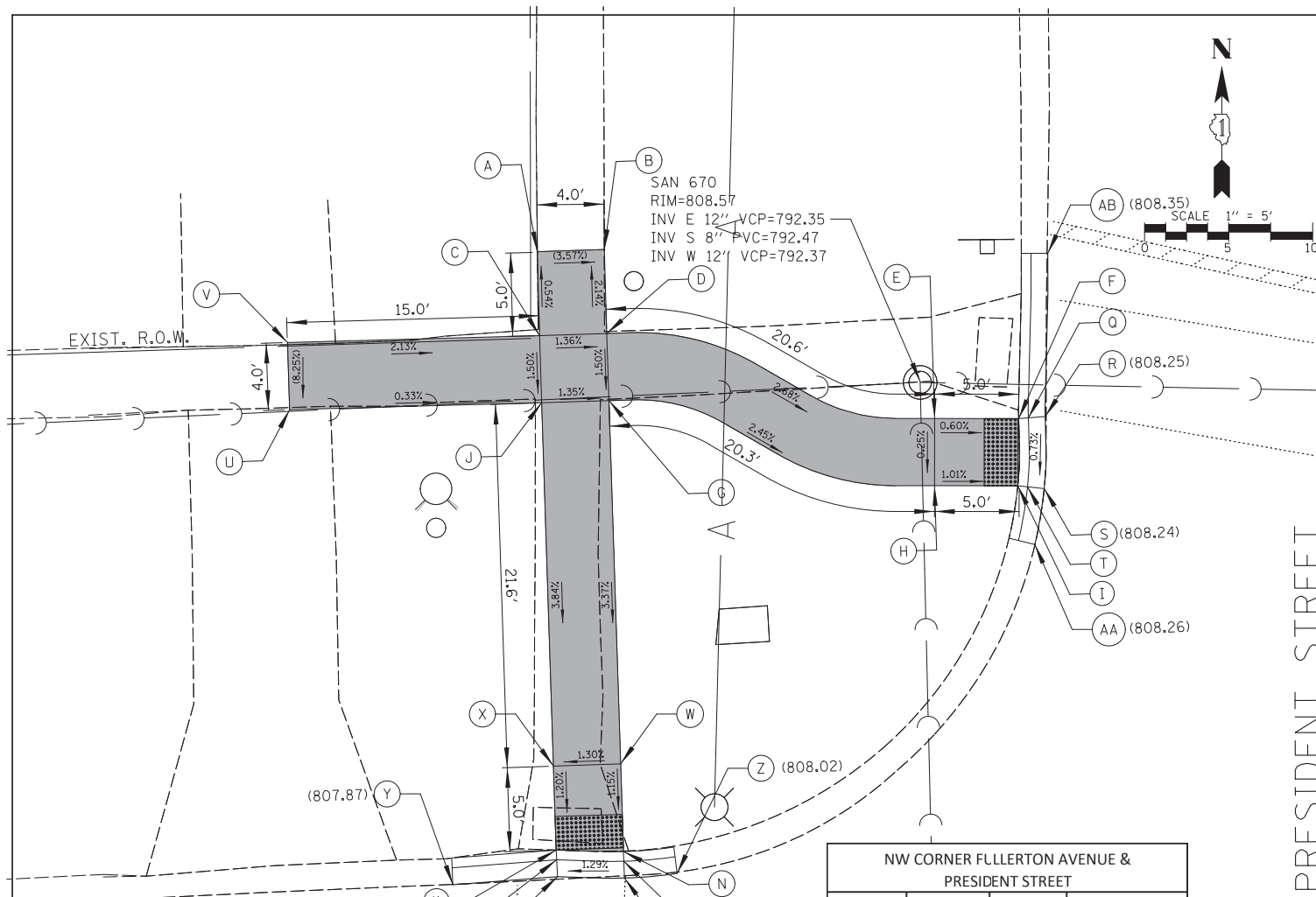
LEGEND	
	THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS (WHITE)
	12" WHITE CROSSWALK
	24" WHITE STOP BAR
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" WHITE @ 11" CTR-CTR
	PROPERTY ADDRESS
	THERMOPLASTIC PAVEMENT MARKING LINE, 4" YELLOW, (10' DASH - 30' SKIP)
	THERMOPLASTIC PAVEMENT MARKING LINE, 6" WHITE, (2' DASH - 6' SKIP)

FILE NAME = N:\GLENDALE\IGHTS\040051D208\Cv1\pmk	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
	05_d2080.sht	DRAWN - KK	REVISED -
	PLOT SCALE = 28'	CHECKED - AMP	REVISED -
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FULLERTON AVENUE RESURFACING PAVEMENT MARKING PLANS			
SCALE: 28'	SHEET NO. 22 OF 40 SHEETS	STA. 146+00.00 TO STA. 153+32.87	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	22
				CONTRACT NO. 61J85
ILLINOIS FED. AID PROJECT				



NW CORNER FULLERTON AVENUE & PRESIDENT STREET

POINT	STATION	OFFSET	ELEVATION
A	126+56.28	55.25'LT	(808.89)
B	126+59.92	55.24'LT	(808.76)
C	126+56.24	50.25'LT	808.92
D	126+59.91	50.24'LT	808.87
E	126+79.25	44.59'LT	808.33
F	126+84.25	44.43'LT	808.30
G	126+59.92	46.24'LT	808.81
H	126+79.13	40.59'LT	808.32
I	126+84.08	40.44'LT	808.27
J	126+56.22	46.25'LT	808.86
K	126+56.10	19.61'LT	807.97
L	126+56.11	19.03'LT	807.96
M	126+56.13	18.03'LT	808.01
N	126+59.97	19.39'LT	808.02
O	126+59.97	18.81'LT	808.01
P	126+59.98	17.81'LT	(808.06)
Q	126+84.83	44.46'LT	808.29
R	126+85.83	44.51'LT	808.34
S	126+85.65	40.22'LT	808.31
T	126+84.65	40.36'LT	808.26
U	126+41.22	46.39'LT	(808.91)
V	126+41.24	50.39'LT	(809.24)
W	126+59.96	24.61'LT	808.08
X	126+56.12	24.61'LT	808.03
Y	126+49.88	17.78'LT	(807.87)
Z	126+63.12	17.99'LT	(808.02)
AA	126+84.98	36.91'LT	(808.26)
AB	126+86.28	54.22'LT	(808.35)

NE CORNER FULLERTON AVENUE & PRESIDENT STREET

POINT	STATION	OFFSET	ELEVATION
A	127+47.48	47.43'LT	808.91
B	127+51.49	47.22'LT	808.96
C	127+24.54	43.16'LT	808.58
D	127+25.14	43.14'LT	808.59
E	127+40.72	42.65'LT	808.78
F	127+47.26	42.44'LT	808.80
G	127+51.26	42.26'LT	808.81
H	127+66.06	41.99'LT	809.04
I	127+25.01	39.15'LT	808.65
J	127+40.59	38.66'LT	808.74
K	127+51.07	38.27'LT	808.75
L	127+66.07	37.99'LT	808.77
M	127+40.03	20.90'LT	808.73
N	127+50.22	19.84'LT	808.58
O	127+22.98	46.15'LT	808.47
P	127+22.96	43.15'LT	808.55
Q	127+23.46	38.88'LT	808.65
R	127+24.12	34.77'LT	808.73
S	127+36.56	20.53'LT	808.77
T	127+39.58	19.39'LT	808.73
U	127+50.16	18.27'LT	808.58
V	127+54.16	18.18'LT	808.51
W	127+23.96	43.16'LT	808.54
X	127+24.44	39.05'LT	808.61
Y	127+39.86	20.34'LT	808.69
Z	127+50.20	19.27'LT	808.54
AA	127+50.25	20.55'LT	808.64

NOTES:
 1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

NOTES:
 1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
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PLOT SCALE = 5'		CHECKED - AMP	REVISED -
PLOT DATE = 9/1/2023		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

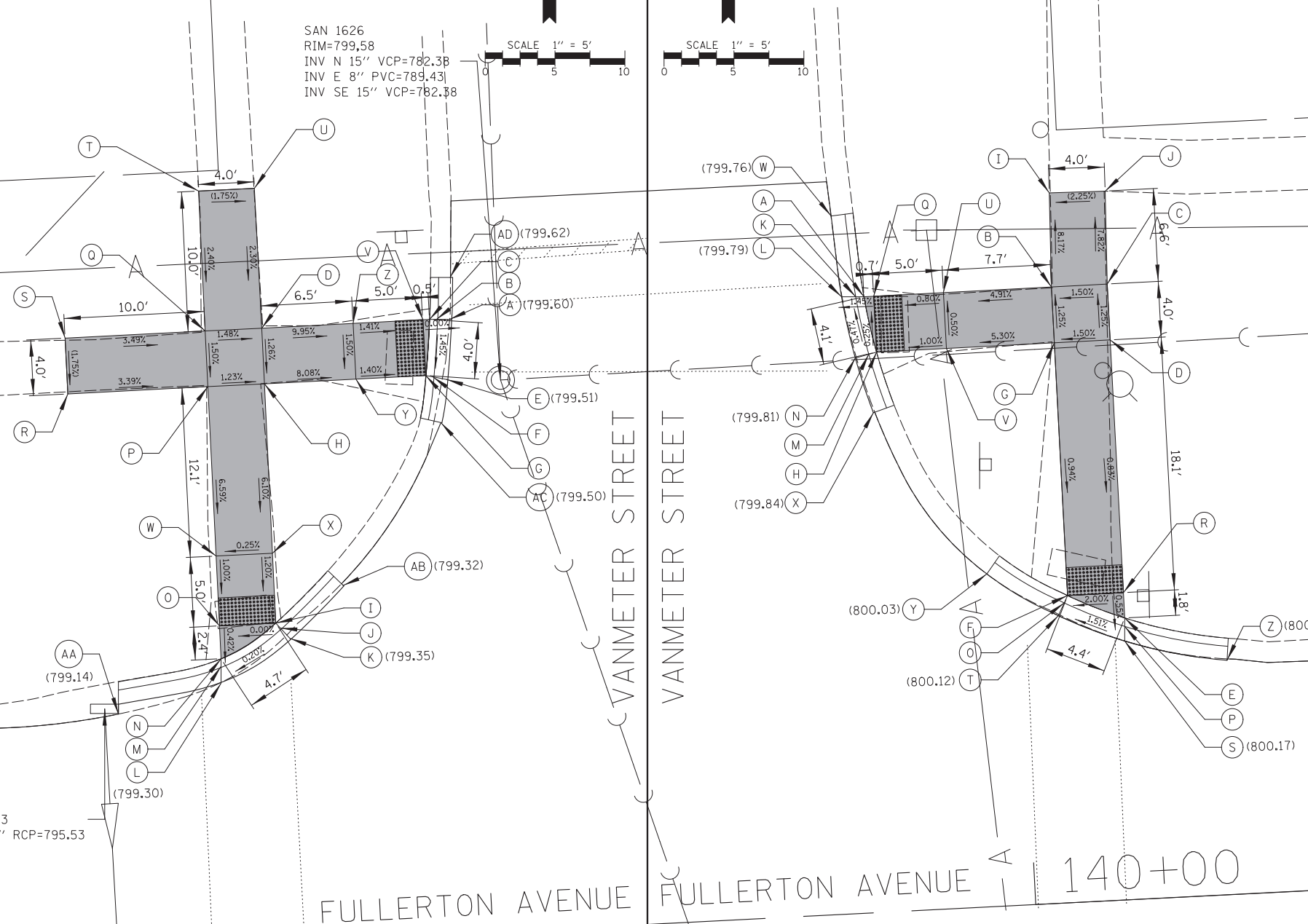
**FULLERTON AVENUE RESURFACING
ADA CURB RAMP DETAILS**

SCALE: 5' SHEET NO. 23 OF 40 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	23
CONTRACT NO. 61J85				
ILLINOIS FED. AID PROJECT				

NW CORNER FULLERTON AVENUE & VANMETER STREET			
POINT	STATION	OFFSET	ELEVATION
A	139+60.42	43.94'LT	799.53
B	139+59.42	43.99'LT	(799.48)
C	139+58.84	44.02'LT	799.49
D	139+46.82	44.00'LT	800.09
E	139+59.93	39.65'LT	799.50
F	139+58.95	39.89'LT	(799.45)
G	139+58.37	40.03'LT	(799.46)
H	139+46.81	40.02'LT	800.04
I	139+46.74	22.89'LT	799.30
J	139+47.12	22.44'LT	799.29
K	139+47.76	21.68'LT	799.34
L	139+42.62	18.84'LT	799.31
M	139+42.68	19.91'LT	799.26
N	139+42.71	20.53'LT	799.27
O	139+42.71	22.89'LT	799.31
P	139+42.76	40.03'LT	800.09
Q	139+42.77	44.02'LT	800.15
R	139+32.73	40.02'LT	(800.50)
S	139+32.74	44.02'LT	(800.57)
T	139+42.80	54.03'LT	(800.46)
U	139+46.80	54.02'LT	(800.39)
V	139+58.34	44.02'LT	799.50
W	139+42.73	27.89'LT	799.36
X	139+46.76	27.89'LT	799.37
Y	139+53.37	40.02'LT	799.53
Z	139+53.36	44.01'LT	799.57
AA	139+35.21	17.66'LT	(799.14)
AB	139+51.74	25.29'LT	(799.32)
AC	139+59.34	36.59'LT	(799.50)
AD	139+60.64	46.93'LT	(799.62)

NE CORNER FULLERTON AVENUE & VANMETER STREET			
POINT	STATION	OFFSET	ELEVATION
A	139+89.95	44.05'LT	799.84
B	140+03.38	44.06'LT	800.27
C	140+07.38	44.07'LT	800.33
D	140+07.38	40.07'LT	800.38
E	140+07.40	20.15'LT	800.22
F	140+03.40	21.96'LT	800.15
G	140+03.38	40.06'LT	800.32
H	139+90.65	40.05'LT	799.86
I	140+03.57	50.79'LT	(799.72)
J	140+07.57	50.72'LT	(799.81)
K	139+89.36	44.08'LT	799.83
L	139+88.35	44.13'LT	799.88
M	139+90.08	39.92'LT	799.85
N	139+89.11	39.70'LT	799.90
O	140+03.13	21.45'LT	800.14
P	140+07.37	19.54'LT	800.21
Q	139+90.64	44.05'LT	799.85
R	140+07.40	21.96'LT	800.23
S	140+07.32	18.50'LT	800.26
T	140+02.67	20.56'LT	800.19
U	139+95.64	44.06'LT	799.89
V	139+95.65	40.06'LT	799.91
W	139+87.90	49.94'LT	(799.76)
X	139+90.20	35.73'LT	(799.84)
Y	139+97.71	23.80'LT	(800.03)
Z	140+14.56	16.75'LT	(800.26)



NOTES:
 1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

INL 1539
 RIM=799.13
 INV S 12" RCP=795.53

INL 1537
 RIM=800.38
 INV SW 12" RCP=796.16

NOTES:
 1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

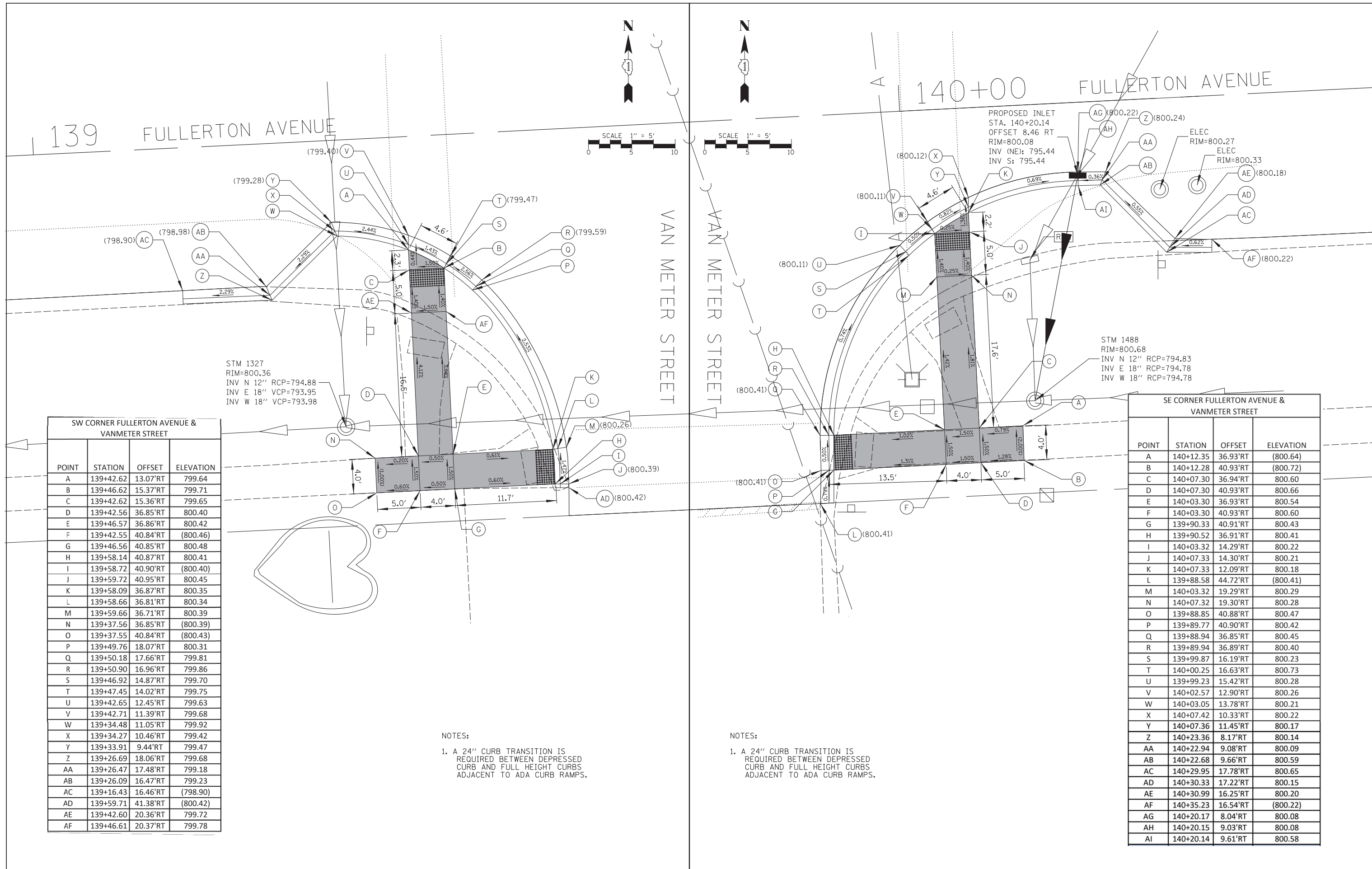
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PLOT SCALE = 5'		CHECKED - AMP	REVISED -
PLOT DATE = 9/1/2023		DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FULLERTON AVENUE RESURFACING
 ADA CURB RAMP DETAILS

SCALE: 5' SHEET NO. 25 OF 40 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	25
				CONTRACT NO. 61J85
ILLINOIS FED. AID PROJECT				



SW CORNER FULLERTON AVENUE & VANMETER STREET

POINT	STATION	OFFSET	ELEVATION
A	139+42.62	13.07'RT	799.64
B	139+46.62	15.37'RT	799.71
C	139+42.62	15.36'RT	799.65
D	139+42.56	36.85'RT	800.40
E	139+46.57	36.86'RT	800.42
F	139+42.55	40.84'RT	(800.46)
G	139+46.56	40.85'RT	800.48
H	139+58.14	40.87'RT	800.41
I	139+58.72	40.90'RT	(800.40)
J	139+59.72	40.95'RT	800.45
K	139+58.09	36.87'RT	800.35
L	139+58.66	36.81'RT	800.34
M	139+59.66	36.71'RT	800.39
N	139+37.56	36.85'RT	(800.39)
O	139+37.55	40.84'RT	(800.43)
P	139+49.76	18.07'RT	800.31
Q	139+50.18	17.66'RT	799.81
R	139+50.90	16.96'RT	799.86
S	139+46.92	14.87'RT	799.70
T	139+47.45	14.02'RT	799.75
U	139+42.65	12.45'RT	799.63
V	139+42.71	11.39'RT	799.68
W	139+34.48	11.05'RT	799.92
X	139+34.27	10.46'RT	799.42
Y	139+33.91	9.44'RT	799.47
Z	139+26.69	18.06'RT	799.68
AA	139+26.47	17.48'RT	799.18
AB	139+26.09	16.47'RT	799.23
AC	139+16.43	16.46'RT	(798.90)
AD	139+59.71	41.38'RT	(800.42)
AE	139+42.60	20.36'RT	799.72
AF	139+46.61	20.37'RT	799.78

STM 1327
RIM=800.36
INV N 12" RCP=794.88
INV E 18" VCP=793.95
INV W 18" VCP=793.98

NOTES:
1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

SE CORNER FULLERTON AVENUE & VANMETER STREET

POINT	STATION	OFFSET	ELEVATION
A	140+12.35	36.93'RT	(800.64)
B	140+12.28	40.93'RT	(800.72)
C	140+07.30	36.94'RT	800.60
D	140+07.30	40.93'RT	800.66
E	140+03.30	36.93'RT	800.54
F	140+03.30	40.93'RT	800.60
G	139+90.33	40.91'RT	800.43
H	139+90.52	36.91'RT	800.41
I	140+03.32	14.29'RT	800.22
J	140+07.33	14.30'RT	800.21
K	140+07.33	12.09'RT	800.18
L	139+88.58	44.72'RT	(800.41)
M	140+03.32	19.29'RT	800.29
N	140+07.32	19.30'RT	800.28
O	139+88.85	40.88'RT	800.47
P	139+89.77	40.90'RT	800.42
Q	139+88.94	36.85'RT	800.45
R	139+89.94	36.89'RT	800.40
S	139+99.87	16.19'RT	800.23
T	140+00.25	16.63'RT	800.73
U	139+99.23	15.42'RT	800.28
V	140+02.57	12.90'RT	800.26
W	140+03.05	13.78'RT	800.21
X	140+07.42	10.33'RT	800.22
Y	140+07.36	11.45'RT	800.17
Z	140+23.36	8.17'RT	800.14
AA	140+22.94	9.08'RT	800.09
AB	140+22.68	9.66'RT	800.59
AC	140+29.95	17.78'RT	800.65
AD	140+30.33	17.22'RT	800.15
AE	140+30.99	16.25'RT	800.20
AF	140+35.23	16.54'RT	(800.22)
AG	140+20.17	8.04'RT	800.08
AH	140+20.15	9.03'RT	800.08
AI	140+20.14	9.61'RT	800.58

PROPOSED INLET
STA. 140+20.14
OFFSET 8.46 RT
RIM=800.08
INV (NE): 795.44
INV S: 795.44

STM 1488
RIM=800.68
INV N 12" RCP=794.83
INV E 18" RCP=794.78
INV W 18" RCP=794.78

NOTES:
1. A 24" CURB TRANSITION IS REQUIRED BETWEEN DEPRESSED CURB AND FULL HEIGHT CURBS ADJACENT TO ADA CURB RAMPS.

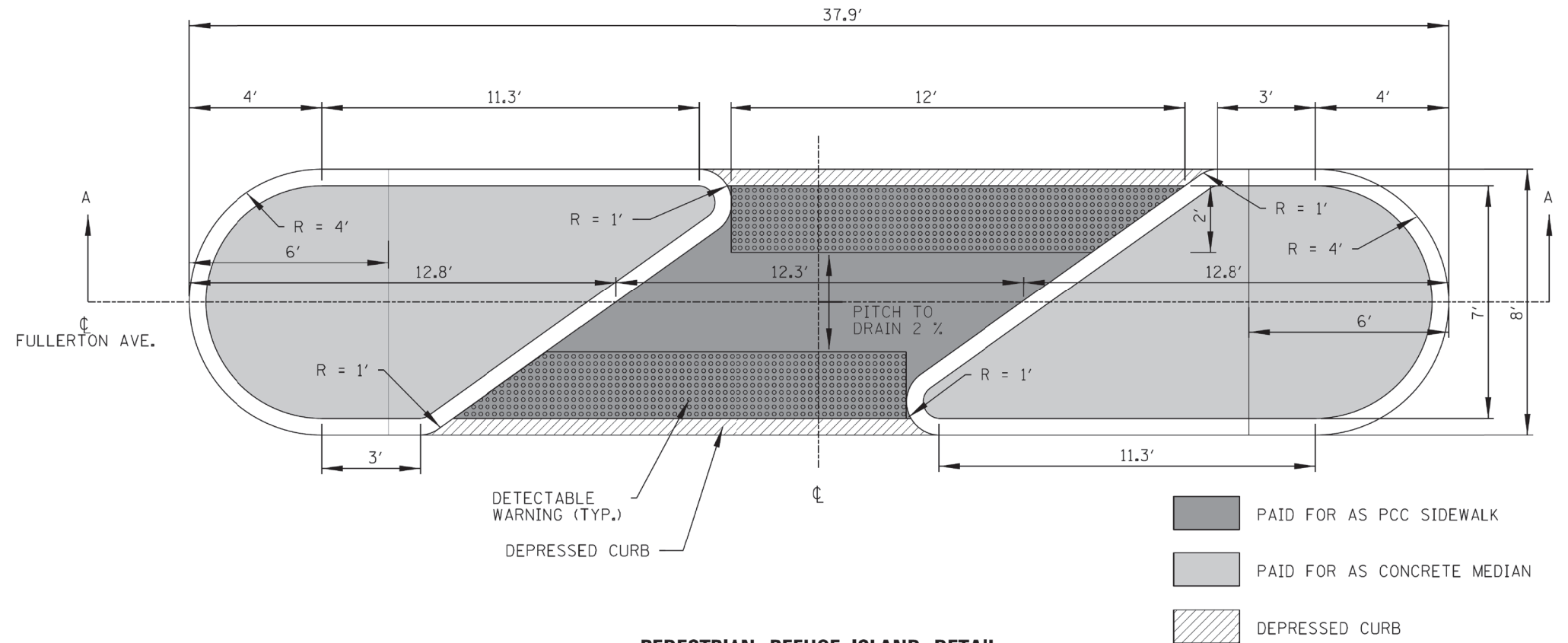
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	PLOT SCALE = 5'	CHECKED - AMP	REVISED -
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

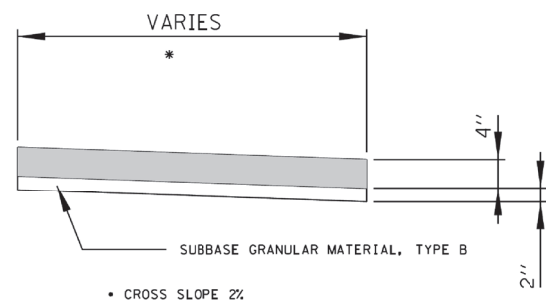
FULLERTON AVENUE RESURFACING ADA CURB RAMP DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: 5'	SHEET NO. 26 OF 40 SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 61J85

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	26
CONTRACT NO. 61J85				

- NOTES:
- CONCRETE ON RAMPED NOSES AND ALL CONCRETE CURB SHALL BE PAINTED YELLOW WILL BE INCLUDED IN THE COST FOR CONCRETE MEDIAN SURFACE, 4 INCH.

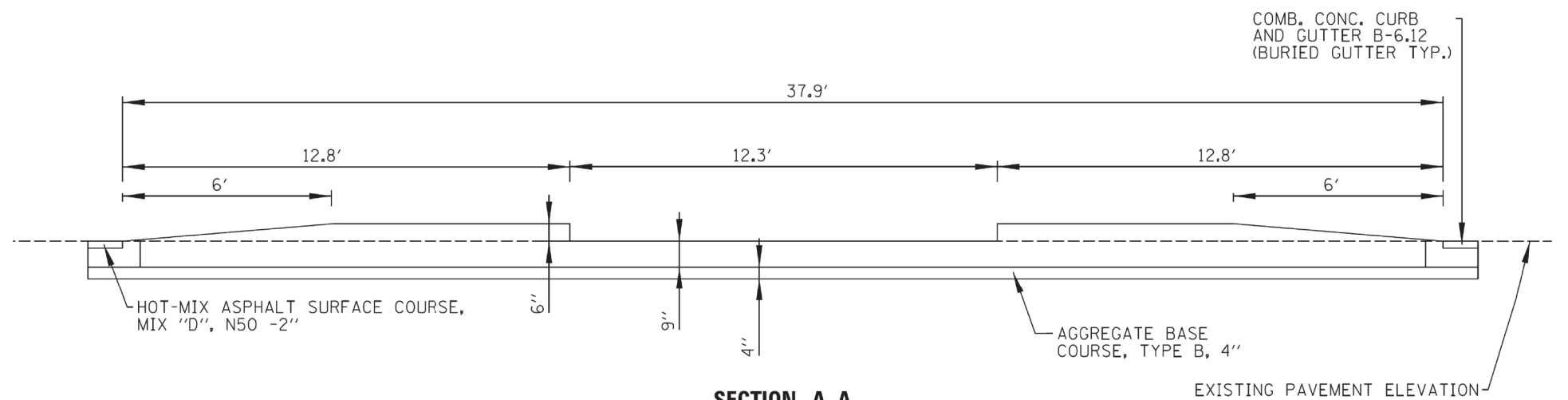


PEDESTRIAN REFUGE ISLAND DETAIL



- NOTES:
- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.
 - WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
 - LANDSCAPE RESTORATION SHALL CONSIST OF TOPSOIL AND SODDING AND SHALL BE PAID FOR SEPARATELY.

P.C.C. SIDEWALK DETAIL



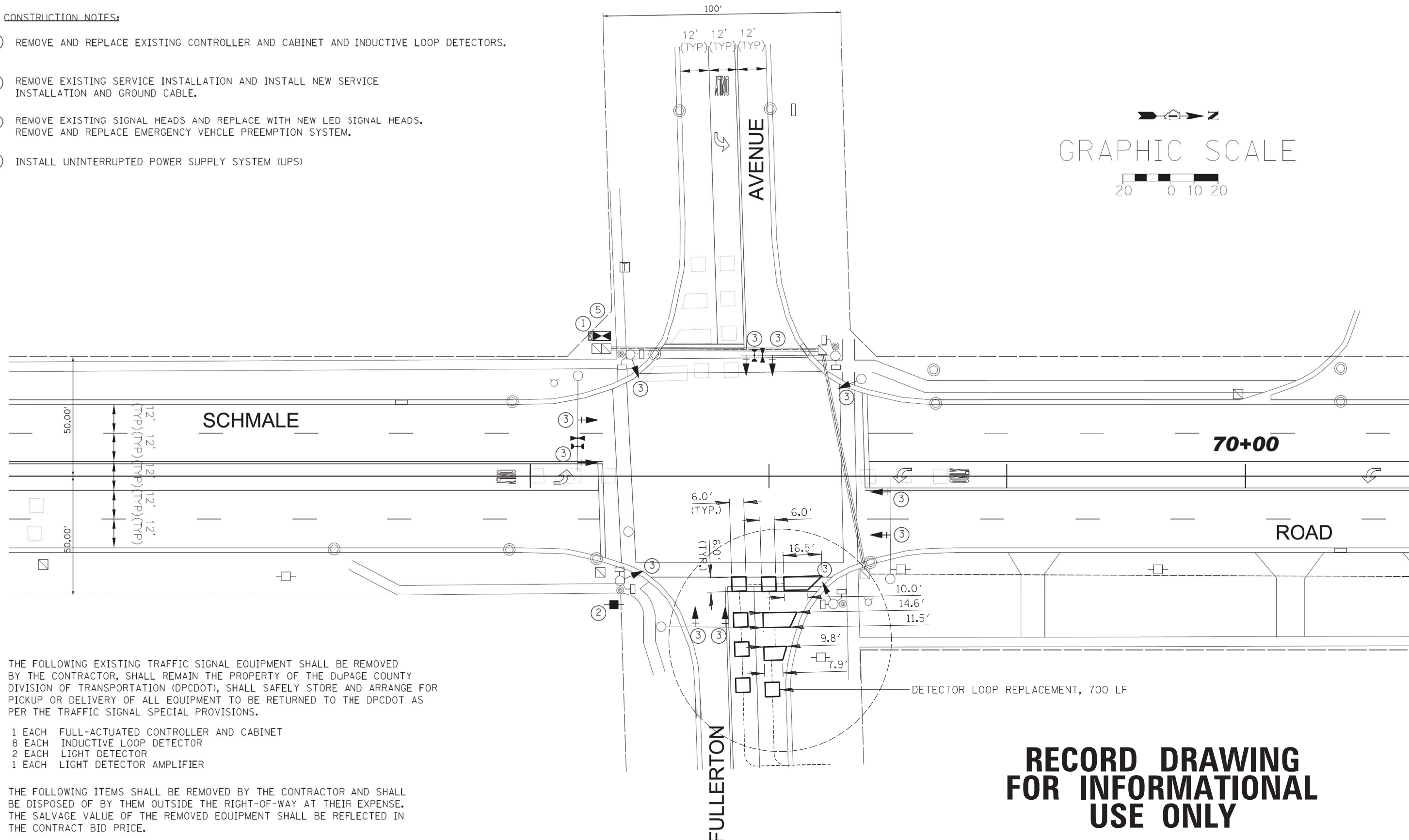
SECTION A-A

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PLCT SCALE = 2'	PLCT DATE = 9/1/2023	DRAWN - EDI	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO.			
		CHECKED - JCS	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE - 06/22/07	REVISED -										

FILE NAME = N:\GLENDALEHEIGHTS\040051D208\Civil\DET-D208.SHT	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FULLERTON AVENUE PEDESTRIAN REFUGEE ISLAND CONSTRUCTION DETAILS				F.A.U. RTE. 1379	SECTION 21-00073-00-R5	COUNTY DUPAGE	TOTAL SHEETS 40	SHEET NO. 27
PLCT SCALE = *NOT TO SCALE*	PLCT DATE = 9/1/2023	DRAWN - KK	REVISED -		SCALE: N.T.S.	SHEET NO. 27	OF SHEETS 40	STA.	TO STA.	CONTRACT NO. 61J85			
		CHECKED - AMP	REVISED -		ILLINOIS FED. AID PROJECT								
		DATE - 9/1/2023	REVISED -										

CONSTRUCTION NOTES:

- ① REMOVE AND REPLACE EXISTING CONTROLLER AND CABINET AND INDUCTIVE LOOP DETECTORS.
- ② REMOVE EXISTING SERVICE INSTALLATION AND INSTALL NEW SERVICE INSTALLATION AND GROUND CABLE.
- ③ REMOVE EXISTING SIGNAL HEADS AND REPLACE WITH NEW LED SIGNAL HEADS. REMOVE AND REPLACE EMERGENCY VEHICLE PREEMPTION SYSTEM.
- ⑤ INSTALL UNINTERRUPTED POWER SUPPLY SYSTEM (UPS)



THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE DUPAGE COUNTY DIVISION OF TRANSPORTATION (DPCDOT), SHALL SAFELY STORE AND ARRANGE FOR PICKUP OR DELIVERY OF ALL EQUIPMENT TO BE RETURNED TO THE DPCDOT AS PER THE TRAFFIC SIGNAL SPECIAL PROVISIONS.

- 1 EACH FULL-ACTUATED CONTROLLER AND CABINET
- 8 EACH INDUCTIVE LOOP DETECTOR
- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

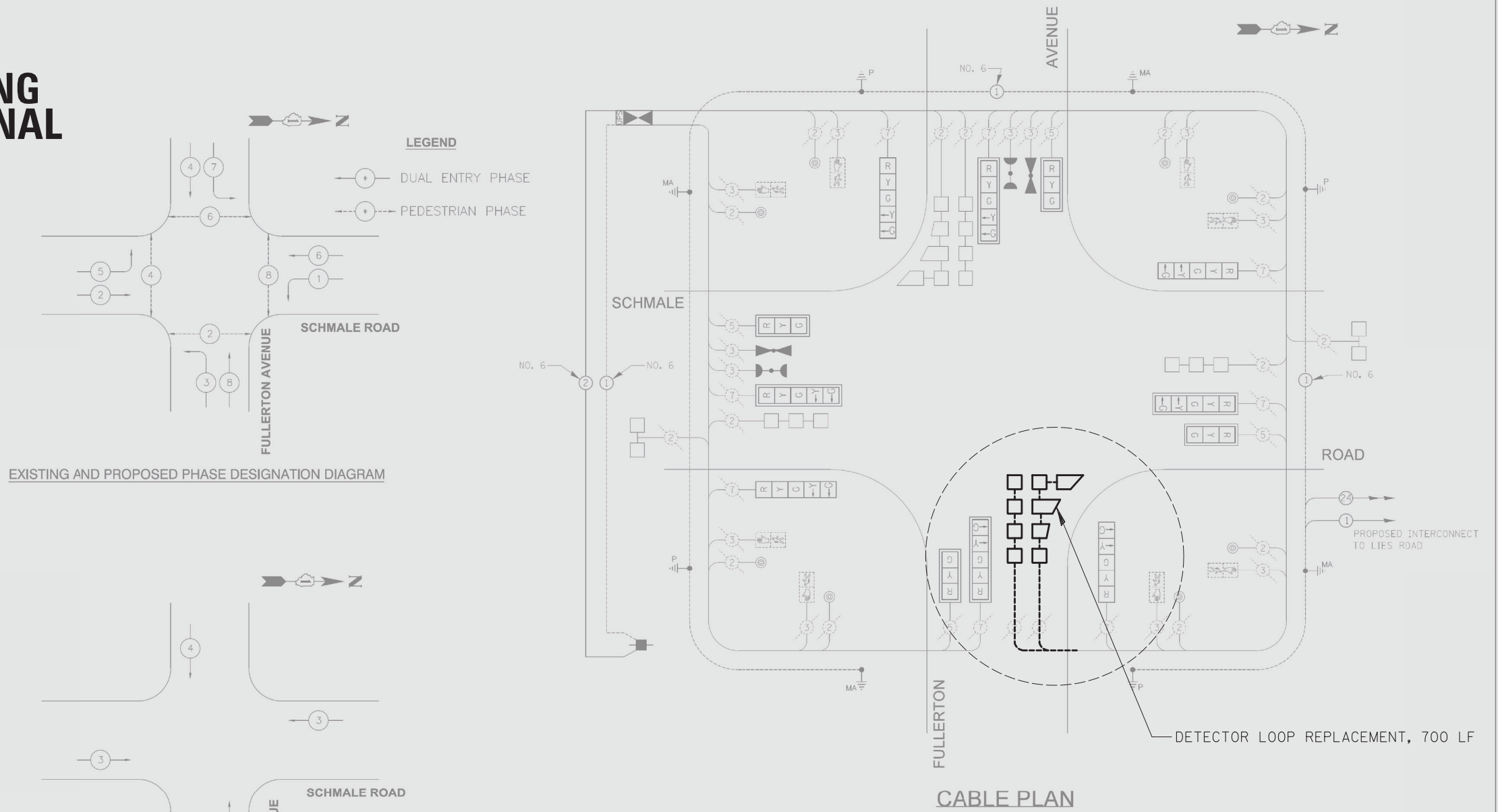
- 4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST-ARM MOUNTED
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, BRACKET MOUNTED
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, MAST-ARM MOUNTED
- 1 EACH SERVICE INSTALLATION

DETECTOR LOOP REPLACEMENT, 700 LF

**RECORD DRAWING
FOR INFORMATIONAL
USE ONLY**

FILE NAME = SIGNAL PLAN SHEETS.dgn	USER NAME = hwtsh	DESIGNED - TH	REVISED - -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODERNIZATION PLAN- SCHMALE ROAD AT FULLERTON AVENUE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 20.0000' / 1" =	DRAWN - TH	REVISED - -			1379	21-00073-00-RS	DuPAGE	40	28	
	PLOT DATE = 03/20/2013	CHECKED - DAZ	REVISED - -			SCALE: 1"=20' SHEET 28 OF 40 SHEETS STA. _____ TO STA. _____					
		DATE - 10/2/2012	REVISED - -			ILLINOIS FED. AID PROJECT CONTRACT NO. 61J85					

RECORD DRAWING FOR INFORMATIONAL USE ONLY



EXISTING AND PROPOSED PHASE DESIGNATION DIAGRAM

CABLE PLAN

LEGEND
 * DUAL ENTRY PHASE
 * PEDESTRIAN PHASE

PROPOSED EMERGENCY VEHICLE PREEMPTORS

EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓

EXISTING AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCHEDULE OF QUANTITIES

PAY CODE NUMBER	PAY ITEM	UNIT	TOTAL
67100100	MOBILIZATION	L SUM	0.2
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	0.2
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	0.2
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	0.2
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.2
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.2
80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
86200200	UNINTERRUPTIBLE POWER SUPPLY, STANDARD	EACH	1
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1
87300010	GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	6
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	527
87900200	DRILL EXISTING HANDHOLE	EACH	1
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8
88500100	INDUCTIVE LOOP DETECTOR	EACH	8
88700200	LIGHT DETECTOR	EACH	2
88700300	LIGHT DETECTOR AMPLIFIER	EACH	1
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
20033046	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE		OPERATION (%)	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12	17	0.50	0.50	102
(YELLOW)	12	25	0.25	75	
(GREEN)	12	15	0.25	45	
ARROW	12	12	0.10	14.4	
PED. SIGNAL	8	25	1.00	200	
CONTROLLER	1	100	1.00	100	
LUMINAIRE	1	310			
TOTAL=					536.4

ENERGY COSTS - BILLED TO: _____

ENERGY SUPPLY - CONTACTS: _____

FILE NAME = CABLE DIAGRAM SHEETS.dgn	USER NAME = hwtvh	DESIGNED - TH	REVISED -
		DRAWN - TH	REVISED -
	PLOT SCALE = 50,0000 / 1 in.	CHECKED - DAZ	REVISED -
	PLOT DATE = 04/01/2013	DATE - 10/2/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE-SCHMALE ROAD AT FULLERTON AVENUE

F.A.U. RTE. 2566	SECTION 11-00279-03-TL	COUNTY DUPAGE	TOTAL SHEETS 19	SHEET NO. 6
CONTRACT NO. 63826				

FILE NAME = N:\GLENDALE\HIGHWAYS\040051D200\Civil\SIGNAL PLANS2.D200.SHT	USER NAME = blibuno	DESIGNED - BRL	REVISED - 1 07/18/23
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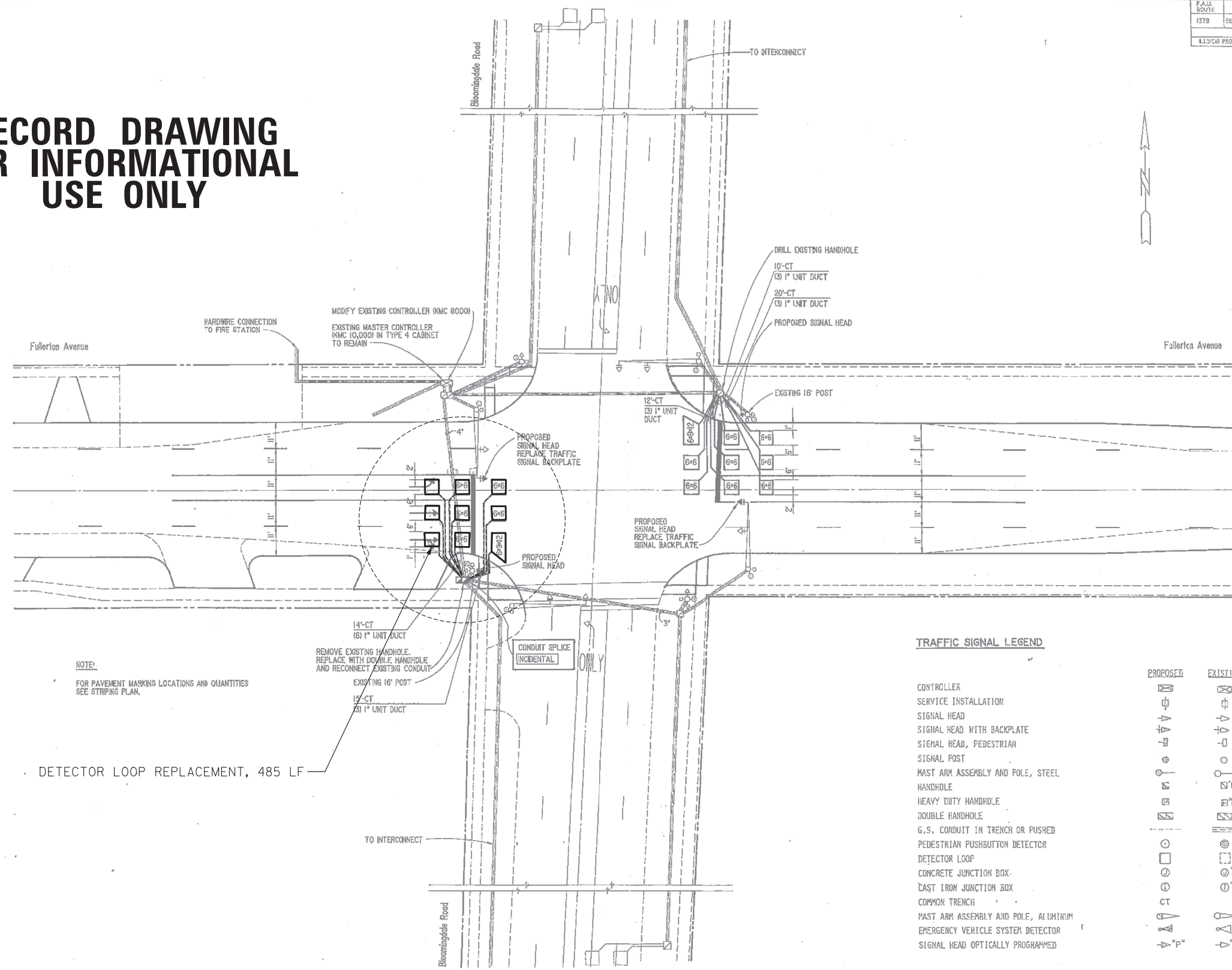
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FULLERTON AVENUE RESURFACING
EXISTING TRAFFIC SIGNAL PLANS - SCHMALE ROAD
(FOR REFERENCE ONLY)

F.A.U. RTE. 1379	SECTION 21-00073-00-R5	COUNTY DUPAGE	TOTAL SHEETS 40	SHEET NO. 29
CONTRACT NO. 61J85				

F.A.U. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	30
ILLINOIS PROJECT 116003 (E54)				

RECORD DRAWING FOR INFORMATIONAL USE ONLY



NOTE:
FOR PAVEMENT MARKING LOCATIONS AND QUANTITIES
SEE STRIPING PLAN.

DETECTOR LOOP REPLACEMENT, 485 LF

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER	[Symbol]	[Symbol]
SERVICE INSTALLATION	[Symbol]	[Symbol]
SIGNAL HEAD	[Symbol]	[Symbol]
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]
SIGNAL POST	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, STEEL	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]
DOUBLE HANDHOLE	[Symbol]	[Symbol]
G.S. CONDUIT IN TRENCH OR PUSHED	[Symbol]	[Symbol]
PEDESTRIAN PUSHBUTTON DETECTOR	[Symbol]	[Symbol]
DETECTOR LOOP	[Symbol]	[Symbol]
CONCRETE JUNCTION BOX	[Symbol]	[Symbol]
CAST IRON JUNCTION BOX	[Symbol]	[Symbol]
COMMON TRENCH	[Symbol]	[Symbol]
MAST ARM ASSEMBLY AND POLE, ALUMINUM	[Symbol]	[Symbol]
EMERGENCY VEHICLE SYSTEM DETECTOR	[Symbol]	[Symbol]
SIGNAL HEAD OPTICALLY PROGRAMMED	[Symbol]	[Symbol]



DESIGNED: BRL	DRAWN: LIBERTY	CHECKED: BEH	APPROVED: REF	DATE: MAY 1, 2023
STANLEY CONSULTANTS				
FULLERTON AVENUE FROM PRESIDENT STREET TO BLOOMINGDALE ROAD			TRAFFIC SIGNAL PLAN	
GLENDALE HEIGHTS, ILLINOIS			SCALE 1" = 20'	REV. 0

NO.	REVISIONS	DRAWN	APPROVED	APPROVED	DATE	NO.	REVISIONS	DRAWN	APPROVED	APPROVED	DATE
1						3					
2						4					

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED - 1 07/18/23
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		CHECKED - AMP	REVISED -
		DATE - 9/1/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FULLERTON AVENUE RESURFACING
EXISTING TRAFFIC SIGNAL PLANS - BLOOMINGDALE ROAD
(FOR REFERENCE ONLY)**

SCALE: N.T.S. SHEET NO. 30 OF 40 SHEETS STA. TO STA.

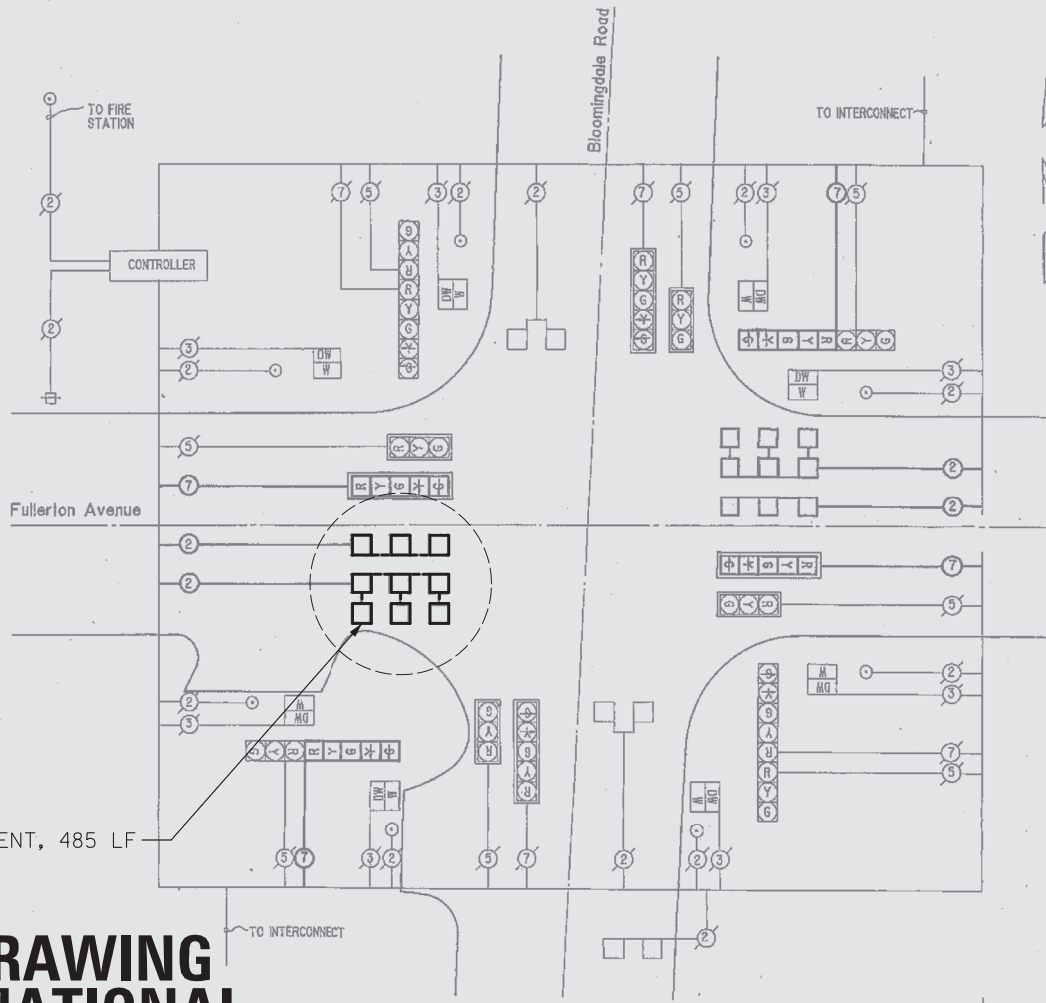
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	30
CONTRACT NO. 61J85				
ILLINOIS FED. AID PROJECT				

F.A.U. ROUTE	MUNICIPAL SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	88-00030-00-11R	DU PAGE	40	31
ILLINOIS PROJECT # 6-603 (554)				

LEGEND

- 8" TRAFFIC SIGNAL SECTION [Symbol]
- 12" TRAFFIC SIGNAL SECTION [Symbol]
- 12" PEDESTRIAN SIGNAL SECTION [Symbol]
- CONTROLLER CABINET [Symbol]
- SERVICE INSTALLATION [Symbol]
- VEHICLE DETECTOR, INDUCTION LOOP [Symbol]
- PUSHBUTTON DETECTOR [Symbol]
- DEMOTES NUMBER OF CONDUCTORS (NEW) ALL LOOP DETECTOR CABLE TO BE SHIELDED, ALL CABLE NO. 14 EXCEPT AS INDICATED [Symbol]
- INDICATES EXISTING CABLE [Symbol]
- EXISTING SIGNAL SECTION [Symbol]
- EXISTING SIGNAL FACE WITH BACKPLATE [Symbol]
- PROPOSED SIGNAL FACE WITH BACKPLATE [Symbol]

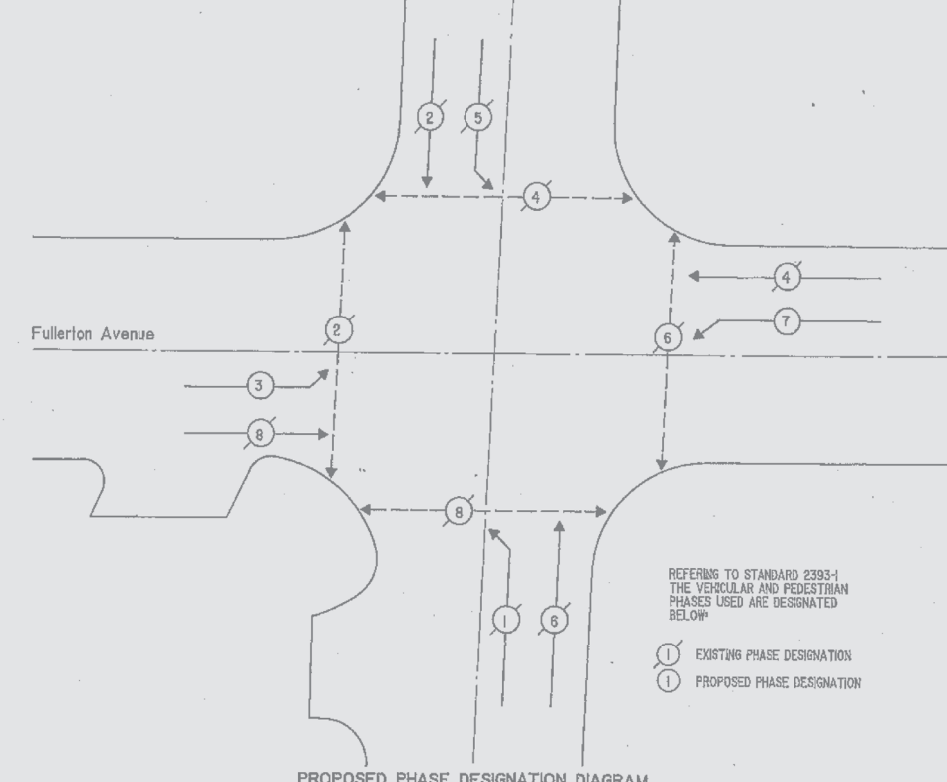
DETECTOR LOOP REPLACEMENT, 485 LF



CABLE PLAN

SCHEDULE OF TRAFFIC SIGNAL HEADS

- 2 SIGNAL HEAD, 1 FACE, 5 SECTION, BRACKET MOUNTED
- 2 SIGNAL HEAD, 1 FACE, 5 SECTION, MAST ARM MOUNTED



REFERING TO STANDARD 2393-1 THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW

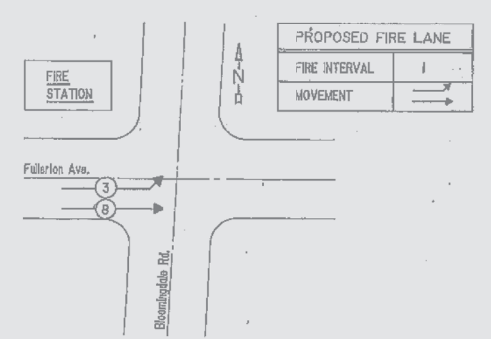
① EXISTING PHASE DESIGNATION
② PROPOSED PHASE DESIGNATION

PROPOSED PHASE DESIGNATION DIAGRAM

SCHEDULE OF QUANTITIES

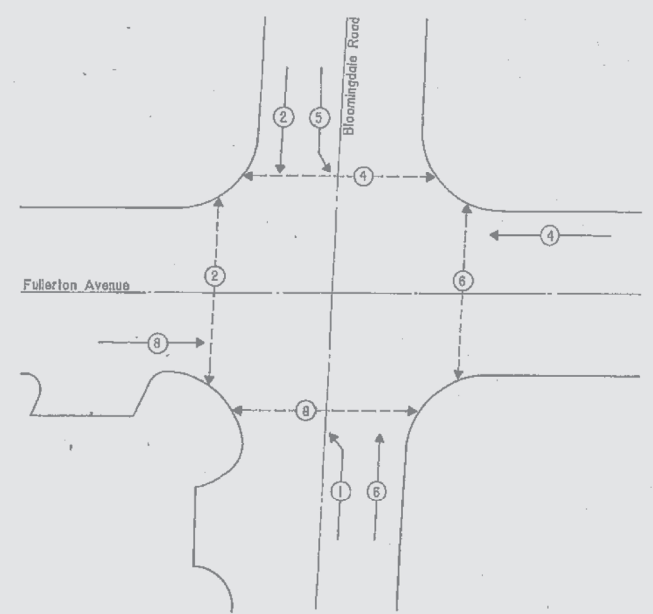
ITEM DESCRIPTION	UNIT	QUANTITY
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	2
FIRE PREEMPTOR	EACH	2
INDUCTION LOOP DETECTOR AMPLIFIER	EACH	4
DETECTOR LOOP	LF	768
UNIT DUCT WITHOUT CABLE IN TRENCH	LF	225
ELECTRIC CABLE IN CONDUIT NO. 14 7/C	LF	579
ELECTRIC CABLE IN CONDUIT NO. 14 2/C TWISTED SHIELDED	LF	701
TRENCH AND BACKFILL	LF	71
MODIFY EXISTING CONTROLLER	EACH	1
PULL AND REINSTALL ELECTRIC CABLE FROM CONDUIT	LF	579
DRILL EXISTING HANDHOLE	EACH	1
PULL ELECTRIC CABLE FROM CONDUIT	LF	579
THERMOPLASTIC PAVEMENT MARKING LINE 4-INCH	LF	5100
THERMOPLASTIC PAVEMENT MARKING LINE 6-INCH	LF	1100
THERMOPLASTIC PAVEMENT MARKING LINE 12-INCH	LF	370
THERMOPLASTIC PAVEMENT MARKING LINE 24-INCH	LF	120
THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS	SF	120
PAVEMENT MARKING TAPE, TYPE III	LF	3000
SIGNAL HEAD, 1 FACE, 5 SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, 1 FACE, 5 SECTION, MAST ARM MOUNTED	EACH	2
CONCRETE DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING HANDHOLE	EACH	1

RECORD DRAWING FOR INFORMATIONAL USE ONLY



FIRE LANE SEQUENCE

1. TERMINATION OF PHASES 1+5, 1+6, 2+5, 5+6, OR 4+7 SHALL BE WITH A CIRCULAR YELLOW FOLLOWED BY A CIRCULAR RED PRIOR TO DISPLAYING THE FIRE LANE INTERVAL.
2. TERMINATION OF PHASES 4+6 OR 3+7 SHALL BE WITH A CIRCULAR YELLOW FOLLOWED BY A CIRCULAR RED PRIOR TO DISPLAYING THE FIRE LANE INTERVAL FOR WESTBOUND FULLERTON AVENUE. FOR EASTBOUND FULLERTON AVENUE, TERMINATION OF PHASES 4+6 OR 3+7 SHALL BE WITH A CIRCULAR GREEN PRIOR TO DISPLAYING THE FIRE LANE INTERVAL.
3. TERMINATION OF THE FIRE LANE INTERVAL SHALL BE WITH A CIRCULAR YELLOW FOR EASTBOUND FULLERTON AVENUE FOLLOWED BY A CIRCULAR RED PRIOR TO RETURNING TO NORMAL PHASE 2+6.
4. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
5. ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, THEREFORE IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE FIRE LANE INTERVAL.



EXISTING PHASE DESIGNATION DIAGRAM

NO.	REVISIONS	DRAWN	APPROVED	DATE	NO.	REVISIONS	DRAWN	APPROVED	DATE
1					2				
2					3				
3					4				
4					5				
5					6				

DESIGNED - LRL	CHECKED - BEH	APPROVED - RPH
DRAWN - LIZRITY	DATE - MAY 1, 89	

STANLEY CONSULTANTS
INTERNATIONAL CONSULTANTS IN ENGINEERING, ARCHITECTURE, PLANNING, AND MANAGEMENT

FULLERTON AVENUE FROM PRESIDENT STREET TO BLOOMINGDALE ROAD
CABLE PLAN AND PHASE DESIGNATION DIAGRAM
GLENDALE HEIGHTS, ILLINOIS



SCALE	NO SCALE
NO.	REV.
9696 - 12	0

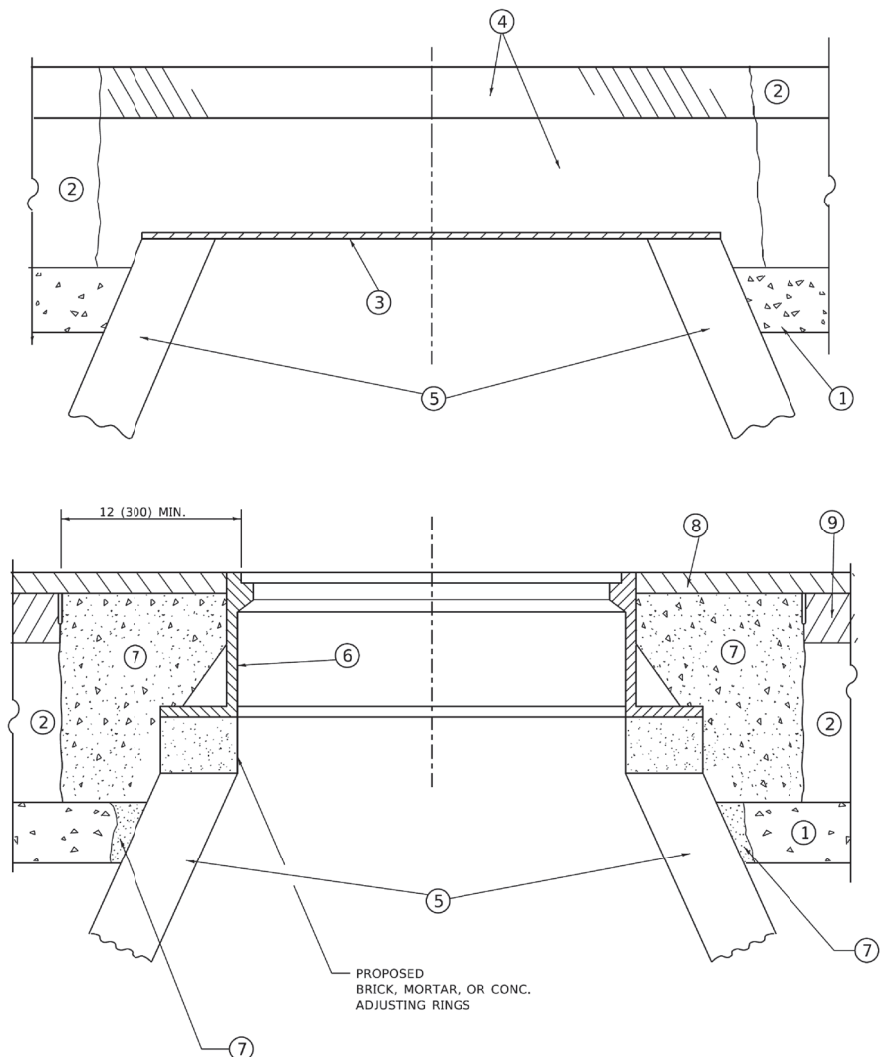
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PLOT SCALE = *NOT TO SCALE*		CHECKED - AMP	REVISED -
PLOT DATE = 9/1/2023		DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FULLERTON AVENUE RESURFACING
EXISTING TRAFFIC SIGNAL PLANS - BLOOMINGDALE ROAD
(FOR REFERENCE ONLY)

SCALE: N.T.S. SHEET NO. 31 OF 40 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1379	21-00073-00-R5	DUPAGE	40	31
CONTRACT NO. 61J85				
ILLINOIS FED. AID PROJECT				



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS*PP-1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS*PP-1 CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	CHECKED -	REVISED - R. BORO 03-09-11		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	BD600-03 (BD-08)		CONTRACT NO.
PLOT SCALE = 100.0000 ' / in.	DATE - 10-25-94	REVISED - R. BORO 12-06-11		ILLINOIS FED. AID PROJECT						
PLOT DATE = 11/18/2022	DATE -	REVISED - K. SMITH 11-18-22								

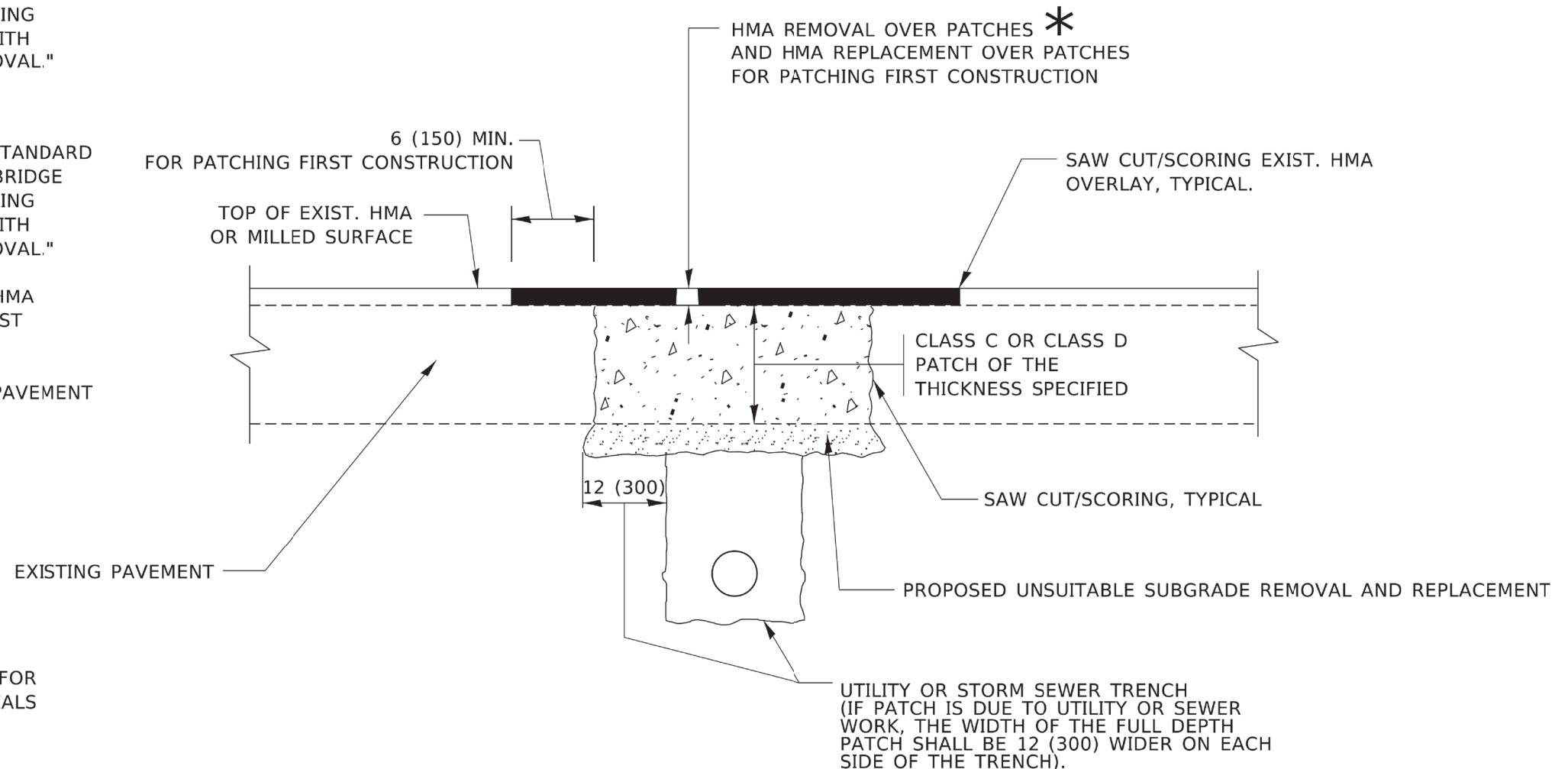
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	PLOT SCALE = 22'	CHECKED - AMP	REVISED -		ILLINOIS FED. AID PROJECT								
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -										

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

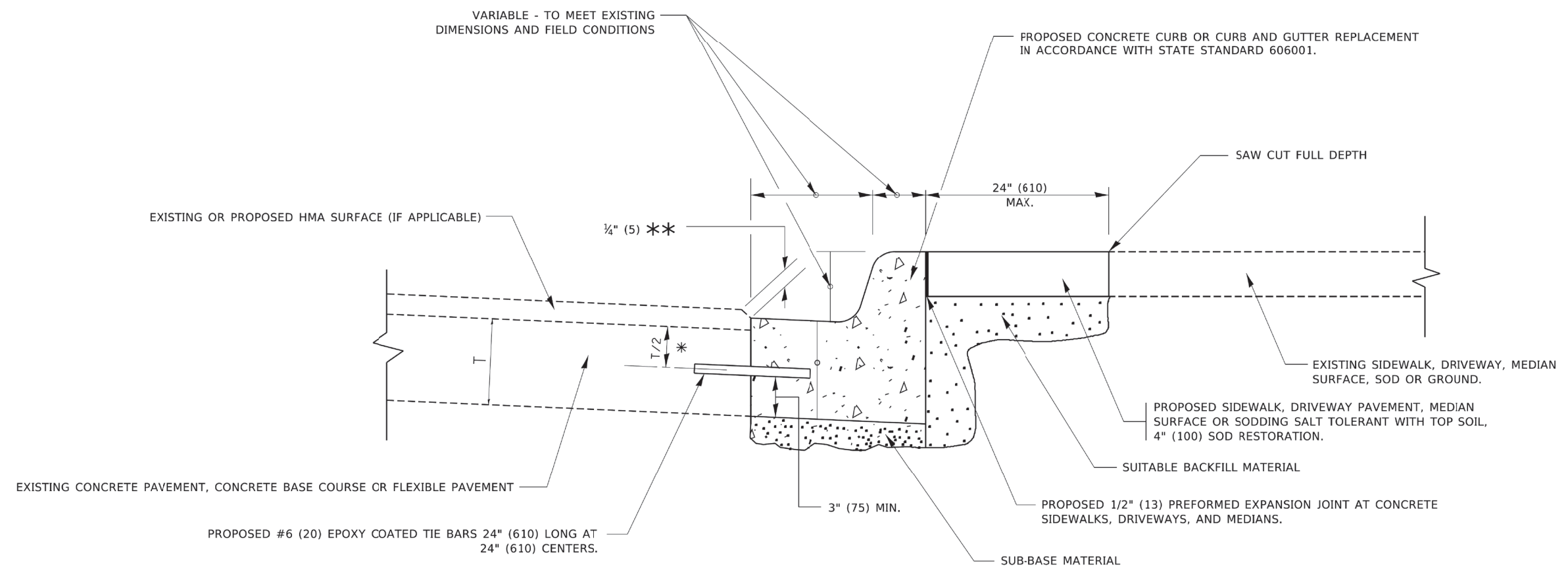
1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Lawrence,DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
DRAWN -	REVISOR - R. BORO 09-04-07	SCALE: NONE			SHEET 1 OF 1 SHEETS	STA. TO STA.	1379	21-00073-00-R5	DUPAGE	40	33
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. ENG 10-27-08			CONTRACT NO. 61J85						
PLOT DATE = 11/19/2022	DATE - 10-25-94	REVISED - K. SMITH 11-18-22			ILLINOIS FED. AID PROJECT						

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
N:\GLENDALE\RIGHTS\040051D208\Civil\STD	BD400-04 (BD-22).D208.SHT	DRAWN - KK	REVISED -			SCALE: #N.T.S.#	SHEET NO. 33 OF 40 SHEETS	STA. TO STA.	21-00073-00-R5	DUPAGE	40	33
	PLOT SCALE = 28'	CHECKED - AMP	REVISED -			CONTRACT NO. 61J85						
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -			ILLINOIS FED. AID PROJECT						



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

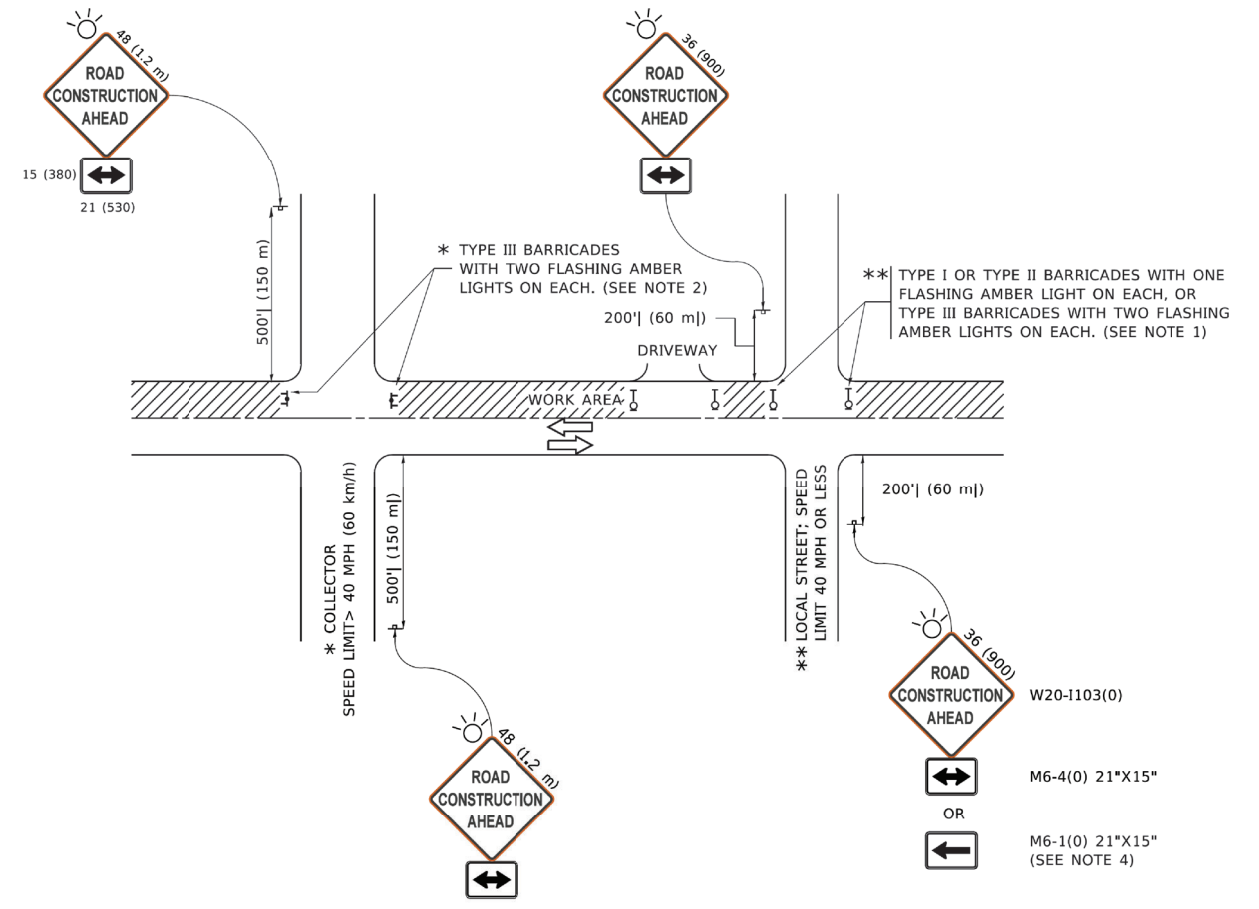
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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USER NAME = footernj	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISOR - M. GOMEZ 01-22-01	1379			21-00073-00-RS	DUPAGE	40	34	
PLOT SCALE = 50,0000' / in.	CHECKED -	REVISOR - R. BORO 12-15-09			CONTRACT NO.				
PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19			ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 22'	CHECKED - AMP	REVISOR -	CONTRACT NO.							
PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -	ILLINOIS FED. AID PROJECT							



NOTES:

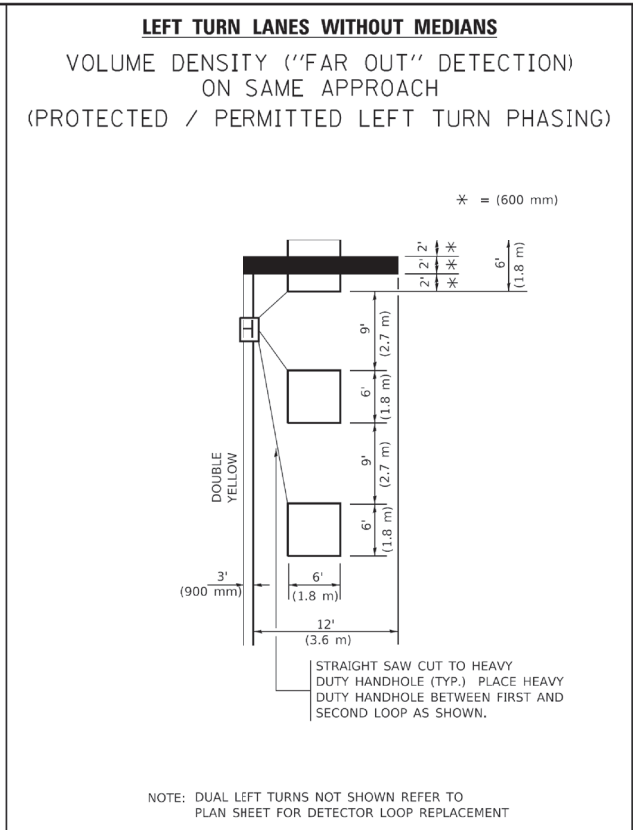
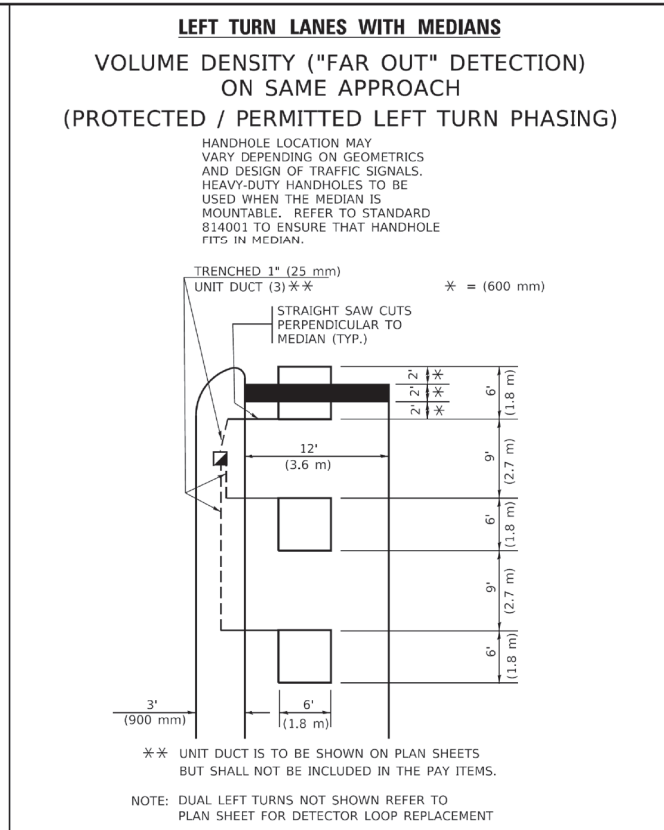
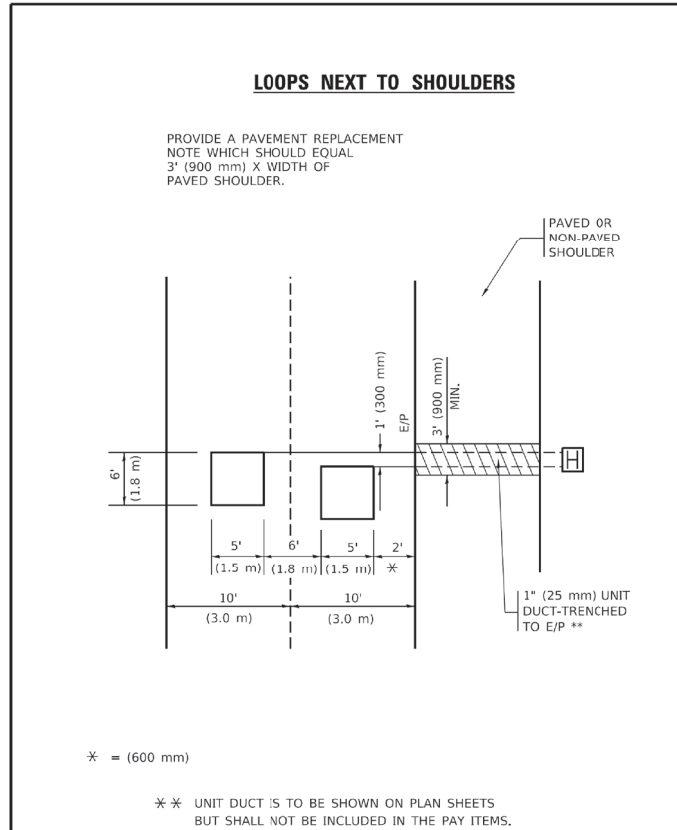
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

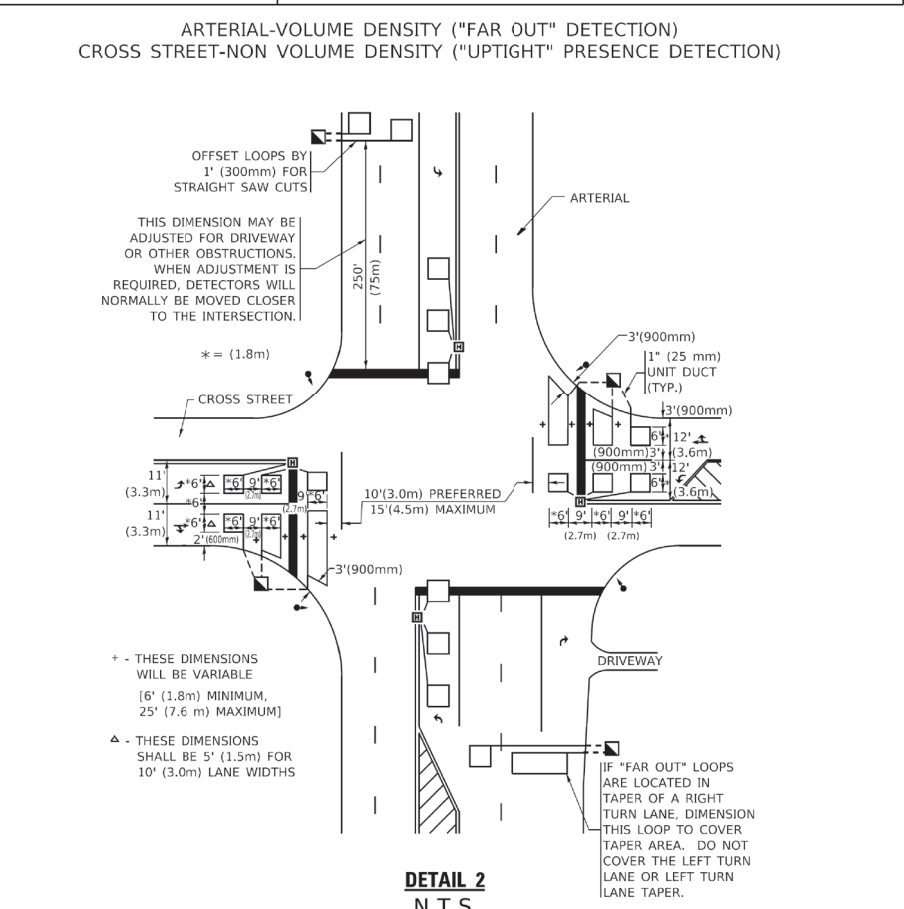
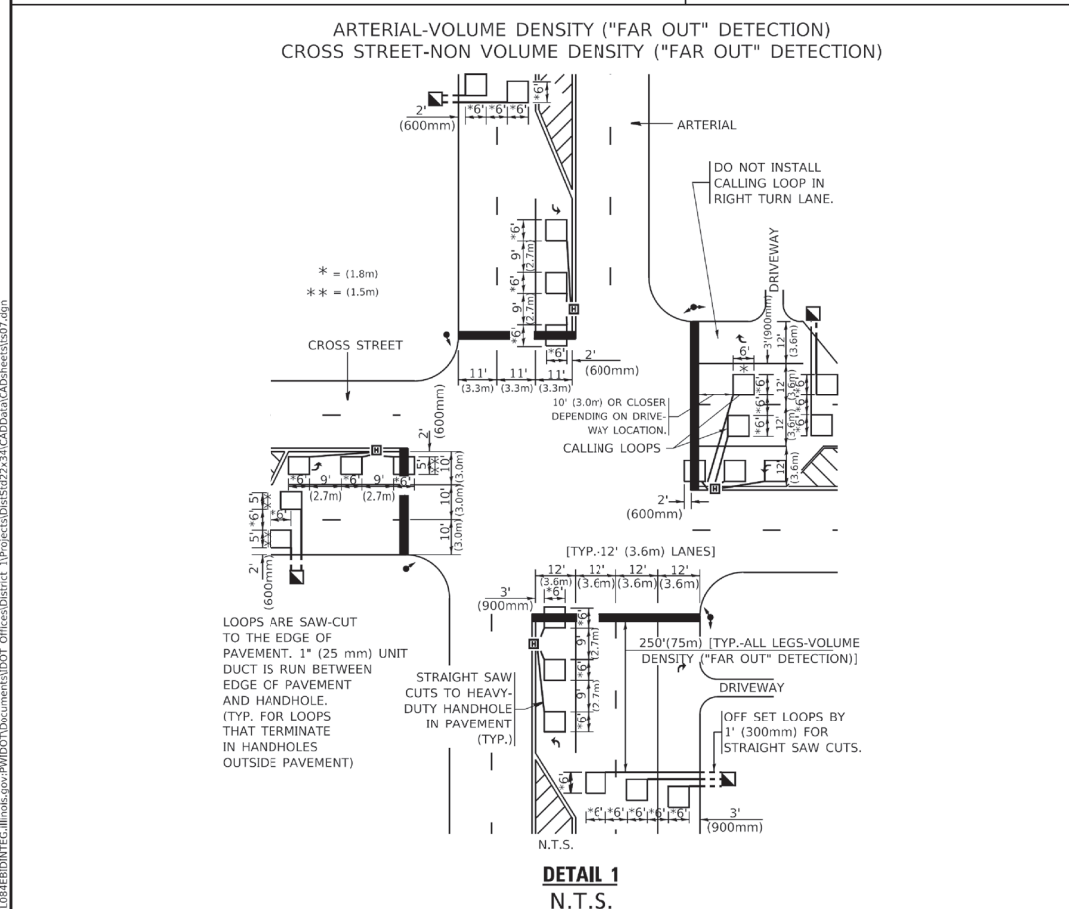
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USER NAME = footernj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN -	REVISOR - T. RAMMACHER 01-06-00	1379			21-00073-00-RS	DUPAGE	40	36		
PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13			TC-10		CONTRACT NO.		ILLINOIS FED. AID PROJECT	
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.			

FILE NAME =	USER NAME = blibunoo	DESIGNED - BRL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 22'	CHECKED - AMP	REVISED -			TC-10		CONTRACT NO.		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -			SCALE: #N.T.S.#	SHEET NO. 36 OF 40 SHEETS	STA. TO STA.			



- NOTES:
- VEHICLES LOOP DETECTORS
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
 - * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
 - * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
 - * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
 - * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
 - * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
 - * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.



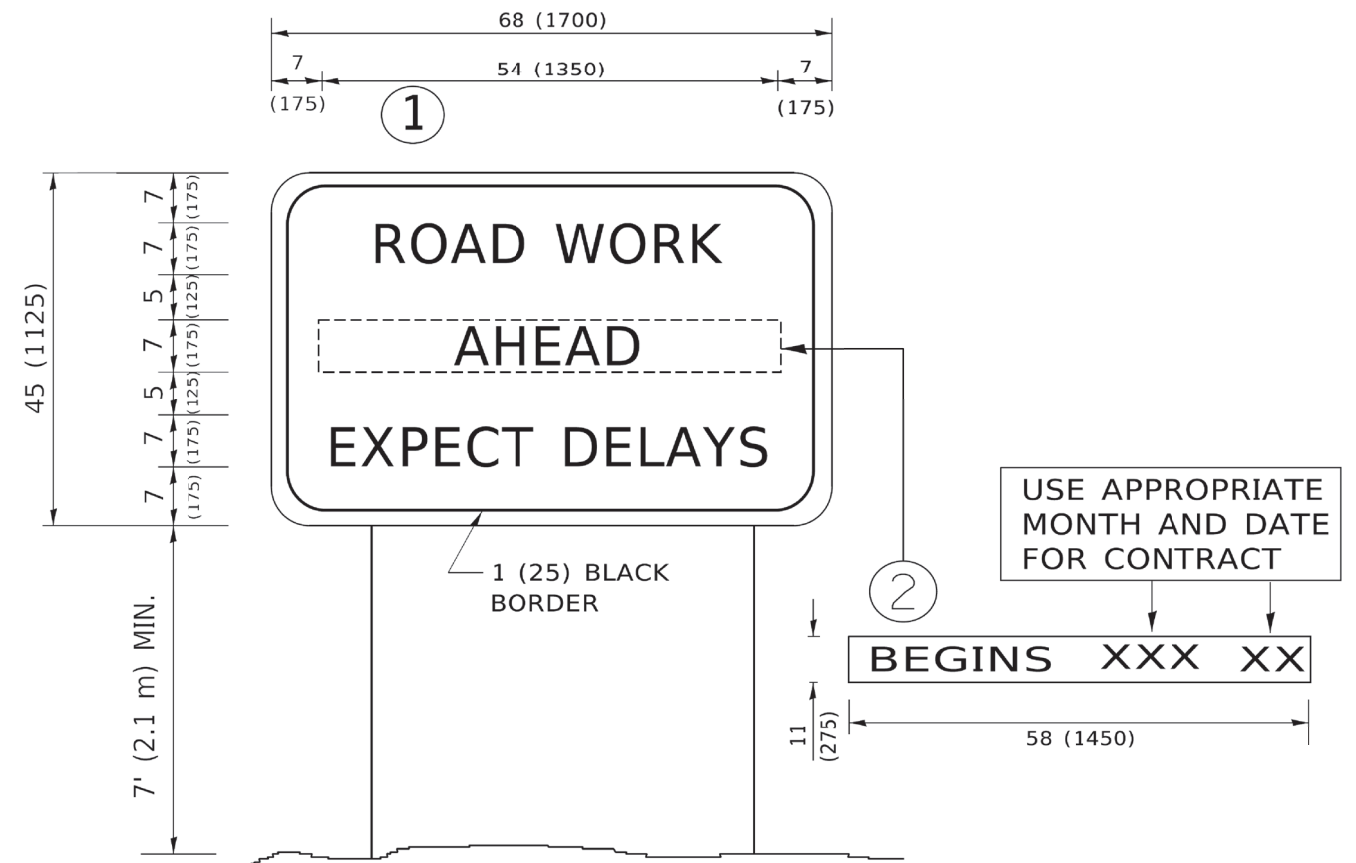
- #### PLACEMENT OF DETECTORS
- THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.
- LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.
- "FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.
- NOTE:
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995
- THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50,0000' / in.	CHECKED - R.K.F.	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 - DETECTOR LOOP INSTALLATION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DETAILS FOR ROADWAY RESURFACING		TS-07		CONTRACT NO.		
SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT		

USER NAME = blibunoo	DESIGNED - BRL	REVISED -
PLOT SCALE = 22'	CHECKED - AMP	REVISED -
PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT 1 - DETECTOR LOOP INSTALLATION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DETAILS FOR ROADWAY RESURFACING		1379		21-00073-00-RS		DUPAGE 40 39	
SCALE: #N.T.S.#		SHEET NO. 39 OF 40 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT			
CONTRACT NO. 61J85									



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = footernj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				TC-22		CONTRACT NO.		
SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT		

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	PLOT SCALE = 22'	CHECKED - AMP	REVISED -
	PLOT DATE = 9/1/2023	DATE - 9/1/2023	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ARTERIAL ROAD INFORMATION SIGN		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				21-00073-00-RS		CONTRACT NO. 61J85		
SCALE: #N.T.S.#		SHEET NO. 40 OF 40 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT		