

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	1
		ILLINOIS	CONTRACT NO. 61F07	

PLANS FOR PROPOSED FEDERAL AID HIGHWAYS

FAU 3517 (KIRCHOFF ROAD)
HICKS ROAD TO NEW WILKE ROAD
RESURFACING
SECTION: 17-00121-00-RS
PROJECT NO.: JRFU(653)
CITY OF ROLLING MEADOWS
COOK COUNTY
JOB NO.: C-91-260-17

SEE SHEET 2 FOR LIST OF HIGHWAY STANDARDS.

SEE SHEET 2 FOR INDEX OF SHEETS.



Gary Rozwadowski
GARY ROZWADOWSKI
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS
ILLINOIS REGISTRATION No. 062-051689
EXPIRATION DATE: 11/30/19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: *Fred Vogt* *Fred Vogt*
7/10/18
DIRECTOR OF PUBLIC WORKS
CITY OF ROLLING MEADOWS

PASSED: *Christopher Holt*
August 29, 2018
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

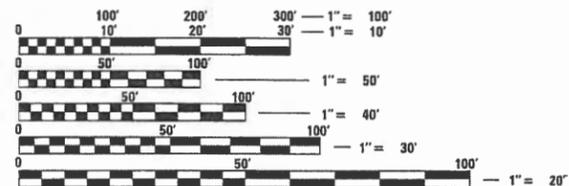
RELEASING FOR BID BASED ON LIMITED REVIEW: *Anthony J. Quigley*
August 29, 2018
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. SCHAUMBURG, IL

TRAFFIC DATA

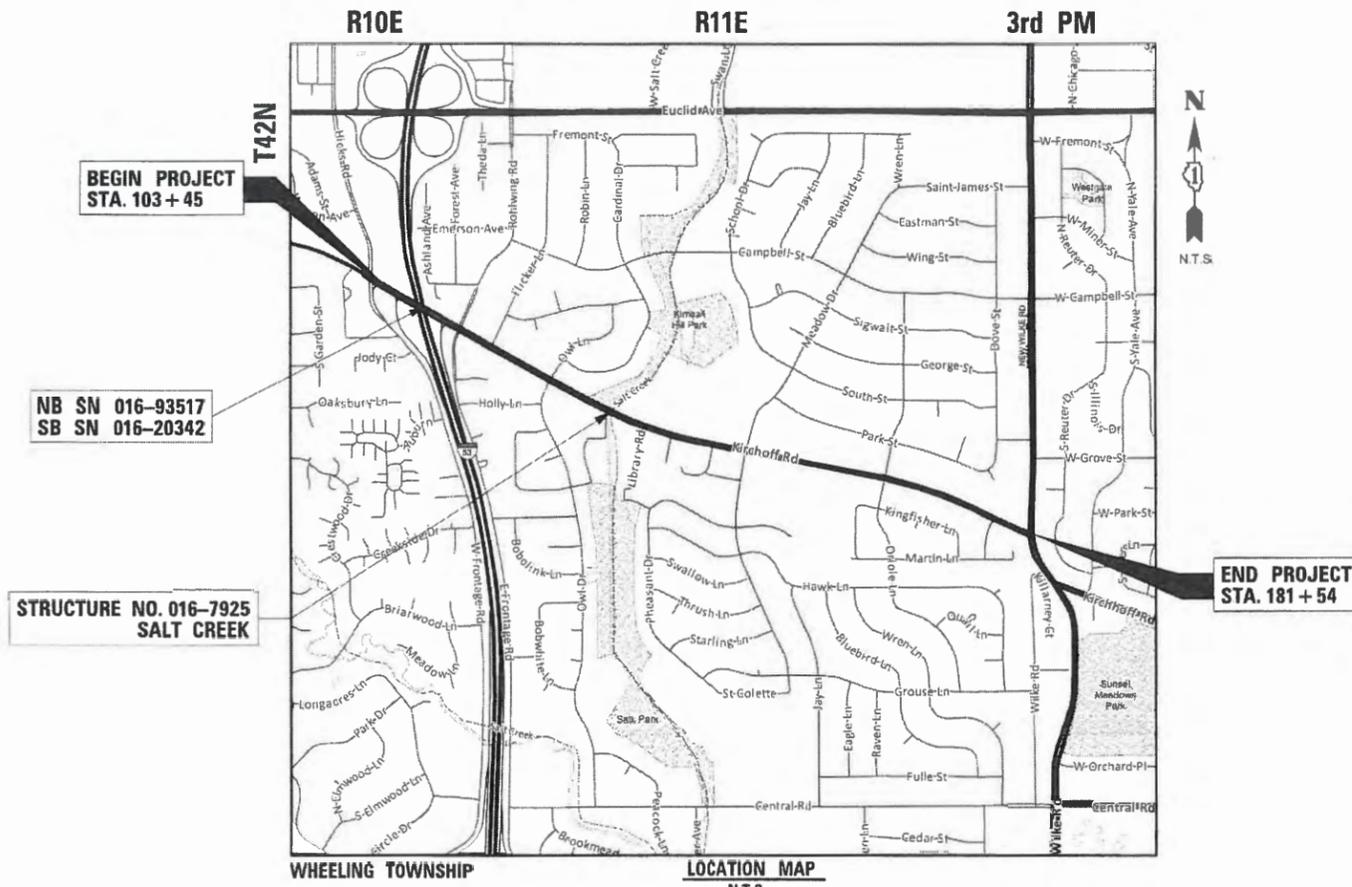
KIRCHOFF RD ADT = 12,800 (2017)
DESIGN DESIGNATION = MINOR ARTERIAL (35 MPH)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61F07



LOCATION MAP
N.T.S.
GROSS LENGTH = 7,806 FT = 1.48 MILE
NET LENGTH = 7,806 FT = 1.48 MILE

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (MUTCD), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 SIXTH EDITION, "THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (MWRD) WATERSHED MANAGEMENT ORDINANCE AND TECHNICAL GUIDANCE MANUAL, THE "DETAILS" IN THE PLANS, LATEST EDITION OF THE MANUAL OF TEST PROCEDURE OF MATERIALS, THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS, THE AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES, THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504), AND THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES. IN CASE OF CONFLICT BETWEEN THE APPLICABLE ORDINANCES NOTED, THE MORE STRINGENT SHALL TAKE PRECEDENCE AND SHALL CONTROL ALL CONSTRUCTION.
2. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE THE MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE OWNER, AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
3. THE CONTRACTOR SHALL COMPLY WITH ALL RULES AND REGULATIONS OF OSHA DURING CONSTRUCTION OF IMPROVEMENTS AND RESTORATION. NEITHER THE DEPARTMENT, CITY, NOR ITS APPOINTED ENGINEER SHALL BE RESPONSIBLE FOR THE CONTRACTOR'S COMPLIANCE WITH OSHA.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
5. THE CONTRACTOR SHALL LIMIT HIS CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.
6. AT THE END OF EACH DAY, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASSURE THAT ALL STREETS ADJACENT TO THE PROJECT ARE FREE OF ALL CONSTRUCTION RELATED DEBRIS INCLUDING DIRT, STONE, NAILS, ETC. THE WORK SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER AND THE CITY OF ROLLING MEADOWS.
7. THE CONTRACTOR SHALL PROVIDE ACCESS TO ADJUTING PROPERTIES AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER.
8. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED)
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
10. ALL PEDESTRIAN ROUTES CONSTRUCTED WITHIN THE LIMITS OF THIS PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
11. THIS PROJECT IS LOCATED IN THE DOWN TOWN DISTRICT OF ROLLING MEADOWS. TRAFFIC CONTROL MUST BE MAINTAINED THROUGHOUT THE SITE FOR BOTH VEHICLES AND PEDESTRIANS. ACCESS TO BUSINESS MUST BE MAINTAINED DURING BUSINESS HOURS. SIDEWALK CLOSURES FOR ADA RAMPS MUST BE CLEARLY SIGNED AND LIMITED TO 5 DAYS. INTERIM WORKING DAYS LISTED IN THE SPECIFICATIONS WILL BE STRICTLY ENFORCED.
12. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE CITY OF ROLLING MEADOWS.
13. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE LIMITS OF IMPROVEMENT SHOWN ON THE PLANS SHALL BE REPLACED BY THE CONTRACTOR.
14. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
15. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
16. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

17. THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
18. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHEET INCLUDED IN THE PLANS.
19. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
20. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
21. FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS. GRINDING OF THE EXISTING CONCRETE BASE MAY BE REQUIRED TO ESTABLISH THE SPECIFIED DEPTH.
22. CLASS B PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE SHALL BE REMOVED TO A DEPTH OF TEN (10) INCHES AND REPLACED WITH TEN (10) INCHES OF SI CONCRETE. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE CONCRETE AS SPECIFIED THE STANDARD SPECIFICATIONS.

**METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO
SOIL EROSION AND SEDIMENT CONTROL CONSTRUCTION NOTES**

1. CONCRETE WASHOUT FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL AND SHALL BE INSTALLED PRIOR TO ANY ON SITE CONSTRUCTION ACTIVITIES INVOLVING CONCRETE.
2. DISTURBED AREAS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) DAYS.

SHEET NO. DESCRIPTION

- 1 COVER SHEET
- 2 GENERAL NOTES
- 3-5 SUMMARY OF QUANTITIES
- 6-7 TYPICAL SECTIONS
- 8-11 SCHEDULES
- 12-13 ALIGNMENT, TIES AND BENCHMARK
- 14-28 PLAN
- 29 MAINTENANCE OF TRAFFIC PLAN
- 30-36 ADA INTERSECTIONS
- 37-48 TRAFFIC SIGNAL, CABLE PLAN (REF ONLY)
- 49 LANDSCAPE PLAN
- 50-53 STRUCTURAL SHEETS
- 54-66 DISTRICT 1 STANDARDS

LIST OF DISTRICT 1 DETAILS

- DISTRICT DETAIL TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
- DISTRICT DETAIL TC-11: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
- DISTRICT DETAIL TC-13: DISTRICT 1 TYPICAL PAVEMENT MARKINGS
- DISTRICT DETAIL TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
- DISTRICT DETAIL TC-16: SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
- DISTRICT DETAIL TC-22: ARTERIAL ROAD INFORMATION SIGN
- DISTRICT DETAIL TC-26: DRIVEWAY ENTRANCE SIGNING
- DISTRICT DETAIL BD-8: FRAMES AND LIDS ADJUSTMENT WITH MILLING
- DISTRICT DETAIL BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
- DISTRICT DETAIL BD-32: BUTT JOINT AND HMA TAPER DETAILS
- DISTRICT DETAIL TS-05: DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
- DISTRICT DETAIL TS-07: DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREV. & PATTERNS
- 442201-08 CLASS B PATCHES
- 604001-04 FRAME AND LIDS, TYPE 1
- 604091-03 FRAME AND GRATE TYPE 24
- 606001-07 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701006-05 OFF ROAD OPERATION 2 LANE 2 WAY 15 FEET TO EDGE OF PAVEMENT
- 701101-05 OFF ROAD OPERATION MULTI LANE LESS THAN 15 FEET TO EDGE OF PAVEMENT
- 701301-04 LANE CLOSURE 2 LANE 2 WAY - SHORT TERM OPERATION
- 701311-03 LANE CLOSURE 2 LANE 2 WAY MOVING OPERATION DAYTIME ONLY
- 701427-05 LANE CLOSURE, MULTI-LANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
- 701501-06 URBAN LANE CLOSURE, 2 LANE 2 WAY UNDIVIDED
- 701502-08 URBAN LANE CLOSURE, 2 LANE 2 WAY WITH BIDIRECT LEFT TURN LANE
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSAL MEDIAN
- 701602-09 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-06 SIDEWALK CORNER OR CROSSWALK CLOSURE
- 701901-07 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-03 SIGN PANEL ERECTION DETAILS

FILE NAME = N:\ROLLINGMEADOWS\18083618\18083618.dwg	US_B NAME = 18083618	D.ESIGNED - GR	REVISED -
	BE 18083618	D.RAWN - PM	REVISED -
		CHECKED - GR	REVISED -
		DATE = 9/18/2018	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
KIRCHOFF RD**

SCALE: 2"	SHEET OF	SHEETS STA. TO STA.	FULL SITE = 3517	SECTION = 17-00121-00-RS	COUNTY = COOK	TOTAL SHEET SHEETS, NO. = 66	CONTRACT NO. 61F07
							ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES				0005
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
*	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	8
*	20101400	NITROGEN FERTILIZER NUTRIENT	POUND	15
*	20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	15
*	20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	15
*	20101700	SUPPLEMENTAL WATERING	UNIT	29
	20200100	EARTH EXCAVATION	CJ YD	100
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1250
*	25200110	SODDING, SALT TOLERANT	SQ YD	1250
	35101500	AGGREGATE BASE COURSE, TYPE B	CJ YD	80
	35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	509
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	37085
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	100
	40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2982
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	230
	40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	2800
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	60
*		INDICATES SPECIALTY ITEM		
S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES				0005
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4833
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	50
	42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	150
	42400800	DETECTABLE WARNINGS	SQ FT	190
	44000100	PAVEMENT REMOVAL	SQ YD	410
	44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	35935
	44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	19440
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	460
	44000300	CURB REMOVAL	FOOT	1495
	44000600	SIDEWALK REMOVAL	SQ FT	3126
	44003100	MEDIAN REMOVAL	SQ FT	2814
	52000050	PREFORMED JOINT SEAL 4"	FOOT	130
	58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	290
	58300100	PORTLAND CEMENT MORTAR FAIRING COURSE	FOOT	640
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	10
*		INDICATES SPECIALTY ITEM		
S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES				0005
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
60266600	VALVE BOXES TO BE ADJUSTED	EACH	9	
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	25	
60404950	FRAMES AND GRATES, TYPE 24	EACH	3	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	10	
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1738	
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	168	
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	75	
66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	
66900530	SOIL DISPOSAL ANALYSIS	EACH	1	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	
67100100	MOBILIZATION	LSUM	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM	1	
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	LSUM	1	
*	INDICATES SPECIALTY ITEM			
S	INDICATES SPECIAL PROVISION			

SUMMARY OF QUANTITIES				0005
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4130	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1380	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	963	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	24210	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	5330	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	3946	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	620	
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3000	
*	72000100 SIGN PANEL - TYPE 1	SQ FT	30	
*	72400310 REMOVE SIGN PANEL - TYPE 1	SQ FT	22.5	
*	72800100 TELESCOPING STEEL SIGN SUPPORT	FOOT	53	
*	78000100 THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	963	
*	78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	24210	
*	78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5330	
*	INDICATES SPECIALTY ITEM			
S	INDICATES SPECIAL PROVISION			

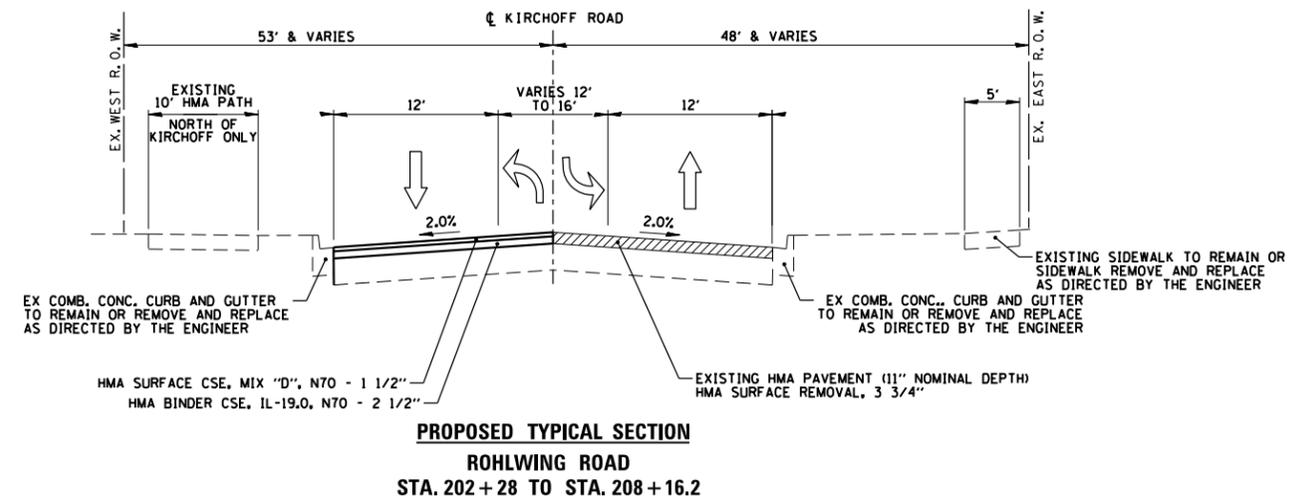
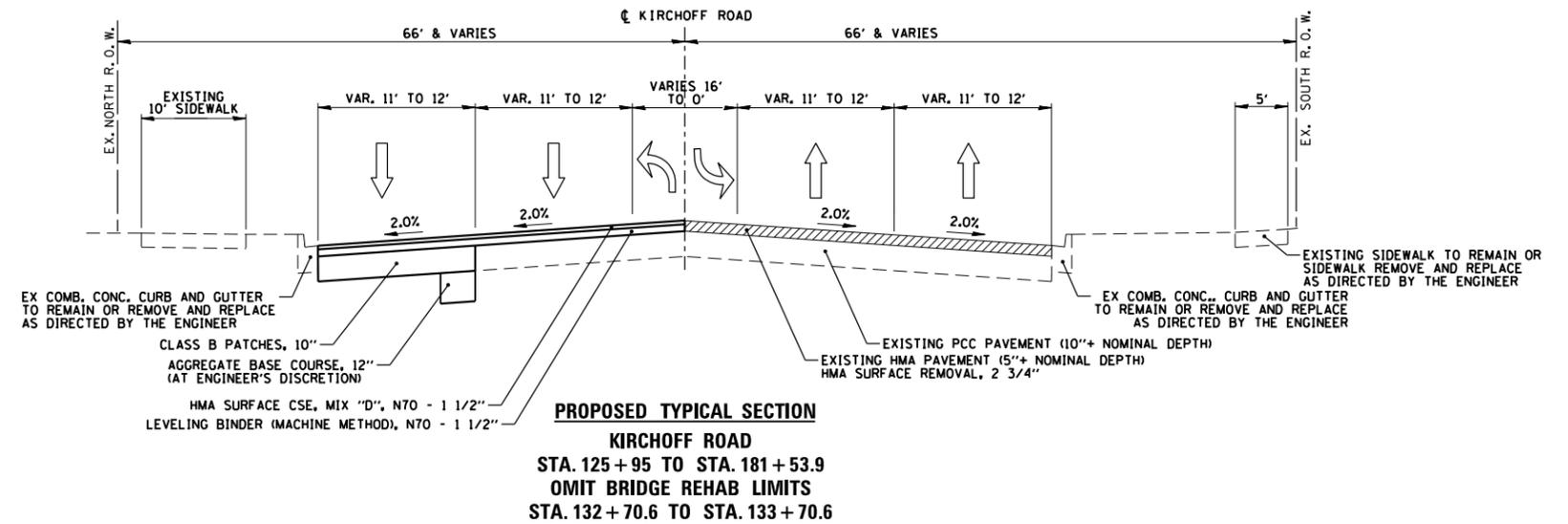
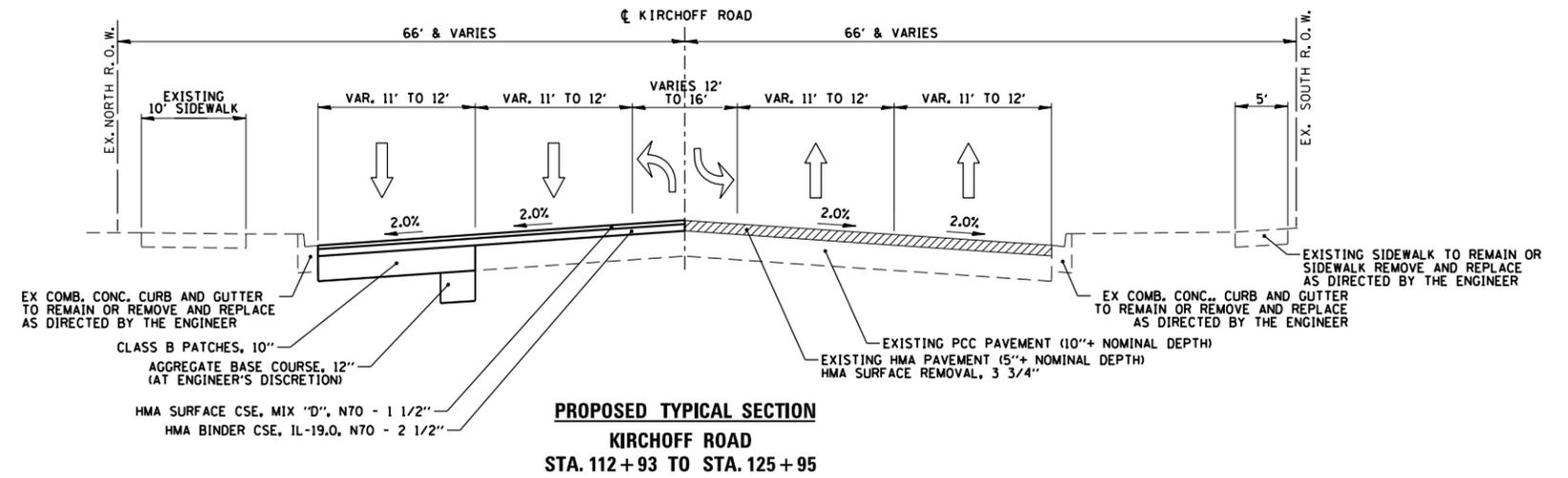
SUMMARY OF QUANTITIES				0005	
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
*		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	3946
*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	620
*		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	760
*		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	760
*		81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	80
*	S	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2.00
*	S	88600600	DETECTOR LOOP REPLACEMENT	FOOT	3982
*	S	89502376	REBUILD EXISTING HANDHOLE	EACH	3
*		A2004820	TREE, GLEDITSIA TRIACANTHOS INERMIS SKYLINE (SKYLINE THORNLESS COMMON HONEYLOCUST), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	1
*		C2C02224	SHRUB, COTONEASTER HESSEI (HESSE COTONEASTER), 2' WIDTH, CONTAINER	EACH	14
*		K0012990	PERENNIAL PLANTS, ORNAMENTAL TYPE, GALLON POT	UNIT	2.77
*	S	K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	14
*		XX009263	SHRUB, FORSYTHIA X INTERMEDIA 'NIMBUS' (SHOW-OFF SUGAR BABY FORSYTHIA), 18" HEIGHT, CONTAINER	EACH	25
*		C2003600	SHRUB, HYDRANGEA PANICULATA LITTLE QUICK FIRE (LITTLE QUICK FIRE HYDRANGEA), CONTAINER GROWN, 5 GALLON	EACH	11
	S	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1
	S	X1700006	STAMPED COLORED PORTLAND CEMENT CONCRETE MEDIAN SURFACE 4 INCH (SPECIAL)	SQ FT	3405
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

SUMMARY OF QUANTITIES				0005	
		CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	S	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	40
	S	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	25
	S	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH (SPECIAL)	SQ FT	2670
	S	X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	4190
	S	X4420682	CLASS B PATCHES, TYPE II, 10 INCH (SPECIAL)	SQ YD	1890
	S	X4420683	CLASS B PATCHES, TYPE III, 10 INCH (SPECIAL)	SQ YD	460
	S	X6024503	INLETS TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)	EACH	50
	S	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	25
		X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	80
	S	Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	290
		Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	4560
		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	206
	S	Z0032700	KEYWAY REPAIR	FOOT	640
Δ		Z0076600	TRAINEES	HOUR	500
Δ		Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500
*			INDICATES SPECIALTY ITEM		
	S		INDICATES SPECIAL PROVISION		

Δ 0042

NOTES:

1. LOCATIONS FOR THE FOLLOWING ITEMS TO BE DETERMINED IN THE FIELD BY THE ENGINEER:
 - . COMB. CONC. CURB & GUTTER REMOVE AND REPLACE
 - . CLASS B PATCHING
 - . PCC SIDEWALK 5"
 - . PARKWAY RESTORATION - SODDING, SALT TOLERANT
2. VERTICAL BRIDGE CLEARANCE UNDER RTE 53 SHALL NOT DECREASE
3. PAVEMENT SHALL BE MILLED PRIOR TO PATCHING.
4. PARKWAY RESTORATION LIMITED TO 18" ADJACENT TO SIDEWALK AND CURB AND GUTTER.



FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -
N:\ROLLINGMEADOWS\980361.BR\5\BR100\5\980361BR195\Civil\TYP_98361BR195-01.SHT		DRAWN - PM	REVISED -
Default	PLOT SCALE = 60'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
KIRCHOFF ROAD**

SCALE: 60' SHEET OF SHEETS STA. TO STA.

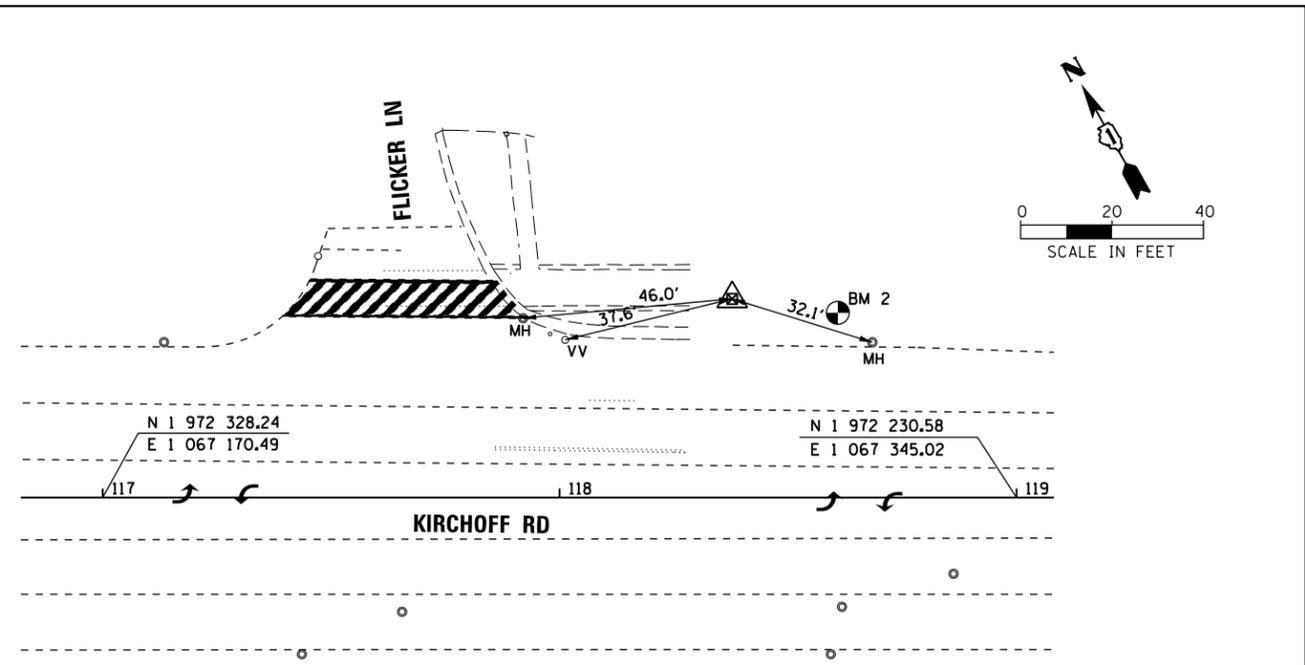
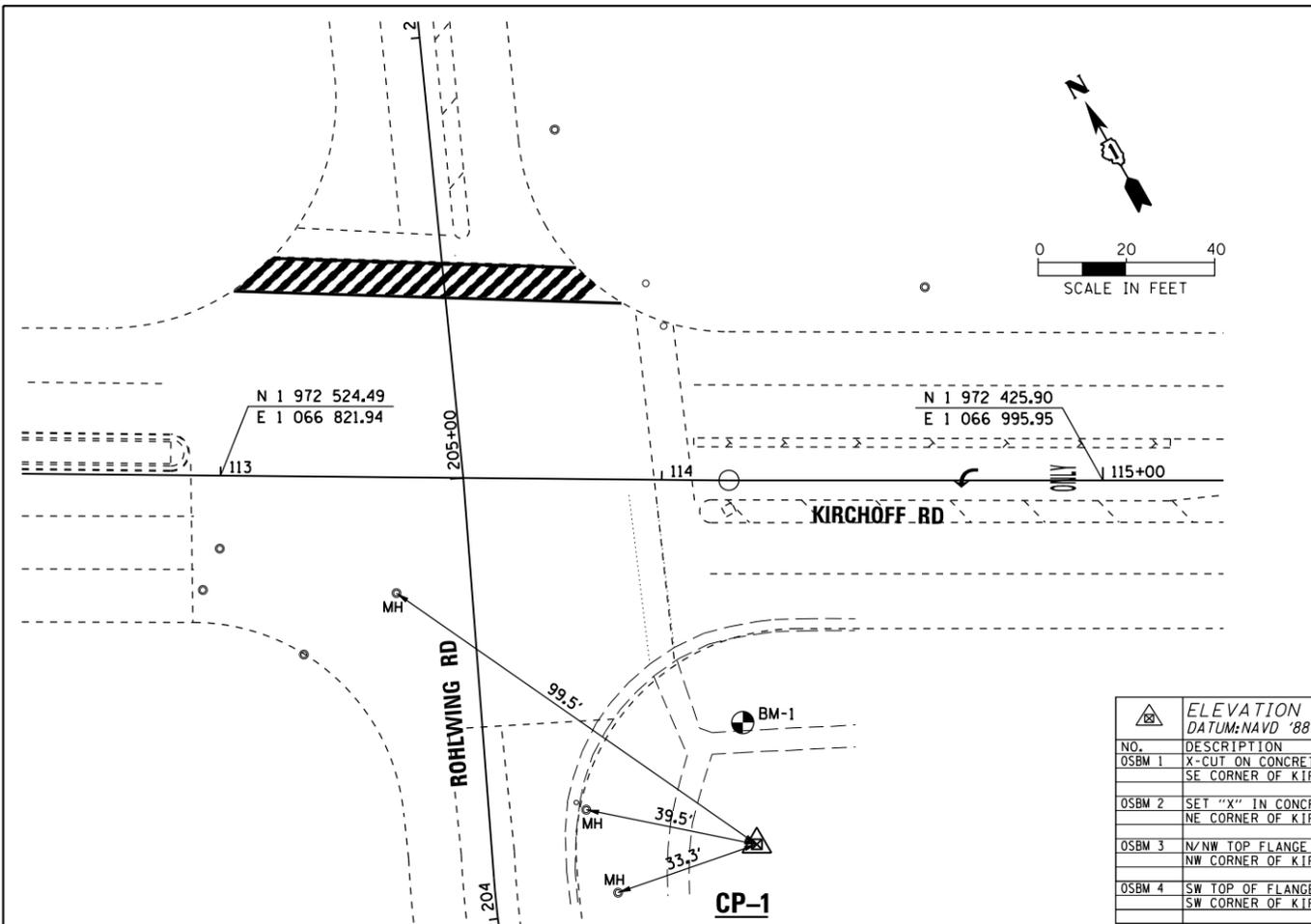
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	7
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

#	RM GIS NUMBER	STATION	O/S	RT/LT	VALVE BOX TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, CLOSED LID	FRAMES AND GRATES, TYPE 24	MANHOLES TO BE RECONSTRUCTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	INLETS TO BE ADJUSTED NEW FRAME AND GRATE (SPECIAL)	IMPROVEMENT COMMENTS
1	CB 7901	104+46	33.75	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
2	MH 7893	105+59	24.20	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
3	CB 7892	105+69	34.70	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
4	MH 7903	106+07	15.40	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
5	CB 7902	106+07	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
6	CB 7904	106+94	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
7	CB 7888	107+02	34.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
8	CB 7887	108+26	34.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
9	CB 7907	108+36	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
10	MH 7908	108+36	23.70	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
11	MH 7909	108+42	18.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
12	CB 7884	109+73	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
13	CB 7883	111+39	34.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
14	CB 7910	111+14	34.10	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
15	MH 7913	111+32	24.30	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
16	MH 7914	112+96	25.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
17	MH 7915	113+00	16.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
18	CB 7917	113+19	40.30	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
19	SAN MH 7916	113+40	26.20	RT								LOCKED LID, MWRD (NO WORK TO BE PERFORMED)
20	WV 7606	114+00	34.80	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
21	CB 7880	115+70	33.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
22	MH 7876	115+63	24.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
23	MH 7877	115+69	16.90	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
24	CB 7875	115+69	34.40	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
25	CB 7870	117+13	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
26	CB 7872	117+44	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
27	WV 7862	117+47	52.70	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
28	MH 7871	117+66	25.00	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
29	SAN MH 7864	117+92	39.10	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
30	WV 7863	118+01	34.40	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
31	CB 7867	118+68	34.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
32	CB 7866	118+59	34.20	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
33	MH 7865	118+62	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
34	MH 8386	118+86	16.70	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
35	CB 8333	119+59	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
36	CB 8380	119+73	34.00	RT						1		REPLACE FRAME AND GRATE
37	MH 8381	119+73	23.70	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
38	INL 8337	120+87	29.30	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
39	MH 8336	120+77	26.70	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
40	MH 8367	120+97	29.40	RT				1				EB OUTER LANE, GOOD CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
41	MH 8368	120+77	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
42	INL 8336	121+07	32.50	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT

#	RM GIS NUMBER	STATION	O/S	RT/LT	VALVE BOX TO BE ADJUSTED	FRAMES AND LIDS, TYPE 1, CLOSED LID	FRAMES AND GRATES, TYPE 24	MANHOLES TO BE RECONSTRUCTED	FRAMES AND LIDS TO BE ADJUSTED	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	INLETS TO BE ADJUSTED NEW FRAME AND GRATE (SPECIAL)	IMPROVEMENT COMMENTS
43	MH 8365	121+32	15.50	RT				1				EB INNER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
44	INL 8364	121+66	31.80	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
45	CB 8360	122+88	31.50	RT			1		1			REPLACE FRAME AND GRATE
46	INL 8358	123+39	31.40	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
47	MH 8359	123+43	23.90	RT				1				EB OUTER LANE, FAIR CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
48	INL 8321	123+42	31.50	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
49	INL 8327	125+06	40.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
50	INL 8325	124+89	32.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
51	MH 8326	124+89	24.20	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
52	CB 9018	124+92	23.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
53	CB 8339	124+96	32.50	RT		1				1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING, REPLACE F&G
54	WV 8328	125+05	35.50	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
55	INL 8340	125+08	42.90	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
56	WV 8329	125+16	50.10	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
57	SAN MH 8330	125+62	39.40	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
58	SAN MH 8345	125+65	48.10	RT					1			ADJUST FOR ADA SIDEWALK RAMP
59	INL 8357	127+56	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
60	MH 8349	127+57	23.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
61	INL 8348	127+54	31.00	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
62	INL 53002	129+51	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
63	MH 52007	129+63	24.20	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
64	INL 52008	129+84	31.00	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
65	INL 52093	130+81	31.40	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
66	INL 52012	131+70	31.00	RT			1		1			REPLACE FRAME AND GRATE
67	INL 52092	131+73	31.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
68	INL 52091	132+27	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
69	MH 52014	132+27	23.90	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
70	INL 52015	132+27	30.40	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
71	INL 52089	133+76	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
72	MH 52088	133+76	24.70	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
73	INL 52020	133+76	31.00	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
74	INL 52087	134+37	31.00	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
75	INL 52086	135+13	30.40	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
76	INL 52084	135+88	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
77	INL 52081	136+73	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
78	INL 52078	137+71	30.90	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
79	INL 52077	138+67	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
80	MH 52076	138+98	30.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
81	INL 52075	139+22	30.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
82	INL 52031	139+23	31.60	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
83	INL 52074	139+88	30.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
84	INL 52073	140+38	30.20	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT

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85	MH 52072	140+84	29.90	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
86	INL 52071	141+04	30.00	LT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
87	INL 52070	141+43	30.20	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
88	INL 52069	142+06	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
89	INL 52068	142+57	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
90	INL 52042	142+82	31.30	RT					1*			DEPRESSED CURB, *IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
91	INL 52067	143+12	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
92	INL 52066	143+71	30.80	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
93	MH 52065	143+98	32.3.	LT								NO IMPROVEMENTS NEEDED
94	INL 52064	144+07	29.90	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
95	INL 52045	144+35	31.00	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
96	INL 52063	145+10	29.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
97	INL 52062	146+10	30.00	LT							1	EJIW 5080 + M1 GRATE
98	INL 52049	146+43	31.00	RT							1	EJIW 5080 + M1 GRATE
99	INL 52060	147+11	30.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
100	INL 52050	147+25	31.00	RT							1	EJIW 5080 + M1 GRATE
101	INL 52059	148+01	30.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
102	INL 52052	148+42	31.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
103	WV 52054	149+10	35.00	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
104	WV 52055	149+21	49.60	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
105	INL 52204	149+81	46.90	RT							1	EJIW 5080 + M1 GRATE
106	SAN MH 52142	149+78	30.00	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
107	SAN MH 52203	149+79	32.10	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
108	WV 52202	149+88	38.80	RT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
109	WV 52141	149+90	38.10	LT	1	1						LID TO BE STAMPED "ROLLING MEADOWS - WATER"
110	CB 52200	150+34	30.80	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
111	INL 52143	150+16	30.80	LT								NO IMPROVEMENTS NEEDED
112	INL 53004	151+61	31.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
113	CB 52198	151+59	30.90	RT							1	EJIW 5080 + M1 GRATE
114	SAN MH 52142	152+96	28.10	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
115	INL 52148	153+25	27.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
116	CB 52196	153+25	27.30	RT								NO IMPROVEMENTS NEEDED
117	INL 52151	154+88	23.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
118	CB 52195	154+88	24.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
119	INL 52154	156+54	23.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
120	CB 52192	156+53	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
121	INL 52158	158+19	24.30	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
122	INL 52189	158+21	24.30	RT							1	EJIW 5080 + M1 GRATE
123	INL 52159	159+83	23.40	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
124	INL 52161	160+64	23.40	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
125	INL 52162	161+54	26.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
126	INL 52184	159+82	23.70	RT								NO IMPROVEMENTS NEEDED

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127	INL 52183	160+64	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
128	INL 52181	161+55	26.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
129	INL 52165	164+03	29.70	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
130	MH 52180	163+98	24.00	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
131	INL 52178	164+08	29.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
132	SAN MH 52166	164+83	27.00	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
133	INL 52167	164+96	29.70	LT								NO IMPROVEMENTS NEEDED
134	INL 52177	164+92	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
135	INL 52170	165+32	30.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
136	INL 52176	165+23	29.00	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
137	MH 51267	166+21	24.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
138	INL 51266	166+99	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
139	SAN MH 52493	167+03	28.30	LT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
140	INL 52494	167+17	29.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
141	INL 52495	167+87	29.80	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
142	INL 51262	167+97	29.90	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
143	INL 52498	168+88	29.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
144	MH 51261	168+70	25.60	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
145	INL 51260	168+90	29.30	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
146	INL 51226	169+91	26.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
147	INL 51227	170+56	25.00	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
148	INL 51259	169+92	27.20	RT							1	EJIW 5080 + M1 GRATE
149	INL 51258	170+42	25.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
150	MH 52520	170+54	23.00	RT				1				EB OUTER LANE, GOOD CONDITION, RECON WITH OFFSET CONETOP TO MOVE F&G OUT OF WHEELPATH
151	INL 51257	170+66	24.50	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
152	INL 51256	170+95	23.80	RT							1	EJIW 5080 + M1 GRATE
153	INL 51228	172+26	23.10	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
154	INL 51253	172+25	23.70	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
155	INL 51229	173+97	24.00	LT								NO IMPROVEMENTS NEEDED
156	INL 51251	173+94	24.80	RT								NO IMPROVEMENTS NEEDED
157	INL 51231	175+79	24.20	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
158	INL 51248	175+79	24.20	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
159	INL 51232	177+55	24.50	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
160	INL 51246	177+58	24.10	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
161	INL 52519	178+53	24.40	RT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
162	INL 51233	179+24	24.60	LT							1	EJIW 7010Z + M3 SINUSOIDAL GRATE
163	SAN MH 51243	179+26	28.20	RT						1		LOWER BEFORE MILLING, RAISE TO FINISH GRADE BEFORE HMA SC PAVING
164	INL 51242	179+54	24.10	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
165	INL 52509	181+14	31.00	LT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
166	INL 51241	180+67	29.30	RT					1*			*IF NECESSARY FOR CONCRETE CURB AND GUTTER REPLACEMENT
TOTALS					9	10	3	10	16	25	50	

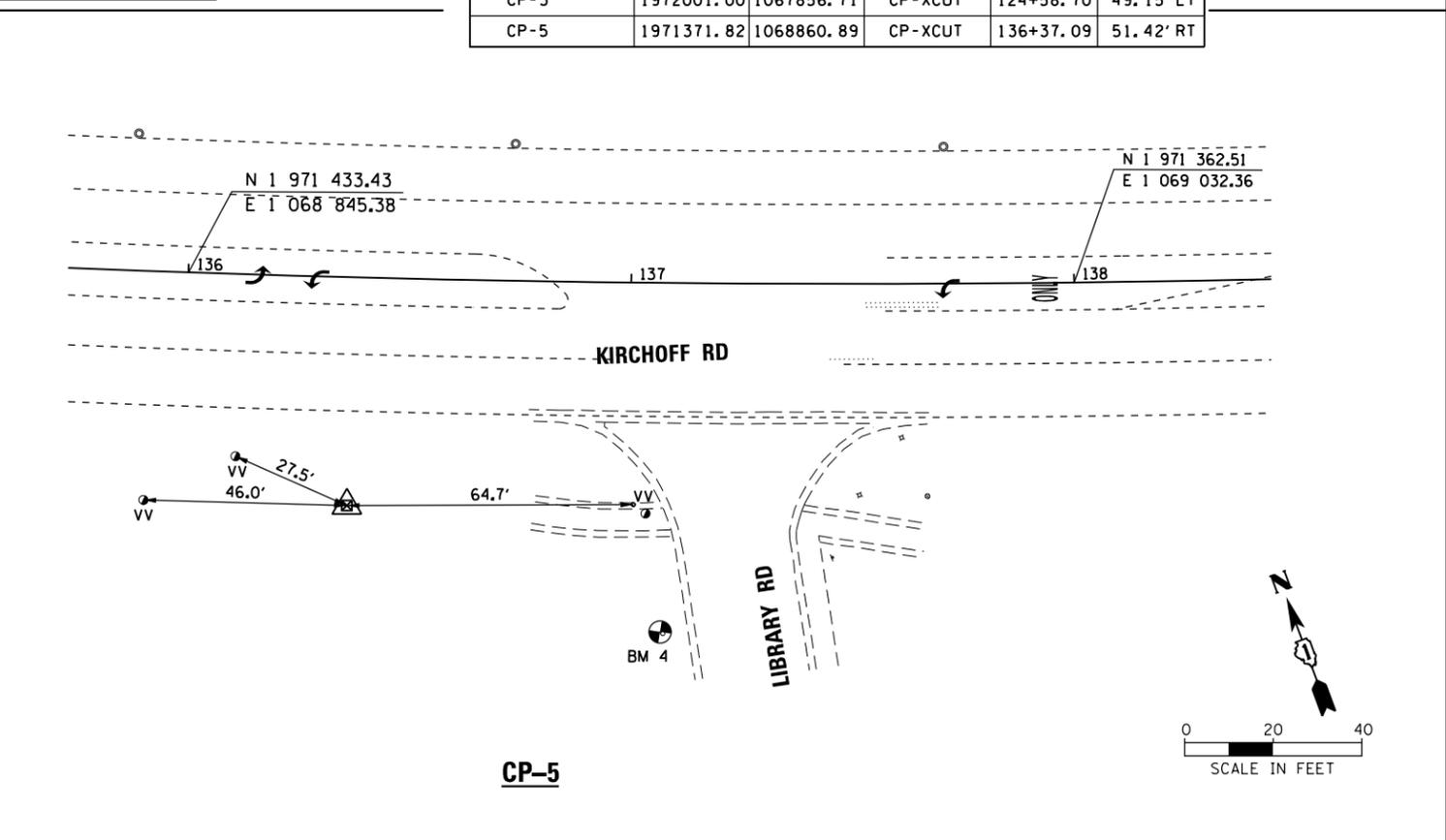
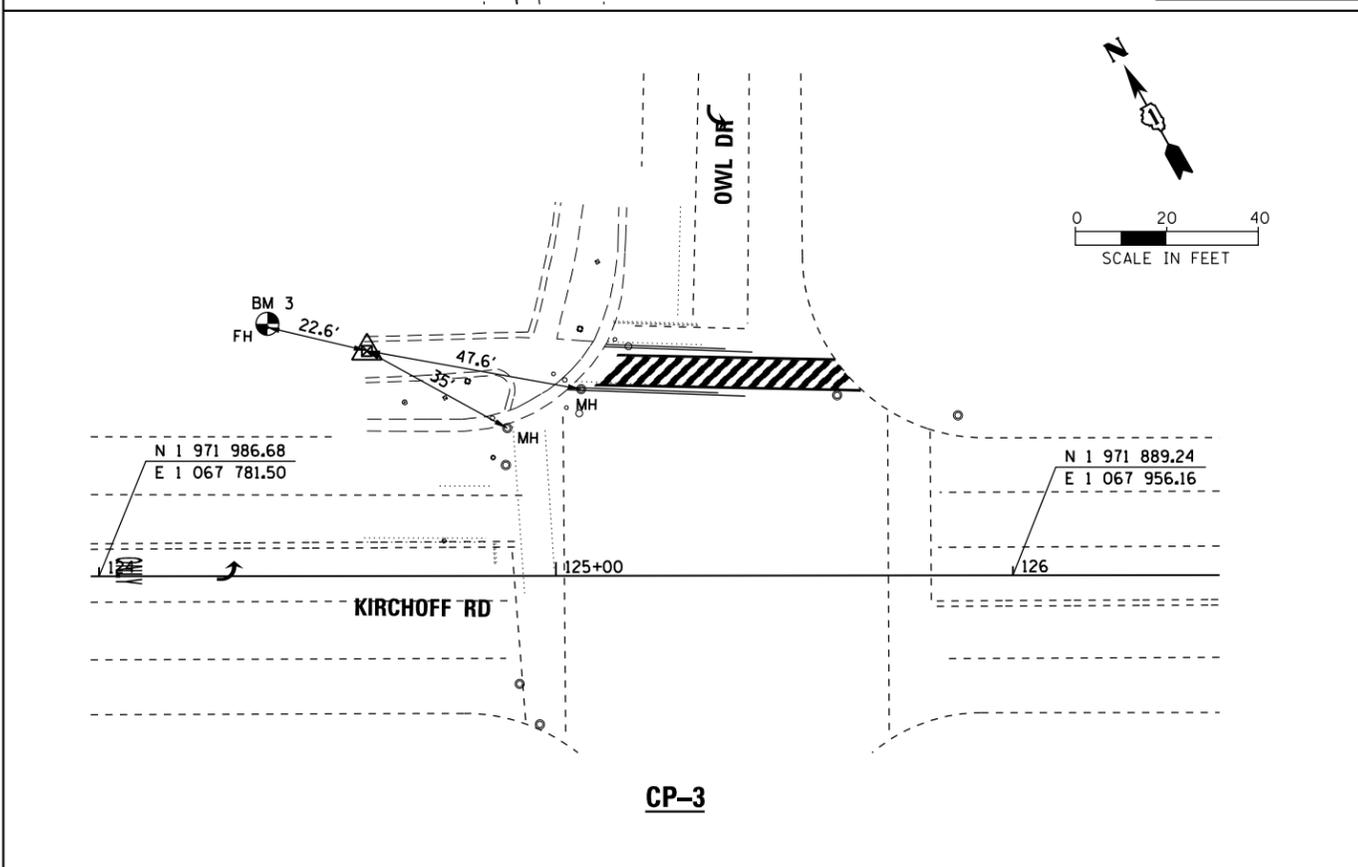


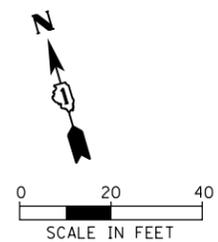
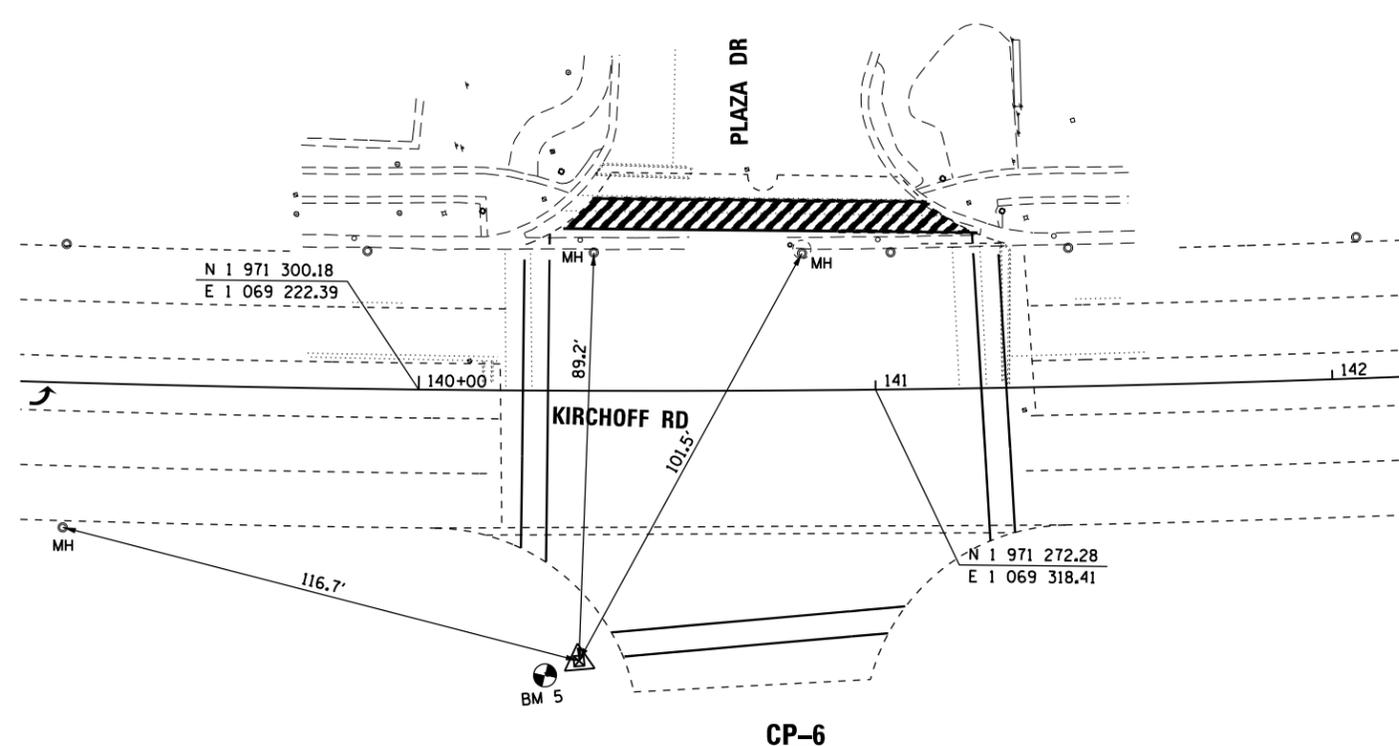
ELEVATION BENCHMARKS		
DATUM: NAVD '88 ROLLING MEADOWS		
NO.	DESCRIPTION	ELEV.
OSBM 1	X-CUT ON CONCRETE BASE OF CONTROL BOX SE CORNER OF KIRCHHOFF & ROHLWING RD.	725.43
OSBM 2	SET "X" IN CONCRETE BASE OF LIGHT STD. NE CORNER OF KIRCHHOFF & FLICKER LANE	718.53
OSBM 3	N/NW TOP FLANGE BOLT ON NW CORNER OF KIRCHHOFF & OWL DR.	719.14
OSBM 4	SW TOP OF FLANGE TAG BOLT ON SW CORNER OF KIRCHHOFF & LIBRARY RD.	714.12

CP-2

HORIZONTAL CONTROL POINTS

CONTROL POINTS	NORTHING	EASTING	DESCRIPTION	STATION	OFFSET
CP-1	1972392.36	1066887.32	CP-XCUT	114+21.58	82.32' RT
CP-2	1972298.83	1067311.88	CP-XCUT	118+37.75	43.38' LT
CP-3	1972001.00	1067856.71	CP-XCUT	124+58.70	49.15' LT
CP-5	1971371.82	1068860.89	CP-XCUT	136+37.09	51.42' RT

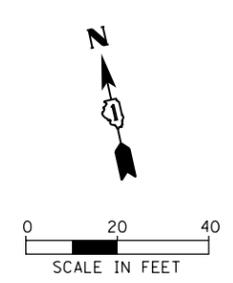
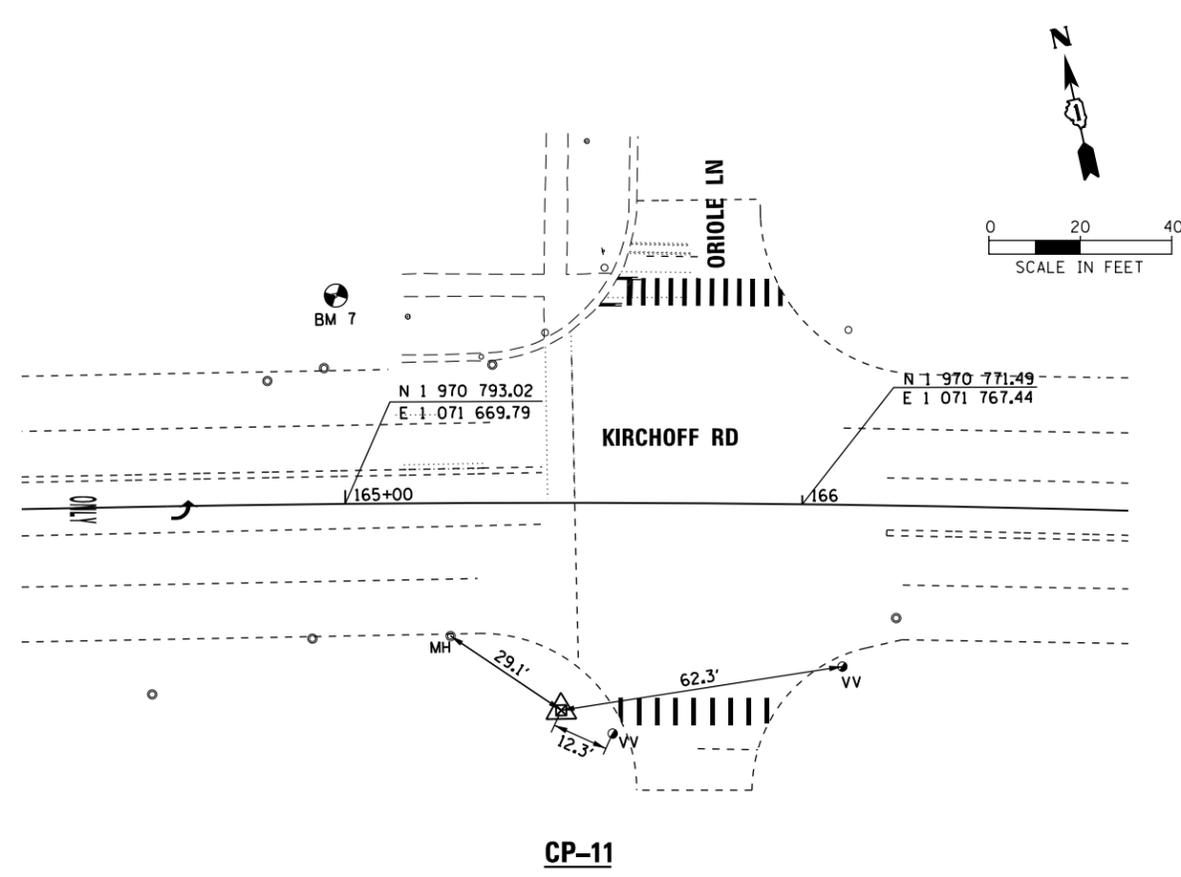
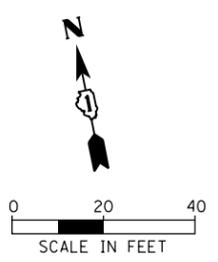
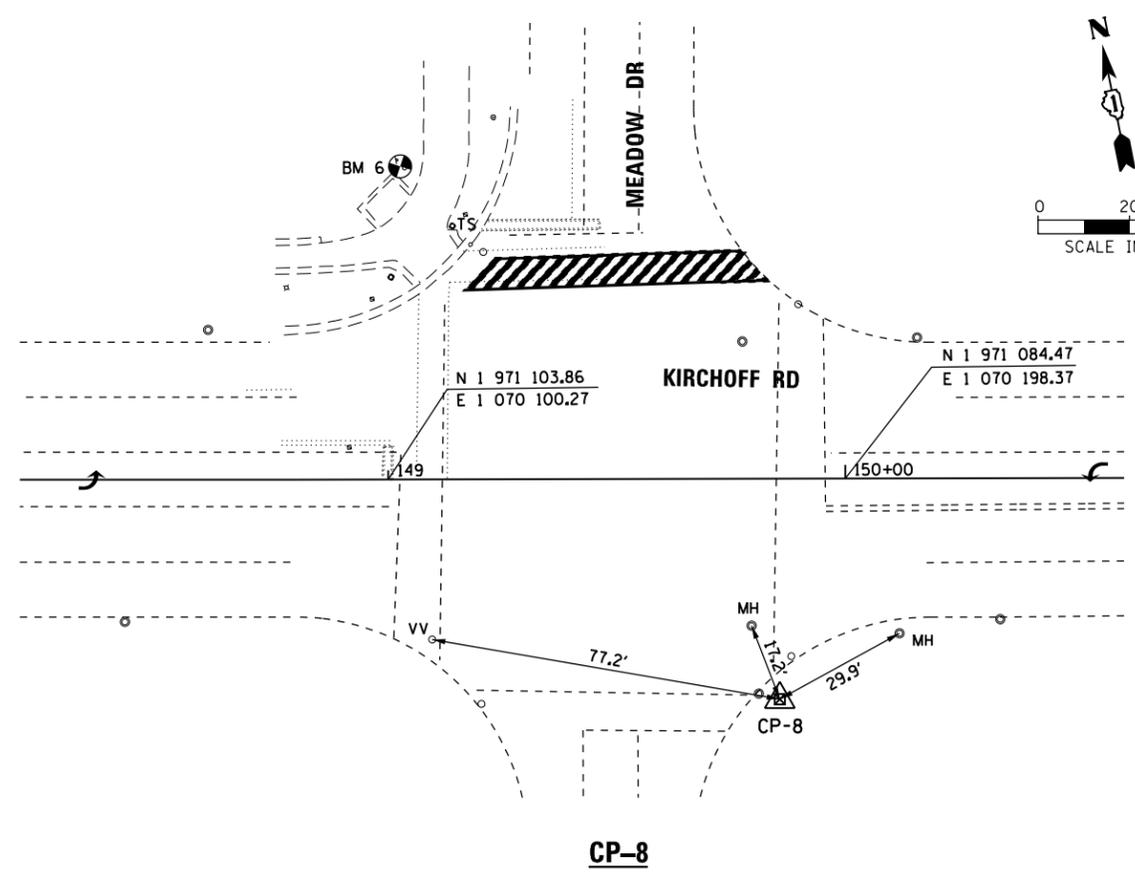




ELEVATION BENCHMARKS DATUM: NAVD '88 ROLLING MEADOWS		
NO.	DESCRIPTION	ELEV.
OSBM 5	TOP SOUTH FLANGE TAG BOLT SW CORNER OF KIRCHOFF & PLAZA DR.	715.48
OSBM 6	TOP SW FLANGE BOLT ON FIRE HYDRANT NW CORNER OF KIRCHOFF & MEADOW DR.	717.36
OSBM 7	N/NW TOP FLANGE BOLT ON HYDRANT NW CORNER OF KIRCHOFF & ORIOLE LANE	722.63

HORIZONTAL CONTROL POINTS

CONTROL POINTS	NORTHING	EASTING	DESCRIPTION	STATION	OFFSET
CP-6	1971233.53	1069239.54	CP-XCUT	140+35.27	58.96' RT
CP-8	1971040.02	1070174.89	CP-XCUT	149+85.58	48.15' RT
CP-11	1970738.88	1071706.27	CP-XCUT	165+47.27	45.30' RT



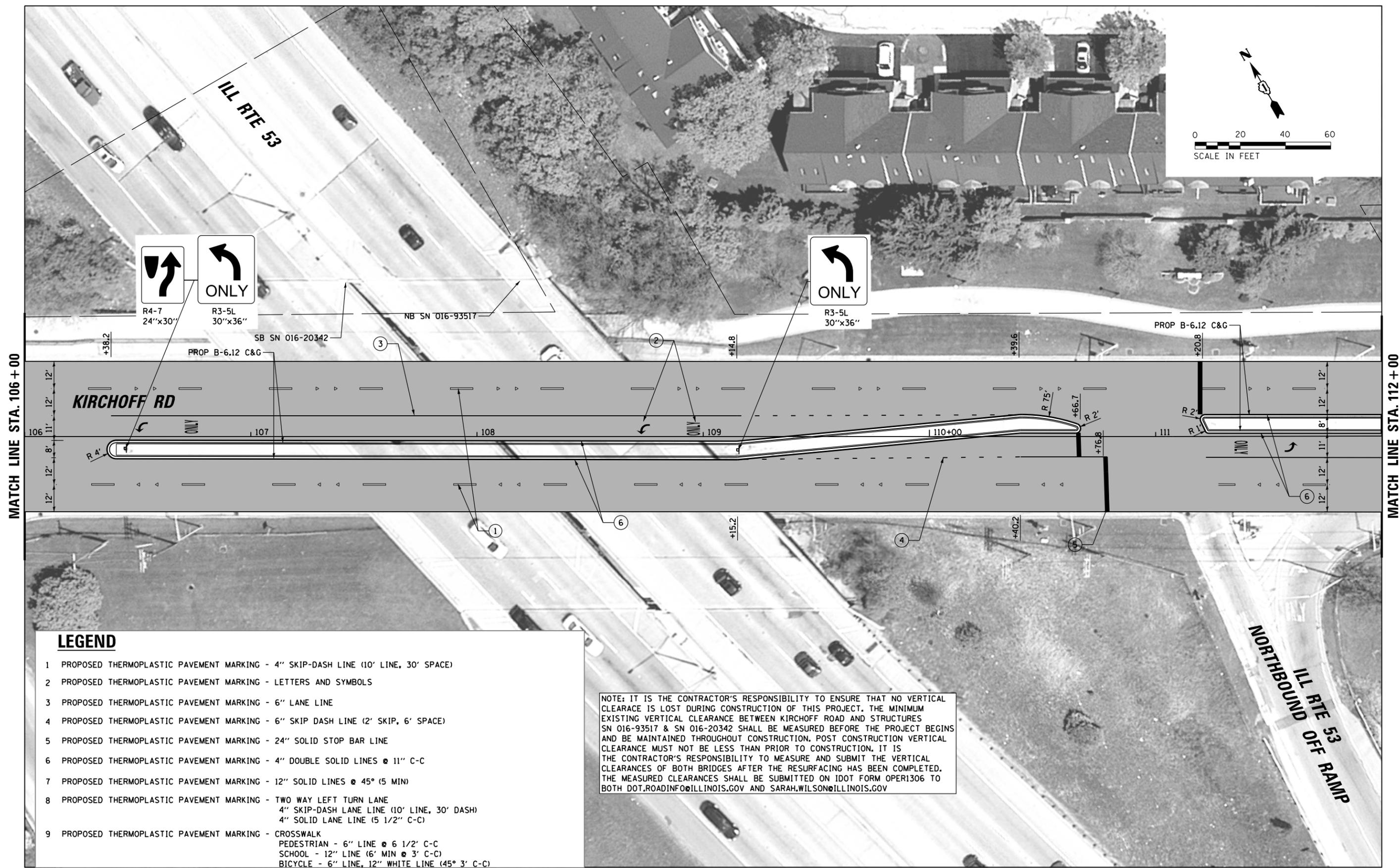
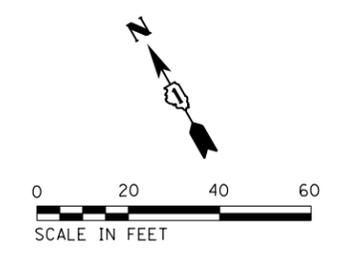
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	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARK

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	13
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 106 + 00

MATCH LINE STA. 112 + 00

LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

NOTE: IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO VERTICAL CLEARANCE IS LOST DURING CONSTRUCTION OF THIS PROJECT. THE MINIMUM EXISTING VERTICAL CLEARANCE BETWEEN KIRCHOFF ROAD AND STRUCTURES SN 016-93517 & SN 016-20342 SHALL BE MEASURED BEFORE THE PROJECT BEGINS AND BE MAINTAINED THROUGHOUT CONSTRUCTION. POST CONSTRUCTION VERTICAL CLEARANCE MUST NOT BE LESS THAN PRIOR TO CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MEASURE AND SUBMIT THE VERTICAL CLEARANCES OF BOTH BRIDGES AFTER THE RESURFACING HAS BEEN COMPLETED. THE MEASURED CLEARANCES SHALL BE SUBMITTED ON IDOT FORM OPER1306 TO BOTH DOT.ROADINFO@ILLINOIS.GOV AND SARAH.WILSON@ILLINOIS.GOV

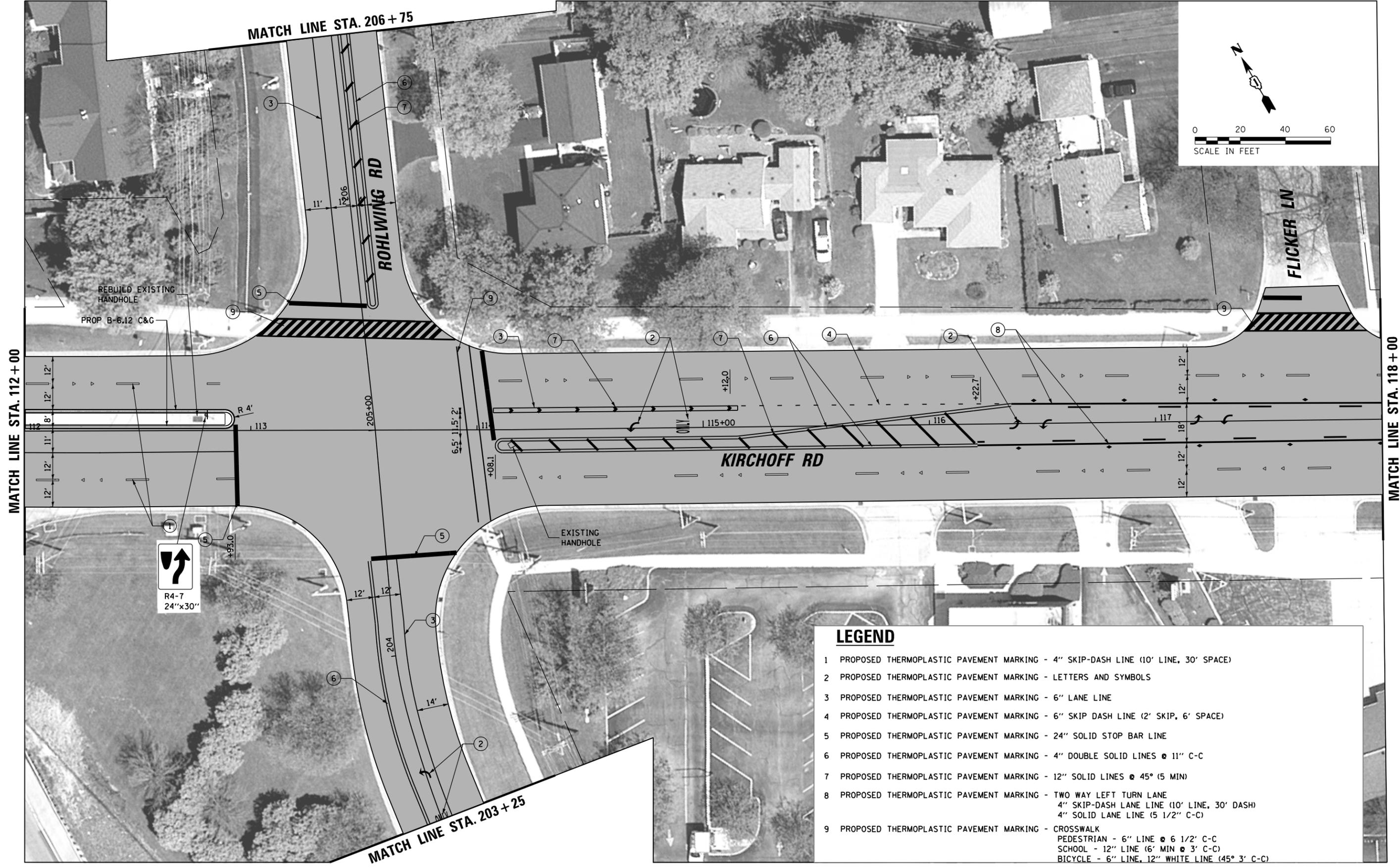
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	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD
PROPOSED ROADWAY PLAN**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	15
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

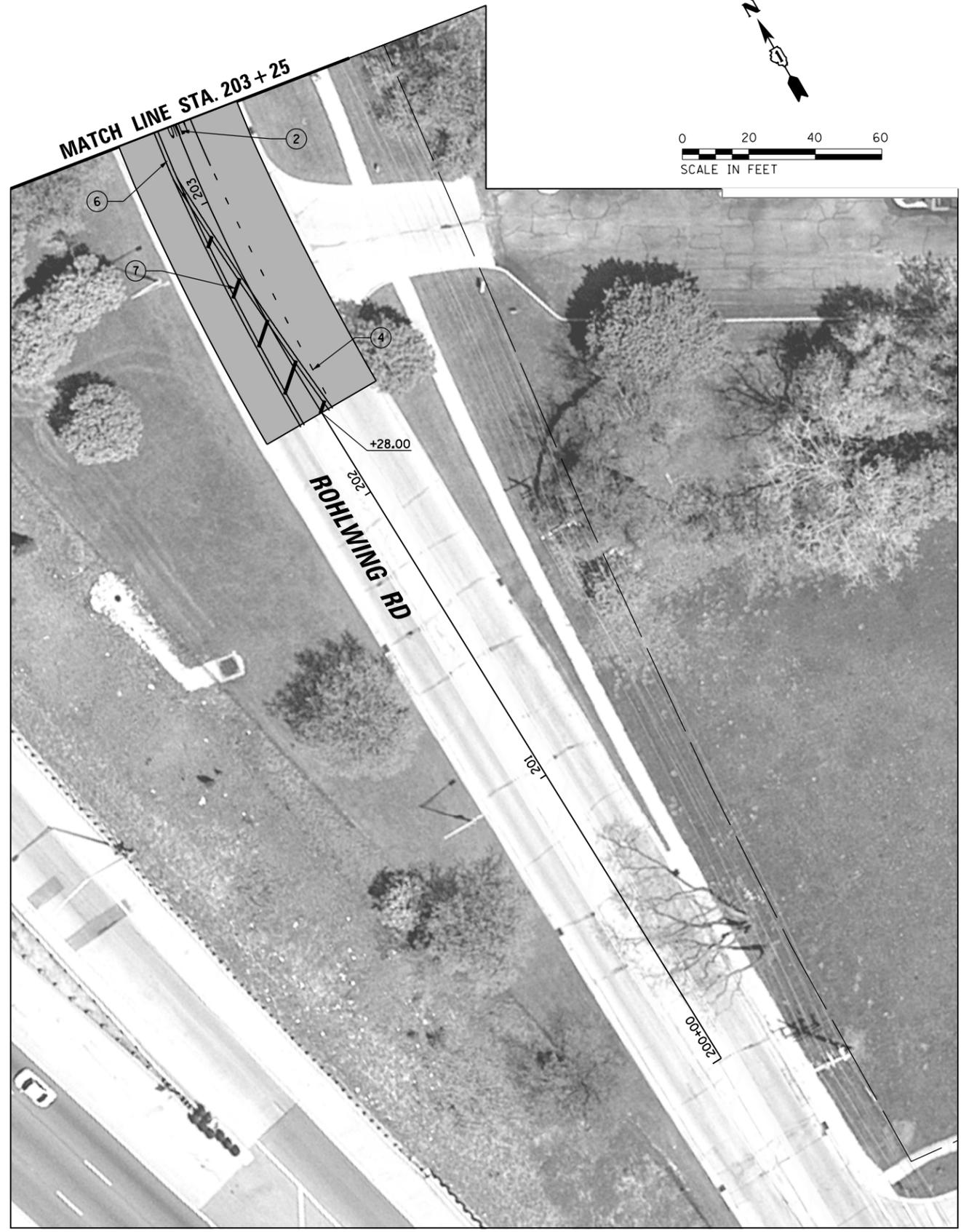
- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	16
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

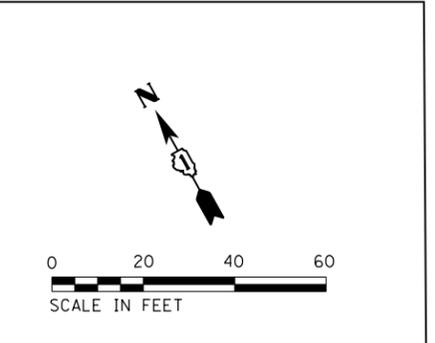


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	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROWLING ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	17
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

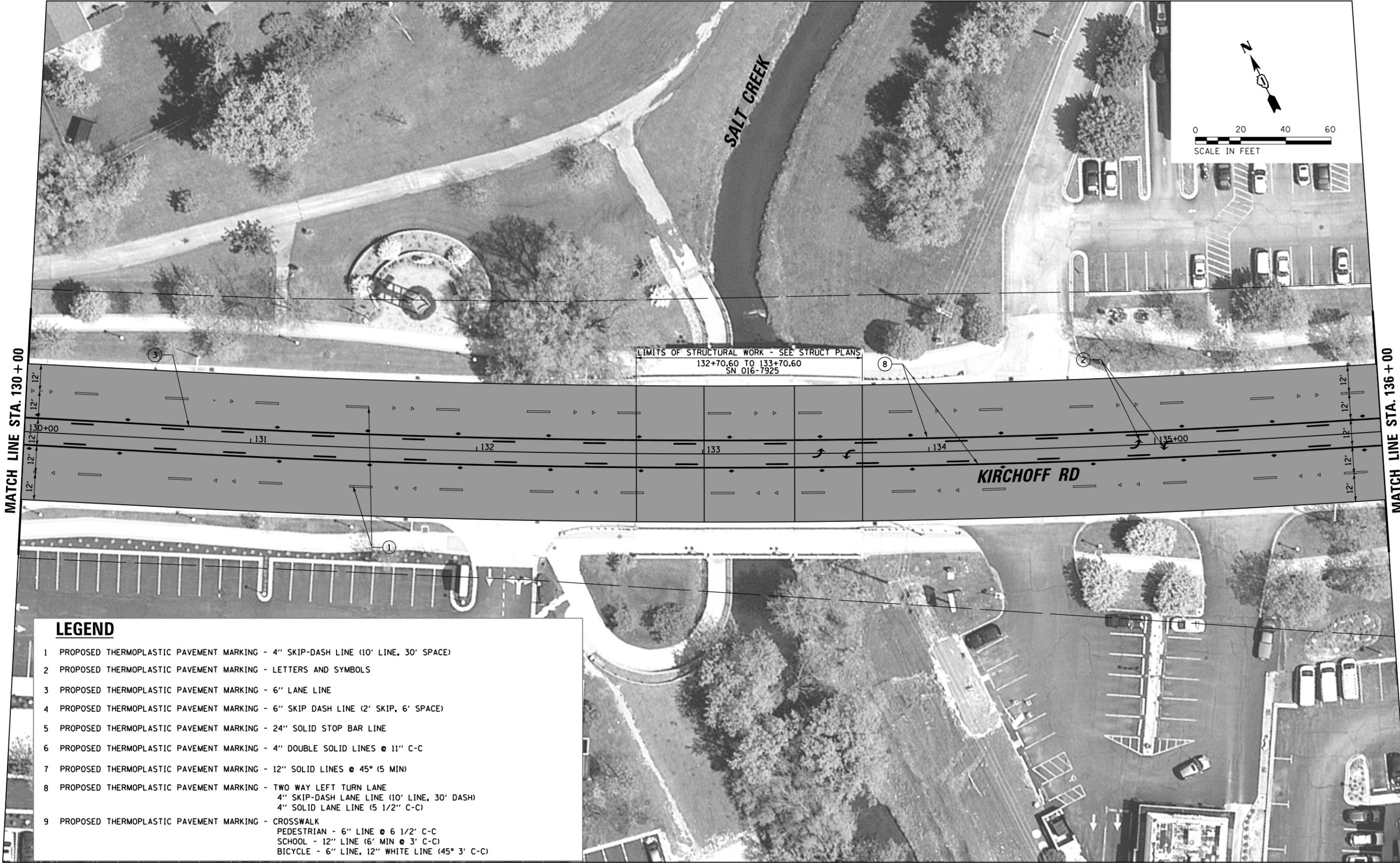
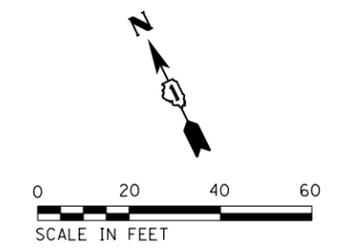
- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
4" SKIP-DASH LANE LINE (10' LINE, 30' DASH)
4" SOLID LANE LINE (5 1/2" C-C)
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
PEDESTRIAN - 6" LINE @ 6 1/2' C-C
SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	19
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

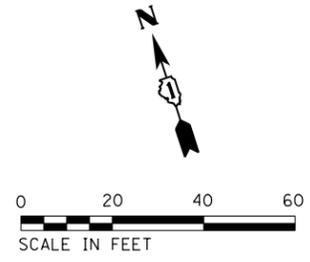
- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
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- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
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SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	20
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

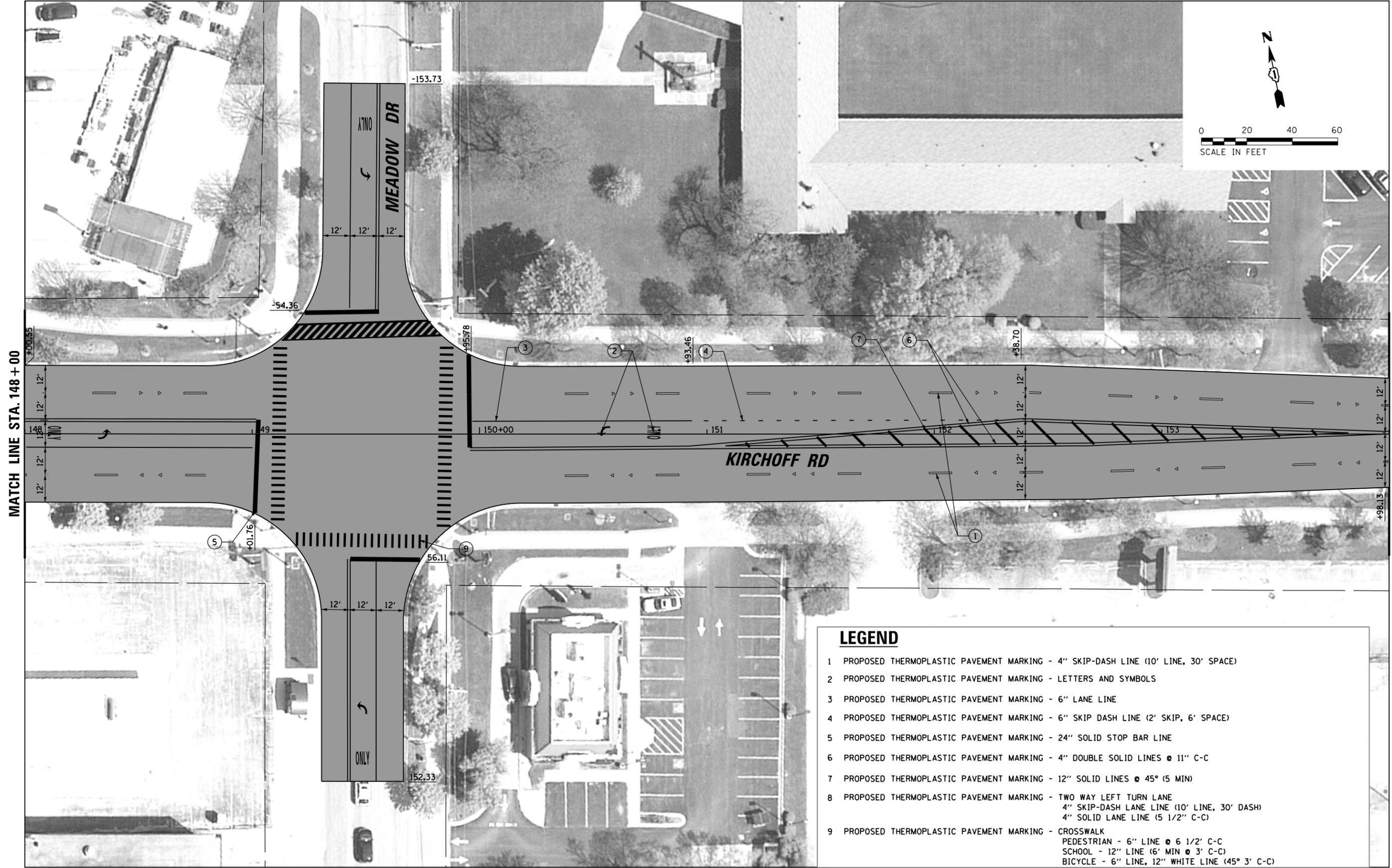
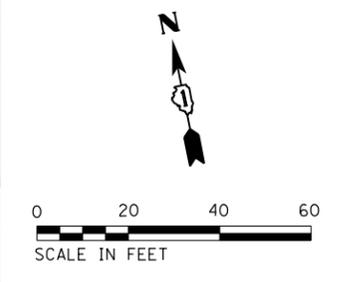
- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
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- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	21
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

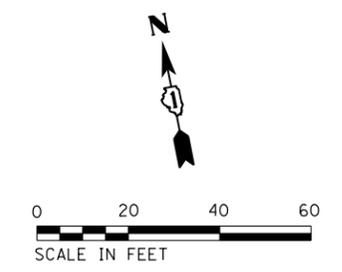
- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
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- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 12" SOLID LINES @ 45° (5 MIN)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING - TWO WAY LEFT TURN LANE
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- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING - CROSSWALK
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BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

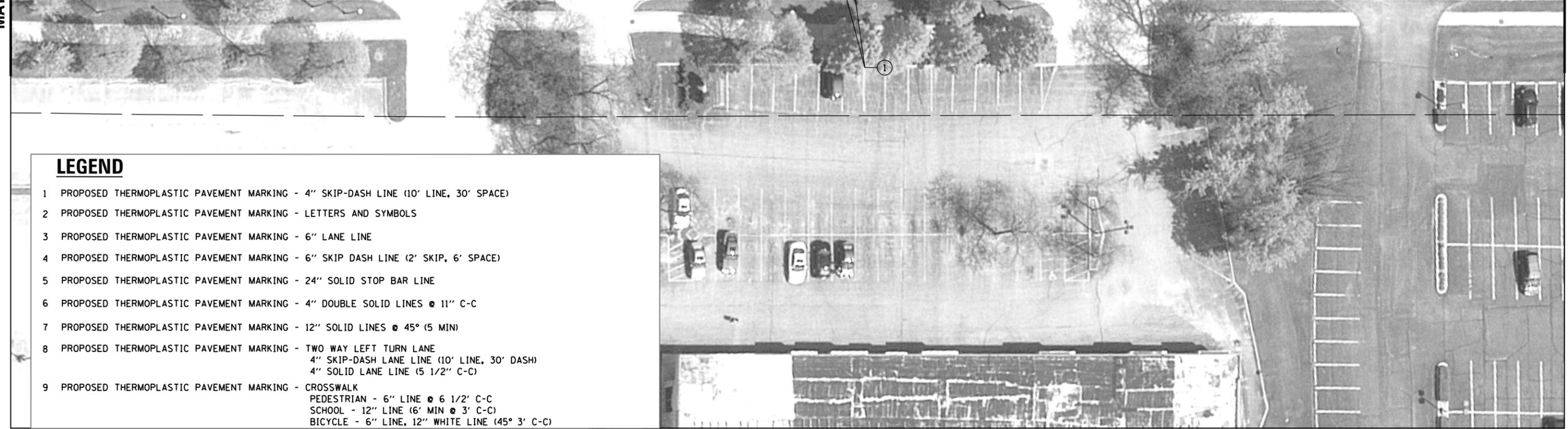
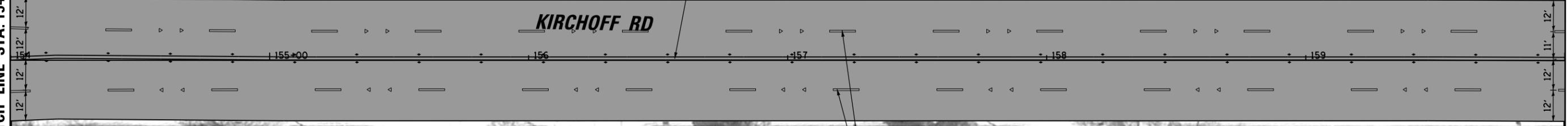
KIRCHOFF ROAD PROPOSED ROADWAY PLAN			
SCALE: 40'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	23
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 154 + 00

MATCH LINE STA. 160 + 00



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
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BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD
PROPOSED ROADWAY PLAN**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	24
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
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SCHOOL - 12" LINE (6' MIN @ 3' C-C)
BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD
PROPOSED ROADWAY PLAN**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	26
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" SKIP-DASH LINE (10' LINE, 30' SPACE)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" LANE LINE
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 6" SKIP DASH LINE (2' SKIP, 6' SPACE)
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 24" SOLID STOP BAR LINE
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING - 4" DOUBLE SOLID LINES @ 11" C-C
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 PEDESTRIAN - 6" LINE @ 6 1/2' C-C
 SCHOOL - 12" LINE (6' MIN @ 3' C-C)
 BICYCLE - 6" LINE, 12" WHITE LINE (45° 3' C-C)

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD
PROPOSED ROADWAY PLAN**

SCALE: 40' SHEET OF SHEETS STA. TO STA.

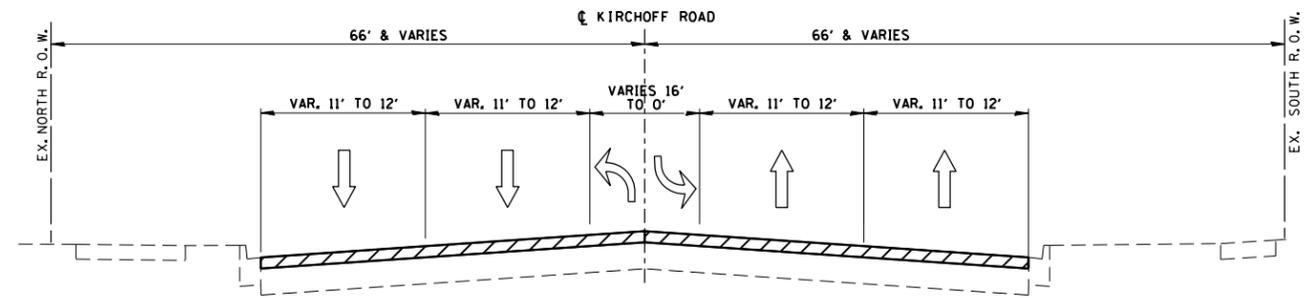
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	27
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

SUGGESTED CONSTRUCTION SEQUENCE

- STAGE 1 - RAISED REFLECTIVE PAVEMENT MARKER REMOVAL (COMPLETE)
 FRAME AND LID ADJUSTMENT (BD-08) PRIOR TO MILLING (COMPLETE)
 HMA SURFACE REMOVAL (OMIT BRIDGE REHABILITATION LIMITS)
 TEMPORARY PAVEMENT MARKINGS - PAINT (COMPLETE)
- STAGE 2 - COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT
 STRUCTURES TO BE RECONSTRUCTED (EXTERIOR LANES EB & WB)
 CLASS B PATCHING (EXTERIOR LANES EB & WB)
 SIDEWALK REMOVAL & REPLACEMENT
 DRIVEWAY AND SIDEROAD ENTRANCE PAVEMENT REMOVAL & REPLACEMENT
 BRIDGE REHABILITATION (EXTERIOR LANES EB & WB)
- STAGE 3 - STRUCTURES TO BE RECONSTRUCTED (INTERIOR LANES EB & WB)
 CLASS B PATCHING (INTERIOR LANES EB & WB)
 BRIDGE REHABILITATION (INTERIOR LANES EB & WB, AND CENTERLINE LANE)
 RECONSTRUCT MEDIANS (STA. 103+45 TO STA. 112+96)
 CLASS B PATCHES (CENTERLINE LANE - LEFT TURN, TWO-WAY LEFT TURN, & STRIPED MEDIAN)
 STRUCTURES TO BE RECONSTRUCTED (CENTERLINE LANE - LEFT TURN, TWO-WAY LEFT TURN, & STRIPED MEDIAN)
- NOTE: DURING STAGE 3, A LEFT TURN LANE MUST BE MAINTAINED DURING CONSTRUCTION AT ALL TIMES AT SIGNALIZED INTERSECTIONS PER TC - 14.
- STAGE 4 - MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS
 HMA BINDER COURSE AND LEVELING BINDER PAVING
 DETECTOR LOOP REPLACEMENT
 FRAME AND LID ADJUSTMENT (BD-08) PRIOR TO HMA SURFACE COURSE
 PARKWAY RESTORATION - SODDING (COMPLETE)
 HMA SURFACE COURSE PAVING
 THERMOPLASTIC PAVEMENT MARKINGS (COMPLETE)
 RAISED REFLECTIVE PAVEMENT MARKERS (COMPLETE)
- NOTE: STANDARDS LISTED BELOW SHOULD BE USED AS APPLICABLE AT EACH STAGE OF MAINTENANCE OF TRAFFIC PLAN

REQUIRED DAY CLOSURE STANDARDS

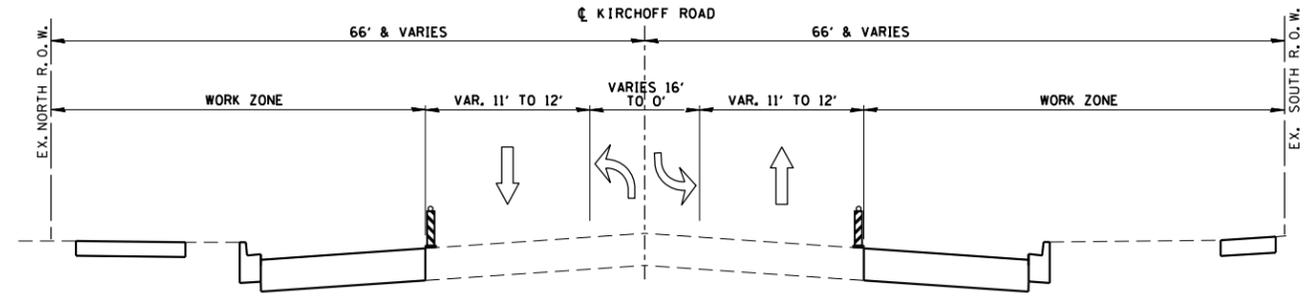
- 701006-05 OFF ROAD OPERATION 2 LANE 2 WAY 15 FEET TO EDGE OF PAVEMENT
- 701101-05 OFF ROAD OPERATION MULTI LANE LESS THAN 15 FEET TO EDGE OF PAVEMENT
- 701311-03 LANE CLOSURE 2 LANE 2 WAY MOVING OPERATION DAYTIME ONLY
- 701427-05 LANE CLOSURE, MULTI-LANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 40 MPH
- 701501-06 URBAN LANE CLOSURE, 2 LANE 2 WAY UNDIVIDED
- 701502-08 URBAN LANE CLOSURE, 2 LANE 2 WAY WITH BIDIRECT LEFT TURN LANE
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSAL MEDIAN
- 701602-09 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-06 SIDEWALK CORNER OR CROSSWALK CLOSURE



STAGE 1 - SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

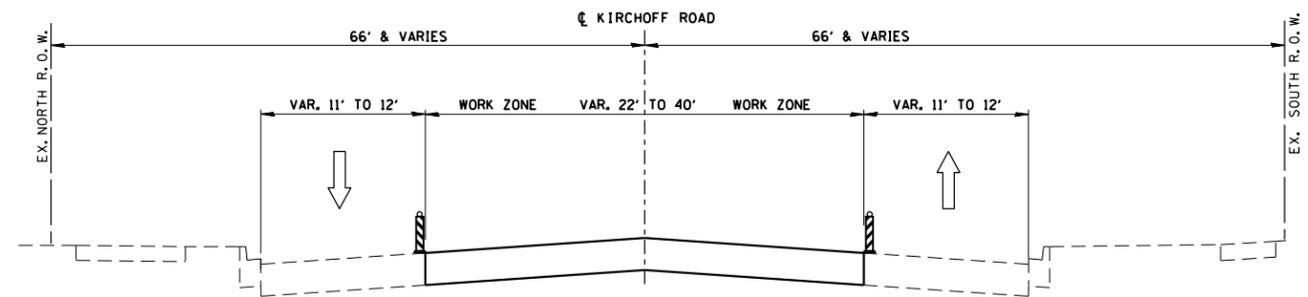
KIRCHHOFF ROAD
 STA. 103+45 TO STA. 181+54



STAGE 2 - SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

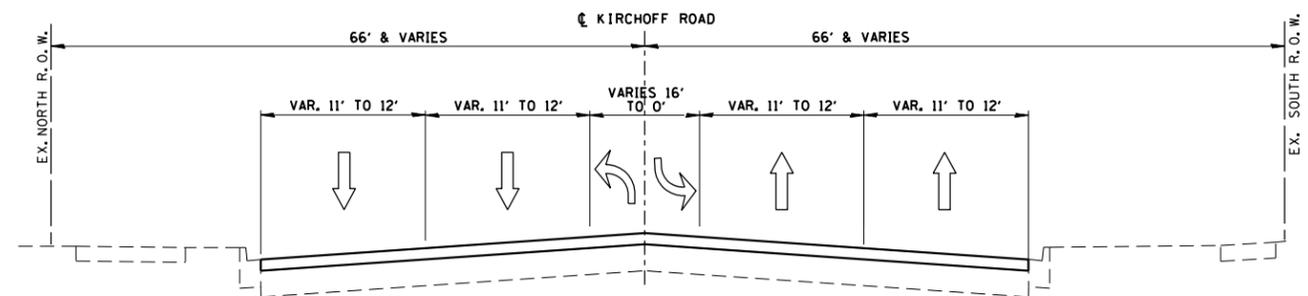
KIRCHHOFF ROAD
 STA. 103+45 TO STA. 181+54



STAGE 3 - SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

KIRCHHOFF ROAD
 STA. 103+45 TO STA. 181+54



STAGE 4 - SUGGESTED MAINTENANCE OF TRAFFIC

TYPICAL SECTION

KIRCHHOFF ROAD
 STA. 103+45 TO STA. 181+54

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N:\ROLLINGMEADOWS\980361.BR\98100\981001.SHT		DRAWN - PM	REVISED -
Default	PLOT SCALE = 60'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUGGESTED MOT
 KIRCHHOFF ROAD

SCALE: 60' SHEET OF SHEETS STA. TO STA.

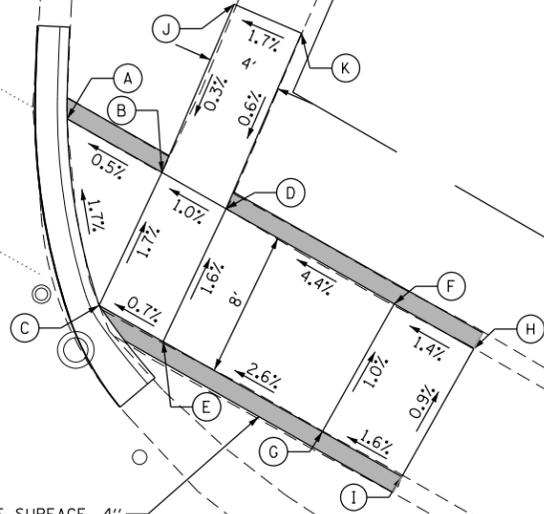
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	29
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

FLICKER LN

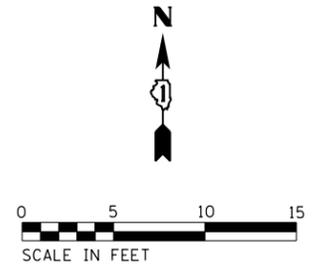
KIRCHOFF RD

1118

STAMPED COLORED CONCRETE SURFACE, 4"



NE FLICKER LN & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	117+85.44	49.90' LT	718.15
B	117+91.40	49.87' LT	718.18
C	117+91.94	41.87' LT	718.32
D	117+95.42	49.86' LT	718.22
E	117+95.96	41.86' LT	718.35
F	118+05.99	49.82' LT	718.69
G	118+05.96	41.82' LT	718.61
H	118+10.99	49.80' LT	718.76
I	118+10.96	41.80' LT	718.69
J	117+90.37	59.82' LT	718.21
K	117+94.35	60.23' LT	718.28



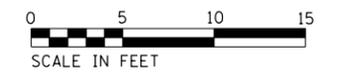
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Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD /FLICKER LANE
PROPOSED ADA PLAN**

SCALE: 10' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	31
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



OWL DR

KIRCHOFF RD

125+00

STAMPED COLORED CONCRETE SURFACE, 4"

NW OWL DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	124+90.05	36.22' LT	715.85
B	124+97.07	39.70' LT	715.69
C	125+02.78	44.31' LT	715.65
D	125+08.08	51.15' LT	715.80
E	124+92.13	44.80' LT	716.12
F	124+98.23	44.52' LT	716.01
G	124+99.93	51.53' LT	716.10
H	124+93.83	51.81' LT	716.22
I	124+76.69	44.44' LT	716.85
J	124+76.99	51.65' LT	716.93
K	124+81.68	44.76' LT	716.77
L	124+81.99	51.78' LT	716.85
M	124+97.43	62.42' LT	716.33
N	125+02.29	61.24' LT	716.25
O	124+98.61	67.28' LT	716.41
P	125+03.46	66.10' LT	716.29

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

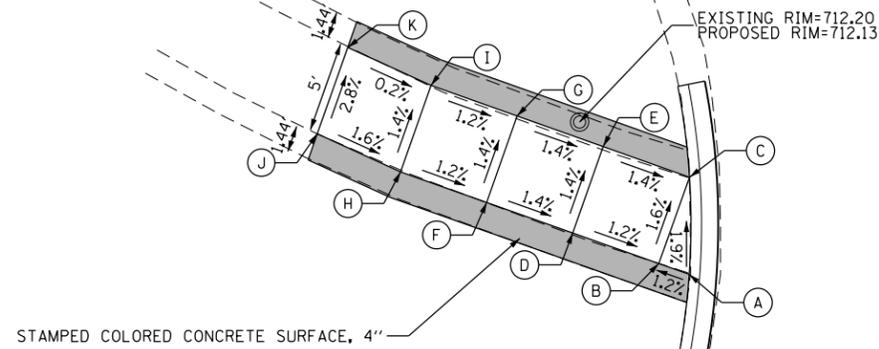
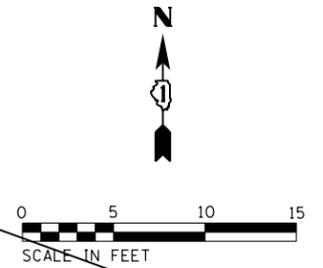
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD /OWL DRIVE
PROPOSED ADA PLAN**

SCALE: 10' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	32
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

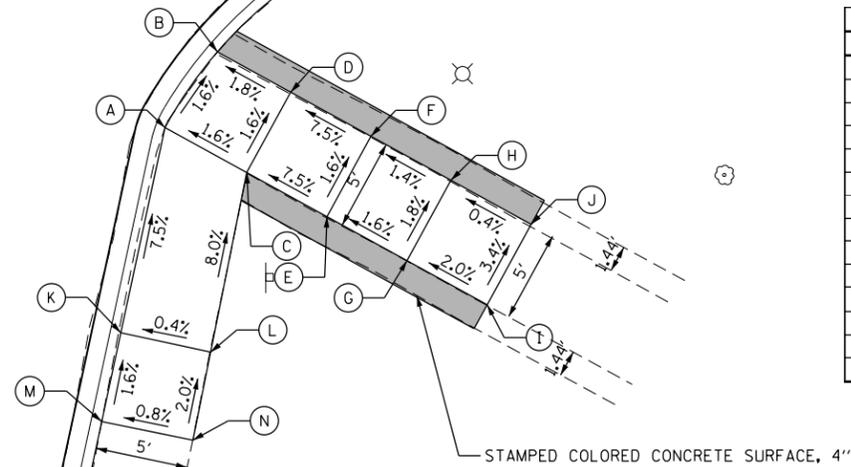
KIRCHOFF RD



STAMPED COLORED CONCRETE SURFACE, 4"

SW LIBRARY RD & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	137+09.47	55.66' RT	712.10
B	137+07.74	55.70' RT	712.08
C	137+07.64	50.70' RT	712.00
D	137+02.80	55.80' RT	712.14
E	137+02.70	50.80' RT	712.07
F	136+97.87	55.91' RT	712.21
G	136+97.76	50.92' RT	712.14
H	136+92.93	56.03' RT	712.27
I	136+92.81	51.03' RT	712.20
J	136+88.01	55.72' RT	712.35
K	136+87.88	50.72' RT	712.21

LIBRARY RD



SE LIBRARY RD & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	137+37.43	55.95' RT	712.33
B	137+38.67	51.08' RT	712.25
C	137+42.41	56.70' RT	712.41
D	137+43.14	51.76' RT	712.33
E	137+47.29	57.45' RT	712.78
F	137+48.03	52.51' RT	712.70
G	137+52.17	58.20' RT	712.86
H	137+52.91	53.26' RT	712.77
I	137+57.05	58.95' RT	712.96
J	137+57.79	54.01' RT	712.79
K	137+39.02	67.29' RT	713.19
L	137+43.91	66.59' RT	713.21
M	137+39.70	72.21' RT	713.27
N	137+44.65	71.43' RT	713.31

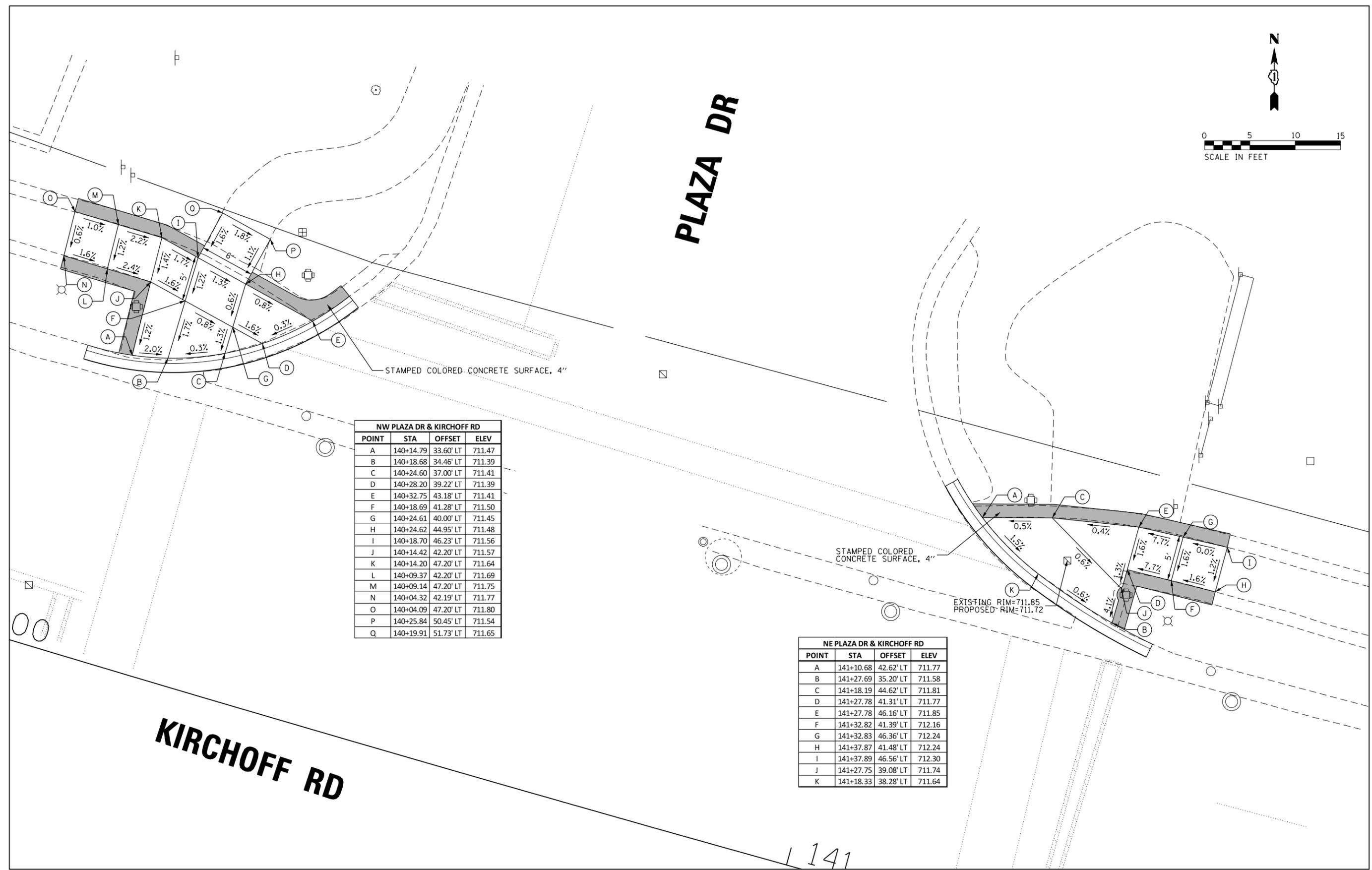
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Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHOFF ROAD / LIBRARY ROAD
PROPOSED ADA PLAN

SCALE: 10' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	33
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



NW PLAZA DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	140+14.79	33.60' LT	711.47
B	140+18.68	34.46' LT	711.39
C	140+24.60	37.00' LT	711.41
D	140+28.20	39.22' LT	711.39
E	140+32.75	43.18' LT	711.41
F	140+18.69	41.28' LT	711.50
G	140+24.61	40.00' LT	711.45
H	140+24.62	44.95' LT	711.48
I	140+18.70	46.23' LT	711.56
J	140+14.42	42.20' LT	711.57
K	140+14.20	47.20' LT	711.64
L	140+09.37	42.20' LT	711.69
M	140+09.14	47.20' LT	711.75
N	140+04.32	42.19' LT	711.77
O	140+04.09	47.20' LT	711.80
P	140+25.84	50.45' LT	711.54
Q	140+19.91	51.73' LT	711.65

NE PLAZA DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	141+10.68	42.62' LT	711.77
B	141+27.69	35.20' LT	711.58
C	141+18.19	44.62' LT	711.81
D	141+27.78	41.31' LT	711.77
E	141+27.78	46.16' LT	711.85
F	141+32.82	41.39' LT	712.16
G	141+32.83	46.36' LT	712.24
H	141+37.87	41.48' LT	712.24
I	141+37.89	46.56' LT	712.30
J	141+27.75	39.08' LT	711.74
K	141+18.33	38.28' LT	711.64

KIRCHOFF RD

PLAZA DR

141

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	PLOT DATE = 8/31/2018	DATE -	REVISED -

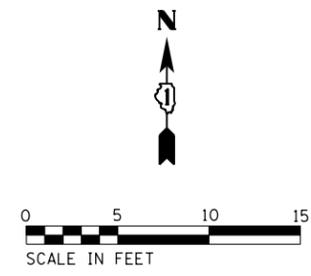
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**KIRCHOFF ROAD /PLAZA DRIVE
PROPOSED ADA PLAN**

SCALE: 10' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	34
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

MEADOW DR



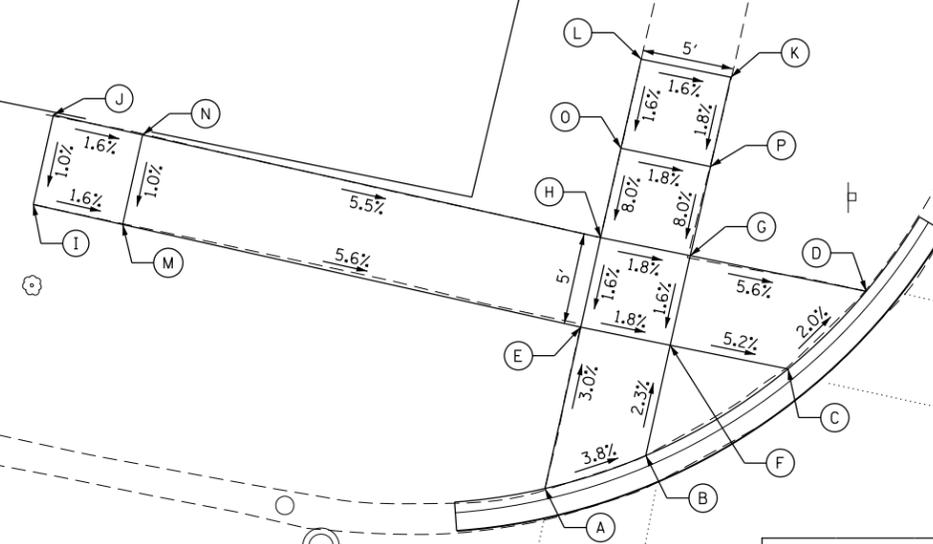
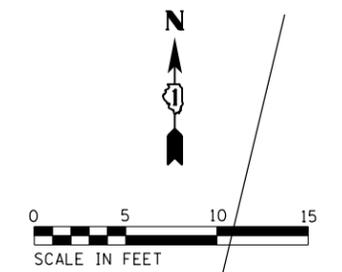
STAMPED COLORED CONCRETE SURFACE, 4"

NW MEADOW DR & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	149+07.09	42.91' LT	714.96
B	149+11.37	46.43' LT	715.05
C	149+15.39	50.62' LT	715.15
D	149+02.10	47.58' LT	715.07
E	149+07.38	51.07' LT	715.16
F	149+13.00	54.79' LT	715.26
G	149+00.64	47.80' LT	715.05
H	148+98.37	55.14' LT	715.16
I	148+95.67	47.25' LT	714.92
J	148+93.76	53.20' LT	714.93
K	149+13.20	56.39' LT	715.29
L	149+04.95	61.87' LT	715.49
M	149+15.35	60.91' LT	715.40
N	149+06.64	65.06' LT	715.52
O	149+01.66	57.71' LT	715.31
P	148+89.08	46.63' LT	714.82
Q	148+88.87	52.17' LT	714.85
R	149+16.89	65.67' LT	715.47
S	149+07.69	69.95' LT	715.61
T	148+99.50	51.47' LT	715.10
U	149+04.52	54.39' LT	715.21
V	149+09.07	59.13' LT	715.34

KIRCHOFF RD

149

ORIOLE LN



NW ORIOLE LN & KIRCHOFF RD			
POINT	STA	OFFSET	ELEV
A	165+43.50	35.85' LT	718.86
B	165+48.46	38.79' LT	718.64
C	165+54.97	45.08' LT	718.16
D	165+58.21	50.14' LT	718.04
E	165+43.52	44.87' LT	718.59
F	165+48.47	44.96' LT	718.50
G	165+48.48	49.96' LT	718.58
H	165+43.54	49.87' LT	718.67
I	165+13.14	45.11' LT	720.10
J	165+13.20	50.11' LT	720.15
K	165+48.56	59.96' LT	719.07
L	165+43.63	59.87' LT	719.15
M	165+18.09	45.06' LT	720.02
N	165+18.14	50.06' LT	720.07
O	165+43.58	54.87' LT	719.07
P	165+48.52	54.96' LT	718.98

165+00
KIRCHOFF RD

FILE NAME =	USER NAME = wbioftus	DESIGNED - GR	REVISED -
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Default	PLOT SCALE = 10'	CHECKED - GR	REVISED -
	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

KIRCHOFF ROAD /ORIOLE LANE PROPOSED ADA PLAN			
SCALE: 10'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	36
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

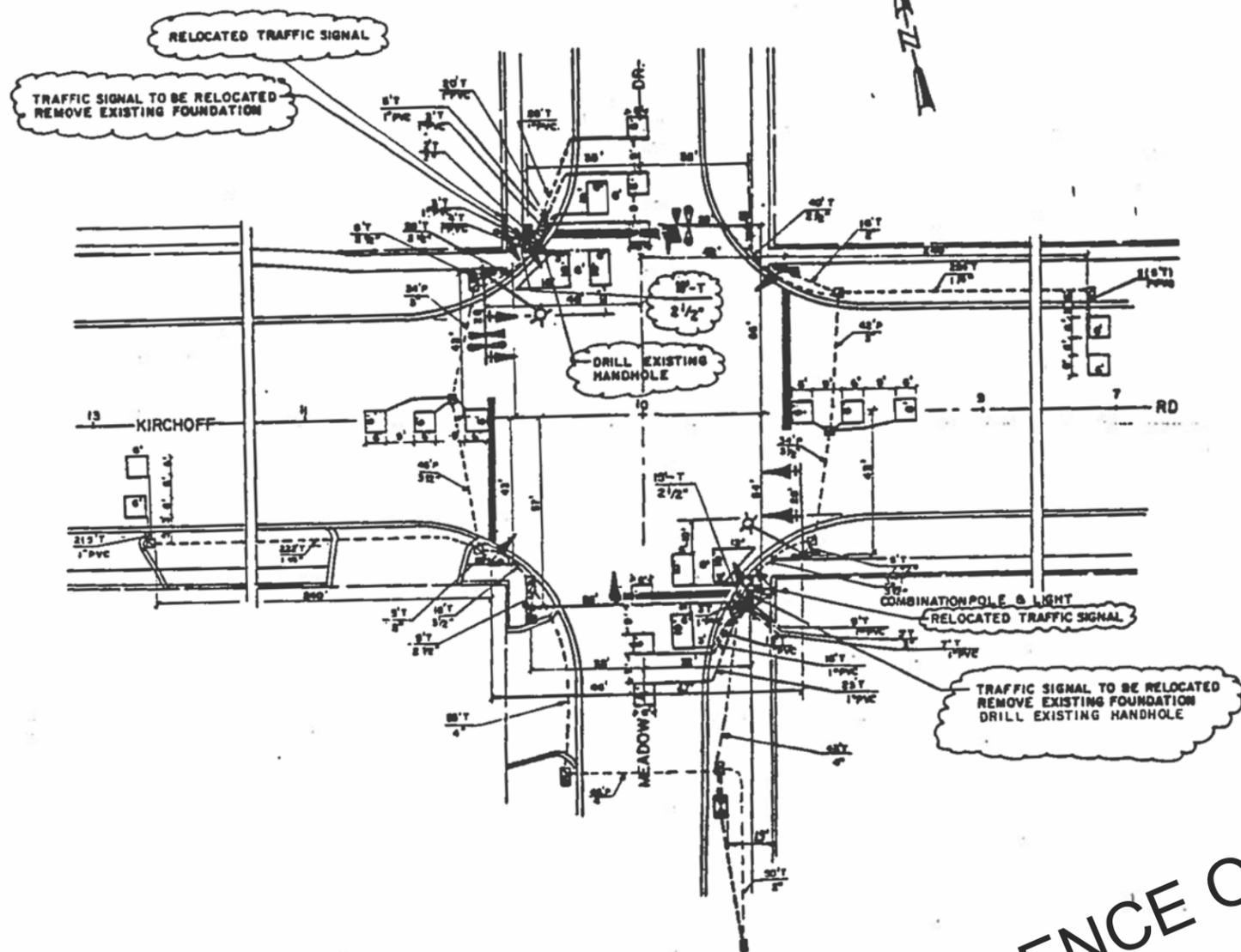
DATE	DESIGNER	CHECKER	SCALE	SHEET NO.
		COOK	44	30

93-0065-00-87

- ALL DETECTOR LOOPS SHALL CONSIST OF THE NUMBER OF TURNS REQUIRED AND SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE LOOP DETECTOR SUPPLIER'S MANUFACTURER'S RECOMMENDATIONS. THE DETECTOR LOOP SHALL BE REARMED FOR THAT PORTION OF SAW CUT BEYOND THE SPICE AS SPECIFIED IN SECTION 740.00 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL, ITDS.
- LEAD-IN WIRES SHALL BE INSTALLED IN STRICT CONFORMITY WITH THE MANUFACTURER'S RECOMMENDATIONS. THE 2/0 SHIELDED CABLE TO BE USED FOR THE DETECTOR LOOP LEAD-IN SHALL BE REARMED FROM THE SPICE TO THE CONTROL AS SPECIFIED IN SECTION 740.00 OF THE SPECIFICATIONS FOR TRAFFIC CONTROL, ITDS. PLAT CABLE W/L NOT BE PERMITTED.
- ALL SIGNAL AND DETECTOR ELECTRIC CABLE THAT IS FURNISHED BY THE CONTRACTOR SHALL BE PROTECTED BY POLYETHYLENE INSULATION WITH A POLYVINYLCHLORIDE JACKET. SERVICE CABLE MAY HAVE AN R/P JACKET.
- THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, REPAIR AND ISLAND SURFACE PAINTING AT HANDHOLES, JACKING PITS, INSPECTION OPENINGS AND CONCRETE JUNCTION BOXES SHALL BE SAW CUT AROUND THE AREA TO BE REMOVED. THE REMOVAL AND REPLACEMENT OF SIDEWALK, DRIVEWAY, REPAIR AND ISLAND SURFACE PAINTING WILL BE PAID FOR SEPARATELY.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR LOCATION OF UTILITIES CALL J.W.I., I.E., TEL. PRICE NUMBER 608-692-0123 AND STATE MAINTAINED TRAFFIC SIGNAL, 312-370-2800.
- ALL SIGNAL POST AND POST AND POLE SHALL BE LOCATED WITH THEIR CENTERLINES A MINIMUM OF FOUR (4) AND SIX (6) FEET, RESPECTIVELY FROM THE BACK OF THE CURB UNLESS NOTED OR BIVEN-SIGNED TO THE CONTRARY ON THE DRAWINGS. IN NON-CURBED AREAS THE POST AND POLE SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER. SIGNAL POSTS SHOULD BE PLACED AT A MINIMUM OF TWO (2) FEET BEHIND THE EDGE OF THE SHOULDER.
- FOR THE LOCATION OF ALL LOOPS CONTACT THE IDOT AREA TRAFFIC SIGNAL ENGINEER AT 804-4139 WHO WILL MARK THE PAVEMENT FOR THE CUTTING OF THE LOOPS.
- HANDHOLE COVERS SHALL BE LABELED "TRAFFIC SIGNAL".

TRAFFIC SIGNAL LEGEND

CONTROLLER	EXISTING
SERVICE INSTALLATION	
SIGNAL HEAD OPTICALLY PROGRAMMED	
SIGNAL HEAD	
EMERGENCY VEHICLE SYSTEM DETECTOR, OPTICAL TYPE	
SIGNAL HEAD PEDESTRIAN	
SIGNAL POST	
POST AND ASSEMBLY AND POLE, ALUMINUM	
POST AND ASSEMBLY AND POLE, STEEL	
HANDHOLE	
HEAVY DUTY HANDHOLE	
DOUBLE HANDHOLE	
6. S. CONDUIT IN TRENCH OR PAVED BACKWAY FOR PROTECTIVE DETECTOR, TYPE I OR TYPE II	
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE	
PEDESTRIAN PULLDOWN DETECTOR	
CONDUIT SPICE	
CONCRETE SPICE	
CONCRETE JUNCTION BOX	
WOOD POLE	
SIGNAL FACE WITH BACKPLATE	
PREEMPTION INDICATOR LIGHT	

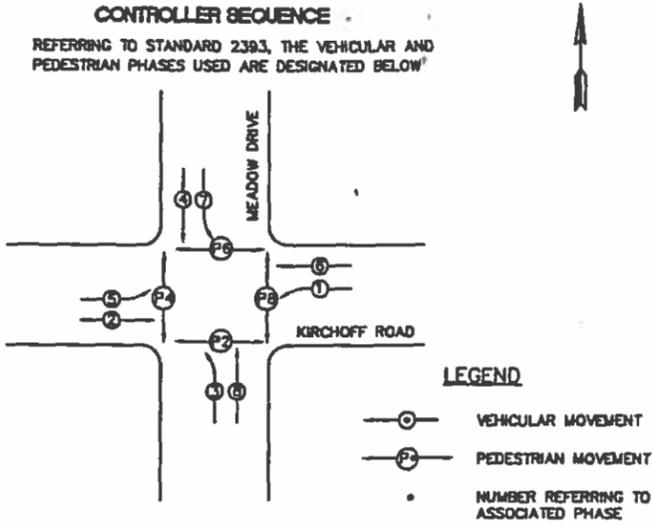


NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

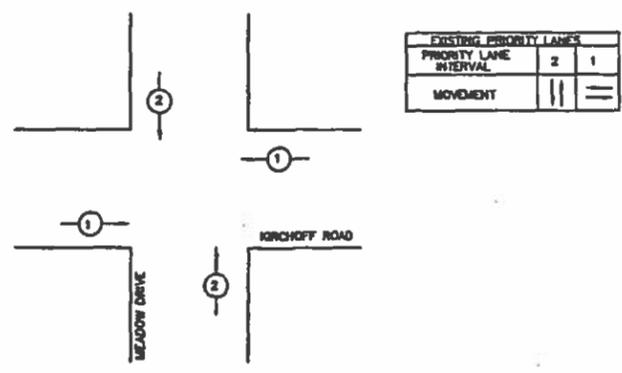
FOR REFERENCE ONLY

REVISIONS		CITY OF ROLLING MEADOWS	
NAME	DATE	TRAFFIC SIGNAL RELOCATION	
		ROLLING MEADOWS BIKE PATH	
		KIRCHHOFF RD. & MEADOW DR.	
		SCALE: 1"=20'	DRAWN BY: VAP
		DATE: 3-14-96	CHECKED BY: IAR

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
S. S. L.	36	COOK	44	31
F. A.				
FED. ROAD DIST. NO. 7		ALPHABET	FED. AID PROJECT-	
36 63-0085-00-8T				



EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



SCHEDULE OF QUANTITIES

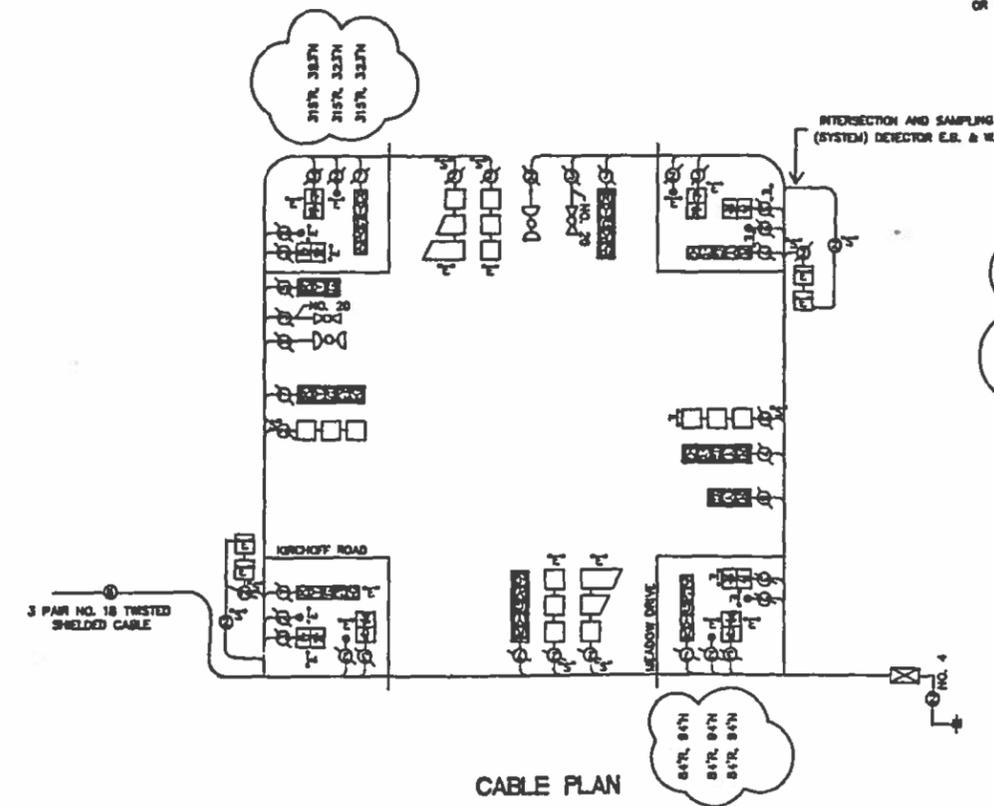
ITEM	UNIT	QUANTITY
GALVANIZED STEEL CONDUIT IN TRENCH 2 1/2"	FOOT	28
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	417
ELECTRIC CABLE IN CONDUIT NO. 14 3/C	FOOT	417
ELECTRIC CABLE IN CONDUIT NO. 14 7/C	FOOT	417
CONCRETE FOUNDATION, TYPE A	FOOT	8
TRENCH AND BACKFILL	FOOT	28
RELOCATE EXISTING SIGNAL HEAD	EACH	2
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	2
RELOCATE EXISTING PEDESTRIAN PUSHBUTTON	EACH	2
REMOVE EXISTING CONCRETE FOUNDATION	EACH	2
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1187
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DRILL EXISTING HANDHOLE	EACH	2

EXISTING PHASE DESIGNATION DIAGRAM

DUAL ENTRY - ALL LEGS
PROTECTED/PERMITTED LEFT TURN PHASING

PRIORITY SEQUENCE GENERAL NOTES

- ONCE PREEMPTION HAS BEEN CALLED, TERMINATION OF A PHASE(S) SHALL BE IDENTICAL TO THE NORMAL SEQUENCE OF OPERATION'S TERMINATION OF A PHASE(S) AS DESCRIBED IN STANDARD 2383.
- CONTINUATION OR TERMINATION OF ALL RIGHT TURN OVERLAPS SHALL BE IDENTICAL TO THE NORMAL SEQUENCE OF OPERATION'S CONTINUATION OR TERMINATION OF RIGHT TURN OVERLAPS AS DESCRIBED IN THE CLEARANCE NOTES FOR RIGHT TURN OVERLAPS.
- TERMINATION OF ALL PEDESTRIAN PHASES SHALL INCLUDE A FULL FLASHING "DON'T WALK" INTERVAL.
- IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN ENTERING OR LEAVING THE PREEMPTION SEQUENCE.



NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

LEGEND

- 100'N = REMOVE 100' OF ELECTRIC CABLE FROM CONDUIT.
- 100'NR = REMOVE AND REINSTALL 100' OF ELECTRIC CABLE FROM CONDUIT.
- 100'N = INSTALL 100' OF NEW CABLE OF THE SIZE SPECIFIED.
- EXCESS CABLE SHALL BE CUT AND DISPOSED OF BY THE CONTRACTOR AS REQUIRED. THIS WORK SHALL BE INCIDENTAL TO REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

CABLE PLAN LEGEND

EXISTING	NEW	DESCRIPTION
[Symbol]	[Symbol]	8" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	CONTROLLER CABINET
[Symbol]	[Symbol]	SERVICE INSTALLATION
[Symbol]	[Symbol]	VEHICLE DETECTOR, INDUCTION LOOP
[Symbol]	[Symbol]	MAGNETIC DETECTOR
[Symbol]	[Symbol]	EMERGENCY VEHICLE LIGHT DETECTOR
[Symbol]	[Symbol]	CONFIRMATION BEACON
[Symbol]	[Symbol]	PUSHBUTTON DETECTOR
[Symbol]	[Symbol]	2 DENOTES NUMBER OF MANUFACTURER (NEW). ALL CABLE NO. 14 (EXCEPT AS INDICATED). ALL LOOP DETECTOR CABLE TO BE SHIELDED.
[Symbol]	[Symbol]	SIGNAL FACE WITH BACKPLATE.
[Symbol]	[Symbol]	"P" INDICATES PROGRAMMED HEAD.
[Symbol]	[Symbol]	"E" INDICATES EXISTING SIGNAL HEAD OR EXISTING PEDESTRIAN SIGNAL HEAD.
[Symbol]	[Symbol]	5' TWISTED AND SHIELDED CABLE

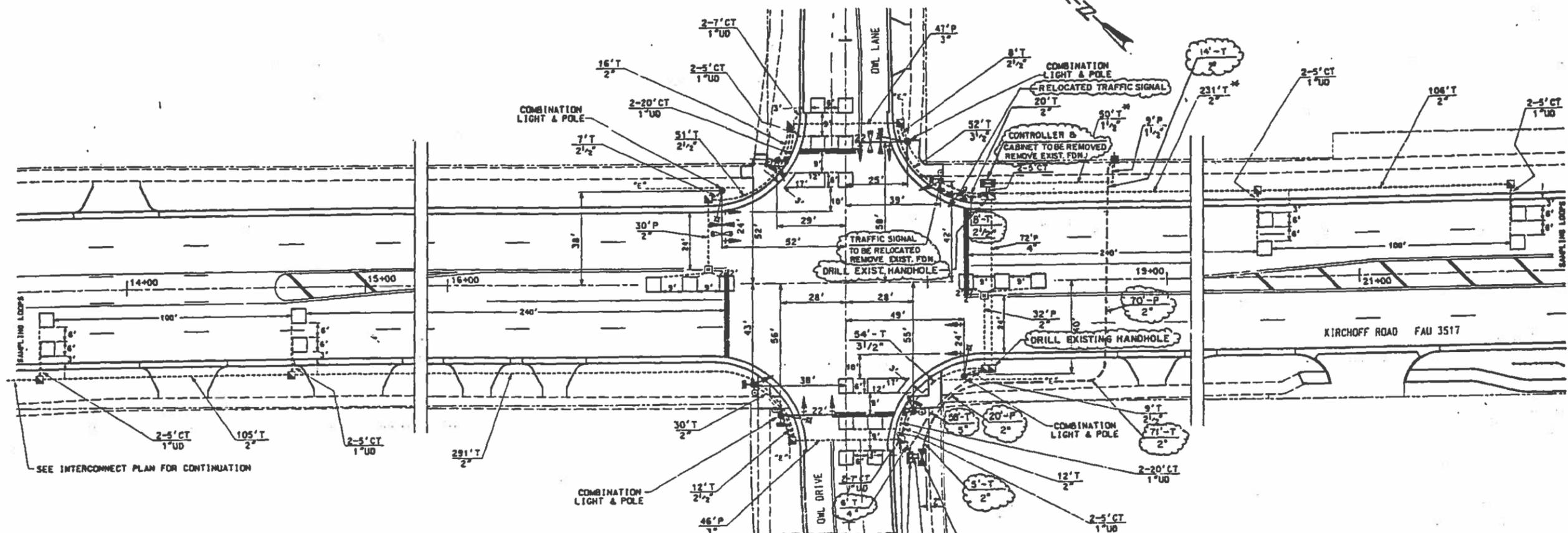
FOR REFERENCE ONLY

CITY OF ROLLING MEADOWS

EXISTING CABLE PLAN, AND PHASE DIAGRAM
ROLLING MEADOWS BIKE PATH
KIRCHOFF ROAD AND MEADOW DRIVE

REVISIONS	
NO.	DATE

DATE	DESIGNED	CHECKED	DRAWN	SHEET NO.
		COOK	44	28
93-0063-00-87				



SEE INTERCONNECT PLAN FOR CONTINUATION

TRAFFIC SIGNAL LEGEND

	EXISTING	CONTROLLER
	EXISTING	SERVICE INSTALLATION
	EXISTING	SIGNAL HEAD
	EXISTING	SIGNAL HEAD WITH BACKPLATE
	EXISTING	SIGNAL HEAD OPTICALLY PROGRAMMED
	EXISTING	SIGNAL HEAD, PEDESTRIAN
	EXISTING	SIGNAL POST
	EXISTING	MAST ARM ASSEMBLY AND POLE, STEEL
	EXISTING	MAST ARM ASSEMBLY AND POLE, ALUMINUM
	EXISTING	HANDHOLE
	EXISTING	HEAVY DUTY HANDHOLE
	EXISTING	DOUBLE HANDHOLE
	EXISTING	G.S. CONDUIT IN TRENCH OR PUSHED
	EXISTING	PEDESTRIAN PUSHBUTTON DETECTOR
	EXISTING	DETECTOR LOOP (6' x 6' UNLESS OTHERWISE NOTED ON PLANS)
	EXISTING	CONCRETE JUNCTION BOX
	EXISTING	CAST IRON JUNCTION BOX
	EXISTING	EMERGENCY VEHICLE SYSTEM DETECTOR
	EXISTING	PREEMPTION INDICATOR LIGHT
	EXISTING	CONDUIT SPLICE
	EXISTING	WOOD POLE
	EXISTING	REACEWAY FOR MAGNETIC DETECTOR TYPE I OR TYPE II
	EXISTING	VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
	EXISTING	STREET LIGHTING CONDUIT (AT COMBINATION LIGHT & POLE)

FOR REFERENCE ONLY

NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

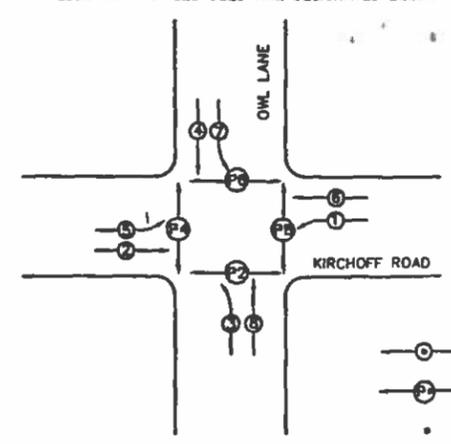
REVISIONS	
NAME	DATE

CITY OF ROLLING MEADOWS
TRAFFIC SIGNAL RELOCATION
 OF
ROLLING MEADOWS BIKE PATH
KIRCHOFF RD. & OWL LANE

SCALE: 1" = 20'
 DATE: 3-14-96

DRAWN BY: VAP
 CHECKED BY: JAR

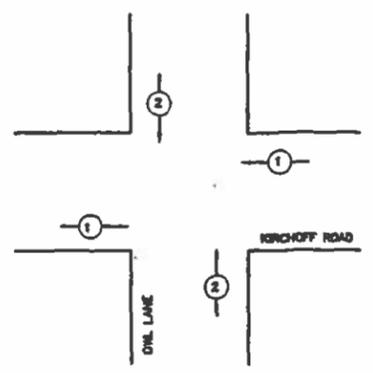
EXISTING CONTROLLER SEQUENCE
 REFERRING TO STANDARD 2393, THE VEHICULAR AND PEDESTRIAN PHASES USED ARE DESIGNATED BELOW



LEGEND

- VEHICULAR MOVEMENT
- ⊙ PEDESTRIAN MOVEMENT
- NUMBER REFERRING TO ASSOCIATED PHASE

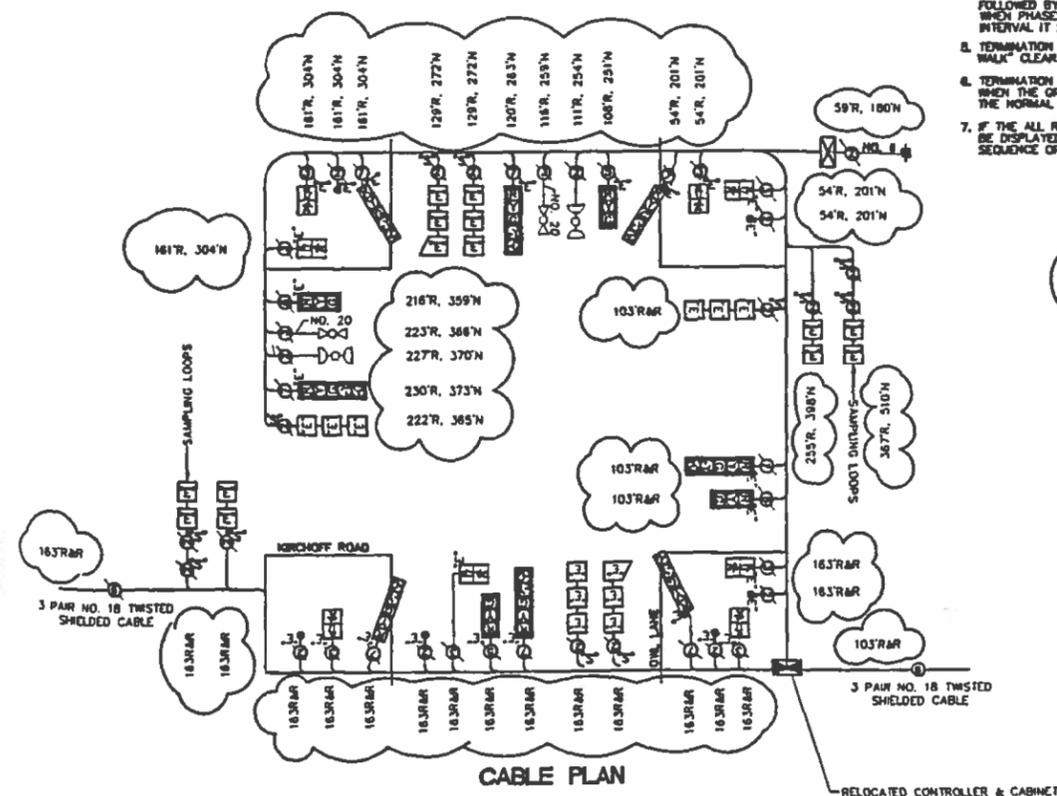
EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE NOTES FOR DUAL ENTRY OPERATION - ALL LEGS



EXISTING PRIORITY LANES	
PRIORITY LANE INTERVAL	MOVEMENT
2	1
	==

EXISTING PHASE DESIGNATION DIAGRAM

DUAL ENTRY - ALL LEGS
 PROTECTED/PERMITTED LEFT TURN PHASING
 GREEN PHASE SHALL REST ON KIRCHOFF ROAD



PRIORITY SEQUENCE GENERAL NOTES

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1,3,5 OR 7 ALONE IN PHASES 1+4, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A YELLOW PRIORITY INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN.
3. TERMINATION OF PHASES 1+6, 3+6, 2+6 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+8 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+8 OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
7. IF THE ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE OF OPERATIONS.

NOTE: ITEMS CIRCLED ARE WORK TASKS TO BE PERFORMED AS PART OF THIS PROJECT. ALL OTHER ITEMS SHOWN ARE EXISTING.

LEGEND

- 100'R - REMOVE 100 FEET OF ELECTRIC CABLE FROM CONDUIT
- 100'RAR - REMOVE AND REINSTALL 100 FEET OF ELECTRIC CABLE FROM CONDUIT
- 100'N - INSTALL 100 FEET OF NEW CABLE OF THE SIZE SPECIFIED.

• EXCESS CABLE SHALL BE CUT AND DISPOSED OF BY THE CONTRACTOR AS REQUIRED. THIS WORK SHALL BE INCIDENTAL TO REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT.

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
GALVANIZED STEEL CONDUIT IN TRENCH 2"	FOOT	90
GALVANIZED STEEL CONDUIT IN TRENCH 2 1/2"	FOOT	8
GALVANIZED STEEL CONDUIT IN TRENCH 4"	FOOT	16
GALVANIZED STEEL CONDUIT IN TRENCH 5"	FOOT	58
GALVANIZED STEEL CONDUIT, PUSHED 2"	FOOT	190
ELECTRIC CABLE IN CONDUIT NO. 6 2/C	FOOT	180
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	1129
ELECTRIC CABLE IN CONDUIT NO. 14 3/C	FOOT	1010
ELECTRIC CABLE IN CONDUIT NO. 14 5/C	FOOT	610
ELECTRIC CABLE IN CONDUIT NO. 14 7/C	FOOT	1141
ELECTRIC CABLE IN CONDUIT NO. 14 2/C	FOOT	1817
TWISTED, SHIELDED		
ELECTRIC CABLE IN CONDUIT NO. 20 3/C	FOOT	625
CONCRETE FOUNDATION, TYPE A	FOOT	4
CONCRETE FOUNDATION, TYPE D	FOOT	4
CONCRETE DOUBLE HANDHOLE	EACH	1
TRENCH AND BACKFILL	FOOT	188
RELOCATE EXISTING SIGNAL HEAD	EACH	1
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	1
RELOCATE EXISTING PEDESTRIAN PUSHBUTTON	EACH	1
REMOVE EXISTING TRAFFIC CONTROLLER AND CABINET	EACH	1
FULL ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	2
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3372
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	3163
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DRILL EXISTING HANDHOLE	EACH	3
INDUCTION LOOP DETECTOR AMPLIFIER	EACH	12

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
⊙	⊙	8" TRAFFIC SIGNAL SECTION
⊙	⊙	12" TRAFFIC SIGNAL SECTION
⊙	⊙	12" PEDESTRIAN SIGNAL SECTION
⊙	⊙	12" PEDESTRIAN SIGNAL SECTION
⊙	⊙	CONTROLLER CABINET
⊙	⊙	SERVICE INSTALLATION
⊙	⊙	VEHICLE DETECTOR, INDUCTION LOOP
⊙	⊙	MAGNETIC DETECTOR
⊙	⊙	EMERGENCY VEHICLE LIGHT DETECTOR
⊙	⊙	CONFIRMATION BEACON
⊙	⊙	PUSHBUTTON DETECTOR
⊙	⊙	⊙ DENOTES NUMBER OF CONDUCTORS (NEW). ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
⊙	⊙	⊙ SIGNAL FACE WITH BACKPLATE. ⊙ INDICATES PROGRAMMED HEAD. ⊙ INDICATES EXISTING SIGNAL HEAD OR EXISTING PEDESTRIAN SIGNAL HEAD.
⊙	⊙	⊙ TWISTED AND SHIELDED CABLE

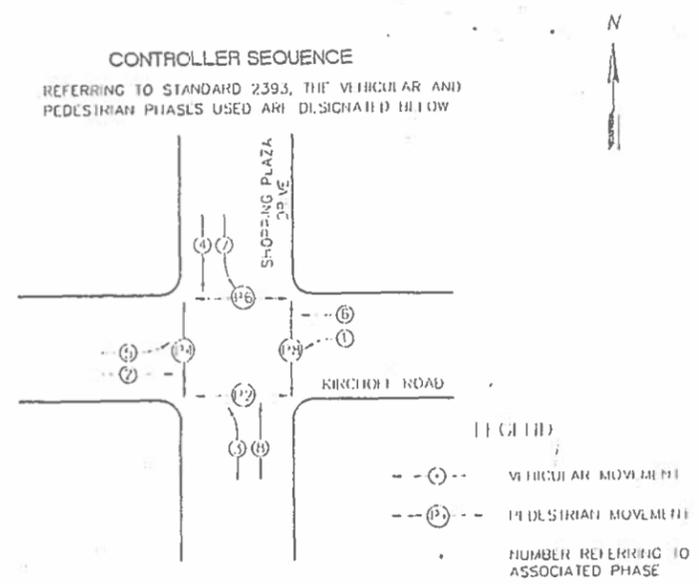
FOR REFERENCE ONLY

CITY OF ROLLING MEADOWS

EXISTING CABLE PLAN AND PHASE DIAGRAM
 ROLLING MEADOWS BIKE PATH
 KIRCHOFF ROAD AND OWL LANE

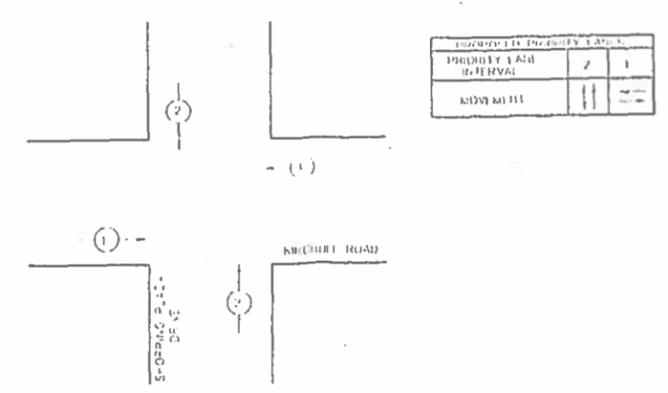
SCALE: NONE
 DRAWN BY: IAR
 DATE: 5/13/98
 CHECKED BY: D.D.L.

REVISIONS	
NO.	DATE



PHASE DESIGNATION DIAGRAM
DUAL ENTRY - ALL LEGS
PROTECTED/PERMITTED LEFT TURN PHASING

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PRIORITY SEQUENCE GENERAL NOTES

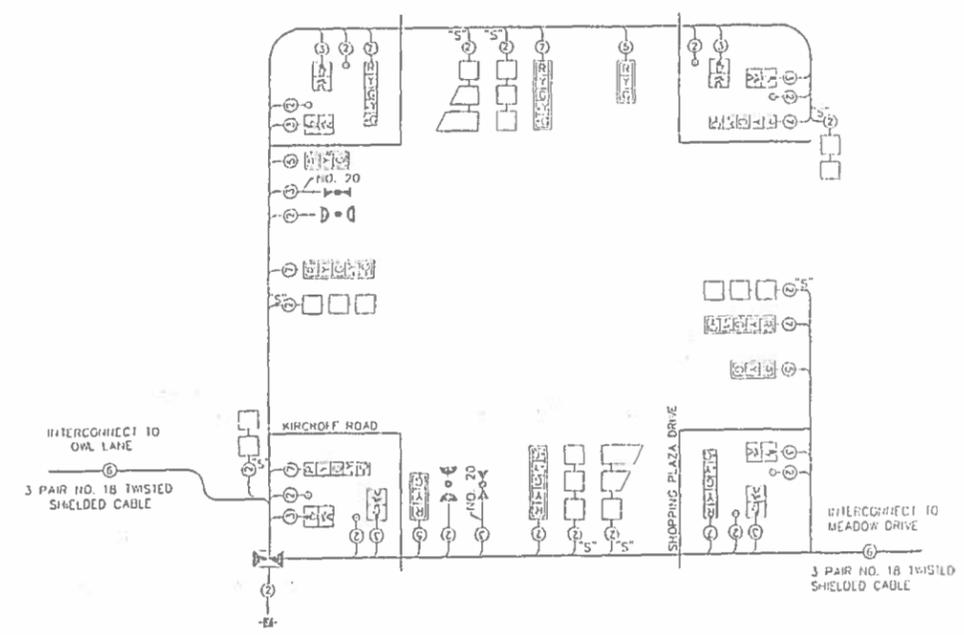
1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED
2. TERMINATION OF PHASES 1,3,5 OR 7 ALONE BY PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A YELLOW A PRIORITY INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN
3. TERMINATION OF PHASES 1+6, 3+8, 2+5 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED WHEN PHASES 2+6 OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED BY THE PRIORITY LANE INTERVAL IT SHALL REMAIN GREEN
5. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION
6. IF THE ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATION IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE OF OPERATIONS

SCHEDULE OF QUANTITIES

CODE NO.	QUAN.	UNIT	PAY ITEM
14010020	4	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
14010110	4	EACH	SIGNAL HEAD, 1-FACE, 5-SECTION, BRACKET MOUNTED
14010120	4	EACH	SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
14020200	8	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE, BRACKET MOUNTED
14030110	8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED
14060150	4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT
14084600	2	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT
14084700	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT
14084800	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT
14110500	1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET (SIGNAL) TRANSCEIVER
14150110	1	EACH	INDUCTION LOOP DETECTOR AMPLIFIER
14160200	8	FOOT	DETECTION LOOP, TYPE I
14160300	724	FOOT	PEDESTRIAN PUSHBUTTON
14190100	8	EACH	GALVANIZED STEEL CONDUIT IN TRENCH, 2"
14200500	200	FOOT	GALVANIZED STEEL CONDUIT IN TRENCH, 2 1/2"
14200800	112	FOOT	GALVANIZED STEEL CONDUIT IN TRENCH, 4"
14202300	107	FOOT	GALVANIZED STEEL CONDUIT, PUSHED, 2"
14201500	107	FOOT	GALVANIZED STEEL CONDUIT, PUSHED, 4"
14201900	200	FOOT	URD DUCT, WITHOUT CABLE, IN TRENCH 1"
14209200	200	FOOT	ELECTRIC CABLE IN CONDUIT NO. 4, 2/C
14212200	245	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 2/C
14214200	1398	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 3/C
14214201	1176	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 5/C
14214203	637	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 7/C
14214205	1275	FOOT	ELECTRIC CABLE IN CONDUIT NO. 14, 2/C TWISTED, SHIELDED
14218600	1617	FOOT	SERVICE INSTALLATION, TYPE J, D
14270100	12	FOOT	CONCRETE FOUNDATION, TYPE A
14270100	35	FOOT	CONCRETE FOUNDATION, TYPE D
14270200	44	FOOT	CONCRETE FOUNDATION, TYPE E, 24-INCH DIAMETER
14280400	5	EACH	CONCRETE HANDHOLE
14280500	4	EACH	CONCRETE HEAVY-DUTY HANDHOLE
14280600	1	EACH	CONCRETE DOUBLE HANDHOLE
14300100	913	FOOT	TRENCH AND BACKFILL
14530010	2	EACH	LIGHT DETECTOR
14530010	1	EACH	LIGHT DETECTOR AMPLIFIER
14530100	224	FOOT	EMERGENCY VEHICLE PRIORITY SYSTEM LEAD-IN CABLE IN CONDUIT

CABLE PLAN LEGEND

EXISTING	PROPOSED	DESCRIPTION
[Symbol]	[Symbol]	8" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" TRAFFIC SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	12" PEDESTRIAN SIGNAL SECTION
[Symbol]	[Symbol]	CONTROLLER CABINET
[Symbol]	[Symbol]	SERVICE INSTALLATION
[Symbol]	[Symbol]	VEHICLE DETECTOR, INDUCTION LOOP
[Symbol]	[Symbol]	MAGNETIC DETECTOR
[Symbol]	[Symbol]	EMERGENCY VEHICLE DETECTOR
[Symbol]	[Symbol]	CONFIRMATION BEACON
[Symbol]	[Symbol]	PUSHBUTTON DETECTOR
[Symbol]	[Symbol]	DENOTES NUMBER OF CONDUITS (NEW) ALL CABLE NO. 18 TWISTED AND SHIELDED ALL LOOP DETECTOR CABLE TO BE SHIELDED
[Symbol]	[Symbol]	SIGNAL FACE WITH BACKPLATE "P" INDICATES PROJECTIONS HEAD "E" INDICATES EXISTING SIGNAL HEAD OR EXISTING PEDIAN SIGNAL HEAD
[Symbol]	[Symbol]	"S" TWISTED AND SHIELDED CABLE



CABLE PLAN

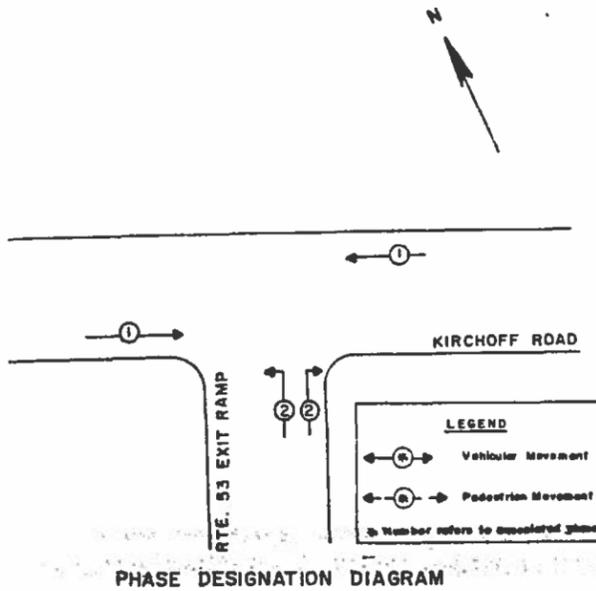
FOR REFERENCE ONLY

DEPARTMENT OF TRANSPORTATION
CABLE PLAN, PHASE DESIGNATION
DIAGRAM AND SCHEDULE OF QUANTITIES
KIRCHHOFF ROAD AND SHOPPING PLAZA DRIVE

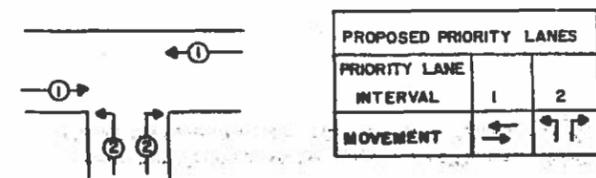
REVISIONS	
NAME	DATE

Name of Intersection KIRCHOFF RD. & RTE. 53 EXIT RAMP
 Controller Specified CONTROLLER SEQUENCE I

Referring to Standard 2303, the vehicular and pedestrian phases used are designated below:
 (Show Movements and Phase Numbers)



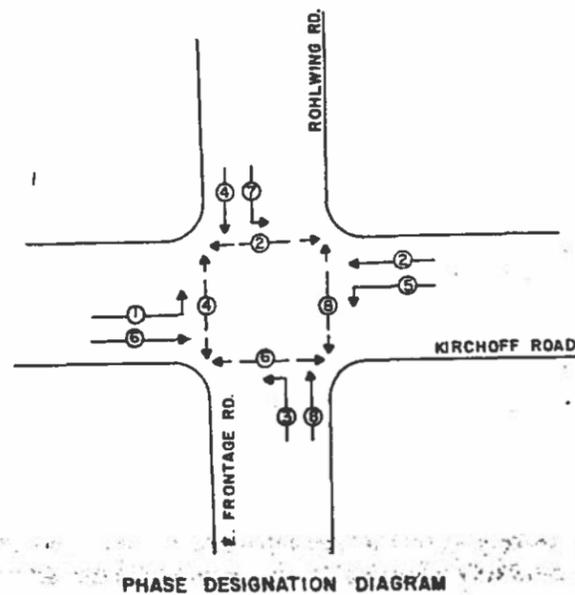
PRIORITY SEQUENCE FOR CONTROLLER SEQUENCE I



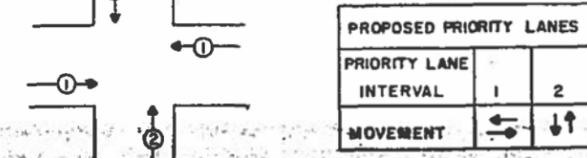
NOTES:

1. TERMINATION OF PHASES 1 OR 2 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 1, OR 2 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
2. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
3. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE.

Name of Intersection KIRCHOFF RD. & ROHLWING RD.-E. FRONTAGE RD.
 Controller Specified CONTROLLER SEQUENCE IV

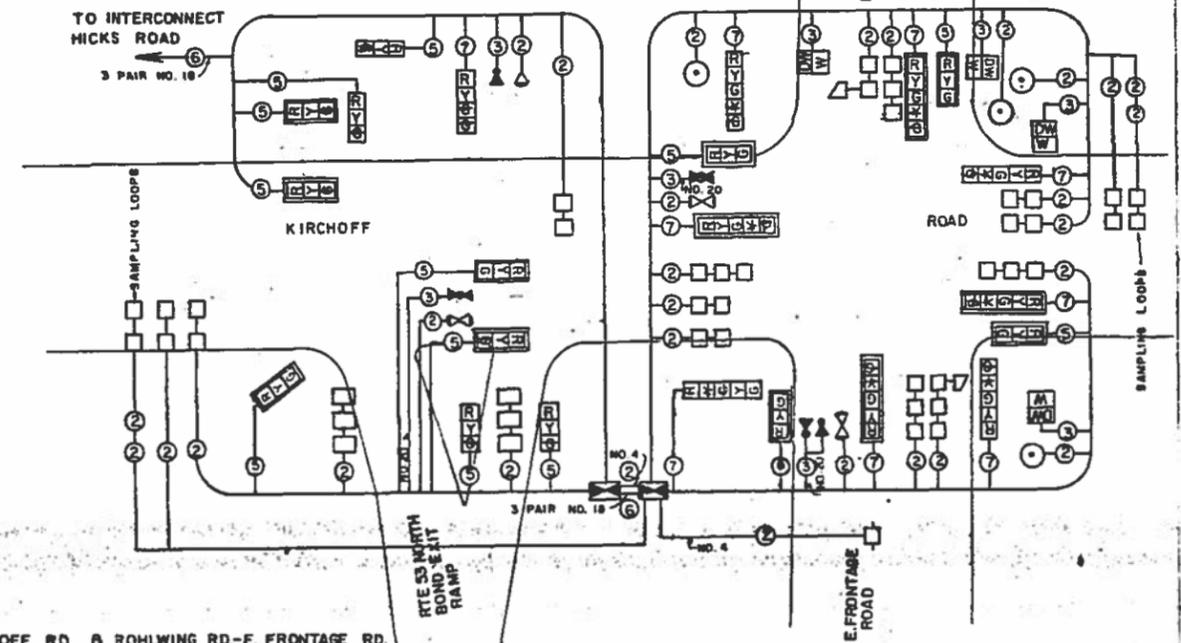


PRIORITY SEQUENCE FOR CONTROLLER SEQUENCE IV



NOTES:

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1, 3, 5, OR 7 ALONE IN PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN.
3. TERMINATION OF PHASES 1+6, 3+8, 2+5 OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+6, OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
7. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN LEAVING THE PRIORITY SEQUENCE.



- KIRCHOFF RD. & ROHLWING RD.-E. FRONTAGE RD. PROPOSED SCHEDULE OF SIGNAL HEADS**
- 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, MAST ARM MOUNTED.
 - 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 5-SECTION, BRACKET MOUNTED.
 - 4 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 5-SECTION, MAST ARM MOUNTED.
 - 4 EACH PEDESTRIAN SIGNAL HEAD, ALUMINUM, 1-FACE, BRACKET MOUNTED.

- KIRCHOFF RD. & RTE. 53 OFF RAMP PROPOSED SCHEDULE OF SIGNAL HEADS**
- 3 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, MAST ARM MOUNTED.
 - 1 EACH SIGNAL HEAD, ALUMINUM, 2-FACE, 1-3 SECTION, BRACKET MOUNTED 1-4 SECTION.
 - 5 EACH SIGNAL HEAD, ALUMINUM, 1-FACE, 3-SECTION, BRACKET MOUNTED.

NOTE: ALL SIGNAL HEADS HAVE 12" LENSES

CABLE PLAN LEGEND

- 12" TRAFFIC SIGNAL SECTION
- CONTROLLER CABINET
- SERVICE INSTALLATION
- VEHICLE DETECTOR, INDUCTION LOOP
- 2 DENOTES NUMBER OF CONDUCTORS (NEW). ALL LOOP DETECTOR CABLE TO BE SHIELDED. ALL CABLE NO. 14 EXCEPT AS INDICATED.
- 2 INDICATES EXISTING CABLE
- MAGNETIC DETECTOR
- OPTICAL DETECTOR
- PEDESTRIAN PUSH BUTTON
- PREEMPTION INDICATOR LIGHT
- SIGNAL FACE WITH BACKPLATE INDICATES PROGRAMMED

FOR REFERENCE ONLY

ILLINOIS DEPARTMENT OF TRANSPORTATION

SEQUENCE OF OPERATION CABLE PLAN

REVISIONS	
NAME	DATE

SCALE: VERT NONE HORIZ NONE
 DATE: DRAWN BY: CHECKED BY:

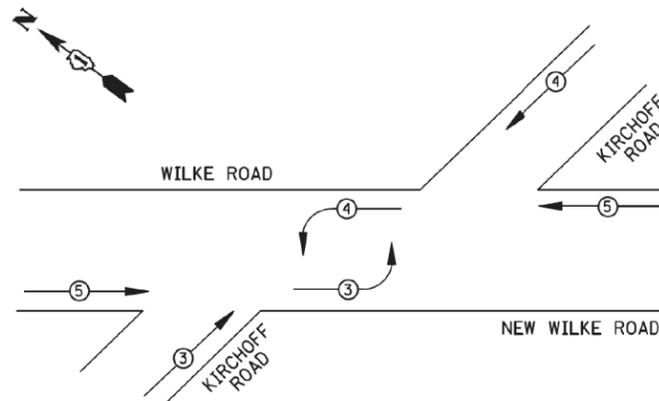
REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 6 EACH PEDESTRIAN SIGNAL HEAD
- 4 EACH PEDESTRIAN PUSH-BUTTON

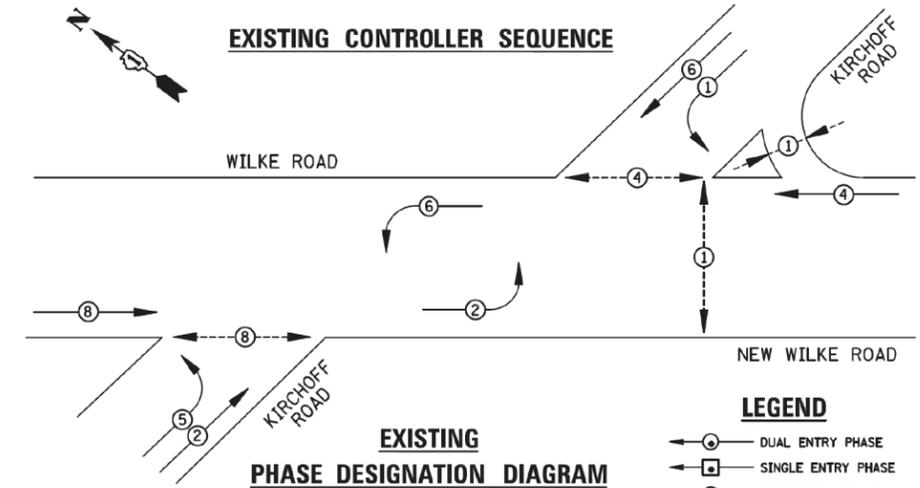
SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	3
PEDESTRIAN SIGNAL HEAD, LED, 3-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1
PEDESTRIAN PUSH-BUTTON, NON-LATCHING	EACH	4



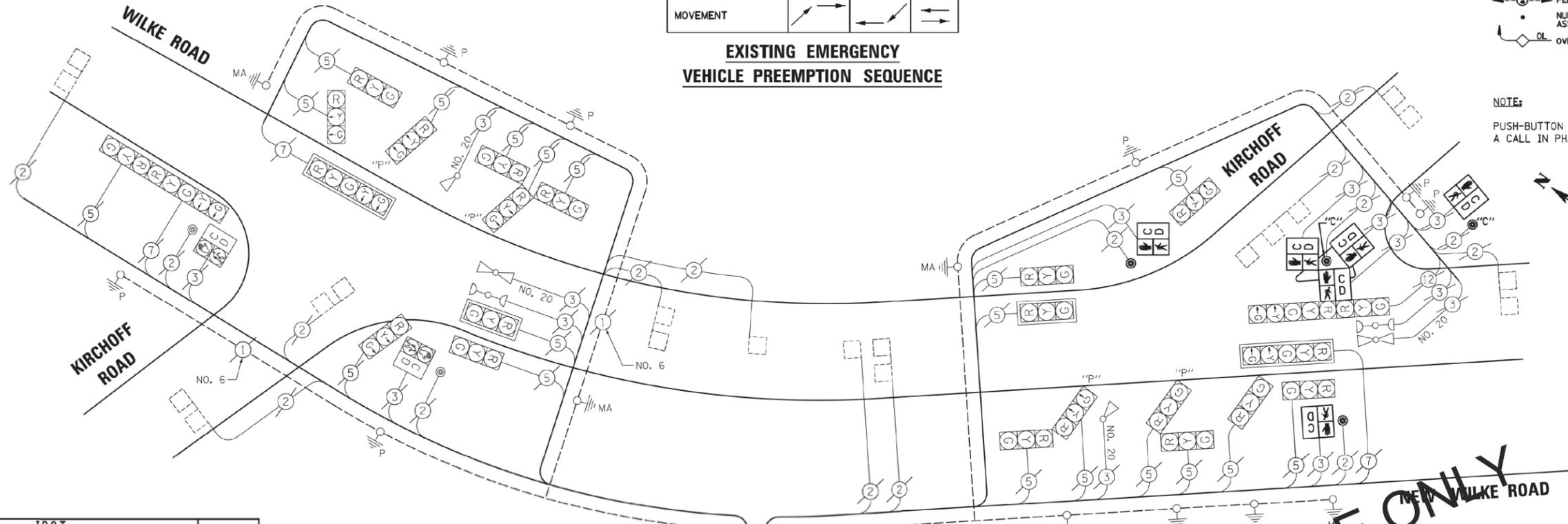
EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



- LEGEND**
- DUAL ENTRY PHASE
 - SINGLE ENTRY PHASE
 - PEDESTRIAN PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE
 - OVERLAP

NOTE:
PUSH-BUTTON "C" SHALL PLACE A CALL IN PHASES 1 AND 4.

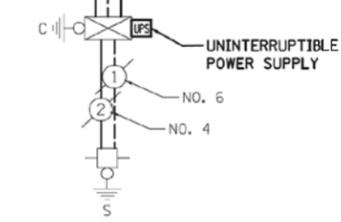


I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	24	135	15	0.50	204
(YELLOW)	24	135	25	0.25	150
(GREEN)	24	135	15	0.25	90
ARROW	8	135	12	0.10	9.6
PED. SIGNAL	8	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		84		0.05	
FLASHER		135	25	0.50	
ENERGY COSTS TO:				TOTAL =	753.6
ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, IL 60196-1096					
ENERGY SUPPLY	CONTACT:	X			
	PHONE:	X			
	COMPANY:	X			

AS - BUILTS

JOHN BURNS CONSTRUCTION CO.

Date: 10/6/14 By: Ryan Bol



CABLE PLAN

FOR REFERENCE ONLY

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

TS* 5395 ROLLING MEADOWS

PREPARED BY:
CEMCON, Ltd.
Consulting Engineers, Land Surveyors & Planners
2280 White Oak Circle, Suite 100
Aurora, Illinois 60504-9675
Ph: 630.866.2100 Fax: 630.866.2199
E-Mail: cedd@cemcon.com Website: www.cemcon.com

FILE NAME: \\MICROST\352105\ 66-5395.DGN	USER NAME: JCC	DESIGNED - BPT	REVISED -
	PLOT SCALE = NOT TO SCALE	DRAWN - JCC/RDS	REVISED -
	PLOT DATE = X	CHECKED - BPT	REVISED -
		DATE - 04-26-13	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM AND EMERGENCY VEHICLE PREEMPTION KIRCHHOFF ROAD AND WILKE ROAD / NEW WILKE ROAD
SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	62	58
CONTRACT NO. 60W37				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FILE NAME: N:\ROLLINGMEADOWS\980361.BR\5\BR100\5\980361BR195\Civil\TR_CAB_98361BR195-01.SHT	USER NAME: wbiofus	DESIGNED - GR	REVISED -
		DRAWN - PM	REVISED -
		CHECKED - GR	REVISED -
		DATE - 8/31/2018	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL AT WILKE / NEW WILKE RD KIRCHHOFF RD
SCALE: 2" SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	47
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				



REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH OPTICALLY PROGRAMMED SIGNAL HEAD, 3 - SECTION

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
4	EACH	OPTICALLY PROGRAMMED SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	MODIFY EXISTING CONTROLLER CABINET
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

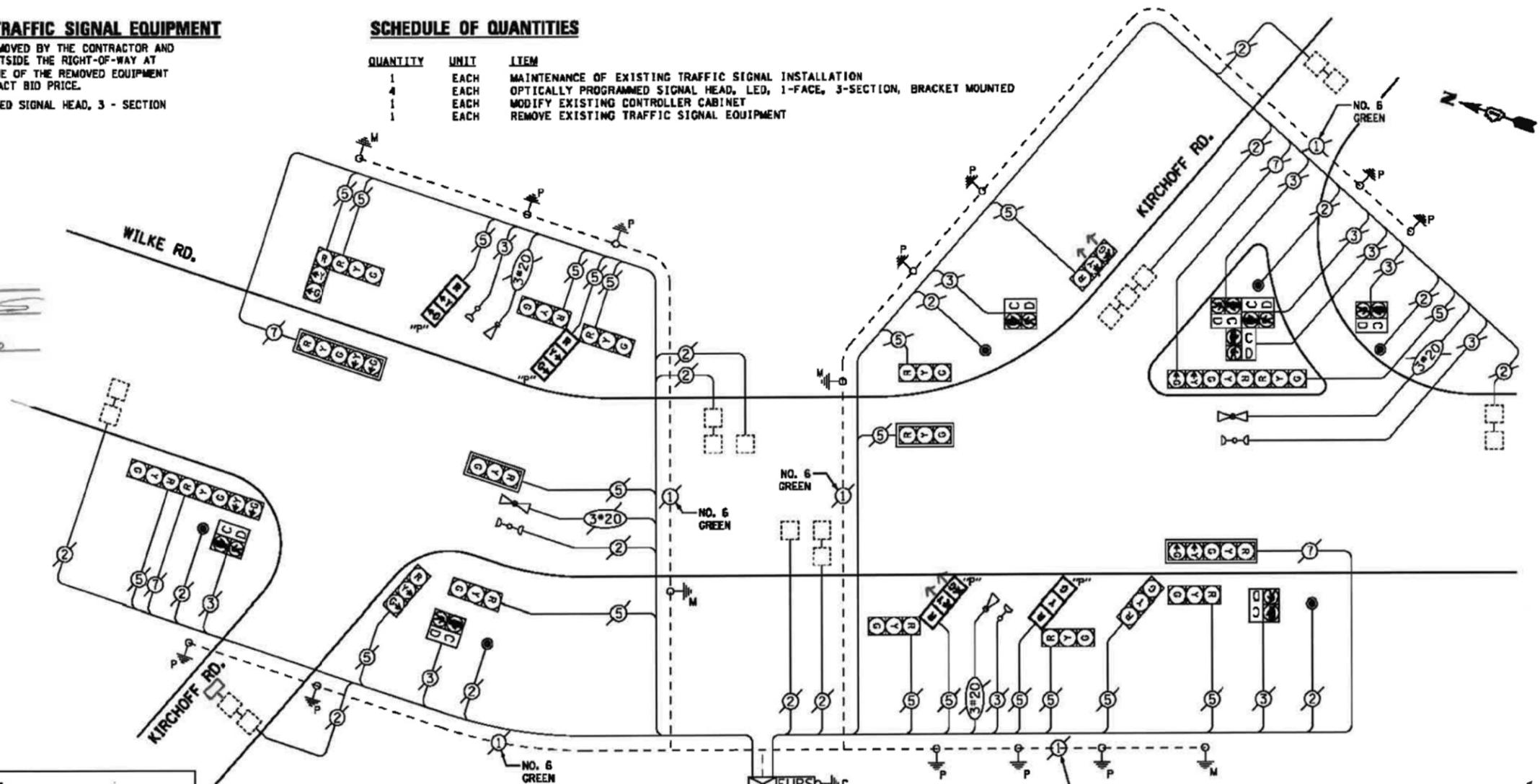


RECORD DRAWINGS

Date (MM/DD/YYYY) 04/05/2016

Signed *Larry Veneziano*

Print(First_Last) *LARRY VENEZIANO*

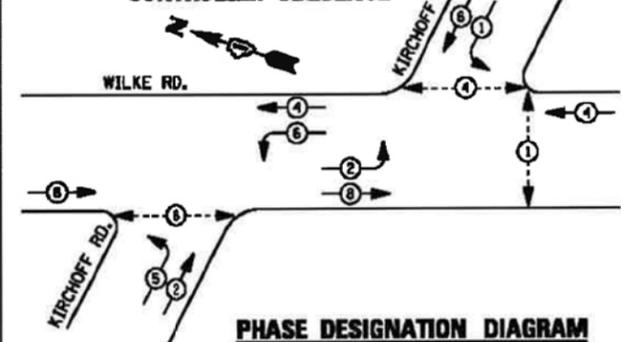


I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE		TOTAL WATTAGE
		INCAND.	LED	
SIGNAL (RED)	24	135	17	204
(YELLOW)	24	135	25	150
(GREEN)	24	135	15	90
ARROW	8	135	12	10
PED. SIGNAL	8	90	25	200
CONTROLLER	1	100	100	100
ILLUM. SIGN		252	25	0.05
VIDEO SYSTEM		150	-	1.00
FLASHER LED				
TOTAL =				754

ENERGY COSTS- **BILLED TO: IDOT DISTRICT 1**
201 WEST CENTER COURT
SCHAMBURG, IL 60196-1096

ENERGY SUPPLY - **CONTACT DAVE SCHACHT**
PHONE 630-437-2129
COMMONWEALTH EDISON

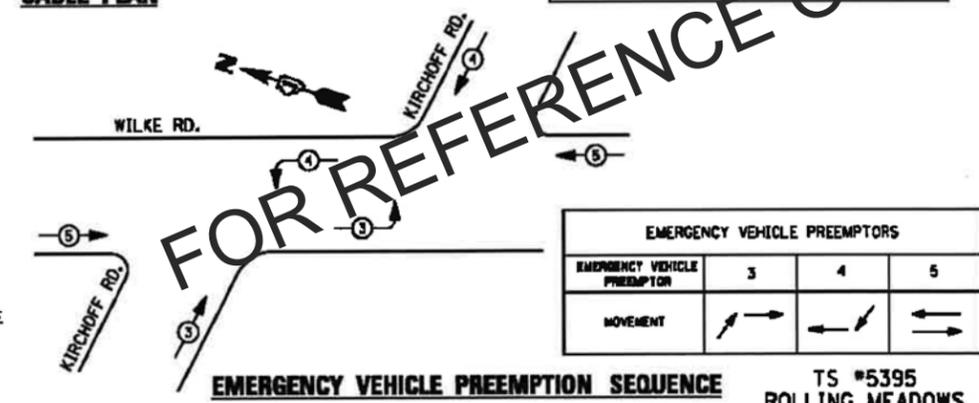
CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

- LEGEND**
- DUAL ENTRY PHASE
 - PEDESTRIAN PHASE
 - SINGLE ENTRY PHASE
 - OVERLAP
- NUMBER REFERS TO ASSOCIATED PHASE

CABLE PLAN



EXISTING UNINTERRUPTIBLE POWER SUPPLY TO BE RECONNECTED TO THE EXISTING CONTROLLER. THIS WORK SHALL BE INCLUDED IN THE BIDDING ITEM "MODIFY EXISTING CONTROLLER CABINET"

FOR REFERENCE ONLY

EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT			

EMERGENCY VEHICLE PREEMPTION SEQUENCE TS #5395 ROLLING MEADOWS

USER NAME = .USERNAME.	DESIGNED - DW	REVISED -
	DRAWN - CC	REVISED -
	CHECKED - JDM	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KIRCHHOFF ROAD AT WILKE ROAD
SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM,
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	76	71

CONTRACT NO. 62A42

USER NAME = .USERNAME.	DESIGNED - GR	REVISED -
	DRAWN - PM	REVISED -
	CHECKED - GR	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

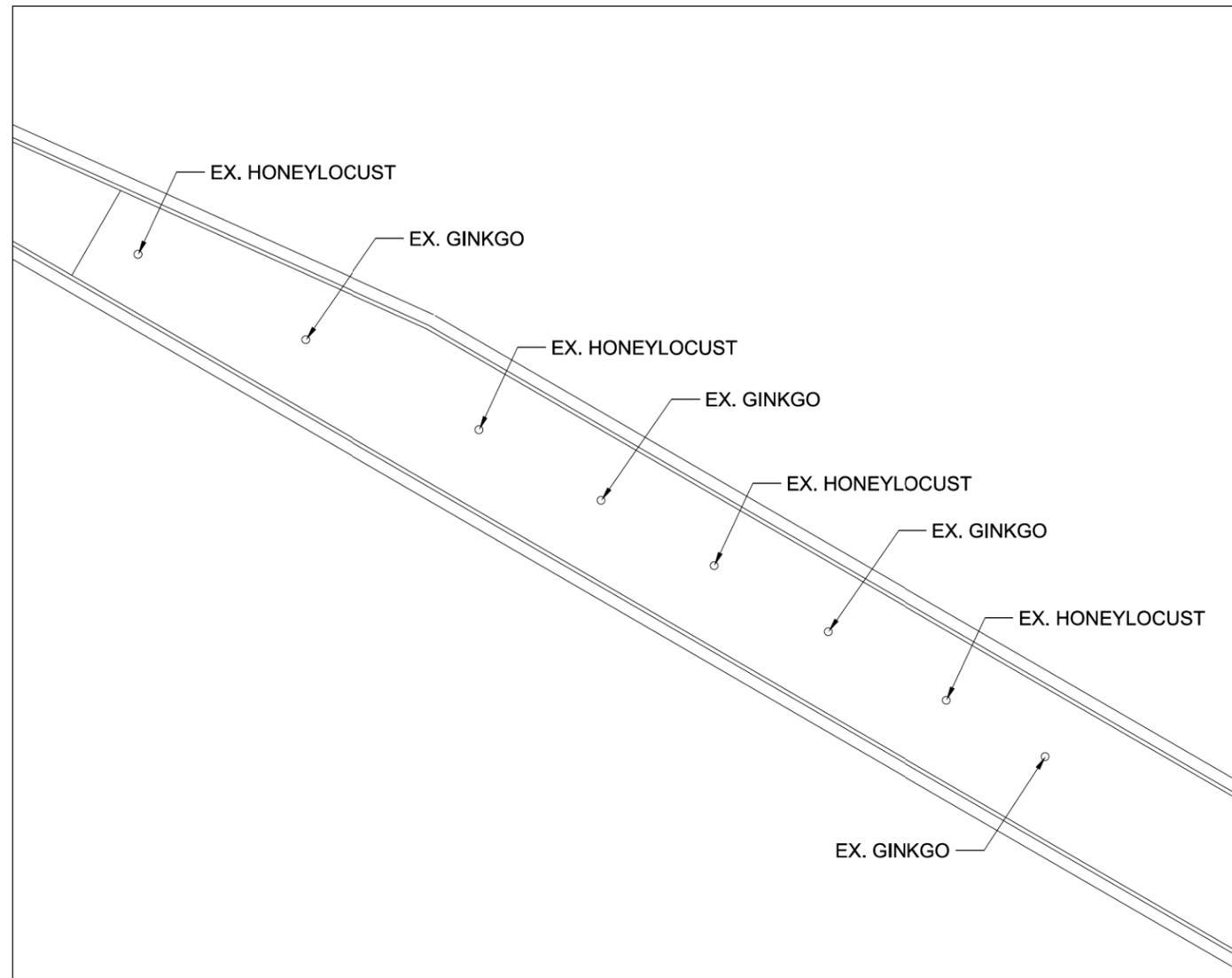
CABLE PLAN AT WILKE RD
KIRCHHOFF RD

SCALE: 2" SHEET OF SHEETS STA. TO STA.

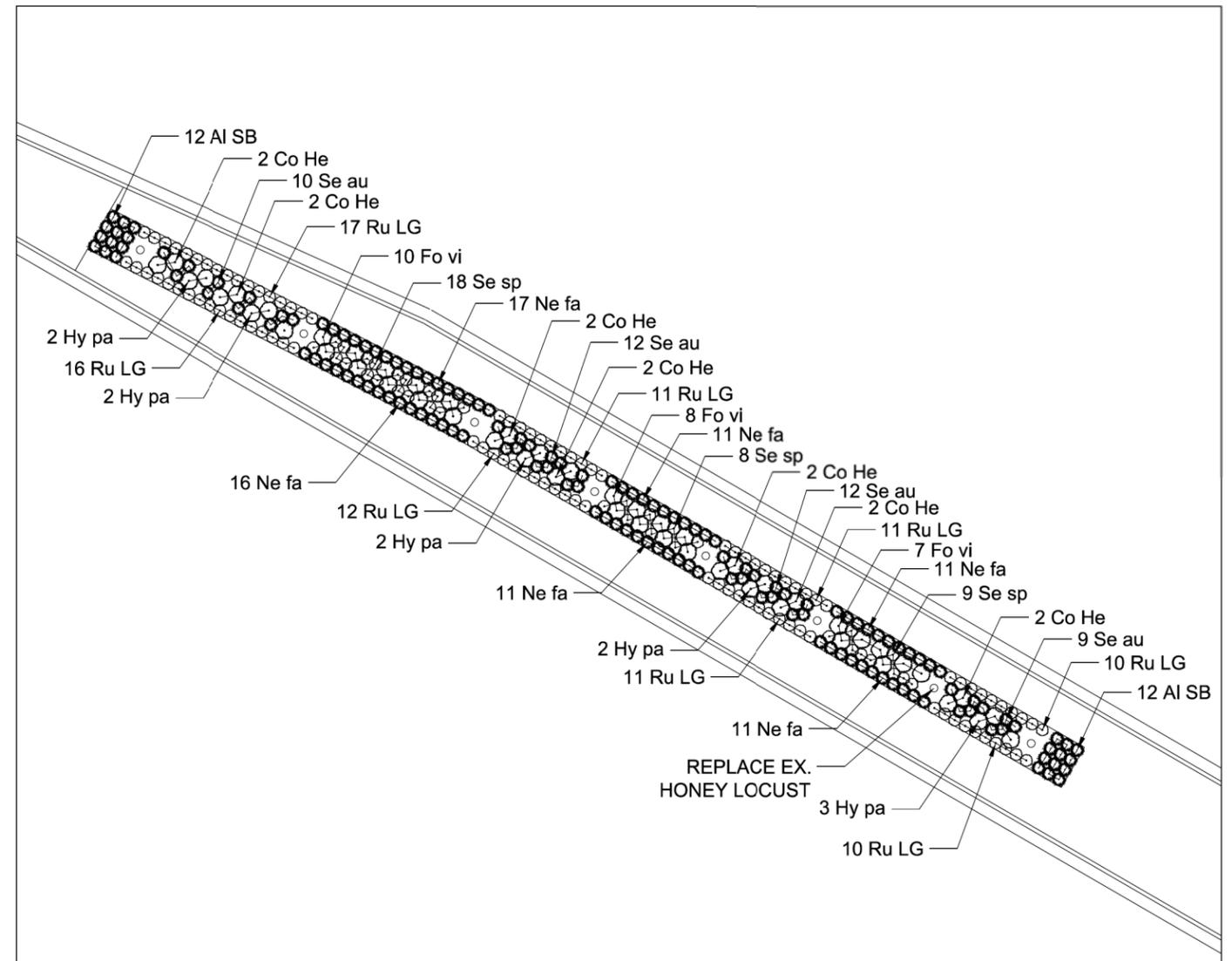
F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	48

CONTRACT NO. 61F07

ILLINOIS FED. AID PROJECT



EXISTING LANDSCAPE



PROPOSED LANDSCAPE



GENERAL CONSTRUCTION NOTES:

1. ALL ALTERATIONS MUST BE APPROVED BY THE LANDSCAPE ARCHITECT.
2. CONTRACTORS MUST VERIFY ALL QUANTITIES AND OBTAIN ALL PROPER PERMITS AND LICENSES FROM THE PROPER AUTHORITIES.
3. ALL MATERIAL MUST MEET INDUSTRY STANDARDS AND THE LANDSCAPE ARCHITECT HAS THE RIGHT TO REFUSE ANY POOR MATERIAL OR WORKMANSHIP.
4. LANDSCAPE ARCHITECT IS NOT RESPONSIBLE FOR UNSEEN SITE CONDITIONS.
5. PROVIDE POSITIVE DRAINAGE AT ALL TIMES. DO NOT OBSTRUCT NATURAL OR DESIGNED DRAINAGE FLOW PATTERN.
6. ALL PLANTINGS SHALL BE SPACED EQUAL DISTANT, BACKFILLED WITH SOIL IN A HOLE TWICE THE ROOTBALL DIAMETER, WATERED, FERTILIZED, PRUNED AND HAVE ALL TAGS AND ROPES REMOVED.
7. TREES SHALL BE STAKED AND GUYED AS NECESSARY AND CREATE WATERING SAUCERS AT BASE.
8. ALL MASS PLANTED SHRUB BEDS TO BE BERMED 2" TO 3" ABOVE GRADE AND MEET DRAINAGE REQUIREMENTS.
9. LAWN AND BED AREAS SHALL BE ROTOTILLED AND CLUMPS OF SOIL, AGGREGATES AND DEBRIS RAKED OUT AND REMOVED FROM THE SITE.
10. ALL BEDS SHALL BE EDGED, HAVE WEED PRE-EMERGENTS APPLIED AT THE RECOMMENDED RATE, AND SHREDDED HARDWOOD MULCH SPREAD AT A MINIMUM OF 3" DEPTH.
11. ALL DEBRIS SHALL BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.

Pay Code	Sym.	Plant List		Qty	Size	Cond.
A2004820		Deciduous Trees Gleditsia triacanthos inermis 'Skyline'	Skyline Honeylocust	1	2 1/2"	B & B
		Deciduous Shrubs				
C2C02224	Co He	Cotoneaster 'Hessei'	Hesse Cotoneaster	14	2' Width	Cont.
	Fo vi	Forsythia x intermedia 'Nimbus'	Show-off Sugar Baby Forsythia	25	18 Height	Cont.
C2003600	Hy pa	Hydrangea paniculata 'SMHPLQF'	Hydrangea Little Quick Fire	11	#3B	Cont.
		Perennials				
K0012990	Al SB	Allium 'Summer Beauty'	Summer Beauty Onion	24	Gallon	Cont.
K0012990	Ne fa	Nepeta x faassenii 'Junior Walker'	Junior Walker Catmint	77	Gallon	Cont.
K0012990	Ru LG	Rudbeckia fulgida 'Little Goldstar'	Little Goldstar Black-eyed Susan	98	Gallon	Cont.
K0012990	Se sp	Sedum spectabile 'Autumn Fire'	Autumn Fire Sedum	35	Gallon	Cont.
K0012990	Se au	Sesleria autumnalis	Autumn Moor Grass	43	Gallon	Cont.

GENERAL NOTES

1. Illinois Department of Transportation (IDOT) Standard Specifications for Road and Bridge Construction, Adopted April 1, 2016, and latest Supplemental Construction Plans and Subsequent Details are all to be considered as part of the Contract. Incidental Items or Accessories necessary to complete this work may not be specifically noted but are considered a part of this Contract.
2. No Construction Plans shall be used for Construction unless specifically marked For Construction. Prior to commencement of construction, the Contractor shall verify all dimensions and conditions affecting the work with the actual conditions. If there are discrepancies between the job site and what is shown on the construction plans, the contractor must immediately report to Engineer before doing any work, otherwise the Contractor shall assume full responsibility. In the event of disagreement between the plans and existing conditions and or details, the Contractor shall secure written instruction from the Engineer prior to proceeding with any part of the work affected by omissions or discrepancies. In failing to secure such instruction, the Contractor will be considered to have proceeded at his own risk and expense. In the event of any doubt or questions arising with respect to the true meaning of the Construction Plans or Specifications, the decision of the Engineer shall be final and conclusive.
3. Contractor shall verify all topographic information and grade elevations adjacent to bridge prior to proceeding, inform Engineer of any variation.
4. All details and sections shown on the drawings are typical and shall be used wherever a similar condition occurs unless noted otherwise.
5. The stability and strength of the completed structure depends upon the interaction of various connected parts such as braces. Contractor shall provide temporary bracing and/or shoring as necessary to complete the work.
6. The existing Bridge plans are available at the City Public Works Building upon written request. Contact Don Wenzel AT (847) 963-0500.
7. The top surface of the existing precast concrete beams shall be prepared in accordance with waterproofing manufacturer's recommendations.
8. Removal of existing waterproofing shall be included in the cost of Hot-Mix Asphalt Surface Removal, Variable Depth.
9. Preparation of tops of the existing precast concrete beams shall be included in the cost of Waterproofing Membrane System.
10. Removal of the existing grout, preparation and installation of repair material shall be included in the cost of "Keyway Repair". The extent of removal of the existing grout shall be as directed by the Engineer.
11. The keyway along the stage construction shall be aligned with clamping devices. This keyway shall be the last keyway to be repaired. The contractor shall furnish all material for clamping devices, including sufficient 1/16" and 1/8" steel shim plates to adjust for differential elevation between the two deck beams.
12. It is unknown if a preformed joint seal is present between the approach slab and adjacent pavement connector. A quantity has been included in these plans for Preformed Joint Seal 4" in the event there is an existing seal and its condition warrants replacement.
13. Hot-Mix Asphalt Surface Removal (Deck) over the beams shall be completed in accordance with the associated Special Provision. Milling of the hot-mix asphalt surface over the bridge deck shall not be allowed. Removal shall be completed in a manner such that the concrete beams are not damaged. Any damage done to the concrete beams shall be corrected at the Contractor's expense.
14. After the existing wearing surface has been removed, all beams shall be inspected by the Engineer to verify their condition.
15. After the existing overlay has been removed, all deteriorated grout in the dowel rod holes and grout joints at the ends of the beams should be removed from the dowel rod holes and grouted joints. Cracks in the existing grout to remain in the dowel holes should be sealed with epoxy. Epoxy grout should then be used to replace deteriorated grout. Repairing dowel rod holes and grouted joints at the end of the beams shall not be measured for payment separately, but shall be included in the cost of Waterproofing Membrane System.

CONSTRUCTION

1. All work shall be done in accordance with Illinois Department of Transportation (IDOT) Standard Specification For Road and Bridge Construction, Adopted April 1, 2016, and latest Supplemental Specifications and recurring Special Provisions, unless noted otherwise.
2. Do not scale dimensions for construction. Scale, if shown, applies only to full size drawings.
3. Any information concerning type or location of underground and other utilities is not guaranteed to be accurate or all inclusive. The Contractor is responsible for making his own determinations as to the type and location of the utilities as may be necessary to avoid damage thereto. Contractor shall call J.U.L.I.E. and City of Rolling Meadows Public Works prior to excavation.
4. Shop working or layout drawings pertaining to the construction of the work, as may be required, shall be submitted to the Engineer for approval prior to the start of construction.
5. Upon completion, the contractor shall collect and remove all construction debris and excess material from the site. Damaged trees, shrubs, and other landscape features resulting from construction activities shall be replaced or repaired.

REMOVAL AND PROTECTION

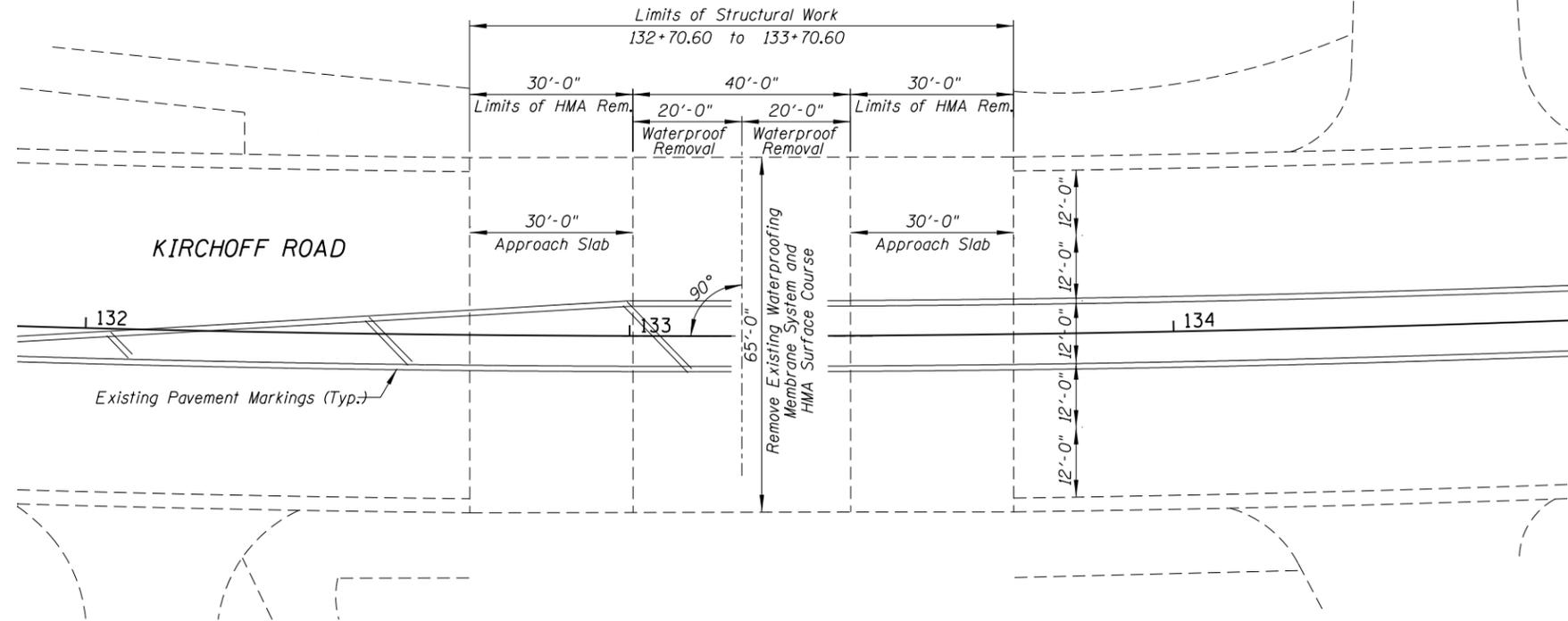
1. Provide shoring and bracing for the existing structural elements during partial removal to avoid any damage to the other existing facilities.
2. Existing structures shall be protected from overloading due to construction loadings.
3. Damage to adjacent facilities during construction shall be repaired at no additional cost to the owner.

SUMMARY OF QUANTITIES

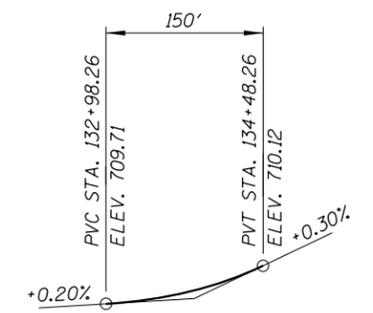
ITEM NO.	ITEM	UNIT	QUANTITY
40603340	Hot-Mix Asphalt Surface Course, Mix "D", N70	Ton	79
52000050	Preformed Joint Seal 4"	Foot	130
58100200	Waterproofing Membrane System	Sq Yd	290
58300100	Portland Cement Mortar Fairing Course	Foot	640
20004556	Hot-Mix Asphalt Surface Removal (Deck)	Sq Yd	290
20032700	Keyway Repair	Foot	640

**KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925**

FILE NAME =	USER NAME = wbiofuss	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\ROLLINGMEADOWS\980361.BR\BRI00\980361BRI95\Struct\SI.GEN NOTES.98361BRI95.SHT	DRAWN - DOC	REVISED -	3517			17-00121-00-RS	COOK	66	50		
Default	PLOT SCALE =	CHECKED - MM	REVISED -			CONTRACT NO. 61F07					
	PLOT DATE = 8/31/2018	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
						ILLINOIS FED. AID PROJECT					



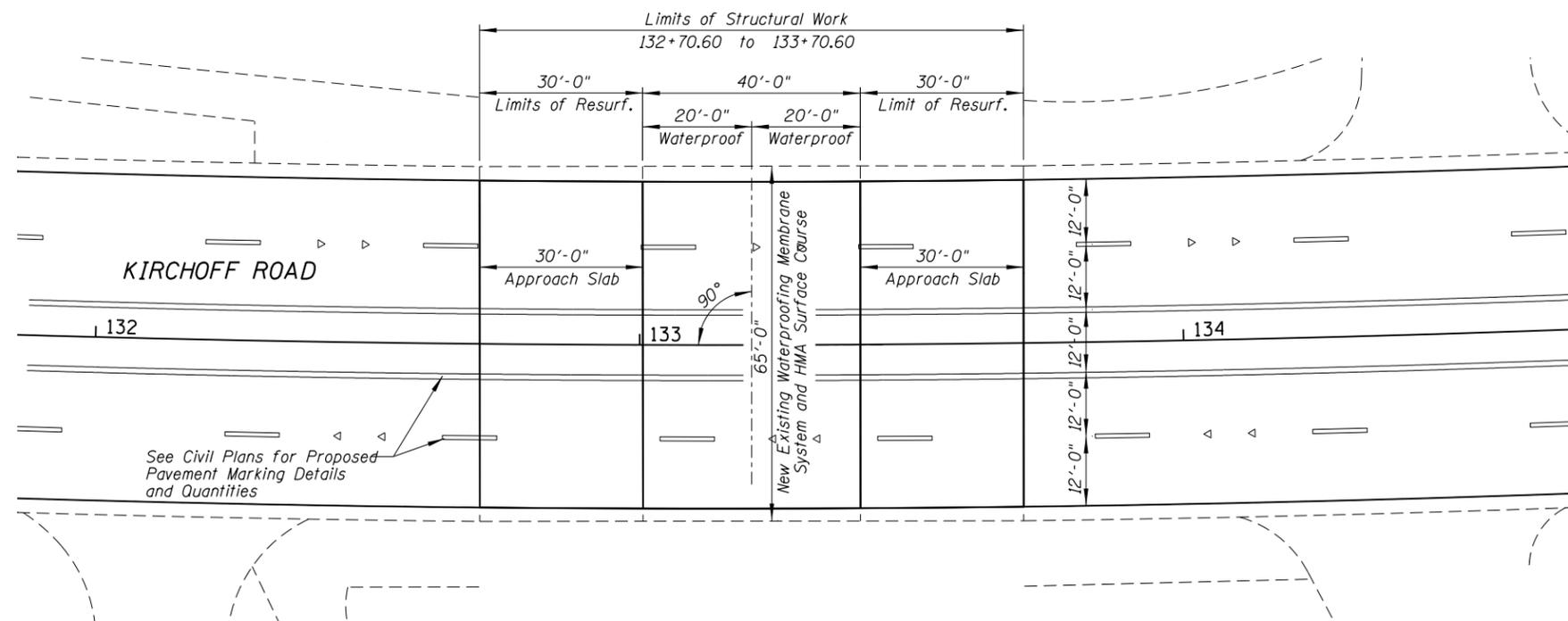
EXISTING PLAN



PROFILE GRADE
Along \bar{C} of Kirchoff Road

LOADING HS20-44

Future Wearing Surface Allowance = 25 #/ Sq.Ft.

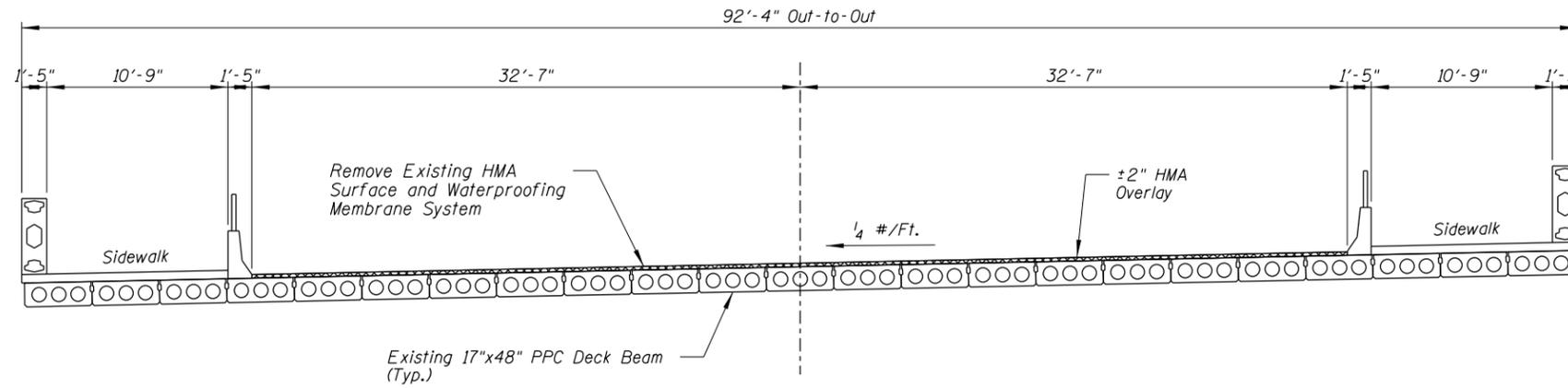


PROPOSED PLAN

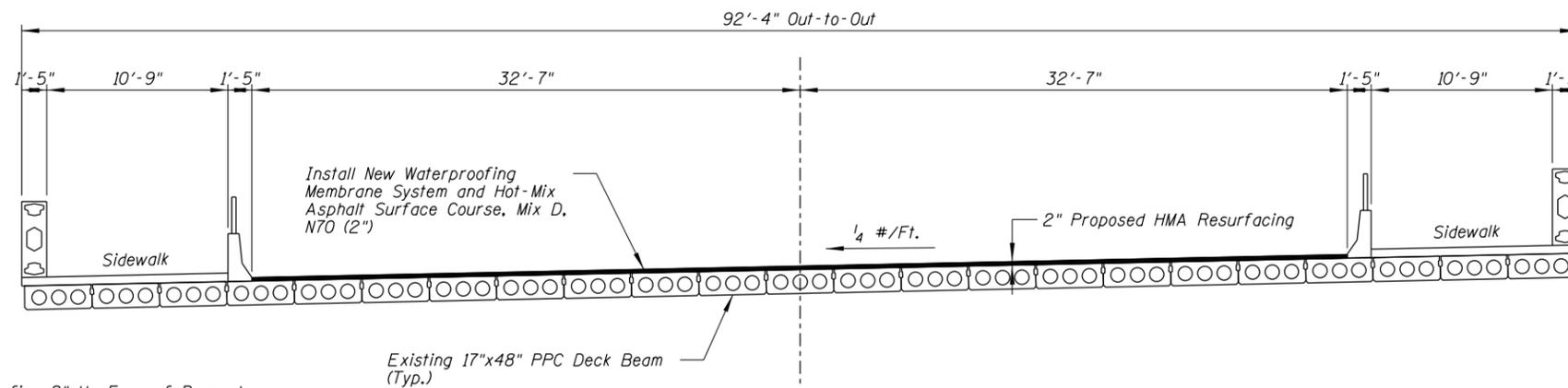


**KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925**

FILE NAME =	USER NAME = wbioftus	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING				F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\BRI00\980361BR195\Struct\S2_PLAN_980361BR195.SHT		DRAWN - DOC	REVISED -		3517	17-00121-00-RS	COOK	66	51				
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	PLOT DATE = 8/31/2018	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



EXISTING CROSS SECTION



PROPOSED CROSS SECTION

NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Parapet

**KIRCHOFF ROAD BRIDGE
OVER SALT CREEK
STRUCTURE NO. 016-7925**

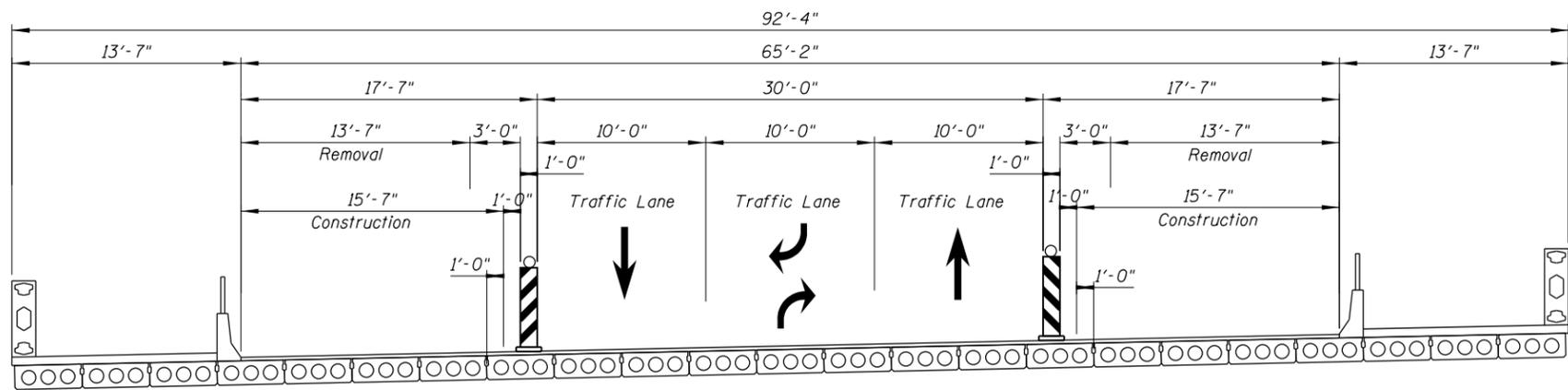
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	PLOT DATE = 8/31/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

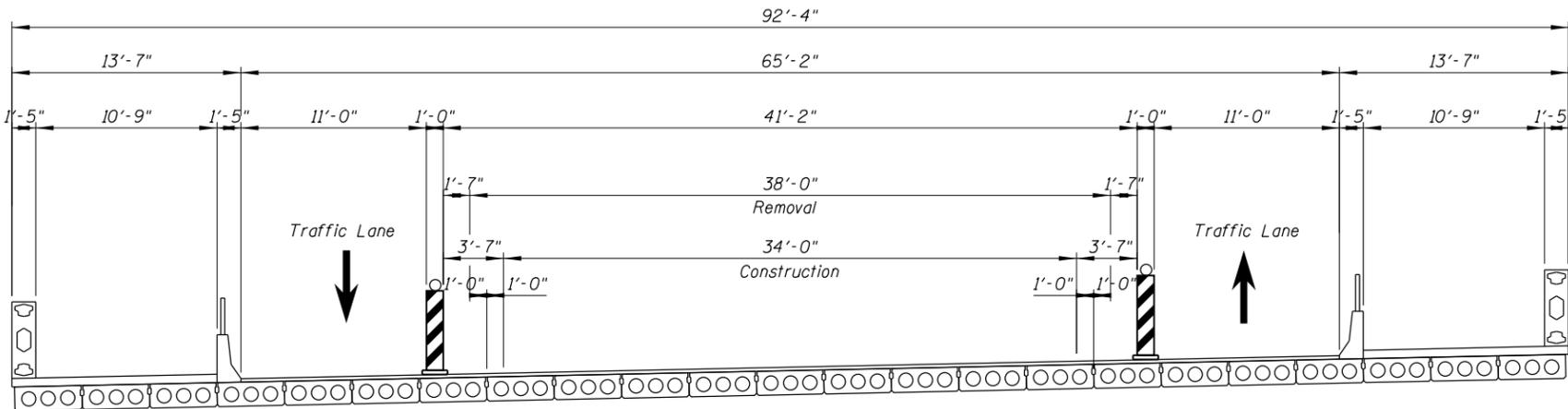
**KIRCHOFF ROAD OVER SALT CREEK
BRIDGE RESURFACING**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	52
CONTRACT NO. 61F07				
ILLINOIS FED. AID PROJECT				

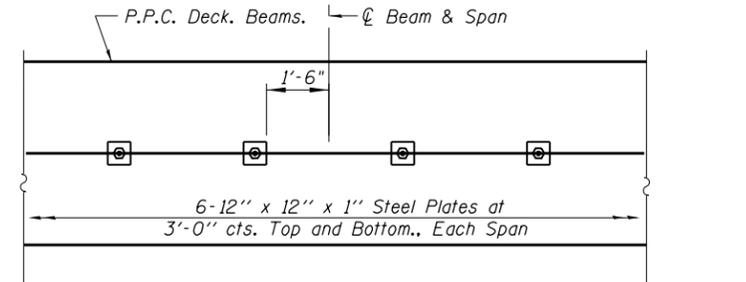


STAGE II CONSTRUCTION

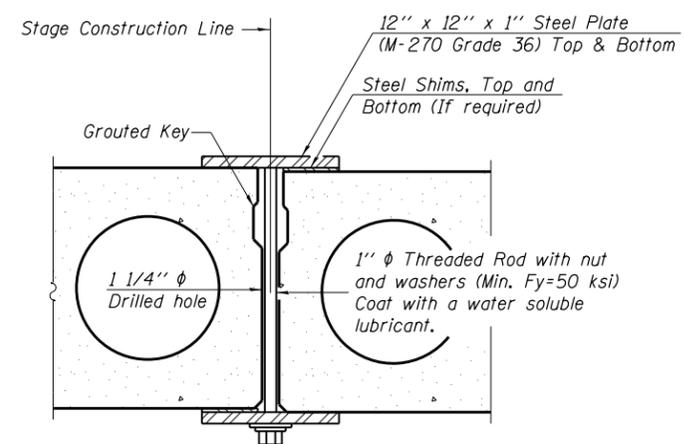


STAGE III CONSTRUCTION

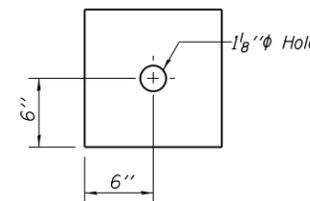
NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Parapet



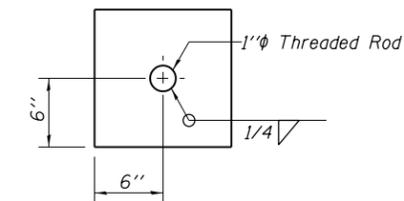
PLAN



SECTION



BOTTOM CLAMPING PLATE

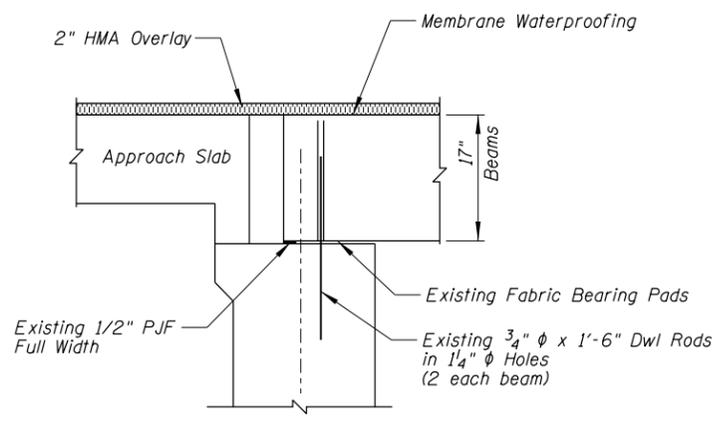


TOP CLAMPING PLATE

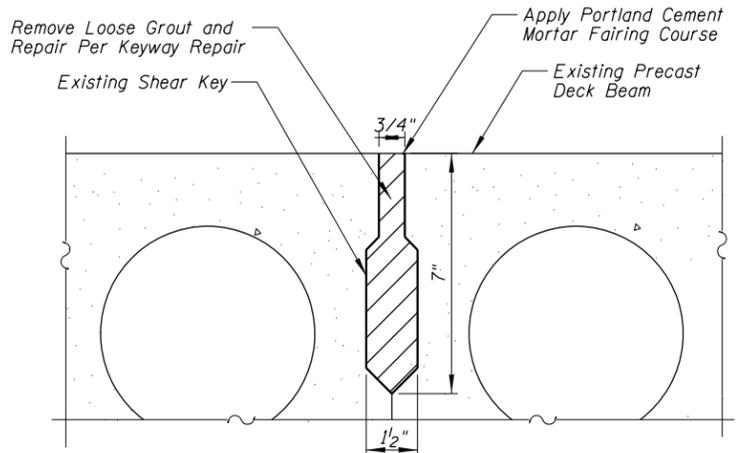
SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

1. Clamping device shall be installed per Section 504 of Standard Specifications at Stage Construction Line.
2. The Cost of installing and removing clamping devices is included with "Keyway Repair".

NOTES:
1. Terminate Membrane Waterproofing 2" Up Face of Curb



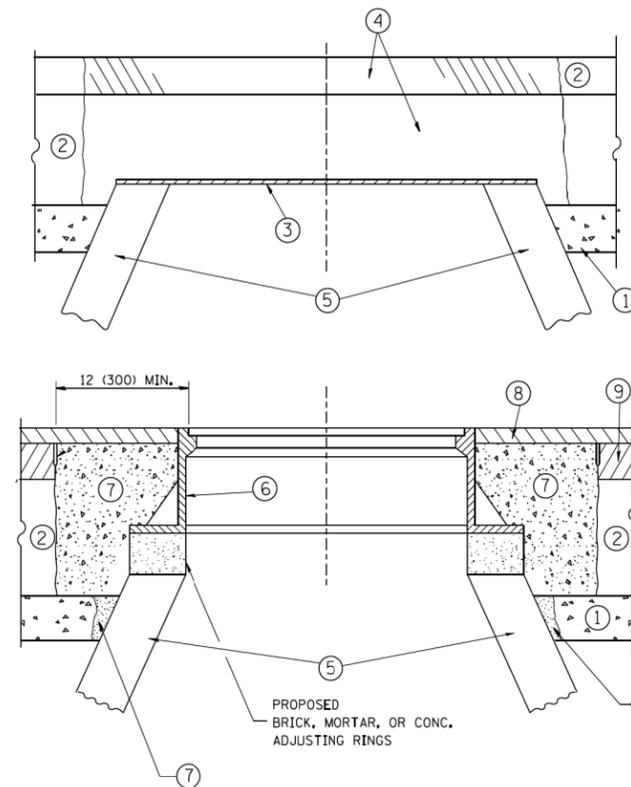
ABUTMENT SECTION



SHEAR KEY REPAIR DETAIL

KIRCHOFF ROAD BRIDGE OVER SALT CREEK STRUCTURE NO. 016-7925

FILE NAME =	USER NAME = wbioftus	DESIGNED - DOC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	KIRCHOFF ROAD OVER SALT CREEK BRIDGE RESURFACING				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ROLLINGMEADOWS\980361.BR\5\BR100\5\980361BR195\Struct\54_DET_98361BR195.SHT		DRAWN - DOC	REVISED -						3517	17-0021-00-RS	COOK	66	53
Default	PLOT SCALE =	CHECKED - MM	REVISED -		CONTRACT NO. 61F07								
	PLOT DATE = 8/31/2018	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

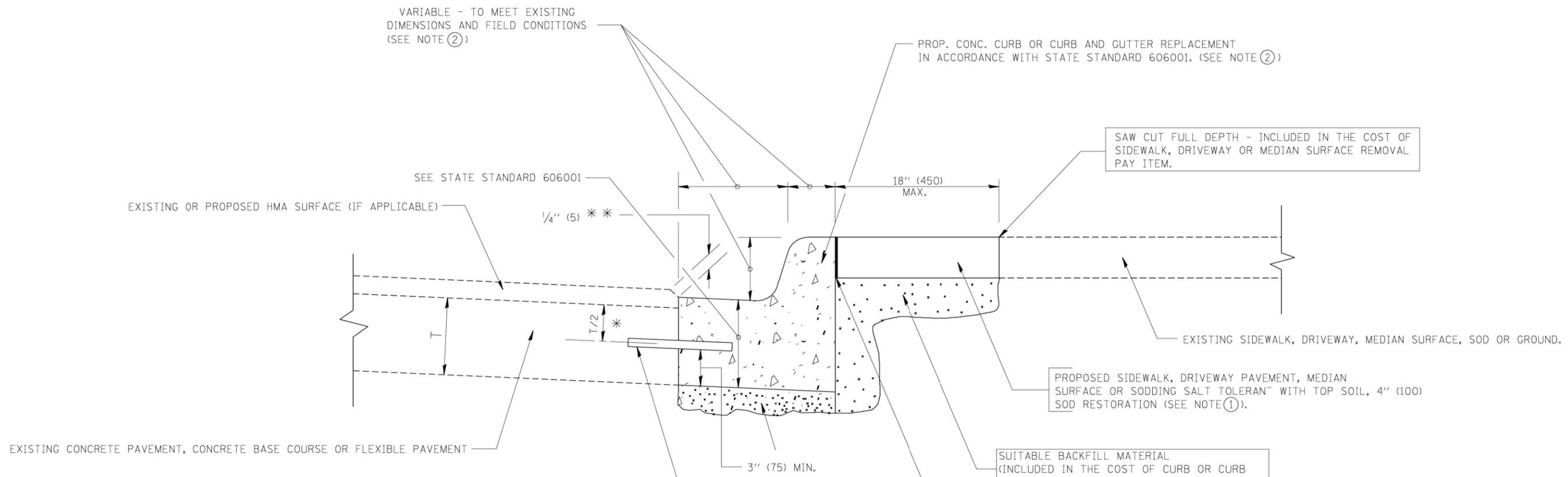
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FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
ca\pw_work\pwidot\bauerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/68,5000' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	54
BD600-03 (BD-8)		CONTRACT NO. 61F07		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

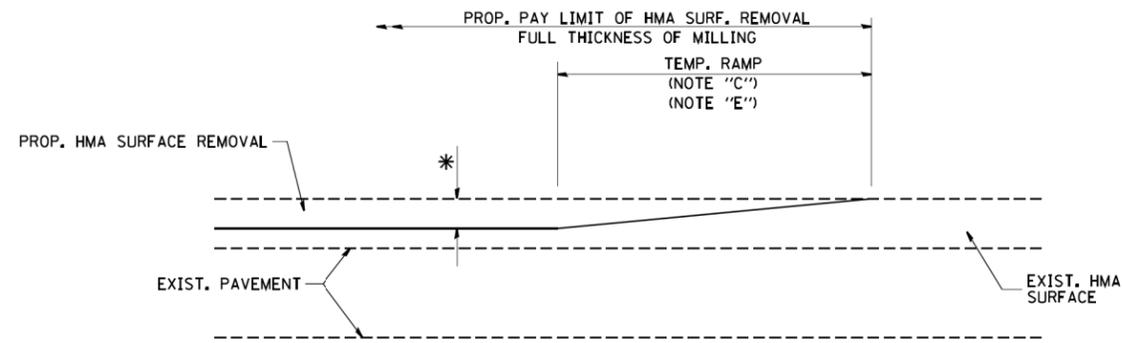
** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

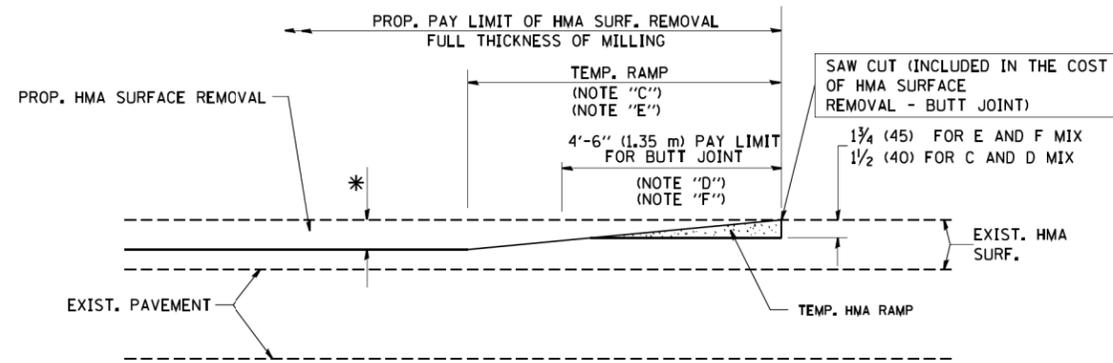
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A. RTE. 3517	SECTION 17-00121-00-RS	COUNTY COOK	TOTAL SHEETS 66	SHEET NO. 55	
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PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		



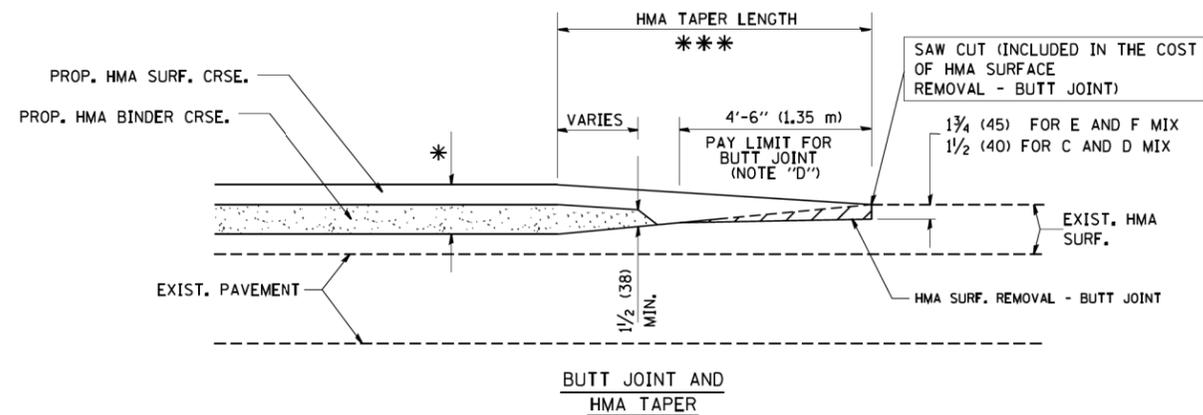
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

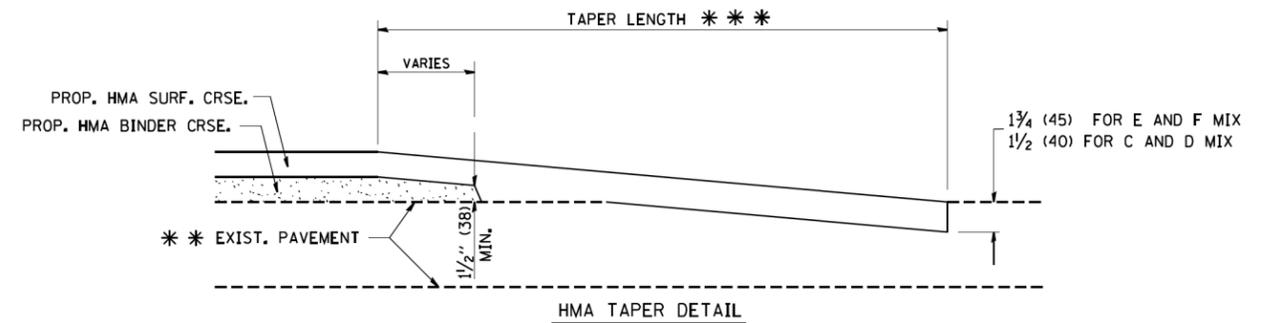
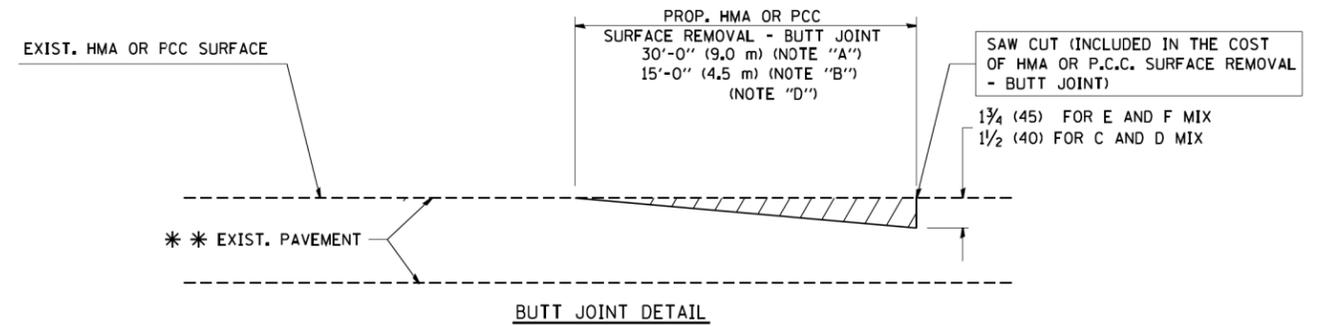


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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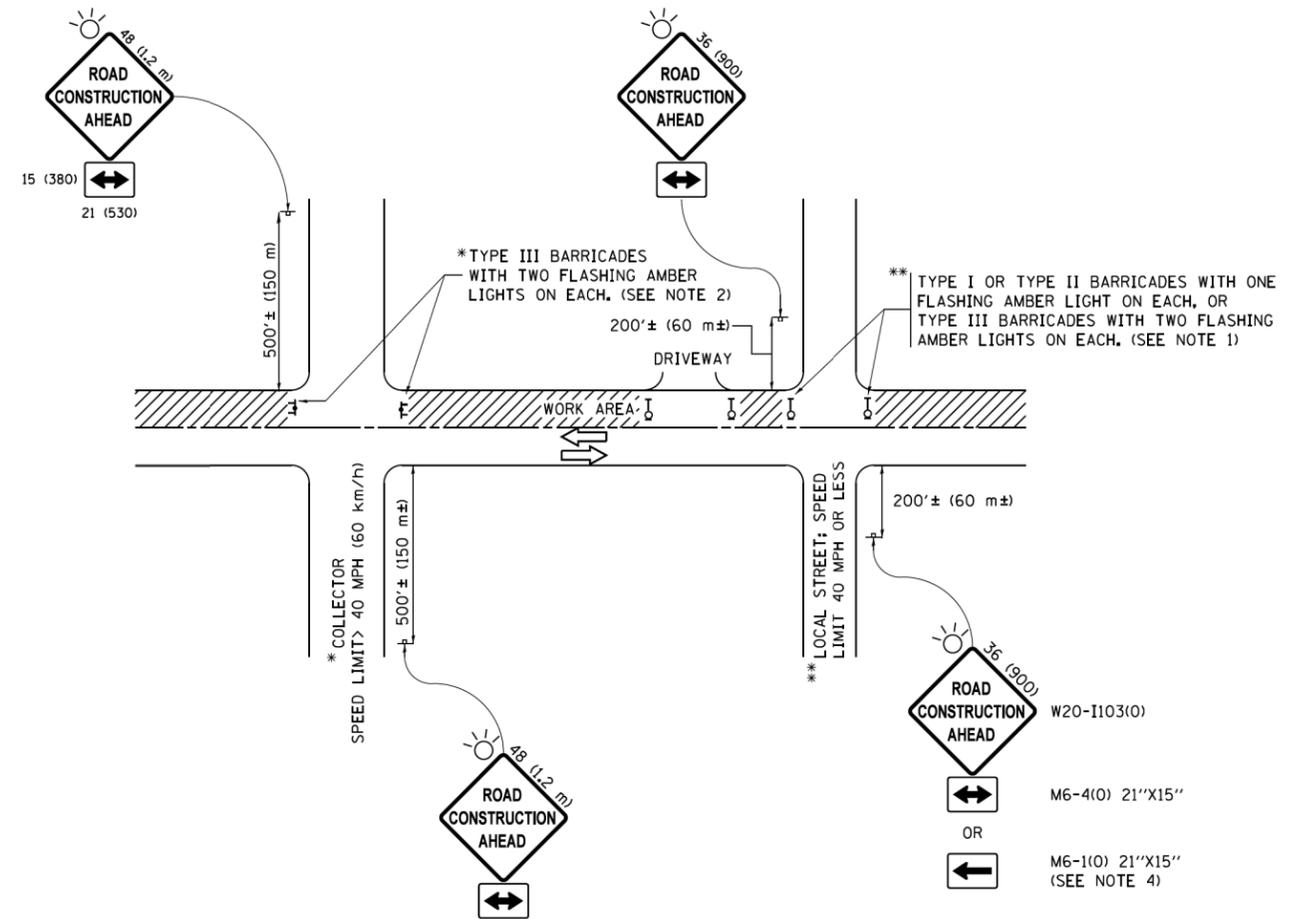
DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	56
BD400-05 BD32			CONTRACT NO. 61F07	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

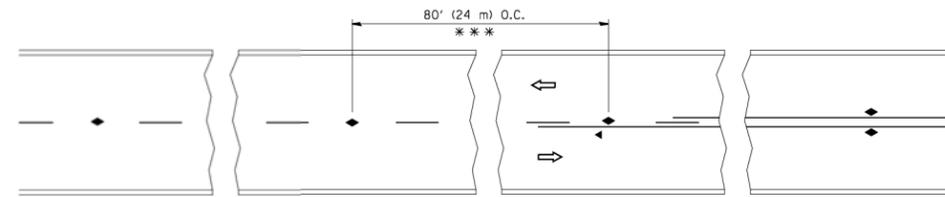
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	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

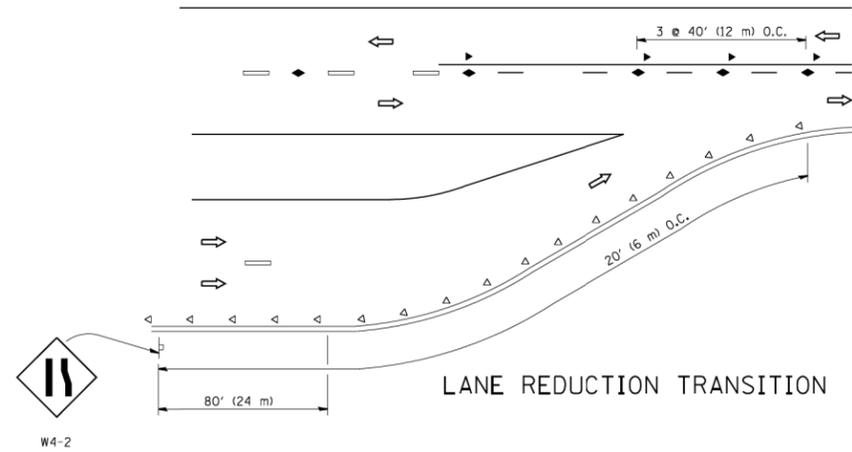
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

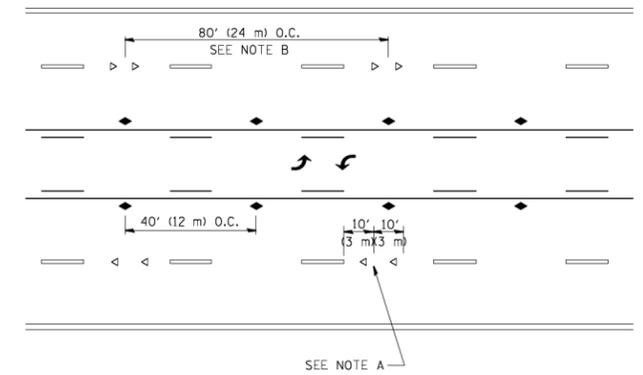
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 61F07	
ILLINOIS FED. AID PROJECT				



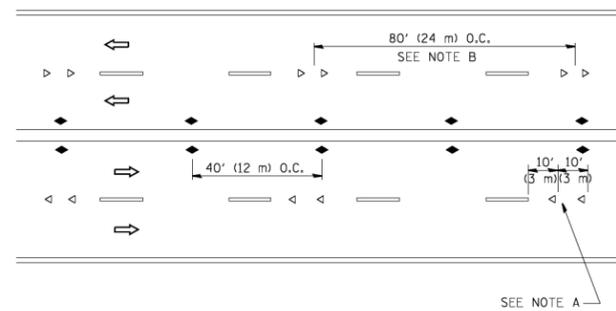
TWO-LANE/TWO-WAY



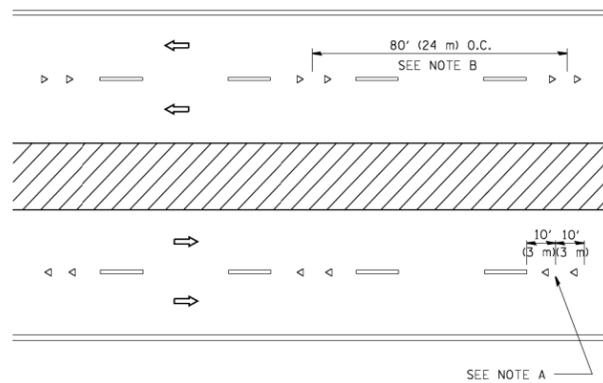
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

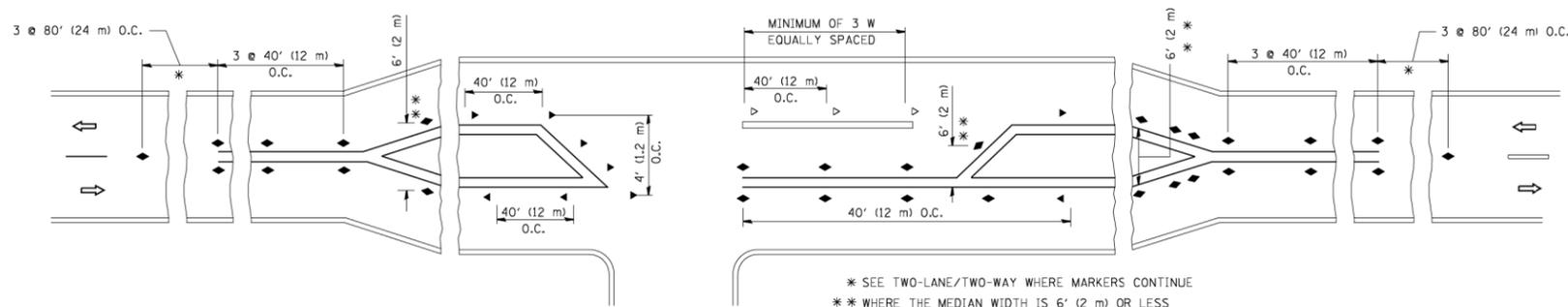
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\leyse\d0108315\tcl1.dgn	USER NAME = leyse	DESIGNED -	REVISED -T, RAMMACHER 09-19-94
		DRAWN -	REVISED -T, RAMMACHER 03-12-99
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -T, RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED -C, JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	58
TC-11			CONTRACT NO. 61F07	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

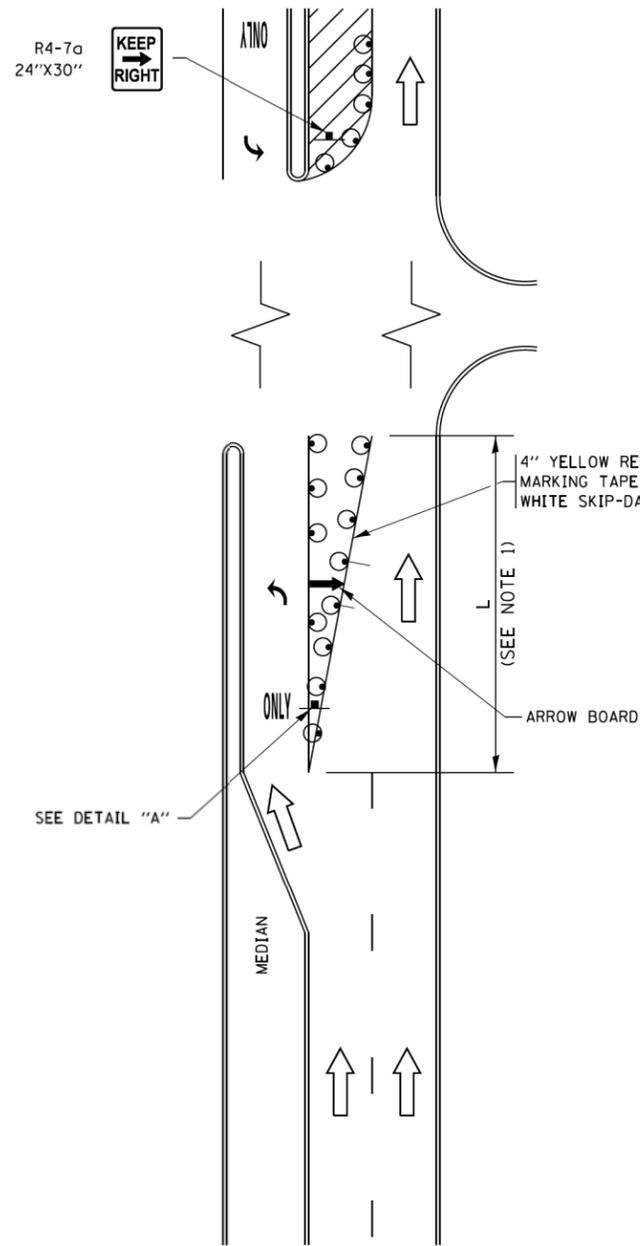
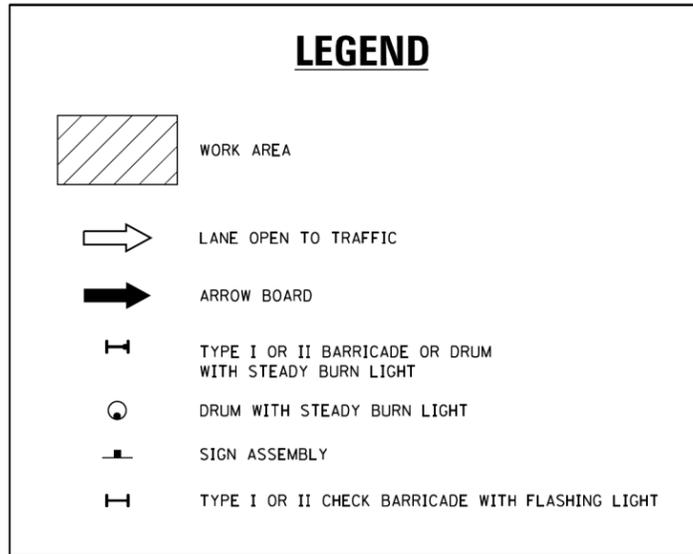


FIGURE 1

LEGEND



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

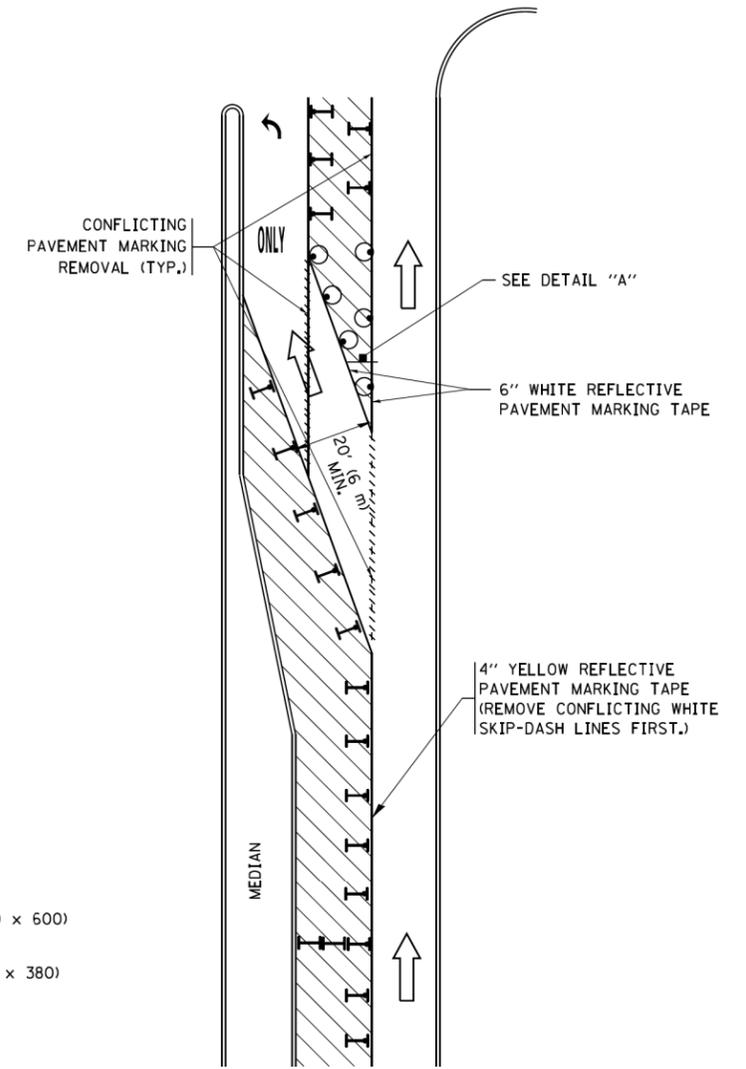
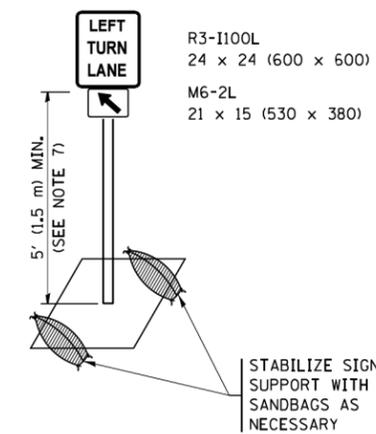


FIGURE 2

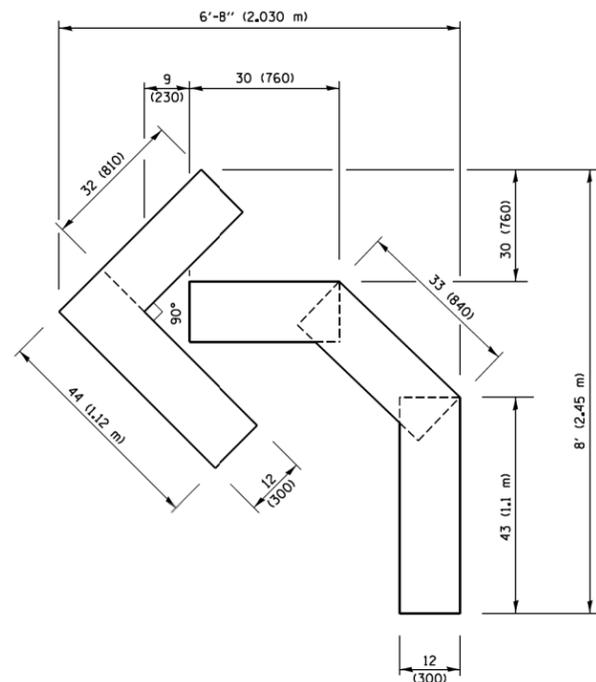


DETAIL A

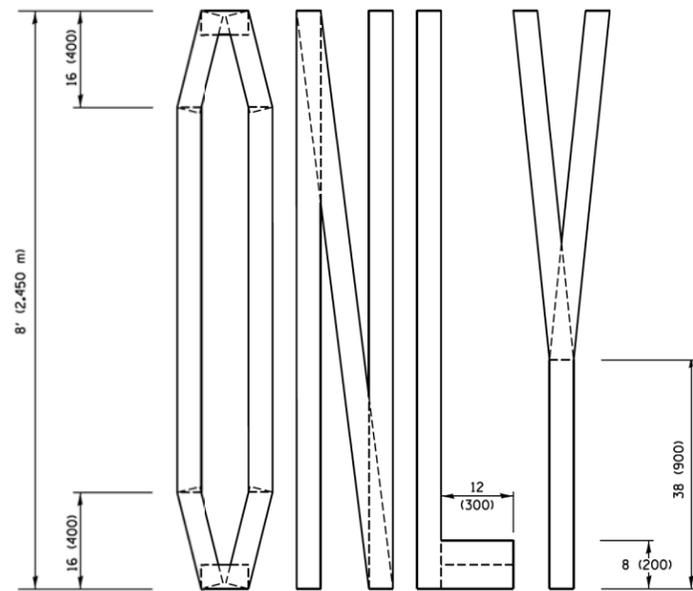
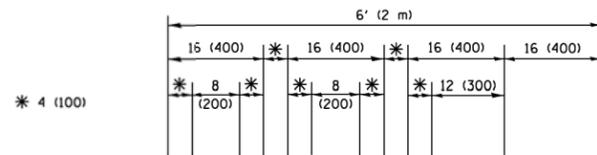
All dimensions are in inches (millimeters) unless otherwise shown.

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Default	Documents\IDOT Offices\District 1\Projects\Distric	REVISED - A. HOUSEH 10-07-95	REVISED - A. SCHUETZE 07-01-13			3517	17-00121-00-RS	COOK	66	60
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	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

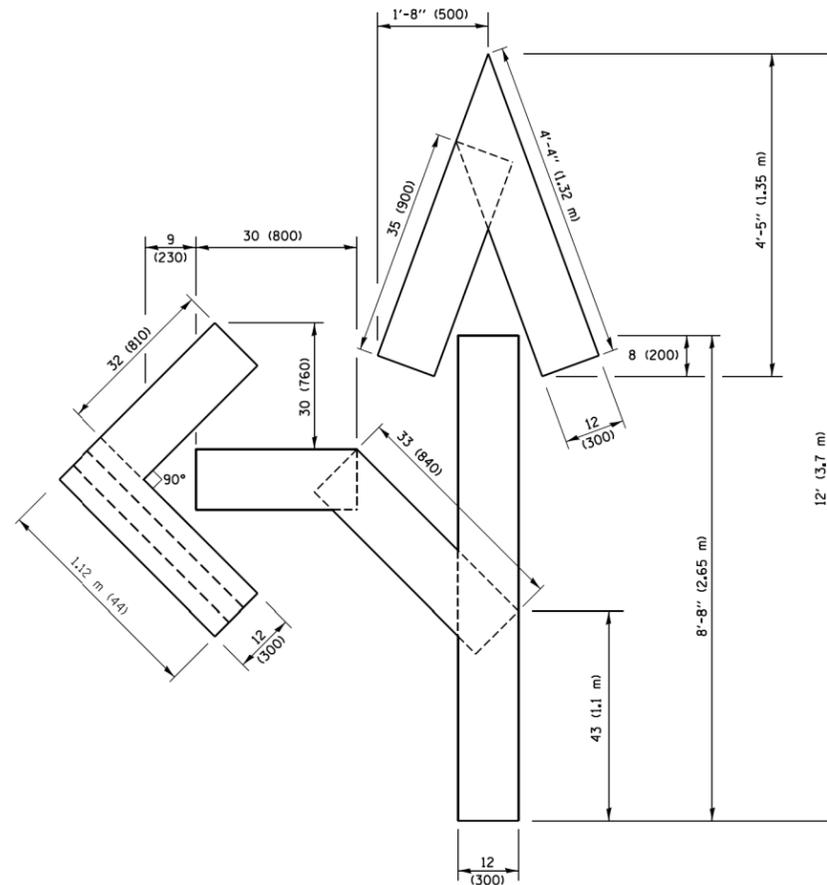
ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)



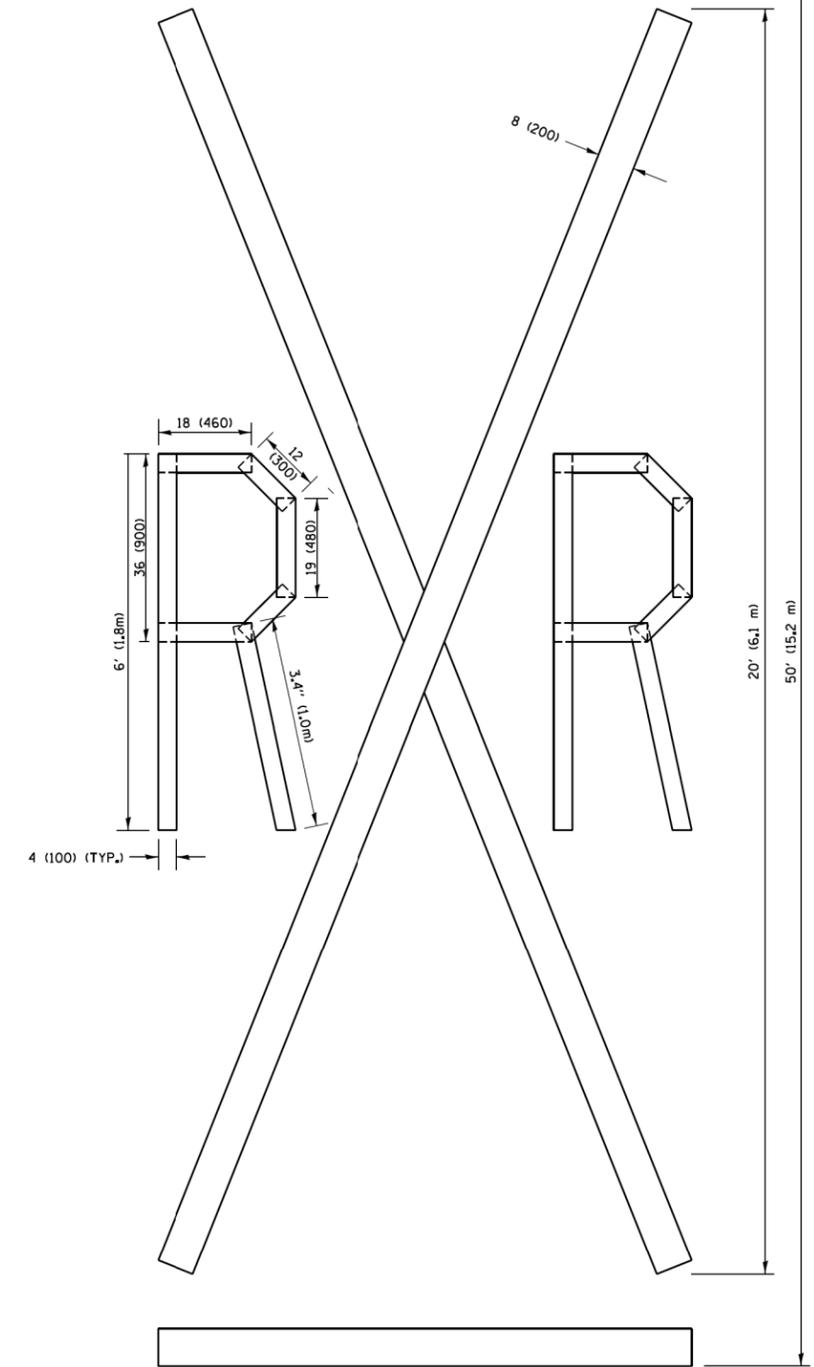
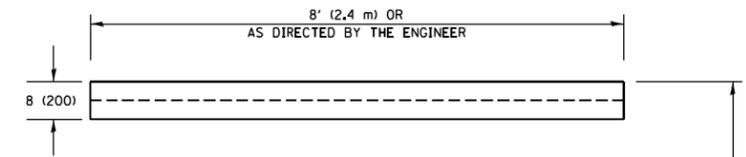
QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

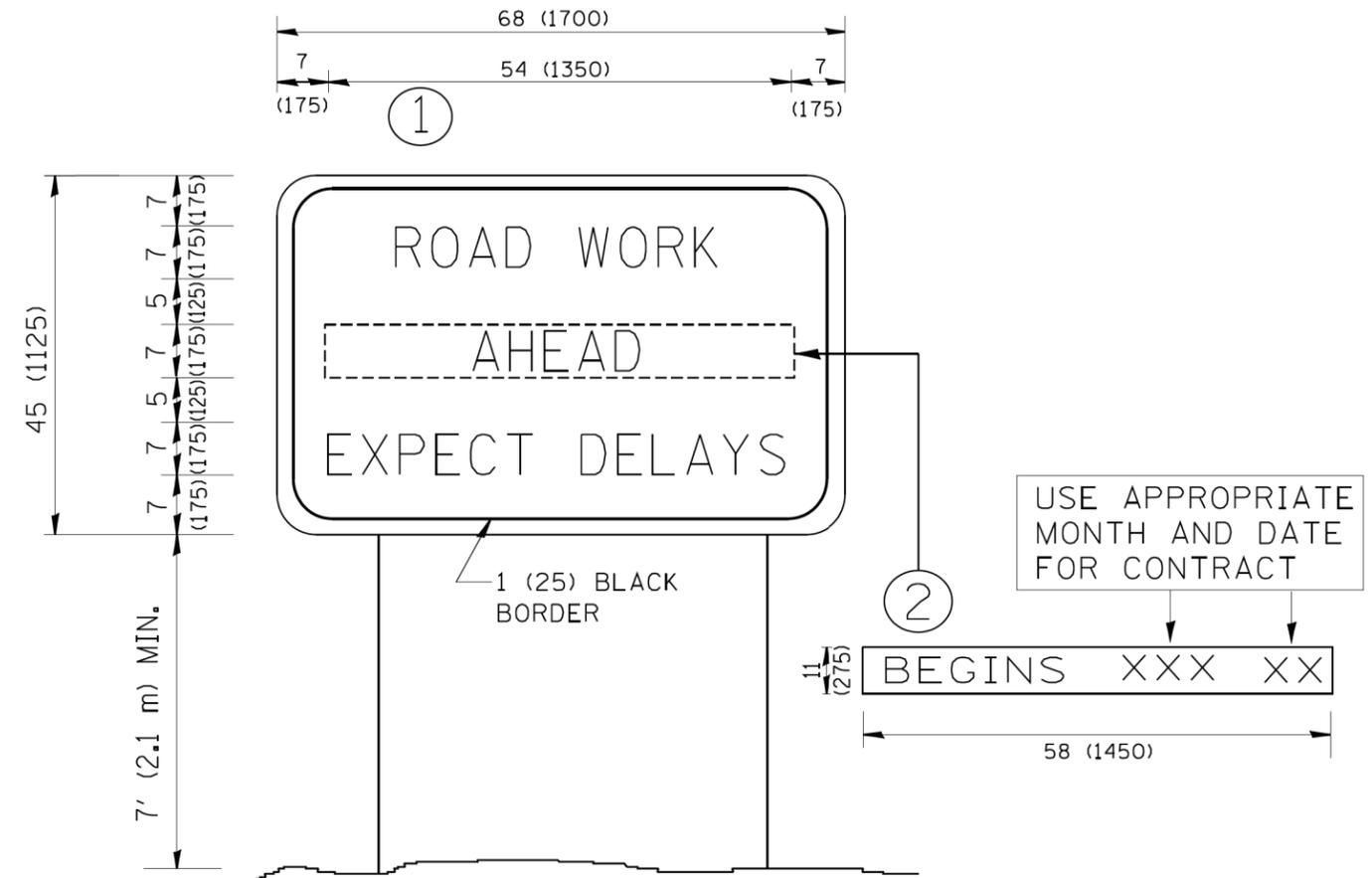
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PROJECT =	DOCUMENTS\DOT Offices\District 1\Projects\Dist	CHECKED -	REVISED -E. GOMEZ 08-28-00
PLOT SCALE = 50,0000 ' / m	DATE - 09-18-94	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00
PLOT DATE = 9/15/2016	DATE - 09-18-94	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	61
TC-16			CONTRACT NO. 61F07	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

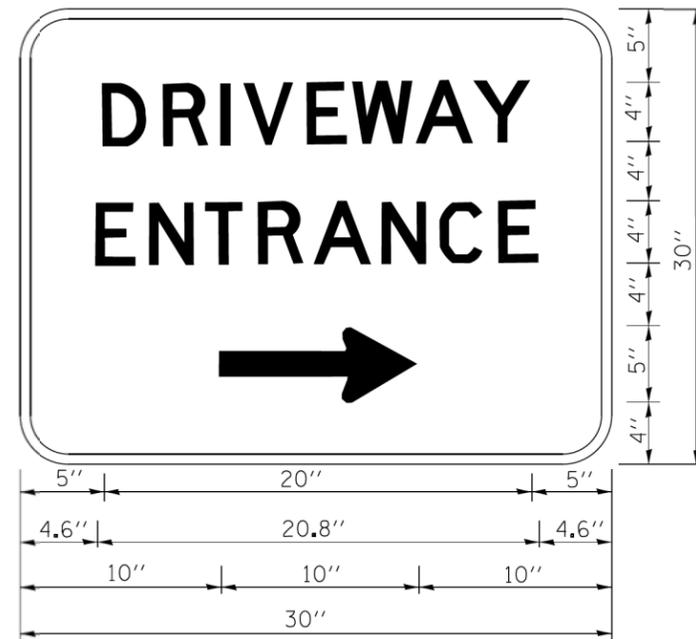
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R, MIRS 09-15-97
		DRAWN -	REVISED - R, MIRS 12-11-97
	PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED - T, RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C, JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 3517	SECTION 17-00121-00-RS	COUNTY COOK	TOTAL SHEETS 66	SHEET NO. 62
TC-22		CONTRACT NO. 61F07		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

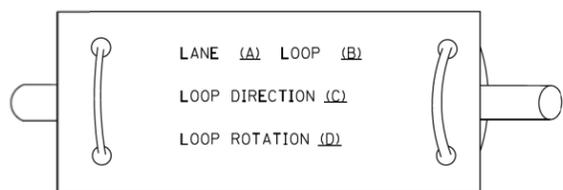
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = geglianob	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\gdg\geglianob\108315\108315.dgn		DRAWN -	REVISED -		3517	17-00121-00-RS	COOK	66	63			
	PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -		TC-26			CONTRACT NO. 61F07				
	PLOT DATE = 12/13/2012	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

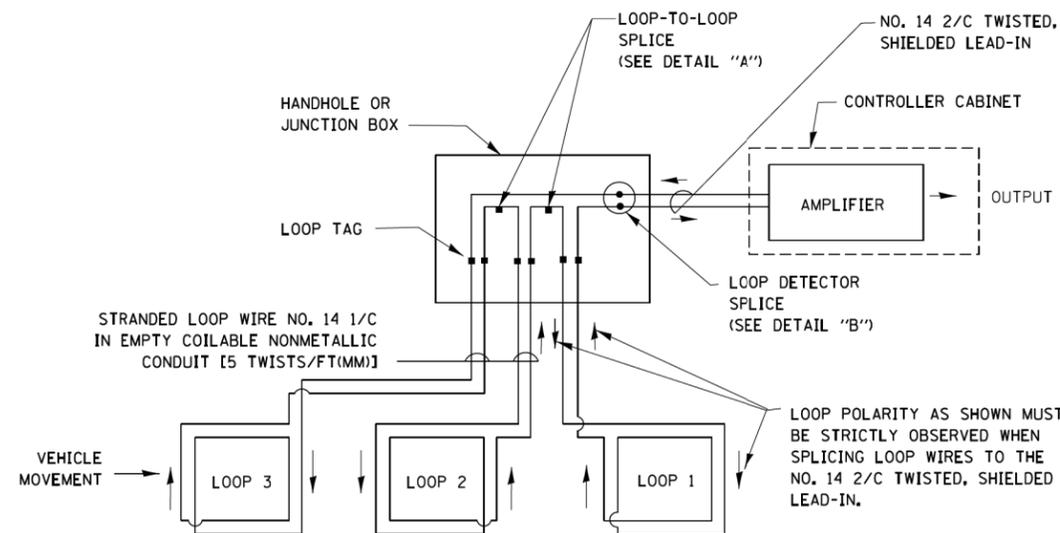
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

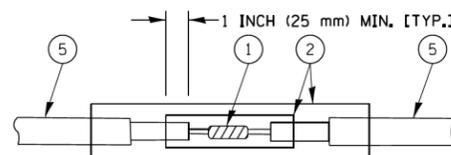


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

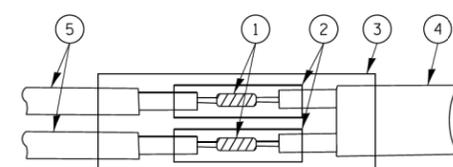


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

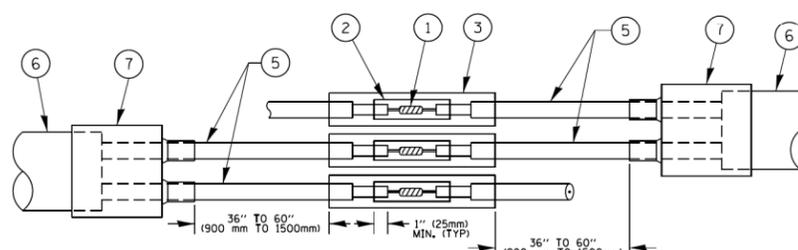


DETAIL "A"
LOOP-TO-LOOP SPLICE

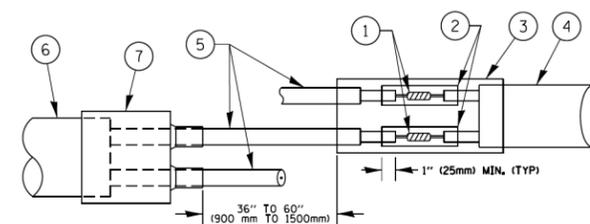


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

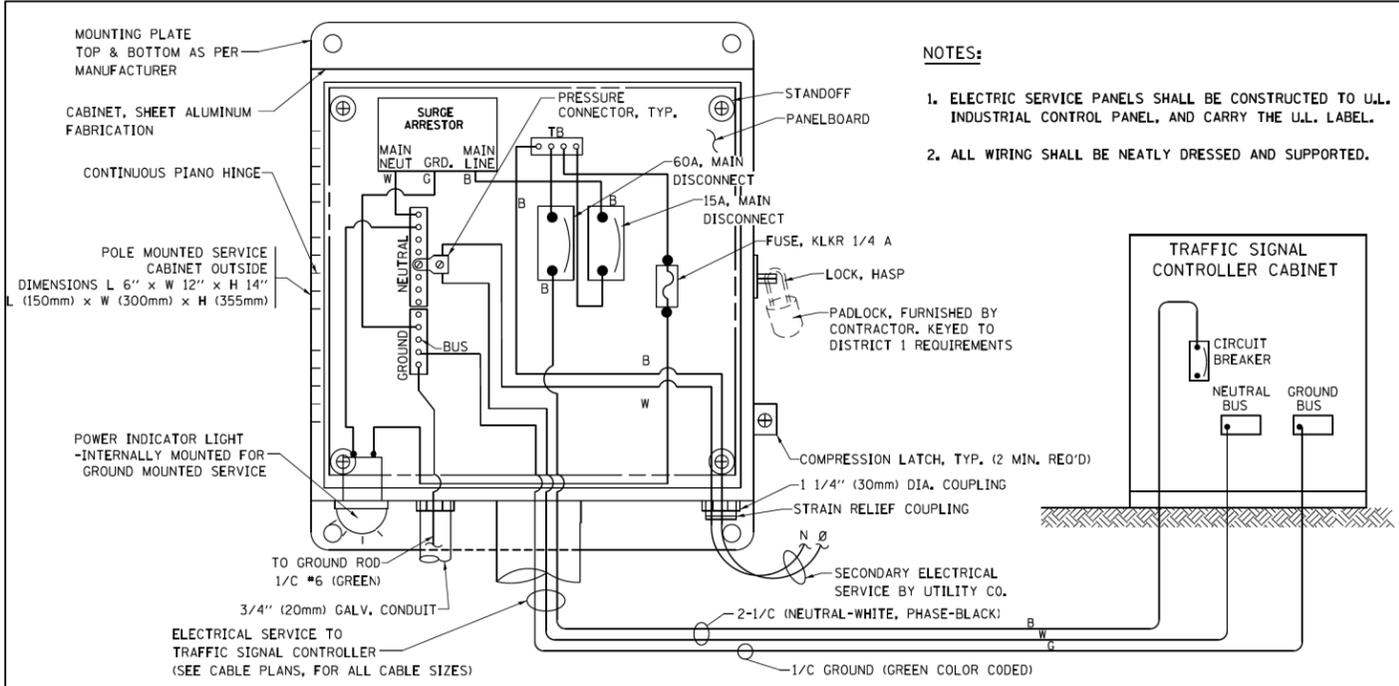
- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
ca:\pwork\pwork\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 58.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

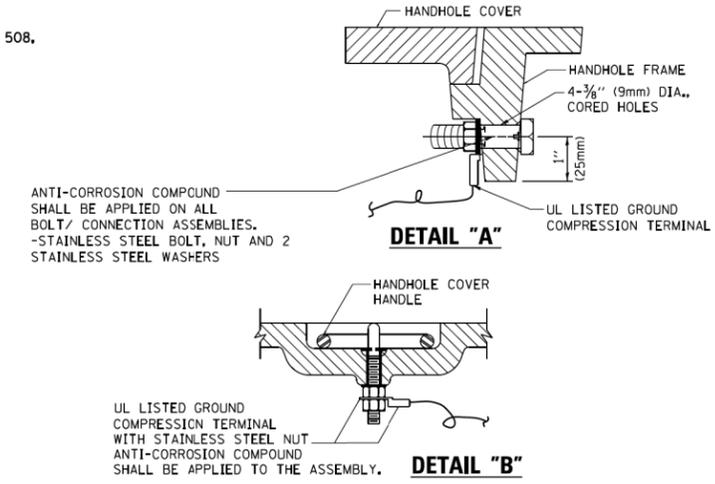
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 2 OF 7 SHEETS
STA.	TO STA.

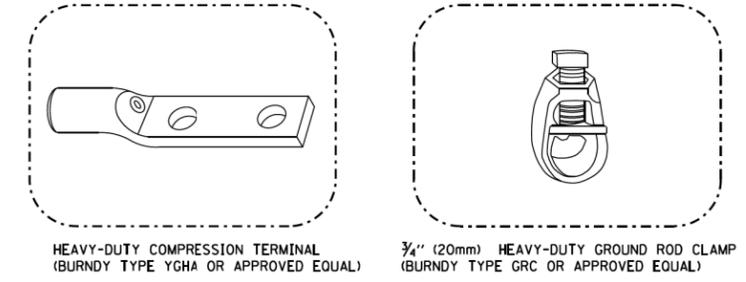
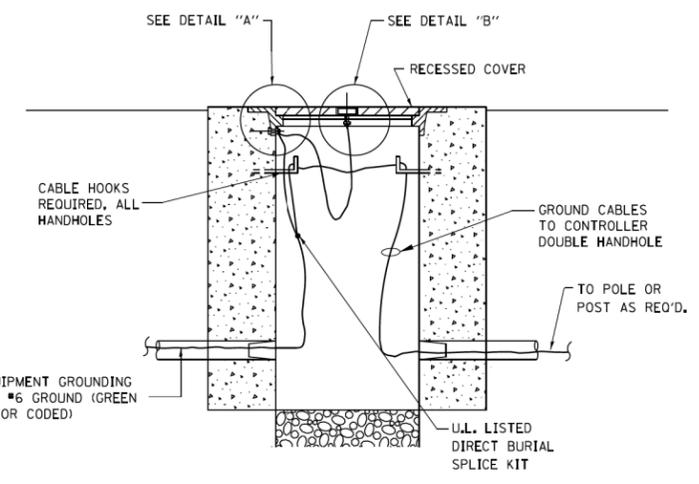
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3517	17-00121-00-RS	COOK	66	64
TS-05			CONTRACT NO. 61F07	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



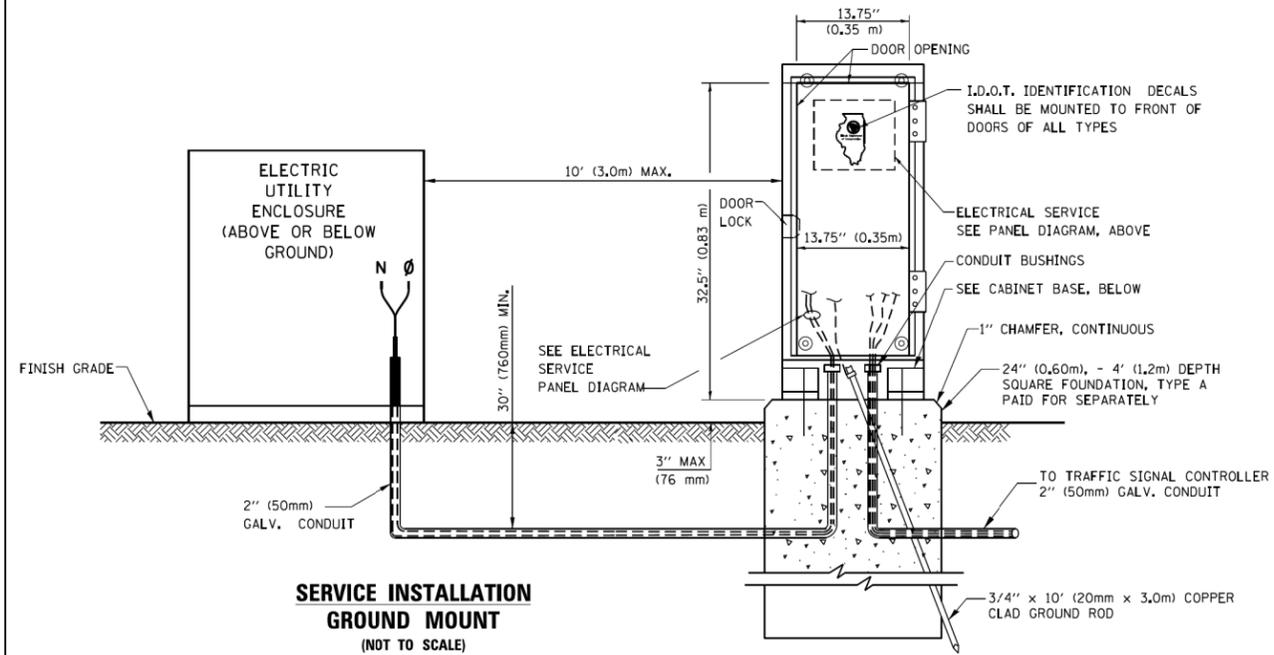
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
 2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
 3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
 4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

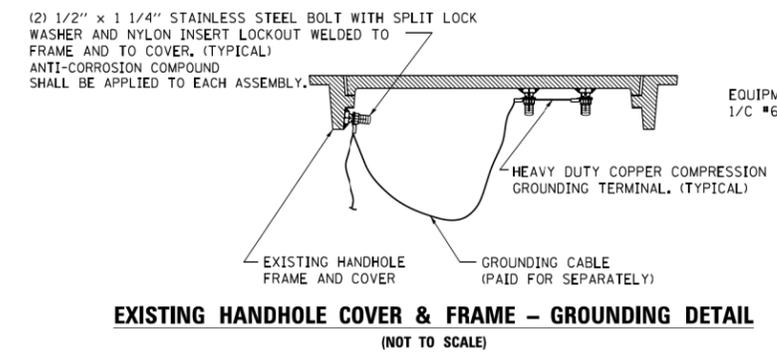


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

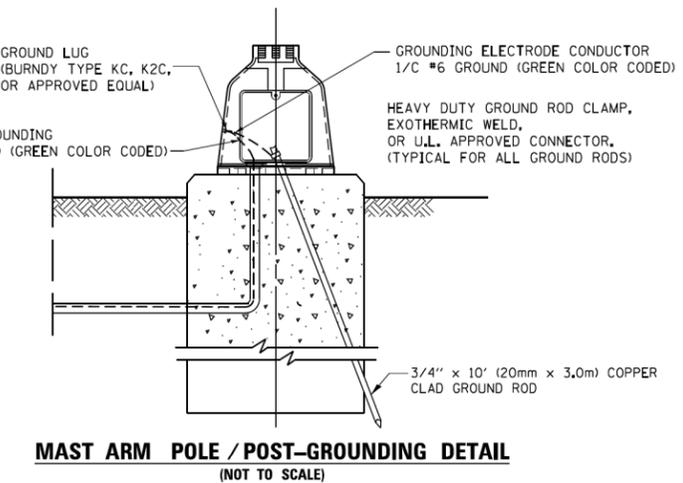


SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)

HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)

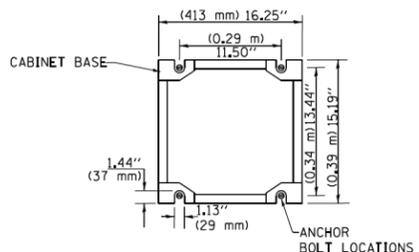


EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)

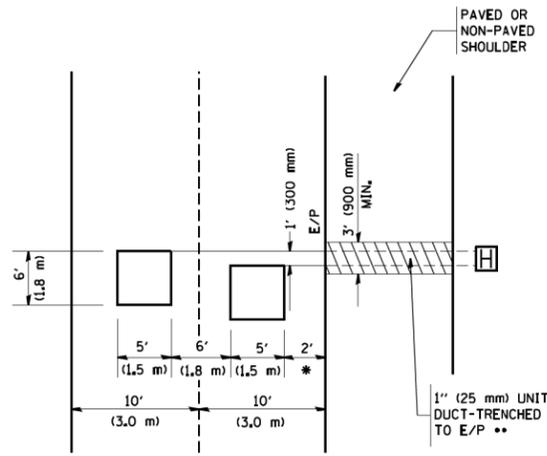
CABINET - BASE BOLT PATTERN (NOT TO SCALE)



FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAC 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\pwork\footemj\d0108315\ts05.dgn	PLOT SCALE = 50,0000 ' / 1"	DRAWN - BCK	REVISED -			3517	17-00121-00-RS	COOK	66	65
PLOT DATE = 1/13/2014	CHECKED - DAD	DATE - 10-28-09	REVISED -			TS-05		CONTRACT NO. 61F07		
						SCALE: NONE		SHEET NO. 4 OF 7 SHEETS		STA. TO STA.
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

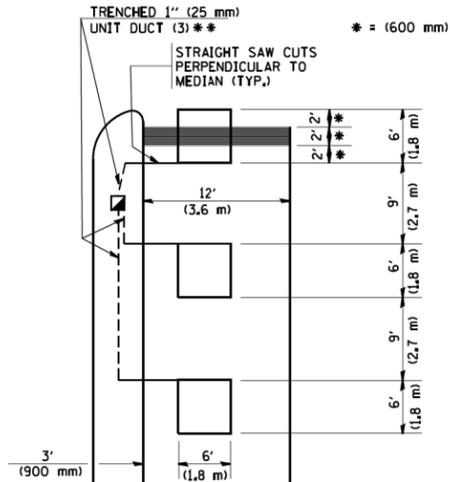


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

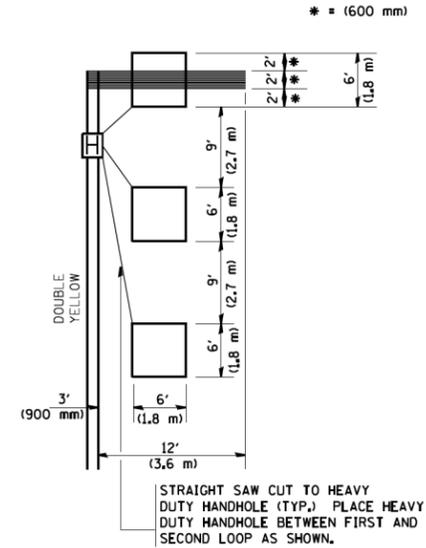
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

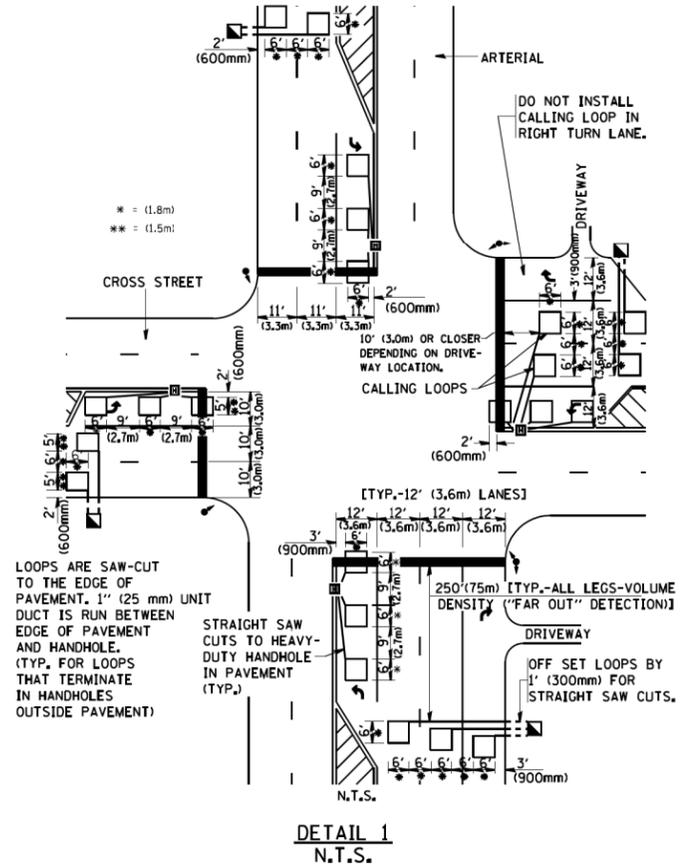
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



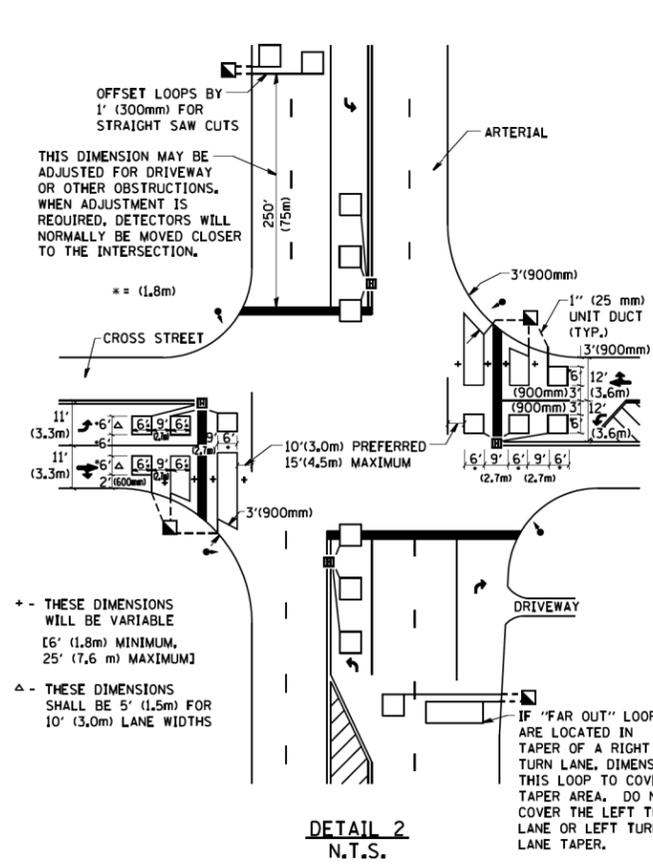
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = gegl:enobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50.0000' / IN.	CHECKED - R.K.F.	REVISED -	REVISED -			3517	17-00121-00-RS	COOK	66	66
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -			TS-07			CONTRACT NO. 61F07	
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	