

FOR INDEX OF SHEETS AND STANDARDS SEE SHEET NO. 2

DESIGN DESIGNATIONS:

SB I-90/94  
NB I-90/94  
RAMP SW  
SB TAYLOR EXIT RAMP  
RAMP SE

91,000(2040) INTERSTATE  
81,000(2040) INTERSTATE  
23,000(2040) INTERSTATE RAMP  
8,000(2040) INTERSTATE RAMP  
5,000(2040) INTERSTATE RAMP

POSTED /DESIGN SPEEDS:

45 /60 MPH  
45 /60 MPH  
35 /35 MPH  
NA /25 MPH  
25 /25 MPH

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
**PROPOSED  
HIGHWAY PLANS**

FAI ROUTE 9094/290  
AT I-290 CONGRESS PARKWAY  
(JANE BYRNE INTERCHANGE)  
JACKSON BLVD ADVANCE WORK  
SECTION 2019-054-1  
PROJECT: NHPP-ZYCH(319)  
COOK COUNTY  
C-91-410-19

NPDES PERMIT INFORMATION	
NPDES Disturbed	
Area =	1.50 Acres
Approximate Location of Roadway is :	
Longitude	87° 39' 6" W
Latitude	41° 52' 32" N

PROJECT LOCATED IN CITY OF CHICAGO

D-91-227-13



*Michael J. Eichten 8/7/19*  
MICHAEL J. EICHEN, P.E. DATE  
LICENSE EXPIRES 11/30/2019  
SHEET RANGE: 1-60, 63-68, 73-75, 81-95,  
149-150, 346-400



*Matthew D. Sanford 8/7/19*  
MATTHEW D. SANFORD, S.E. DATE  
LICENSE EXPIRES 11/30/2020  
SHEET RANGE: 226-227, 293-313



*Jennifer M. Golemba 8/7/19*  
JENNIFER M. GOLEMPA, P.E. DATE  
LICENSE EXPIRES 11/30/2019  
SHEET RANGE: 40-43, 63-65, 69-72, 76-80,  
80A, 88-89, 92-93, 96-119,  
178-182, 190-193



*Arish T. Bhatt 8/7/19*  
ARISH T. BHATT, P.E., S.E. DATE  
LICENSE EXPIRES 11/30/2020  
SHEET RANGE: 61-62



*Matthew J. Letourneau 8/7/19*  
MATTHEW J. LETOURNEAU, P.E. DATE  
LICENSE EXPIRES 11/30/2019  
SHEET RANGE: 215-225



*William D. Stearner 8/7/19*  
WILLIAM D. STEARNER, P.E. DATE  
LICENSE EXPIRES 11/30/2019  
SHEET RANGE: 204-214



*Moussa A. Issa 8/7/19*  
MOUSSA A. ISSA, S.E. DATE  
LICENSE EXPIRES 11/30/2020  
SHEET RANGE: 194-203

RAMP SW  
PROJECT LIMIT  
STA 1315 + 34.11 TO  
STA 1321 + 00.00

SB TAYLOR EXIT RAMP  
PROJECT LIMIT  
STA 6400 + 00.00 TO  
STA 6404 + 00.00

SW RETAINING WALL  
JACKSON BLVD  
SN 016-1702  
STA 1314 + 97.11 TO  
STA 1315 + 57.01

RETAINING WALL 38  
SN 016-1827  
STA 1315 + 57.01 TO  
STA 1318 + 74.91

RAMP SE PROJECT LIMIT  
STA 1400 + 00.00 TO  
STA 1401 + 40.00



LOCATION MAP  
NOT TO SCALE

GROSS LENGTH = 1,105.89 FT (0.211 MILES)  
NET LENGTH = 1,105.89 FT (0.211 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.  
CHICAGO UTILITY ALERT NETWORK  
1-312-744-7000

PROJECT MANAGER: BRIAN KUTTAB, P.E.

CONTRACT NO. 62J31

**AECOM** 303 EAST WACKER DRIVE, SUITE 1400  
CHICAGO, IL 60601-5275  
PHONE: (312) 313-7700  
FAX: (312) 313-6600

**TranSystems** 1475 EAST WOODFIELD ROAD, SUITE 600  
SCHLAUBURG, IL 60173  
PHONE: (847) 405-9500  
FAX: (847) 463-0563

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *August 9* 20 *19*

*Anthony J. Quigley* / *AKS*  
REGIONAL ENGINEER

*Oct 9* 20 *19*

ENGINEER OF DESIGN AND ENVIRONMENT

*Oct 9* 20 *19*

*Paul P. Ch...*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

DISTRICT 1 DESIGN /CONSULTANT SERVICES: BRIAN KUTTAB, P.E. (847)705-4431 SCHAUMBURG, ILLINOIS

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001006	DECIMAL OF AN INCH AND OF A FOOT
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420111-04	PCC PAVEMENT ROUNDOUTS
420401-13	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB
483001-05	PCC SHOULDER
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601001-05	PIPE UNDERDRAINS
602001-02	CATCH BASIN TYPE A
602301-04	INLET - TYPE A
602401-06	PRECAST MANHOLE TYPE A 4' (1.22 m) DIAMETER
602402-02	PRECAST MANHOLE TYPE A 5' (1.52 m) DIAMETER
602601-06	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
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701400-09	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-12	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS * 45 MPH
701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
701446-09	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
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TY-ITSC-418*10	PREFORMED LOOP TYPICAL INSTALLATION NON-BRIDGE APPLICATION
TY-ITSC-663*13	FIBER OPTIC WIRING DETAIL

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS AND STANDARDS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	2
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

1. ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88). THE CONVERSION OF NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) TO CITY OF CHICAGO DATUM IS APPROXIMATELY 579.19 FEET.
2. A MINIMUM OF SEVENTY-TWO (72) HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 TO HAVE THE LOCATION OF EXISTING UNDERGROUND UTILITIES MARKED IN THE FIELD.
3. A MINIMUM OF SEVENTY-TWO (72) HOURS PRIOR TO ANY PLACEMENT OR RELOCATION OF MAINTENANCE OF TRAFFIC DEVICES, THE CONTRACTOR SHALL CONTACT ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) DISTRICT 1 BUREAU OF TRAFFIC AT (847) 705-4151.
4. THE CONTRACTOR MUST CALL THE IDOT ELECTRICAL MAINTENANCE CONTRACTOR TO LOCATE IDOT FACILITY CABLES.
5. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.
6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR THE CITY OF CHICAGO.
7. ALL ROADWAY WIDTHS AND RADII SHOWN ON THE PLANS ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
8. A QUANTITY OF 100 FEET OF EXPLORATION TRENCH 52 INCH DEPTH AND A QUANTITY OF 100 FEET OF EXPLORATION TRENCH 84 INCH DEPTH HAVE BEEN INCLUDED IN THE PLANS FOR THE PURPOSE OF IDENTIFYING ANY BURIED OBSTACLE. THE ENGINEER SHALL APPROVE THE LOCATIONS OF THE EXPLORATION TRENCH 52 INCH DEPTH AND EXPLORATION TRENCH 84 INCH BEFORE ANY EXCAVATION MAY BEGIN.
9. EXCEPT WHERE DESIGNATED OTHERWISE, THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM OFFICE RECORD INFORMATION FURNISHED BY THE UTILITY OWNERS AND THE SUE SURVEYS. ALL UNDERGROUND UTILITIES MUST BE CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN IN THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTORS EXPENSE.
10. DRAINAGE
  - I. STORM SEWER (WATER MAIN REQUIREMENTS) IS TO BE USED AT LOCATIONS WHERE LATERAL SEPARATION BETWEEN THE SEWER AND WATER MAIN IS LESS THAN 10 FT AND THE VERTICAL SEPARATION IS LESS THAN 1.5 FT. DUCTILE IRON PIPE WITH RUBBER GASKET JOINTS SHALL BE USED FOR ALL STORM SEWERS (WATER MAIN REQUIREMENTS).
  - II. THE OFFSETS AND TOP OF FRAME OR LID ELEVATIONS FOR DRAINAGE STRUCTURES WERE DETERMINED USING THE CRITERIA LISTED BELOW UNLESS OTHERWISE NOTED:
    - A. THE OFFSETS TO ALL INLETS AND CATCH BASINS IN ROADWAYS WITH BARRIER WALL ARE TO THE EDGE OF SHOULDER.
    - B. THE OFFSETS TO ALL INLETS AND CATCH BASINS IN CURBED ROADWAYS ARE TO THE EDGE OF PAVEMENT OR EDGE OF SHOULDER. STRUCTURES LOCATED IN THE GUTTER SHALL BE TURNED SO THAT THE FRAME IS FURTHEST FROM THE CENTER LINE OF THE ROAD UNLESS OTHERWISE NOTED ON THE PLANS.
    - C. THE OFFSETS TO MANHOLES, STRUCTURES IN GORE AREAS, AND STRUCTURES IN INFIELD AREAS ARE TO THE CENTER OF THE STRUCTURE.
    - D. THE OFFSETS TO INLETS AND CATCH BASINS IN CONCRETE GUTTER ARE TO BE THE FLOWLINE.
  - III. DRAINAGE GRADES SHALL BE VERIFIED IN FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS.

11. ALL REINFORCEMENT BARS, DOWEL BARS, AND TIE BARS SHOULD BE EPOXY COATED UNLESS OTHERWISE NOTED IN THE PLANS.
12. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
13. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW SITE REVIEW (BDE 2289) AND USE/WASTE SITE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR WILL NEED TO SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION IIG.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. IF ANY UNUSUAL MATERIALS ARE UNCOVERED OR THERE ARE SUSPICIONS OF EXISTING UNDERGROUND STORAGE TANKS, THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK (LUST) CLEANUPS OR THAT IS PREQUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION
15. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07(B,C) OF THE SSRBC WILL NOT BE ALLOWED.

**COMMITMENTS**

1. CONSTRUCTION NOISE AND CONSTRUCTION VIBRATION SHALL FOLLOW THE ENVIRONMENTAL COMMITMENT OUTLINED IN THE ENVIRONMENTAL ASSESSMENT AND ERRATA - INCLUDED IN THE CONTRACT DOCUMENTS.
2. PROVISIONS FOR THE AIR QUALITY MONITORING PROGRAM SHALL FOLLOW THE ENVIRONMENTAL COMMITMENT OUTLINED IN THE ENVIRONMENTAL ASSESSMENT AND ERRATA - INCLUDED IN THE CONTRACT DOCUMENTS.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES AND COMMITMENTS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	3
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

**CITY OF CHICAGO  
GENERAL NOTES:**

1. THE CONVERSION OF NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) TO CITY OF CHICAGO DATUM IS APPROXIMATELY 579.19 FEET.
2. ALL WORK WITHIN CITY RIGHT OF WAY MUST CONFORM TO THE MOST CURRENT CITY OF CHICAGO STANDARDS FOR CONSTRUCTION IN COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT, (ADA) AVAILABLE ON THE CITY OF CHICAGO WEBSITE.
3. WITHIN CITY RIGHT OF WAY, PAVEMENT CROSS SLOPES SHALL VARY FROM A MINIMUM OF 1.4% TO A MAXIMUM OF 3.6%. CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 3 INCHES AND A MAXIMUM OF 9 INCHES. THE LONGITUDINAL SLOPE ALONG A STRAIGHT CONCRETE GUTTER SECTION AND CURVED GUTTER SECTION SHALL BE A MINIMUM OF 0.4% AND 0.65%, RESPECTIVELY.
4. TEMPORARY HOT-MIX ASPHALT RAMPS MUST BE CONSTRUCTED AROUND ALL UTILITY STRUCTURES WITHIN CITY RIGHT OF WAY REGARDLESS OF SHAPE WHEN THE ROAD IS OPEN TO TRAFFIC PRIOR TO PLACEMENT OF BINDER AND/OR SURFACE COURSE. THE RAMPS MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY TEMPORARY HMA RAMP DETAILS AND REMOVED PRIOR TO PLACEMENT OF BINDER AND/OR SURFACE COURSE. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UTILITY STRUCTURES.
5. THE LOCATIONS AND ELEVATIONS OF EXISTING SEWERS AND SEWER STRUCTURES SHOWN ON THE PLANS AND PROFILES HAVE BEEN OBTAINED FROM DRAWINGS AND ATLASES AND THE INFORMATION IS NOT GUARANTEED. THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL FIELD VERIFY THE CITY'S EXISTING SEWER FACILITIES INCLUDING PUBLIC AND PRIVATE DRAIN CONNECTIONS IN THE LIMITS OF THE REFERENCED PROJECT FOR ANY CONFLICTS DUE TO THE PROPOSED IMPROVEMENTS. ANY CONFLICT SHOULD BE RESOLVED WITH THE DEPARTMENT OF WATER MANAGEMENT PRIOR TO START OF CONSTRUCTION.
6. IN LOCATIONS WHERE THE MAIN SEWER IS NOT BEING REPLACED AND THE EXISTING DRAINAGE FACILITIES ARE DISTURBED OR DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND REPLACE THE DAMAGED FACILITIES AT HIS/HER EXPENSE TO THE SATISFACTION OF THE DEPARTMENT OF WATER MANAGEMENT. THE SEWER FLOWS MUST BE MAINTAINED AT ALL TIMES.
7. IN CASE OF ANY DAMAGE TO THE CITY'S SEWER SYSTEM, PRIVATE AND PUBLIC DRAIN CONNECTIONS, THE CONTRACTOR SHALL CONTACT THE CHICAGO DEPARTMENT OF WATER MANAGEMENT IMMEDIATELY AT (312) 747-8117 OR (312) 747-7893. THE CONTRACTOR SHALL AT HIS/HER OWN EXPENSE, REPLACE THE AFFECTED SEWERS, DRAIN CONNECTIONS, AND SEWER STRUCTURES AS NECESSARY. THE SEWER FLOW MUST BE MAINTAINED AT ALL TIMES.
8. CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT UNLESS SPECIFICALLY IDENTIFIED FOR REMOVAL, RECONSTRUCTION OR ADJUSTMENT WITHIN THESE PLANS.
9. THE COST OF CATCH BASIN RESTRICTORS SHALL BE INCLUDED IN THE COST OF THE CATCH BASINS.
10. AS-BUILT PLANS FOR WORK WITHIN THE CITY RIGHT OF WAY MUST BE SUBMITTED RIGHT AFTER WORK COMPLETION. FINAL PAYMENT SHALL NOT BE MADE TO THE CONTRACTOR UNTIL THE DEPARTMENT OF WATER MANAGEMENT ACKNOWLEDGES RECEIPT OF AS-BUILT PLANS.
11. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE DEPARTMENT OF TRANSPORTATION OF ANY DAMAGE TO CITY OWNED AND MAINTAINED TRAFFIC SIGNS, SIGNALS, GUARDRAILS, FENCES, ETC.
12. WITHIN CITY RIGHT OF WAY, THE CONTRACTOR SHALL SAWCUT A MINIMUM DEPTH OF ONE AND A HALF INCHES (1 1/2") WITH A CONCRETE SAWING MACHINE, TO PREVENT THE SURFACE FROM SPALLING WHEN THE SURFACE COURSE AND THE BASE COURSE ARE BROKEN. THE WORK SHALL BE DONE IN SUCH A MANNER THAT A STRAIGHT JOINT IS SECURED. THE CONTRACTOR SHALL SAWCUT THE PAVEMENT FULL DEPTH FOR PATCHES AND AROUND STRUCTURES. AT CONCRETE CURB AND GUTTER, CONTRACTOR SHALL SAWCUT TO THE BASE OF THE CURB AND GUTTER. ALL SAWCUTTING REQUIRED WHETHER OR NOT SPECIFIED ON THE PLANS SHALL BE INCLUDED IN THE COST OF THE ADJACENT REMOVAL PAY ITEMS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.

13. WITHIN CITY RIGHT OF WAY, 1/2-INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, AND MANHOLES WHICH EXTEND THROUGH THE SIDEWALK UNLESS OTHERWISE NOTED ON THE PLANS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PORTLAND CEMENT CONCRETE SIDEWALK PAY ITEM. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
14. THE CITY'S SEPARATION STANDARDS ARE AS FOLLOWS: THE MINIMUM VERTICAL CLEARANCE (EDGE-TO-EDGE) FROM ALL WATER MAINS IS 18-INCHES. FOR FEEDER MAINS (WATER MAINS 16-INCHES AND LARGER), THE MINIMUM HORIZONTAL CLEARANCE (EDGE-TO-EDGE) IS FIVE (5) FEET, AND FOR GRID MAINS (WATER MAINS LESS THAN 16-INCHES), THE MINIMUM HORIZONTAL CLEARANCE (EDGE-TO-EDGE) IS THREE (3) FEET. FOR ABOVE GROUND FACILITIES, THE MINIMUM HORIZONTAL CLEARANCE (EDGE-TO-EDGE) IS FIVE (5) FEET. IN NO CASE SHALL THE INSTALLATION OF ANY PROPOSED FACILITY BE CLOSER THAN FIVE (5) FEET FROM A FIRE HYDRANT OR FIRE HYDRANT LEAD. ALL CURB INSTALLATION ADJACENT TO FIRE HYDRANTS MUST BE PAINTED 'SAFETY YELLOW' FOR 15 FEET ON EACH SIDE OF THE FIRE HYDRANT EXCEPT WHERE THE 15 FOOT DIMENSION INTERSECTS A CROSSWALK, DRIVEWAY OR SIMILAR FEATURE.
15. THERE ARE NUMEROUS MANHOLES, CATCH BASINS AND INLETS WITHIN CITY RIGHT OF WAY, WHICH MUST BE PROTECTED FROM ENTRY OF ASPHALT/DEBRIS INTO THE SEWER SYSTEM DURING CONSTRUCTION. THE CONTRACTOR MUST MARK THE LOCATIONS OF ALL SEWER STRUCTURES ON THE SIDEWALK BEFORE STARTING PAVEMENT REMOVAL/REPLACEMENT. ALL NECESSARY ADJUSTMENTS TO SEWER FACILITIES, INCLUDING VERTICAL ADJUSTMENT OF FRAMES AND LIDS, MUST BE INCLUDED IN THE CONTRACT AND PERFORMED BY IDOT'S CONTRACTOR PRIOR TO STREET RESURFACING. PLEASE NOTE THAT A PERMIT WILL BE REQUIRED FROM THE SEWER SECTION FOR THE ABOVE-MENTIONED ADJUSTMENT WORK.
16. A PERMIT FROM THE DEPARTMENT OF WATER MANAGEMENT SHOULD BE OBTAINED IN ADVANCE FOR ANY UNDERGROUND SEWER WORK WITHIN CITY RIGHT OF WAY INCLUDING ADJUSTMENT OF STRUCTURES, REMOVAL/REPLACEMENT OF FRAMES AND LIDS, TELEVISION SURVEYS, CLEANING, LINING AND INSPECTIONS BY A LICENSED SEWER CONTRACTOR AT 333 S. STATE STREET, SUITE 410, CHICAGO, ILLINOIS 60604.
17. IF CONSTRUCTION REQUIRES THE USE OF WATER FROM A CITY FIRE HYDRANT, OR ADJUSTMENT REPAIRS ARE REQUIRED TO ANY CITY SEWER FACILITIES IN PROXIMITY TO THE PROJECT SITE, PERMITS MUST BE OBTAINED FROM THE DEPARTMENT OF WATER MANAGEMENT, WATER AND SEWER SECTIONS.
18. EXTREME CAUTION IS TO BE TAKEN TO ENSURE THAT NO FACILITY OWNED AND MAINTAINED BY THIS DEPARTMENT IS DAMAGED DURING CONSTRUCTION. IF DAMAGE OCCURS TO ANY FACILITIES, THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR THE COST OF REPAIRING OR REPLACING THEM.

**CITY OF CHICAGO DIVISION OF TRANSPORTATION STANDARD CONSTRUCTION DETAILS**

A-2-2C	PAVEMENT PATCHING AND PORTLAND CEMENT CONCRETE PAVEMENT
A-2-3A	TYPICAL JOINT LAYOUT FOR P.C. CONCRETE PAVEMENTS
A-2-3C	P.C.C. BASE COURSE JOINT DETAILS
A-2-6	CONCRETE CURB & GUTTER DETAIL
A-2-10A	DETAILS OF STRUCTURE CASTING ISOLATION BOX
A-2-10B	DETAILS OF STRUCTURE CASTING ISOLATION BOX FOR P.C.C. PAVEMENT AND BASE COURSE
A-2-12	FRAME ADJUSTMENT IN PAVEMENT

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D162J31-SHT-GENNOTE-03.dgn	DESIGNED - AFC	REVISED -
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PLOT SCALE = 100.0000' / 1"	CHECKED - MJE	REVISED -
PLOT DATE = 8/14/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO  
GENERAL NOTES AND STANDARDS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	4
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004 URBAN	0044 016-1827	0010 016-1702	0010 016-0588	0021 URBAN
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	207	207				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	107	107				
20101000	TEMPORARY FENCE	FOOT	75	75				
20101100	TREE TRUNK PROTECTION	EACH	5	5				
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	4	4				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	1	1				
20200100	EARTH EXCAVATION	CU YD	5,640	5,640				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	3,085	3,085				
20400800	FURNISHED EXCAVATION	CU YD	940	940				
20700220	POROUS GRANULAR EMBANKMENT	CU YD	6	6				
20800150	TRENCH BACKFILL	CU YD	1,005.4	1,005.4				
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2,812	2,812				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3,486	3,486				
21301052	EXPLORATION TRENCH 52" DEPTH	FOOT	100	100				

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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 PLOT DATE = 8/16/2019

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 DATE - 8/16/2019      REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 1 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
21301084	EXPLORATION TRENCH 84" DEPTH	FOOT	100	100				
25000210	SEEDING, CLASS 2A	ACRE	0.75	0.75				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	80	80				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	80	80				
25100115	MULCH, METHOD 2	ACRE	2.75	2.75				
25100630	EROSION CONTROL BLANKET	SQ YD	2,533	2,533				
25200110	SODDING, SALT TOLERANT	SQ YD	953	953				
25200200	SUPPLEMENTAL WATERING	UNIT	29	29				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	275	275				
28000400	PERIMETER EROSION BARRIER	FOOT	1,827	1,827				
28000510	INLET FILTERS	EACH	41	41				
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	635	635				
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	3,218	3,218				
30300124	AGGREGATE SUBGRADE IMPROVEMENT 24"	SQ YD	253	253				

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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 PLOT SCALE = 2.0000' / 1"v  
 PLOT DATE = 8/16/2019

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 2 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	3,577	3,577				
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	72	72				
31101600	SUBBASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	1,428	1,428				
31200500	STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"	SQ YD	2,127	2,127				
35300300	PORTLAND CEMENT CONCRETE BASE COURSE 8"	SQ YD	60	60				
40200500	AGGREGATE SURFACE COURSE, TYPE A 6"	SQ YD	231	231				
40200900	AGGREGATE SURFACE COURSE, TYPE B	CU YD	32	32				
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	100	100				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	27	27				
40604060	HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50	TON	7	7				
42000080	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB	SQ YD	86	86				
42000401	PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)	SQ YD	139	139				
42000521	PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)	SQ YD	1,758	1,758				
42001300	PROTECTIVE COAT	SQ YD	8,131	8,131				

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE:      SHEET 3 OF 16 SHEETS      STA.      TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	7
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
44000100	PAVEMENT REMOVAL	SQ YD	3,870	3,870				
44000400	GUTTER REMOVAL	FOOT	120	120				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	287	287				
44001980	CONCRETE BARRIER REMOVAL	FOOT	2,023	2,023				
44003100	MEDIAN REMOVAL	SQ FT	8,620	8,620				
44004250	PAVED SHOULDER REMOVAL	SQ YD	731	731				
48300400	PORTLAND CEMENT CONCRETE SHOULDERS 9"	SQ YD	50	50				
48300600	PORTLAND CEMENT CONCRETE SHOULDERS 11"	SQ YD	892	892				
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1				1	
50157300	PROTECTIVE SHIELD	SQ YD	2,065				2,065	
50200100	STRUCTURE EXCAVATION	CU YD	878		783	95		
50300225	CONCRETE STRUCTURES	CU YD	340.3	15	325.3			
50300255	CONCRETE SUPERSTRUCTURE	CU YD	165.6	21	144.6			
50300300	PROTECTIVE COAT	SQ YD	51	51				

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE:      SHEET 4 OF 16 SHEETS      STA.      TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	8
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
50800105	REINFORCEMENT BARS	POUND	682,950		572,630	110,320		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	38,080	9,350	28,730			
50800530	MECHANICAL SPLICERS	EACH	888		744	144		
51500100	NAME PLATES	EACH	1		1			
51602000	PERMANENT CASING	FOOT	150			150		
* 51603000	DRILLED SHAFT IN SOIL	CU YD	2,906.1		2,434.8	471.3		
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	393		393			
52200255	TREATED TIMBER LAGGING	SQ FT	382			382		
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	150	150				
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	135	135				
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	24	24				
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	184	184				
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	93	93				
550A0500	STORM SEWERS, CLASS A, TYPE 2 60"	FOOT	210	210				

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 5 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	9
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
55100300	STORM SEWER REMOVAL 8"	FOOT	38	38				
55100400	STORM SEWER REMOVAL 10"	FOOT	272	272				
55100500	STORM SEWER REMOVAL 12"	FOOT	170	170				
58700300	CONCRETE SEALER	SQ FT	9,454		9,454			
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	2	2				
60108206	PIPE UNDERDRAINS, TYPE 2, 6"	FOOT	839	839				
60200105	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1				
60200205	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1				
60200805	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	EACH	2	2				
60201310	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 20 FRAME AND GRATE	EACH	7	7				
60218400	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2				
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	8	8				
60237420	INLETS, TYPE A, TYPE 20 FRAME AND GRATE	EACH	1	1				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4				

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 6 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	10
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING	
				0004	0044	0010	0010	0021	
				URBAN	016-1827	016-1702	016-0588	URBAN	
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	2	2					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					
60500040	REMOVING MANHOLES	EACH	3	3					
60500050	REMOVING CATCH BASINS	EACH	9	9					
60500060	REMOVING INLETS	EACH	3	3					
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	28	28					
63200310	GUARDRAIL REMOVAL	FOOT	56	56					
63700805	CONCRETE BARRIER TRANSITION	FOOT	15	15					
63700900	CONCRETE BARRIER BASE	FOOT	15	15					
64300260	IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	5	5					
64300370	IMPACT ATTENUATORS (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	1	1					
67100100	MOBILIZATION	L SUM	1	1					
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	322	322					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	60	60					

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 7 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING	
				0004	0044	0010	0010	0021	
				URBAN	016-1827	016-1702	016-0588	URBAN	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	7,155	7,155					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	255	255					
70300560	PAVEMENT MARKING TAPE, TYPE III 12"	FOOT	44	44					
70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	14	14					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3,350	3,350					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2,450	2,450					
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	3	3					
70600290	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, WIDE), TEST LEVEL 3	EACH	3	3					
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	3	3					
70600360	IMPACT ATTENUATORS, RELOCATE (SEVERE USE), TEST LEVEL 3	EACH	2	2					
* 72000300	SIGN PANEL - TYPE 3	SQ FT	352	352					
72100100	SIGN PANEL OVERLAY	SQ FT	166	166					
* 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	2					
* 72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	1182	1182					

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

% 0042



D162J31-SHT-500.dgn  
 USER NAME = dshevoZ  
 PLOT SCALE = 2.0000' / in.  
 PLOT DATE = 8/16/2019

DESIGNED - ZND  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 8 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	12
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

FILE PATH = p:\aecom\m-n\m\1\encomon\local\AECOM\_D502\_MN\Documents\01\_Americas\Transportation\60269938\_Circle Phase 1\000\_CAD\006\_Roadway\Sheets\62J31\_Contract\0162J31-SHT-500.dgn

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING	
				0004	0044	0010	0010	0021	
				URBAN	016-1827	016-1702	016-0588	URBAN	
* 73300100	OVERHEAD SIGN STRUCTURE - SPAN, TYPE I-A (4'-0" X 4'-6")	FOOT	57	57					
* 73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	17.4	17.4					
* 73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	1	1					
* 73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	4	4					
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	682	682					
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	274						274
* 81100605	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., PVC COATED GALVANIZED STEEL	FOOT	155						155
* 81100805	CONDUIT ATTACHED TO STRUCTURE, 3" DIA., PVC COATED GALVANIZED STEEL	FOOT	15						15
* 81101005	CONDUIT ATTACHED TO STRUCTURE, 4" DIA., PVC COATED GALVANIZED STEEL	FOOT	240						240
* 81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	310						310
* 81300530	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12" X 10" X 6"	EACH	2						2
* 81300948	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 24" X 24" X 10"	EACH	5						5
* 81300960	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 42" X 36" X 12"	EACH	1						1
* 81400200	HEAVY-DUTY HANDHOLE	EACH	3						3

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



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 PLOT SCALE = 2.0000' / in.  
 PLOT DATE = 8/16/2019

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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 9 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	13
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
* 81603081	UNIT DUCT, 600V, 3-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT	56					56
* 81702140	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4	FOOT	355					355
* 81702150	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 2	FOOT	1,065					1,065
* 81800300	AERIAL CABLE, 3-1/C NO. 2 WITH MESSENGER WIRE	FOOT	896					896
* 82110016	LUMINAIRE, LED, HIGHMAST, OUTPUT DESIGNATION I	EACH	18					18
* 83505500	LIGHT TOWER, 130 FT. MOUNTING HEIGHT, LUMINAIRE MT. - 8	EACH	2					2
* 84100110	REMOVAL OF TEMPORARY LIGHTING UNIT	EACH	2					2
* 84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	19					19
* 84400405	RELOCATE EXISTING WOOD POLES	EACH	3					3
* 87200400	SPAN WIRE	FOOT	2,867					2,867
* 87900205	DRILL EXISTING HEAVY DUTY HANDHOLE	EACH	6					6
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1,300					1,300
* 89502380	REMOVE EXISTING HANDHOLE	EACH	1					1
Z0004552	APPROACH SLAB REMOVAL	SQ YD	66	66				
Z0005872	BONDED PREFORMED JOINT SEALER, 2 INCH	FOOT	28	28				

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

% 0042



D162J31-SHT-500.dgn  
 USER NAME = ChiuA  
 PLOT SCALE = 2.0000' / in.  
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DESIGNED - ZND  
 DRAWN - ZND  
 CHECKED - MJJ  
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REVISED -  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 10 OF 16 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING	
				0004	0044	0010	0010	0021	
				URBAN	016-1827	016-1702	016-0588	URBAN	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1					
Z0018911	DRILL AND GROUT #6 TIE BARS	EACH	26	26					
Z0019600	DUST CONTROL WATERING	UNIT	100	100					
Z0022800	FENCE REMOVAL	FOOT	242	242					
Z0028415	GEOTECHNICAL REINFORCEMENT	SQ YD	2,295	2,295					
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	590	590					
* Z0033028	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	11						11
Z0037300	PAVEMENT GROOVING	SQ YD	1,986	1,986					
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	314		314				
Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	16	16					
Z0056610	STORM SEWER (WATER MAIN REQUIREMENTS) 15 INCH	FOOT	120	120					
Z0062456	TEMPORARY PAVEMENT	SQ YD	4,082	4,082					

\* DENOTES SPECIALTY ITEM      \*\* DENOTES NON-PARTICIPATING ITEM      % 0042



D162J31-SHT-500.dgn  
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 PLOT SCALE = 2.0000' / 1" =  
 PLOT DATE = 8/16/2019

DESIGNED - ZND	REVISED -
DRAWN - ZND	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 11 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
X0320051	CROSSHOLE SONIC LOGGING ACCESS DUCTS	FOOT	2,775		2,325	450		
X0320052	CROSSHOLE SONIC LOGGING TESTING	EACH	9		7	2		
X0320085	MONITORING ADJACENT STRUCTURES	L SUM	1	1				
X0321750	REMOVE TEMPORARY CONCRETE BARRIER, STATE OWNED	FOOT	122	122				
X0322141	REMOVE TEMPORARY WOOD POLE	EACH	5					5
* X0324198	REMOVAL OF ASBESTOS CEMENT CONDUIT	FOOT	2,000	2,000				
* X0324599	ROD AND CLEAN EXISTING CONDUIT	FOOT	630					630
X0324761	DRAINAGE SYSTEM (SPECIAL)	L SUM	1	1				
X0325207	TELEVISION INSPECTION OF SEWER	FOOT	1,730	1,730				
X0325279	CLASS SI CONCRETE (MISCELLANEOUS)	CU YD	237.1		237.1			
X0325349	TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)	FOOT	1400	1400				
X0327004	TEMPORARY WOOD POLE, 60 FT., CLASS 4	EACH	23					23
X0327236	TEMPORARY WOOD POLE, 50 FT., CLASS 4	EACH	1					1
X0327267	SLOPE INCLINOMETER	EACH	1		1			

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 12 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	16
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
X0327357	CONSTRUCTION VIBRATION MONITORING	L SUM	1	1				
* X0327607	FIBER OPTIC SPLICE-MAINLINE	EACH	2					2
* X0327616	MAINTAINING ITS DURING CONSTRUCTION	CAL MO	11					11
X0327757	FOUNDATION CONSTRUCTION AT EXISTING OBSTRUCTIONS	EACH	1			1		
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	12,406	12,406				
X0370077	LOCATE TUNNEL, CHICAGO	EACH	1	1				
X0370078	BULKHEAD TUNNEL, CHICAGO	EACH	1	1				
X0370135	CONCRETE CURB, TYPE B (SPECIAL) (CDOT)	FOOT	67	67				
X1200007	TEMPORARY WOOD POLE, 80 FEET, CLASS 4	EACH	2					2
X1200011	STORM SEWERS, TYPE 2, DUCTILE IRON PIPE 8"	FOOT	40	40				
* X1400172	ELECTRIC CABLE IN CONDUIT, COMMUNICATION, NO. 19 6 PAIR	FOOT	233					233
* X1400174	REMOVE AND REINSTALL EXISTING CCTV CAMERA AND EQUIPMENT	L SUM	1					1
* X1400240	FIBER OPTIC CABLE IN CONDUIT, 96 FIBERS, SINGLE MODE	FOOT	1,200					1,200
* X1400241	FIBER OPTIC CABLE, AERIAL, 96 FIBERS, SINGLE MODE	FOOT	3,210					3,210

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

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D162J31-SHT-500.dgn  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 13 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	17
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
* X1400262	ELECTRIC CABLE, AERIAL INSTALLATION, NO. 19 25 PAIR	FOOT	7,734					7,734
X1700036	CONCRETE BARRIER BASE (SPECIAL NO. 1)	FOOT	186	186				
X1700037	CONCRETE BARRIER BASE (SPECIAL NO. 2)	FOOT	385	385				
X1700038	CONCRETE BARRIER BASE (SPECIAL NO. 3)	FOOT	83	83				
X1700039	CONCRETE BARRIER BASE (SPECIAL NO. 4)	FOOT	314	314				
X1700073	CONCRETE BARRIER WALL (SPECIAL NO. 1)	FOOT	314	314				
X5012502	CONCRETE REMOVAL (SPECIAL)	CU YD	17	17				
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	43	43				
X5610716	WATER MAIN REMOVAL, 16"	FOOT	85	85				
X6022110	MANHOLES, TYPE A, 10' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1				
X6370050	CONCRETE BARRIER WALL (SPECIAL)	FOOT	83	83				
X6370279	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)	FOOT	571	571				
X6431120	REMOVE IMPACT ATTENUATOR SAND MODULE	EACH	1	1				
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	11	11				

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE:      SHEET 14 OF 16 SHEETS      STA.      TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	18
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE					
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING	
				0004	0044	0010	0010	0021	
				URBAN	016-1827	016-1702	016-0588	URBAN	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1					
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1					
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	322	322					
X7035104	TEMPORARY EPOXY PAVEMENT MARKING - LINE 4"	FOOT	13,336	13,336					
X7035105	TEMPORARY EPOXY PAVEMENT MARKING - LINE 5"	FOOT	1,955	1,955					
X7035108	TEMPORARY EPOXY PAVEMENT MARKING - LINE 8"	FOOT	10,811	10,811					
X7035112	TEMPORARY EPOXY PAVEMENT MARKING - LINE 12"	FOOT	784	784					
X7040010	TEMPORARY CONCRETE BARRIER (SPECIAL)	FOOT	317	317					
X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	1,080	1,080					
* X8130115	DRILL EXISTING JUNCTION BOX	EACH	4						4
* X8130120	RELOCATE EXISTING JUNCTION BOX	EACH	1						1
* X8420502	REMOVAL OF LIGHT TOWER, NO SALVAGE	EACH	1						1
* X8420510	REMOVAL OF TOWER FOUNDATION	EACH	1						1
* X8730246	ELECTRIC CABLE IN CONDUIT, NO. 19 25 PAIR	FOOT	1,959						1,959

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET 15 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

FILE PATH = p:\aecom\ms-ams\ecommon\local\aecom\0502\_mn\documents\01\_america\transportation\60269938\_Circle Phase\11\000\_CAD\006\_Roadway\Sheets\62J31\_Contract\0162J31-SHT-500.dgn

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE				
				90% FED 10% STATE ROADWAY	90% FED 10% STATE RETAINING WALL	90% FED 10% STATE WINGWALL	90% FED 10% STATE BRIDGE	90% FED 10% STATE HIGHWAY LIGHTING
				0004	0044	0010	0010	0021
				URBAN	016-1827	016-1702	016-0588	URBAN
* X8730312	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 18 4/C, TWISTED, SHIELDED	FOOT	1,481					1,481
* X8772115	TEMPORARY MAST ARM, ALUMINUM, 15FT	EACH	8					8
* X8850109	PREFORMED INDUCTION LOOP	FOOT	174					174
* X8950510	REMOVE FIBER OPTIC CABLE FROM CONDUIT	FOOT	200					200
* X8951011	REMOVE AERIAL CABLE	FOOT	340					340
* X1400386	RELOCATE EXISTING WIRELESS VEHICLE DETECTION SYSTEM	EACH	1					1
* X1400387	REMOVE AND REINSTALL AERIAL CABLE	FOOT	3,296					3,296
X6640706	TEMPORARY CHAIN LINK FENCE WITH SCREENING, 6'	FOOT	621	621				
Ø Z0076600	TRAINEES	HOUR	500	500				
X0100025	FLOWABLE FILL	CU YD	52	52				
Ø Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500				
* X1400385	LIGHT TOWER, 150 FT. MOUNTING HEIGHT, LUMINAIRE MT. - 8, MATERIALS ONLY	EACH	1					1

\* DENOTES SPECIALTY ITEM

\*\* DENOTES NON-PARTICIPATING ITEM

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D162J31-SHT-500.dgn	DESIGNED - ZND	REVISED -
USER NAME = ChiuA	DRAWN - ZND	REVISED -
PLOT SCALE = 2.0000' / 1" =	CHECKED - MJE	REVISED -
PLOT DATE = 8/16/2019	DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

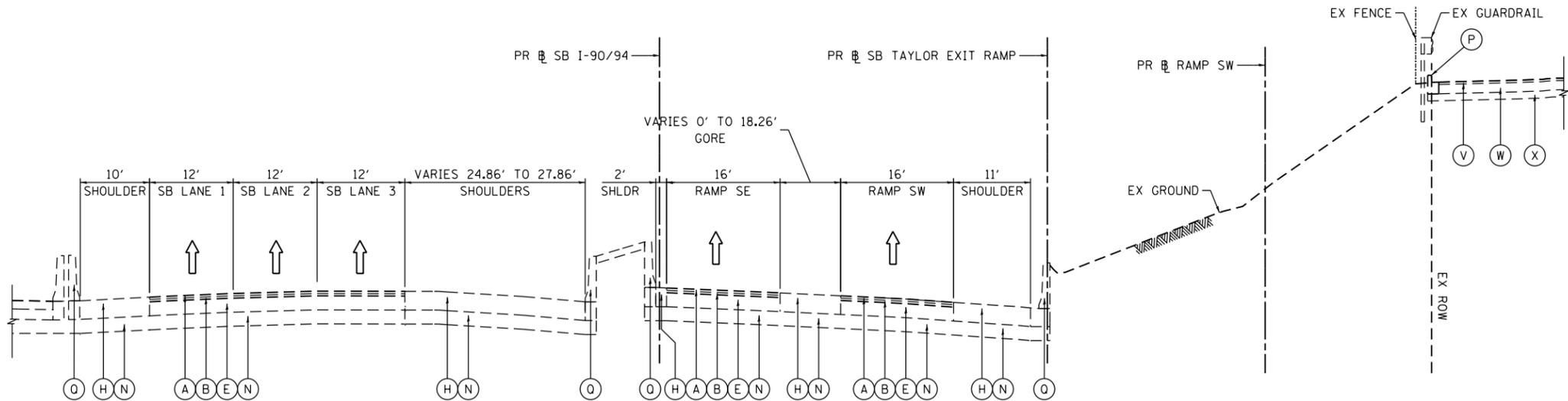
SUMMARY OF QUANTITIES

SCALE: SHEET 16 OF 16 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	20
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



FILE PATH = p:\aecom\m-n\si\encom\line\local\AECOM\_DS02\_MA\Documents\01\_Americas\Transportation\60269938\_Circle Phase II\000\_CAD\006\_Roadway\Sheets\62J31\_Contract\0162J31-SHT-Typical-02.dgn

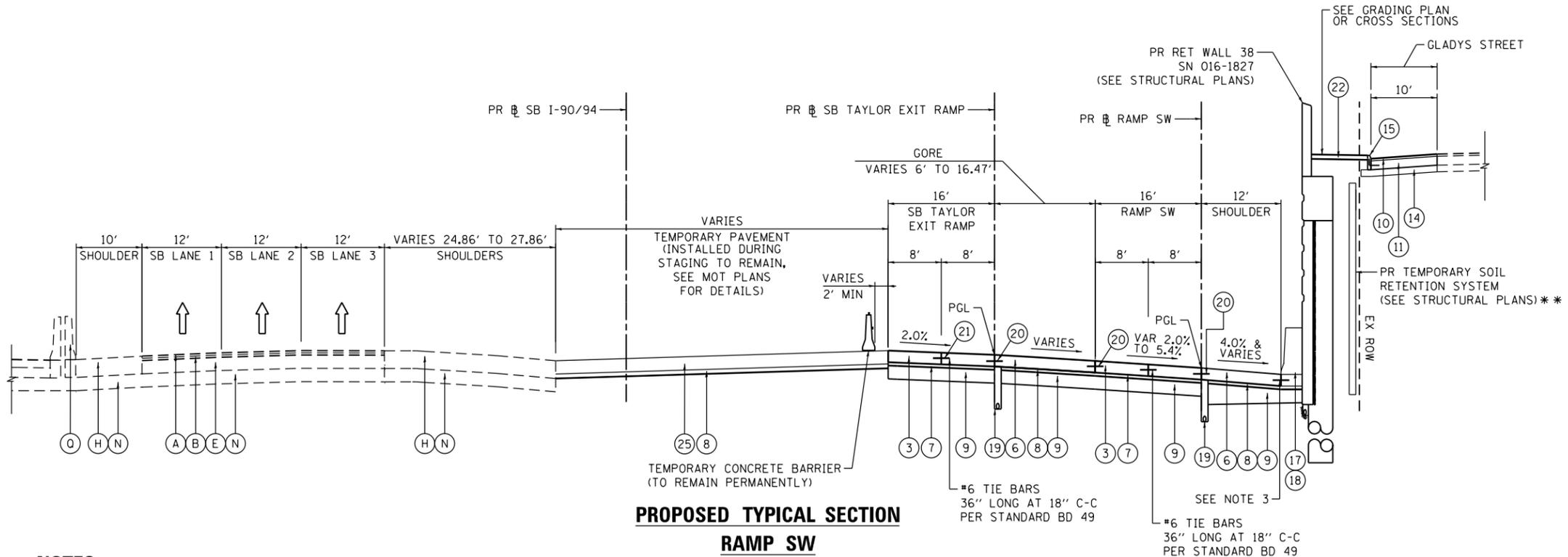


**EXISTING TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1316+39.53 TO STA 1317+21.11 (PR SB RAMP SW)  
STA 6401+14.43 TO STA 6401+97.27 (PR SB TAYLOR EXIT RAMP)

**EXISTING**

- (A) HOT MIX ASPHALT SURFACE COURSE, 1.5"
- (B) HOT MIX ASPHALT BINDER COURSE, 2.5"
- (C) HOT MIX ASPHALT PAVEMENT, 5" TO 11"
- (D) CONTINUOUSLY REINFORCED PCC PAVEMENT, 13"
- (E) PORTLAND CEMENT CONCRETE BASE COURSE, 10"
- (F) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (G) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (H) BITUMINOUS SHOULDER, 13"
- (I) TEMPORARY PAVEMENT (PCC/HMA)
- (J) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (K) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (L) SUBBASE GRANULAR MATERIAL, TYPE B 8"
- (M) SUBBASE GRANULAR MATERIAL, 12"
- (N) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (O) POROUS GRANULAR EMBANKMENT, SPECIAL, 0" TO 30"
- (P) COMBINATION CONCRETE CURB AND GUTTER
- (Q) CONCRETE BARRIER
- (R) TEMPORARY CONCRETE BARRIER (STATE OWNED)
- (S) GUARDRAIL
- (T) PIPE UNDERDRAINS
- (U) TOPSOIL
- (V) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50-2" (ASSUMED)
- (W) PORTLAND CEMENT CONCRETE BASE COURSE 8" (ASSUMED)
- (X) SUBBASE GRANULAR MATERIAL, TYPE B, 6" (ASSUMED)
- (Y) PORTLAND CEMENT CONCRETE PAVEMENT, 11" JOINTED



**PROPOSED TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1316+39.53 TO STA 1317+21.11 (PR SB RAMP SW)  
STA 6401+14.43 TO STA 6401+97.27 (PR SB TAYLOR EXIT RAMP)

\*\* TEMPORARY SOIL RETENTION SYSTEM - STA 1315+75.00 TO STA 1316+68.41 (PR RAMP SW) - SEE STRUCTURAL PLANS

**PROPOSED**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) OMITTED
- (3) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (4) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (5) OMITTED
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (7) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (8) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT 12"  
(SEE SHEET 26 FOR ADDITIONAL INFORMATION)
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50: 2"
- (11) PORTLAND CEMENT CONCRETE BASE COURSE 8"
- (12) POROUS GRANULAR EMBANKMENT
- (13) CONCRETE MEDIAN SURFACE, 4"
- (14) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- (15) CONCRETE CURB, TYPE B (SPECIAL), (CDOT)
- (16) CONCRETE GUTTER, TYPE B
- (17) CONCRETE BARRIER WALL (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (18) CONCRETE BARRIER BASE (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (19) PIPE UNDERDRAINS 4" OR 6" (SEE DRAINAGE PLANS)
- (20) #6 TIE BARS, 24" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC SHOULDER OR CURB AND GUTTER)
- (21) #6 TIE BARS, 30" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC PAVEMENT)
- (22) TOPSOIL FURNISH AND PLACE, 4" AND SEEDING OR SODDING  
(SEE EROSION CONTROL PLANS)
- (23) TOPSOIL FURNISH AND PLACE, 24" AND SEEDING
- (24) OMITTED
- (25) TEMPORARY PAVEMENT (PCC/HMA), SEE SHEET 26 FOR DETAILS

**NOTES:**

1. THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE IMPROVEMENT UNDER THE SHOULDER TO DRAIN TO UNDERDRAINS SHALL BE INCLUDED IN THE COST PER SQ. YD. OF AGGREGATE SUBGRADE IMPROVEMENT 24".
2. THE MAXIMUM ROLLOVER BETWEEN THE PAVEMENT AND THE SHOULDER ON THE HIGH SIDE OF THE SUPERELEVATION IS 8.0%.
3. FOR TIE BAR WITH BARRIER BASE, SEE CONCRETE BARRIER DETAILS.



D162J31-SHT-Typical-02.dgn  
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PLOT SCALE = 20.0000' / in.  
PLOT DATE = 8/15/2019

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CHECKED - MJE  
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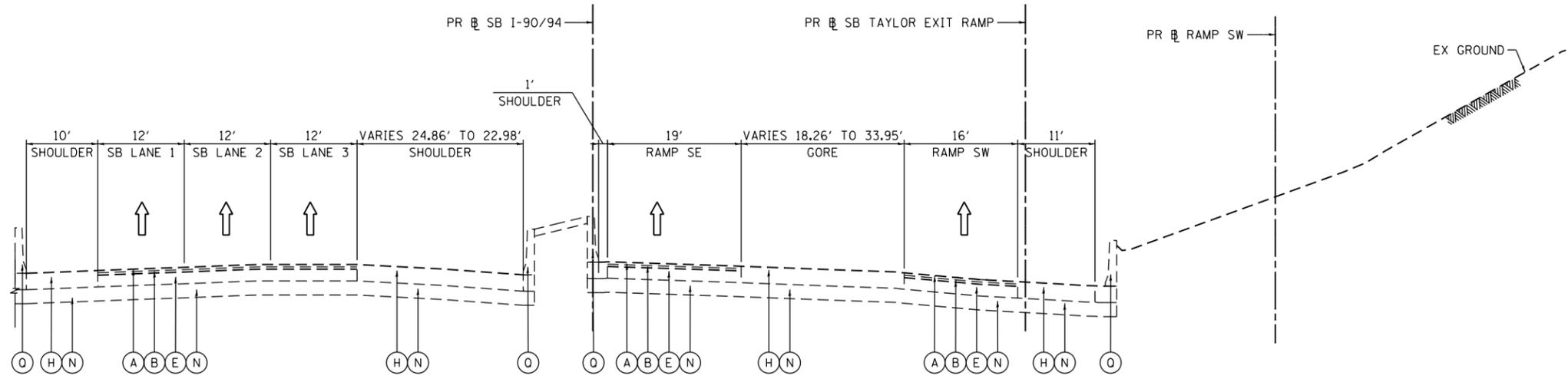
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP

SCALE: NONE SHEET 2 OF 6 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	22
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

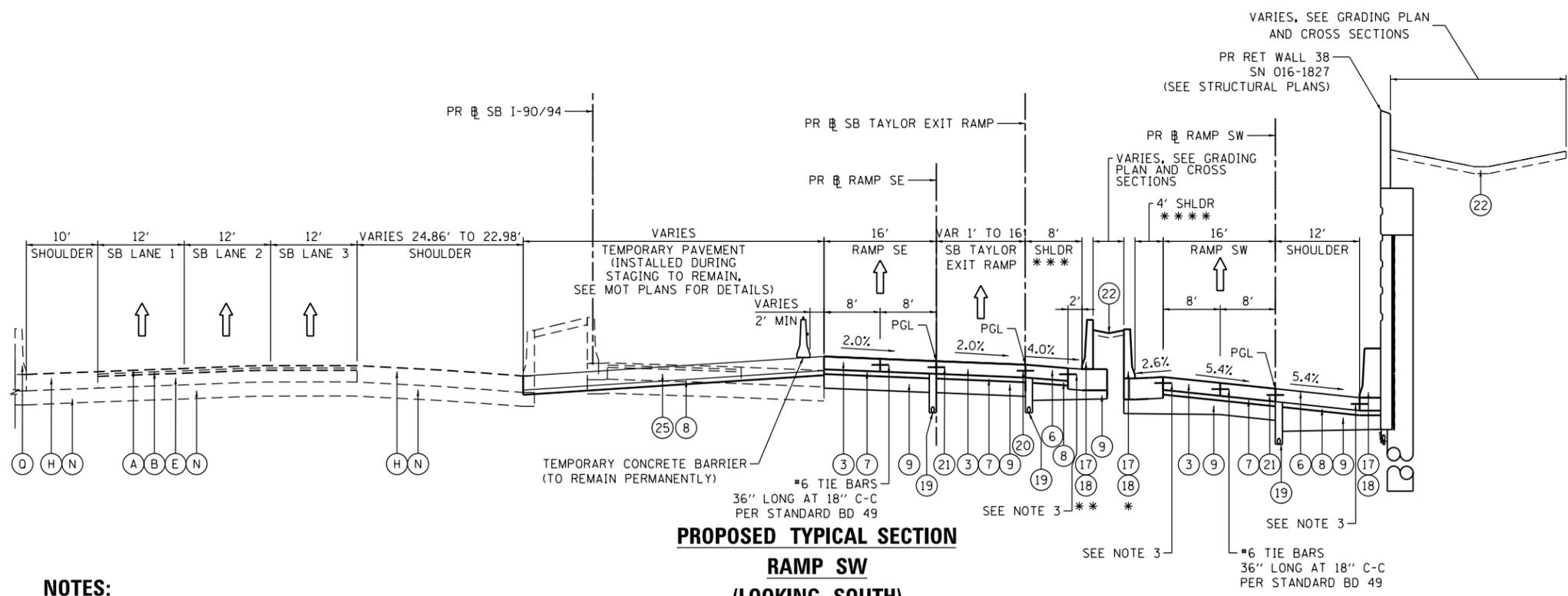


**EXISTING TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1317+21.11 TO STA 1317+90.92 (PR SB RAMP SW)  
STA 6401+97.27 TO STA 6402+76.39 (PR SB TAYLOR EXIT RAMP)

**EXISTING**

- (A) HOT MIX ASPHALT SURFACE COURSE, 1.5"
- (B) HOT MIX ASPHALT BINDER COURSE, 2.5"
- (C) HOT MIX ASPHALT PAVEMENT, 5" TO 11"
- (D) CONTINUOUSLY REINFORCED PCC PAVEMENT, 13"
- (E) PORTLAND CEMENT CONCRETE BASE COURSE, 10"
- (F) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (G) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (H) BITUMINOUS SHOULDER, 13"
- (I) TEMPORARY PAVEMENT (PCC/HMA)
- (J) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (K) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (L) SUBBASE GRANULAR MATERIAL, TYPE B 8"
- (M) SUBBASE GRANULAR MATERIAL, 12"
- (N) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (O) POROUS GRANULAR EMBANKMENT, SPECIAL, 0" TO 30"
- (P) COMBINATION CONCRETE CURB AND GUTTER
- (Q) CONCRETE BARRIER
- (R) TEMPORARY CONCRETE BARRIER (STATE OWNED)
- (S) GUARDRAIL
- (T) PIPE UNDERDRAINS
- (U) TOPSOIL
- (V) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50-2" (ASSUMED)
- (W) PORTLAND CEMENT CONCRETE BASE COURSE 8" (ASSUMED)
- (X) SUBBASE GRANULAR MATERIAL, TYPE B, 6" (ASSUMED)
- (Y) PORTLAND CEMENT CONCRETE PAVEMENT, 11" JOINTED



**PROPOSED TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1317+21.11 TO STA 1317+90.92 (PR SB RAMP SW)  
STA 6401+97.27 TO STA 6402+76.39 (PR SB TAYLOR EXIT RAMP)

**PROPOSED**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) OMITTED
- (3) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (4) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (5) OMITTED
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (7) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (8) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (SEE SHEET 26 FOR ADDITIONAL INFORMATION)
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50: 2"
- (11) PORTLAND CEMENT CONCRETE BASE COURSE 8"
- (12) POROUS GRANULAR EMBANKMENT
- (13) CONCRETE MEDIAN SURFACE, 4"
- (14) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- (15) CONCRETE CURB, TYPE B (SPECIAL), (CDOT)
- (16) CONCRETE GUTTER, TYPE B
- (17) CONCRETE BARRIER WALL (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (18) CONCRETE BARRIER BASE (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (19) PIPE UNDERDRAINS 4" OR 6" (SEE DRAINAGE PLANS)
- (20) #6 TIE BARS, 24" LONG AT 36" C-C (INCLUDED IN PRICE FOR BID FOR PCC SHOULDER OR CURB AND GUTTER)
- (21) #6 TIE BARS, 30" LONG AT 36" C-C (INCLUDED IN PRICE FOR BID FOR PCC PAVEMENT)
- (22) TOPSOIL FURNISH AND PLACE, 4" AND SEEDING OR SODDING (SEE EROSION CONTROL PLANS)
- (23) TOPSOIL FURNISH AND PLACE, 24" AND SEEDING
- (24) OMITTED
- (25) TEMPORARY PAVEMENT (PCC/HMA), SEE SHEET 26 FOR DETAILS

**NOTES:**

1. THE THICKNESS OF AGGREGATE SUBGRADE IMPROVEMENT UNDER THE SHOULDER TO DRAIN TO UNDERDRAINS SHALL BE INCLUDED IN THE COST PER SQ. YD. OF AGGREGATE SUBGRADE IMPROVEMENT 24".
2. THE MAXIMUM ROLLOVER BETWEEN THE PAVEMENT AND THE SHOULDER ON THE HIGH SIDE OF THE SUPERELEVATION IS 8.0%.
3. FOR TIE BAR WITH BARRIER BASE, SEE CONCRETE BARRIER DETAILS.

- \* CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) BEGINS AT STA 1317+21.11 (PR SB RAMP SW)
- \*\* CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) BEGINS AT STA 6401+97.27 (PR SB TAYLOR EXIT RAMP)
- \*\*\* RT PORTLAND CEMENT CONCRETE SHOULDER VARIES FROM 8.65' TO 8' STA 6401+97.27 TO STA 6402+09.04 (PR SB TAYLOR EXIT RAMP)
- \*\*\* LT PORTLAND CEMENT CONCRETE SHOULDER VARIES FROM 4.82' TO 4' STA 1317+21.11 TO STA 1317+35.47 (PR SB RAMP SW)

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USER NAME = oshok.kc  
PLOT SCALE = 20.0000' / in.  
PLOT DATE = 8/15/2019

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DATE - 8/16/2019

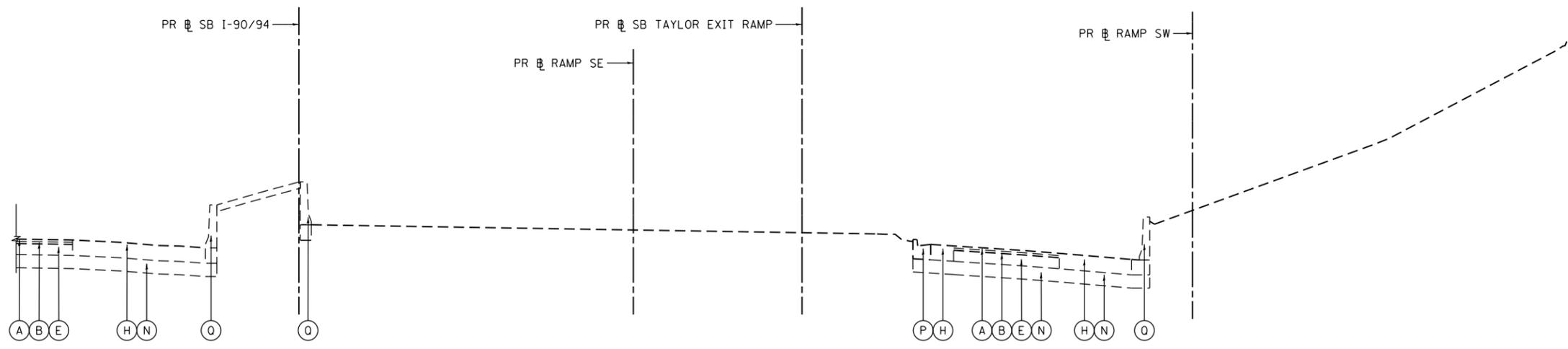
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

SCALE: NONE    SHEET 3 OF 6 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	23
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

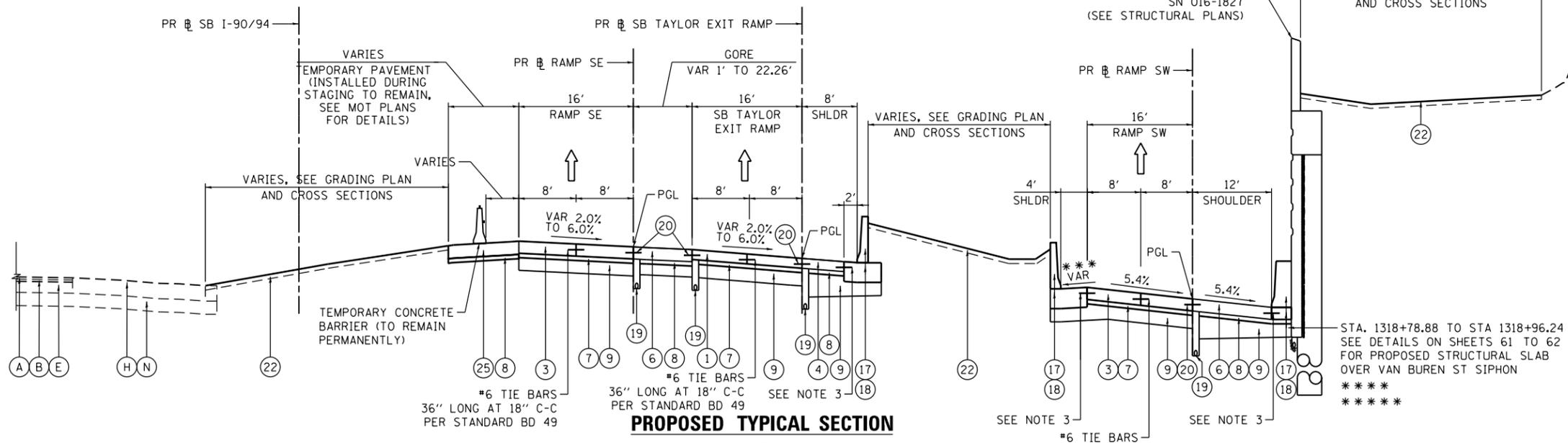


**EXISTING TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1317+90.92 TO STA 1319+75.65 (PR SB RAMP SW)  
 STA 6402+76.39 TO STA 6404+00.00 (PR SB TAYLOR EXIT RAMP)  
 STA 1400+00.00 TO STA 1401+40.00 (PR RAMP SE)

**EXISTING**

- (A) HOT MIX ASPHALT SURFACE COURSE, 1.5"
- (B) HOT MIX ASPHALT BINDER COURSE, 2.5"
- (C) HOT MIX ASPHALT PAVEMENT, 5" TO 11"
- (D) CONTINUOUSLY REINFORCED PCC PAVEMENT, 13"
- (E) PORTLAND CEMENT CONCRETE BASE COURSE, 10"
- (F) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (G) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (H) BITUMINOUS SHOULDER, 13"
- (I) TEMPORARY PAVEMENT (PCC/HMA)
- (J) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (K) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (L) SUBBASE GRANULAR MATERIAL, TYPE B 8"
- (M) SUBBASE GRANULAR MATERIAL, 12"
- (N) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (O) POROUS GRANULAR EMBANKMENT, SPECIAL, 0" TO 30"
- (P) COMBINATION CONCRETE CURB AND GUTTER
- (Q) CONCRETE BARRIER
- (R) TEMPORARY CONCRETE BARRIER (STATE OWNED)
- (S) GUARDRAIL
- (T) PIPE UNDERDRAINS
- (U) TOPSOIL
- (V) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50-2" (ASSUMED)
- (W) PORTLAND CEMENT CONCRETE BASE COURSE 8" (ASSUMED)
- (X) SUBBASE GRANULAR MATERIAL, TYPE B, 6" (ASSUMED)
- (Y) PORTLAND CEMENT CONCRETE PAVEMENT, 11" JOINTED



**PROPOSED TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1317+90.92 TO STA 1319+75.65 (PR SB RAMP SW)  
 STA 6402+76.39 TO STA 6404+00.00 (PR SB TAYLOR EXIT RAMP)  
 STA 1400+00.00 TO STA 1401+40.00 (PR RAMP SE)

- \*\* PR RET WALL 38 (SN 016-1827) ENDS AT STA 1318+74.91 (PR RAMP SW)
- \*\*\* FOR SHOULDER TRANSITION, SEE PAVEMENT ELEVATION AND SUPERELEVATION DETAILS ON SHEET 47
- \*\*\*\* PR PVT CON PCC BR APP SL, STA 1318+60.93 TO STA 1318+78.88 (PR RAMP SW)
- \*\*\*\*\* PR PVT CON PCC BR APP SL, STA 1318+96.24 TO STA 1319+16.94 (PR RAMP SW)

**PROPOSED**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) OMITTED
- (3) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (4) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (5) OMITTED
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (7) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (8) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT 12"  
(SEE SHEET 26 FOR ADDITIONAL INFORMATION)
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50: 2"
- (11) PORTLAND CEMENT CONCRETE BASE COURSE 8"
- (12) POROUS GRANULAR EMBANKMENT
- (13) CONCRETE MEDIAN SURFACE, 4"
- (14) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- (15) CONCRETE CURB, TYPE B (SPECIAL), (CDOT)
- (16) CONCRETE GUTTER, TYPE B
- (17) CONCRETE BARRIER WALL (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (18) CONCRETE BARRIER BASE (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (19) PIPE UNDERDRAINS 4" OR 6" (SEE DRAINAGE PLANS)
- (20) #6 TIE BARS, 24" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC SHOULDER OR CURB AND GUTTER)
- (21) #6 TIE BARS, 30" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC PAVEMENT)
- (22) TOPSOIL FURNISH AND PLACE, 4" AND SEEDING OR SODDING  
(SEE EROSION CONTROL PLANS)
- (23) TOPSOIL FURNISH AND PLACE, 24" AND SEEDING
- (24) OMITTED
- (25) TEMPORARY PAVEMENT (PCC/HMA), SEE SHEET 26 FOR DETAILS

**NOTES:**

1. THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE IMPROVEMENT UNDER THE SHOULDER TO DRAIN TO UNDERDRAINS SHALL BE INCLUDED IN THE COST PER SQ. YD. OF AGGREGATE SUBGRADE IMPROVEMENT 24".
2. THE MAXIMUM ROLLOVER BETWEEN THE PAVEMENT AND THE SHOULDER ON THE HIGH SIDE OF THE SUPERELEVATION IS 8.0%.
3. FOR TIE BAR WITH BARRIER BASE, SEE CONCRETE BARRIER DETAILS.

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PLOT SCALE = 20.0000' / in.	CHECKED - MJE	REVISED -
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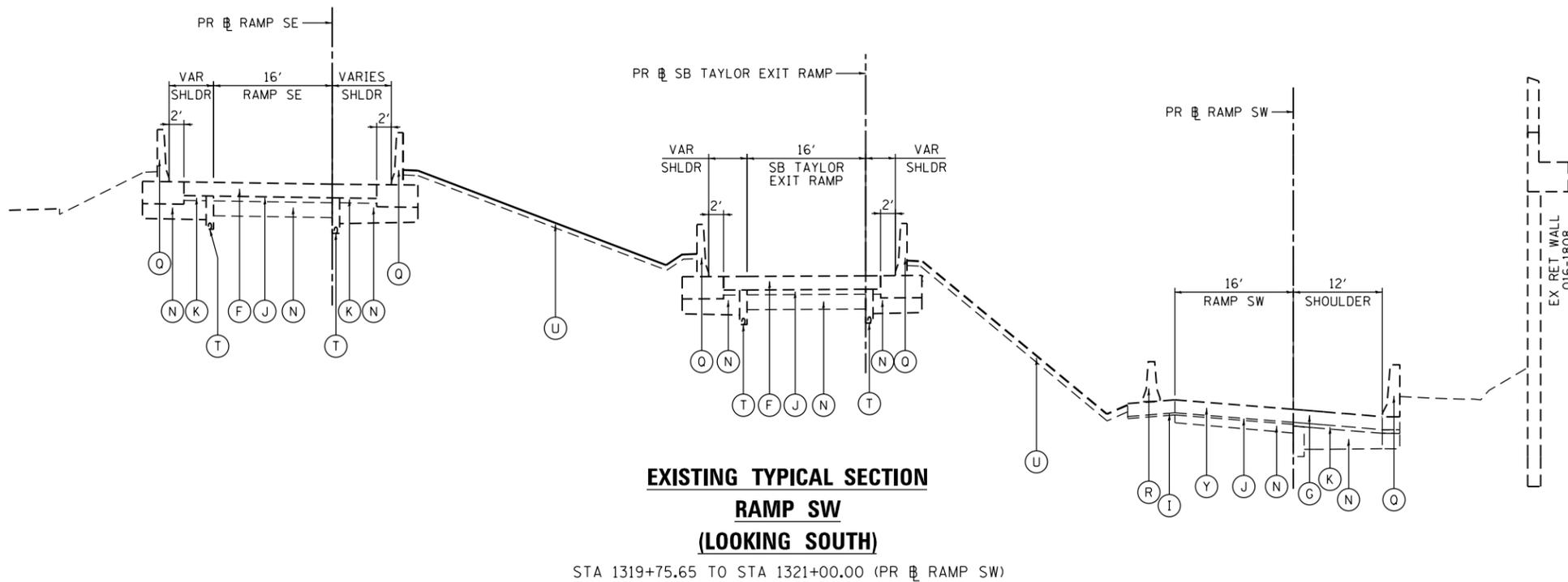
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

SCALE: NONE    SHEET 4 OF 6 SHEETS    STA. TO STA.

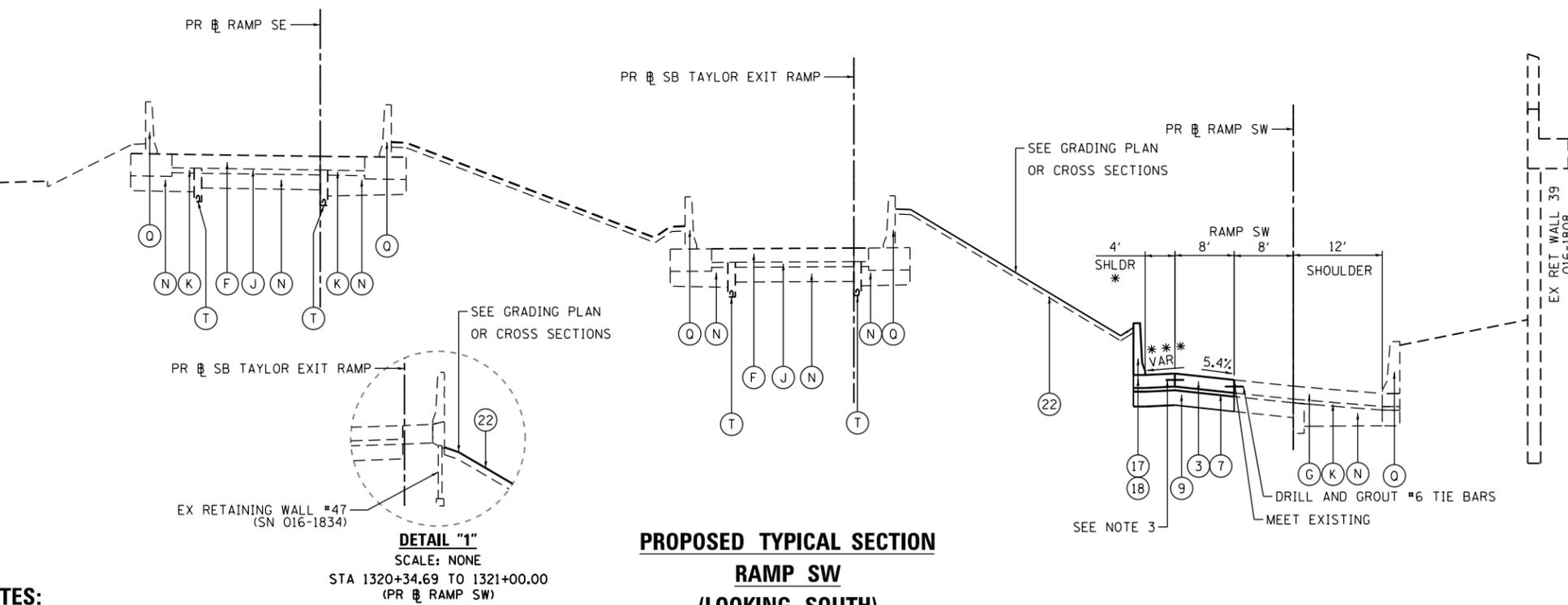
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90/94/290	2019-054-I	COOK	400	24
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**EXISTING TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1319+75.65 TO STA 1321+00.00 (PR SB RAMP SW)



**PROPOSED TYPICAL SECTION  
RAMP SW  
(LOOKING SOUTH)**

STA 1319+75.65 TO STA 1321+00.00 (PR SB RAMP SW)

**EXISTING**

- (A) HOT MIX ASPHALT SURFACE COURSE, 1.5"
- (B) HOT MIX ASPHALT BINDER COURSE, 2.5"
- (C) HOT MIX ASPHALT PAVEMENT, 5" TO 11"
- (D) CONTINUOUSLY REINFORCED PCC PAVEMENT, 13"
- (E) PORTLAND CEMENT CONCRETE BASE COURSE, 10"
- (F) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (G) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (H) BITUMINOUS SHOULDER, 13"
- (I) TEMPORARY PAVEMENT (PCC/HMA)
- (J) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (K) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (L) SUBBASE GRANULAR MATERIAL, TYPE B 8"
- (M) SUBBASE GRANULAR MATERIAL, 12"
- (N) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (O) POROUS GRANULAR EMBANKMENT, SPECIAL, 0" TO 30"
- (P) COMBINATION CONCRETE CURB AND GUTTER
- (Q) CONCRETE BARRIER
- (R) TEMPORARY CONCRETE BARRIER (STATE OWNED)
- (S) GUARDRAIL
- (T) PIPE UNDERDRAINS
- (U) TOPSOIL
- (V) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50-2" (ASSUMED)
- (W) PORTLAND CEMENT CONCRETE BASE COURSE 8" (ASSUMED)
- (X) SUBBASE GRANULAR MATERIAL, TYPE B, 6" (ASSUMED)
- (Y) PORTLAND CEMENT CONCRETE PAVEMENT, 11" JOINTED

**PROPOSED**

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- (2) OMITTED
- (3) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (4) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (5) OMITTED
- (6) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (7) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (8) SUBBASE GRANULAR MATERIAL, TYPE B 4"
- (9) AGGREGATE SUBGRADE IMPROVEMENT 12"  
(SEE SHEET 26 FOR ADDITIONAL INFORMATION)
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50: 2"
- (11) PORTLAND CEMENT CONCRETE BASE COURSE 8"
- (12) POROUS GRANULAR EMBANKMENT
- (13) CONCRETE MEDIAN SURFACE, 4"
- (14) SUBBASE GRANULAR MATERIAL, TYPE B, 6"
- (15) CONCRETE CURB, TYPE B (SPECIAL), (CDOT)
- (16) CONCRETE GUTTER, TYPE B
- (17) CONCRETE BARRIER WALL (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (18) CONCRETE BARRIER BASE (OF VARIOUS TYPES, SEE ROADWAY DETAILS)
- (19) PIPE UNDERDRAINS 4" OR 6" (SEE DRAINAGE PLANS)
- (20) #6 TIE BARS, 24" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC SHOULDER OR CURB AND GUTTER)
- (21) #6 TIE BARS, 30" LONG AT 36" C-C  
(INCLUDED IN PRICE FOR BID FOR PCC PAVEMENT)
- (22) TOPSOIL FURNISH AND PLACE, 4" AND SEEDING OR SODDING  
(SEE EROSION CONTROL PLANS)
- (23) TOPSOIL FURNISH AND PLACE, 24" AND SEEDING
- (24) OMITTED
- (25) TEMPORARY PAVEMENT (PCC/HMA), SEE SHEET 26 FOR DETAILS

**NOTES:**

1. THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE IMPROVEMENT UNDER THE SHOULDER TO DRAIN TO UNDERDRAINS SHALL BE INCLUDED IN THE COST PER SQ. YD. OF AGGREGATE SUBGRADE IMPROVEMENT 24".
2. THE MAXIMUM ROLLOVER BETWEEN THE PAVEMENT AND THE SHOULDER ON THE HIGH SIDE OF THE SUPERELEVATION IS 8.0%.
3. FOR TIE BAR WITH BARRIER BASE, SEE CONCRETE BARRIER DETAILS.

- \* LT PORTLAND CEMENT CONCRETE SHOULDER AND CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) ENDS AT STA 1321+00.00 (PR SB RAMP SW)
- \*\* 8' RAMP SW PAVEMENT FROM STA 1319+75.95 TO STA 1320+51.90 (PR SB RAMP SW)
- \*\*\* FOR SHOULDER TRANSITION, SEE PAVEMENT ELEVATION AND SUPERELEVATION DETAILS ON SHEET 47



D162J31-SHT-Typical-05.dgn  
 USER NAME = ashok.kc  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - OPS  
 DRAWN - OPS  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

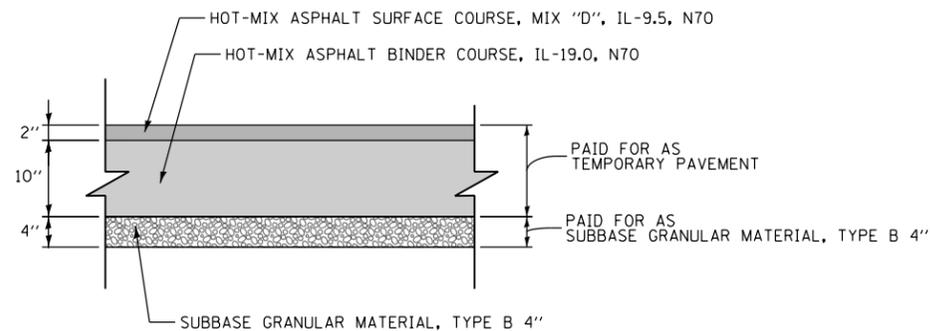
TYPICAL SECTIONS  
 RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP

SCALE: NONE SHEET 5 OF 6 SHEETS STA. TO STA.

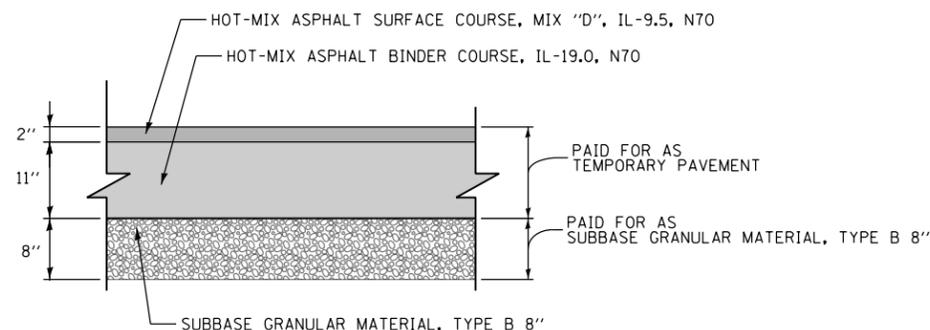
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	25
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**TEMPORARY PAVEMENT DETAILS:**

**DETAIL A: FULL DEPTH TEMPORARY HMA PAVEMENT**

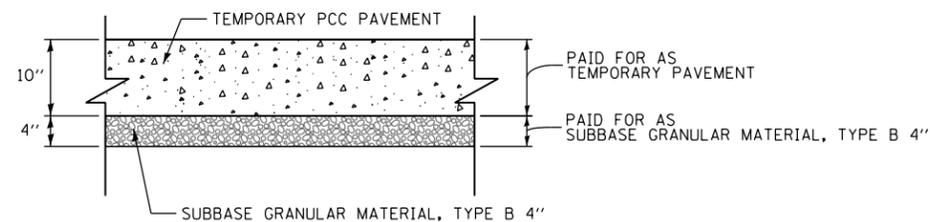


**TEMPORARY HMA PAVEMENT FOR RAMP SW, SE, SB TAYLOR EXIT RAMP**

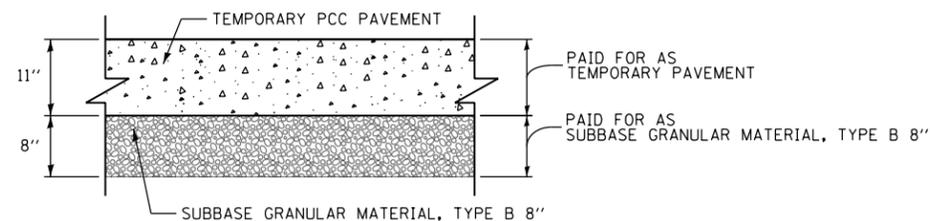


**TEMPORARY HMA PAVEMENT FOR I-90/94 MAINLINE/SHOULDERS**

**DETAIL B: TEMPORARY PCC PAVEMENT**



**TEMPORARY PCC PAVEMENT FOR RAMP SW, SE, SB TAYLOR EXIT RAMP**



**TEMPORARY PCC PAVEMENT FOR I-90/94 MAINLINE/SHOULDERS**

**TEMPORARY PAVEMENT GENERAL NOTES:**

1. THE CONTRACTOR SHALL HAVE THE OPTION OF USING HMA OR PCC SECTION FOR TEMPORARY PAVEMENT, UNLESS OTHERWISE SHOWN ON THE PLANS.
2. TEMPORARY HMA TEMPORARY PAVEMENT SHALL CONSIST OF TWO ITEMS: HMA BINDER COURSE AND HMA SURFACE COURSE.
3. PORTLAND CEMENT CONCRETE TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ARTICLE 1020 OF THE STANDARD SPECIFICATIONS. TEMPORARY PCC PAVEMENT DOES NOT REQUIRE DOWEL BARS.

**AGGREGATE SUBGRADE IMPROVEMENT FOR UNDERCUT**

ALIGNMENT	START STATION	END STATION	LEFT OFFSET	RIGHT OFFSET	DEPTH (IN)	AREA (SQ FT)	AGGREGATE SUBGRADE IMPROVEMENT (CU YD)	GEOTECHNICAL REINFORCEMENT (SQ YD)	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (SQ YD)
RAMP SW	1315+34.11	1318+78.88	33.75' LT	0'	12	8,092	300	900	1,059
RAMP SW	1315+57.01	1318+78.88	0'	15' RT	9	4,251	119	473	614
RAMP SW	1319+16.94	1319+75.65	21.58' LT	0'	12	1,292	48	144	182
RAMP SW	1319+16.94	1319+75.65	0'	15.55' RT	9	911	26	102	136
SB TAYLOR EXIT RAMP	6400+09.57	6401+00.00	16' LT	0'	12	1,447	54	161	210
SB TAYLOR EXIT RAMP	6401+00.00	6402+00.00	8' LT	0'	12	802	30	90	139
ALLOWANCE FOR OTHER AREAS (10% OF AREA ASSUMED)						1,680	58	187	234
<b>TOTAL</b>							<b>635</b>	<b>2,057</b>	<b>2,574</b>

1. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
2. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED, AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
3. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
4. AREAS IDENTIFIED AS 9 INCHES OR 12 INCHES OF DEPTH SHALL HAVE GEOTECHNICAL REINFORCEMENT INSTALLED PER THE REQUIREMENTS OF THE SPECIAL PROVISION.

**HOT MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS (%) @ND5	QMP
<b>TEMPORARY PAVEMENT (IF HMA OPTION IS SELECTED BY CONTRACTOR)</b>		
RAMP SW, RAMP SE, SB TAYLOR EXIT RAMP		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70: 2"	4% @ 70 GYR	QC/OA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70: 10" (IN 4 LIFTS)	4% @ 70 GYR	QC/OA
NB I-90/94		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70: 2"	4% @ 70 GYR	QC/OA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70: 11" (IN 4 LIFTS)	4% @ 70 GYR	QC/OA
<b>PAVEMENT RECONSTRUCTION</b>		
RAMP SW, RAMP SE, SB TAYLOR EXIT RAMP		
STABILIZED SUBBASE - HOT-MIX ASPHALT, 4" (HMA BINDER IL-19 MM)	3% @ 50 GYR	QC/OA
GLADYS AVENUE		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50: 2"	4% @ 50 GYR	QC/OA
<b>QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP)</b>		

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXTURES SHALL BE "SBS/SBR PG 76-22" AND FOR NON POLYMERIZED HMA THE AC TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.
3. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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**REMOVAL SCHEDULE**

SHEET	STATION	BASELINE	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	TREE REMOVAL (OVER 15 UNITS DIAMETER)	TEMPORARY FENCE	TREE TRUNK PROTECTION	TREE PRUNING (1 TO 10 INCH DIAMETER)	TREE PRUNING (OVER 10 INCH DIAMETER)	PAVEMENT REMOVAL	GUTTER REMOVAL	COMBINATION CURB AND GUTTER REMOVAL	CONCRETE BARRIER REMOVAL	MEDIAN REMOVAL	PAVED SHOULDER REMOVAL	GUARDRAIL REMOVAL	REMOVE TEMPORARY CONCRETE BARRIER, STATE OWNED	REMOVE IMPACT ATTENUATOR SAND MODULE	APPROACH SLAB REMOVAL	FENCE REMOVAL
			UNIT	UNIT	FOOT	EACH	EACH	EACH	SQ YD	FOOT	FOOT	FOOT	SQ FT	SQ YD	FOOT	FOOT	EACH	SQ YD	FOOT
1	1310+00.00 TO 1315+00.00	PR RAMP SW	0	0	0	0	0	0	668	0	131	700	5,444	474	0	0	1	66	1
2	1315+00.00 TO 1320+51.92	PR RAMP SW	207	107	75	5	4	1	2,398	120	156	1,324	3,176	257	56	122	0	0	241
TOTALS			207	107	75	5	4	1	3,066	120	287	2,023	8,620	731	56	122	1	66	242

**ROADWAY SCHEDULE**

SHEET	STATION	BASELINE	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	POROUS GRANULAR EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT 12"	AGGREGATE SUBGRADE IMPROVEMENT 24"	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SUBBASE GRANULAR MATERIAL, TYPE B 6"	STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"	PORTLAND CEMENT CONCRETE BASE COURSE 8"	BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	AGGREGATE SURFACE COURSE, TYPE A 6"	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB	PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)	PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)	PROTECTIVE COAT	PORTLAND CEMENT CONCRETE SHOULDERS 9"	PORTLAND CEMENT CONCRETE SHOULDERS 11"	CONCRETE MEDIAN SURFACE, 4 INCH	CONCRETE BARRIER TRANSITION	CONCRETE BARRIER BASE	
			SQ YD	CU YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	POUND	TON	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ FT	FOOT
1	1310+00.00 TO 1315+00.00	PR RAMP SW	0	0	0	0	0	0	0	0	0	0	209	0	0	0	0	0	0	0	0	0	0
2	1315+00.00 TO 1321+07.13	PR RAMP SW	163	6	3,218	163	922	72	2,127	60	27	7	22	86	139	1,758	4,049	50	892	28	15	15	
TOTALS			163	6	3,218	163	922	72	2,127	60	27	7	231	86	139	1,758	4,049	50	892	28	15	15	

SHEET	STATION	BASELINE	IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	TEMPORARY PAVEMENT MARKING - LINE 6"	BARRIER WALL REFLECTORS, TYPE C	PAVEMENT GROOVING	TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)	CONCRETE BARRIER BASE (SPECIAL NO. 1)	CONCRETE BARRIER BASE (SPECIAL NO. 2)	CONCRETE BARRIER BASE (SPECIAL NO. 3)	CONCRETE BARRIER BASE (SPECIAL NO. 4)	CONCRETE BARRIER WALL (SPECIAL NO. 1)	CONCRETE CURB, TYPE B (SPECIAL) (CDOIT)	CONCRETE BARRIER WALL (SPECIAL)	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)	GEOTECHNICAL REINFORCEMENT	DRILL AND GROUT #6 TIE BARS
			EACH	EACH	FOOT	EACH	SQ YD	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ YD	EACH
1	1310+00.00 TO 1315+00.00	PR RAMP SW	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2	1315+00.00 TO 1321+07.13	PR RAMP SW	2	1	250	101	1,986	250	186	385	83	314	314	67	83	571	163	26
TOTALS			2	1	250	101	1,986	250	186	385	83	314	314	67	83	571	163	26

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DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES  
 SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	27
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**EARTHWORK SCHEDULE**

ALIGNMENT	STATION TO STATION		EARTH EXCAVATION			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	EXCAVATION TO BE USED IN EMBANKMENT (NOT A PAY ITEM) [EARTH EXCAVATION X 0.85]			EMBANKMENT (NOT A PAY ITEM) (NOTE 2)			EARTHWORK BALANCE WASTE (+) (NOT A PAY ITEM) OR SHORTAGE (-) (FURNISHED EXCAVATION) (NOTE 3)		
			CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	
			STAGE 1	STAGE 2	STAGE 3	STAGE ALL	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3	STAGE 1	STAGE 2	STAGE 3
SB I-90/94 62J31 RAMP LANES	"P-CIR-SB"	"P-CIR-SB"													
	1313+76.10	TO 1314+26.10	0	0	0	0	0	0	0	35	0	0	-35	0	
	1314+26.10	TO 1314+76.10	0	17	0	0	0	8	0	0	69	0	0	-61	0
	1314+76.10	TO 1314+86.54	0	8	0	0	0	4	0	0	16	0	0	-12	0
	1314+86.54	TO 1314+96.94	0	15	0	0	0	7	0	0	17	0	0	-10	0
	1314+96.94	TO 1315+00.00	6	5	0	3	3	3	0	1	8	0	+2	-5	0
	1315+00.00	TO 1315+50.00	336	59	0	93	143	26	0	16	124	0	+127	-98	0
	1315+50.00	TO 1316+00.00	651	56	0	91	277	24	0	26	55	0	+251	-31	0
	1316+00.00	TO 1316+50.00	863	34	0	101	367	15	0	36	8	0	+331	+7	0
	1316+50.00	TO 1317+00.00	880	31	0	93	374	14	0	33	6	0	+341	+8	0
	1317+00.00	TO 1317+50.00	728	14	28	84	310	6	12	97	1	30	+213	+5	-18
	1317+50.00	TO 1318+00.00	522	6	36	97	222	3	16	240	0	81	-18	+3	-65
	1318+00.00	TO 1318+50.00	395	26	22	116	168	12	10	425	1	95	-257	+11	-85
	1318+50.00	TO 1319+00.00	225	97	28	96	96	42	12	574	1	110	-478	+41	-98
	1319+00.00	TO 1319+50.00	50	129	27	37	22	55	12	309	1	124	-287	+54	-112
	1319+50.00	TO 1320+00.00	44	52	26	22	19	23	12	0	0	107	+19	+23	-95
	1320+00.00	TO 1320+50.00	75	0	16	34	32	0	7	0	0	88	+32	0	-81
	1320+50.00	TO 1321+00.00	42	0	16	33	18	0	7	0	0	68	+18	0	-61
	1321+00.00	TO 1321+50.00	7	0	14	35	3	0	6	0	0	55	+3	0	-49
	1321+50.00	TO 1322+00.00	0	0	17	60	0	0	8	0	0	37	0	0	-29
	1322+00.00	TO 1322+50.00	0	0	26	86	0	0	12	0	0	42	0	0	-30
	1322+50.00	TO 1323+00.00	0	0	9	72	0	0	4	0	0	92	0	0	-88
	1323+00.00	TO 1323+50.00	0	0	0	39	0	0	0	0	0	108	0	0	-108
1323+50.00	TO 1323+60.00	0	0	0	5	0	0	0	0	0	18	0	0	-18	
1323+60.00	TO 1324+00.00	0	0	0	13	0	0	0	0	0	50	0	0	-50	
JACKSON ENTRANCE RAMP (NB I-90/94)			0	0	0	1,875	0	0	0	0	149	0	0	-149	0
TOTALS			4,824	549	265	3,085	2,054	242	118	1,757	491	1,105	+297	-249	-987
ROUNDED TOTAL (NOTE 4)			5,640			3,085	2,415			3,355			-940		

**NOTES:**

- ESTIMATED SHRINKAGE FACTOR = 15%
- APPROXIMATE EMBANKMENT QUANTITY IS SHOWN FOR INFORMATION ONLY.
- APPROXIMATE EARTHWORK BALANCE IS SHOWN FOR INFORMATION ONLY.
- EARTH EXCAVATION TOTAL IS ROUNDED UP TO THE NEAREST 5 CU YD.
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS INCLUDES UNSUITABLE MATERIALS IN EARTH EXCAVATION AND TOPSOIL. TOPSOIL DEPTH IS ESTIMATED AT A 9" THICKNESS.
- 50% OF THE MATERIAL EXCAVATED AS EARTH EXCAVATION IS ASSUMED TO BE APPROPRIATE AS EMBANKMENT.

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DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

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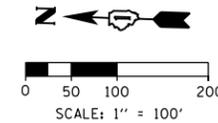
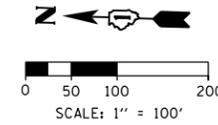
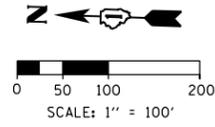
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	28
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				





MATCH LINE STA 1205+50.00 (PR RAMP WS)

MATCH LINE STA 1205+50.00 (PR RAMP WS)

MATCH LINE STA 1327+50.00 (PR RAMP SW)

MATCH LINE STA 1327+50.00 (PR RAMP SW)

MATCH LINE STA 6212+50.00 (PR SB I-90/94)  
SEE SHEET NO. 29

MATCH LINE STA 6240+00.00 (PR SB I-90/94)  
SEE SHEET NO. 31

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DATE - 8/16/2019

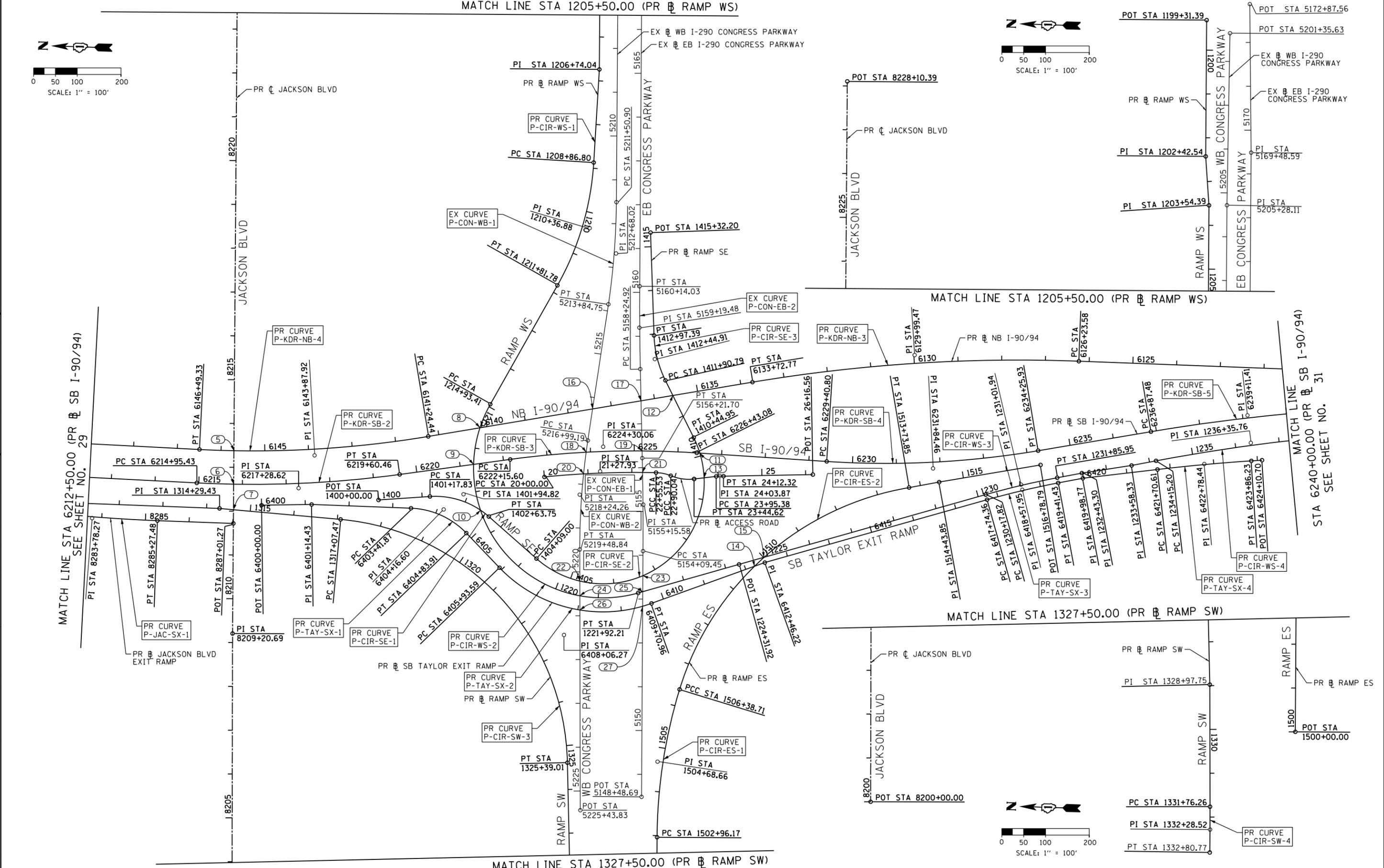
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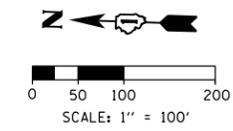
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: SHEET 2 OF 11 SHEETS STA. 6212+50.00 TO STA. 6240+00.00

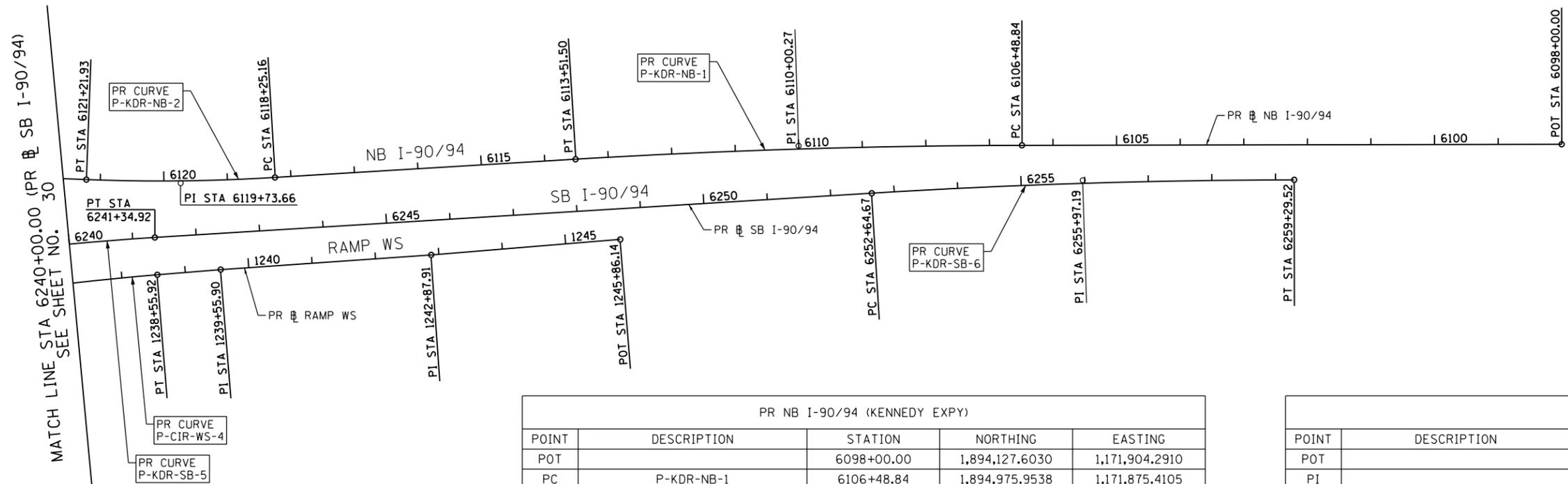
F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	30
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				





PR SB I-90/94 (KENNEDY EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		6200+00.00	1,900,448.7559	1,171,425.9287
PC	P-KDR-SB-1	6206+50.45	1,899,798.6420	1,171,446.7969
PI	P-KDR-SB-1	6207+41.31	1,899,707.8289	1,171,449.7119
PT	P-KDR-SB-1	6208+32.13	1,899,616.9863	1,171,447.9406
PC	P-KDR-SB-2	6214+95.43	1,898,953.8079	1,171,435.0096
PI	P-KDR-SB-2	6217+28.62	1,898,720.6616	1,171,430.4636
PT	P-KDR-SB-2	6219+60.46	1,898,490.7137	1,171,469.2172
PC	P-KDR-SB-3	6222+15.60	1,898,239.1233	1,171,511.6183
PI	P-KDR-SB-3	6224+30.06	1,898,027.6457	1,171,547.2591

PR SB I-90/94 (KENNEDY EXPY)				
PT	P-KDR-SB-3	6226+43.08	1,897,813.3051	1,171,540.1066
PC	P-KDR-SB-4	6229+40.80	1,897,515.7561	1,171,530.1774
PI	P-KDR-SB-4	6231+84.46	1,897,272.2283	1,171,522.0508
PT	P-KDR-SB-4	6234+25.93	1,897,033.3678	1,171,570.1906
PC	P-KDR-SB-5	6236+87.48	1,896,776.9793	1,171,621.8630
PI	P-KDR-SB-5	6239+11.41	1,896,557.4628	1,171,666.1042
PT	P-KDR-SB-5	6241+34.92	1,896,334.5125	1,171,687.0321
PC	P-KDR-SB-6	6252+64.67	1,895,209.7116	1,171,792.6148
PI	P-KDR-SB-6	6255+97.19	1,894,878.6443	1,171,823.6913
PT	P-KDR-SB-6	6259+29.52	1,894,546.3142	1,171,835.0049



PR NB I-90/94 (KENNEDY EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		6098+00.00	1,894,127.6030	1,171,904.2910
PC	P-KDR-NB-1	6106+48.84	1,894,975.9538	1,171,875.4105
PI	P-KDR-NB-1	6110+00.27	1,895,327.1816	1,171,863.4536
PT	P-KDR-NB-1	6113+51.50	1,895,677.1115	1,171,831.0036
PC	P-KDR-NB-2	6118+25.16	1,896,148.7423	1,171,787.2679
PI	P-KDR-NB-2	6119+73.66	1,896,296.6138	1,171,773.5554
PT	P-KDR-NB-2	6121+21.93	1,896,445.1173	1,171,774.3975
PC	P-KDR-NB-3	6126+23.58	1,896,946.7562	1,171,777.2424
PI	P-KDR-NB-3	6129+99.47	1,897,322.6399	1,171,779.3740
PT	P-KDR-NB-3	6133+72.77	1,897,691.2163	1,171,705.5873
PC	P-KDR-NB-4	6141+24.44	1,898,428.2614	1,171,558.0356
PI	P-KDR-NB-4	6143+87.92	1,898,686.6176	1,171,506.3144
PT	P-KDR-NB-4	6146+49.33	1,898,950.0500	1,171,511.4509
PC	P-KDR-NB-5	6156+44.09	1,899,944.6234	1,171,530.8436
PI	P-KDR-NB-5	6159+31.09	1,900,231.5637	1,171,536.4385
PT	P-KDR-NB-5	6162+17.96	1,900,518.4107	1,171,527.2272
PC	P-KDR-NB-6	6169+86.63	1,901,286.6853	1,171,502.5563
PI	P-KDR-NB-6	6171+80.29	1,901,480.2512	1,171,496.3405
PCC	P-KDR-NB-6/P-KDR-NB-7	6173+72.78	1,901,669.1180	1,171,453.4956
PI	P-KDR-NB-7	6174+40.21	1,901,734.8790	1,171,438.5776
PT	P-KDR-NB-7	6175+07.63	1,901,800.1030	1,171,421.4635

PR RAMP WS				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		1199+31.39	1,898,090.9472	1,173,149.4167
PI		1202+42.54	1,898,083.0859	1,172,838.3707
PI		1203+54.39	1,898,072.0202	1,172,727.0678
PI		1206+74.04	1,898,063.9440	1,172,407.5197
PC	P-CIR-WS-1	1208+86.80	1,898,070.0806	1,172,194.8475
PI	P-CIR-WS-1	1210+36.88	1,898,074.4092	1,172,044.8348
PT	P-CIR-WS-1	1211+81.78	1,898,144.0652	1,171,911.9039
PC	P-CIR-WS-2	1214+93.41	1,898,288.7014	1,171,635.8818
PI	P-CIR-WS-2	1222+12.72	1,898,622.5635	1,170,998.7427
PT	P-CIR-WS-2	1221+92.21	1,897,936.9621	1,171,216.3684
POT		1224+31.92	1,897,708.4901	1,171,288.8906
PC	P-CIR-WS-3	1230+17.82	1,897,150.0454	1,171,466.1538
PI	P-CIR-WS-3	1231+01.94	1,897,069.8686	1,171,491.6037
PT	P-CIR-WS-3	1231+85.95	1,896,987.7565	1,171,509.8686
PI		1232+43.30	1,896,931.7753	1,171,522.3210
PI		1233+58.33	1,896,818.6854	1,171,543.3788
PC	P-CIR-WS-4	1234+15.20	1,896,763.1697	1,171,555.7276
PI	P-CIR-WS-4	1236+35.76	1,896,547.8714	1,171,603.6182
PT	P-CIR-WS-4	1238+55.92	1,896,328.7351	1,171,628.6403
PI		1239+55.90	1,896,229.4008	1,171,639.9828
PI		1242+87.91	1,895,899.1013	1,171,673.6721
POT		1245+86.14	1,895,602.7960	1,171,707.5057

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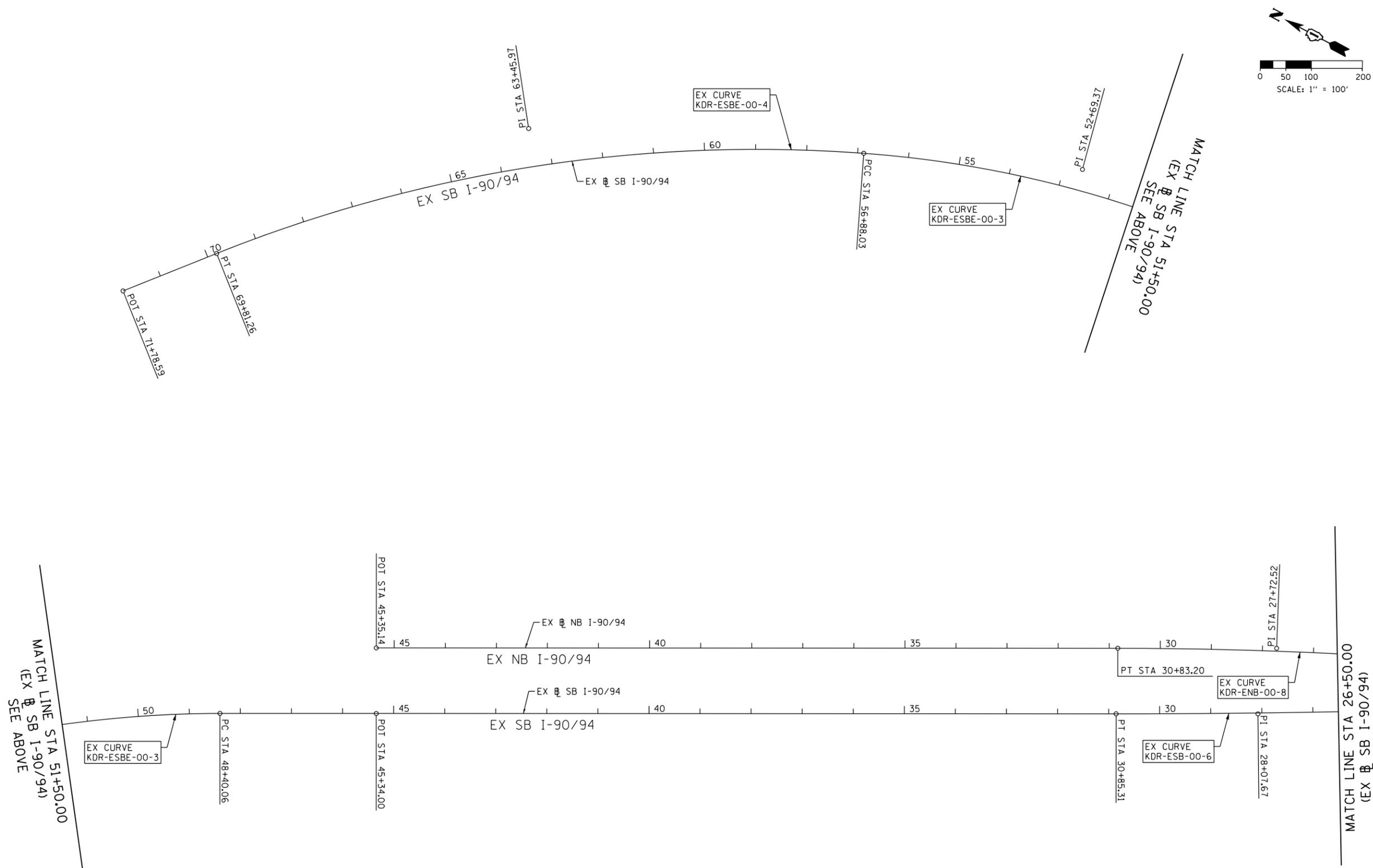
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: 1"=100'    SHEET 3 OF 11 SHEETS    STA. 6240+00.00 TO STA. 6259+29.52

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	31
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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MATCH LINE STA 51+50.00  
(EX SB I-90/94)  
SEE ABOVE

MATCH LINE STA 26+50.00  
(EX SB I-90/94)  
SEE SHEET NO. 33



D162J31-SHT-ATB-03A.dgn  
USER NAME = oshok.kc  
PLOT SCALE = 200.0000' / in.  
PLOT DATE = 8/14/2019

DESIGNED - OPS  
DRAWN - ZND  
CHECKED - MJJ  
DATE - 8/16/2019

REVISED -  
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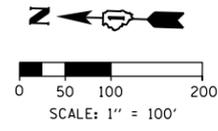
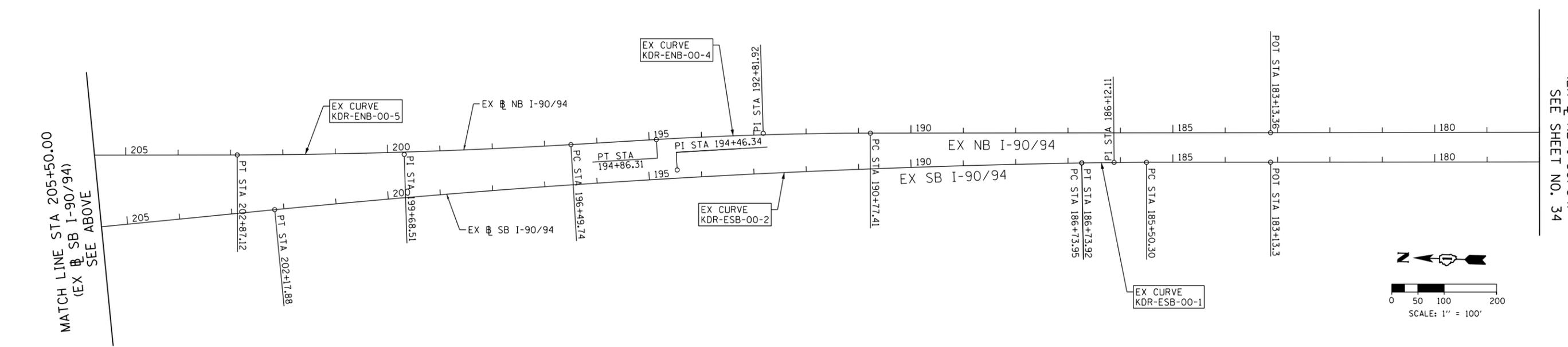
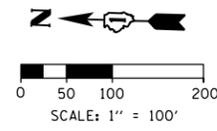
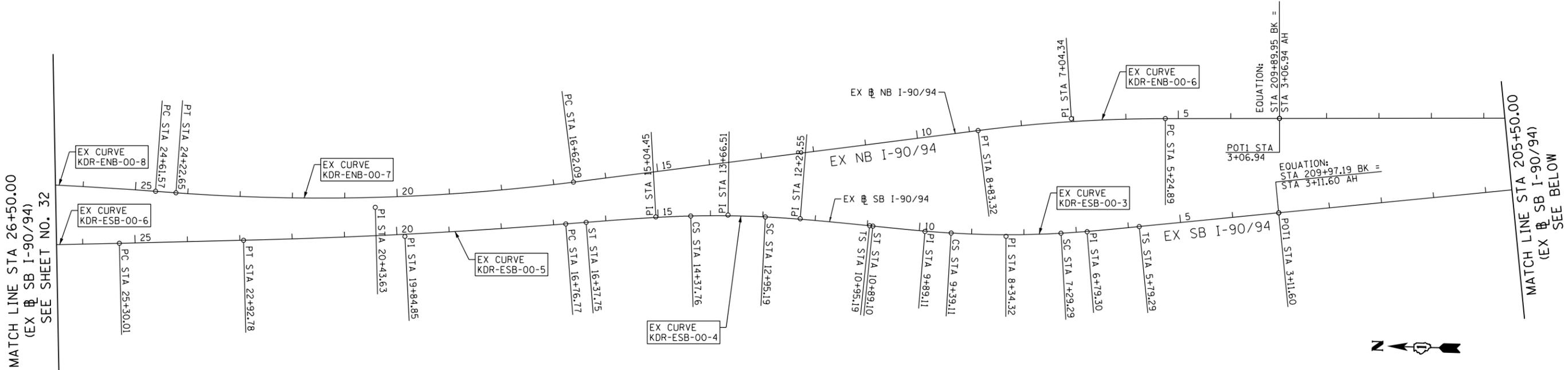
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: SHEET 4 OF 11 SHEETS STA. TO STA. 178+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	32
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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 CHECKED - MJE  
 DATE - 8/16/2019

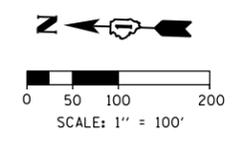
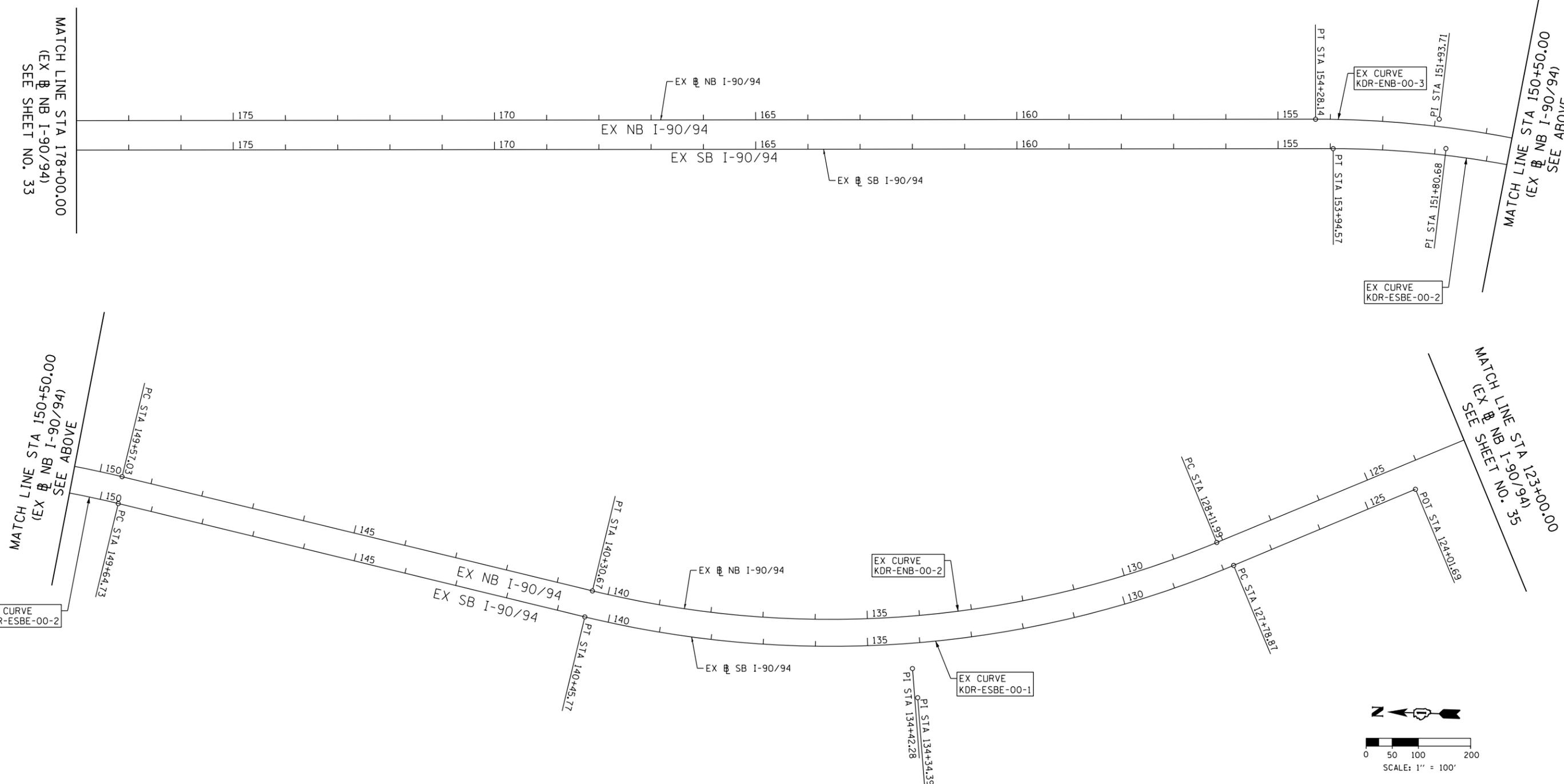
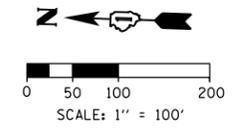
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: SHEET 5 OF 11 SHEETS STA. TO STA. 178+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	33
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



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D162J31-SHT-ATB-03C.dgn  
 USER NAME = oshok.kc  
 PLOT SCALE = 200.0000' / in.  
 PLOT DATE = 8/14/2019

DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

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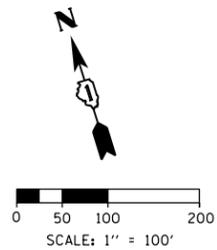
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: SHEET 6 OF 11 SHEETS STA. 6212+50.00 TO STA. 6240+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	34
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**EXISTING CURVE DATA**

**EXISTING EB I-290 CONGRESS PARKWAY**

EX CURVE P-CON-EB-1 PI STA. = 5155+15.58 Δ = 1° 35' 42" (LT) D = 0° 45' 05" R = 7,624.00' T = 106.13' L = 212.25' E = 0.74' P.C. STA. = 5154+09.45 P.T. STA. = 5156+21.70	EX CURVE P-CON-EB-2 PI STA. = 5159+19.48 Δ = 1° 39' 04" (RT) D = 0° 52' 23" R = 6,562.00' T = 94.56' L = 189.11' E = 0.68' P.C. STA. = 5158+24.92 P.T. STA. = 5160+14.03
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**EXISTING WB I-290 CONGRESS PARKWAY**

EX CURVE P-CON-WB-1 PI STA. = 5212+68.02 Δ = 8° 12' 18" (RT) D = 3° 30' 31" R = 1,633.00' T = 117.13' L = 233.85' E = 4.20' P.C. STA. = 5211+50.90 P.T. STA. = 5213+84.75	EX CURVE P-CON-WB-2 PI STA. = 5218+24.26 Δ = 8° 42' 22" (LT) D = 3° 29' 14" R = 1,643.00' T = 125.07' L = 249.65' E = 4.75' P.C. STA. = 5216+99.19 P.T. STA. = 5219+48.84
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**EXISTING CURVE DATA**

**EXISTING NB I-90/94**

EX CURVE KDR-ENB-00-1 PI STA. = 117+55.38 Δ = 48° 27' 45" (RT) D = 4° 29' 25" R = 1,276.00' T = 574.29' L = 1,079.28' E = 123.28' P.C. STA. = 111+81.09 P.T. STA. = 122+60.37	EX CURVE KDR-ENB-00-2 PI STA. = 134+42.28 Δ = 36° 04' 00" (RT) D = 2° 57' 34" R = 1,936.00' T = 630.29' L = 1,218.67' E = 100.02' P.C. STA. = 128+11.99 P.T. STA. = 140+30.67	EX CURVE KDR-ENB-00-3 PI STA. = 151+93.71 Δ = 13° 40' 27" (LT) D = 2° 54' 09" R = 1,974.00' T = 236.68' L = 471.12' E = 14.14' P.C. STA. = 149+57.03 P.T. STA. = 154+28.14	EX CURVE KDR-ENB-00-4 PI STA. = 192+81.92 Δ = 3° 17' 58" (LT) D = 0° 48' 25" R = 7,100.63' T = 204.51' L = 408.90' E = 2.94' P.C. STA. = 190+77.41 P.T. STA. = 194+86.31	EX CURVE KDR-ENB-00-5 PI STA. = 199+68.51 Δ = 3° 11' 12" (RT) D = 0° 30' 00" R = 11,460.32' T = 318.77' L = 637.38' E = 4.43' P.C. STA. = 196+49.74 P.T. STA. = 202+87.12	EX CURVE KDR-ENB-00-6 PI STA. = 7+04.34 Δ = 7° 10' 07" (LT) D = 2° 00' 00" R = 2,864.80' T = 179.45' L = 358.43' E = 5.61' P.C. STA. = 5+24.89 P.T. STA. = 8+83.32	EX CURVE KDR-ENB-00-7 PI STA. = 20+43.63 Δ = 11° 24' 28" (RT) D = 1° 30' 00" R = 3,819.89' T = 381.54' L = 760.55' E = 19.01' P.C. STA. = 16+62.09 P.T. STA. = 24+22.65	EX CURVE KDR-ENB-00-8 PI STA. = 27+72.52 Δ = 4° 08' 39" (LT) D = 0° 40' 00" R = 8,594.42' T = 310.95' L = 621.63' E = 5.62' P.C. STA. = 24+61.57 P.T. STA. = 30+83.20
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**EXISTING SB I-90/94**

EX CURVE KDR-ESB-00-1 PI STA. = 186+12.11 Δ = 0° 55' 38" (LT) D = 0° 45' 00" R = 7,640.04' T = 61.81' L = 123.62' E = 0.25' P.C. STA. = 185+50.30 P.T. STA. = 186+73.92	EX CURVE KDR-ESB-00-2 PI STA. = 194+46.34 Δ = 4° 37' 53" (LT) D = 0° 18' 00" R = 19,100.00' T = 772.39' L = 1,543.93' E = 15.61' P.C. STA. = 186+73.95 P.T. STA. = 202+17.88	EX CURVE KDR-ESB-00-3 PI STA. = 8+34.32 Δ = 6° 40' 44" (RT) D = 3° 10' 59" R = 1,800.01' T = 105.03' L = 209.82' E = 3.06' S.C. STA. = 7+29.29 C.S. STA. = 9+39.11	EX CURVE KDR-ESB-00-4 PI STA. = 13+66.51 Δ = 4° 16' 36" (LT) D = 2° 59' 59" R = 1,910.09' T = 71.32' L = 142.57' E = 1.33' S.C. STA. = 12+95.19 C.S. STA. = 14+37.76	EX CURVE KDR-ESB-00-5 PI STA. = 19+84.85 Δ = 3° 04' 48" (RT) D = 0° 30' 00" R = 11,459.27' T = 308.08' L = 616.01' E = 4.14' P.C. STA. = 16+76.77 P.T. STA. = 22+92.78	EX CURVE KDR-ESB-00-6 PI STA. = 28+07.67 Δ = 1° 23' 18" (RT) D = 0° 15' 00" R = 22,918.30' T = 277.67' L = 555.31' E = 1.68' P.C. STA. = 25+30.01 P.T. STA. = 30+85.31	EX CURVE KDR-ESBE-00-1 PI STA. = 134+34.39 Δ = 36° 17' 38" (RT) D = 2° 51' 53" R = 2,000.00' T = 655.52' L = 1,266.90' E = 104.69' P.C. STA. = 127+78.87 P.T. STA. = 140+45.77	EX CURVE KDR-ESBE-00-2 PI STA. = 151+80.68 Δ = 13° 40' 56" (LT) D = 3° 10' 59" R = 1,800.00' T = 215.95' L = 429.84' E = 12.91' P.C. STA. = 149+64.73 P.T. STA. = 153+94.57	EX CURVE KDR-ESBE-00-3 PI STA. = 52+69.37 Δ = 22° 05' 03" (LT) D = 2° 36' 16" R = 2,200.00' T = 429.31' L = 847.97' E = 41.50' P.C. STA. = 48+40.06 P.T. STA. = 56+88.03
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EX CURVE KDR-ESBE-00-4  
PI STA. = 63+45.97  
Δ = 25° 59' 56" (LT)  
D = 2° 00' 37"  
R = 2,850.00'  
T = 657.94'  
L = 1,293.23'  
E = 74.96'  
P.C. STA. = 56+88.03  
P.T. STA. = 69+81.26



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: SHEET 7 OF 11 SHEETS STA. 6212+50.00 TO STA. 6240+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	35
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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PR RAMP SW				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		1300+00.00	1,900,329.0273	1,171,396.7549
PC	P-CIR-SW-1	1304+11.65	1,899,917.6707	1,171,381.2291
PI	P-CIR-SW-1	1304+86.54	1,899,842.8325	1,171,378.4045
PT	P-CIR-SW-1	1305+61.40	1,899,767.9492	1,171,379.5129
PC	P-CIR-SW-2	1306+81.58	1,899,647.7811	1,171,381.2915
PI	P-CIR-SW-2	1307+47.72	1,899,581.6443	1,171,382.2704
PT	P-CIR-SW-2	1308+13.86	1,899,515.5013	1,171,381.9013
PI		1314+29.43	1,898,899.9445	1,171,378.4667
PC	P-CIR-SW-3	1317+07.47	1,898,622.3715	1,171,362.3663
PI	P-CIR-SW-3	1322+16.98	1,898,113.7170	1,171,332.8621
PT	P-CIR-SW-3	1325+39.01	1,898,086.2098	1,170,824.0956
PI		1328+97.75	1,898,066.8422	1,170,465.8764
PC	P-CIR-SW-4	1331+76.26	1,898,057.3708	1,170,187.5283
PI	P-CIR-SW-4	1332+28.52	1,898,055.5938	1,170,135.3038
PT	P-CIR-SW-4	1332+80.77	1,898,054.6988	1,170,083.0568

PR RAMP ES				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		1500+00.00	1,897,868.0913	1,170,364.4958
PC	P-CIR-ES-1	1502+96.17	1,897,875.8640	1,170,660.5601
PI	P-CIR-ES-1	1504+68.66	1,897,880.3909	1,170,832.9935
PCC	P-CIR-ES-1/P-CIR-ES-2	1506+38.71	1,897,835.2490	1,170,999.4747
PI	P-CIR-ES-2	1510+49.08	1,897,727.8543	1,171,395.5407
PT	P-CIR-ES-2	1513+73.85	1,897,325.5749	1,171,476.6158
PI		1514+43.85	1,897,257.1559	1,171,491.4250
POT		1516+78.79	1,897,026.8481	1,171,537.8411

PR JACKSON BOULEVARD				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		8200+00.00	1,898,833.4016	1,170,173.8624
PI		8209+20.69	1,898,860.9430	1,171,094.1439
POT		8228+10.39	1,898,907.7815	1,172,983.2643

PR RAMP SE				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		1400+00.00	1,898,537.2242	1,171,409.4324
PC	P-CIR-SE-1	1401+17.83	1,898,419.9713	1,171,421.0951
PI	P-CIR-SE-1	1401+94.82	1,898,343.3569	1,171,428.7157
PT	P-CIR-SE-1	1402+63.75	1,898,283.9574	1,171,379.7306
PC	P-CIR-SE-2	1404+09.00	1,898,171.8982	1,171,287.3185
PI	P-CIR-SE-2	1415+83.08	1,897,266.0992	1,170,540.3314
PT	P-CIR-SE-2	1410+44.95	1,897,821.3989	1,171,574.7896
PC	P-CIR-SE-3	1411+90.79	1,897,890.3731	1,171,703.2804
PI	P-CIR-SE-3	1412+44.91	1,897,915.9717	1,171,750.9676
PT	P-CIR-SE-3	1412+97.39	1,897,919.5546	1,171,804.9724
POT		1415+32.20	1,897,935.0987	1,172,039.2687

PR SB ACCESS ROAD				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
PC	P-ACC-SB-1	20+00.00	1,898,159.6257	1,171,474.0589
PI	P-ACC-SB-1	21+27.93	1,898,032.8191	1,171,490.9407
PCC	P-ACC-SB-1/P-ACC-SB-2	22+55.53	1,897,904.8998	1,171,492.1937
PI	P-ACC-SB-2	22+72.94	1,897,887.4898	1,171,492.3642
PRC	P-ACC-SB-2/P-ACC-SB-3	22+89.95	1,897,871.1971	1,171,486.2252
PI	P-ACC-SB-3	23+18.32	1,897,844.6516	1,171,476.2228
PT	P-ACC-SB-3	23+45.46	1,897,816.5979	1,171,480.4300
PC	P-ACC-SB-4	23+85.30	1,897,777.1985	1,171,486.3386
PI	P-ACC-SB-4	23+98.41	1,897,764.2307	1,171,488.2834
PT	P-ACC-SB-4	24+11.50	1,897,751.1342	1,171,488.9385
POT		26+16.53	1,897,546.3571	1,171,499.1807

PR SB TAYLOR EXIT RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		6400+00.00	1,898,804.0600	1,171,388.9319
PI		6401+14.43	1,898,689.7507	1,171,394.2832
PC	P-TAY-SX-1	6403+41.87	1,898,462.3146	1,171,393.0142
PI	P-TAY-SX-1	6404+16.60	1,898,387.5914	1,171,392.5973
PT	P-TAY-SX-1	6404+83.91	1,898,334.1197	1,171,340.4004
PC	P-TAY-SX-2	6405+93.59	1,898,255.6322	1,171,263.7841
PI	P-TAY-SX-2	6408+06.27	1,898,103.4429	1,171,115.2232
PT	P-TAY-SX-2	6409+70.96	1,897,905.7145	1,171,193.5519
PI		6412+46.22	1,897,649.8052	1,171,294.9285
PC	P-TAY-SX-3	6417+74.36	1,897,146.4148	1,171,454.7162
PI	P-TAY-SX-3	6418+57.95	1,897,066.7445	1,171,480.0054
PT	P-TAY-SX-3	6419+41.43	1,896,985.1509	1,171,498.1549
PI		6419+98.77	1,896,929.1697	1,171,510.6073
PC	P-TAY-SX-4	6421+70.61	1,896,759.2385	1,171,536.1268
PI	P-TAY-SX-4	6422+78.44	1,896,652.6012	1,171,552.1412
PT	P-TAY-SX-4	6423+86.23	1,896,545.2793	1,171,562.6280
POT		6424+10.70	1,896,520.9244	1,171,565.0078

EX EB I-290 CONGRESS PARKWAY (P-CON-EB)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		5148+48.69	1,897,914.2658	1,170,751.5739
PC	P-CON-EB-1	5154+09.45	1,897,928.9827	1,171,312.1431
PI	P-CON-EB-1	5155+15.58	1,897,931.7681	1,171,418.2406
PT	P-CON-EB-1	5156+21.70	1,897,937.5059	1,171,524.2195
PC	P-CON-EB-2	5158+24.92	1,897,948.4919	1,171,727.1366
PI	P-CON-EB-2	5159+19.48	1,897,953.6041	1,171,821.5611
PT	P-CON-EB-2	5160+14.03	1,897,955.9933	1,171,916.0936
PI		5169+48.59	1,897,979.6057	1,172,850.3581
POT		5172+87.56	1,897,993.5071	1,173,189.0386

EX WB I-290 CONGRESS PARKWAY (P-CON-WB)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		5201+35.63	1,898,036.6318	1,173,121.1308
PI		5205+28.11	1,898,031.0482	1,172,728.6943
PC	P-CON-WB-1	5211+50.90	1,898,015.3129	1,172,106.1021
PI	P-CON-WB-1	5212+68.02	1,898,012.3536	1,171,989.0129
PT	P-CON-WB-1	5213+84.75	1,898,026.1351	1,171,872.6999
PC	P-CON-WB-2	5216+99.19	1,898,063.1327	1,171,560.4465
PI	P-CON-WB-2	5218+24.26	1,898,077.8485	1,171,436.2477
PT	P-CON-WB-2	5219+48.84	1,898,073.5952	1,171,311.2524
POT		5225+43.83	1,898,053.3612	1,170,716.6100

PR ADAMS STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		8300+00.00	1,899,295.5376	1,170,159.8855
PI		8309+20.47	1,899,323.7706	1,171,079.9215
POT		8325+09.26	1,899,362.9921	1,172,668.2273



D162J31-SHT-ATB-04.dgn  
 USER NAME = dshevoZ  
 PLOT SCALE = 200.0000' / 1"

DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJJ  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1" = 100' SHEET 8 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	36
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

FILE PATH = p:\aecom\m-nw\sl\ecomm\local\AECOM\_0502\_NA\_Documents\01\_Americas\Transportation\60269938\_Circle Phase I\000\_CAD\006\_Roadway\Sheets\62J31\_Contract\0162J31-SHT-ATB-04A.dgn

EX SB I-90/94 (KDR-ESB)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		124+01.69	1,888,401.4348	1,172,010.6587
PC	KDR-ESBE-00-1	127+78.87	1,888,744.6255	1,171,854.1657
PI	KDR-ESBE-00-1	134+34.39	1,889,341.0586	1,171,582.1956
PT	KDR-ESBE-00-1	140+45.77	1,889,982.7651	1,171,716.0351
PC	KDR-ESBE-00-2	149+64.73	1,890,882.3653	1,171,903.6631
PI	KDR-ESBE-00-2	151+80.68	1,891,093.7643	1,171,947.7542
PT	KDR-ESBE-00-2	153+94.57	1,891,309.5934	1,171,940.5902
POT		183+13.36	1,894,226.7720	1,171,843.7613
PC	KDR-ESB-00-1	185+50.30	1,894,463.5855	1,171,835.9008
PI	KDR-ESB-00-1	186+12.11	1,894,525.3637	1,171,833.8503
PT	KDR-ESB-00-1	186+73.92	1,894,587.1007	1,171,830.8004
PC	KDR-ESB-00-2	186+73.95	1,894,587.1274	1,171,830.7991
PI	KDR-ESB-00-2	194+46.34	1,895,358.5739	1,171,792.6885
PT	KDR-ESB-00-2	202+17.88	1,896,124.4241	1,171,692.4111
POT1		3+11.60	1,896,897.1317	1,171,591.2358
TS	KDR-ESB-00-3B	5+79.29	1,897,162.5525	1,171,556.4826
PI	KDR-ESB-00-3B	6+79.30	1,897,261.7152	1,171,543.4986
SC	KDR-ESB-00-3B	7+29.29	1,897,311.5276	1,171,539.0772
PC	KDR-ESB-00-3	7+29.29	1,897,311.5276	1,171,539.0772
PI	KDR-ESB-00-3	8+34.32	1,897,416.1453	1,171,529.7912
CS	KDR-ESB-00-3A	9+39.11	1,897,521.1330	1,171,532.7356
PI	KDR-ESB-00-3A	9+89.11	1,897,571.1183	1,171,534.1374
ST	KDR-ESB-00-3A	10+89.10	1,897,670.8779	1,171,541.1020
TS	KDR-ESB-00-4B	10+95.19	1,897,676.9607	1,171,541.5267
PI	KDR-ESB-00-4B	12+28.55	1,897,809.9894	1,171,550.8139
SC	KDR-ESB-00-4B	12+95.19	1,897,876.6634	1,171,551.9707
PC	KDR-ESB-00-4	12+95.19	1,897,876.6634	1,171,551.9707
PI	KDR-ESB-00-4	13+66.51	1,897,947.9712	1,171,553.2078
PT	KDR-ESB-00-4	14+37.76	1,898,019.1728	1,171,549.1239
CS	KDR-ESB-00-4A	14+37.76	1,898,019.1728	1,171,549.1239
PI	KDR-ESB-00-4A	15+04.45	1,898,085.7441	1,171,545.3056
ST	KDR-ESB-00-4A	16+37.75	1,898,218.2892	1,171,530.7143
PC	KDR-ESB-00-5	16+76.77	1,898,257.0735	1,171,526.4446
PI	KDR-ESB-00-5	19+84.85	1,898,563.3008	1,171,492.7332
PT	KDR-ESB-00-5	22+92.78	1,898,870.8971	1,171,475.5242
PC	KDR-ESB-00-6	25+30.01	1,899,107.7534	1,171,462.2729
PI	KDR-ESB-00-6	28+07.67	1,899,384.9870	1,171,446.7626
PT	KDR-ESB-00-6	30+85.31	1,899,662.5150	1,171,437.9753
POT		45+34.00	1,901,110.4797	1,171,392.1175
PC	KDR-ESBE-00-3	48+40.06	1,901,416.3840	1,171,382.4298
PI	KDR-ESBE-00-3	52+69.37	1,901,845.4807	1,171,368.8406
PCC	KDR-ESBE-00-3/KDR-ESBE-004	56+88.03	1,902,237.9869	1,171,194.9225
PI	KDR-ESBE-00-4	63+45.97	1,902,839.5235	1,170,928.3836
PT	KDR-ESBE-00-4	69+81.26	1,903,263.3486	1,170,425.1325
POT		71+78.59	1,903,390.4655	1,170,274.1935

EX NB I-90/94 (KDR-ENB)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		102+59.88	1,887,308.8238	1,173,788.3601
PC	KDR-ENB-00-1	111+81.09	1,887,581.9126	1,172,908.5568
PI	KDR-ENB-00-1	117+55.38	1,887,752.1587	1,172,360.0790
PT	KDR-ENB-00-1	122+60.37	1,888,275.5977	1,172,123.8097
PC	KDR-ENB-00-2	128+11.99	1,888,778.3777	1,171,896.8654
PI	KDR-ENB-00-2	134+42.28	1,889,352.8541	1,171,637.5590
PT	KDR-ENB-00-2	140+30.67	1,889,969.8828	1,171,766.1612
PC	KDR-ENB-00-3	149+57.03	1,890,876.7537	1,171,955.1729
PI	KDR-ENB-00-3	151+93.71	1,891,108.4576	1,172,003.4651
PT	KDR-ENB-00-3	154+28.14	1,891,345.0103	1,171,995.6132
POT		183+13.36	1,894,228.6347	1,171,899.8969
PC	KDR-ENB-00-4	190+77.41	1,894,992.2698	1,171,874.5496
PI	KDR-ENB-00-4	192+81.92	1,895,196.6626	1,171,867.7651
PT	KDR-ENB-00-4	194+86.31	1,895,400.3262	1,171,849.2283
PC	KDR-ENB-00-5	196+49.74	1,895,563.0812	1,171,834.4148
PI	KDR-ENB-00-5	199+68.51	1,895,880.5418	1,171,805.5204
PT	KDR-ENB-00-5	202+87.12	1,896,199.1177	1,171,794.3177
POT1		3+06.94	1,896,901.5590	1,171,770.9004
PC	KDR-ENB-00-6	5+24.89	1,897,119.3934	1,171,763.6385
PI	KDR-ENB-00-6	7+04.34	1,897,298.7419	1,171,757.6595
PT	KDR-ENB-00-6	8+83.32	1,897,475.9425	1,171,729.3467
PC	KDR-ENB-007	16+62.09	1,898,244.9598	1,171,606.4743
PI	KDR-ENB-007	20+43.63	1,898,621.7192	1,171,546.2763
PT	KDR-ENB-007	24+22.65	1,899,002.9421	1,171,561.7870
PC	KDR-ENB-008	24+61.57	1,899,041.8342	1,171,563.3694
PI	KDR-ENB-008	27+72.52	1,899,352.5300	1,171,576.0107
PT	KDR-ENB-008	30+83.20	1,899,663.3270	1,171,566.1658
POT		45+35.14	1,901,114.5368	1,171,520.1970



D162J31-SHT-ATB-04A.dgn  
 USER NAME = dshevoZ  
 PLOT SCALE = 200.0000' / in.  
 PLOT DATE = 8/14/2019

DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1" = 100' SHEET 9 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	37
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**PROPOSED CURVE DATA**

**NOTE:**

FOR PROPOSED SUPERELEVATION RATES AND TRANSITION LENGTHS, SEE PAVEMENT ELEVATION AND SUPERELEVATION DETAILS ON SHEET 47 .

**PROPOSED NB I-90/94 (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-KDR-NB-1  
PI STA. = 6110+00.27  
Δ = 3° 20' 54" (LT)  
D = 0° 28' 35"  
R = 12,024.00'  
T = 351.43'  
L = 702.66'  
E = 5.13'  
P.C. STA. = 6106+48.84  
P.T. STA. = 6113+51.50

PROP. CURVE P-KDR-NB-2  
PI STA. = 6119+73.66  
Δ = 5° 37' 23" (RT)  
D = 1° 53' 41"  
R = 3,024.00'  
T = 148.51'  
L = 296.77'  
E = 3.64'  
P.C. STA. = 6118+25.16  
P.T. STA. = 6121+21.93

PROP. CURVE P-KDR-NB-3  
PI STA. = 6129+99.47  
Δ = 11° 38' 44" (LT)  
D = 1° 33' 16"  
R = 3,686.00'  
T = 375.89'  
L = 749.19'  
E = 19.12'  
P.C. STA. = 6126+23.58  
P.T. STA. = 6133+72.77

PROP. CURVE P-KDR-NB-4  
PI STA. = 6143+87.92  
Δ = 12° 26' 15" (RT)  
D = 2° 22' 10"  
R = 2,418.00'  
T = 263.48'  
L = 524.89'  
E = 14.31'  
P.C. STA. = 6141+24.44  
P.T. STA. = 6146+49.33

PROP. CURVE P-KDR-NB-5  
PI STA. = 6159+31.09  
Δ = 2° 57' 23" (LT)  
D = 0° 30' 55"  
R = 11,122.00'  
T = 286.99'  
L = 573.86'  
E = 3.70'  
P.C. STA. = 6156+44.09  
P.T. STA. = 6162+17.96

PROP. CURVE P-KDR-NB-6  
PI STA. = 6171+80.29  
Δ = 10° 56' 32" (LT)  
D = 2° 50' 01"  
R = 2,022.00'  
T = 193.67'  
L = 386.15'  
E = 9.25'  
P.C. STA. = 6169+86.63  
P.C.C. STA. = 6173+72.78

PROP. CURVE P-KDR-NB-7  
PI STA. = 6174+40.21  
Δ = 1° 55' 16" (LT)  
D = 1° 25' 28"  
R = 4,022.00'  
T = 67.43'  
L = 134.85'  
E = 0.57'  
P.C.C. STA. = 6173+72.78  
P.T. STA. = 6175+07.63

**PROPOSED SB I-90/94 (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-KDR-SB-1  
PI STA. = 6207+41.31  
Δ = 2° 57' 20" (RT)  
D = 1° 37' 36"  
R = 3,522.00'  
T = 90.86'  
L = 181.68'  
E = 1.17'  
P.C. STA. = 6206+50.45  
P.T. STA. = 6208+32.13

PROP. CURVE P-KDR-SB-2  
PI STA. = 6217+28.62  
Δ = 10° 41' 00" (LT)  
D = 2° 17' 50"  
R = 2,494.00'  
T = 233.19'  
L = 465.03'  
E = 10.88'  
P.C. STA. = 6214+95.43  
P.T. STA. = 6219+60.46

PROP. CURVE P-KDR-SB-3  
PI STA. = 6224+30.06  
Δ = 11° 28' 39" (RT)  
D = 2° 41' 06"  
R = 2,134.00'  
T = 214.46'  
L = 427.48'  
E = 10.75'  
P.C. STA. = 6222+15.60  
P.T. STA. = 6226+43.08

PROP. CURVE P-KDR-SB-4  
PI STA. = 6231+84.46  
Δ = 13° 18' 21" (LT)  
D = 2° 44' 34"  
R = 2,089.00'  
T = 243.66'  
L = 485.13'  
E = 14.16'  
P.C. STA. = 6229+40.80  
P.T. STA. = 6234+25.93

PROP. CURVE P-KDR-SB-5  
PI STA. = 6239+11.41  
Δ = 6° 01' 56" (RT)  
D = 1° 20' 53"  
R = 4,250.00'  
T = 223.93'  
L = 447.45'  
E = 5.90'  
P.C. STA. = 6236+87.48  
P.T. STA. = 6241+34.92

PROP. CURVE P-KDR-SB-6  
PI STA. = 6255+97.19  
Δ = 3° 24' 46" (RT)  
D = 0° 30' 48"  
R = 11,162.00'  
T = 332.52'  
L = 664.85'  
E = 4.95'  
P.C. STA. = 6252+64.67  
P.T. STA. = 6259+29.52

PROP. CURVE P-ADM-SX-1  
PI STA. = 8386+35.08  
Δ = 6° 41' 46.11" (LT)  
D = 1° 49' 39.42"  
R = 3,135.00'  
T = 183.40'  
L = 366.38'  
E = 5.36'  
P.C. STA. = 8384+51.68  
P.T. STA. = 8388+18.07

**PROPOSED ADAMS STREET EXIT RAMP (SHOWN FOR INFORMATION ONLY)**

**PROPOSED RAMP SW**

PROP. CURVE P-CIR-SW-1  
PI STA. = 1304+86.54  
Δ = 3° 00' 34" (LT)  
D = 2° 00' 35"  
R = 2,851.00'  
T = 74.89'  
L = 149.75'  
E = 0.98'  
P.C. STA. = 1304+11.65  
P.T. STA. = 1305+61.40

PROP. CURVE P-CIR-SW-2  
PI STA. = 1307+47.72  
Δ = 1° 10' 04" (RT)  
D = 0° 52' 58"  
R = 6,491.00'  
T = 66.14'  
L = 132.28'  
E = 0.34'  
P.C. STA. = 1306+81.58  
P.T. STA. = 1308+13.86

PROP. CURVE P-CIR-SW-3  
PI STA. = 1322+16.98  
Δ = 83° 35' 08" (RT)  
D = 0° 03' 07"  
R = 570.00'  
T = 509.51'  
L = 831.54'  
E = 194.53'  
P.C. STA. = 1317+07.47  
P.T. STA. = 1325+39.01

PROP. CURVE P-CIR-SW-4  
PI STA. = 1332+28.52  
Δ = 0° 58' 03" (RT)  
D = 10° 03' 07"  
R = 6,189.00'  
T = 52.25'  
L = 104.51'  
E = 0.22'  
P.C. STA. = 1331+76.26  
P.T. STA. = 1332+80.77

**PROPOSED RAMP SE**

PROP. CURVE P-CIR-SE-1  
PI STA. = 1401+94.82  
Δ = 45° 11' 30" (RT)  
D = 0° 55' 33"  
R = 185.00'  
T = 76.99'  
L = 145.92'  
E = 15.38'  
P.C. STA. = 1401+17.83  
P.T. STA. = 1402+63.75

PROP. CURVE P-CIR-SE-2  
PI STA. = 1415+83.08  
Δ = 157° 44' 18" (LT)  
D = 24° 48' 12"  
R = 231.00'  
T = 1,174.08'  
L = 635.96'  
E = 965.59'  
P.C. STA. = 1404+09.00  
P.T. STA. = 1410+44.95

PROP. CURVE P-CIR-SE-3  
PI STA. = 1412+44.91  
Δ = 24° 25' 53" (RT)  
D = 22° 55' 06"  
R = 250.00'  
T = 54.12'  
L = 106.60'  
E = 5.79'  
P.C. STA. = 1411+90.79  
P.T. STA. = 1412+97.39

**PROPOSED RAMP WS (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-CIR-WS-1  
PI STA. = 1210+36.88  
Δ = 26° 00' 07" (RT)  
D = 8° 48' 53"  
R = 650.00'  
T = 150.08'  
L = 294.98'  
E = 17.10'  
P.C. STA. = 1208+86.80  
P.T. STA. = 1211+81.78

PROP. CURVE P-CIR-WS-2  
PI STA. = 1222+12.72  
Δ = 135° 15' 55" (LT)  
D = 19° 21' 24"  
R = 296.00'  
T = 719.31'  
L = 698.80'  
E = 481.83'  
P.C. STA. = 1214+93.41  
P.T. STA. = 1221+92.21

PROP. CURVE P-CIR-WS-3  
PI STA. = 1231+01.94  
Δ = 5° 04' 12" (RT)  
D = 3° 00' 56"  
R = 1,900.00'  
T = 84.12'  
L = 168.13'  
E = 1.86'  
P.C. STA. = 1230+17.82  
P.T. STA. = 1231+85.95

PROP. CURVE P-CIR-WS-4  
PI STA. = 1236+35.76  
Δ = 6° 01' 35" (RT)  
D = 1° 22' 03"  
R = 4,190.00'  
T = 220.56'  
L = 440.71'  
E = 5.80'  
P.C. STA. = 1234+15.20  
P.T. STA. = 1238+55.92

**PROPOSED RAMP ES (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-CIR-ES-1  
PI STA. = 1504+68.66  
Δ = 16° 40' 30" (RT)  
D = 4° 52' 05"  
R = 1,177.00'  
T = 172.49'  
L = 342.55'  
E = 12.57'  
P.C. STA. = 1502+96.17  
P.C.C. STA. = 1506+38.71

PROP. CURVE P-CIR-ES-2  
PI STA. = 1510+49.08  
Δ = 63° 26' 03" (RT)  
D = 8° 37' 44"  
R = 664.00'  
T = 410.37'  
L = 735.14'  
E = 116.58'  
P.C.C. STA. = 1506+38.71  
P.T. STA. = 1513+73.85

**PROPOSED JACKSON BLVD EXIT RAMP (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-JAC-SX-1  
PI STA. = 8283+78.27  
Δ = 5° 01' 56" (LT)  
D = 1° 41' 07"  
R = 3,400.00'  
T = 149.40'  
L = 298.61'  
E = 3.28'  
P.C. STA. = 8282+28.87  
P.T. STA. = 8285+27.48

**PROPOSED MADISON STREET ENTRANCE RAMP (SHOWN FOR INFORMATION ONLY)**

PROP. CURVE P-MAD-ST-1  
PI STA. = 8562+69.46  
Δ = 3° 07' 15" (RT)  
D = 5° 24' 19"  
R = 1,060.00'  
T = 28.88'  
L = 57.74'  
E = 0.39'  
P.C. STA. = 8562+40.59  
P.T. STA. = 8562+98.32

**PROPOSED SB TAYLOR EXIT RAMP**

PROP. CURVE P-TAY-SX-1  
PI STA. = 6404+16.60  
Δ = 43° 59' 21" (RT)  
D = 30° 58' 14"  
R = 185.00'  
T = 74.72'  
L = 142.03'  
E = 14.52'  
P.C. STA. = 6403+41.87  
P.T. STA. = 6404+83.91

PROP. CURVE P-TAY-SX-2  
PI STA. = 6408+06.27  
Δ = 65° 55' 10" (LT)  
D = 17° 28' 06"  
R = 328.00'  
T = 212.68'  
L = 377.37'  
E = 62.92'  
P.C. STA. = 6405+93.59  
P.T. STA. = 6409+70.96

PROP. CURVE P-TAY-SX-3  
PI STA. = 6418+57.95  
Δ = 5° 04' 12" (RT)  
D = 3° 02' 05"  
R = 1,888.00'  
T = 83.59'  
L = 167.07'  
E = 1.85'  
P.C. STA. = 6417+74.36  
P.T. STA. = 6419+41.43

PROP. CURVE P-TAY-SX-4  
PI STA. = 6422+78.44  
Δ = 2° 57' 35" (RT)  
D = 1° 22' 22"  
R = 4,174.00'  
T = 107.83'  
L = 215.62'  
E = 1.39'  
P.C. STA. = 6421+70.61  
P.T. STA. = 6423+86.23

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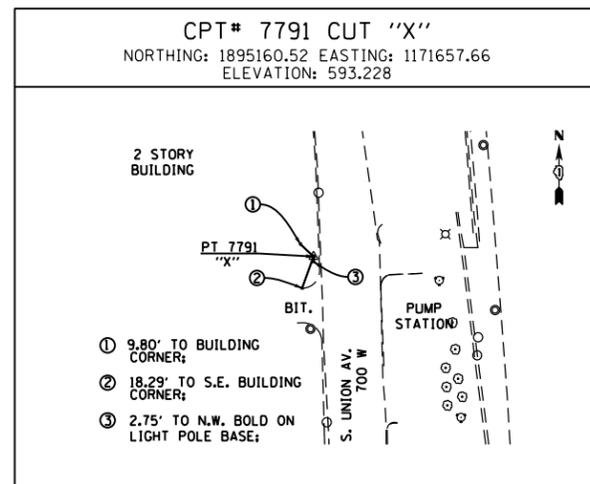
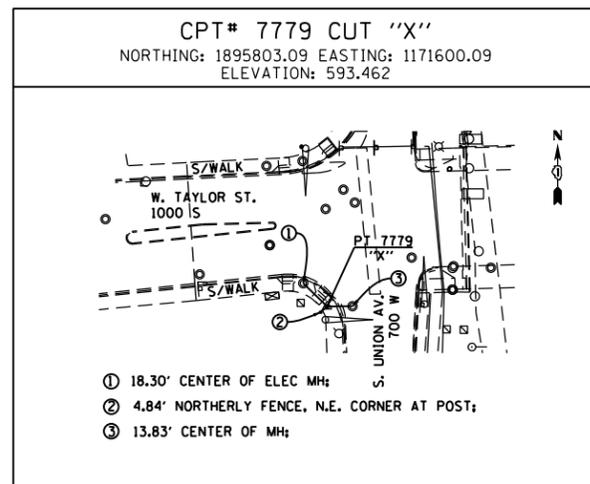
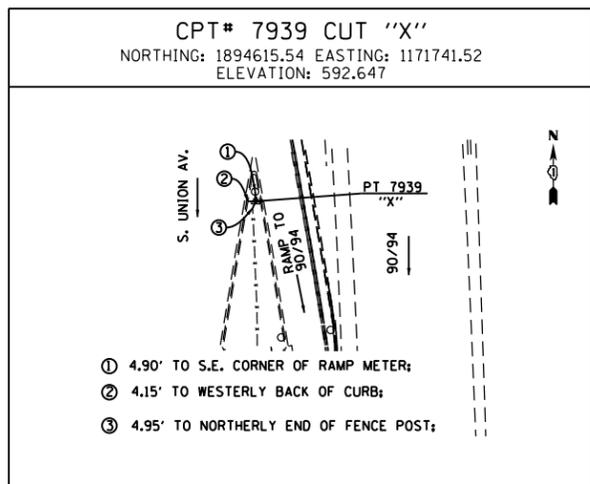
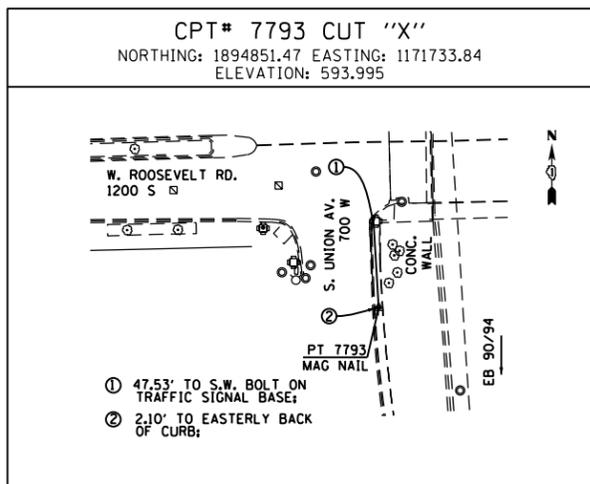
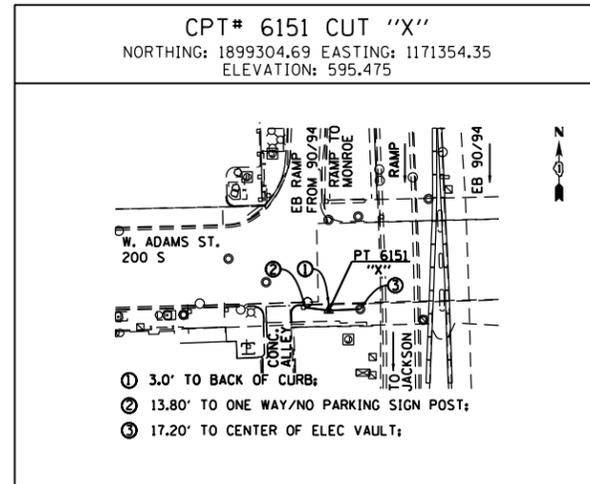
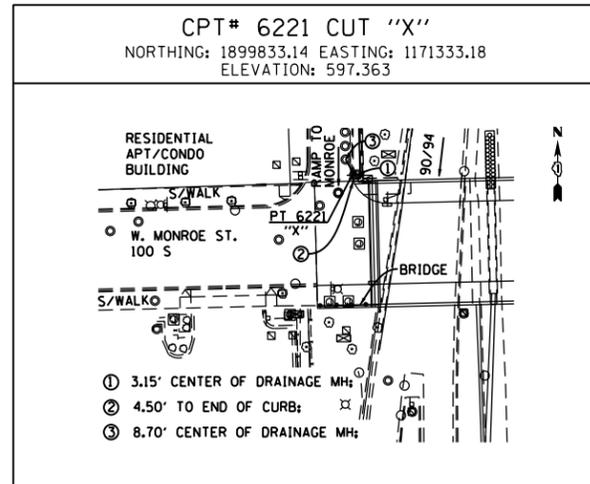
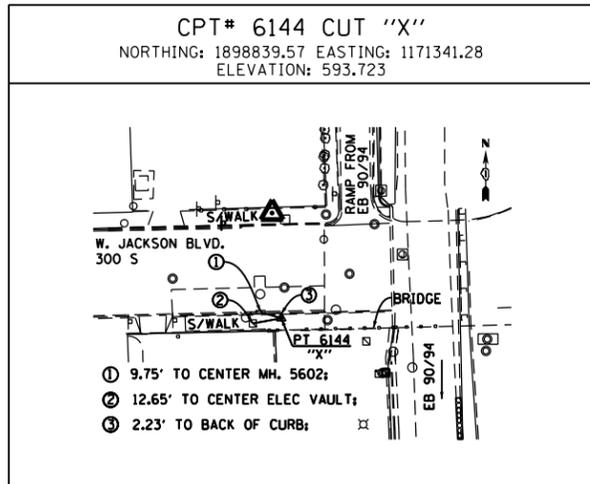
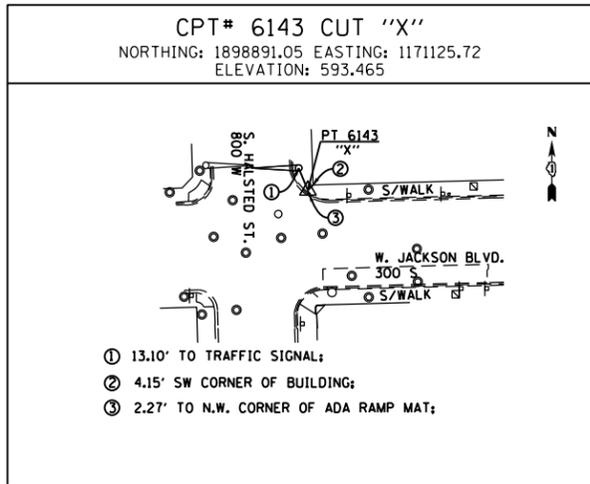
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1" = 100' SHEET 10 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	38
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

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BENCHMARKS

BENCHMARKS		
MONUMENT	ELEVATION	DESCRIPTION
BM 1344	589.0884	PUNCH MARK TOP OF STEEL GUARD RING AROUND DRAWN PIPE NW SIDE NORTHERLY CONCRETE PIER EB I-290, W. SIDE DES PLAINES, TAPE TO BM ON PARAPET WALL ABOVE
BM 1345	594.1988	"+" CUT WSW FLANGE BOLT FH W. SIDE DES PLAINES ST. +/- 200' S. OF C HARRISON ST.
BM 1346	594.6435	MK CUT TOP OF CONCRETE RETAINING WALL WITH C/L FNC +/- C VERNON @ W 90/94 (W. OF DES PLAINES)
BM 1365	598.6500	CUT SQUARE ON SOUTHERLY PARAPET WALL OVER I-90 ON WEST END. ±2.5' A/G.
BM 1142	575.9003	SET "X" ON WESTERLY JAYWALL OF I-90 @ C OF INBOUND I-290 OVERPASS
BM 1145	586.1391	CHISEL "X" ON WESTERLY BOLT OF H.M.L.P. C OF I-90 APP 200' SOUTH OF PUMP/LIFT STATION
BM 1161	576.1662	SET MAG EASTBOUND SHOULDER OF I-290 APPX 260' EAST OF RACINE
BM 1291	578.9833	SET PK @ JAYWALL END FOR RAMP TO RACINE FROM I-290 EASTBOUND

BENCHMARKS

BM 1292	579.5813	SET PK 1' SOUTH OF CURB FLAG @ CONTROL BOX I-290 EASTBOUND
BM 1384	594.1735	CHISEL "X" ON CHAIN BOLT OF FILL @ NW CORNER OF JEFFERSON & TILDEN ST.
BM 1395	593.3599	CHISEL "X" ON NE BOLT OF TRAFFIC SIGNAL ON SOUTHWEST CORNER OF ROOSEVELT AND UNION ST.
BM 1398	594.3625	CHISEL "X" ON CHAINBOLT OF F.H. S. SIDE OF JACKSON, FIRST HYDRANT E. OF HALSTED.



D162J31-SHT-ATB-06.dgn  
 USER NAME = ashok.kc  
 PLOT SCALE = 100.0000' / 1" =  
 PLOT DATE = 8/14/2019

DESIGNED - OPS  
 DRAWN - ZND  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
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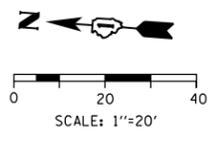
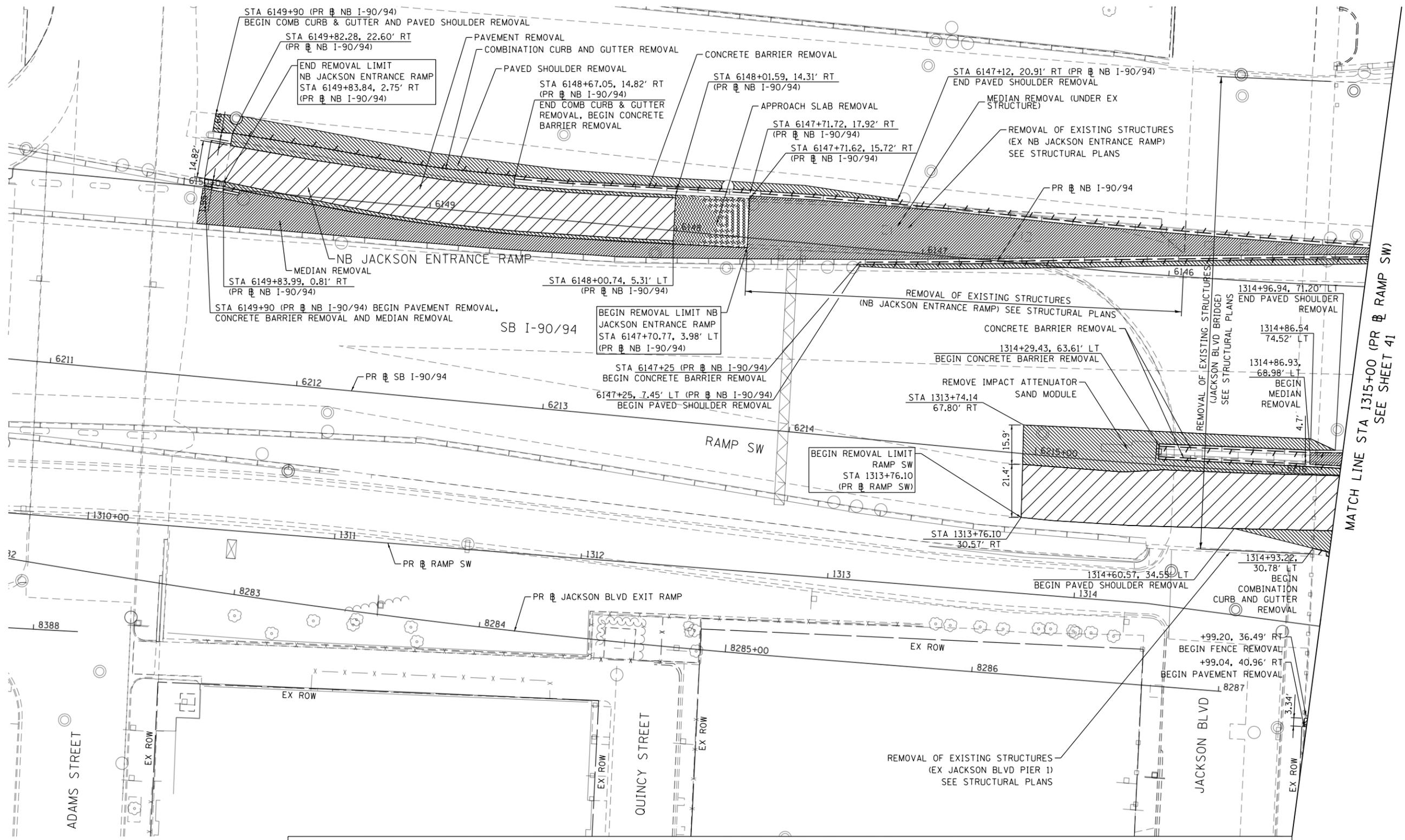
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: NONE SHEET 11 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	39
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**REMOVAL LEGEND**

- PAVEMENT REMOVAL
- APPROACH SLAB REMOVAL
- MEDIAN REMOVAL
- PAVED SHOULDER REMOVAL
- LINEAR REMOVAL
- TREE REMOVAL
- TREE TRUNK PROTECTION, TEMPORARY FENCE, TREE PRUNING
- SAPLING REMOVAL (NOT MEASURED FOR PAYMENT)

**NOTES**

1. SEE ALIGNMENT AND TIES SHEETS NO. 29 TO 39 FOR CURVE DATA.
2. ALL STATIONS AND OFFSETS ARE MEASURED FROM PR @ RAMP SW UNLESS OTHERWISE NOTED.



D162J31-SHT-Removal-01.dgn
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PLOT SCALE = 40.0000' / in.
PLOT DATE = 8/15/2019

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DRAWN - ZND	REVISED -
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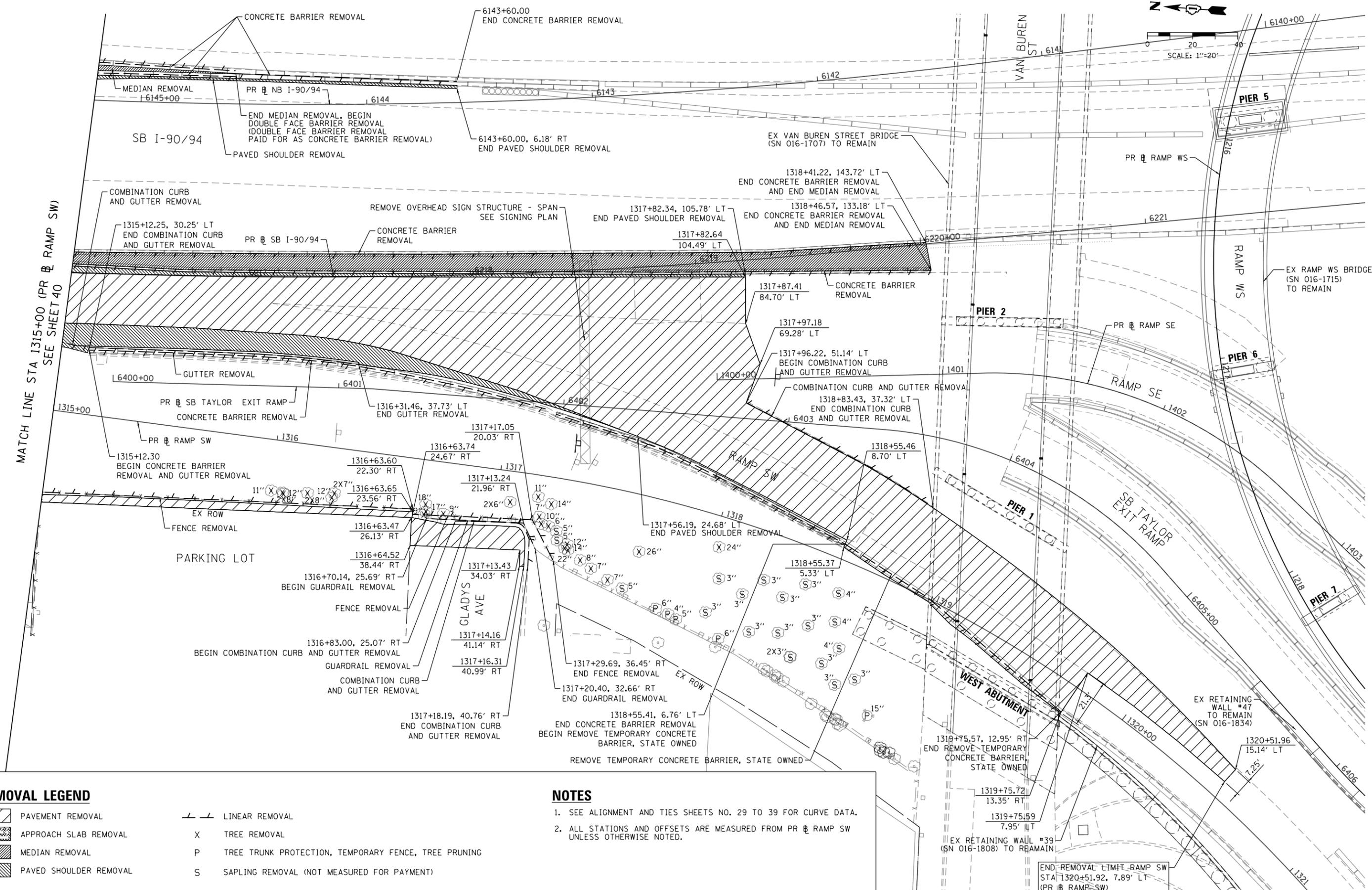
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
NB JACKSON ENTRANCE RAMP**

SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. 1310+00.00 TO STA. 1315+00.00

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 40
CONTRACT NO. 62J31				ILLINOIS FED. AID PROJECT

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**REMOVAL LEGEND**

- |  |                        |   |  |
|--|------------------------|---|--|
|  | PAVEMENT REMOVAL       |   | LINEAR REMOVAL                                       |
|  | APPROACH SLAB REMOVAL  | X | TREE REMOVAL   |
|  | MEDIAN REMOVAL         | P | TREE TRUNK PROTECTION, TEMPORARY FENCE, TREE PRUNING |
|  | PAVED SHOULDER REMOVAL | S | SAPLING REMOVAL (NOT MEASURED FOR PAYMENT)           |

**NOTES**

- SEE ALIGNMENT AND TIES SHEETS NO. 29 TO 39 FOR CURVE DATA.
- ALL STATIONS AND OFFSETS ARE MEASURED FROM PR RAMP SW UNLESS OTHERWISE NOTED.

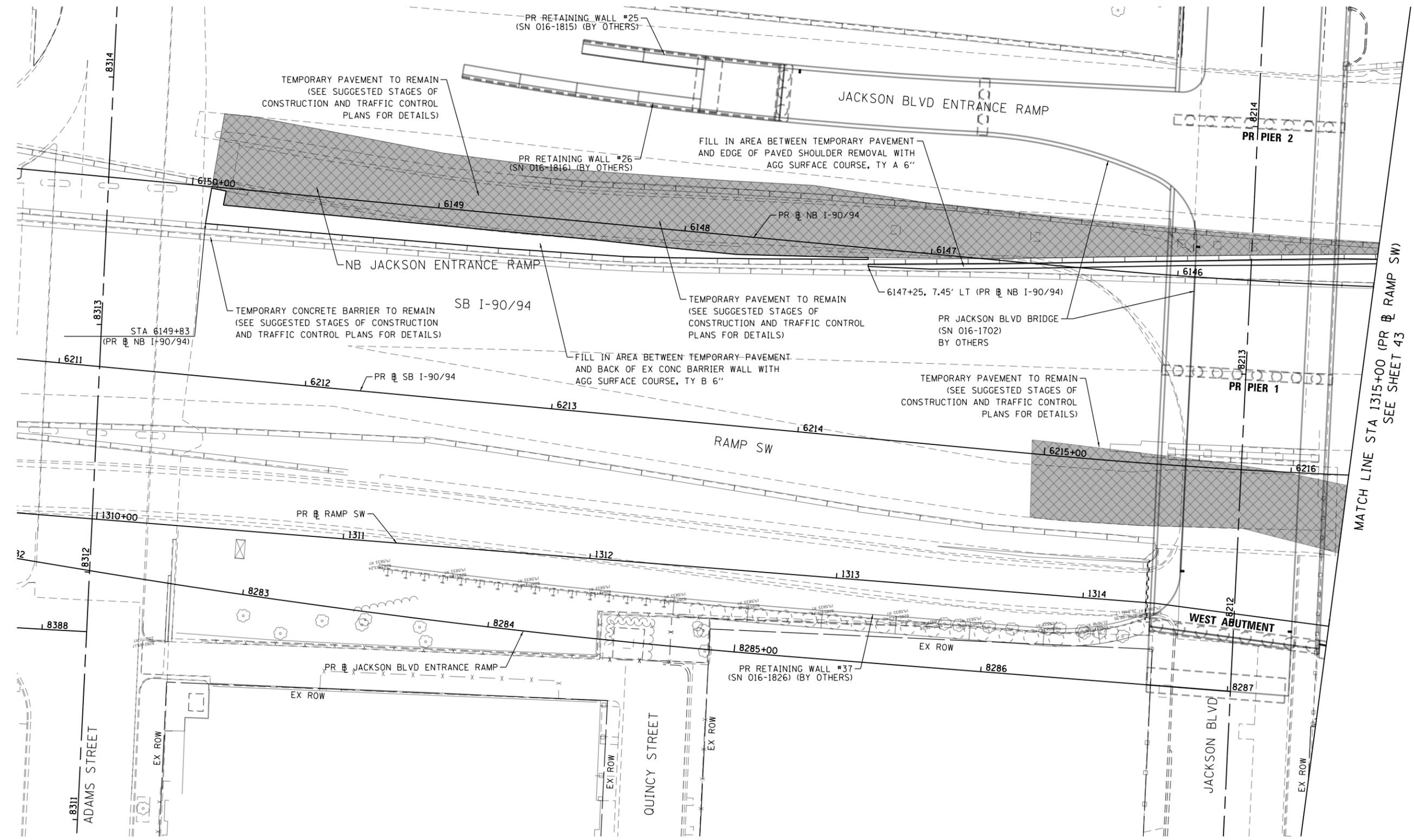
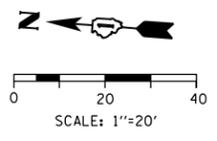


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PLOT DATE = 8/15/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**  
**RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**  
SCALE: 1"=20' SHEET 2 OF 2 SHEETS STA. 1315+00.00 TO STA. 1320+51.92

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	41
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



**ROADWAY LEGEND**  
 TEMPORARY PAVEMENT TO REMAIN

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 PLOT SCALE = 40.0000' / in.  
 PLOT DATE = 8/15/2019

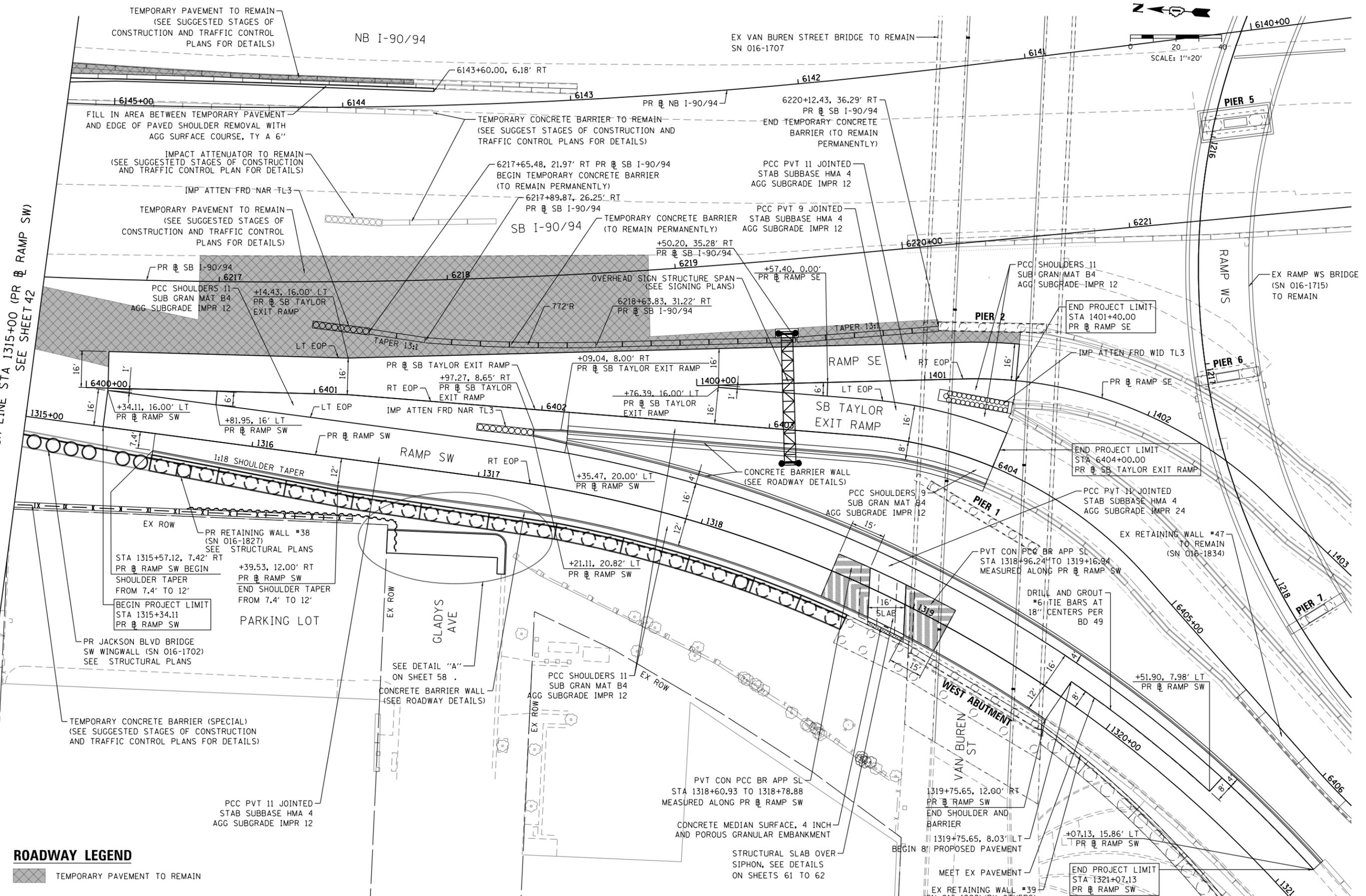
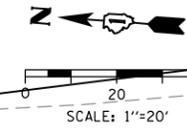
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DRAWN - ZND	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN**  
**RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

SCALE: 1"=20'    SHEET 1 OF 2 SHEETS    STA. 1310+00.00 TO STA. 1315+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	42
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



**ROADWAY LEGEND**  
 TEMPORARY PAVEMENT TO REMAIN

MATCH LINE STA 1315+00 (PR SB RAMP SW) SEE SHEET 42

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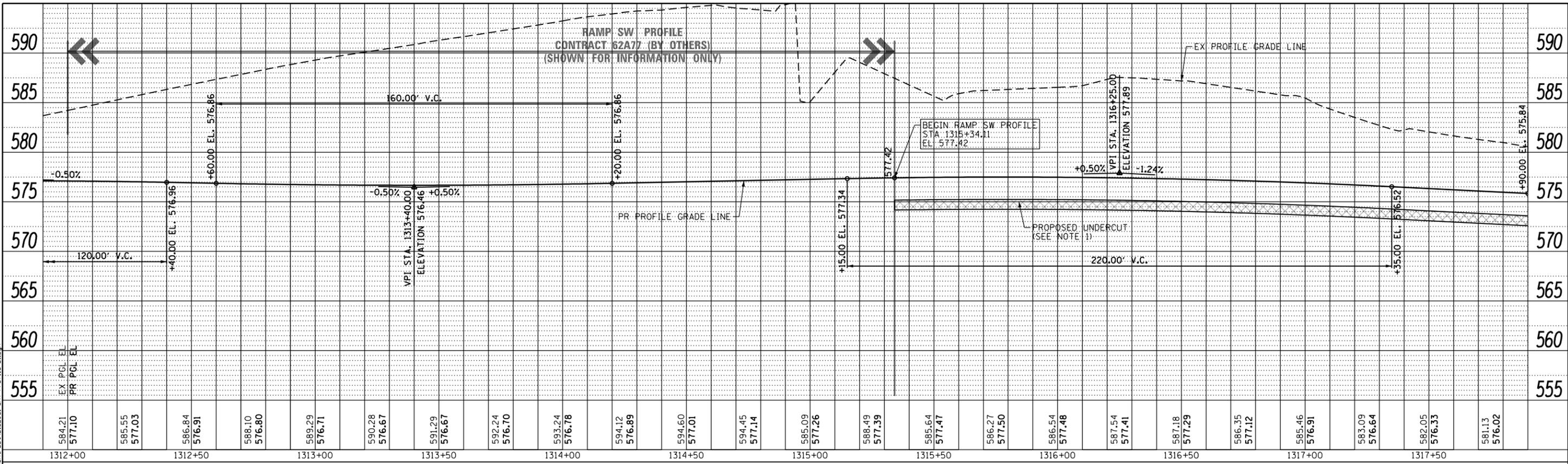
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

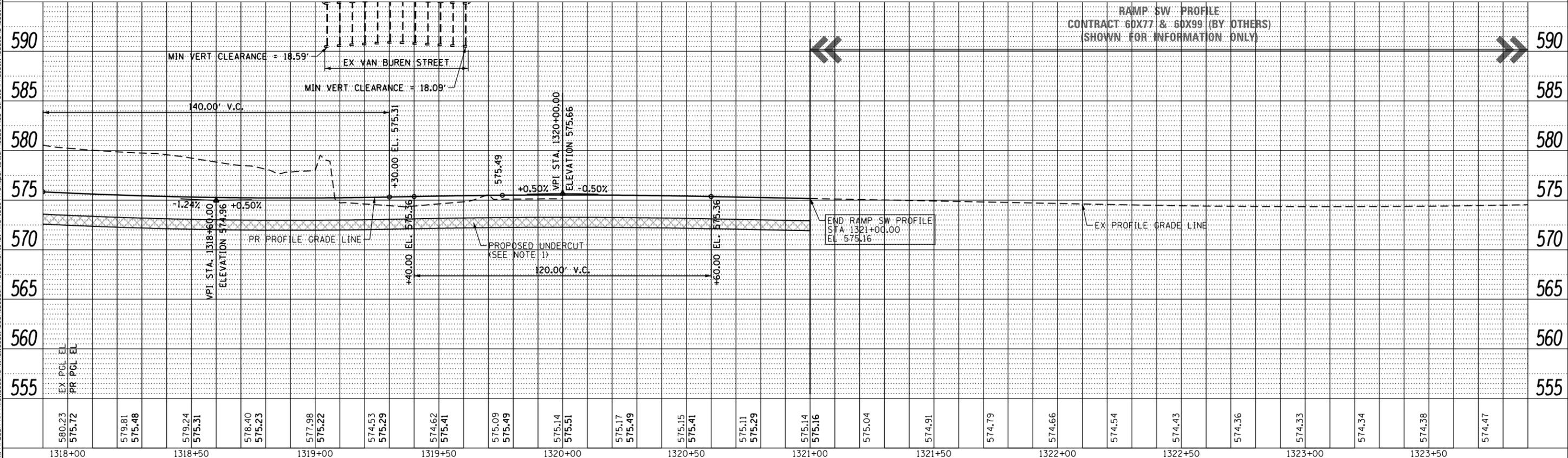
<b>ROADWAY PLAN</b>	
<b>RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP</b>	
SCALE: 1"=20'	SHEET 2 OF 2 SHEETS
STA. 1315+00.00 TO STA. 1321+00.00	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	43
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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NOTE:  
 1. AGGREGATE SUBGRADE IMPROVEMENT SHOWN ON THESE SHEETS IS SCHEMATIC AND THE DEPTHS SHOWN MAY REQUIRE REDUCTION DUE TO PROXIMITY TO RETAINING WALLS 38 (SN 016-1827) AND 39 (SN 016-1808) AND VAN BUREN STREET NW WINGWALL (SN 016-1707). SEE SHEET NO. 26 FOR MORE INFORMATION.



D162J31-SHT-Profile-01.dgn  
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 PLOT SCALE = 40.000000' / in.  
 PLOT DATE = 8/14/2019

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DRAWN - OPS	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

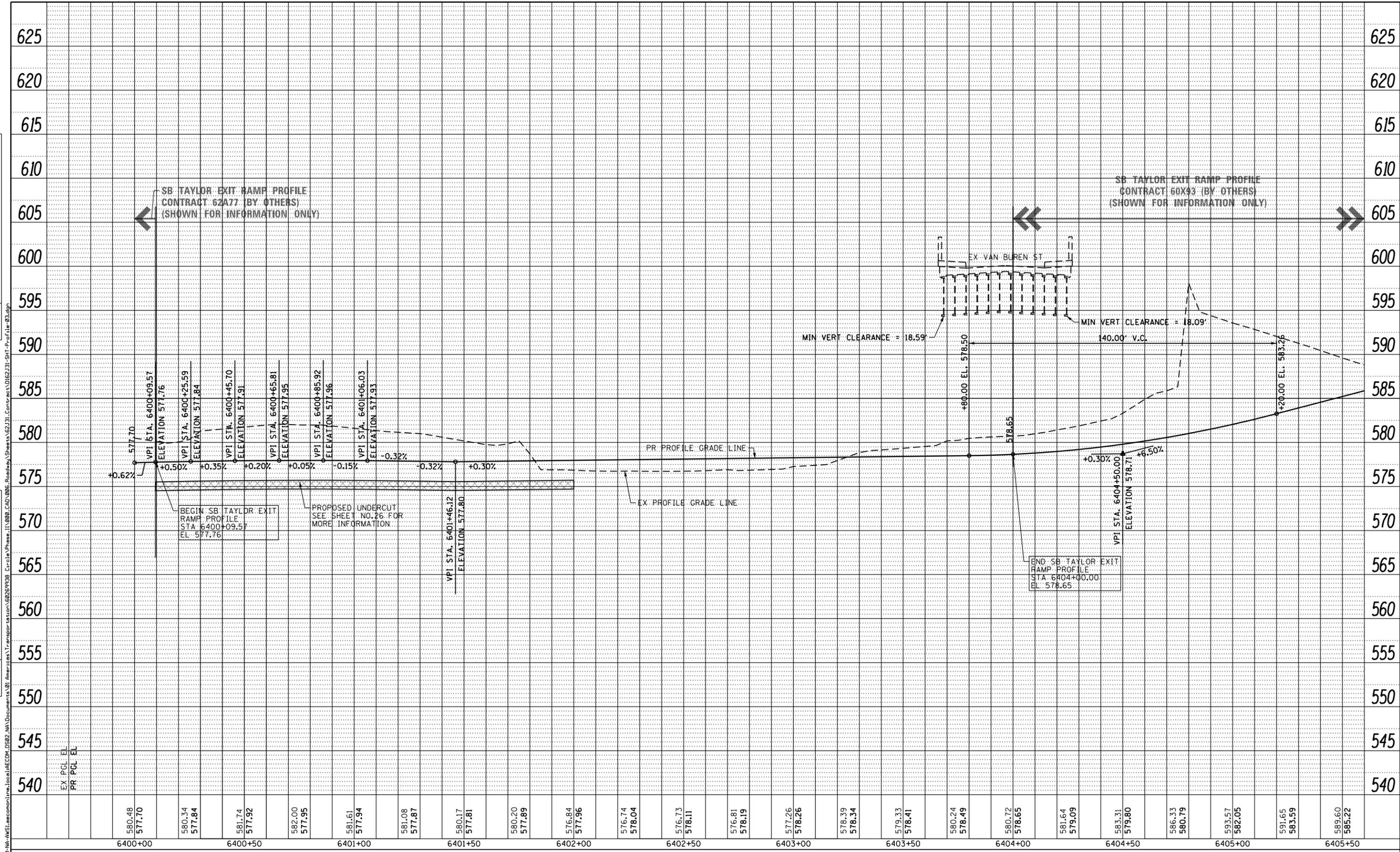
ROADWAY PROFILE  
 RAMP SW  
 SCALE: 1"=20'  
 SHEET 1 OF 3 SHEETS  
 STA. 1311+90.00 TO STA. 1323+90.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	44
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	



PLAN	SUBMITTED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NO. _____		
	CARD FILE NAME		

PROFILE	SUBMITTED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIS CHKO		
	NO. _____		



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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

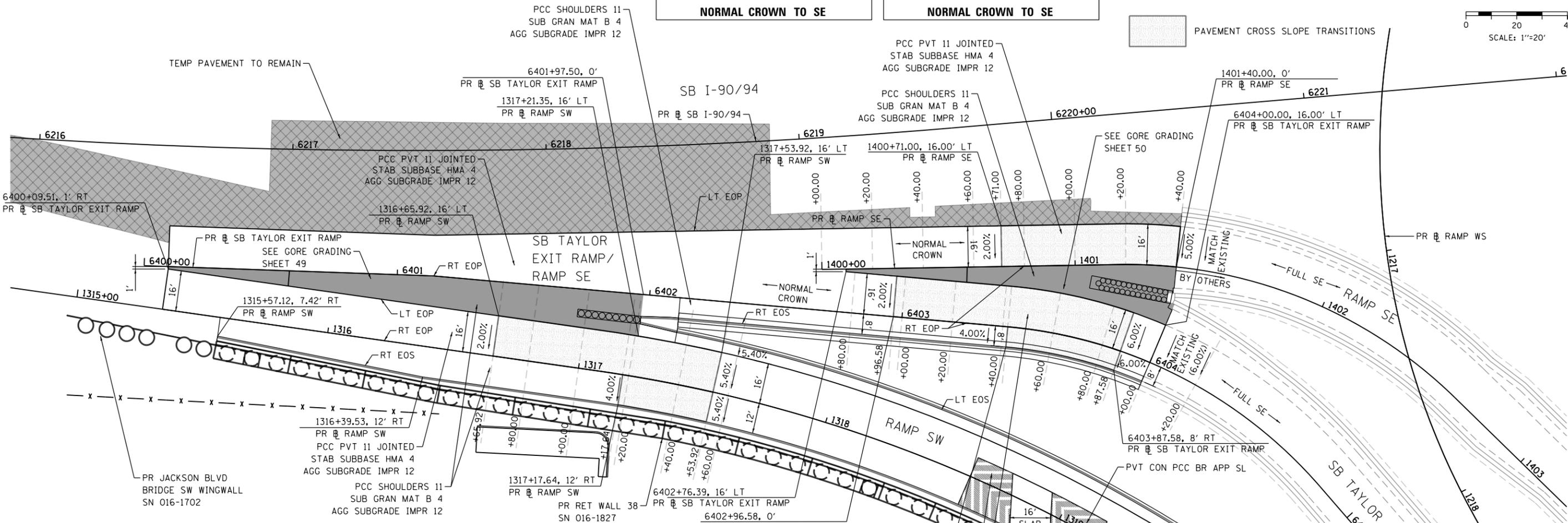
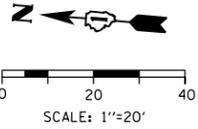
<b>ROADWAY PROFILE</b>	
<b>SB TAYLOR EXIT RAMP</b>	
SCALE: 1"=20'	SHEET 3 OF 3 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	46
CONTRACT NO. 62J31				ILLINOIS FED. AID PROJECT

**CURVE P-CIR-SE-1**  
**SUPERELEVATION TRANSITION FROM**  
**NORMAL CROWN TO SE**

**CURVE P-TAY-SX-1**  
**SUPERELEVATION TRANSITION FROM**  
**NORMAL CROWN TO SE**

**NOTES:**  
 1. UNLESS OTHERWISE NOTED THE PGL IS ASSUMED TO BE AT THE BASELINE



**PAVEMENT CROSS SLOPE AND ELEVATIONS SB TAYLOR EXIT RAMP**

STATION	LANE 1			CENTER JOINT			PGL			RT SHOULDER		
	O/S (LT EOP)	%	EL	O/S	%	EL	O/S (RT EOP)	%	EL	O/S	%	EL
6402+80.00	-16.00	2.00%	578.52	-8.00	2.00%	578.36	0.00	N/A	578.20			
6402+96.58	-16.00	2.00%	578.57	-8.00	2.00%	578.41	0.00	N/A	578.25			
6403+00.00	-16.00	2.16%	578.61	-8.00	2.16%	578.43	0.00	N/A	578.26			
6403+20.00	-16.00	3.03%	578.81	-8.00	3.03%	578.56	0.00	N/A	578.32	8.00	-4.00%	578.00
6403+40.00	-16.00	3.91%	579.01	-8.00	3.91%	578.69	0.00	N/A	578.38	8.00	-4.00%	578.06
6403+41.87	-16.00	4.00%	579.03	-8.00	4.00%	578.71	0.00	N/A	578.39	8.00	-4.00%	578.07
6403+60.00	-16.00	4.81%	579.21	-8.00	4.81%	578.83	0.00	N/A	578.44	8.00	-4.75%	578.06
6403+80.00	-16.00	5.66%	579.41	-8.00	5.66%	578.96	0.00	N/A	578.50	8.00	-5.67%	578.05
6403+87.58	-16.00	6.00%	579.50	-8.00	6.00%	579.01	0.00	N/A	578.54	8.00	-6.00%	578.06
6404+00.00	-16.00	6.00%	579.61	-8.00	6.00%	579.13	0.00	N/A	578.65	8.00	-6.00%	578.17

**PAVEMENT CROSS SLOPE AND ELEVATIONS RAMP SW**

STATION	LANE 1			CENTER JOINT			PGL			RT SHOULDER		
	O/S (LT EOP)	%	EL	O/S	%	EL	O/S (RT EOP)	%	EL	O/S	%	EL
1316+40.00	-16.00	2.00%	577.66	-8.00	2.00%	577.50	0.00	N/A	577.34			
1316+60.00	-16.00	2.00%	577.55	-8.00	2.00%	577.39	0.00	N/A	577.23			
1316+65.92	-16.00	2.00%	577.52	-8.00	2.00%	577.35	0.00	N/A	577.20			
1316+80.00	-16.00	2.52%	577.49	-8.00	2.52%	577.29	0.00	N/A	577.09			
1317+00.00	-16.00	3.30%	577.44	-8.00	3.30%	577.17	0.00	N/A	576.91	12.00	-4.00%	576.43
1317+17.64	-16.00	4.00%	577.36	-8.00	4.00%	577.04	0.00	N/A	576.72	12.00	-4.00%	576.24
1317+20.00	-16.00	4.09%	577.35	-8.00	4.09%	577.03	0.00	N/A	576.70	12.00	-4.09%	576.21
1317+40.00	-16.00	4.86%	577.24	-8.00	4.86%	576.85	0.00	N/A	576.46	12.00	-4.87%	575.87
1317+53.92	-16.00	5.40%	577.15	-8.00	5.40%	576.72	0.00	N/A	576.28	12.00	-5.40%	575.64
1317+60.00	-16.00	5.40%	577.07	-8.00	5.40%	576.64	0.00	N/A	576.21	12.00	-5.40%	575.56
1317+80.00	-16.00	5.40%	576.82	-8.00	5.40%	576.39	0.00	N/A	575.96	12.00	-5.40%	575.31

**PAVEMENT CROSS SLOPE AND ELEVATIONS RAMP SE**

STATION	LANE 1			CENTER JOINT			PGL		
	O/S (LT EOP)	%	EL	O/S	%	EL	O/S (RT EOP)	%	EL
1400+20.00	-16.00	2.00%	578.90	-8.00	2.00%	578.74	0.00	N/A	578.58
1400+40.00	-16.00	2.00%	579.08	-8.00	2.00%	578.92	0.00	N/A	578.76
1400+60.00	-16.00	2.00%	579.32	-8.00	2.00%	579.16	0.00	N/A	579.00
1400+71.00	-16.00	2.00%	579.46	-8.00	2.00%	579.29	0.00	N/A	579.14
1400+80.00	-16.00	2.39%	579.62	-8.00	2.39%	579.43	0.00	N/A	579.24
1401+00.00	-16.00	3.26%	580.03	-8.00	3.26%	579.77	0.00	N/A	579.51
1401+20.00	-16.00	4.14%	580.57	-8.00	4.14%	580.24	0.00	N/A	579.90
1401+40.00	-16.00	5.00%	581.25	-8.00	5.00%	580.85	0.00	N/A	580.45



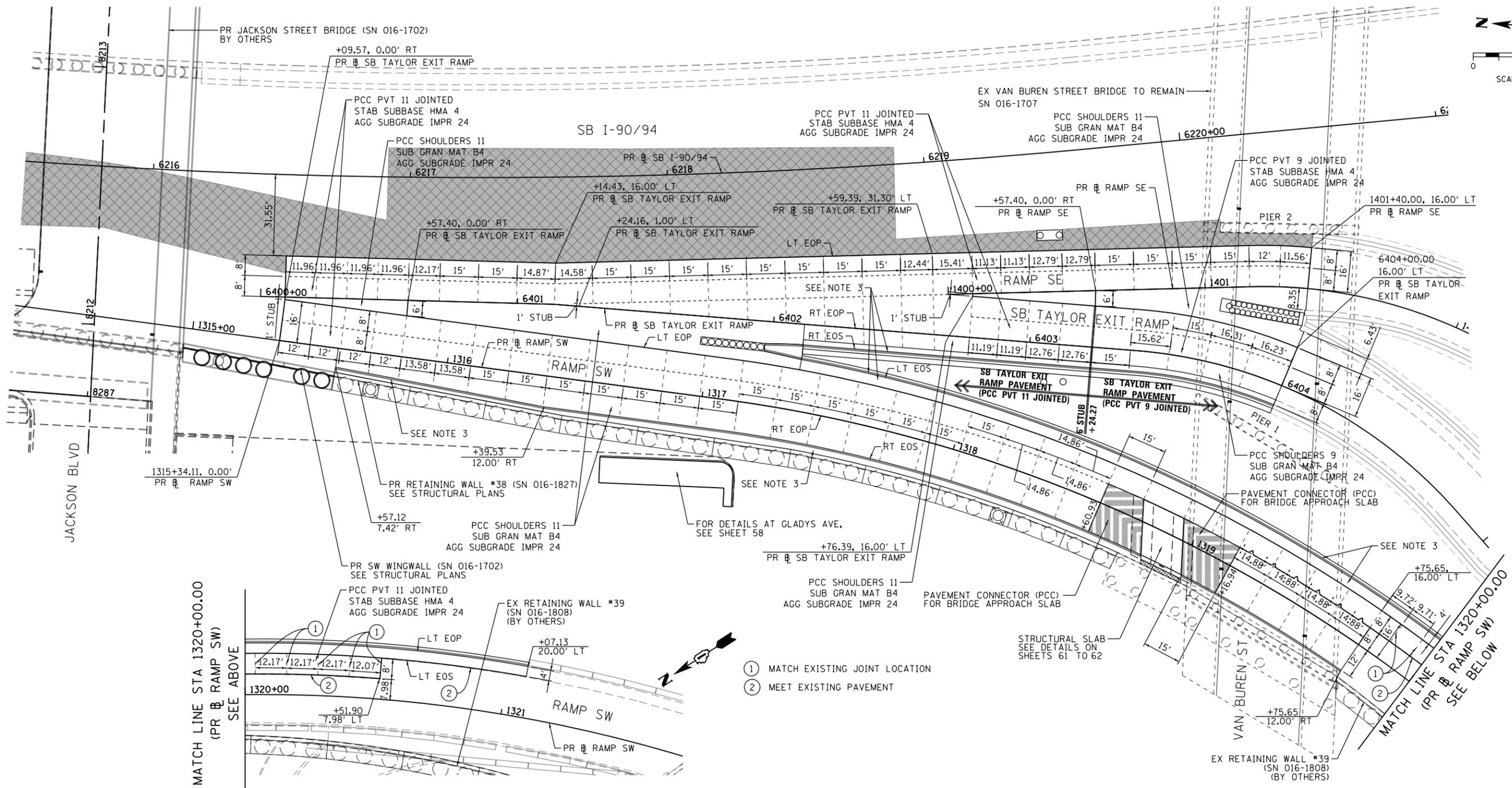
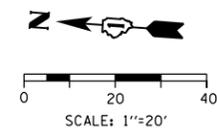
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PLOT SCALE = 40.0027' / in.	CHECKED - OPS	REVISED -
PLOT DATE = 8/15/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT ELEVATION AND SUPERELEVATION DETAILS**  
**RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

SCALE: 1"=20'    SHEET 1 OF 1 SHEETS    STA. 6200+00.00 TO STA. 6204+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	47
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



- ① MATCH EXISTING JOINT LOCATION
- ② MEET EXISTING PAVEMENT

**LEGEND**

- - - - - CONSTRUCTION JOINT
  - o LONGITUDINAL CONSTRUCTION JOINT
  - 1. NO. 6 X 2'-6" LONG DEFORMED TIE BARS (EPOXY COATED) FORMED IN PLACE OR MECHANICALLY INSERTED AT 2' C-C (STANDARD 420001-09) OR
  - 2. NO. 6 X 2' LONG DEFORMED TIE BARS (EPOXY COATED) GROUTED IN PLACE AT 2' C-C (STANDARD 420001-09)
  - o TRANSVERSE CONSTRUCTION JOINT
  - 1'-6" LONG DOWEL BARS (EPOXY COATED) AT 1' C-C (STANDARD 420101-06)
- - - - - SAWED JOINT (MAINLINE)
  - o LONGITUDINAL SAWED JOINT
  - NO. 6 X 2'-6" LONG DEFORMED TIE BARS (EPOXY COATED) AT 2'-6" C-C (STANDARD 420001-09)
  - o TRANSVERSE CONSTRUCTION JOINT
  - 1'-6" LONG DOWEL BARS, 11#2 DIAMETER (EPOXY COATED) AT 1' C-C (STANDARD 420001-09)
- - - - - SAWED JOINT (RAMPS)
  - o LONGITUDINAL SAWED JOINT
  - NO. 6 X 3' LONG DEFORMED TIE BARS AT 1'-6" C-C (DISTRICT STANDARD BD-49)
  - o TRANSVERSE CONSTRUCTION JOINT
  - 1'-6" LONG DOWEL BARS, 11#2 DIAMETER (EPOXY COATED) AT 1' C-C (DISTRICT STANDARD BD-49)
- ===== EXPANSION JOINT
  - o 1'-6" LONG DOWEL BARS, 11#2 DIAMETER (EPOXY COATED) AT 1' C-C (STANDARD 420001-09)

**NOTES**

1. UNLESS NOTED OTHERWISE, ALL STATIONS AND OFFSETS ARE TAKEN FROM PR RAMP SW.
2. UNLESS OTHERWISE NOTED THE PGL IS ASSUMED TO BE AT THE BASELINE.
3. FOR CONCRTE BARRIER DETAILS SEE SHEETS 51 TO 57.
4. SEE PAVEMENT ELEVATION AND SUPERELEVATION DETAILS FOR SHOULDER SLOPES ON SHEET 47.
5. ADDITIONAL SAWED TRANSVERSE CONTRACTION JOINTS MAY BE REQUIRED IN THE FIELD, AS DIRECTED BY THE ENGINEER. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
6. ALL TRANSVERSE JOINTS SHALL BE A MAXIMUM OF 15.0 FEET UNLESS OTHERWISE NOTED. SEE STANDARD 420101-06 FOR ADDITIONAL INFORMATION, REGARDING TRANSVERSE JOINT SPACING REQUIREMENTS.
7. SEE STANDARD 420001-09 FOR INFORMATION REGARDING SEALING REQUIREMENTS FOR TRANSVERSE EXPANSION JOINTS.
8. PAVEMENT ROUNDOUTS FOR MANHOLES AND VALVE VAULTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD 420111-04.
9. ALL ELEVATIONS NOT SHOWN SHALL FOLLOW THE TYPICAL SECTIONS AND PROFILES.



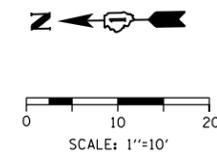
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**JOINING PLAN  
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

SCALE: 1"=20' SHEET 1 OF 1 SHEETS STA. 6215+50.00 TO STA. 6221+05.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	48
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

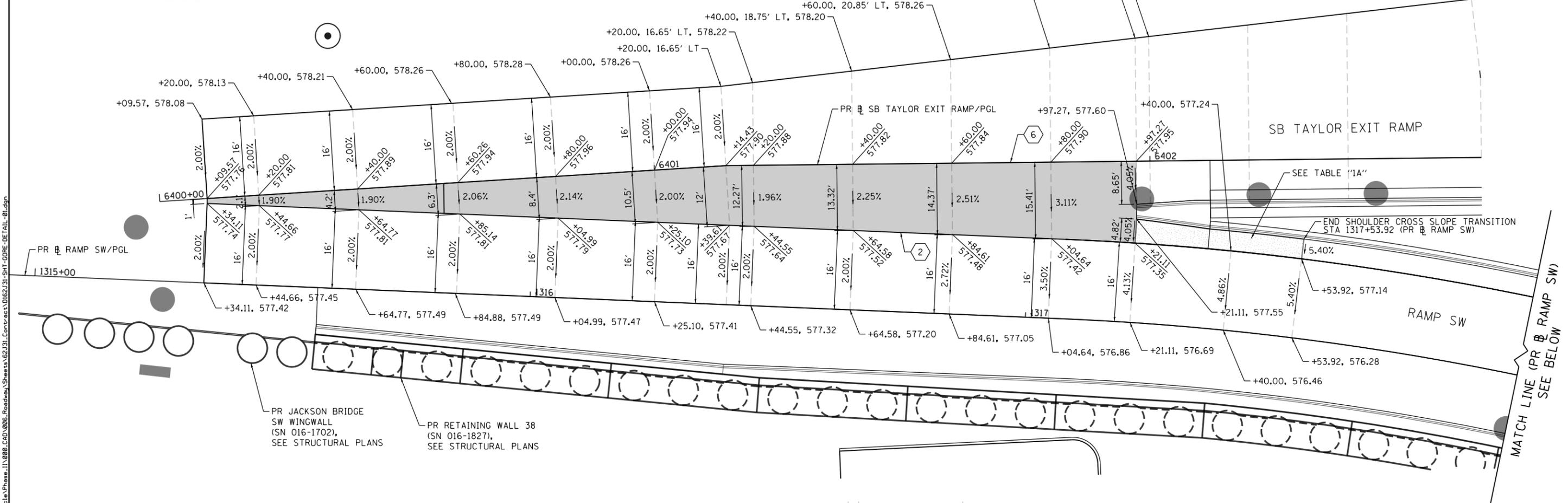


**LEGEND**

- (599.00) EXISTING ELEVATION
- 599.00 PROPOSED ELEVATION
- PROPOSED GORE
- SHOULDER CROSS SLOPE TRANSITIONS

**NOTES:**

1. PARTIAL STATIONING ARE BASED ON THE FOLLOWING BASELINES:
- PR SB I-90/94
  - PR RAMP SW
  - PR MADISON ST ENTRANCE RAMP
  - PR ADAMS ST EXIT RAMP
  - PR JACKSON BLVD EXIT RAMP
  - PR SB TAYLOR ST EXIT RAMP
  - EX RAMP SE

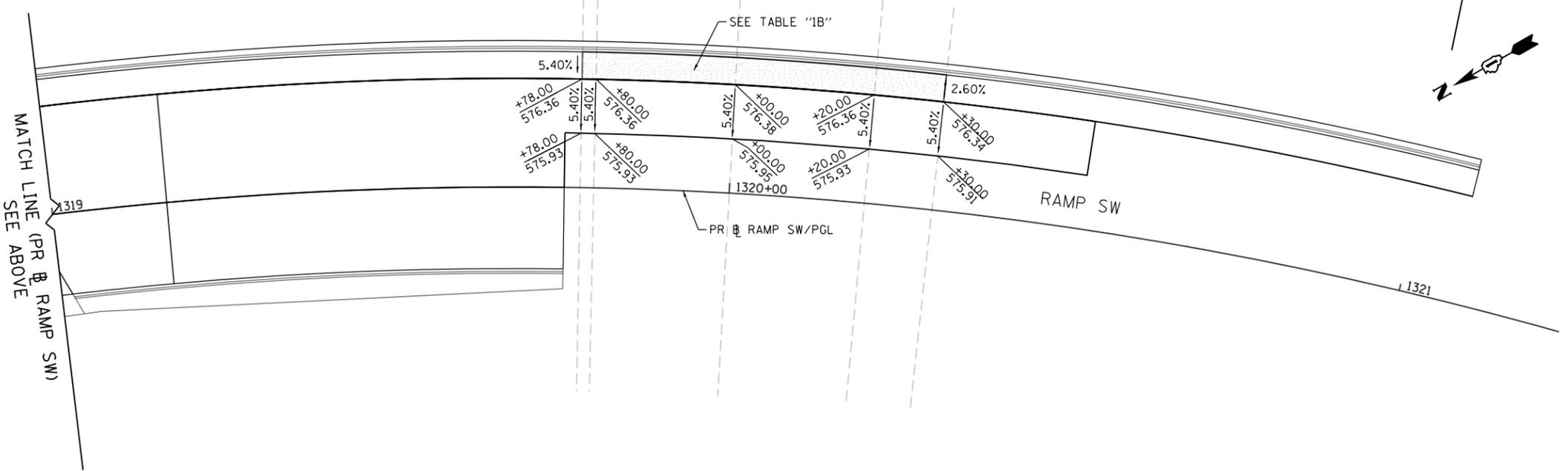


**TABLE "1A"**

SHOULDER CROSS SLOPE AND ELEVATIONS RAMP SW						
STATION	PGL		LANE 1			
RAMP SW	O/S (LT EOP)	%	ELEV	O/S (LT EOP)	%	ELEV
1317+21.11	0.00	-	576.69	-16.00	4.13%	577.35
1317+40.00	0.00	-	576.46	-16.00	4.86%	577.24
1317+53.92	0.00	-	576.28	-16.00	5.40%	577.15

**TABLE "1B"**

SHOULDER CROSS SLOPE AND ELEVATIONS RAMP SW						
STATION	LANE 1			LT SHOULDER		
RAMP SW	O/S (LT EOP)	%	ELEV	O/S (LT EOS)	%	ELEV
1319+78.00	-16.00	5.40%	576.36	-20.00	5.40%	576.58
1319+80.00	-16.00	5.40%	576.38	-20.00	5.09%	576.58
1320+00.00	-16.00	5.40%	576.36	-20.00	2.02%	576.44
1320+20.00	-15.00	5.40%	576.36	-20.00	-1.06%	576.31
1320+30.00	-16.00	5.40%	576.34	-20.00	-2.60%	576.24



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 CHICAGO, IL 60601-5276  
 PHONE: (312) 373-1700 FAX: (312) 373-6800



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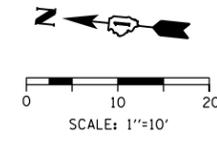
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 REVISED -  
 REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GORE GRADING DETAILS**  
**RAMP SW AND SB TAYLOR EXIT RAMP**

SCALE: 1" = 10' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	49
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

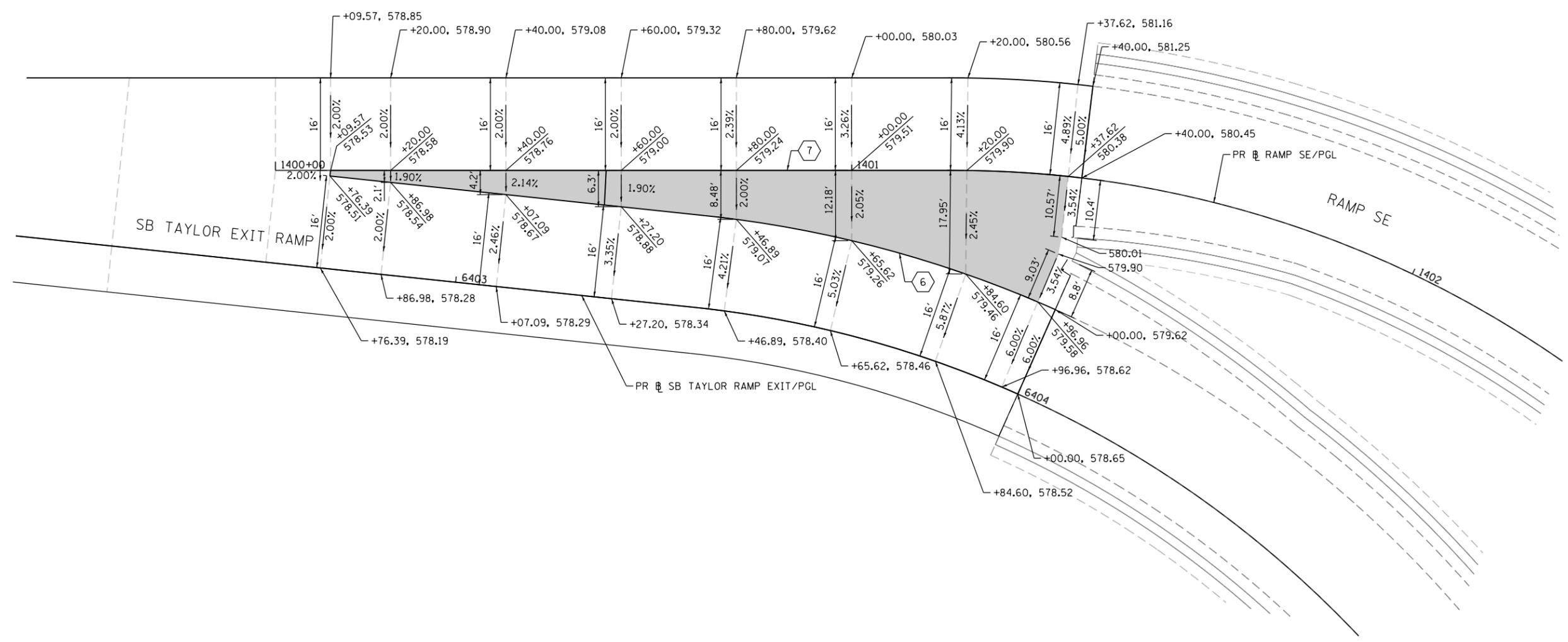


**LEGEND**

- (599.00) EXISTING ELEVATION
- 599.00 PROPOSED ELEVATION
- PROPOSED GORE
- SHOULDER CROSS SLOPE TRANSITIONS

**NOTES:**

1. PARTIAL STATIONING ARE BASED ON THE FOLLOWING BASELINES:
- |   |   |  |
|---|---|--|
| <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">1</span> PR SB I-90/94               | <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">4</span> PR ADAMS ST EXIT RAMP     | <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">7</span> EX RAMP SE |
| <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">2</span> PR RAMP SW                  | <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">5</span> PR JACKSON BLVD EXIT RAMP |  |
| <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">3</span> PR MADISON ST ENTRANCE RAMP | <span style="border: 1px solid black; border-radius: 50%; padding: 2px;">6</span> PR SB TAYLOR ST EXIT RAMP |  |



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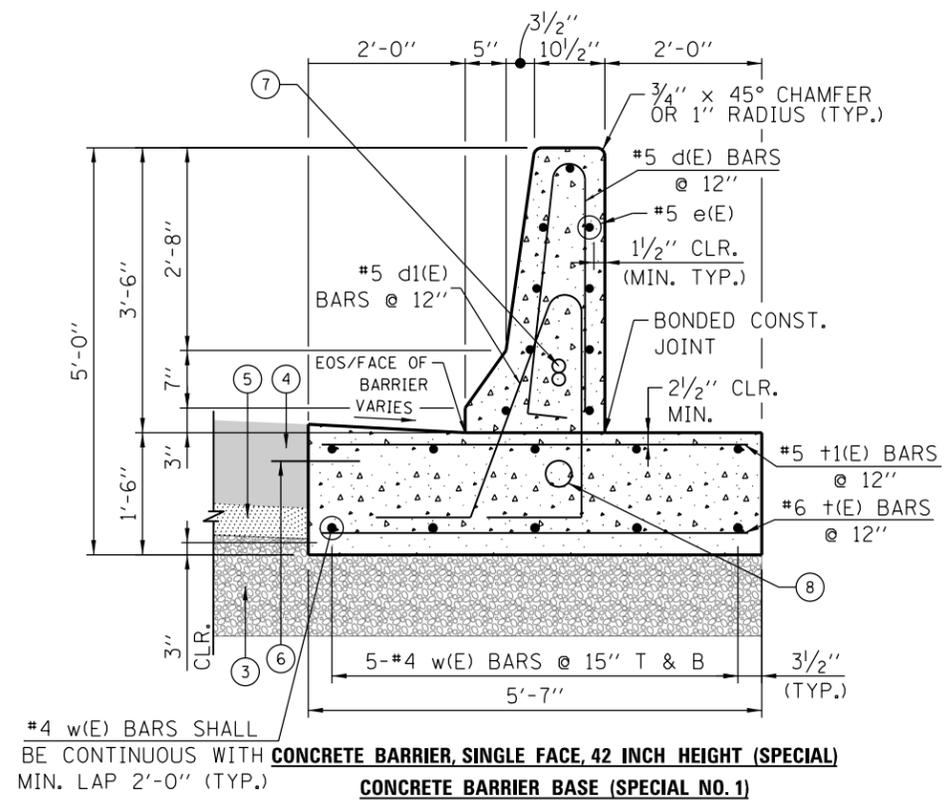
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

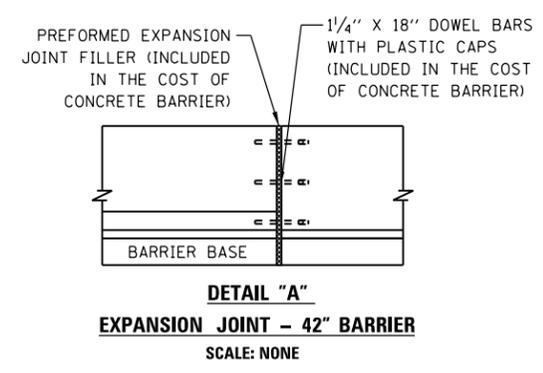
GORE GRADING DETAILS RAMP SE AND SB TAYLOR EXIT RAMP			
SCALE: 1" = 10'	SHEET 2 OF 2 SHEETS	STA. _____	TO STA. _____

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	50
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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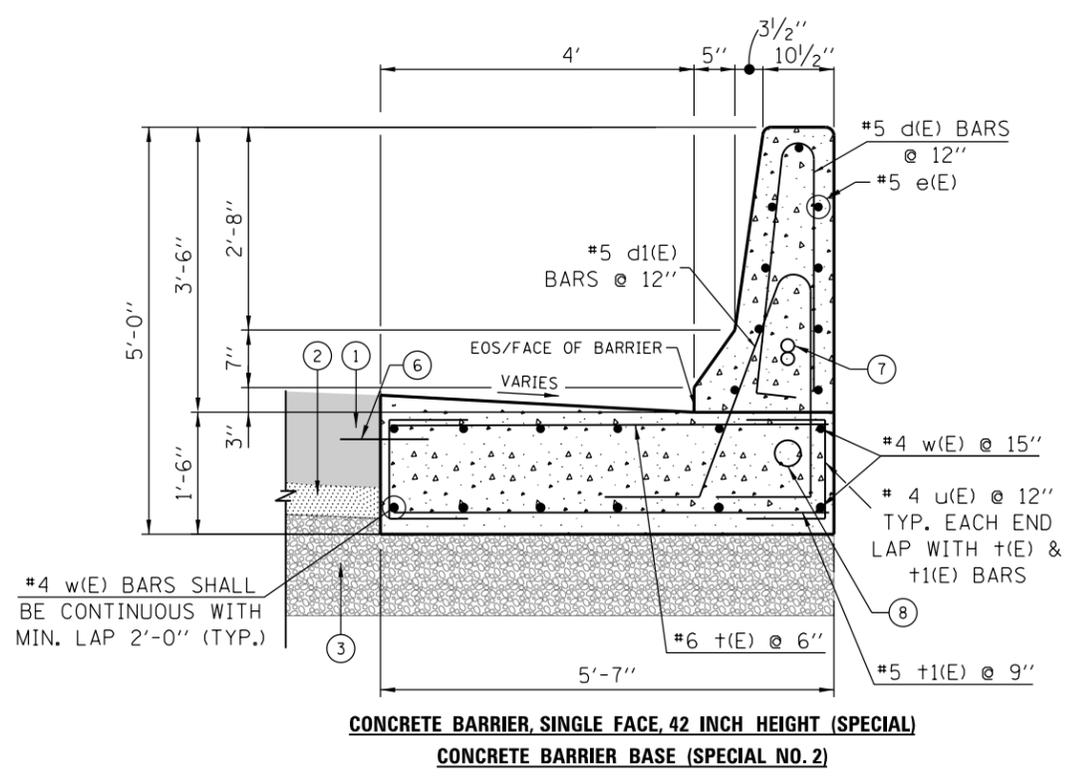


#4 w(E) BARS SHALL BE CONTINUOUS WITH CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) MIN. LAP 2'-0" (TYP.)  
**CONCRETE BARRIER BASE (SPECIAL NO. 1)**

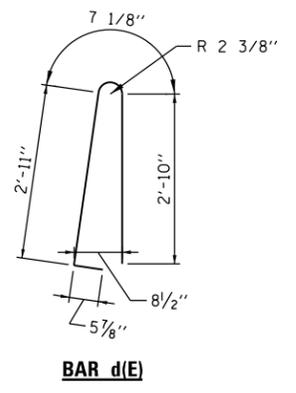


- LEGEND:**
- ① PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED): 9" OR 11"
  - ② STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
  - ③ AGGREGATE SUBGRADE IMPROVEMENT 12"
  - ④ PORTLAND CEMENT CONCRETE SHOULDERS: 9" OR 11"
  - ⑤ SUBBASE GRANULAR MATERIAL, TYPE B 4"
  - ⑥ EPOXY COATED TIE BARS, NO. 8, 30" LONG @ 24" C-C (ADJACENT TO PCC PAVEMENT OR SHOULDER, INCLUDED IN PRICE FOR BID OF VARIOUS TYPES OF CONCRETE BARRIER BASES)
  - ⑦ 2-2" DIAMETER CONDUITS IN BARRIER, (WHERE APPLICABLE) - LOCATIONS AS SPECIFIED IN ELECTRICAL OR ITS PLANS
  - ⑧ 1-4" DIAMETER CONDUIT, (WHERE APPLICABLE) - LOCATIONS AS SPECIFIED IN ELECTRICAL OR ITS PLANS

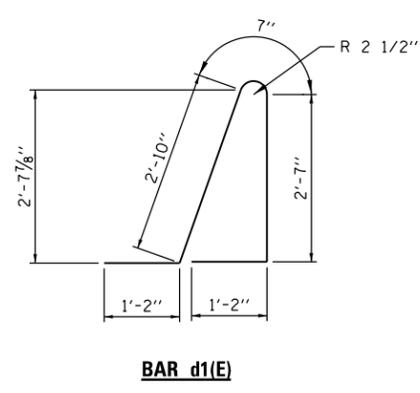
- NOTES:**
1. TOP SHOULDER EDGE OF BARRIER BASE GUTTER SHALL MATCH THE TOP OF SHOULDER ELEVATION.
  2. 1" DEEP CONTRACTION JOINTS SHALL BE CONSTRUCTED IN BOTH THE REINFORCED CONCRETE BARRIER WALL AND BASE.
  3. THE FORMING OF CONTRACTION JOINTS SHALL BE DONE WITH AN APPROVED FINISHING TOOL OR BY SAWING AT THE DISCRETION OF THE ENGINEER SUBJECT TO THE SATISFACTORY CONTROL OF CRACKING.
  4. REINFORCING BARS DESIGNATED "(E)" SHALL BE EPOXY COATED.
  5. REINFORCEMENT BENDING DETAILS SHALL BE IN ACCORDANCE WITH THE "MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES", ACI 315, LATEST EDITION.
  6. EXPANSION JOINTS SHOWN ON THIS DRAWING SHALL BE PREFORMED JOINT MATERIAL (BITUMINOUS TYPE) FILLER AND SHALL MEET AASHTO DESIGNATION M-33.
  7. ALL WORK AND MATERIALS DETAILED HEREIN SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
  8. PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER WALL PAY ITEMS.
  9. JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS
  10. TWO VERTICAL EPOXY COATED, NO. 8 TIE BARS, 10" LONG, SHALL BE PLACED STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 48" CENTERS ALONG THE CONCRETE BARRIER. TIE BARS SHALL BE INCLUDED IN THE COST OF CONCRETE BARRIERS.
  11. EXPANSION JOINTS SHALL BE CONSTRUCTED IN BARRIER WALL AT MAXIMUM JOINT SPACING OF 90 FEET.
  12. VERTICAL TIE BARS ARE REQUIRED WHEN CONCRETE BARRIER AND CONCRETE BARRIER BASE ARE NOT POURED MONOLITHICALLY.
  13. FOR SUPERELEVATION SECTIONS, 1'-6" BARRIER BASE THICKNESS SHALL BE MAINTAINED FOR CONCRETE BARRIER BASE (SPECIAL NO. 1) AND CONCRETE BARRIER BASE (SPECIAL NO. 2).
  14. ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.



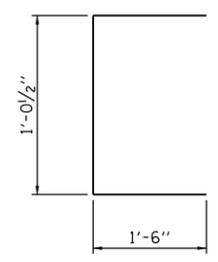
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**CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)**  
**CONCRETE BARRIER BASE (SPECIAL NO. 2)**



**BAR d(E)**



**BAR d1(E)**



**BAR u(E)**



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 PLOT SCALE = 100.0000' / 1" / 1"  
 PLOT DATE = 8/15/2019

DESIGNED - OPS  
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 DATE - 8/16/2019

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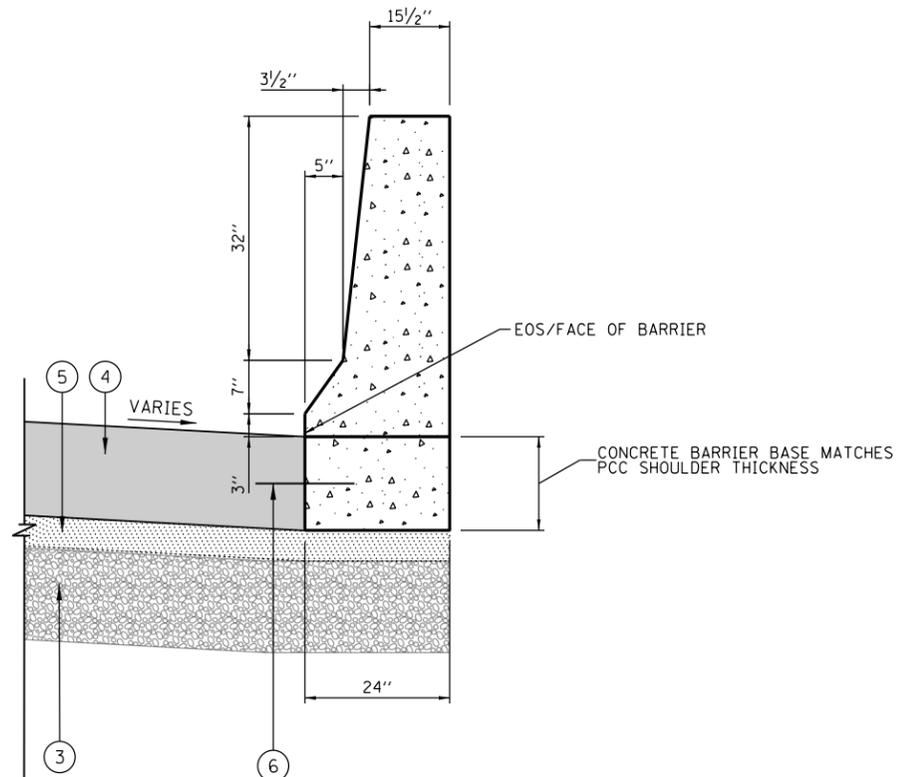
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CONCRETE BARRIER DETAILS

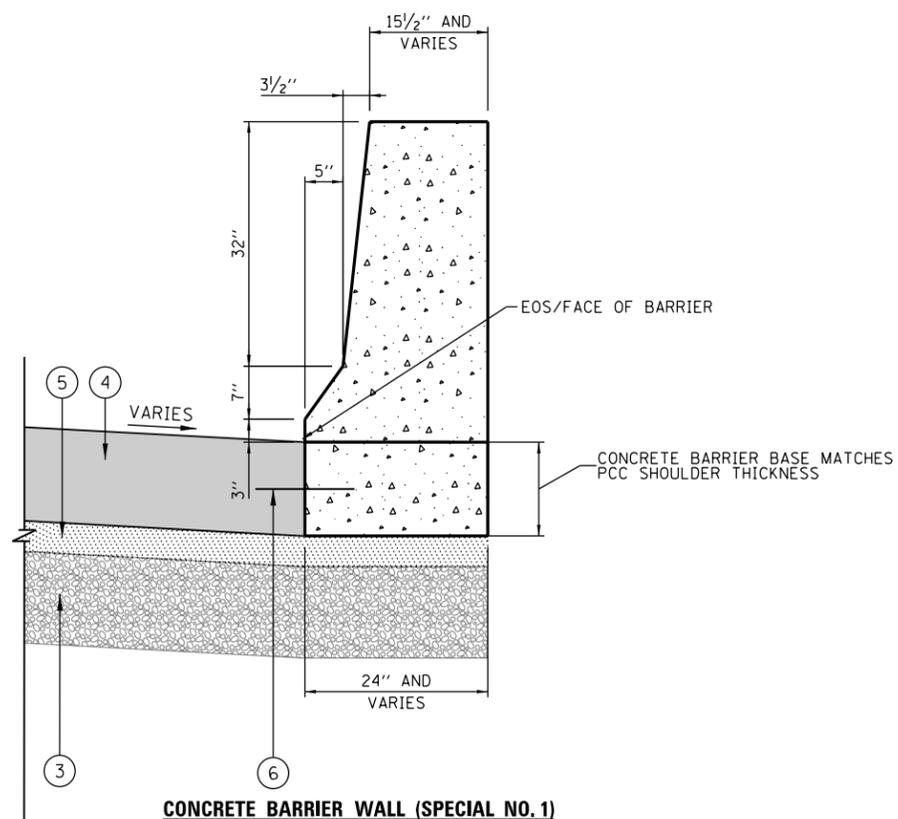
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CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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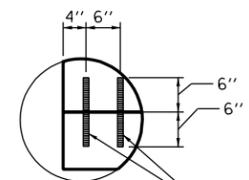
**CONCRETE BARRIER WALL (SPECIAL)**  
**CONCRETE BARRIER BASE (SPECIAL NO. 3)**



**CONCRETE BARRIER WALL (SPECIAL NO. 1)**  
**CONCRETE BARRIER BASE (SPECIAL NO. 4)**

**LEGEND:**

- ① PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- ② STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- ③ AGGREGATE SUBGRADE IMPROVEMENT 12"
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 11"
- ⑤ SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ⑥ EPOXY COATED TIE BARS, NO. 6, 30" LONG AT 36" C-C (INCLUDED IN PRICE FOR BID FOR PCC SHOULDER)



**DETAIL**  
**VERTICAL TIE BARS**  
SCALE: NONE

NO. 6 TIE BARS, 12" LONG, STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 30" CENTER WHERE APPLICABLE

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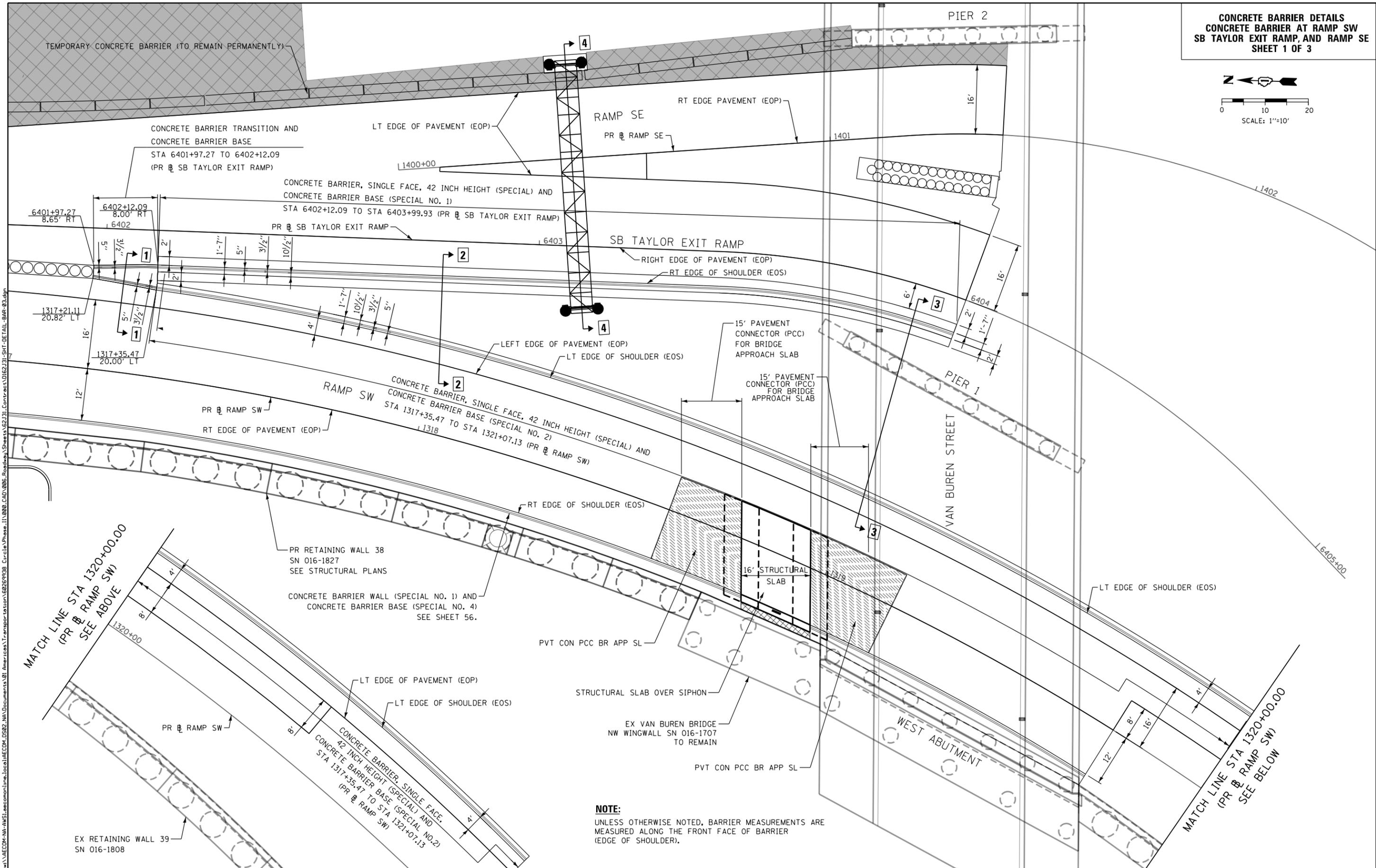
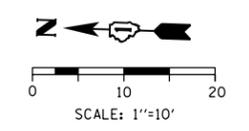
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**CONCRETE BARRIER DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	52
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	

**CONCRETE BARRIER DETAILS  
CONCRETE BARRIER AT RAMP SW  
SB TAYLOR EXIT RAMP, AND RAMP SE  
SHEET 1 OF 3**



**NOTE:**  
UNLESS OTHERWISE NOTED, BARRIER MEASUREMENTS ARE MEASURED ALONG THE FRONT FACE OF BARRIER (EDGE OF SHOULDER).

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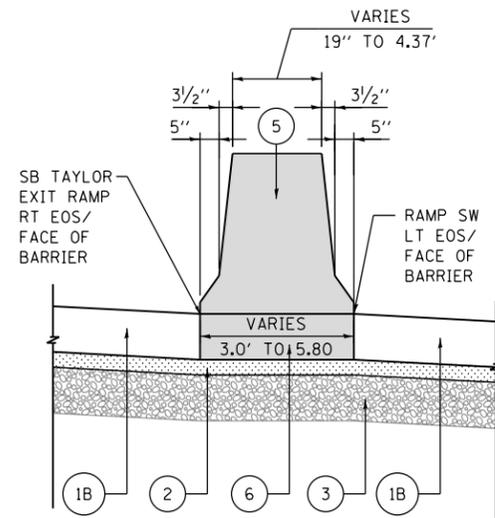


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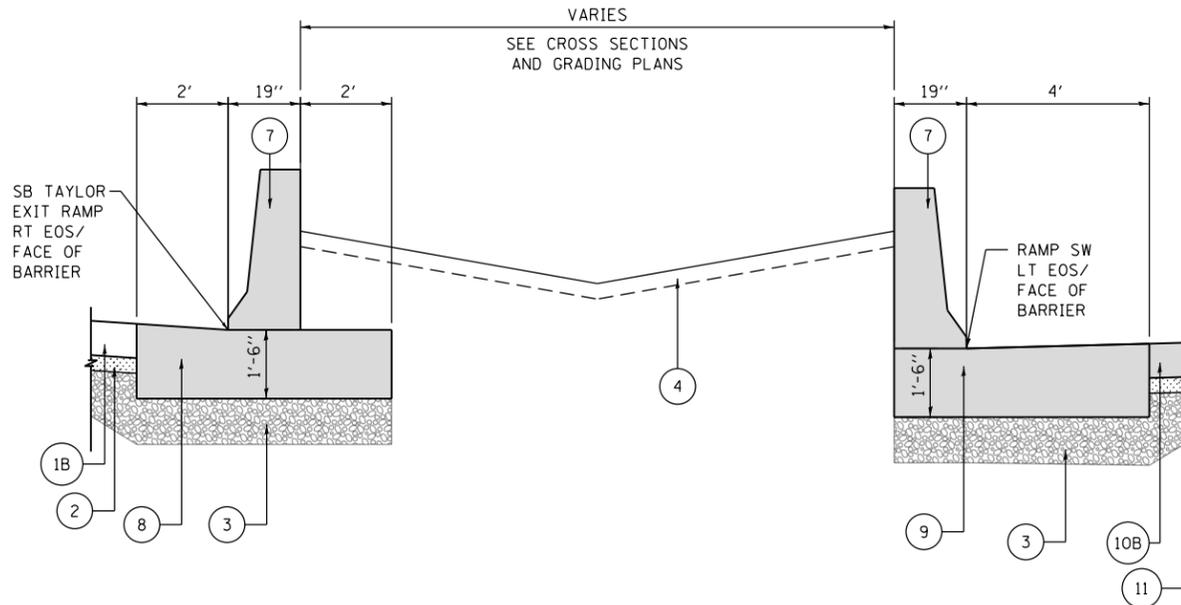
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CONCRETE BARRIER DETAILS CONCRETE BARRIER AT RAMP SW, SB TAYLOR EXIT RAMP, RAMP SE</b>	
SCALE: NONE	SHEET 3 OF 7 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



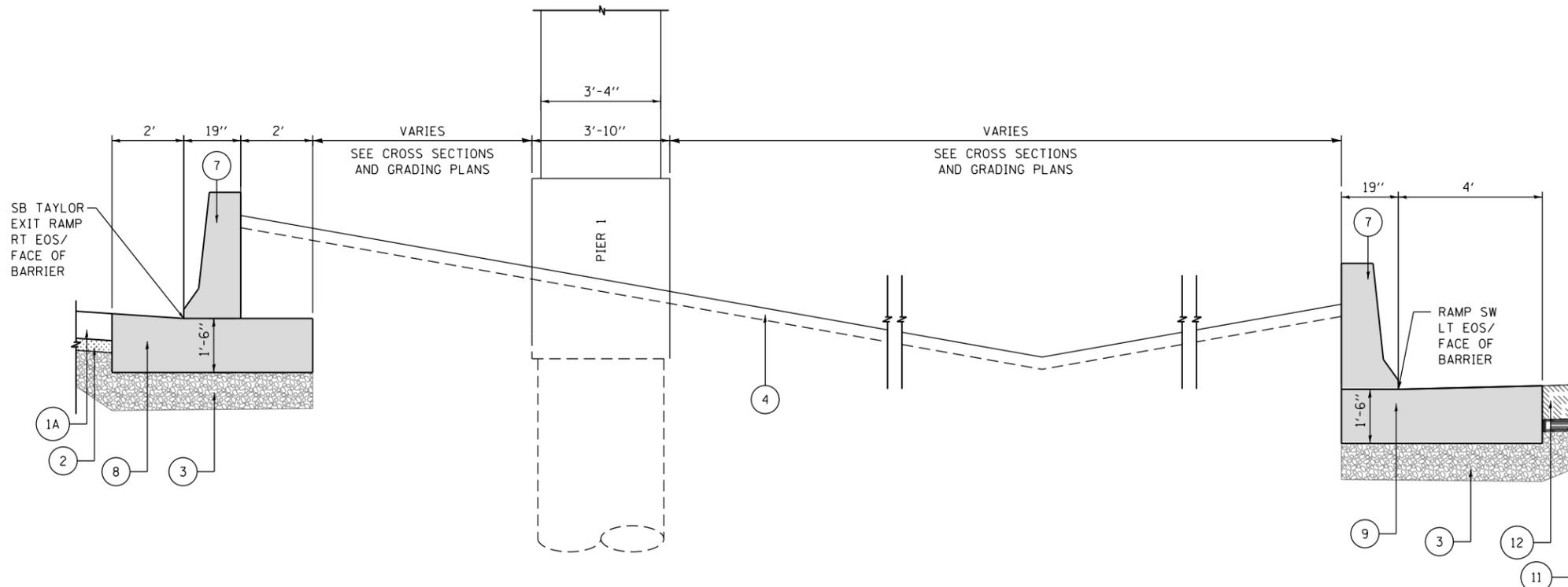
**SECTION 1-1**  
 CONCRETE BARRIER TRANSITION AT GORE  
 SCALE: NONE



**SECTION 2-2**  
 CONCRETE BARRIER WALL  
 SCALE: NONE

**LEGEND:**

- 1A PORTLAND CEMENT CONCRETE SHOULDERS 9"
- 1B PORTLAND CEMENT CONCRETE SHOULDERS 11"
- 2 SUBBASE GRANULAR MATERIAL, TYPE B 4"
- 3 AGGREGATE SUBGRADE IMPROVEMENT 12"
- 4 TOPSOIL FURNISH AND PLACE, 4"
- 5 CONCRETE BARRIER TRANSITION
- 6 CONCRETE BARRIER BASE
- 7 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- 8 CONCRETE BARRIER BASE (SPECIAL NO. 1)
- 9 CONCRETE BARRIER BASE (SPECIAL NO. 2)
- 10A PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- 10B PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- 11 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- 12 PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB



**SECTION 3-3**  
 CONCRETE BARRIER WALLS AT VAN BUREN ST BRIDGE PIER 1  
 SCALE: NONE

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D162J31-SHT-DETAIL-BAR-04.dgn  
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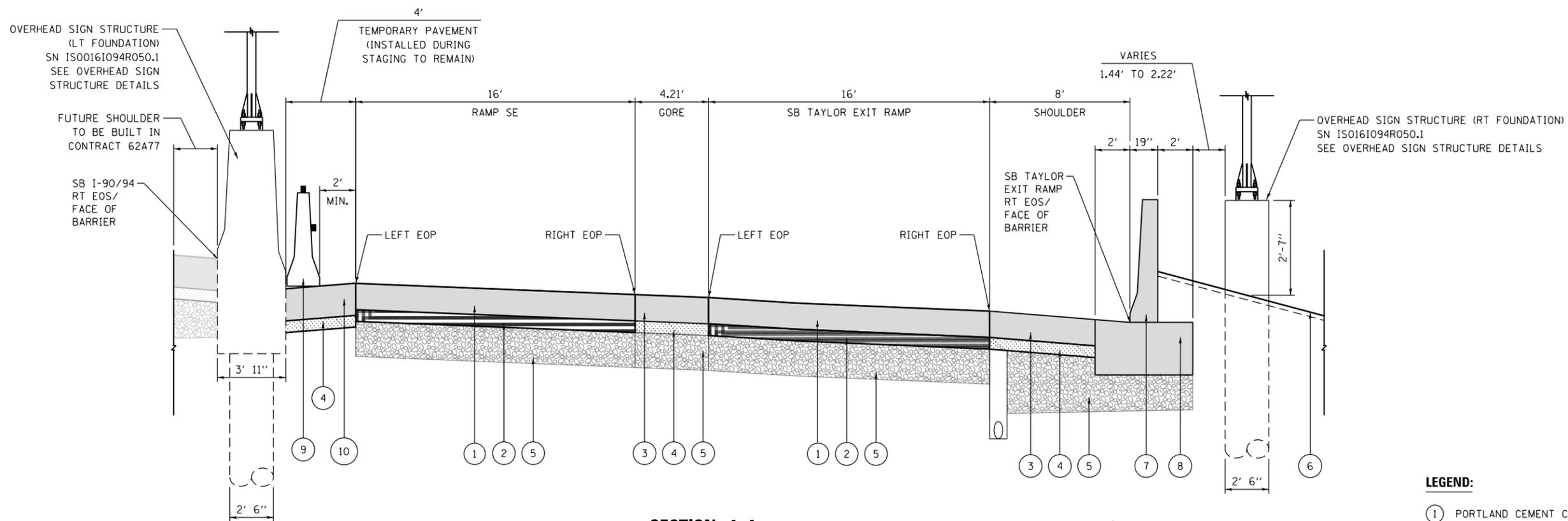
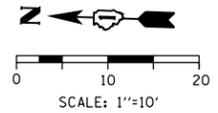
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 DEPARTMENT OF TRANSPORTATION

CONCRETE BARRIER DETAILS  
 CONCRETE BARRIERS AT  
 RAMP SW, SB TAYLOR EXIT RAMP, RAMP SE

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	54
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



**SECTION 4-4**  
**CONCRETE BARRIER WALL**  
**AT OVERHEAD SIGN STRUCTURE**

SCALE: NONE

**LEGEND:**

- ① PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- ② STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- ③ PORTLAND CEMENT CONCRETE SHOULDERS 11"
- ④ SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ⑤ AGGREGATE SUBGRADE IMPROVEMENT 12"
- ⑥ TOPSOIL FURNISH AND PLACE, 4"
- ⑦ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- ⑧ CONCRETE BARRIER BASE (SPECIAL NO. 1)
- ⑨ TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)
- ⑩ TEMPORARY PAVEMENT (PCC/HMA)

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 PLOT DATE = 8/15/2019

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 DATE - 8/16/2019

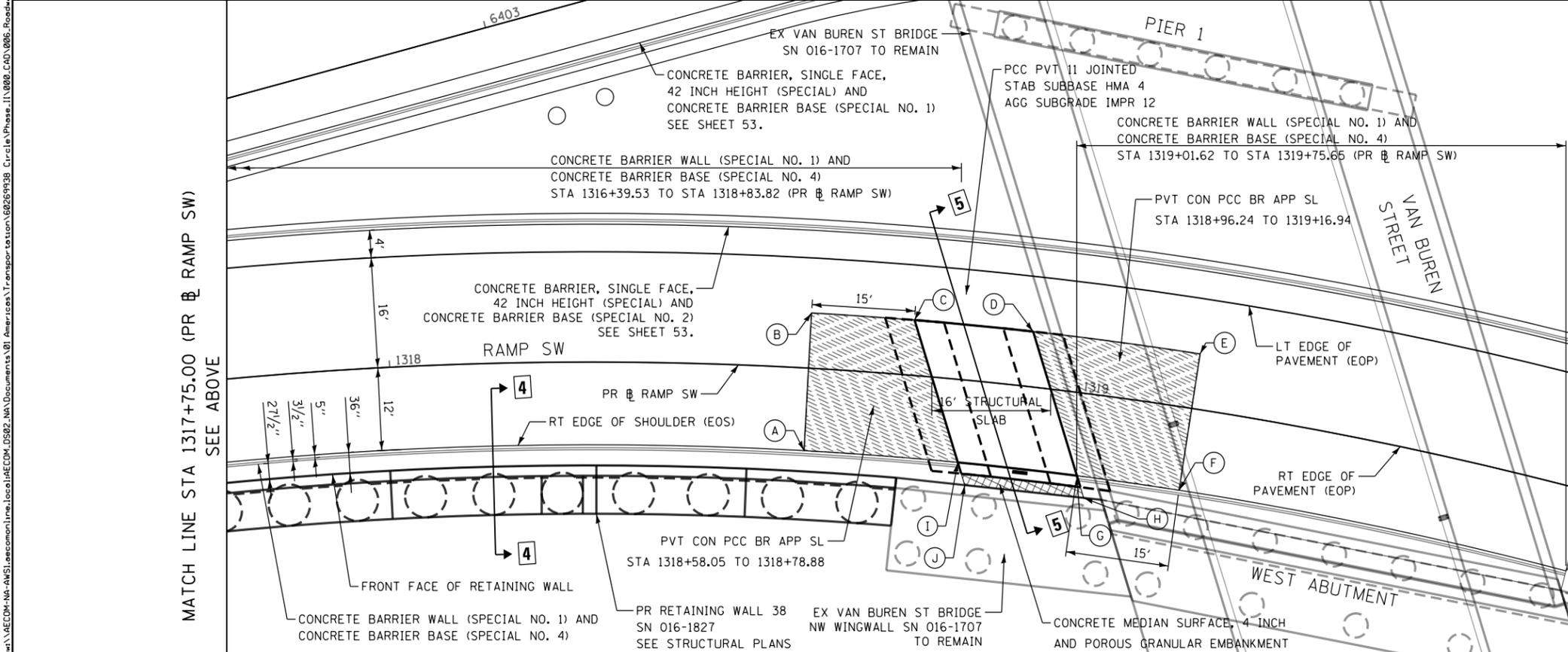
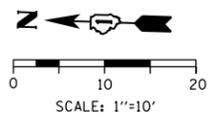
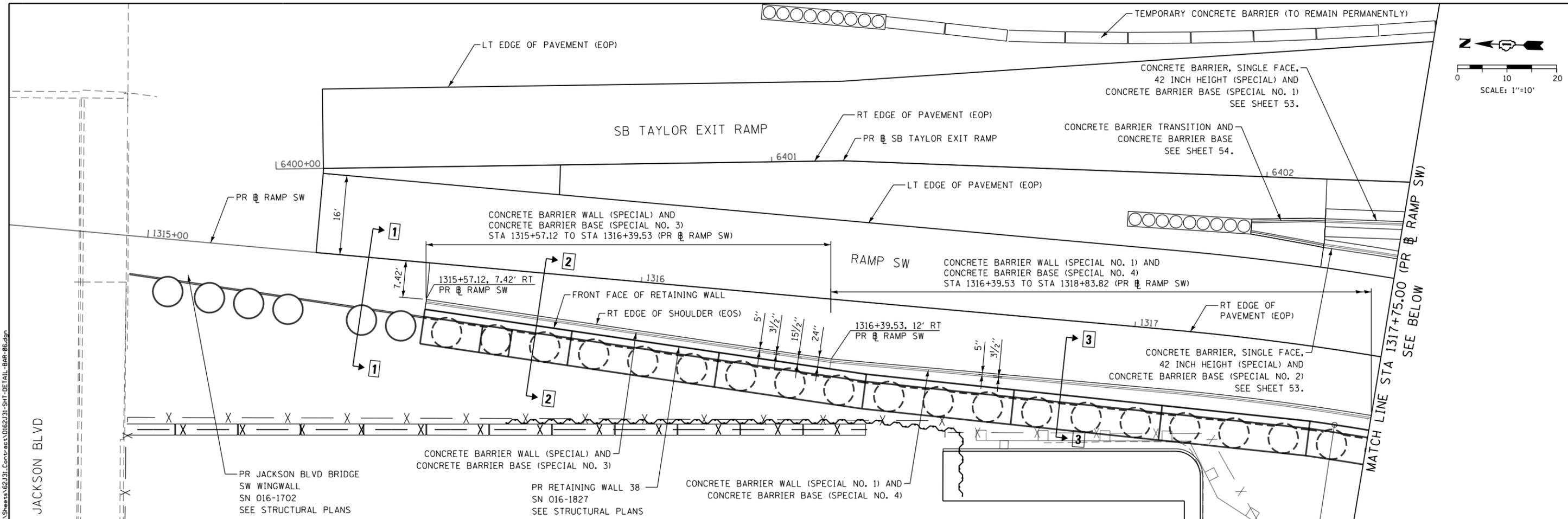
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CONCRETE BARRIER DETAILS  
 CONCRETE BARRIER AT OVERHEAD SIGN STRUCTURE  
 SB TAYLOR EXIT RAMP AND RAMP SE

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

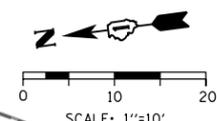
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	55
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



POINT	STATION	OFFSET	LT/RT	BASELINE	DESCRIPTION
(A)	1318+60.93	12'	RT	RAMP SW	EDGE OF PVT CON PCC BR APP SL
(B)	1318+60.93	8'	LT	RAMP SW	EDGE OF PVT CON PCC BR APP SL
(C)	1318+75.71	8'	LT	RAMP SW	EDGE OF STRUCTURAL SLAB
(D)	1318+92.79	8'	LT	RAMP SW	EDGE OF STRUCTURAL SLAB
(E)	1319+16.94	8'	LT	RAMP SW	EDGE OF PVT CON PCC BR APP SL
(F)	1319+16.94	12'	RT	RAMP SW	EDGE OF PVT CON PCC BR APP SL
(G)	1319+01.62	12'	RT	RAMP SW	RT EOS, FACE OF PARAPET
(H)	1319+03.05	15.07'	RT	RAMP SW	BACK CONCRETE MEDIAN SURFACE
(I)	1318+83.82	12'	RT	RAMP SW	RT EOS, FACE OF PARAPET
(J)	1318+85.18	15.18'	RT	RAMP SW	BACK CONCRETE MEDIAN SURFACE

**NOTE:**

- UNLESS OTHERWISE NOTED, BARRIER MEASUREMENTS ARE MEASURED ALONG THE FRONT FACE OF BARRIER (EDGE OF SHOULDER).
- FOR SECTIONS 1-1, 2-2, 3-3, 4-4 AND 5-5, SEE SHEET 57.



**CONCRETE BARRIER DETAILS  
CONCRETE BARRIER AT  
PROPOSED RETAINING WALL 38  
SHEET 1 OF 2**

<p><b>AECOM</b> 303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-5276 PHONE: (312) 373-1700 FAX: (312) 373-6800</p>	<p>D162J31-SHT-DETAIL-BAR-06.dgn USER NAME = pimsarno PLOT SCALE = 20.0000' / in. PLOT DATE = 8/15/2019</p>	<p>DESIGNED - OPS DRAWN - OPS/ZND CHECKED - MJE DATE - 8/16/2019</p>	<p>REVISED - REVISED - REVISED - REVISED -</p>
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

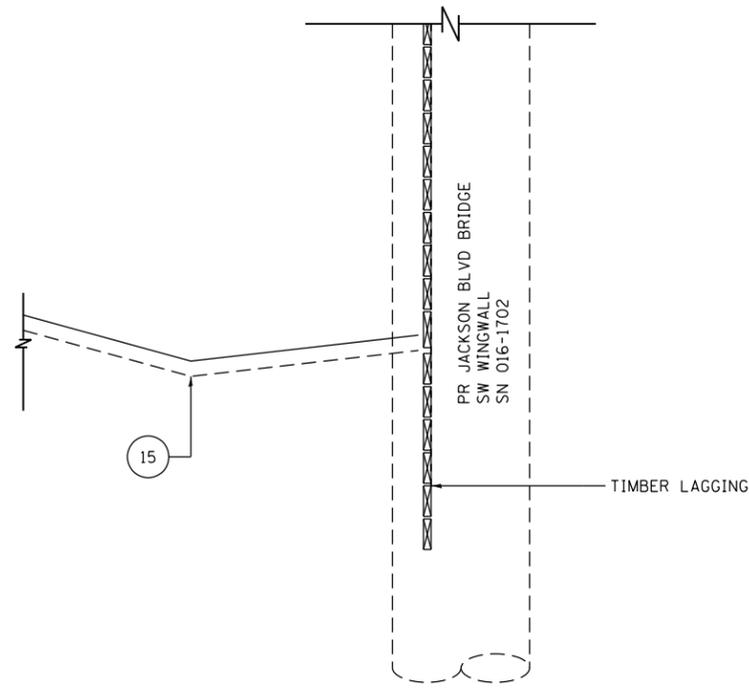
**CONCRETE BARRIER DETAILS  
CONCRETE BARRIER ALONG  
PROPOSED RETAINING WALL 38**

SCALE: NONE    SHEET 6 OF 7 SHEETS    STA.    TO STA.

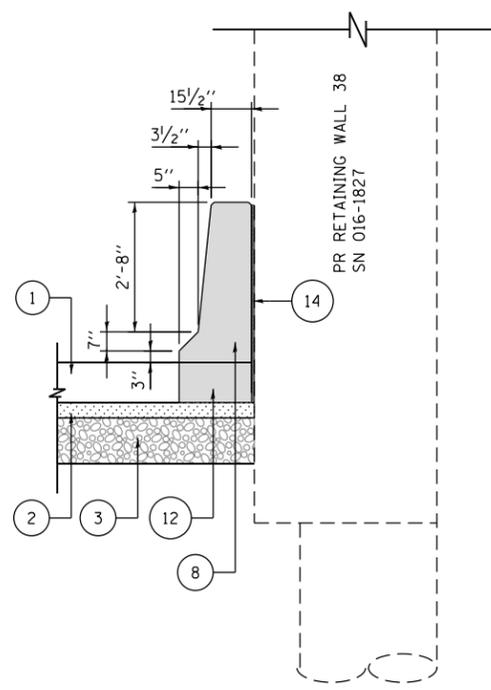
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CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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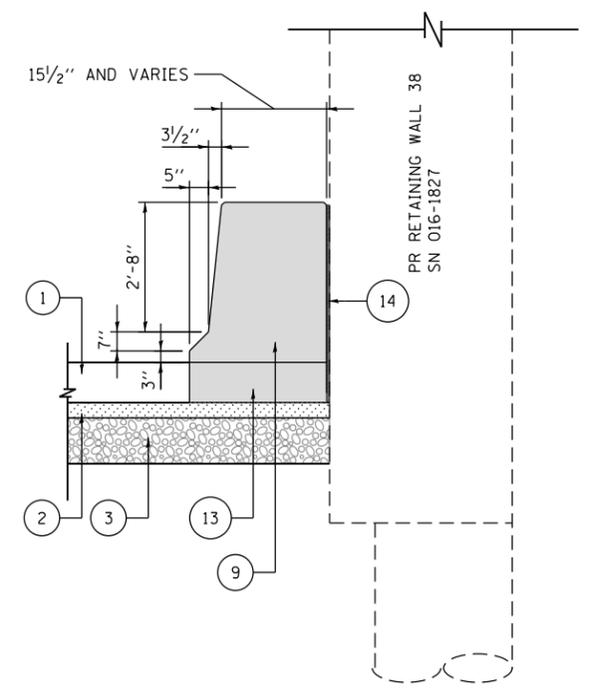
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 303 EAST WACKER DRIVE, SUITE 1400  
 CHICAGO, IL 60601-5276  
 PHONE: (312) 373-1700 FAX: (312) 373-6800



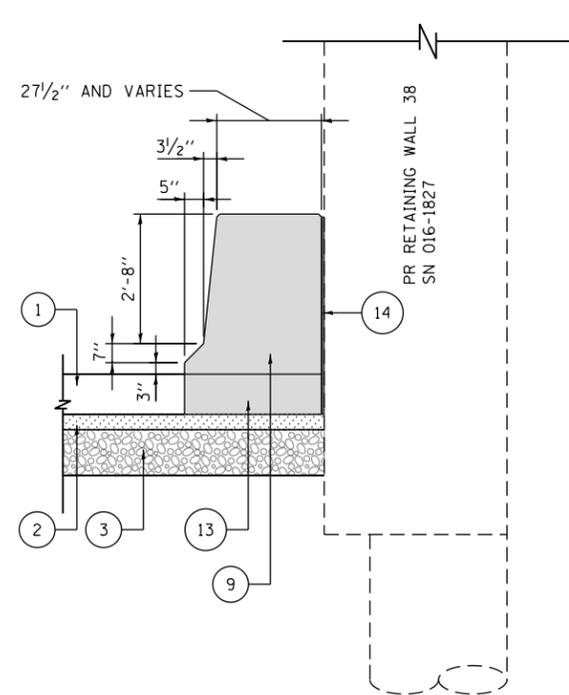
**SECTION 1-1**  
 ALONG SW WINGWALL OF  
 PR JACKSON BLVD BRIDGE  
 SCALE: NONE



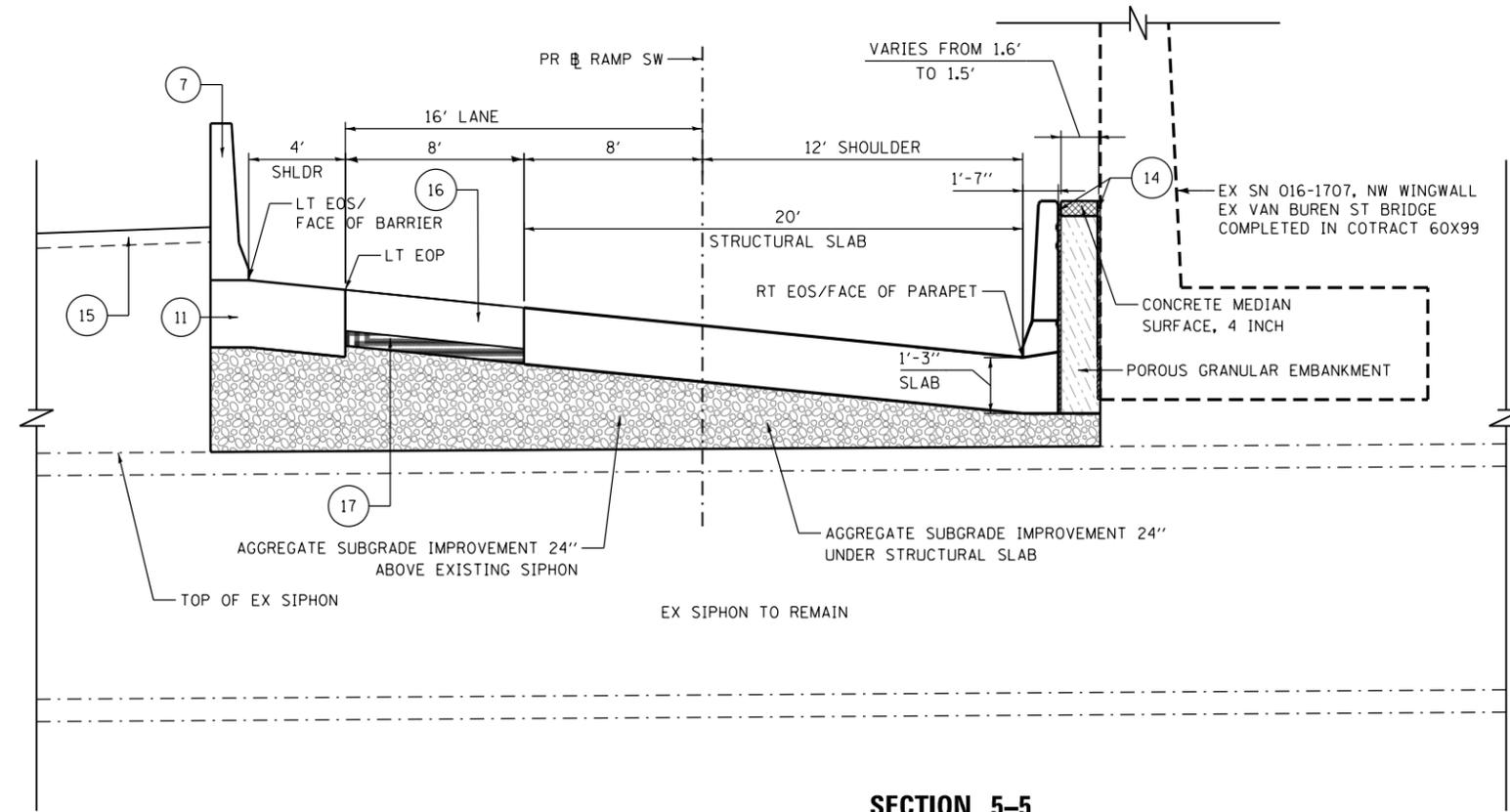
**SECTION 2-2**  
 CONCRETE BARRIER WALL ALONG PR RETAINING WALL 38  
 SCALE: NONE



**SECTION 3-3**  
 CONCRETE BARRIER WALL ALONG PR RETAINING WALL 38  
 SCALE: NONE



**SECTION 4-4**  
 CONCRETE BARRIER WALL ALONG PR RETAINING WALL 38  
 SCALE: NONE



**SECTION 5-5**  
 PARAPET AND STRUCTURAL SLAB OVER SYPHON /  
 EX VAN BUREN ST BRIDGE NW WINGWALL  
 SCALE: NONE

**LEGEND:**

- ① PORTLAND CEMENT CONCRETE SHOULDERS 11"
- ② SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ③ AGGREGATE SUBGRADE IMPROVEMENT 12"
- ④ AGGREGATE SURFACE COURSE, TYPE B
- ⑤ CONCRETE BARRIER TRANSITION
- ⑥ CONCRETE BARRIER BASE
- ⑦ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- ⑧ CONCRETE BARRIER WALL (SPECIAL)
- ⑨ CONCRETE BARRIER WALL (SPECIAL NO. 1)
- ⑩ CONCRETE BARRIER BASE (SPECIAL NO. 1)
- ⑪ CONCRETE BARRIER BASE (SPECIAL NO. 2)
- ⑫ CONCRETE BARRIER BASE (SPECIAL NO. 3)
- ⑬ CONCRETE BARRIER BASE (SPECIAL NO. 4)
- ⑭ 3/4" PREFORMED JOINT FILLER (PJF), INCLUDED IN THE COST OF CONCRETE BARRIER
- ⑮ TOPSOIL FURNISH AND PLACE, 4"
- ⑯ PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- ⑰ STABILIZED SUBBASE - HOT MIX ASPHALT, 4"

**CONCRETE BARRIER DETAILS  
 CONCRETE BARRIER AT  
 PROPOSED RETAINING WALL 38  
 SHEET 2 OF 2**



D162J31-SHT-DETAIL-BAR-07.dgn  
 USER NAME = pimsarno  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - OPS  
 DRAWN - APA  
 CHECKED - MJE  
 DATE - 8/16/2019

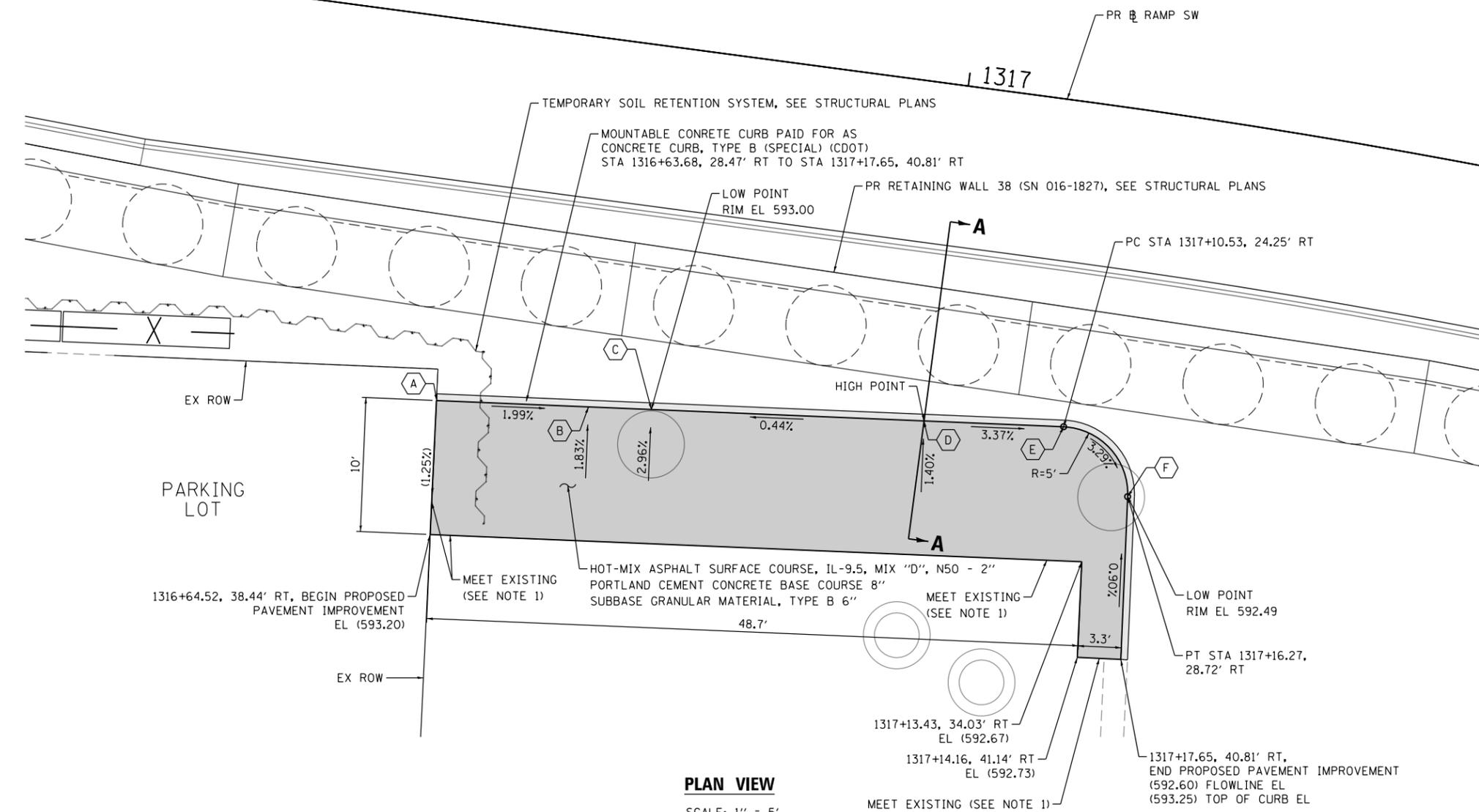
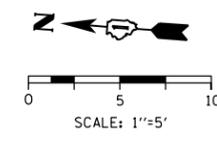
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CONCRETE BARRIER DETAILS  
 CONCRETE BARRIER AT  
 PROPOSED RETAINING WALL 38**

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	57
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

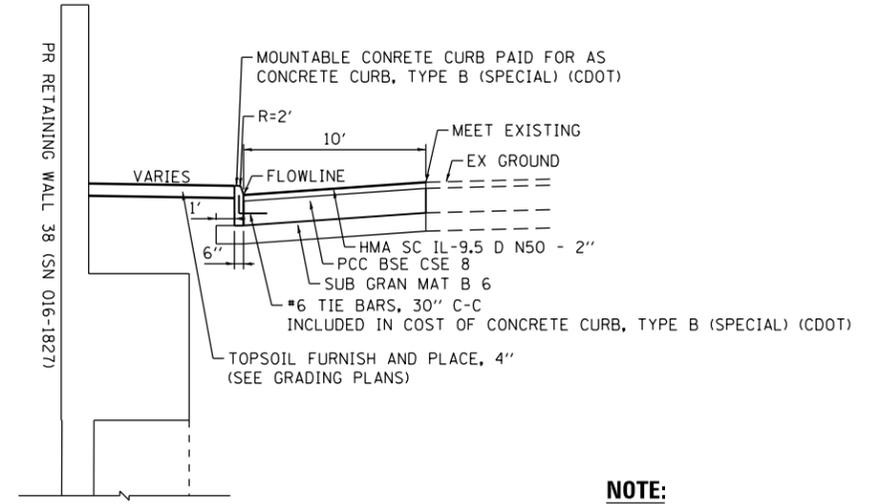


**PLAN VIEW**  
SCALE: 1" = 5'

**LEGEND**

- (592.00) EXISTING ELEVATION
- 592.00 PROPOSED ELEVATION

POINT	STATION	OFFSET	EDGE OF PAVEMENT ELEVATION	TOP OF CURB ELEVATION
A	1316+63.68	28.47' RT	(593.32)	593.57
B	1316+75.00	27.45' RT	593.09	593.34
C	1316+79.68	27.03' RT	593.00	593.25
D	1317+00.00	25.20' RT	593.09	593.34
E	1317+10.53	24.25' RT	592.74	592.99
F	1317+16.27	28.72' RT	592.49	592.74



**SECTION A-A**

**NOTE:**

1. EXISTING ELEVATIONS SHOWN SHALL BE FIELD VERIFIED.

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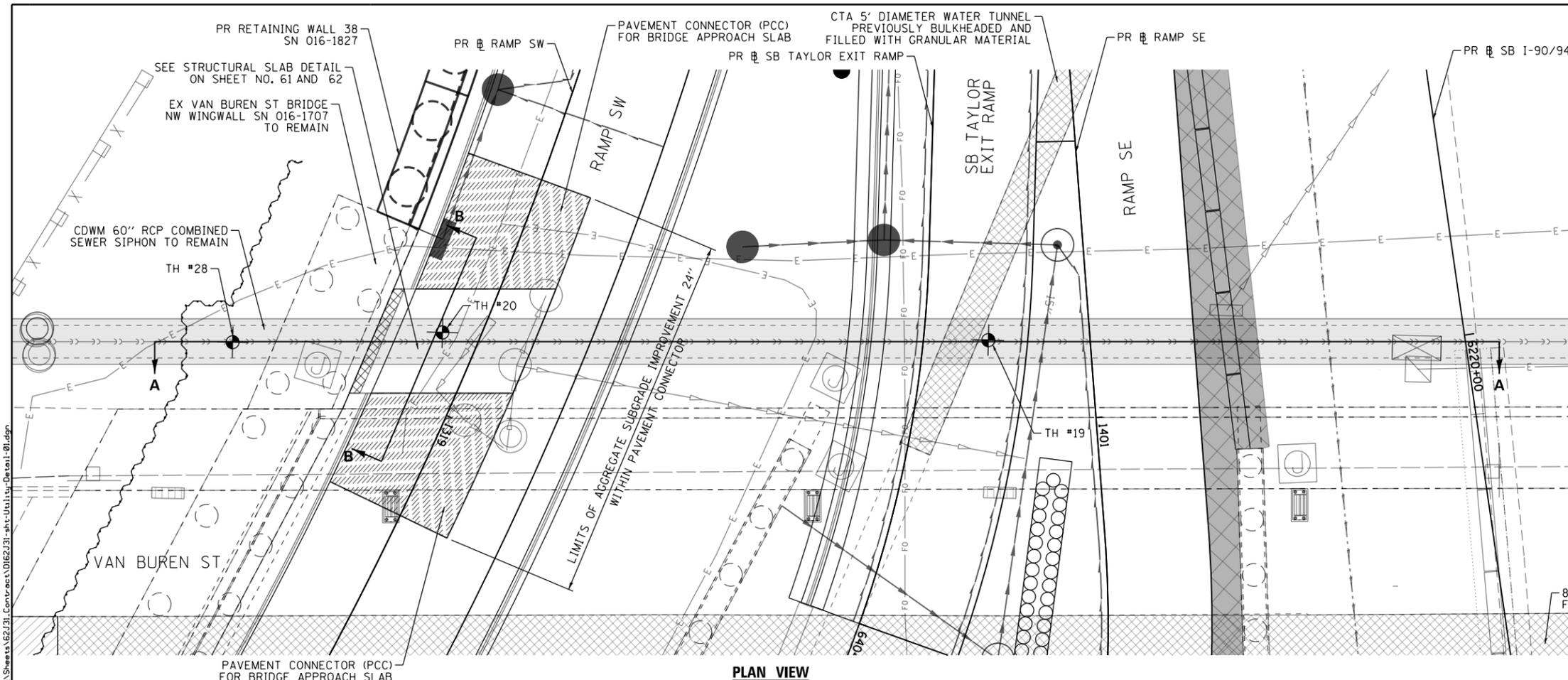
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PLOT DATE = 8/14/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

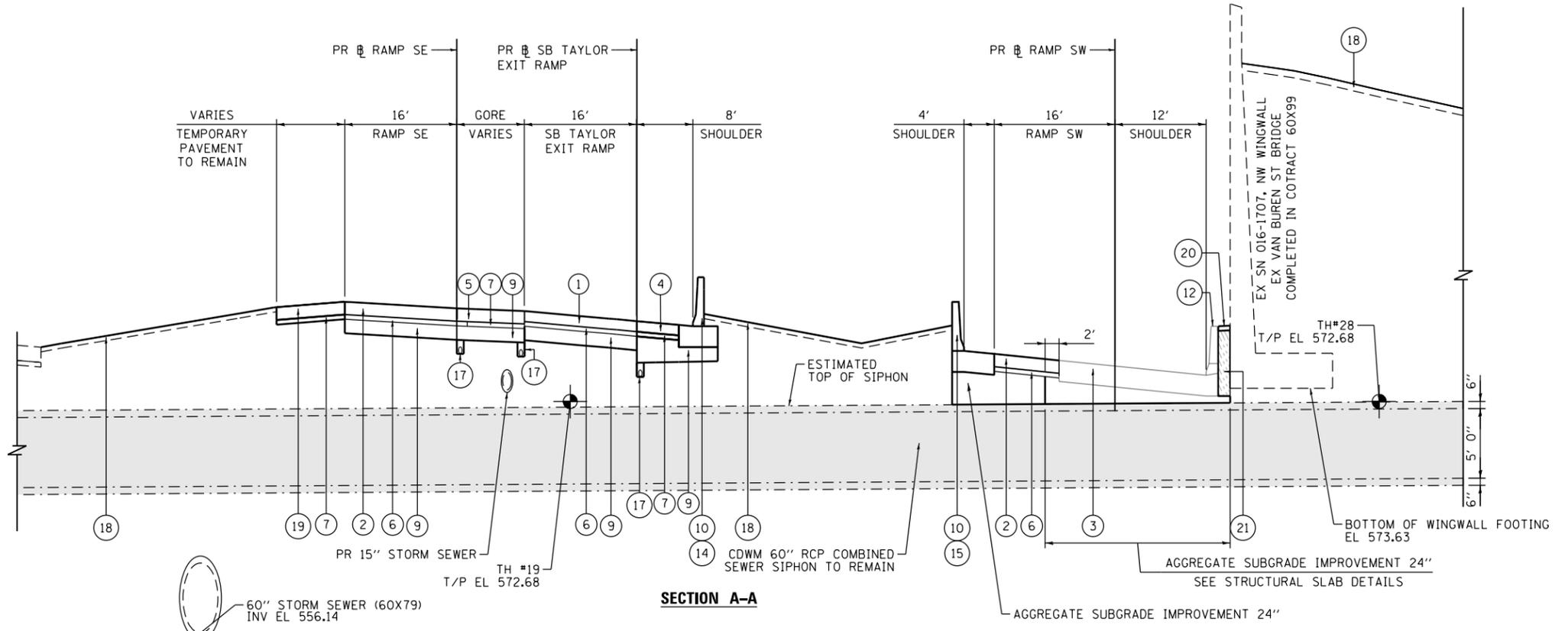
**ROADWAY DETAILS  
GLADYS STREET**

SCALE: 1"=5' SHEET 1 OF 1 SHEETS STA. 1315+00.47 TO STA. 1319+75.65

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	58
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



PLAN VIEW



SECTION A-A

**PROPOSED**

- ① PORTLAND CEMENT CONCRETE PAVEMENT 9" (JOINTED)
- ② PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- ③ STRUCTURAL SLAB (SEE STRUCTURAL SLAB DETAILS)
- ④ PORTLAND CEMENT CONCRETE SHOULDERS 9"
- ⑤ PORTLAND CEMENT CONCRETE SHOULDERS 11"
- ⑥ STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- ⑦ SUBBASE GRANULAR MATERIAL, TYPE B 4"
- ⑧ OMITTED
- ⑨ AGGREGATE SUBGRADE IMPROVEMENT 12"
- ⑩ SEE SHEET NO. 24 FOR MORE INFORMATION
- ⑪ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- ⑫ CONCRETE BARRIER WALL (SPECIAL NO. 1)
- ⑬ PARAPET (SEE STRUCTURAL SLAB DETAILS)
- ⑭ CONCRETE BARRIER BASE (SPECIAL NO. 1)
- ⑮ CONCRETE BARRIER BASE (SPECIAL NO. 2)
- ⑯ CONCRETE BARRIER BASE (SPECIAL NO. 4)
- ⑰ PIPE UNDERDRAINS, TYPE 2, 6" (SEE DRAINAGE PLANS)
- ⑱ TOPSOIL FURNISH AND PLACE, 4" AND SEEDING OR SODDING
- ⑲ TEMPORARY PAVEMENT
- ⑳ CONCRETE MEDIAN SURFACE, 4 INCH
- ㉑ POROUS GRANULAR EMBANKMENT

\* FOR SECTION B-B, SEE SHEET 60

**LEGEND:**

EXISTING UTILITY



D162J31-sht-Utility-Detail-01.dgn  
 USER NAME = ChiuA  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - MSC  
 DRAWN - APA  
 CHECKED - MJE  
 DATE - 8/16/2019

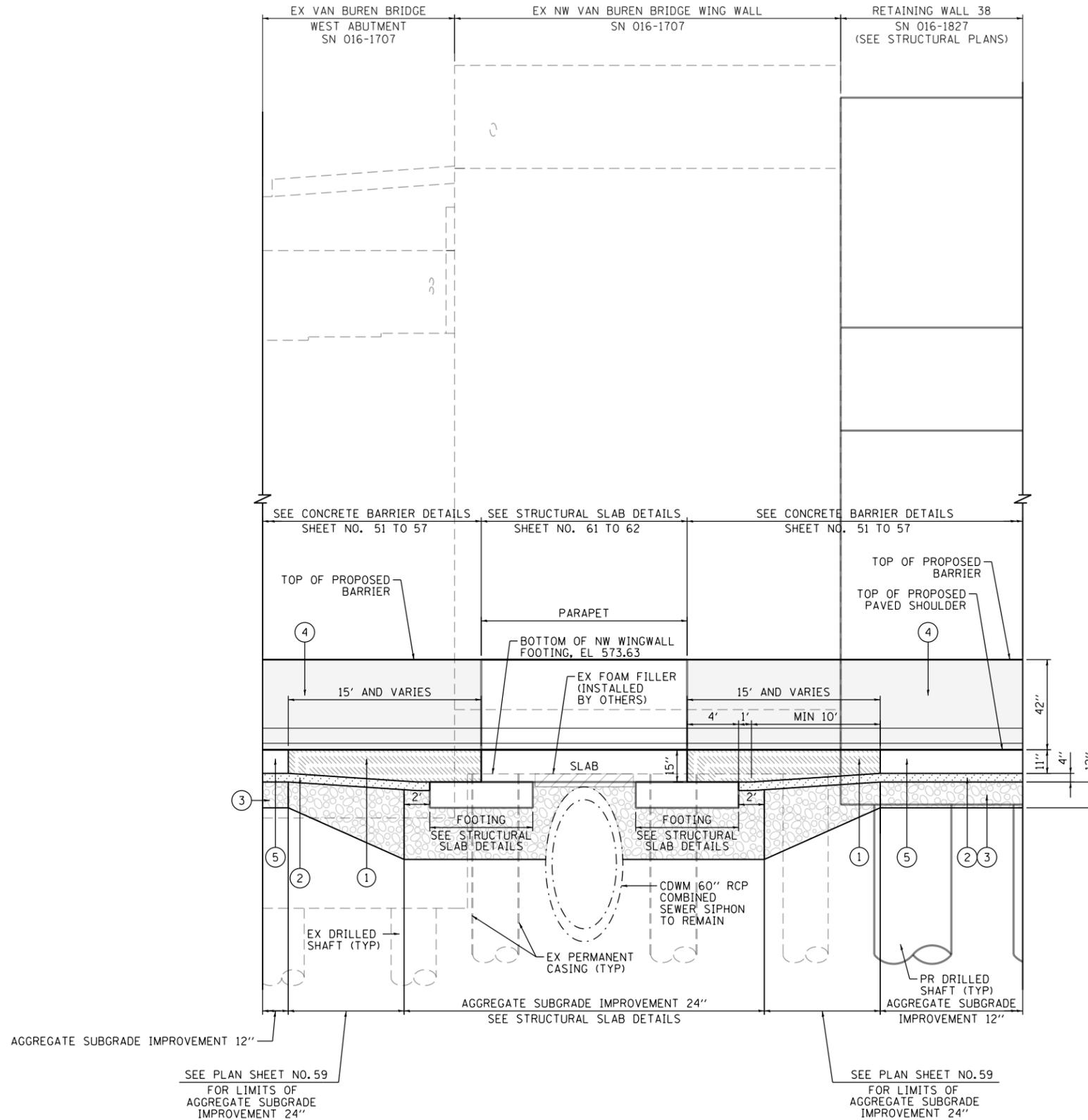
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT DETAIL AT VAN BUREN SIPHON  
 ALONG RAMP SW

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	59
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**PROPOSED**

- ① PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB
- ② SUBBASE GRANULAR MATERIAL, TYPE B 4''
- ③ AGGREGATE SUBGRADE IMPROVEMENT 12''  
SEE SHEET NO. 26 FOR MORE INFORMATION
- ④ CONCRETE BARRIER WALL (SPECIAL NO. 1)
- ⑤ CONCRETE BARRIER BASE (SPECIAL NO. 4)

**LEGEND**

- PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB
- SUBBASE GRANULAR MATERIAL, TYPE B 4''
- AGGREGATE SUBGRADE IMPROVEMENT 12'' OR 24''

**SECTION B-B**



D162J31-sht-Utility-Detail-02.dgn  
 USER NAME = ChiuA  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - MSC  
 DRAWN - APA  
 CHECKED - MJE  
 DATE - 8/16/2019

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

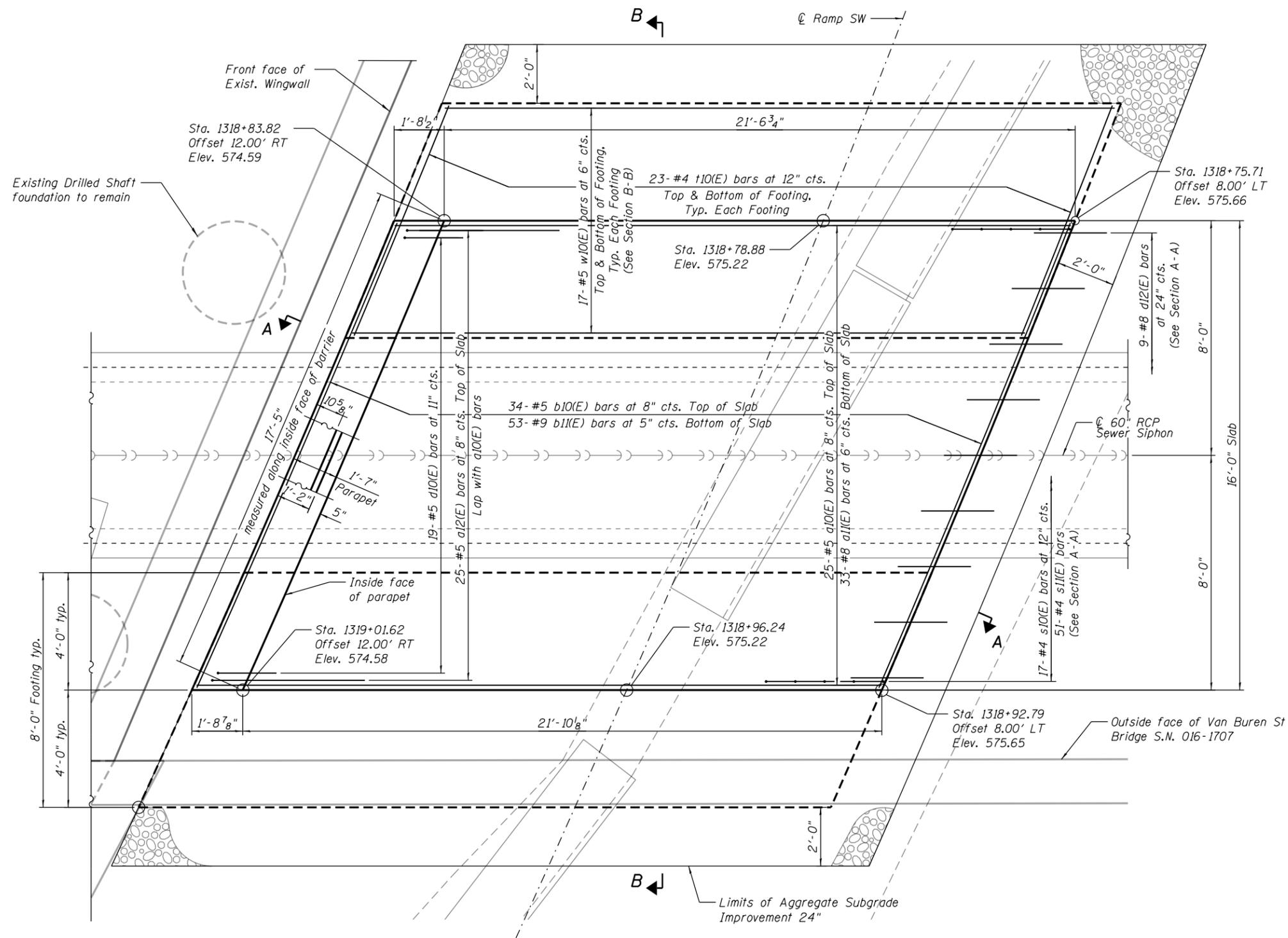
**PAVEMENT DETAIL AT VAN BUREN SIPHON**  
**ALONG RAMP SW**

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	60
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a10(E)	25	#5	23'-3"	—
a11(E)	33	#8	23'-3"	—
a12(E)	25	#5	7'-4"	—
b10(E)	34	#5	17'-0"	—
b11(E)	53	#9	17'-0"	—
d10(E)	19	#5	7'-8"	⌒
d11(E)	19	#5	6'-10"	⌒
d12(E)	9	#8	2'-6"	—
e10(E)	8	#4	17'-1"	—
e11(E)	1	#8	17'-1"	—
s10(E)	17	#4	10'-5"	□
s11(E)	51	#4	2'-0"	⌒
t10(E)	92	#4	8'-3"	—
w10(E)	68	#5	23'-4"	—
Reinforcement Bars, Epoxy Coated		Pound	9,350	
Concrete Superstructure		Cu. Yd.	21	
Concrete Structure		Cu. Yd.	15	
Bridge Deck grooving (Longitudinal)		Sq. Yd.	43	
Protective Coat		Sq. Yd.	51	
Aggregate Subgrade Improvement 24"		Sq. Yd.	90	
Geotechnical Fabric for Ground Stabilization		Sq. Yd.	75	
Geotechnical Reinforcement		Sq. Yd.	75	



**PLAN VIEW**

**DESIGN SPECIFICATIONS**

2017 AASHTO LRFD Bridge Design Specifications, 8th Edition

**LOADING HL-93**

Allow 50#/sq. ft. for future wearing surface.

**DESIGN STRESSES**

FIELD UNITS

$f'_c = 3,500$  psi  
 $f'_c = 4,000$  psi (Superstructure)  
 $f_y = 60,000$  psi (Reinforcement)

**NOTES:**

- Parapet concrete shall be paid for as Concrete Superstructure.
- Footing concrete shall be paid for as Concrete Structures.
- The slab footing maximum applied service bearing pressure ( $Q_{max}$ ) = 2.0 ksf.
- Cost of excavation for the footing included with Concrete Structures.
- Stations and offsets are measured along  $\phi$  Ramp SW.
- The Contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge and other load applied will not have detrimental effect on the adjacent utilities and structure foundations. See Contract Special provisions for the details.
- The Contractor shall field verify location of existing utilities prior to construction. The Contractor shall take precautions not to damage existing utilities. Any damage shall be repaired by the Contractor at no additional cost.
- For Section A-A and B-B see sheet S1-02.
- Field cut a10(E), a11(E) and w10(E) bars to fit.

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 USER NAME = bhatta  
 PLOT SCALE = 4x8 "1" / in.  
 PLOT DATE = 8/14/2019

DESIGNED - MCC  
 DRAWN - GF  
 CHECKED - ATB  
 DATE - 8/16/2019

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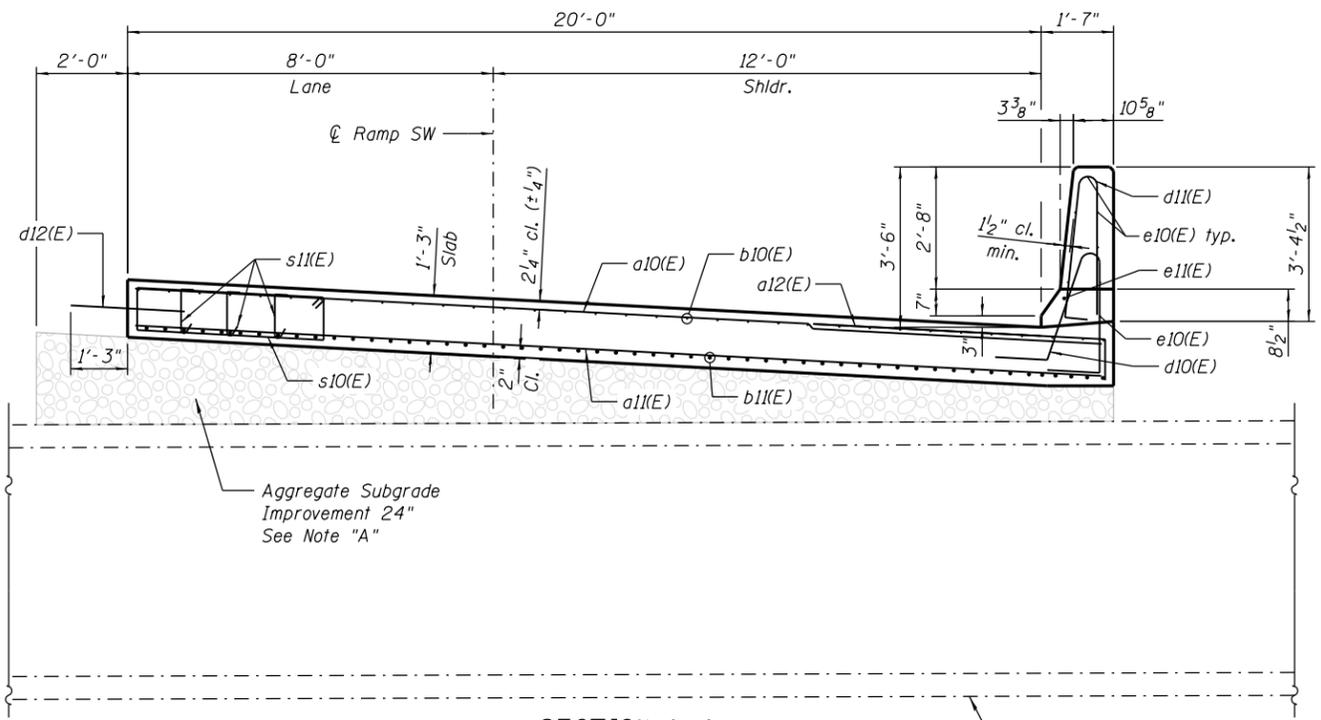
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL SLAB OVER SIPHON I  
 RAMP SW**

SCALE: SHEET S1-01 OF S1-02 SHEETS STA. TO STA.

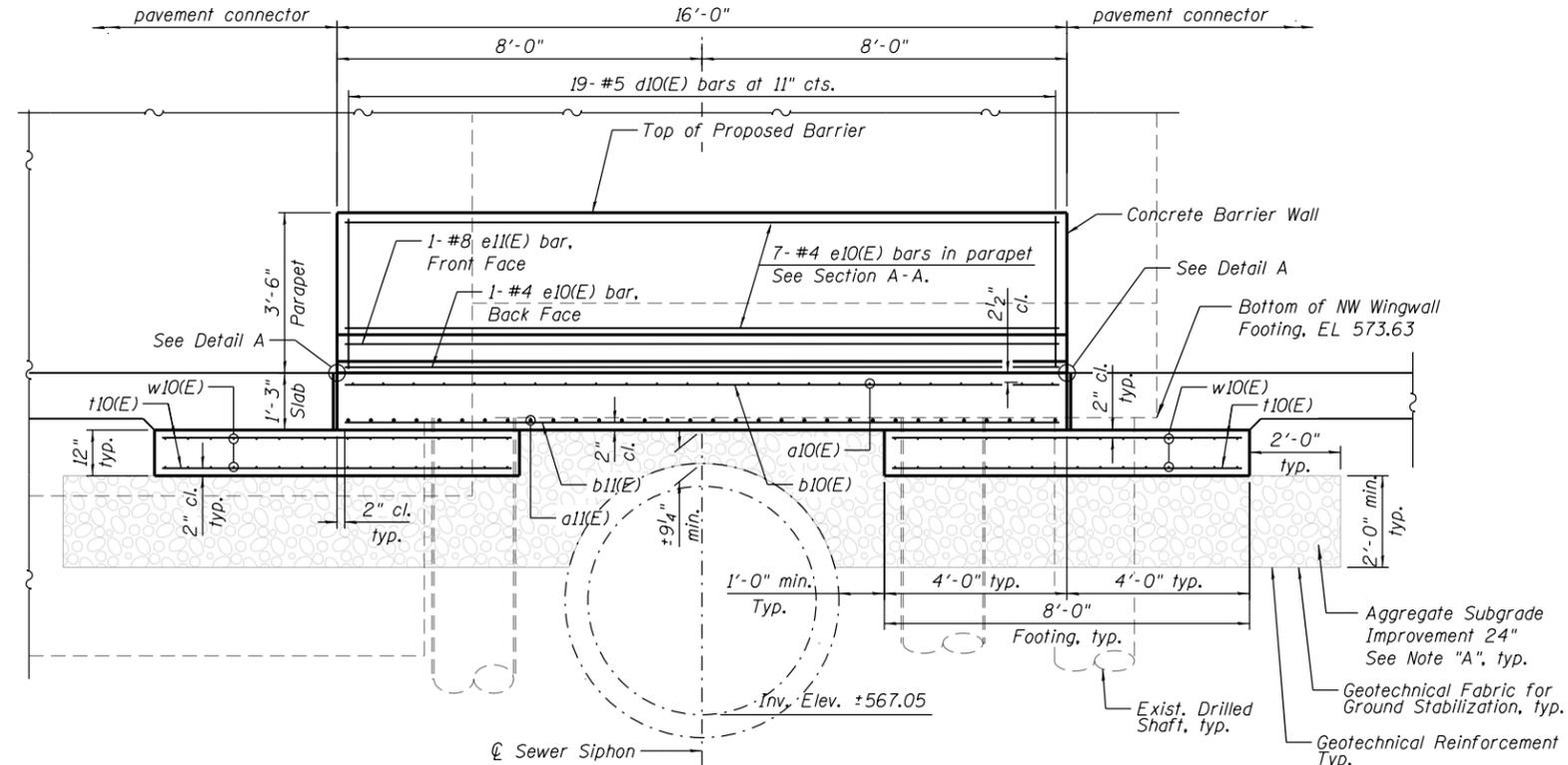
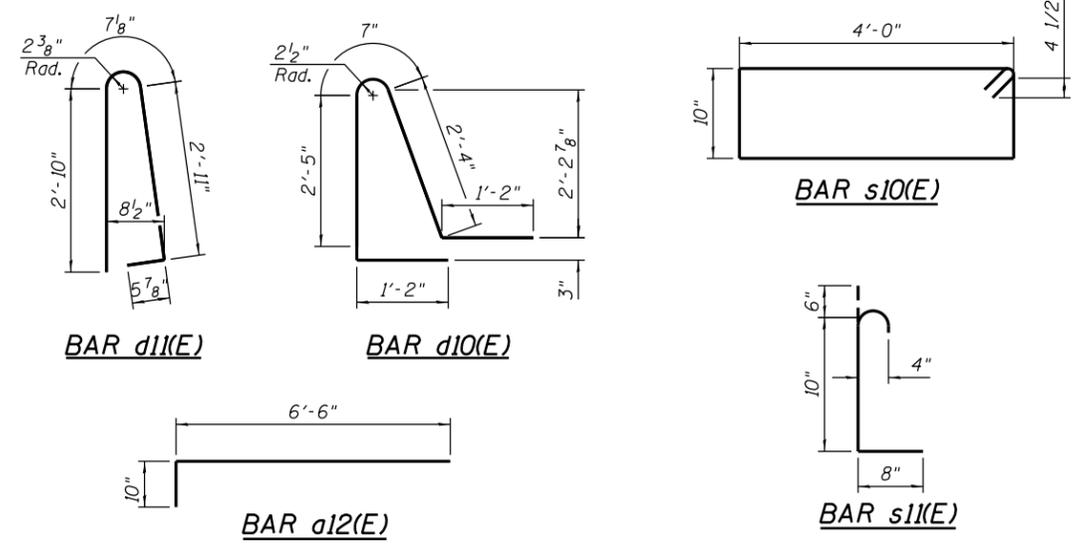
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	61
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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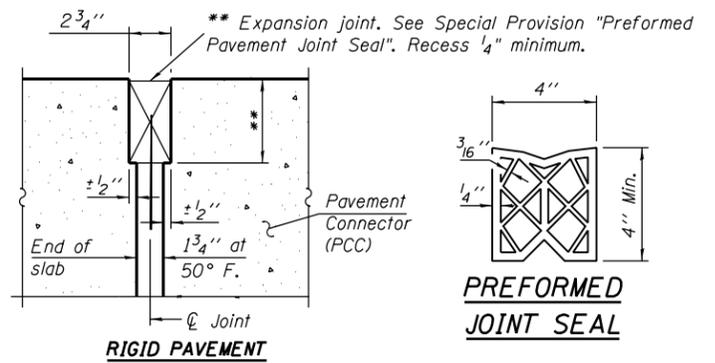
**SECTION A-A**  
 (Looking Upstation)  
 Dimensions shown are measured perpendicular to  $\phi$  Ramp SW unless otherwise noted

CDWM 60" RCP Combined Sewer Siphon to remain



**SECTION B-B**

**NOTE "A":**  
 Aggregate Subgrade Improvement 24" shall be per Special Provision Aggregate Subgrade Improvement (D-1). Aggregate lifts shall not be compacted from the bottom of the footing elevation to the bottom of slab elevation. Contractor shall exercise extreme caution while placing aggregate subgrade improvement around and on top of existing siphon pipe. Any damage shall be repaired by the Contractor at no additional cost.



**DETAIL A** \*\* Cost included with Concrete Superstructure.

**NOTES:**  
 1. Work this sheet with sheet S1-01.



D162J31-sht-StructureSlab-Detail-02.dgn	DESIGNED - ATB	REVISED -
USER NAME = bhatta	DRAWN - GF	REVISED -
PLOT SCALE = 4/8" = 1' / in.	CHECKED - ATB	REVISED -
PLOT DATE = 8/14/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL SLAB OVER SIPHON II**  
**RAMP SW**

SCALE: SHEET S1-02 OF S1-02 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	62
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**TRAFFIC CONTROL GENERAL NOTES**

1. THE CONTRACTOR SHALL CONTACT THE DISTRICT OF ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AND THE EXPRESSWAYS TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. CONTRACTOR SHALL REGISTER AN ACCOUNT AT WWW.IDOTLCS.COM AND USE WEBSITE TO REQUEST LANE CLOSURES AND COORDINATE AND STAGE CHANGES AND LANE CLOSURES.
2. NOTIFY CDOT AND OEMC AT LEAST 72 HOURS BEFORE COMMENCING CONSTRUCTION.
3. UNLESS OTHERWISE NOTED IN THE SPECIAL PROVISIONS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER 28 DAYS PRIOR TO ANY ANTICIPATED CLOSURES.
4. TYPE A LOW INTENSITY FLASHING WARNING LIGHTS SHALL BE USED ON EACH SIGN IN ADVANCE OF THE WORK DURING HOURS OF DARKNESS.
5. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR II BARRICADE USED.
6. WHERE ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
7. EXACT LOCATION OF ALL WARNING SIGNS AND BARRICADES SHALL BE STAKED IN THE FIELD FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION.
8. PRIOR TO START OF CONSTRUCTION ACTIVITIES, ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
9. ITEMS REQUIRED WITHIN TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) AND AS SHOWN ON THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SHEETS WILL REQUIRE CLOSE COORDINATION BETWEEN CONTRACTS. OTHER CONTRACTOR EQUIPMENT AND PERSONNEL WILL REQUIRE ACCESS THROUGH PORTIONS OF WORK ZONES AND CLOSED PORTIONS OF THE EXPRESSWAY AND/OR RAMPS IDENTIFIED ON THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SHEETS. SEE CONTRACTOR COOPERATION SPECIAL PROVISION.
10. A MINIMUM 11' LANE WIDTH SHALL BE MAINTAINED ON ALL INTERSTATE LANES OPEN TO TRAFFIC DURING CONSTRUCTION UNLESS OTHERWISE NOTED.
11. OFFSETS BETWEEN THE EDGE OF TRAVEL LANE AND THE FACE OF TEMPORARY CONCRETE BARRIER ARE ASSUMED TO BE 1 FT UNLESS DESIGNATED OTHERWISE.
12. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL BUILDING ACCESS, COORDINATE WITH BUILDING OWNERS AND LOCAL AUTHORITIES AND PROVIDE FULL ACCESS TO BUSINESSES OR PROPERTIES DURING THEIR NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND APPLICABLE CODE REQUIREMENTS.
13. THE CONTRACTOR SHALL MAINTAIN TRAFFIC ON ALL STREETS EXCEPT WHERE NOTED ON THE PLANS AND PROHIBIT PARKING WITHIN FIFTY (50) FEET OF THE CONSTRUCTION AREA AT ALL TIMES.
14. PROVIDE CONTINUOUS TEMPORARY ACCESS TO ALL SIDE STREETS, ALLEYS, DRIVEWAYS, AND PARKING LOTS UNLESS SPECIFICALLY IDENTIFIED ON THE PLANS FOR TEMPORARY CLOSURE. LOTS WITH MORE THAN ONE DRIVEWAY MUST BE STAGED TO KEEP AT LEAST ONE DRIVEWAY OPEN AT ALL TIMES.
15. MAINTAIN ACCESS TO FIRE HYDRANTS, BUILDING STANDPIPES AND OTHER EMERGENCY FACILITIES WITHIN THE CONSTRUCTION ZONE.
16. SIGNS W21-1 AND W20-7 SHALL BE TAKEN DOWN OR COVERED WHEN THE WORKERS ARE NOT PRESENT.
17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ACCESS RESTRICTIONS TO THE SITE AS MAY REQUIRED BY THE ENGINEER. IN AREAS WHERE SILT FENCE IS NOT INSTALLED, ORANGE CONSTRUCTION FENCING MAY BE REQUIRED TO RESTRICT ACCESS TO WORK ZONES. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE NEED FOR ACCESS RESTRICTIONS AND THEIR CONFIGURATION. ORANGE CONSTRUCTION FENCING (IF NECESSARY) WILL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).

18. A 24" DEFLECTION AREA IS REQUIRED FROM THE BACK SIDE OF THE TEMPORARY BARRIER WALL TO ANY OBSTRUCTION OR DROP OFF IN THE WORK ZONE. IF THIS 24" DEFLECTION AREA CANNOT BE MAINTAINED, THE TEMPORARY CONCRETE BARRIER WALL SHALL BE ANCHORED TO THE PAVEMENT (EXCLUDING NEW BRIDGE DECKS) IN ACCORDANCE WITH THE IDOT SAFETY ENGINEERING POLICY MEMORANDUM 4-15. THIS WORK SHALL BE PAID FOR AS PINNING TEMPORARY CONCRETE BARRIER, EXCEPT THE COST OF ANCHORING TO EXISTING AND PROPOSED BRIDGE DECKS ARE INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER. SEE STRUCTURAL PLANS (WHEN APPLICABLE) FOR DETAILS OF TEMPORARY CONCRETE BARRIER ANCHOR DEVICES.
19. PER IDOT SAFETY ENGINEERING POLICY MEMORANDUM 4-15, DROP-OFF DEPTH > 4 IN AND < 12 IN IS PERMITTED FOR LESS THAN 0.5 MILE LENGTH OF DROP OFF EXPOSURE IN WORK ZONE FOR LESS THAN 48 HOUR CLOSURE TIME. LENGTH AND DURATION OF DROP-OFF IN EXCESS OF THESE LIMITS SHALL REQUIRE TEMPORARY LONGITUDINAL CONCRETE BARRIER. ADJACENT WORK SPACES THAT ARE ESSENTIALLY CONTINUOUS IN DROP-OFF EXPOSURE SHOULD BE CONSIDERED AS ONE WORK ZONE.
20. ALL UPSTREAM LEADING ENDS OF TEMPORARY CONCRETE BARRIER WALL SHALL BE FLARED AT A 12:1 TAPER RATE FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH AND 8:1 TAPER FOR SPEEDS LESS THAN 45 MPH UNLESS OTHERWISE NOTED.
21. TYPE II BARRICADE OR DRUM, WITH LIGHTS PER THE LIGHTS ON BARRICADE SPECIAL PROVISION AND CURRENT IDOT STANDARDS, @ 50' C-C ON TAPERS AND RAMPS @ 100' C-C ON TANGENTS.
22. DIRECTIONAL INDICATOR BARRICADE, WITH LIGHTS PER THE LIGHTS ON BARRICADE SPECIAL PROVISION AND CURRENT IDOT STANDARDS, @ 50' C-C ON TAPERS AND RAMPS.
23. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND REPLACE ALL REFLECTORS FROM EXISTING RAISED REFLECTIVE PAVEMENT MARKERS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLANS. THIS WORK SHALL BE PAID FOR AS RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL AND RAISED PAVEMENT MARKER REFLECTOR.
24. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR ONE PLACEMENT AND ONE REPLACEMENT HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT AFTER THE THIRD REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.
25. ADDITIONAL ESTIMATED QUANTITIES FOR VARIOUS MAINTENANCE OF TRAFFIC ITEMS HAVE BEEN INCLUDED IN THE CONTRACT FOR THE PURPOSE OF SUPPLEMENTING MAINTENANCE OF TRAFFIC OPERATIONS AND FOR MAINTENANCE OF DETOUR ROUTES. THESE QUANTITIES ARE NOTED AS ALLOWANCES IN THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN - SCHEDULE. USE OF THESE ADDITIONAL QUANTITIES IS AT THE DISCRETION OF THE ENGINEER.
26. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL) FOR THOSE SIGNS ALONG LOCAL STREETS AND INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) FOR THOSE SIGNS ALONG EXPRESSWAYS AND EXPRESSWAY RAMPS. ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST TRAFFIC CONTROL AND PROTECTION, (SPECIAL) FOR THOSE SIGNS ALONG LOCAL STREETS AND INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) FOR THOSE SIGNS ALONG EXPRESSWAYS.
27. THE CONTRACTOR SHALL ONLY SET UP AND STORE EQUIPMENT DURING CONSTRUCTION AT THE SUGGESTED STAGING AREAS AS SHOWN IN THE PLANS OR AS APPROVED BY THE ENGINEER. THE SUGGESTED STAGING AREAS SHOWN IN THE PLANS, IF ANY, ARE SUBJECT TO FIELD MODIFICATION AS DETERMINED BY THE ENGINEER. THE SUGGESTED STAGING AREAS MAY BE SHARED WITH OTHER ADJACENT CONTRACTS WHICH MAY BE UNDER CONSTRUCTION DURING THE DURATION OF THIS PROJECT. CONTRACTOR COOPERATION IS REQUIRED. ADDITIONALLY, UTILITIES AND UTILITY CONTRACTORS MAY PERFORM ASBESTOS ABATEMENT ON CONTRACTOR REMOVED CONDUITS WITHIN A SECURE AREA PROVIDED WITHIN THE STAGING AREAS.
28. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC AS SOON AS THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.

29. MANHOLES LIDS ON EXPRESSWAYS WITHIN THE PROJECT STAGING LIMITS SHALL BE WELDED DOWN PRIOR TO BEGINNING ANY WORK. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
30. AN ESTIMATED QUANTITY OF 100 TONS OF AGGREGATE FOR TEMPORARY ACCESS HAS BEEN INCLUDED IN THIS CONTRACT FOR THE PURPOSE OF MAINTAINING CONSTRUCTION POINTS OF ACCESS IN THE CIRCLE INTERCHANGE AREA AND FOR MAINTAINING ANY NECESSARY ACCESS TO PRIVATE PROPERTIES AND SIDE STREETS DURING THIS CONTRACT.
31. PINNING TEMPORARY BARRIER WALL IS NOT ALLOWED ON PERMANENT CONCRETE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

**TEMPORARY INFORMATION SIGNING DETAIL**

**SIGN DETAIL**  
1:100

SIGN NUMBER	SB-TIS-01
WIDTH x HGHT.	18'-0" x 16'-6"
BORDER WIDTH	2"
CORNER RADIUS	12"
MOUNTING	Overhead
BACKGROUND	TYPE: Z7 Retro Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Z7 Retro Reflective COLOR: Black/Black

SYMBOL	ROT	X	Y	WID	HT
M1_1	0	20.3	121.5	45	36
ARDOWN	0	88.1	5.2	30	20.6

Panel Style: guide\_exp\_advance.b.ssi  
Dimensions are in inches, tenths  
Letter locations are panel edge to lower left corner

LETTER POSITIONS (X)											LENGTH	SERIES/SIZE				
E	X	I	T	S								E				
83.9	93.3	104	107.8	117.2								41.3	10			
5	1	H	-	J									EM			
137.6	153.4	161.8	177.6	192.8								66.4	15			
W	E	S	T										E			
80.3	98.8	110.1	122.1									50.7	15,12			
E	i	s	e	n	h	o	w	e	r	E	x	p	w	y	EM	
80.3	88.8	92.5	99.3	107	115.2	122.7	129.6	139.2	146.8	150.7	158.7	166.6	175.1	181.8	191.2	8/6
A	u	r	o	r	a											EM
66.7	87	103.3	113.5	129	139.1										82.6	16/12
I	d	a		B		W	e	l	l	s		D	r		EM	
16.4	25	40.2	50.4	66.4	79.1	95.1	115.6	130.9	140	147.5	157.7	173.7	191.8		183.3	16/12
T	a	y	l	o	r		S	t								EM
48.9	63.7	78.3	95.8	103.7	119.1	126.9	142.9	159.1							118.2	16/12
E	X	I	T													E
44.7	55.9	68.8	73.3												37.5	12
O	N	L	Y													E
124.1	137.1	149.8	159.4												47.3	12

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
TRAFFIC CONTROL - GENERAL NOTES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2015-018R	COOK	400	63
CONTRACT NO. 62A77				
ILLINOIS FED. AID PROJECT				

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL SCHEDULE**

SHEET	SO YD	SO YD	SO YD	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	EACH	SO FT	EACH	FOOT	SO FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT
	SUBBASE GRANULAR MATERIAL, TYPE B 4"	PAVEMENT REMOVAL	PROTECTIVE COAT	TEMPORARY PAVEMENT MARKING - LINE 4"	TEMPORARY PAVEMENT MARKING - LINE 6"	PAVEMENT MARKING TAPE, TYPE III 4"	PAVEMENT MARKING TAPE, TYPE III 12"	PAVEMENT MARKING TAPE, TYPE III 24"	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW, TEST LEVEL 3)	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, WIDE, TEST LEVEL 3)	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW, TEST LEVEL 3)	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, TEST LEVEL 3)	SIGN PANEL OVERLAY	BARRIER WALL, REFLECTORS, TYPE C	TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)	PAVEMENT MARKING REMOVAL - WATER BLASTING	TEMPORARY EPOXY PAVEMENT MARKING - LINE 4"	TEMPORARY EPOXY PAVEMENT MARKING - LINE 5"	TEMPORARY EPOXY PAVEMENT MARKING - LINE 8"	TEMPORARY EPOXY PAVEMENT MARKING - LINE 12"	TEMPORARY CONCRETE BARRIER (SPECIAL)		
STAGE 1 - 01	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STAGE 1 - 02	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
STAGE 1 - 03	76	0	76	0	963	0	0	0	963	0	2	0	0	0	77	0	1441	700	0	1148	122	0	0	0	
STAGE 1 - 04	1356	0	1356	0	700	0	0	0	700	0	0	0	0	0	56	0	683	1744	0	150	12	0	0	0	
STAGE 2 - 01	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
STAGE 2 - 02	0	0	0	0	0	0	0	0	0	0	0	0	0	56	0	0	0	0	0	0	0	0	0	0	
STAGE 2 - 03	749	0	1981	0	1813	0	0	0	600	413	0	0	1	0	88	145	800	3300	2106	1060	2791	31	0	0	
STAGE 2 - 04	127	0	136	0	1175	0	0	0	650	525	0	2	0	0	94	0	1701	2796	640	910	21	0	0		
STAGE 3 - 01	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
STAGE 3 - 02	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
STAGE 3 - 03	0	0	0	0	613	0	0	0	0	613	0	0	1	0	49	0	1108	784	0	1413	94	0	0	0	
STAGE 3 - 04	0	699	0	0	625	0	0	0	0	575	0	0	1	0	50	50	960	1771	0	132	5	0	0	0	
RAMP SE / SB TAYLOR EXIT RAMP - 01	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	936	0	0	1682	299	0	0	0	
RAMP SE / SB TAYLOR EXIT RAMP - 02	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	658	1695	0	1174	97	0	0	0	
JACKSON BOULEVARD - WEST	0	0	0	0	75	0	0	0	0	0	0	0	0	0	28	75	0	0	0	0	0	0	275	0	
JACKSON BOULEVARD - EAST	0	0	0	52	75	221	38	12	0	0	0	0	0	0	6	75	0	0	0	0	0	0	0	0	
DETOUR - JACKSON BOULEVARD (ROADWAY)	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
DETOUR - JACKSON BOULEVARD (SOUTHBOUND I-90/94 EXIT)	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
DETOUR - JACKSON BOULEVARD (PEDESTRIAN)	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
DETOUR - JACKSON BOULEVARD (BICYCLE)	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
TOTAL	2308	699	3549	52	6038	221	38	12	2913	2125	2	2	2	1	144	505	1000	10787	11596	1700	9400	681	275	0	
ALLOWANCE	347	105	533	8	906	34	6	2	437.5	325	1	1	1	1	22	76	150	1619	1740	255	1411	103	42	0	
TOTALS WITH ALLOWANCE	2655	804	4082	60	6944	255	44	14	3350.0	2450.0	3	3	3	2	166	581	1150	12406	13336	1955	10811	784	317	0	

SHEET	PINNING TEMPORARY CONCRETE BARRIER	TEMPORARY INFORMATION SIGNING	TEMPORARY PAVEMENT	AGGREGATE FOR TEMPORARY ACCESS	SUBBASE GRANULAR MATERIAL, TYPE B 8"	REMOVE SIGN PANEL - TYPE 3	IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
	EACH	SO FT	SO YD	TON	SO YD	SO FT	EACH
STAGE 1 - 01	0	0	0	0	0	0	0
STAGE 1 - 02	0	0	0	0	0	0	0
STAGE 1 - 03	195	0	76	0	0	0	0
STAGE 1 - 04	96	0	1356	0	0	0	0
STAGE 2 - 01	0	0	0	0	0	0	0
STAGE 2 - 02	0	0	0	0	0	0	0
STAGE 2 - 03	300	297	1981	0	1232	539	1
STAGE 2 - 04	216	0	136	0	9	0	0
STAGE 3 - 01	0	0	0	0	0	0	0
STAGE 3 - 02	0	0	0	0	0	0	0
STAGE 3 - 03	132	0	0	0	0	0	0
STAGE 3 - 04	0	0	0	0	0	0	1
RAMP SE / SB TAYLOR EXIT RAMP - 01	0	0	0	0	0	0	0
RAMP SE / SB TAYLOR EXIT RAMP - 02	0	0	0	0	0	0	0
JACKSON BOULEVARD - WEST	0	78	0	0	0	0	0
JACKSON BOULEVARD - EAST	0	0	0	0	0	0	0
DETOUR - JACKSON BOULEVARD (ROADWAY)	0	45	0	0	0	0	0
DETOUR - JACKSON BOULEVARD (SOUTHBOUND I-90/94 EXIT)	0	27	0	0	0	0	0
DETOUR - JACKSON BOULEVARD (PEDESTRIAN)	0	33	0	0	0	0	0
DETOUR - JACKSON BOULEVARD (BICYCLE)	0	33	0	0	0	0	0
TOTAL	939	513	3549	0	1241	539	2
ALLOWANCE	141	77	533	0	187	81	1
TOTALS WITH ALLOWANCE	1080	590	4082	100	1428	620	3

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 PLOT DATE = 8/16/2019

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 CHECKED - MJE  
 DATE - 8/16/2019

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND  
 TRAFFIC CONTROL - SCHEDULE**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	64
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

## STAGING NARRATIVE

DUE TO WORK PERFORMED BY OTHERS, SEQUENCING MAY REQUIRE ALTERATION AS DEFINED BY THE AVAILABLE WORK AREAS AND SEQUENCING REQUIREMENTS SPECIAL PROVISION.

### STAGE 1

#### MAINTENANCE OF TRAFFIC ALONG MAINLINE I-90/94

- NB I-90/94
  - o TRAFFIC MAINTAINED BY OTHERS. COORDINATE WITH CONTRACTS 60X79 AND 60X93 AS NECESSARY.
- SB I-90/94
  - o TRAFFIC MAINTAINED BY CONTRACT 60X93 PRIOR TO INSTALLATION OF SIGNING AND TRAFFIC CONTROL DEVICES UNDER THIS CONTRACT. PLACE PROPOSED STRIPING AND TEMPORARY CONCRETE BARRIER WITHIN THE GORE AREA BETWEEN SOUTHBOUND I-90/94 AND RAMP SW. INSTALL TEMPORARY CONCRETE BARRIER ALONG RIGHT SIDE OF SOUTHBOUND I-90/94 AND MEET EXISTING TEMPORARY CONCRETE BARRIER PREVIOUSLY INSTALLED IN CONTRACT 60X93. CONTINUE TO COORDINATE ACCESS FOR CONTRACTS 60X79 AND 60X93.

#### MAINTENANCE OF TRAFFIC ON MAINLINE RAMPS

- RAMP SE
  - o MAINTAIN RAMP CLOSURE CONTINUED FROM CONTRACT 60X93.
- SB TAYLOR EXIT RAMP
  - o MAINTAIN RAMP CLOSURE UNDER CONTRACT 60X93.
- RAMP SW
  - o INSTALL TEMPORARY CONCRETE BARRIER ALONG LEFT SIDE OF RAMP. NARROW RAMP TO 12' SHIFTED TO WEST AS SHOWN ON THE PLANS.
- SOUTHBOUND I-90/94 EXIT TO ADAMS STREET
  - o RAMP TO REMAIN OPEN AND WITHOUT RESTRICTIONS.
- SOUTHBOUND I-90/94 EXIT TO JACKSON BOULEVARD
  - o RAMP TO REMAIN OPEN AND WITHOUT RESTRICTIONS PRIOR TO CLOSURE AFTER APPROVAL BY THE ENGINEER. RAMP SHALL NOT BE CLOSED PRIOR TO THE CLOSURE OF JACKSON BOULEVARD BETWEEN HALSTED STREET AND DES PLAINES STREET.
  - o INSTALL DETOUR AS SHOWN ON THE PLANS PRIOR TO RAMP CLOSURE.
- JACKSON BOULEVARD ENTRANCE TO NORTHBOUND I-90/94
  - o RAMP TO REMAIN OPEN AND WITHOUT RESTRICTIONS PRIOR TO CLOSURE AFTER APPROVAL BY THE ENGINEER. RAMP SHALL NOT BE CLOSED PRIOR TO THE CLOSURE OF JACKSON BOULEVARD BETWEEN HALSTED STREET AND DES PLAINES STREET.
  - o INSTALL DETOUR AS SHOWN ON THE PLANS PRIOR TO RAMP CLOSURE.

#### MAINTENANCE OF TRAFFIC ON LOCAL STREETS

- JACKSON BOULEVARD
  - o MAINTAIN FULL ACCESS BETWEEN HALSTED STREET AND DES PLAINES STREET IN ADVANCE OF FULL CLOSURE.
  - o ONCE CLOSURE IS APPROVED BY THE ENGINEER, INSTALL DETOUR AS SHOWN IN THE PLANS PRIOR TO STREET CLOSURE. INSTALL STRIPING, SIGNING AND TRAFFIC CONTROL DEVICES FOR LOCAL ACCESS AS SHOWN IN THE PLANS.
  - o INSTALL PEDESTRIAN DETOUR AND SIDEWALK CLOSURE DEVICES FOR PEDESTRIAN ACCESS.

#### CONSTRUCTION TO BE COMPLETED DURING STAGE 1

- BEGIN AND COMPLETE INSTALLATION OF PROTECTIVE SHIELDING FOR BRIDGE REMOVAL IN ADVANCE OF BRIDGE DEMOLITION WORK.
- PRIOR TO BEGINNING THE DEMOLITION OF THE EXISTING JACKSON BOULEVARD BRIDGE, REMOVE THE EXISTING JACKSON BOULEVARD UNDERPASS LUMINAIRES.
- INSTALL THE TEMPORARY ITS CABLES AND ASSOCIATED TEMPORARY WOOD POLES BETWEEN JACKSON BOULEVARD AND LAKE STREET. REFER TO ITS-01 FOR ADDITIONAL SEQUENCE AND STAGING NOTES. THE TEMPORARY ITS CABLES MUST BE ACTIVE PRIOR TO THE REMOVAL OF EXISTING CABLES AND CONDUIT ATTACHED TO THE EXISTING JACKSON BOULEVARD BRIDGE STRUCTURE.
- PRIOR TO DE-ENERGIZING AND REMOVING EXISTING LIGHT TOWER 5 UAB5, INSTALL TEMPORARY LIGHTING UNITS 4 UAB5 AND 4 UAB6 INCLUDING ASSOCIATED TEMPORARY POWER FEEDS.
- REMOVE EXISTING LIGHT TOWER 5 UAB5 ONCE TEMPORARY LIGHTING UNITS 4 UAB5 AND 4 UAB6 ARE ENERGIZED.
- BEGIN CONSTRUCTION OF RETAINING WALL 38 AND THE DRILLED SHAFTS OF THE PROPOSED SOUTH WINGWALL OF THE JACKSON BOULEVARD WEST ABUTMENT.
- CONSTRUCT THE SOUTH PORTION OF STORM SEWER P1-01.
- PERFORM EXISTING PAVEMENT REMOVAL AND CONSTRUCT PORTIONS OF TEMPORARY AND PERMANENT PAVEMENT.

- BEGIN DEMOLITION OF THE EXISTING JACKSON BOULEVARD BRIDGE INCLUDING THE JACKSON BOULEVARD ENTRANCE RAMP STRUCTURE AND RETAINED FILL STRUCTURE AS THE STAGING ALLOWS AND IN COORDINATION WITH OTHER ADJACENT CONTRACTS.
- BEGIN FREIGHT TUNNEL FILLING WORK AS SHOWN IN THE PLANS.

### STAGE 2

#### MAINTENANCE OF TRAFFIC ALONG MAINLINE I-90/94

- NB I-90/94
  - o TRAFFIC FOR THIS STAGE IN THE NORTHBOUND DIRECTION WILL BE STRIPED AND MAINTAINED BY THIS CONTRACT. TRAFFIC WILL BE SHIFTED TO THE EAST NORTH OF VAN BUREN STREET, MAINTAINING FOUR THROUGH LANES TO ALLOW FOR JACKSON BOULEVARD PIER AND ENTRANCE RAMP REMOVAL. COORDINATE WITH CONTRACTS 60X79 AND 60X93 AS NECESSARY.
  - o ADDITIONAL ACCESS AND STAGING REQUIRED DURING JACKSON BOULEVARD BRIDGE AND RAMP STRUCTURE REMOVAL SHALL BE COORDINATED WITH CONTRACT 60X79 AND PERFORMED USING STANDARDS AND AS ALLOWED UNDER THE KEEPING THE EXPRESSWAY OPEN TO TRAFFIC SPECIAL PROVISION.
- SB I-90/94
  - o INSTALL TEMPORARY IMPACT ATTENUATOR AND TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENT) ALONG EXISTING BARRIER WALL ALONG EAST SIDE OF SOUTHBOUND I-90/94 AS SHOWN IN THE PLANS TO ALLOW JACKSON BOULEVARD ENTRANCE RAMP REMOVAL TO OCCUR. SHIFT SOUTHBOUND LANES AS SHOWN IN THE PLANS TO ACCOMMODATE TEMPORARY BARRIER (TO REMAIN PERMANENT) INSTALLATION. INSTALL TEMPORARY PAVEMENT MARKINGS AND BARRICADES TO CLOSE OFF EXISTING RAMP SW EXIT FOR THE EXIT DIVERGENCE RELOCATION. RELOCATE TEMPORARY CONCRETE BARRIER ALONG RIGHT SIDE OF SOUTHBOUND I-90/94 SOUTH OF ADAMS STREET. INSTALL TEMPORARY PAVEMENT MARKINGS TO CARRY LANE FOUR SOUTH OF JACKSON BOULEVARD TO CARRY THE EXIT ONLY CONDITION TO RAMP SW. INSTALL TEMPORARY IMPACT ATTENUATOR, WIDE AT GORE TO RAMP SW. CONTINUE TO COORDINATE ACCESS FOR CONTRACTS 60X79 AND 60X93.

#### MAINTENANCE OF TRAFFIC ON MAINLINE RAMPS

- RAMP SE
  - o MAINTAIN RAMP CLOSURE CONTINUED FROM CONTRACT 60X93.
- SB TAYLOR EXIT RAMP
  - o MAINTAIN RAMP CLOSURE UNDER CONTRACT 60X93.
- RAMP SW
  - o SHIFT RAMP EXIT LOCATION TO EAST SIDE OF EXISTING JACKSON BOULEVARD BRIDGE PIER AND ONTO A COMBINATION OF PROPOSED OR TEMPORARY PAVEMENT BUILT IN THE PREVIOUS STAGE AND EXISTING PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER ALONG LEFT SIDE AND RIGHT SIDE OF RAMP. NARROW RAMP TO 12' SHIFTED TO WEST AS SHOWN ON THE PLANS.
- SOUTHBOUND I-90/94 EXIT TO ADAMS STREET
  - o RAMP TO REMAIN OPEN. MAINTAIN STRIPING, SIGNING AND TRAFFIC CONTROL DEVICES FOR THE CLOSURE OF THE SOUTHBOUND I-90/94 EXIT TO JACKSON BOULEVARD.
- SOUTHBOUND I-90/94 EXIT TO JACKSON BOULEVARD
  - o MAINTAIN RAMP CLOSED TO TRAFFIC. MAINTAIN DETOUR AS SHOWN IN THE PLANS.
- JACKSON BOULEVARD ENTRANCE TO NORTHBOUND I-90/94
  - o MAINTAIN RAMP CLOSED TO TRAFFIC. MAINTAIN DETOUR AS SHOWN IN THE PLANS.

#### MAINTENANCE OF TRAFFIC ON LOCAL STREETS

- JACKSON BOULEVARD
  - o MAINTAIN STREET CLOSURE TO THROUGH TRAFFIC BETWEEN HALSTED STREET AND DES PLAINES STREET. MAINTAIN DETOUR AS SHOWN IN THE PLANS. MAINTAIN LOCAL ACCESS AS SHOWN IN THE PLANS.
  - o MAINTAIN PEDESTRIAN DETOUR AND SIDEWALK CLOSURE DEVICES FOR PEDESTRIAN ACCESS.

#### CONSTRUCTION TO BE COMPLETED DURING STAGE 2

- MAINTAIN ACCESS FOR CONTRACTS 60X79 AND 60X93
- CONTINUE AND COMPLETE CONSTRUCTION OF RETAINING WALL 38 AND THE DRILLED SHAFTS OF THE PROPOSED SOUTH WINGWALL OF THE JACKSON BOULEVARD WEST ABUTMENT. EXCAVATE ALL EXISTING MATERIAL FOR DRAINAGE AND PAVEMENT IMPROVEMENTS.
- CONSTRUCT THE NORTH PORTION OF STORM SEWER P1-01.
- PERFORM EXISTING PAVEMENT REMOVAL AND CONSTRUCT PORTIONS OF TEMPORARY AND PERMANENT PAVEMENT.
- CONSTRUCT STRUCTURAL SLAB OVER SIPHON AS DETAILED IN THE PLANS.

- RELOCATE THE TEMPORARY ITS AERIAL CABLES IN CONFLICT WITH CONSTRUCTION OF RETAINING WALL 38. REFER TO ITS-01 FOR ADDITIONAL SEQUENCE AND STAGING NOTES.
- CONTINUE AND COMPLETE DEMOLITION OF THE EXISTING JACKSON BOULEVARD BRIDGE. CONTINUE AND COMPLETE DEMOLITION OF THE JACKSON BOULEVARD ENTRANCE RAMP STRUCTURE AND RETAINED FILL STRUCTURE TO THE LIMITS SHOWN ON THE PLANS.
- PLACE TEMPORARY PAVEMENT ALONG NORTHBOUND I-90/94 AS SHOWN ON THE PLANS IN THE AREA OF THE VACATED EXISTING JACKSON BOULEVARD ENTRANCE RAMP.
- CONTINUE FREIGHT TUNNEL FILLING WORK AS SHOWN IN THE PLANS.
- RELOCATE TEMPORARY POWER FEEDS FOR TEMPORARY LIGHTING UNITS 4 UAB5 AND 4 UAB6 BEHIND THE PROPOSED RETAINING WALL.
- INSTALL PROPOSED LIGHT TOWERS 5 UAB5 AND 5 UAB6 ON RETAINING WALL 38.

### STAGE 3

#### MAINTENANCE OF TRAFFIC ALONG MAINLINE I-90/94

- NB I-90/94
  - o TRAFFIC MAINTAINED BY OTHERS. COORDINATE WITH CONTRACTS 60X79, 60X93 AND 62A76 AS NECESSARY.
- SB I-90/94
  - o RELOCATE TEMPORARY CONCRETE BARRIER ALONG RIGHT SIDE OF SOUTHBOUND I-90/94 SOUTH OF ADAMS STREET AND RELOCATE TEMPORARY IMPACT ATTENUATOR, WIDE AT GORE TO RAMP SW.
  - o CONTINUE TO COORDINATE ACCESS FOR CONTRACTS 60X79 AND 60X93. COORDINATE PLACEMENT OF SIGNAGE AND TRAFFIC CONTROL DEVICES WITH CONTRACT 62A77.

#### MAINTENANCE OF TRAFFIC ON MAINLINE RAMPS

- RAMP SE
  - o MAINTAIN RAMP CLOSURE CONTINUED FROM CONTRACT 60X93.
- SB TAYLOR EXIT RAMP
  - o MAINTAIN RAMP CLOSURE UNDER CONTRACT 60X93.
- RAMP SW
  - o SHIFT RAMP BACK TO THE WEST AS SHOWN ON THE PLANS. SOUTH OF JACKSON BOULEVARD, RAMP SW WILL BE PLACED ON PROPOSED OR TEMPORARY PAVEMENT BUILT IN THE PREVIOUS STAGE. RELOCATE TEMPORARY CONCRETE BARRIER ALONG LEFT SIDE OF RAMP. NARROW RAMP TO 12' AS SHOWN ON THE PLANS.
- SOUTHBOUND I-90/94 EXIT TO ADAMS STREET
  - o RAMP TO REMAIN OPEN UNTIL CLOSURE UNDER CONTRACT 60X94. MAINTAIN STRIPING, SIGNING AND TRAFFIC CONTROL DEVICES FOR THE CLOSURE OF THE SOUTHBOUND I-90/94 EXIT TO JACKSON BOULEVARD.
- SOUTHBOUND I-90/94 EXIT TO JACKSON BOULEVARD
  - o MAINTAIN RAMP CLOSED TO TRAFFIC. MAINTAIN DETOUR AS SHOWN IN THE PLANS.
- JACKSON BOULEVARD ENTRANCE TO NORTHBOUND I-90/94
  - o MAINTAIN RAMP CLOSED TO TRAFFIC. MAINTAIN DETOUR AS SHOWN IN THE PLANS.

#### MAINTENANCE OF TRAFFIC ON LOCAL STREETS

- JACKSON BOULEVARD
  - o MAINTAIN STREET CLOSURE TO THROUGH TRAFFIC BETWEEN HALSTED STREET AND DES PLAINES STREET. MAINTAIN DETOUR AS SHOWN IN THE PLANS. MAINTAIN LOCAL ACCESS AS SHOWN IN THE PLANS.
  - o MAINTAIN PEDESTRIAN DETOUR AND SIDEWALK CLOSURE DEVICES FOR PEDESTRIAN ACCESS.

#### CONSTRUCTION TO BE COMPLETED DURING STAGE 3

- COMPLETE ANY REMAINING CONSTRUCTION OF RETAINING WALL 38 AND THE DRILLED SHAFTS OF THE PROPOSED SOUTH WINGWALL OF THE JACKSON BOULEVARD WEST ABUTMENT.
- PERFORM REMAINING EXISTING PAVEMENT REMOVAL AND CONSTRUCT REMAINING PORTIONS OF TEMPORARY AND PERMANENT PAVEMENT.
- CONTINUE AND COMPLETE FREIGHT TUNNEL FILLING WORK AS SHOWN IN THE PLANS.
- AFTER PROPOSED LIGHT TOWERS 5 UAB5 AND 5 UAB6 ARE INSTALLED AND ENERGIZED, REMOVE TEMPORARY LIGHTING UNITS 4 UAB5 AND 4 UAB6 INCLUDING ASSOCIATED TEMPORARY WOOD POLES AND AERIAL CABLES.
- COMPLETE THE INSTALLATION OF MAINLINE RAMP DETECTION AND COIL CABLES FOR FUTURE CONNECTION.

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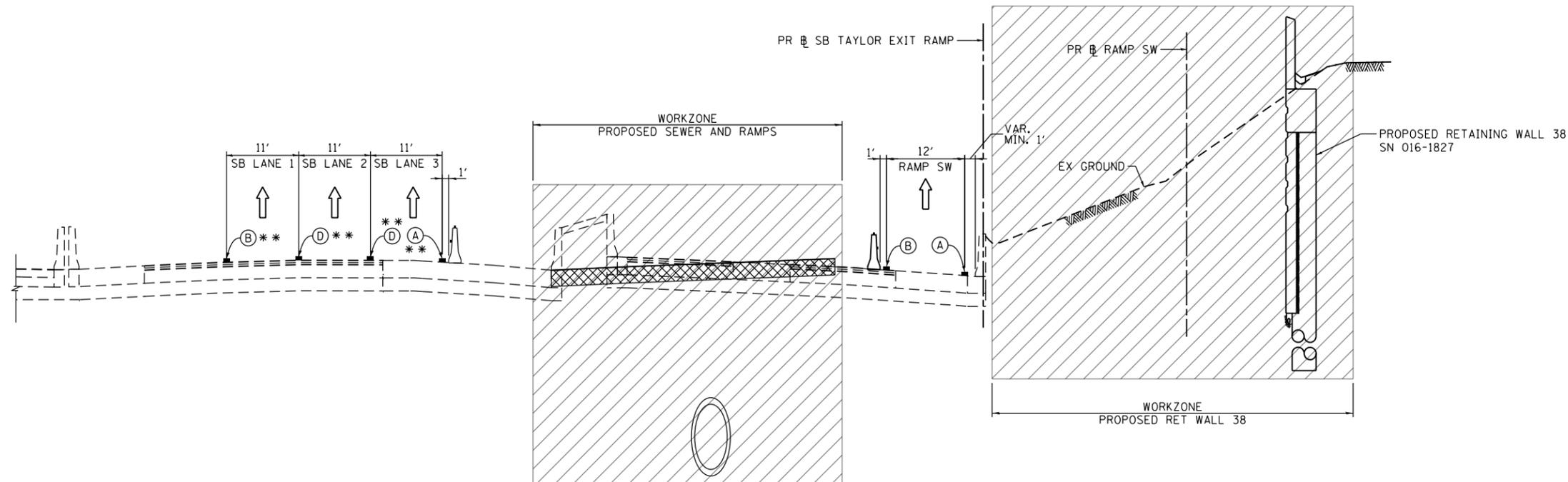
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL  
 NARRATIVE

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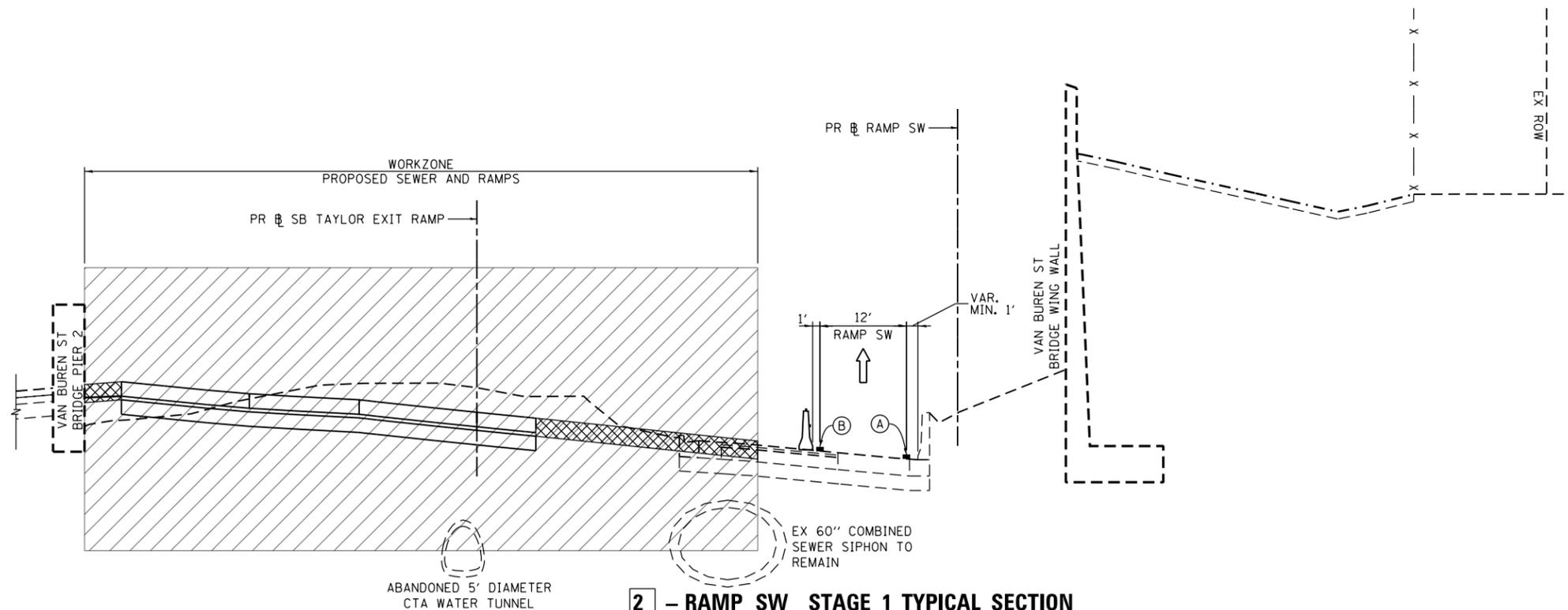
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ILLINOIS FED. AID PROJECT				

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**1 - RAMP SW STAGE 1 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO. 85



**2 - RAMP SW STAGE 1 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO. 85

**PROPOSED TEMP PAVEMENT MARKINGS  
AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001

- \* FROM PREVIOUS STAGE TO REMAIN
- \*\* FROM CONTRACT 60X93 TO REMAIN



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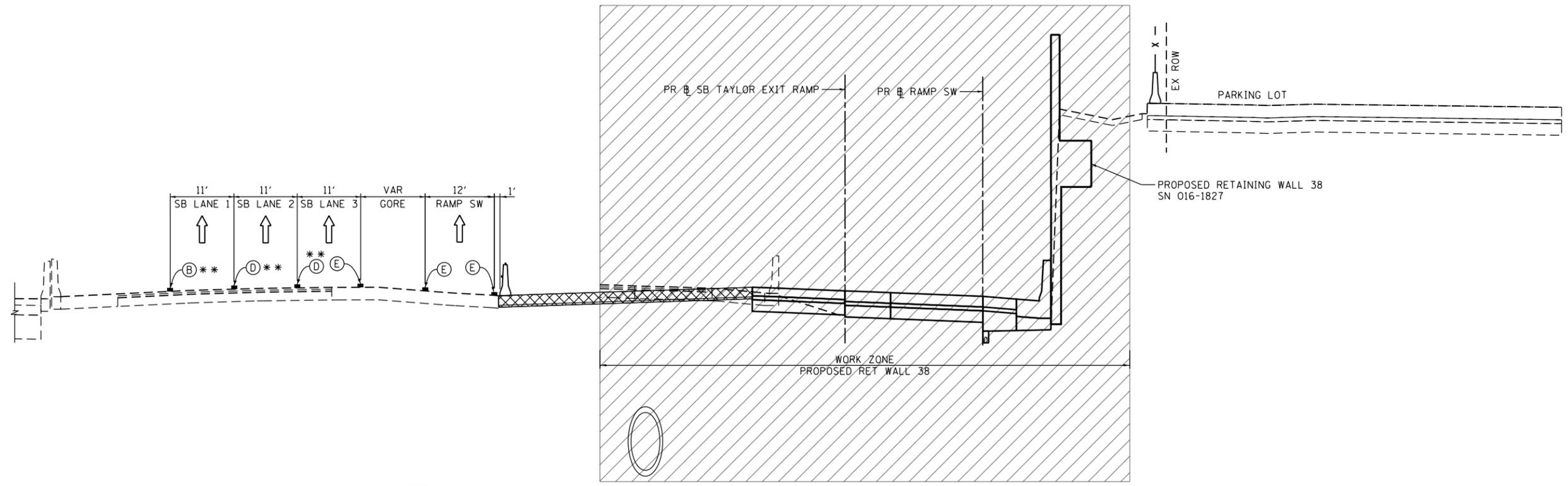
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS**  
**TYPICAL SECTIONS**  
**RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP**

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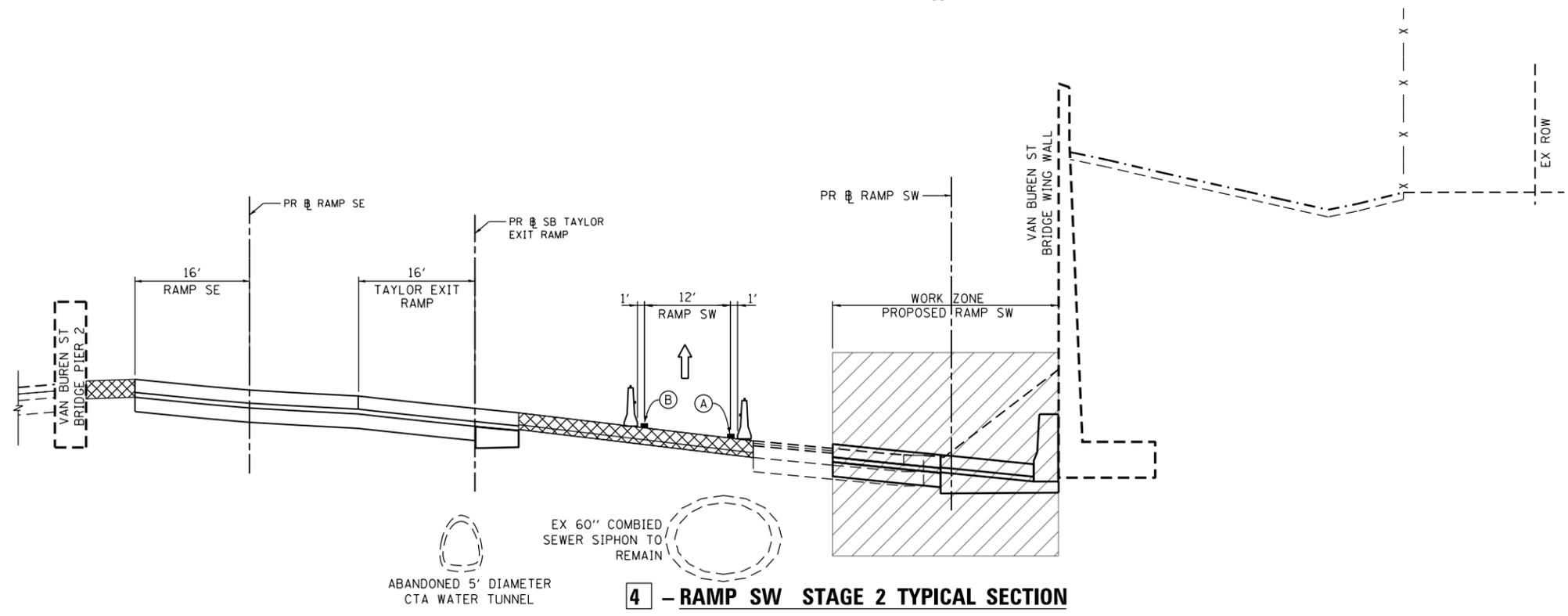
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ILLINOIS FED. AID PROJECT				

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**3 - RAMP SW STAGE 2 TYPICAL SECTION (LOOKING SOUTH)**

SCALE: NONE  
 SEE SHEET NO. 88



**4 - RAMP SW STAGE 2 TYPICAL SECTION (LOOKING SOUTH)**

SCALE: NONE  
 SEE SHEET NO. 89

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001

\* FROM PREVIOUS STAGE TO REMAIN  
 \*\* FROM CONTRACT 60X93 TO REMAIN



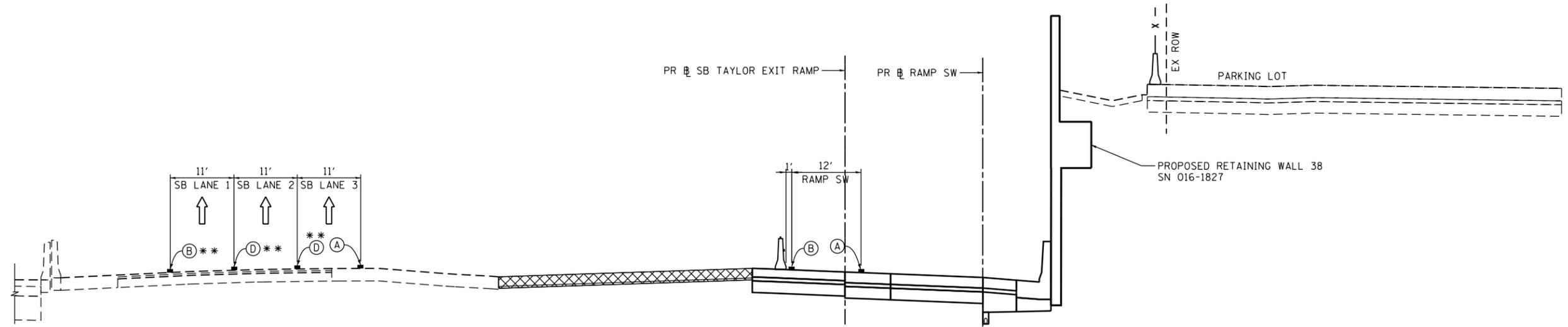
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS			
TYPICAL SECTIONS			
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP			
SCALE: NONE	SHEET 2 OF 7 SHEETS	STA. TO STA.	

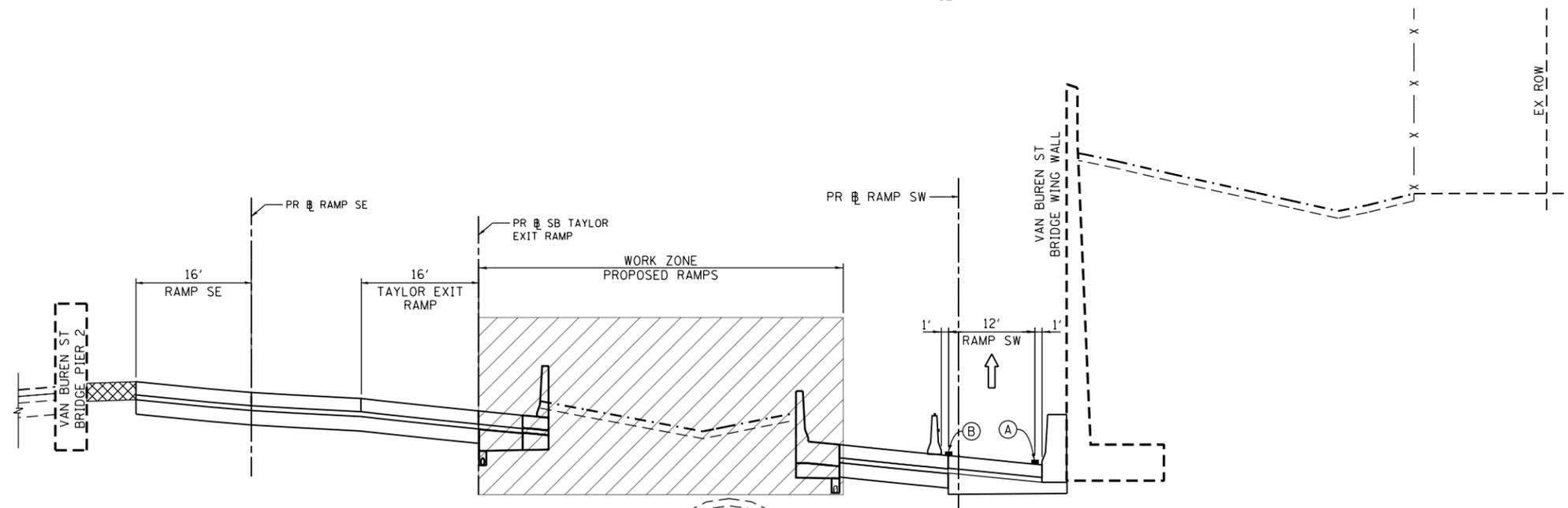
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ILLINOIS FED. AID PROJECT				

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**5 - RAMP SW STAGE 3 TYPICAL SECTION (LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO. 92



**6 - RAMP SW STAGE 3 TYPICAL SECTION (LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO. 93

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001
- \* FROM PREVIOUS STAGE TO REMAIN
- \*\* FROM CONTRACT 60X93 TO REMAIN



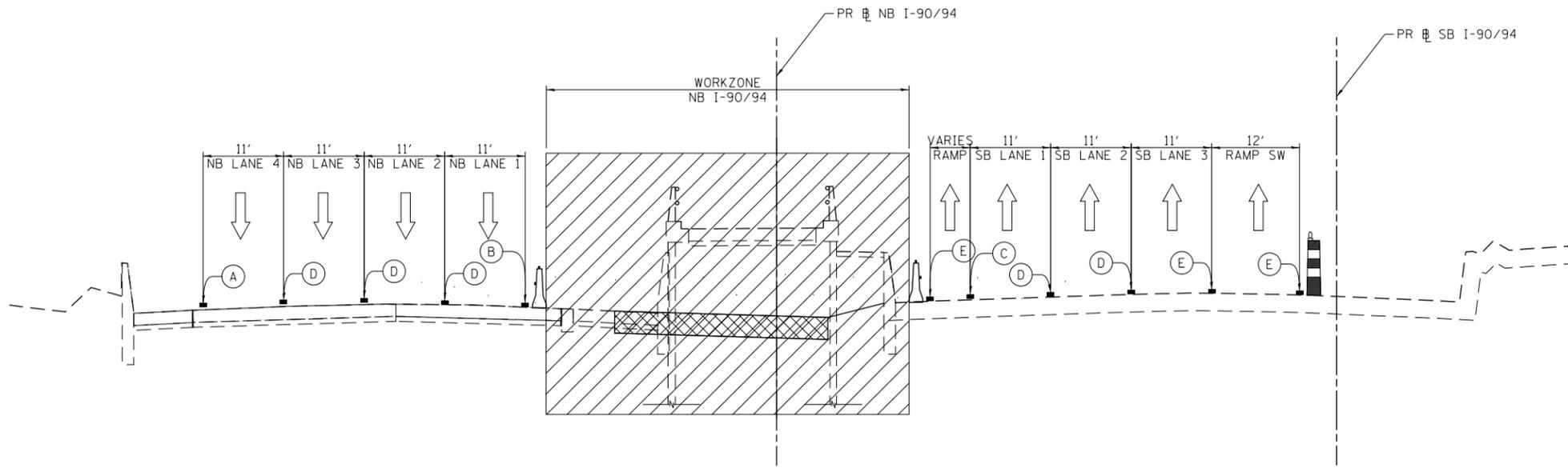
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS			
TYPICAL SECTIONS			
RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP			
SCALE: NONE	SHEET 3 OF 7 SHEETS	STA. TO STA.	

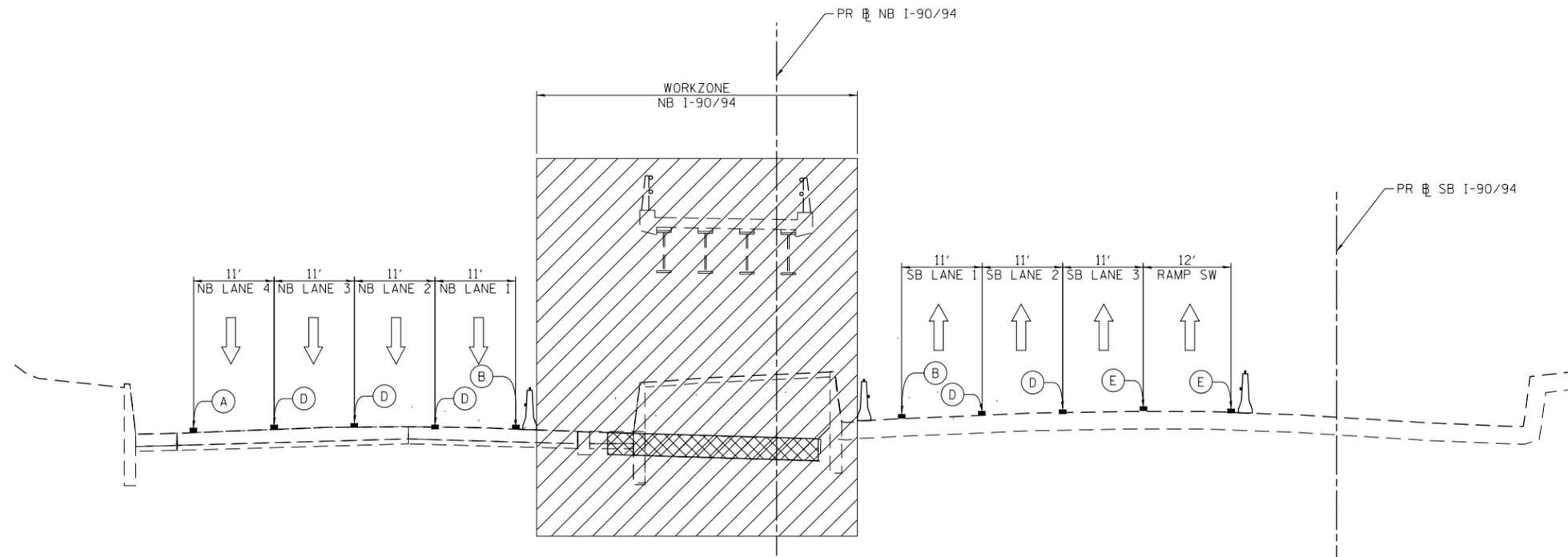
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CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**7 NB I-90/94 STAGE 2 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.88



**8 NB I-90/94 STAGE 2 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.88

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001
- \* FROM PREVIOUS STAGE TO REMAIN



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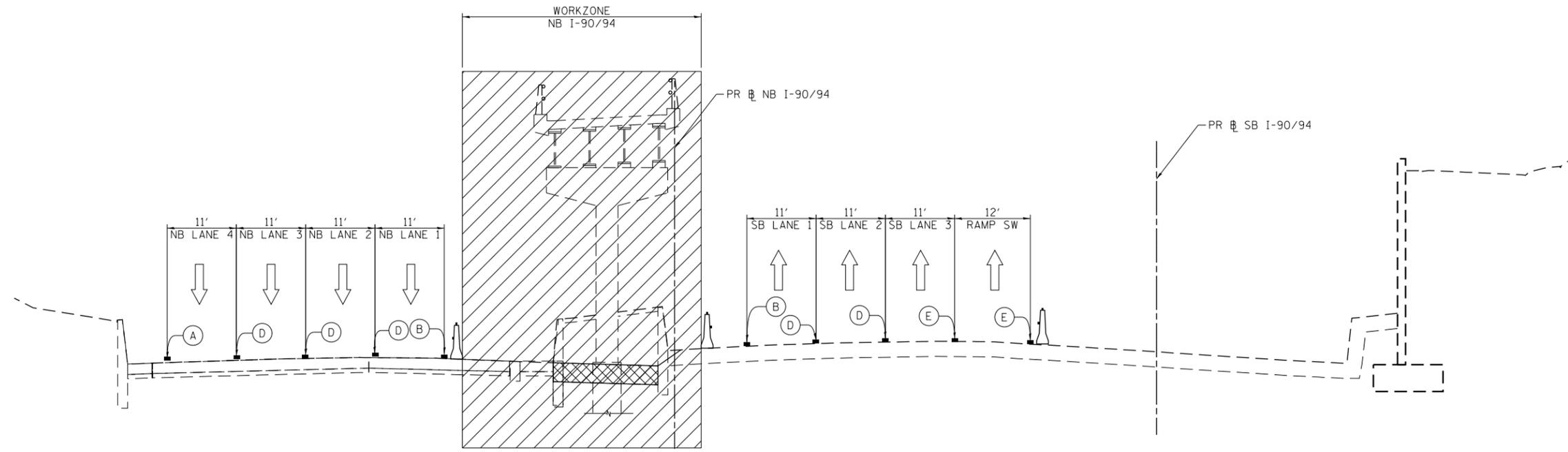
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS			
TYPICAL SECTIONS			
NORTHBOUND I-90/94			
SCALE: NONE	SHEET 4 OF 7 SHEETS	STA.	TO STA.

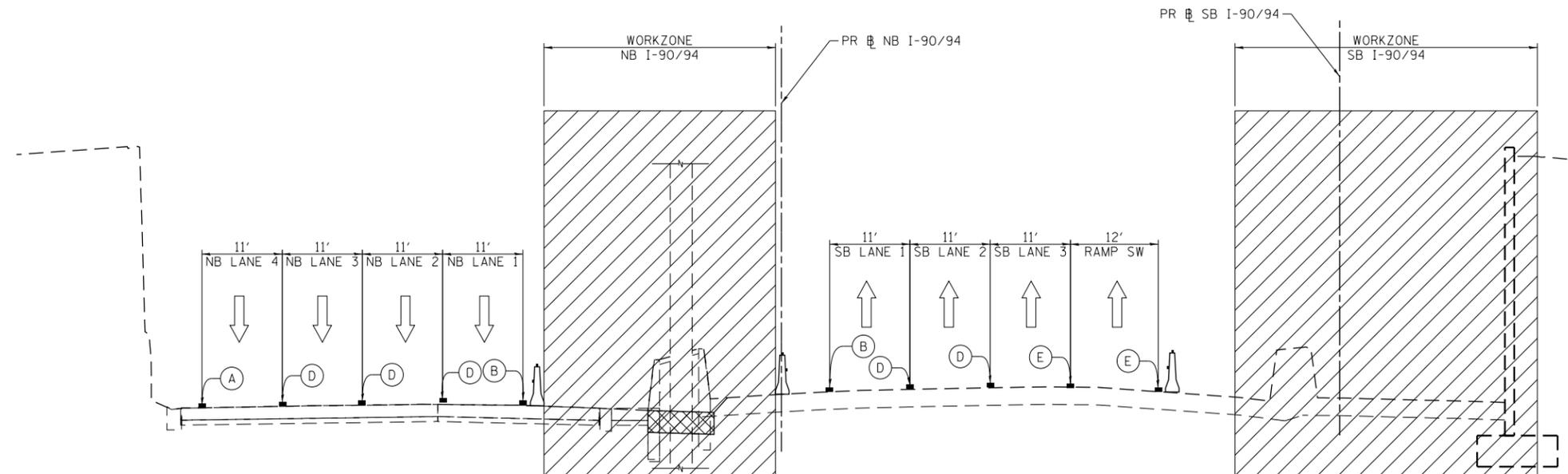
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**9 NB I-90/94 STAGE 2 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.88



**10 NB I-90/94 STAGE 2 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.88

**PROPOSED TEMP PAVEMENT MARKINGS  
AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001
- \* FROM PREVIOUS STAGE TO REMAIN



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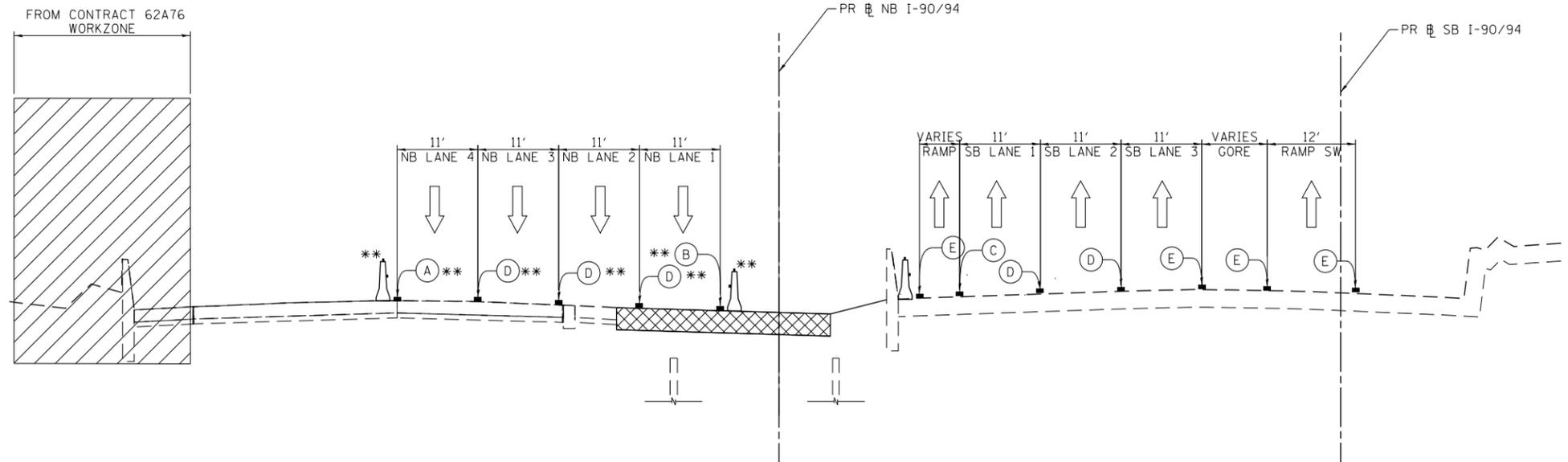
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS  
TYPICAL SECTIONS  
NORTHBOUND I-90/94**

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

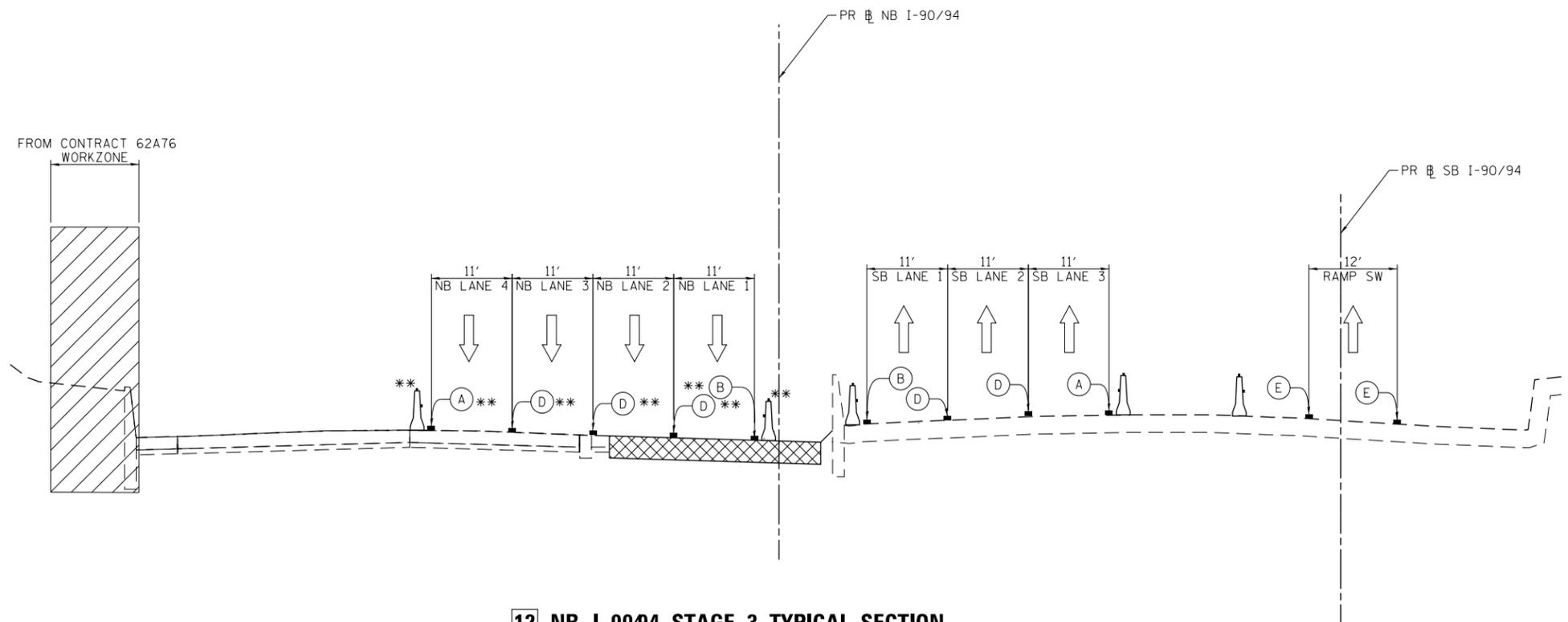
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**11 NB I-90/94 STAGE 3 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.92



**12 NB I-90/94 STAGE 3 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.92

**PROPOSED TEMP PAVEMENT MARKINGS  
AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC

- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001

- \* FROM PREVIOUS STAGE TO REMAIN
- \*\* FROM CONTRACT 62A76



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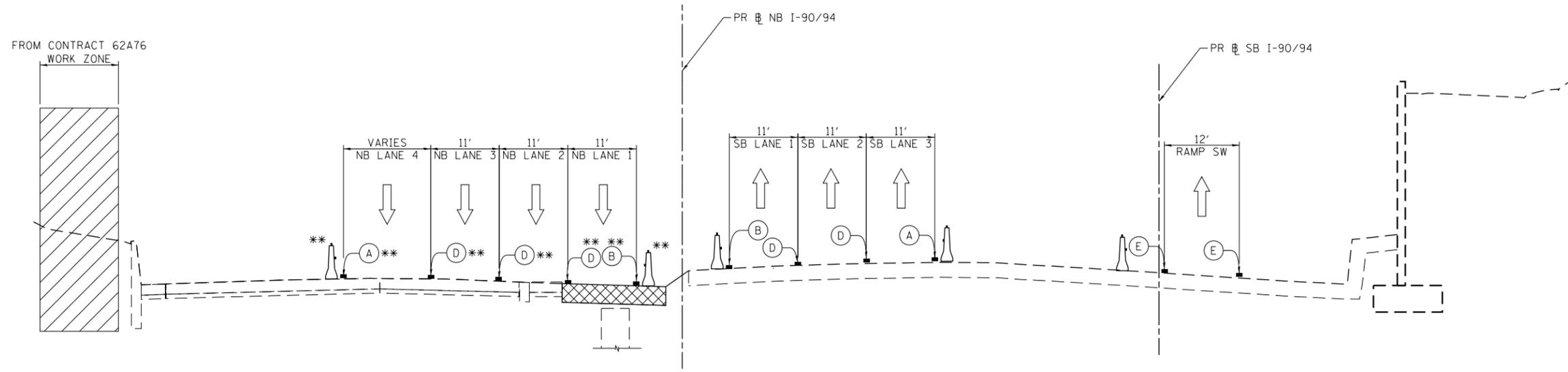
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS  
TYPICAL SECTIONS  
NORTHBOUND I-90/94**

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	71
				CONTRACT NO. 62J31
ILLINOIS FED. AID PROJECT				

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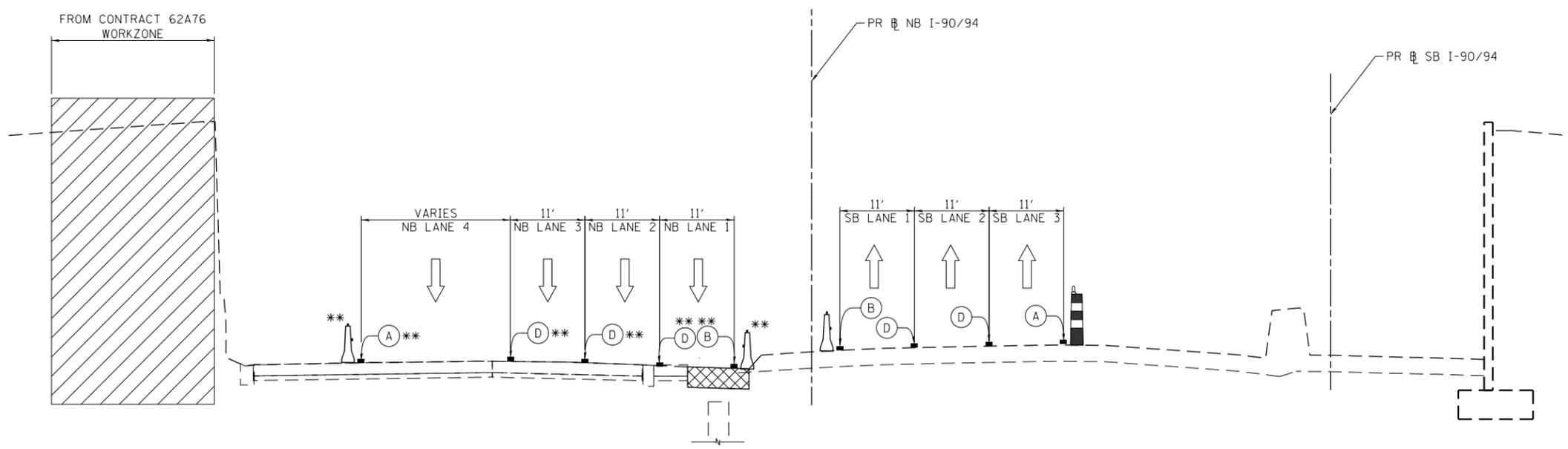
**13 NB I-90/94 STAGE 3 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.92

**PROPOSED TEMP PAVEMENT MARKINGS  
AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)

- WORK ZONE
- TEMPORARY PAVEMENT
- EXISTING PAVEMENT
- COMPLETED PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN LIGHT
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTOR, TYPE C (2 EACH @ 25' SPACINGS) PER STANDARD 704001
- \* FROM PREVIOUS STAGE TO REMAIN
- \*\* FROM CONTRACT 62A76



**14 NB I-90/94 STAGE 3 TYPICAL SECTION  
(LOOKING SOUTH)**

SCALE: NONE  
SEE SHEET NO.92

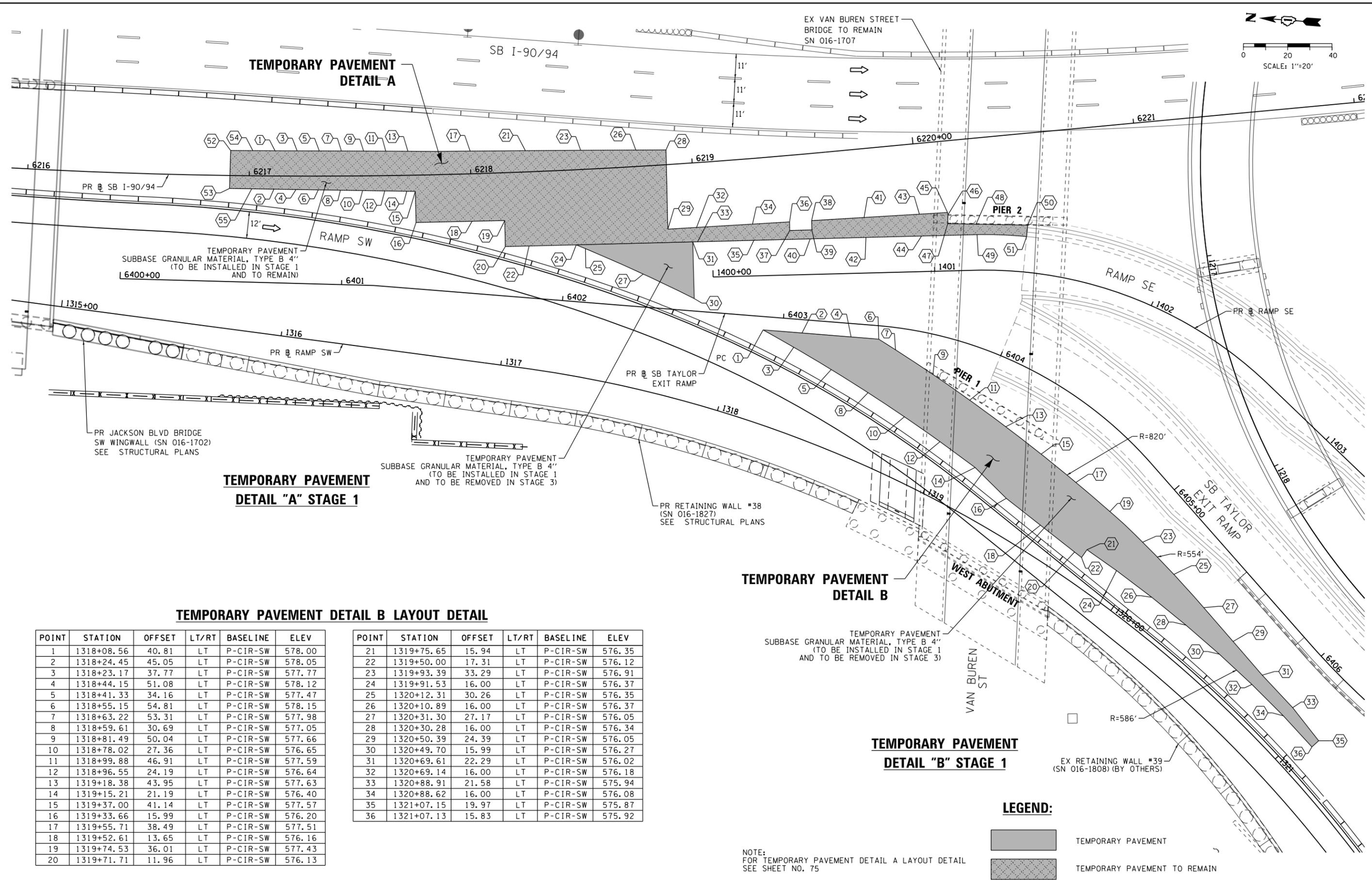
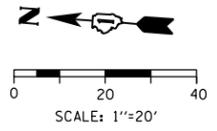


DI62J31-SHT-Staging-Typical-07.dgn	DESIGNED - CEC	REVISED -
USER NAME = mrc1ss	DRAWN - AMK	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JAF	REVISED -
PLOT DATE = 8/13/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS			
TYPICAL SECTIONS			
NORTHBOUND I-90/94			
SCALE: NONE	SHEET 7 OF 7 SHEETS	STA.	TO STA.

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 72
CONTRACT NO. 62J31				ILLINOIS FED. AID PROJECT



**TEMPORARY PAVEMENT  
DETAIL A**

**TEMPORARY PAVEMENT  
DETAIL "A" STAGE 1**

**TEMPORARY PAVEMENT  
DETAIL B**

**TEMPORARY PAVEMENT  
DETAIL "B" STAGE 1**

**TEMPORARY PAVEMENT DETAIL B LAYOUT DETAIL**

POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV
1	1318+08.56	40.81	LT	P-CIR-SW	578.00
2	1318+24.45	45.05	LT	P-CIR-SW	578.05
3	1318+23.17	37.77	LT	P-CIR-SW	577.77
4	1318+44.15	51.08	LT	P-CIR-SW	578.12
5	1318+41.33	34.16	LT	P-CIR-SW	577.47
6	1318+55.15	54.81	LT	P-CIR-SW	578.15
7	1318+63.22	53.31	LT	P-CIR-SW	577.98
8	1318+59.61	30.69	LT	P-CIR-SW	577.05
9	1318+81.49	50.04	LT	P-CIR-SW	577.66
10	1318+78.02	27.36	LT	P-CIR-SW	576.65
11	1318+99.88	46.91	LT	P-CIR-SW	577.59
12	1318+96.55	24.19	LT	P-CIR-SW	576.64
13	1319+18.38	43.95	LT	P-CIR-SW	577.63
14	1319+15.21	21.19	LT	P-CIR-SW	576.40
15	1319+37.00	41.14	LT	P-CIR-SW	577.57
16	1319+33.66	15.99	LT	P-CIR-SW	576.20
17	1319+55.71	38.49	LT	P-CIR-SW	577.51
18	1319+52.61	13.65	LT	P-CIR-SW	576.16
19	1319+74.53	36.01	LT	P-CIR-SW	577.43
20	1319+71.71	11.96	LT	P-CIR-SW	576.13

POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV
21	1319+75.65	15.94	LT	P-CIR-SW	576.35
22	1319+50.00	17.31	LT	P-CIR-SW	576.12
23	1319+93.39	33.29	LT	P-CIR-SW	576.91
24	1319+91.53	16.00	LT	P-CIR-SW	576.37
25	1320+12.31	30.26	LT	P-CIR-SW	576.35
26	1320+10.89	16.00	LT	P-CIR-SW	576.37
27	1320+31.30	27.17	LT	P-CIR-SW	576.05
28	1320+30.28	16.00	LT	P-CIR-SW	576.34
29	1320+50.39	24.39	LT	P-CIR-SW	576.05
30	1320+49.70	15.99	LT	P-CIR-SW	576.27
31	1320+69.61	22.29	LT	P-CIR-SW	576.02
32	1320+69.14	16.00	LT	P-CIR-SW	576.18
33	1320+88.91	21.58	LT	P-CIR-SW	575.94
34	1320+88.62	16.00	LT	P-CIR-SW	576.08
35	1321+07.15	19.97	LT	P-CIR-SW	575.87
36	1321+07.13	15.83	LT	P-CIR-SW	575.92

**LEGEND:**

- TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT TO REMAIN

NOTE:  
FOR TEMPORARY PAVEMENT DETAIL A LAYOUT DETAIL  
SEE SHEET NO. 75

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 CHICAGO, IL 60601-5276  
 PHONE: (312) 373-1700 FAX: (312) 373-6800



D162J31-SHT-Staging-Temp-Pavement-01.dgn  
 USER NAME = ashok.kc  
 PLOT SCALE = 40.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - PHP  
 DRAWN - PHP  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

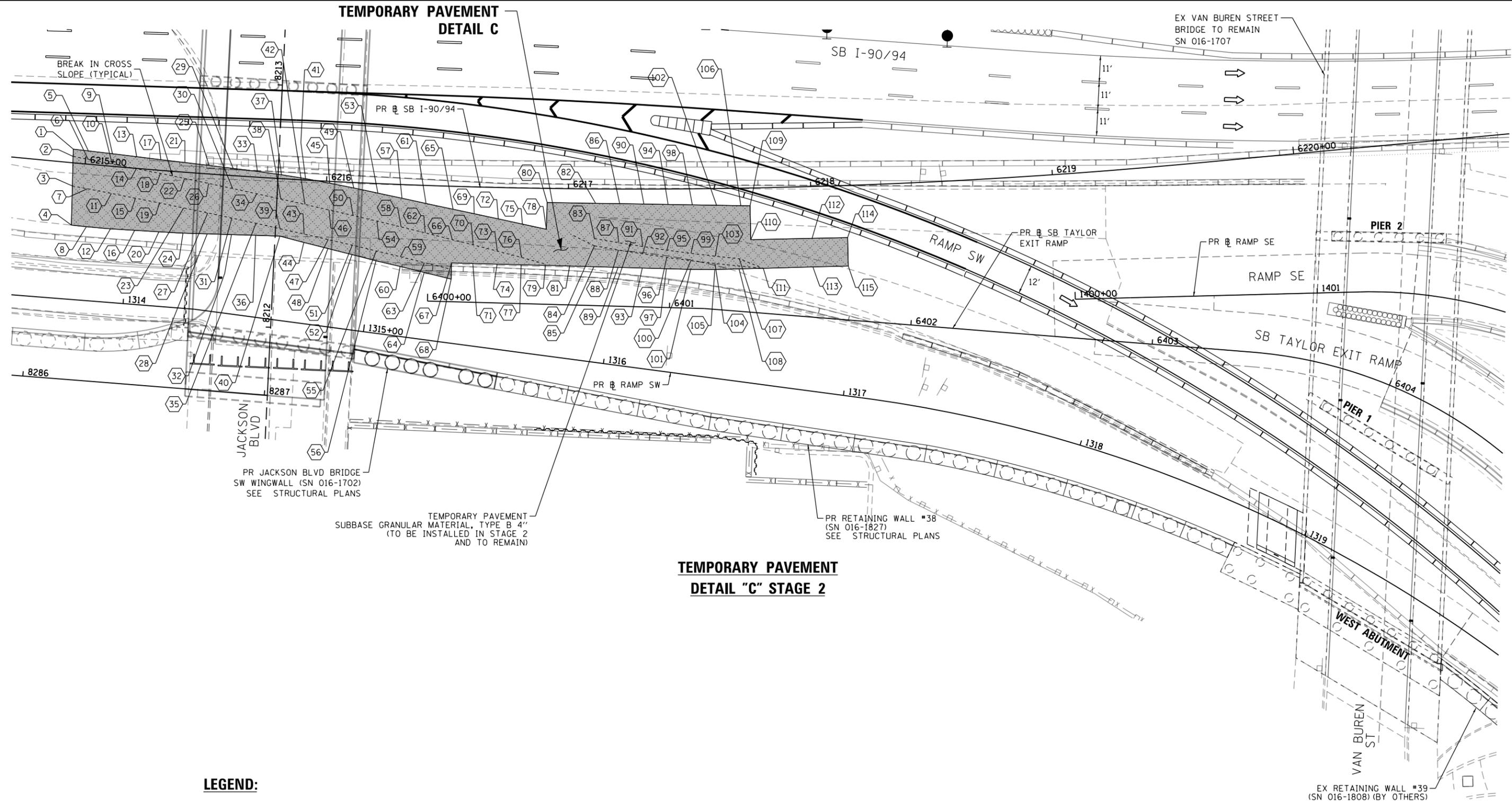
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
 TEMPORARY PAVEMENT DETAILS (STAGE 1)**

SCALE: 1"=20' SHEET 1 OF 3 SHEETS STA. 1315+00.47 TO STA. 1319+75.65

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	73
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

**TEMPORARY PAVEMENT  
DETAIL C**

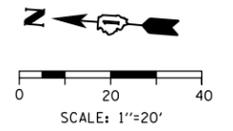


**TEMPORARY PAVEMENT  
DETAIL "C" STAGE 2**

**LEGEND:**

-  TEMPORARY PAVEMENT
-  TEMPORARY PAVEMENT TO REMAIN

NOTE:  
FOR TEMPORARY PAVEMENT LAYOUT DETAIL  
SEE SHEET NO. 75



FILE PATH = p:\aecom\m-n\m\1\local\AECOM\_0502\_MN\Documents\01\_Americas\Transportation\60269938\_Circle Phase 1\1000 CAD\006\_Roadway\Sheets\62J31\_Contract\162J31-SHT-5-Staging-Temp-Pavement-02.dgn



D162J31-SHT-5-Staging-Temp-Pavement-02.dgn  
 USER NAME = oshok.kc  
 PLOT SCALE = 40.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - PHP  
 DRAWN - PHP  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISIONS:  
 REVISION NO. | DATE | BY | DESCRIPTION  
 - | - | - | -  
 - | - | - | -  
 - | - | - | -  
 - | - | - | -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
TEMPORARY PAVEMENT DETAIL "B" (STAGE 2)**  
 SCALE: 1"=20' SHEET 2 OF 3 SHEETS STA. 1313+80 TO STA. 1320+40

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	74
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

TEMPORARY PAVEMENT DETAIL A LAYOUT DETAIL

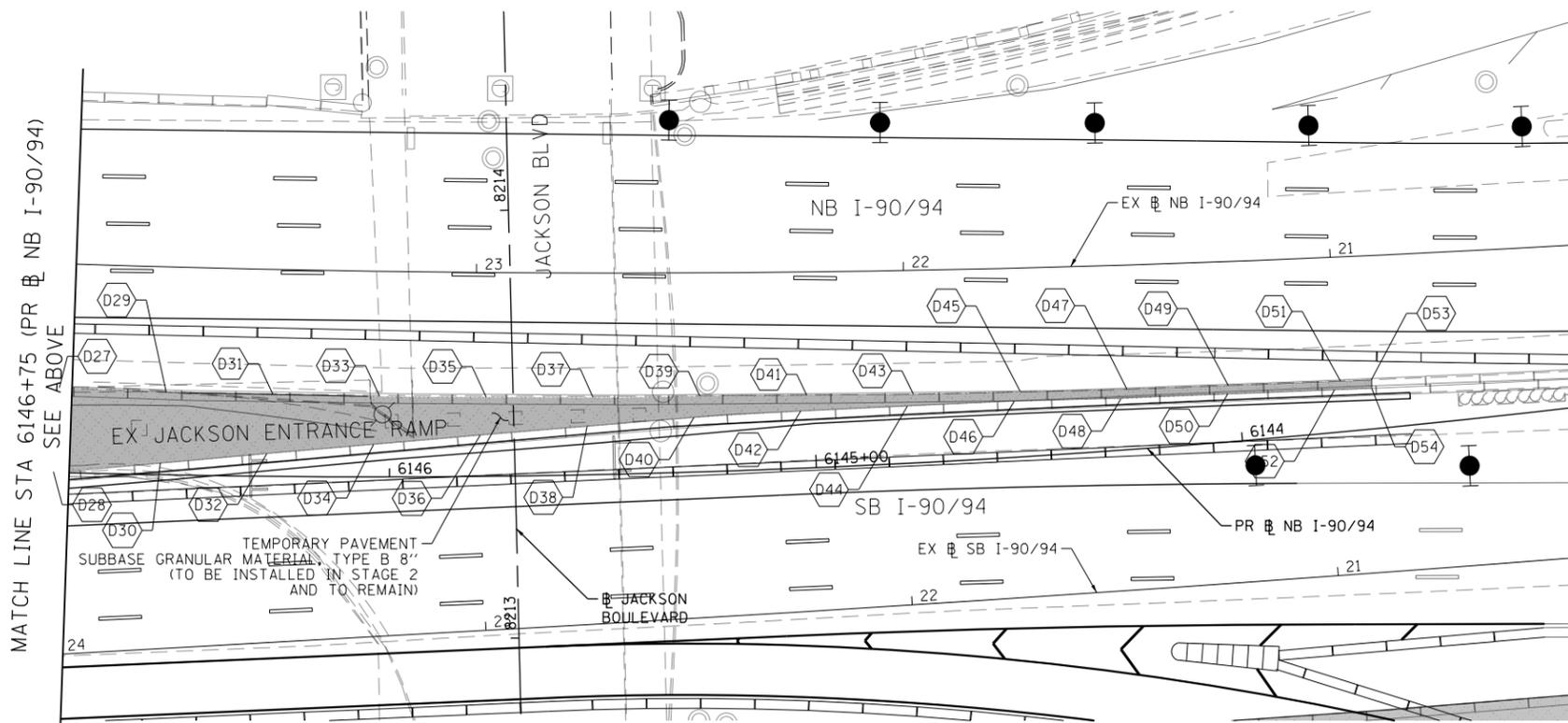
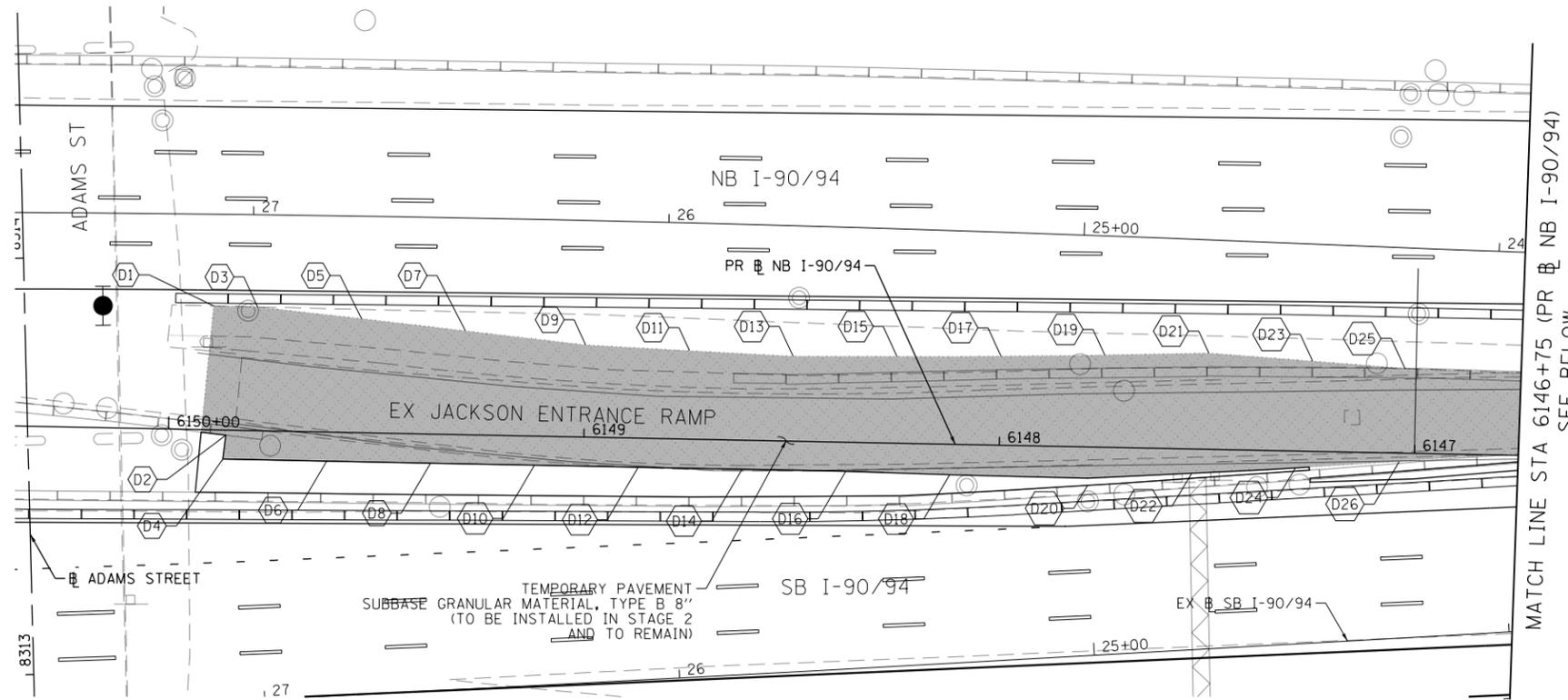
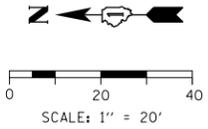
POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV	TEMPORARY PAVEMENT
1	6217+11.54	11.39	LT	P-KDR-SB	577.51	TO REMAIN
2	6217+11.18	5.90	RT	P-KDR-SB	577.81	TO REMAIN
3	6217+21.59	11.40	LT	P-KDR-SB	577.53	TO REMAIN
4	6217+21.16	6.01	RT	P-KDR-SB	577.82	TO REMAIN
5	6217+31.63	11.37	LT	P-KDR-SB	577.56	TO REMAIN
6	6217+31.14	6.16	RT	P-KDR-SB	577.83	TO REMAIN
7	6217+41.68	11.29	LT	P-KDR-SB	577.55	TO REMAIN
8	6217+41.11	6.36	RT	P-KDR-SB	577.83	TO REMAIN
9	6217+51.73	11.17	LT	P-KDR-SB	577.54	TO REMAIN
10	6217+51.08	6.59	RT	P-KDR-SB	577.82	TO REMAIN
11	6217+61.77	11.02	LT	P-KDR-SB	577.53	TO REMAIN
12	6217+61.05	6.86	RT	P-KDR-SB	577.81	TO REMAIN
13	6217+71.82	10.82	LT	P-KDR-SB	577.52	TO REMAIN
14	6217+71.02	7.17	RT	P-KDR-SB	577.80	TO REMAIN
15	6217+74.84	7.30	RT	P-KDR-SB	577.80	TO REMAIN
16	6217+74.89	21.30	RT	P-KDR-SB	578.02	TO REMAIN
17	6218+00.00	10.24	LT	P-KDR-SB	577.49	TO REMAIN
18	6218+00.00	21.34	RT	P-KDR-SB	578.04	TO REMAIN
19	6218+14.56	21.48	RT	P-KDR-SB	578.09	TO REMAIN
20	6218+14.42	33.26	RT	P-KDR-SB	578.32	TO REMAIN
21	6218+25.00	9.58	LT	P-KDR-SB	577.45	TO REMAIN
22	6218+25.00	33.38	RT	P-KDR-SB	578.38	TO REMAIN
23	6218+50.00	8.64	LT	P-KDR-SB	577.39	TO REMAIN
24	6218+45.98	33.76	RT	P-KDR-SB	578.49	TO REMAIN
25	6218+50.00	35.68	RT	P-KDR-SB	578.47	
26	6218+75.00	7.41	LT	P-KDR-SB	577.29	TO REMAIN
27	6218+75.00	48.50	RT	P-KDR-SB	578.36	
28	6218+88.63	6.63	LT	P-KDR-SB	577.23	TO REMAIN
29	6218+87.16	29.02	RT	P-KDR-SB	578.49	TO REMAIN
30	6218+96.79	60.97	RT	P-KDR-SB	578.25	
31	6218+97.85	35.46	RT	P-KDR-SB	578.76	TO REMAIN
32	6219+00.00	29.11	RT	P-KDR-SB	578.51	TO REMAIN
33	6219+00.00	35.56	RT	P-KDR-SB	578.77	TO REMAIN
34	6219+25.00	29.48	RT	P-KDR-SB	578.61	TO REMAIN
35	6219+25.00	36.79	RT	P-KDR-SB	578.90	TO REMAIN
36	6219+41.52	29.86	RT	P-KDR-SB	578.71	TO REMAIN
37	6219+41.29	33.72	RT	P-KDR-SB	578.87	TO REMAIN
38	6219+51.40	30.15	RT	P-KDR-SB	578.81	TO REMAIN
39	6219+51.13	34.34	RT	P-KDR-SB	578.98	TO REMAIN
40	6219+50.00	38.28	RT	P-KDR-SB	579.13	TO REMAIN
41	6219+75.00	30.93	RT	P-KDR-SB	579.06	TO REMAIN
42	6219+75.00	39.96	RT	P-KDR-SB	579.43	TO REMAIN
43	6220+00.00	31.78	RT	P-KDR-SB	579.44	TO REMAIN
44	6220+00.00	41.66	RT	P-KDR-SB	579.72	TO REMAIN
45	6220+12.66	32.21	RT	P-KDR-SB	579.46	TO REMAIN
46	6220+12.78	33.22	RT	P-KDR-SB	579.54	TO REMAIN
47	6220+12.33	37.19	RT	P-KDR-SB	579.82	TO REMAIN
48	6220+25.00	38.62	RT	P-KDR-SB	580.23	TO REMAIN
49	6220+25.00	43.37	RT	P-KDR-SB	580.55	TO REMAIN
50	6220+48.14	41.24	RT	P-KDR-SB	580.93	TO REMAIN
51	6220+47.18	46.31	RT	P-KDR-SB	581.25	TO REMAIN
52	6216+91.44	11.25	LT	P-KDR-SB	577.46	TO REMAIN
53	6216+91.23	5.80	RT	P-KDR-SB	578.11	TO REMAIN
54	6217+01.49	11.34	LT	P-KDR-SB	577.49	TO REMAIN
55	6217+01.21	5.83	RT	P-KDR-SB	578.08	TO REMAIN

TEMPORARY PAVEMENT DETAIL C LAYOUT DETAIL

POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV	TEMPORARY PAVEMENT
1	6214+94.56	5.58	LT	P-KDR-SB	576.25	TO REMAIN
2	6214+94.76	2.58	LT	P-KDR-SB	576.16	TO REMAIN
3	6214+95.56	9.54	RT	P-KDR-SB	575.80	TO REMAIN
4	6214+96.62	25.65	RT	P-KDR-SB	575.30	TO REMAIN
5	6215+00.84	5.46	LT	P-KDR-SB	576.28	TO REMAIN
6	6215+01.06	1.99	LT	P-KDR-SB	576.15	TO REMAIN
7	6215+01.84	10.14	RT	P-KDR-SB	575.82	TO REMAIN
8	6215+02.82	25.73	RT	P-KDR-SB	575.28	TO REMAIN
9	6215+10.90	5.15	LT	P-KDR-SB	576.26	TO REMAIN
10	6215+11.15	0.97	LT	P-KDR-SB	576.16	TO REMAIN
11	6215+11.88	11.17	RT	P-KDR-SB	575.89	TO REMAIN
12	6215+12.74	25.73	RT	P-KDR-SB	575.29	TO REMAIN
13	6215+20.96	4.80	LT	P-KDR-SB	576.32	TO REMAIN
14	6215+21.24	0.13	RT	P-KDR-SB	576.21	TO REMAIN
15	6215+21.91	12.28	RT	P-KDR-SB	575.97	TO REMAIN
16	6215+22.64	25.51	RT	P-KDR-SB	575.39	TO REMAIN
17	6215+31.01	4.40	LT	P-KDR-SB	576.42	TO REMAIN
18	6215+31.31	1.30	RT	P-KDR-SB	576.32	TO REMAIN
19	6215+31.94	13.47	RT	P-KDR-SB	576.07	TO REMAIN
20	6215+32.55	25.33	RT	P-KDR-SB	575.59	TO REMAIN
21	6215+41.06	3.97	LT	P-KDR-SB	576.54	TO REMAIN
22	6215+41.38	2.56	RT	P-KDR-SB	576.47	TO REMAIN
23	6215+41.96	14.73	RT	P-KDR-SB	576.23	TO REMAIN
24	6215+42.45	25.14	RT	P-KDR-SB	575.81	TO REMAIN
25	6215+51.11	3.49	LT	P-KDR-SB	576.69	TO REMAIN
26	6215+51.44	3.90	RT	P-KDR-SB	576.67	TO REMAIN
27	6215+51.97	16.08	RT	P-KDR-SB	576.43	TO REMAIN
28	6215+52.34	24.68	RT	P-KDR-SB	576.08	TO REMAIN
29	6215+61.16	2.98	LT	P-KDR-SB	576.85	TO REMAIN
30	6215+61.49	5.31	RT	P-KDR-SB	576.88	TO REMAIN
31	6215+61.97	17.51	RT	P-KDR-SB	576.64	TO REMAIN
32	6215+62.23	24.30	RT	P-KDR-SB	576.36	TO REMAIN
33	6215+71.20	2.39	LT	P-KDR-SB	577.00	TO REMAIN
34	6215+71.53	6.81	RT	P-KDR-SB	577.09	TO REMAIN
35	6215+71.96	19.02	RT	P-KDR-SB	576.85	TO REMAIN
36	6215+72.13	23.96	RT	P-KDR-SB	576.70	TO REMAIN
37	6215+81.24	1.59	LT	P-KDR-SB	577.14	TO REMAIN
38	6215+81.55	8.39	LT	P-KDR-SB	577.30	TO REMAIN
39	6215+81.94	20.61	RT	P-KDR-SB	577.06	TO REMAIN
40	6215+82.03	23.67	RT	P-KDR-SB	577.00	TO REMAIN
41	6215+91.28	0.55	LT	P-KDR-SB	577.29	TO REMAIN
42	6215+91.57	10.05	RT	P-KDR-SB	577.51	TO REMAIN
43	6215+91.91	22.28	RT	P-KDR-SB	577.27	TO REMAIN
44	6215+92.00	25.73	RT	P-KDR-SB	577.20	TO REMAIN
45	6216+01.31	0.72	RT	P-KDR-SB	577.45	TO REMAIN
46	6216+01.57	11.79	RT	P-KDR-SB	577.72	TO REMAIN
47	6216+01.86	24.03	RT	P-KDR-SB	577.48	TO REMAIN
48	6216+01.95	27.71	RT	P-KDR-SB	577.40	TO REMAIN
49	6216+11.34	2.24	RT	P-KDR-SB	577.58	TO REMAIN
50	6216+11.57	13.60	RT	P-KDR-SB	577.90	TO REMAIN
51	6216+11.80	25.85	RT	P-KDR-SB	577.66	TO REMAIN
52	6216+11.88	29.73	RT	P-KDR-SB	577.58	TO REMAIN
53	6216+21.36	3.92	RT	P-KDR-SB	577.61	TO REMAIN
54	6216+21.54	15.50	RT	P-KDR-SB	578.05	TO REMAIN
55	6216+21.73	27.76	RT	P-KDR-SB	577.80	TO REMAIN
56	6216+21.80	31.86	RT	P-KDR-SB	577.72	TO REMAIN
57	6216+31.37	5.64	RT	P-KDR-SB	577.75	TO REMAIN
58	6216+31.51	17.48	RT	P-KDR-SB	578.15	TO REMAIN
59	6216+31.65	29.75	RT	P-KDR-SB	577.91	TO REMAIN
60	6216+31.69	33.75	RT	P-KDR-SB	577.83	TO REMAIN
61	6216+41.36	7.41	RT	P-KDR-SB	577.87	TO REMAIN
62	6216+41.45	19.54	RT	P-KDR-SB	578.23	TO REMAIN
63	6216+41.55	31.83	RT	P-KDR-SB	577.98	TO REMAIN
64	6216+41.58	35.83	RT	P-KDR-SB	577.90	TO REMAIN
65	6216+52.58	9.44	RT	P-KDR-SB	577.96	TO REMAIN
66	6216+52.62	21.96	RT	P-KDR-SB	578.27	TO REMAIN
67	6216+52.65	31.57	RT	P-KDR-SB	578.08	TO REMAIN
68	6216+52.67	38.25	RT	P-KDR-SB	577.95	TO REMAIN
69	6216+61.30	11.05	RT	P-KDR-SB	578.00	TO REMAIN
70	6216+61.30	23.90	RT	P-KDR-SB	578.28	TO REMAIN
71	6216+61.29	31.55	RT	P-KDR-SB	578.12	TO REMAIN
72	6216+71.25	12.94	RT	P-KDR-SB	578.54	TO REMAIN
73	6216+71.19	26.21	RT	P-KDR-SB	578.28	TO REMAIN
74	6216+71.17	31.58	RT	P-KDR-SB	578.17	TO REMAIN
75	6216+81.18	14.86	RT	P-KDR-SB	578.07	TO REMAIN

TEMPORARY PAVEMENT DETAIL C LAYOUT DETAIL

POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV	TEMPORARY PAVEMENT
76	6216+81.07	28.62	RT	P-KDR-SB	578.26	TO REMAIN
77	6216+81.04	31.64	RT	P-KDR-SB	578.20	TO REMAIN
78	6216+91.10	16.80	RT	P-KDR-SB	578.06	TO REMAIN
79	6216+90.92	31.74	RT	P-KDR-SB	578.24	TO REMAIN
80	6216+91.23	5.80	RT	P-KDR-SB	578.05	TO REMAIN
81	6217+00.79	31.88	RT	P-KDR-SB	578.26	TO REMAIN
82	6217+11.18	5.90	RT	P-KDR-SB	577.81	TO REMAIN
83	6217+10.89	20.43	RT	P-KDR-SB	578.06	TO REMAIN
84	6217+10.83	23.43	RT	P-KDR-SB	578.11	TO REMAIN
85	6217+10.66	32.06	RT	P-KDR-SB	578.26	TO REMAIN
86	6217+21.16	6.01	RT	P-KDR-SB	578.01	TO REMAIN
87	6217+20.77	22.10	RT	P-KDR-SB	578.45	TO REMAIN
88	6217+20.70	25.10	RT	P-KDR-SB	578.42	TO REMAIN
89	6217+20.53	32.28	RT	P-KDR-SB	578.27	TO REMAIN
90	6217+31.14	6.16	RT	P-KDR-SB	577.98	TO REMAIN
91	6217+30.65	23.68	RT	P-KDR-SB	578.41	TO REMAIN
92	6217+30.56	26.68	RT	P-KDR-SB	578.39	TO REMAIN
93	6217+30.40	32.54	RT	P-KDR-SB	578.28	TO REMAIN
94	6217+41.11	6.36	RT	P-KDR-SB	577.94	TO REMAIN
95	6217+40.51	25.17	RT	P-KDR-SB	578.35	TO REMAIN
96	6217+40.41	28.17	RT	P-KDR-SB	578.35	TO REMAIN
97	6217+40.27	32.84	RT	P-KDR-SB	578.29	TO REMAIN
98	6217+51.08	6.59	RT	P-KDR-SB	577.90	TO REMAIN
99	6217+50.37	26.57	RT	P-KDR-SB	578.30	TO REMAIN
100	6217+50.26	29.57	RT	P-KDR-SB	578.32	TO REMAIN
101	6217+50.13	33.18	RT	P-KDR-SB	578.24	TO REMAIN
102	6217+61.05	6.86	RT	P-KDR-SB	577.86	TO REMAIN
103	6217+60.22	27.88	RT	P-KDR-SB	578.24	TO REMAIN
104	6217+60.10	30.88	RT	P-KDR-SB	578.27	TO REMAIN
105	6217+60.00	33.35	RT	P-KDR-SB	578.22	TO REMAIN
106	6217+71.02	7.17	RT	P-KDR-SB	577.82	TO REMAIN
107	6217+70.06	29.10	RT	P-KDR-SB	578.19	TO REMAIN
108	6217+69.88	33.25	RT	P-KDR-SB	578.21	TO REMAIN
109	6217+74.84	7.30	RT	P-KDR-SB	577.82	TO REMAIN
110	6217+74.89	21.30	RT	P-KDR-SB	578.04	TO REMAIN
111	6217+79.76	33.18	RT	P-KDR-SB	578.20	TO REMAIN
112	6218+00.00	21.34	RT	P-KDR-SB	578.04	TO REMAIN
113	6218+00.00	33.17	RT	P-KDR-SB	578.25	TO REMAIN
114	6218+14.56					



POINT	STATION	OFFSET	LT/RT	BASELINE	ELEV
D1	6149+89.74	29.75'	RT	P-KDR-NB	577.61
D2	6149+86.22	1.25'	LT	P-KDR-NB	577.11
D3	6149+79.12	29.71'	RT	P-KDR-NB	577.50
D4	6149+86.82	7.00'	LT	P-KDR-NB	577.02
D5	6149+54.07	27.39'	RT	P-KDR-NB	577.22
D6	6149+61.81	7.00'	LT	P-KDR-NB	576.78
D7	6149+29.02	24.97'	RT	P-KDR-NB	576.86
D8	6149+36.81	7.00'	LT	P-KDR-NB	576.48
D9	6149+00.00	22.14'	RT	P-KDR-NB	576.44
D10	6149+11.80	7.00'	LT	P-KDR-NB	576.17
D11	6148+74.98	21.24'	RT	P-KDR-NB	576.14
D12	6148+86.80	7.00'	LT	P-KDR-NB	575.84
D13	6148+50.00	20.35'	RT	P-KDR-NB	575.88
D14	6148+61.80	7.00'	LT	P-KDR-NB	575.57
D15	6148+25.00	20.82'	RT	P-KDR-NB	575.64
D16	6148+36.79	7.00'	LT	P-KDR-NB	575.34
D17	6148+00.01	21.44'	RT	P-KDR-NB	575.45
D18	6148+11.78	7.34'	LT	P-KDR-NB	575.11
D19	6147+75.02	22.14'	RT	P-KDR-NB	575.29
D20	6147+78.54	9.23'	LT	P-KDR-NB	574.83
D21	6147+50.00	23.08'	RT	P-KDR-NB	575.25
D22	6147+53.60	5.93'	LT	P-KDR-NB	574.82
D23	6147+24.97	21.66'	RT	P-KDR-NB	575.17
D24	6147+28.63	4.10'	LT	P-KDR-NB	574.80
D25	6147+02.70	20.31'	RT	P-KDR-NB	575.09
D26	6147+03.67	2.06'	LT	P-KDR-NB	574.76
D27	6146+77.69	19.83'	RT	P-KDR-NB	575.12
D28	6146+78.71	0.20'	RT	P-KDR-NB	574.83
D29	6146+52.68	19.34'	RT	P-KDR-NB	575.22
D30	6146+53.75	2.68'	RT	P-KDR-NB	574.96
D31	6146+27.50	19.13'	RT	P-KDR-NB	575.37
D32	6146+28.75	5.29'	RT	P-KDR-NB	575.17
D33	6146+02.30	18.77'	RT	P-KDR-NB	575.53
D34	6146+03.70	7.62'	RT	P-KDR-NB	575.35
D35	6145+77.10	18.47'	RT	P-KDR-NB	575.72
D36	6145+78.60	9.63'	RT	P-KDR-NB	575.58
D37	6145+51.92	18.02'	RT	P-KDR-NB	575.93
D38	6145+53.46	11.35'	RT	P-KDR-NB	575.81
D39	6145+26.73	17.69'	RT	P-KDR-NB	576.20
D40	6145+28.29	12.74'	RT	P-KDR-NB	576.11
D41	6145+01.56	17.07'	RT	P-KDR-NB	576.48
D42	6145+03.12	13.35'	RT	P-KDR-NB	576.37
D43	6144+76.40	16.07'	RT	P-KDR-NB	576.74
D44	6144+77.95	13.38'	RT	P-KDR-NB	576.68
D45	6144+51.24	15.29'	RT	P-KDR-NB	576.98
D46	6144+52.79	13.08'	RT	P-KDR-NB	576.93
D47	6144+26.11	14.12'	RT	P-KDR-NB	577.20
D48	6144+27.68	12.00'	RT	P-KDR-NB	577.16
D49	6144+02.25	13.17'	RT	P-KDR-NB	577.39
D50	6144+02.55	11.18'	RT	P-KDR-NB	577.36
D51	6143+75.86	11.87'	RT	P-KDR-NB	577.48
D52	6143+77.45	9.94'	RT	P-KDR-NB	577.51
D53	6143+68.61	11.45'	RT	P-KDR-NB	577.58
D54	6143+68.67	9.45'	RT	P-KDR-NB	577.55

**TEMPORARY PAVEMENT  
DETAIL "D" STAGE 2**

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D162J31-SHT-Staging-Temp-Pavement-03.dgn  
 USER NAME = mrc155  
 PLOT SCALE = 40.0000' / in.  
 PLOT DATE = 8/14/2019

DESIGNED - MJL  
 DRAWN - JRM  
 CHECKED - JMG  
 DATE - 8/16/2019

REVISED -  
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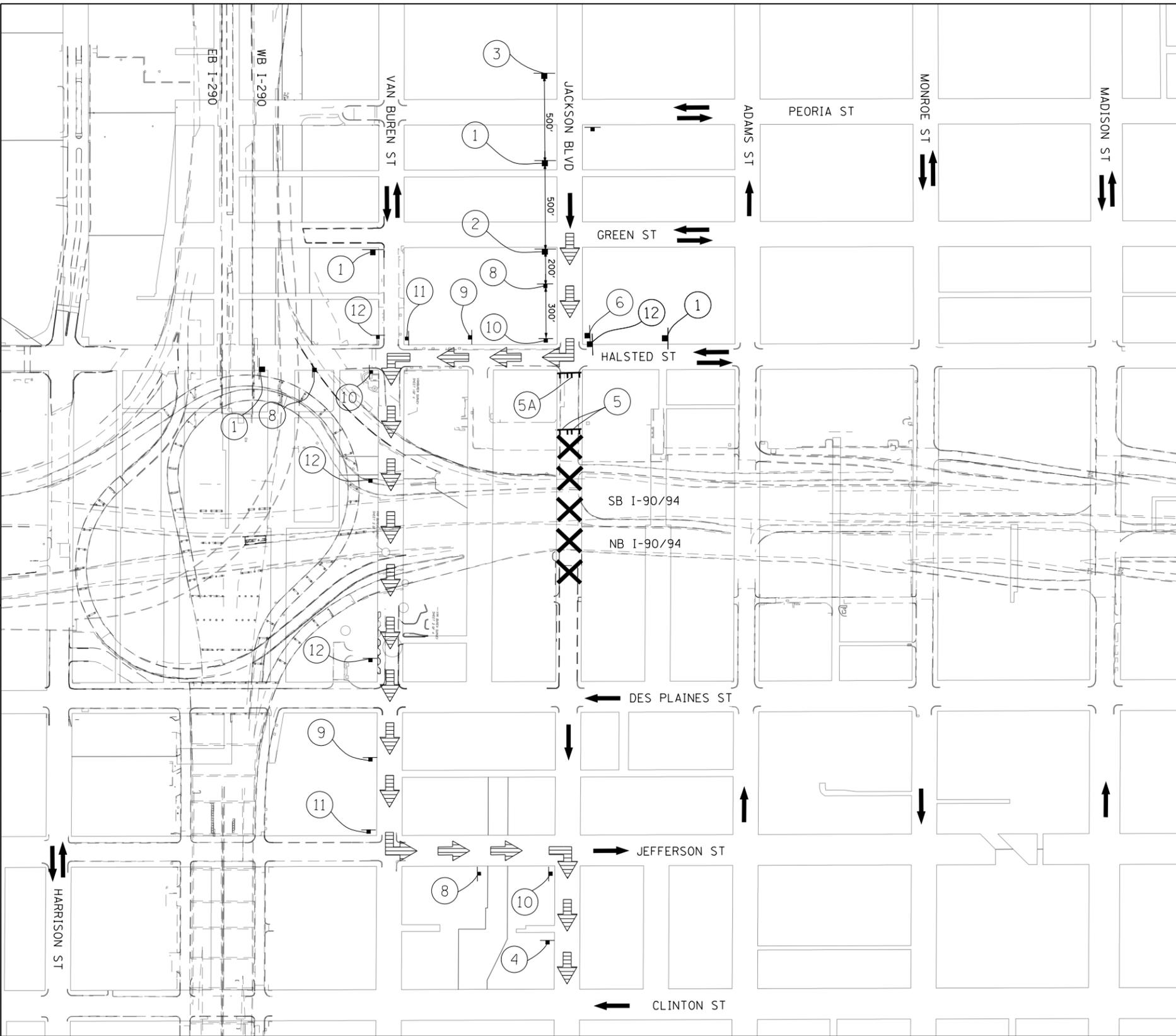
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
TEMPORARY PAVEMENT DETAIL (STAGE 2)**

SCALE: 1"=20' SHEET 4 OF 4 SHEETS STA. 20+00 TO STA. 28+00

F.A.I. R.T.E. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 76
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	

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**LEGEND**

- TEMPORARY SIGN
- SIGN BARRICADE TYPE III
- CLOSURE AREA
- DIRECTION OF TRAFFIC
- JACKSON BOULEVARD DETOUR ROUTE

SCALE: 1" = 150'

**DETOUR SIGNS TYPE & SIZE**  
 (ALL SIGN COLORS SHALL BE ACCORDING TO THE LATEST EDITION OF THE MUTCD)

W20-3 48"x48" \*  
 W20-3 48"x48"  
 W20-2 48"x48"  
 M4-9L 30"x24"  
 M4-9R 30"x24"  
 M4-10L 48"x18"  
 R11-2 48"x30"  
 M4-8A 24"x18"  
 R11-4 60"x30"  
 M3-2 24" X 12"  
 M5-1R 30"x24"  
 M5-1L 30"x24"  
 M3-4 \* 24" X 12"  
 TO BE PAID FOR AS TEMPORARY INFORMATION SIGNING

**SIGN ASSEMBLIES**

- TRAFFIC CONTROL NOTES**
1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO ANY CLOSURE OF JACKSON BOULEVARD.
  2. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
  3. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE SIGN COVERS SHALL BE REMOVED AT THE SAME TIME AS THE DETOUR ROUTE SIGNS ARE REMOVED OR COVERED.
  4. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARDS 701901.
  5. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
  6. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR. WHEN SIGNS ARE POST-MOUNTED IN THE GROUND THEY SHALL BE INSTALLED PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901 UNLESS OTHERWISE APPROVED BY THE ENGINEER.



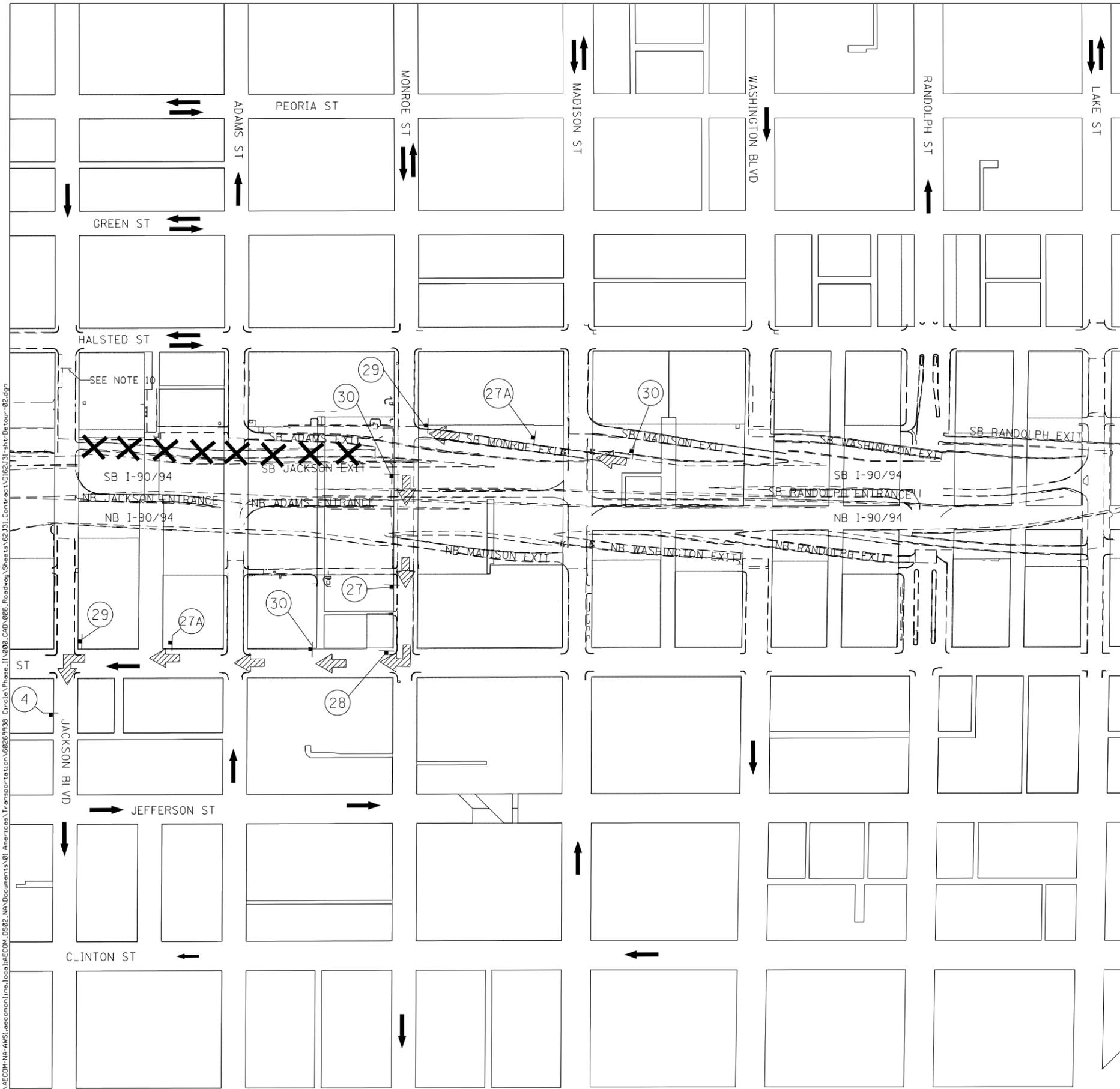
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PLOT SCALE = 300.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 8/13/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN  
 JACKSON BOULEVARD BRIDGE**

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 77
CONTRACT NO. 62J31				ILLINOIS FED. AID PROJECT



**LEGEND**

- TEMPORARY SIGN
- SIGN BARRICADE TYPE III
- DIRECTION OF TRAFFIC
- SOUTHBOUND JACKSON BLVD EXIT DETOUR ROUTE
- CLOSURE AREA

SCALE: 1" = 150'

**DETOUR SIGNS TYPE & SIZE**  
 (ALL SIGN COLORS SHALL BE ACCORDING TO THE LATEST EDITION OF THE MUTCD)

END DETOUR M4-8A 24"x18"

EAST M3-2 24" X 12"

JACKSON BOULEVARD \* CUSTOM SPECIAL 6" UPPERCASE BLACK LETTERS ON ORANGE BACKGROUND 30"x18"

DETOUR M4-8 (24"x12")

M5-1 (21"x15") M6-3 (21"x15") M6-1 (21"x15")

TO BE PAID AS TEMPORARY INFORMATION SIGNING \*

SIGN NEEDED ONLY WHILE RAMP SE DETOUR IS ALSO IN PLACE UNDER CONTRACTS 62J31 AND 62A77 \*\*

**SIGN ASSEMBLIES**

4 END DETOUR JACKSON BOULEVARD \* 27 EAST JACKSON BLVD DETOUR 27A EAST JACKSON BLVD DETOUR 28 EAST JACKSON BLVD DETOUR 29 EAST JACKSON BLVD DETOUR 30 EAST JACKSON BLVD DETOUR

- TRAFFIC CONTROL NOTES**
1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO THE CLOSURE OF THE JACKSON BOULEVARD EXIT RAMP.
  2. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
  3. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE SIGN COVERS SHALL BE REMOVED AT THE SAME TIME AS THE DETOUR ROUTE SIGNS ARE REMOVED.
  4. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARDS 701901.
  5. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
  6. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR. WHEN SIGNS ARE POST-MOUNTED IN THE GROUND THEY SHALL BE INSTALLED PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901 UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  7. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONTRACTOR ACCESS IS NO LONGER REQUIRED ON THE JACKSON BOULEVARD EXIT RAMP AND THE RAMP IS REOPENED TO TRAFFIC.
  8. SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR ADDITIONAL INFORMATION AND ADVANCED RAMP CLOSURE SIGNING.
  9. RAMP CLOSURE SHALL FOLLOW THE LATEST VERSION OF THE IDOT HIGHWAY STANDARDS AND DISTRICT 1 STANDARD DETAIL TC-08.



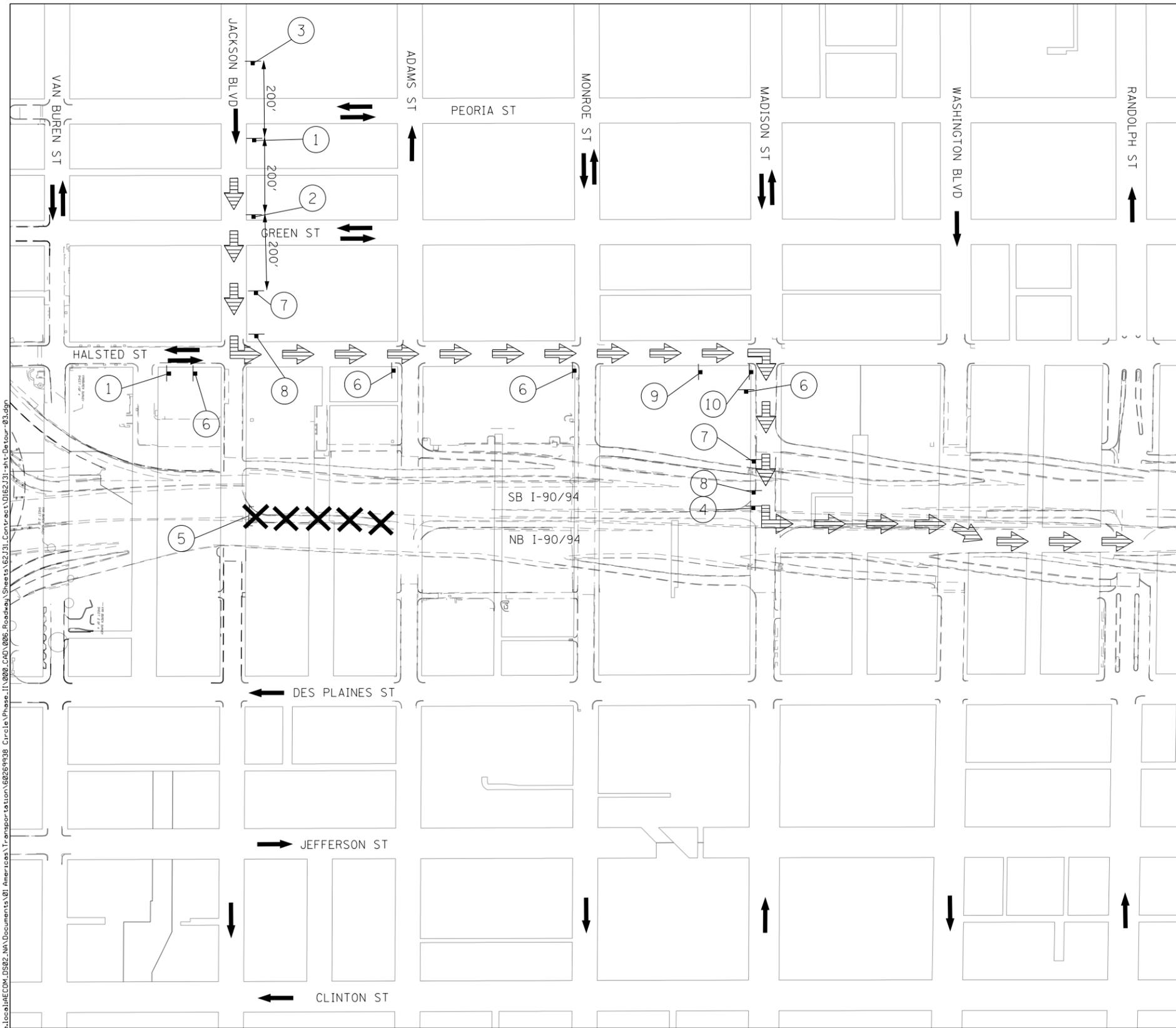
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PLOT DATE = 8/13/2019	DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**DETOUR PLAN**  
**JACKSON BOULEVARD EXIT RAMP**

SCALE: SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	78
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- TEMPORARY SIGN
- SIGN BARRICADE TYPE III
- DIRECTION OF TRAFFIC
- NORTHBOUND JACKSON BLVD ENTRANCE DETOUR ROUTE
- CLOSURE AREA

SCALE: 1" = 150'

**DETOUR SIGNS TYPE & SIZE**  
 (ALL SIGN COLORS SHALL BE ACCORDING TO THE LATEST EDITION OF THE MUTCD)

W20-2 (48"x48")	W20-30-48 (48"x48")	W20-1103-48 (48"x48")	R11-2 (48"x30")	M4-8A (24"x18")	M3-4 (24" X 12")
M4-8 (24"x12")	M5-1 (21"x15")	M6-3 (21"x15")	M6-1 (21"x15")	INTERSTATE 90	INTERSTATE 94
				M1-1 (24"x24")	

**SIGN ASSEMBLIES**

1	2	3	4	5
6	7	8	9	10

- TRAFFIC CONTROL NOTES**
1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO THE CLOSURE OF THE JACKSON BOULEVARD ENTRANCE RAMP.
  2. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
  3. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE SIGN COVERS SHALL BE REMOVED AT THE SAME TIME AS THE DETOUR ROUTE SIGNS ARE REMOVED.
  4. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARDS 701901.
  5. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
  6. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR. WHEN SIGNS ARE POST-MOUNTED IN THE GROUND THEY SHALL BE INSTALLED PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901 UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  7. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE ON THE JACKSON BOULEVARD ENTRANCE RAMP AND THE RAMP IS REOPENED TO TRAFFIC.
  8. SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR ADDITIONAL INFORMATION AND ADVANCED RAMP CLOSURE SIGNING.
  9. RAMP CLOSURE SHALL FOLLOW THE LATEST VERSION OF THE IDOT HIGHWAY STANDARDS AND DISTRICT 1 STANDARD DETAIL TC-08.



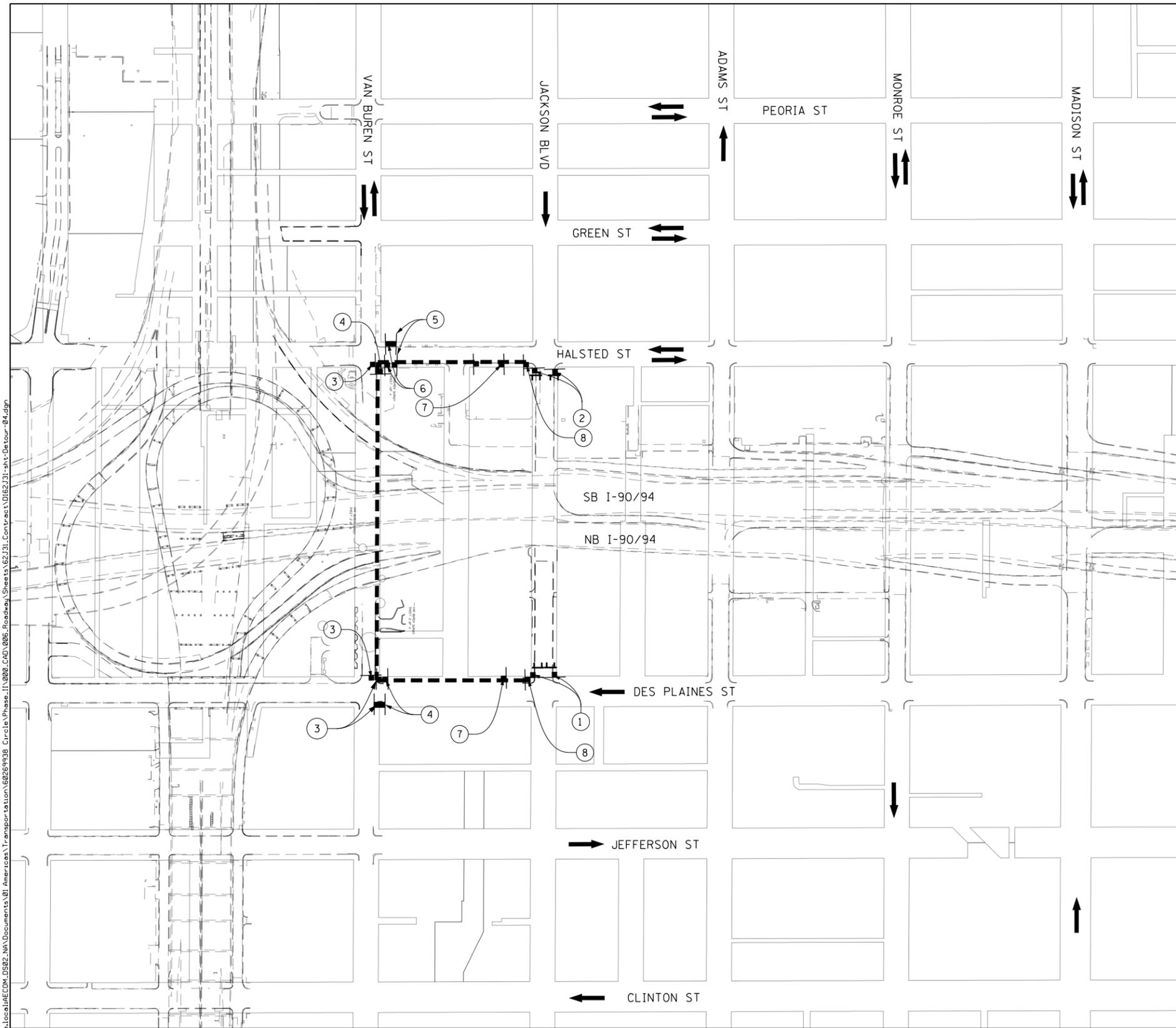
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PLOT DATE = 8/13/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETOUR PLAN JACKSON BOULEVARD ENTRANCE RAMP</b>	
SCALE:	SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	79
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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**LEGEND**

- TEMPORARY SIGN
- SIGN BARRICADE TYPE III
- ROADWAY DIRECTION
- DETOUR ROUTE

SCALE: 1" = 150'

**SIGNAGE SPECIFICATIONS:**

- SIDEWALK CLOSED:** R11-1101, 24" X 18"
- EAST:** M3-2, 24" X 12"
- WEST:** M3-4, 24" X 12"
- DETOUR:** M4-9b (R), 30" X 24"
- DETOUR:** M4-9b (L), 30" X 24"
- DETOUR:** M4-9, 30" X 24"
- END DETOUR:** M4-8A, 24" X 18"
- JACKSON BOULEVARD:** CUSTOM SPECIAL \* 6" UPPERCASE BLACK LETTERS ON ORANGE BACKGROUND 24" X 18"
- PEDESTRIAN:** W11-2, 30" X 30"

**DETOUR ROUTE SIGNAGE SEQUENCE:**

1. SIDEWALK CLOSED, DETOUR (Left)
2. SIDEWALK CLOSED, DETOUR (Right)
3. WEST JACKSON BOULEVARD, DETOUR (Left)
4. WEST JACKSON BOULEVARD, DETOUR (Right)
5. WEST JACKSON BOULEVARD, DETOUR (Left)
6. EAST JACKSON BOULEVARD, DETOUR (Right)
7. JACKSON BOULEVARD, PEDESTRIAN
8. END DETOUR, DETOUR (Up)

\* TO BE PAID FOR AS TEMPORARY INFORMATION SIGNING

**NOTES**

1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO CLOSING THE JACKSON BOULEVARD SIDEWALKS. THE PROPOSED DETOUR SHALL BE UTILIZED DURING ALL STAGES.
2. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
3. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE COVERS SHALL BE REMOVED AT THE TIME AT THE DETOUR ROUTE SIGNS ARE REMOVED.
4. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARDS 701801 AND 701901.
5. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
6. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR, AND SHALL BE POST-MOUNTED IN THE GROUND PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901.
7. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SIDEWALK IS REOPENED TO PEDESTRIANS.
8. THIS SHEET SHOWS A SCHEMATIC OF THE PEDESTRIAN DETOUR AND LOCATION OF THE DETOUR SIGNAGE.



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 DRAWN - JRM  
 CHECKED - JMG  
 DATE - 8/16/2019

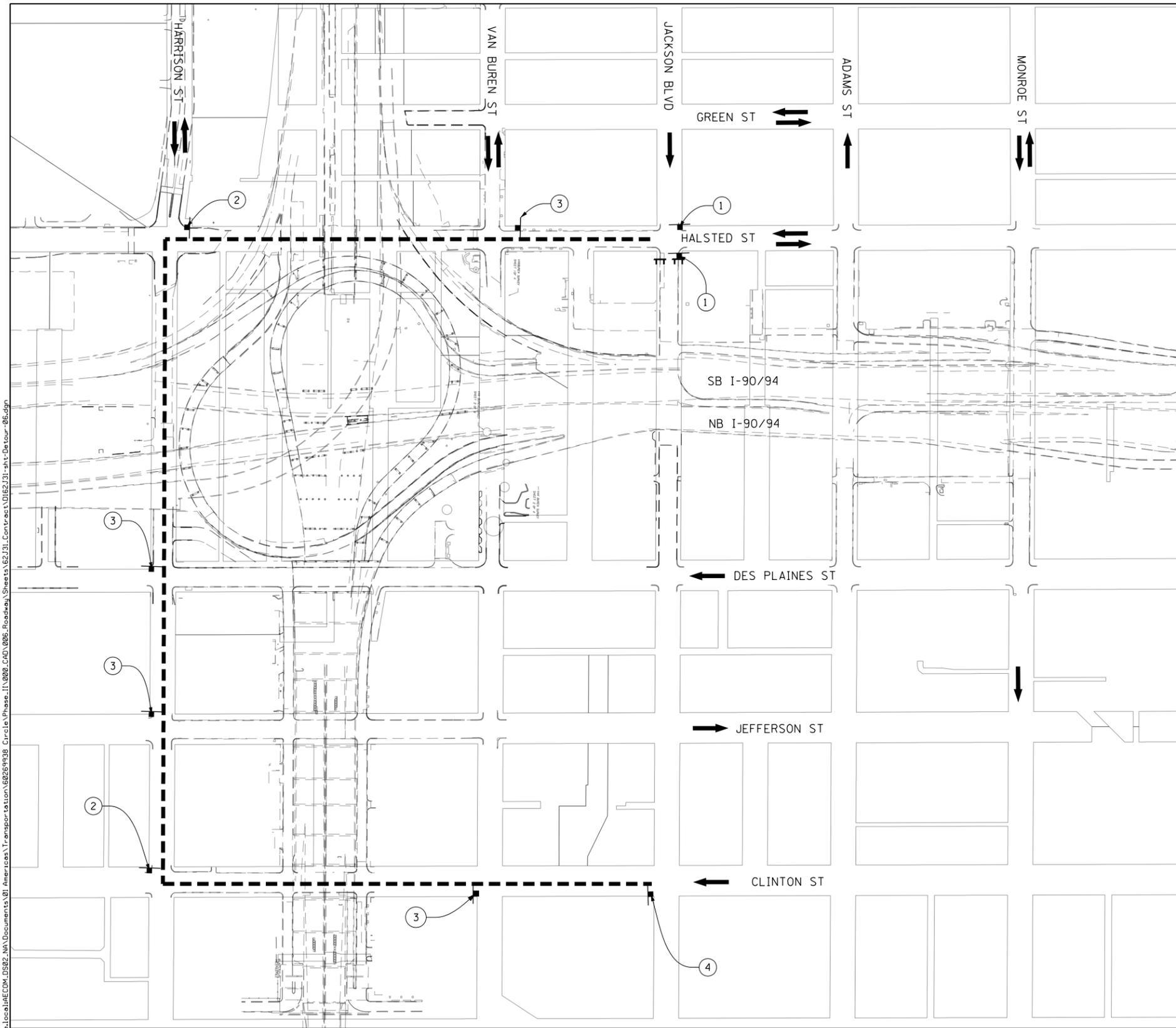
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN DETOUR PLAN  
 JACKSON BOULEVARD**

SCALE: SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	80
CONTRACT NO. 62J31			ILLINOIS FED. AID PROJECT	



**LEGEND**

- TEMPORARY SIGN
- SIGN BARRICADE TYPE III
- ROADWAY DIRECTION
- DETOUR ROUTE

0 150 300  
SCALE: 1" = 150'

**DETOUR SIGNS TYPE & SIZE**  
(ALL SIGN COLORS SHALL BE ACCORDING TO THE LATEST EDITION OF THE MUTCD)

EAST M3-2  
24" X 12"

DETOUR M4-9c (R)  
30" X 24"

DETOUR M4-9c (L)  
30" X 24"

DETOUR M4-9  
30" X 24"

END DETOUR M4-8A  
24" X 18"

JACKSON BOULEVARD  
CUSTOM SPECIAL \*  
6" UPPERCASE BLACK  
LETTERS ON ORANGE  
BACKGROUND  
24"x18"

W11-1  
30" X 30"

**SIGN ASSEMBLIES**

①

②

③

④

- \* TO BE PAID FOR AS TEMPORARY INFORMATION SIGNING
- NOTES**
1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO CLOSING THE JACKSON BOULEVARD BIKE LANE. THE PROPOSED DETOUR SHALL BE UTILIZED DURING ALL STAGES.
  2. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
  3. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE COVERS SHALL BE REMOVED AT THE TIME AT THE DETOUR ROUTE SIGNS ARE REMOVED.
  4. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARD 701901.
  5. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
  6. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR, AND SHALL BE POST-MOUNTED IN THE GROUND PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901.
  7. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE BIKE LANE IS REOPENED TO CYCLISTS.



FILE PATH = p:\NECOM\NA-NV\electrom\local\I-90\DS02\NA\Documents\01_Americas\Transportation\60269938_Circle\Phase-1\000_CAD\006_Roadway_Sheets\62J31_Contract\162J31-sht-Detour-06.dgn	DESIGNED - MJL	REVISED -
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PLOT DATE = 8/15/2019	DATE - 8/16/2019	REVISED -

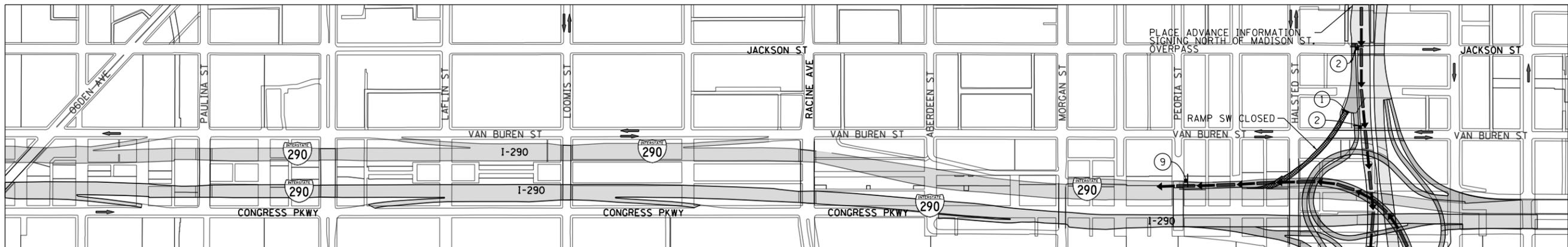
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BICYCLE DETOUR PLAN  
JACKSON BOULEVARD**

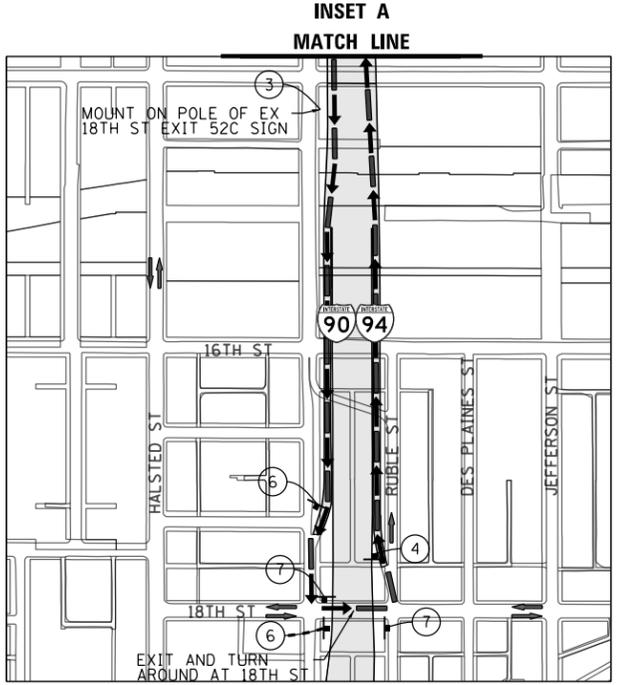
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	80A
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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- ### TRAFFIC CONTROL NOTES
1. PRIMARY DETOUR ROUTE: CONTINUE ON I-90/94 E TO 18TH ST, RETURN NORTH ON I-90/94 W, EXITING AND RE-ENTERING AT ROOSEVELT RD.
  2. DETOUR SIGNING MUST BE INSTALLED PRIOR TO CLOSURE RAMP SW.
  3. RAMP CLOSURE SHALL FOLLOW THE LATEST VERSION OF THE IDOT HIGHWAY STANDARDS AND DISTRICT 1 STANDARD DETAIL TC-08. SEE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS FOR ADDITIONAL INFORMATION.
  4. PLACEMENT AND SPACING OF SIGNS AND BARRICADES SHALL BE IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARD DETAIL TC-08 AND TC-21.
  5. ALL DETOUR SIGNS SHALL HAVE AN ORANGE BACKGROUND WITH BLACK LETTERS.
  6. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA. COORDINATE SIGN ASSEMBLY INSTALLATION LOCATIONS WITH OTHER DETOUR ROUTES UTILIZING THE SAME RAMPS AND STREETS.
  7. THE COST OF SUPPLYING, ERECTING AND MAINTAINING BARRICADES, WARNING LIGHTS AND STANDARD SIGNS ALONG ALL DETOUR ROUTES SHALL BE INCLUDED IN THE CONTRACT COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

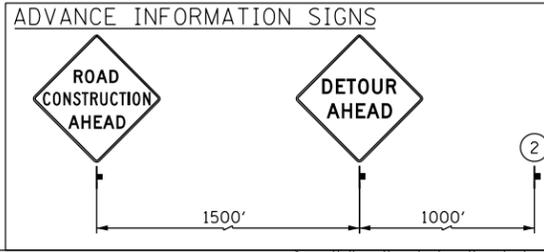
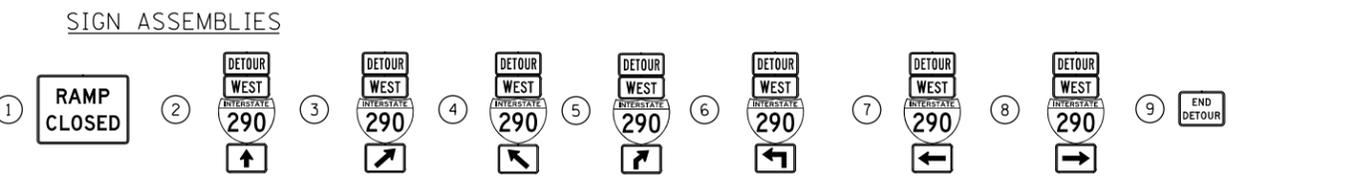


### DETOUR SIGNS TYPE & SIZE

(ALL SIGN COLORS SHALL BE ACCORDING TO THE LATEST EDITION OF THE MUTCD)

- |  |   |
|--|---|
|  | M1-1 (30"x24")  |
|  | M3-4 (24"x12")  |
|  | M4-8 (24"x12")  |
|  | M4-8A (24"x18")   |
|  | M5-1 (21"x15")  |
|  | M5-2 (21"x15")  |
|  | M6-1 (21"x15")  |
|  | M6-2 (21"x15")  |
|  | M6-3 (21"x15")  |
|  | W20-2 (48"x48")   |
|  | W20-30-48   |
|  | SEE TC-08 FOR SIGN INFORMATION BLACK LETTERS ON ORANGE BACKGROUND (48"x36") |

- ### LEGEND
- SUGGESTED PRIMARY ROUTE
  - TEMPORARY SIGN
  - EXISTING SIGN
  - EXISTING POLE
  - SIGN BARRICADE TYPE III
  - TWO-WAY STREET
  - ONE-WAY STREET
  - RAMP CLOSURE



D162J31-sht-Detour-05.dgn  
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 PLOT DATE = 8/15/2019

DESIGNED - PHP	REVISED -
DRAWN - PHP	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

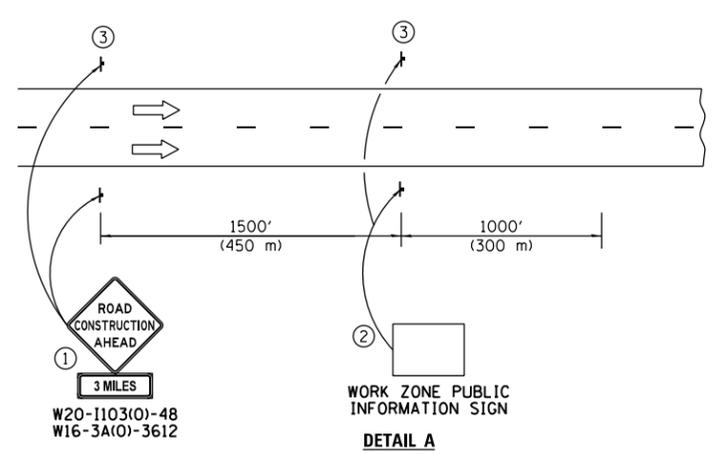
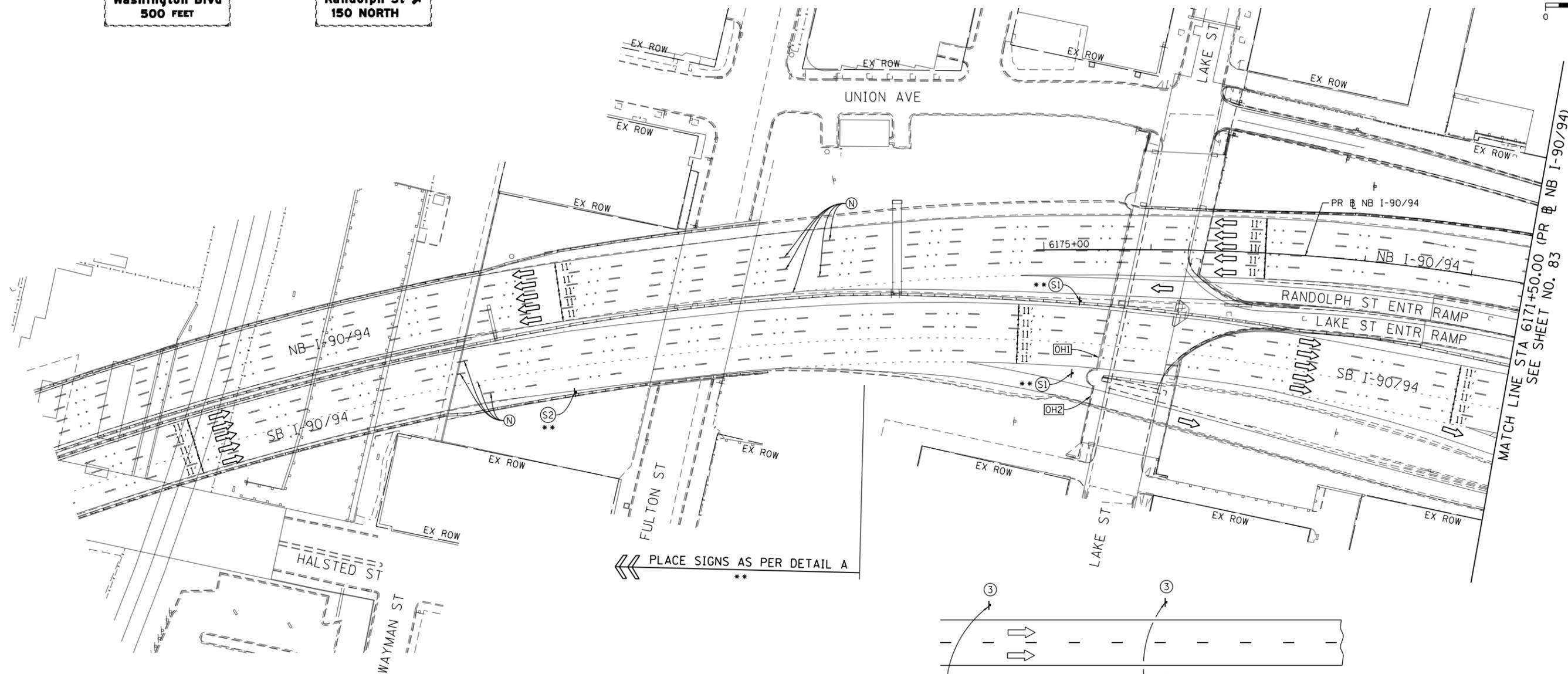
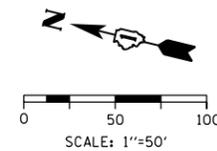
DETOUR PLAN  
 RAMP SW  
 SCALE: 1" = 300' SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 81
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

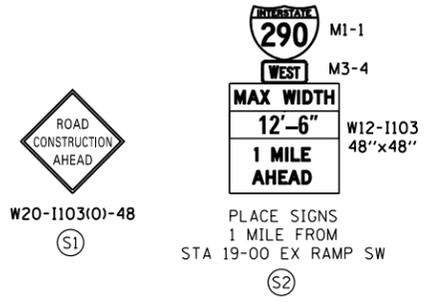
OH1 EXISTING OVERHEAD SIGN      OH2 EXISTING OVERHEAD SIGN

EXIT 51C  
**EAST**  
**Washington Blvd**  
**500 FEET**

EXIT 51B  
**WEST**  
**Randolph St**  
**150 NORTH**



- ① The Road Construction Ahead sign shall be located 3 to 5 miles in advance of the project limits.
- ② The message and size of the Work Zone Public Information Sign shall be as specified by the Department.
- ③ This sign shall be omitted when median width is less than 10' (3 m).



**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M) IMPACT ATTENUATOR, RELOCATE
(E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)		(N) EXISTING PAVEMENT MARKING



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

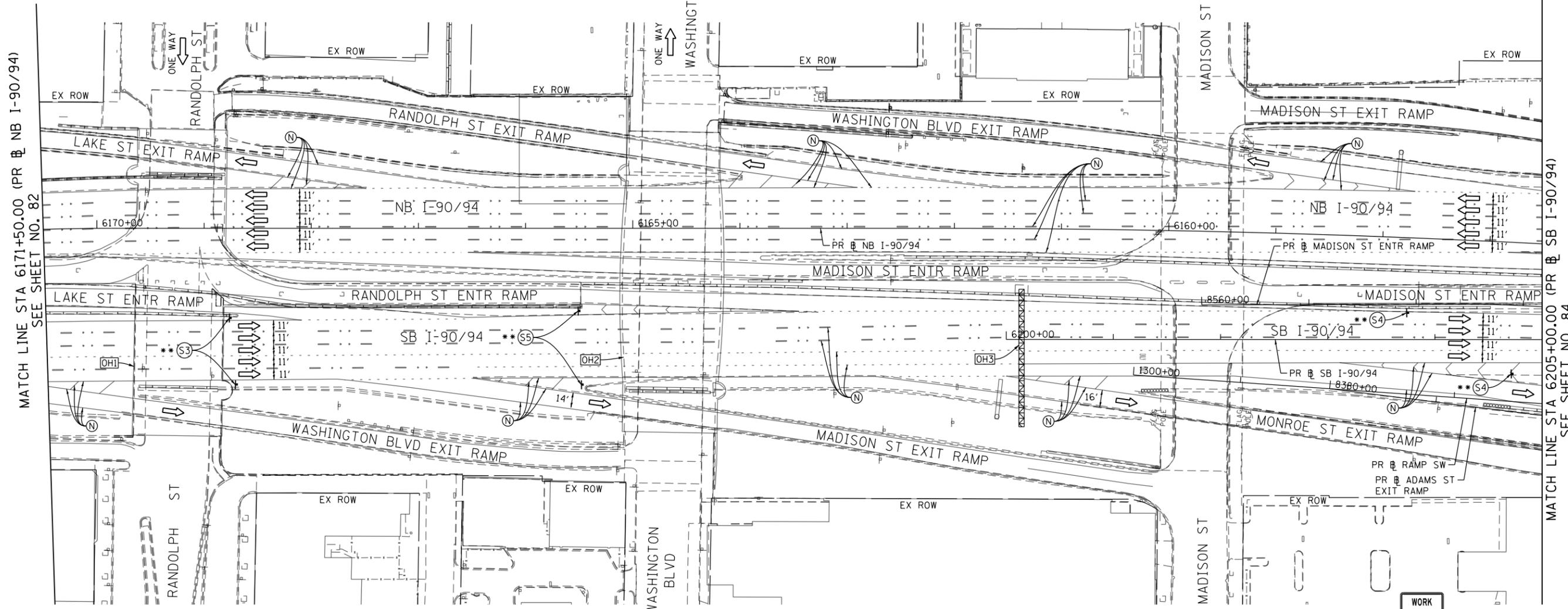
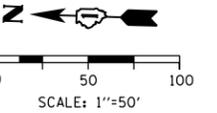
**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**

**STAGE 1**

SCALE: 1"=50'	SHEET 1 OF 4 SHEETS	STA. TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	82
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

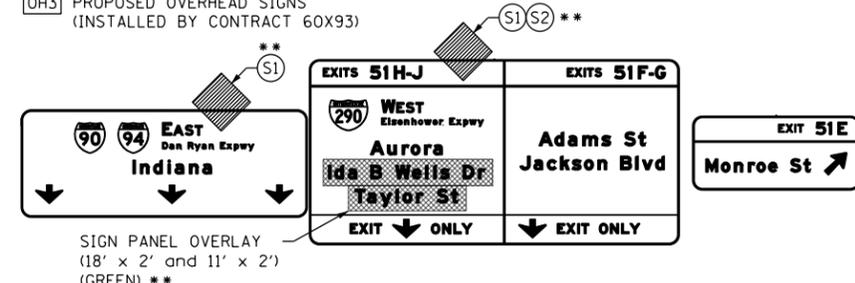
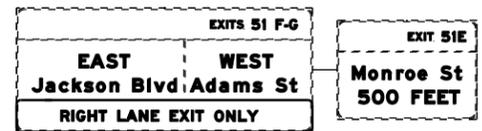
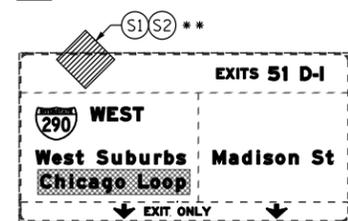
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[OH1] EXISTING OVERHEAD SIGN

[OH2] EXISTING OVERHEAD SIGNS

[OH3] PROPOSED OVERHEAD SIGNS (INSTALLED BY CONTRACT 60X93)



W21-1115(0)-3618  
R2-1-3648  
R10-1108P-3618  
R2-1106P-3618  
(S5)

**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- |   |  |  |
|---|--|--|
| (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)              | (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)                            | (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED |
| (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)             | (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)          | (K) PINNING TEMPORARY CONCRETE BARRIER   |
| (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)   | (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS  | (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3                                       |
| (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE) | (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED | (M) IMPACT ATTENUATOR, RELOCATE  |
| (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)              |  | (N) EXISTING PAVEMENT MARKING  |



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

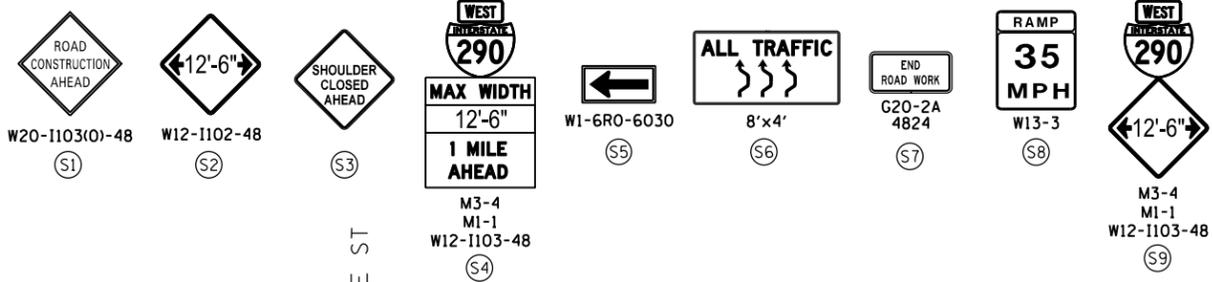
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
SB I-90/94 - STAGE 1

SCALE: 1"=50' SHEET 2 OF 4 SHEETS STA. 6171+50(NB) TO STA. 6205+00(SB)

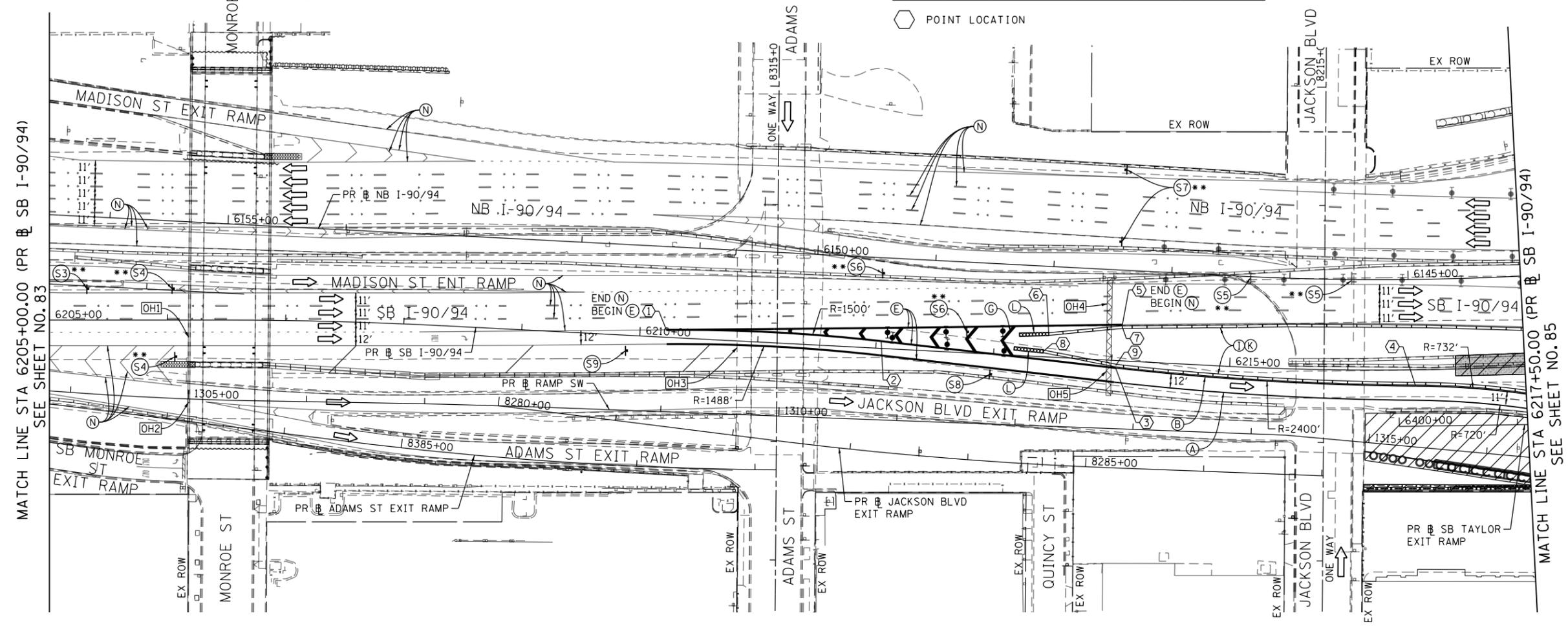
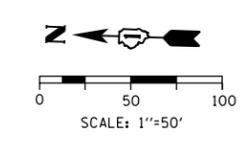
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90/94/290	2019-054-I	COOK	400	83
CONTRACT NO. 62J31				

ILLINOIS FED. AID PROJECT

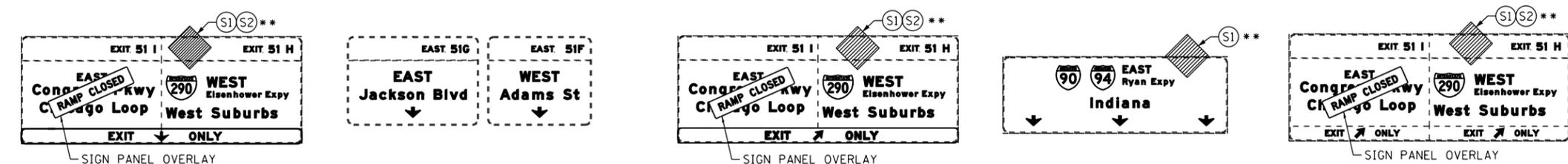
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POINT	STATION	OFFSET	LT/RT	BASELINE
1	PC 6210+22.33	6.26'	LT	P-KDR-SB
2	PT 6212+04.23	6.10'	LT	P-KDR-SB
3	PC 6214+03.60	6.18'	RT	P-KDR-SB
4	PRC 6216+57.21	13.62'	RT	P-KDR-SB
5	6214+07.47	30.54'	LT	P-KDR-SB
6	6213+45.75	19.51'	LT	P-KDR-SB
7	6214+07.60	28.50'	LT	P-KDR-SB
8	6213+41.85	4.63'	LT	P-KDR-SB
9	6214+03.72	4.19'	RT	P-KDR-SB



[OH1] EXISTING OVERHEAD SIGN TO REMAIN    [OH2] EXISTING OVERHEAD SIGNS TO REMAIN    [OH3] EXISTING OVERHEAD SIGNS TO REMAIN    [OH4] EXISTING OVERHEAD SIGN    [OH5] EXISTING OVERHEAD SIGNS TO REMAIN



LEGEND	
	WORK ZONE
	TEMPORARY CONCRETE BARRIER
	TEMPORARY IMPACT ATTENUATOR
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
	DIRECTION OF TRAFFIC FLOW
	TYPE III BARRICADE
	ARROW BOARD
	SIGN
	TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY PAVEMENT
*	FROM PREVIOUS STAGE TO REMAIN
**	FROM CONTRACT 60X93 TO REMAIN
***	FROM CONTRACT 60X79 TO REMAIN
****	FROM CONTRACT 62A76

PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES		
(A)	TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B)	TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C)	TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D)	TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(M) IMPACT ATTENUATOR, RELOCATE
(E)	TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)	(N) EXISTING PAVEMENT MARKING
(F)	TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	
(G)	TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	
(H)	TEMP EPOXY PVT MK LETTERS AND SYMBOLS	
(I)	TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	



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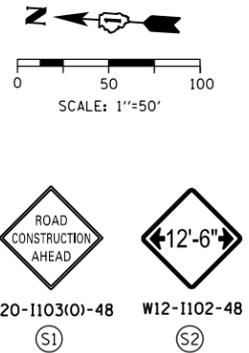
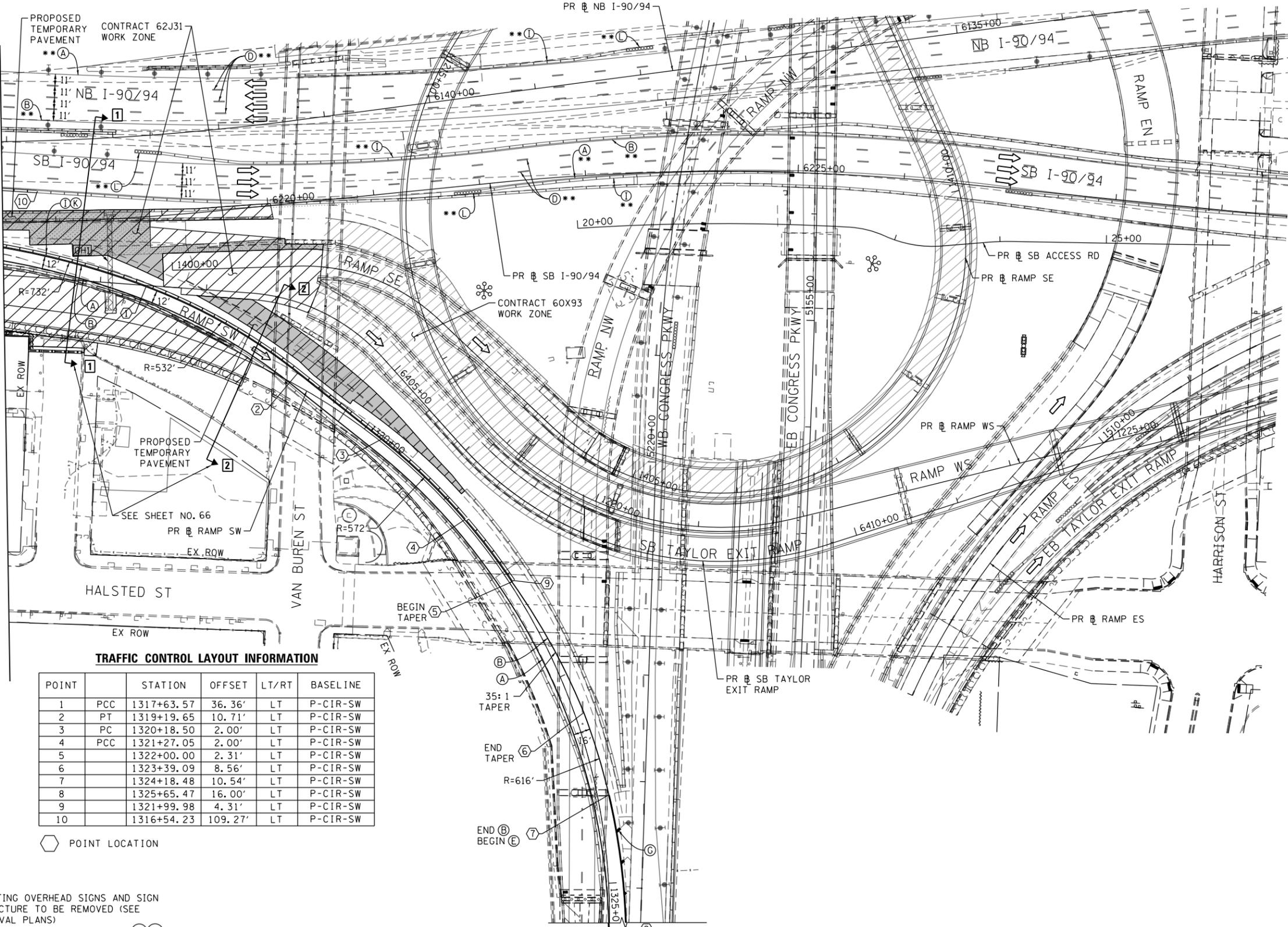
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
 STAGE 1  
 SCALE: 1"=50'    SHEET 3 OF 4 SHEETS    STA. 6205+00(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	84
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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MATCH LINE STA 6217+50.00 (PR @ SB I-90/94)  
SEE SHEET NO. 84

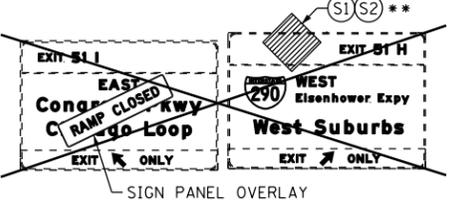


**TRAFFIC CONTROL LAYOUT INFORMATION**

POINT		STATION	OFFSET	LT/RT	BASELINE
1	PCC	1317+63.57	36.36'	LT	P-CIR-SW
2	PT	1319+19.65	10.71'	LT	P-CIR-SW
3	PC	1320+18.50	2.00'	LT	P-CIR-SW
4	PCC	1321+27.05	2.00'	LT	P-CIR-SW
5		1322+00.00	2.31'	LT	P-CIR-SW
6		1323+39.09	8.56'	LT	P-CIR-SW
7		1324+18.48	10.54'	LT	P-CIR-SW
8		1325+65.47	16.00'	LT	P-CIR-SW
9		1321+99.98	4.31'	LT	P-CIR-SW
10		1316+54.23	109.27'	LT	P-CIR-SW

POINT LOCATION

[OH1] EXISTING OVERHEAD SIGNS AND SIGN STRUCTURE TO BE REMOVED (SEE REMOVAL PLANS)



- LEGEND**
- [Hatched Box] WORK ZONE
  - [Dashed Line] TEMPORARY CONCRETE BARRIER
  - [Solid Line] TEMPORARY IMPACT ATTENUATOR
  - [Wavy Line] TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
  - [Arrow] DIRECTION OF TRAFFIC FLOW
  - [Type III Barricade] TYPE III BARRICADE
  - [Arrow Board] ARROW BOARD
  - [Sign] SIGN
  - [Type II Barricade] TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
  - [Directional Barricade] DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
  - [Grey Box] TEMPORARY PAVEMENT
  - [\*] FROM PREVIOUS STAGE TO REMAIN
  - [\*\*] FROM CONTRACT 60X93 TO REMAIN
  - [\*\*\*] FROM CONTRACT 60X79 TO REMAIN
  - [\*\*\*\*] FROM CONTRACT 62A76

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (C) TEMP EPOXY PVT MK LINE 4 (10' DASH 30' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (SOLID WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (SOLID YELLOW)
- (H) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)
- (I) TEMP EPOXY PVT MK LETTERS AND SYMBOLS
- (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (K) PINNING TEMPORARY CONCRETE BARRIER
- (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
- (M) IMPACT ATTENUATOR, RELOCATE
- (N) EXISTING PAVEMENT MARKING



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CHECKED - MJE  
DATE - 8/16/2019

REVISED -  
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REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
**STAGE 1**  
SCALE: 1"=50' SHEET 4 OF 4 SHEETS STA. 6217+50(SB) TO STA. 6217+50(SB)

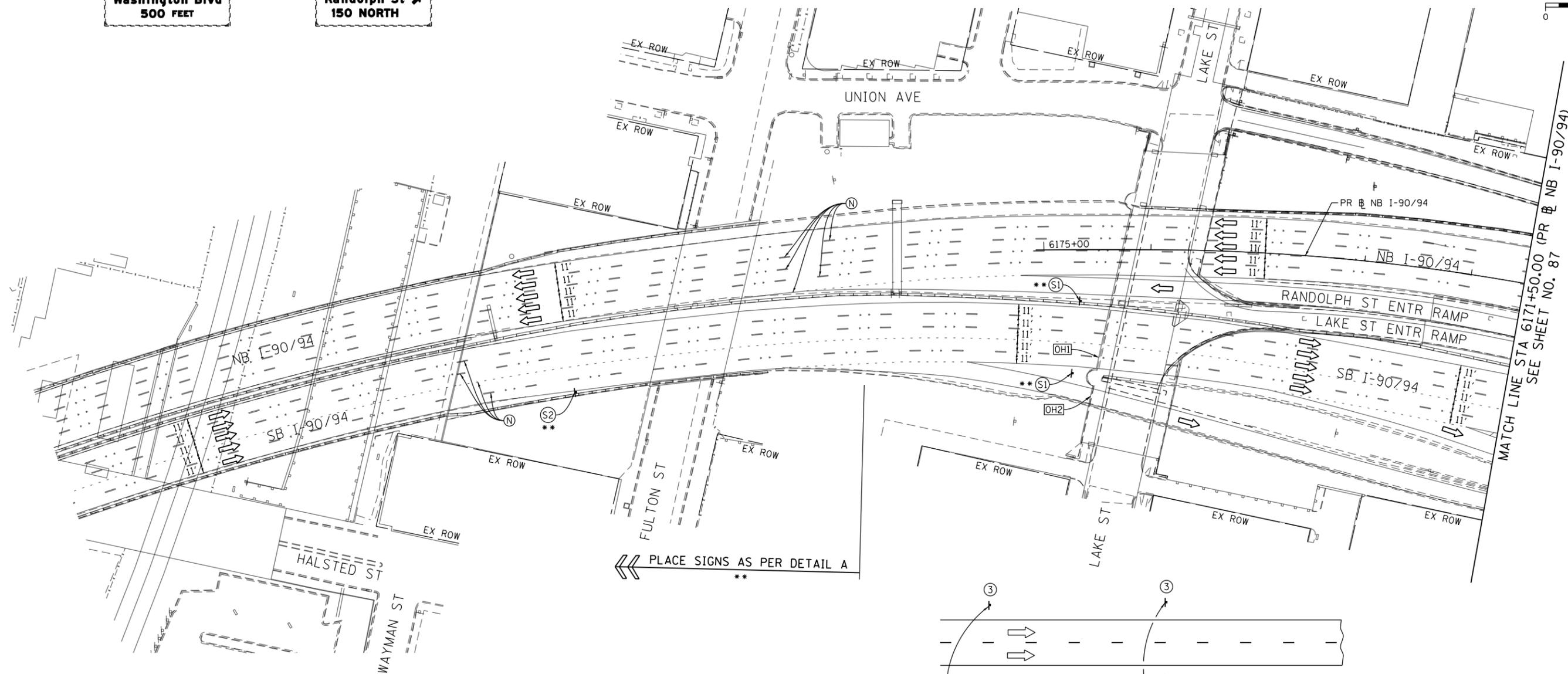
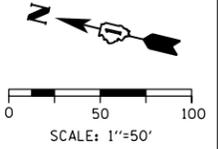
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	85

CONTRACT NO. 62J31  
ILLINOIS FED. AID PROJECT

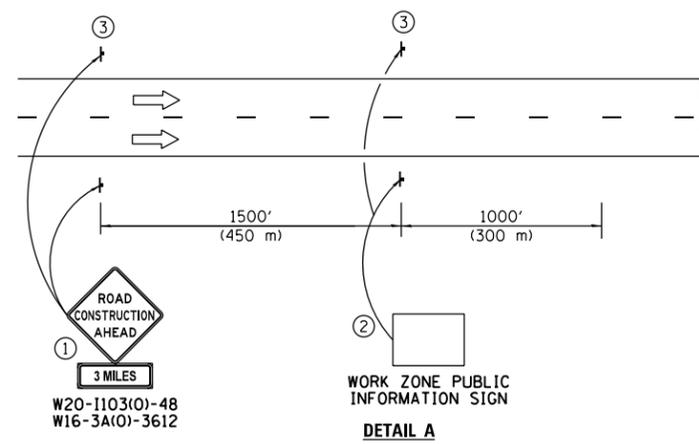
OH1 EXISTING OVERHEAD SIGN      OH2 EXISTING OVERHEAD SIGN

EXIT 51C  
**EAST**  
**Washington Blvd**  
**500 FEET**

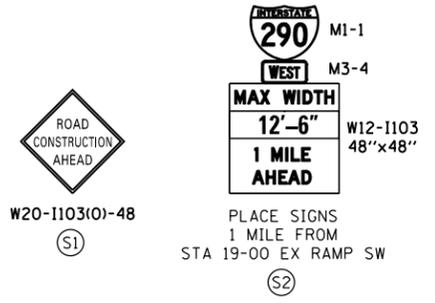
EXIT 51B  
**WEST**  
**Randolph St**  
**150 NORTH**



PLACE SIGNS AS PER DETAIL A  
 \*\*



- ① The Road Construction Ahead sign shall be located 3 to 5 miles in advance of the project limits.
- ② The message and size of the Work Zone Public Information Sign shall be as specified by the Department.
- ③ This sign shall be omitted when median width is less than 10' (3 m).



**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M) IMPACT ATTENUATOR, RELOCATE
(E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)		(N) EXISTING PAVEMENT MARKING

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PLOT DATE = 8/15/2019	DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS	DESIGNED - PHP	REVISED -
DEPARTMENT OF TRANSPORTATION	DRAWN - PHP	REVISED -
	CHECKED - MJE	REVISED -
	DATE - 8/16/2019	REVISED -

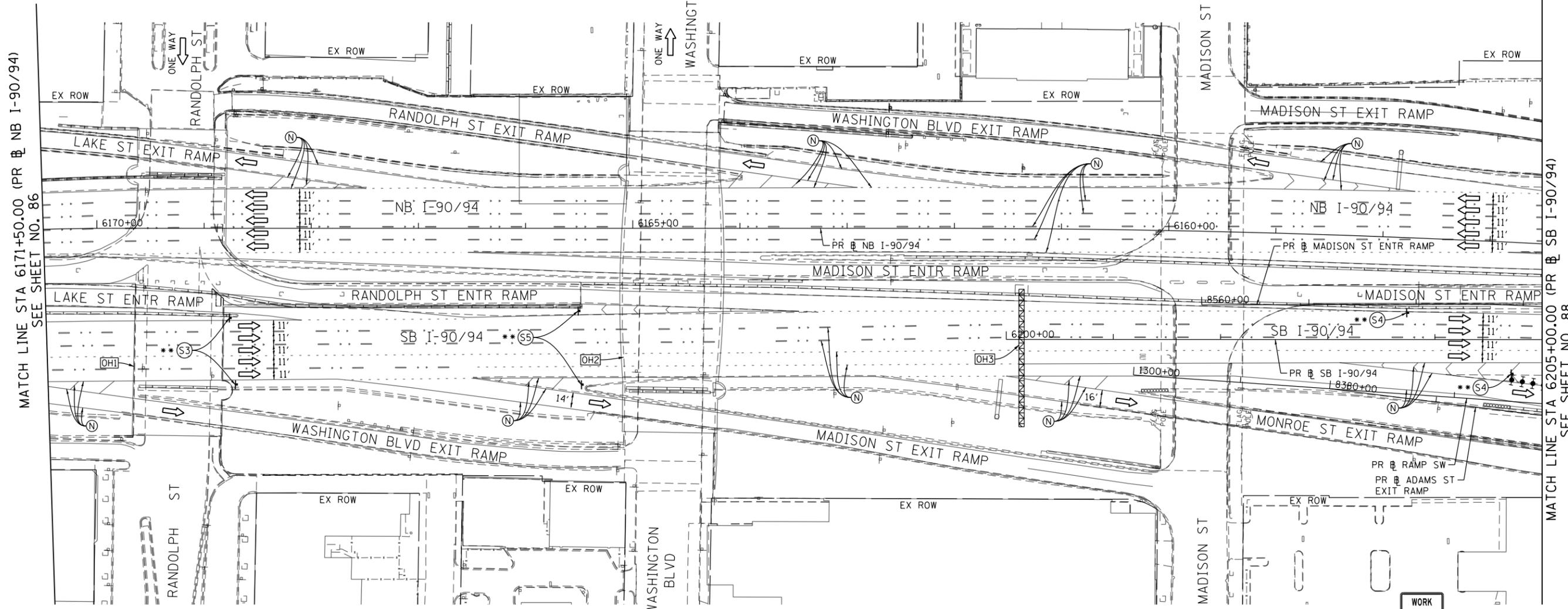
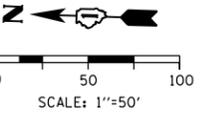
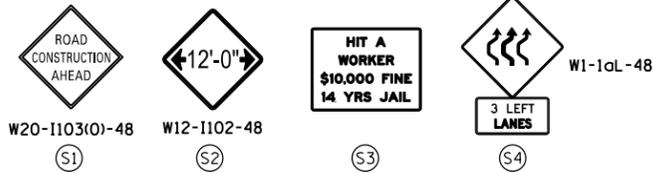
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**

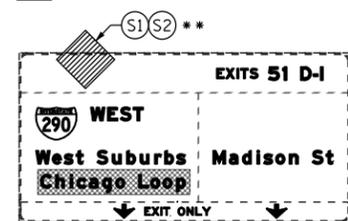
**STAGE 2**

SCALE: 1"=50'	SHEET 1 OF 4 SHEETS	STA. TO STA.
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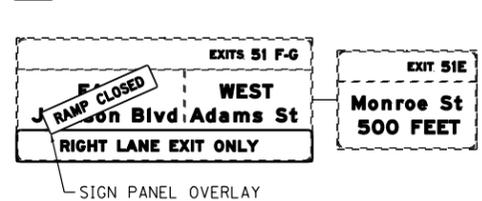
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	86
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



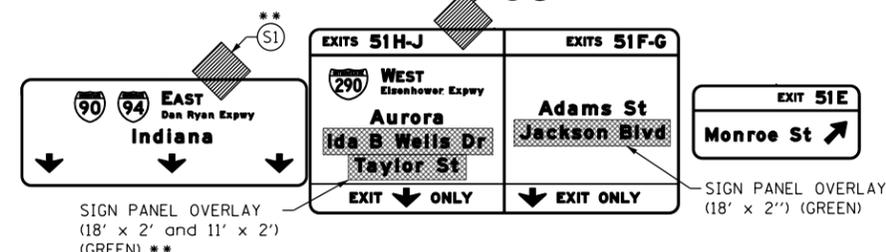
[OH1] EXISTING OVERHEAD SIGN



[OH2] EXISTING OVERHEAD SIGNS



[OH3] PROPOSED OVERHEAD SIGNS (INSTALLED BY CONTRACT 60X93)



**WORK ZONE**  
**SPEED LIMIT 45**  
 PHOTO ENFORCED  
 SXXX FINE MINIMUM

W21-1115(O)-3618  
 R2-1-3648  
 R10-1108P-3618  
 R2-1106P-3618  
 (S5)

**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A)	TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F)	TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J)	REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B)	TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G)	TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K)	PINNING TEMPORARY CONCRETE BARRIER
(C)	TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H)	TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L)	IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D)	TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I)	TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M)	IMPACT ATTENUATOR, RELOCATE
(E)	TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)	(N)	EXISTING PAVEMENT MARKING		



D162J31-SHT-Staging-ML2-02.dgn  
 USER NAME = ashok.kc  
 PLOT SCALE = 100.0000' / in.  
 PLOT DATE = 8/15/2019

DESIGNED - PHP	REVISED -
DRAWN - PHP	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

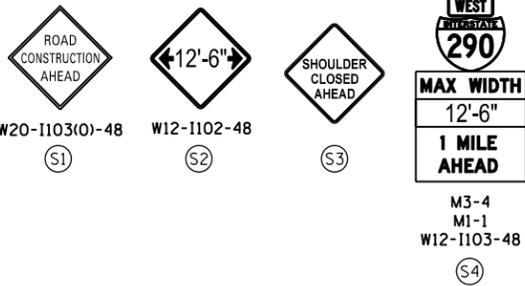
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
**STAGE 2**

SCALE: 1"=50'	SHEET 2 OF 4 SHEETS	STA. 6171+50(NB) TO STA. 6205+00(SB)
---------------	---------------------	--------------------------------------

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	87
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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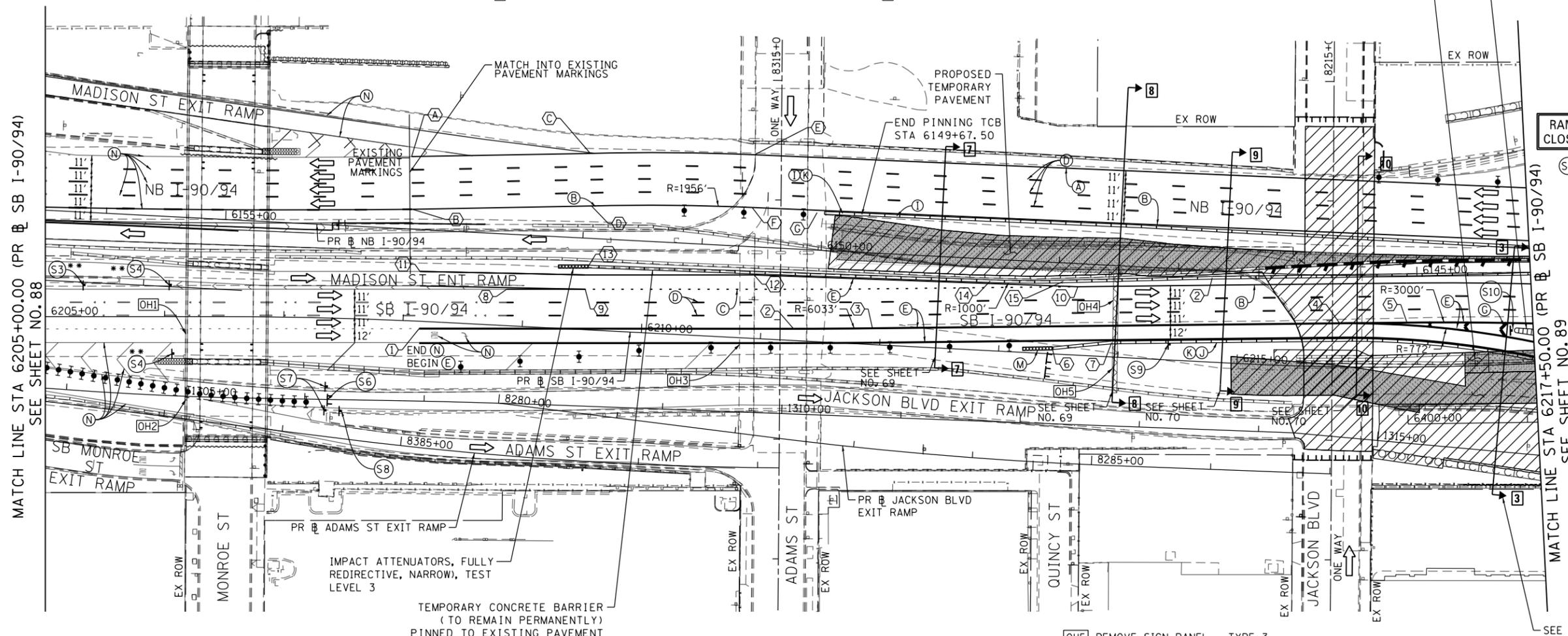
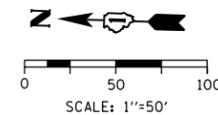


**TRAFFIC CONTROL LAYOUT INFORMATION (SB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
1	POT	6208+13.79	6.21'	RT	P-KDR-SB
2	PC	6211+27.36	9.87'	LT	P-KDR-SB
3	PT	6211+93.39	13.63'	LT	P-KDR-SB
4	POT/PC	6215+85.68	36.44'	LT	P-KDR-SB
5	PC	6216+43.01	37.27'	LT	P-KDR-SB
6		6213+44.82	4.71'	LT	P-KDR-SB
7		6213+94.26	73.04'	LT	P-KDR-SB
8		6208+85.36	30.46'	LT	P-KDR-SB

**TRAFFIC CONTROL LAYOUT INFORMATION (SB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
9		6209+51.09	33.84'	LT	P-KDR-NB
10		6213+63.53	57.32'	LT	P-KDR-NB
11		6208+12.29	41.57'	LT	P-KDR-NB
12	PI	6210+91.98	53.11'	LT	P-KDR-NB
13		6209+52.62	52.95'	LT	P-KDR-NB
14	PC	6212+89.77	58.18'	LT	P-KDR-NB
15	PT	6213+51.05	61.01'	LT	P-KDR-NB
16	PI	6214+75.69	70.61'	LT	P-KDR-NB



[OH1] EXISTING OVERHEAD SIGN TO REMAIN

[OH2] EXISTING OVERHEAD SIGNS TO REMAIN

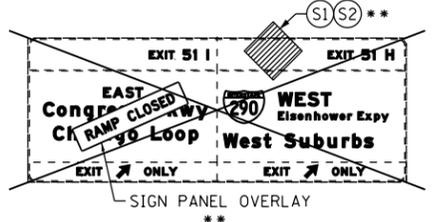
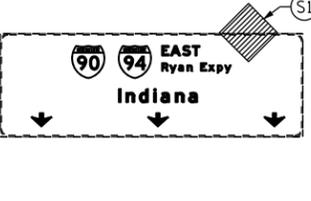
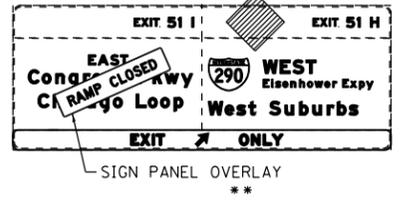
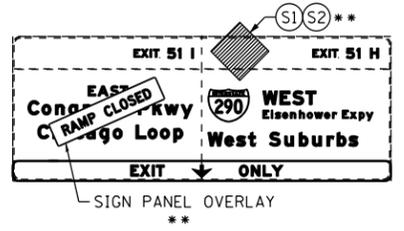
[OH3] EXISTING OVERHEAD SIGNS TO REMAIN

[OH4] EXISTING OVERHEAD SIGN

[OH5] REMOVE SIGN PANEL - TYPE 3  
INSTALL TEMPORARY INFORMATION SIGNING  
SIGN SB-TIS-01 WITH THE DOWN ARROW  
POINTING OVER LANE 4

**TRAFFIC CONTROL LAYOUT INFORMATION (NB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
A	POT	6153+46.97	60.55'	RT	P-KDR-NB
B	POT	6153+43.91	16.65'	RT	P-KDR-NB
C	PT	6151+97.48	70.99'	RT	P-KDR-NB
D	PT	6151+94.24	27.11'	RT	P-KDR-NB
E	PC	6150+60.02	76.94'	RT	P-KDR-NB
F	PC	6150+59.63	32.94'	RT	P-KDR-NB
G		6150+05.00	31.40'	RT	P-KDR-NB



**SIGN SB-TIS-01**



**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION		FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW		** FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE		*** FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD		
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)
- (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS
- (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (K) PINNING TEMPORARY CONCRETE BARRIER
- (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
- (M) IMPACT ATTENUATOR, RELOCATE
- (N) EXISTING PAVEMENT MARKING



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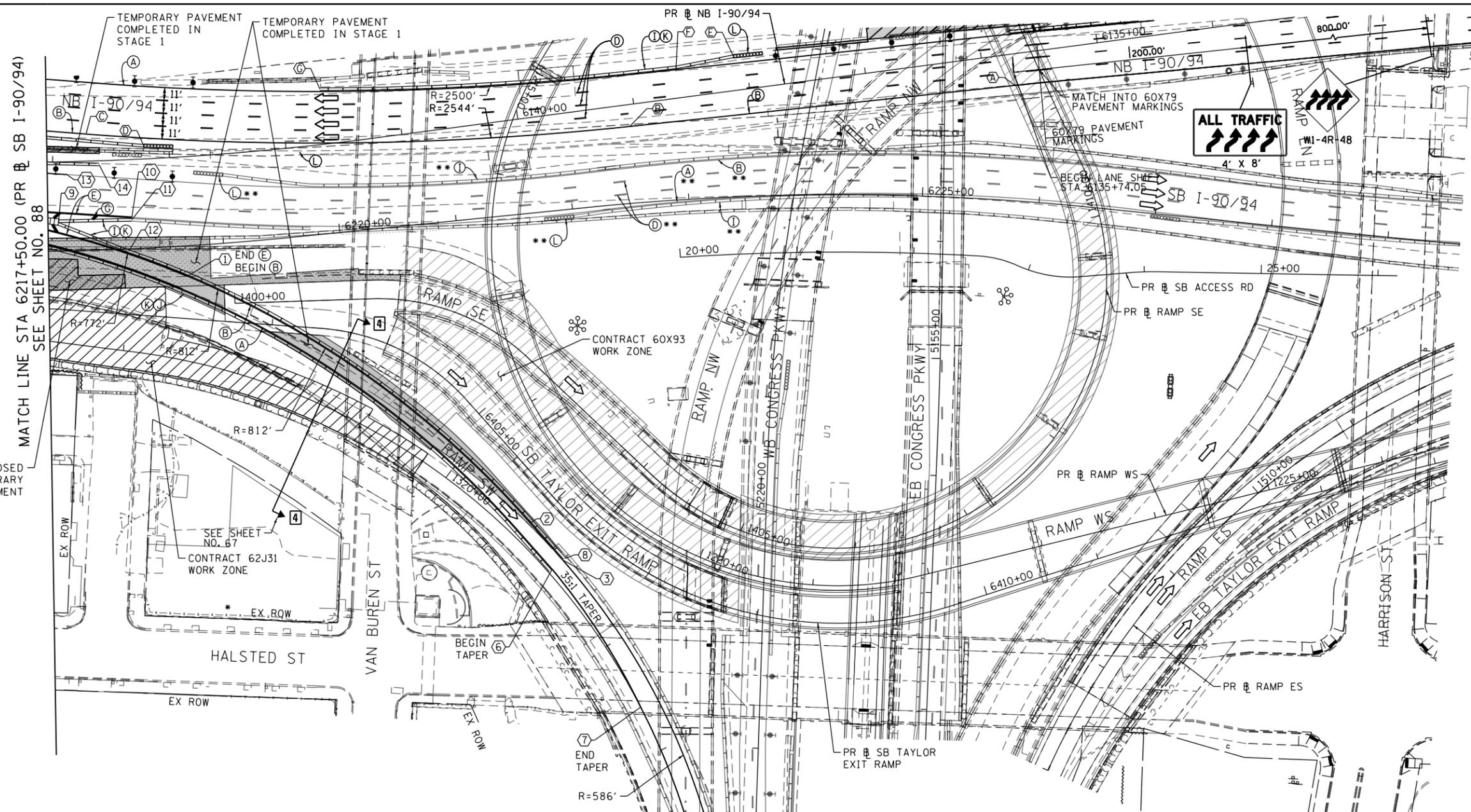
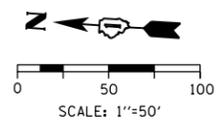
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 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
**STAGE 2**  
 SCALE: 1"=50' SHEET 3 OF 4 SHEETS STA. 6205+00(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	88

CONTRACT NO. 62J31  
 ILLINOIS FED. AID PROJECT



**TRAFFIC CONTROL LAYOUT INFORMATION (SB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
1	PCC	6218+67.49	25.88'	RT	P-KDR-SB
2	PT	1320+79.12	17.49'	LT	P-CIR-SW
3	PC	1321+19.65	16.00'	LT	P-CIR-SW
4	POC	1324+18.48	16.00'	LT	P-CIR-SW
5	PT	1325+39.06	16.00'	LT	P-CIR-SW
6	POC	1321+19.65	4.01'	LT	P-CIR-SW
7	POC	1322+59.45	0.0'	-	P-CIR-SW
8		1321+19.73	18.00'	LT	P-CIR-SW
9		6217+59.26	25.54'	LT	P-KDR-SB
10		6218+22.62	26.75'	LT	P-KDR-SB
11		6218+22.40	24.76'	LT	P-KDR-SB
12		6218+18.69	1.96'	RT	P-KDR-SB
13		6217+67.24	65.23'	LT	P-KDR-SB
14		6217+95.77	73.08'	LT	P-KDR-SB

**TRAFFIC CONTROL LAYOUT INFORMATION (NB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
A	POT	6135+74.32	26.95'	LT	P-KDR-NB
B	PC	6139+05.77	18.64'	LT	P-KDR-NB
C	PT	6143+70.73	23.11'	RT	P-KDR-NB
D		6143+21.53	11.25'	RT	P-KDR-NB
E		6138+10.10	29.15'	RT	P-KDR-NB
F		6138+60.00	26.23'	RT	P-KDR-NB
G		6141+65.38	47.14'	RT	P-KDR-NB

POINT LOCATION

**LEGEND**

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
- DIRECTION OF TRAFFIC FLOW
- TYPE III BARRICADE
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
- DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TEMPORARY PAVEMENT
- FROM PREVIOUS STAGE TO REMAIN
- FROM CONTRACT 60X93 TO REMAIN
- FROM CONTRACT 60X79 TO REMAIN
- FROM CONTRACT 62A76

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)
- (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS
- (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (K) PINNING TEMPORARY CONCRETE BARRIER
- (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
- (M) IMPACT ATTENUATOR, RELOCATE
- (N) EXISTING PAVEMENT MARKING

POINT LOCATION

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

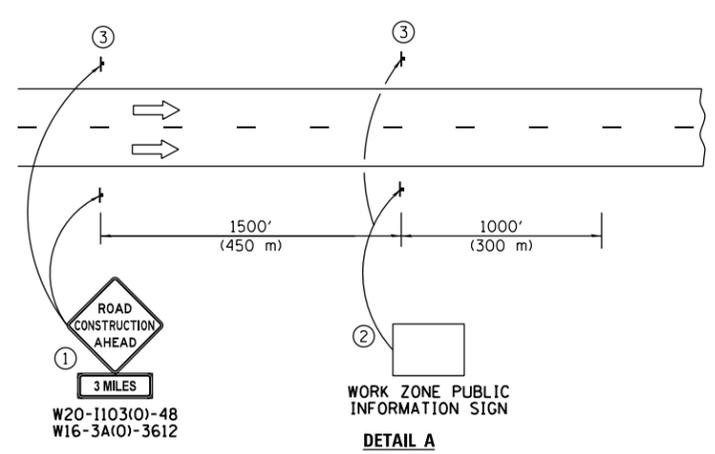
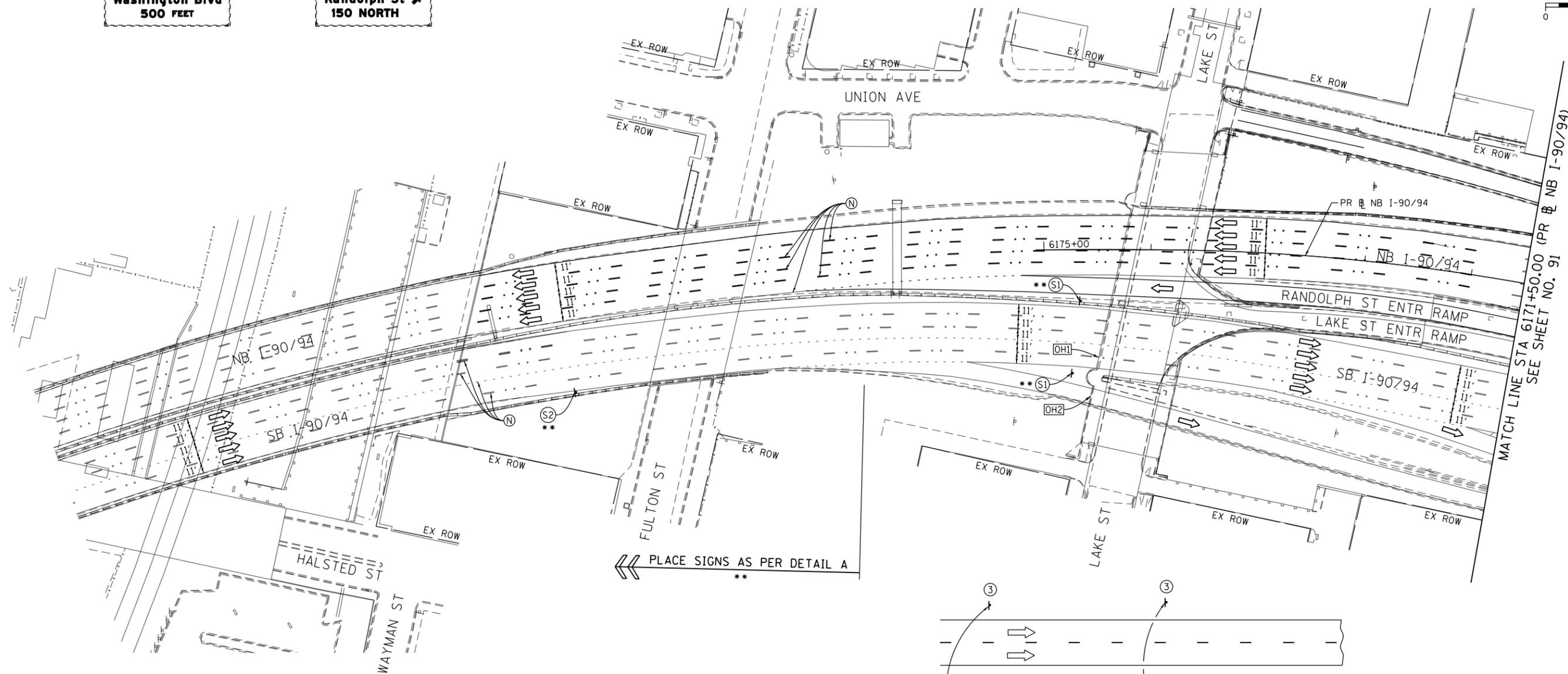
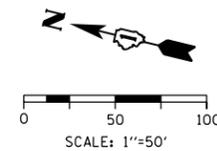
<b>SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN</b>		
<b>STAGE 2</b>		
SCALE: 1"=50'	SHEET 4 OF 4 SHEETS	STA. 6217+50(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	89
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

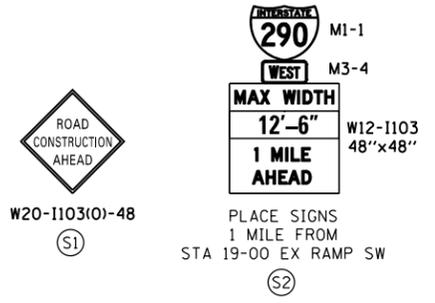
OH1 EXISTING OVERHEAD SIGN      OH2 EXISTING OVERHEAD SIGN

EXIT 51C  
**EAST**  
**Washington Blvd**  
**500 FEET**

EXIT 51B  
**WEST**  
**Randolph St**  
**150 NORTH**



- ① The Road Construction Ahead sign shall be located 3 to 5 miles in advance of the project limits.
- ② The message and size of the Work Zone Public Information Sign shall be as specified by the Department.
- ③ This sign shall be omitted when median width is less than 10' (3 m).



**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M) IMPACT ATTENUATOR, RELOCATE
(E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)		(N) EXISTING PAVEMENT MARKING

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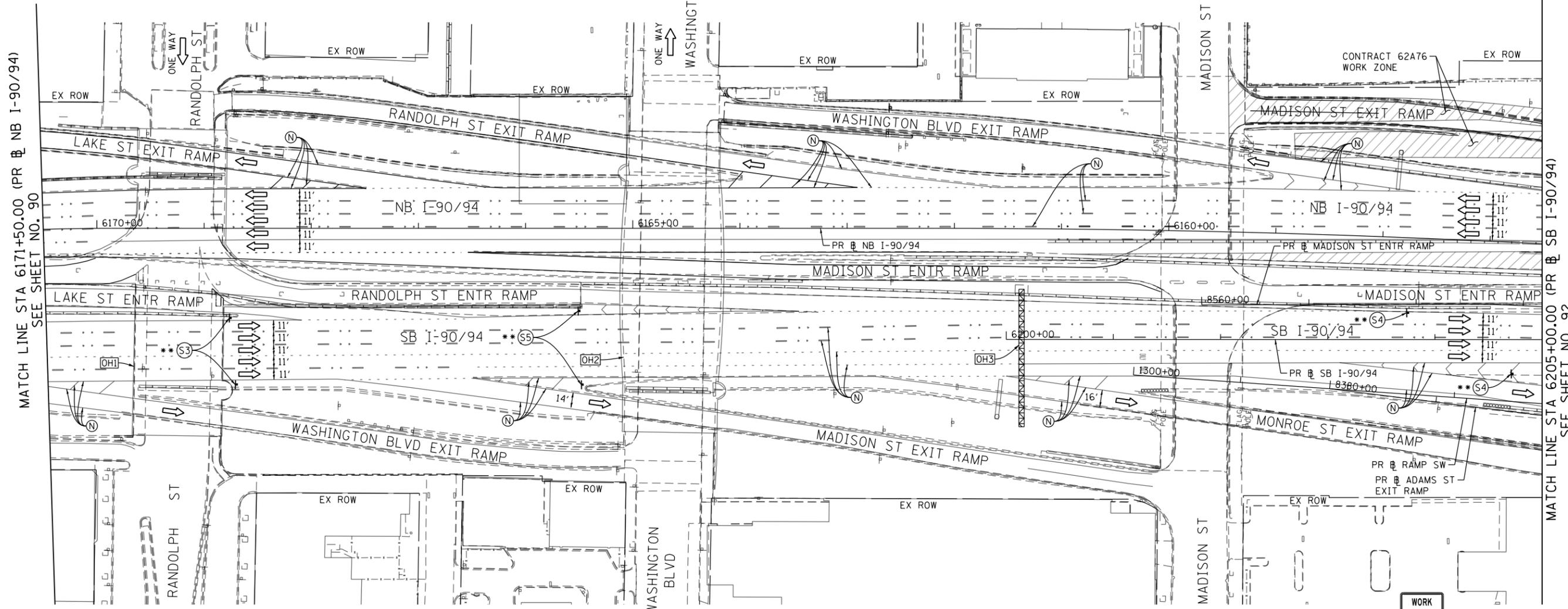
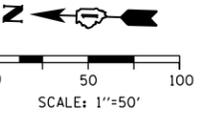
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**

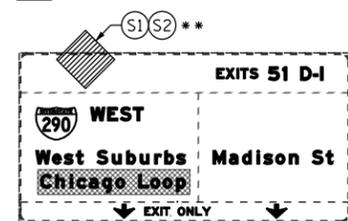
**STAGE 3**

SCALE: 1"=50'	SHEET 1 OF 4 SHEETS	STA. TO STA.
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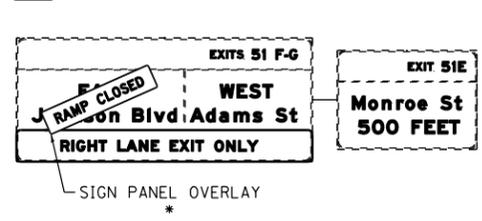
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CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



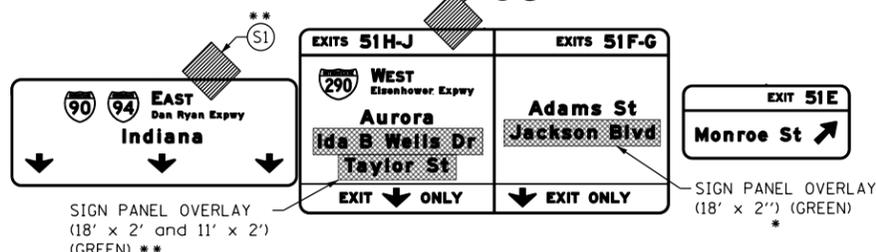
[OH1] EXISTING OVERHEAD SIGN



[OH2] EXISTING OVERHEAD SIGNS



[OH3] PROPOSED OVERHEAD SIGNS (INSTALLED BY CONTRACT 60X93)



W21-1115(0)-3618  
R2-1-3648  
R10-1108P-3618  
R2-1106P-3618  
(S5)

**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- |   |  |  |
|---|--|--|
| (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)              | (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)                            | (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED |
| (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)             | (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)          | (K) PINNING TEMPORARY CONCRETE BARRIER   |
| (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)   | (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS  | (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3                                       |
| (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE) | (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED | (M) IMPACT ATTENUATOR, RELOCATE  |
| (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)              |  | (N) EXISTING PAVEMENT MARKING  |



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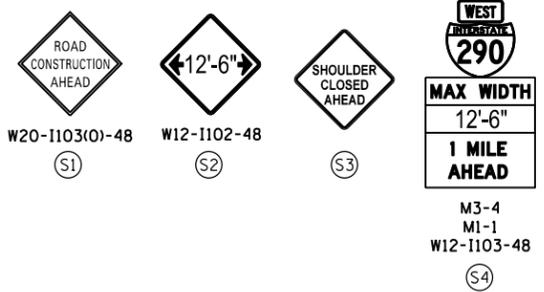
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DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
STAGE 3  
SCALE: 1"=50' SHEET 2 OF 4 SHEETS STA. 6171+50(NB) TO STA. 6205+00(SB)

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	91
CONTRACT NO. 62J31				

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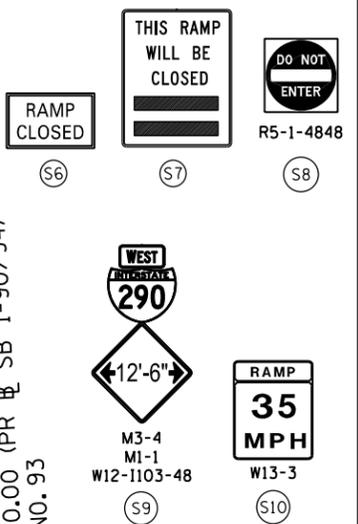
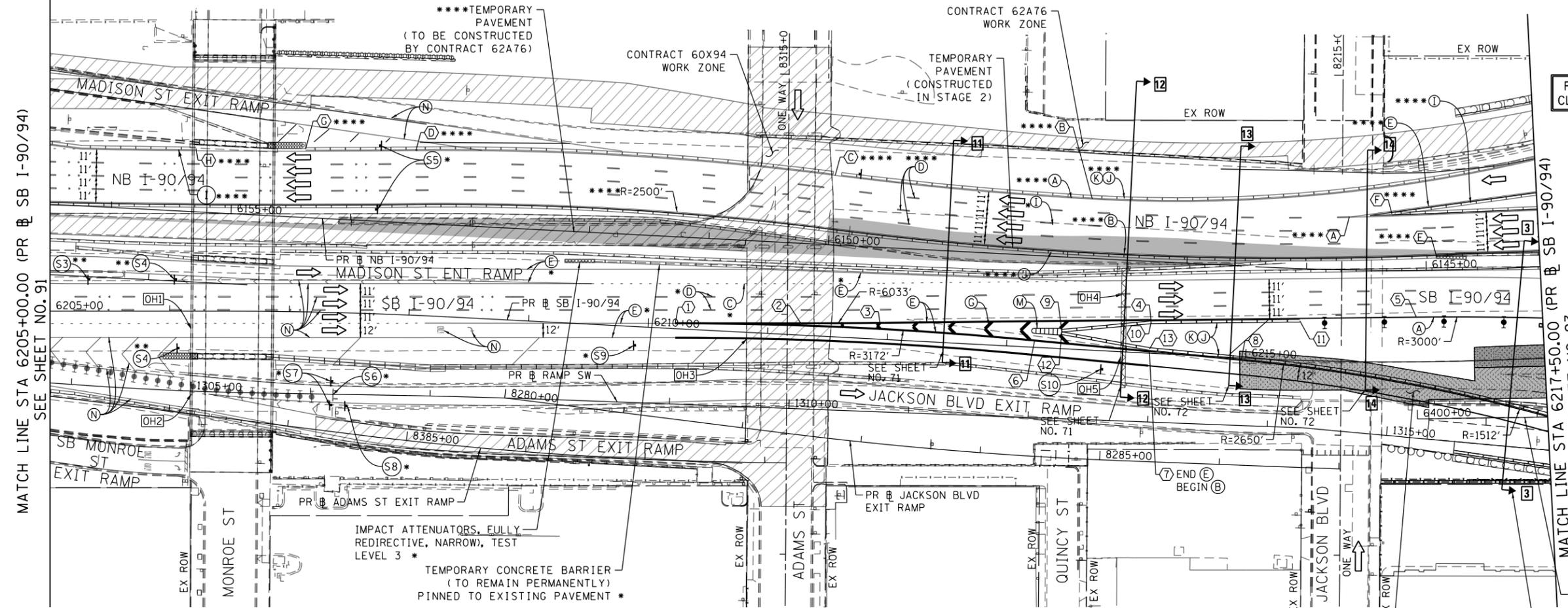
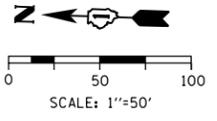


**TRAFFIC CONTROL LAYOUT INFORMATION (SB)**

POINT		STATION	OFFSET	LT/RT	BASELINE
1	PC	6210+22.44	4.47'	LT	P-KDR-SB
2	PC	6211+27.36	9.87'	LT	P-KDR-SB
3	PT	6211+93.39	13.63'	LT	P-KDR-SB
4	POT	6214+19.44	27.75'	LT	P-KDR-SB
5	PC	6216+43.01	37.27'	LT	P-KDR-SB
6	PT	6213+23.19	5.67'	LT	P-KDR-SB
7	POT	6214+19.38	1.49'	LT	P-KDR-SB

POINT		STATION	OFFSET	LT/RT	BASELINE
8	PC	6214+96.37	1.86'	RT	P-KDR-SB
9		6213+45.35	14.94'	LT	P-KDR-SB
10		6214+19.57	25.75'	LT	P-KDR-SB
11		6215+44.27	33.03'	LT	P-KDR-SB
12		6213+45.39	12.88'	LT	P-KDR-SB
13	PCC	6214+19.47	3.49'	LT	P-KDR-SB

NOTE:  
NB I-90/94 TRAFFIC IN CURRENT STAGE TO BE INSTALLED AND MAINTAINED BY CONTRACT 62A76.



**TRAFFIC CONTROL LAYOUT INFORMATION (NB)**

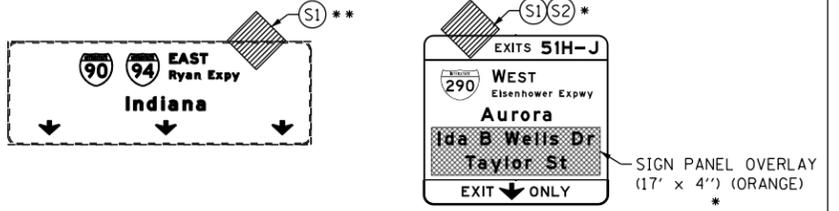
POINT		STATION	OFFSET	LT/RT	BASELINE
A	PI	6145+65.04	43.94'	RT	P-KDR-NB
B	PCC	6147+81.78	49.29'	RT	P-KDR-NB
C	PCC	6149+97.37	57.89'	RT	P-KDR-NB
D	POC	6153+48.37	60.46'	RT	P-KDR-NB
E		6144+91.37	11.19'	RT	P-KDR-NB
F		6145+28.78	47.11'	RT	P-KDR-NB
G		6154+40.37	57.22'	RT	P-KDR-NB
H		6155+31.86	52.90'	RT	P-KDR-NB

TEMPORARY PAVEMENT COMPLETED IN STAGE 1 AND STAGE 2 TO REMAIN

- OH1 EXISTING OVERHEAD SIGN TO REMAIN
- OH2 EXISTING OVERHEAD SIGNS TO REMAIN
- OH3 EXISTING OVERHEAD SIGNS TO REMAIN



- OH4 EXISTING OVERHEAD SIGN
- OH5 RELOCATE TEMPORARY INFORMATION SIGNING SIGN SB-T15-01 WITH THE DOWN ARROW POINTING OVER RAMP SW



**LEGEND**

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
- DIRECTION OF TRAFFIC FLOW
- TYPE III BARRICADE
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
- DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL
- TEMPORARY PAVEMENT
- FROM PREVIOUS STAGE TO REMAIN
- FROM CONTRACT 60X93 TO REMAIN
- FROM CONTRACT 60X79 TO REMAIN
- FROM CONTRACT 62A76

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)
- (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)
- (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)
- (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)
- (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)
- (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)
- (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)
- (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS
- (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
- (K) PINNING TEMPORARY CONCRETE BARRIER
- (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
- (M) IMPACT ATTENUATOR, RELOCATE
- (N) EXISTING PAVEMENT MARKING



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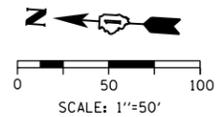
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**

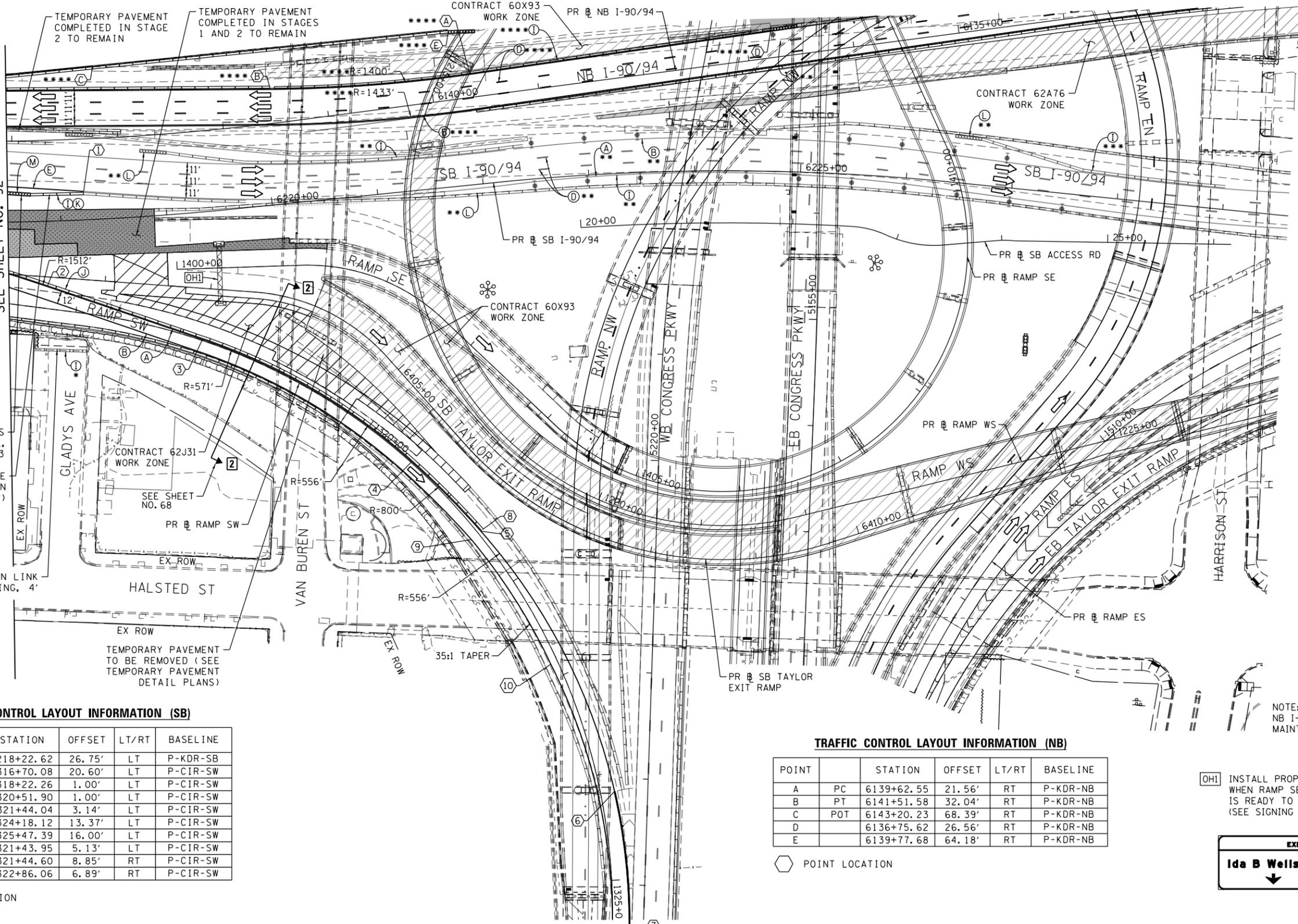
**STAGE 3**

SCALE: 1"=50' SHEET 3 OF 4 SHEETS STA. 6205+00(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	92
				CONTRACT NO. 62J31
ILLINOIS FED. AID PROJECT				



MATCH LINE STA 6217+50.00 (PR @ SB I-90/94) SEE SHEET NO. 92



IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3  
 TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)  
 TEMPORARY CHAIN LINK FENCE WITH SCREENING, 4'

**TRAFFIC CONTROL LAYOUT INFORMATION (SB)**

POINT	STATION	OFFSET	LT/RT	BASELINE
1	6218+22.62	26.75'	LT	P-KDR-SB
2	PT 1316+70.08	20.60'	LT	P-CIR-SW
3	PC 1318+22.26	1.00'	LT	P-CIR-SW
4	PCC 1320+51.90	1.00'	LT	P-CIR-SW
5	PCC 1321+44.04	3.14'	LT	P-CIR-SW
6	POC 1324+18.12	13.37'	LT	P-CIR-SW
7	PT 1325+47.39	16.00'	LT	P-CIR-SW
8	1321+43.95	5.13'	LT	P-CIR-SW
9	1321+44.60	8.85'	RT	P-CIR-SW
10	1322+86.06	6.89'	RT	P-CIR-SW

POINT LOCATION

**TRAFFIC CONTROL LAYOUT INFORMATION (NB)**

POINT	STATION	OFFSET	LT/RT	BASELINE
A	PC 6139+62.55	21.56'	RT	P-KDR-NB
B	PT 6141+51.58	32.04'	RT	P-KDR-NB
C	POT 6143+20.23	68.39'	RT	P-KDR-NB
D	6136+75.62	26.56'	RT	P-KDR-NB
E	6139+77.68	64.18'	RT	P-KDR-NB

POINT LOCATION

NOTE:  
NB I-90/94 TRAFFIC IN CURRENT STAGE TO BE MAINTAINED BY CONTRACT 62A76.

[OH] INSTALL PROPOSED TRUSS AND SIGN PANEL WHEN RAMP SE AND SB TAYLOR EXIT RAMP IS READY TO OPEN TO TRAFFIC (SEE SIGNING PLAN)



**LEGEND**

	WORK ZONE		TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION		FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW		FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE		FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD		FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

- |   |  |  |
|---|--|--|
| (A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)              | (F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)                            | (J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED |
| (B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)             | (G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)          | (K) PINNING TEMPORARY CONCRETE BARRIER   |
| (C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)   | (H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS  | (L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3                                       |
| (D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE) | (I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED | (M) IMPACT ATTENUATOR, RELOCATE  |
| (E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)              |  | (N) EXISTING PAVEMENT MARKING  |



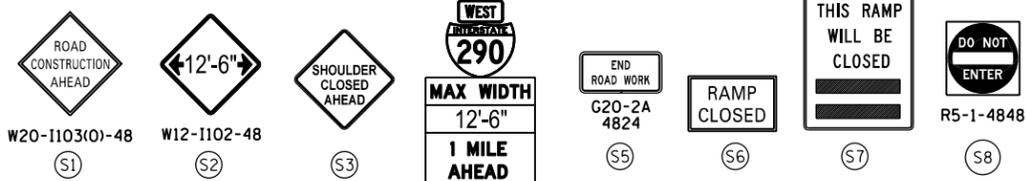
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CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
**STAGE 3**  
 SCALE: 1"=50' SHEET 4 OF 4 SHEETS STA. 6217+50(SB) TO STA. 6217+50(SB)

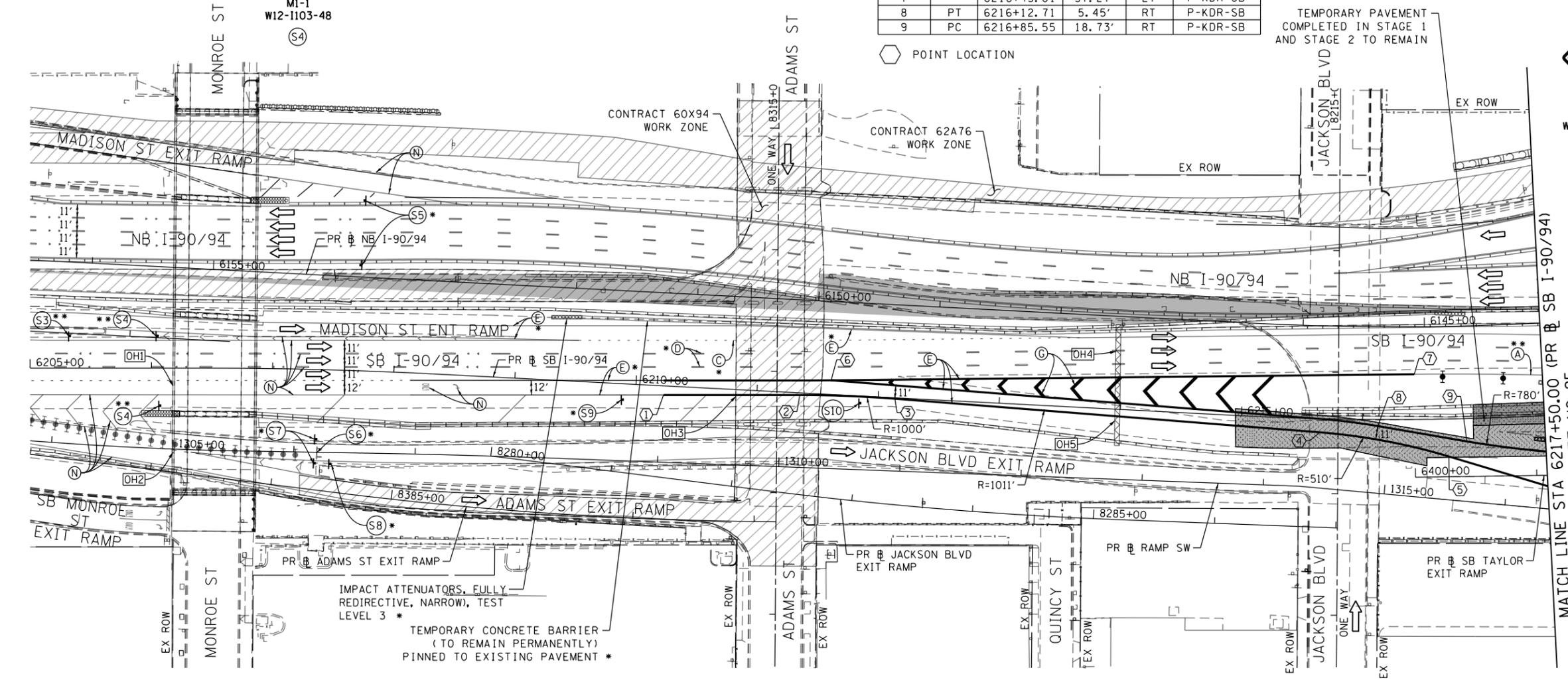
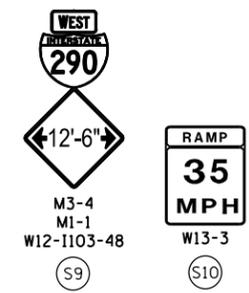
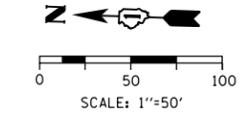
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	93
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



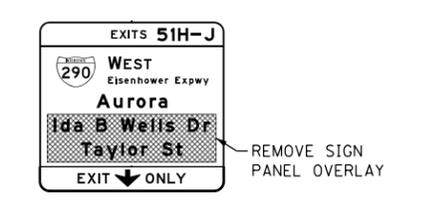
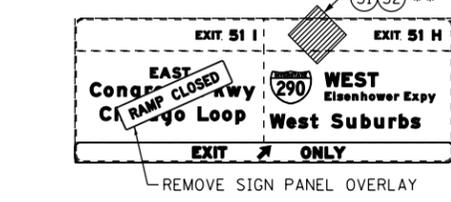
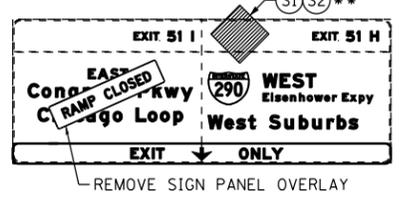
**TRAFFIC CONTROL LAYOUT INFORMATION**

POINT	STATION	OFFSET	LT/RT	BASELINE
1	6210+23.06	7.51'	RT	P-KDR-SB
2	PC 6211+35.44	1.73'	RT	P-KDR-SB
3	PT 6212+13.82	0.77'	RT	P-KDR-SB
4	PC 6215+65.19	11.25'	RT	P-KDR-SB
5	PT 6216+70.56	30.83'	RT	P-KDR-SB
6	6211+60.55	11.67'	LT	P-KDR-SB
7	6216+43.01	37.27'	LT	P-KDR-SB
8	PT 6216+12.71	5.45'	RT	P-KDR-SB
9	PC 6216+85.55	18.73'	RT	P-KDR-SB

NOTE: NB I-90/94 TRAFFIC IN CURRENT STAGE TO BE MAINTAINED BY CONTRACT 62A76.



- [OH1] EXISTING OVERHEAD SIGN TO REMAIN
- [OH2] EXISTING OVERHEAD SIGNS TO REMAIN
- [OH3] EXISTING OVERHEAD SIGNS TO REMAIN
- [OH4] EXISTING OVERHEAD SIGN
- [OH5] RELOCATE TEMPORARY INFORMATION SIGNING SIGN SB-TIS-01. SIGN TO REMAIN AT END OF CONTRACT.



**LEGEND**

WORK ZONE	TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
TEMPORARY CONCRETE BARRIER	DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
TEMPORARY IMPACT ATTENUATOR	TEMPORARY PAVEMENT
TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	* FROM PREVIOUS STAGE TO REMAIN
DIRECTION OF TRAFFIC FLOW	** FROM CONTRACT 60X93 TO REMAIN
TYPE III BARRICADE	*** FROM CONTRACT 60X79 TO REMAIN
ARROW BOARD	**** FROM CONTRACT 62A76
SIGN	

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M) IMPACT ATTENUATOR, RELOCATE
(E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)		(N) EXISTING PAVEMENT MARKING



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 PLOT DATE = 8/15/2019

DESIGNED - PHP  
 DRAWN - PHP  
 CHECKED - MJE  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

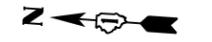
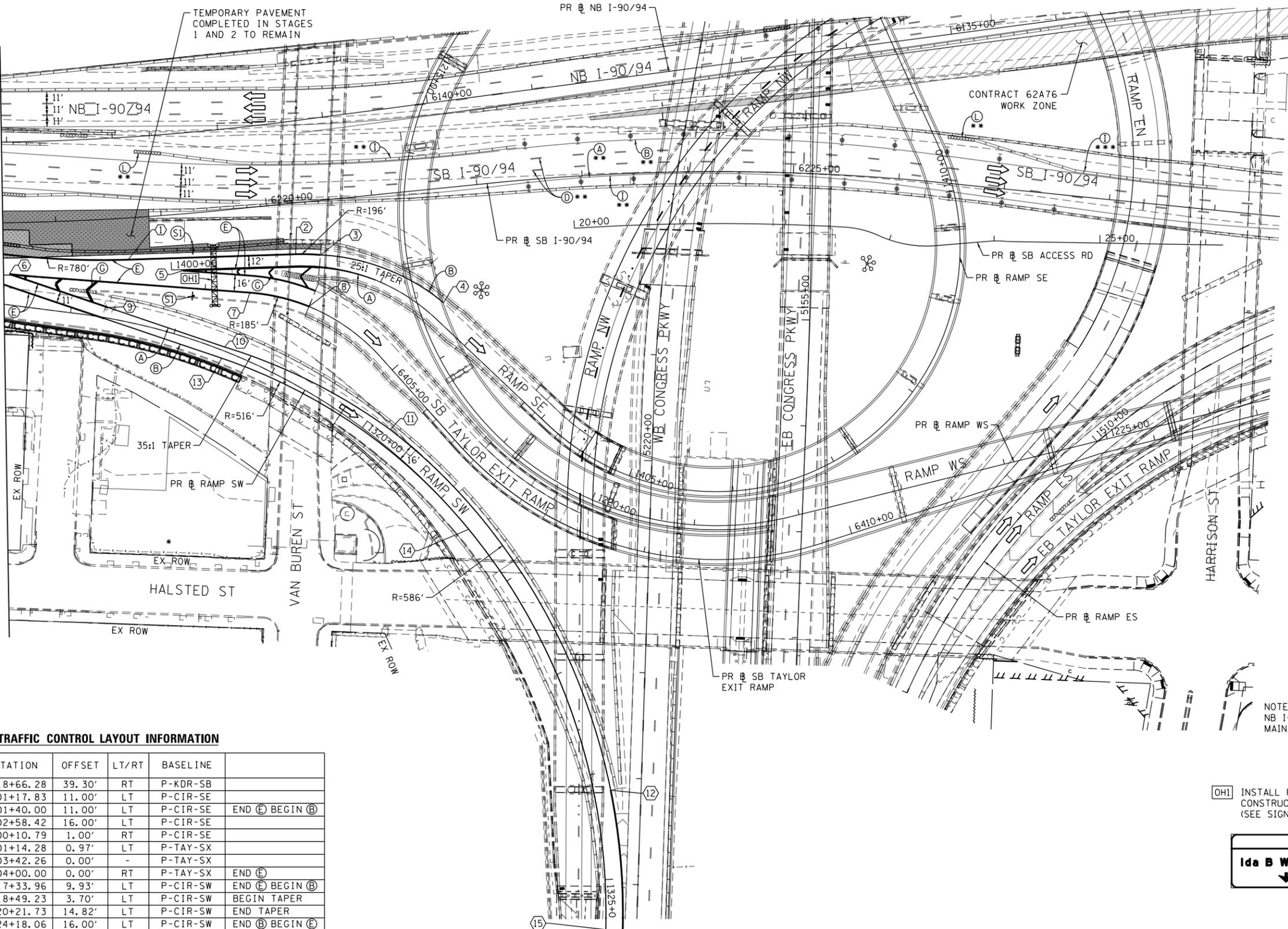
**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN**  
**RAMP SE AND SB TAYLOR ST EXIT RAMP OPENING**

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 6205+00(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	94
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

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MATCH LINE STA 6217+50.00 (PR @ SB I-90/94)  
SEE SHEET NO. 94



SCALE: 1"=50'



**TRAFFIC CONTROL LAYOUT INFORMATION**

POINT		STATION	OFFSET	LT/RT	BASELINE	
1	PT	6218+66.28	39.30'	RT	P-KDR-SB	
2	PC	1401+17.83	11.00'	LT	P-CIR-SE	
3	PT	1401+40.00	11.00'	LT	P-CIR-SE	END (E) BEGIN (B)
4	PC	1402+58.42	16.00'	LT	P-CIR-SE	
5		1400+10.79	1.00'	RT	P-CIR-SE	
6		6401+14.28	0.97'	LT	P-TAY-SX	
7	PC	6403+42.26	0.00'	-	P-TAY-SX	
8	PT	6404+00.00	0.00'	RT	P-TAY-SX	END (E)
9		1317+33.96	9.93'	LT	P-CIR-SW	END (E) BEGIN (B)
10		1318+49.23	3.70'	LT	P-CIR-SW	BEGIN TAPER
11	PC	1320+21.73	14.82'	LT	P-CIR-SW	END TAPER
12	POT	1324+18.06	16.00'	LT	P-CIR-SW	END (B) BEGIN (E)
13	PC	1318+50.52	7.25'	RT	P-CIR-SW	
14	PCC	1321+39.21	0.00'	-	P-CIR-SW	
15	PT	1325+47.16	0.00'	-	P-CIR-SW	

POINT LOCATION

**LEGEND**

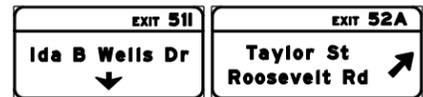
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	TEMPORARY CONCRETE BARRIER		DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
	TEMPORARY IMPACT ATTENUATOR		TEMPORARY PAVEMENT
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION	*	FROM PREVIOUS STAGE TO REMAIN
	DIRECTION OF TRAFFIC FLOW	**	FROM CONTRACT 60X93 TO REMAIN
	TYPE III BARRICADE	***	FROM CONTRACT 60X79 TO REMAIN
	ARROW BOARD	****	FROM CONTRACT 62A76
	SIGN		

**PROPOSED TEMP PAVEMENT MARKINGS AND TRAFFIC CONTROL DEVICES**

(A) TEMP EPOXY PVT MK LINE 4 (SOLID WHITE)	(F) TEMP EPOXY PVT MK LINE 8 (3' DASH 9' SKIP, WHITE)	(J) REL TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED
(B) TEMP EPOXY PVT MK LINE 4 (SOLID YELLOW)	(G) TEMP EPOXY PVT MK LINE 12 (WHITE CHEVRONS @ 45°, 30' SPACING) (TYP)	(K) PINNING TEMPORARY CONCRETE BARRIER
(C) TEMP EPOXY PVT MK LINE 4 (2' DASH 6' SKIP, WHITE)	(H) TEMP EPOXY PVT MK LETTERS AND SYMBOLS	(L) IMPACT ATTENUATOR, TEMPORARY, TEST LEVEL 3
(D) TEMP EPOXY PVT MK LINE 5 (10' DASH 30' SKIP, WHITE)	(I) TEMP CONC BARRIER ALONG LANE LINE OR @ 12:1 ON TAPERS UNLESS OTHERWISE NOTED	(M) IMPACT ATTENUATOR, RELOCATE
(E) TEMP EPOXY PVT MK LINE 8 (SOLID WHITE)		(N) EXISTING PAVEMENT MARKING

NOTE:  
NB I-90/94 TRAFFIC IN CURRENT STAGE TO BE MAINTAINED BY CONTRACT 62A76.

(OH1) INSTALL PROPOSED SIGN ON NEWLY CONSTRUCTED SIGN TRUSS (SEE SIGNING PLAN)



D162J31-SHT-Staging-RampSE-TaylorExit-Striping-02.dgn  
USER NAME = ashok.kc  
PLOT SCALE = 100.0000' / in.  
PLOT DATE = 8/15/2019

DESIGNED - PHP	REVISED -
DRAWN - PHP	REVISED -
CHECKED - MJE	REVISED -
DATE - 8/16/2019	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

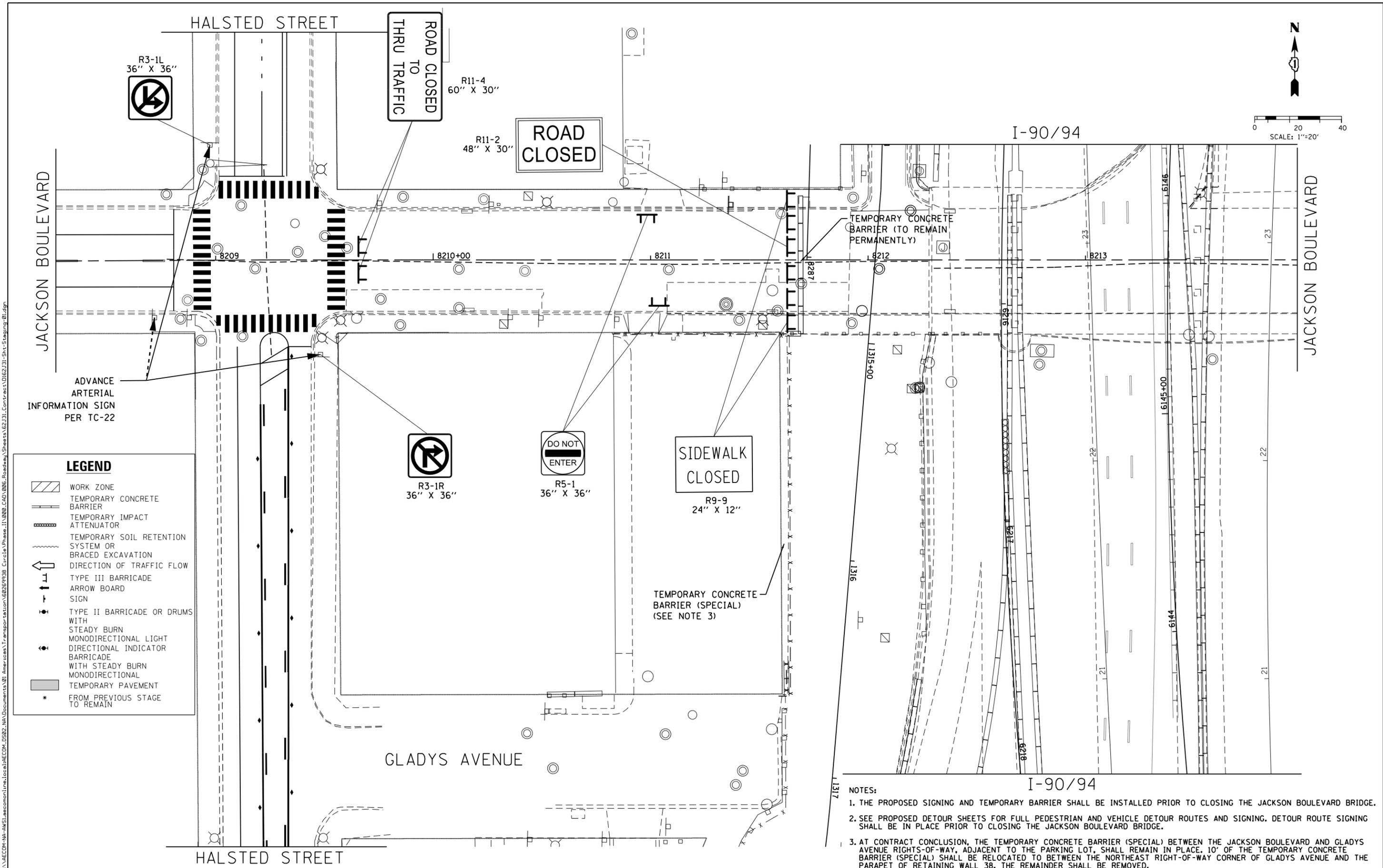
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN  
RAMP SE AND SB TAYLOR ST EXIT RAMP OPENING

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. 6217+50(SB) TO STA. 6217+50(SB)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	95
				CONTRACT NO. 62J31
ILLINOIS FED. AID PROJECT				



0 20 40  
SCALE: 1"=20'



**LEGEND**

	WORK ZONE
	TEMPORARY CONCRETE BARRIER
	TEMPORARY IMPACT ATTENUATOR
	TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
	DIRECTION OF TRAFFIC FLOW
	TYPE III BARRICADE
	ARROW BOARD SIGN
	TYPE II BARRICADE OR DRUMS WITH STEADY BURN
	MONODIRECTIONAL LIGHT
	DIRECTIONAL INDICATOR
	BARRICADE WITH STEADY BURN
	MONODIRECTIONAL
	TEMPORARY PAVEMENT
	FROM PREVIOUS STAGE TO REMAIN

- NOTES:**
1. THE PROPOSED SIGNING AND TEMPORARY BARRIER SHALL BE INSTALLED PRIOR TO CLOSING THE JACKSON BOULEVARD BRIDGE.
  2. SEE PROPOSED DETOUR SHEETS FOR FULL PEDESTRIAN AND VEHICLE DETOUR ROUTES AND SIGNING. DETOUR ROUTE SIGNING SHALL BE IN PLACE PRIOR TO CLOSING THE JACKSON BOULEVARD BRIDGE.
  3. AT CONTRACT CONCLUSION, THE TEMPORARY CONCRETE BARRIER (SPECIAL) BETWEEN THE JACKSON BOULEVARD AND GLADYS AVENUE RIGHTS-OF-WAY, ADJACENT TO THE PARKING LOT, SHALL REMAIN IN PLACE. 10' OF THE TEMPORARY CONCRETE BARRIER (SPECIAL) SHALL BE RELOCATED TO BETWEEN THE NORTHEAST RIGHT-OF-WAY CORNER OF GLADYS AVENUE AND THE PARAPET OF RETAINING WALL 38. THE REMAINDER SHALL BE REMOVED.



D162J31-Sht-Staging-01.dgn  
 USER NAME = jmontanez  
 PLOT SCALE = 40.0000' / 1" IN.  
 PLOT DATE = 8/15/2019

DESIGNED - MJL  
 DRAWN - JRM  
 CHECKED - JMG  
 DATE - 8/16/2019

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL  
 PLAN - JACKSON BOULEVARD**

SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. 8308+00 TO STA. 8313+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	96
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				

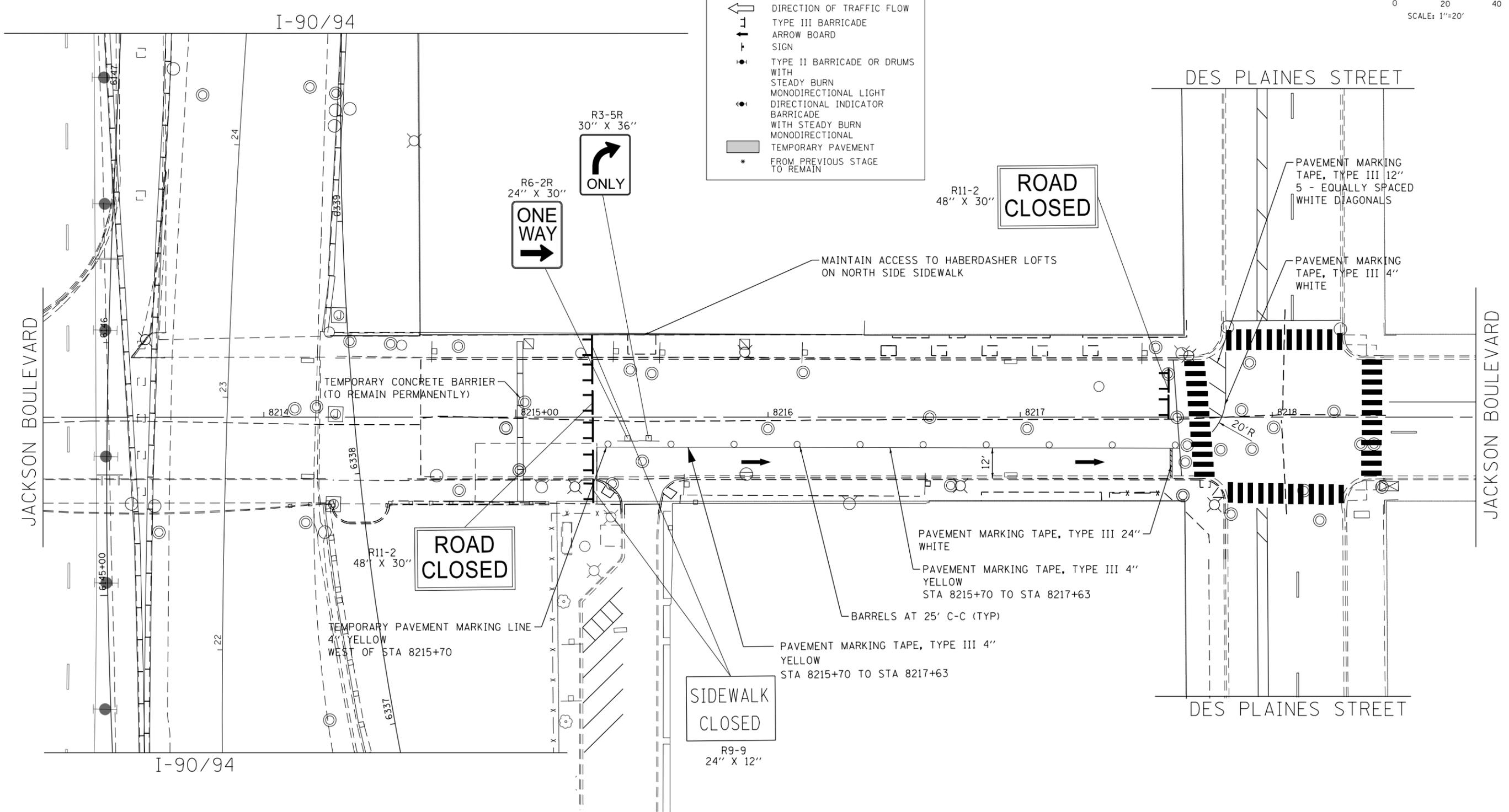
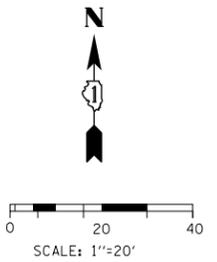
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**NOTES:**

1. THE PROPOSED SIGNING AND TEMPORARY BARRIER SHALL BE INSTALLED PRIOR TO CLOSING THE JACKSON BOULEVARD BRIDGE.
2. SEE PROPOSED DETOUR SHEETS FOR FULL PEDESTRIAN AND VEHICLE DETOUR ROUTES AND SIGNING. DETOUR ROUTE SIGNING SHALL BE IN PLACE PRIOR TO CLOSING THE JACKSON BOULEVARD BRIDGE.

**LEGEND**

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY SOIL RETENTION SYSTEM OR BRACED EXCAVATION
- DIRECTION OF TRAFFIC FLOW
- TYPE III BARRICADE
- ARROW BOARD
- SIGN
- TYPE II BARRICADE OR DRUMS WITH STEADY BURN MONODIRECTIONAL LIGHT
- DIRECTIONAL INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL
- TEMPORARY PAVEMENT
- FROM PREVIOUS STAGE TO REMAIN



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D162J31-Sht-Staging_02.dgn
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PLOT SCALE = 40.0000' / in.
PLOT DATE = 8/13/2019

DESIGNED - MJL	REVISED -
DRAWN - JRM	REVISED -
CHECKED - JMG	REVISED -
DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL			
PLAN - JACKSON BOULEVARD			
SCALE: 1"=20'	SHEET 2	OF 2 SHEETS	STA.8308+00 TO STA.8313+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	97
				CONTRACT NO. 62J31
ILLINOIS FED. AID PROJECT				



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**TEMPORARY EROSION CONTROL SCHEDULE**

	MULCH METHOD, 2	TEMPORARY EROSION CONTROL SEEDING	PERIMETER EROSION BARRIER	TEMPORARY CHAIN LINK FENCE WITH SCREENING, 6'	INLET FILTERS	DUST CONTROL WATERING	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	STORM SEWERS, CLASS A, TYPE 1 12"
SHEET	ACRE	POUND	FOOT	FOOT	EACH	UNIT	EACH	FOOT
STAGE 1	1.00	100	738	207	17	33	2	50
STAGE 2	1.00	100	693	207	19	33	2	50
STAGE 3	0.75	75	396	207	5	33	2	50
TOTAL	2.75	275	1827	621	41	100	6	150

**PERMANENT EROSION CONTROL SCHEDULE**

	SUPPLEMENTAL WATERING	TOPSOIL FURNISH AND PLACE, 4"	SEEDING, CLASS 2A	NITROGEN FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	EROSION CONTROL BLANKET	SODDING, SALT TOLERANT	AGGREGATE SURFACE COURSE, TYPE B
	UNIT	SQ YD	ACRE	POUND	POUND	SQ YD	SQ YD	CU YD
	29	3486	0.75	80	80	2533	953	32
TOTAL	29	3486	0.75	80	80	2533	953	32

**INLET FILTER SCHEDULE (STAGE 1)**

LOCATION			AMOUNT
STATION	OFFSET	ALIGNMENT	EACH
1318+99.49	2.87' LT	PR @ RAMP SW	1
1318+97.00	3.05' RT	PR @ RAMP SW	1
1318+88.77	6.77' LT	PR @ RAMP SW	1
1318+86.07	0.12' RT	PR @ RAMP SW	1
1318+76.88	7.05' LT	PR @ RAMP SW	1
1315+14.43	39.59' LT	PR @ RAMP SW	1
1315+06.42	28.44' LT	PR @ RAMP SW	1
1317+14.99	28.94' RT	PR @ RAMP SW	1
1316+80.00	29.61' RT	PR @ RAMP SW	1
6402+01.91	5.54' LT	PR @ SB TAYLOR EXIT RAMP	1
6401+98.34	3.45' RT	PR @ SB TAYLOR EXIT RAMP	1
1400+85.09	21.30' LT	PR @ RAMP SE	1
1400+23.03	16.79' LT	PR @ RAMP SE	1
1400+33.21	58.20' LT	PR @ RAMP SE	1
1400+38.37	58.29' LT	PR @ RAMP SE	1
1318+00.00	32.02' RT	PR @ RAMP SW	1
1315+25.00	18.13' RT	PR @ RAMP SW	1
TOTAL			17

**INLET FILTER SCHEDULE (STAGE 2)**

LOCATION			AMOUNT
STATION	OFFSET	ALIGNMENT	EACH
6148+07.66	9.81' LT	PR @ NB I-90/94	1
6149+75.55	3.53' LT	PR @ NB I-90/94	1
6147+80.91	19.94' RT	PR @ NB I-90/94	1
6147+09.47	21.41' RT	PR @ NB I-90/94	1
6147+32.80	6.57' RT	PR @ NB I-90/94	1
6147+69.65	11.22' LT	PR @ NB I-90/94	1
6147+37.75	8.87' LT	PR @ NB I-90/94	1
6147+27.53	8.01' LT	PR @ NB I-90/94	1
6145+35.37	18.99' RT	PR @ NB I-90/94	1
6145+36.20	9.82' RT	PR @ NB I-90/94	1
6147+17.00	1.45' LT	PR @ NB I-90/94	1
6146+97.00	0.79' RT	PR @ NB I-90/94	1
6146+77.00	3.02' RT	PR @ NB I-90/94	1
1318+75.00	11.00' RT	PR @ RAMP SW	1
1318+50.00	11.00' RT	PR @ RAMP SW	1
1318+00.00	11.00' RT	PR @ RAMP SW	1
1315+25.93	3.59' RT	PR @ RAMP SW	1
1315+20.00	10.62' LT	PR @ RAMP SW	1
6401+98.27	7.60' RT	PR @ SB TAYLOR EXIT RAMP	1
TOTAL			19

**INLET FILTER SCHEDULE (STAGE 3)**

LOCATION			AMOUNT
STATION	OFFSET	ALIGNMENT	EACH
6402+21.91	7.00' RT	PR @ SB TAYLOR EXIT RAMP	1
6402+45.54	7.00' RT	PR @ SB TAYLOR EXIT RAMP	1
6402+81.63	7.00' RT	PR @ SB TAYLOR EXIT RAMP	1
6403+40.00	7.00' RT	PR @ SB TAYLOR EXIT RAMP	1
1318+60.00	32.58' LT	PR @ SB TAYLOR EXIT RAMP	1
TOTAL			5



D162J31-SHT-Erosion-Schedule-01.dgn  
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 PLOT DATE = 8/15/2019

DESIGNED - MRC  
 DRAWN - NLD  
 CHECKED - JMG  
 DATE - 8/16/2019

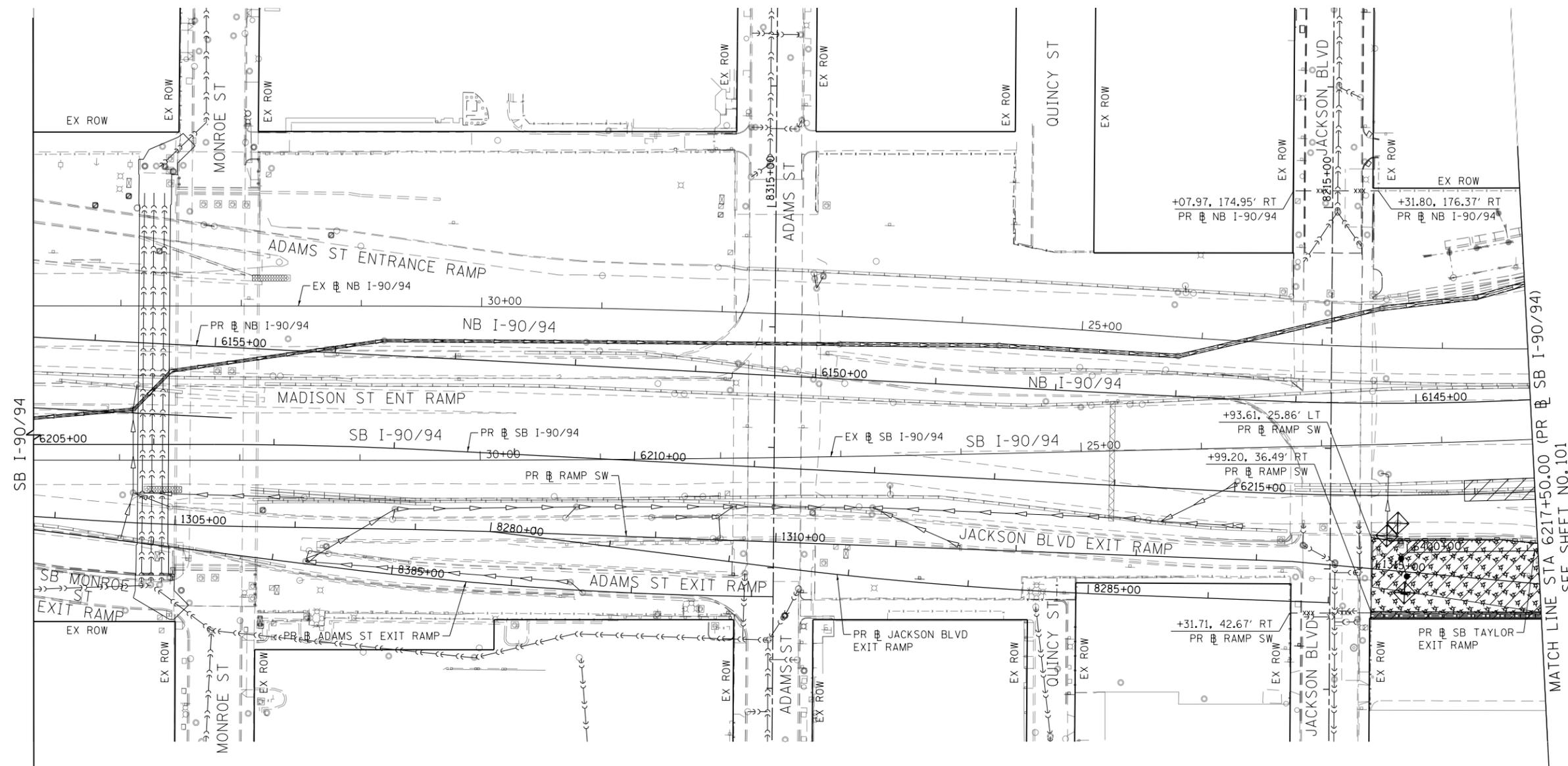
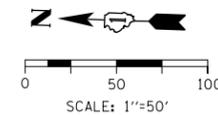
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 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENTATION CONTROL  
 SCHEDULES

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2019-054-I	COOK	400	99
CONTRACT NO. 62J31				
ILLINOIS FED. AID PROJECT				



**TEMPORARY EROSION CONTROL LEGEND**

-  INLET FILTER
-  INLET FILTER (TO REMAIN FROM PREVIOUS STAGE)
-  PERIMETER EROSION BARRIER
-  MULCH, METHOD 2
-  TEMPORARY EROSION CONTROL SEEDING OR SURFACE ROUGHENING
-  WORK ZONE
-  TEMPORARY CHAIN LINK FENCE WITH SCREENING, 6'
-  STORM SEWER TO BE INSTALLED IN THIS CONTRACT
-  STORM SEWER TO BE INSTALLED BY OTHERS
-  STORM SEWER TO BE INSTALLED BY OTHERS (60X93)

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USER NAME = mrc155	DRAWN - MRC	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED - JMG	REVISED -
PLOT DATE = 8/13/2019	DATE - 8/16/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>EROSION AND SEDIMENTATION CONTROL RAMP SW, RAMP SE AND SB TAYLOR EXIT RAMP - STAGE 1</b>		
SCALE: 1"=50'	SHEET 1 OF 2 SHEETS	STA. 6205+00(SB) TO STA. 6217+50(SB)

F.A.I. RTE. 90/94/290	SECTION 2019-054-I	COUNTY COOK	TOTAL SHEETS 400	SHEET NO. 100
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J31	