



Illinois Department of Transportation

Office of Intermodal Project Implementation / Division of Aeronautics
1 Langhorne Bond Drive / Springfield, Illinois 62707-8415

October 27, 2025

SUBJECT: Aurora Municipal Airport
Aurora, Illinois
Kane County
Illinois Project Number: ARR-5201
SBG Project Number: 3-17-SBGP-TBD
Contract No. AU075
Item No. 06A, November 7, 2025 Letting
Addendum A

NOTICE TO PROSPECTIVE BIDDERS

Attached is an addendum to the plans or proposal. This addendum involves revised and/or added material.

Reason for Addendum:

Revise plans, special provisions, and schedule of prices.

To All Plan Holders:

Please refer to plan changes, special provisions changes, and schedule of prices changes.

Plan Changes:

1. Sheet 2 – Site Plan and Project Control Plan/Summary of Quantities
 - a. Summary of Quantities table – Revise AR107812 L-807 W C-12' Internally Lit to 1 Each.
2. Sheet 4 – Sequence of Construction – 2
 - a. Revise table to reference Phase 2.
3. Sheet 5 – Sequence of Construction – 3
 - a. Revise table to reference Phase 3.
 - b. Revise allowable work periods for Work Area 3 to three (3) calendar days.
4. Sheet 6 – Sequence of Construction Notes
 - a. Revise total contract time to 35 calendar days.
5. Sheet 13 – Proposed Improvements – 1
 - a. Revise Electrical Note 1.
6. Sheet 14 – Proposed Improvements – 2
 - a. Revise Electrical Note 1.
7. Sheet 15 – Proposed Improvements – 3
 - a. Revise Electrical Note 1.
8. Sheet 20 – Electrical Details – 4
 - a. Add note to Detail 15.
 - b. Leave in Detail 14.
9. Sheet 23 - Electrical Details – 7
 - a. Revise title for Detail 25.
 - b. Revise Detail 24.
10. Sheet 25 – Electrical Vault Details
 - a. Revise Vault Nomenclature Note 34.
 - b. Revise Vault Nomenclature Note 38.

c. Add Note 3.

Special Provisions Changes:

Page 27 – Item 108.2.2 CABLE, Revise last bullet point to “600V, 2 - 1/C #10 type use, 1#10 in 1” UD”.

Schedule of Prices Changes:

Revise AR107812 L-807 W C-12’ Internally Lit – 1 Each.

Proposal Changes:

Replace Proposal pages 2 and 8 with attached revised Proposal pages 2 and 8.

Prime contractors must utilize the enclosed material when preparing their bid and must include any changes to the Schedule of Prices in their bid.

Questions on this addendum may be directed to Sheue Torng Lee of Crawford, Murphy & Tilly, Inc. at 630.907.7079.

ITEM 108 – INSTALLATION OF UNDERGROUND CABLE FOR AIRPORTS

DESCRIPTION

108-1.1

DELETE: The 3rd sentence of the first paragraph.

ADD:

This item of work shall consist of the underground installation of 5000V cables in PVC duct or duct bank at the locations shown on the plans and in accordance with these specifications. When crossing existing utilities or as required by the Engineer, the Contractor shall hand dig the trenches for the proposed cables.

Contractor shall color code all airfield lighting cables in ducts, manholes and handholes as directed by the Engineer. All costs of color coding shall be considered incidental to the contract unit price for the associated item.

This item of work shall also consist of the removal of cables and disposed of off Airport property.

This item of work shall consist of the underground installation of 600V cables in PVC duct or duct bank at the locations shown on the plans and in accordance with these specifications. When crossing existing utilities or as required by the Engineer, the Contractor shall hand dig the trenches for the proposed cables.

EQUIPMENT AND MATERIALS

108-2.2 CABLE

ADD:

Airfield Lighting cable and temporary cable under this item shall be:

- L-824, 1 - 1/C #8, 5,000 V, Type C, in 3/4" UD
- L-824, 2 - 1/C #8, 5,000 V, Type C, in 1-1/4" UD
- 600V, 2 – 1/C #6 type use, 1#10 in 1" UD
- 600V, 2 - 1/C #8 type use, 1#10 in 3/4" UD
- 600V, 2 - 1/C #10 type use, 1#10 in ~~3/4" UD~~ **1" UD**

108-2.4 CABLE CONNECTIONS

DELETE: The first and second sentence of paragraph **d. The taped or heat-shrink splice.**

ADD:

To further reduce the possibility of water (moisture) entrance into the connector between the cable and the field attached connector, heat shrinkable tubing with interior adhesive shall be applied over all cable connections.

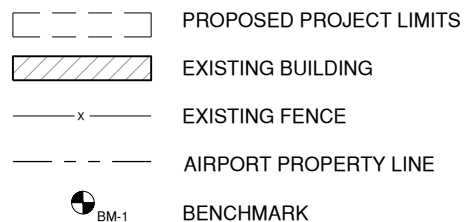
The heat shrinkable tubing shall cover the entire L-823 connector. All connections shall be at manholes or light bases. No direct burial splicing will be allowed.

FINAL

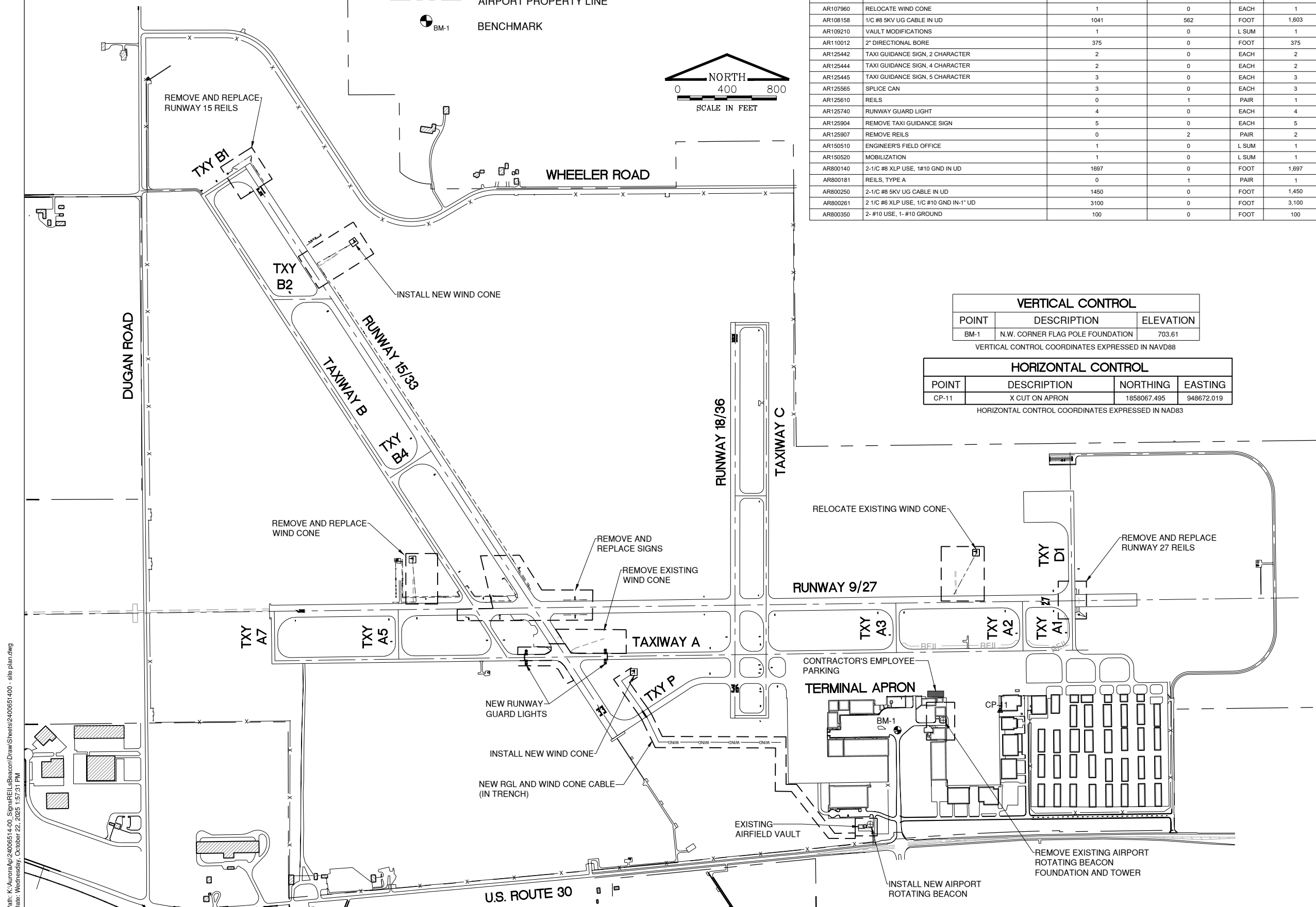
SEPTEMBER 12, 2025



SITE PLAN AND PROJECT CONTROL PLAN SUMMARY OF QUANTITIES



ITEM	DESCRIPTION	FEDERAL/STATE/ LOCAL QUANTITY	LOCAL ONLY QUANTITY	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR101510	AIRPORT ROTATING BEACON	1	0	EACH	1	
AR101900	BEACON REMOVAL	1	0	EACH	1	
AR107508	L-806 W C 8" INTERNALLY LIT	2	0	EACH	2	
AR107812	L-807 W C-12" INTERNALLY LIT	1 2	0	EACH	1 2	
AR107900	REMOVE WIND CONE	3	0	EACH	3	
AR107960	RELOCATE WIND CONE	1	0	EACH	1	
AR108158	1/C #8 5KV UG CABLE IN UD	1041	562	FOOT	1,603	
AR109210	VAULT MODIFICATIONS	1	0	L SUM	1	
AR110012	2" DIRECTIONAL BORE	375	0	FOOT	375	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	2	0	EACH	2	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	2	0	EACH	2	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	3	0	EACH	3	
AR125565	SPLICE CAN	3	0	EACH	3	
AR125610	REILS	0	1	PAIR	1	
AR125740	RUNWAY GUARD LIGHT	4	0	EACH	4	
AR125904	REMOVE TAXI GUIDANCE SIGN	5	0	EACH	5	
AR125907	REMOVE REILS	0	2	PAIR	2	
AR150510	ENGINEER'S FIELD OFFICE	1	0	L SUM	1	
AR150520	MOBILIZATION	1	0	L SUM	1	
AR800140	2-1/C #8 XLP USE, 1#10 GND IN UD	1697	0	FOOT	1,697	
AR800181	REILS, TYPE A	0	1	PAIR	1	
AR800250	2-1/C #8 5KV UG CABLE IN UD	1450	0	FOOT	1,450	
AR800261	2 1/C #6 XLP USE, 1/C #10 GND IN-1" UD	3100	0	FOOT	3,100	
AR800350	2- #10 USE, 1- #10 GROUND	100	0	FOOT	100	

VERTICAL CONTROL COORDINATES EXPRESSED IN NAVD88HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAD83

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CONSULTANTS

FINAL

REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025



10/22/25 REVISE TABLE

MARK DATE DESCRIPTION

IL PROJECT NO:

CMT PROJECT NO: 25007812.00

CAD DWG FILE:

DESIGNED BY: CMJ

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: DKP

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SHEET TITLE

SEQUENCE OF
CONSTRUCTION - 2

PHASE 2 NOTES

1. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA.
2. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
3. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
4. EXISTING WIND CONE FOUNDATION AND CABLE TO BE REMOVED AND NEW CABLE TO BE INSTALLED TO WIND CONE WORK PREVIOUSLY COMPLETED IN PHASE 1.
5. EXISTING WIND CONE TO BE RELOCATED TO FOUNDATION INSTALLED IN PHASE 1 AND WIND CONE FOUNDATION AND CABLE TO BE REMOVED.
6. CONTRACTOR SHALL MAINTAIN ALL RUNWAY AND TAXIWAY LIGHTS AND SIGNS IN OPERATION AT ALL TIMES FOR ALL OPEN PAVEMENTS. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE INCIDENTAL TO THE CONTRACT.

LEGEND

PHASE 2 WORK AREAS

WORK ZONE LIMITS

CLOSED PAVEMENT AREAS PHASE 2A

- RWY 15/33 = OPEN
- RWY 9/27 = DAILY CLOSURES

CLOSED PAVEMENT AREAS PHASE 2B

LOW PROFILE BARRICADES (PHASE 2A)

LOW PROFILE BARRICADES (PHASE 2B)

EXISTING BUILDING

EXISTING FENCE

AIRPORT PROPERTY LINE

RSA

RUNWAY SAFETY AREA

ROFA

RUNWAY OBJECT FREE AREA

TOFA

TAXIWAY OBJECT FREE AREA

RUNWAY CLOSURE MARKING

TAXIWAY CLOSURE MARKING

CONTRACTOR'S ACCESS ROUTE

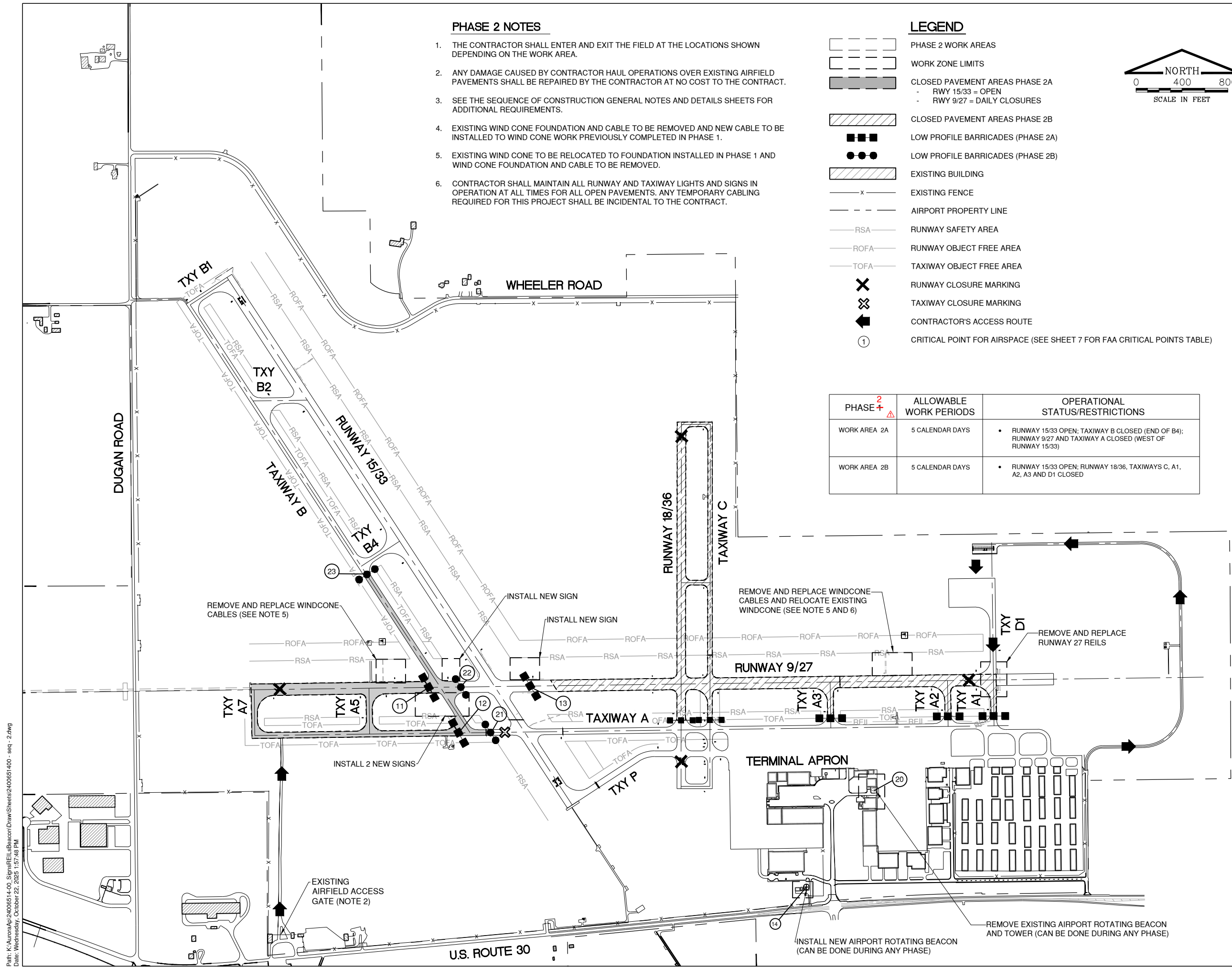
CRITICAL POINT FOR AIRSPACE (SEE SHEET 7 FOR FAA CRITICAL POINTS TABLE)

NORTH

0 400 800

SCALE IN FEET

PHASE 2	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 2A	5 CALENDAR DAYS	RUNWAY 15/33 OPEN; TAXIWAY B CLOSED (END OF B4); RUNWAY 9/27 AND TAXIWAY A CLOSED (WEST OF RUNWAY 15/33)
WORK AREA 2B	5 CALENDAR DAYS	RUNWAY 15/33 OPEN; RUNWAY 18/36, TAXIWAYS C, A1, A2, A3 AND D1 CLOSED



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CONSULTANTS

FINAL

REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025



10/22/25 REVISE CALENDAR DAYS
REVISE TABLE

MARK DATE DESCRIPTION

IL PROJECT NO:

CMT PROJECT NO: 25007812.00

CAD DWG FILE:

DESIGNED BY: CMJ

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: DKP

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SHEET TITLE

SEQUENCE OF
CONSTRUCTION - 3

PHASE 3 NOTES

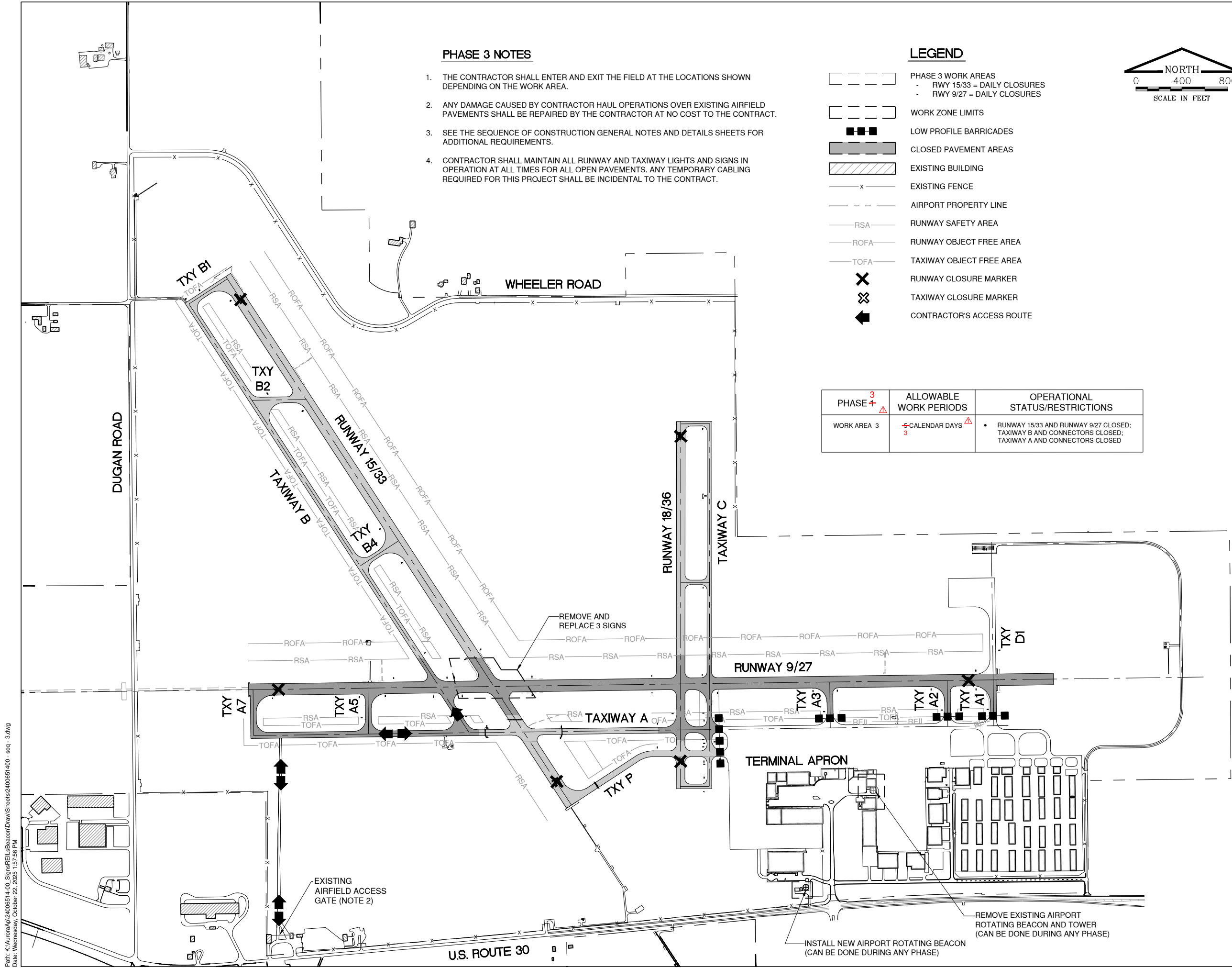
1. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA.
2. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
3. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
4. CONTRACTOR SHALL MAINTAIN ALL RUNWAY AND TAXIWAY LIGHTS AND SIGNS IN OPERATION AT ALL TIMES FOR ALL OPEN PAVEMENTS. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE INCIDENTAL TO THE CONTRACT.

LEGEND

- PHASE 3 WORK AREAS
 - RWY 15/33 = DAILY CLOSURES
 - RWY 9/27 = DAILY CLOSURES
- WORK ZONE LIMITS
- LOW PROFILE BARRICADES
- CLOSED PAVEMENT AREAS
- EXISTING BUILDING
- EXISTING FENCE
- AIRPORT PROPERTY LINE
- RSA RUNWAY SAFETY AREA
- ROFA RUNWAY OBJECT FREE AREA
- TOFA TAXIWAY OBJECT FREE AREA
- RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER
- CONTRACTOR'S ACCESS ROUTE



PHASE 3	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 3	5 CALENDAR DAYS	- RUNWAY 15/33 AND RUNWAY 9/27 CLOSED; - TAXIWAY B AND CONNECTORS CLOSED; - TAXIWAY A AND CONNECTORS CLOSED



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GENERAL 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2 (LATEST EDITION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS. 2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST EDITION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD. 3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS. 4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS. 5. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL. 6. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE AIRPORT. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT. 7. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 8. ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION LIMITS AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED , CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COST SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 10. PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, FLAGGER, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 11. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED. 1. COORDINATION 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT. 2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT. 3. THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY. 2. PHASING 1. TOTAL CONTRACT TIME SHALL BE 27³⁵ CALENDAR DAYS. ⚠ 2. PHASING SHALL BE AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEET.	3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET. 2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT. 3. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES. 4. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER. 4. PROTECTION OF NAVIGATION AIDS (NAVAIDS) 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES. 5. CONTRACTOR ACCESS 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS. 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS IF APPLICABLE AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE. 3. SUPERVISORY PERSONNEL SHALL DEMONSTRATE IN THE PRESENCE OF THE AIRPORT MANAGER THAT THEY ARE FAMILIAR WITH AIRPORT RADIO AND AIRPORT DRIVING PROCEDURES IN ORDER TO PERFORM WORK. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. KNOWLEDGE OF THE AIRPORTS PROCEDURES BY THE SUPERVISORY PERSONNEL MUST BE DEMONSTRATED PRIOR TO THE START OF CONSTRUCTION. 4. THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET. 5. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. 6. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT MONITORING AIRPORT TRAFFIC ON THE RADIO. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN AND AIRPORT RADIO. 7. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK. 8. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR. 6. WILDLIFE MANAGEMENT 1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT. 2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING. 3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS. 7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS. 2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS. 8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.	9. NOTIFICATION OF CONSTRUCTION ACTIVITIES 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER. 2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT. 3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED. 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911. 5. CONTACTS FOR THIS PROJECT WILL BE DETERMINED AT THE PRECONSTRUCTION MEETING PRIOR TO THE PROJECT START. 10. INSPECTION REQUIREMENTS 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX C OF FAA AC 150/5370-2 (LATEST EDITION) MAY BE USED TO AID IN THE INSPECTIONS. 2. THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS. SEE PART 1 SECTION 50-13 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. 11. UNDERGROUND UTILITIES 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE PART 1 SECTION 70-16 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER. 2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-16 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION. 3. SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE. 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 12. PENALTIES 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW. 13. SPECIAL CONDITIONS 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. 14. RUNWAY AND TAXIWAY VISUAL AIDS 1. RUNWAY OR TAXIWAY CLOSURES (IF REQUIRED) ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2 (LATEST EDITION).	15. MARKING AND SIGNS FOR ACCESS ROUTES 1. MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
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16. HAZARD MARKING AND LIGHTING

1.

THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.

2.

ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 AND 150/5210-5 (LATEST EDITIONS) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY.

3.

BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASNG PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

4.

THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

5.

ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON IDOT TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "UNAUTHORIZED ACCESS NOT ALLOWED".

17. PROTECTION

1.

CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS, RUNWAY OBSTACLE FREE ZONE (ROFZ) AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

2.

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A SWEEPER.

3.

THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

4.

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

5.

THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO. IN THE EVENT THAT THE AIRPORT MANAGER NEEDS TO CONTACT THE CONTRACTOR DIRECTLY, THE OPERATOR OF SAID RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND TUNED INTO THE GROUND CONTROL FREQUENCY.

6.

BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

AIRFIELD LIGHTS AND SIGNS NOTES

1.

CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS INCLUDING LIGHTS/SIGNS ON OTHER SIDE OF BARRICADE LINES WITHIN THE AIRCRAFT MOVEMENT AREA UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.

2.

CONTRACTOR SHALL TURN OFF RUNWAY/TAXIWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY/TAXIWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY/TAXIWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY/TAXIWAY CIRCUIT.

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT ROTATING BEACON

SEPTEMBER 12, 2025



AURORA MUNICIPAL AIRPORT

⚠

10/22/25

REVISE TOTAL CONTRACT TIME

MARK | DATE | DESCRIPTION

IL PROJECT NO:

CMT PROJECT NO: 25007812.00

CAD DWG FILE:

DESIGNED BY: CMJ

DRAWN BY: JRO

CHECKED BY: DKP

APPROVED BY: DKP

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SHEET TITLE

SEQUENCE OF CONSTRUCTION NOTES

SHEET 6 OF 26

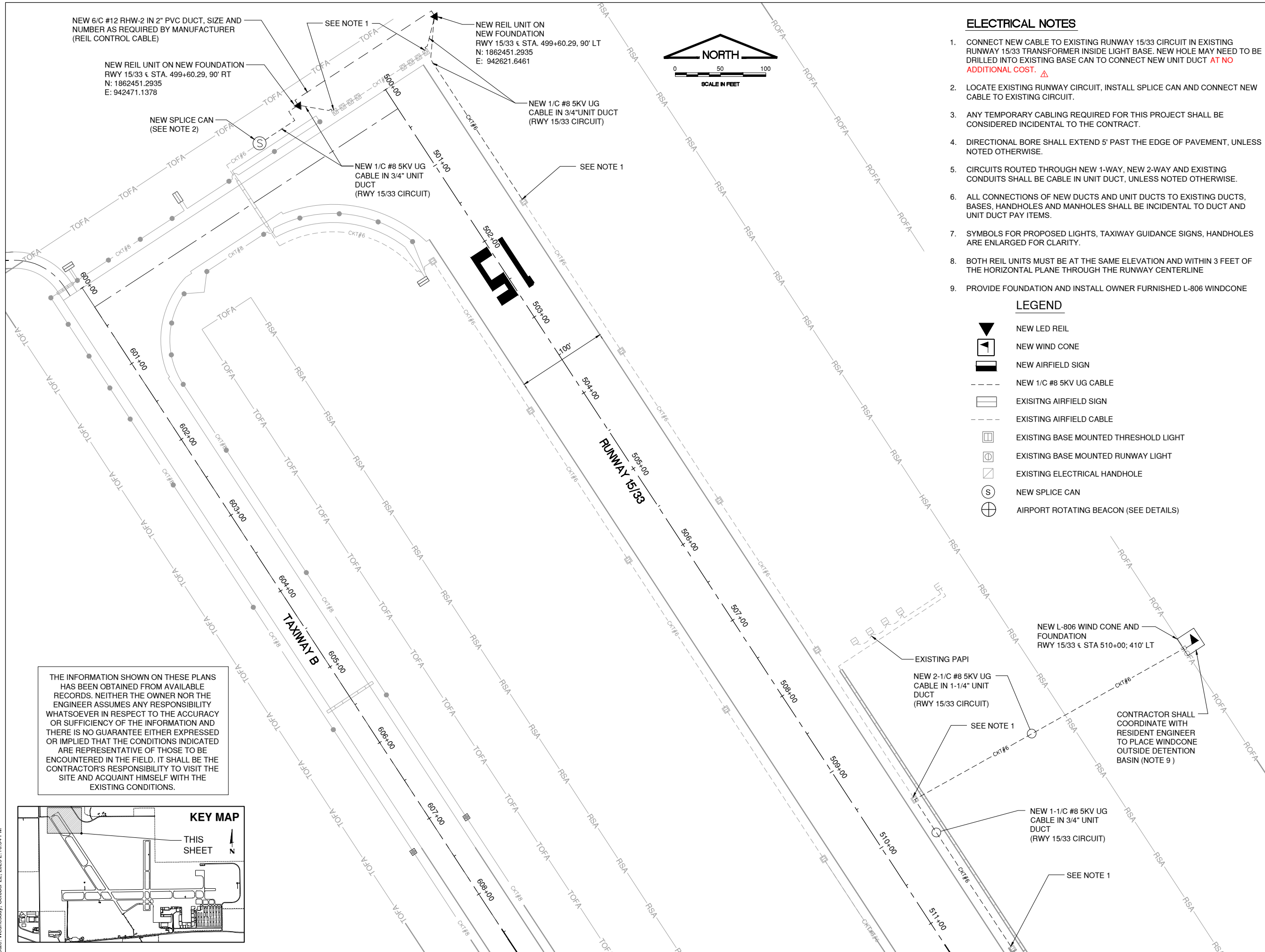
FINAL

REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025

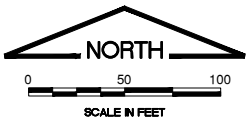
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PROPOSED IMPROVEMENTS - 1



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THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

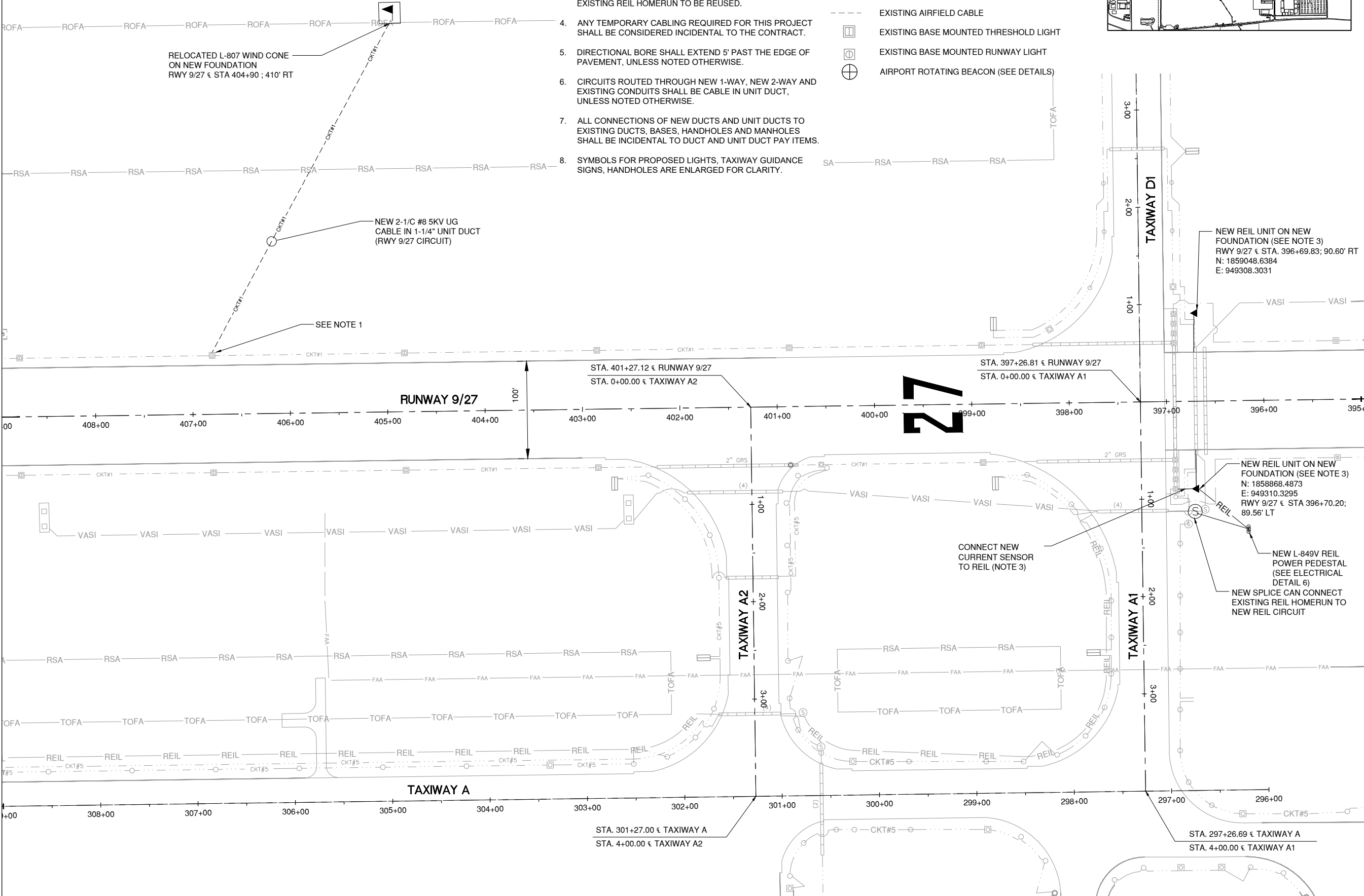
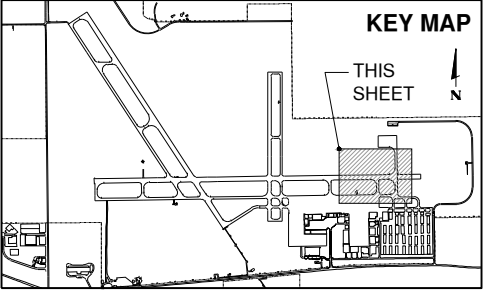


ELECTRICAL NOTES

1. CONNECT NEW CABLE TO EXISTING RUNWAY 9/27 CIRCUIT IN EXISTING RUNWAY 9/27 LIGHT BASE. NEW HOLE MAY NEED TO BE DRILLED INTO EXISTING BASE CAN TO CONNECT NEW UNIT DUCT **AT NO ADDITIONAL COST**.
2. LOCATE EXISTING RUNWAY CIRCUIT, INSTALL SPLICE CAN AND CONNECT NEW CABLE TO EXISTING CIRCUIT.
3. CONNECT NEW REILS TO EXISTING RUNWAY CIRCUIT. EXISTING REIL HOMERUN TO BE REUSED.
4. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
5. DIRECTIONAL BORE SHALL EXTEND 5' PAST THE EDGE OF PAVEMENT, UNLESS NOTED OTHERWISE.
6. CIRCUITS ROUTED THROUGH NEW 1-WAY, NEW 2-WAY AND EXISTING CONDUITS SHALL BE CABLE IN UNIT DUCT, UNLESS NOTED OTHERWISE.
7. ALL CONNECTIONS OF NEW DUCTS AND UNIT DUCTS TO EXISTING DUCTS, BASES, HANDHOLES AND MANHOLES SHALL BE INCIDENTAL TO DUCT AND UNIT DUCT PAY ITEMS.
8. SYMBOLS FOR PROPOSED LIGHTS, TAXIWAY GUIDANCE SIGNS, HANDHOLES ARE ENLARGED FOR CLARITY.

LEGEND

- NEW LED REIL
- NEW WIND CONE
- NEW AIRFIELD SIGN
- NEW 1/C #8 5KV UG CABLE
- EXISTING AIRFIELD SIGN
- EXISTING AIRFIELD CABLE
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- AIRPORT ROTATING BEACON (SEE DETAILS)



CONSULTANTS

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REPLACE AIRFIELD SIGNAGE, WIND CONES, RUNWAY 15 AND 27 RUNWAY END IDENTIFIER LIGHTS, AND AIRPORT ROTATING BEACON

SEPTEMBER 12, 2025



10/22/25 REVISE ELECTRICAL NOTE 1

MARK DATE DESCRIPTION

IL PROJECT NO:
CMT PROJECT NO: 25007812.00
CAD DWG FILE:
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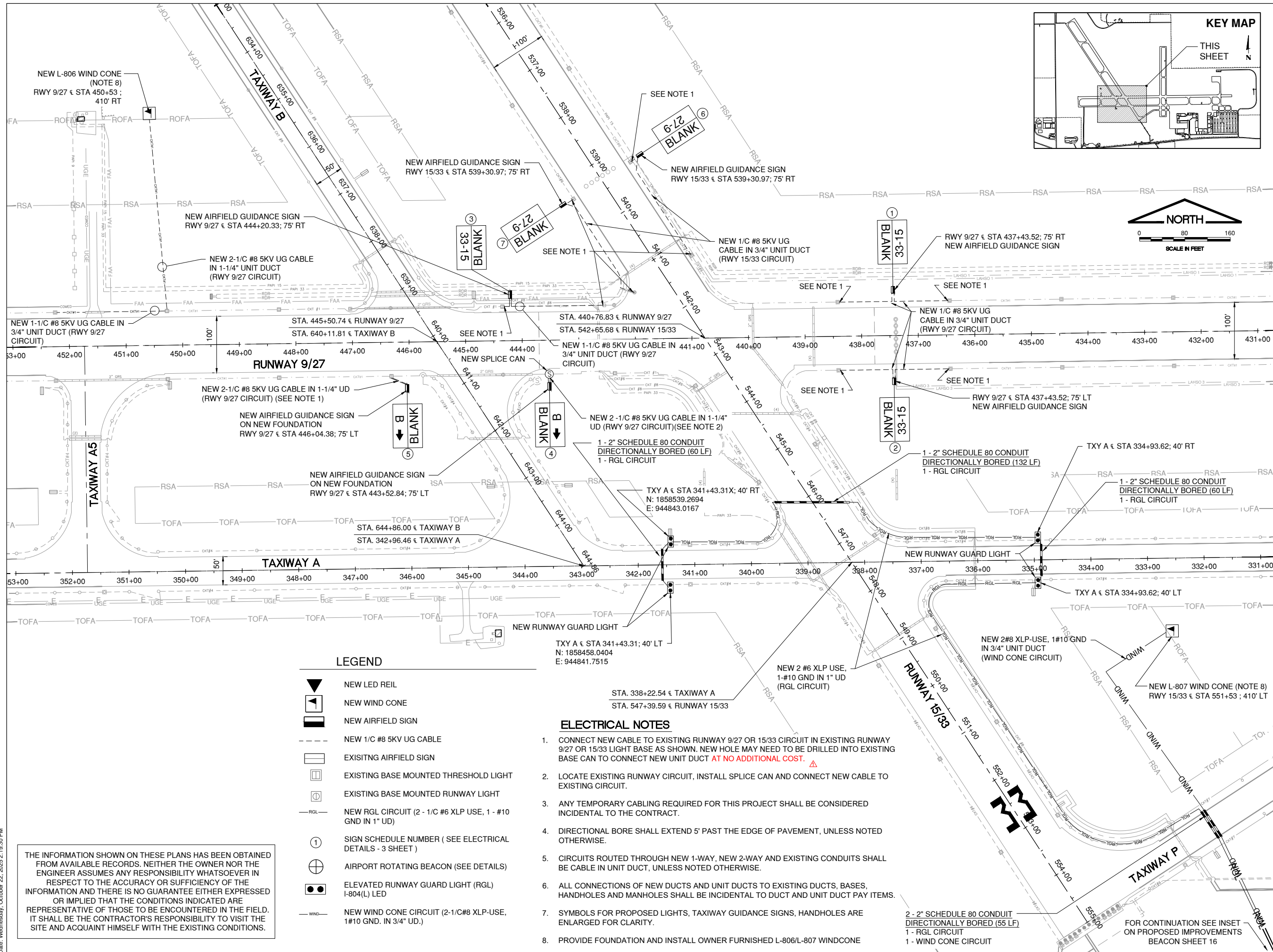
PROPOSED IMPROVEMENTS - 2

FINAL

SEPTEMBER 12, 2025

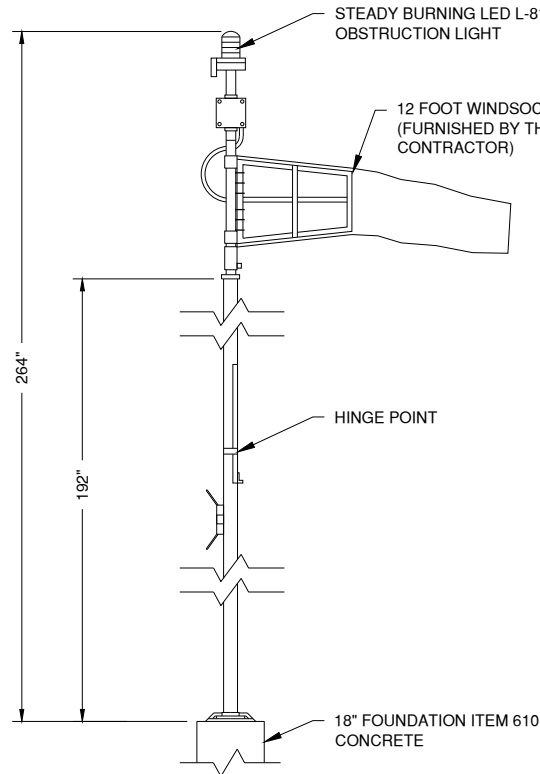


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MARK	DATE	DESCRIPTION
IL PROJECT NO:		
CMT PROJECT NO:	25007812.00	
CAD DWG FILE:		
DESIGNED BY:	CMJ	
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PROPOSED IMPROVEMENTS - 3		
SHEET	15	OF 26



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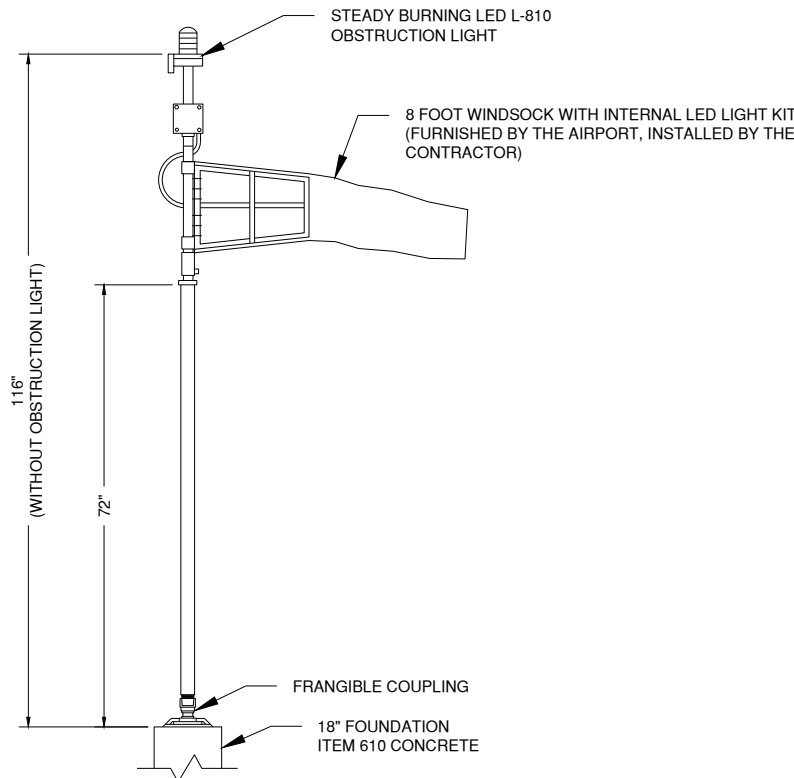
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L-807 LED PRIMARY WIND CONE INTERNALLY LIGHTED

12

N.T.S.



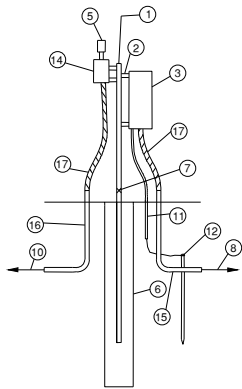
L-806 LED SUPPLEMENTAL WIND CONE INTERNALLY LIGHTED

13

N.T.S.

L-807 WIND CONE POWER LEGEND

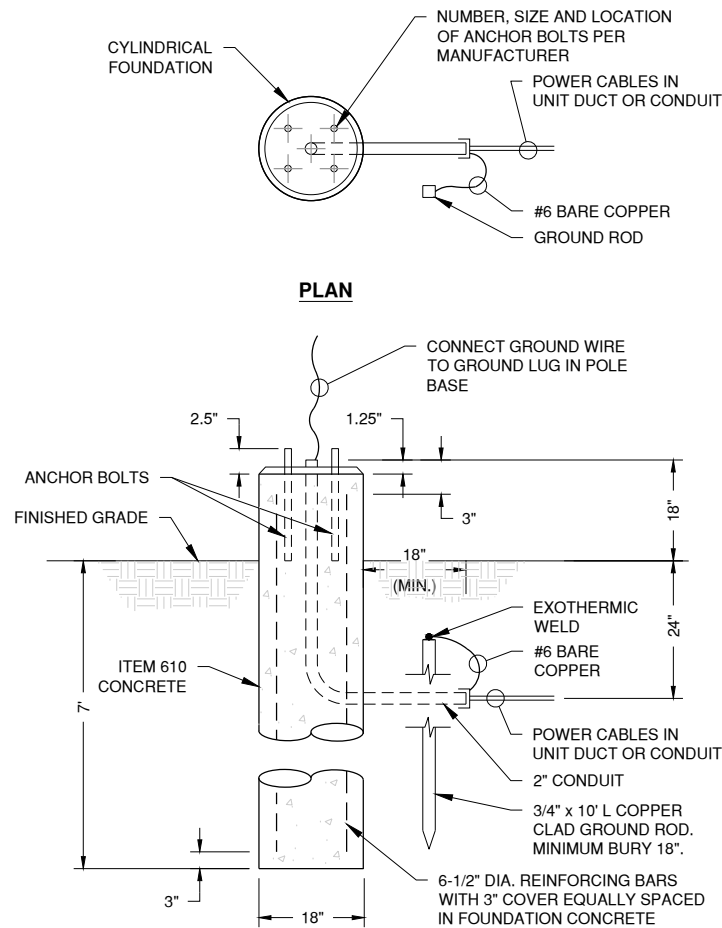
- 2" GALVANIZED STEEL SUPPORT POST WITH END CAPS (TYP. OF 2).
- STRUT-TYPE SUPPORT, UNISTRUT 2000, OR EQUIVALENT (TYP. OF 5).
- HEAVY-DUTY 30A, 600V UNFUSED DISCONNECT IN NEMA 3R ENCLOSURE. PROVIDE GROUND LUGS. PROVIDE LABEL READING: "CAUTION: 120 VOLTS."
- NOT USED.
- WIND CONE PHOTOCELL, TORK MODEL #2101, OR EQUIVALENT. (NOTE: PHOTOCELL OPERATION IS AS FOLLOWS: DURING THE DAYTIME THE WIND CONE LIGHTS ARE OFF AND THE OBSTRUCTION LIGHT IS ON. AFTER DARK THE WIND CONE LIGHTS AND OBSTRUCTION LIGHT ARE ON).
IMPORTANT NOTE:
LOCATE DISCONNECT/ TRANSFORMER/ PHOTOCELL INSTALLATION SUCH THAT WIND CONE LIGHT WILL NOT ADVERSELY AFFECT THE PHOTOCELL OPERATION.
- 12" DIAMETER X 4'-0" DEEP (MIN.) ITEM 610 CONCRETE FOUNDATION. (TYP. OF TWO).
- FRANGIBLE COUPLINGS (TYP. OF 2). INSTALL FRANGIBLE COUPLING NOT MORE THAN 3" ABOVE GRADE.
- TWO #8 XLP-USE (120V). ONE #10 GROUND IN UNITDUCT TO VAULT.
- NOT USED.
- TWO #12 USE (OBSTRUCTION LIGHT 120V POWER), TWO #12 USE (WIND CONE LIGHTS 120V POWER), ONE #10 GROUND IN 2" CONDUIT TO WIND CONE.
- #8 GROUND WIRE IN 1/2" PVC CONDUIT TO GROUND ROD.
- 3/4" DIAMETER X 10' LONG COPPERCLAD GROUND ROD. MIN. BURIAL: 1'-0". BOND GROUND WIRES TO GROUND ROD USING EXOTHERMIC WELD, CADWELD, OR EQUIVALENT. CLAMPED CONNECTIONS SHALL NOT BE ACCEPTABLE.
- NOT USED.
- NEMA 4 JUNCTION BOX SIZED AS REQUIRED TO HOUSE THREE 10A IN-LINE FUSES. PHOTOCELL BYPASS SELECTOR SWITCH AND WEATHERPROOF GFCI CONVENIENCE RECEPTACLE. (SEE WIND CONE SCHEMATIC.)
- 1/2" GRS CONDUIT TO 1'-6" BELOW GRADE.
- 1" GRS CONDUIT TO 1'-6" BELOW GRADE
- LIQUIDTITE FLEXIBLE CONDUIT.



VOLTAGE POWERED WIND CONE

14

N.T.S.

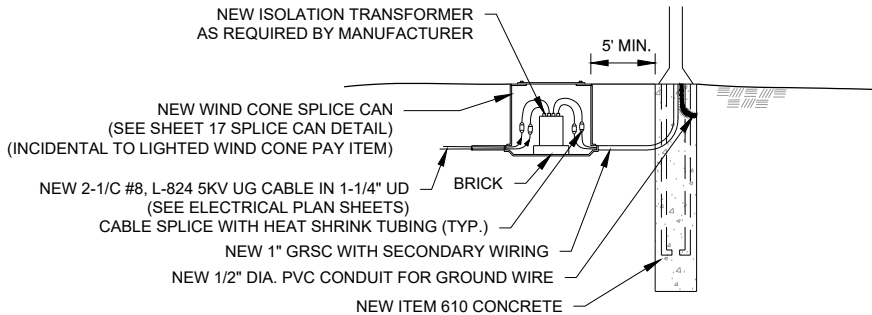


WIND CONE FOUNDATION

15

N.T.S.

NOTE: THIS FOUNDATION DETAIL SHALL ALSO BE USED FOR L-907 WIND CONE RELOCATION.



CURRENT POWERED WIND CONE

16

N.T.S.

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REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025

AURORA
ILLINOIS
CITY OF LIGHTS

AURORA
MUNICIPAL AIRPORT
AURORA MUNICIPAL AIRPORT

10/22/25 ADD NOTE TO DETAIL 15
LEAVE IN DETAIL 14

MARK DATE DESCRIPTION

IL PROJECT NO:

CMT PROJECT NO:

25007812.00

CAD DWG FILE:

DESIGNED BY:

CMJ

DRAWN BY:

JRO

CHECKED BY:

DKP

APPROVED BY:

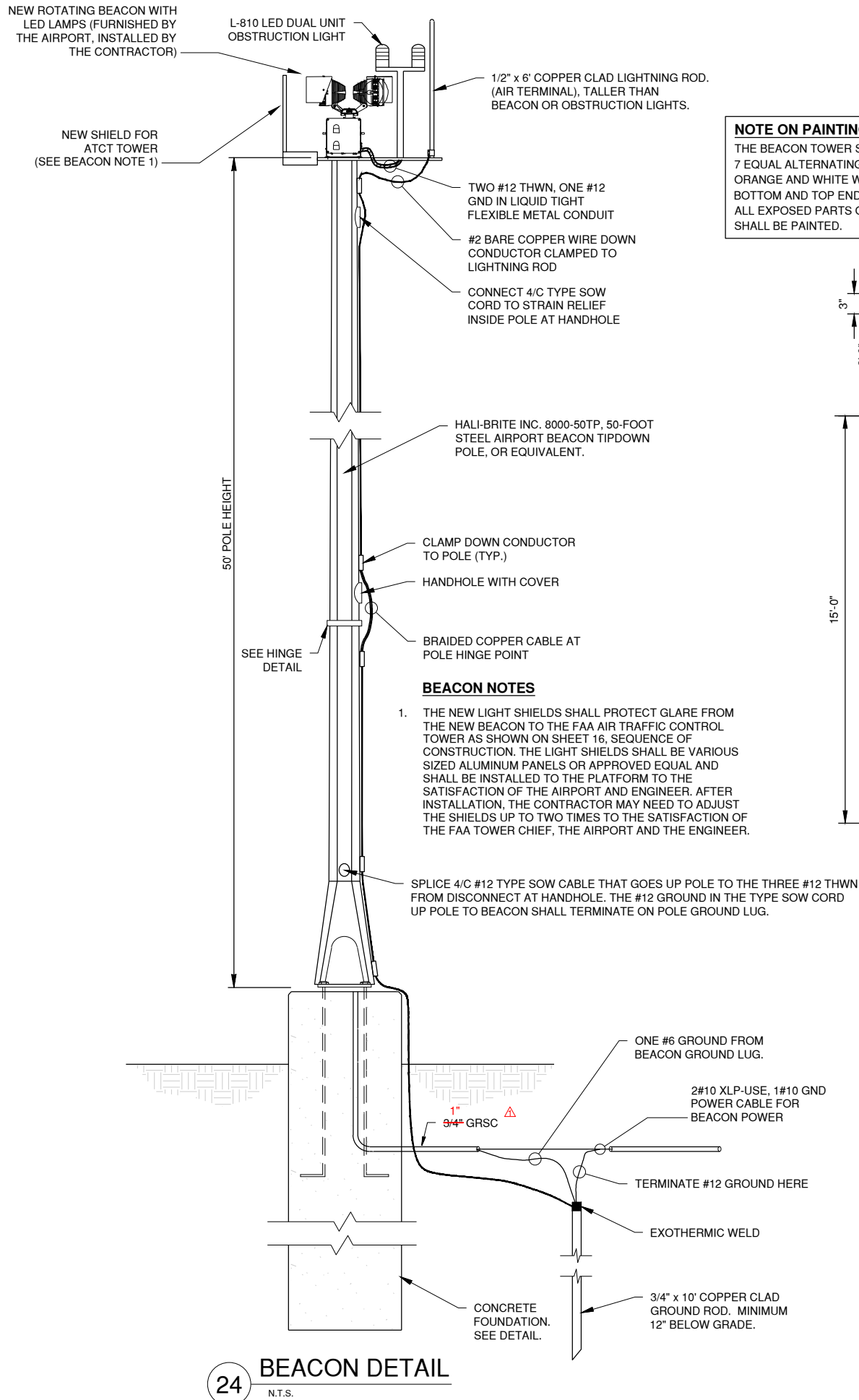
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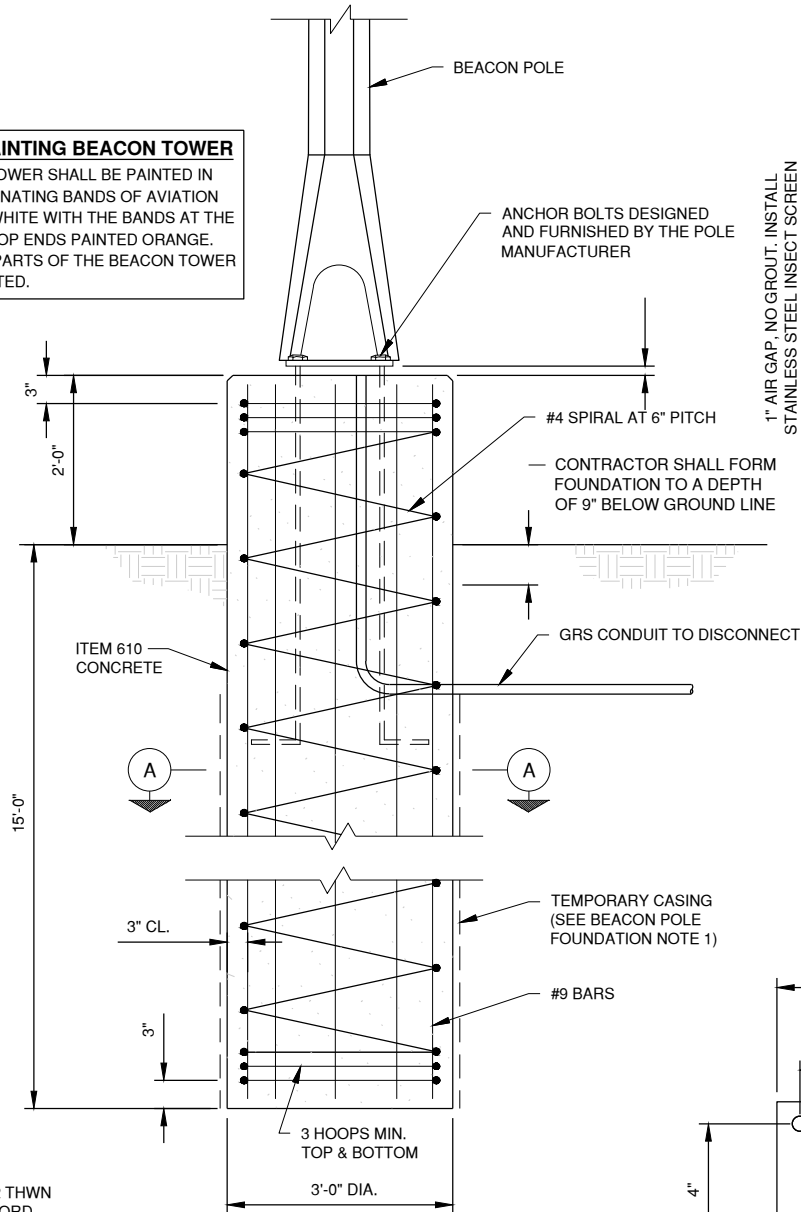
SHEET TITLE

**ELECTRICAL
DETAILS - 4**

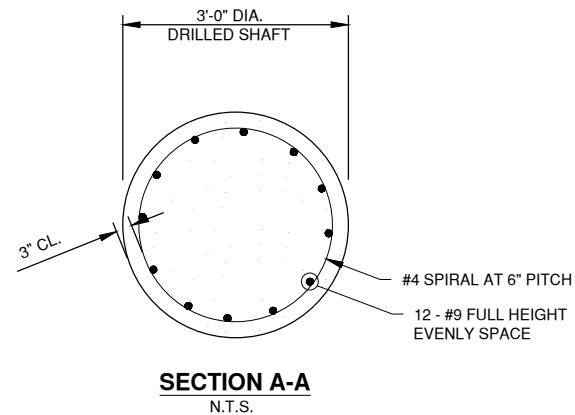
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Date: Friday, October 24, 2025 9:13:02 AM



NOTE ON PAINTING BEACON TOWER
THE BEACON TOWER SHALL BE PAINTED IN 7 EQUAL ALTERNATING BANDS OF AVIATION ORANGE AND WHITE WITH THE BANDS AT THE BOTTOM AND TOP ENDS PAINTED ORANGE. ALL EXPOSED PARTS OF THE BEACON TOWER SHALL BE PAINTED.



1" AIR GAP, NO GROUT, INSTALL STAINLESS STEEL INSECT SCREEN AROUND AIR GAP



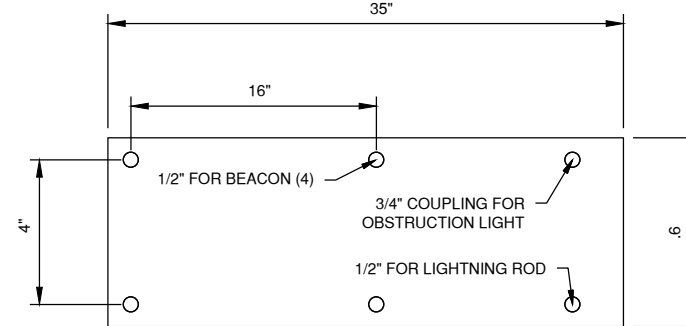
BEACON POLE FOUNDATION NOTES

1. FOUNDATION FOR BEACON SHALL BE BORED/DRILLED. CONSTRUCTION OF DRILLED LIGHT POLE FOUNDATIONS MAY REQUIRE THE USE OF A TEMPORARY CASING, THAT SHALL BE REMOVED DURING THE CONCRETE POURING PROCESS.
2. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 P.S.I. AT 14 DAYS.
3. ALL REINFORCING STEEL SHALL CONFORM TO ASTM A-615, GRADE 60.
4. BEACON FOUNDATION SHALL BE MONOLITHIC. NO CONSTRUCTION JOINTS WILL BE PERMITTED.
5. IF REQUIRED THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN OPERATION PUMPING EQUIPMENT FOR THE COMPLETE DEWATERING OF FOUNDATION EXCAVATION. NO STRUCTURE SHALL BE PERMITTED TO BE CONSTRUCTED IN WHICH ANY AMOUNT OF WATER FLOWS OR IS POOLED. THE COST OF DEWATERING SHALL BE INCIDENTAL TO THE AIRPORT ROTATING BEACON, IN PLACE PAY ITEM.

BEACON POLE FOUNDATION DESIGN

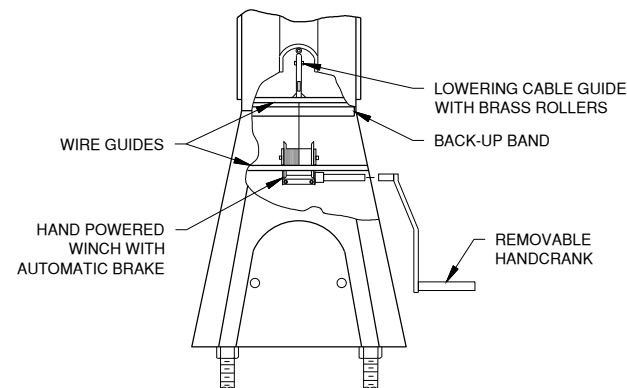
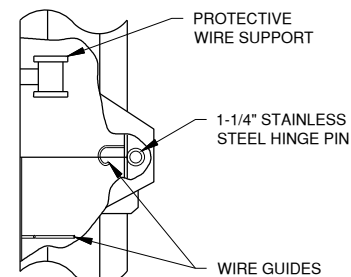
DESIGN LOAD: AASHTO-STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 2001.

DESIGN WIND SPEED: 100 M.P.H.



BEACON PLATFORM DETAIL

N.T.S.



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REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025

AURORA
ILLINOIS
CITY OF LIGHTS

AURORA
MUNICIPAL AIRPORT
AURORA MUNICIPAL AIRPORT

10/22/25 REVISE TITLE FOR DETAIL 25
REVISE DETAIL 24

MARK DATE DESCRIPTION

IL PROJECT NO:
CMT PROJECT NO: 25007812.00
CAD DWG FILE:
DESIGNED BY: CMJ
DRAWN BY: JRO
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SHEET TITLE

ELECTRICAL
DETAILS - 7

SHEET 23 OF 26

FINAL

REPLACE AIRFIELD
SIGNAGE, WIND CONES,
RUNWAY 15 AND 27
RUNWAY END
IDENTIFIER LIGHTS,
AND AIRPORT
ROTATING BEACON

SEPTEMBER 12, 2025



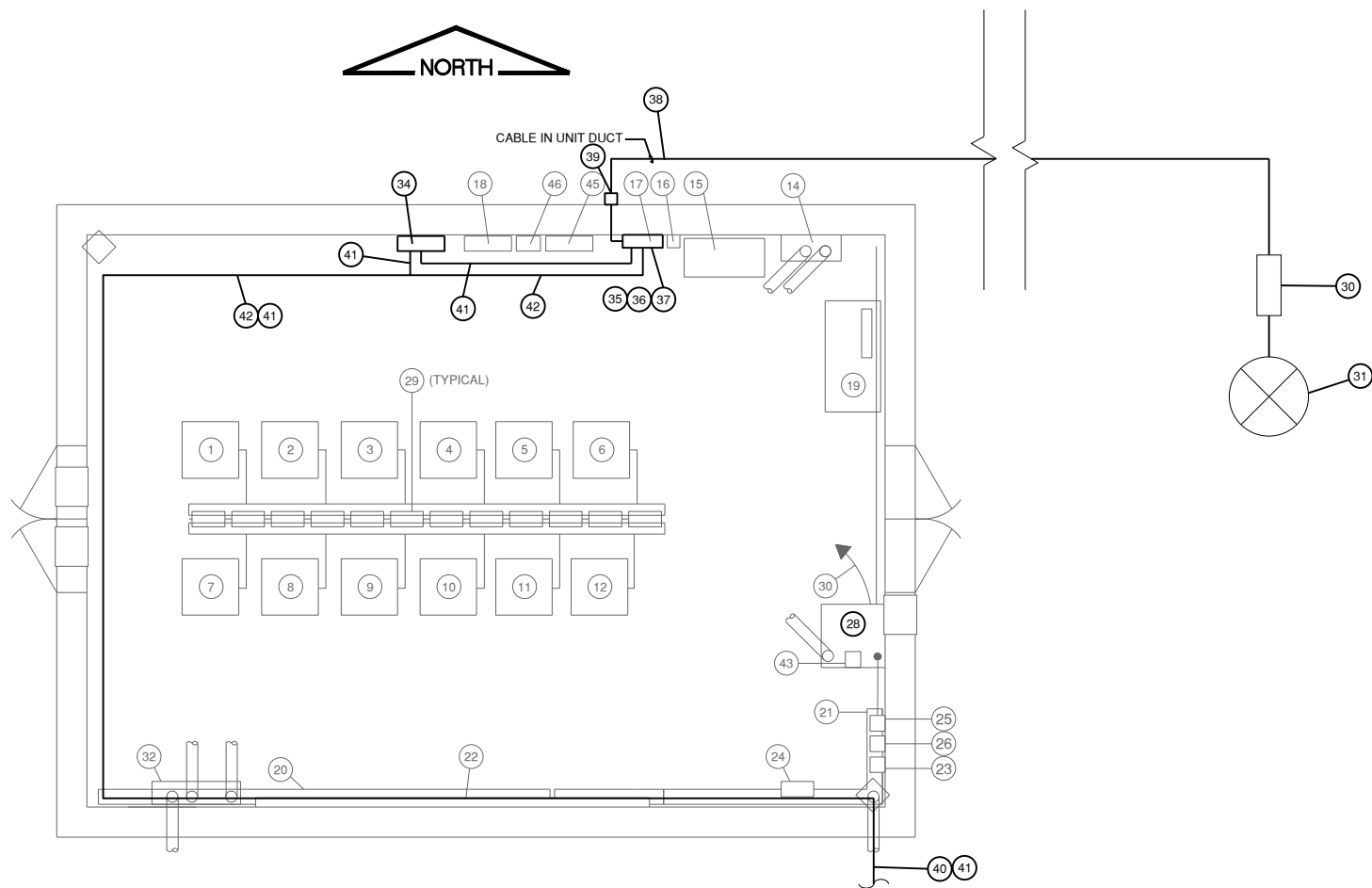
AURORA MUNICIPAL AIRPORT

	10/22/25	REVISE VAULT NOMENCLATURE NOTE 34 REVISE VAULT NOMENCLATURE NOTE 38 ADD NOTE 3
MARK	DATE	DESCRIPTION

IL PROJECT NO:	
CMT PROJECT NO:	25007812.00
CAD DWG FILE:	
DESIGNED BY:	CMJ
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP

SHEET TITLE

ELECTRICAL VAULT DETAILS



EXISTING ELECTRICAL VAULT PLAN

NOT TO SCALE

NOTES

1. EXISTING EQUIPMENT TO REMAIN IS SHOWN FOR INFORMATION ONLY. NEW WORK IS SHOWN IN BOLD.
2. INSTALL LAMACOID NAMEPLATES ON ALL EQUIPMENT.
3. CONTRACTOR SHALL DRILL THROUGH EXISTING VAULT BUILDING FOR NEW CIRCUIT AND SEAL CONDUIT AROUND NEW ENTRANCE AT NO ADDITIONAL COST. 

- ## AURORA MUNICIPAL AIRPORT MAINTENANCE SCHEDULE
-
- | | |
|-----|---|
| 1. | EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "A" EAST. |
| 2. | EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "A" WEST. |
| 3. | EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY "B". |
| 4. | EXISTING 20KW 3-STEP REGULATOR FOR TAXIWAY "C". |
| 5. | EXISTING 15KW 3-STEP REGULATOR FOR TAXIWAY "P". |
| 6. | EXISTING 30KW 3-STEP REGULATOR FOR TAXIWAY SPARE. |
| 7. | EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY SPARE. |
| 8. | EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY 15-33. |
| 9. | EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY SPARE. |
| 10. | EXISTING 30KW 5-STEP REGULATOR FOR RUNWAY 9-27. |
| 11. | EXISTING 7.5KW 3-STEP REGULATOR FOR RUNWAY DISTANCE REMAINING 9-27. |
| 12. | EXISTING 7.5KW 3-STEP REGULATOR FOR RUNWAY DISTANCE REMAINING 9-27 SPARE. |
| 13. | NOT USED. |
| 14. | EXISTING HIGH VOLTAGE LIGHTING PANEL. 600A, 480/277V, 3 PHASE, 4-WIRE (NOTE 10). |
| 15. | EXISTING 150KVA 480-120/208Y, 3 PHASE LIGHTING TRANSFORMER. |
| 16. | EXISTING NORTH AIRFIELD BOOSTER TRANSFORMER / WIND-TEE BOOSTER TRANSFORMER. |
| 17. | EXISTING LOW VOLTAGE LIGHTING PANEL. 225A, 120/208Y, 3 PHASE, 4 WIRE (NOTE 10). |
| 18. | EXISTING APRON LIGHTING PANEL, 225A, 480/277V, 3-PHASE, 4-WIRE (NOTE 10). |
| 19. | EXISTING DESK. |
| 20. | EXISTING 8" BY 8" HIGH VOLTAGE WIREWAY. |
| 21. | EXISTING 8" BY 8" LOW VOLTAGE WIREWAY. |
| 22. | EXISTING PLUG CUTOUT, GROUNDING SWITCH AND INDICATOR LIGHT FOR REGULATORS. |
| 23. | EXISTING INTERFACE CONTROL PANEL FOR LOW VOLTAGE AIRFIELD CIRCUITS, BEACON, WINDTEE AND REILS. |
| 24. | EXISTING CONTACTORS FOR RUNWAY 27 REILS. |
| 25. | EXISTING L-854 RADIO CONTROLLER. INSTALL NEW ANTENNA AND CABLE |
| 26. | EXISTING FIBER OPTIC PATCH PANEL. |
| 27. | NOT USED. |
| 28. | EXISTING L-890 AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM (ALCMS)EQUIPMENT RACK WITH TOUCHSCREEN MONITOR AND PRINTER SHALL BE UPDATED TO INCLUDE NEW PRIMARY WINDCONE, NEW BEACON, AND NEW RUNWAY GUARD LIGHTS. |
| 29. | EXISTING INTERFACE CONTROL PANEL AND INSULATION RESISTANCE MEASURING SYSTEM MOUNTED ON UNISTRUT FRAME. (NOTE 9) |
| 30. | NEW BEACON PEDESTAL WITH DISCONNECT AND PHOTOCELL. |
| 31. | NEW ROTATING BEACON WITH LED LAMPS. SEE DETAILS |
| 32. | EXISTING L-823 SPLICE ENCLOSURE. SEE DETAILS. |
| 33. | EXISTING C.T. CABINET AND UTILITY METER. |
| 34. | NEW LIGHTING CONTACTOR FOR RGL CIRCUIT WITH HOA SELECTOR SWITCH. △ |
| 35. | NEW 20A/2P CIRCUIT BREAKER FOR RGL CIRCUIT. |
| 36. | NEW 20A/1P CIRCUIT BREAKER FOR NEW BEACON |
| 37. | NEW 20A/1P CIRCUIT BREAKER FOR NEW L-807 WIND CONE |
| 38. | NEW 2#10 XLP-USE, 1#10 GND IN 1" PVC SCH. 80 CONDUIT FOR BEACON. △ |
| 39. | NEW NEMA 3R JUNCTION BOX 4" X 4"X 4" FOR BEACON CIRCUIT. |
| 40. | NEW CIRCUITS FROM VAULT TO LOW VOLTAGE DUCT BANK
-NEW RGL CIRCUIT, 2#6 XLP-USE, 1#10 GND
-NEW WIND CONE CIRCUIT, 2#3 XLP-USE, 1#10 GND |
| 41. | NEW RGL CIRCUIT, 2#6 XLP-USE, 1#10 GND. |
| 42. | NEW WIND CONE CIRCUIT, 2#3 XLP-USE, 1#10 GND. |
| 43. | EXISTING FIBER OPTIC CABLE PATCH PANEL (NOTE 8) |
| 44. | NOT USED. |
| 45. | EXISTING 200A, 480V, 3-POLE CIRCUIT BREAKER DISCONNECT FOR APRON LIGHTING. |
| 46. | EXISTING 200A, 3-POLE LIGHTING CONTACTOR WITH H-O-A SELECTOR SWITCH AND PHOTOCELL FOR APRON LIGHTING. |

8. BIDDING REQUIREMENTS AND BASIS OF AWARD. When alternates are included in the proposal, the following shall apply:

a. Additive Alternates

(1) Bidders must submit a bid for the Base Bid and for all Additive Alternates.

(2) Award of this contract will be made to the lowest responsible qualified bidder computed as follows:

The lowest aggregate amount of (i) the Base Bid plus (ii) any Additive Alternate(s) which the Department elects to award based on the availability of funding.

Award of this contract will be limited to the following bid alternate combinations:

- I. Base Bid
- II. Base Bid + Additive Alternate 1
- III. Base Bid + Additive Alternate 1 + Additive Alternate 2
- IV. Base Bid + Additive Alternate 1 + Additive Alternate 2 + Additive Alternate 3

The Department may elect not to award any Additive Alternates. In that case, award will be to the lowest responsible qualified bidder of the Base Bid.

b. Optional Alternates

(1) Bidders must submit a bid for the Base Bid and for either Alternate A or Alternate B or for both Alternate A and Alternate B.

(2) Award of this contract will be made to the lowest responsible qualified bidder computed as follows:

The lower of the aggregate of either (i) the Base Bid plus Alternate A or (ii) the Base Bid plus Alternate B.

9. CONTRACT TIME. The Contractor shall complete all work within the specified contract time. Any calendar day extension beyond the specified contract time must be fully justified, requested by the Contractor in writing, and approved by the Engineer, or be subject to liquidated damages.

The contract time for this contract is 2735 calendar days.

10. INDEPENDENT WEIGHT CHECKS. The Department reserves the right to conduct random unannounced independent weight checks on any delivery for bituminous, aggregate or other pay item for which the method of measurement for payment is based on weight. The weight checks will be accomplished by selecting, at random, a loaded truck and obtaining a loaded and empty weight on an independent scale. In addition, the department may perform random weight checks by obtaining loaded and empty truck weights on portable scales operated by department personnel.

11. MATERIAL COST ADJUSTMENTS. Federal Aviation Administration rules prohibit the use of escalation clauses for materials. Therefore, the Illinois Department of Transportation, Division of Aeronautics cannot offer any material cost adjustment provisions for projects that utilize Federal Funds.

12. GOOD FAITH COMPLIANCE. The Illinois Department of Transportation has made a good faith effort to include all statements, requirements, and other language required by federal and state law and by various offices within federal and state governments whether that language is required by law or not. If anything of this nature has been left out or if additional language etc. is later required, the bidder/contractor shall cooperate fully with the Department to modify the contract or bid documents to correct the deficiency. If the change results in increased operational costs, the Department shall reimburse the contractor for such costs as it may find to be reasonable.

By Order of the
Illinois Department of Transportation

Gia Biagi,
Secretary

It being understood and agreed that the completion within the time limit is an essential part of the contract, the bidder agrees to complete the work within ~~27~~³⁵ calendar days, unless additional time is granted by the Engineer in accordance with the provisions of the specifications. In case of failure to complete the work on or before the time named herein, or within such extra time as may have been allowed by extensions, the bidder agrees that the Department of Transportation shall withhold from such sum as may be due him/her under the terms of this contract, the costs, as set forth in Section 80-09 Failure to Complete on Time of the Standard Specifications, which costs shall be considered and treated not as a penalty but as damages due to the State from the bidder by reason of the failure of the bidder to complete the work within the time specified in the contract.

ILLINOIS WORKS APPRENTICESHIP INITIATIVE – STATE FUNDED CONTRACTS (BDE)

Effective: June 2, 2021

Revised: April 2, 2024

Illinois Works Jobs Program Act (30 ILCS 559/20-1 et seq.). For contracts having an awarded contract value of \$500,000 or more, the Contractor shall comply with the Illinois Works Apprenticeship Initiative (30 ILCS 559/20-20 to 20-25) and all applicable administrative rules. The goal of the Illinois Apprenticeship Works Initiative is that apprentices will perform either 10% of the total labor hours actually worked in each prevailing wage classification or 10% of the estimated labor hours in each prevailing wage classification, whichever is less. Of this goal, at least 50% of the labor hours of each prevailing wage classification performed by apprentices shall be performed by graduates of the Illinois Works Pre-Apprenticeship Program, the Illinois Climate Works Pre-Apprenticeship Program, or the Highway Construction Careers Training Program.

The Contractor may seek from the Department of Commerce and Economic Opportunity (DCEO) a waiver or reduction of this goal in certain circumstances pursuant to 30 ILCS 559/20-20(b). The Contractor shall ensure compliance during the term of the contract and will be required to report on and certify its compliance. An apprentice use plan, apprentice hours, and a compliance certification shall be submitted to the Engineer on forms provided by the Department and/or DCEO.

SPECIAL PROVISION FOR SUBMISSION OF BIDDERS LIST INFORMATION (BDE)

Effective: January 2, 2025

Revised: March 2, 2025

In accordance with 49 CFR 26.11(c) all DBE and non-DBEs who bid as prime contractors and subcontractors shall provide bidders list information, including all DBE and non-DBE firms from whom the bidder has received a quote or bid to work as a subcontractor, whether or not the bidder has relied upon that bid in placing its bid as the prime contractor.

The bidders list information shall be submitted with the bid using the link provided within the “Integrated Contractor Exchange (iCX)” application of the Department’s “EBids System”.