

Bench Mark: Survey marker on top of parapet at NW corner of bridge. Sta. 49+04.62, Offset 18.57' Rt., Elev. 699.62

Existing Structure: S.N. 081-0107 originally built in 1971 as F.A.I. Rte. 74, Section 81-1HB-4. The concrete deck was replaced in 1991. The existing structure consists of a 2 span continuous steel plate girder bridge with reinforced concrete deck. The approaches consist of single span vaulted abutments with prestressed concrete girders. The abutments, approach bents, and hammerhead pier are supported on drilled piers. The back to back approach bents length is 251'-1³/₄" and the out to out deck width is 32'-8". The bridge will be closed and no staging will be required during construction.

No salvage.

INDEX OF SHEETS

- 1. General Plan & Elevation
- 2. Structure Removal Details
- 3. Superstructure Details - 1
- 4. Superstructure Details - 2
- 5. Bicycle Railing, Curved & Parapet Railing - 1
- 6. Bicycle Railing, Curved & Parapet Railing - 2
- 7. Preformed Joint Strip Seal - Sidewalk - 1
- 8. Preformed Joint Strip Seal - Sidewalk - 2

DESIGN SPECIFICATIONS

2002 Standard Specifications for Highway Bridges, 17th Edition
2006 Seismic Retrofitting Manual for Highway Structures:
Part I - Bridges (FHWA-HRT-06-032)

SEISMIC DATA

Seismic Retrofit Category (SRC) = A
Design Spectral Acceleration at 1.0 sec (SD1) = 0.087g
Design Spectral Acceleration at 0.2 sec (SDS) = 0.127g
Soil Site Class = D
Performance Level = 1

LOADING HS20-44 & ALT.

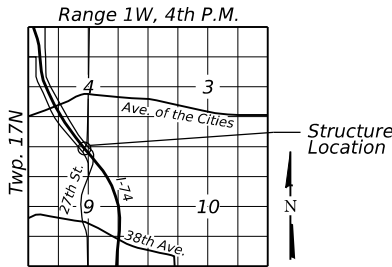
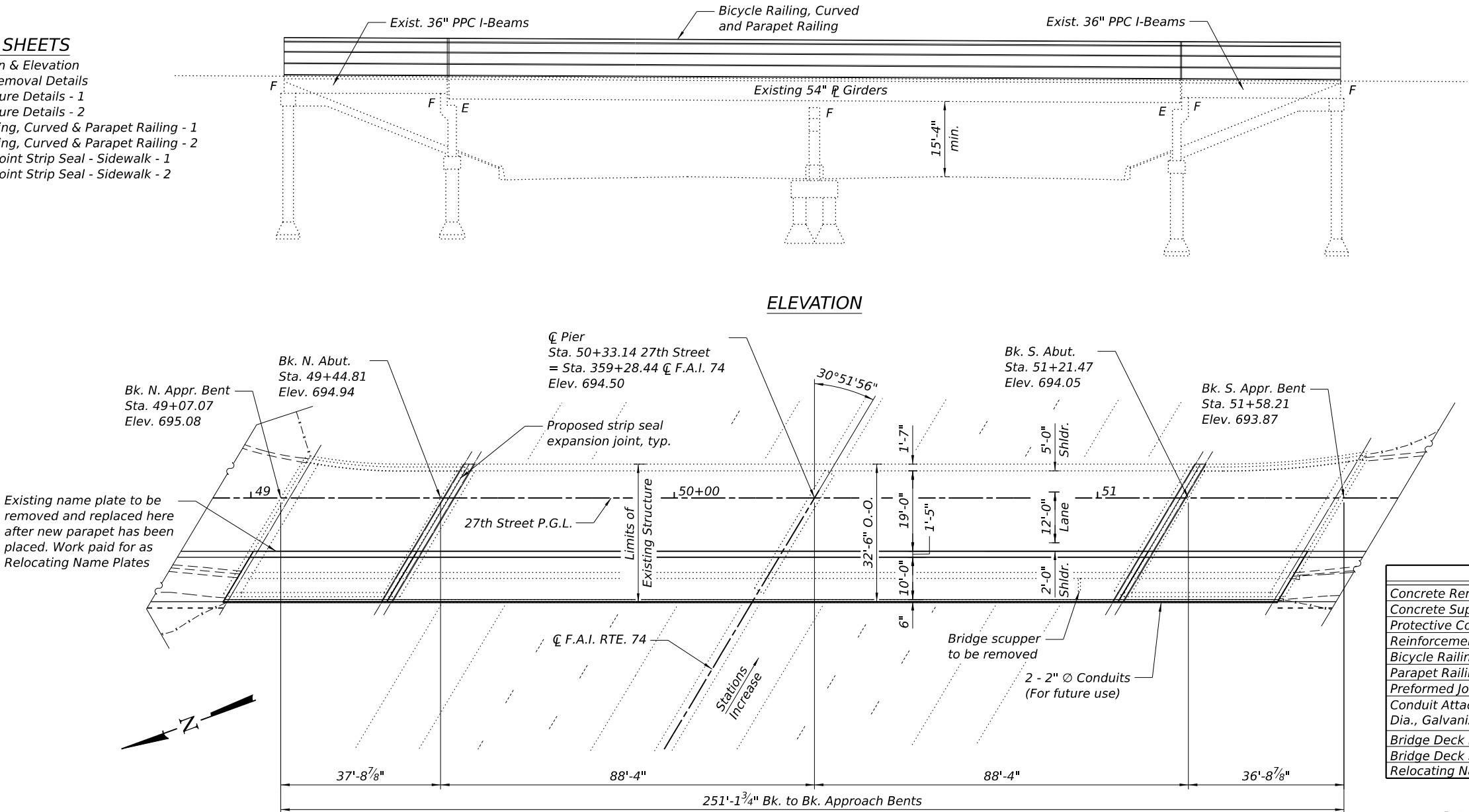
Allow 25#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS
NEW CONSTRUCTION
f_c = 4,000 psi (Superstructure Concrete)
f_y = 60,000 psi (Reinforcement)

EXISTING STRUCTURE
f_c = 3,500 psi (Superstructure Concrete)
f_y = 60,000 psi (Reinforcement)
f_s = 20,000 psi (Structural Steel)

EXISTING PRECAST PRESTRESSED UNITS
f_c = 5,000 psi
f_s = 248,000 psi (7/16"Ø strands)
f_{si} = 173,600 psi (7/16"Ø strands)
f_{ci} = 4,000 psi

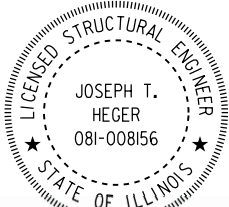
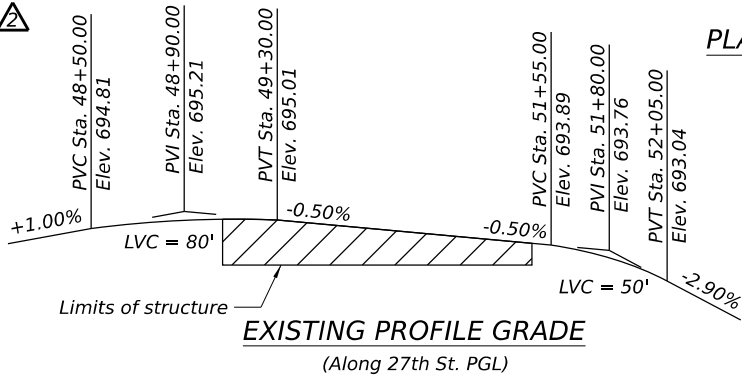


TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	94.2		94.2
Concrete Superstructure	Cu. Yd.	44.6		44.6
Protective Coat	Sq. Yd.	114		114
Reinforcement Bars, Epoxy Coated	Pound	7,440		7,440
Bicycle Railing, Curved	Foot	249		249
Parapet Railing	Foot	249		249
Preformed Joint Strip Seal	Foot	77		77
Conduit Attached to Structure, 2" Dia., Galvanized Steel Finish Only	Foot	498		498
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	291		291
Bridge Deck Scarification 2 1/4"	Sq. Yd.	291		291
Relocating Name Plates	Each	1		1

SCOPE OF WORK

- 1. Set-up traffic control and close roadway.
- 2. Remove raised sidewalk, barriers at each side of sidewalk, and perform deck scarification and add a latex concrete overlay within limits of proposed 10' path.
- 3. Replace expansion joints full width with new strip seal expansion joints.
- 4. Overlay top 2" of deck at the 10' path and construct new barriers each side of path.



"I certify that to the best of my knowledge, information and belief, this design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of the structure and complies with the requirements of the current Design Specifications listed"

APPROVED
For Structural Adequacy Only
Joseph T. Heger
Engineer of Bridges & Structures

GENERAL PLAN & ELEVATION
27TH STREET SB OVER F.A.I. RTE. 74
F.A.U. RTE. 5836 - SECTION 23-00291-00-BT
ROCK ISLAND COUNTY
STATION 50+33.14
STRUCTURE NO. 081-0107

MODEL: D:\default
FILE NAME: 23-00291-00-BT-General Plan.dgn
License No. 184-000613



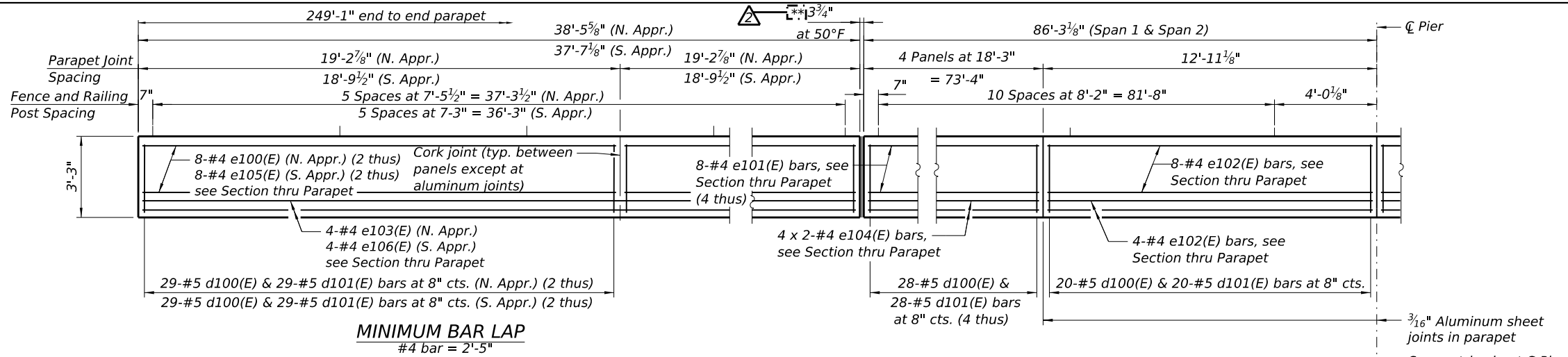
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	DATE - OCTOBER 2024	REvised -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 081-0107

SHEET 1 OF 8 SHEETS

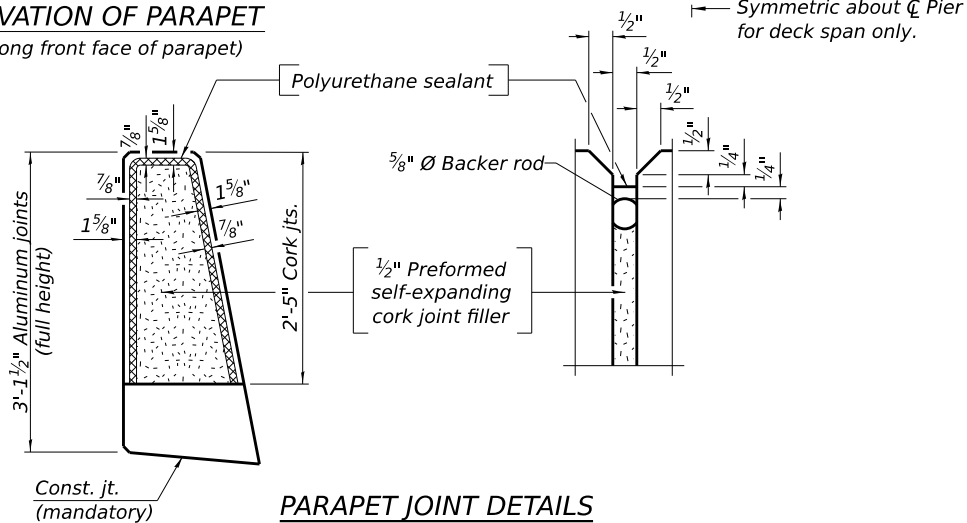
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5836	23-00291-00-BT	ROCK ISLAND	184	90
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



2
** Measured along front face of parapet without consideration of 2" chamfer.

INSIDE ELEVATION OF PARAPET

(Measured along front face of parapet)

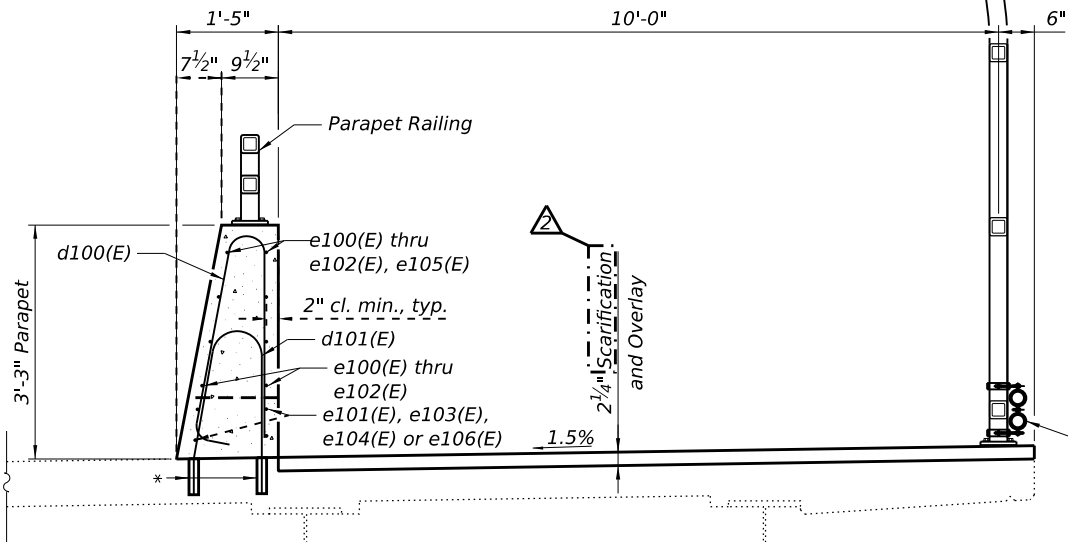


PARAPET JOINT DETAILS

Notes:

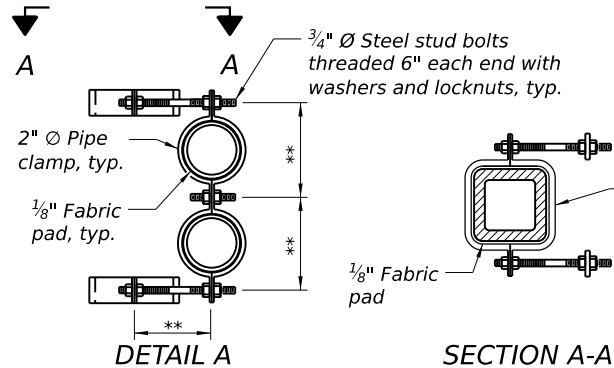
The $\frac{3}{16}$ " aluminum sheet shall be ASTM B 209 alloy 3003-H14 and coated with 5 mils of either bitumen paint or epoxy paint to minimize reaction with wet concrete.
The polyurethane sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray.
The clamping device for 2" conduits shall be galvanized according to AASHTO M 232. Cost of clamping device included with Conduit Attached to Structure.
Bicycle Railing, Curved and Parapet Railings shall be galvanized. See Sheet 6 of 8.

2 - 2" Ø Galvanized Steel Conduits for future use supported at each bicycle railing post, see Detail A. Include pipe end caps on each end of bridge.



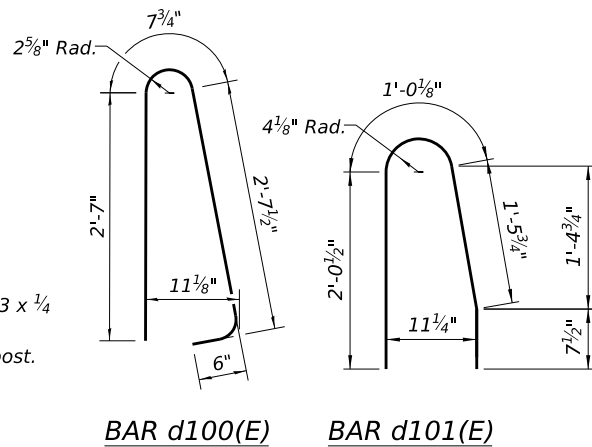
* Drill and set #5 d101(E) bar according to Article 509.06 of the Standard Specifications. Drilled holes shall be roughened or scored per manufacturer's recommendations. Maximum depth of hole shall not exceed 6". Contractor shall take all necessary precautions to prevent drilled hole interference with deck reinforcement bars. Locate longitudinal bars to miss drilled locations. Locate drilled holes to miss transverse bars in deck.

SECTION THRU SIDEWALK



** Dimension as required for pipe clamp

SECTION A-A



BAR d100(E)

BAR d101(E)

SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a100(E)	24	#5	37'-5"	—
d100(E)	380	#5	6'-5"	Λ
d101(E)	380	#5	5'-2"	Λ
e100(E)	16	#4	18'-11"	—
e101(E)	64	#4	17'-11"	—
e102(E)	16	#4	12'-7"	—
e103(E)	4	#4	38'-2"	—
e104(E)	16	#4	39'-0"	—
e105(E)	16	#4	18'-4"	—
e106(E)	4	#4	37'-4"	—
Concrete Superstructure			Cu. Yds.	44.6
Protective Coat			Sq. Yd.	114
Reinforcement Bars, Epoxy Coated			Pound	7,440
Conduit Attached to Structure, 2" Dia. Galvanized Steel			Foot	498
Bridge Deck Latex Concrete Overlay, 2 1/4"			Sq. Yds.	291
Bridge Deck Scarification, 2 1/4"			Sq. Yds.	291

Bars indicated thus 4 x 2-#4 etc. indicates 4 line of bars with 2 lengths per line.



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CHECKED - JTH/DH
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DATE - SEPTEMBER 2024

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DRAWN - DRC
CHECKED - JTH/DH
DATE - SEPTEMBER 2024

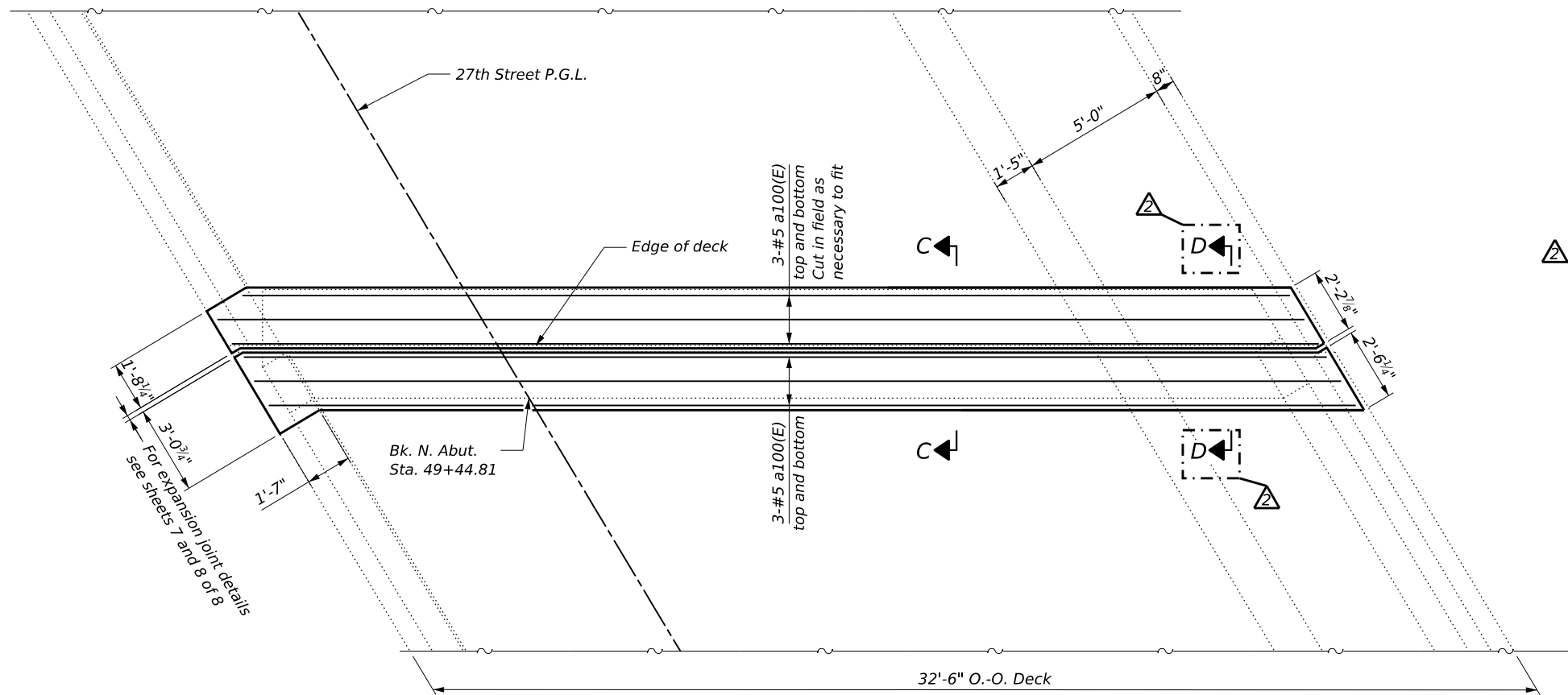
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REVISED - 11/04/2025
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

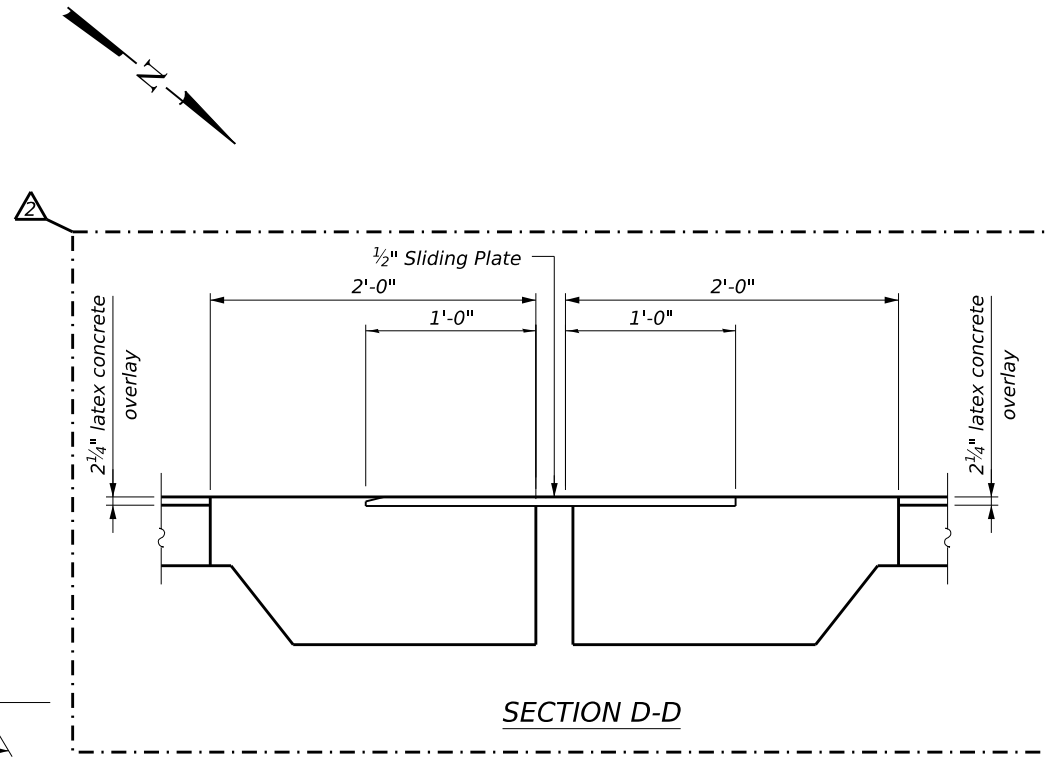
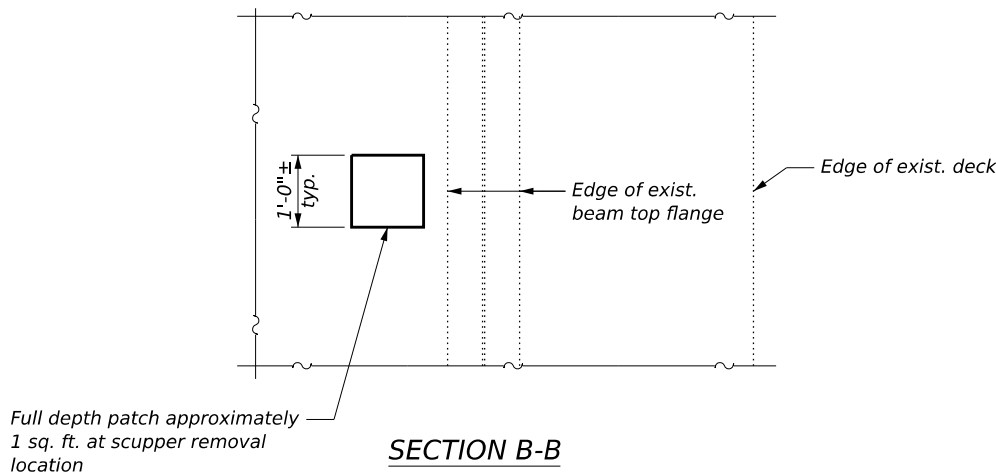
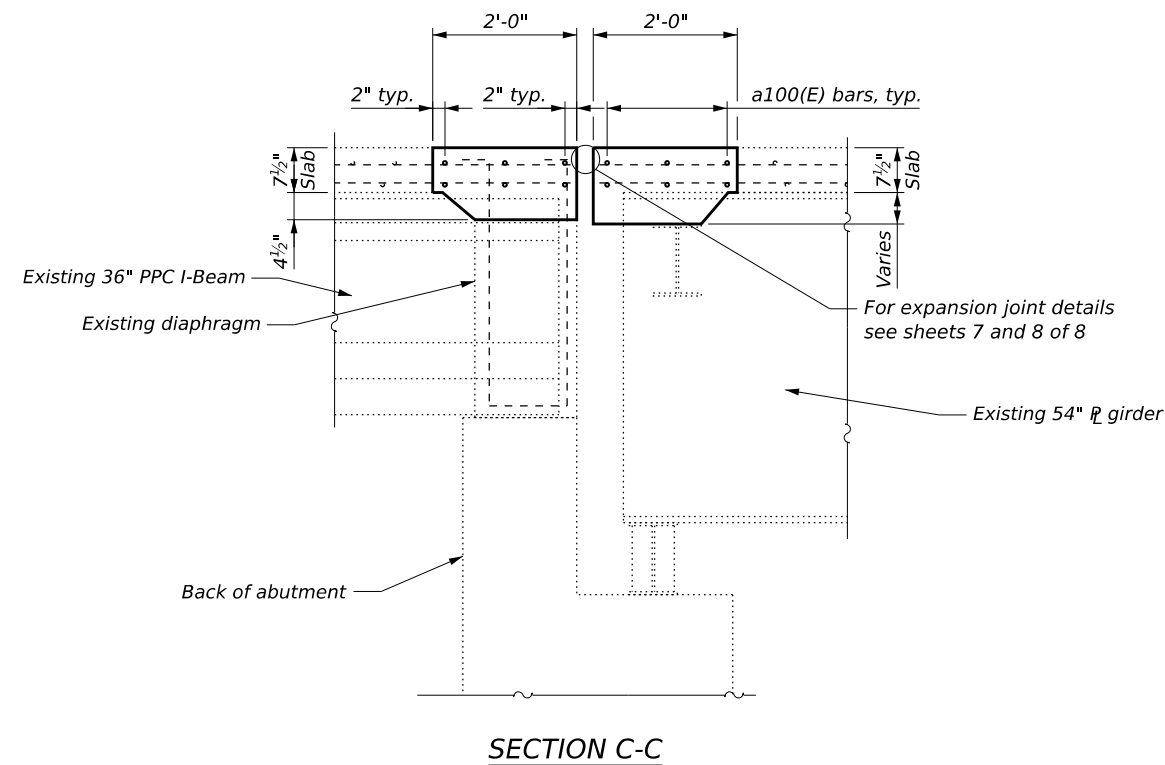
SUPERSTRUCTURE DETAILS - 1
STRUCTURE NO. 081-0107

SHEET 3 OF 8 SHEETS

F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
5834 23-00291-00-BT ROCK ISLAND 184 92
CONTRACT NO.
ILLINOIS FED. AID PROJECT



PLAN - NORTH ABUTMENT
(North abut. shown, south abut. similar)



Notes:
See sheets 7 and 8 of 8 for Preformed Joint Details.
The a100(E) bars shall be placed parallel to the end of deck.
Full depth patching at existing scupper removal location shall be completed prior to placing new barrier and deck overlay.
See sheet 2 of 8 for location of Section B-B.

MODEL: Deck Overlay (sheet)
FILE NAME: 49000001 Superstructure Details-2.dgn



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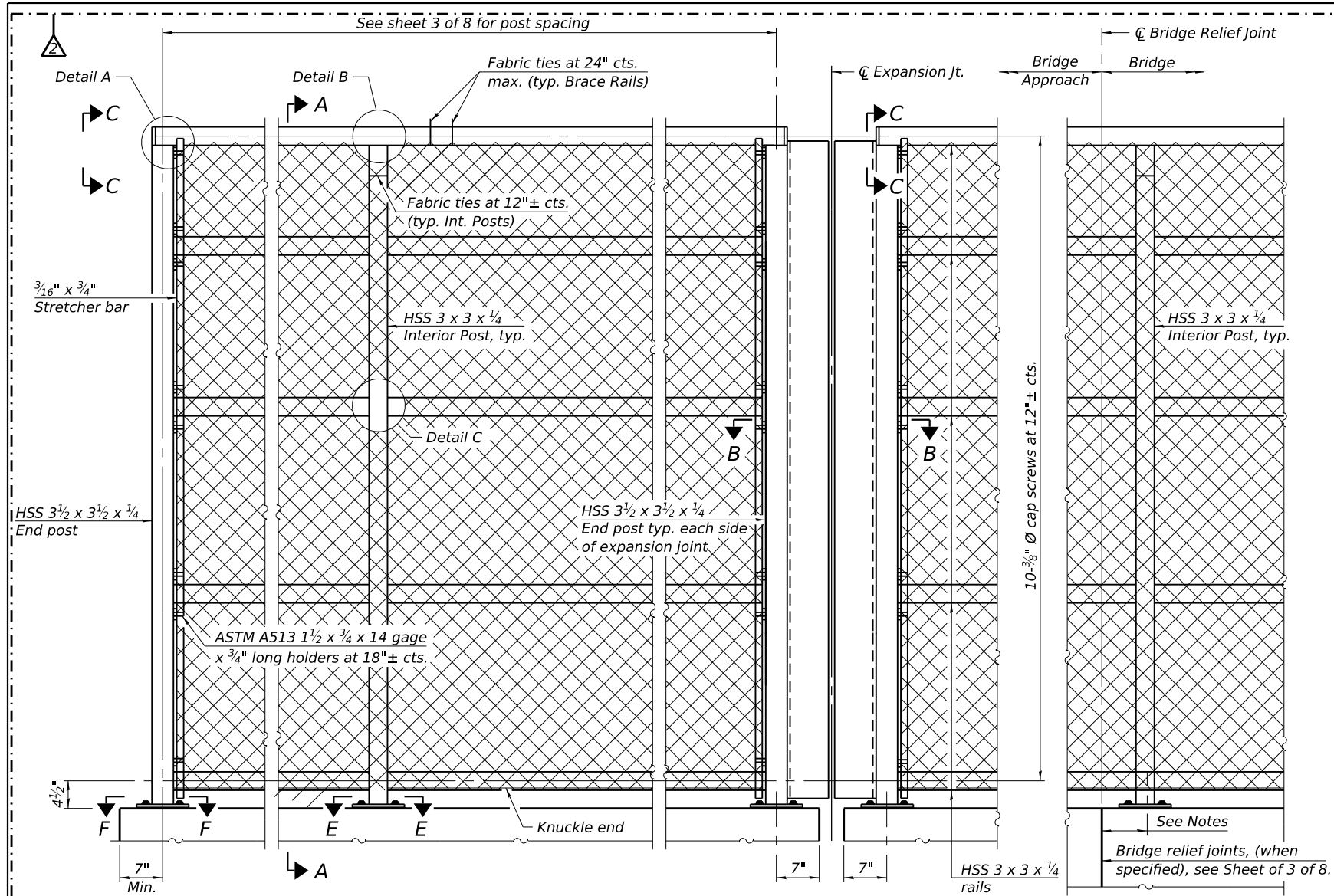
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

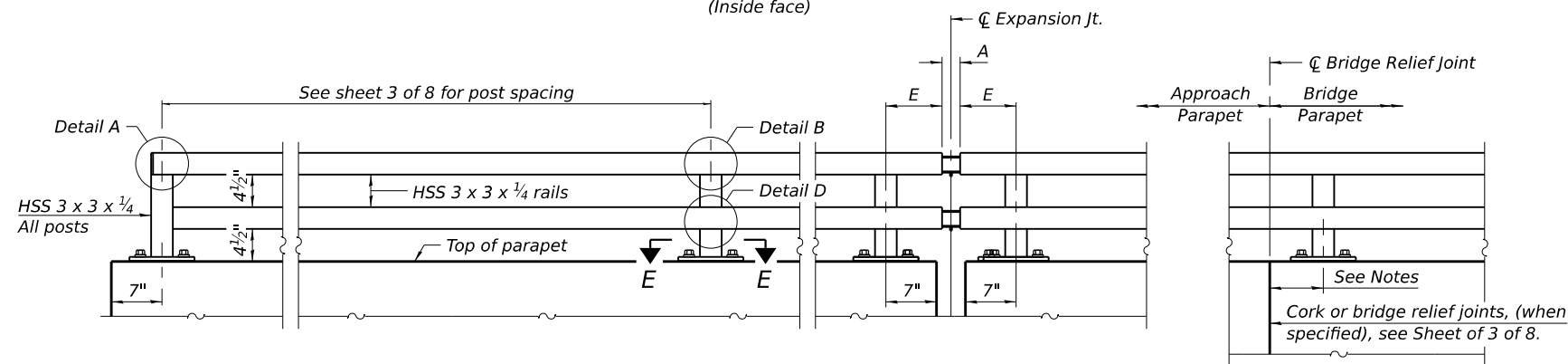
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STRUCTURE NO. 081-0107

SHEET 4 OF 8 SHEETS

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CONTRACT NO.				
ILLINOIS FED. AID PROJECT				



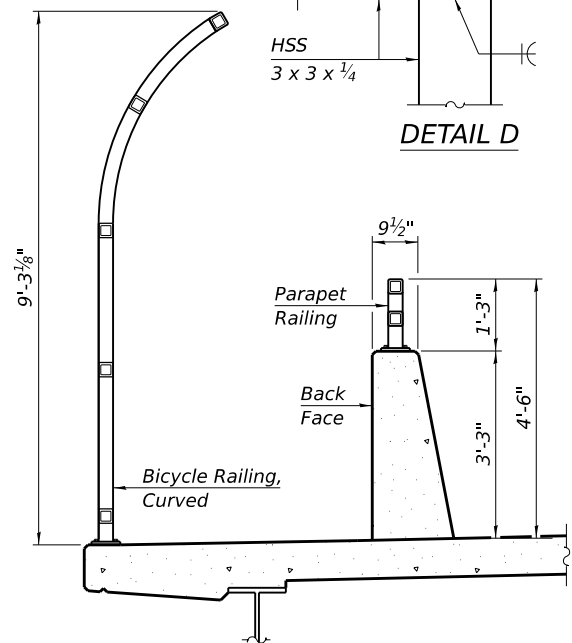
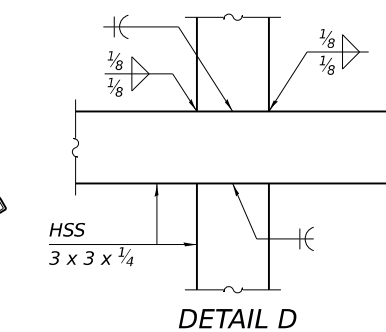
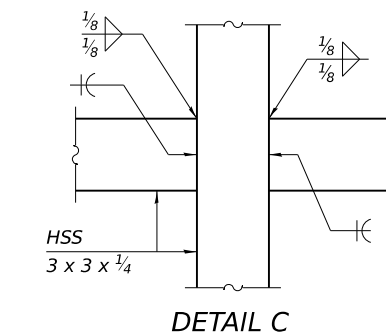
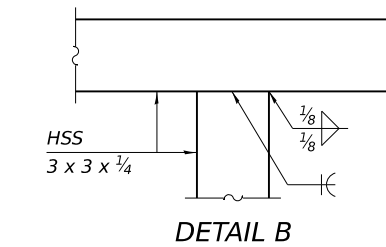
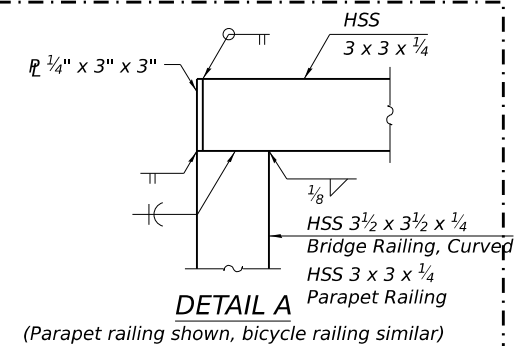
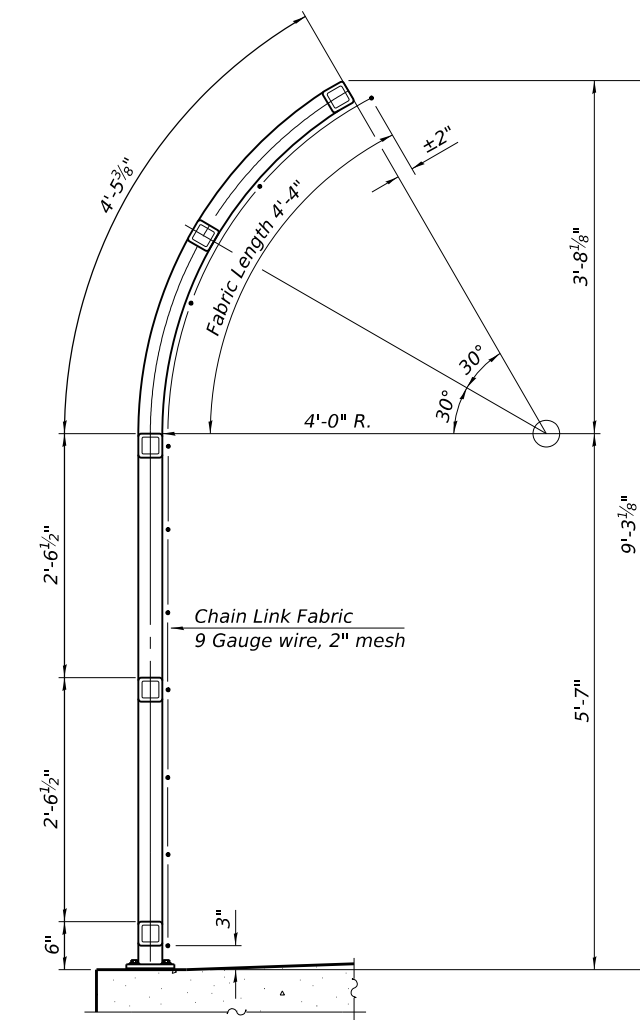
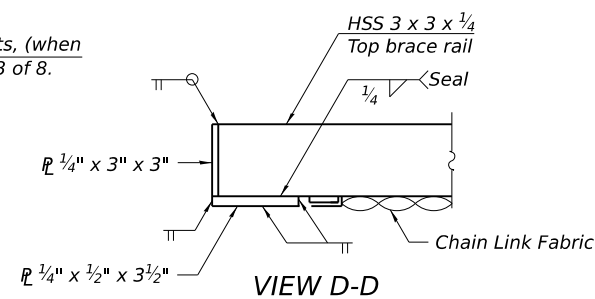
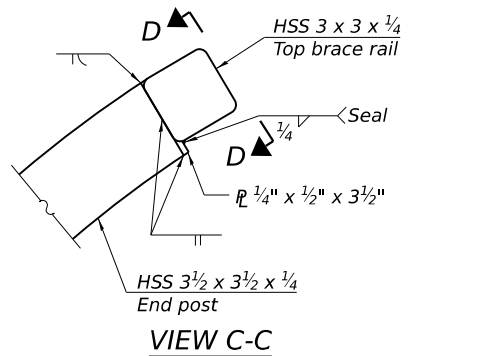
ELEVATION BICYCLE RAILING, CURVED
(Inside face)



RAILING CRITERIA

MASH 2016 Test Level	4
Parapet Railing Weight (plf)	25
Bicycle Railing Weight (plf)	75
Max Post Spacing	10'-0"

ELEVATION PARAPET RAILING
(Inside face)



MODEL: Railing Details Sheet 1 of 2
FILE NAME: R080824Railing_01.dgn

R-33

4-4-2025



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PLOT DATE	= 11/4/2025 - 10:49:33 AM

DESIGNED	- DRC
DRAWN	- DRC
CHECKED	- JTH/DH
DATE	- OCTOBER 2024

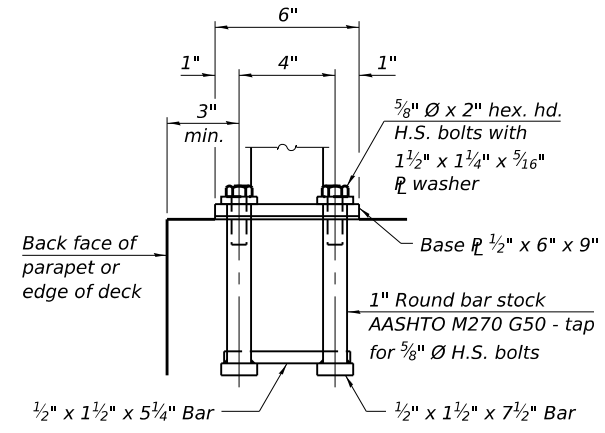
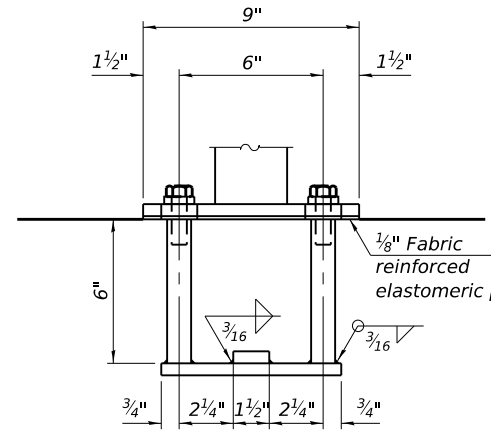
REVISED	- 11/04/2025
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BICYCLE RAILING, CURVED & PARAPET RAILING - 1
STRUCTURE NO. 081-0107

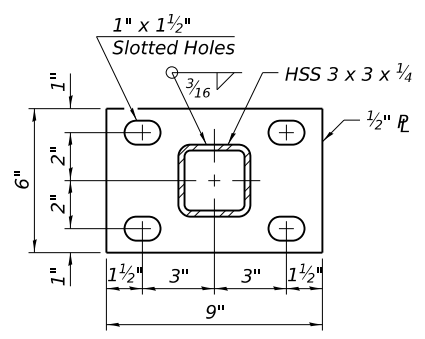
SHEET 5 OF 8 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

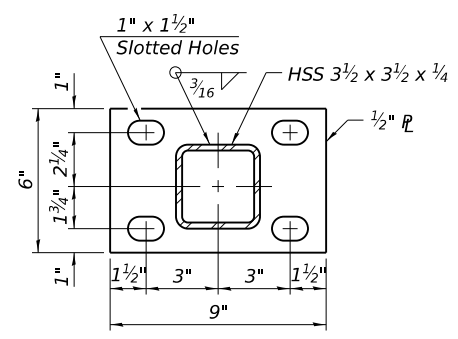


ANCHORAGE ASSEMBLY

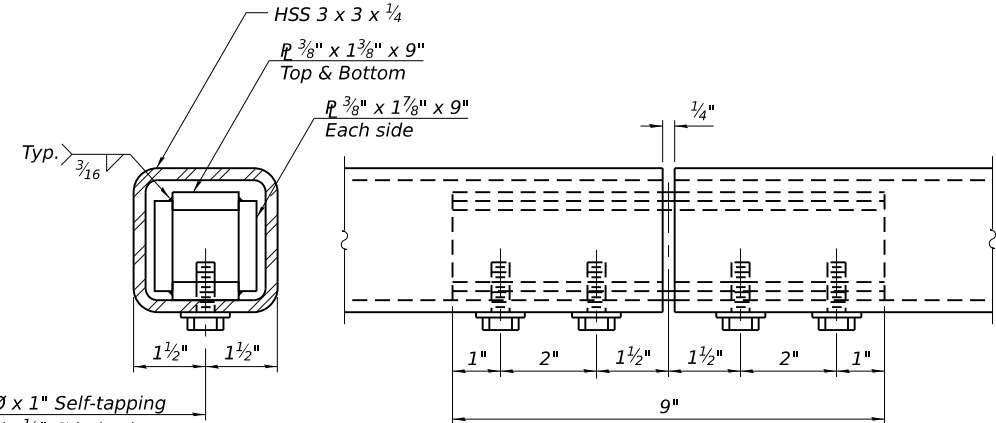
The Bicycle Railing, Curved fasteners for end posts near expansion joints may need to be installed prior to installing the bent plates. In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" Ø fully threaded anchor rods with the same plate washers as specified above and heavy hex lock nuts according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



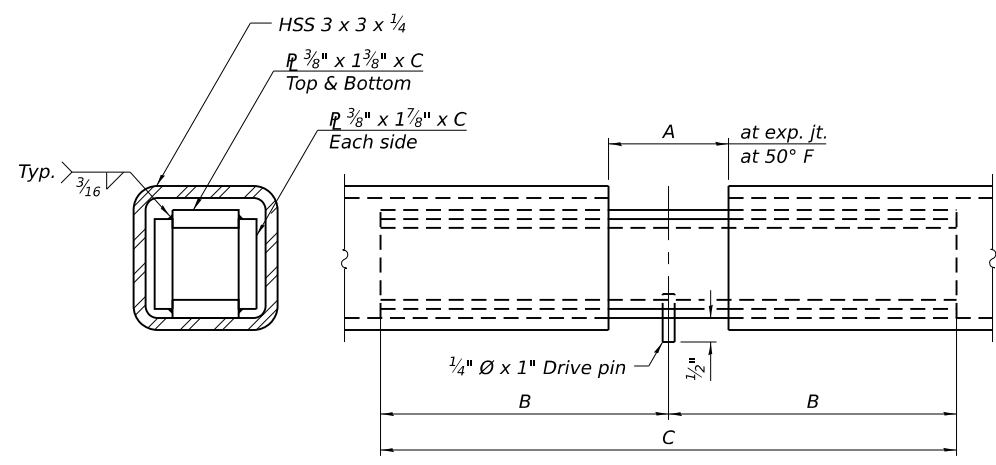
SECTION E-E



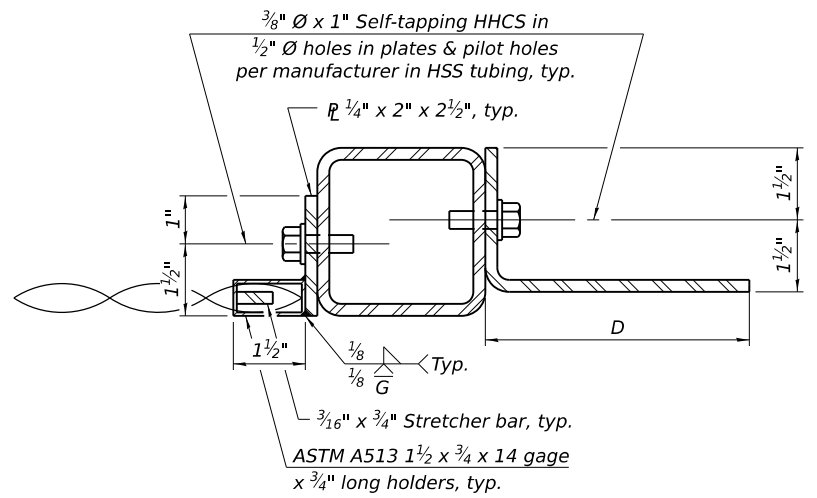
SECTION F-F



MATERIAL SPLICE



EXPANSION SPLICE



SECTION B-B

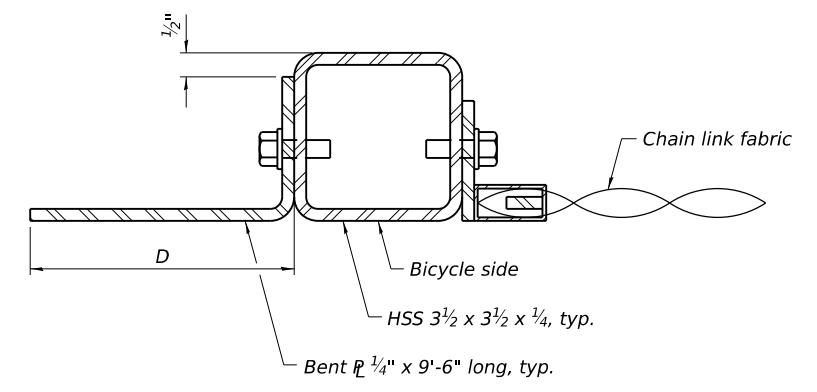


TABLE OF DIMENSIONS

Location	T	A	B	C	D	E
Over Strip Seal Jt.	≤4"	2 1/2"	1'-2"	2'-4"	3 3/4"	6 1/4"
Over Finger or Modular Jt.	≤9 1/2"	5 1/2"	1'-7 3/4"	3'-3 1/2"	6"	9 3/4"
Over Finger or Modular Jt.	≤15"	8 1/4"	2'-1 1/4"	4'-2 1/2"	8 3/4"	13 7/8"
At Light Poles	8"	NA - Coordinate 8" Wide Access With Fabricator				
At Bridge Relief Joints		NA - Material Splices Per Fabricator				

T= ; total movement based on total temperature range from -20°F to 120°F along centerline of roadway at expansion joint.
OR
8" hand-hole access for maintenance of light poles (when present; replace splice with 8" gap).

BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing, Curved	Foot	249
Parapet Railing	Foot	249

R-33

4-4-2025



USER NAME	= Vincent Tan
PLOT SCALE	= N/A
PLOT DATE	= 11/4/2025 • 10:48:55 AM

DESIGNED	- DRC
DRAWN	- DRC
CHECKED	- JTH/DH
DATE	- OCTOBER 2024

REVISED	- 11/04/2025
REVISED	-
REVISED	-
REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BICYCLE RAILING, CURVED & PARAPET RAILING - 2
STRUCTURE NO. 081-0107**

SHEET 6 OF 8 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5836	23-00291-00-BT	ROCK ISLAND	184	95
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

BENCHMARK

Survey marker on top of parapet at NW corner of bridge
S.N. 081-0107. Sta. 49+04+62, Offset 18.57' Rt., Elev 699.62

SEISMIC DATA

Sismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec (SD1) = 0.087g
Design Spectral Acceleration at 0.2 sec (SDS) = 0.127g
Soil Site Class = D

DESIGN STRESSES

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (Structural Steel)

EXISTING STRUCTURE

None

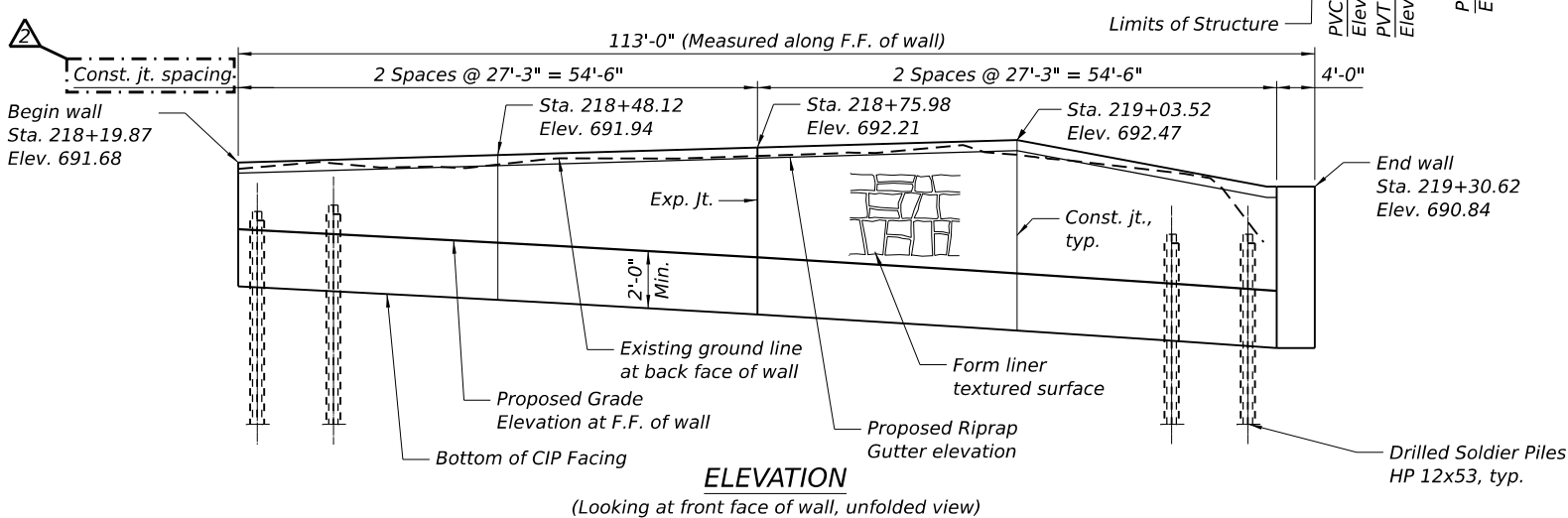
DESIGN SPECIFICATIONS

2024 AASHTO LRFD Bridge Design Specifications, 10th Edition

HORIZONTAL CURVE DATA

EX CURVE 19th Street (SB)
PI STA = 221+30.47
Δ = 60°04'53" (RT)
D = 04°48'53"
R = 1,190.00'
T = 688.17'
L = 1247.86'
E = 184.66'
e =
PC STA = 214+42.30
PT STA = 226+90.15

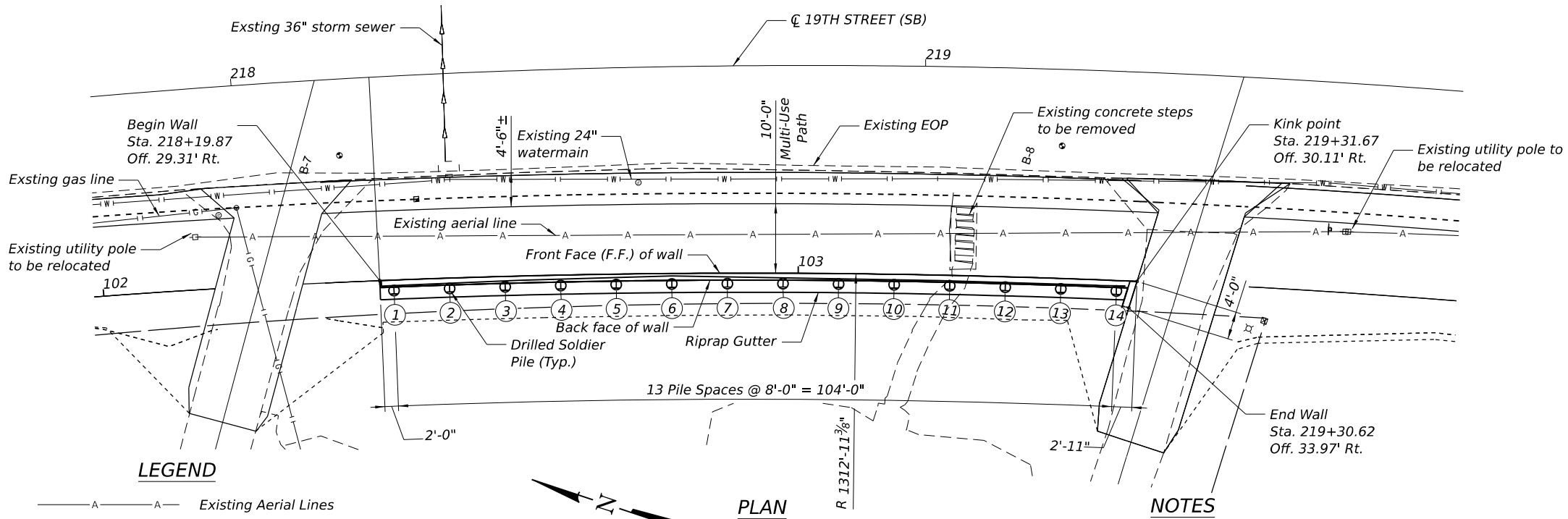
PR CURVE Multi-Use Path
PI STA = 102+91.12
Δ = 06°35'56" (RT)
D = 04°21'50"
R = 1,312.95'
T = 75.69'
L = 151.22
E = 2.18'
e =
PC STA = 102+15.42
PT STA = 103+66.64



PROPOSED PROFILE GRADE
(Along Multi-Use Path)

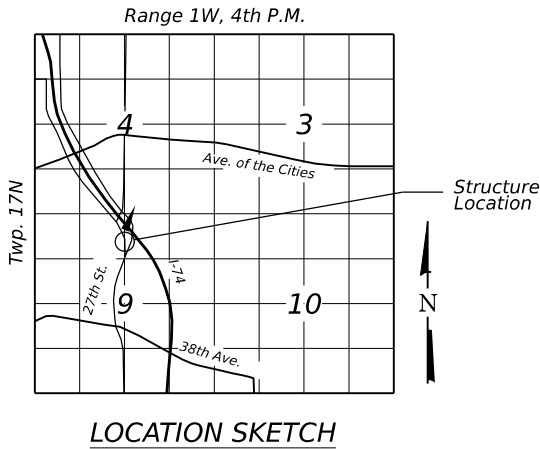
TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structure Excavation	Cu. Yd.	34
Concrete Encasement	Cu. Yd.	19.6
Form Liner Textured Surface	Sq. Ft.	676
Stud Shear Connectors	Each	124
Reinforcement Bars, Epoxy Coated	Pound	1,970
Furnishing Soldier Piles (HP Section)	Foot	223
Drilling and Setting Piles (In Soil)	Cu. Ft.	723
Untreated Timber Lagging	Sq. Ft.	110.438
Concrete Structures (Retaining Wall)	Cu. Yd.	25.8
Geocomposite Wall Drain	Sq. Yd.	1.936
Controlled Low-Strength Material	Cu. Yd.	6.0
Pipe Underdrains For Structures 4"	Foot	109



NOTES

- Wall Stations and Offsets are given to the front face of the wall and are measured from the centerline of 19th Street.
- Horizontal dimensions measured along front face of precast panels.
- The drain shall be placed behind the lagging with the previous side toward the soil according to Section 591 of the Standard Specifications and shall extend to within 6 in. of either side soldier pile flange. The drain shall be installed in stages as the excavation proceeds downward making sure that drain splices as well as the top side edges are covered as required to protect the drain core from soil intrusion.



GENERAL PLAN & ELEVATION

RETAINING WALL 1

ALONG 19TH ST SOUTHBOUND

F.A.U. RTE. 5834 - SECTION 23-00291-00-BT

ROCK ISLAND COUNTY

STATION 218+19.97 TO STATION 219+31.67

STRUCTURE NO. 081-W001



USER NAME = Vincent Tan	DESIGNED - DRC	REVISED - 10/24/2025
PLOT SCALE = 0.16666633' / in.	DRAWN - DRC	REVISED - 11/04/2025
PLOT DATE = 11/4/2025 - 10:00:33 AM	CHECKED - JTH/DH	REVISED -
	DATE - AUGUST 2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 081-W001

SHEET 1 OF 5 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5834	23-00291-00-BT	ROCK ISLAND	184	98
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

BENCHMARK

Survey marker on top of
parapet at NW corner of bridge
S.N. 081-0107. Sta. 49+04.62,
Offset 18.57' Rt., Elev 699.62

SEISMIC DATA

Sesmic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec (SD1) = 0.087g
Design Spectral Acceleration at 0.2 sec (SDS) = 0.127g
Soil Site Class = D

EXISTING STRUCTURE

None

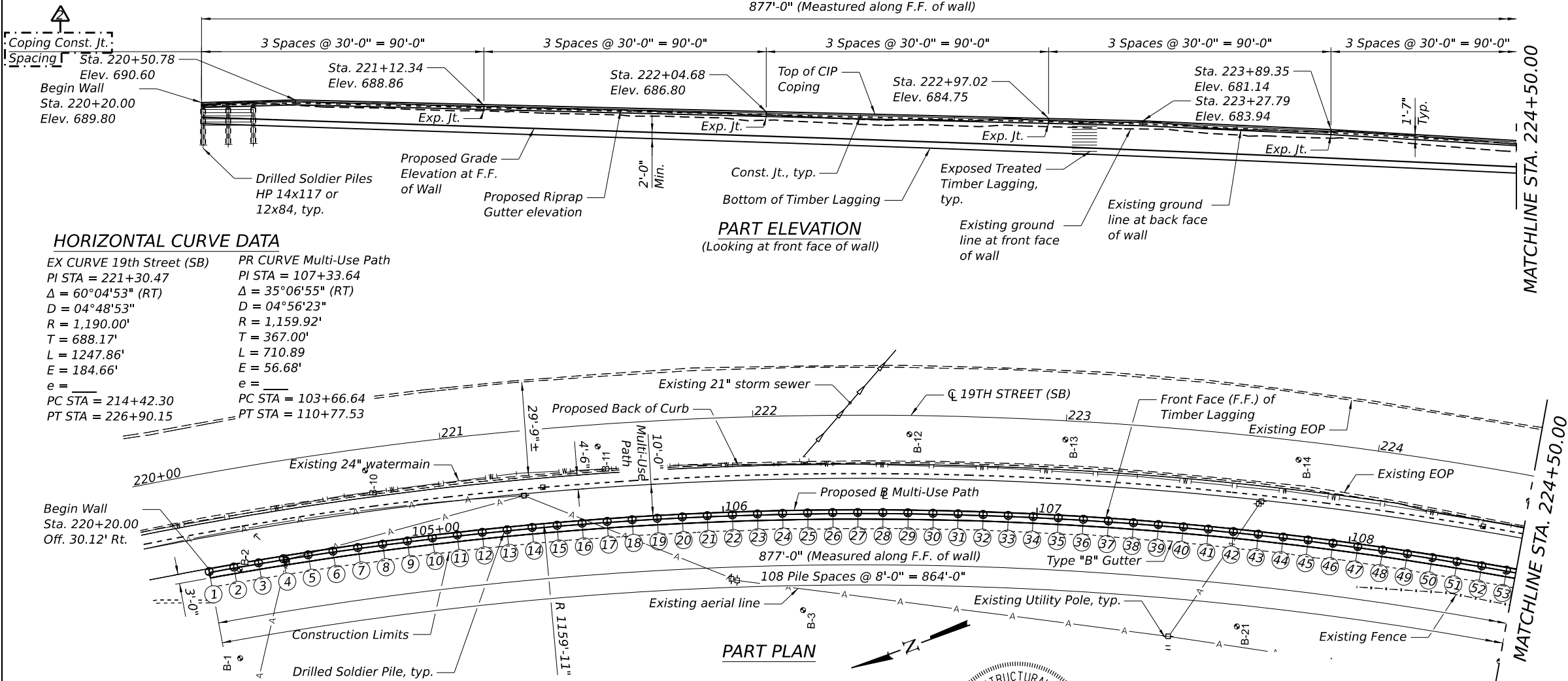
DESIGN SPECIFICATIONS

2024 AASHTO LRFD Bridge Design
Specifications, 10th Edition

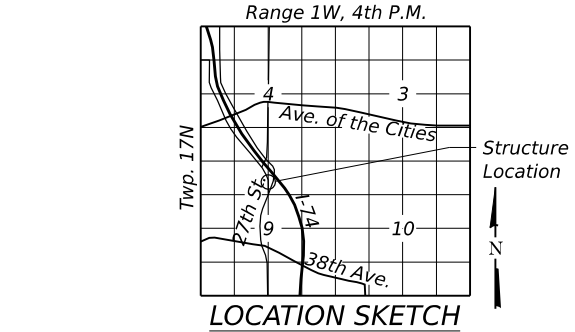
DESIGN STRESSES

FIELD UNITS

$f_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (Structural Steel)



TOTAL BILL OF MATERIAL		
ITEM	UNIT	TOTAL
Structure Excavation	Cu. Yd.	477
Concrete Encasement	Cu. Yd.	335.7
Reinforcement Bars, Epoxy Coated	Pound	6,070
Furnishing Soldier Piles (HP Section)	Foot	3,030
Drilling and Setting Soldier Piles (In Soil)	Cu. Ft.	13,920
Treated Timber Lagging	Sq. Ft.	6,759
Concrete Structures (Retaining Wall)	Cu. Yd.	65.4
Geocomposite Wall Drain	Sq. Yd.	5468.718
Pipe Underdrains For Structures 4"	Foot	877



GENERAL PLAN & ELEVATION
RETAINING WALL 2
ALONG 19TH ST SOUTHBOUND
F.A.U. RTE. 5834 - SECTION 23-00291-00-BT
ROCK ISLAND COUNTY
STATION 220+20.00 TO STATION 229+14.24
STRUCTURE NO. 081-W003

NOTES

- Wall Stations and Offsets are given to the front face of the wall and are measured from the centerline of 19th Street (SB).
- Horizontal dimensions measured along front face of Timber Lagging.
- The treated timber lagging shall be according to Article 507 and 1007 of the Standard Specifications.
- The drain shall be placed behind the lagging with the previous side toward the soil according to Section 591 of the Standard Specifications and shall extend to within 6 in. of either side soldier pile flange. The drain shall be installed in stages as the excavation proceeds downward making sure that drain splices as well as top side edges are covered as required to protect the drain core from soil intrusion.

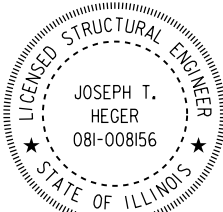
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 081-W003

SHEET 1 OF 23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5834	23-00291-00-BT	ROCK ISLAND	184	103
CONTRACT NO.				

ILLINOIS FED. AID PROJECT



"I certify that to the best of my knowledge, information and belief, this design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of the structure and complies with the requirements of the current Design Specifications listed"

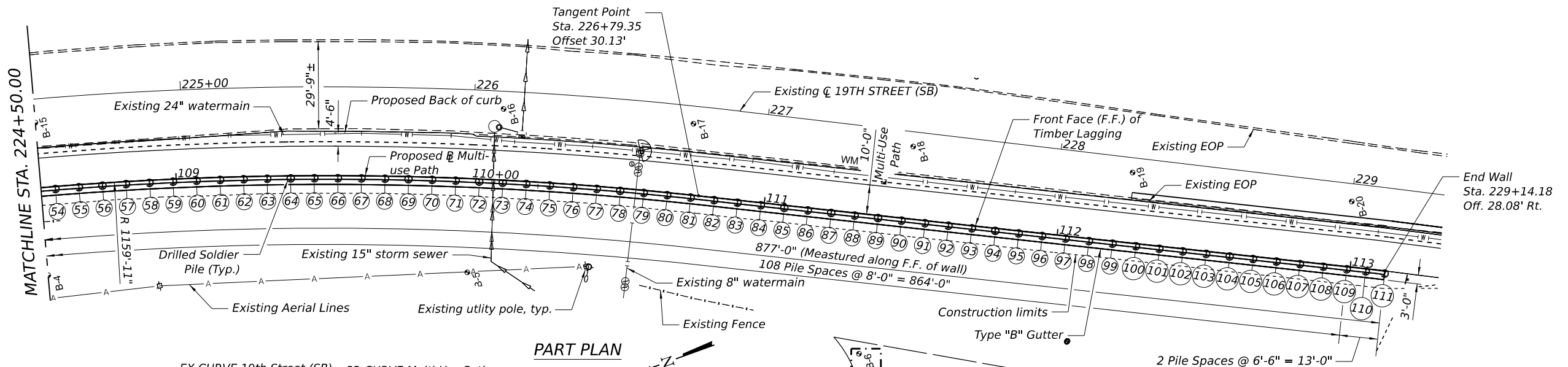
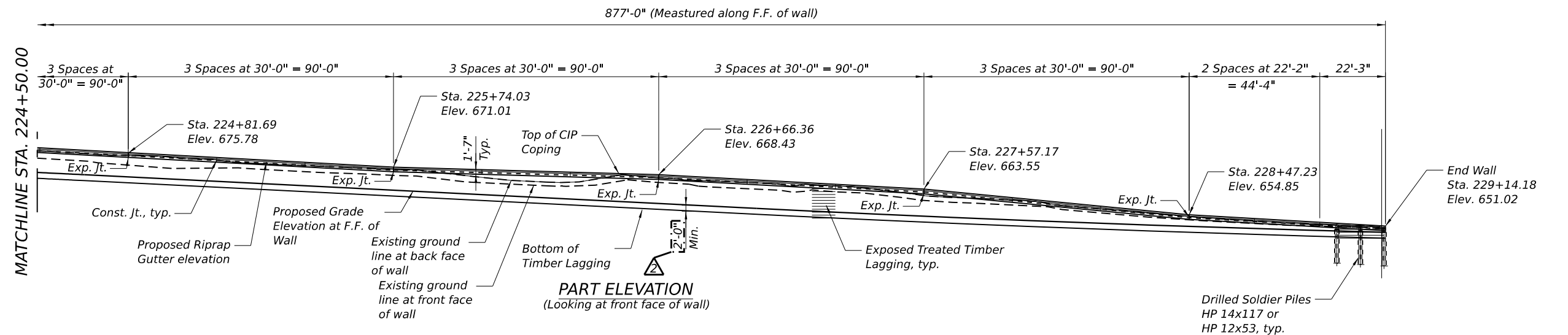
Exp. Date 11/30/2026

LEGEND

- Existing Aerial Lines
- Existing Storm Sewer
- Existing Watermain
- Proposed Curb
- Proposed Construction Limits



USER NAME = Vincent Tan	DESIGNED - DRC	REVISED - 10/24/2025
PLOT SCALE = 0.16666633" / in.	DRAWN - DRC	REVISED - 11/04/2025
PLOT DATE = 11/4/2025 - 10:53:02 AM	CHECKED - JTH/DH	REVISED -
	DATE - AUGUST 2025	REVISED -



LEGEND

- A — A — Existing Aerial Lines
- > > > — Existing Storm Sewer
- W — W — Existing Watermain
- — — Proposed Curb
- - - - - Proposed Construction Limits

EX CURVE 19th Street (SB)
 PI STA = 221+30.47
 $\Delta = 60^\circ 04' 53''$ (RT)
 $D = 04^\circ 48' 53''$
 $R = 1,190.00'$
 $T = 688.17'$
 $L = 1247.86'$
 $E = 184.66'$
 $e =$
 PC STA = 214+42.30
 PT STA = 226+90.15

PR CURVE Multi-Use Path
 PI STA = 107+33.64
 $\Delta = 35^\circ 06' 55''$ (RT)
 $D = 04^\circ 56' 23''$
 $R = 1,159.92'$
 $T = 367.00'$
 $L = 710.89'$
 $E = 56.68'$
 $e =$
 PC STA = 103+66.64
 PT STA = 110+77.53

PART PLAN

NOTES

- Wall Stations and Offsets are given to the front face of the wall and are measured from the centerline of 19th Street.
- Horizontal dimensions measured along front face of Timber Lagging.

GENERAL PLAN & ELEVATION
RETAINING WALL 2
ALONG 19TH ST SOUTHBOUND
F.A.U. RTE. 5834 - SECTION 23-00291-00-BT
ROCK ISLAND COUNTY
STATION 220+20.00 TO STATION 229+14.24
STRUCTURE NO. 081-W003



USER NAME = Vincent Tan	DESIGNED - DRC	REVISED - 11/04/2025
PLOT SCALE = 0.16666633' / in.	DRAWN - DRC	REVISED -
PLOT DATE = 11/4/2025 - 9:54:33 AM	CHECKED - JTH/DH	REVISED -
	DATE - AUGUST 2025	REVISED -

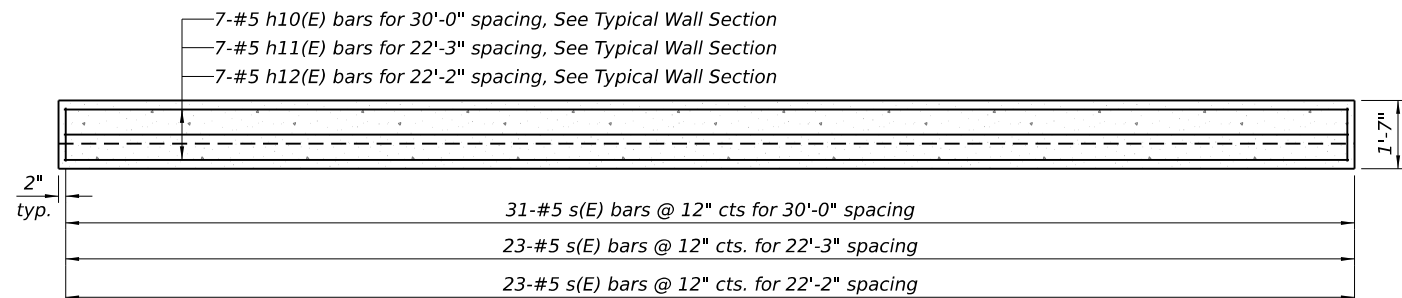
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 081-W003

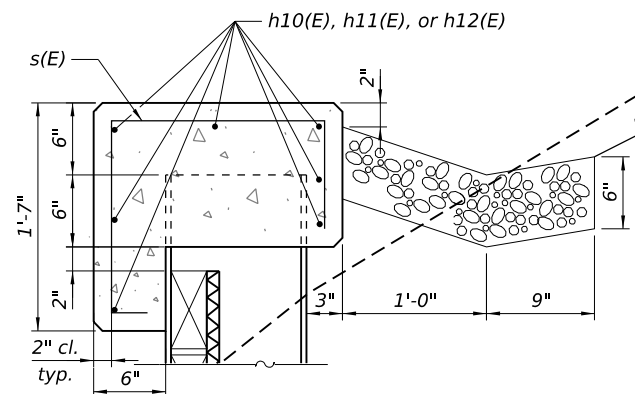
SHEET 2 OF 23 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5834	23-00291-00-BT	ROCK ISLAND	184	104
CONTRACT NO.				

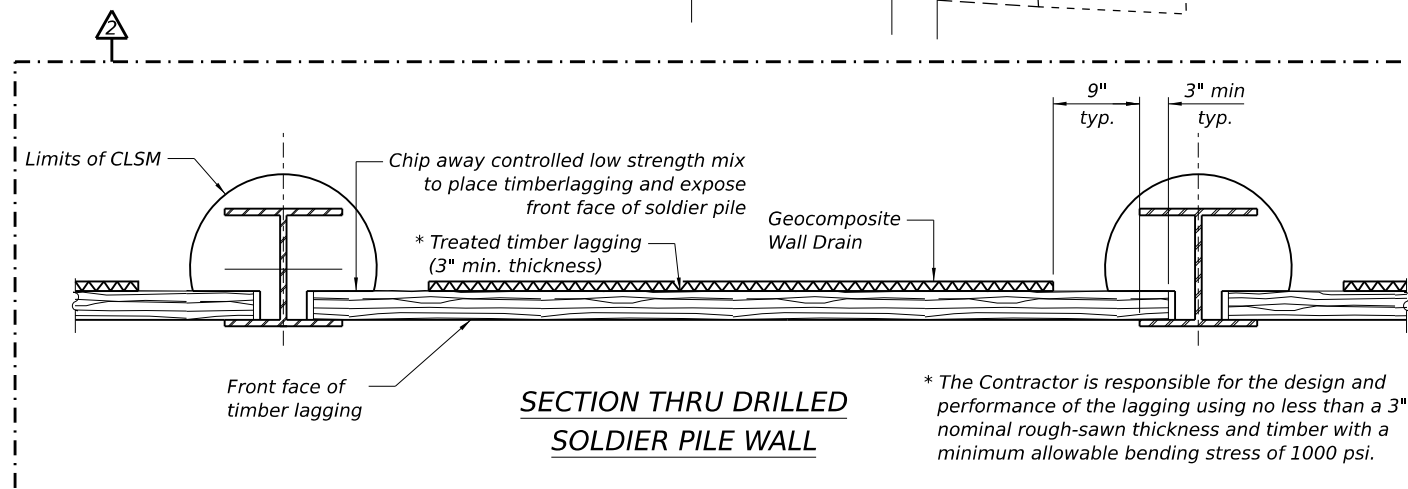
ILLINOIS FED. AID PROJECT



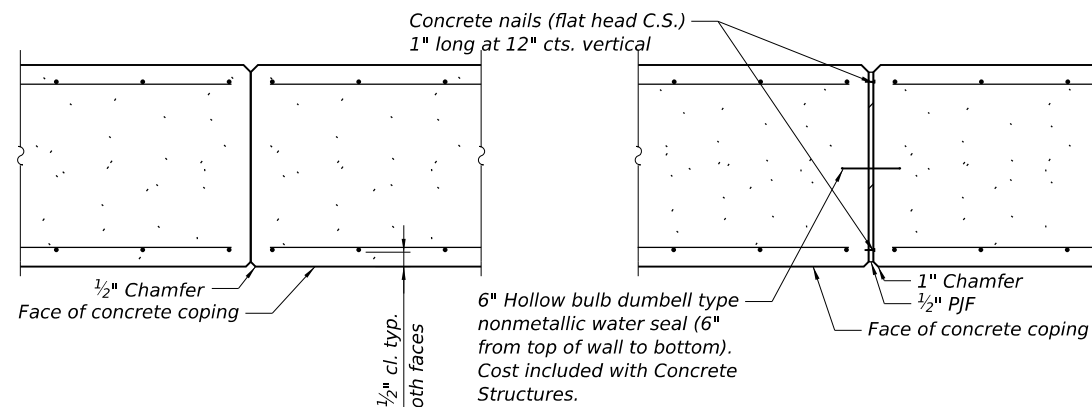
CIP CONCRETE COPING ELEVATION



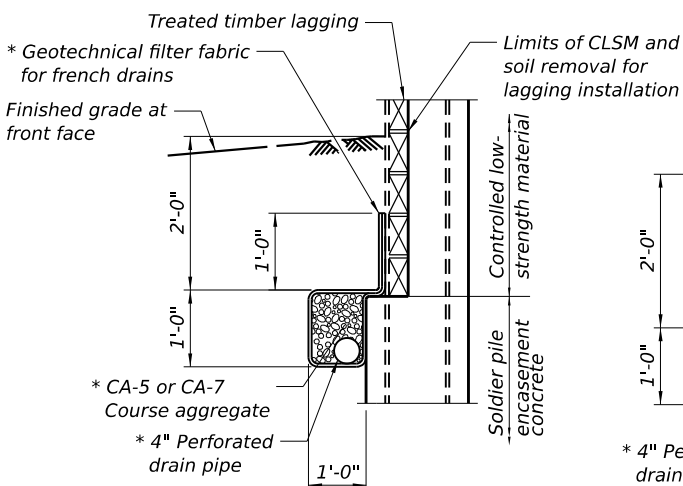
CIP CONCRETE COPING



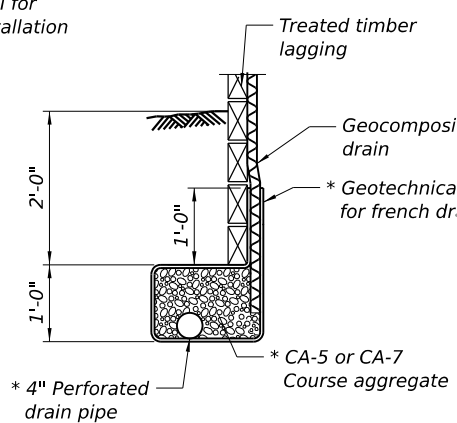
* The Contractor is responsible for the design and performance of the lagging using no less than a 3" nominal rough-sawn thickness and timber with a minimum allowable bending stress of 1000 psi.



TYPICAL WALL SECTION
(Looking South)



* Included in the cost of Pipe Underdrains for Structures



UNDERDRAIN DETAIL

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h10(E)	189	#4	29'-8"	
h11(E)	7	#4	21'-11"	
h12(E)	14	#4	21'-10"	
s(E)	906	#4	3'-4"	
Reinforcement Bars, Epoxy Coated			Pound	6,070
Concrete Structures (Retaining Wall)			Cu. Yd.	65.4



USER NAME = Vincent Tan
 PLOT SCALE = 0.16666633' / in.
 PLOT DATE = 11/4/2025 - 11:34:12 AM

DESIGNED - DRC
 DRAWN - DRC
 CHECKED - DH
 DATE - AUGUST 2025

REVISED - 11/04/2025
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DETAILS
 STRUCTURE NO. 081-W003

SHEET 3 OF 23 SHEETS

F.A.U. RTE. 5834 SECTION 23-00291-00-BT COUNTY ROCK ISLAND TOTAL SHEETS 184 SHEET NO. 105 CONTRACT NO. ILLINOIS FED. AID PROJECT