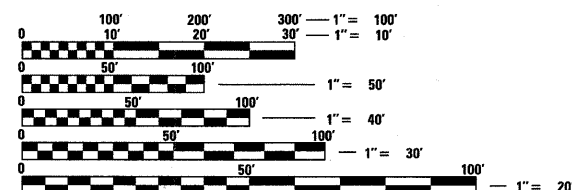


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ARLINGTON HEIGHTS ROAD (NORTH OF DEVON AVENUE) = COOK COUNTY HIGHWAY V69  
ARLINGTON HEIGHTS ROAD (SOUTH OF DEVON AVENUE) = DUPAGE COUNTY HIGHWAY 10  
DEVON AVENUE = COOK COUNTY HIGHWAY B11



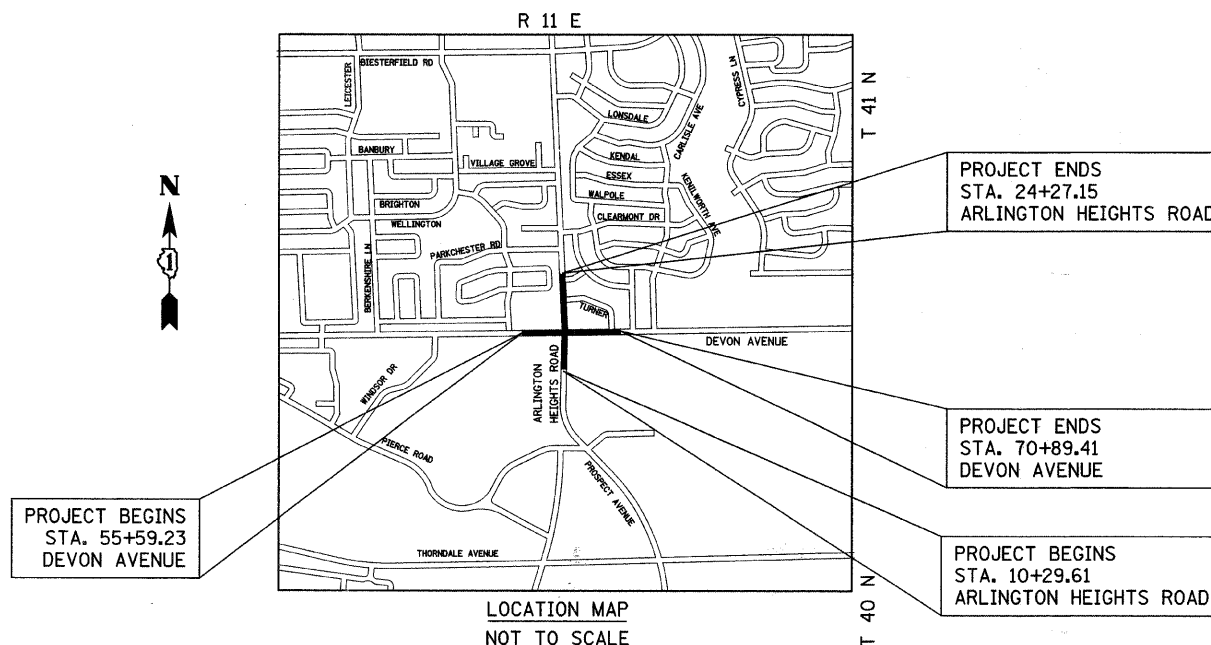
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

**J.U.L.I.E.**  
**JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION**  
**1-800-892-0123 OR 811**

**CONTRACT NO. 63055**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED FEDERAL AID HIGHWAY**  
**F.A.U. ROUTE 2626 (ARLINGTON HEIGHTS ROAD)**  
**F.A.U. ROUTE 1346 (DEVON AVENUE)**  
**SECTION 05-00050-00-CH**  
**PROJECT NO. M-8003(569)**  
**INTERSECTION IMPROVEMENT**  
**AND TRAFFIC SIGNAL MODERNIZATION**  
**VILLAGE OF ELK GROVE VILLAGE**  
**COOK AND DUPAGE COUNTIES**  
**C-91-144-06**



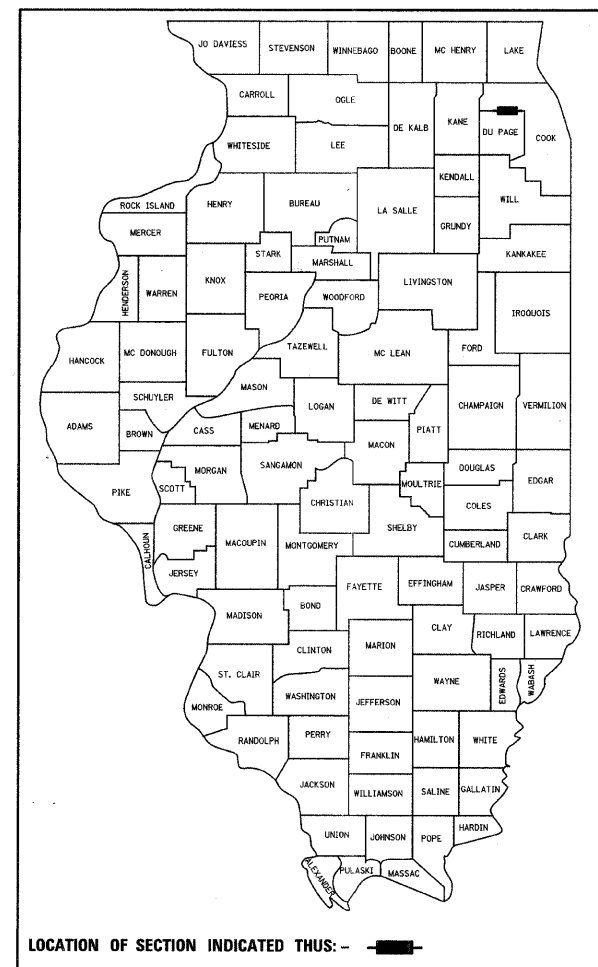
DEVON AVENUE - 1,530.2 FT (0.290 MILE) (NET & GROSS)  
ARLINGTON HEIGHTS ROAD - 1,397.5 FT (0.265 MILE) (NET & GROSS)  
PROJECT TOTAL - 2,927.8 FT (0.555 MILE) (NET & GROSS)

## DESIGN DESIGNATIONS:

DEVON AVENUE - 30,950 (2010) • MAJOR ARTERIAL TWS-2 • 7.36 (HMA OVERLAY-15)  
ARLINGTON HEIGHTS ROAD - 24,350 (2010) • MAJOR ARTERIAL TWS-2 • 1.96 (HMA OVERLAY-15)

POSTED AND DESIGN SPEEDS:

DEVON AVENUE (WEST LEG) - 45 MPH	ARLINGTON HEIGHTS ROAD (NORTH LEG) - 40 MPH
DEVON AVENUE (EAST LEG) - 40 MPH	ARLINGTON HEIGHTS ROAD (SOUTH LEG) - 35 MPH



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED Alan J. Boggie JUNE 11 20 08  
ELK GROVE VILLAGE, DIRECTOR OF ENGINEERING AND COMMUNITY DEVELOPMENT

PASSED SEPTEMBER 2 2008  
CHRISTOPHER HOLT  
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW SEPTEMBER 3, 20 08  
Diana M. O'Keefe  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER



2.172 8-20-08 11-30-08  
REGISTERED P.E., STATE OF ILLINOIS EXPIRES  
PLANS PREPARED BY:



450 E. Devon Ave, Suite 300 - Itasca, Illinois 60143  
Tel: 630.773.3900 - Fax: 630.773.3975  
[www.civiltechinc.com](http://www.civiltechinc.com)

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SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ("STANDARD SPECIFICATIONS"), ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2008; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED FROM THE VILLAGE OF ELK GROVE VILLAGE, THE COOK COUNTY HIGHWAY DEPARTMENT, AND THE DUPAGE COUNTY DIVISION OF TRANSPORTATION.
- ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

STAKING

- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.
- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
- ESTIMATED LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
- ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON U.S.G.S. DATUM.
- THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE OF RIGHT-OF-WAY.

PAVING AND CURB & GUTTER

- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, MEDIAN AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE ITEM BEING REMOVED.

THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.

- IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.
- BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE BINDER OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
- THE ENGINEER SHALL APPROVE ALL DRIVEWAYS TO BE REMOVED AND THE CONFIGURATION OF ANY DRIVEWAY TO BE RECONSTRUCTED.
- ALL PROPOSED DRIVEWAYS SHALL BE PAID FOR AS "PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL" UNLESS OTHERWISE SPECIFIED.

TREE REMOVAL, CLEARING AND HEDGE REMOVAL

- THE CONTRACTOR SHALL NOTE THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF ELK GROVE VILLAGE. THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. TEMPORARY FENCE SHALL BE INSTALLED AROUND TREES AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER.
- TEMPORARY FENCE SHALL BE ERECTED ALONG THE DRIP LINE OF EXISTING TREES TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION OR AS DIRECTED BY THE ENGINEER. AFTER TREES ARE SAFELY FENCED NOTHING IS TO BE STORED, DRIVEN, OR DISTURBED INSIDE THE FENCE. REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
- ALL CLEARING AND REMOVAL OF TREES UNDER 6" IN DIAMETER SHALL BE INCIDENTAL TO THE COST OF EARTH EXCAVATION.
- ALL CLEARING AND THE REMOVAL AND/OR RELOCATION OF BUSHES, AS DIRECTED BY THE ENGINEER, SHALL BE INCIDENTAL TO THE COST OF "EARTH EXCAVATION".
- ALL LIMBS, BRANCHES AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.

UTILITIES

- THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE "STANDARD SPECIFICATIONS." THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF HIS CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.
- COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
- WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
- ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE VILLAGE.
- ALL AUXILIARY VALVES, FRAMES, GRATES, LIDS AND BOXES REMOVED FROM EXISTING WATER SERVICE OR SEWER STRUCTURES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ALL HYDRANTS TO BE REMOVED SHALL BE REMOVED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND THE CONTRACTOR SHALL CONTACT THE VILLAGE TO DETERMINE IF THE VILLAGE WANTS THEM RETURNED TO PUBLIC WORKS OR DISPOSED OF BY THE CONTRACTOR.
- THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. COMPLIANCE WITH THE ABOVE WILL BE INCIDENTAL TO THE UTILITY INSTALLATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS AND WATER MAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT OR RELOCATION OF THEIR FACILITIES, IF NECESSARY.
- PROPOSED FIRE HYDRANTS SHALL BE LOCATED A MINIMUM OF THREE FEET BEHIND THE PROPOSED BACK OF CURB OR TWO FEET BEHIND THE BACK OF PROPOSED SIDEWALK (MEASURED TO THE CENTER OF THE HYDRANT).
- WATER MAIN SHUT-OFFS SHALL ONLY BE PERFORMED ON WEEKEND DAYS, AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL NOTIFY THE VILLAGE 48 HOURS PRIOR TO COMMENCING THE WORK.

ROADWAY EXCAVATION

- ALL EXCESS MATERIAL (BROKEN CONCRETE, SEWER PIPE, WASTE ROADWAY EXCAVATION AND SURPLUS MATERIAL FROM SEWER TRENCHES) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEMS IN THE CONTRACT.
- POROUS GRANULAR EMBANKMENT, SUBGRADE HAS BEEN PROVIDED TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE CURB AND GUTTER AND COME UP AT A 1:1 SLOPE TO EXISTING GROUND SURFACE. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED BETWEEN THE EXISTING SUBGRADE AND THE PROPOSED PGES. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."

STORM & SANITARY SEWER

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCIDENTAL TO THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.  
  
DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.
- DRAINAGE STRUCTURE FLAT-TOPS AND CONES SHALL BE TURNED SO THAT THE FRAMES ARE CLOSEST TO THE CENTERLINE OF THE ROAD. ALL FLAT-TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
- ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.

CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	2
GENERAL NOTES AND STATE STANDARDS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	M-8003(669)	

ILLINOIS DEPARTMENT OF TRANSPORTATION  
GENERAL NOTES  
AND STATE STANDARDS

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	3
GENERAL NOTES AND STATE STANDARDS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT W-8003(569)				

SIGNING, STRIPING & LANDSCAPING

- WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS.
- THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK SHALL BE REMOVED, A RECORD MADE OF THEIR CONDITION, AND SAFELY STORED AND SAFEGUARDED BY THE CONTRACTOR UNTIL THE ENGINEER DETERMINES THAT THEY BE REINSTALLED IN THE PERMANENT LOCATIONS.
- IMMEDIATELY AFTER EACH SIGN IS REMOVED, A TEMPORARY SIGN OF THE SAME TYPE SHALL BE INSTALLED ON A SIGN SUPPORT APPROVED BY, AND AT A LOCATION DETERMINED BY, THE ENGINEER. THESE SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN UNTIL THE PERMANENT SIGNS ARE REINSTALLED.
- ANY SIGN WHICH IS DAMAGED DURING THE TIME IT IS STORED SHALL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT HIS OWN EXPENSE PRIOR TO PERMANENT REINSTALLATION.
- ALL UNUSED SIGNS AND POSTS SHALL BE RETURNED TO THE VILLAGE OF ELK GROVE'S PUBLIC WORKS FACILITY.
- THE COST OF STORING AND SAFEGUARDING THE PERMANENT SIGNS AND POSTS, AND REINSTALLING THE PERMANENT SIGNS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "RELOCATE SIGN PANEL ASSEMBLY" OF THE TYPE SPECIFIED. NEW SIGN SUPPORTS SHALL BE USED FOR REINSTALLED SIGNS. FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE TEMPORARY SIGNS SHALL BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION." THE NEW SUPPORTS SHALL BE PAID FOR AS "TELESCOPING STEEL SIGN SUPPORT."
- PAVEMENT MARKING TAPE, TYPE III, IF REQUIRED AND AT THE DIRECTION OF THE ENGINEER, SHALL BE APPLIED TO THE FINAL PAVEMENT SURFACE PRIOR TO APPLICATION OF THE PERMANENT PAVEMENT MARKING, IN ACCORDANCE WITH SECTIONS 703 AND 1095 OF THE STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL NOTIFY THE COOK COUNTY TRAFFIC OPERATIONS DIVISION AT (847)827-7824 OR (312)603-1660, TEN (10) DAYS PRIOR TO THE ESTIMATED DATE THAT THE ROADWAY WILL BE READY FOR THE APPLICATION OF PERMANENT PAVEMENT MARKINGS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE ROADWAY CLEANED OF ANY DIRT, GRAVEL, OIL, ETC. ON THE DAY THE PAVEMENT MARKINGS ARE APPLIED.

EROSION CONTROL

- ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- ALL EROSION CONTROL MEASURES SHALL BE IN PLACE BEFORE ANY WORK BEGINS.
- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
- ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
- ALL SLOPES SHALL BE COVERED WITH SOD AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SODDING SHALL BE THE LIMITS OF GRADING.
- INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES.
- THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
- SEE STANDARD 280001 FOR ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL DETAILS AND REQUIREMENTS.
- FOR EACH TREE IN THE PROJECT LIMITS WITHIN THE RIGHT-OF-WAY, QUANTITIY HAS BEEN INCLUDED FOR : 30 FT TEMPORARY FENCE, 1 EACH TREE TRUNK PROTECTION, AND 1 EACH TREE ROOT PRUNING. USE OF THESE ITEMS WILL BE AS DETERMINED BY THE ENGINEER.
- WHEN A TOPSOIL STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES MEETING THE APPROVAL OF THE ENGINEER SHALL BE PROVIDED. THIS WORK SHALL BE PAID FOR AT THE UNIT PRICE FOR THE INDIVIDUAL ITEMS USED.
- THE SURFACE OF ALL STRIPPED AREAS SHALL BE PERMANENTLY OR TEMPORARILY PROTECTED FROM SOIL EROSION WITHIN 14 DAYS AFTER FINAL GRADE IS REACHED. STRIPPED AREAS THAT WILL REMAIN UNDISTURBED FOR MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE SHALL BE PROTECTED FROM EROSION WITH THE USE OF TEMPORARY EROSION CONTROL SEEDING. TEMPORARY SEDIMENT AND EROSION CONTROL MEASUREAS SHALL BE MAINTAINED CONTINUOUSLY UNTIL PERMANENT COVER IS ESTABLISHED.
- SOIL STOCK PILES SHALL NOT BE ALLOWED WITHIN SPECIAL MANAGEMENT AREAS.

MATERIALS QC/QA POLICY

- ALL HOT-MIX ASPHALT AND P.C. CONCRETE MATERIALS USED ON THIS PROJECT SHALL BE TESTED AND INSPECTED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S QC/QA REQUIREMENTS.
- THE CONTRACTOR SHALL PROVIDE QC TESTING TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION BUREAU OF MATERIALS ORDER BOARD (PHONE: 847-705-4337 OR FAX: 847-705-4529) BY 4:00 P.M., 24-HOURS IN ADVANCE OF CONSTRUCTION FOR INSPECTION OF ALL HOT-MIX ASPHALT AND CONCRETE MATERIALS USED ON THIS PROJECT.
- THE CONTRACTOR IS TO SUBMIT A QC PLAN FOR HMA AND CONCRETE MATERIALS TO THE QA MANAGER FOR APPROVAL PRIOR TO CONSTRUCTION OPERATIONS COMMENCING. THE QA MANAGER WILL APPROVE THIS PLAN AND COPY THE DISTRICT MATERIALS OFFICE ON THE APPROVAL LETTER.
- QC AND QA REPORTS FOR CONCRETE WILL BE SENT TO THE DISTRICT BUREAU OF MATERIALS OFFICE AFTER REVIEW AND APPROVAL BY THE QA MANAGER.
- QC REPORT FOR HOT-MIX ASPHALT MIXTURES WILL BE TRANSMITTED DIRECTLY BY THE CONTRACTOR DAILY DURING PRODUCTION. THE DISTRICT WILL REVIEW AND RETAIN THE QA PLANT REPORTS. THE QA FIELD REPORTS CAN BE SUBMITTED BY THE QA MANAGER TO THE DISTRICT VIA THE DISTRICT LOCAL ROADS OFFICE.
- THE COSTS TO COMPLY WITH THESE REQUIREMENTS SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT AND P.C. CONCRETE ITEMS.

MISCELLANEOUS

- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING NO. 701801-03 WHEN CLOSING ANY SIDEWALK TO PERMIT CONSTRUCTION OF THE IMPROVEMENTS.
- UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-04	TEMPORARY EROSION CONTROL SYSTEMS
424001-05	SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED
442101-07	CLASS B PATCHES
542606	REINFORCED CONCRETE PIPE TEE
602001	CATCH BASIN TYPE A
602011	CATCH BASIN TYPE C
602301-01	INLET TYPE A
602306-01	INLET TYPE B
602401-01	MANHOLE TYPE A
602601-01	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-01	CAST IRON STEPS
604001-02	FRAME AND LIDS TYPE 1
604036-01	GRATE TYPE 8
604086-01	FRAME AND GRATE TYPE 23
604091-01	FRAME AND GRATE TYPE 24
606001-03	PC CONCRETE ISLANDS AND MEDIANS
606306-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701421-01	LANE CLOSURE, MULTI LANE, DAY OPERATIONS ONLY, FOR SPEEDS >= 45-55 MPH
701422-01	LANE CLOSURE, MULTI LANE, FOR SPEEDS >= 45-55 MPH
701501-04	LANE CLOSURE 2L, 2W, DAY OR NIGHT OPERATIONS, FOR SPEEDS < 45 MPH
701606-05	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-05	URBAN LANE CLOSURE, MULTILANE, INTERSECTION, FOR SPEEDS < 45 MPH
701801-03	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE, FOR SPEEDS < 45 MPH
701901	TRAFFIC CONTROL DEVICES
720006-01	SIGN PANEL ERECTION DETAILS
720016-01	MAST ARM MOUNTED STREET NAME SIGNS
728001	TELESCOPING STEEL SIGN SUPPORT
780001-01	TYPICAL PAVEMENT MARKINGS
805001	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-01	CONCRETE HANDHOLES
814006-01	DOUBLE HANDHOLES
857001	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-01	TRAFFIC SIGNAL GROUNDING & BONDING
877001-03	STEEL MAST ARM ASSEMBLY AND POLE
878001-06	CONCRETE FOUNDATION DETAILS
880001	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006	TRAFFIC SIGNAL MOUNTING DETAILS
886001	DETECTOR LOOP INSTALLATIONS
886006	TYPICAL LAYOUT FOR DETECTION LOOPS

ILLINOIS DEPARTMENT OF TRANSPORTATION  
GENERAL NOTES  
AND STATE STANDARDS

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

CONTRACT NO. 63055				
F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	4
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	I000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	84	63	21			
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	43	43				
20101000	TEMPORARY FENCE	FOOT	120	120				
● 20101100	TREE TRUNK PROTECTION	EACH	4	4				
X ● 20101200	TREE ROOT PRUNING	EACH	4	4				
● 20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	4	4				
X 20200100	EARTH EXCAVATION	CU YD	3993	2875	1118			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	3220	2477	743			
X 20400800	FURNISHED EXCAVATION	CU YD	531	452	79			
X 20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	3220	2477	743			
X 20800150	TRENCH BACKFILL	CU YD	261	233	28			
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	9660	7431	2229			
● 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3615	3615				
● 21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	730		730			
X 21300010	EXPLORATION TRENCH, SPECIAL	FOOT	1350	1000	250			100
● 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	54	45	9			
● 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	54	45	9			
● 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	54	45	9			
● 25200100	SODDING	SQ YD	3851	3615	236			
● 25200110	SODDING, SALT TOLERANT	SQ YD	494		494			
● 25200200	SUPPLEMENTAL WATERING	UNIT	100	75	25			
● 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	90	75	15			
28000510	INLET FILTERS	EACH	95	74	21			
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	9581	7352	2229			
35300500	PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	3853	3165	688			
X 35301300	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE 10"	SQ YD	794	544	250			
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	328		328			
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4220	3502	718			
40600300	AGGREGATE (PRIME COAT)	TON	43	35	8			
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	28	28				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2				

- X INDICATES SPECIAL PROVISION
- INDICATES SPECIALTY ITEM

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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CONTRACT NO. 63055				
F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	5
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
40600982	HOT-MIX ASPHALT SURFACE REMOVAL -- BUTT JOINT	SQ YD	276	115	161			
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL -- BUTT JOINT	SQ YD	742	742				
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	771		771			
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	4046	4046				
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	37		37			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	352		352			
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1717	1717				
42000500	PORTLAND CEMENT CONCRETE PAVEMENT 10"	SQ YD	11	11				
42001200	PAVEMENT FABRIC	SQ YD	1520	1520				
42001300	PROTECTIVE COAT	SQ YD	12178	10049	2129			
X 42300800	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH, SPECIAL	SQ YD	1108	838	270			
X 42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	18161	15508	2653			
42400800	DETECTABLE WARNINGS	SQ FT	177	50	127			
44000100	PAVEMENT REMOVAL	SQ YD	1350	1160	190			
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	805	136	669			
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1795	1231	552			
44000300	CURB REMOVAL	FOOT	303	215	88			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	6427	4720	1707			
44000600	SIDEWALK REMOVAL	SQ FT	15119	13727	1392			
X 44003100	MEDIAN REMOVAL	SQ FT	22139	17886	4253			
44200966	CLASS B PATCHES, TYPE I, 10 INCH	SQ YD	170	145	25			
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	975	825	150			
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	155	105	50			
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	170	145	25			
44213200	SAW CUTS	FOOT	2300	1900	400			
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	4300		4300			
45200100	JOINT OR CRACK ROUTING (PC CONCRETE PAVEMENT AND SHOULDER)	FOOT	16860	12560	4300			
45200300	JOINT OR CRACK FILLING	POUND	3933	2930	1003			
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	72	72				
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	231	182	49			
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	549	440	109			

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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	6
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

	CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
					DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
					70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
	550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	64	32	32			
	550B0320	STORM SEWERS, CLASS B, TYPE 2 8"	FOOT	8	8				
X	55100300	STORM SEWER REMOVAL 8"	FOOT	8	8				
X	55100500	STORM SEWER REMOVAL 12"	FOOT	227	167	60			
X	55101200	STORM SEWER REMOVAL 24"	FOOT	79	47	32			
X	55102300	STORM SEWER REMOVAL 72"	FOOT	10	10				
X ●	56106400	ADJUSTING WATER MAIN 8"	FOOT	20	20				
X ●	56106600	ADJUSTING WATER MAIN 12"	FOOT	20	20				
X ●	56400100	FIRE HYDRANTS TO BE MOVED	EACH	2	2				
	60107700	PIPE UNDERDRAINS, 6"	FOOT	133	133				
X	60109510	PIPE UNDERDRAINS, FABRIC LINED TRENCH 4"	FOOT	500	350	150			
	60200205	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2				
	60201330	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 23 FRAME AND GRATE	EACH	3	3				
	60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	26	23	3			
	60205040	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	1	1				
	60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1	1				
	60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1	1				
	60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	3	1	2			
	60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	3	2	1			
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1		1			
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1				
	60222240	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	3		3			
	60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	2	1	1			
	60237470	INLETS, TYPE A, TYPE 24 FRAME AND GRATE	EACH	2	2				
	60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	1		1			
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1				
	60253100	CATCH BASINS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1				
	60255500	MANHOLES TO BE ADJUSTED	EACH	4	1	3			
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	3	1	2			
	60260100	INLETS TO BE ADJUSTED	EACH	2	1	1			
	60260400	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1		1			

X INDICATES SPECIAL PROVISION  
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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	7
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	6	6				
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3				
60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	2	2				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2				
60500050	REMOVING CATCH BASINS	EACH	6	4	2			
60500060	REMOVING INLETS	EACH	19	15	4			
X 60500080	REMOVING CATCH BASINS TO MAINTAIN FLOW	EACH	2	1	1			
X 60500090	REMOVING INLETS TO MAINTAIN FLOW	EACH	6	5	1			
X 60500110	FILLING MANHOLES, SPECIAL	EACH	1	1				
X 60601005	CONCRETE CURB, TYPE B (SPECIAL)	FOOT	95	80	15			
X 60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	316	179	137			
X 60604200	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (SPECIAL)	FOOT	20	20				
X 60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	56	56				
X 60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	4469	3773	696			
X 60605400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (SPECIAL)	FOOT	901	663	238			
X 60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	595		595			
	60618300 CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	45		45			
X 60618400	CONCRETE MEDIAN, TYPE C-4	SQ FT	2973	2973				
	60619600 CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	2537	1527	1010			
	60624600 CORRUGATED MEDIAN	SQ FT	10809	10702	107			
● 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	225	225				
● 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1				
X ● 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)	EACH	1	1				
	63200310 GUARDRAIL REMOVAL	FOOT	188	188				
● 66900200	NON SPECIAL WASTE DISPOSAL	CU YD	250	250				
● 66900450	SPECIAL WASTE PLANS AND REPORT	L. SUM	1	1				
● 66900510	BETX-PNAS SOIL ANALYSIS	EACH	3	3				
X ● 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
	67000400 ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	9	3			
● 67100100	MOBILIZATION	L. SUM	1	0.75	0.25			

X INDICATES SPECIAL PROVISION  
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ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

DATE: 5/27/08  
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CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
X 70101700	TRAFFIC CONTROL AND PROTECTION	L. SUM	1	0.75	0.25			
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	9	3			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1477	1215	262			
70300510	PAVEMENT MARKING TAPE, TYPE III LETTER AND SYMBOLS	SQ FT	724	557	167			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	13428	11334	2094			
70300540	PAVEMENT MARKING TAPE, TYPE III 6"	FOOT	3142	2372	770			
70300560	PAVEMENT MARKING TAPE, TYPE III 12"	FOOT	702	702				
70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	248	248				
70300610	TEMPORARY PAINT PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	546	400	146			
70300625	TEMPORARY PAINT PAVEMENT MARKING LINE 4"	FOOT	12887	10786	2101			
70300635	TEMPORARY PAINT PAVEMENT MARKING LINE 6"	FOOT	2236	1657	579			
70300645	TEMPORARY PAINT PAVEMENT MARKING LINE 12"	FOOT	354	354				
70300660	TEMPORARY PAINT PAVEMENT MARKING LINE 24"	FOOT	293	293				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	15009	12535	2474			
● 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5				
● 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	2				
● 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	19	12	7			
● 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	2	1	1			
● 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	573	394	179			
● 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	724	557	167			
● 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10948	9304	1644			
● 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3142	2372	770			
● 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	702	702				
● 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	248	248				
● 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	276	276				
● 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	98	98				
● 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	360	360				
● 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	18	18				
● 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	123	123				
X ● 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1				

X INDICATES SPECIAL PROVISION  
● INDICATES SPECIALTY ITEM

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

DATE: 5/27/08  
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	9
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
78300100	PAVEMENT MARKING REMOVAL	SQ FT	447	447				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	24	10	14			
X ● 80400100	ELECTRIC SERVICE INSTALLATION	EACH	1					1
X ● 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L. SUM	1					1
X ● 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	2100				1357	
X ● 81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	241				141	100
X ● 81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	43				43	
X ● 81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	84				46	38
X ● 81001100	CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL	FOOT	55				55	
X ● 81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	887				517	370
X ● 81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	93					93
X ● 81019000	CONDUIT PUSHED, 5" DIA., GALVANIZED STEEL	FOOT	362				362	
X ● 81400100	HANDHOLE	EACH	6				6	
X ● 81400200	HEAVY-DUTY HANDHOLE	EACH	4				4	
X ● 81400300	DOUBLE HANDHOLE	EACH	3				3	
● 81603085	UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	5506					5506
● 81702460	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 3/0	FOOT	115					115
X ● 81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	6096				1637	4459
X ● 82500505	LIGHTING CONTROLLER, SPECIAL	EACH	1					1
● 83017200	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 45 FT. M.H., 6 FT. DAVIT ARM	EACH	2					2
● 83017400	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 45 FT. M.H., 10 FT. DAVIT ARM	EACH	22					22
● 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	230					230
X ● 83600215	LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	15					15
● 84200500	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	2					2
● 84200700	LIGHTING FOUNDATION REMOVAL	EACH	2					2
X ● 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1				1	
X ● 85700305	FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL	EACH	1				1	
● 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1				1	
X ● 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1676				1676	
X ● 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2353				2353	
X ● 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1452				1452	

X INDICATES SPECIAL PROVISION  
● INDICATES SPECIALTY ITEM

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
X ● 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2925				2925	
X ● 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN NO. 14 1 PAIR	FOOT	6333				6333	
X ● 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	111				111	
X ● 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	EACH	4				4	
X ● 87700200	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1				1	
X ● 87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1				1	
X ● 87700240	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2				2	
X ● 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16				16	
X ● 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4				4	
X ● 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30				30	
X ● 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	30				30	
● 87900200	DRILL EXISTING HANDHOLE	EACH	1				1	
X ● 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4				4	
X ● 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	6				6	
X ● 88030220	SIGNAL HEAD, LED, 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2				2	
X ● 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2				2	
X ● 88102710	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	8				8	
X ● 88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10				10	
X ● 88500100	INDUCTIVE LOOP DETECTOR	EACH	19				19	
X ● 88600100	DETECTOR LOOP, TYPE I	FOOT	1137				1137	
X ● 88700200	LIGHT DETECTOR	EACH	3				3	
X ● 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1				1	
X ● 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8				8	
X ● 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1				1	
X ● 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1				1	
X ● 89502380	REMOVE EXISTING HANDHOLE	EACH	13				13	
X ● 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	9				9	
X Z0022800	FENCE REMOVAL	FOOT	123	123				
X Δ Z0076600	TRAINEES	HOURS	1000	1000				
X XX000613	MODULAR BLOCK RETAINING WALL	SQ FT	321	321				

X INDICATES SPECIAL PROVISION Δ Y080  
● INDICATES SPECIALTY ITEM

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	1000-2A			Y031-1F	Y030-1E
				DEVON AVE & N. ARLINGTON HEIGHTS RD (COOK COUNTY)	S. ARLINGTON HEIGHTS RD (DUPAGE COUNTY)	VILLAGE ITEMS	TRAFFIC SIGNALS	LIGHTING
				70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL	70% FEDERAL 30% LOCAL
X ● XX001368	PULL EXISTING CABLE FROM UNIT DUCT	FOOT	345					345
X ● XX006257	RECESSED REFLECTIVE PAVEMENT MARKER	EACH	43		43			
X X0301766	DRILL AND GROUT #6 TIE BARS	EACH	6165	5100	1065			
X X0321556	SANITARY MANHOLES TO BE ADJUSTED	EACH	9	8	1			
X ● X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	292	183	109			
X ● X0322925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	3885				3885	
X X0323381	STORM SEWERS, (WATER MAIN REQUIREMENTS) TYPE 1, 12"	FOOT	116	116				
X X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	190	148	42			
X X0323973	SEDIMENT CONTROL, SILT FENCE	FOOT	2907	2907				
X X0323974	SEDIMENT CONTROL, SILT FENCE MAINTENANCE	FOOT	1460	1460				
X X0325405	FILL EXISTING STORM SEWERS	CU YD	1.2		1.2			
X ● X0325737	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1				1	
X X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	24	19	5			
X X4023000	TEMPORARY ACCESS (ROAD)	EACH	4	4				
X ● X8050015	SERVICE INSTALLATION, POLE MOUNT	EACH	1				1	
X ● X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1				1	
X ● X8710020	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	3897				3897	
X ● X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	837				837	
X ● X8730250	ELECTRIC CABLE IN CONDUIT, NO. 20 3C, TWISTED, SHIELDED	FOOT	620				620	
X X0007129	DETECTABLE WARNINGS, SPECIAL	SQ FT	83	83				
X X0007130	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 15 1/2"	SQ YD	62		62			
X ● X0325705	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM - LEVEL II	EACH	1				1	
X ● X0007147	REMOVE AND REPLACE LAWN SPRINKLER SYSTEM	FOOT	300			300		
X ● X0007131	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT (SPECIAL)	EACH	24					24
X ● 72000100	SIGN PANEL - TYPE 1	SQ FT	125	101	24			
X ● 72000200	SIGN PANEL - TYPE 2	SQ FT	130	32			98	

X INDICATES SPECIAL PROVISION  
● INDICATES SPECIALTY ITEM



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	13
STA. 60+00 TO STA. 65+50		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)		

# PAVEMENT DESIGN

ITEM	DEVON AVENUE WEST LEG	DEVON AVENUE EAST LEG	ARLINGTON HEIGHTS ROAD NORTH LEG	ARLINGTON HEIGHTS ROAD SOUTH LEG
STRUCTURAL DESIGN TRAFFIC (2024)	30,833	30,833	24,333	24,333
ROAD CLASSIFICATION	I	I	I	I
PASSENGER CARS	30,279	27,534	23,992	23,457
SINGLE UNITS	370	2,189	226	584
MULTIPLE UNITS	184	1,110	115	292
TRAFFIC FACTOR	1.25	7.36	0.77	1.96
DESIGN THICKNESS	20	20	20	20

THICKNESS = (HMA SURF + HMA BIND + PCC BASE CSE + AGG SUB-GRADE) INCHES

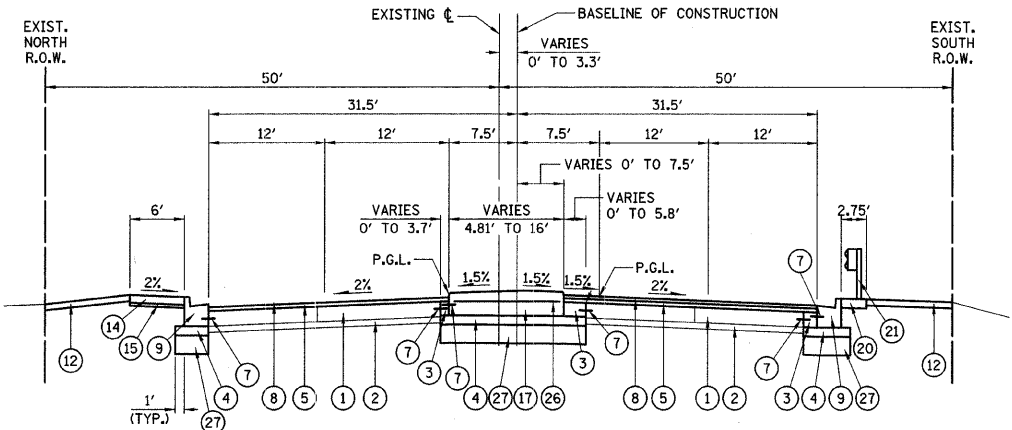
# LEGEND

- EXISTING CONCRETE PAVEMENT (TO REMAIN IN PLACE)
- EXISTING GRANULAR SUB-BASE (TO REMAIN IN PLACE)
- PROPOSED P.C. CONCRETE BASE COURSE, 10"
- PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 (VAR. THICKNESS, MIN. 2.25")
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (VAR. THICKNESS, MIN. 2.25")
- PROPOSED #6 TIE BAR, 24" LONG @ 30" C-C (DRILLED & GROUTED) (EMBED 8" MIN)
- PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (1 3/4")
- PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (FLAG DEPTH = 14")
- PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12 (FLAG DEPTH = 14")
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
- PROPOSED 4" TOPSOIL & SODDING
- PROPOSED HOT-MIX ASPHALT BASE COURSE, 8"
- PROPOSED P.C. CONCRETE SIDEWALK 5", SPECIAL
- PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 2" (INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL)
- PROPOSED CONCRETE MEDIAN, TYPE SB-6.12
- PROPOSED CORRUGATED MEDIAN
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 3/4")
- PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- PROPOSED HOT-MIX ASPHALT SHOULDERS, 6"
- PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE A
- PROPOSED CONCRETE MEDIAN, TYPE C-4
- LONGITUDINAL CONSTRUCTION JOINT - TYPE B (SEE SHEET NO. 75)
- PROPOSED 6" TOPSOIL & SODDING
- PROPOSED 6" TOPSOIL & SODDING, SALT TOLERANT
- PROPOSED PAVEMENT FABRIC
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL POROUS GRANULAR EMBANKMENT, SUBGRADE SEE ROADWAY SOILS INVESTIGATION BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED JUNE 10, 2008 AND THE CROSS SECTIONS FOR ADDITIONAL INFORMATION.

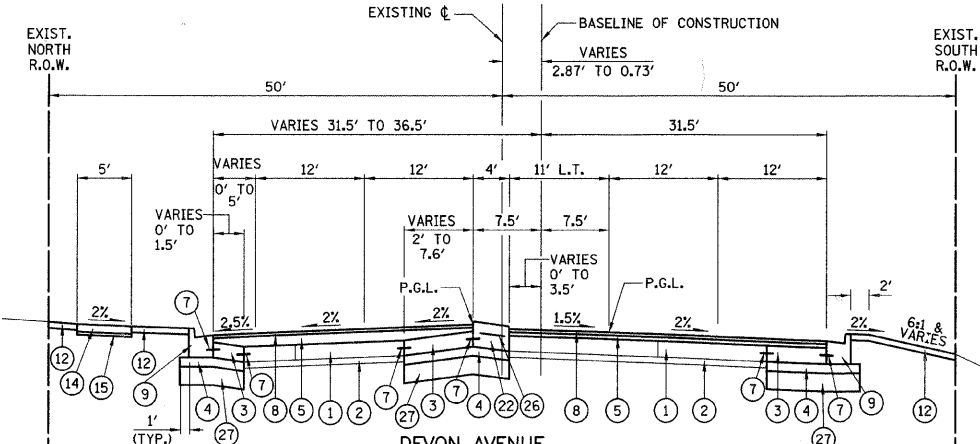
P.G.L. PROPOSED PROFILE GRADE LINE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
PROPOSED TYPICAL SECTIONS  
DEVON AVENUE

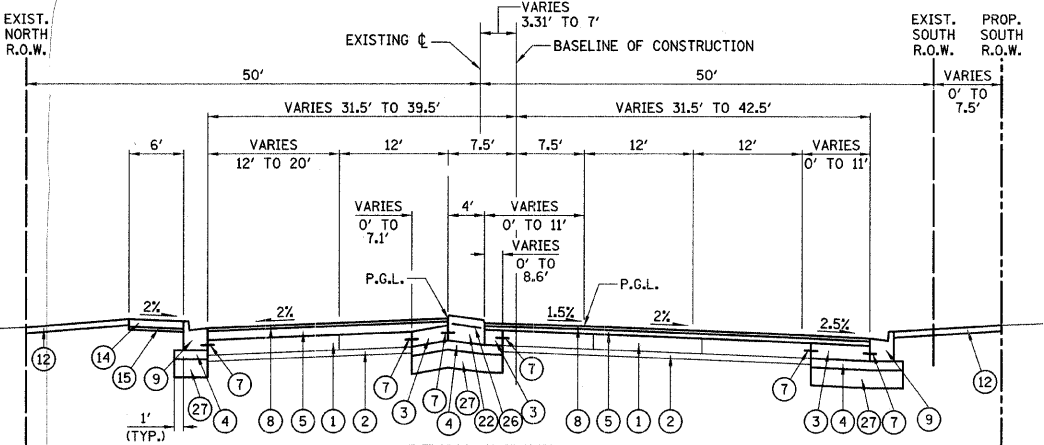
DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



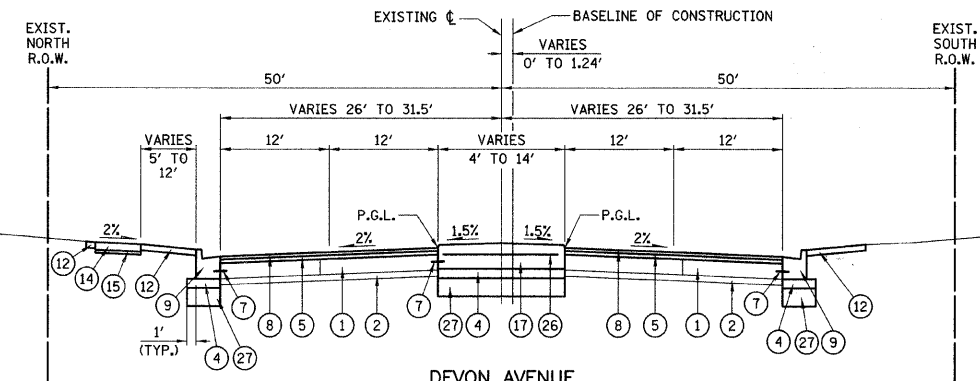
DEVON AVENUE  
STA. 55+59.23 TO STA. 58+73.32



DEVON AVENUE  
STA. 66+86 TO STA. 68+50



DEVON AVENUE  
STA. 58+73.32 TO STA. 62+42.83



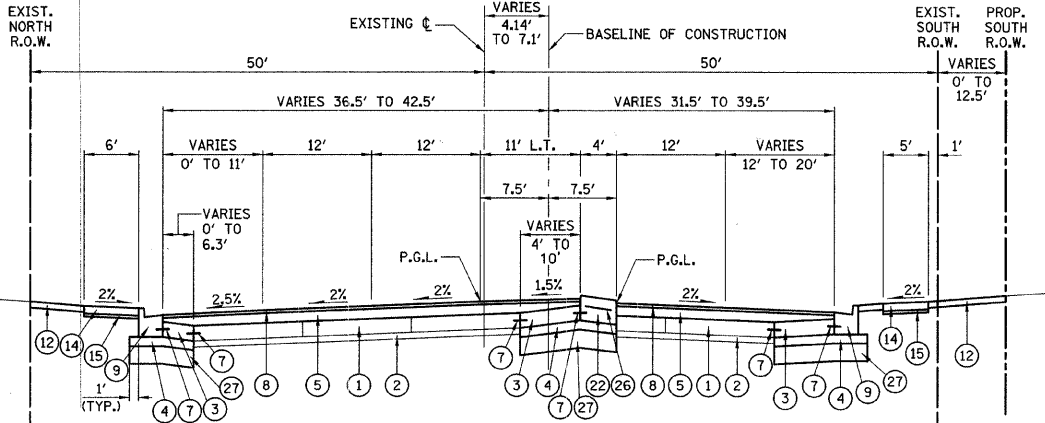
DEVON AVENUE  
STA. 68+50 TO STA. 70+89.41

# HOT MIX ASPHALT MIXTURE REQUIREMENT

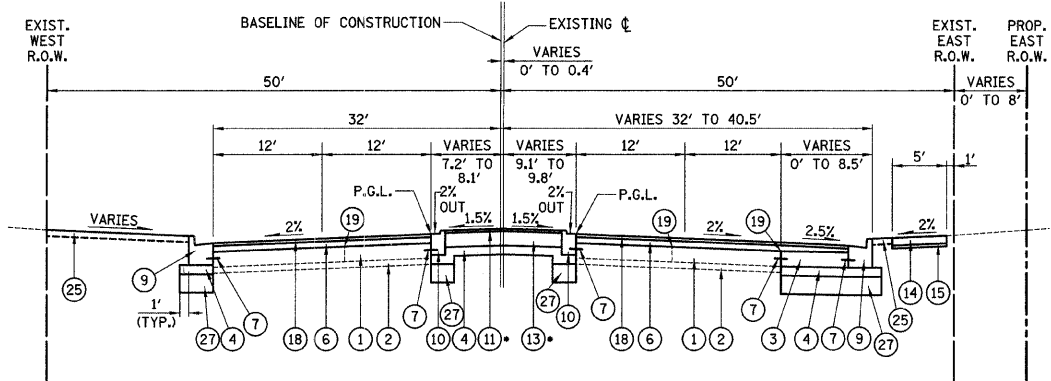
PAY ITEM	AC TYPE	VOIDS
HOT-MIX ASPHALT RESURFACING (DEVON AVENUE)		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 MM) (1 3/4" THICKNESS)	SBS/SBR PG 70-22	4% @ 90 GYR.
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 (2 1/4" THICKNESS, MIN.)	SBS/SBR PG 70-22	4% @ 90 GYR.
HOT-MIX ASPHALT RESURFACING (ARLINGTON HEIGHTS ROAD)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 19MM) (1 3/4" THICKNESS)	PG 64-22	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (2 1/4" THICKNESS, MIN.)	PG 64-22*	4% @ 70 GYR.
LEVELING BINDER (TURNER AVENUE)		
LEVELING BINDER (MACHINE METHOD), N50 (IL 19MM)	PG 64-22*	4% @ 50 GYR.
HOT-MIX ASPHALT MEDIAN SURFACE		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5MM) (2" THICKNESS)	PG 64-22	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL 19MM) (8" THICKNESS)	PG 64-22*	4% @ 50 GYR.
HOT-MIX ASPHALT SHOULDERS		
HOT-MIX ASPHALT SHOULDER (6" THICKNESS)	PG 64-22*	2% @ 30 GYR.
HOT-MIX ASPHALT DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5MM) (1 1/2" THICKNESS)	PG 64-22	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 MM) (8" THICKNESS)	PG 64-22*	4% @ 50 GYR.

NOTE: THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.

\* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22



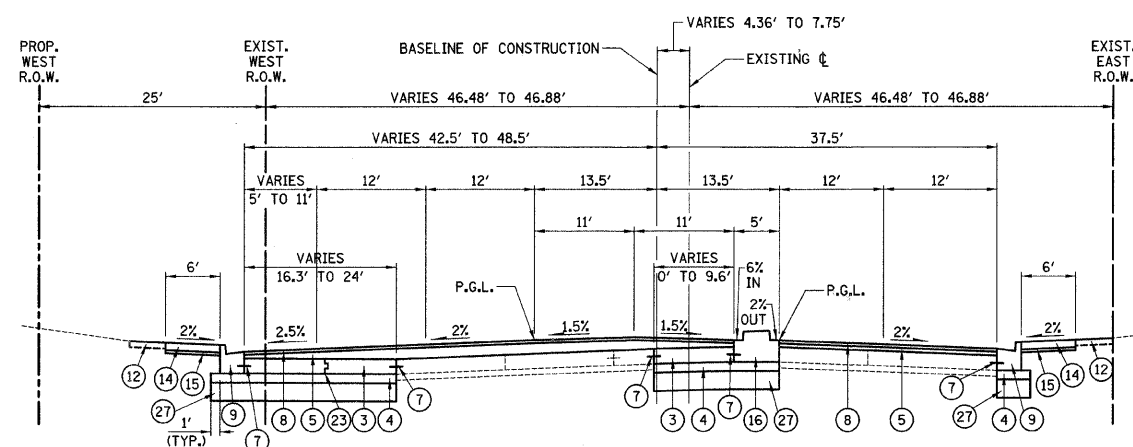
DEVON AVENUE  
STA. 62+42.83 TO STA. 66+86



ARLINGTON HEIGHTS ROAD

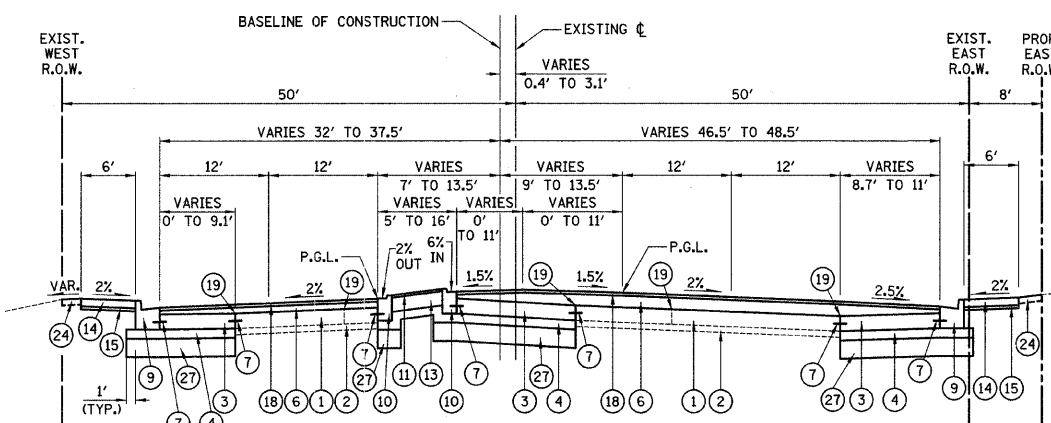
STA. 10+29.61 TO STA. 11+44.61

PAID FOR AS "HOT-MIX ASPHALT MEDIAN SURFACE, 9 1/2" "



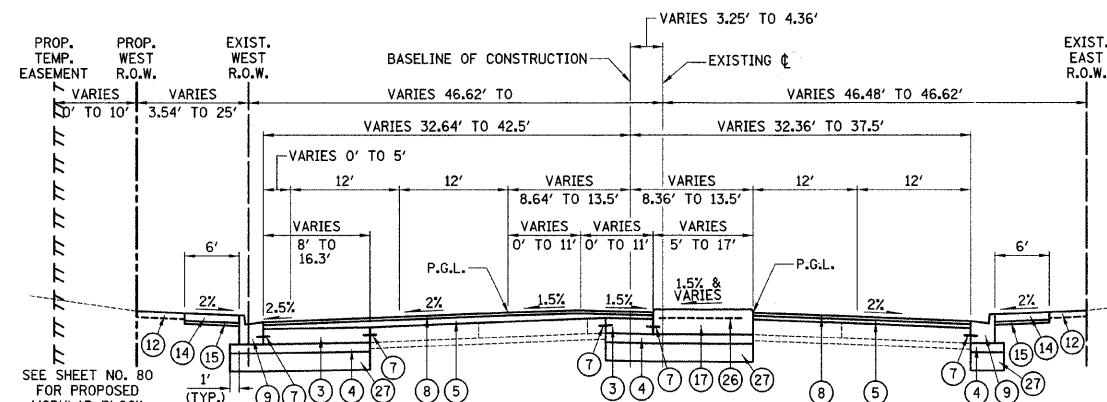
ARLINGTON HEIGHTS ROAD

STA. 16+99.04 TO STA. 19+67.15



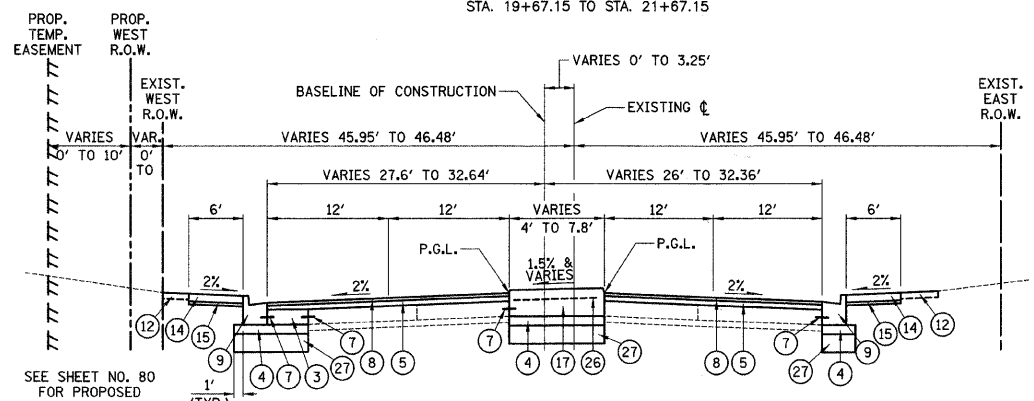
ARLINGTON HEIGHTS ROAD

STA. 11+44.61 TO STA. 13+39.61



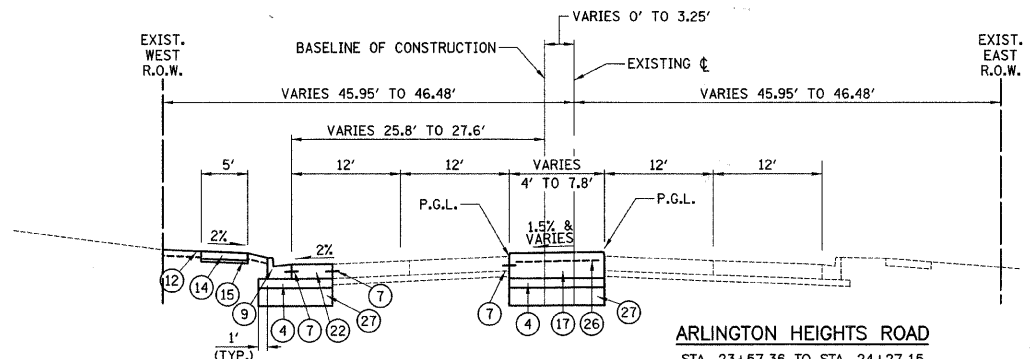
ARLINGTON HEIGHTS ROAD

STA. 19+67.15 TO STA. 21+67.15



ARLINGTON HEIGHTS ROAD

STA. 21+67.15 TO STA. 23+57.36



ARLINGTON HEIGHTS ROAD

STA. 23+57.36 TO STA. 24+27.15

# LEGEND

- 1 EXISTING CONCRETE PAVEMENT (TO REMAIN IN PLACE)
- 2 EXISTING GRANULAR SUB-BASE (TO REMAIN IN PLACE)
- 3 PROPOSED P.C. CONCRETE BASE COURSE, 10"
- 4 PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- 5 PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 (VAR. THICKNESS, MIN. 2.25")
- 6 PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (VAR. THICKNESS, MIN. 2.25")
- 7 PROPOSED #6 TIE BAR, 24" LONG @ 30" C-C (DRILLED & GROUTED) (EMBED 8" MIN)
- 8 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (1 3/4")
- 9 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (FLAG DEPTH = 14")
- 10 PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE M-2.12 (FLAG DEPTH = 14")
- 11 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 2"
- 12 PROPOSED 4" TOPSOIL & SODDING
- 13 PROPOSED HOT-MIX ASPHALT BASE COURSE, 8"
- 14 PROPOSED P.C. CONCRETE SIDEWALK 5", SPECIAL
- 15 PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B 2" (INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL)
- 16 PROPOSED CONCRETE MEDIAN, TYPE SB-6.12
- 17 PROPOSED CORRUGATED MEDIAN
- 18 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 3/4")
- 19 PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 20 PROPOSED HOT-MIX ASPHALT SHOULDERS, 6"
- 21 PROPOSED STEEL PLATE BEAM GUARDRAIL, TYPE A
- 22 PROPOSED P.C. CONCRETE PAVEMENT, 10"
- 23 LONGITUDINAL CONSTRUCTION JOINT - TYPE B (SEE DETAIL SHEET NO. 75)
- 24 PROPOSED 6" TOPSOIL & SODDING
- 25 PROPOSED 6" TOPSOIL & SODDING, SALT TOLERANT
- 26 PROPOSED PAVEMENT FABRIC
- 27 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL POROUS GRANULAR EMBANKMENT, SUBGRADE SEE ROADWAY SOILS INVESTIGATION BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED JUNE 10, 2008 AND THE CROSS SECTIONS FOR ADDITIONAL INFORMATION.

P.G.L. PROPOSED PROFILE GRADE LINE

## ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED TYPICAL SECTIONS ARLINGTON HEIGHTS ROAD

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



SCHEDULE OF DRIVEWAYS			
STATION	DRIVEWAY REMOVAL	PCC DRIVEWAY, 8 INCH, SPECIAL	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 15 1/2"
	(SQ YD)	(SQ YD)	(SQ YD)
55+85, RT	37	36	
58+13, LT	37	50	
60+50, RT	112	52	
61+60, RT	307	42	
63+90, RT	44	37	
64+64, LT	65	34	
65+15, LT	61	51	
65+31, RT	38	33	
65+58, LT	57	28	
66+04, LT	36	23	
67+24, LT	35	41	
68+03, LT	55	70	
68+88, LT	30	31	
69+90, LT	61	72	
10+79, RT	38		28
11+58, RT	218	137	
12+54, RT	153	86	
14+00, LT	85	46	
14+50, RT	58		34
17+48, RT	42	43	
18+85, RT	44	28	
20+54, LT	92	71	
21+67, RT	23	38	
22+63, RT	68	59	

SCHEDULE OF TREE REMOVAL		
STATION	6 TO 15 UNIT DIAMETER	OVER 15 UNIT DIAMETER
60+96, RT	6	
61+12, RT	8	
14+46, LT	10	
14+59, LT	11	
18+19, RT	13	
19+74, LT		19
21+26, LT	12, 12	
21+81, LT	12	24

EARTHWORK SCHEDULE					
		PRE-STAGE 1	STAGE 1	STAGE 2	STAGE 3
ITEM	UNIT				
EARTH EXCAVATION	C.Y.	28	3145	426	394
EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	C.Y.	24	2673	362	335
EMBANKMENT REQUIRED	C.Y.	0	380	6	145
EARTHWORK BALANCE					
WASTE (+) OR SHORTAGE (-)	C.Y.	+24	+2293	+356	+190

SHRINKAGE CALCULATED USING 15% SHRINKAGE FACTOR

A PAY ITEM FOR "FURNISHED EXCAVATION" HAS BEEN INCLUDED ON THE ASSUMPTION THAT, DUE TO LIMITED WORKING SPACE, ALL EMBANKMENT MAY HAVE TO BE BROUGHT IN FROM OUTSIDE THE PROJECT LIMITS.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

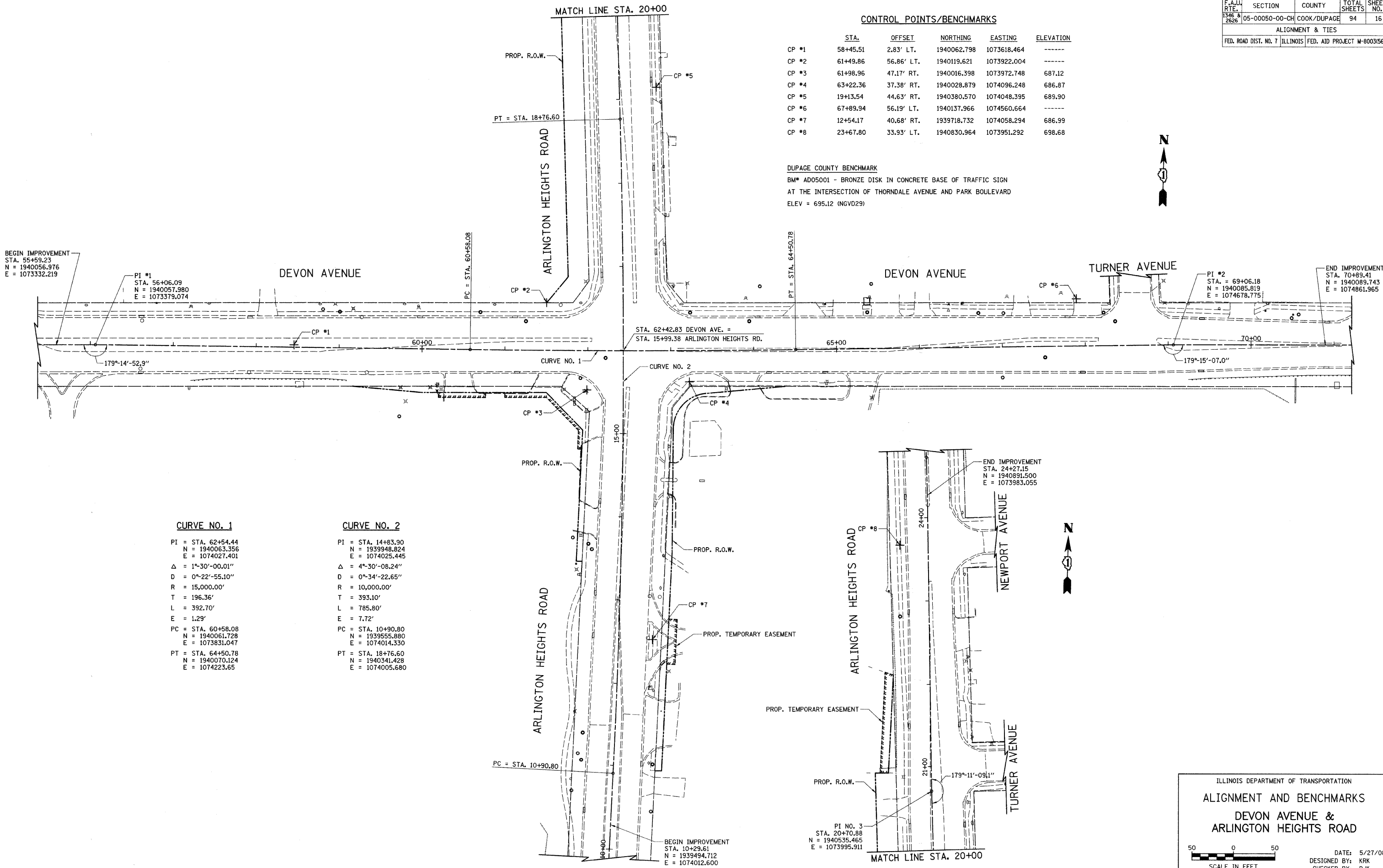
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	16
ALIGNMENT & TIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT W-8003(569)				

# CONTROL POINTS/BENCHMARKS

	STA.	OFFSET	NORTHING	EASTING	ELEVATION
CP #1	58+45.51	2.83' LT.	1940062.798	1073618.464	-----
CP #2	61+49.86	56.86' LT.	1940119.621	1073922.004	-----
CP #3	61+98.96	47.17' RT.	1940016.398	1073972.748	687.12
CP #4	63+22.36	37.38' RT.	1940028.879	1074096.248	686.87
CP #5	19+13.54	44.63' RT.	1940380.570	1074048.395	689.90
CP #6	67+89.94	56.19' LT.	1940137.966	1074560.664	-----
CP #7	12+54.17	40.68' RT.	1939718.732	1074058.294	686.99
CP #8	23+67.80	33.93' LT.	1940830.964	1073951.292	698.68

## DUPAGE COUNTY BENCHMARK

BM\* AD05001 - BRONZE DISK IN CONCRETE BASE OF TRAFFIC SIGN  
AT THE INTERSECTION OF THORNDALE AVENUE AND PARK BOULEVARD  
ELEV = 695.12 (NGVD29)



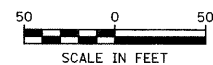
## CURVE NO. 1

PI = STA. 62+54.44  
N = 1940063.356  
E = 1074027.401  
Δ = 1°-30'-00.01"  
D = 0°-22'-55.10"  
R = 15,000.00'  
T = 196.36'  
L = 392.70'  
E = 1.29'  
PC = STA. 60+58.08  
N = 1940061.728  
E = 1073831.047  
PT = STA. 64+50.78  
N = 1940070.124  
E = 1074223.65

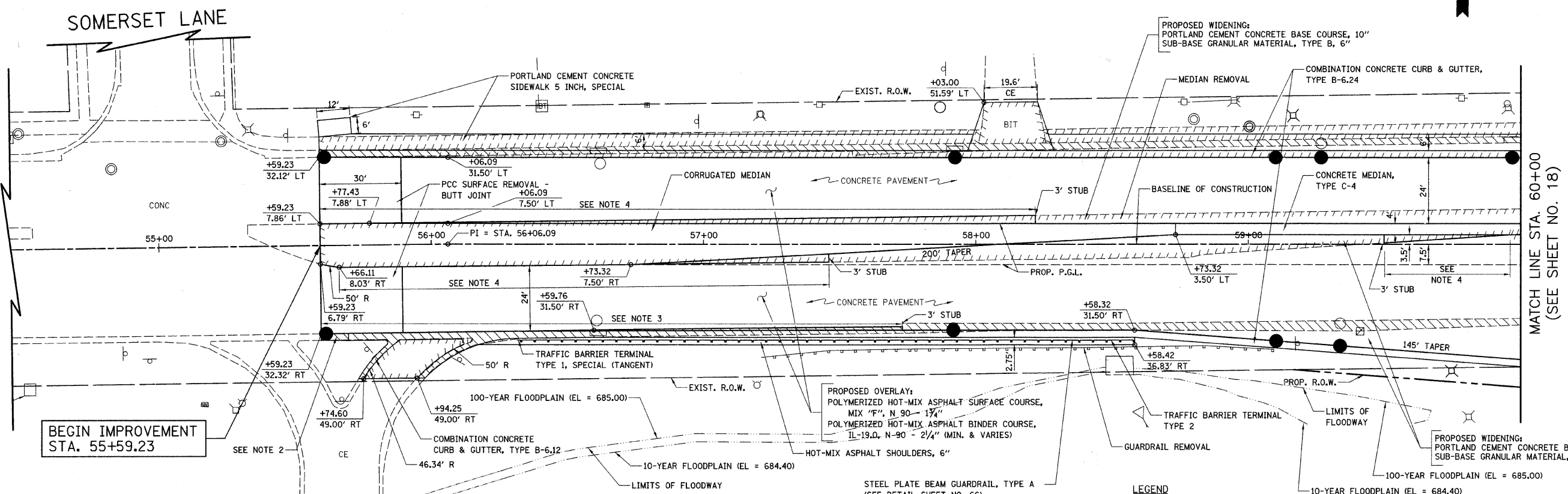
## CURVE NO. 2

PI = STA. 14+83.90  
N = 1939948.824  
E = 1074025.445  
Δ = 4°-30'-08.24"  
D = 0°-34'-22.65"  
R = 10,000.00'  
T = 393.10'  
L = 785.80'  
E = 7.72'  
PC = STA. 10+90.80  
N = 1939555.880  
E = 1074014.330  
PT = STA. 18+76.60  
N = 1940341.428  
E = 1074005.680

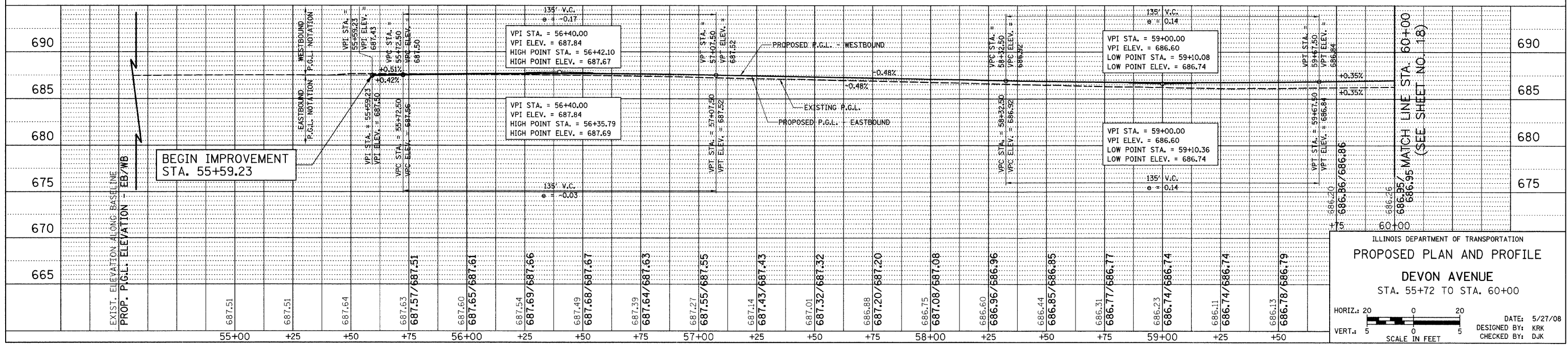
ILLINOIS DEPARTMENT OF TRANSPORTATION  
ALIGNMENT AND BENCHMARKS  
DEVON AVENUE &  
ARLINGTON HEIGHTS ROAD



DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



- NOTES:
- SEE TYPICAL SECTIONS AND CCHD DETAIL (SHEET NO. 75) FOR LOCATIONS OF PROPOSED TIE BARS.
  - TRANSITION CURB FROM B-6.24 TO MATCH EXISTING B-6.18 OVER 10'. PAID FOR AS "COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24".
  - CONSTRUCT BASE COURSE MONOLITHICALLY WITH CURB AND GUTTER. SEE DETAIL SHEET NO. 63.
  - CONSTRUCT BASE COURSE WIDENING MONOLITHICALLY WITH MEDIAN. SEE DETAIL SHEET NO. 63.
  - WHEN REMOVING THE EXISTING CURB AND GUTTER FROM PAVEMENT TO REMAIN, THE CONTRACTOR SHALL SAW CUT THE EXISTING CURB AND GUTTER TO SEVER THE EXISTING TIE BARS TO MAINTAIN A CLEAN, VERTICAL EDGE. THIS WORK SHALL BE INCLUDED IN THE COST OF "COMBINATION CURB AND GUTTER REMOVAL".



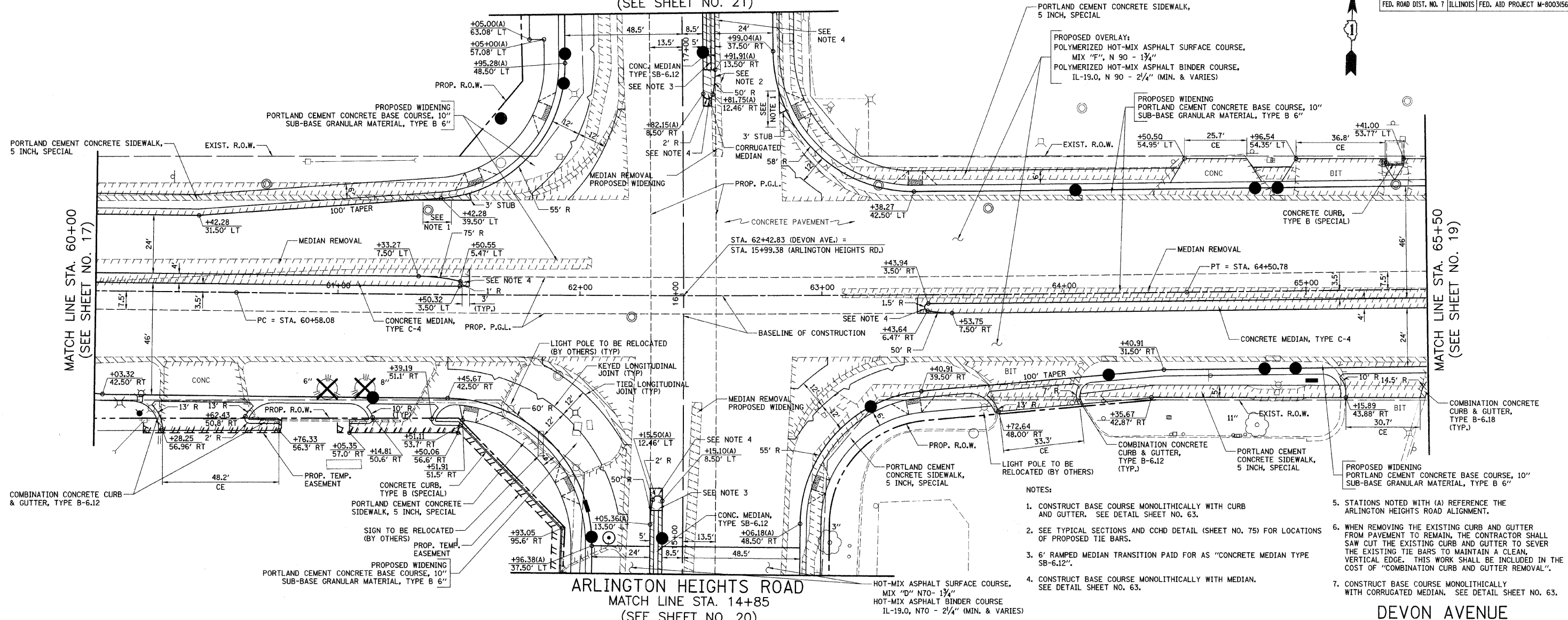
ILLINOIS DEPARTMENT OF TRANSPORTATION  
PROPOSED PLAN AND PROFILE  
DEVON AVENUE  
STA. 55+72 TO STA. 60+00

HORIZ.: 20  
VERT.: 5  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

ARLINGTON HEIGHTS ROAD  
MATCH LINE STA. 17+15  
(SEE SHEET NO. 21)

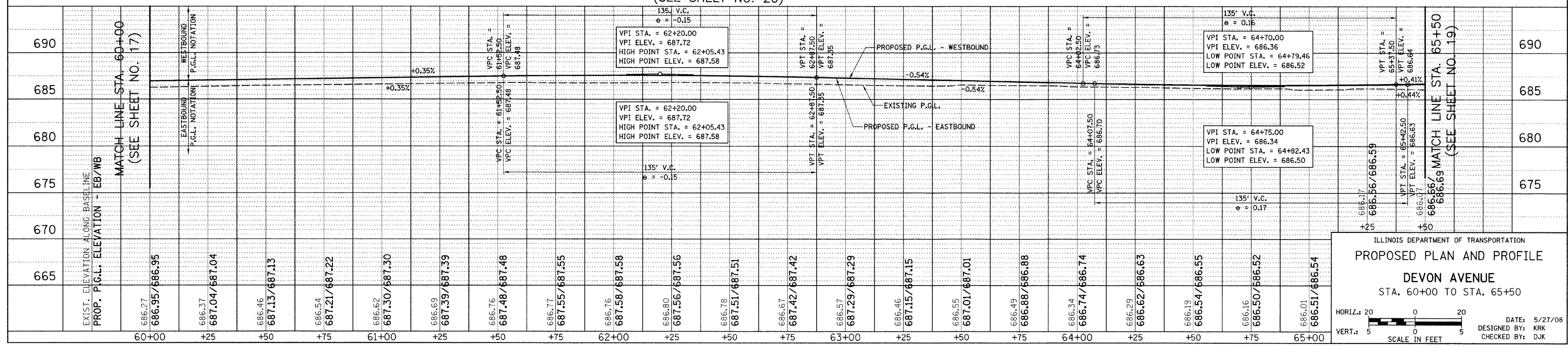
CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	18
STA. 60+00		TO STA. 65+50		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(569)	



- NOTES:
1. CONSTRUCT BASE COURSE MONOLITHICALLY WITH CURB AND GUTTER. SEE DETAIL SHEET NO. 63.
  2. SEE TYPICAL SECTIONS AND CCHD DETAIL (SHEET NO. 75) FOR LOCATIONS OF PROPOSED TIE BARS.
  3. 6" RAMPED MEDIAN TRANSITION PAID FOR AS "CONCRETE MEDIAN TYPE SB-6.12".
  4. CONSTRUCT BASE COURSE MONOLITHICALLY WITH MEDIAN. SEE DETAIL SHEET NO. 63.
  5. STATIONS NOTED WITH (A) REFERENCE THE ARLINGTON HEIGHTS ROAD ALIGNMENT.
  6. WHEN REMOVING THE EXISTING CURB AND GUTTER FROM PAVEMENT TO REMAIN, THE CONTRACTOR SHALL SAW CUT THE EXISTING CURB AND GUTTER TO SEVER THE EXISTING TIE BARS TO MAINTAIN A CLEAN, VERTICAL EDGE. THIS WORK SHALL BE INCLUDED IN THE COST OF "COMBINATION CURB AND GUTTER REMOVAL".
  7. CONSTRUCT BASE COURSE MONOLITHICALLY WITH CORRUGATED MEDIAN. SEE DETAIL SHEET NO. 63.

ARLINGTON HEIGHTS ROAD  
MATCH LINE STA. 14+85  
(SEE SHEET NO. 20)

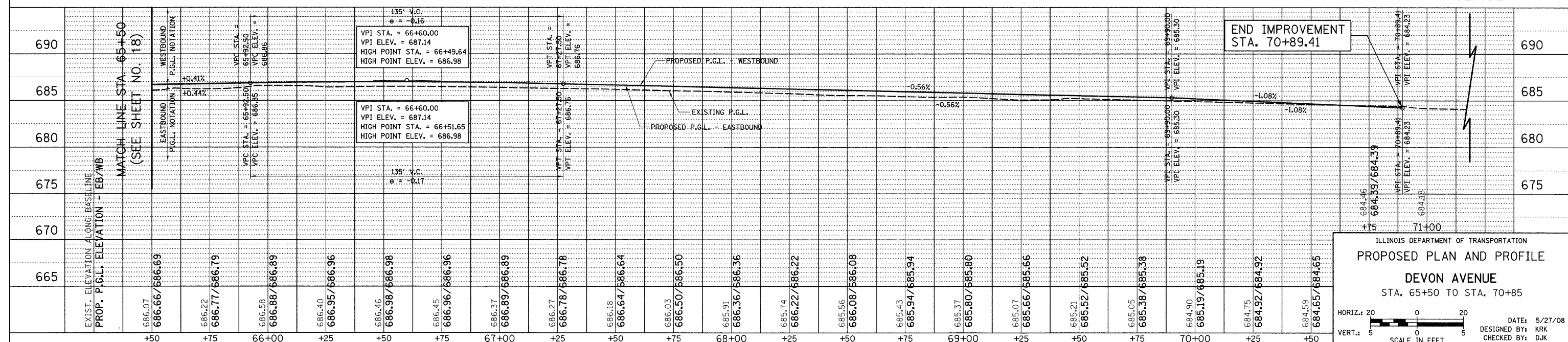
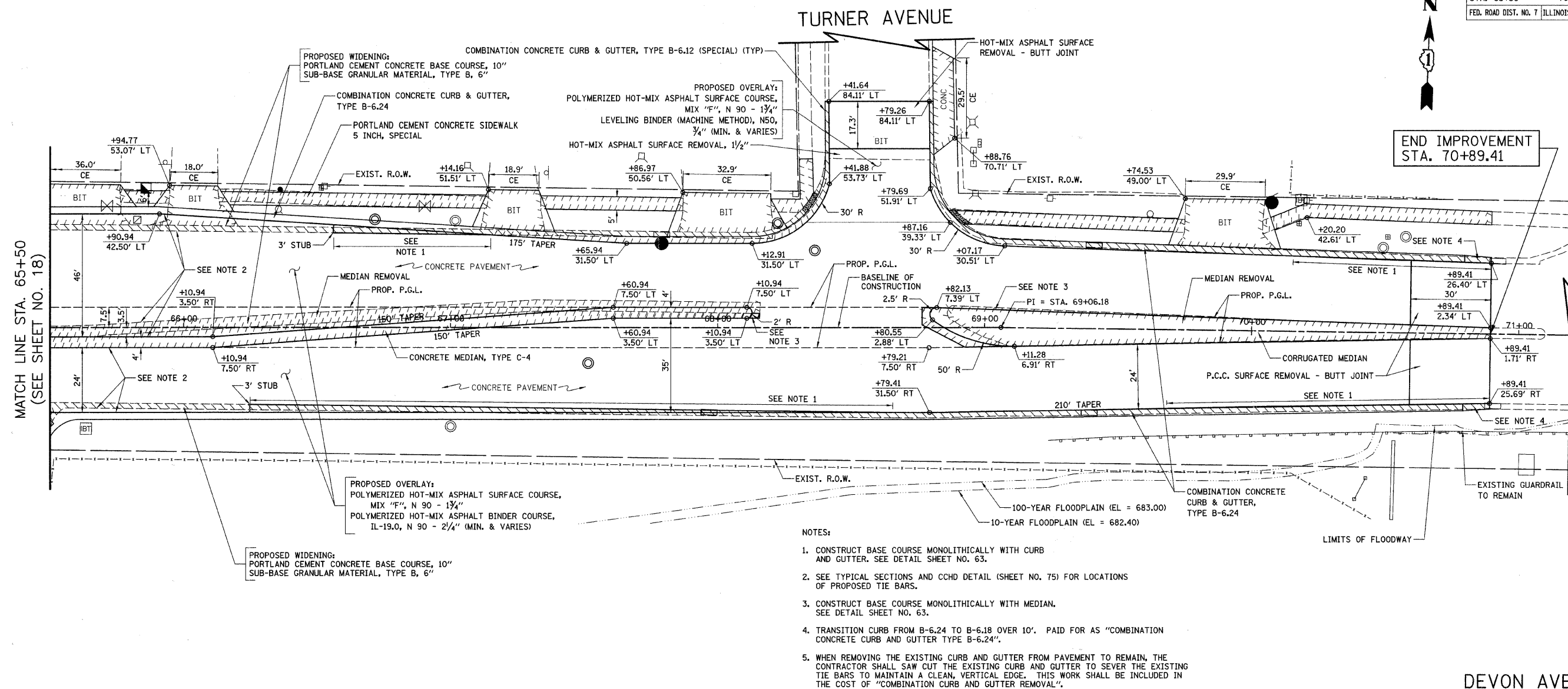
DEVON AVENUE



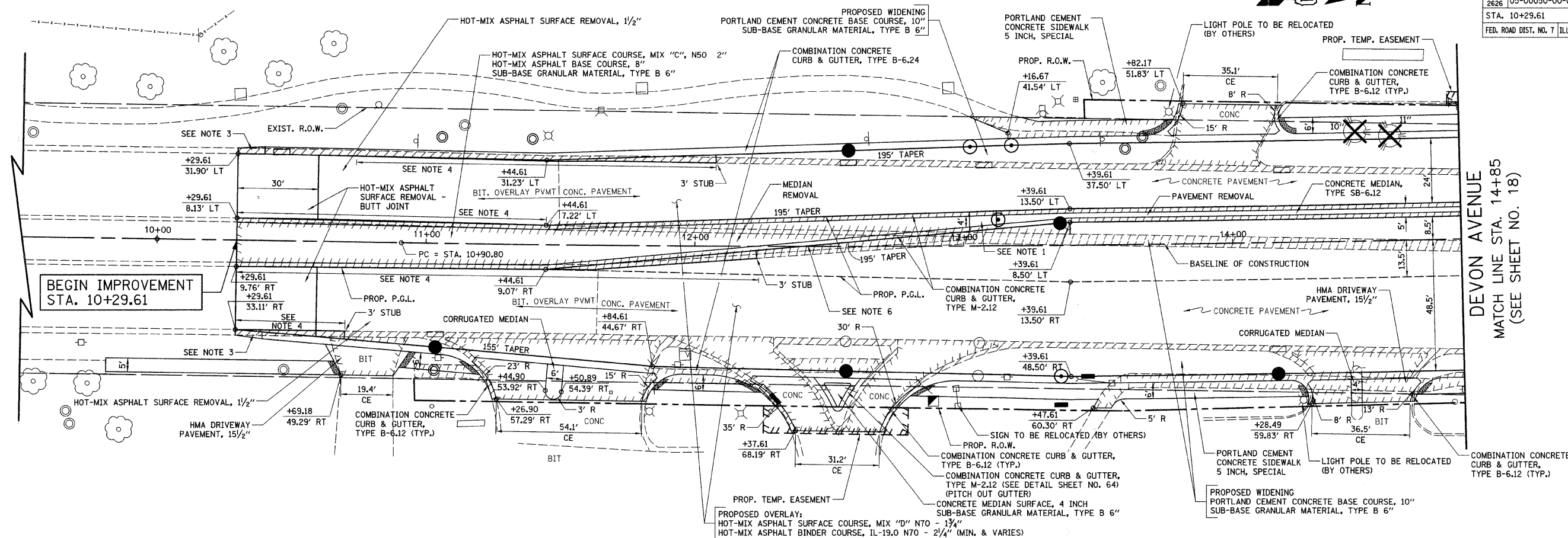
ILLINOIS DEPARTMENT OF TRANSPORTATION  
PROPOSED PLAN AND PROFILE  
DEVON AVENUE  
STA. 60+00 TO STA. 65+50

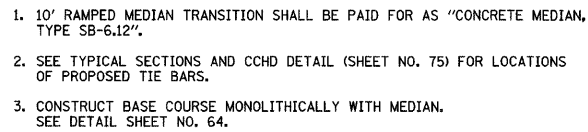
HORIZ.: 20  
VERT.: 5  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

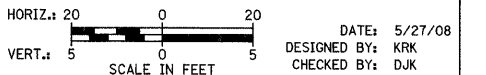


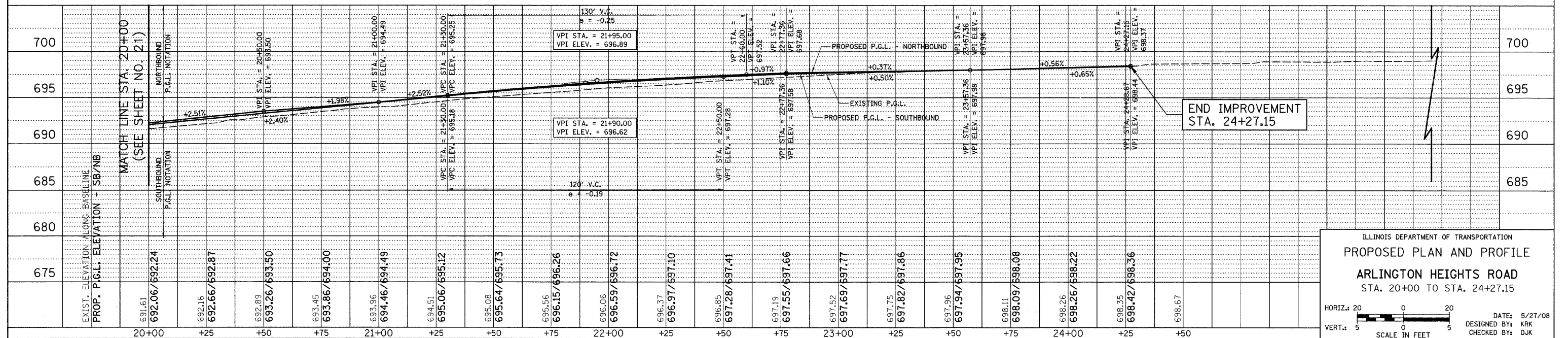
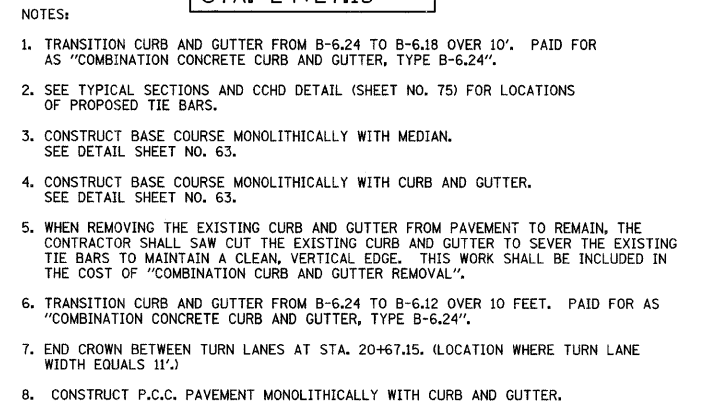






ARLINGTON HEIGHTS ROAD





F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	23
MAINTENANCE OF TRAFFIC GENERAL NOTES				
FED. ROAD DIST. NO. 7   ILLINOIS   FED. AID PROJECT M-8003(569)				

PRE-STAGE 1 (NOT ILLUSTRATED)

- INSTALL AND ACTIVATE TEMPORARY TRAFFIC SIGNALS AT THE INTERSECTION OF DEVON AVENUE AND ARLINGTON HEIGHTS ROAD. (NOTE: THE EXISTING SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE TEMPORARY SIGNALS ARE ACTIVATED.)
- REMOVE EXISTING SIGNAL POLES AND EQUIPMENT.
- TEMPORARILY REMOVE BUS STOP SIGNS FROM THE PROJECT LIMITS. THE CONTRACTOR SHALL COORDINATE THE NEW SIGN LOCATIONS WITH PACE.
- REMOVE CORRUGATED MEDIAN ON THE SOUTH LEG OF ARLINGTON HEIGHTS ROAD AND ON DEVON AVENUE FROM STA. 61+52 TO STA. 62+04 AND REPLACE WITH PCC BASE COURSE USING DAILY LANE CLOSURES PER STANDARD 701701.

STAGE 1

- PLACE ALL CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS, AND BARRICADES. ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
- CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES OUTSIDE EXISTING EDGES OF PAVEMENT ALONG THE SOUTH SIDE OF DEVON AVENUE AND BOTH SIDES OF ARLINGTON HEIGHTS ROAD.
- REMOVE EXISTING CORNER RADII AT DEVON AVENUE AND ARLINGTON HEIGHTS ROAD AND CONSTRUCT BASE COURSE WIDENING. TRUCK TURNING RESTRICTIONS MAY BE REQUIRED AS DIRECTED BY THE ENGINEER. ALL TRAFFIC CONTROL REQUIRED TO REMOVE AND RECONSTRUCT THE CORNER RADII SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION".
- CONSTRUCT ALL PAVEMENT, CURB AND GUTTER, SIDEWALKS AND ENTRANCES OUTSIDE THE EXISTING EDGES OF PAVEMENT ALONG THE SOUTH SIDE OF DEVON AVENUE AND BOTH SIDES OF ARLINGTON HEIGHTS ROAD.

STAGE 2

- REMOVE EXISTING RAISED REFLECTIVE PAVEMENT MARKERS.
- PLACE BINDER COURSE AS SHOWN, MAINTAINING TRAFFIC ON DEVON AVENUE WEST OF ARLINGTON HEIGHTS ROAD PER STANDARD 701421, AND ON DEVON AVENUE EAST OF ARLINGTON HEIGHTS ROAD AND ON ARLINGTON HEIGHTS ROAD PER STANDARD 701606.
- REMOVE ALL CONFLICTING PAVEMENT MARKINGS.
- PLACE CONSTRUCTION SIGNS, TEMPORARY PAVEMENT MARKINGS AND REPLACE BARRICADES.
- CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES IN THE MEDIANS ON ARLINGTON HEIGHTS ROAD.
- CONSTRUCT MEDIANS ON DEVON AVENUE AND ARLINGTON HEIGHTS ROAD.

STAGE 3

- REMOVE ALL CONFLICTING PAVEMENT MARKINGS.
- PLACE TEMPORARY PAVEMENT MARKINGS AND BARRICADES. RE-ADJUST TEMPORARY TRAFFIC SIGNALS AND SHIFT TRAFFIC AS INDICATED.
- CONSTRUCT STORM SEWERS AND DRAINAGE STRUCTURES OUTSIDE EXISTING EDGES OF PAVEMENT ALONG THE NORTH SIDE OF DEVON AVENUE.
- CONSTRUCT ALL PAVEMENT, CURB AND GUTTER, SIDEWALKS AND ENTRANCES OUTSIDE THE EXISTING EDGES OF PAVEMENT ALONG THE NORTH SIDE OF DEVON AVENUE.

STAGE 4 (NOT ILLUSTRATED)

- PLACE BINDER COURSE ON THE NORTH SIDE OF DEVON AVENUE, MAINTAINING TRAFFIC ON DEVON AVENUE WEST OF ARLINGTON HEIGHTS ROAD PER STANDARD 701421, AND EAST OF ARLINGTON HEIGHTS ROAD PER STANDARD 701606.
- INSTALL PROPOSED STREET LIGHTING.
- COMPLETE ALL LANDSCAPING.
- PLACE SURFACE COURSE TO FINISHED GRADE, MAINTAINING TRAFFIC ON DEVON AVENUE WEST OF ARLINGTON HEIGHTS ROAD PER STANDARD 701421, AND ON DEVON AVENUE EAST OF ARLINGTON HEIGHTS ROAD AND ON ARLINGTON HEIGHTS ROAD PER STANDARD 701606.
- PLACE PERMANENT PAVEMENT MARKINGS, REFLECTIVE PAVEMENT MARKERS AND SIGNS.
- INSTALL AND ACTIVATE PERMANENT TRAFFIC SIGNALS AT THE INTERSECTION OF DEVON AVENUE AND ARLINGTON HEIGHTS ROAD. (NOTE: THE TEMPORARY SIGNALS SHALL BE TURNED OFF AT THE SAME TIME THE PERMANENT SIGNALS ARE ACTIVATED.)
- REMOVE CONSTRUCTION SIGNS AND OPEN ALL LANES TO TRAFFIC.

MAINTENANCE OF TRAFFIC GENERAL NOTES

- TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
- ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
- BARRICADES WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT THE BARRICADES SHALL BE DRUMS, NON-METALLIC WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS. SPACING SHALL BE AS SHOWN ON THE CONSTRUCTION STAGING PLANS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 701901.
- ALL DRUMS AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS.
- DRUMS EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII.
- DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
- DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
- TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 701901 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT.
- THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
- EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED.
- THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
- EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701501-03 OR 701601-04 UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
- "FRESH OIL" SIGNS (W21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- THE CONTRACTOR SHALL ERECT TEMPORARY STREET NAME SIGNS ON METAL POSTS THROUGHOUT CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED ON ALL SURFACES OUTSIDE OF THE RECONSTRUCTION LIMITS. THIS WORK SHALL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING TAPE, TYPE III" OF THE SIZE SPECIFIED.
- ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION".
- THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS, TEMPORARY PAVEMENT MARKINGS, AND WORK ZONE PAVEMENT MARKING REMOVAL ARE NOT INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.
- A QUANTITY FOR "CHANGEABLE MESSAGE SIGN" HAS BEEN INCLUDED FOR USE WHEN DIRECTED BY THE ENGINEER.
- ACCESS TO PROPERTIES SHALL BE MAINTAINED AT ALL TIMES BY STAGE CONSTRUCTING THE IMPROVEMENTS IN FRONT OF ENTRANCES. A QUANTITY FOR "TEMPORARY INFORMATION SIGNING" HAS BEEN INCLUDED FOR USE WHEN DIRECTED BY THE ENGINEER TO PROVIDE GUIDANCE SIGNS WHEN A DRIVEWAY MUST BE CLOSED TEMPORARILY FOR CONSTRUCTION OF THE DRIVEWAY APRON. PROPERTIES WITH MULTIPLE ENTRANCES SHALL ONLY HAVE ONE ENTRANCE CLOSED AT A TIME.
- A QUANTITY OF ONE "AGGREGATE FOR TEMPORARY ACCESS (ROAD)" HAS BEEN INCLUDED FOR USE IN PROVIDING PEDESTRIAN ACCESS ACROSS THE WIDENING AT THE INTERSECTION OF DEVON AVENUE AND ARLINGTON HEIGHTS ROAD.

- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE COOK COUNTY HIGHWAY DEPARTMENT'S INTENT TO KEEP THE ROADWAYS (DEVON AVENUE AND ARLINGTON HEIGHTS ROAD) OPEN FOR ALL TRAFFIC AT ALL TIMES, EXCEPT DURING CONSTRUCTION OPERATIONS. DURING CONSTRUCTION OPERATIONS, AT LEAST ONE THROUGH LANE FOR EACH DIRECTION OF TRAFFIC SHALL BE MAINTAINED ALONG THE ROADWAYS AT ALL TIMES, AND LEFT TURN LANES SHALL BE MAINTAINED FOR ALL APPROACHES AT THE INTERSECTION OF DEVON AVENUE AND ARLINGTON HEIGHTS ROAD. ANY SHORT TERM CONSTRUCTION ACTIVITY THAT REQUIRES ENCROACHMENT ON THE LANES OPEN FOR TRAFFIC SHALL BE RESTRICTED TO WITHIN THE HOURS OF 9:00 A.M. TO 3:00 P.M. LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE APPLICABLE I.D.O.T. TRAFFIC CONTROL STANDARDS.
- ALL INTERSECTING STREETS SHALL BE KEPT OPEN TO TRAFFIC, AS DIRECTED BY THE ENGINEER.



W21-2  
48"x48"

DATE XX-XX-XX

9"x30"



W8-1  
30"x30"

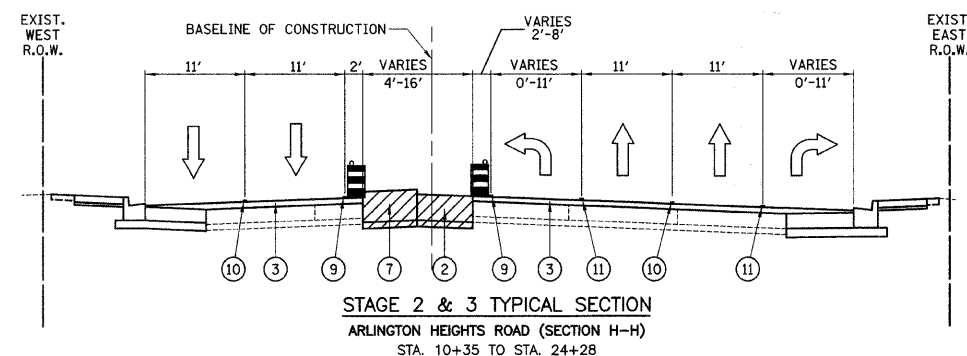
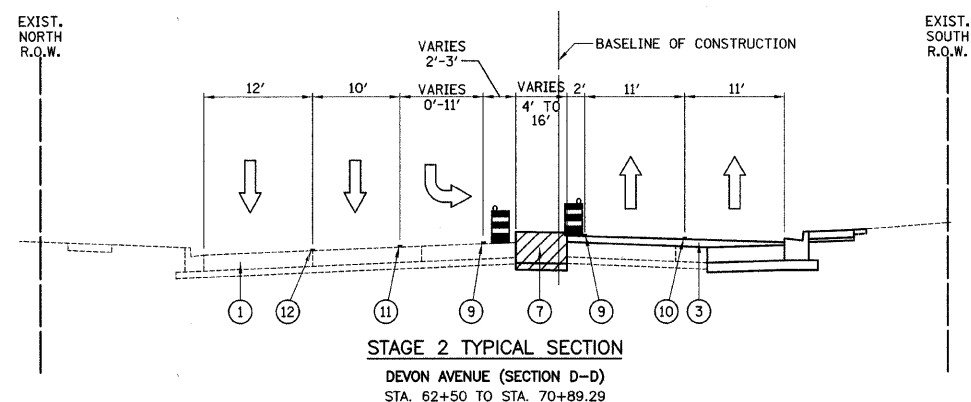
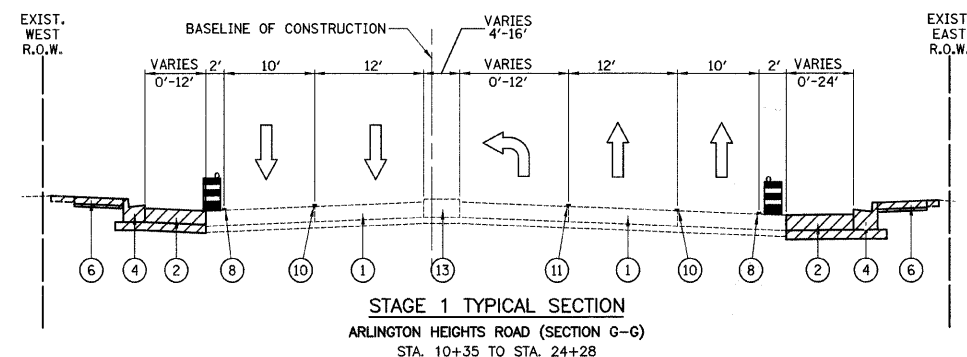
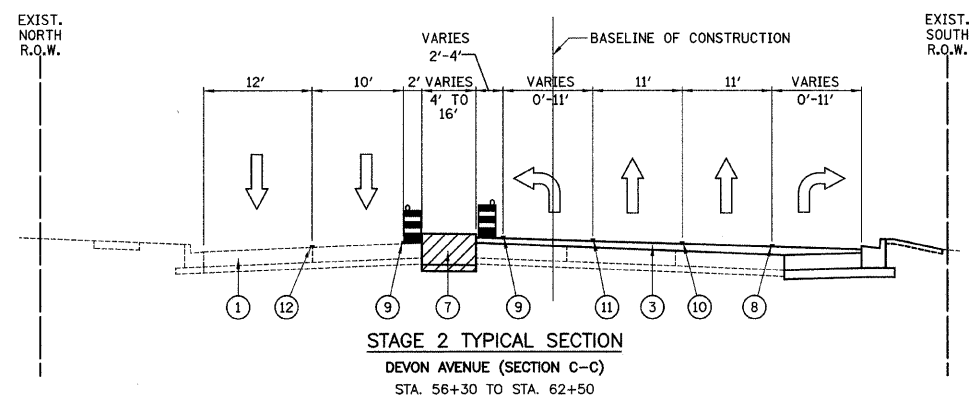
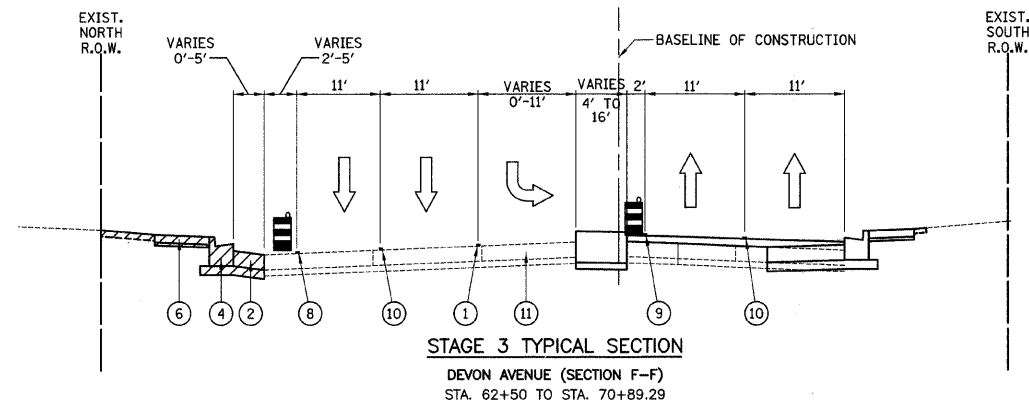
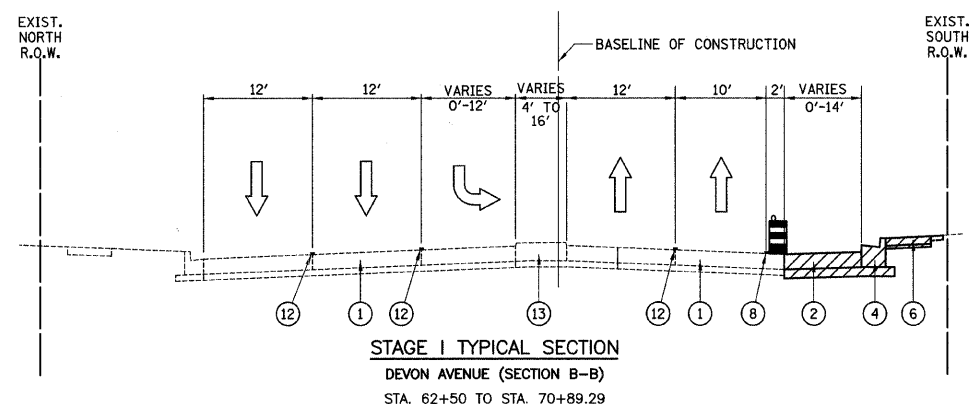
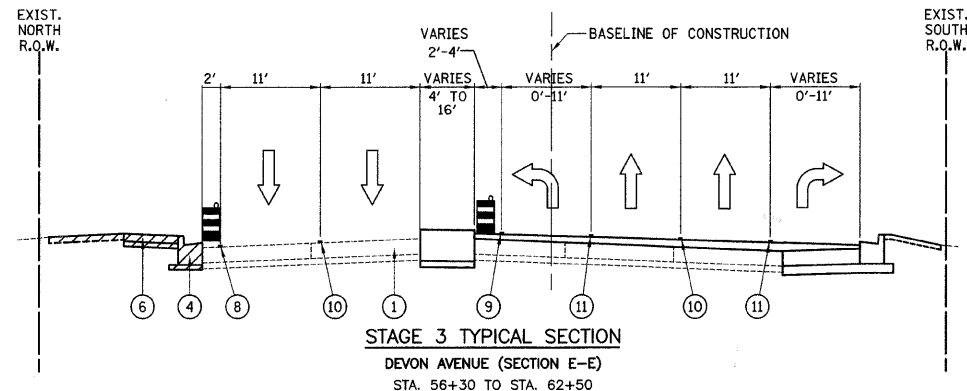
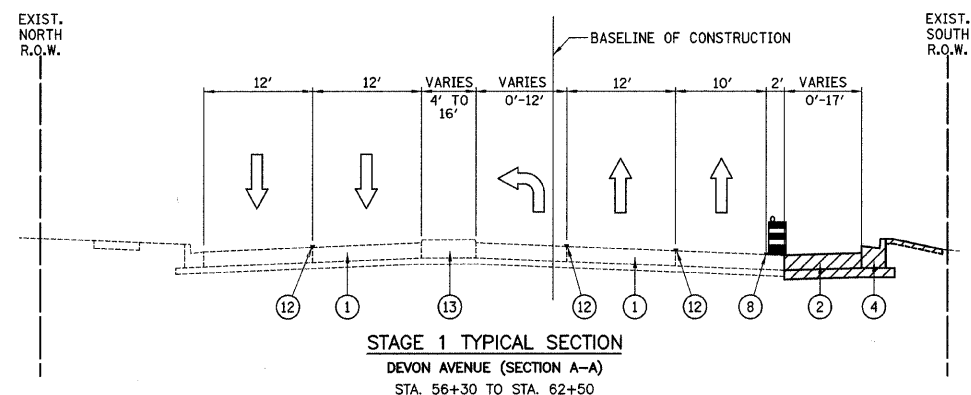
TO BE PLACED ON DEVON AVENUE, ARLINGTON HEIGHTS ROAD AND ALL SIDE STREETS

ILLINOIS DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC  
GENERAL NOTES  
DEVON AVENUE &  
ARLINGTON HEIGHTS ROAD

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	24
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

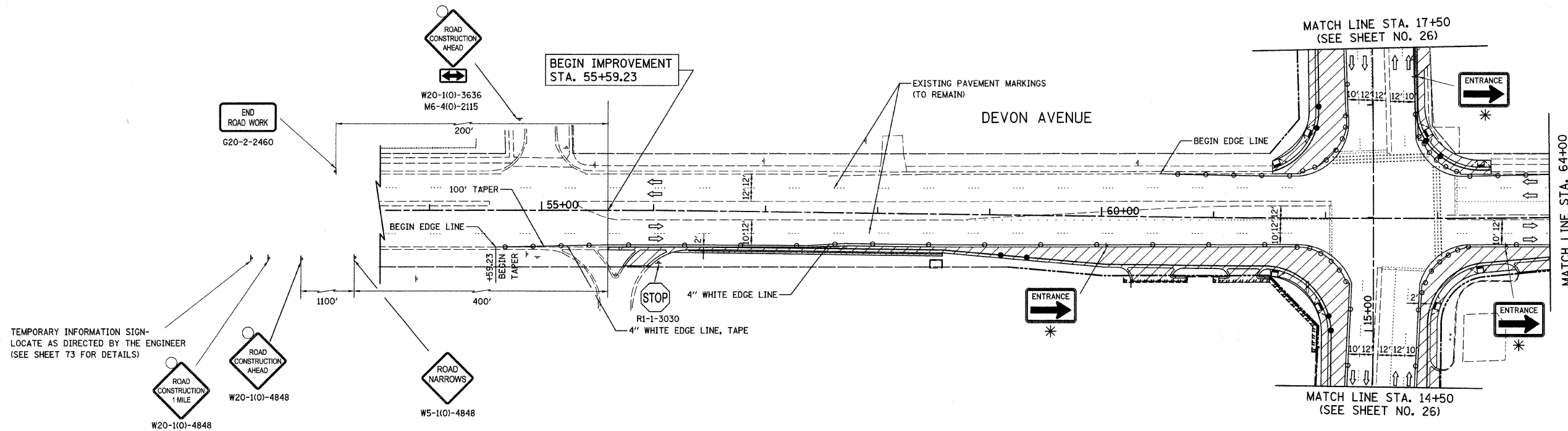


- LEGEND**
- ① EXISTING PAVEMENT
  - ② PROPOSED PAVEMENT WIDENING
  - ③ \*\*PROPOSED HOT-MIX ASPHALT BINDER COURSE
  - ④ PROPOSED COMB. CONCRETE CURB AND GUTTER
  - ⑥ PROPOSED SIDEWALK
  - ⑦ PROPOSED MEDIAN
  - ⑧ TEMPORARY PAINT PAVEMENT MARKING - LINE 4" (WHITE EDGE LINE)
  - ⑨ TEMPORARY PAINT PAVEMENT MARKING - LINE 4" (YELLOW EDGE LINE)
  - ⑩ TEMPORARY PAINT PAVEMENT MARKING - LINE 4" (WHITE SKIP-DASH LANE LINE)
  - ⑪ TEMPORARY PAINT PAVEMENT MARKING - LINE 6" (WHITE LANE LINE, SOLID OR DOTTED)
  - ⑫ EXISTING WHITE LANE LINE (SOLID, DOTTED, OR SKIP-DASH)
  - ⑬ EXISTING MEDIAN
- ↑ TRAFFIC FLOW
- DRUMS WITH MONO DIRECTIONAL STEADY BURN LIGHT
- \*\* PLACE AT BEGINNING OF STAGE 2

ILLINOIS DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC  
TYPICAL SECTIONS  
DEVON AVENUE &  
ARLINGTON HEIGHTS ROAD

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



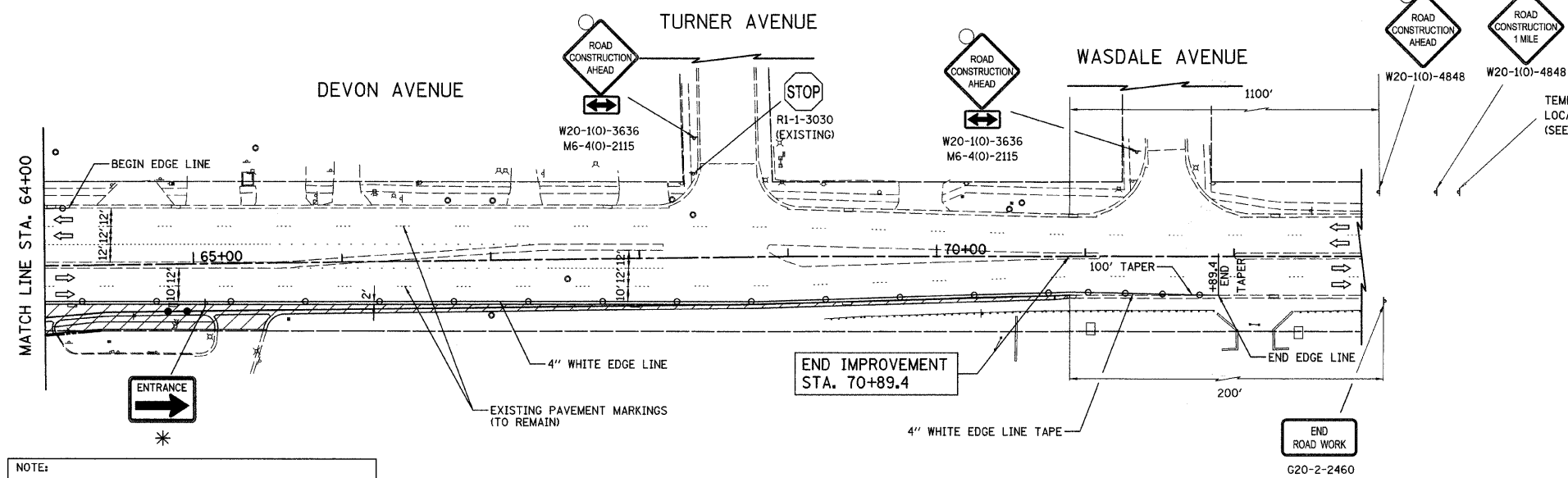


TEMPORARY INFORMATION SIGN-  
LOCATE AS DIRECTED BY THE ENGINEER  
(SEE SHEET 73 FOR DETAILS)



#### LEGEND

- TYPE II DRUM
- ⇒ DIRECTION OF TRAFFIC
- ⊥ TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ▨ WORK ZONE



NOTE:  
AT THE SPEEDWAY GAS STATION AND MOBIL GAS STATION,  
AT LEAST ONE DRIVEWAY ON DEVON AVENUE SHALL REMAIN  
OPEN AT ALL TIMES.

- NOTE:
- ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PAINT UNLESS OTHERWISE NOTED.
  - ALL TAPE PAVEMENT MARKINGS SHALL BE PAID FOR AS "PAVEMENT MARKING TAPE, TYPE III".
  - ADDITIONAL SIGNS WILL BE REQUIRED FOR LANE CLOSURES WHICH ARE NOT SHOWN ON THE PLANS BUT WILL BE REQUIRED AS PER I.D.O.T. STANDARD 701701.
- \* SEE SHEET NO. 74 FOR SIGN DETAILS.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC PLAN**

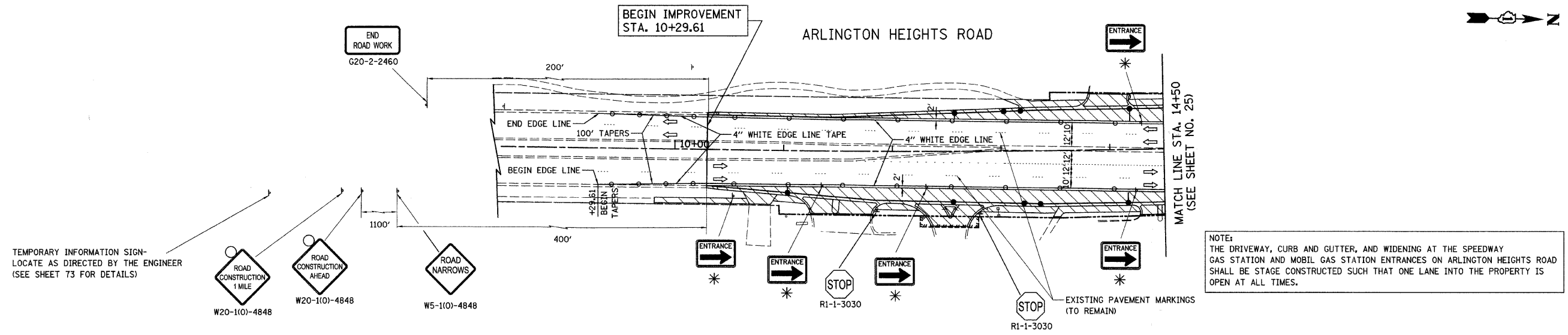
**DEVON AVENUE**

**STAGE 1**

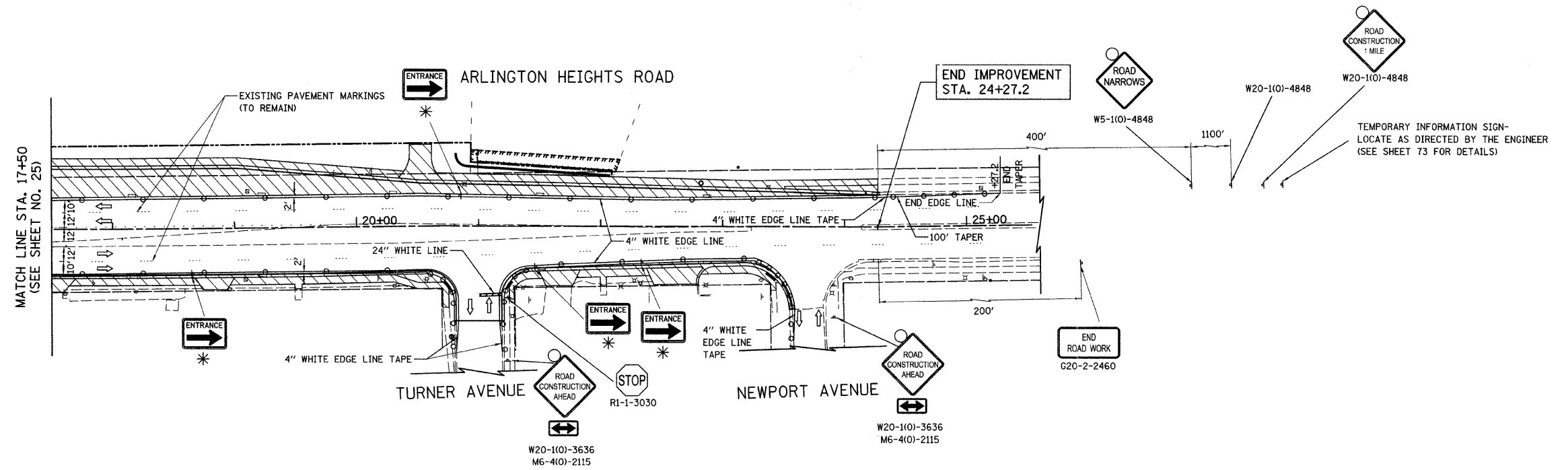
50 0 50  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BLG  
CHECKED BY: DJK

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	105-00050-00-CH	COOK/DUPAGE	94	26
MAINTENANCE OF TRAFFIC - STAGE 1				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



NOTE:  
THE DRIVEWAY, CURB AND GUTTER, AND WIDENING AT THE SPEEDWAY  
GAS STATION AND MOBIL GAS STATION ENTRANCES ON ARLINGTON HEIGHTS ROAD  
SHALL BE STAGE CONSTRUCTED SUCH THAT ONE LANE INTO THE PROPERTY IS  
OPEN AT ALL TIMES.



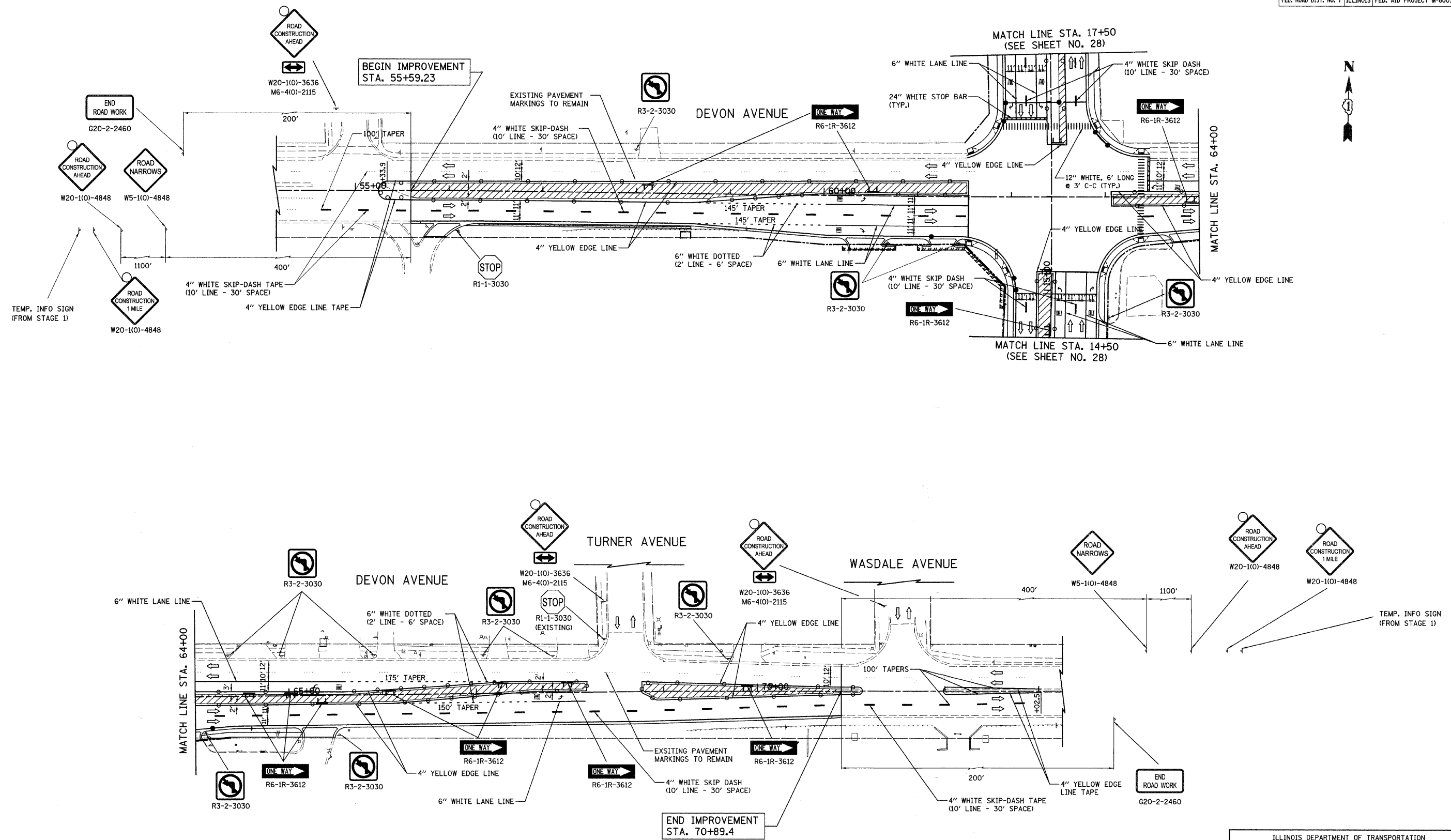
ILLINOIS DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC PLAN  
ARLINGTON HEIGHTS ROAD  
STAGE 1

50 0 50  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BLG  
CHECKED BY: DJK

\* SEE SHEET NO. 74 FOR SIGN DETAILS

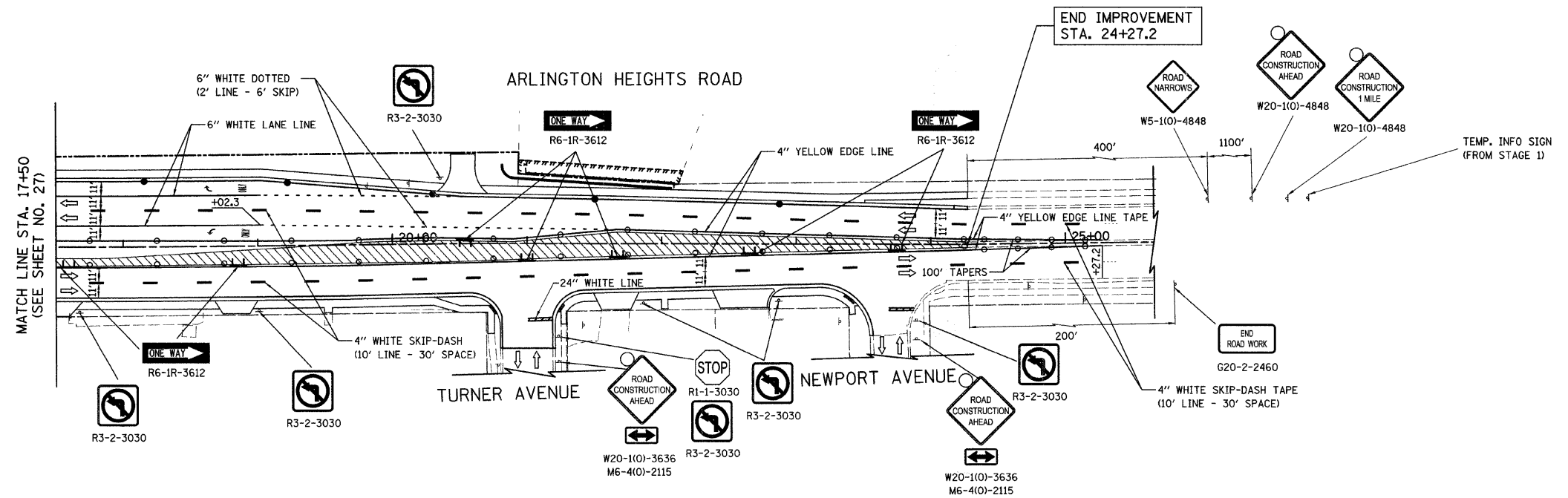
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	27
MAINTENANCE OF TRAFFIC - STAGE 2				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



ILLINOIS DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC PLAN  
DEVON AVENUE  
STAGE 2

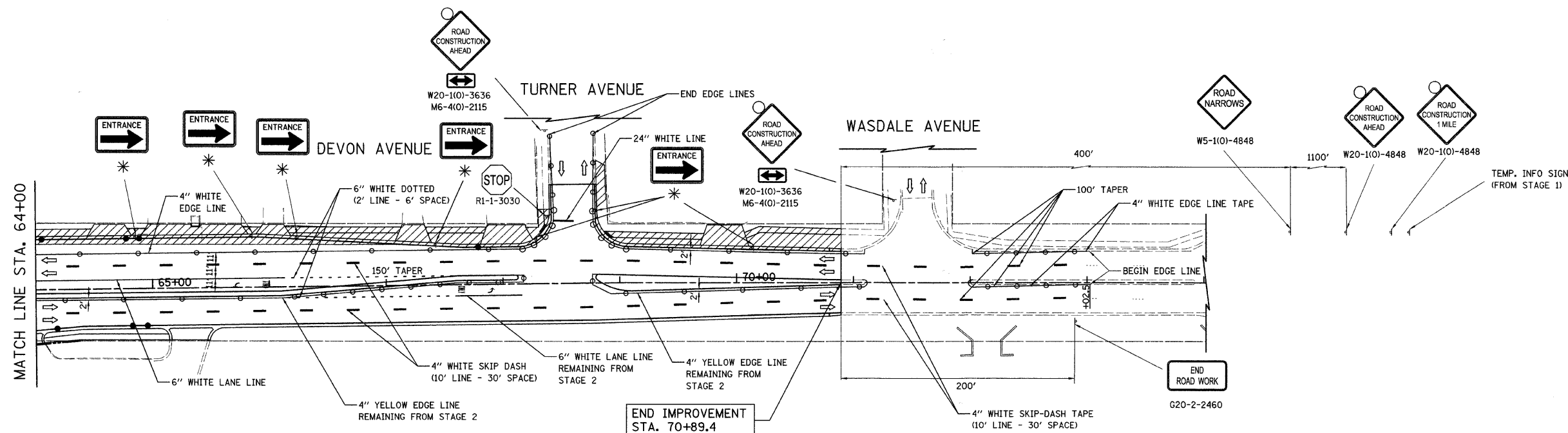
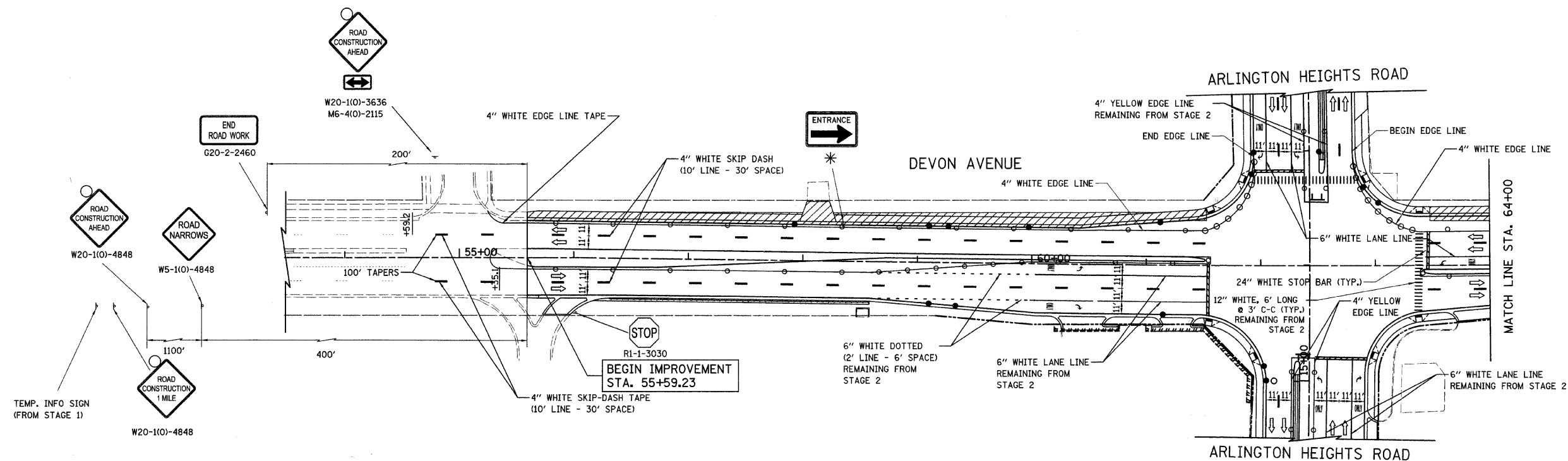
50 0 50  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BLG  
CHECKED BY: DJK



DATE: 5/27/08  
DESIGNED BY: BLG  
CHECKED BY: DJK

CONTRACT NO. 63055				
F.A.U. RTE. 1346 & 2626	SECTION 05-00050-00-CH	COUNTY COOK/DUPAGE	TOTAL SHEETS 94	SHEET NO. 29
MAINTENANCE OF TRAFFIC - STAGE 3				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



- NOTE:
- 1) TRAFFIC STAGING ON ARLINGTON HEIGHTS ROAD REMAINS UNCHANGED FROM STAGE 2.
- \* SEE SHEET NO. 74 FOR SIGN DETAILS

ILLINOIS DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PLAN

DEVON AVENUE

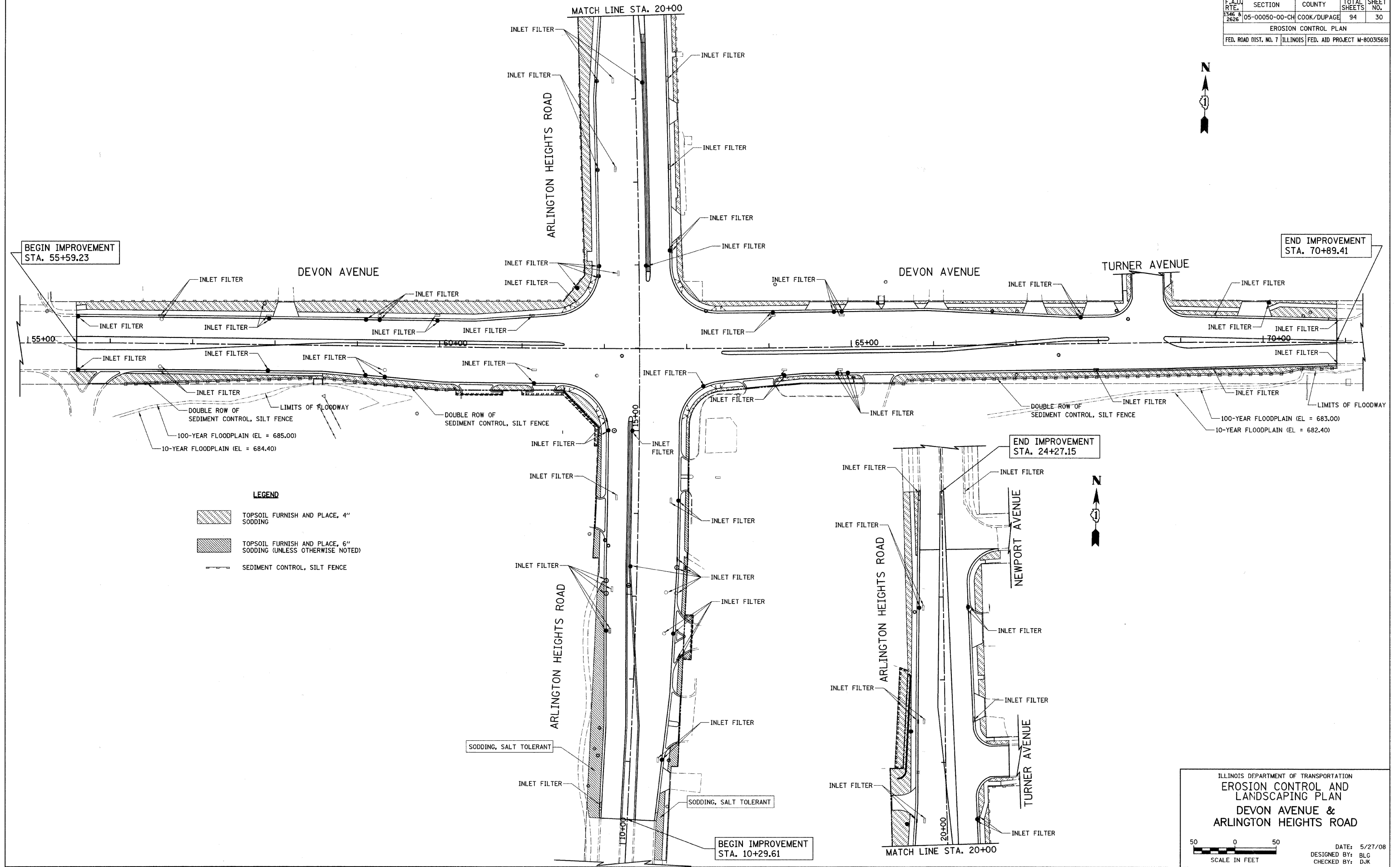
STAGE 3

50 0 50

SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BLG  
CHECKED BY: DJK

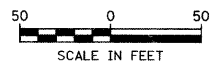
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	30
EROSION CONTROL PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



LEGEND

- TOPSOIL FURNISH AND PLACE, 4\"/>
- TOPSOIL FURNISH AND PLACE, 6\"/>
- SEDIMENT CONTROL, SILT FENCE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
EROSION CONTROL AND  
LANDSCAPING PLAN  
DEVON AVENUE &  
ARLINGTON HEIGHTS ROAD



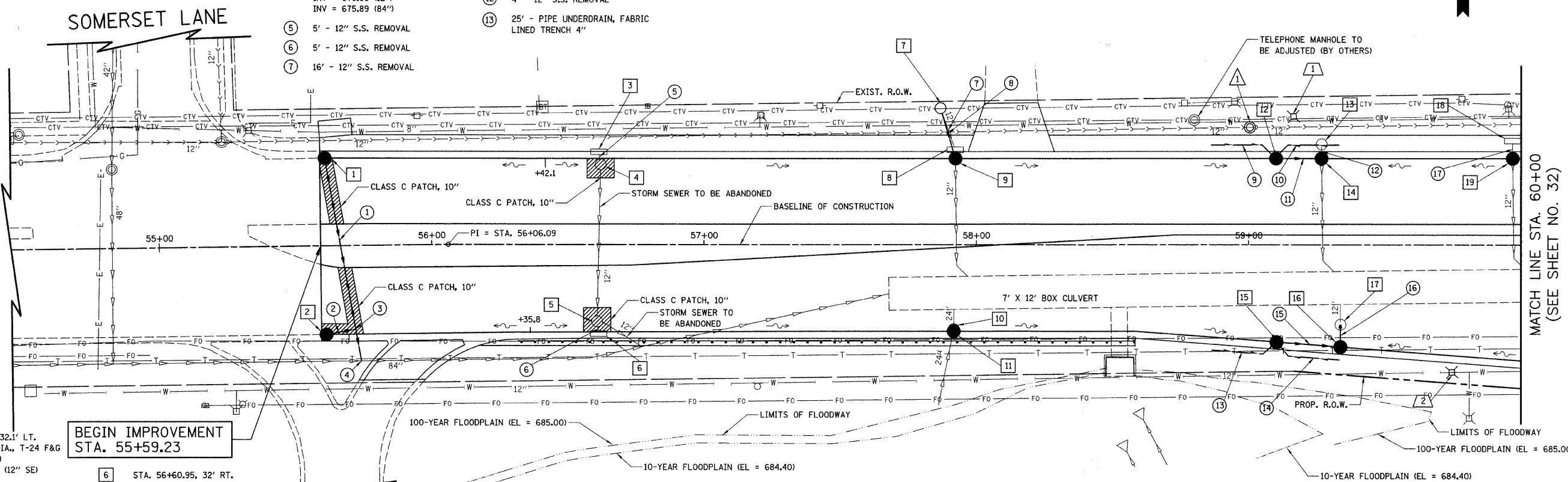
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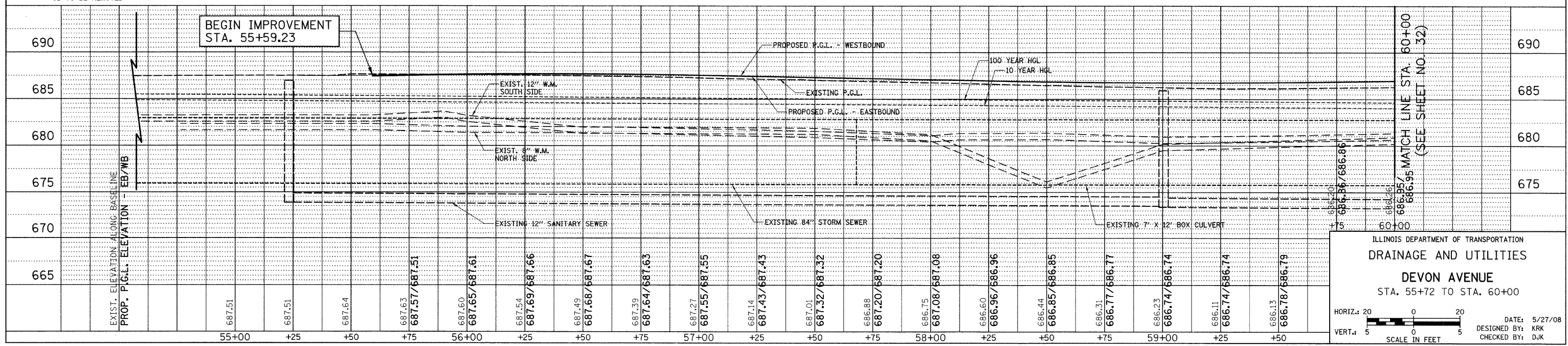
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	31
STA. 55+72		TO STA. 60+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003(569)		



- 1 72' - 12" S.S. CL. A, T-2 @ 2.0% T.B.F. = 47.9 CU. YD.
  - 2 10' - 12" S.S. CL. A, T-2 @ 2.0% T.B.F. = 7.5 CU. YD.
  - 3 12" STORM SEWER TEE INV = 680.06 (12") (INCLUDED IN COST OF PIPE 1)
  - 4 CONNECT TO EXISTING PIPE SEE DETAIL SHEET 62 INV = 679.89 (12") INV = 675.89 (84")
  - 5 5' - 12" S.S. REMOVAL
  - 6 5' - 12" S.S. REMOVAL
  - 7 16' - 12" S.S. REMOVAL
  - 8 19' - 12" S.S. W.M.R., T-1 @ 0.5% T.B.F. = 2.3 CU. YD.
  - 9 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
  - 10 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
  - 11 13' - 12" S.S. CL. A, T-1 @ 0.5% T.B.F. = 2.0 CU. YD.
  - 12 4' - 12" S.S. REMOVAL
  - 13 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
  - 14 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"
  - 15 19' - 12" S.S. CL. A, T-2 @ 0.5% T.B.F. = 3.5 CU. YD.
  - 16 9' - 12" S.S. CL. A, T-2 @ 0.5% T.B.F. = 6.0 CU. YD.
  - 17 6' - 12" S.S. REMOVAL
- 1 STA. 59+00.43, 42.84' LT. SAN. MH TO BE ADJUSTED EX RIM = 685.93 PR RIM = 686.29
- 1 STA. 59+16.45, 46.8' LT. V.V. TO BE ADJUSTED EX RIM = 685.94 PR RIM = 686.00
- 2 STA. 59+74.65, 46.3' RT. V.V. TO BE ADJUSTED EX RIM = 685.71 PR RIM = 686.12



- 1 STA. 55+61, 32.1' LT. CB T-A, 4' DIA., T-24 F&G RIM = 686.99 INV = 681.33 (12" SE)
- 2 STA. 55+61, 32.3' RT. CB T-A, 4' DIA., T-23 F&G RIM = 686.94 INV = 680.13 (12" SE)
- 3 STA. 56+61.85, 34' LT. INLET TO BE REMOVED
- 4 STA. 56+62.40, 29' LT. CB TO BE REMOVED
- 5 STA. 56+60.44, 28' RT. CB TO BE REMOVED
- 6 STA. 56+60.95, 32' RT. INLET TO BE REMOVED
- 7 STA. 57+86.84, 49.9' LT. CB TO BE ADJUSTED EX RIM = 685.95 PR RIM = 686.03 INV = 683.12 (12" SE)
- 8 STA. 57+92.30, 35' LT. INLET TO BE REMOVED
- 9 STA. 57+92.34, 31.5' LT. CB T-A, 4' DIA., T-24 F&G RIM = 686.64 INV = 683.02 (12" N) INV = 681.72 (EX 12" S)
- 10 STA. 57+91.76, 31' RT. CB TO BE REMOVED
- 11 STA. 57+91.75, 31.5' RT. CB T-A, 5' DIA., T-24 F&G RIM = 686.64 INV = 681.12 (EX 24" N) INV = 681.13 (EX 24" S)
- 12 STA. 59+10.00, 31.5' LT. CB, T-A, 4' DIA., T-24 F&G RIM = 686.27 INV = 682.57 (12" E) INV = 682.77 (4" NE & NW)
- 13 STA. 59+26.78, 37' LT. CB TO BE REMOVED
- 14 STA. 59+26.72, 31.5' LT. CB T-A, 4' DIA., T-24 F&G RIM = 686.29 INV = 682.29 (EX 12" S) INV = 682.50 (12" W)
- 15 STA. 59+10.00, 35.4' RT. CB, T-A, 4' DIA., T-24 F&G RIM = 686.19 INV = 682.19 (12" E) INV = 682.69 (4" SE & SW)
- 16 STA. 59+33.73, 37.2' RT. CB T-A, 4' DIA., T-24 F&G RIM = 686.17 INV = 679.91 (12" N) INV = 682.10 (12" W)
- 17 STA. 59+33.79, 29' RT. REMOVING CATCH BASIN TO MAINTAIN FLOW INV = 679.86 (EX 12" N)
- 18 STA. 59+96.91, 38' LT. INLET TO BE REMOVED
- 19 STA. 59+96.93, 31.5' LT. CB T-A, 4' DIA., T-24 F&G RIM = 686.46 INV = 680.49 (EX 12" S)



ILLINOIS DEPARTMENT OF TRANSPORTATION  
DRAINAGE AND UTILITIES  
DEVON AVENUE  
STA. 55+72 TO STA. 60+00

HORIZ. 20  
VERT. 5  
SCALE IN FEET

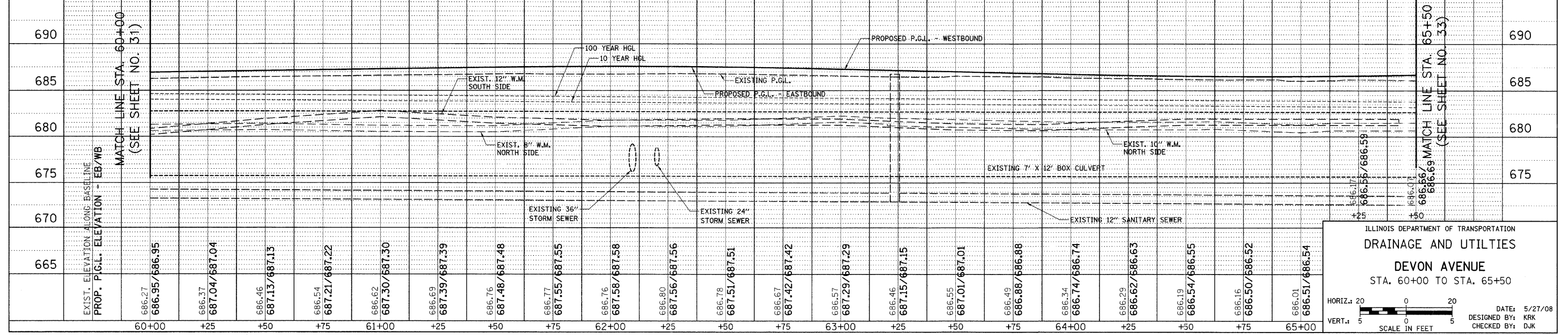
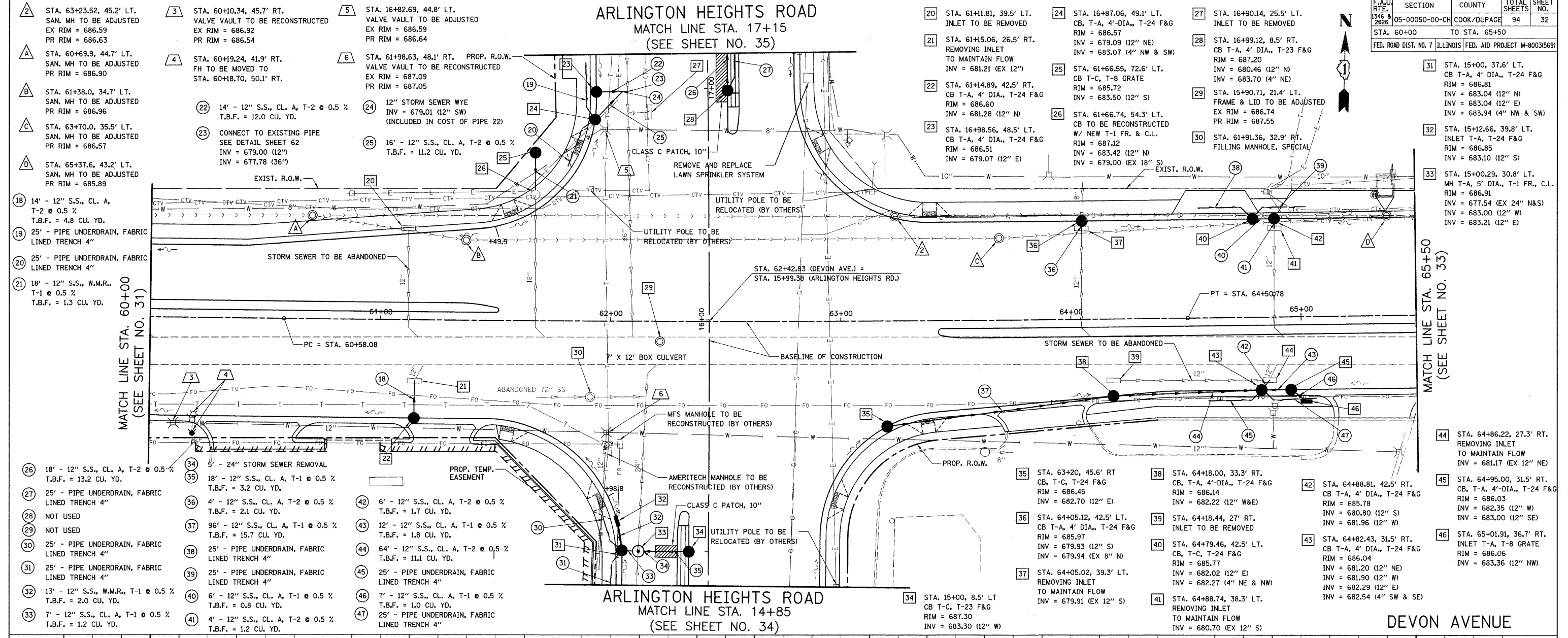
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DESIGNED BY: KRK  
CHECKED BY: DJK

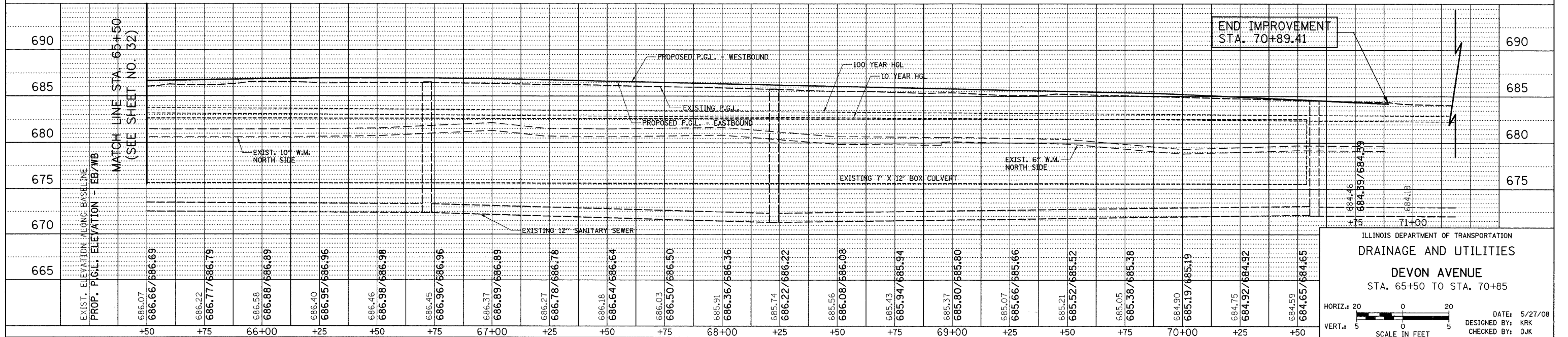
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	32
STA. 60+00 TO STA. 65+50		FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)		

ARLINGTON HEIGHTS ROAD  
MATCH LINE STA. 17+15  
(SEE SHEET NO. 35)

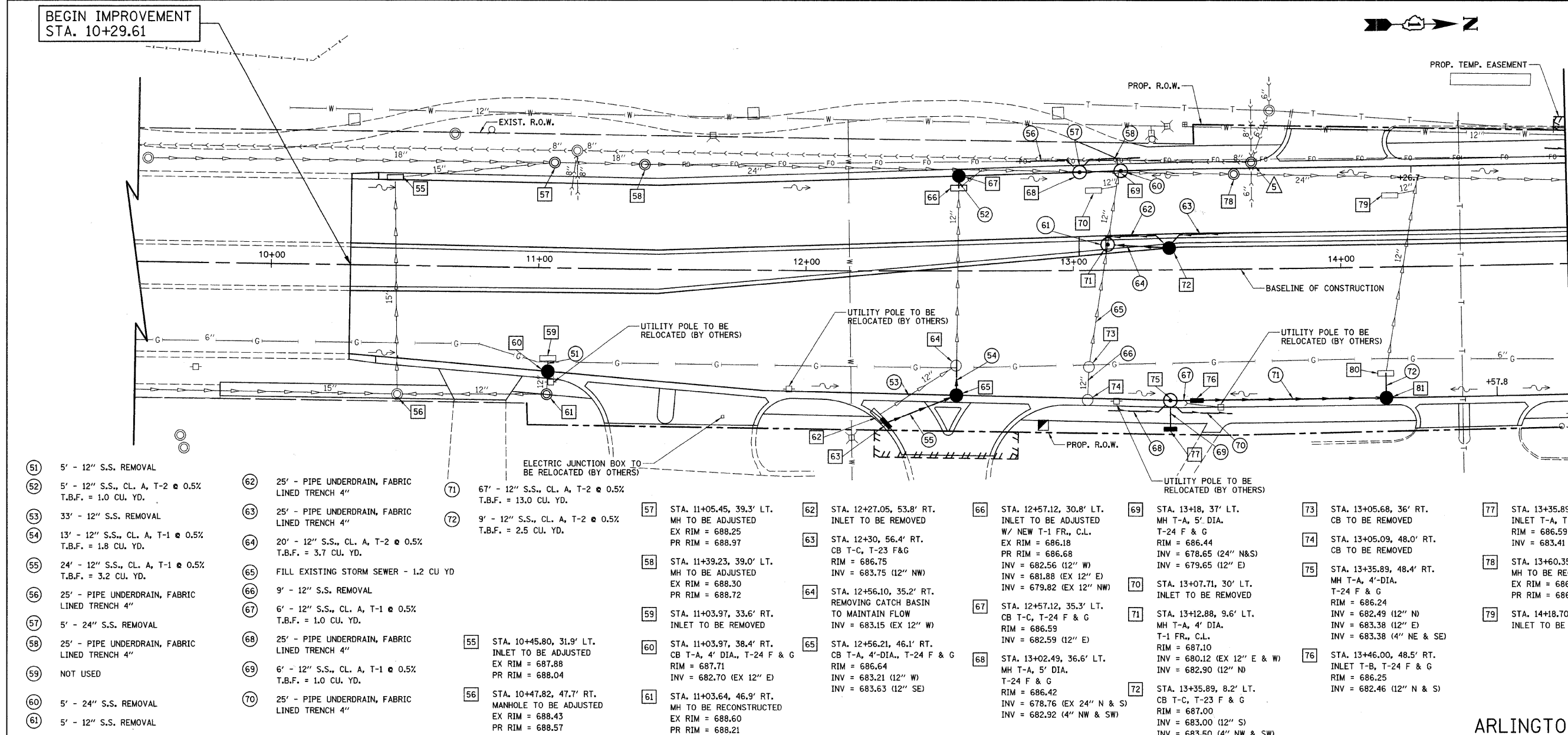
ARLINGTON HEIGHTS ROAD  
MATCH LINE STA. 14+85  
(SEE SHEET NO. 34)

DEVON AVENUE

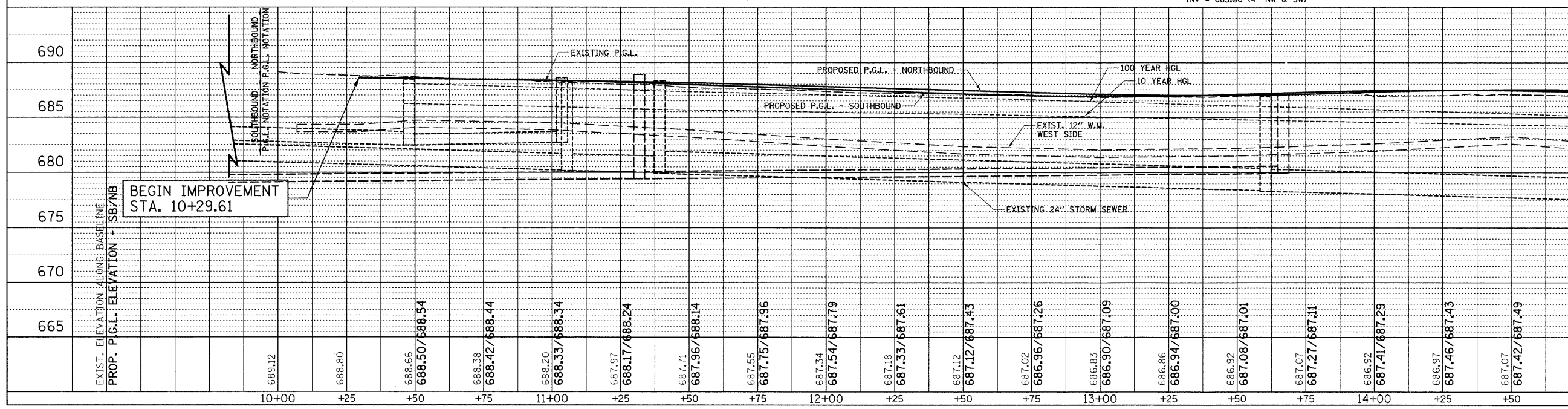




F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	34
STA. 10+29.61		TO STA. 14+85		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003(569)		



ARLINGTON HEIGHTS ROAD



ILLINOIS DEPARTMENT OF TRANSPORTATION  
DRAINAGE AND UTILITIES  
ARLINGTON HEIGHTS ROAD  
STA. 10+29.61 TO STA. 14+90

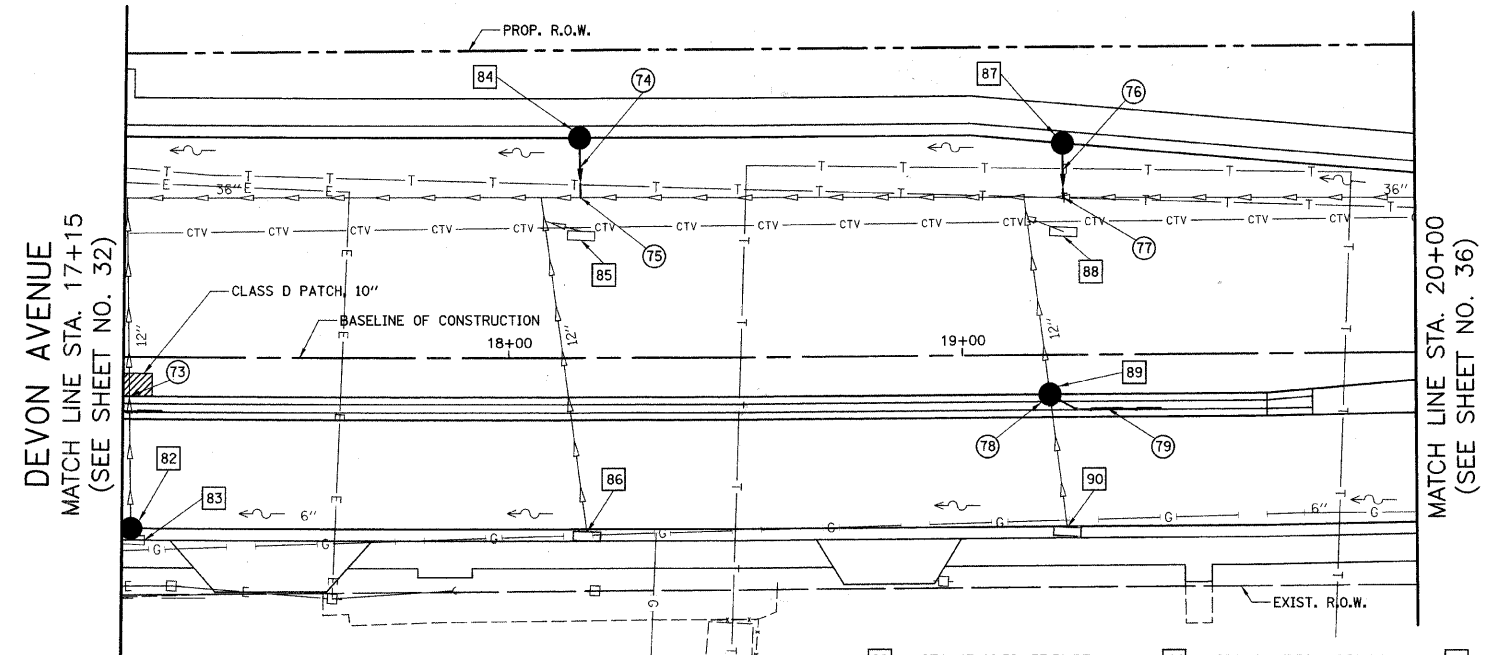
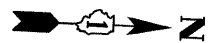
HORIZ.: 20  
VERT.: 5

SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: KRM  
CHECKED BY: DJK



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	35
STA. 17+15		TO STA. 20+00		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003(569)		



- 73 CONNECT TO EXISTING PIPE WITH STORM SEWER TEE  
SEE DETAIL SHEET 62  
INV = 680.37 (12")  
INV = 680.37 (12")

74 13' - 12" S.S. CL. A, T-2 @ 0.5%  
T.B.F. = 6.7 CU. YD.

75 CONNECT TO EXISTING PIPE  
SEE DETAIL SHEET 62  
INV = 681.53 (12")  
INV = 679.53 (36")

76 12' - 12" S.S. CL. A, T-2 @ 0.5%  
T.B.F. = 9.0 CU. YD.

77 CONNECT TO EXISTING PIPE  
SEE DETAIL SHEET 62  
INV = 682.75 (12")  
INV = 681.13 (36")

78 4' - 12" STORM SEWER REMOVAL

79 25' - PIPE UNDERDRAIN, FABRIC LINED TRENCH 4"

82 STA. 17+16.59, 37.5' RT  
CB T-A, 4' DIA., T-24 F&G  
RIM = 686.91  
INV = 682.12 (EX. 12" W)

83 STA. 17+17.08, 40.0' RT  
INLET TO BE REMOVED

84 STA. 18+16.00, 48.5' LT  
CB T-A, 4' DIA., T-24 F&G  
RIM = 687.44  
INV = 681.60 (12" E)

85 STA. 18+16.03, 27.0' LT  
INLET TO BE REMOVED

86 STA. 18+17.30, 37.5' RT  
INLET TO BE ADJUSTED  
W/ NEW T-24 F&G  
EX RIM = 687.47  
PR RIM = 687.83

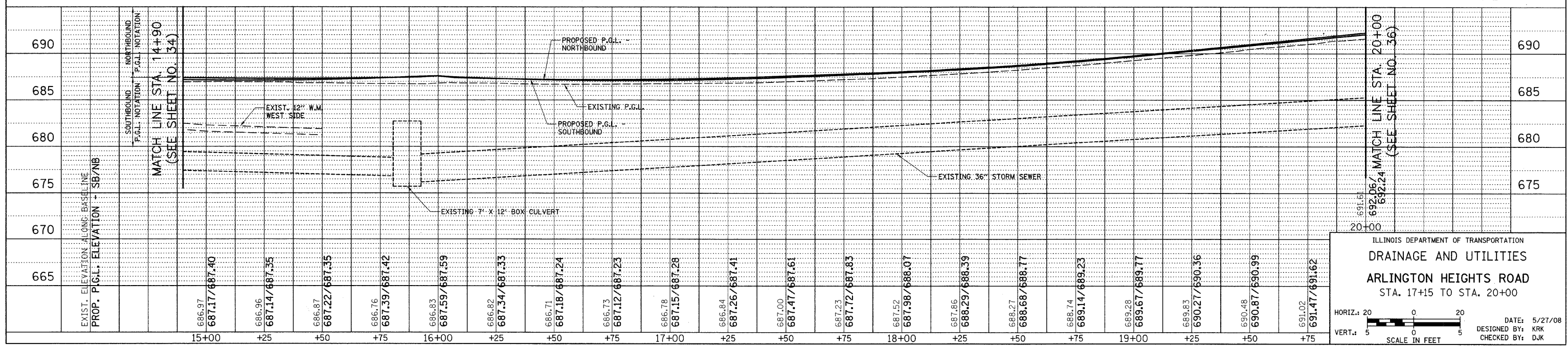
87 STA. 19+22.00, 46.7' LT  
CB T-A, 4' DIA., T-24 F&G  
RIM = 689.50  
INV = 682.81 (12" E)

88 STA. 19+22.54, 27.1' LT  
INLET TO BE REMOVED

89 STA. 19+19.26, 8.5' RT  
CB T-A, 4' DIA., T-23 F&G  
RIM = 690.13  
INV = 682.79 (EX 12" E&W)  
INV = 686.63 (4" NE)

90 STA. 19+22.94, 38.6' RT  
INLET TO BE ADJUSTED  
W/ NEW T-24 F&G  
EX RIM = 689.53  
PR RIM = 689.85

ARLINGTON HEIGHTS ROAD

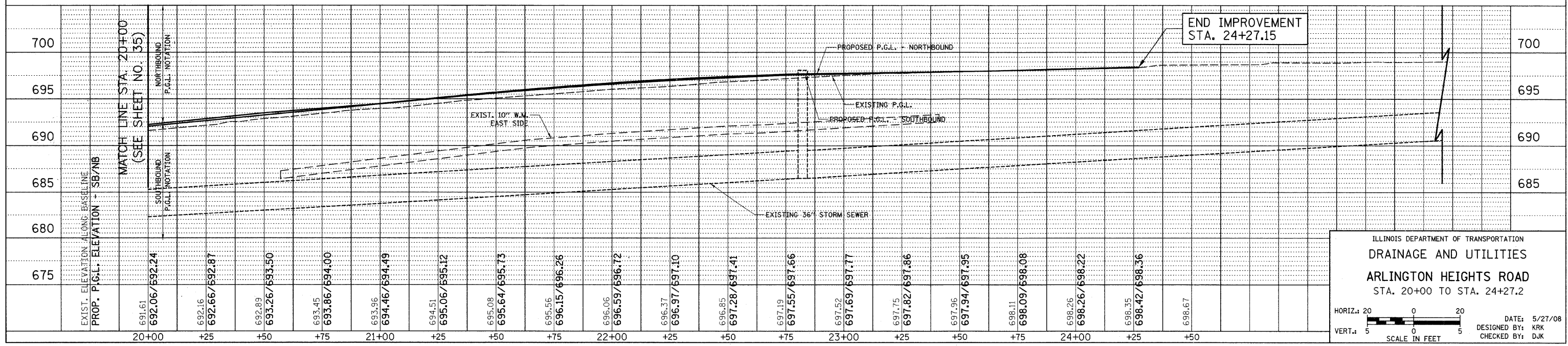
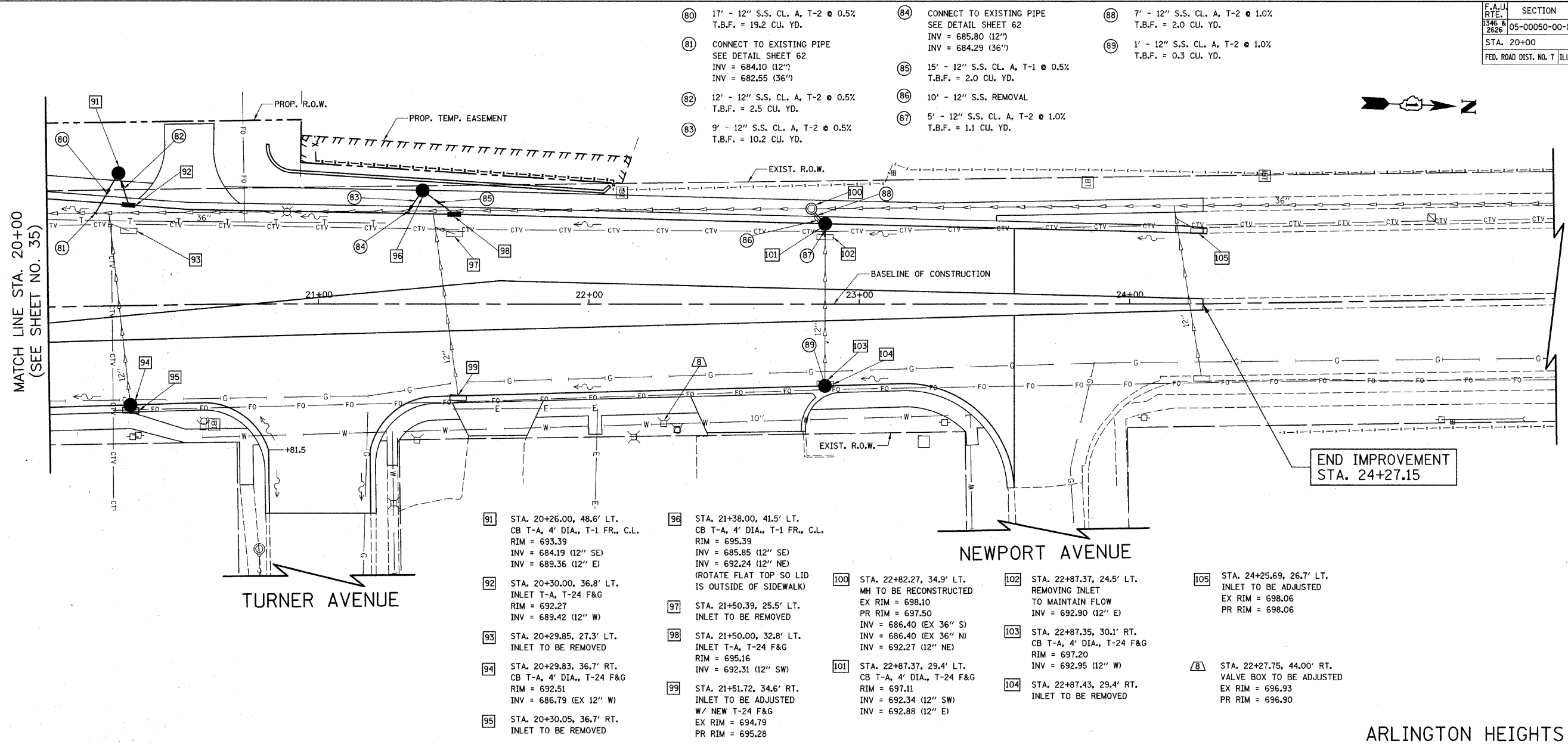


ILLINOIS DEPARTMENT OF TRANSPORTATION  
DRAINAGE AND UTILITIES  
ARLINGTON HEIGHTS ROAD  
STA. 17+15 TO STA. 20+00

HORIZ.: 20  
VERT.: 5  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	36
STA. 20+00		TO STA. 24+27.2		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003(569)		



ILLINOIS DEPARTMENT OF TRANSPORTATION  
DRAINAGE AND UTILITIES  
ARLINGTON HEIGHTS ROAD  
STA. 20+00 TO STA. 24+27.2

HORIZ.: 20  
VERT.: 5

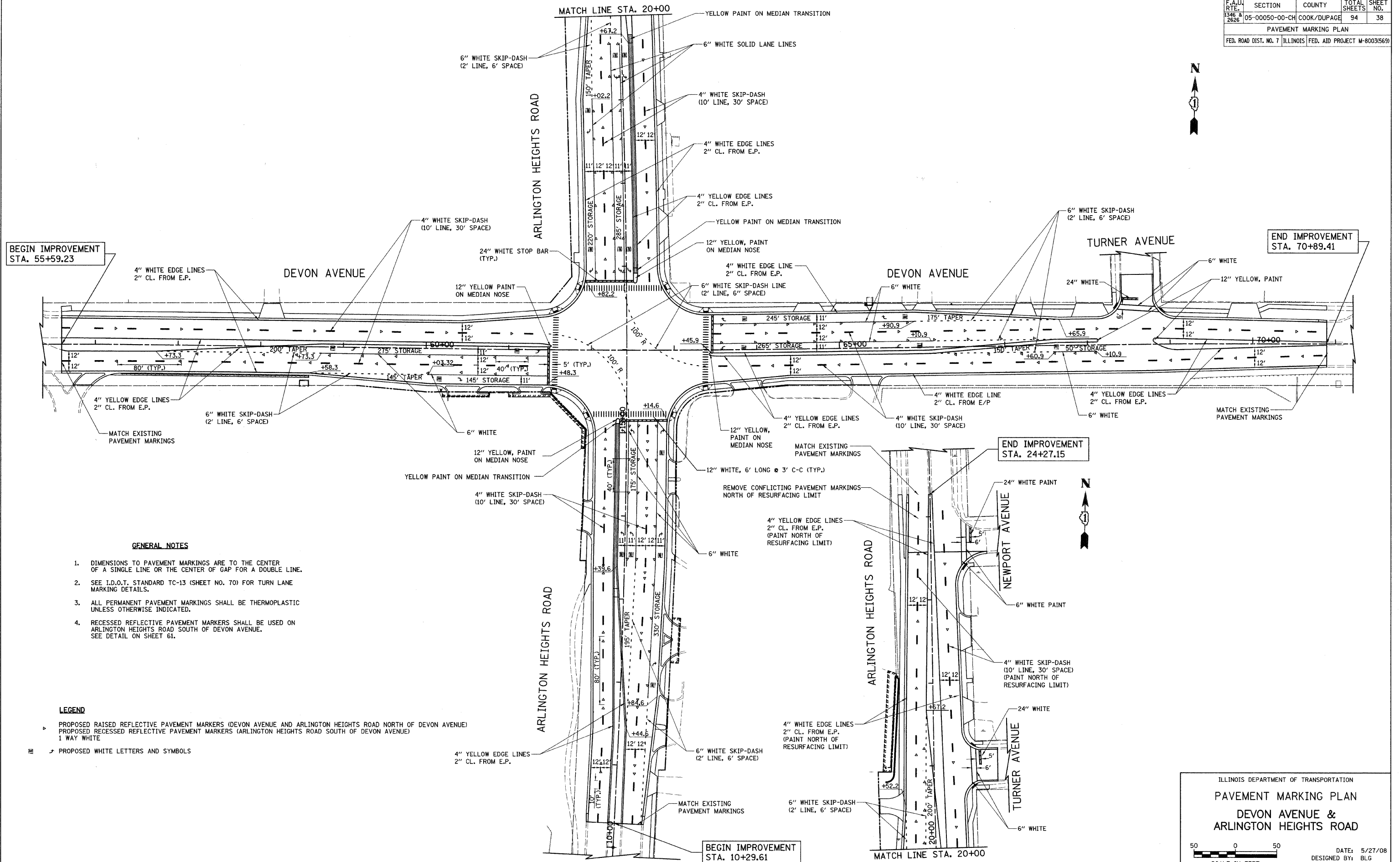
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DATE: 5/27/08  
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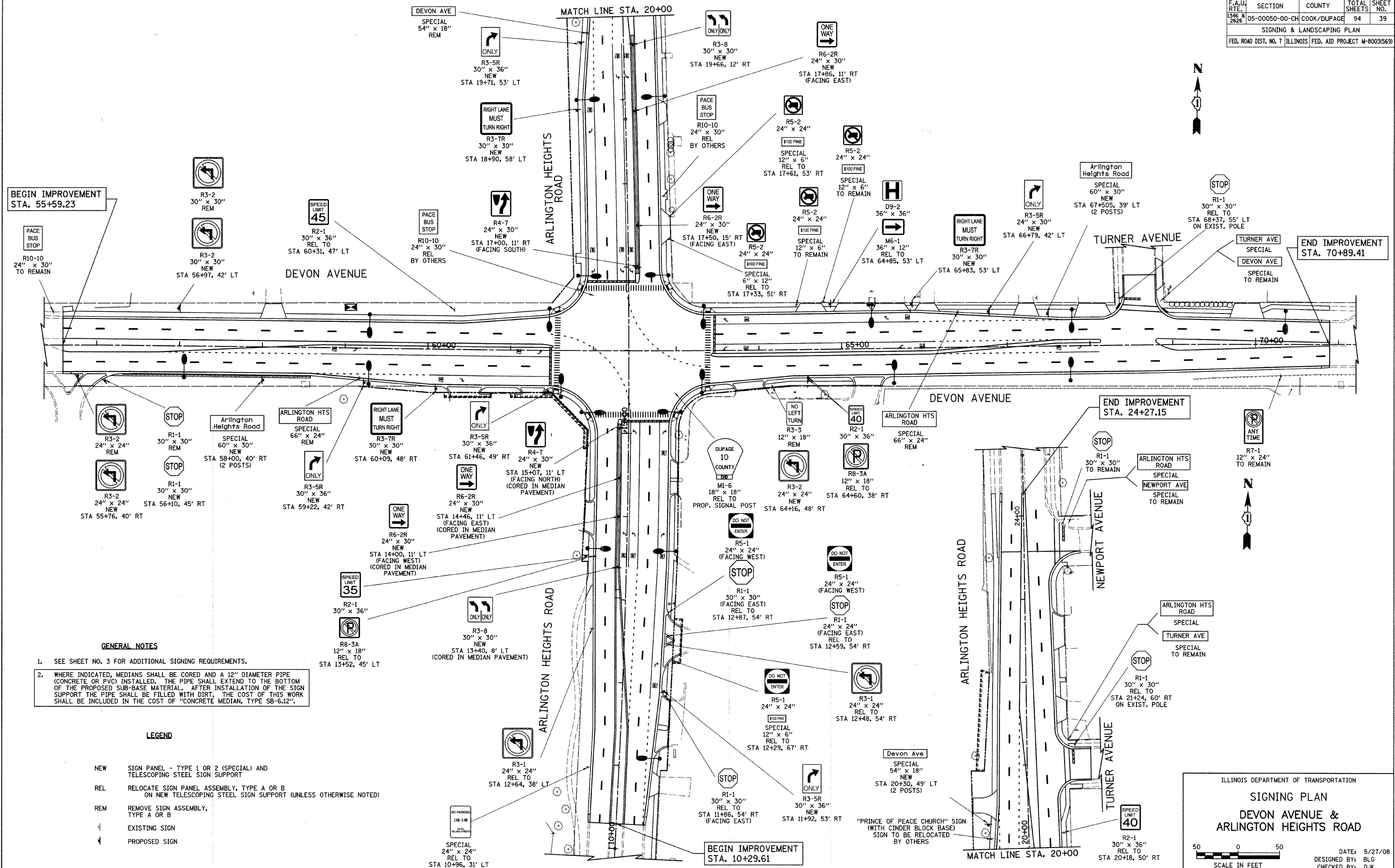




F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	38
PAVEMENT MARKING PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	39
SIGNING & LANDSCAPING PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	40
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT W-8003569				

# NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY CCHD OR IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS, INCLUDING FOLDING STOP SIGNS, SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE, WOOD POLE, OR TEMPORARY POST AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCIDENTAL TO THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- ALL TEMPORARY TRAFFIC SIGNAL SECTIONS NOT IN USE DURING A STAGE OF CONSTRUCTION SHALL BE BAGGED AND DEACTIVATED. ALL PERMANENT TRAFFIC SIGNAL SECTIONS SHALL BE BAGGED OR REMOVED WHILE THE TEMPORARY TRAFFIC SIGNAL IS IN OPERATION.
- THE CONTRACTOR MUST PROVIDE ASSURANCE THAT THE RADIO DEVICE WILL OPERATE PROPERLY AT ALL TIMES AND DURING ALL CONSTRUCTION STAGES. IF WIRELESS INTERCONNECT FAILS DURING TESTING OR OPERATIONS, CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING NECESSARY POLES, FIBER OPTIC CABLE AND OTHER INFRASTRUCTURE FOR PROVIDING TEMPORARY FIBER INTERCONNECT AT NO COST TO THE CONTRACT.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE COOK COUNTY HIGHWAY DEPARTMENT. THE CONTRACTOR SHALL SAFELY STORE AND DELIVER TO THE COUNTY ALL EQUIPMENT TO BE RETURNED TO THE COUNTY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- |   |      |  |
|---|------|--|
| 1 | EACH | CONTROLLER AND CABINET, COMPLETE                   |
| 4 | EACH | SIGNAL HEAD, LED, 1-FACE, 3-SECTION                |
| 4 | EACH | SIGNAL HEAD, LED, 1-FACE, 5-SECTION                |
| 4 | EACH | SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION |
| 2 | EACH | PEDESTRIAN SIGNAL HEAD, LED, 1-FACE                |
| 1 | EACH | PEDESTRIAN SIGNAL HEAD, LED, 2-FACE                |

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE VILLAGE OF ELK GROVE. THE CONTRACTOR SHALL SAFELY STORE AND DELIVER TO THE VILLAGE ALL EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- |   |      |                          |
|---|------|--------------------------|
| 1 | EACH | LIGHT DETECTOR AMPLIFIER |
| 2 | EACH | LIGHT DETECTOR           |

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

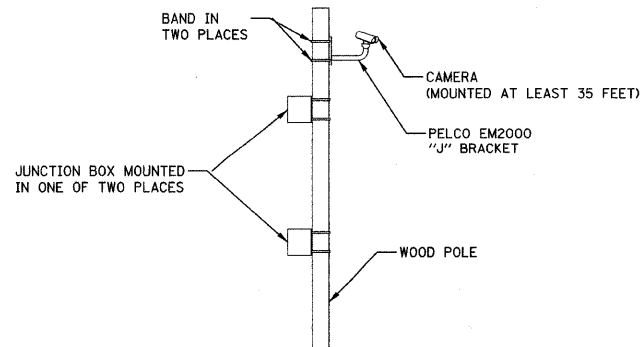
- |   |      |                          |
|---|------|--------------------------|
| 3 | EACH | PEDESTRIAN PUSH-BUTTON   |
| 8 | EACH | TRAFFIC SIGNAL BACKPLATE |
| 4 | EACH | STEEL MAST ARM AND POLE  |
| 4 | EACH | SIGNAL POST              |
| 1 | EACH | SERVICE INSTALLATION     |

TEMPORARY RADIO INTERCONNECT TO  
BIESTERFIELD ROAD/JOHN F. KENNEDY  
BOULEVARD

## TEMPORARY TRAFFIC SIGNAL LEGEND

- TEMPORARY TRAFFIC SIGNAL HEAD  
SPAN WIRE MOUNTED ORIGINAL LOCATION
- TEMPORARY TRAFFIC SIGNAL HEAD  
SPAN WIRE MOUNTED SECONDARY LOCATION
- TEMPORARY WOOD POLE (CLASS 5 OR  
BETTER) 45 FOOT (13.7m) MINIMUM
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND  
CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD,  
BRACKET MOUNTED
- TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR
- VIDEO VEHICLE DETECTOR
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- COMMON TRENCH
- UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- HEAVY-DUTY HANDHOLE
- RADIO ANTENNA

RESTORATION OF WORK AREA.  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



VIDEO DETECTION MOUNTING DETAIL

## EXISTING EQUIPMENT TO BE REMOVED LEGEND

- EXISTING SIGNAL HEAD TO BE REMOVED
- EXISTING SERVICE INSTALLATION TO BE  
REMOVED
- EXISTING SIGNAL POST AND FOUNDATION  
TO BE REMOVED
- EXISTING ALUMINUM MAST ARM POLE  
AND FOUNDATION TO BE REMOVED
- EXISTING CONTROLLER TO BE REMOVED
- EXISTING HANDHOLE TO BE REMOVED
- EXISTING PEDESTRIAN SIGNAL HEAD  
TO BE REMOVED
- EXISTING PEDESTRIAN PUSH BUTTON  
TO BE REMOVED
- EMERGENCY VEHICLE LIGHT DETECTOR TO  
BE REMOVED
- CONFIRMATION BEACON TO BE REMOVED
- EXISTING HEAVY-DUTY HANDHOLE TO BE  
REMOVED
- EXISTING STEEL MAST ARM POLE  
AND FOUNDATION TO BE REMOVED
- EXISTING ILLUMINATED SIGN  
TO BE RELOCATED

TEMPORARY RADIO INTERCONNECT MAY BE USED ON THIS PROJECT AS SHOWN IN THE PLANS. RADIO ANTENNAS SHOULD BE INSTALLED ON THE WOOD POLE ADJACENT TO THE TEMPORARY CONTROLLER AND ON THE SOUTHWEST MAST ARM AT THE INTERSECTION OF ARLINGTON HEIGHTS ROAD AND BEISTERFIELD ROAD/JOHN F. KENNEDY BOULEVARD. ALL WORK RELATED TO INSTALLATION AND ENSURING PROPER OPERATION OF THE TEMPORARY INTERCONNECT SHALL BE INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

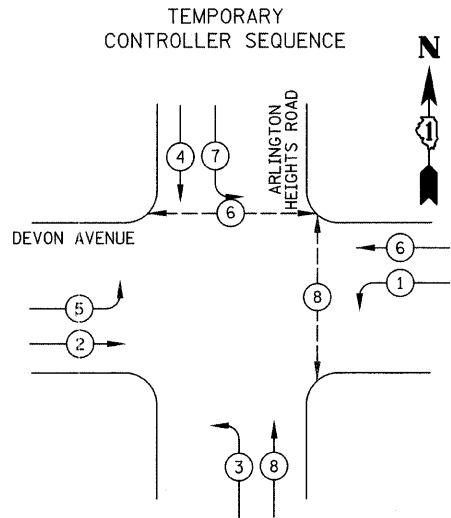
NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT  
FOR THIS PROJECT SHALL BE "ECONOLITE" TO  
MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TEMPORARY TRAFFIC SIGNAL  
INSTALLATION AND REMOVE EXISTING  
TRAFFIC SIGNAL EQUIPMENT

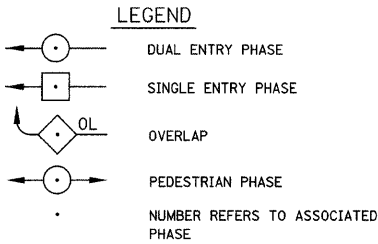
DEVON AVENUE &  
ARLINGTON HEIGHTS ROAD

SCALE IN FEET

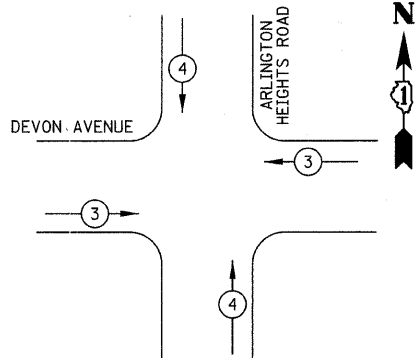
DATE: 5/27/08  
DESIGNED BY: BRD  
CHECKED BY: JJE



TEMPORARY PHASE DESIGNATION DIAGRAM

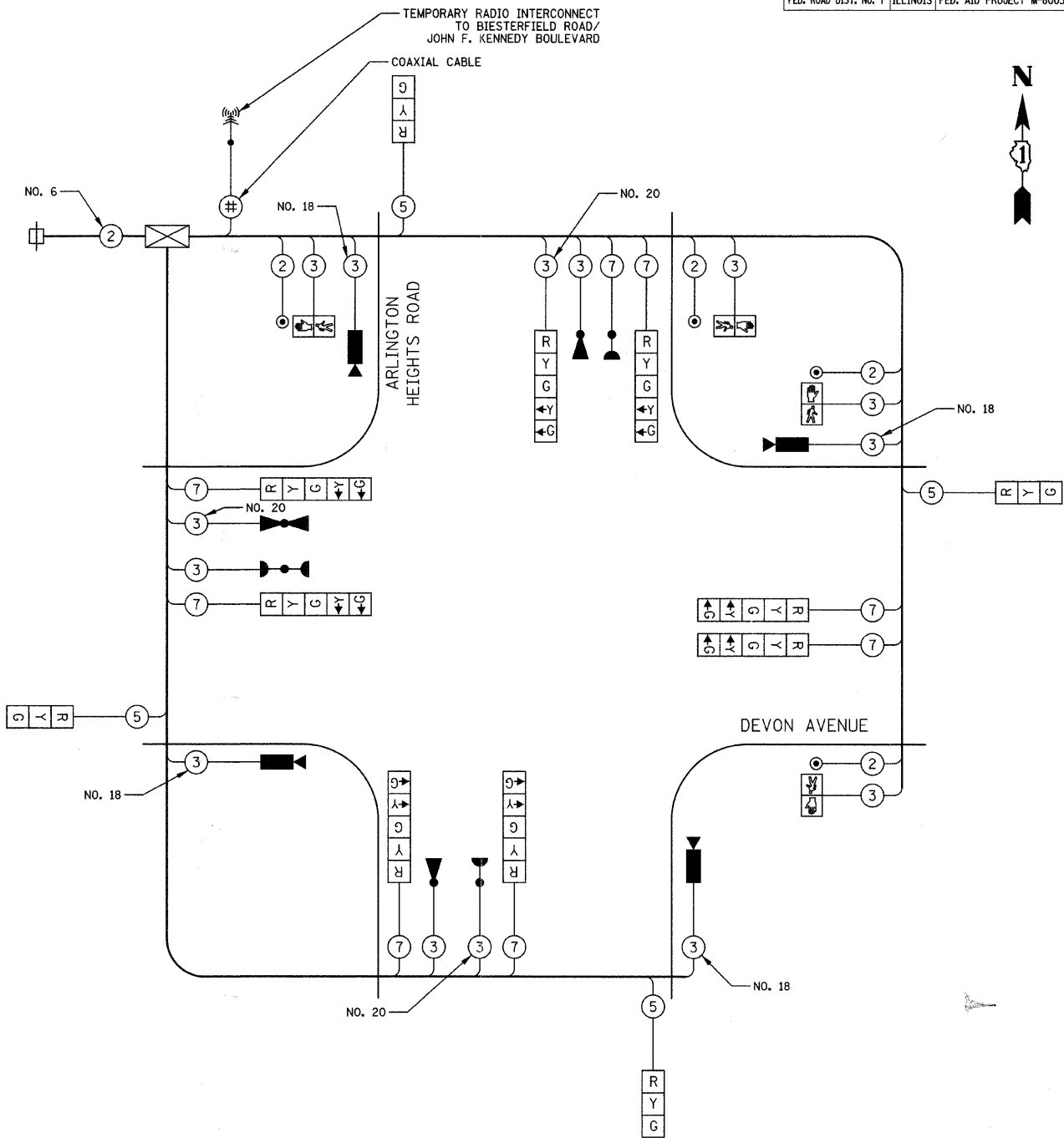
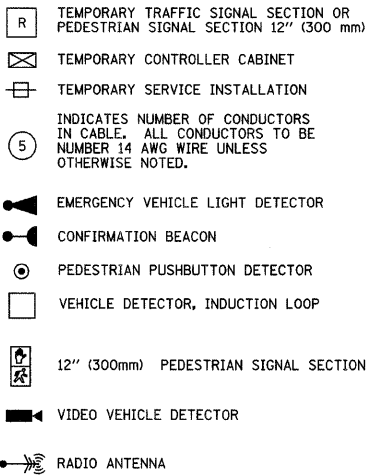


TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

TEMPORARY CABLE PLAN LEGEND



TEMPORARY CABLE PLAN  
NOT TO SCALE

ESTIMATED BILL OF MATERIALS		
PAY ITEM	UNIT	QUANTITY
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT MINIMUM	EACH	5
SIGN PANEL - TYPE I	SQ FT	20
FULL-ACTUATED CONTROLLER AND TEMPORARY CABINET	EACH	1
SPAN WIRE	FOOT	668
TETHER WIRE	FOOT	569
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 2C	FOOT	790
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	1,428
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	2,823
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	1,882
ELECTRIC CABLE AERIAL SUSPENDED, COMMUNICATION, NO. 18 3 PAIR	FOOT	914
ELECTRIC CABLE AERIAL SUSPENDED, SERVICE, NO. 6 2C	FOOT	130
ELECTRIC CABLE AERIAL SUSPENDED NO. 20 3/C, TWISTED, SHIELDED	FOOT	638
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED	EACH	12
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, SPAN WIRE MOUNTED	EACH	8
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED	EACH	1
LIGHT DETECTOR	EACH	3
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	1
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
RADIO ANTENNA	EACH	2
RADIO TRANSCEIVER	EACH	2
MODIFY EXISTING CONTROLLER	EACH	1
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM, LEVEL II	EACH	2
COAXIAL CABLE, AERIAL SUSPENDED	FOOT	51
COAXIAL CABLE IN CONDUIT	FOOT	65

TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	12		17	50	102
(YELLOW)	12		25	25	75
(GREEN)	12		15	25	45
ARROW	16		12	10	19
PED SIGNAL	4		25	100	100
CONTROLLER	1		100	100	100
VIDEO SYSTEMS	1		150	100	150
FLASHER				50	
TOTAL =					591
ENERGY COSTS TO: VILLAGE OF ELK GROVE 901 WELLINGTON AVENUE ELK GROVE VILLAGE, IL 60007					
ENERGY SUPPLY: CONTACT: JOHN STRYZAK PHONE: (630) 691-4363 COMPANY: COM ED					

TEMPORARY RADIO INTERCONNECT MAY BE USED ON THIS PROJECT AS SHOWN IN THE PLANS. RADIO ANTENNAS SHOULD BE INSTALLED ON THE WOOD POLE ADJACENT TO THE TEMPORARY CONTROLLER AND ON THE SOUTHWEST MAST ARM AT THE INTERSECTION OF ARLINGTON HEIGHTS ROAD AND BEISTERFIELD ROAD/JOHN F. KENNEDY BOULEVARD. ALL WORK RELATED TO INSTALLATION AND ENSURING PROPER OPERATION OF THE TEMPORARY INTERCONNECT SHALL BE INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



# TRAFFIC SIGNAL NOTES

1. THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER AT (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
2. THE CONTRACTOR SHALL MARK THE LOCATIONS OF LOOPS AND CONTACT THE CCHD DESIGN ENGINEER AT (312) 603-1730 FOR LOCATION APPROVAL PRIOR TO THE CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE EXISTING TRAFFIC SIGNAL CABLES AND CONDUITS.
4. ALL MAST ARM MOUNTED SIGNAL HEADS ARE TO BE ATTACHED 2'-0" FROM THE END OF MAST ARM UNLESS OTHERWISE NOTED.
5. ALL SIGNAL POSTS SHALL BE SET BACK FOUR (4) FEET MINIMUM AND ALL MAST ARM POLES SHALL BE SET BACK SIX (6) FEET MINIMUM FROM THEIR CENTERLINE TO THE BACK OF CURB UNLESS OTHERWISE NOTED. IN NON-CURBED AREAS, THE MAST ARM POLE AND SIGNAL POST SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER.
6. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR THE LOCATIONS OF THE UTILITIES, CALL J.U.L.I.E. TOLL-FREE AT (800) 892-0123.
7. VEHICLE LOOP DETECTORS SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT.
8. ALL ELECTRICAL CABLE SHALL HAVE POLYVINYL CHLORIDE JACKET.
9. CONDUIT UNDER ROADWAYS AND DRIVEWAYS IS TO BE PLACED IN TRENCH BEFORE PAVEMENT IS PLACED.
10. ALL 1" CONDUIT IS TO BE 1" EMPTY DUCT.
11. THE CORNERS OF ALL NEW DETECTOR LOOPS SHALL BE CORE DRILLED, INCLUDING PAVEMENT JOINTS AND CRACKS.
12. MAST ARM MOUNTED STREET NAME SIGNS AND LEFT TURN REGULATORY SIGNS SHALL BE PAID FOR PER SQUARE FOOT AS "SIGN PANEL - TYPE 1 (SPECIAL)" OR "SIGN PANEL - TYPE 2 (SPECIAL)". FOLDING STOP SIGNS SHALL BE PAID FOR PER SQUARE FOOT AS "SIGN PANEL - TYPE 1 (SPECIAL)".
13. THE TRAFFIC SIGNAL SYSTEM SHALL BE RE-OPTIMIZED BY A COUNTY-APPROVED CONSULTANT.

CONTRACT NO. 63055

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	42
TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

## TRAFFIC SIGNAL LEGEND

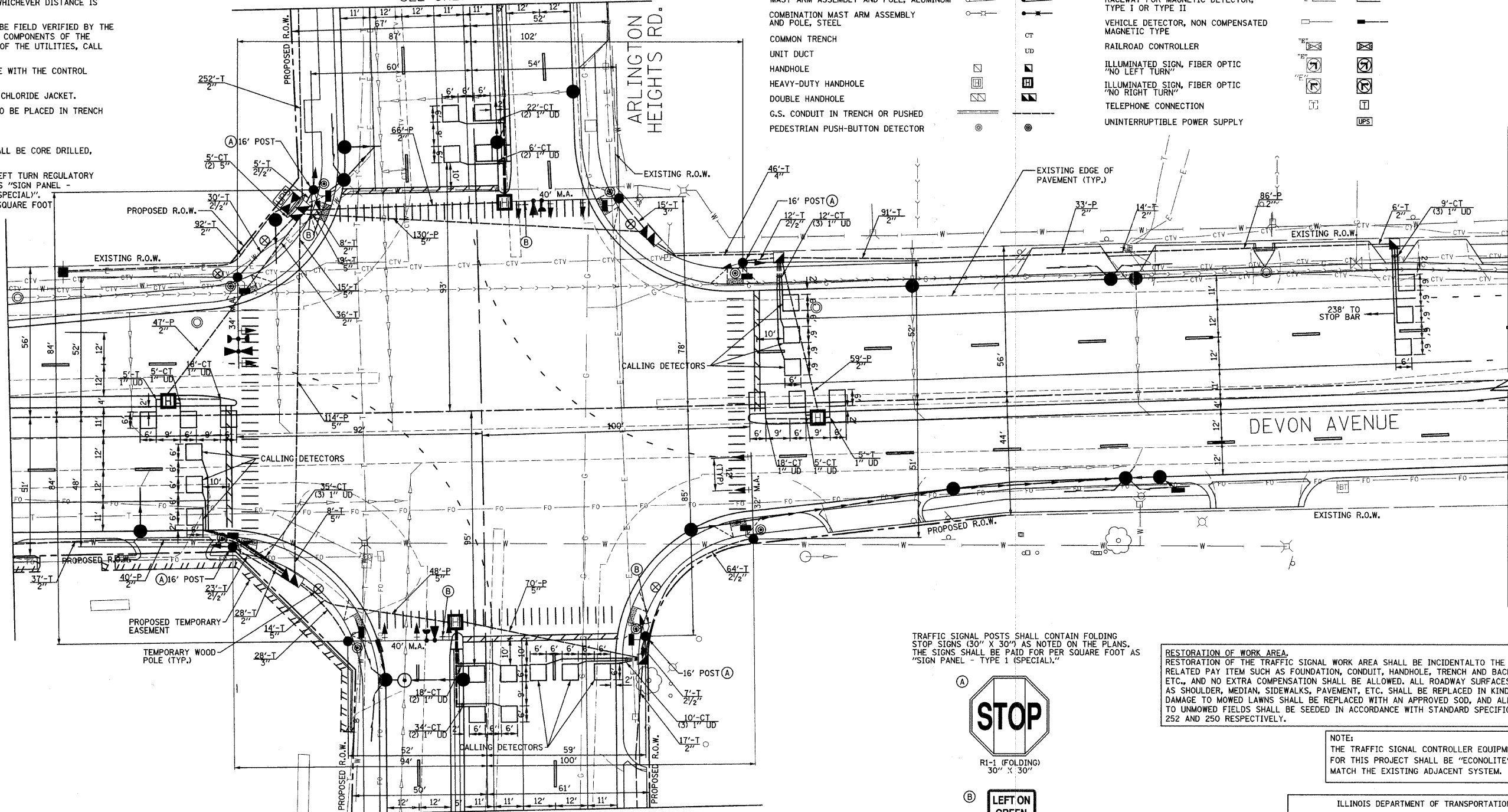
EXISTING PROPOSED

CONTROLLER  
SERVICE INSTALLATION  
SIGNAL HEAD  
SIGNAL HEAD WITH BACKPLATE  
SIGNAL HEAD, PEDESTRIAN  
SIGNAL POST  
MAST ARM ASSEMBLY AND POLE, STEEL  
MAST ARM ASSEMBLY AND POLE, ALUMINUM  
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL  
COMMON TRENCH  
UNIT DUCT  
HANDHOLE  
HEAVY-DUTY HANDHOLE  
DOUBLE HANDHOLE  
G.S. CONDUIT IN TRENCH OR PUSHED  
PEDESTRIAN PUSH-BUTTON DETECTOR

EXISTING PROPOSED

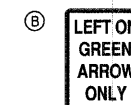
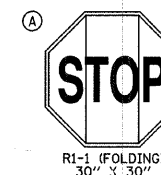
DETECTOR LOOP  
CAST IRON JUNCTION BOX  
EMERGENCY VEHICLE SYSTEM DETECTOR  
CONFIRMATION BEACON  
SIGNAL HEAD PROGRAMMED  
CONDUIT SPLICE  
WOOD POLE  
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II  
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE  
RAILROAD CONTROLLER  
ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"  
ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"  
TELEPHONE CONNECTION  
UNINTERRUPTIBLE POWER SUPPLY

MATCHLINE A  
SEE SHEET 2 OF 2



MATCHLINE B  
SEE SHEET 2 OF 2

TRAFFIC SIGNAL POSTS SHALL CONTAIN FOLDING STOP SIGNS (30" X 30") AS NOTED ON THE PLANS. THE SIGNS SHALL BE PAID FOR PER SQUARE FOOT AS "SIGN PANEL - TYPE 1 (SPECIAL)".



R 10-5  
24" X 30"  
(TYP.)

RESTORATION OF WORK AREA.  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC SIGNAL  
INSTALLATION PLAN  
DEVON AVENUE AND  
ARLINGTON HEIGHTS ROAD  
SHEET 1 OF 2

20 0 20  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BRD  
CHECKED BY: JJE



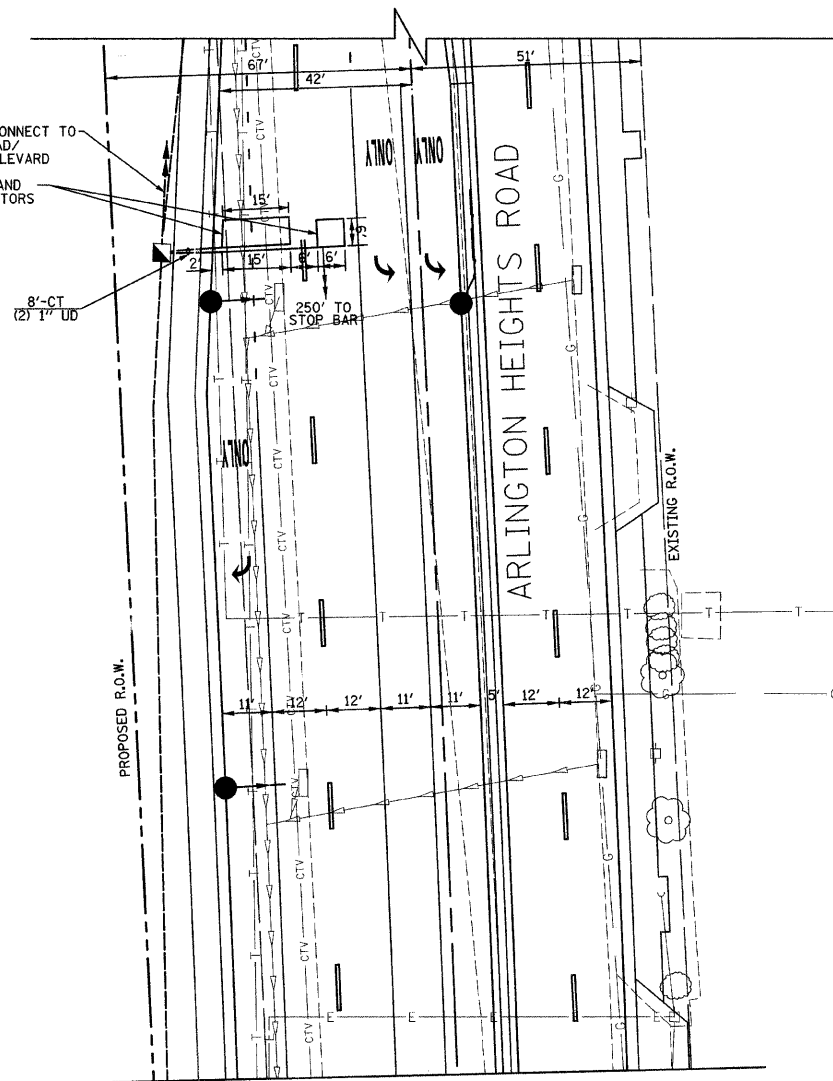
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	43
TRAFFIC SIGNAL INSTALLATION PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

## TRAFFIC SIGNAL NOTES

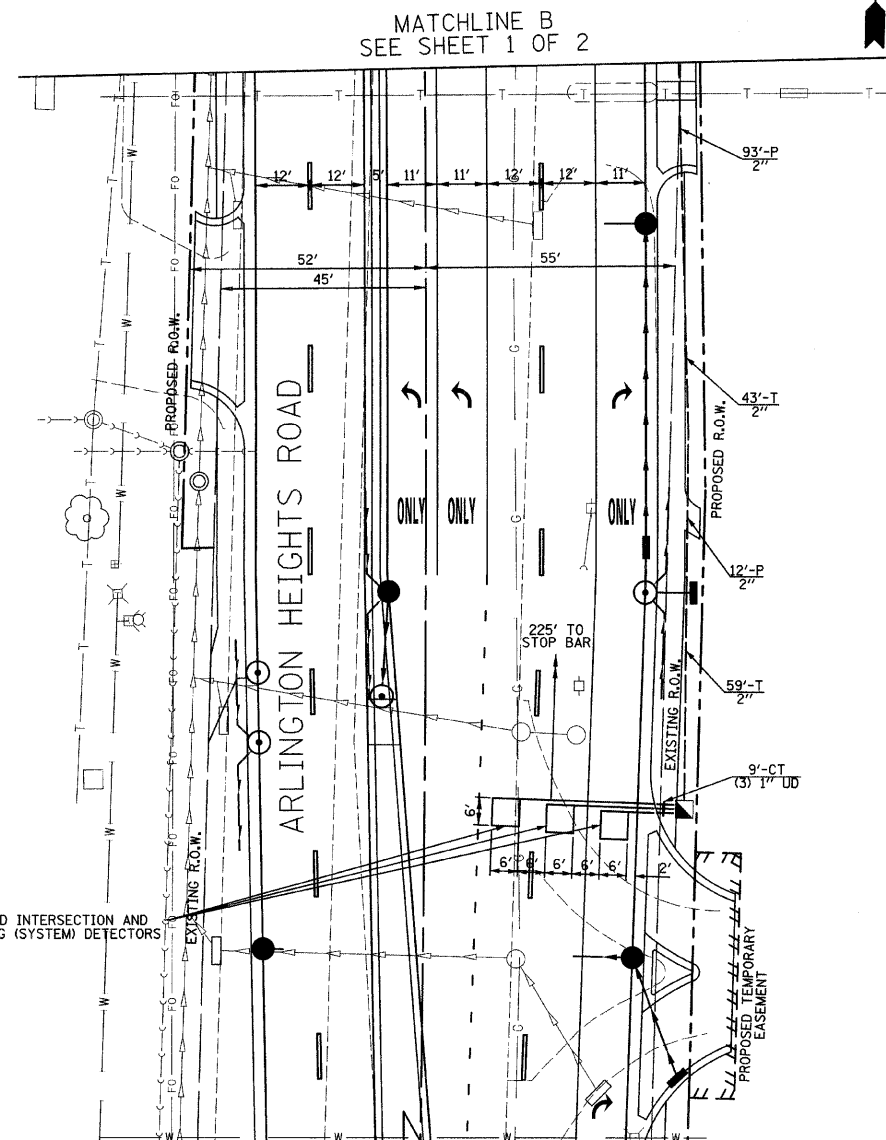
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7. VEHICLE LOOP DETECTORS SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT.
8. ALL ELECTRICAL CABLE SHALL HAVE POLYVINYL CHLORIDE JACKET.
9. CONDUIT UNDER ROADWAYS AND DRIVEWAYS IS TO BE PLACED IN TRENCH BEFORE PAVEMENT IS PLACED.
10. ALL 1" CONDUIT IS TO BE 1" EMPTY DUCT.
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13. THE TRAFFIC SIGNAL SYSTEM SHALL BE RE-OPTIMIZED BY A COUNTY-APPROVED CONSULTANT.

PROPOSED INTERCONNECT TO  
BIESTERFIELD ROAD/  
J.F. KENNEDY BOULEVARD

PROPOSED INTERSECTION AND  
SAMPLING (SYSTEM) DETECTORS



MATCHLINE C  
SEE SHEET 1 OF 2



## TRAFFIC SIGNAL LEGEND

CONTROLLER  
SERVICE INSTALLATION  
SIGNAL HEAD  
SIGNAL HEAD WITH BACKPLATE  
SIGNAL HEAD, PEDESTRIAN  
SIGNAL POST  
MAST ARM ASSEMBLY AND POLE, STEEL  
MAST ARM ASSEMBLY AND POLE, ALUMINUM  
COMBINATION MAST ARM ASSEMBLY  
AND POLE, STEEL  
COMMON TRENCH  
UNIT DUCT  
HANDHOLE  
HEAVY-DUTY HANDHOLE  
DOUBLE HANDHOLE  
G.S. CONDUIT IN TRENCH OR PUSHED  
PEDESTRIAN PUSH-BUTTON DETECTOR

EXISTING PROPOSED

DETECTOR LOOP  
CAST IRON JUNCTION BOX  
EMERGENCY VEHICLE SYSTEM DETECTOR  
CONFIRMATION BEACON  
SIGNAL HEAD PROGRAMMED  
CONDUIT SPLICE  
WOOD POLE  
RACEWAY FOR MAGNETIC DETECTOR,  
TYPE I OR TYPE II  
VEHICLE DETECTOR, NON COMPENSATED  
MAGNETIC TYPE  
RAILROAD CONTROLLER  
ILLUMINATED SIGN, FIBER OPTIC  
"NO LEFT TURN"  
ILLUMINATED SIGN, FIBER OPTIC  
"NO RIGHT TURN"  
TELEPHONE CONNECTION  
UNINTERRUPTIBLE POWER SUPPLY

EXISTING PROPOSED

RESTORATION OF WORK AREA.  
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AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL  
DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE  
TO UNMOWED FIELDS SHALL BE SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS  
252 AND 250 RESPECTIVELY.

NOTE:  
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MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC SIGNAL  
INSTALLATION PLAN  
DEVON AVENUE AND  
ARLINGTON HEIGHTS ROAD  
SHEET 2 OF 2

20 0 20  
SCALE IN FEET

DATE: 5/27/08  
DESIGNED BY: BRD  
CHECKED BY: JJE

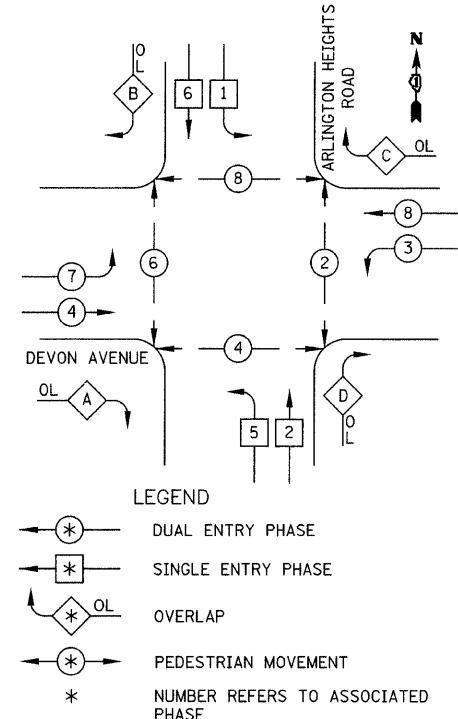
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	44
CABLE PLAN AND SEQUENCE OF OPERATIONS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

# CABLE PLAN LEGEND

EXISTING PROPOSED

- 8" (200mm) TRAFFIC SIGNAL SECTION
- 12" (300mm) TRAFFIC SIGNAL SECTION
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- CONTROLLER CABINET
- SERVICE INSTALLATION
- TELEPHONE CONNECTION
- MAGNETIC DETECTOR
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- PUSHBUTTON DETECTOR
- VEHICLE DETECTOR, INDUCTION LOOP
- DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
- MICROWAVE VEHICLE SENSOR
- SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD
- RAILROAD CONTROL CABINET
- ILLUMINATED SIGN "NO LEFT TURN"
- ILLUMINATED SIGN "NO RIGHT TURN"
- GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C).
- GROUND ROD AT POST (P), OR MAST ARM POLE (MA).
- GROUND ROD AT ELECTRIC SERVICE INSTALLATION
- GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)
- FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F
- UNINTERRUPTIBLE POWER SUPPLY

## PROPOSED CONTROLLER SEQUENCE

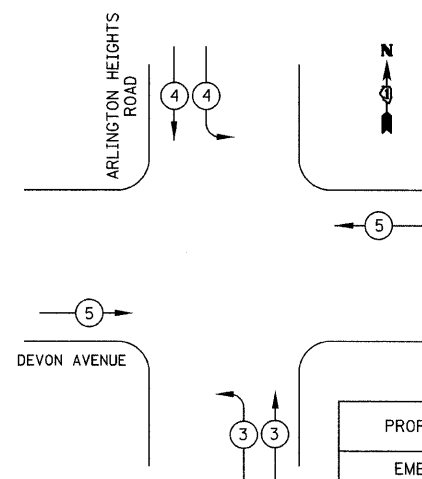


## PHASE DESIGNATION DIAGRAM

RIGHT TURN OVERLAP PHASE DESIGNATION

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	2 +	3
B	4 +	5
C	6 +	7
D	8 +	1

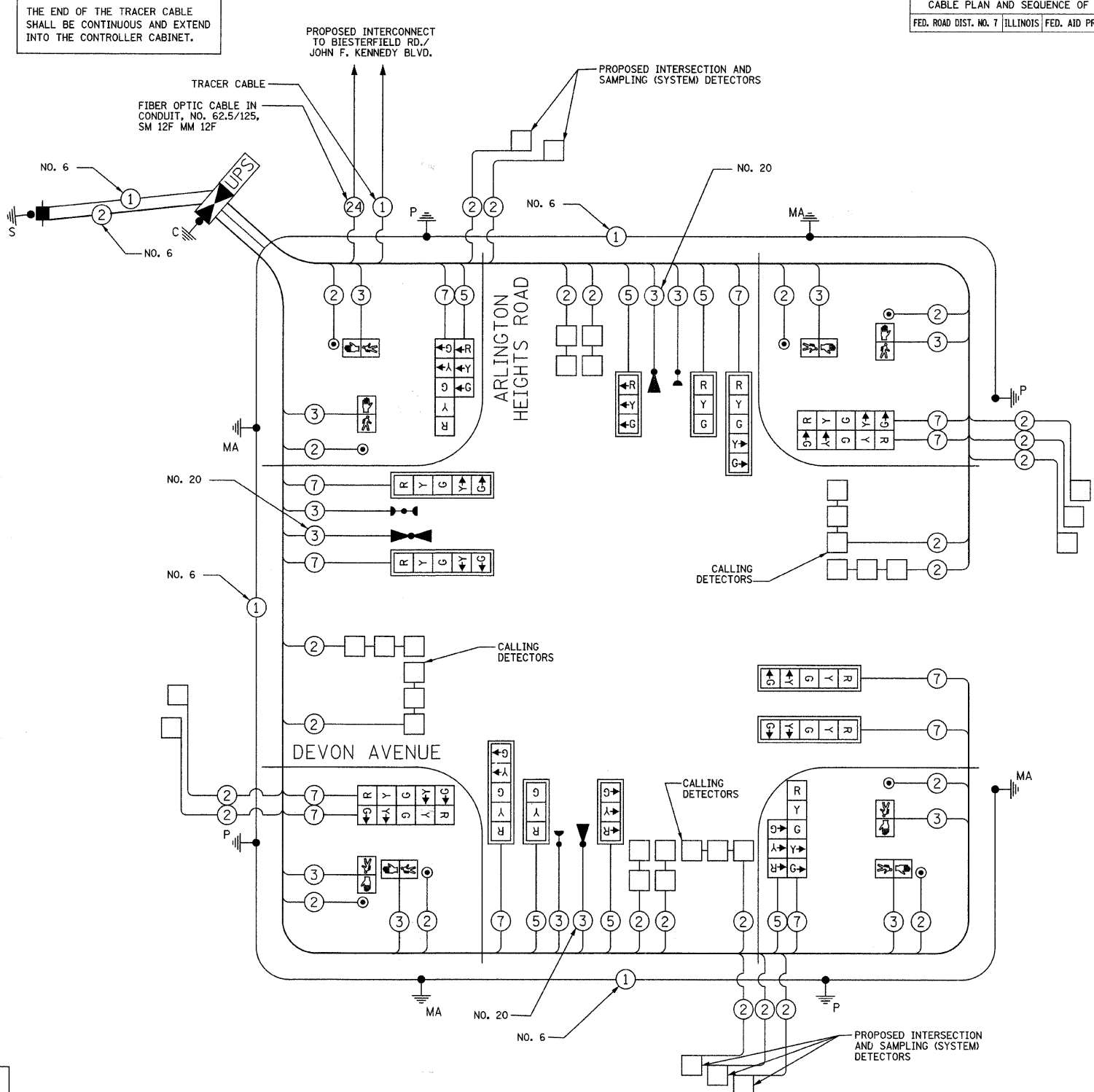
## PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	←	→	→

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (2.0)
D- CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-H-2=
E- M. ARM POLE		SIGNAL POST	2 (1.0)	6m-H-0.6m=	
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
36" (900mm)	15 (4.6)	ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.



CABLE PLAN NOT TO SCALE

NOTE: THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	14		17	50	119
(YELLOW)	14		25	25	88
(GREEN)	14		15	25	53
ARROW	16		12	10	19
PED SIGNAL	8		25	100	200
CONTROLLER	1		100	100	100
FLASHER				50	
TOTAL =					579

ENERGY COSTS TO: VILLAGE OF ELK GROVE  
901 WELLINGTON AVENUE  
ELK GROVE VILLAGE, IL 60007  
CONTACT: JOHN STRYZAK  
PHONE: (630) 691-4363  
COMPANY: COM ED

ILLINOIS DEPARTMENT OF TRANSPORTATION  
CABLE PLAN AND SEQUENCE OF OPERATIONS  
DEVON AVENUE AND ARLINGTON HEIGHTS ROAD  
SCALE : NONE  
DATE: 5/27/08  
DESIGNED BY: JSH  
CHECKED BY: JJE

CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	45
TRAFFIC SIGNAL SCHEDULE OF QUANTITIES				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-8003(569)		

SCHEDULE OF QUANTITIES		
PAY ITEM	UNIT	QUANTITY
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	815
CONDUIT IN TRENCH, 2½" DIA., GALVANIZED STEEL	FOOT	141
CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	43
CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	46
CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL	FOOT	55
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	485
CONDUIT PUSHED, 5" DIA., GALVANIZED STEEL	FOOT	362
HANDHOLE	EACH	6
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	3
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1,095
FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL	EACH	1
TRANSCIEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	EACH	1,676
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,353
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,452
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	2,925
ELECTRIC CABLE IN CONDUIT, LEAD-IN NO. 14 1 PAIR	FOOT	6,333
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	111
TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	EACH	4
STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	30
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	30
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4
SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	6
SIGNAL HEAD, L.E.D., 2-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, L.E.D., 1-FACE, BRACKET MOUNTED	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	19
DETECTOR LOOP, TYPE I	FOOT	1,137
LIGHT DETECTOR	EACH	3
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	13
REMOVE EXISTING CONCRETE FOUNDATION	EACH	9
SIGN PANEL - TYPE 1 (SPECIAL)	SQ FT	73
SIGN PANEL - TYPE 2 (SPECIAL)	SQ FT	25
SERVICE INSTALLATION, POLE MOUNT	EACH	1
UNINTERRUPTIBLE POWER SUPPLY	EACH	1
ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	837
ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED	FOOT	620
TEMPORARY TRAFFIC SIGNAL TIMINGS	EACH	1

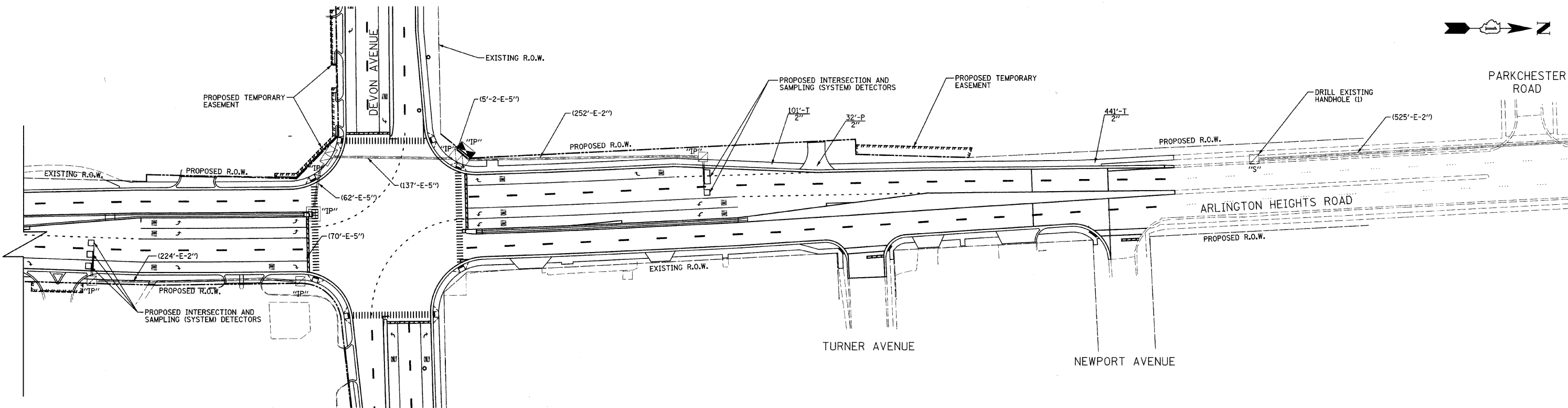
NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT  
FOR THIS PROJECT SHALL BE "ECONOLITE" TO  
MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL  
SCHEDULE OF QUANTITIES

DEVON AVENUE AND  
ARLINGTON HEIGHTS ROAD

SCALE : NONE  
DATE: 5/27/08  
DESIGNED BY: JSH  
CHECKED BY: JJE

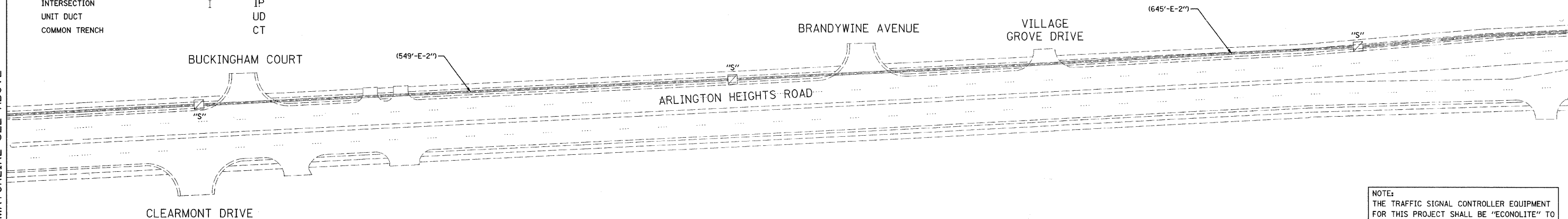


MATCHLINE SEE BELOW

INTERCONNECT PLAN LEGEND

	EXISTING	PROPOSED
CONTROLLER		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
SYSTEM	S	S
INTERSECTION	I	IP
UNIT DUCT		UD
COMMON TRENCH		CT

MATCHLINE SEE ABOVE

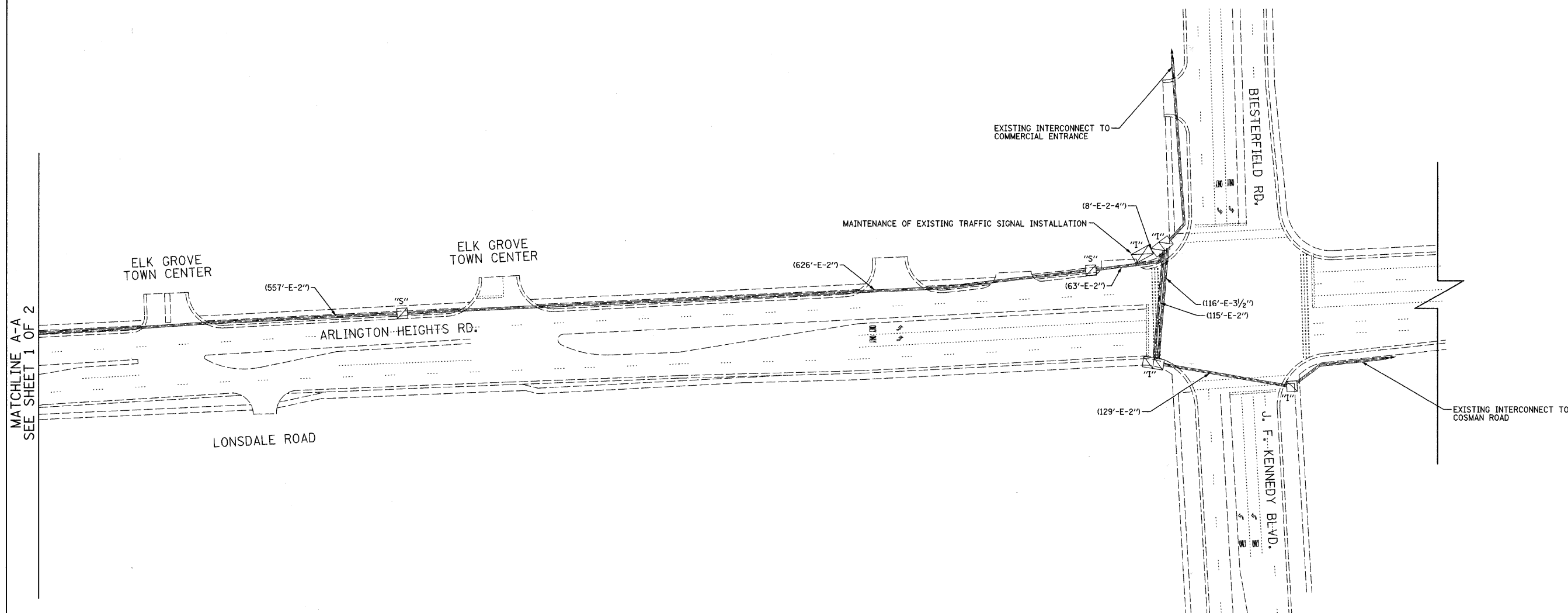


MATCHLINE A-A SEE SHEET 2 OF 2

NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA.  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	47
INTERCONNECT PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				



MATCHLINE A-A  
SEE SHEET 1 OF 2

# INTERCONNECT PLAN LEGEND

	EXISTING	PROPOSED
CONTROLLER		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
SYSTEM	S	S
INTERSECTION	I	IP
UNIT DUCT		UD
COMMON TRENCH		CT

NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT  
FOR THIS PROJECT SHALL BE "ECONOLITE" TO  
MATCH THE EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA.  
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE  
RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL,  
ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH  
AS SHOULDER, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL  
DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE  
TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS  
252 AND 250 RESPECTIVELY.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**INTERCONNECT PLAN**  
ARLINGTON HEIGHTS ROAD  
DEVON AVENUE TO BIESTERFIELD ROAD/  
J.F. KENNEDY BOULEVARD  
SHHET 2 OF 2

50 0 50  
SCALE IN FEET

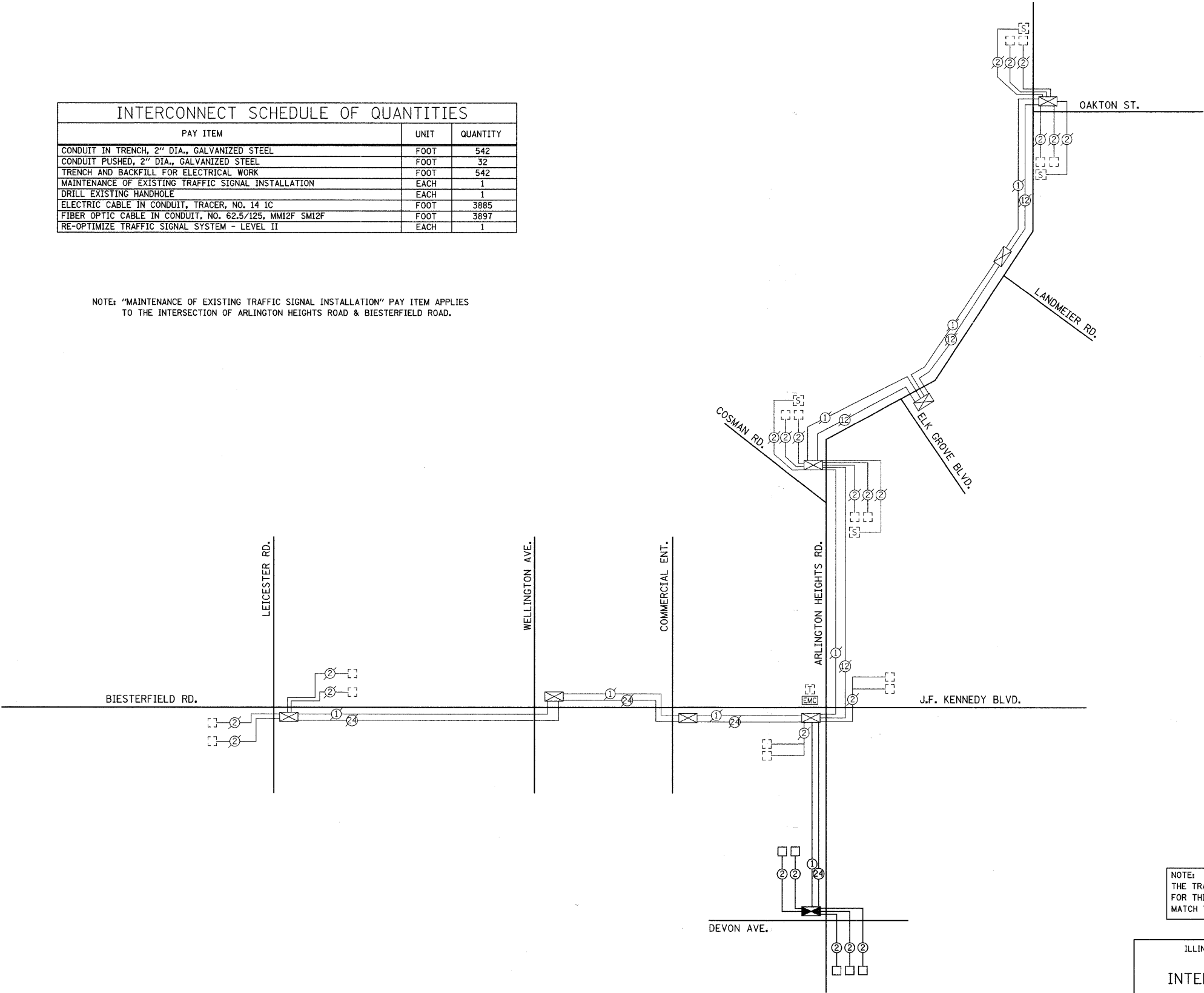
DATE: 5/27/08  
DESIGNED BY: BRO  
CHECKED BY: JJE

INTERCONNECT SCHEMATIC LEGEND

EXISTING INTERSECTION CONTROLLER	
PROPOSED INTERSECTION CONTROLLER	
EXISTING MASTER CONTROLLER	
PROPOSED MASTER CONTROLLER	
EXISTING MASTER MASTER CONTROLLER	
EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS	
PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS	
EXISTING INTERSECTION DETECTORS	
PROPOSED SAMPLING (SYSTEM) DETECTORS	
EXISTING SAMPLING (SYSTEM) DETECTORS	
PROPOSED SAMPLING (SYSTEM) DETECTORS	
EXISTING SAMPLING (SYSTEM) DETECTORS	
PROPOSED SAMPLING (SYSTEM) DETECTORS	
EXISTING SAMPLING (SYSTEM) DETECTORS	
PROPOSED SAMPLING (SYSTEM) DETECTORS	
EXISTING PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS	
PROPOSED PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS	
EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS	
PROPOSED SAMPLING (SYSTEM) PREFORMED DETECTORS	
EXISTING FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	
PROPOSED FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	
EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE	
PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE	
EXISTING INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED	
PROPOSED INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED	
EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED	
PROPOSED LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED	
EXISTING ELECTRIC CABLE 1/C (AS SPECIFIED)	
PROPOSED ELECTRIC CABLE 1/C (AS SPECIFIED)	
EXISTING TELEPHONE CONNECTION	
PROPOSED TELEPHONE CONNECTION	
EXISTING SPEED DETECTOR	

INTERCONNECT SCHEDULE OF QUANTITIES		
PAY ITEM	UNIT	QUANTITY
CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	542
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	32
TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	542
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
DRILL EXISTING HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	3885
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F	FOOT	3897
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM - LEVEL II	EACH	1

NOTE: "MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION" PAY ITEM APPLIES TO THE INTERSECTION OF ARLINGTON HEIGHTS ROAD & BIESTERFIELD ROAD.



NOTE:  
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT  
FOR THIS PROJECT SHALL BE "ECONOLITE" TO  
MATCH THE EXISTING ADJACENT SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION

INTERCONNECT SCHEMATIC

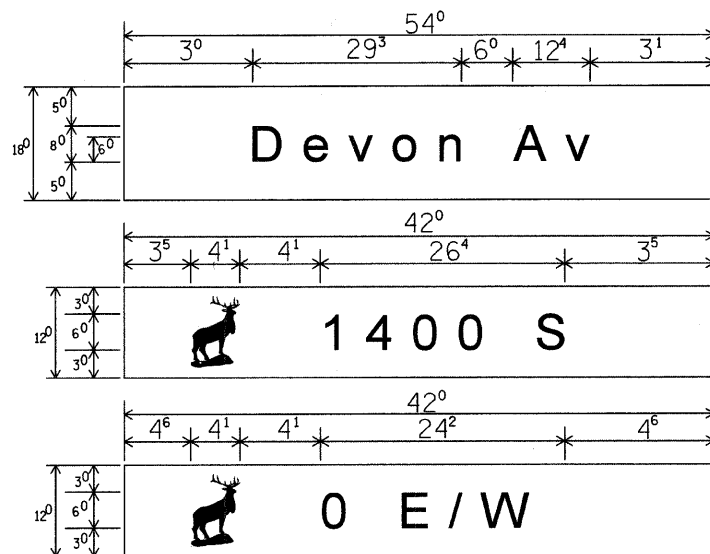
DEVON AVENUE AND  
ARLINGTON HEIGHTS ROAD

SCALE: NONE  
DATE: 5/27/08  
DESIGNED BY: BRD  
CHECKED BY: JJE



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	49
MAST ARM MOUNTED STREET NAME SIGNS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

PANEL SIGN DESIGN TYPE 1

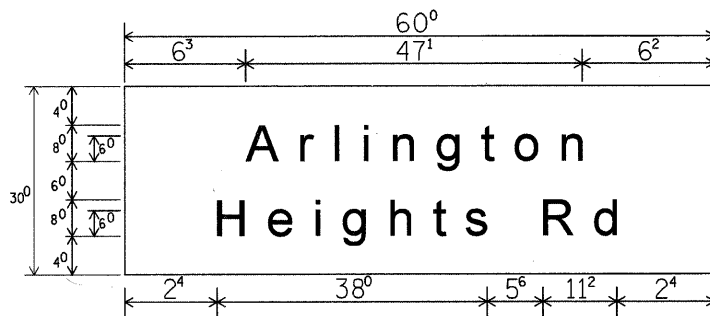


Sq. M Each  
6.75 Sq. Ft. Each  
2 Required  
Design Series D

Sq. M Each  
3.5 Sq. Ft. Each  
2 Required  
Design Series D

Sq. M Each  
3.5 Sq. Ft. Each  
2 Required  
Design Series D

PANEL SIGN DESIGN TYPE 2



Sq. M Each  
12.5 Sq. Ft. Each  
2 Required  
Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS.

## GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" X 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE AP REFLECTORIZED SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

\* A.K.T. CORPORATION  
SCHAUMBURG, IL  
\* TUCKER COMPANY, INC.  
WAUWATOSA, WI

\* AMERICAN FABRICATION CO.  
CHICAGO HEIGHTS, IL  
\* WESTERN TRAFFIC CONTROL, INC.  
CICERO, IL

## PARTS LISTING:

SIGN CHANNEL  
SIGN SCREWS

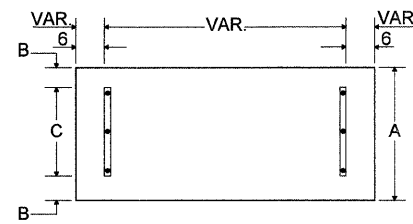
PART #HPN053 (MED. CHANNEL)  
1/4" X 14 X 1" H.W.H #3  
SELF TAPPING WITH NEOPRENE WASHER  
PART #HPN034 (UNIVERSAL)

## BRACKETS

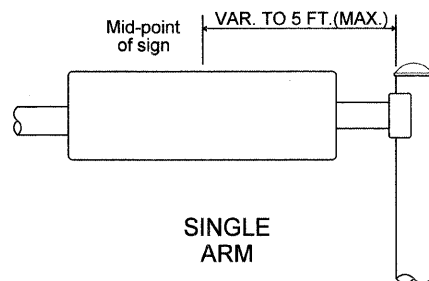
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

SUPPORTING CHANNELS

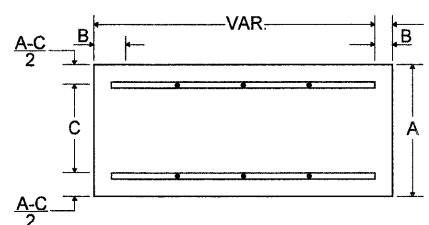


A	B	C
18"	2"	14"

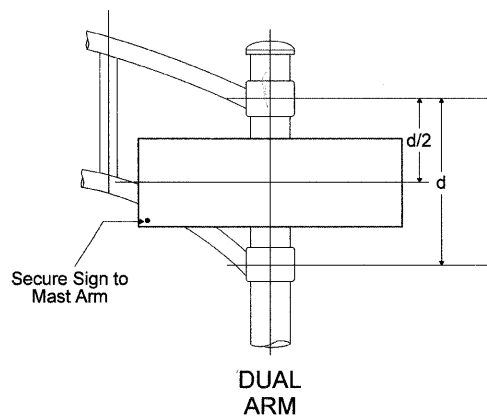


SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"

UPPER TO LOWER CASE  
SPACING CHART 8-6 INCH SERIES "C" & "D"

SERIES	SECOND LETTER															
	a c d e	b h i k	f w	j	s t	v y	x	z								
	g o q	l m n p r u														
A W X	1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>4</sup> 1 <sup>5</sup>	1 <sup>2</sup> 1 <sup>4</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>4</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>4</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>4</sup> 0 <sup>6</sup> 1 <sup>0</sup>														
B	1 <sup>4</sup> 1 <sup>5</sup> 2 <sup>0</sup> 2 <sup>1</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup>															
C E G	1 <sup>4</sup> 1 <sup>5</sup> 2 <sup>0</sup> 2 <sup>1</sup> 1 <sup>4</sup> 1 <sup>5</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup>															
D O Q R	1 <sup>4</sup> 1 <sup>5</sup> 2 <sup>0</sup> 2 <sup>1</sup> 1 <sup>4</sup> 1 <sup>5</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup>															
F	0 <sup>5</sup> 0 <sup>6</sup> 1 <sup>4</sup> 1 <sup>5</sup> 0 <sup>6</sup> 1 <sup>0</sup> 0 <sup>5</sup> 0 <sup>6</sup> 0 <sup>6</sup> 1 <sup>0</sup> 0 <sup>6</sup> 1 <sup>0</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>4</sup>															
H I M N	2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>2</sup> 2 <sup>4</sup> 2 <sup>0</sup> 2 <sup>1</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>6</sup> 1 <sup>7</sup> 2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>2</sup>															
J U	2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>0</sup> 2 <sup>1</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>6</sup> 1 <sup>7</sup> 2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>2</sup>															
K L	1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>1</sup> 1 <sup>2</sup> 0 <sup>5</sup> 0 <sup>6</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup>															
P	1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>2</sup> 1 <sup>4</sup> 0 <sup>5</sup> 0 <sup>6</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup>															
S	1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>2</sup> 1 <sup>4</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup>															
T	1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>6</sup> 1 <sup>7</sup> 0 <sup>6</sup> 1 <sup>0</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup>															
V	0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>4</sup> 1 <sup>5</sup> 1 <sup>1</sup> 1 <sup>2</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>5</sup>															
Y	0 <sup>5</sup> 0 <sup>6</sup> 1 <sup>4</sup> 1 <sup>5</sup> 0 <sup>6</sup> 1 <sup>0</sup> 0 <sup>5</sup> 0 <sup>6</sup> 0 <sup>5</sup> 0 <sup>7</sup> 0 <sup>5</sup> 0 <sup>6</sup> 0 <sup>6</sup> 1 <sup>0</sup> 1 <sup>1</sup> 1 <sup>2</sup> 1 <sup>4</sup>															
Z	1 <sup>6</sup> 1 <sup>7</sup> 2 <sup>2</sup> 2 <sup>4</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>2</sup> 1 <sup>4</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>6</sup> 1 <sup>7</sup> 1 <sup>6</sup> 1 <sup>7</sup> 2 <sup>0</sup> 2 <sup>1</sup> 2 <sup>2</sup>															

EXAMPLE, 2<sup>3</sup>— DENOTES 3/8"UPPER AND LOWER CASE  
LETTER WIDTHS

L E T T E R S	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		L E T T E R S	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>	4 <sup>7</sup>
W	4 <sup>4</sup>	5 <sup>2</sup>	6 <sup>0</sup>	7 <sup>0</sup>	w	5 <sup>5</sup>	6 <sup>4</sup>
X	3 <sup>4</sup>	4 <sup>0</sup>	4 <sup>5</sup>	5 <sup>3</sup>	x	4 <sup>4</sup>	5 <sup>1</sup>
Y	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>6</sup>	y	4 <sup>6</sup>	5 <sup>3</sup>
Z	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	z	3 <sup>6</sup>	4 <sup>3</sup>

N U M B E R	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	2 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>
2	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
3	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
4	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	5 <sup>7</sup>
5	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
6	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
7	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
8	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
9	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
0	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>

NUMBER TO NUMBER

SPACING CHART 8 INCH SERIES "C" &amp; "D"

F I R S T  N U M B E R	SECOND NUMBER																				
	0		1		2		3		4		5		6		7		8		9		
	SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
	0 9	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
	1	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
	2 3 4	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>
	5	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
	6	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
	7	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>
	8	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>

ILLINOIS DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED  
STREET NAME SIGNSDEVON AVENUE AND  
ARLINGTON HEIGHTS ROAD

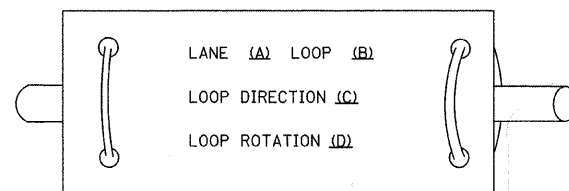
SCALE: NONE  
DATE: 5/27/08  
DESIGNED BY: BRD  
CHECKED BY: JUE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	50
IDOT DIST. 1 TRAFFIC SIGNAL DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

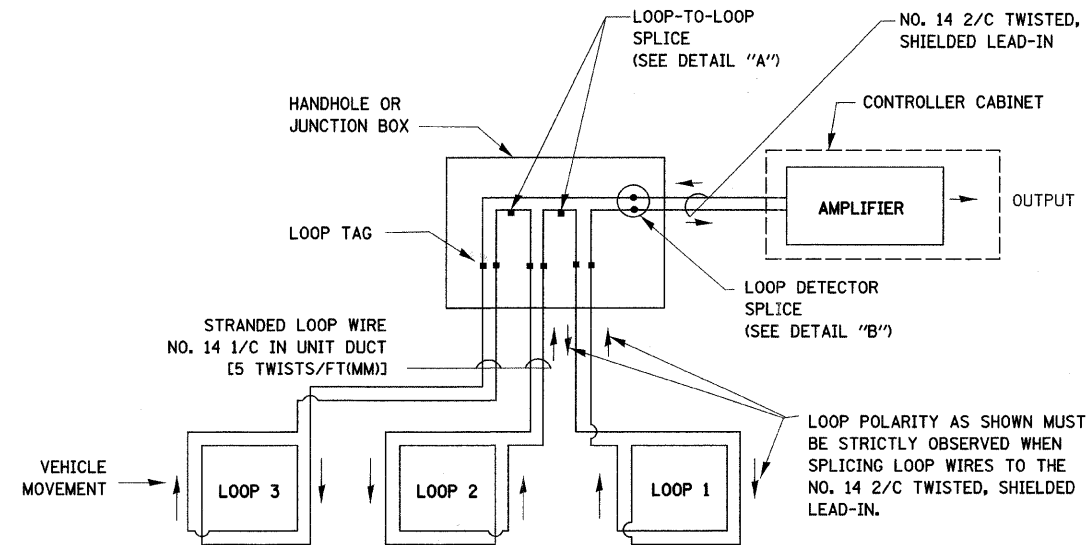
## LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

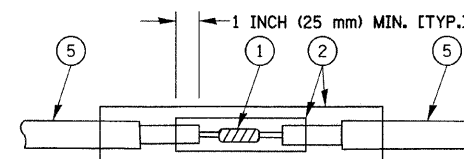


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

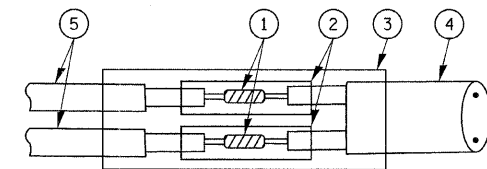


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

ILLINOIS DEPARTMENT OF TRANSPORTATION

### DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

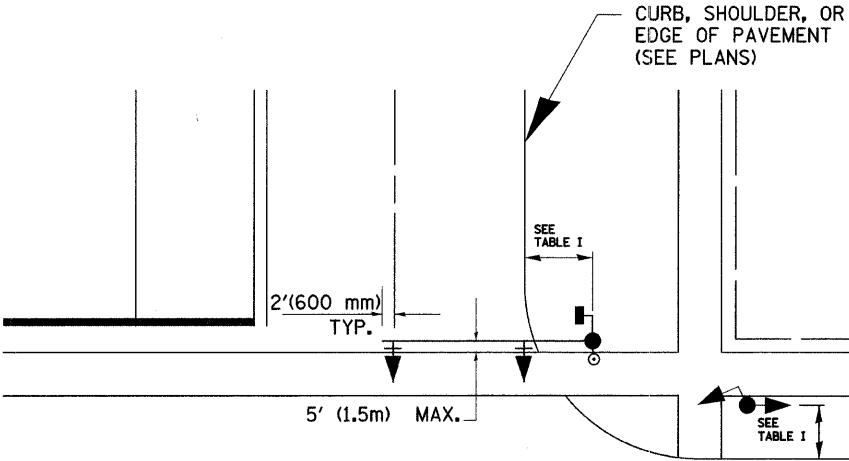
VERT. NONE  
HORIZ. NONE  
DATE 1-01-02

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 1 OF 4

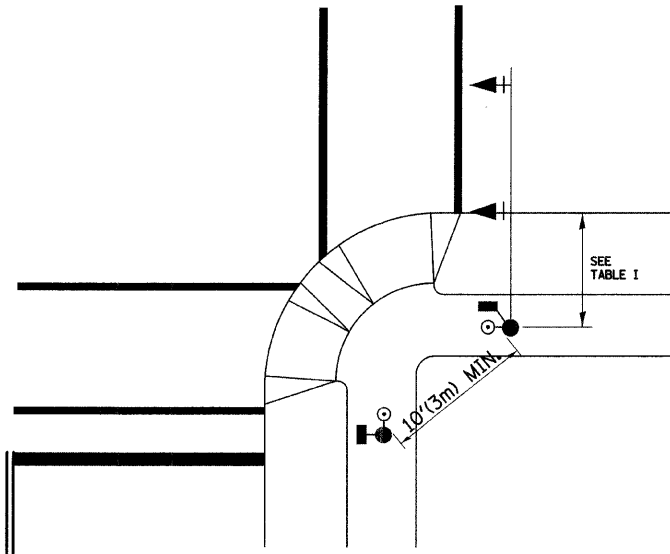
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	51
IDOT DIST. 1 TRAFFIC SIGNAL DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED  
& FUTURE SIDEWALK AREA. INTERSECTION  
SHOWN WITH PEDESTRIAN SIGNAL AND  
PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS  
SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET  
MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE  
MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH  
PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE  
PEDESTRIAN SIGNALS.  
  
AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE  
WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE  
ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS  
WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD  
POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED  
LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE  
PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m).  
THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE  
APPROPRIATE PUSHBUTTON.  
  
PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:  
  
A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR,  
AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.  
  
B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.  
  
C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.  
  
D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).  
  
E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m)  
ABOVE ADJACENT SIDEWALK  
  
2. PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS  
THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A  
PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK  
BEING USED.  
  
3. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE  
AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE  
PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.  
  
4. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING  
TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST  
POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD  
AND PEDESTRIAN PUSHBUTTON  
DETECTOR LOCATION

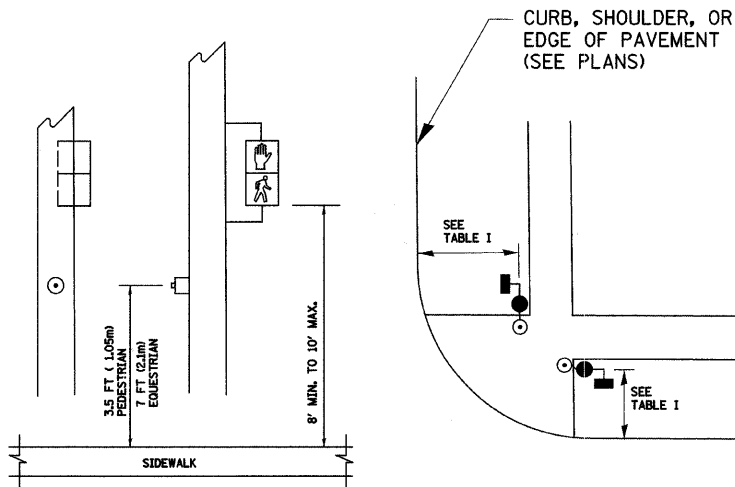


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

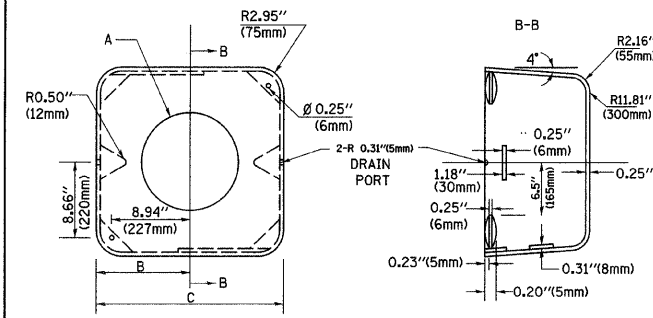
ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS

VERT. NONE  
HORIZ. NONE  
DATE 1-01-02  
DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 2 OF 4



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	53
IDOT DIST. 1 TRAFFIC SIGNAL DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

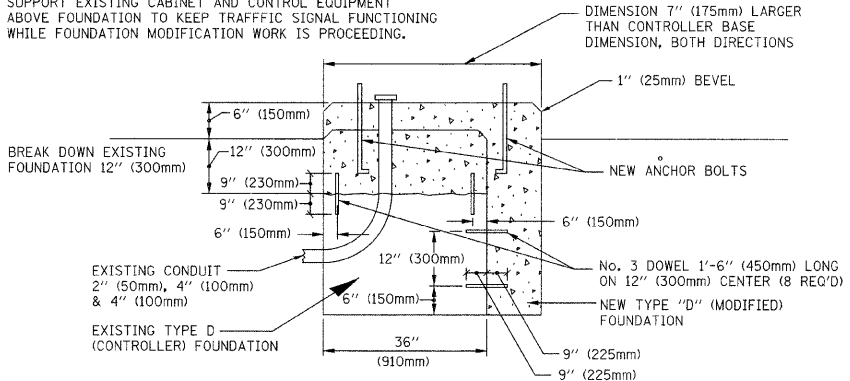


MATERIAL:  
- ASTM A48 CLASS 30 GREY IRON  
- ASTM A123 HOT DIPPED GALVANIZED

TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125" (257mm)	9.5" (241mm)	19" (483mm)	12" (300mm)	24kg
II	Ø 11.125" (283mm)	10.75" (273mm)	21.5" (546mm)	12" (300mm)	26kg

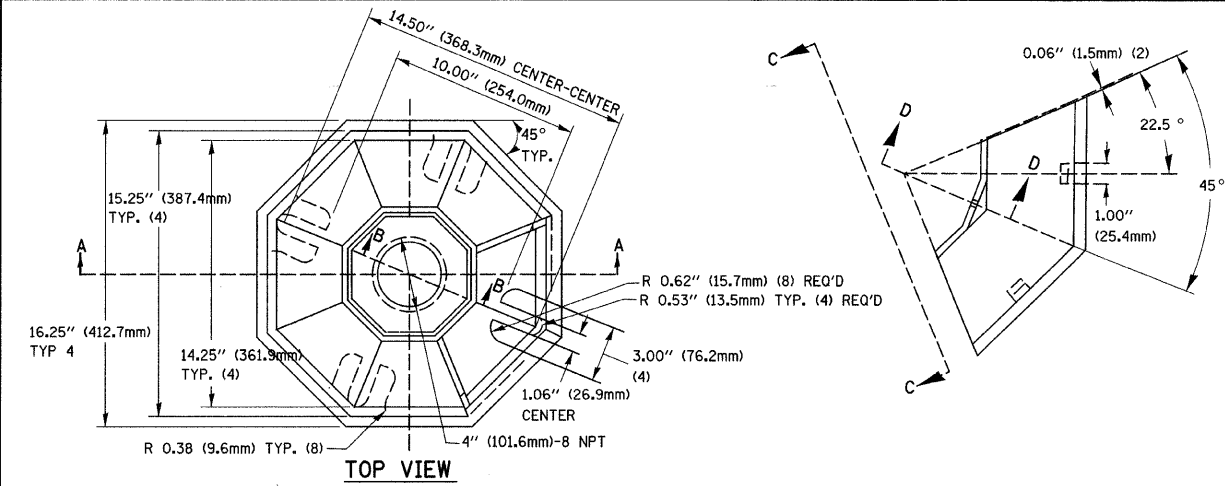
SHROUD DETAIL

NOTE:  
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



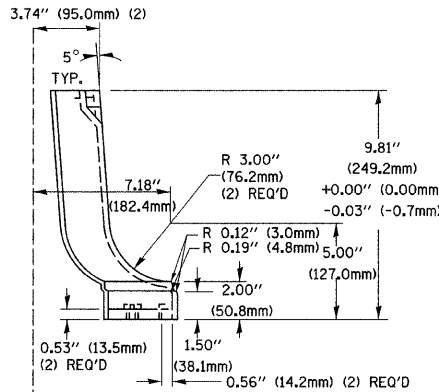
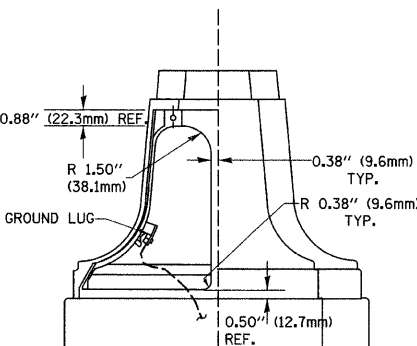
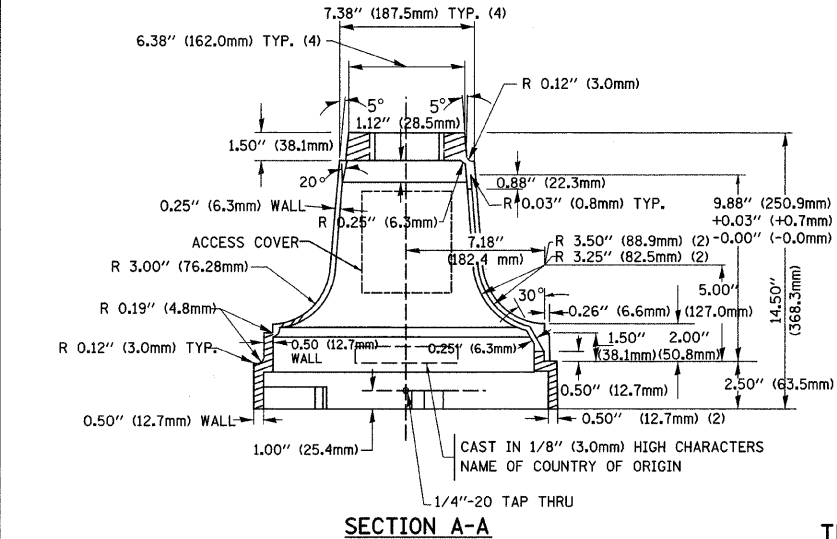
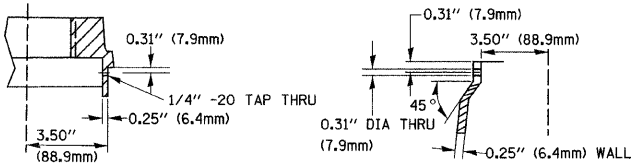
MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)



SECTION B-B

SECTION D-D



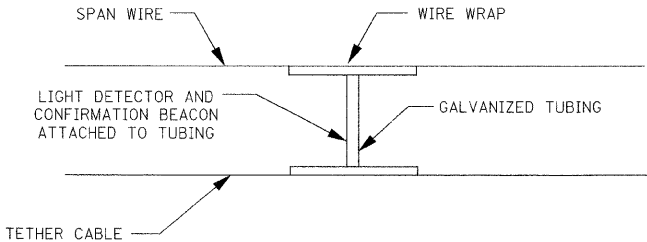
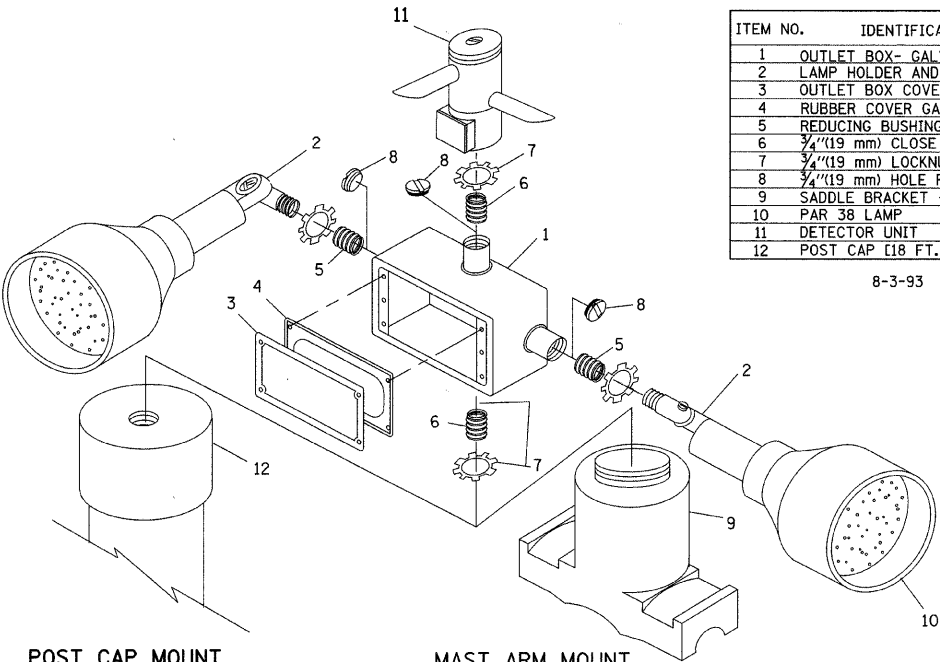
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

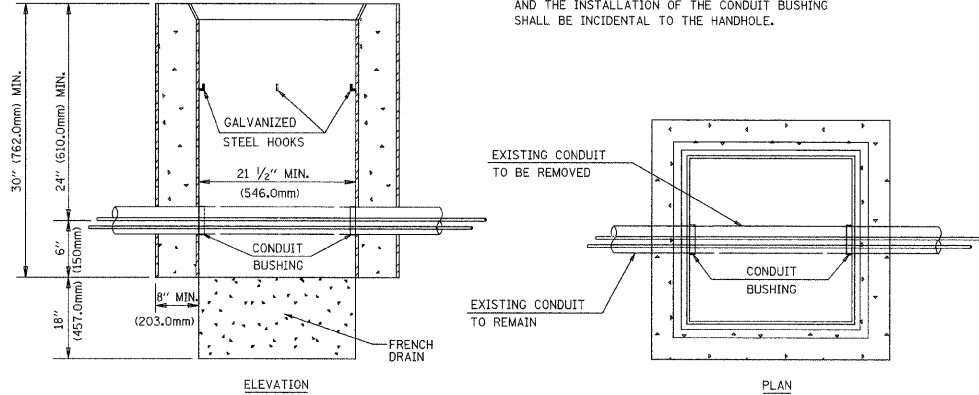
ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

(NOT TO SCALE)



ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1

STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: VERT. NONE  
HORIZ. NONE  
DATE 1-01-02

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: PAZ  
SHEET 4 OF 4

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	54
DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION ON JANUARY 1, 2007.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS"; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2007; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE "SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS" INCLUDED IN THE CONTRACT DOCUMENTS; NATIONAL ELECTRICAL CODE (NEC) 2005 EDITION; AND THE ILLUMINATING ENGINEERING SOCIETY OF NORTH AMERICA (IESNA) "AMERICAN NATIONAL STANDARD PRACTICE FOR ROADWAY LIGHTING" (RP-8-00).
- ANY REFERENCE TO "STANDARD" THROUGHOUT THE PLANS, SPECIAL PROVISIONS OR THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE LATEST VERSION OF THAT IDOT STANDARD DRAWING.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE "STANDARD SPECIFICATIONS".
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER. ALL REFERENCES TO "VILLAGE" SHALL BE INTERPRETED TO MEAN THE VILLAGE OF ELK GROVE VILLAGE.








UTILITIES

- THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION THAT THE VILLAGE MAY WANT TO INITIATE DURING THE CONTRACTOR'S OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATIONS OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM, IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE "STANDARD SPECIFICATIONS". THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IN NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
- COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
- THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE LIGHTING SYSTEM. FOR THE LOCATIONS OF THE UTILITIES, CALL JULIE TOLL FREE AT 1-800-892-0123. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE EXISTING TRAFFIC SIGNAL CABLES AND CONDUITS.
- THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT, AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
- CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUITS, DETECTOR AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED, THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT NO COST TO THE COUNTY, STATE OR THE VILLAGE.

MISCELLANEOUS

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL 2 WEIGHTED SANDBAGS ON ANY TYPE I OR TYPE II BARRICADE USED (1 SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL).
- PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, ANY WORK IS NOT REQUIRED, THAT ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL WASTE MATERIAL SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE R.O.W. AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROMPT CLEANUP AND DISPOSAL OF ANY AND ALL DEBRIS GENERATED BY OR INCIDENTAL TO WORK COVERED BY THIS CONTRACT.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL CODES, ORDINANCES AND REGULATIONS CONCERNING MAINTENANCE OF TRAFFIC.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE ANY LIGHT STANDARD IS ERECTED.
- ALL PUSHED CONDUITS ON PLANS SHALL BE DIRECTIONAL BORE AND QUANTITY SHALL BE PAID FOR AS CONDUIT PUSHED.
- INSTALLATION METHOD OF THE UNIT DUCT SHALL BE DIRECTIONAL BORING. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR TRENCHING WHEN DIRECTIONAL BORING IS NOT POSSIBLE BECAUSE OF UTILITY OR OTHER CONFLICTS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES FOR EXAMINATION AND CONFIRMATION WITH THE ENGINEER PRIOR TO INSTALLATION OF LIGHTING FOUNDATIONS.
- GROUNDING CONNECTIONS AT THE FOUNDATION SHALL BE EXOTHERMICALLY WELDED AND SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO POURING CONCRETE OR BACKFILLING AS APPLICABLE.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK." THE INSTALLATION OF THE TAPE SHALL BE INSPECTED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO BACKFILLING OR DURING FLOWING OPERATIONS, AS APPLICABLE.
- THE LIGHTING CONTROLLER CABINET SHALL BE PLACED SO THAT THE DOOR IS ORIENTED OPPOSITE OF TRAFFIC.
- NO POLES SHALL BE ERECTED UNTIL THE RESPECTIVE FOUNDATIONS HAVE CURED, AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF THE FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.
- EQUIPMENT GROUND CONDUCTORS SHALL BE SPLICED AND BONDED AT EACH LIGHT POLE OR OTHER PIECE OF EQUIPMENT.
- CONDUIT AND UNIT DUCT MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREES, BUSHES, DRAINS AND OTHER UTILITIES AND LANDSCAPING.
- ALL DISTURBED AREA WHERE RESTORATION IS NOT COVERED BY APPLICABLE SECTIONS OF THE SPECIAL PROVISIONS MUST BE RESTORED TO THE SATISFACTION OF THE ENGINEER. THE WORK MUST BE CONSIDERED INCIDENTAL TO THE CONTRACT. SEPARATE PAYMENT WILL NOT BE MADE.

LEGEND

-  PROPOSED LIGHTING UNIT ALUMINUM, 45 FT MOUNTING HEIGHT, 10 FT DAVIT ARM, (6' DAVIT ARM AS SHOWN IN THE PLAN), TRANSFORMER BASE, LUMINAIRE - 400 WATT METAL HIGH PRESSURE SODIUM LAMP, 240 VOLT AUTO REG BALLAST CWA
-  PROPOSED LIGHTING CONTROLLER
-  UNIT DUCT WITH CABLES, 600V (XLP-TYPE USE).
-  PROPOSED GALVANIZED STEEL CONDUIT, SIZE AS SPECIFIED
-  EXISTING LIGHTING UNIT, ELK GROVE VILLAGE
-  EXISTING LIGHTING UNIT, OTHER
-  LIGHT POLE AND FOUNDATION REMOVAL

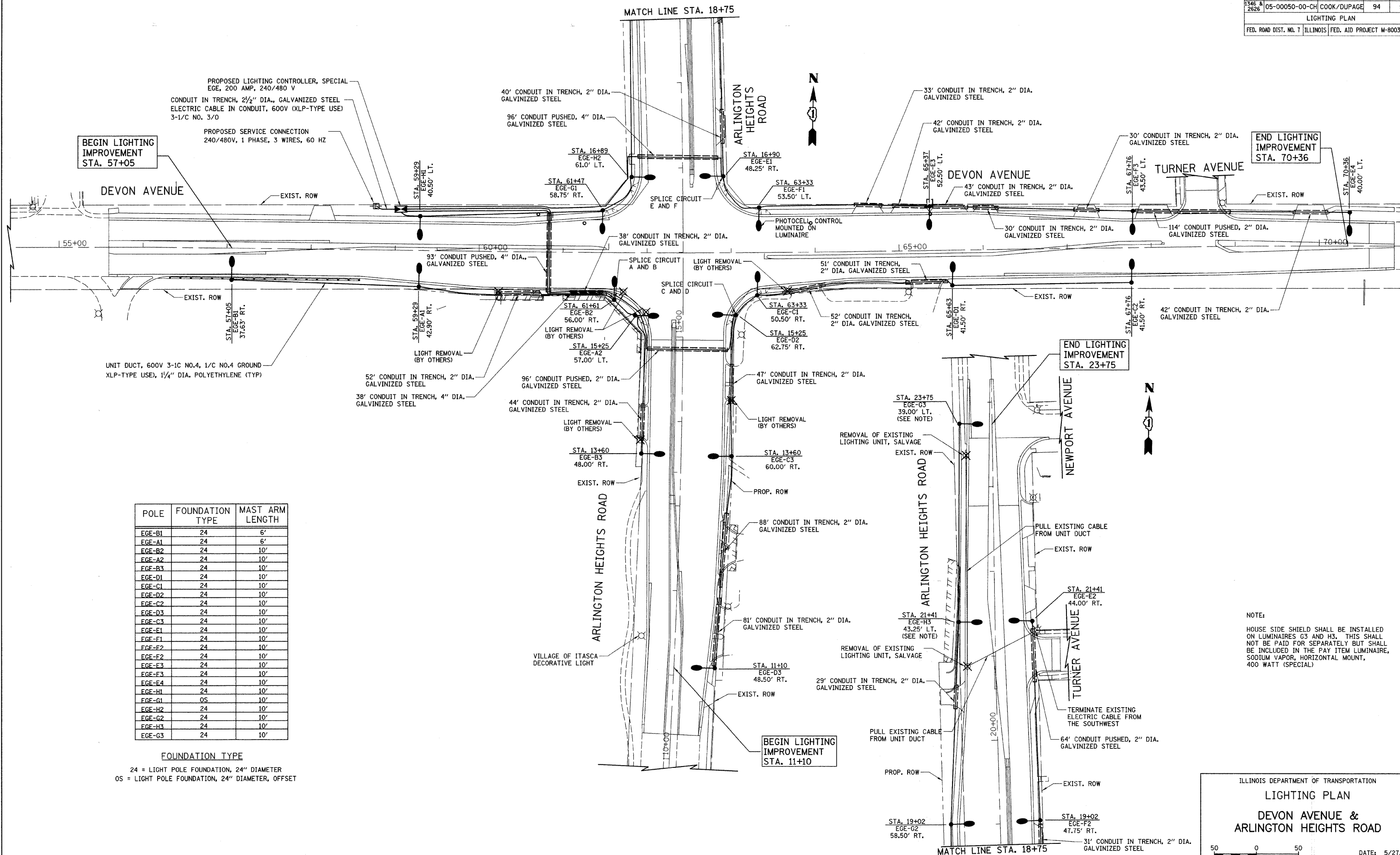
CODED PAY ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY (Y030-1E)
21300010	EXPLORATION TRENCH, SPECIAL	FOOT	100
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1
80400200	ELECTRIC UTILITY SERVICE CONNECTION	EACH	1
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	743
81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	100
81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	38
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	370
81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	93
81603085	UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	5506
81702460	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 3/0	FOOT	115
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	4459
82500505	LIGHTING CONTROLLER, SPECIAL	EACH	1
83017200	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 45 FT. M.H., 6 FT. DAVIT ARM	EACH	2
83017400	LIGHT POLE, ALUMINUM, TRANSFORMER BASE, 45 FT. M.H., 10 FT. DAVIT ARM	EACH	22
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	230
83600215	LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	15
84200500	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	2
84200700	LIGHTING FOUNDATION REMOVAL	EACH	2
XX001368	PULL EXISTING CABLE FROM UNIT DUCT	FOOT	345
	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT (SPECIAL)	EACH	24

ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING NOTES

DATE: 5/27/08  
DESIGNED BY: JSM  
CHECKED BY: DNM  
NOT TO SCALE





POLE	FOUNDATION TYPE	MAST ARM LENGTH
EGE-B1	24	6'
EGE-A1	24	6'
EGE-B2	24	10'
EGE-A2	24	10'
EGE-B3	24	10'
EGE-D1	24	10'
EGE-C1	24	10'
EGE-D2	24	10'
EGE-C2	24	10'
EGE-D3	24	10'
EGE-C3	24	10'
EGE-E1	24	10'
EGE-F1	24	10'
EGE-F2	24	10'
EGE-F3	24	10'
EGE-E3	24	10'
EGE-E4	24	10'
EGE-H1	24	10'
EGE-G1	OS	10'
EGE-H2	24	10'
EGE-G2	24	10'
EGE-H3	24	10'
EGE-G3	24	10'

FOUNDATION TYPE

24 = LIGHT POLE FOUNDATION, 24" DIAMETER

OS = LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET

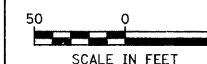
NOTE:

HOUSE SIDE SHIELD SHALL BE INSTALLED ON LUMINAIRES G3 AND H3. THIS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PAY ITEM LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT (SPECIAL)

ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING PLAN

DEVON AVENUE & ARLINGTON HEIGHTS ROAD



DATE: 5/27/08

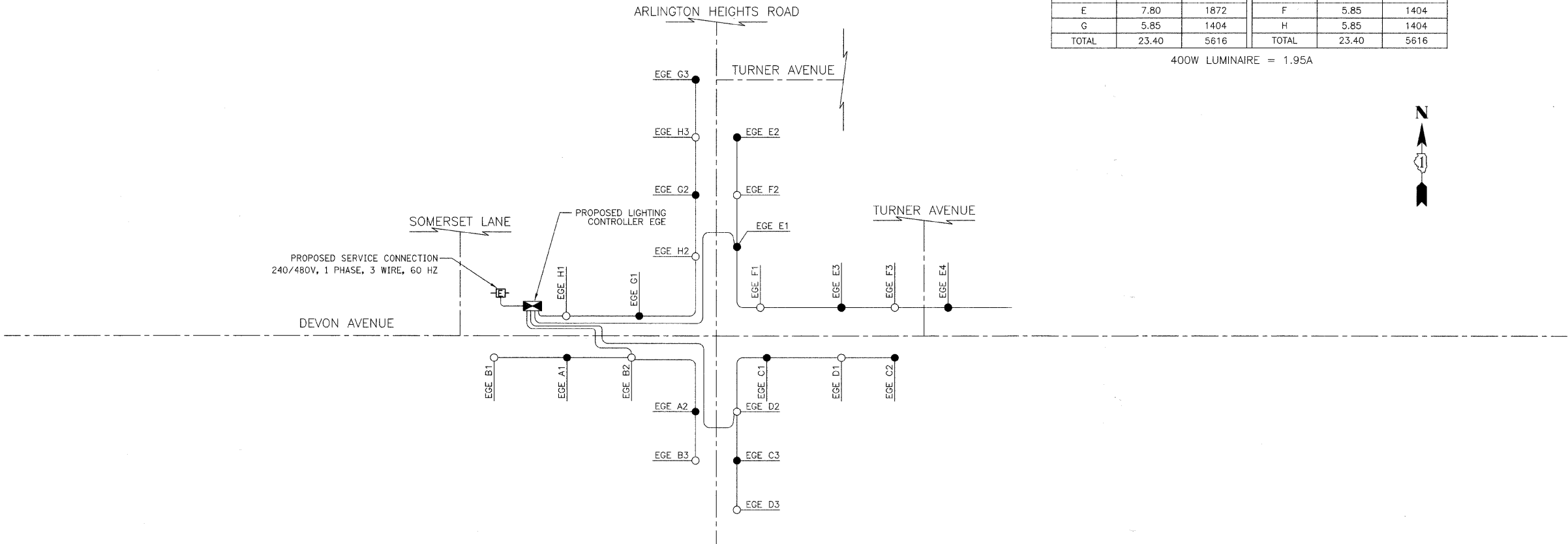
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	56
LIGHTING WIRING PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)				

CONTROLLER EGE CIRCUIT LOAD TABLE					
CIRCUIT	BLACK PHASE		CIRCUIT	RED PHASE	
	AMPS	WATTS		AMPS	WATTS
A	3.90	936	B	5.85	1404
C	5.85	1404	D	5.85	1404
E	7.80	1872	F	5.85	1404
G	5.85	1404	H	5.85	1404
TOTAL	23.40	5616	TOTAL	23.40	5616

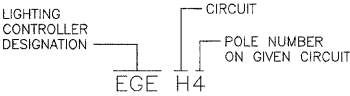
400W LUMINAIRE = 1.95A



LEGEND

- LITHONIA LUMINAIRE, SODIUM VAPOR  
400W, 240V ON BLACK WIRE
- LITHONIA LUMINAIRE, SODIUM VAPOR  
400W, 240V ON RED WIRE
- ⊠ PROPOSED LIGHTING CONTROLLER  
200 AMP, 240/480V, 1Ø, 3-WIRE
- UNIT DUCT, 600V 3-1C NO.4, 1/2" NO.4  
GROUND (XLP-TYPE USE), 1 1/4" DIA. POLY.

CIRCUIT DESIGNATION SCHEME  
(TYPICAL)

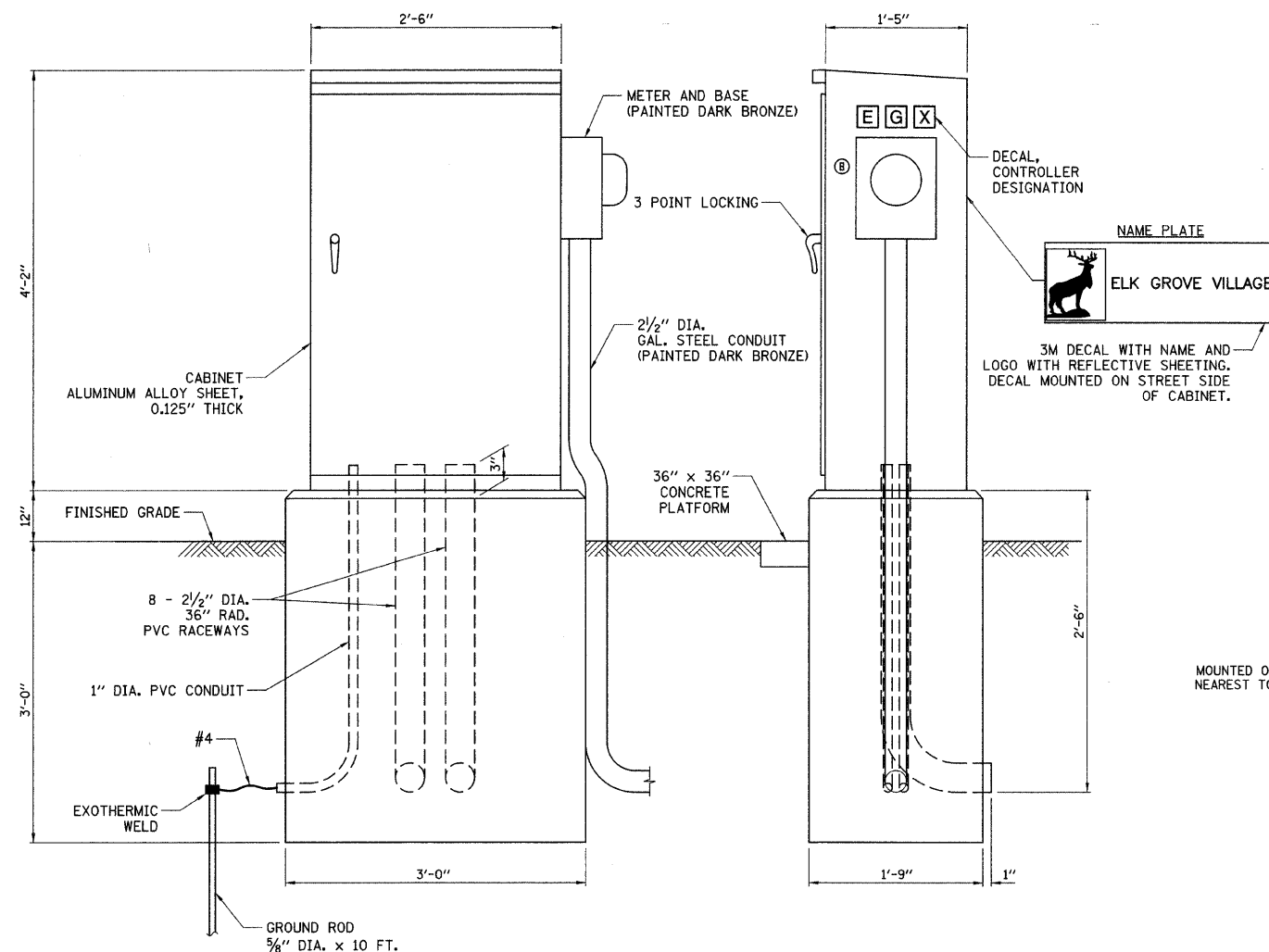


ILLINOIS DEPARTMENT OF TRANSPORTATION  
LIGHTING WIRING  
DIAGRAM

DATE: 5/27/08  
DESIGNED BY: JSM  
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NOT TO SCALE





LIGHTING CONTROLLER, SPECIAL  
CONTROL CABINET DETAILS

DEVICE SCHEDULE		
ITEM	QUANT.	DESCRIPTION
(A)	1	200 AMP CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, SINGLE THROW, 600V FRAME, NON-INTERCHANGEABLE TRIP, BOLT ON TYPE, INTERRUPTING CAPACITY OF NOT LESS THAN 25,000 RMS SYMMETRICAL AMPERES AT 600V
(B)	1	200 A., ELECTRICALLY OPERATED AND MECHANICALLY HELD LIGHTING CONTACTOR, 2-POLE, 600 V. WITH 120 V. COIL
(C)	12	240/480 V. 1 PHASE PANEL BOARD WITH 200 A. COPPER MAINS SINGLE POLE, 30 A., 277 V. BOLT ON BRANCH CIRCUIT BREAKERS INTERRUPTING CAPACITY OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277 V.
(D)	1	PHOTO-ELECTRIC CELL, 120 V, MOUNT ON LIGHT POLE NEAREST CONTROLLER
(E)	2	THERMAL MAGNETIC, MOLDED CASE CIRCUIT BREAKER, 1 POLE, 15 A., 277 V. BOLT ON TYPE, INTERRUPTING CAPACITY OF NOT LESS THAN 14,000 RMS SYMMETRICAL AMPERES AT 277 V.
(F)	1	CONTROL SWITCH, MOMENTARY CONTACT, SPDT, 15 A., 240 V.
(G)	1	CONTROL SWITCH, TOGGLE TYPE, SPDT, 20 A., 240 V. SPEC. GRADE
(H)	1	240/120V. STEP DOWN CONTROL TRANSFORMER 750 VA RATED
(I)	1	MICRO SWITCH (MOUNTED WITH ACTUATOR TO SWITCH WHEN DOOR IS OPEN)
(J)	1	60 WATT LIGHT FIXTURE VAPORTIGHT WITH GLOBE, GUARD AND MOUNTING BOX
(K)	1	120 VOLT, 15 A. GFCI RECEPTACLE, SPECIFICATION GRADE IN NEMA 5-15R WEATHERPROOF BOX WITH FLAP-TYPE COVER
(L)	1	POWER RELAY WITH CONTACTS RATED FOR CONTACTOR INRUSH CURRENT - 120 V COIL
(M)	1	SURGE ARRESTOR, 5 WIRE EDCO "4803" OR EQUAL

NOTES

THE CABINET SHALL BE FABRICATED FROM 0.125" THICK ALUMINUM ALLOY SHEET AND SHALL BE REINFORCED WITH ALUMINUM ANGLES. THE CABINET DOOR SHALL BE NEMA TYPE 3R CONSTRUCTION WITH NEOPRENE GASKET. THE DOOR SHALL HAVE STAINLESS STEEL HINGES AND THREE POINT LOCKING SYSTEM.

THE CONTRACTOR SHALL REMOVE VEGETATION AND TOPSOIL, LEVEL THE AREA IN FRONT OF THE CONTROL CABINET DOOR AND PLACE LENGTHWISE, PARALLEL TO CONTROL CABINET, A PRECAST PAD, 36" x 36" x 3" MINIMUM SIZE. THE COST OF LABOR AND MATERIALS SHALL BE INCIDENTAL TO THE CONTROL CABINET.

CONTROL WIRE SHALL BE #12 AWG, 600V, TYPE "SIS" STRANDED COPPER GRAY SWITCH BOARD WIRE. THE ENDS OF ALL CONTROL WIRES SHALL BE IDENTIFIED.

ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.  
R = RED BL = BLUE W = WHITE  
B = BLACK Y = YELLOW G = GREEN

THE ELECTRIC METER BOX SHALL BE MOUNTED ON THE SIDE OF THE CONTROL CABINET, NEAR THE SERVICE POLE AND/OR AS DIRECTED BY THE ENGINEER.

ALL CONTROL CABINET ITEMS SHALL HAVE SUITABLE IDENTIFICATION. OPEN CIRCUIT BREAKERS, CONTACTORS AND OTHER OPEN DEVICES SHALL HAVE PERMANENT SELF STICKING TAGS. DEVICES IN ENCLOSURES SHALL HAVE ENGRAVED 2-COLOR LAMINATED PLASTIC NAMEPLATES ATTACHED TO ENCLOSURES WITH SCREWS. NAMEPLATES SHALL BE ENGRAVED TO CORRESPOND TO DESIGNATIONS ON THE DRAWINGS. INTERNAL CABINET WIRING SHALL BE IDENTIFIED AS INDICATED OR AS DIRECTED BY THE ENGINEER BY MEANS OF SELF-STICKING TAGS APPLIED AT EACH CONNECTED END. IDENTIFICATION SHALL BE MADE BY THE CABINET MANUFACTURER.

ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

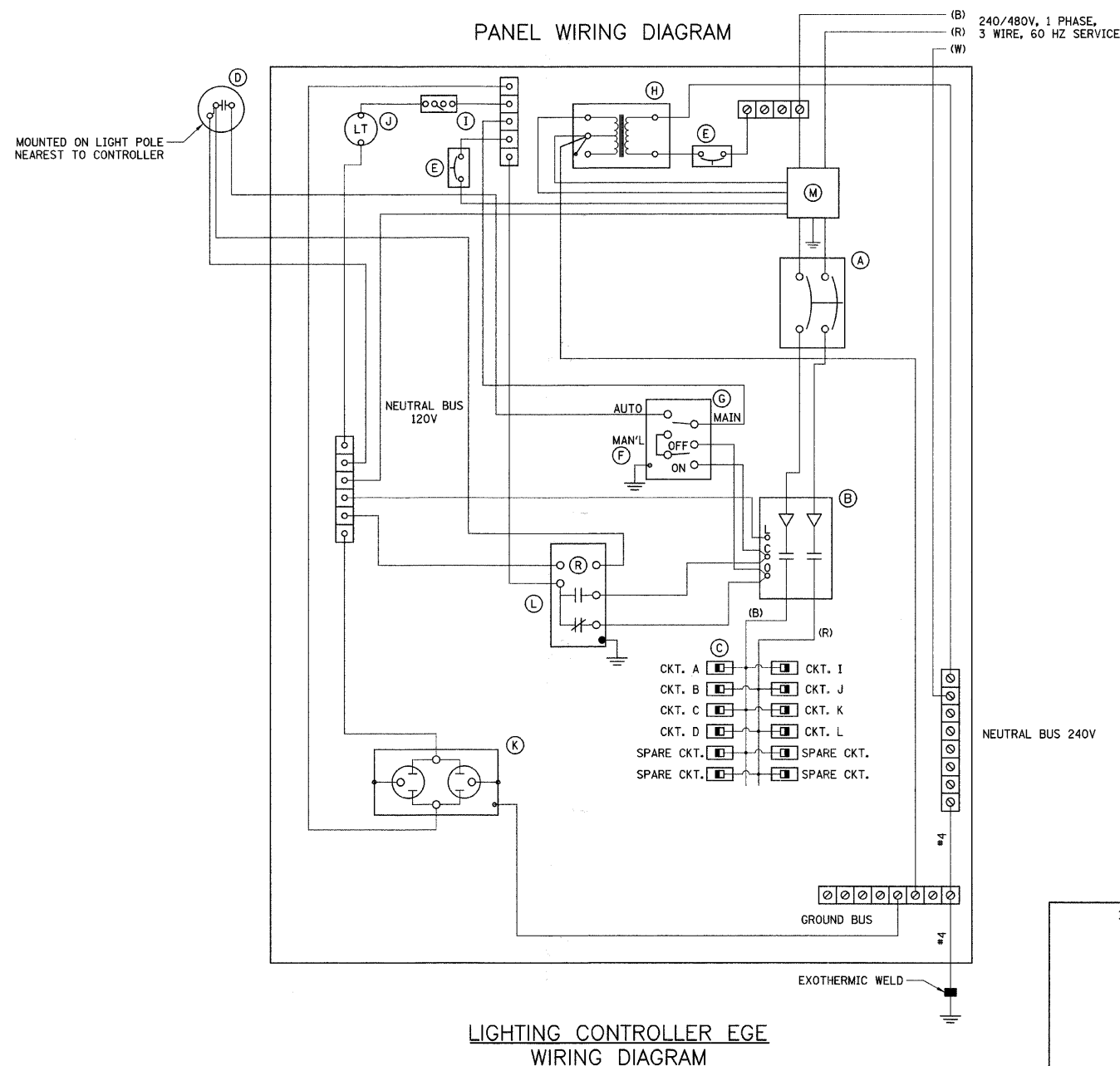
THE HEADS OF CONNECTOR SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BUSS CONNECTION AND GREEN FOR GROUND BUSS CONNECTORS.

PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES WITHIN THE CONTROL CABINET.

THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".

PROVIDE A HOLDER AND WATERPROOF POUCH ON THE INNER SIDE OF THE CONTROLLER DOOR. THE HOLDER AND POUCH SHALL BE MOUNTED SO THAT RAINWATER OR CONDENSED WATER CANNOT ENTER THE POUCH WITH THE CABINET DOOR OPEN. FURNISH THE APPROVED COPY OF THE "CONTROL CABINET WIRING DIAGRAM".

THE CABINET, METER AND BASE, AND CONDUIT FOR THE METER SHALL BE PAINTED IN DARK BRONZE FINISH.

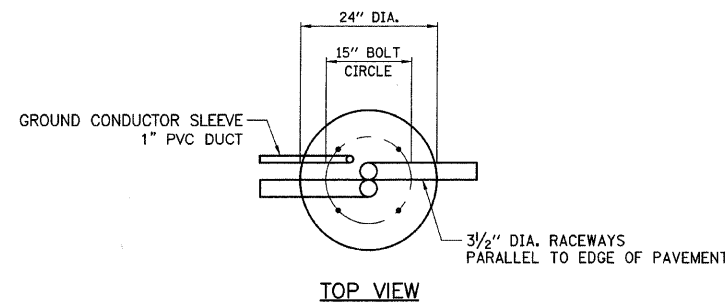


ILLINOIS DEPARTMENT OF TRANSPORTATION

LIGHTING DETAILS

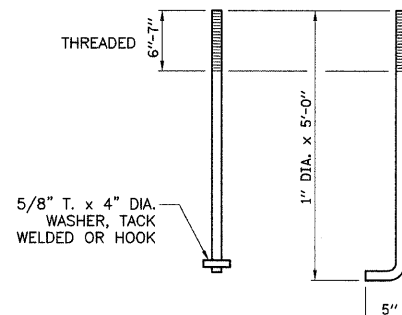
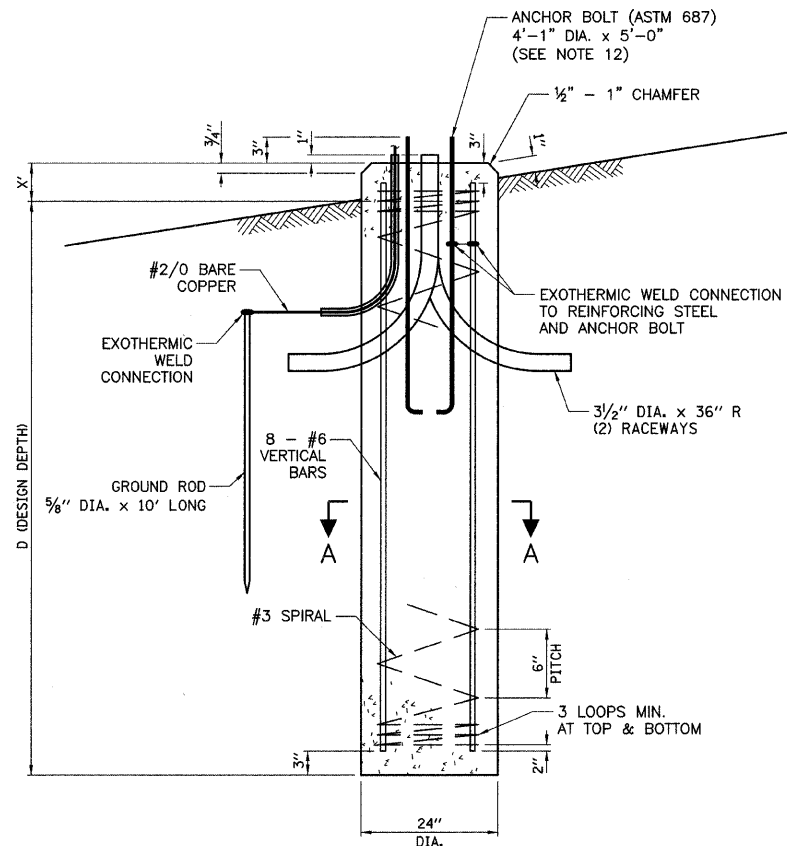
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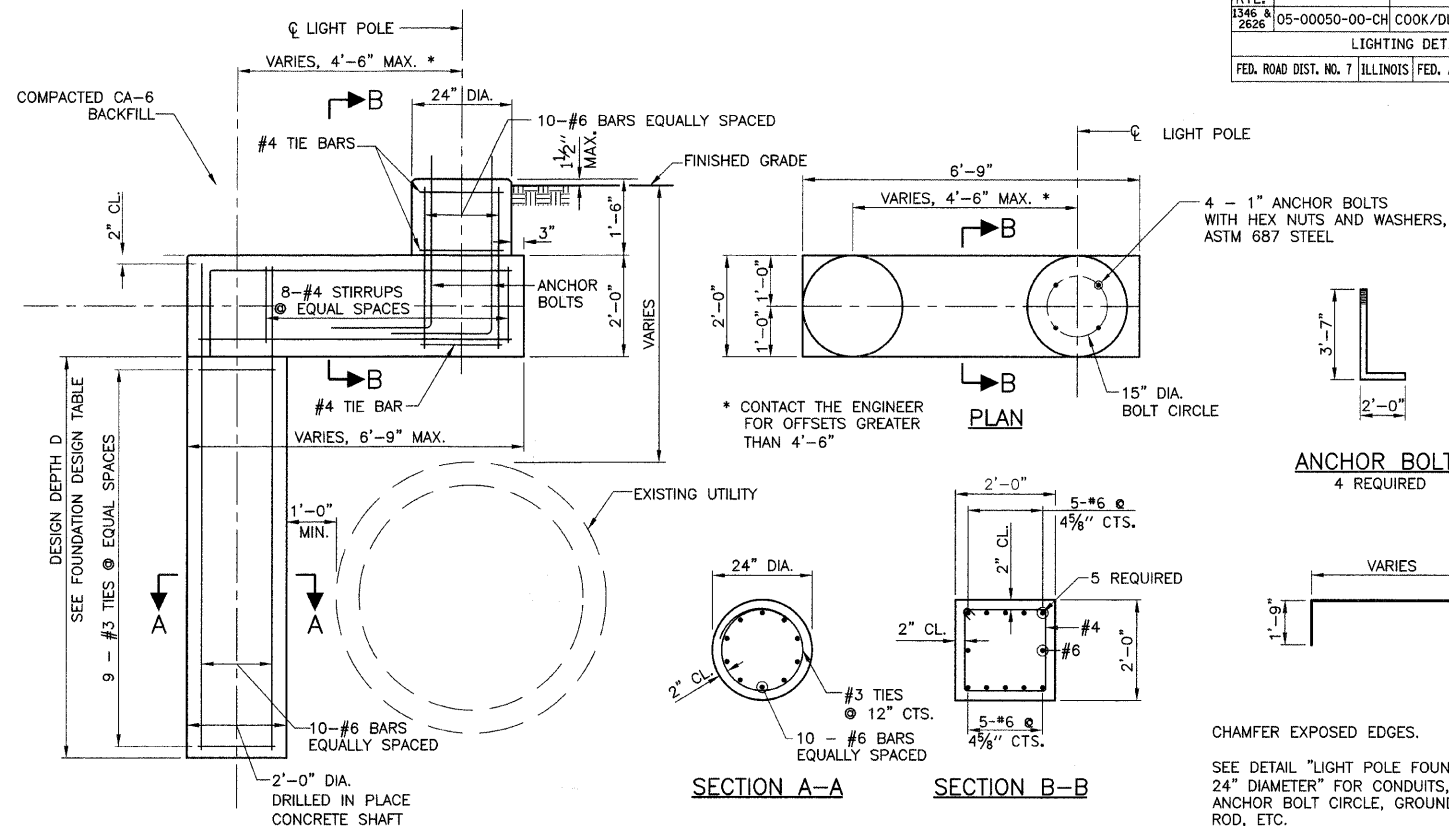


DESIGN TABLE  
LIGHT POLE FOUNDATION,  
24" DIAMETER, OFFSET

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION D
SOFT CLAY	13'-0"
MEDIUM CLAY	9'-6"
STIFF CLAY	7'-0"
LOOSE SAND	9'-0"
MEDIUM SAND	8'-3"
DENSE SAND	7'-9"



ANCHOR BOLT DETAIL



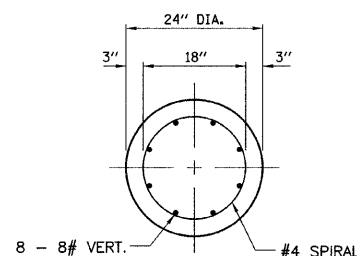
LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET

NOTES:

1. THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE. THE CONTRACTOR SHALL NOT ORDER REINFORCEMENT BARS UNTIL THE OFFSET AND DIMENSION D ARE DETERMINED.
2. EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" OR 30" IN DIAMETER.
3. THE CONTRACTOR SHALL USE #3 SPIRAL AT 6" PITCH OR AT HIS OPTION MAY SUBSTITUTE #3 TIES AT 12" CENTER.
4. THE ANCHOR SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.
5. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
6. THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A 153.
7. CONCRETE SHALL BE CLASS "SI". CONCRETE FOUNDATION MUST BE CURED FOR (10) TEN DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
8. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.
9. ANCHOR BOLTS SHALL PROJECT 3" ABOVE THE TOP OF THE FOUNDATION.
10. RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.
11. THE CONTRACTOR SHALL COORDINATE THE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
12. A MINIMUM OF 3" OF THE THREADING ON THE ANCHOR BOLTS SHALL REMAIN BELOW THE TOP OF THE FOUNDATION.

DESIGN TABLE - LIGHT POLE FOUNDATION, 24" DIAMETER

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION D	REINFORCEMENT IN FOUNDATION	
		SINGLE ARM	
		VERT. BARS	SPIRAL
SOFT CLAY	13'-0"	8-#6 x 12'-6"	#3 x 122'
MEDIUM CLAY	9'-6"	8-#6 x 9'-0"	#3 x 90'
STIFF CLAY	7'-0"	8-#6 x 6'-6"	#3 x 66'
LOOSE SAND	10'-0"	8-#6 x 9'-6"	#3 x 94'
MEDIUM SAND	8'-3"	8-#6 x 8'-0"	#3 x 78'
DENSE SAND	7'-9"	8-#6 x 7'-6"	#3 x 73'
ROCK OR SOLIDIFIED SLAG	5'-0"	NONE	NONE

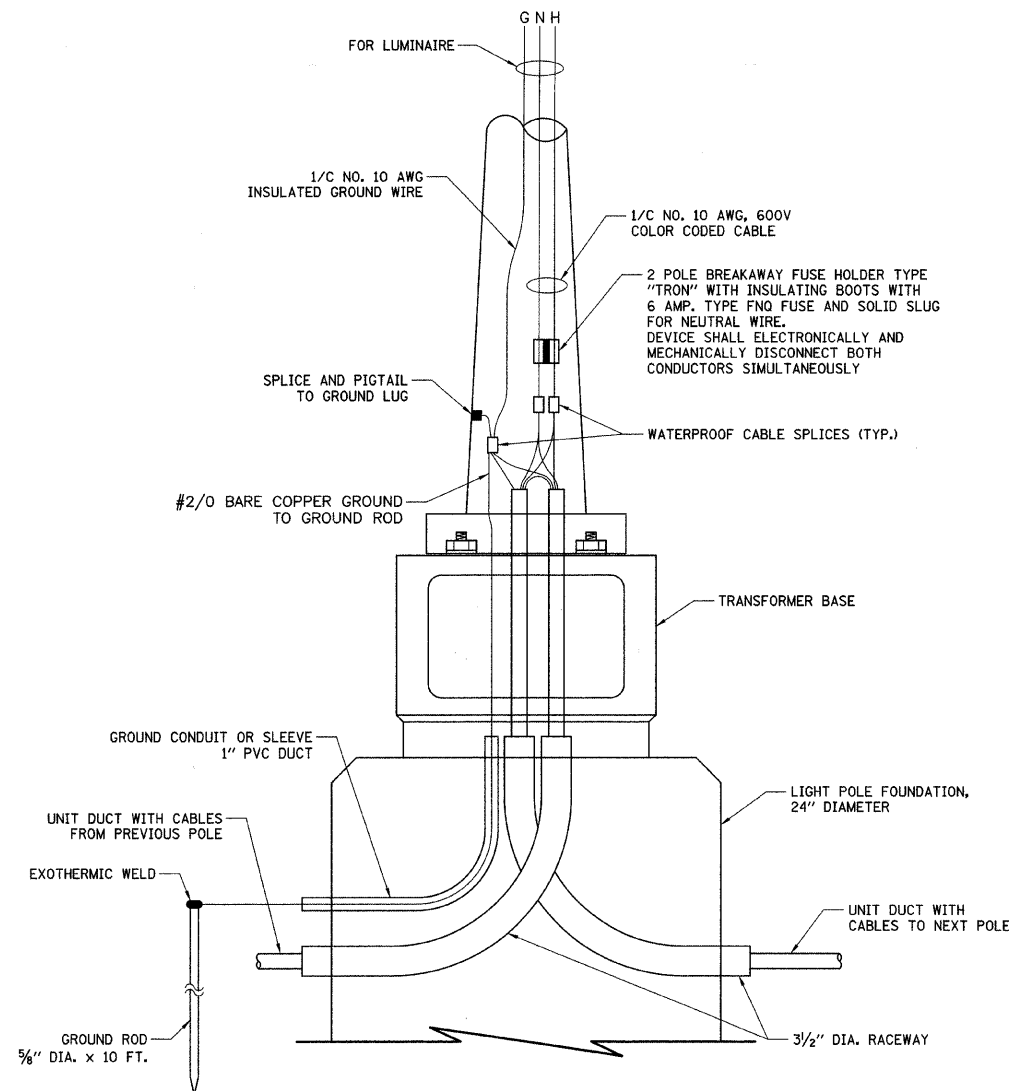


SECTION A-A

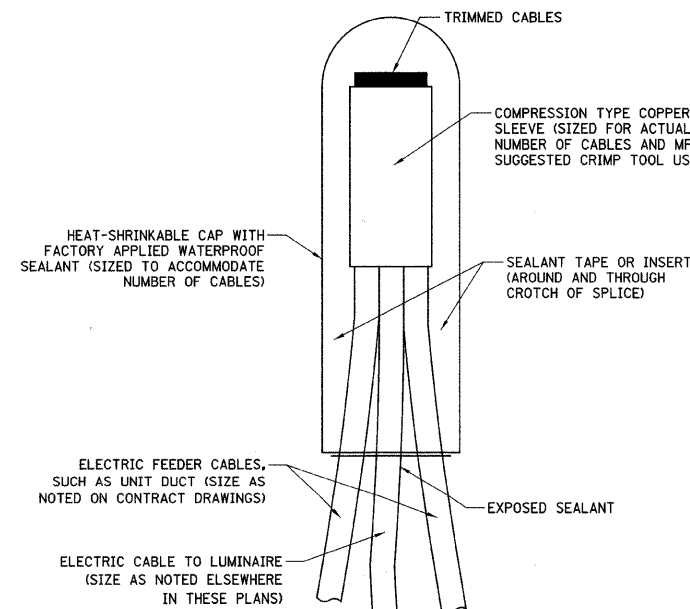
LIGHT POLE FOUNDATION, 24" DIAMETER

ILLINOIS DEPARTMENT OF TRANSPORTATION  
LIGHTING DETAILS

DATE: 5/27/08  
DESIGNED BY: JSM  
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NOT TO SCALE



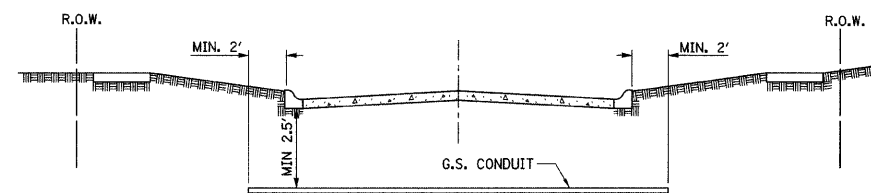
ROADWAY POLE HANDHOLE WIRING DIAGRAM



NOTE: NUMBER OF CABLES IN SPLICE MAY VARY

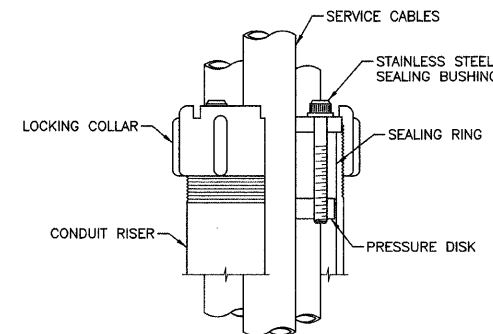
**SPLICING ELECTRIC CABLES**

**BASIC MATERIALS AND METHODS**



- STREET CROSSING**
- ① CONDUIT SHALL BE HEAVY WALL RIGID G.S. CONDUIT.
  - ② CONDUIT SHALL EXTEND A MINIMUM OF 2 FT. BEYOND BACK OF CURB.
  - ③ CONDUIT SHALL BE A MINIMUM OF 2.5 FT. BELOW BOTTOM OF CURB.

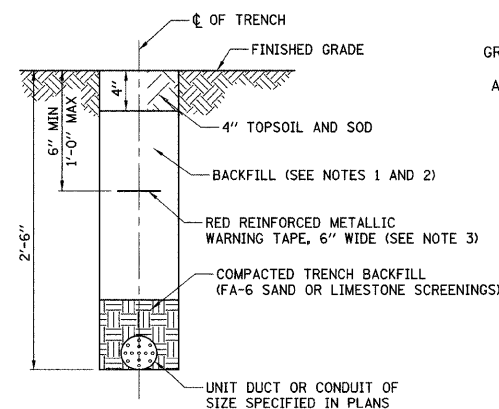
**ELECTRICAL CONDUIT UNDER PAVEMENT**



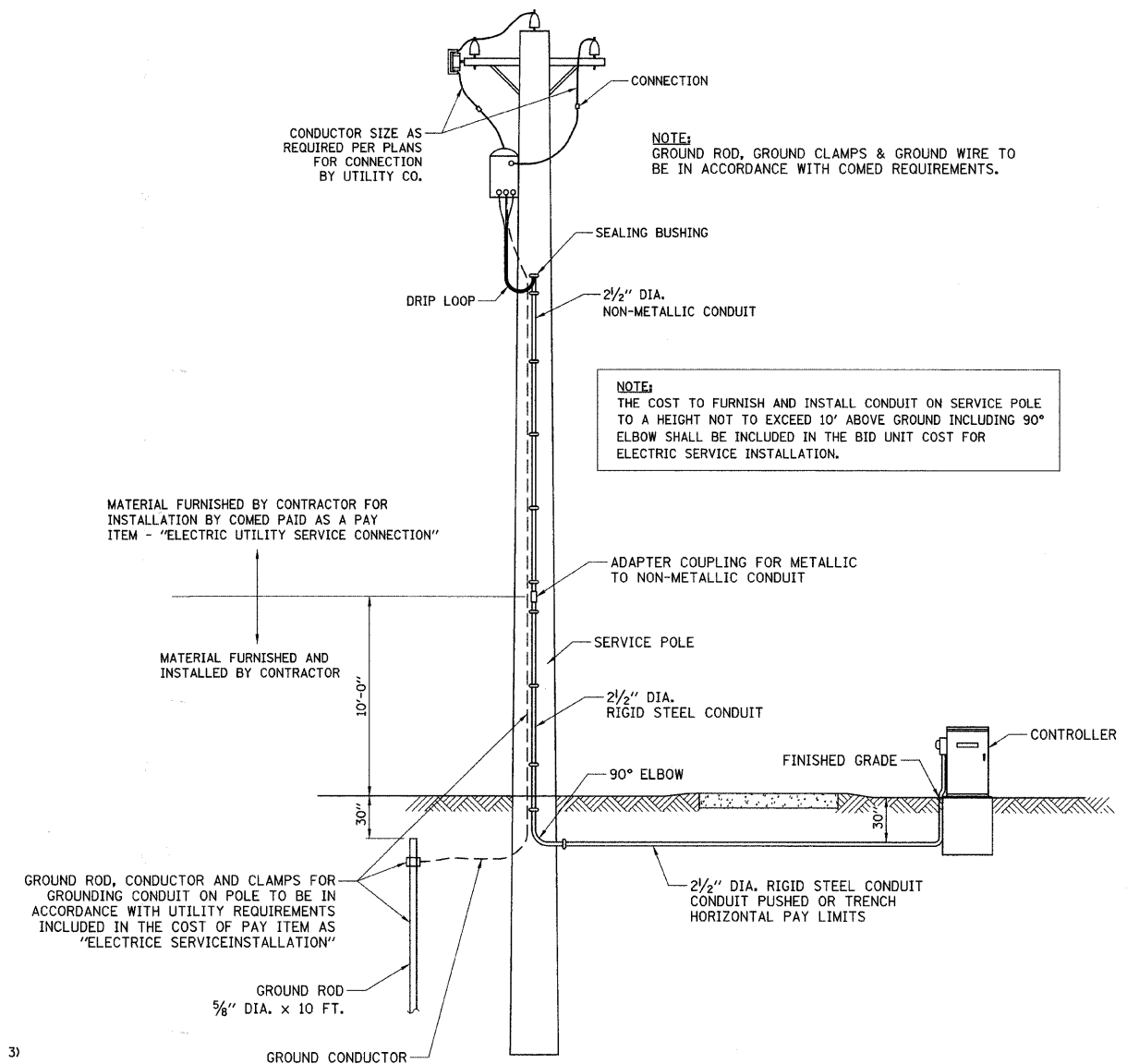
SEALING BUSHING DETAIL

**NOTES:**

1. IN GRASS COVERED AREAS, THE BACKFILL MAY BE COMPACTED EARTH.
2. TRENCHES WITHIN 2' OF PROPOSED OR EXISTING STREETS, DRIVEWAYS, OR SIDEWALKS WILL BE BACKFILLED WITH COMPACTED FA-6 SAND OR LIMESTONE SCREENINGS.
3. WARNING TAPE WILL BE RED WITH BLACK LETTERING TO READ "CAUTION - ELECTRIC LINE BURIED BELOW".
4. ALL GRASS COVERED AREAS DISTURBED DURING CONSTRUCTION WILL BE RESTORED WITH 4" OF TOPSOIL AND SOD.



TYPICAL TRENCH CROSS SECTION



PROPOSED SERVICE INSTALLATION

POLE TOP MOUNTED TRANSFORMER

ILLINOIS DEPARTMENT OF TRANSPORTATION

**LIGHTING DETAILS**

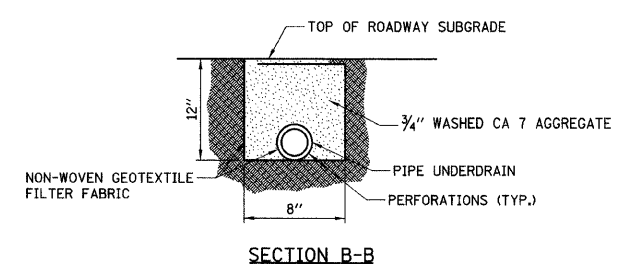
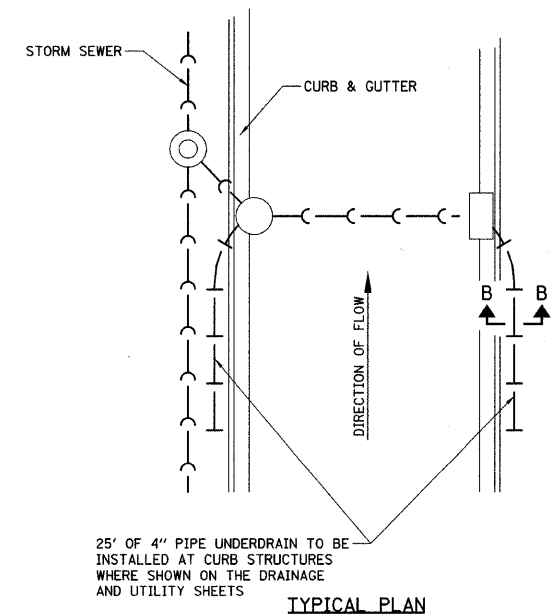
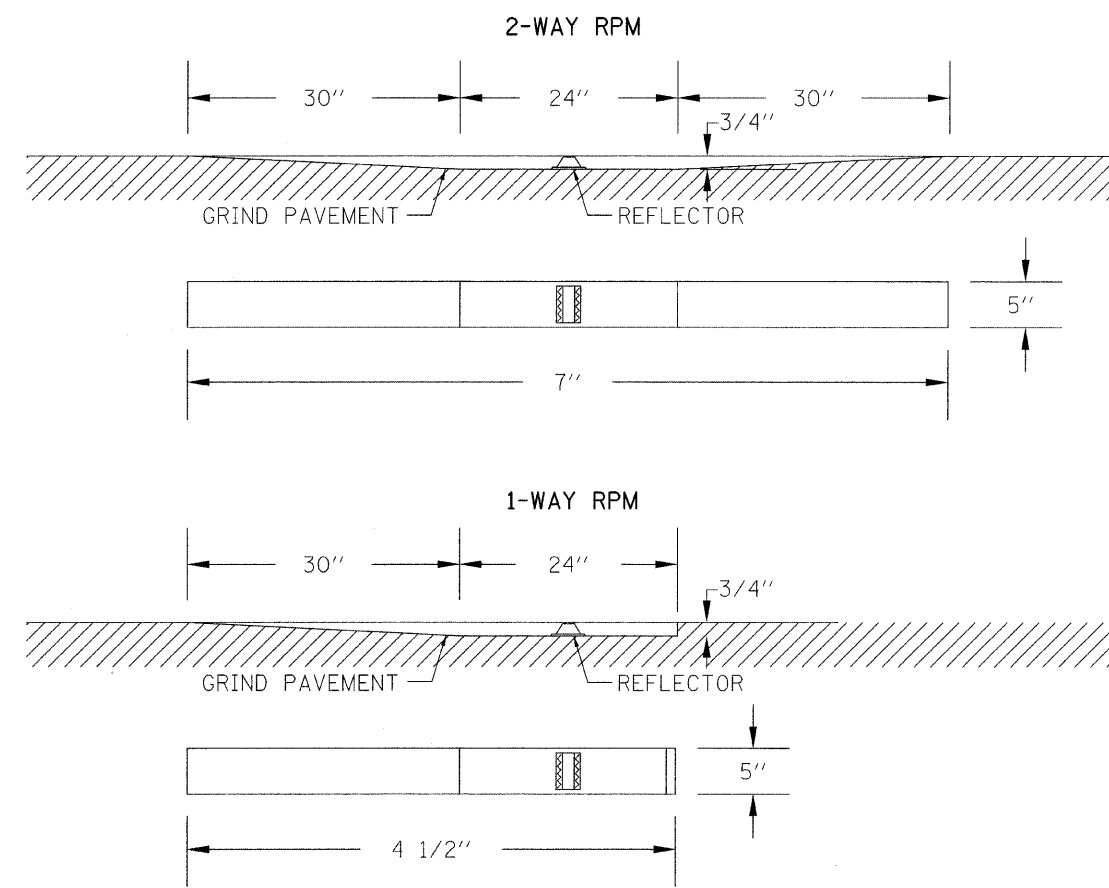
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CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	61
DETAILS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	M-8003(569)	

RECESSED REFLECTIVE PAVEMENT MARKER



- GENERAL NOTES:
1. BOTH THE TRENCH AND DRAIN TILE SHALL BE WRAPPED WITH NON-WOVEN GEOTEXTILE FILTER FABRIC.
  2. WASHED AGGREGATE SHALL BE PLACED AROUND THE DRAIN TILE.
  3. HOLE SHALL BE DRILLED INTO STRUCTURE.
  4. HYDRAULIC CEMENT SHALL BE PLACED AROUND THE PIPE TO SEAL THE OPENING, BOTH INSIDE AND OUTSIDE THE STRUCTURE.

PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"

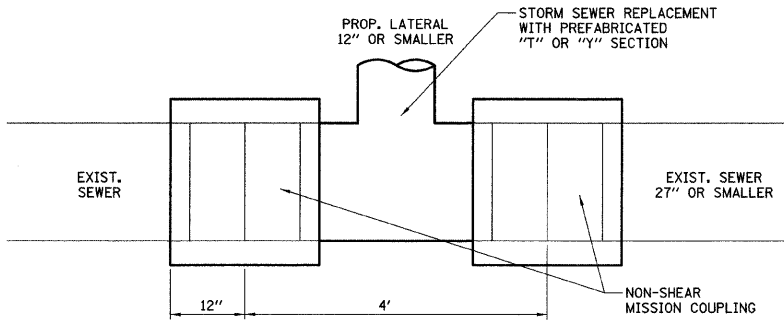
ILLINOIS DEPARTMENT OF TRANSPORTATION

ROADWAY DETAILS

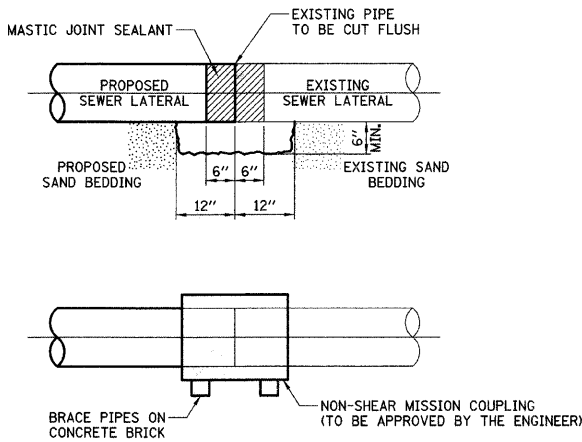
DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

NOT TO SCALE

CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	62
DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-800315691				



DETAIL A  
LATERAL CONNECTION TO EXISTING  
SEWER OF 27" OR SMALLER



CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" X 6" DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. INSTALL MISSION COUPLING.
5. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
6. SUPPORT EACH PIPE END WITH CONCRETE BRICK.

DETAIL B  
NON-SHEAR MISSION COUPLING  
NOT TO SCALE

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
- A) PROPOSED STORM SEWER CONNECTIONS TO EXISTING SEWER OF 27" OR SMALLER SEE DETAIL "A" AND "B".
  - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

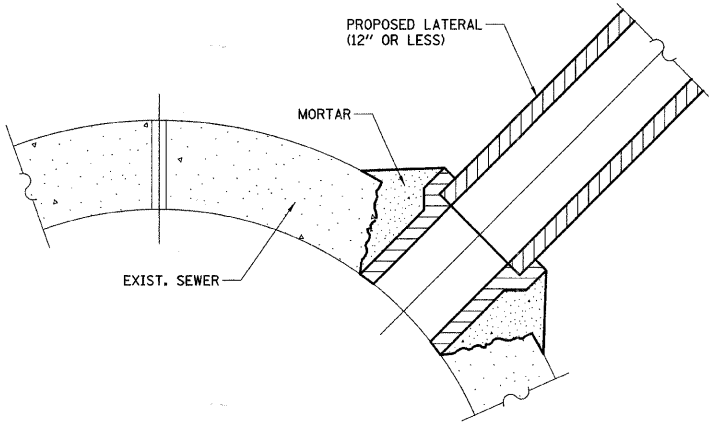
GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE STORM SEWER BEING CONSTRUCTED.

THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS BEING CONSTRUCTED.



DETAIL C  
PROPOSED LATERAL CONNECTION TO  
EXISTING SEWER OF 30" OR LARGER

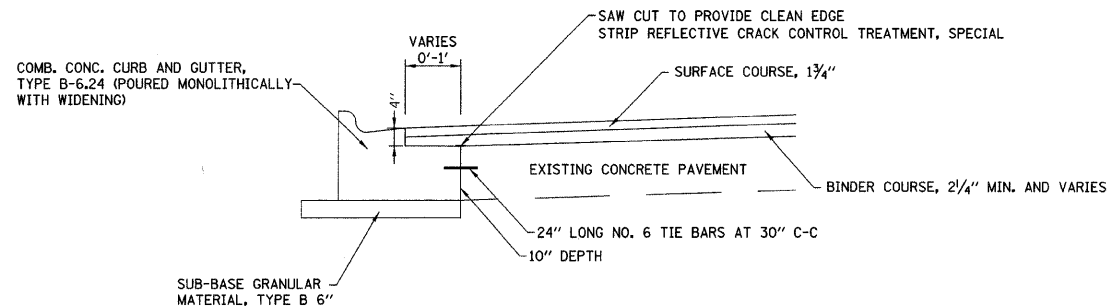
ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAIL OF  
STORM SEWER CONNECTION  
TO EXISTING SEWER

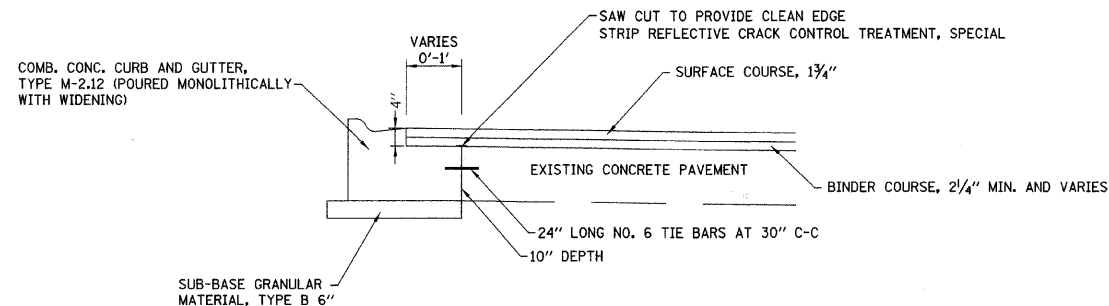
DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

NOT TO SCALE

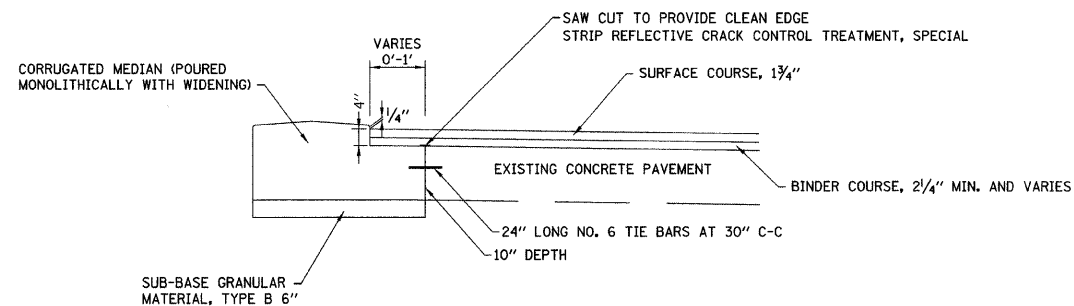
CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	63
DETAILS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	M-8003/569	



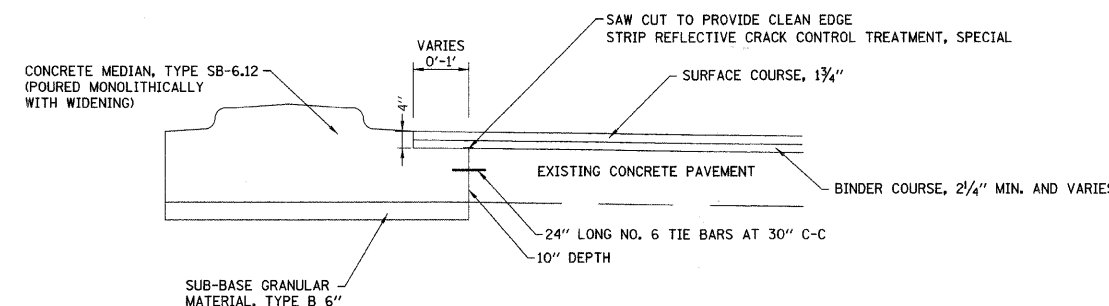
**COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24**  
AT LOCATIONS WHERE WIDENING IS 1' OR LESS



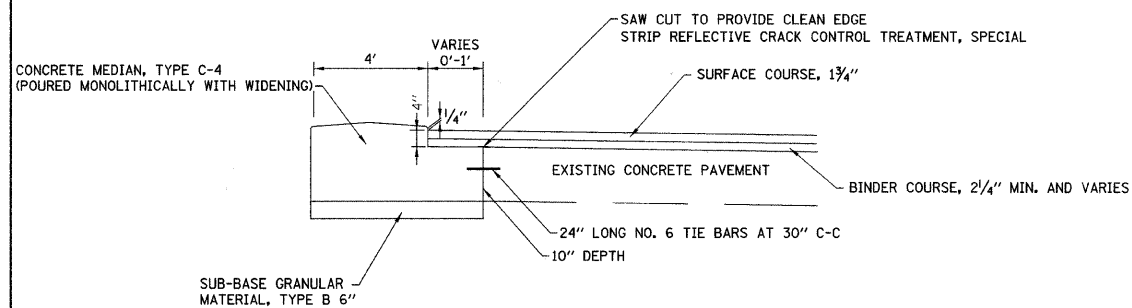
**COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12**  
AT LOCATIONS WHERE WIDENING IS 1' OR LESS



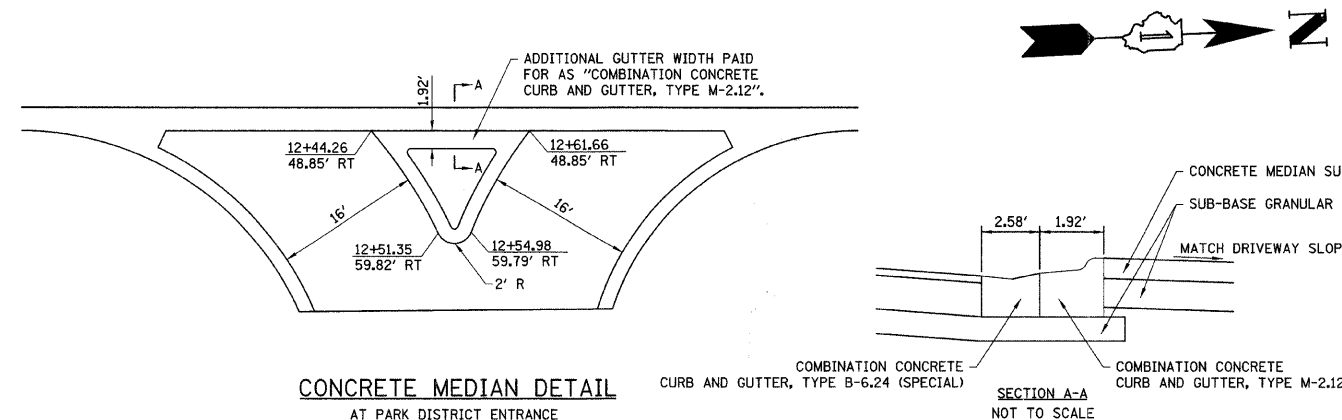
**CORRUGATED MEDIAN**  
AT LOCATIONS WHERE WIDENING IS 1' OR LESS



**CONCRETE MEDIAN, TYPE SB-6.12**  
AT LOCATIONS WHERE WIDENING IS 1' OR LESS



**CONCRETE MEDIAN, TYPE C-4**  
AT LOCATIONS WHERE WIDENING IS 1' OR LESS

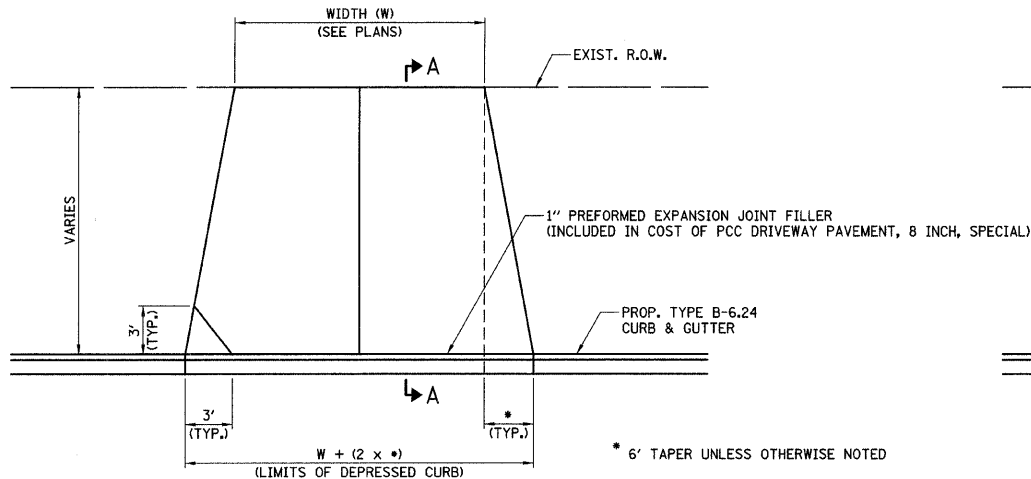


**CONCRETE MEDIAN DETAIL**  
AT PARK DISTRICT ENTRANCE

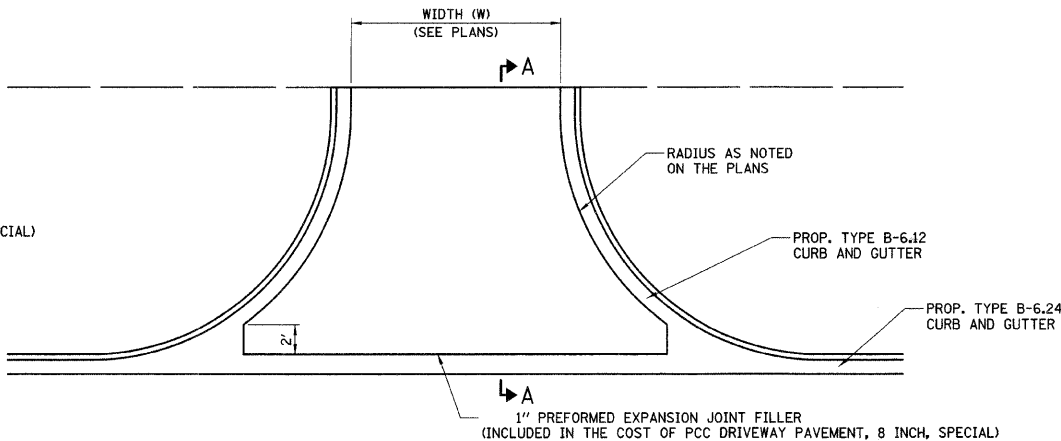
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY DETAILS**

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK  
NOT TO SCALE

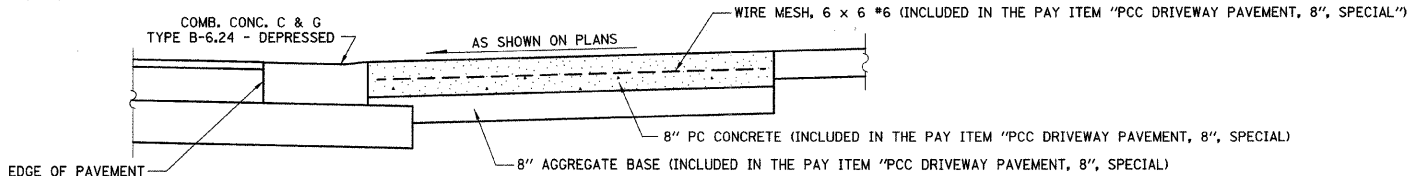
CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	64
DETAILS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-8003(569)		



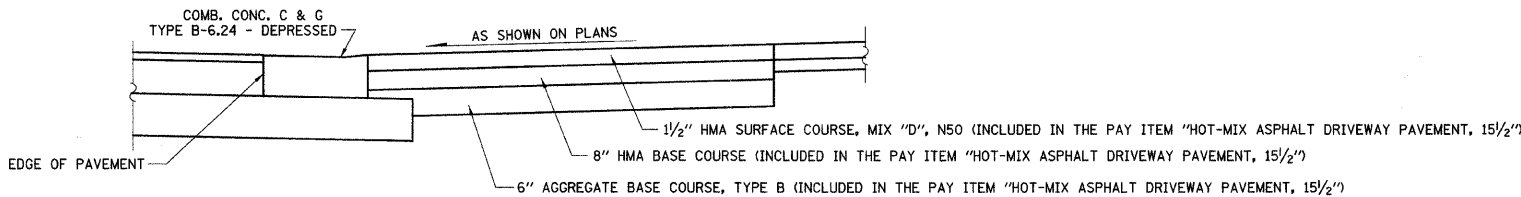
STRAIGHT TAPER PLAN VIEW



RADIUS TAPER PLAN VIEW



SECTION A-A  
FOR PCC DRIVEWAY



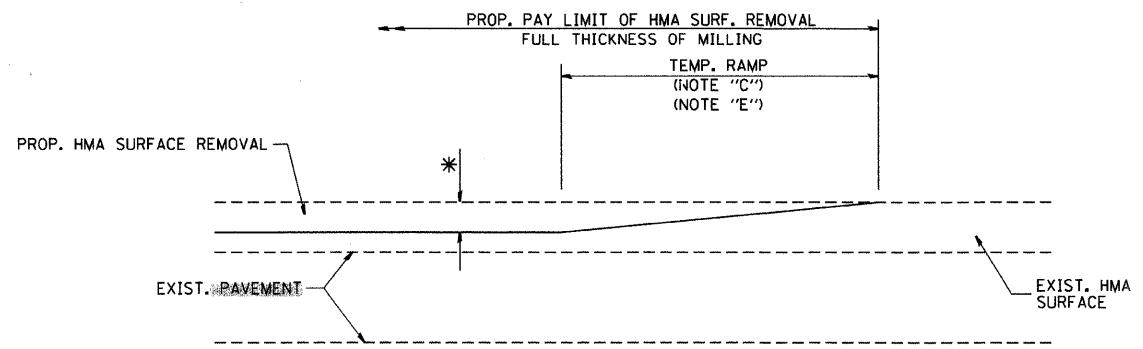
SECTION A-A  
FOR HMA DRIVEWAY

DRIVEWAY DETAIL  
NOT TO SCALE

ILLINOIS DEPARTMENT OF TRANSPORTATION

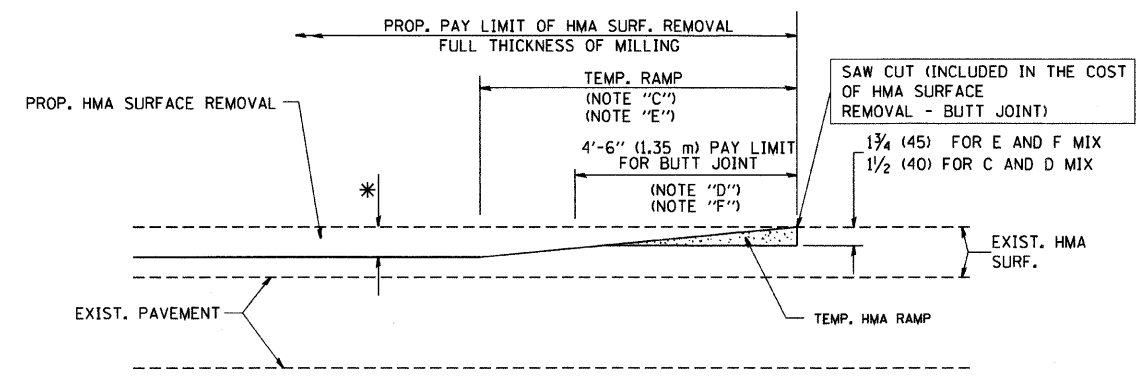
DETAILS

DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

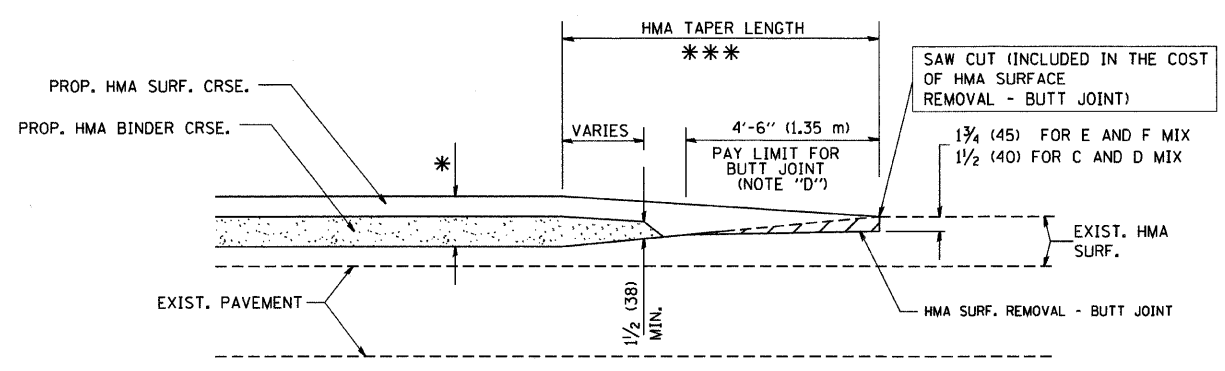
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

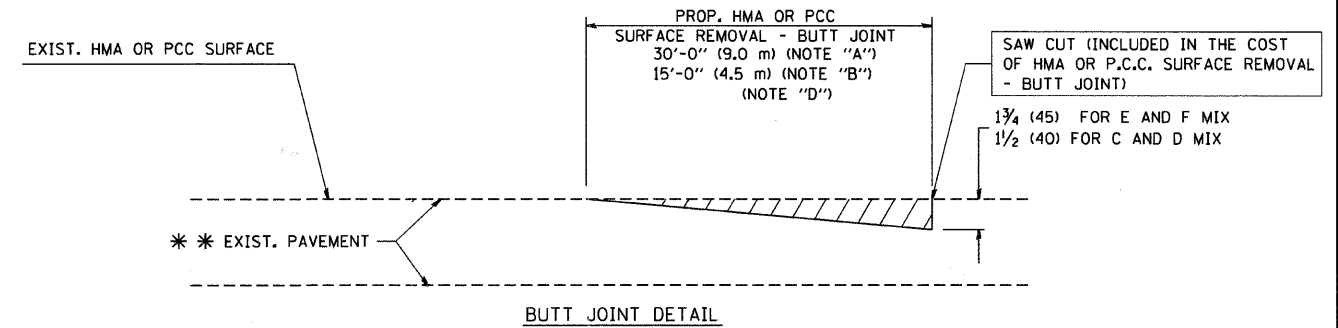
OPTION 2

TYPICAL TEMPORARY RAMP

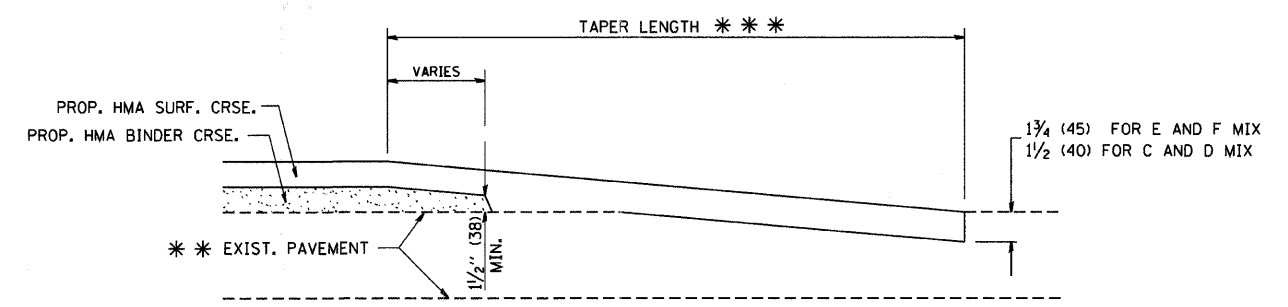


BUTT JOINT AND  
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

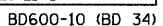
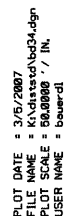
ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER  
DETAILS

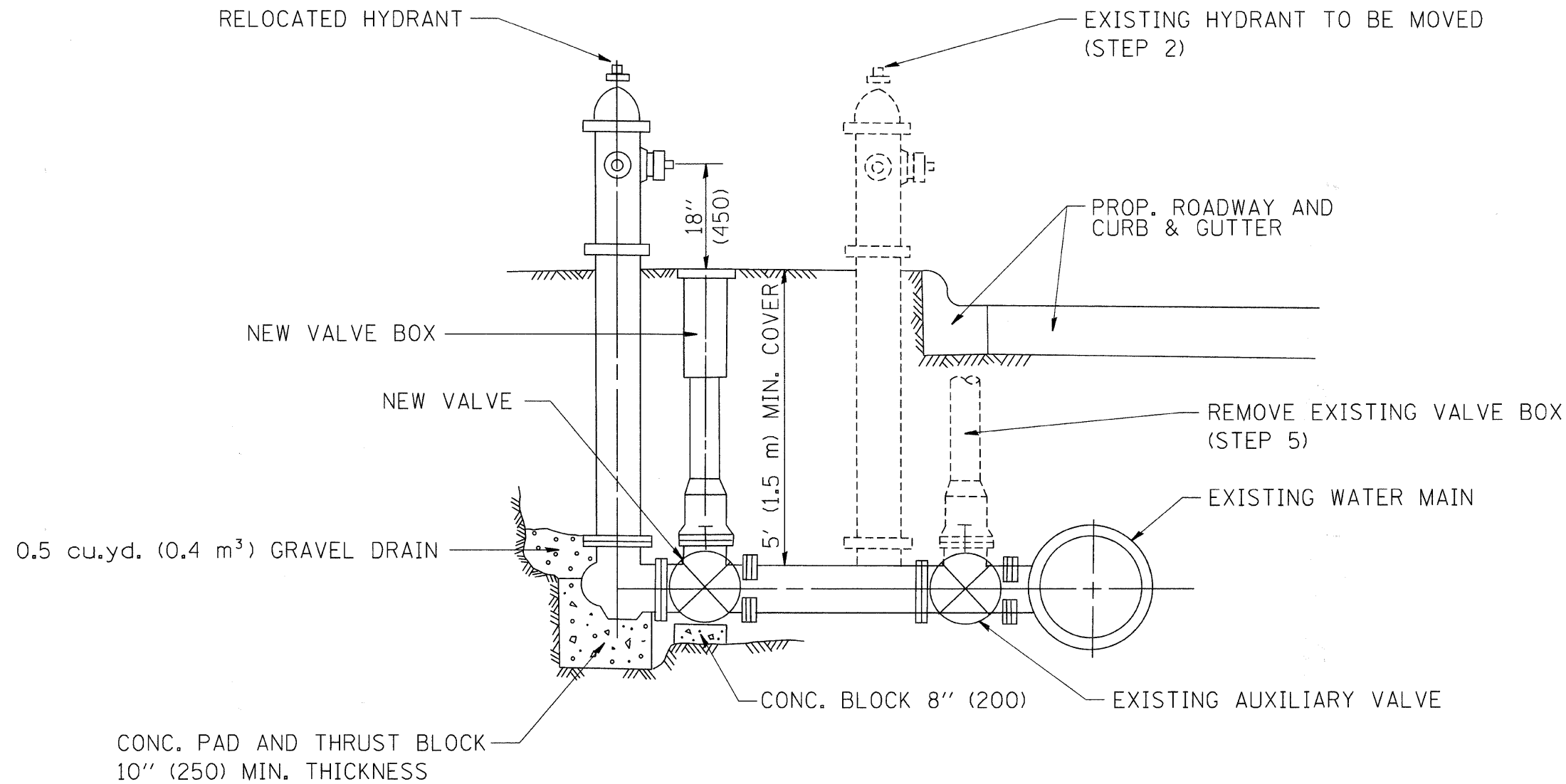
SCALE: VERT. NONE  
HORIZ.

DRAWN BY  
CHECKED BY

BD400-05 (VI-BD32)







SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	09/09/94
R. SHAH	10/25/94

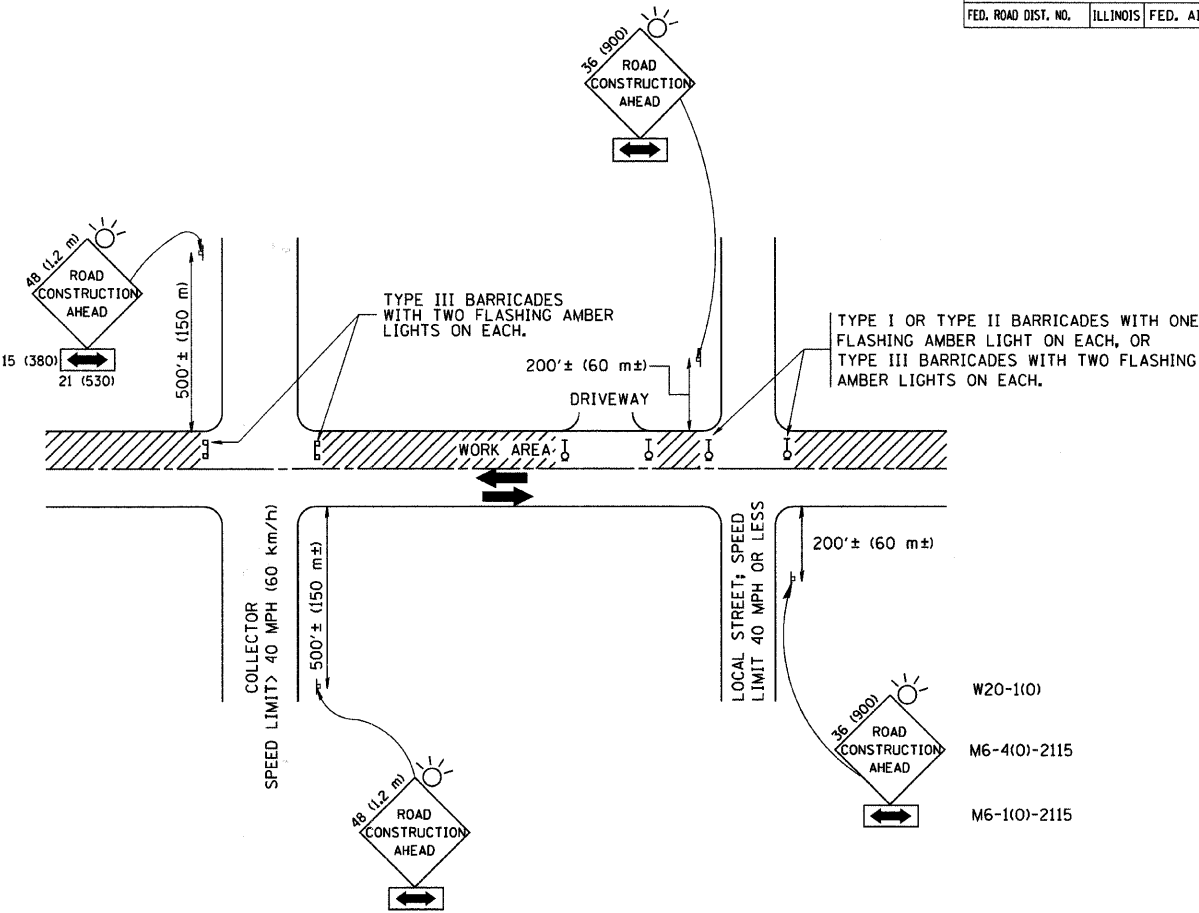
ILLINOIS DEPARTMENT OF TRANSPORTATION

FIRE HYDRANT  
TO BE MOVED

SCALE: VERT. NONE  
HORIZ.

DRAWN BY  
CHECKED BY

BD500-03 (BD-36)



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

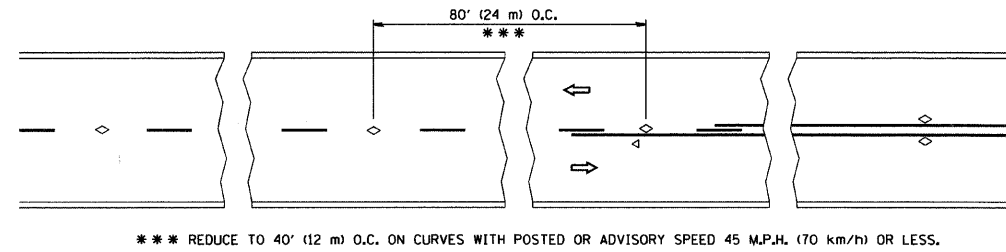
ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL AND PROTECTION  
FOR  
SIDE ROADS, INTERSECTIONS, AND  
DRIVEWAYS

SCALE: NONE

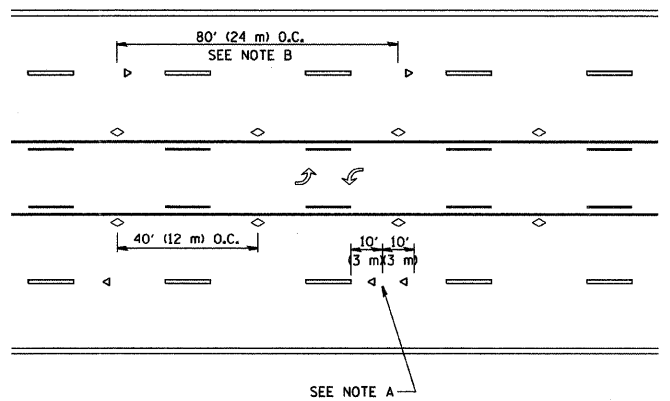
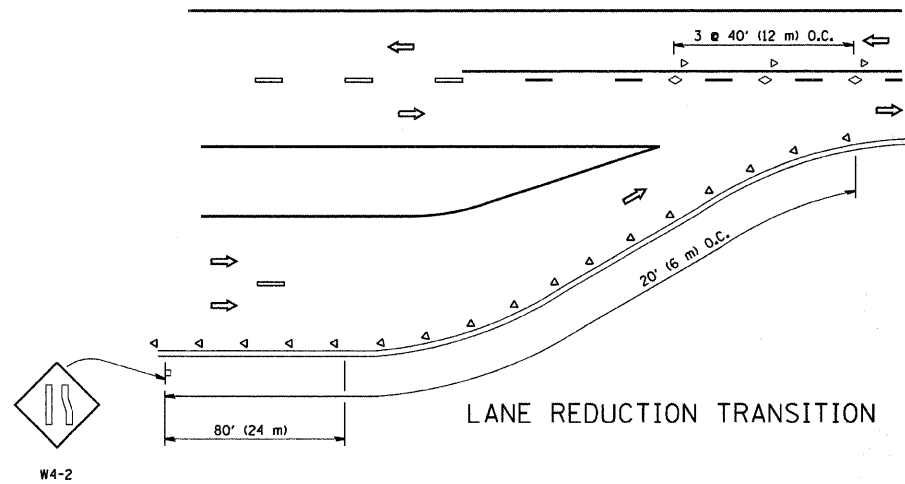
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CHECKED BY

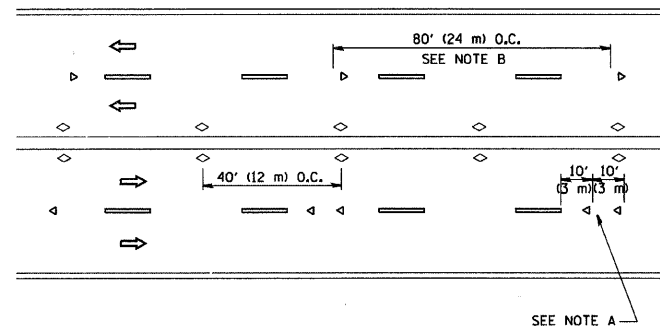
TC-10



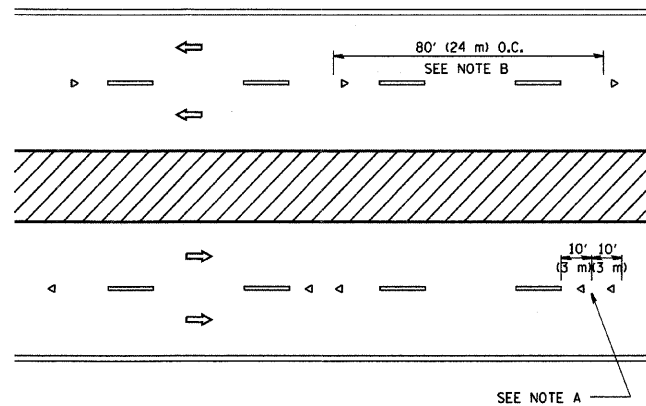
TWO-LANE/TWO-WAY



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

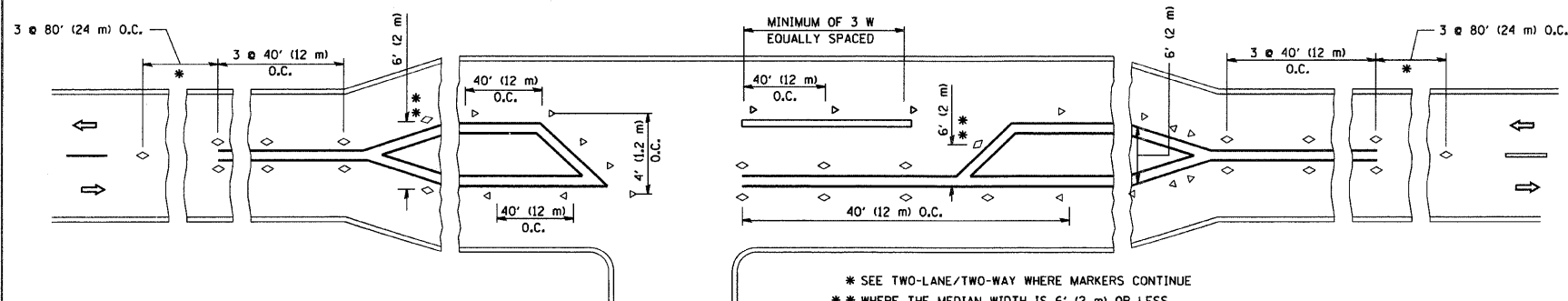
All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS		
NAME	DATE	
T. RAMMACHER	09-19-94	
T. RAMMACHER	03-12-99	
T. RAMMACHER	01-06-00	

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT  
MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE

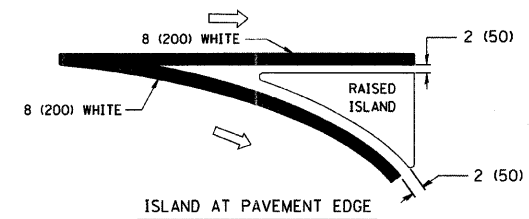
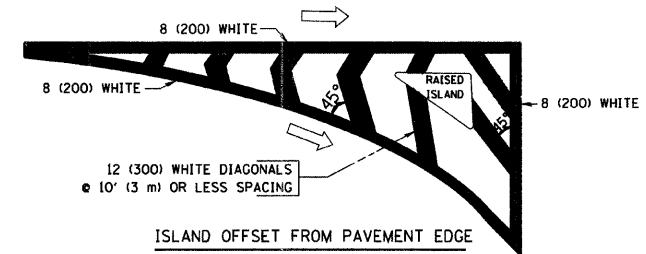
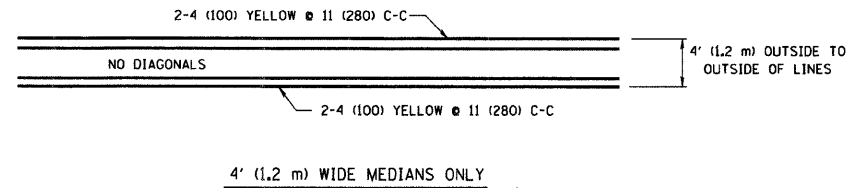
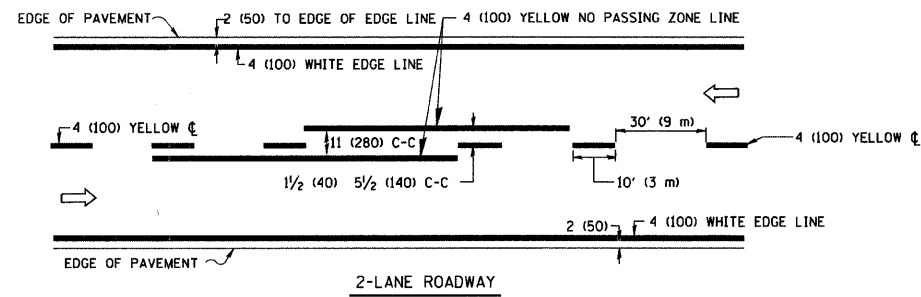
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CHECKED BY  
TC-11



LEFT TURN

- \* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
\*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS  
USE TWO-WAY MARKERS.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346/2626	05-00050-00-CH	COOK/DUPAGE	94	70
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-8003(569)	



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACH	10-27-94
ALEX HOUSE	10-09-96
ALEX HOUSE	10-17-96
T. RAMMACH	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

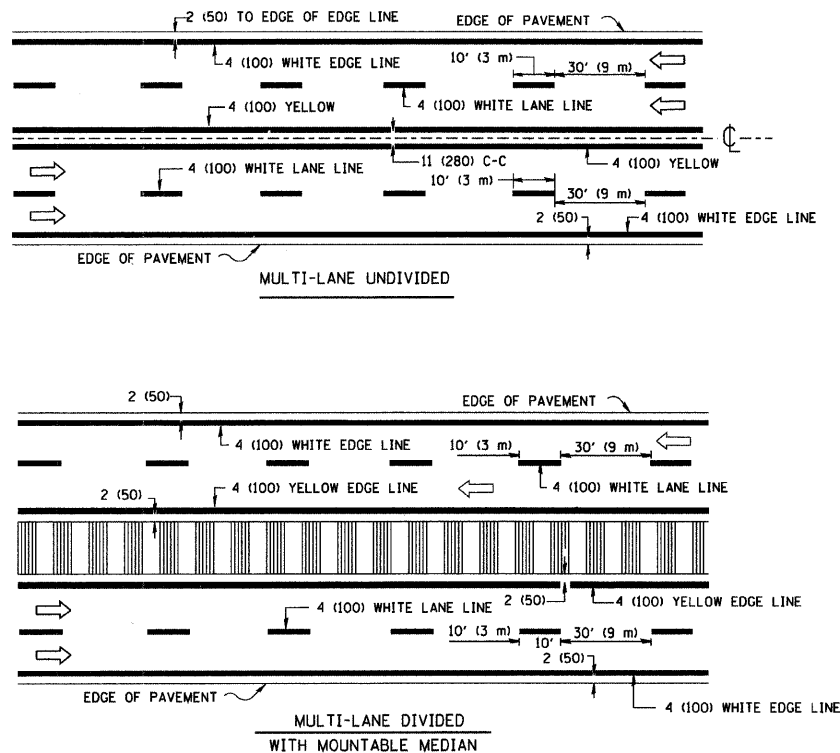
DISTRICT ONE  
TYPICAL PAVEMENT  
MARKINGS

SCALE: NONE

DRAWN BY CADD

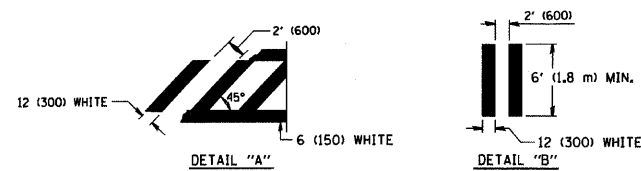
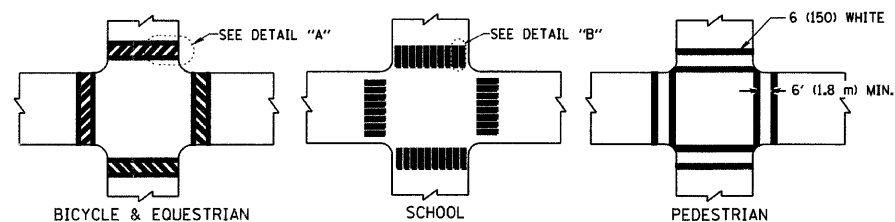
CHECKED BY

TC-13

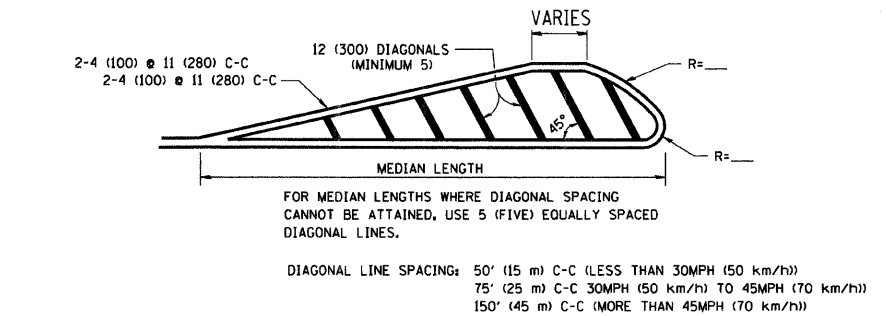


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

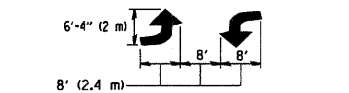
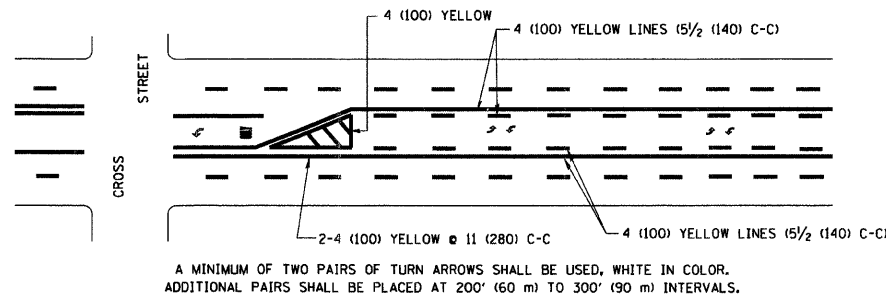
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

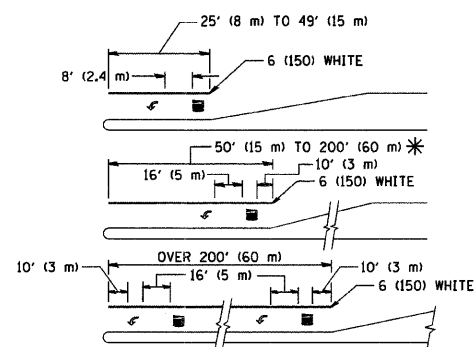


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



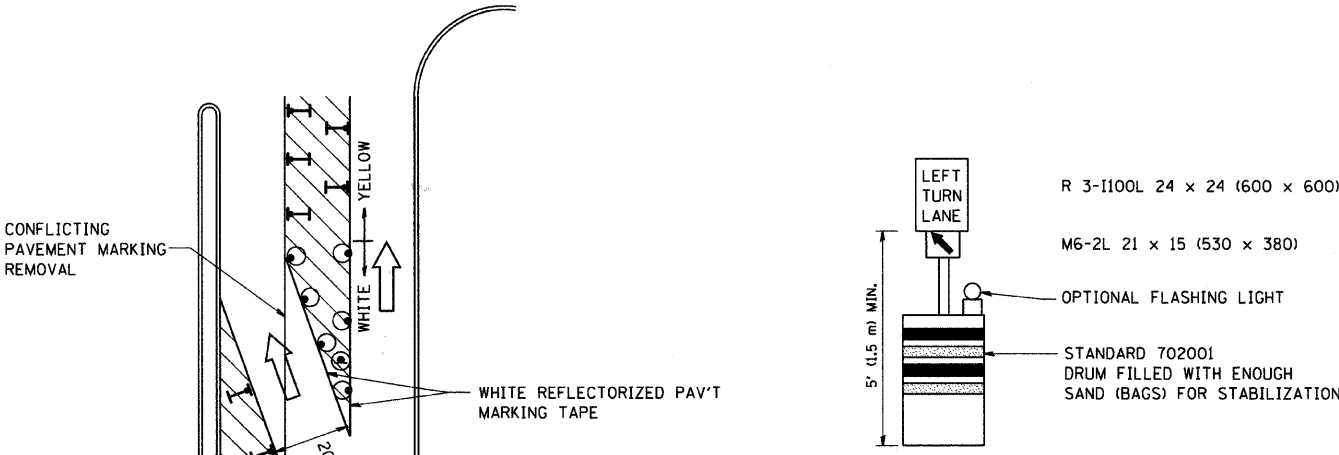
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
\* AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

CONTRACT NO. 63055				
F.A.U. RTE. 1326/ 2626	SECTION 05-00050-00-CH	COUNTY COOK/DUPAGE	TOTAL SHEETS 94	SHEET NO. 71
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT M-8003(669)		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

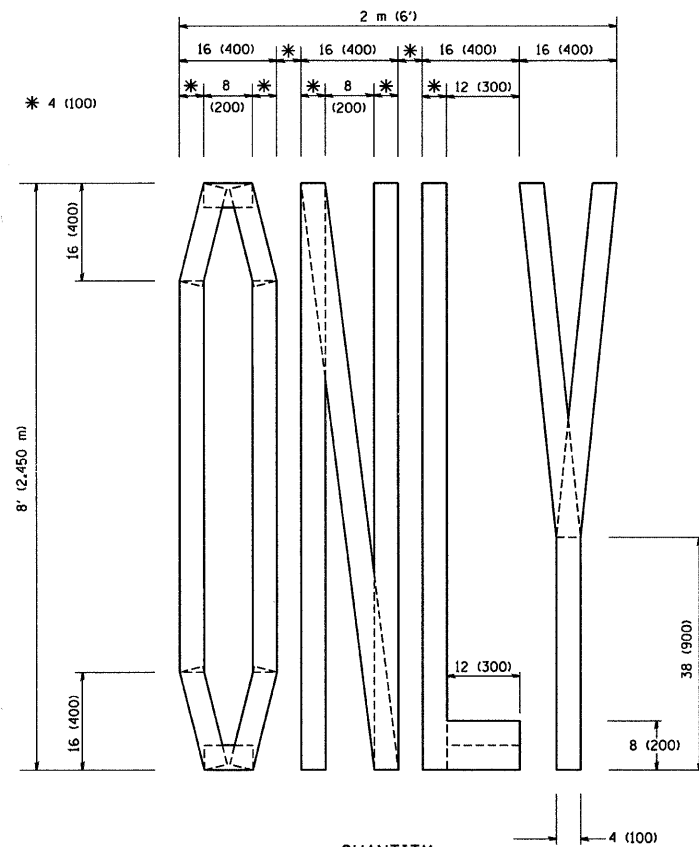
ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION  
AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

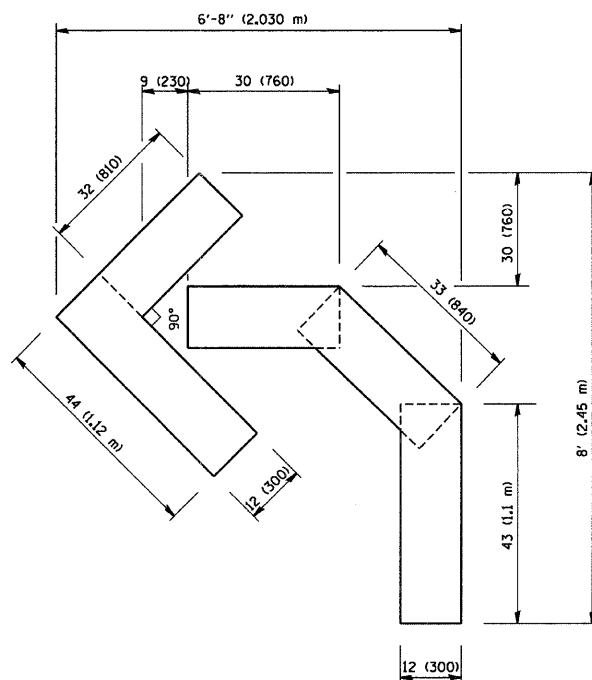
SCALE: NONE

DRAWN BY  
CHECKED BY LHA  
TC-14

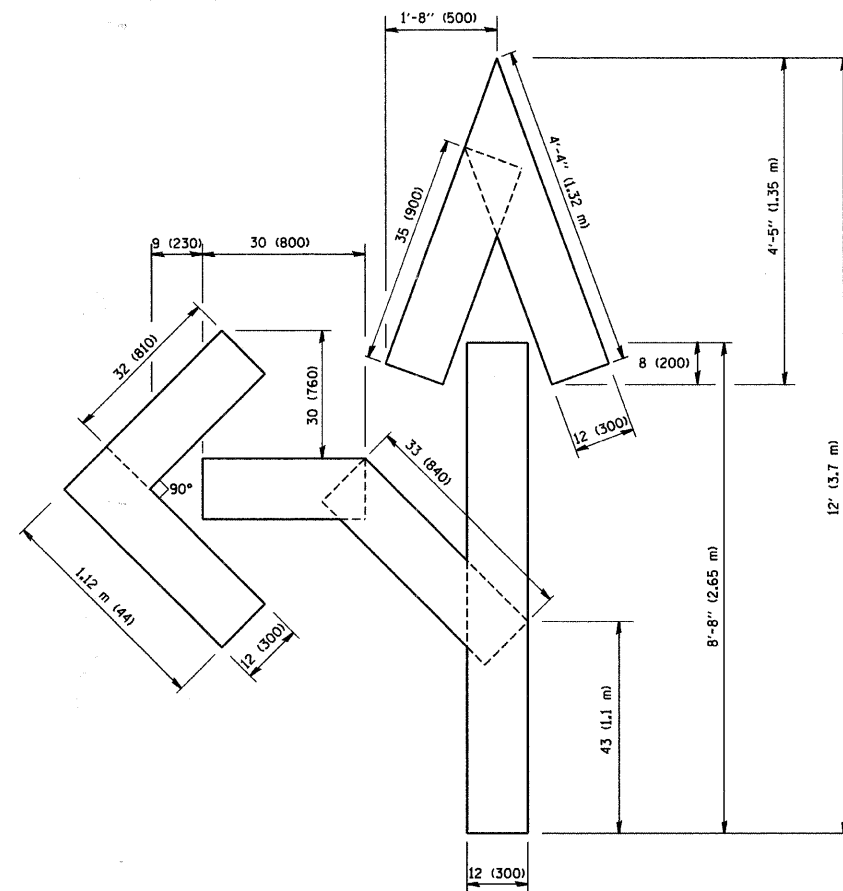
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346/ 2626	05-00050-00-CH	COOK/DUPAGE	94	72
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	
M-8003(569)				



QUANTITY  
4 (100) LINE = 64.1 ft. (19.7 m)  
21.1 sq. ft. (1.97 sq. m)



QUANTITY  
4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.39 sq. m)



QUANTITY  
4 (100) LINE = 82.5 ft. (25.3 m)  
27.5 sq. ft. (2.53 sq. m)

All dimensions are in inches (millimeters)  
unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING  
LETTERS AND SYMBOLS  
FOR TRAFFIC STAGING

SCALE: NONE

DRAWN BY CADD

CHECKED BY

TC-16





1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99
C. JUCIUS	1-31-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

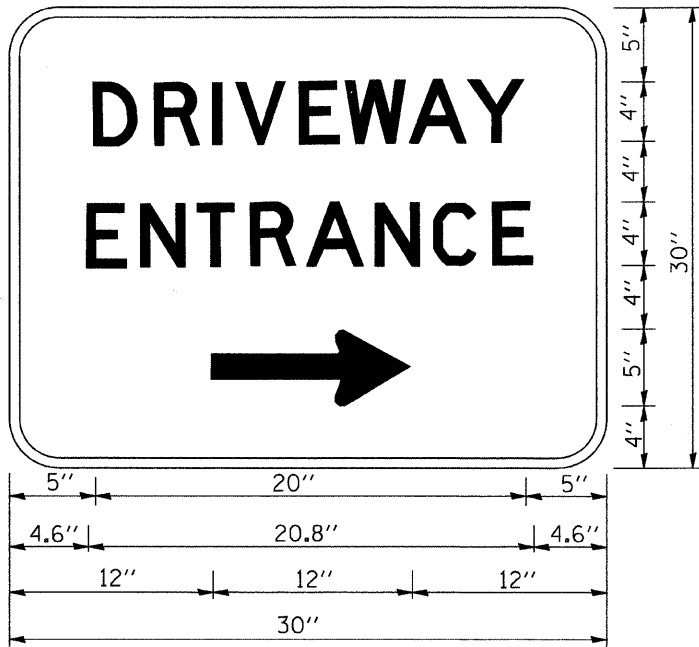
ARTERIAL ROAD  
INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN

CHECKED BY

TC22



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)  
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

REVISIONS	
NAME	DATE
C. JUCIUS	02/15/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE  
SIGNING

SCALE: NONE  
DATE

DRAWN BY R.J.H.  
CHECKED BY

County Highway	Fiscal Year	Sheet No.	Total Sheets
		94	75

Section 05-00050-00-CH

NOTES (SUPPLEMENTAL TO I.D.O.T. STANDARDS)

STANDARDS 606101, 606201 AND 606006 - TYPE A GUTTER, TYPE B GUTTER AND OUTLET FOR CURB AND GUTTER

TIE BARS SHALL BE NO. 15 (NO. 5) SPACED AT 750 mm (30 IN.) CENTERS.

CONCRETE INLETS, ENTRANCES AND OUTLETS FOR GUTTER OR COMBINATION CURB AND GUTTER SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR THE PARTICULAR TYPE OF GUTTER OR COMBINATION CURB AND GUTTER SPECIFIED.

STANDARD 606001 - CURB AND COMBINATION CURB AND GUTTER

VARIABLE CURB TABLE

TYPE	A	B	C	D	R
B-V.30 (B-V.12)	300 (12)	50 (2)	125 (5)	VARIABLE	25 (1)
B-V.45 (B-V.18)	450 (18)	50 (2)	125 (5)	VARIABLE	25 (1)
B-V.60 (B-V.24)	600 (24)	50 (2)	125 (5)	VARIABLE	25 (1)

VARIABLE CURB AND GUTTER SHALL BE CONSTRUCTED WITH THE HEIGHT OF CURB VARYING BETWEEN 100 mm (4 IN.) AND 225 mm (9 IN.), AS REQUIRED. THE TOP OF CURB ELEVATION SHALL BE DETERMINED BY THE ENGINEER.

LONGITUDINAL JOINT TIE BARS SHALL BE NO. 15 (NO. 5) SPACED AT 750 mm (30 IN.) CENTERS. AT THE CONTRACTOR'S OPTION, TIE BARS MAY BE BENT AT RIGHT ANGLES AGAINST THE EDGE OF THE CONCRETE PAVEMENT AND STRAIGHTENED INTO FINAL POSITION BEFORE THE CURB AND GUTTER IS PLACED, SUBJECT TO THE APPROVAL OF THE ENGINEER.

WHERE EXISTING CURB AND GUTTER IS TO BE REMOVED AND REPLACED ADJACENT TO EXISTING CONCRETE PAVEMENT REMAINING IN PLACE, THE CONTRACTOR SHALL PERFORM THE REMOVAL SO THAT THE EXISTING TIE BARS CAN BE USED IN TYING THE NEW CURB AND GUTTER TO THE EXISTING PAVEMENT. ANY TIE BARS WHICH ARE DAMAGED OR MISSING SHALL BE REPLACED WITH NO. 20 (NO. 6) TIE BARS, 600 mm (24 INCHES) LONG, EMBEDDED 200 mm (8 INCHES) AT 600 mm (24 INCH) CENTERS IN ACCORDANCE WITH THE ARTICLE 420.10 (B) OF THE STANDARD SPECIFICATIONS. THE COST OF FURNISHING AND INSTALLING THE TIE BARS SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PARTICULAR TYPE OF CURB AND GUTTER SPECIFIED.

THE DEPTH OF THE CURB OR COMBINATION CURB AND GUTTER SHALL BE EQUIVALENT TO THE DEPTH OF THE ADJACENT PAVEMENT, REGARDLESS OF PAVEMENT COMPOSITION.

STANDARD 353001 - PCC BASE WITH HOT MIX ASPHALT BINDER AND SURFACE COURSE

TIE BARS SHALL BE NO. 15 (NO. 5) SPACED AT 750 mm (30 IN.) CENTERS.

STANDARD 420001 - PAVEMENT JOINTS

JOINTS AND REINFORCEMENT BARS SHALL BE PLACED IN ACCORDANCE WITH THE P.C. CONCRETE PAVEMENT DETAIL SHOWN ON THIS SHEET.

LONGITUDINAL CONSTRUCTION JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAIL SHOWN ON THIS SHEET. AT THE CONTRACTOR'S OPTION, TIE BARS MAY BE BENT AT RIGHT ANGLES AGAINST THE TYPE C METAL JOINT AND STRAIGHTENED INTO FINAL POSITION BEFORE THE ADJACENT CONCRETE PAVEMENT POUR, SUBJECT TO THE APPROVAL OF THE ENGINEER.

TIE BARS SHALL BE NO. 15 (NO. 5).

TRANSVERSE SAWED CONTRACTION JOINTS, TRANSVERSE CONSTRUCTION JOINTS AND JOINTS AT CONTRACT TERMINALS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS SHOWN ON THIS SHEET.

STANDARD 420701 - PAVEMENT FABRIC

PAVEMENT FABRIC DEPTH SHALL BE IN ACCORDANCE WITH THE DETAIL SHOWN ON THIS SHEET.

DISREGARD ALL DETAILS AND NOTES REGARDING PAVEMENTS BLOCK-OUTS, AND COMPLY WITH STANDARD 420111-P.C.C. PAVEMENT ROUNDOUTS AT ALL DRAINAGE/UTILITY STRUCTURE LOCATIONS.

STANDARD 442101 - CLASS B PATCHES

IF A SAWED LONGITUDINAL JOINT IS REQUIRED, TIE BARS SHALL BE NO. 15 (NO. 5) SPACED AT 750 mm (30 IN.) CENTERS.

STANDARD 442201 - CLASS C AND D PATCHES

CLASS C PATCHES SHALL BE TIED TO THE EXISTING PAVEMENT ALONG ALL SIDES OF THE PATCH WITH NO. 20 (NO. 6) TIE BARS, 600 mm (24 INCHES) LONG, EMBEDDED 200 mm (8 INCHES) AT 600 mm (24 INCH) CENTERS IN ACCORDANCE WITH ARTICLE 420.10 (B) OF THE STANDARD SPECIFICATIONS. MINIMUM 3 BARS ON EACH SIDE. THE COST OF FURNISHING AND INSTALLING THE TIE BARS SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR CLASS C PATCHES.

ALL GUARDRAIL AND TRAFFIC BARRIER TERMINAL STANDARDS

HOT MIX ASPHALT SHOULDERS 150 mm (6 IN.) SHALL BE PLACED UNDERNEATH THE ENTIRE LENGTH OF ALL PROPOSED GUARDRAIL AND TRAFFIC BARRIER TERMINALS AND EXTEND 0.6 m (2 FT.) BEYOND THE TERMINAL ENDS. THE WIDTH OF THE SHOULDER SHALL EXTEND ONE FOOT BEYOND EITHER SIDE OF THE POSTS FOR THE GUARDRAIL AND TRAFFIC BARRIER TERMINALS AND AS SHOWN ON THE PLANS. EARTH EXCAVATION NECESSARY FOR PLACEMENT OF THE SHOULDER SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT MIX ASPHALT SHOULDER, IL-19.0, N30 (6 IN.).

REVISED: AUG. 2007 (HOT MIX ASPHALT SHOULDERS)  
REVISED: OCT. 2004 (REMOVED SAWED CONTRACT JT. DETAILS)  
REVISED: SEPTEMBER 2004 (20 FT. SAWED CONTRACT, JT.)

REVISED: JUNE 2004 (SAWED CONTRACT, JT.)  
REVISED: DEC. 2002  
REVISED: NOVEMBER 2000

COUNTY OF COOK DEPARTMENT OF HIGHWAYS	
DETAILS AND NOTES FOR CONSTRUCTION OF PAVEMENT AND PAVEMENT APPURTENANCES (SUPPLEMENTAL TO I.D.O.T. STANDARDS)	
COMPUTED	S.M.V.
DRAWN	V.M.
CHECKED	D.J.S.

NOTE:

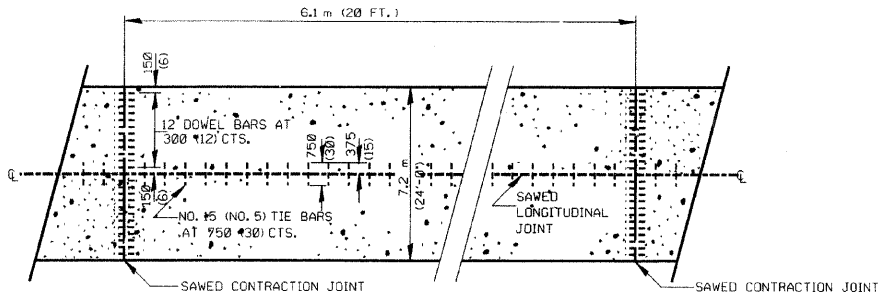
THE INTENT OF THIS STANDARD IS TO REFLECT ADDITIONS OR VARIATIONS TO I.D.O.T. STANDARDS CONTAINED IN THE PLANS. THIS STANDARD SHALL GOVERN OVER THE I.D.O.T. STANDARDS.

DETAIL NOTES

SEE I.D.O.T. STANDARDS FOR DETAILS NOT SHOWN.  
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

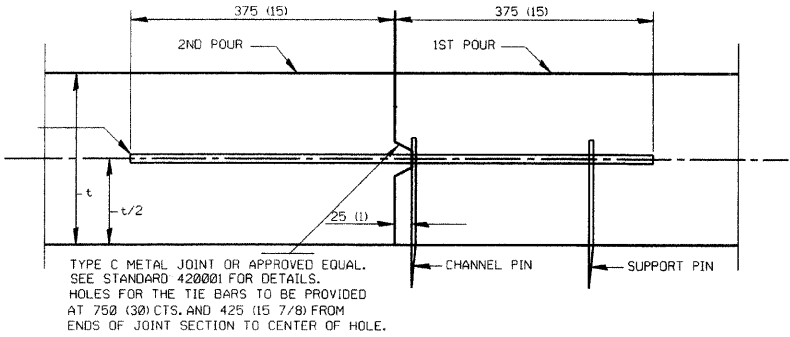
ALL TIE BAR SIZES ARE METRIC (STANDARD)

t = PAVEMENT THICKNESS

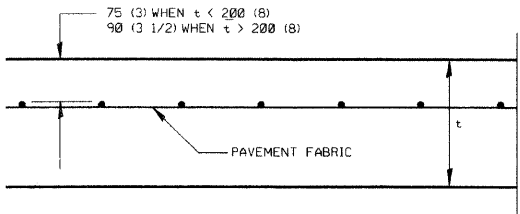


P.C. CONCRETE PAVEMENT DETAIL  
SEE I.D.O.T. STANDARDS 420001 AND 420701 FOR ADDITIONAL DETAILS

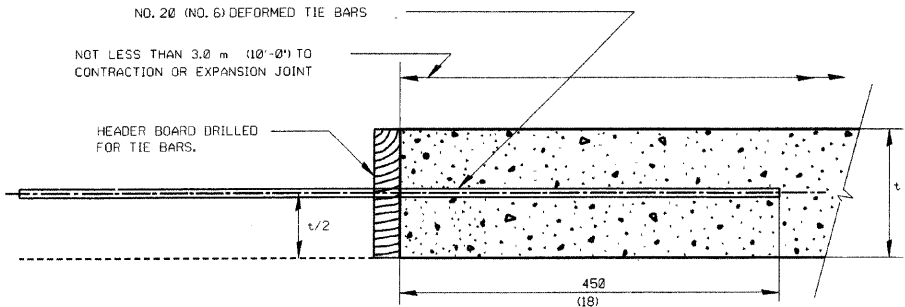
NO. 15 (NO. 5) DEFORMED TIE BAR,  
750 (30) LONG AT 750 (30) CTS.



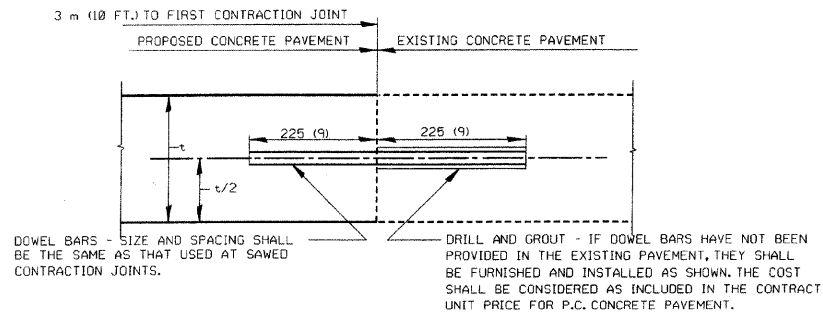
LONGITUDINAL CONSTRUCTION JOINT DETAIL  
TYPE A - INSTALL TIE BARS  
TYPE B - OMIT TIE BARS



PAVEMENT FABRIC DETAIL  
SEE I.D.O.T. STANDARD 420701 FOR ADDITIONAL DETAILS



TRANSVERSE CONSTRUCTION JOINT DETAIL



JOINT DETAIL AT CONTRACT TERMINAL(S)

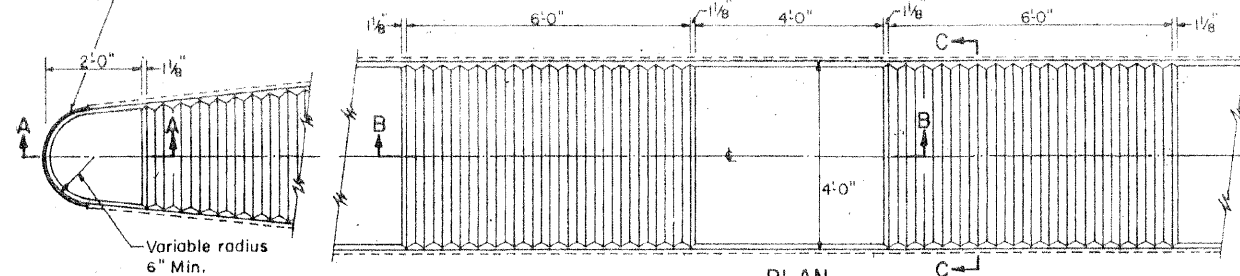
County	Fiscal Year	Sheet No.	Total Sheets
Madison	76	94	

Section 05-00050-00-CH

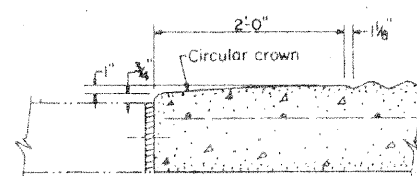
# STANDARD DESIGN CONCRETE MEDIANS

TYPE C-4

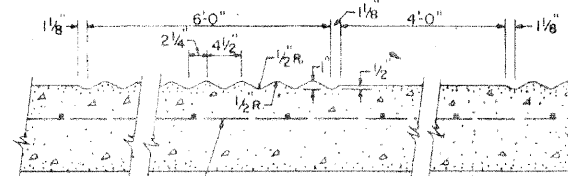
3/4" Preformed expansion joint filler  
between rigid pavement and median  
edge at end radius



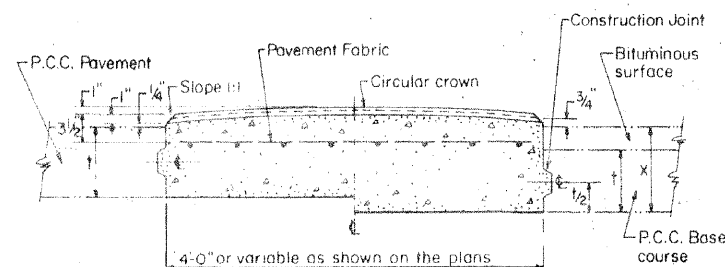
NOTE: OMIT TAPERED SECTION UNLESS SHOWN ON PLANS



SECTION A-A



SECTION B-B



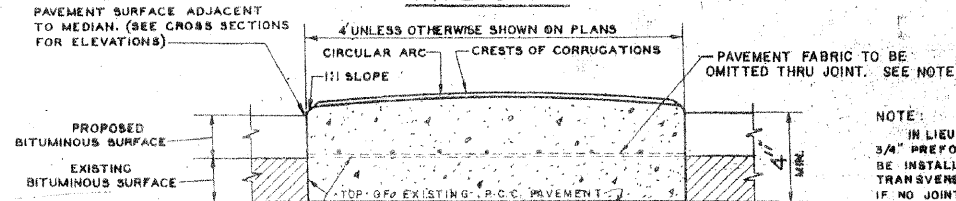
HALF SECTION FOR  
P.C.C. PAVEMENT

HALF SECTION FOR  
P.C.C. BASE COURSE

SECTION C-C

PAVEMENT SURFACE ADJACENT  
TO MEDIAN (SEE CROSS SECTIONS  
FOR ELEVATIONS)

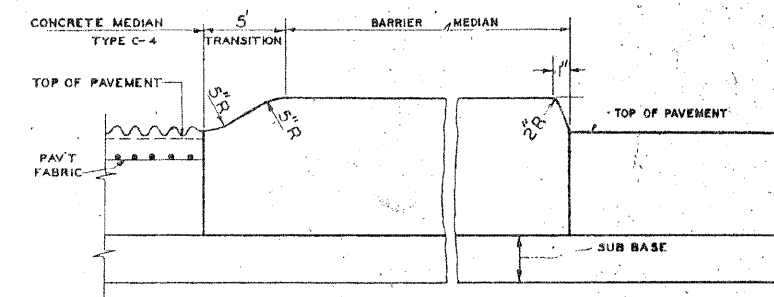
TYPE C-4A



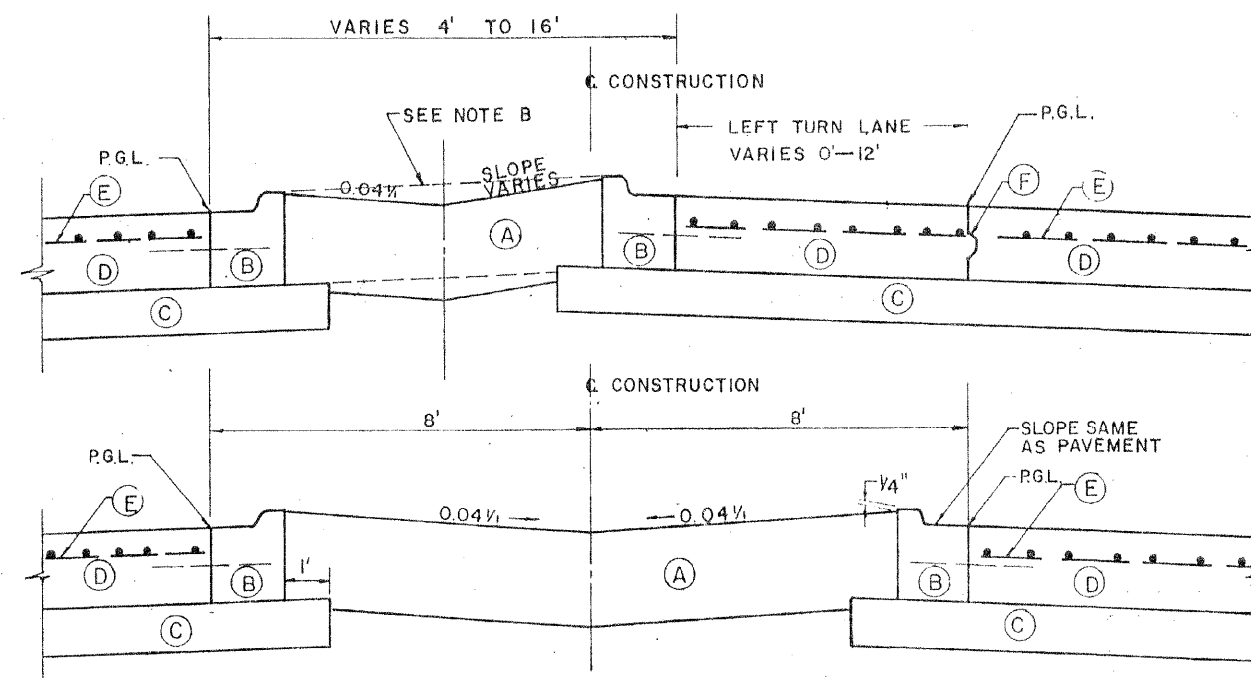
EXISTING BITUMINOUS SURFACE TO BE REMOVED TO  
A WIDTH SUFFICIENT TO ENABLE CONSTRUCTION OF  
THE MEDIAN, OR AS SHOWN ON THE PLANS AND PAID  
FOR AT THE CONTRACT UNIT PRICE BID PER SQUARE  
YARD FOR BITUMINOUS SURFACE REMOVAL AND PER  
LINEAL FOOT FOR CUTTING BITUMINOUS SURFACE.

THE SURFACE CONFIGURATION FOR TYPE C-4A MEDIAN  
IS THE SAME AS TYPE C-4 MEDIAN.

NOTE:  
IN LIEU OF CONTRACTION JOINTS A  
3/4" PREFORMED EXPANSION JOINT FILLER SHALL  
BE INSTALLED AT LOCATIONS COINCIDENT WITH  
TRANSVERSE JOINTS IN THE EXISTING PAVEMENT.  
IF NO JOINTS ARE DISCERNIBLE IN THE EXISTING  
PAVEMENT, JOINTS SHALL BE INSTALLED IN MEDIAN  
AT 20' INTERVALS OR AS DIRECTED BY  
THE ENGINEER



LONGITUDINAL SECTION SHOWING TRANSITION  
BETWEEN TYPE C-4 AND BARRIER MEDIAN  
TRANSITIONAL AREA IS TO BE PAID FOR AS THE TYPE  
OF BARRIER MEDIAN SHOWN ON THE PLANS

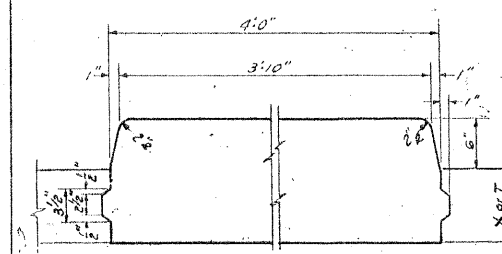


NOTE B: AT LOCATION NOTED ON PLANS  
THE MEDIAN SURFACE SHALL BE A STRAIGHT  
LINE FROM TOP OF CURB TO TOP OF CURB.

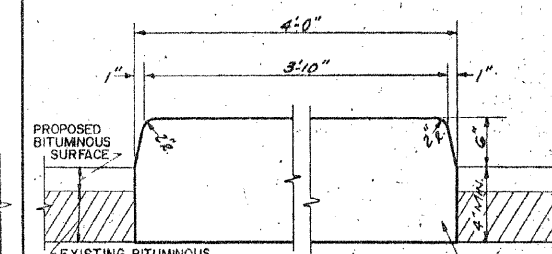
TYPE M-7

- (A) BITUMINOUS SHOULDERS (12 INCH)
- (B) COMBINATION CONCRETE CURB AND GUTTER, M-2.12
- (C) SUB-BASE GRANULAR MATERIAL, TYPE B (6 INCH)
- (D) PORTLAND CEMENT CONCRETE PAVEMENT (10 INCH)
- (E) PAVEMENT FABRIC
- (F) CONSTRUCTION JOINT (FOR TYPE SEE PLANS)

TYPE 6



TYPE 6A



EXISTING BITUMINOUS SURFACE TO BE REMOVED TO  
A WIDTH SUFFICIENT TO ENABLE CONSTRUCTION OF  
THE MEDIAN, OR AS SHOWN ON THE PLANS AND PAID  
FOR AT THE CONTRACT UNIT PRICE BID PER SQUARE  
YARD FOR BITUMINOUS SURFACE REMOVAL AND PER  
LINEAL FOOT FOR CUTTING BITUMINOUS SURFACE.

## GENERAL NOTES

Dimension "t" is equal to the adjacent pavement edge thickness or base course edge thickness.  
Dimension "x" is equal to the adjacent base course edge thickness plus the bituminous surface edge thickness.

The cost of furnishing and placing pavement fabric in the median shall be included in the unit price bid per square foot for CONCRETE MEDIAN, TYPE C-4 and TYPE C-4A respectively.

EXPANSION JOINTS - Shall be of a type similar to that in the adjacent pavement except dowel bars will not be required.

CONTRACTION JOINTS - Shall be constructed and sealed in the same manner as the joints in the adjacent pavement, or shall be constructed with a 3/4" preformed expansion joint filler conforming to the full cross-section of the median. Dowel Bars to be omitted in median joints.

The earth work shall be considered as being incidental to Earth Excavation.

Plan views of medians shown are typical. Median layout and radii shall be as shown on the plans.

Medians may vary in width as required on the plans, but regardless of any width variation, shall be measured and paid for at the contract unit price per square foot for concrete median of the type specified.

The vertical edge dimension for Type C-4 Median shall be 10" when constructed adjacent to flexible pavement. Also, keyway shall be omitted and contraction joints shall be installed at 20' intervals.

Contraction and expansion joints shall be installed in Type C-4 median in prolongation with joints in adjacent P.C.C. Pavement. For median adjacent to bituminous surfaces, contraction joints shall be installed at 20' intervals.

Rev. 7-16-79 W.J.W.

Revised 5-27-70-L.O.S.

Revised 7-8-81 W.J.W.

NOT TO SCALE

COUNTY OF COOK  
DEPARTMENT OF HIGHWAYS

GEORGE W. DUNNE  
PRESIDENT

DETAILS OF CONCRETE MEDIANS

COMPUTED BY: M.K.  
DRAWN BY: W.J.W.  
CHECKED BY: W.J.W.

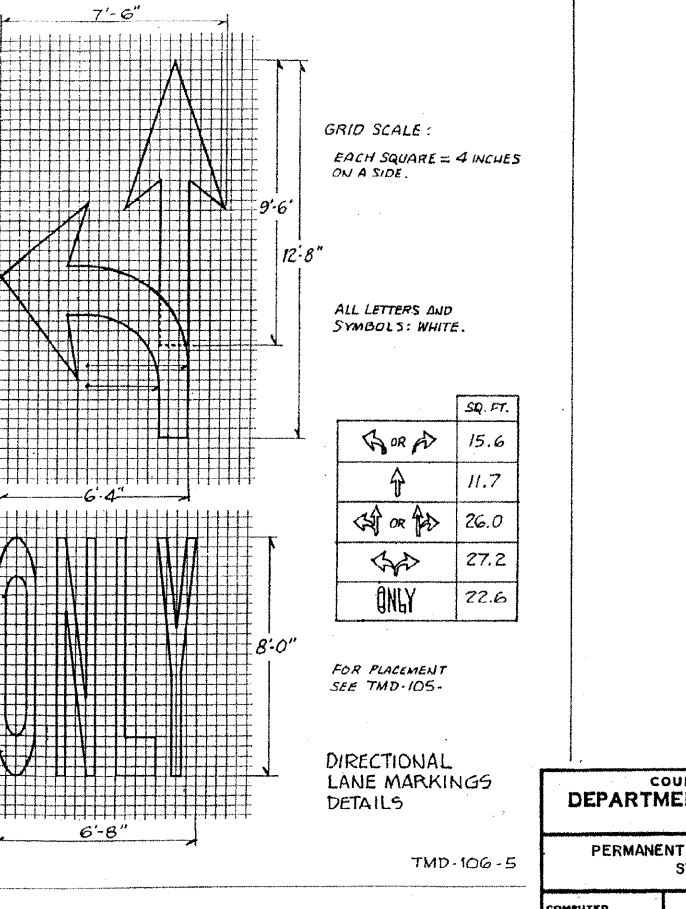
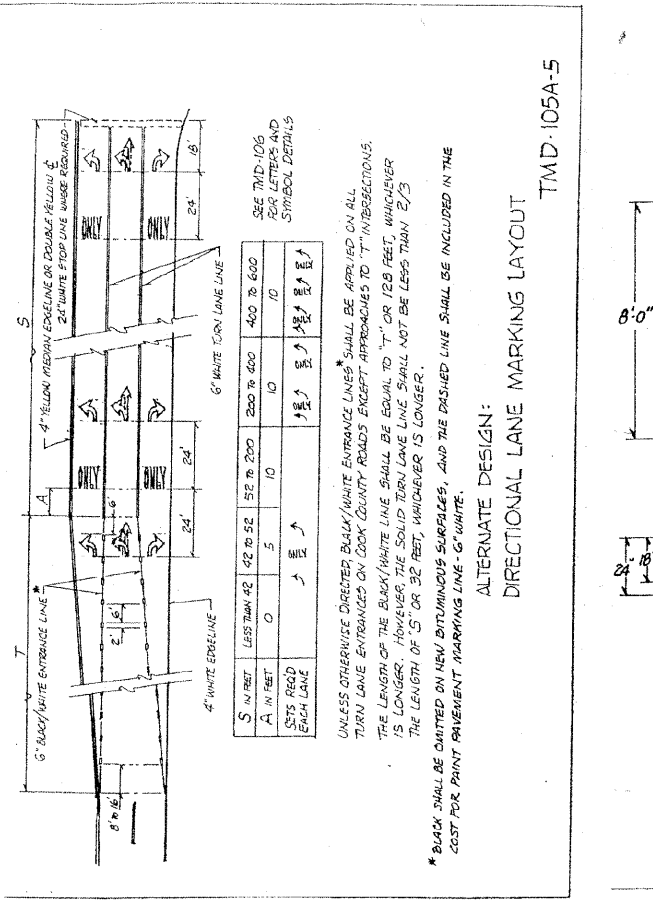
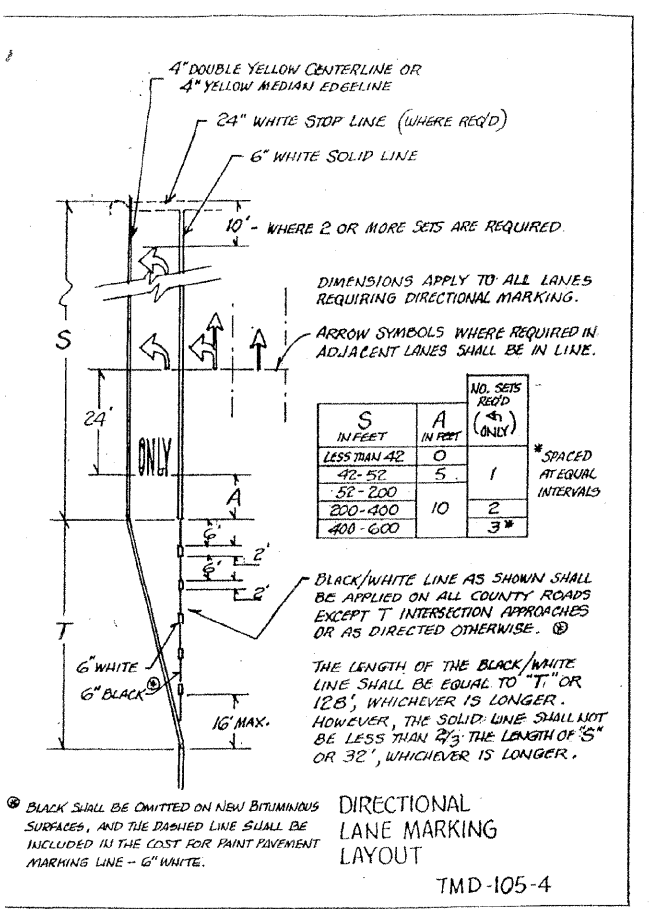
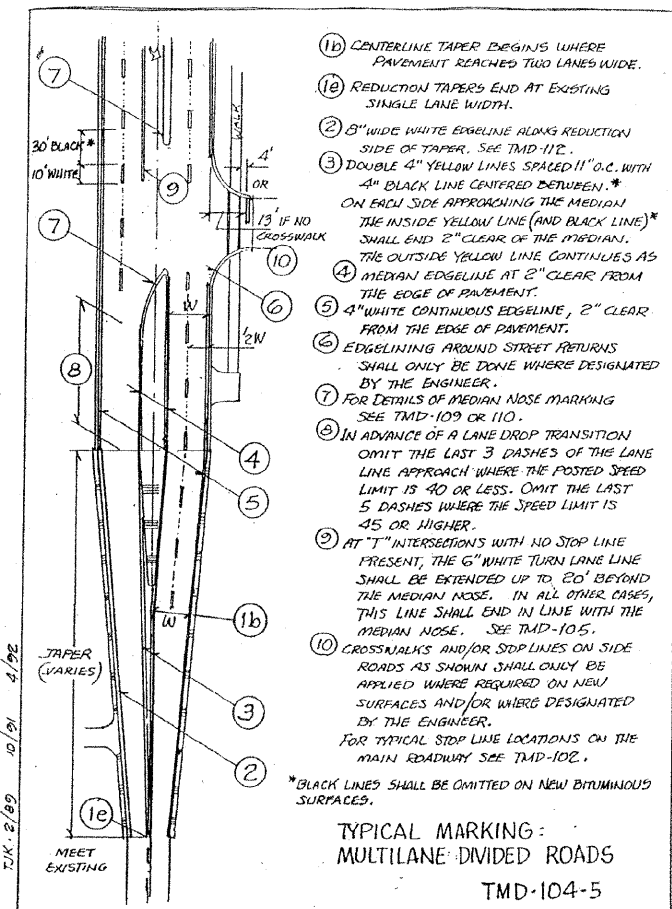
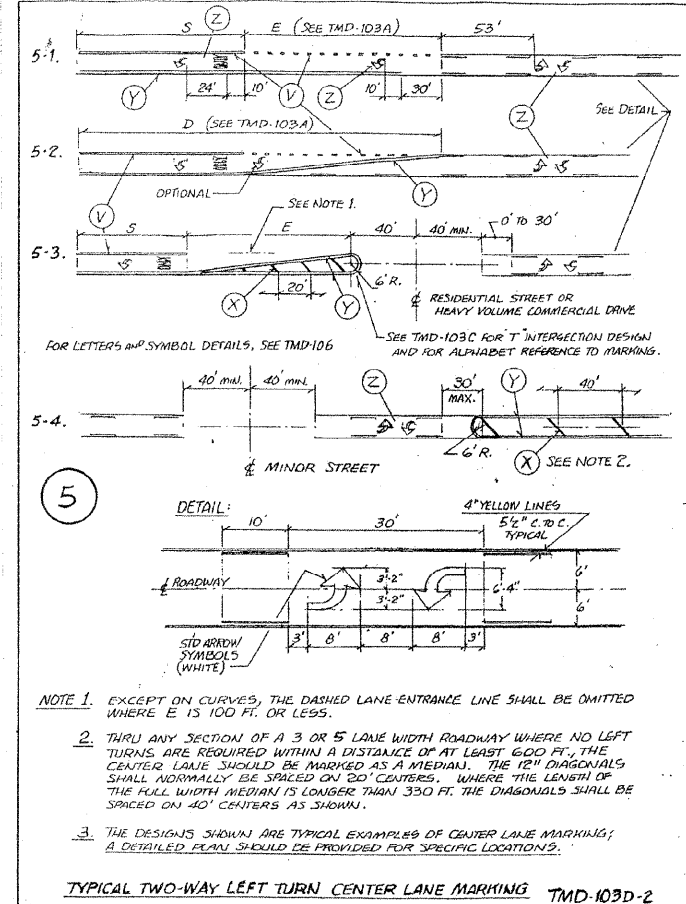
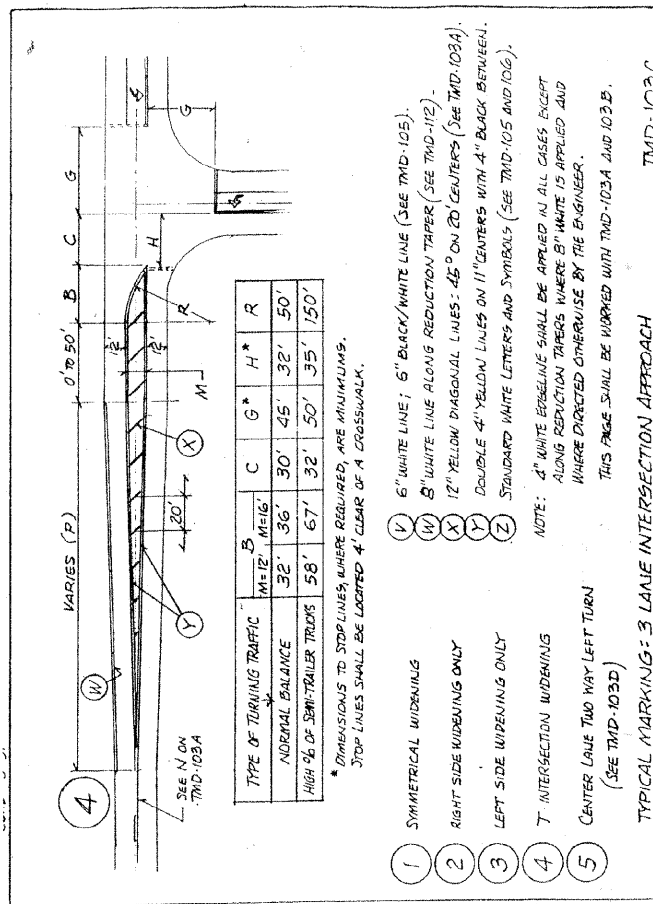
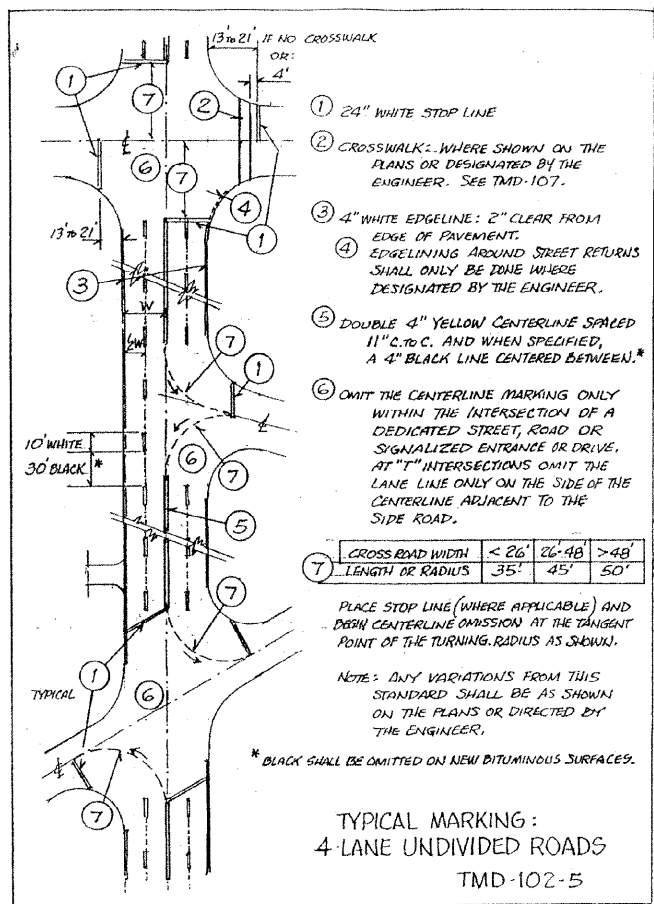
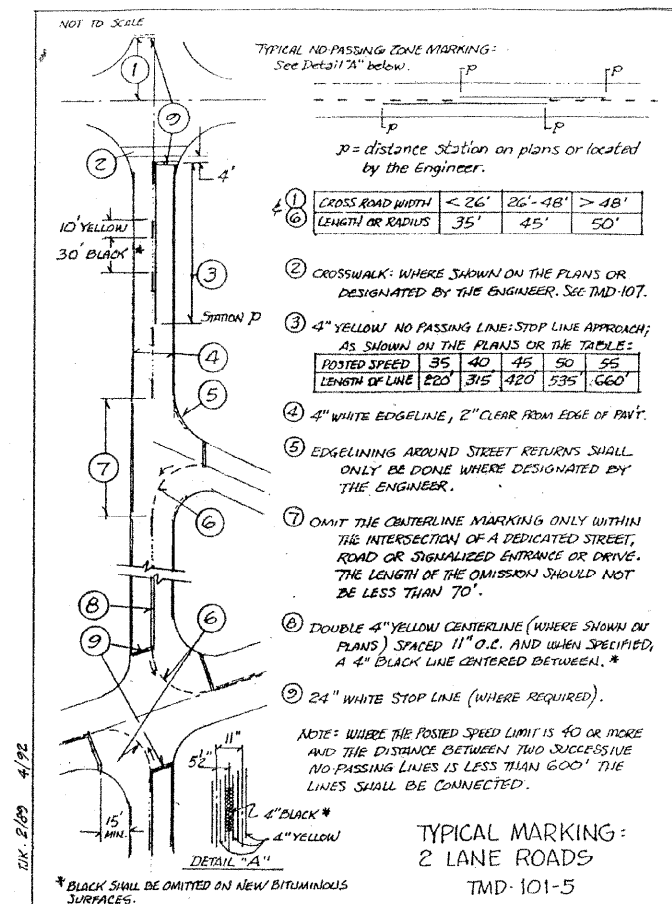
(Full Size)

W.P.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ISSUED 12-12-62	
PASSED May 29 1975		REVISIONS	
APPROVED May 29 1975		W.F.	8-27-68
		W.F.	1-15-69
		W.F.	10-1-69
		D.W.	9-4-73
		D.W.	10-31-74
		D.W.	5-29-75
		D.W.	7-15-77
		Redrawn 8-27-68	

County	Fiscal Year	Sheet No.	Total Sheets
Highway		77	94

Section 05-00050-00-CH



COUNTY OF COOK  
DEPARTMENT OF HIGHWAYS

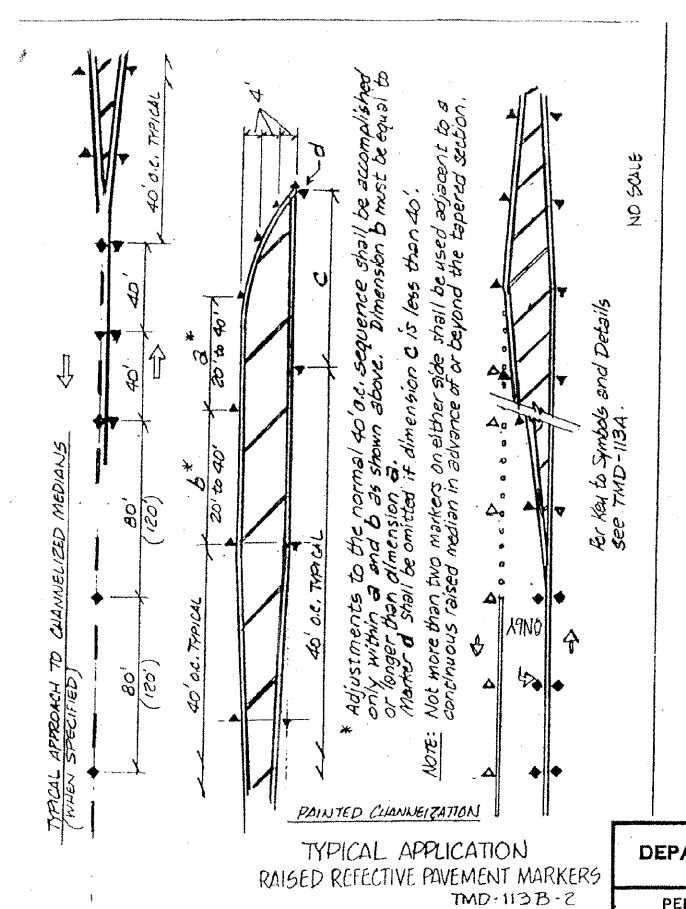
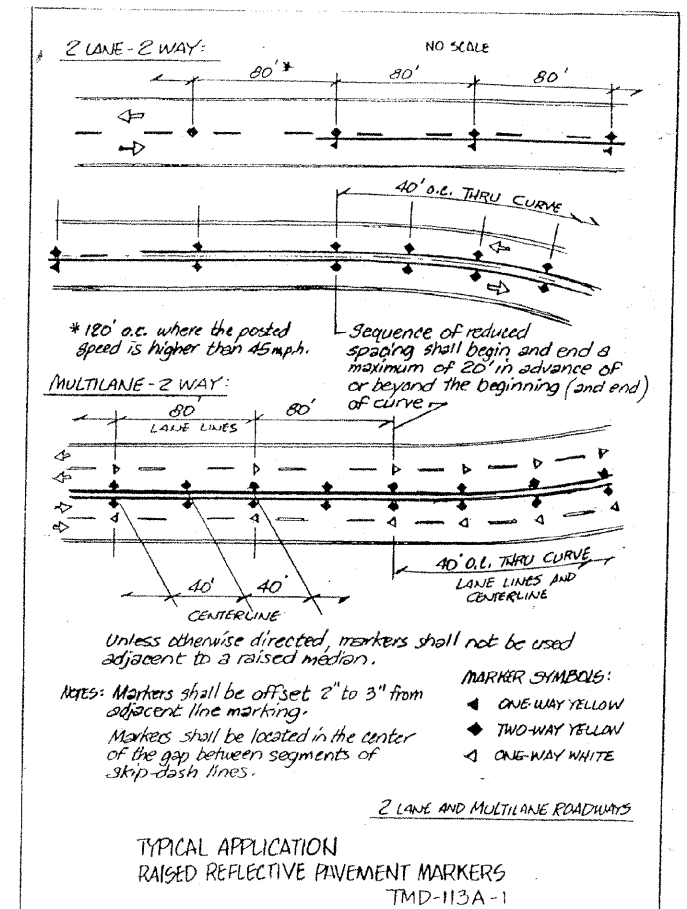
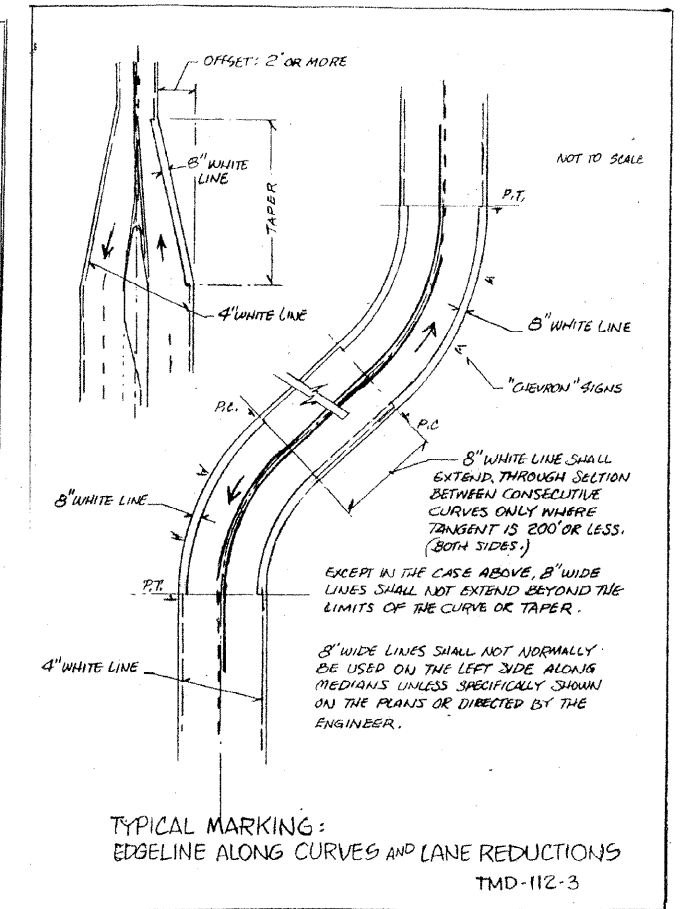
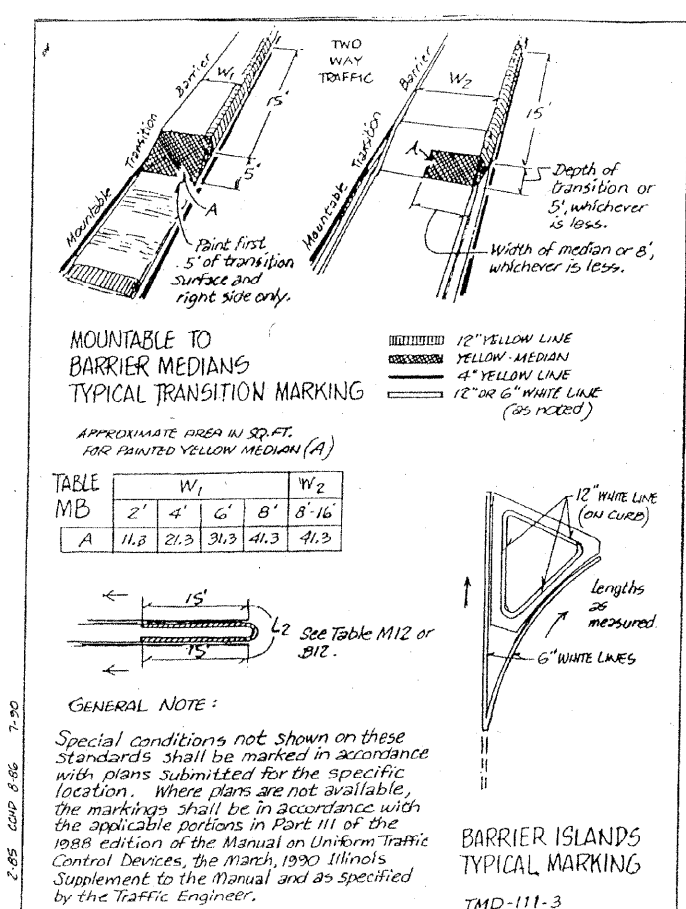
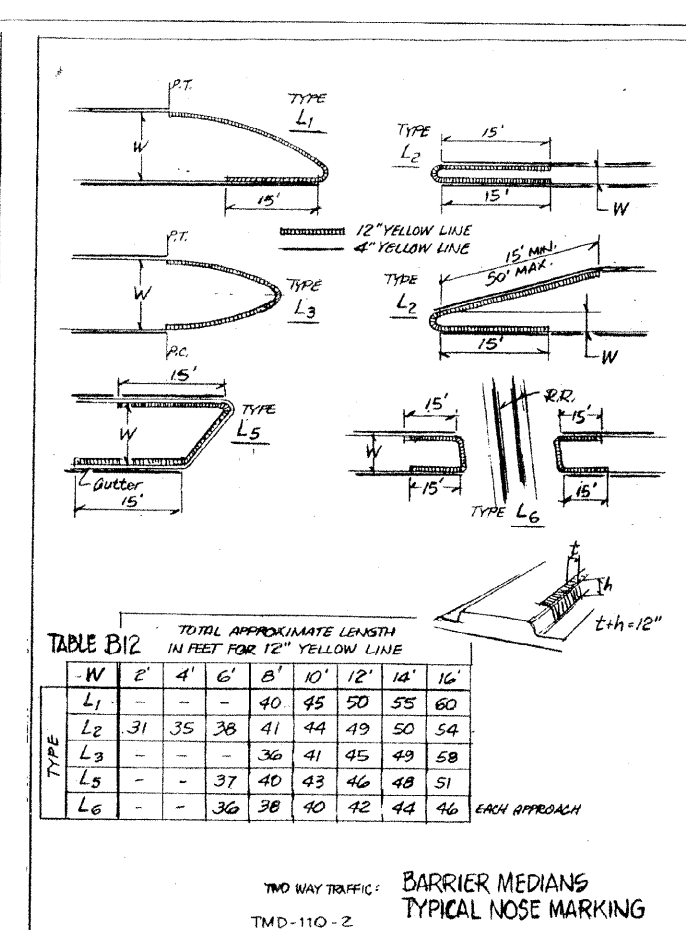
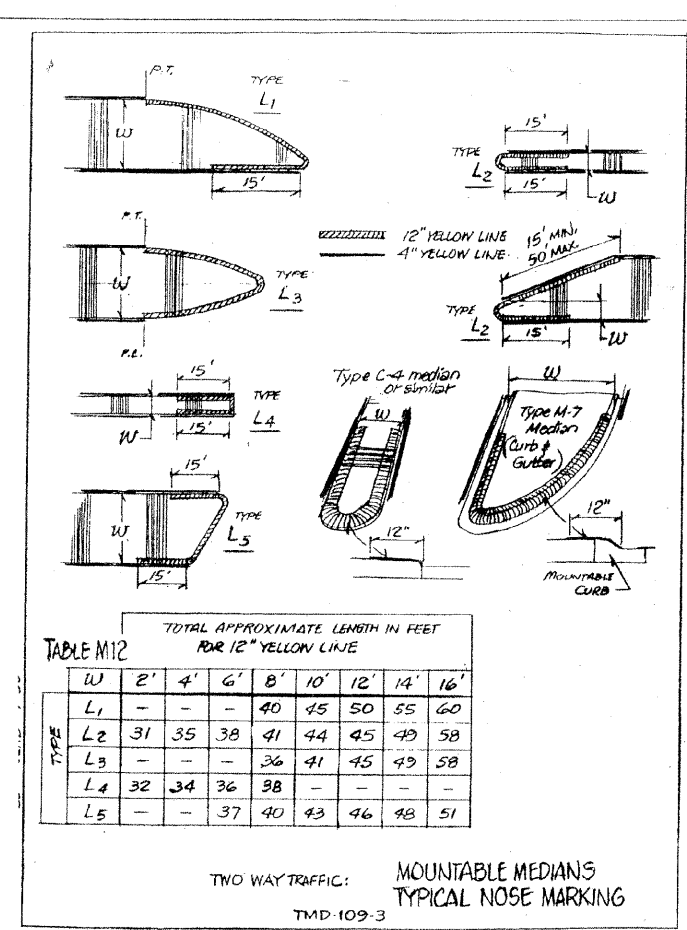
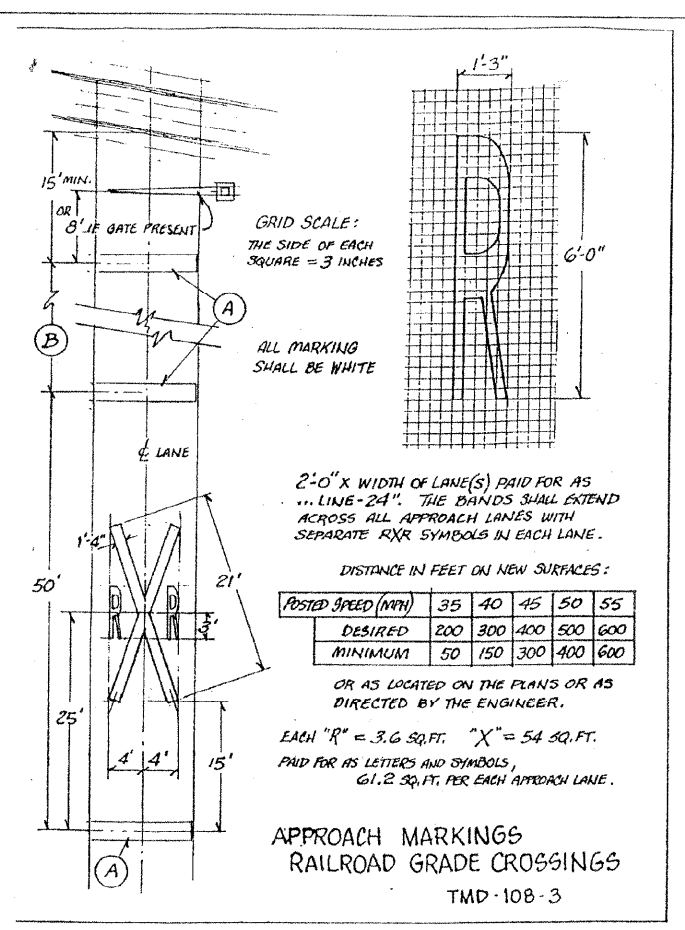
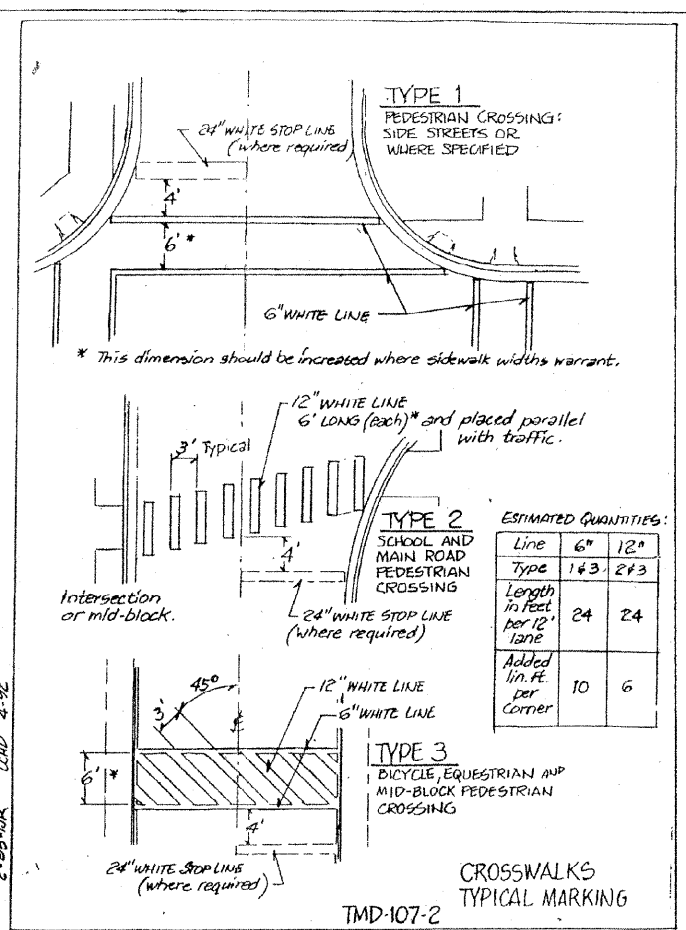
PERMANENT PAVEMENT MARKING STANDARDS  
(SHEET 1 OF 2)

COMPUTED \_\_\_\_\_  
DRAWN \_\_\_\_\_  
CHECKED \_\_\_\_\_

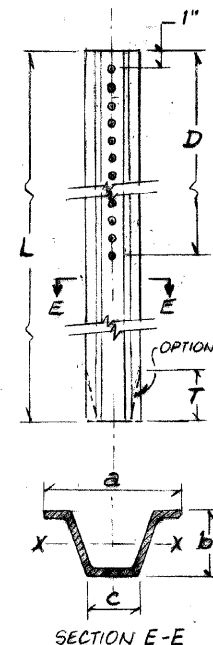
APPROVED \_\_\_\_\_  
DATE \_\_\_\_\_

SEAL OF COUNTY OF COOK









STEEL	TYPE A	TYPE B	TYPE C
a	3 1/2"	3 3/4"	2"
b	1 1/4"	1 1/2"	1 1/2"
c	1 1/2"	1 1/2"	1"
Sx-x IN <sup>3</sup>	0.223	0.341	-
Wt/Lb/FT	2.00	3.00	1.12
D	55 HOLES (MIN)	18 MIN.	-
L	VARIABLE	7.0	-
T	3"	1 1/4"	-

ALUMINUM	TYPE A	TYPE B
a	3 1/2"	4 5/8"
b	1 5/8"	2 1/4"
c	1 7/8"	2 3/8"
Sx-x IN <sup>3</sup>	0.435	0.888
Wt/Lb/FT	0.90	1.30
D	55 HOLES (MIN)	-
L	VARIABLE	-
T	3"	-

ALL HOLES ARE 3/8" DIA. ON 1" CENTERS.  
NO SPICES ALLOWED.  
TYPE C USED FOR DELINEATORS WHEN SPECIFIED ON THE PLAN.

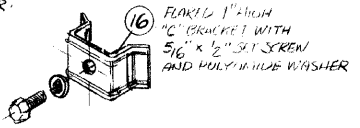
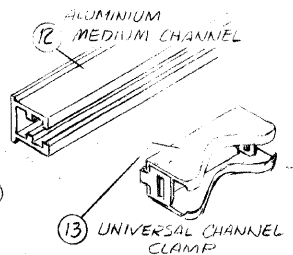
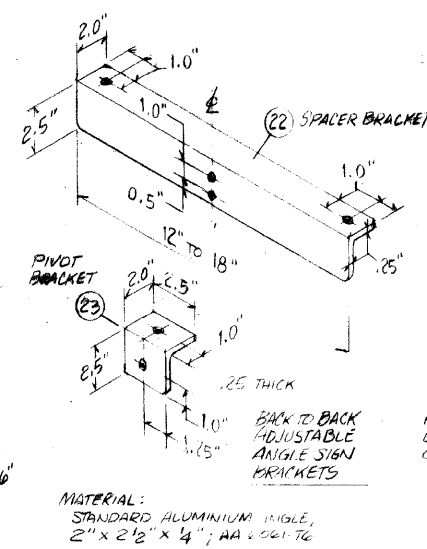
### 7 METAL POST- TYPE A, B AND C

- SIGN PANEL: WIDER THAN 30" AND 24" OR MORE IN DEPTH.
- SIGN PANEL: VARIABLE X 18" - 72" WIDE, MAXIMUM.
- SIGN PANEL: WIDER THAN 42", 2 OR MORE POSTS.
- SIGN PANEL: 6.5 SQ. FT. OR LARGER IN AREA (UNLESS OTHERWISE INDICATED ON THE PLAN); 2 POSTS.
- SIGN PANEL: LESS THAN 6.5 SQ. FT. IN AREA AND NOT WIDER THAN 30". SINGLE POST OR OTHER SUPPORT.
- SIGN PANEL: 36" MAX. DIAMETER ON SINGLE WOOD OR STEEL POST.
- ROUND POST OR POLE; LIGHT STANDARD OR TRAFFIC SIGNAL.
- TRAFFIC SIGNAL MAST ARM.

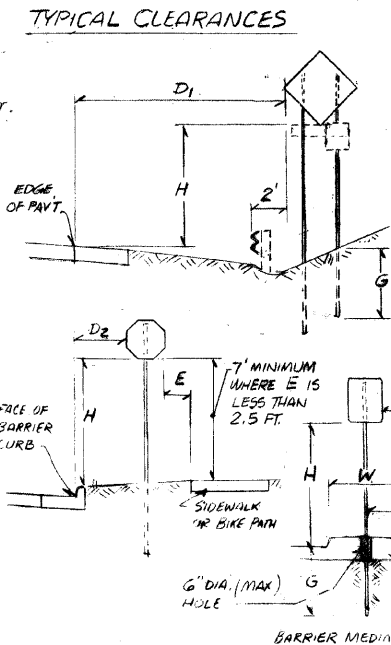
### 10 TELESCOPING STEEL SIGN SUPPORT

2" X 2" SQUARE TUBULAR TOP SECTION

- 4" X 6" WOOD SIGN SUPPORT.
- 3/4" WIDE X 0.030" THICK STAINLESS STEEL BAND, TYPE 201. (REGULAR BAND)
- STAINLESS STEEL DUCKLE, TYPE 201 TO FIT REGULAR BAND.
- 1/4" X 1/4" X 1" H.W. #3 SELF TAPPING SIGN SCREW WITH NEOPRENE WASHER.
- SIGN PANEL MOUNTING HOLES LOCATED AS PER DETAIL OR BLANK STANDARD. ALL HOLES WHERE CHANNELS ARE NOT USED SHALL BE 3/8" DIAMETER.
- 5/16" DIA. SQUARE HEAD BOLT, W/NER AND LOCKNUT.
- 5/16" DIA. HEX HEAD BOLT, NYLON WASHER, REG. WASHER AND LOCKNUT.
- NON SLIP WASHER.
- 2 1/2" X 2 1/2" BASE SECTION.
- 2 1/2" X 2 1/2" SLEEVE SECTION.



- NOT LESS THAN 12 FT. (6 FT. IF ALLOWED BY THE ENGINEER IN AREAS OF LIMITED SIGHT DISTANCE OR OTHER RESTRICTIONS).
- 6 FT. OR MORE DESIRED. NOT LESS THAN 2 FT. WHERE 40 M.P.H. OR HIGHER IS POSTED. 1 FT. MIN. MAY BE ALLOWED IN AREAS OF 35 M.P.H. AND LOWER.
- NORMALLY NOT LESS THAN 7 FT. MAY BE 5 FT. MIN. IN RURAL AND FOREST PRESERVE AREAS WITH NO PARKING.
- METAL POSTS: TYPE A - 3 1/2" FT. MIN., TYPE B - 4 FT. MIN., 4" X 6" WOOD - 5 FT. MIN. FOR OTHER SUPPORTS SEE THE APPLICABLE SPECIFICATIONS.



### EXPLANATION OF SYMBOLS

- METAL POST(S)-TYPE A.
- METAL POST(S)-TYPE B.
- METAL POST- TYPE B, SUPPORTING BACK TO BACK ADJUSTABLE ANGLE SIGNS.
- SIGN MOUNTED ON LIGHT STANDARD, TRAFFIC SIGNAL POST OR MAST ARM.
- OTHER SUPPORT TYPE AS SPECIFIED ON THE PLAN.
- EXISTING SIGN ASSEMBLY TO REMAIN IN PLACE, BE RE-ERECTED OR RELOCATED AS DIRECTED BY THE ENGINEER AND IN ACCORDANCE WITH ARTICLE 107.22 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- EXISTING SIGN ASSEMBLY BEYOND THE CONSTRUCTION LIMITS TO BE REMOVED.

NOTE: EXCEPT FOR SIGNS SHOWN: B AND/OR OTHERWISE SPECIFIED, ALL EXISTING TRAFFIC SIGN ASSEMBLIES WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED. SEE THE SPECIAL PROVISION.

### GENERAL NOTES FOR SIGNING

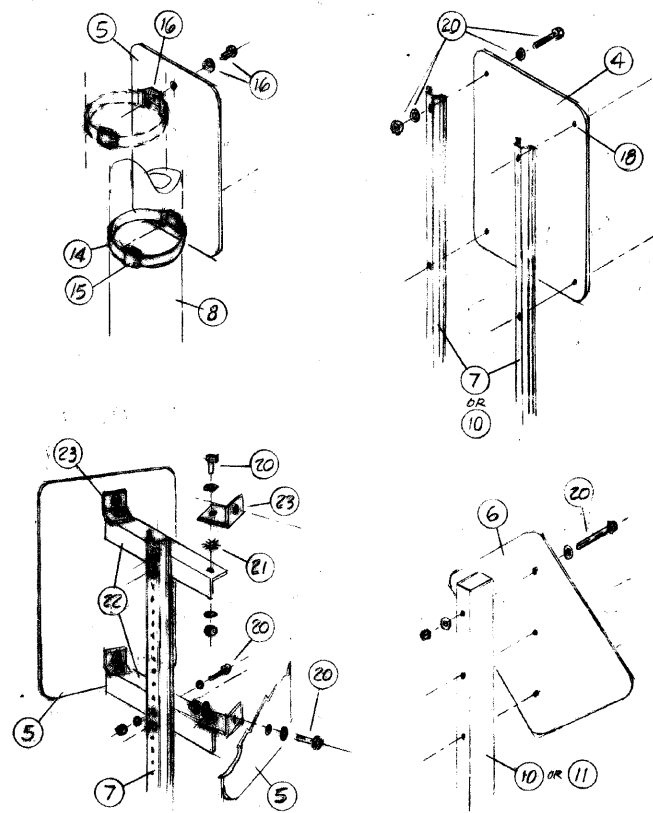
- THE DESIGN OF ALL STANDARD TRAFFIC SIGNS SHALL CONFORM WITH THE STATE OF ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS. (M.U.T.C.D.). SPECIAL AND VARIABLE MESSAGE SIGNS SHALL CONFORM WITH THE DETAILS AS SHOWN ON THE PLANS.
- ALL SIGNS, SUPPORTS, MATERIAL AND RELATED WORK SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS, THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE SPECIAL PROVISIONS AND THE PLANS.
- SIGN SUPPORT LENGTHS SHALL BE DETERMINED AT THE SITE IN ACCORDANCE WITH THE CLEARANCES AND OFFSET LOCATIONS SHOWN.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE LOCATION OF ANY UNDERGROUND ELECTRIC CABLES, UTILITY LINES OR DRAINAGE STRUCTURES IN THE VICINITY BEFORE BEGINNING WORK. AN ASSEMBLY SHALL BE RELOCATED FROM THE STATION SHOWN ON THE PLAN WHERE NECESSARY TO AVOID DAMAGING ANY UNDERGROUND INSTALLATION.
- WHERE METAL POSTS ARE SPECIFIED, 2 POSTS SHALL SUPPORT A SIGN PANEL ASSEMBLY HAVING A TOTAL AREA OF 6.5 SQ. FT. OR MORE. A 6 FT. X 2.5 FT. AND LARGER SIGN PANEL ASSEMBLY SHALL BE SUPPORTED WITH 3 POSTS.
- THE TRAFFIC OPERATIONS DIVISION OF THE COOK COUNTY HIGHWAY DEPARTMENT SHALL BE NOTIFIED TEN (10) DAYS PRIOR TO THE ESTIMATED DATE OF THE INSTALLATION OF THE PERMANENT TRAFFIC CONTROL DEVICES.

### MOUNTING ASSEMBLY NOTES

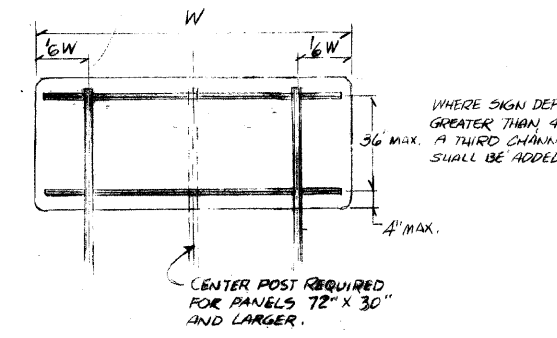
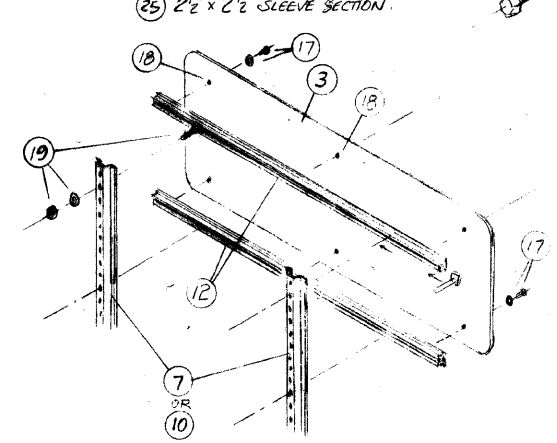
EXCEPT FOR NYLON AND NEOPRENE WASHERS WHERE INDICATED, ALL MOUNTING HARDWARE SHALL BE ZINC OR CADMIUM PLATED STEEL, ALUMINUM OR STAINLESS STEEL. ALL BOLTS AND NUTS SHALL HAVE NATIONAL COARSE (UNC) THREAD.

SUPPORTING CHANNELS SHALL BE USED ON RECTANGULAR PANELS WIDER THAN 36" ON A SINGLE SUPPORT AND ON PANELS WIDER THAN 48" WHEN MOUNTED ON MORE THAN ONE POST, AND ON DIAMOND SHAPED 48" X 48" PANELS. CHANNELS MAY BE USED TO MOUNT 2 TYPE 1 ADJACENT SIGN PANELS.

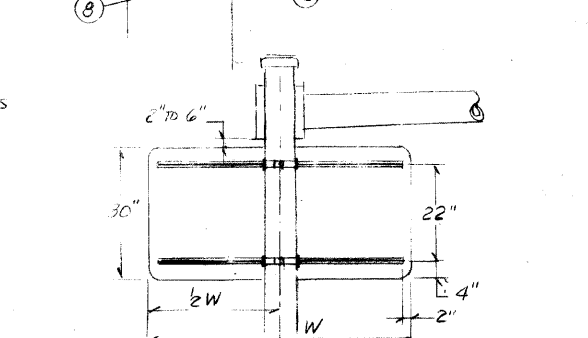
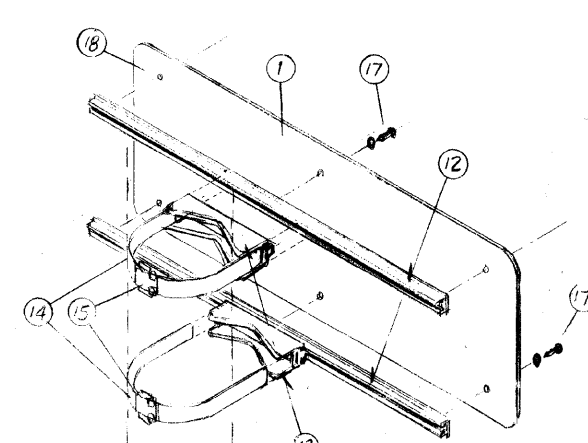
MOUNTING METHODS AND MATERIAL OTHER THAN THAT SHOWN ARE ACCEPTABLE UPON THE APPROVAL OF THE ENGINEER AND WHERE COMPLETELY INTERCHANGEABLE WITH EXISTING INSTALLATIONS ON COUNTY AND STATE ROADWAYS.



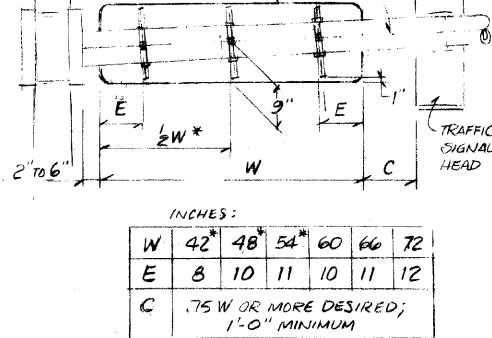
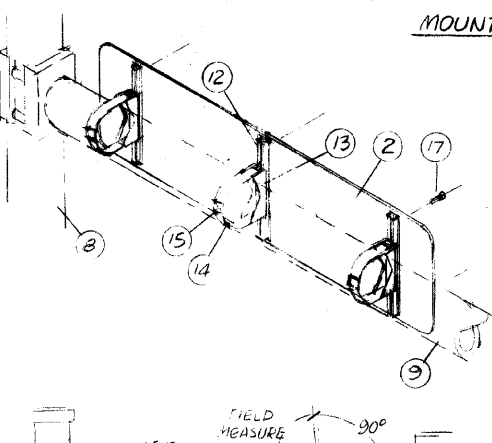
BACK TO BACK ADJUSTABLE ANGLE MOUNTING



SIGN PANEL MOUNTING DETAILS



SIGN PANEL MOUNTING DETAILS



SIGN PANEL MOUNTING DETAILS

INCHES:

W	42"	48"	54"	60"	66"	72"
E	8	10	11	10	11	12
C	.75 W OR MORE DESIRED; 1'-0" MINIMUM					

\*NO CENTER MOUNTING REQ'D

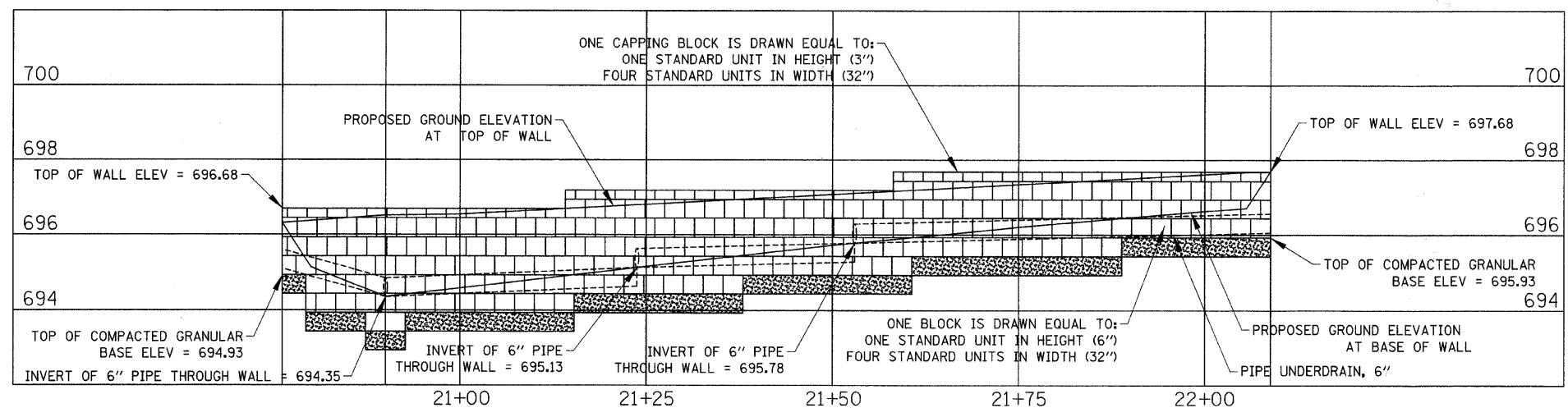
### TRAFFIC SIGN MOUNTING DETAILS

TRAFFIC DIVISION  
COOK COUNTY HIGHWAY DEPARTMENT

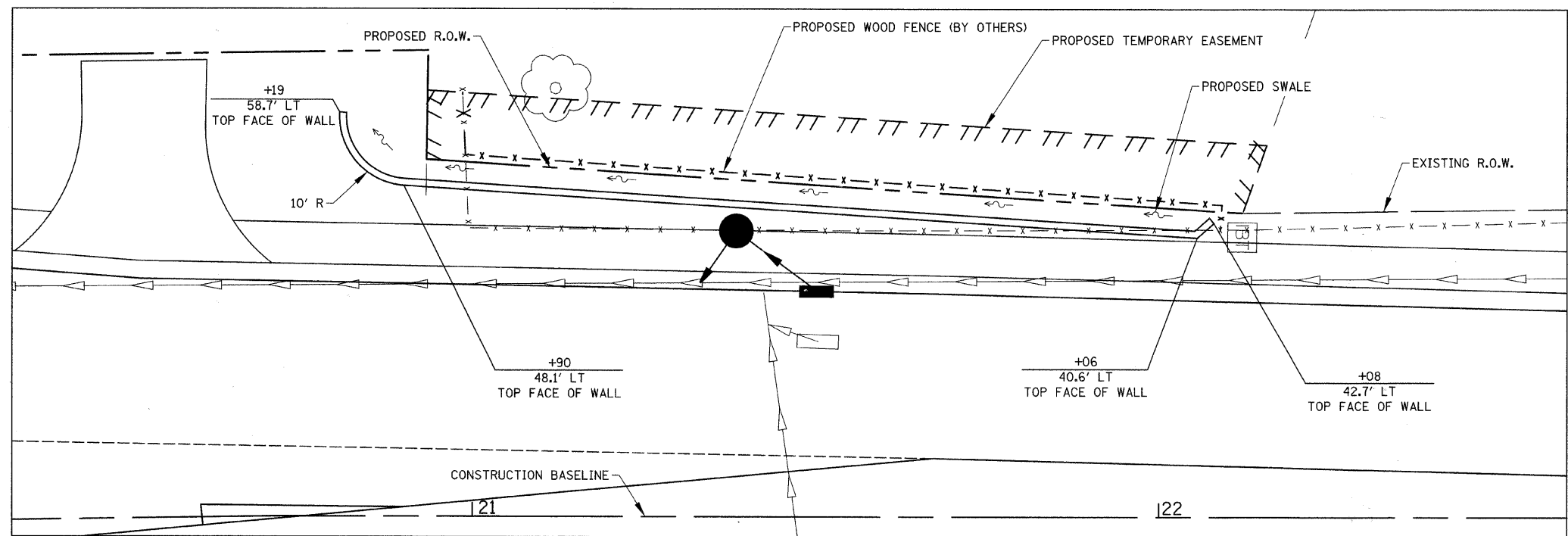
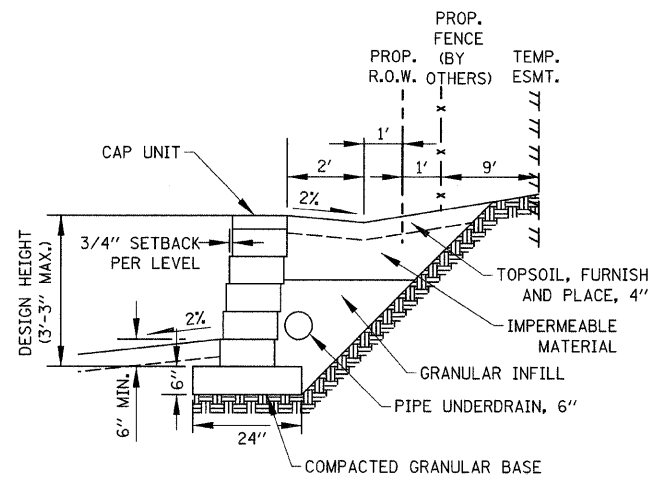
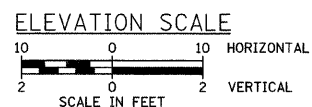
### STANDARD 304-2

F.A.U. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	80
RETAINING WALL PLAN & ELEVATION				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-80035691				

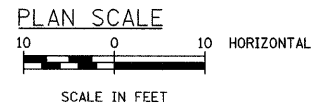
NOTE:  
SEE CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.



PROFILE VIEW (FACING WEST)



PLAN VIEW

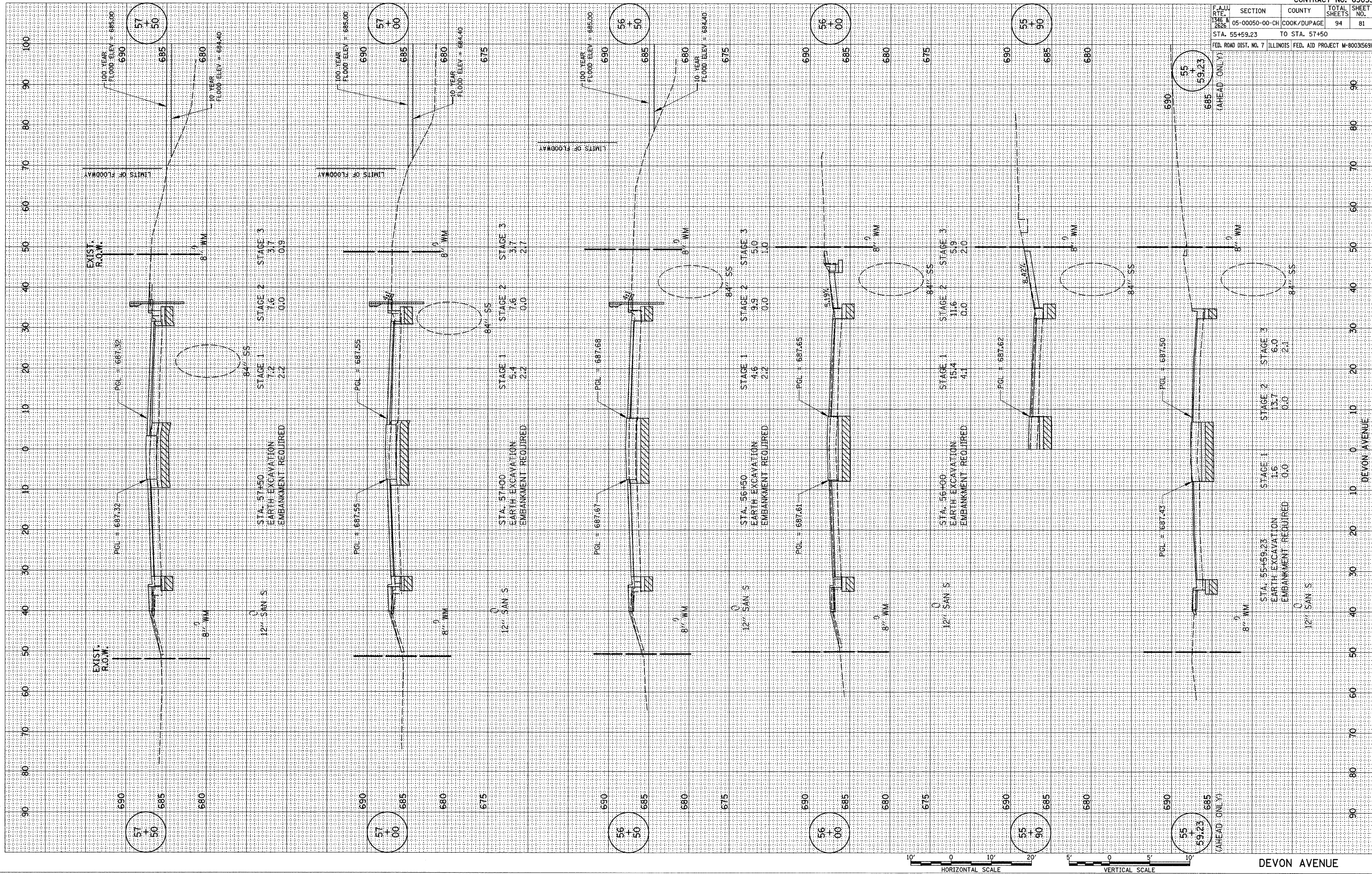


ILLINOIS DEPARTMENT OF TRANSPORTATION  
RETAINING WALL PLAN & ELEVATION  
ARLINGTON HEIGHTS ROAD  
DATE: 5/27/08  
DESIGNED BY: KRK  
CHECKED BY: DJK

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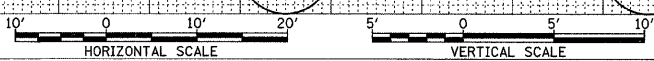
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SURVEYED	PLOTTED		
NOTE BOOK	TEMPLATE		
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FINAL	SURVEY	BY	DATE
SURVEYED	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		



SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
05-00050-00-CH	COOK/DUPAGE	94	81
STA. 55+59.23 TO STA. 57+50			

FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(569)



DEVON AVENUE

STA. 55+59.23 TO STA. 57+50

NOTE: ALL EARTHWORK QUANTITIES ARE SHOWN IN SQUARE FEET



REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL / POROUS GRANULAR EMBANKMENT, SUBGRADE

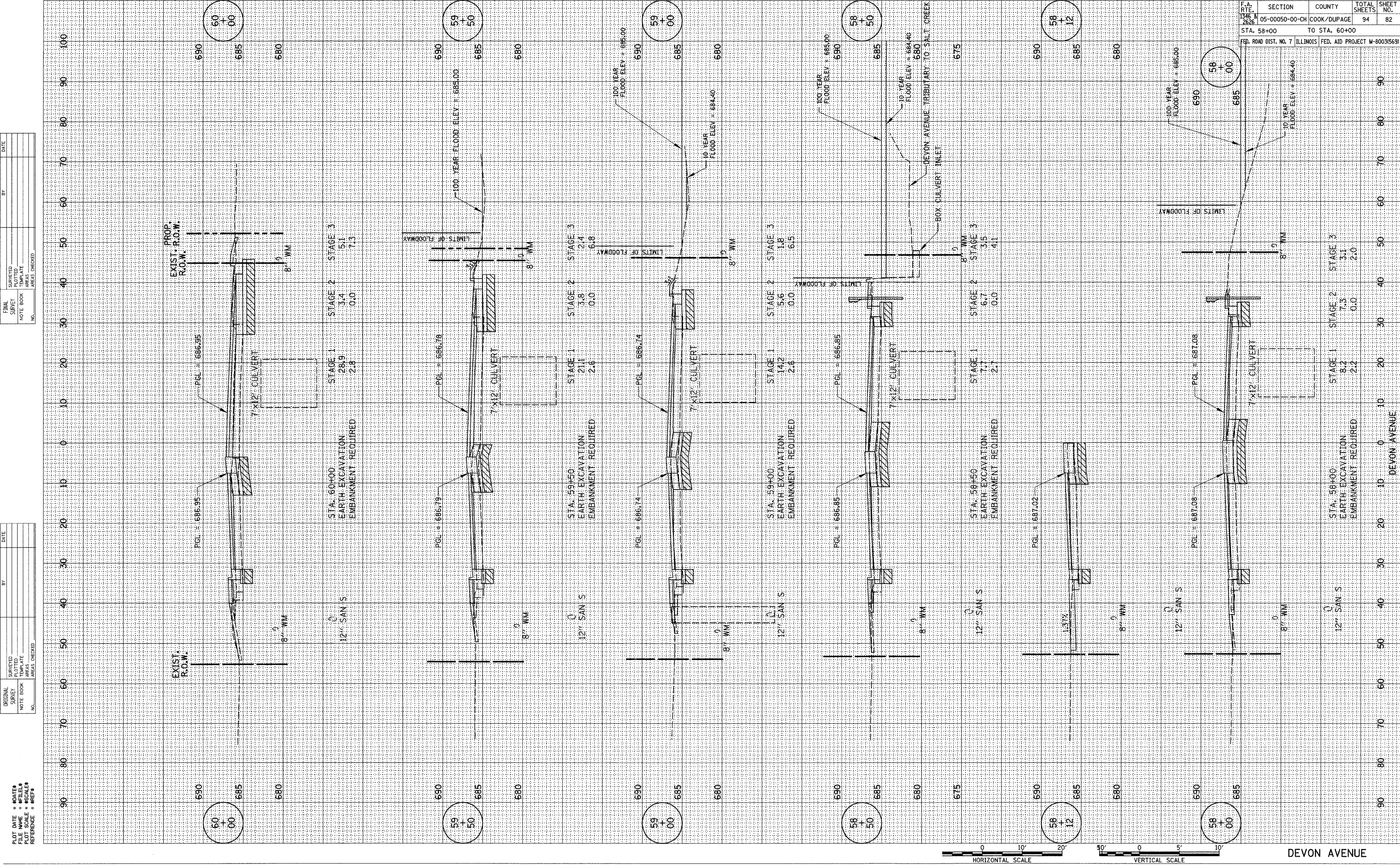


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1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	82
STA. 58+00		TO STA. 60+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(569)	

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	
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	AREAS CHECKED	

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NOTE BOOK	PLOTTED	
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DEVON AVENUE

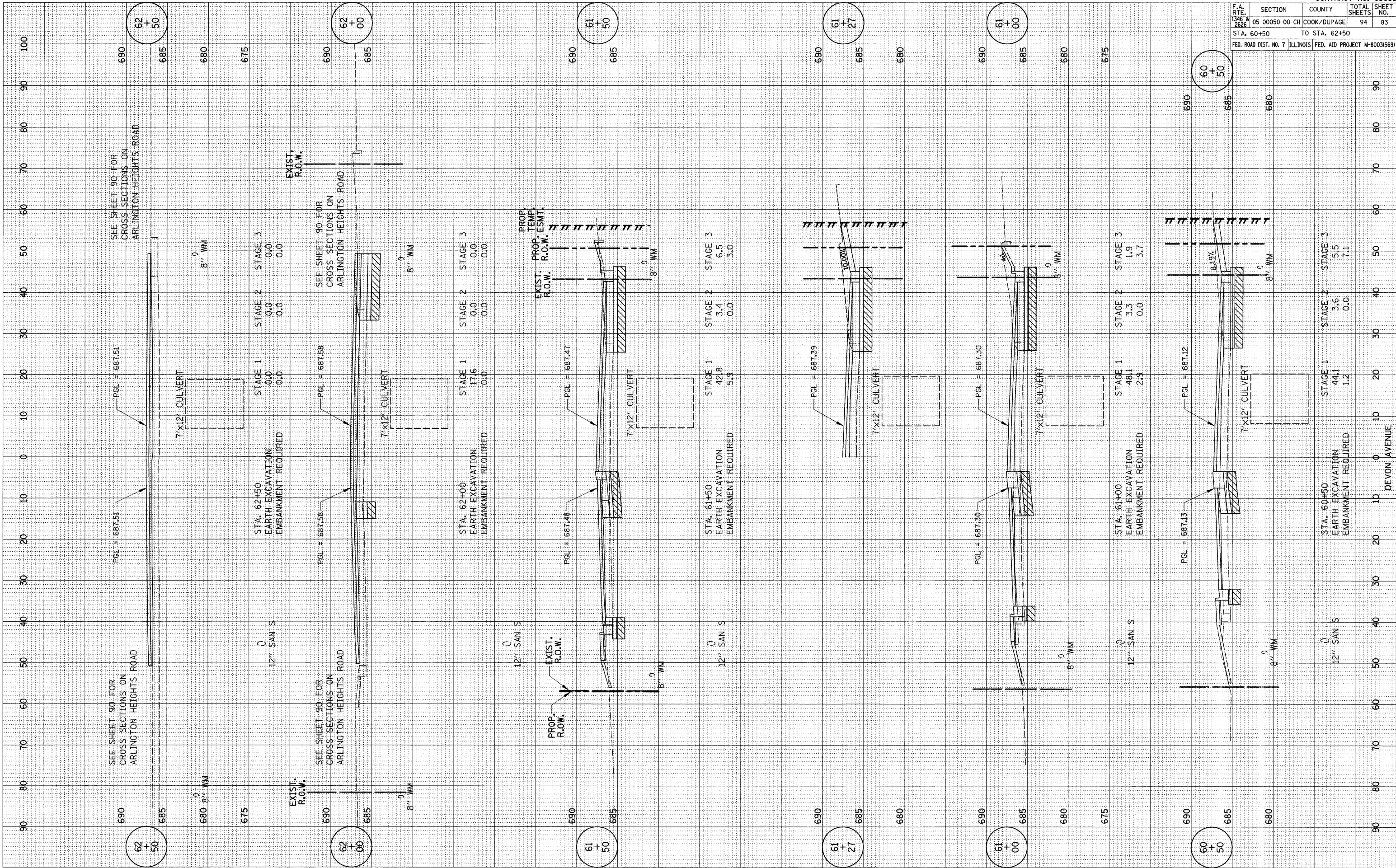
STA. 58+00 TO STA. 60+00



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SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		

FINAL	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	83
STA. 60+50 TO STA. 62+50				
FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT M-8003(569)		

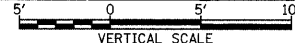
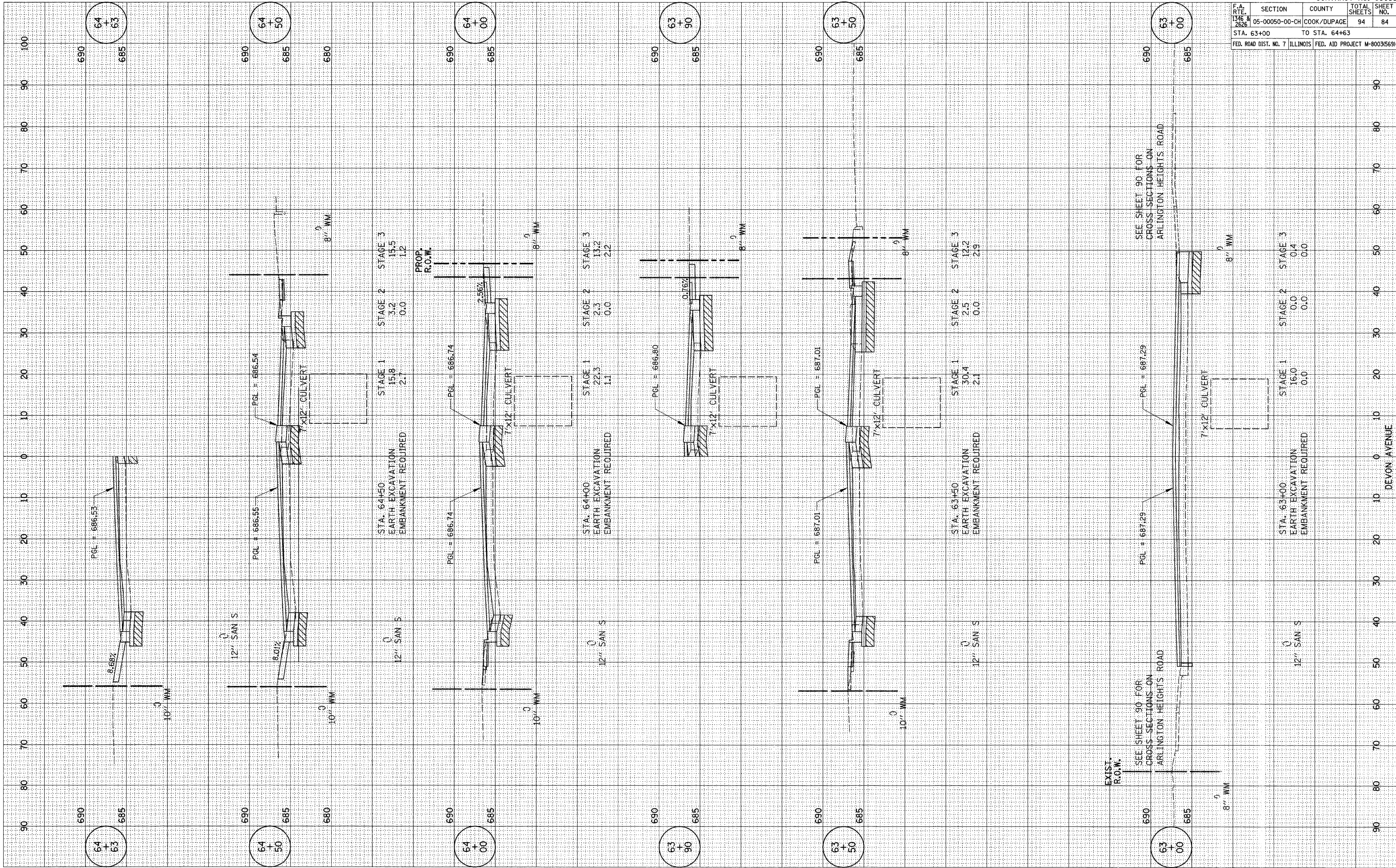
DEVON AVENUE  
STA. 60+50 TO STA. 62+50



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SURVEY	NOTE BOOK	NO.	
NO.	AREAS CHECKED		

FINAL	SURVEYED	BY	DATE
NOTE BOOK	NOTE BOOK	NO.	
NO.	AREAS CHECKED		



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	84
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FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT M-8003(569)		



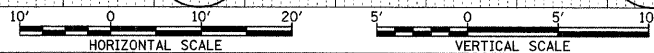
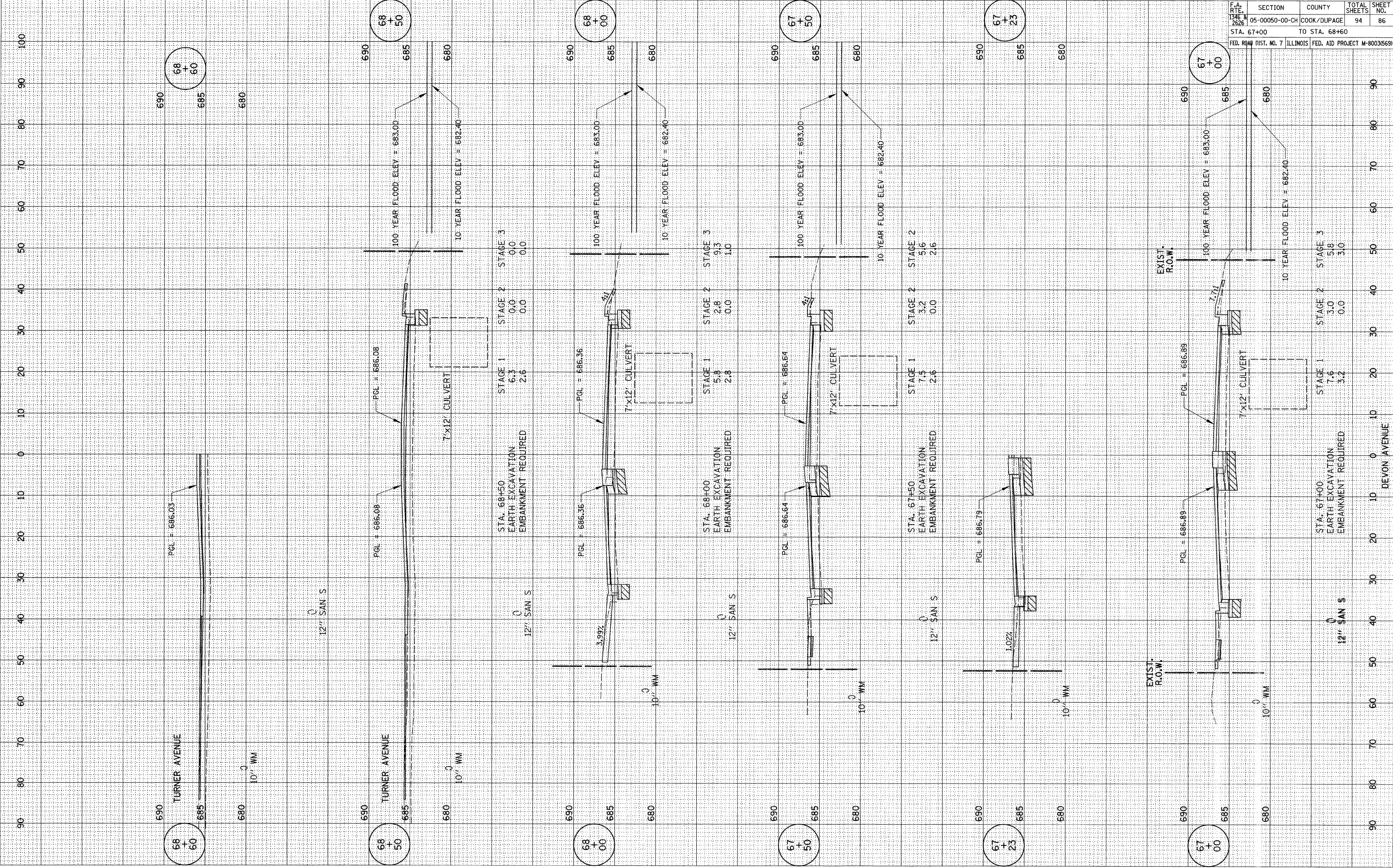




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SURVEY	
NOTE BOOK	
AREAS CHECKED	

FINAL	DATE
SURVEY	
NOTE BOOK	
AREAS CHECKED	



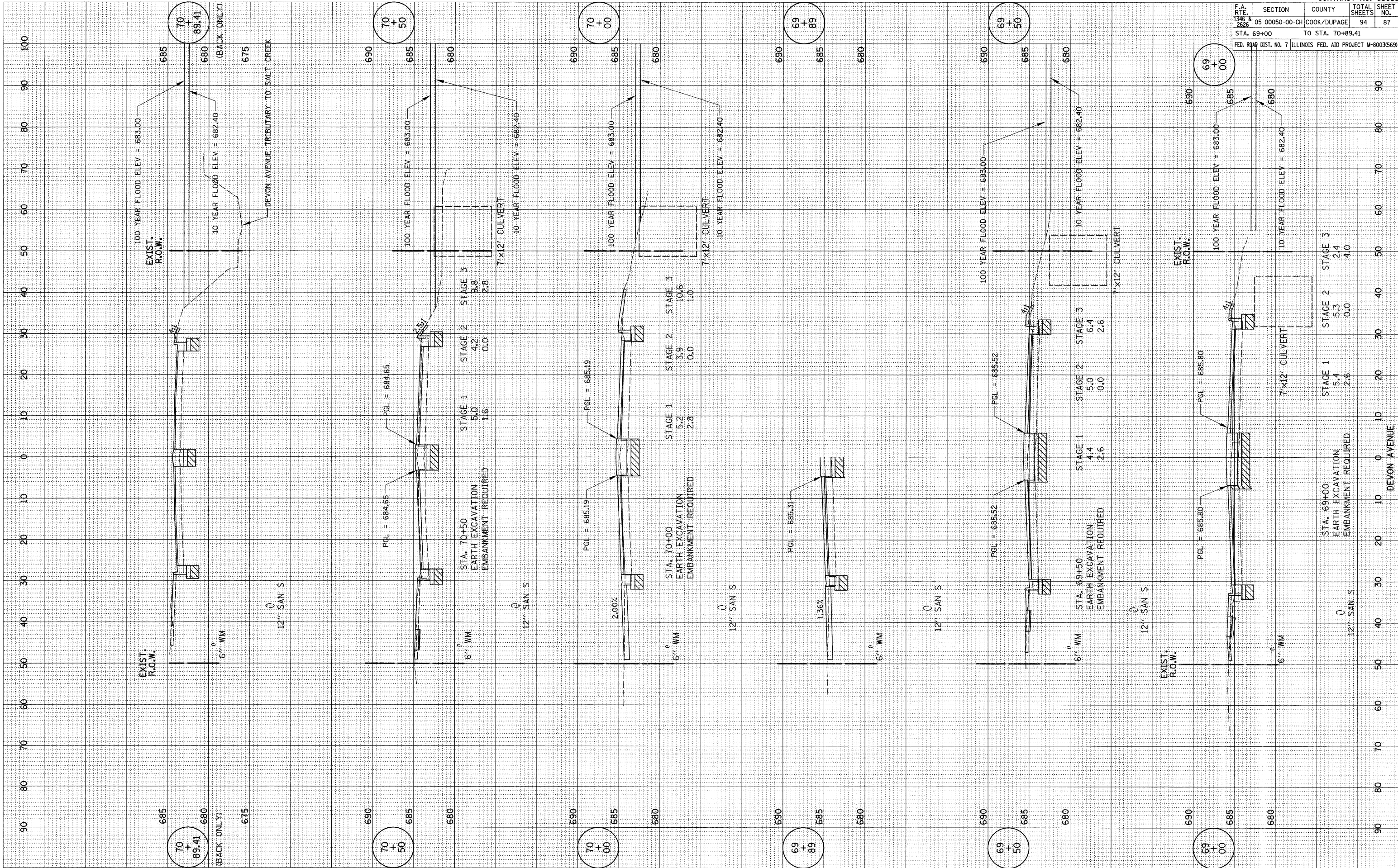
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STA. 67+00	TO STA. 68+60			
FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT M-8003(569)		



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AREAS CHECKED			
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FINAL		BY	DATE
SURVEY	SURVEYED		
	ADJUSTED		
	TEMPLATE		
	NO. BOOK		
AREAS CHECKED			
NO.			



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	87
STA. 69+00		TO STA. 70+89.41		
FED. ROAD DIST. NO. 7		ILLINOIS		
		FED. AID PROJECT M-8003(559)		

DEVON AVENUE

STA. 69+00 TO STA. 70+89.41



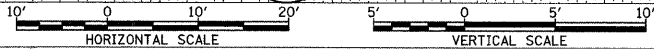
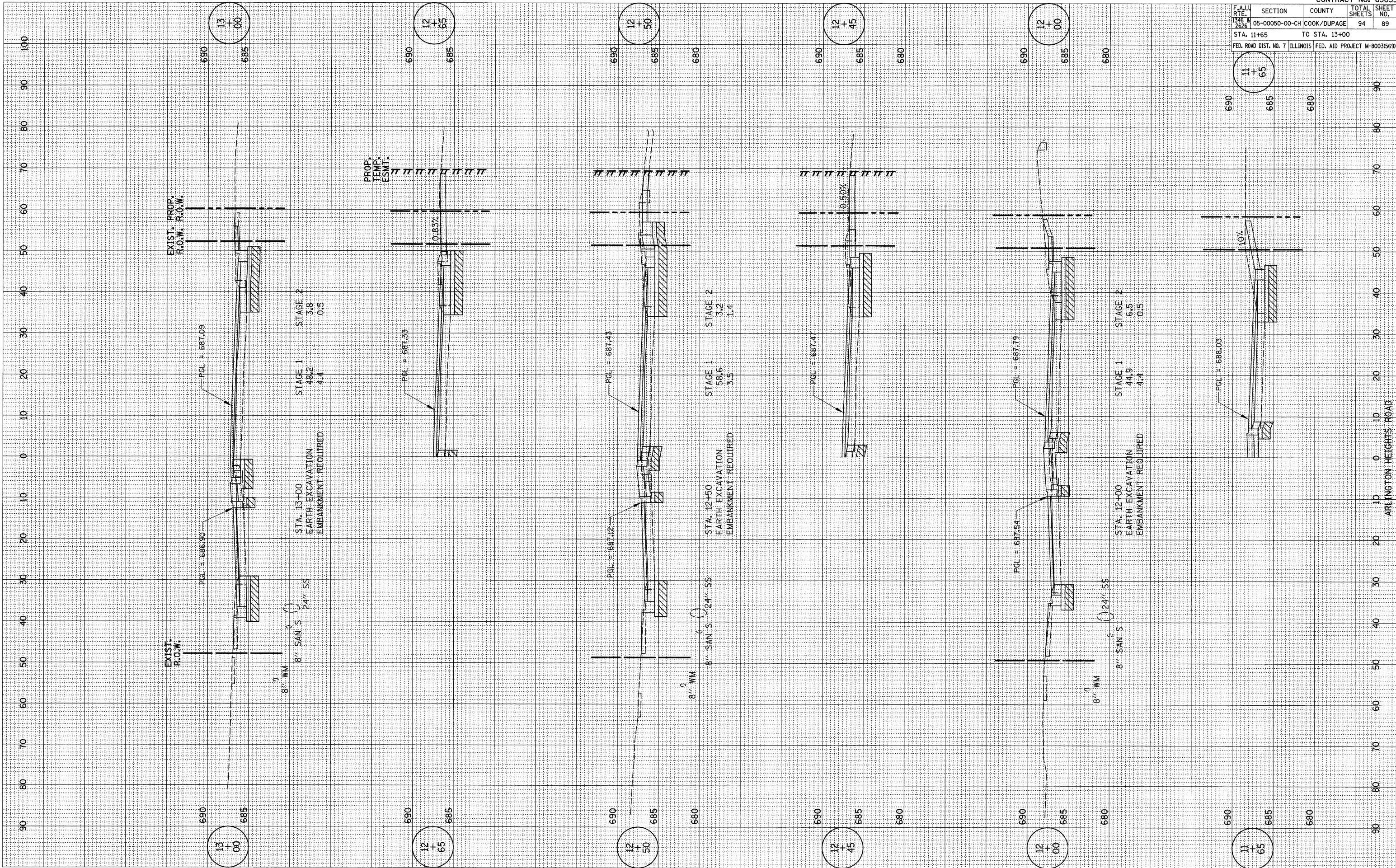




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ORIGINAL	DATE	BY
SURVEY		
NOTE BOOK		
AREAS CHECKED		

FINAL	DATE	BY
SURVEY		
NOTE BOOK		
AREAS CHECKED		



ARLINGTON HEIGHTS ROAD

STA. 11+65 TO STA. 13+00

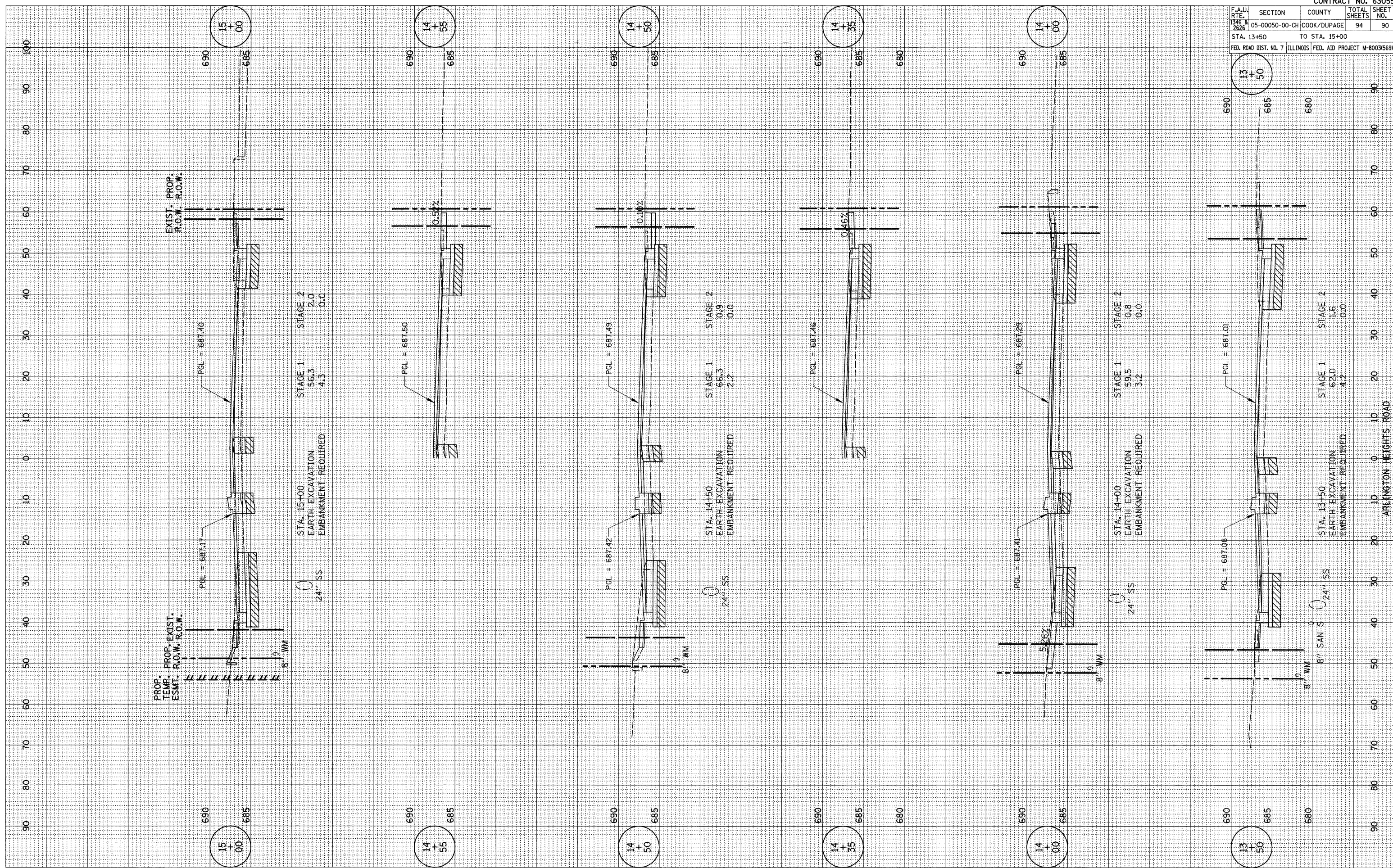
F.A.U. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. 11+65	TO STA. 13+00			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-8003(569)		



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CONTRACT NO. 63055				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	90
STA. 13+50		TO STA. 15+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(569)	

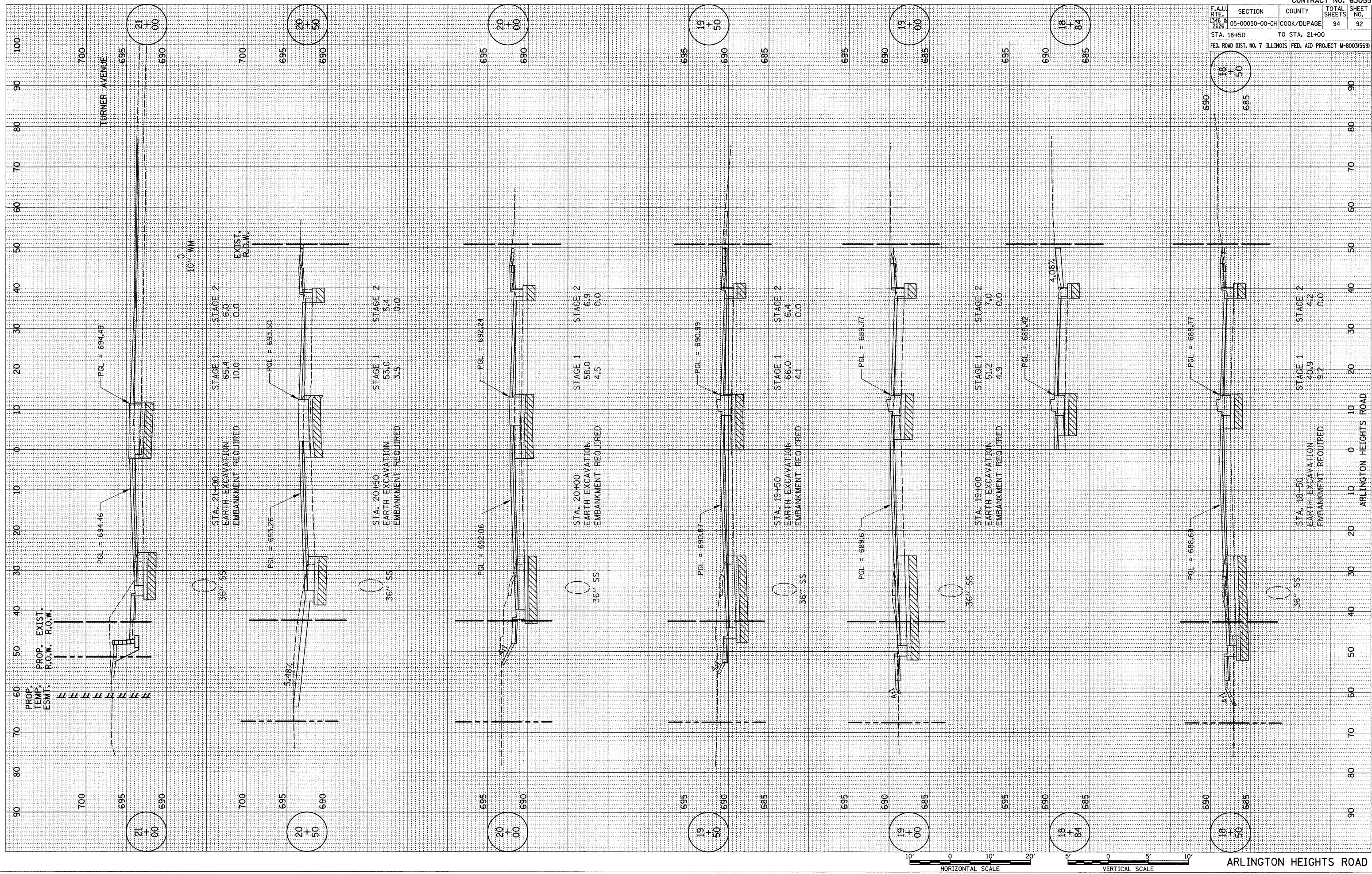






ORIGINAL SURVEY		BY _____	DATE _____
NOTE BOOK		SURVEYED _____	
		PLOTTED _____	
		TEMPLATE _____	
		AREAS _____	
		AREAS CHECKED _____	
NO.			

CONTRACT NO. 63055				
F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	92
STA. 18+50		TO STA. 21+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(569)	



ARLINGTON HEIGHTS ROAD

STA. 18+50 TO STA. 21+00

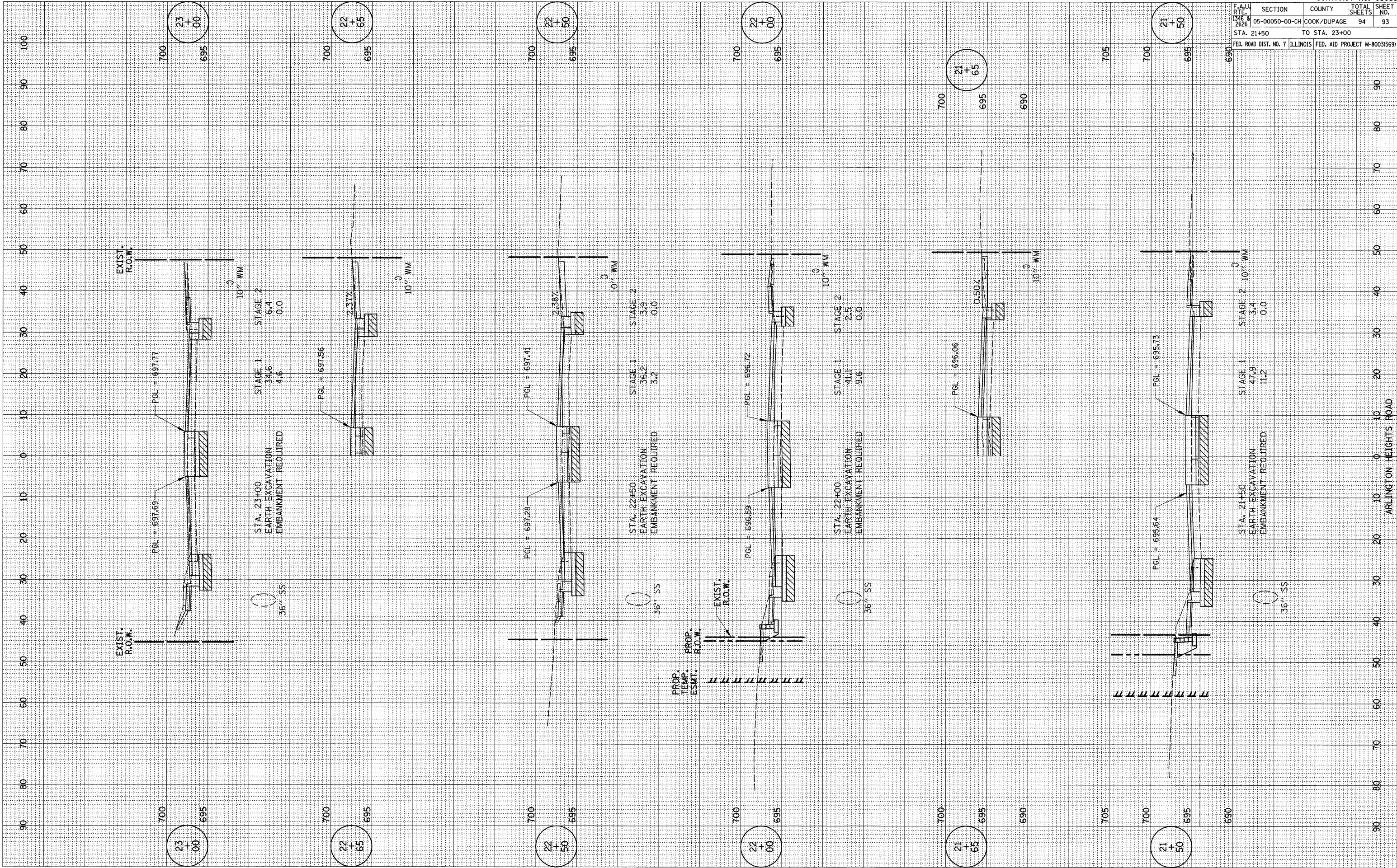


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	93
STA. 21+50		TO STA. 23+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(569)	

FINAL SURVEY	SURVEYED	PLOTTED	DATE
NOTE BOOK	TEMPLATE	AREAS	CHECKED
NO.			

ORIGINAL SURVEY	SURVEYED	PLOTTED	DATE
NOTE BOOK	TEMPLATE	AREAS	CHECKED
NO.			

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PLOT SCALE = #SCALE#  
REFERENCE = #REF#

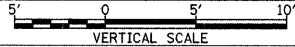
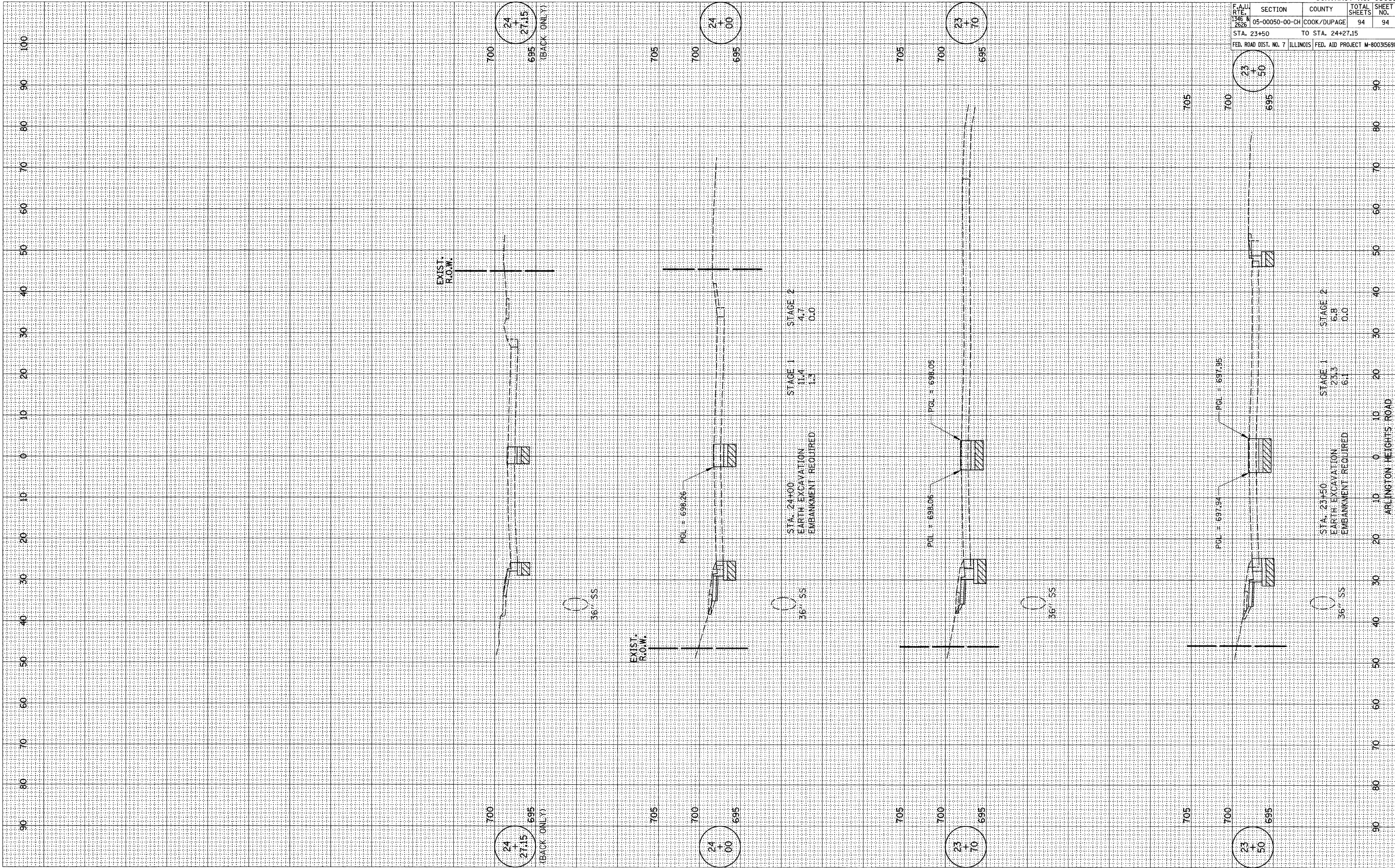




PLOT DATE = \*DATE\*  
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REFERENCE = \*REF\*

ORIGINAL	DATE	BY
SURVEY		
NOTE BOOK		
PLotted		
AREAS CHECKED		

FINAL	DATE	BY
SURVEY		
NOTE BOOK		
PLotted		
AREAS CHECKED		



ARLINGTON HEIGHTS ROAD

STA. 23+50 TO STA. 24+27.2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1346 & 2626	05-00050-00-CH	COOK/DUPAGE	94	94
STA. 23+50		TO STA. 24+27.15		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003(6569)		

CONTRACT NO. 63055