

CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY SPRINGFIELD, IL

IL. PROJ. NO: SPI-4755
 AIP PROJ. NO: 3-17-0096-XX

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

SEPTEMBER 21, 2020

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 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

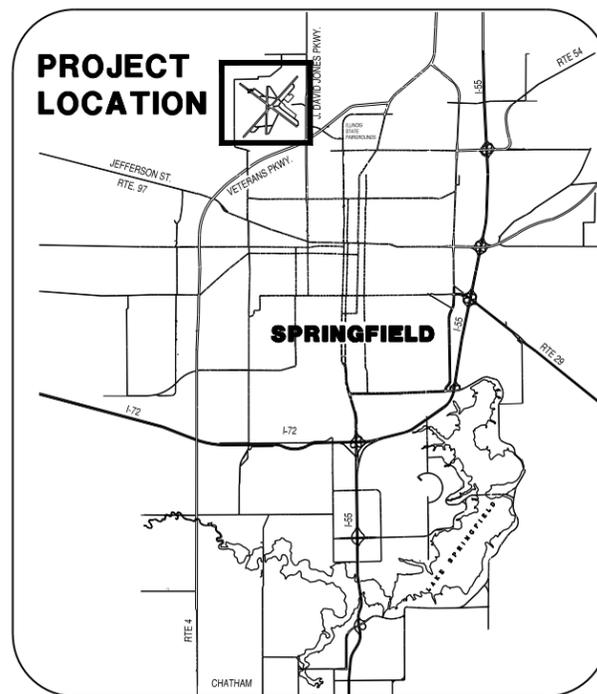
TAXIWAY CONSTRUCTION

APPROACH CATEGORY C
 AIRPLANE DESIGN GROUP III
 TAXIWAY DESIGN GROUP 3

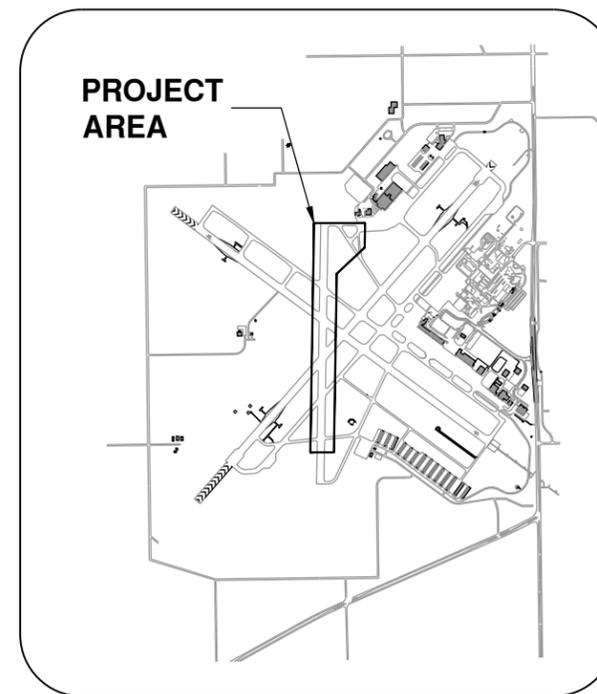
TAXIWAY SAFETY AREA: 118'
 TAXIWAY OBJECT FREE AREA: 186'
 TAXIWAY WIDTH: 50'
 RUNWAY SAFETY AREA: 500'

ABRAHAM LINCOLN CAPITAL
 AIRPORT

TOWNSHIP: 16 NORTH
 RANGE: 5 WEST OF THE 4TH P.M.
 SECTION: 8, 9, 16 & 17
 COUNTY: SANGAMON
 CIVIL TOWNSHIP: CAPITAL



LOCATION MAP



SITE PLAN

H. WESLEY IOERGER
 062.069455
 LICENSED
 PROFESSIONAL
 ENGINEER
 STATE OF ILLINOIS
 EXPIRES: 11/30/2021
W. Ioerger

ABRAHAM LINCOLN
 CAPITAL AIRPORT
 SPRINGFIELD AIRPORT AUTHORITY
 SPRINGFIELD, ILLINOIS
 APPROVED *Mark S. Hansen*
 EXECUTIVE DIRECTOR OF AVIATION
 DATE 9/18/2020

■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
 SUBMITTED BY *W. Ioerger*
 DATE 9/21/2020
 CMT JOB NUMBER: 180035-05-00

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SUMMARY OF QUANTITIES			
BASE BID			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR 106910	REMOVE LIGHT FIXTURE	EA	27
AR 106930	REPLACE LIGHT FIXTURE	EA	27
AR 108108	1/C #8 5 KV UG CABLE	LF	750
AR 108158	1/C #8 5 KV UG CABLE IN UD	LF	3000
AR 108258	2/C #8 5 KV UG CABLE IN UD	LF	250
AR 108706	1/C #6 COUNTERPOISE	LF	4000
AR 110501	1-WAY CONCRETE ENCASED DUCT	LF	20
AR 110504	4-WAY CONCRETE ENCASED DUCT	LF	80
AR 125100	ELEVATED RETROREFLECTIVE MARKER	EA	26
AR 125416	MIL - BASE MOUNTED - LED	EA	24
AR 125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EA	2
AR 125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EA	6
AR 125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EA	2
AR 125470	MODIFY SIGN PANEL	EA	4
AR 125515	HIRL, BASE MOUNTED	EA	2
AR 125525	HIRL, IN-PAVEMENT	EA	1
AR 125565	SPLICE CAN	EA	2
AR 125901	REMOVE STAKE MOUNTED LIGHT	EA	33
AR 125902	REMOVE BASE MOUNTED LIGHT	EA	95
AR 125904	REMOVE TAXI GUIDANCE SIGN	EA	56
AR 125905	REMOVE RWY DISTANCE REMAIN SIGN	EA	8
AR 125906	REMOVE SPLICE CAN	EA	3
AR 150510	ENGINEER'S FIELD OFFICE	LS	1
AR 152410	UNCLASSIFIED EXCAVATION	CY	9870
AR 152621	REMOVE CONCRETE	CY	2
AR 154506	GRANULAR SUBBASE - 6"	SY	4000
AR 156520	INLET PROTECTION	EA	6
AR 156530	TEMPORARY SEEDING	AC	1
AR 401610	BITUMINOUS SURFACE COURSE	TON	600
AR 401650	BITUMINOUS PAVEMENT MILLING	SY	900
AR 501527	17.5" PCC PAVEMENT	SY	3800
AR 501905	REMOVE PAVEMENT	SY	19595
AR 603510	BITUMINOUS TACK COAT	GAL	6210
AR 620520	PAVEMENT MARKING - WATERBORNE	SF	15800
AR 620525	PAVEMENT MARKING - BLACK BORDER	SF	3350
AR 620900	PAVEMENT MARKING REMOVAL	SF	14600
AR 701512	12" RCP, CLASS IV	LF	350
AR 701515	15" RCP, CLASS IV	LF	225
AR 701518	18" RCP, CLASS IV	LF	190
AR 701900	REMOVE PIPE	LF	25
AR 705524	4" PERFORATED UNDERDRAIN W/SOCK	LF	1125
AR 705544	4" NON-PERFORATED UNDERDRAIN	LF	130
AR 705635	UNDERDRAIN COLLECTION STRUCTURE	EA	2
AR 705640	UNDERDRAIN CLEANOUT	EA	4
AR 705645	UNDERDRAIN CONNECTION	EA	2
AR 751410	INLET	EA	4
AR 751530	MANHOLE	EA	2
AR 751900	REMOVE INLET	EA	1
AR 751940	ADJUST INLET	EA	1
AR 800308	EXPLORATORY EXCAVATION	EA	2
AR 800337	UNCLASSIFIED EXCAVATION (NIGHT WORK)	CY	3330
AR 800346	REMOVE PAVEMENT (NIGHT WORK)	SY	3800
AR 901510	SEEDING	AC	8.7
AR 904510	SODDING	SY	1370
AR 908515	HEAVY-DUTY HYDRAULIC MULCH	AC	8.7

SUMMARY OF QUANTITIES			
ADDITIVE ALTERNATE 1			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AS 152410	UNCLASSIFIED EXCAVATION	CY	2770
AS 156520	INLET PROTECTION	EA	5
AS 501905	REMOVE PAVEMENT	SY	11175
AS 901510	SEEDING	AC	3.5
AS 908515	HEAVY-DUTY HYDRAULIC MULCH	AC	3.5



License No. 184-000613

CONSULTANTS

SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-GI002.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

SHEET TITLE
INDEX TO SHEETS & SUMMARY OF QUANTITIES

GI002
SHEET 2 OF 63

0. GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORTS APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2 (LATEST VERSION), AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- 2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2 (LATEST VERSION). NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- 3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- 4. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- 5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- 6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- 2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- 3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS ALLOWED IN THE PLANS.
- 2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- 3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- 1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN IN THE PLANS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS OR THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
- 3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
- 4. CONTRACTOR EMPLOYEES MAY BE REQUIRED TO OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS.
- 5. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.

- 6. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER THE PLAN DETAILS AND SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.

- 7. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) WILL NOT NEED TO OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE UNDER THE CONTROL OF AND SUPERVISED BY THE CONTRACTOR.

- 8. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH AIR TRAFFIC AND THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN WITHIN THE MOVEMENT AREA. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY PASSED THE APPROVED AIRPORT TESTS MAY OPERATE THESE RADIOS.

- 9. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.

- 10. THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN IN THE PLANS.

- 11. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.

- 12. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE RETURNED TO & STORED AT THE STAGING AREA.

- 13. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.

- 14. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.

- 15. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE AIRPORT. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.

5. CONTRACTOR ACCESS (CONTINUED)

- 16. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.

- 17. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

- 18. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY MAY REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF ANY WILDLIFE IS SEEN ON OR ENTERING THE AIRPORT.

- 2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.

- 3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

- 4. THE CONTRACTOR SHALL CONTROL GRASS HEIGHTS THROUGH MOWING TO ASSIST WITH WILDLIFE CONTROL.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.

- 2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.

- 2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO THE AIRPORT PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.

- 3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.

- 4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 (LATEST VERSION) MAY BE USED TO AID IN THE INSPECTIONS.

- 2. THE CONTRACTOR SHALL REQUEST AND ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS SUITABLE TO BE OPENED.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.

- 2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. THE AIRPORT, AND CONTACT THE LOCAL FAA OFFICE (847-294-7336) TO ARRANGE FOR UTILITY LOCATES.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP, THE CONTRACTOR'S APPROVED SPCD OR THE SECURITY PLAN MAY RESULT IN FINES AS ALLOWED BY LAW.

- 2. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE PLANS.

- 2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF THE PLANS & FAA AC 150/5370-2 (LATEST VERSION.)

- 3. IMMEDIATELY UPON THE INITIATION OF EACH PROJECT PHASE, THE CONTRACTOR SHALL DE-ENERGIZE OR COVER LIGHTS FOR ALL CLOSED RUNWAYS AND TAXIWAYS DURING PAVEMENT CLOSURES. TEMPORARILY COVER OR DE-ENERGIZE AIRFIELD SIGNAGE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED IN THE PLANS.

16. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.

- 2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5D (OR LATEST) AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.

- 3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE AIRPORT.

- 4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- 1. THE CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING DURING NIGHTTIME CONSTRUCTION.

- 2. ARTIFICIAL AREA LIGHTING SHALL CONSIST OF VEHICLE OR POLE MOUNTED FLOODLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL ONLY BE ALLOWED IN ADDITION TO THE AREA LIGHTING.

- 3. ARTIFICIAL AREA LIGHTING SHALL NOT INTERFERE WITH AIR TRAFFIC OR ATCT OPERATIONS.

- 4. PLACEMENT & AIMING OF ARTIFICIAL LIGHTING SHALL BE APPROVED BY THE AIRPORT PRIOR TO START OF OPERATIONS.

18. PROTECTION OF AREAS & SURFACES

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 4-22, 13-31 OR 18/36 SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

- 2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

- 3. ALL WORK REQUIRED ON AN ACTIVE TAXILANE OBJECT FREE AREA, WHICH EXTENDS 81' FROM THE TAXILANE/APRON CENTERLINE, WILL REQUIRE A PORTION OF THAT APRON TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

(NOTES CONTINUE ON SHEET GC002)



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SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



SPRINGFIELD AIRPORT AUTHORITY ABRAHAM LINCOLN CAPITAL AIRPORT SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA021

IL PROJ. NO: SPI-4755

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH1-GC000.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: RLV

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SHEET TITLE

CONSTRUCTION ACTIVITY PLAN NOTES 1

GC001

SHEET 5 OF 63

(NOTES CONTINUED FROM SHEET GC001)

19. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE SPECIFICATIONS.

20. AIRPORT SECURITY REQUIREMENTS

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR. THE SUPERINTENDENT AND FOREMAN THAT ARE ISSUED BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD. BADGES SHALL BE RETURNED TO THE AIRPORT AT THE FINAL INSPECTION OR WHEN THE PERSON IS NO LONGER EMPLOYED BY THE CONTRACTOR. THE CONTRACTOR WILL PAY A FEE OF \$200.00 WITHIN 15 DAYS FOR EACH ACCESS BADGE THAT IS LOST, DESTROYED, STOLEN, OR NOT RETURNED AT THE FINAL INSPECTION.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES.
4. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
5. THE CONTRACTOR SHALL INSTALL AND USE TEMPORARY GATES FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, THE MAINTENANCE SUPERVISOR, AND THE SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR THE INSTALLATION AND REMOVAL OF TEMPORARY ACCESS GATES.
6. AS A MINIMUM, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURITY DURING CONSTRUCTION AS FOLLOWS:
 - a. POSSESS A COPY OF THE AIRPORT'S PROJECT SECURITY PLAN.
 - b. VISIBLY DELINEATE HIS CONSTRUCTION ZONE BY PLACING A LINE OF BARRICADES OR FLAGGING AROUND THE ENTIRE WORK ZONE DURING EACH PHASE OF THE CONTRACT.
 - c. COMPLY WITH THE AIRPORT'S SECURITY PLAN ASSOCIATED WITH THE CONSTRUCTION PROJECT AND ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SECURITY PROCEDURES AND REGULATIONS ON THE AIRPORT.
 - d. ENSURE THAT NO CONSTRUCTION EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE AIRCRAFT OPERATIONS AREA FROM CONSTRUCTION SITE UNLESS AUTHORIZED.
 - e. THE AIRPORT MAY REQUIRE THAT ALL SECURITY GUARDS UNDERGO ADDITIONAL TRAINING NECESSARY TO MEET THE AIRPORT'S SECURITY NEEDS.
 - f. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN SECURITY ON THE AIRPORT AS SPECIFIED OR AS DIRECTED BY THE AIRPORT.
 - g. THE CONTRACTOR'S SUPERINTENDENT, FOREMAN, SECURITY GUARDS, AND ANY SUPERVISORY PERSONNEL IN CHARGE OF OTHER WORKERS SHALL OBTAIN AN AIRPORT AUTHORITY SECURITY BADGE AND DISPLAY THIS BADGE WHILE ON SITE IN ACCORDANCE WITH FAA AND TSA REGULATIONS. CONTRACTOR PERSONNEL WITH BADGES SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD.

20. AIRPORT SECURITY REQUIREMENTS (CONT'D)

- h. TO OBTAIN AIRPORT SECURITY BADGES, CONTRACTORS MUST COMPLETE A CRIMINAL HISTORY RECORDS CHECK TWO WEEKS PRIOR TO EMPLOYEES BEING ALLOWED ACCESS TO THE SITE. THE TWO-WEEK PERIOD IS NECESSARY FOR AN ADEQUATE TIME OF PROCESSING FINGERPRINTS FOR COMPLETING THE CRIMINAL HISTORY CHECK. THE CONTRACTOR IS REQUIRED TO DEPOSIT A \$200 FEE TO THE AIRPORT PER BADGE, WHICH IS 80% REFUNDABLE AFTER EACH BADGE IS RETURNED. THE AIRPORT MUST RECEIVE THIS FEE/DEPOSIT PRIOR TO CONDUCTING ANY OF THE SECURITY BADGE ISSUE PROCESS. THE CONTRACTOR IS REQUIRED TO CONTACT THE AIRPORT AT LEAST THREE CALENDAR DAYS PRIOR TO SCHEDULING FINGERPRINTING AND BADGE TRAINING. TRAINING LASTS APPROXIMATELY TWO HOURS AND CAN BE CONDUCTED INDIVIDUALLY OR WITH A GROUP.
- i. THE UNITED STATES DEPARTMENT OF HOMELAND SECURITY HAS DESIGNATED THE SECURITY LEVEL AS HIGH RISK AT ABRAHAM LINCOLN CAPITAL AIRPORT. SUBSEQUENTLY, THE AIRPORT MUST HAVE ALL SECURITY ACTIVITY AND PROGRAMS APPROVED BY THE UNITED STATES DEPARTMENT OF HOMELAND SECURITY TRANSPORTATION SECURITY ADMINISTRATION. TO OBTAIN AIRPORT AUTHORITY ACCESS BADGES, THE CONTRACTOR SHALL PROVIDE COMPLETED ACCESS CONTROL FORMS PROVIDED BY THE AIRPORT FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THE CONTRACTORS WILL NEED TO PROVIDE THE AIRPORT AUTHORITY WITH DOCUMENTATION FROM THE LIST OF ACCEPTABLE DOCUMENTS, WHICH ESTABLISHES BOTH IDENTITY AND EMPLOYMENT AUTHORIZATION TWO WEEKS PRIOR TO EMPLOYEES BEING ALLOWED ACCESS TO THE SITE. THE TWO-WEEK PERIOD IS NECESSARY FOR AN ADEQUATE TIME OF PROCESSING OF A SECURITY THREAT ASSESSMENT (STA) FOR EACH EMPLOYEE REQUESTING ACCESS BY THE HOMELAND SECURITY TRANSPORTATION SECURITY ADMINISTRATION. THE CONTRACTOR IS REQUIRED TO CONTACT THE DIRECTOR OF OPERATIONS AND PUBLIC SAFETY AT 217-494-0787 FOR THE SCHEDULING OF ALL ACCESS BADGE APPLICATIONS.
- j. A MINIMUM OF SEVEN (7) DAYS PRIOR TO CONSTRUCTION, A MANDATORY SAFETY MEETING WILL BE CONDUCTED FOR ALL PARTIES INVOLVED. THIS SAFETY MEETING WILL INCLUDE A REVIEW OF A TRANSPORTATION SECURITY ADMINISTRATION (TSA) APPROVED ABRAHAM LINCOLN CAPITAL AIRPORT CHANGE OF CONDITION FOR THIS CONSTRUCTION PROJECT. THIS REVIEW WILL INCLUDE A SET OF SECURITY REQUIREMENTS AND PROCEDURES THAT THE CONTRACTOR WILL BE REQUIRED TO FOLLOW AT ALL TIMES WHILE OPERATING AT THE AIRPORT.
- k. THE CONTRACTOR WILL DESIGNATE A MINIMUM OF ONE INDIVIDUAL TO BE THE 24-HOUR POINT OF CONTACT AND ASSUME ALL ON-SITE SECURITY RESPONSIBILITIES FOR ALL EMPLOYEES DURING THE PROJECT. THIS INDIVIDUAL SHALL PROVIDE THE AIRPORT AUTHORITY A 24-HOUR CONTACT FOR EMERGENCY PURPOSES. THIS INDIVIDUAL WILL ALSO BE REQUIRED TO HAVE AVAILABLE AND PRESENT UPON REQUEST AT ANY TIME; A COPY OF THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) APPROVED CHANGE OF CONDITION DURING THE PROJECT.
- l. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED AND LOCKED DURING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE OPEN, THEN HE SHALL POST A COMPETENT, PROPERLY TRAINED SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED BY THE AIRPORT.
- m. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER AND AIRPORT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
- n. THE CONTRACTOR SHALL PROVIDE A SIGN AT ALL ACCESS GATES STATING " AUTHORIZED PERSONNEL ONLY." ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

20. AIRPORT SECURITY REQUIREMENTS (CONT'D)

- o. THE AIRPORT OPERATOR HAS A PROGRAM IN WHICH THE CONTRACTOR HAS THE ABILITY TO HAVE PERSONNEL APPROVED TO ACQUIRE ACCESS TO THE AIR OPERATIONS AREA (AOA) WITHOUT DRIVING PRIVILEGES FOR PURPOSES OF THIS CONTRACT. THOSE PERSON(S) HAVING ACCESS MUST SUCCESSFULLY COMPLETE SECURITY TRAINING AND PROVIDE PROPER DOCUMENTATION AS REQUIRED BY THE AIRPORT. PERSON(S) WITH ACCESS PRIVILEGES MUST SUCCESSFULLY COMPLETE THE NECESSARY SECURITY TRAINING IN ORDER THAT THEY CAN ESCORT ADDITIONAL WORKERS LIMITED TO HAVING ONLY CONTROLLED ACCESS PRIVILEGES.
- p. THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) REQUIRED A TEMPORARY MODIFICATION TO THE AIRPORT SECURITY PLAN, KNOWN AS A CHANGE OF CONDITION PLAN (CCP) AND THE CONTRACTOR AND MAJOR SUBCONTRACTORS WILL BE REQUIRED TO SIGN THE PLAN INDICATING THEIR COMPLIANCE WITH THE SECURITY REQUIREMENTS SPECIFIED IN THE CCP. A MANDATORY SAFETY BRIEFING WILL BE HELD AT THE PUBLIC SAFETY FACILITY PRIOR TO THE START OF THE PROJECT.



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SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



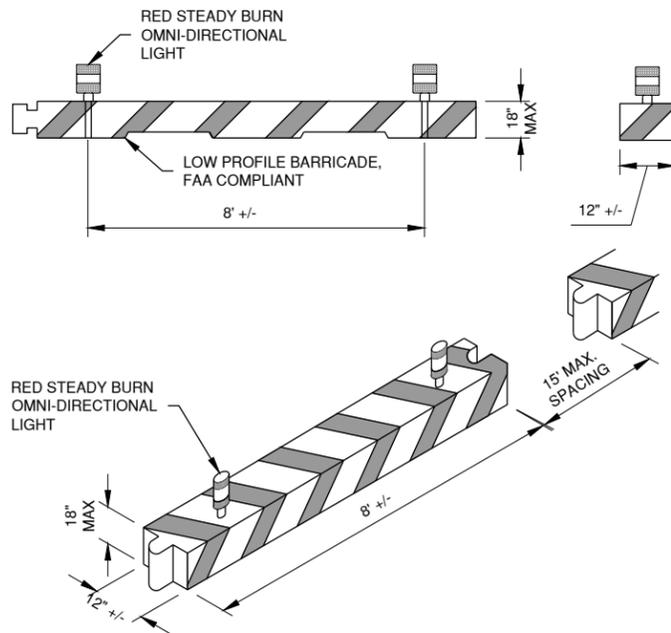
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

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IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-GC000.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN
NOTES 2**

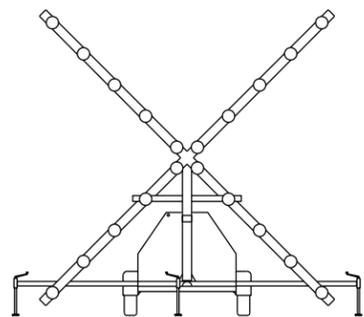
GC002
SHEET 6 OF 63



1 BEAM BARRICADE DETAIL
N.T.S.

BEAM BARRICADE NOTES

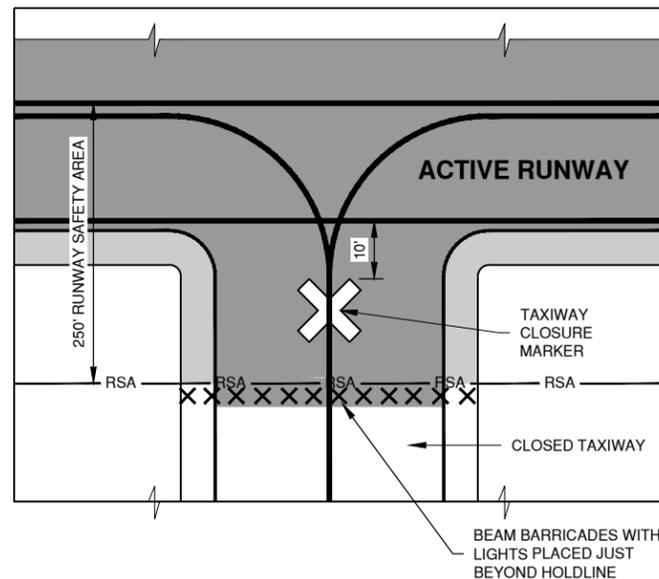
1. BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY WIND, JET OR PROP BLAST.
2. BARRICADE MUST BE OF LOW MASS AND EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AS SHOWN IN PLANS AND AS DIRECTED BY THE ENGINEER.
5. BARRICADES SHALL BE COMPLIANT WITH FAA AC 150/5370-2 (LATEST VERSION).



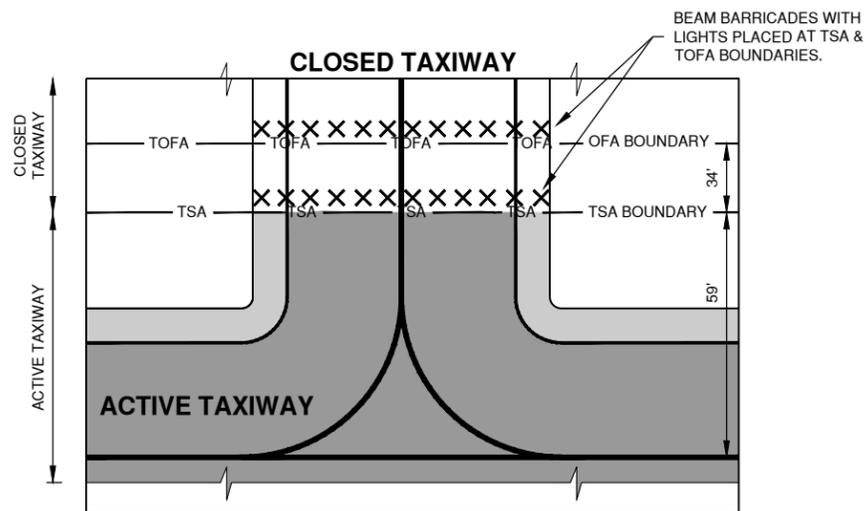
2 LIGHTED RUNWAY CLOSURE MARKER
N.T.S.

LIGHTED RUNWAY CLOSURE MARKER NOTES

1. TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS FOR NIGHTTIME CLOSURE.
2. THE CONTRACTOR SHALL PROVIDE FOUR CLOSURE MARKERS (2 PAIR) AND MAINTAIN THEM (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS ASSOCIATED WITH PROVIDING AND MAINTAINING THIS ITEM SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



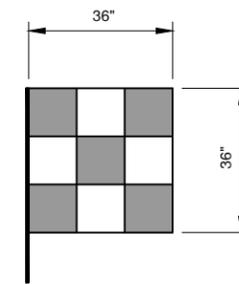
3 CLOSED TAXIWAY/ACTIVE RUNWAY BARRICADE DETAIL
N.T.S.



4 CLOSED TAXIWAY/ACTIVE TAXIWAY BARRICADE DETAIL
N.T.S.

NIGHT WORK RUNWAY CLOSURE NOTES

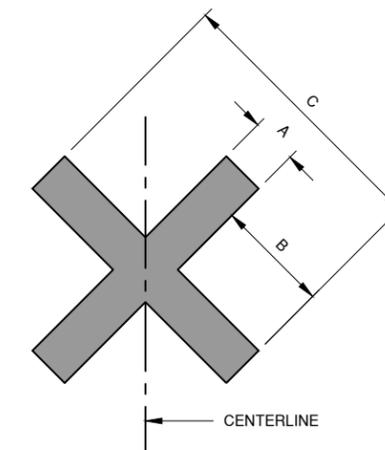
1. WORK WITHIN THE RUNWAY SAFETY AREA MAY NOT BEGIN UNTIL THE RUNWAY HAS BEEN CLOSED, NAVAIDS DISABLED, EDGE LIGHTING, CENTERLINE LIGHTING, AND TDZ LIGHTING DISABLED IN THE VAULT AND THE LIGHTED CLOSURE MARKERS ARE IN PLACE.
2. UPON COMPLETION OF WORK IN NIGHTTIME OFF-PEAK CLOSURE, NO EXCAVATIONS OR STOCKPILES SHALL EXIST IN EXCESS OF 3" AND ALL GRADES SHALL MEET FAA REQUIREMENTS. BARRICADES AND CLOSURE MARKERS SHALL BE REMOVED. AIRPORT OPERATIONS WILL INSPECT RUNWAY AT 4:30 AM.
3. SEE SECTION 80 OF THE SPECIFICATIONS FOR INFORMATION REGARDING LIQUIDATED DAMAGES AND REOPENING THE RUNWAY.
4. PRIOR TO REOPENING THE RUNWAY, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE DEBRIS WITHIN THE RUNWAY SAFETY AREA.



5 EQUIPMENT & VEHICLE SIGNAL FLAG
N.T.S. (ORANGE / WHITE)

SIGNAL FLAG NOTES

1. ALL CONTRACTOR VEHICLES AND EQUIPMENT SHALL DISPLAY COMPANY LOGO PLACARDS AND FLAG.
2. WHEN WORKING PRIOR TO DAWN OR AFTER DUSK, A 360 DEGREE ROTATING AMBER BEACON IS REQUIRED ON ALL EQUIPMENT AND TRUCKS.
3. CONTRACTOR SHALL REPLACE FLAGS THAT ARE WORN AND INEFFECTIVE.



SYMBOL TYPE	DIMENSION	A	B	C
CLOSED TAXIWAY		5'-0"	12'-6"	30'-0"
CLOSED RUNWAY		10'-0"	25'-0"	60'-0"

6 NON-LIGHTED CLOSURE MARKER
N.T.S.

NOTES

1. CLOSURE MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
3. MARKERS SHALL BE PLACED ON RUNWAYS TO COVER THE NUMERALS ON BOTH ENDS.
4. MARKERS MAY BE CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
5. MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.

SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

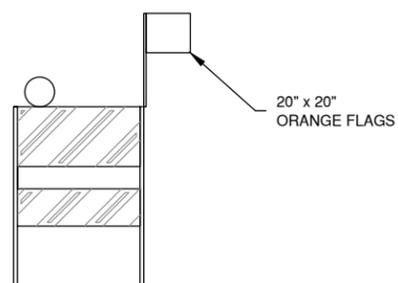


SPRINGFIELD AIRPORT AUTHORITY
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SPRINGFIELD, IL

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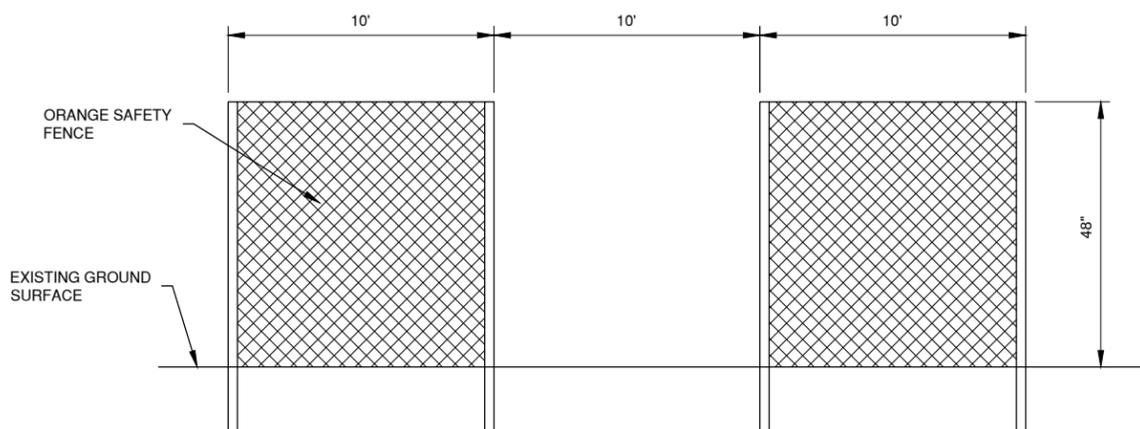
SHEET TITLE
CONSTRUCTION ACTIVITY PLAN DETAILS 1



1 **FLASHER BARRICADE DETAIL - IDOT TYPE 1**
N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 20' INTERVALS.



2 **BARRIER FENCE**
N.T.S.

BARRIER FENCE NOTES

1. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2. PLACE AT 10' INTERVALS.

SEPT 21 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
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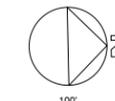
SHEET TITLE

CONSTRUCTION ACTIVITY PLAN DETAILS 2

GC004
SHEET **8** OF **63**



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

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SPRINGFIELD AIRPORT AUTHORITY
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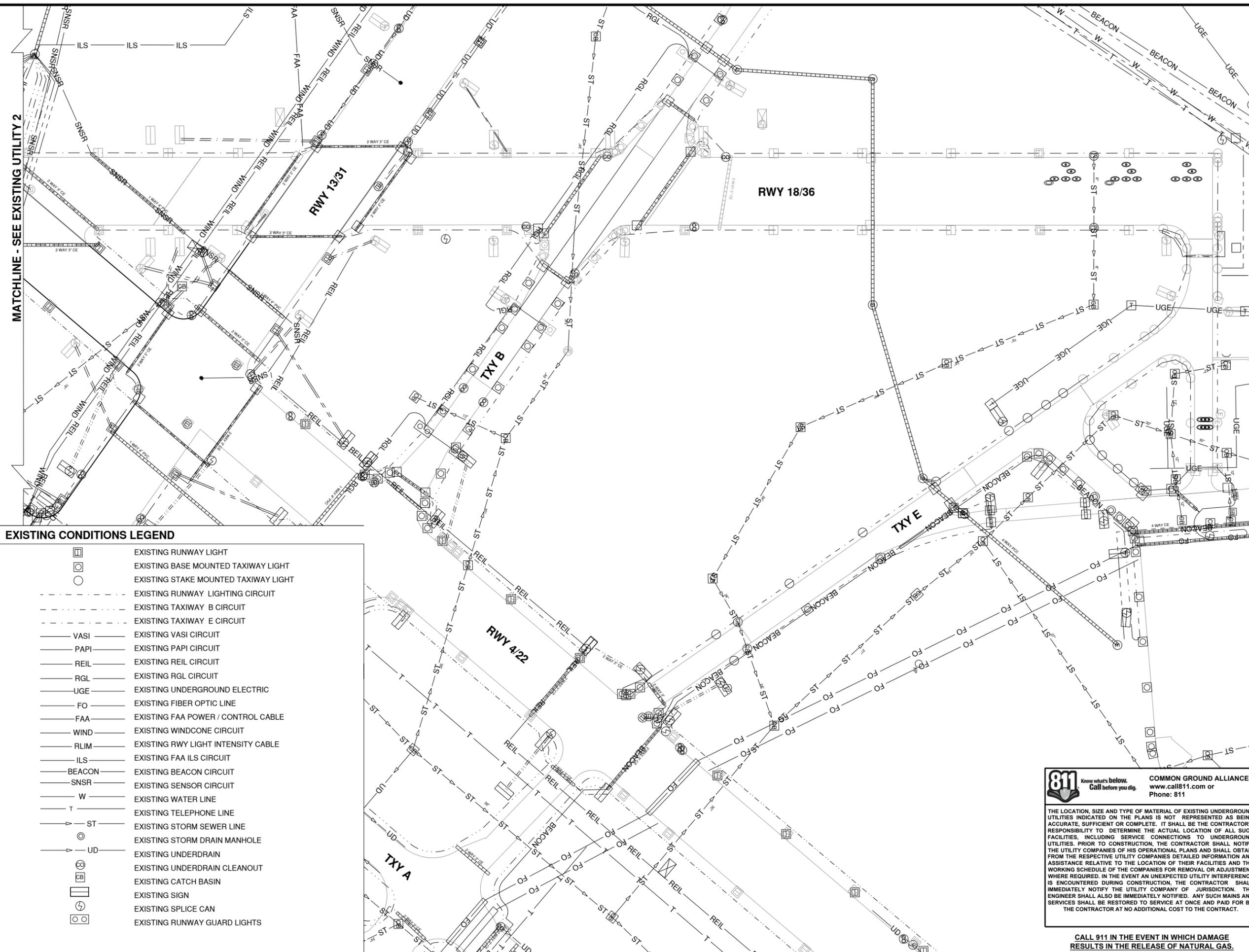
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SHEET TITLE

EXISTING UTILITIES 2

CD003

SHEET 10 OF 63



MATCHLINE - SEE EXISTING UTILITY 2

EXISTING CONDITIONS LEGEND

- EXISTING RUNWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING RUNWAY LIGHTING CIRCUIT
- EXISTING TAXIWAY B CIRCUIT
- EXISTING TAXIWAY E CIRCUIT
- VASI EXISTING VASI CIRCUIT
- PAPI EXISTING PAPI CIRCUIT
- REIL EXISTING REIL CIRCUIT
- RGL EXISTING RGL CIRCUIT
- UGE EXISTING UNDERGROUND ELECTRIC
- FO EXISTING FIBER OPTIC LINE
- FAA EXISTING FAA POWER / CONTROL CABLE
- WIND EXISTING WINDCONE CIRCUIT
- RLIM EXISTING RWY LIGHT INTENSITY CABLE
- ILS EXISTING FAA ILS CIRCUIT
- BEACON EXISTING BEACON CIRCUIT
- SNSR EXISTING SENSOR CIRCUIT
- W EXISTING WATER LINE
- T EXISTING TELEPHONE LINE
- ST EXISTING STORM SEWER LINE
- EXISTING STORM DRAIN MANHOLE
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING CATCH BASIN
- EXISTING SIGN
- EXISTING SPLICE CAN
- EXISTING RUNWAY GUARD LIGHTS

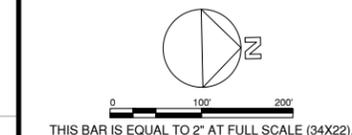
811 Know what's below. Call before you dig.

COMMON GROUND ALLIANCE
www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

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SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

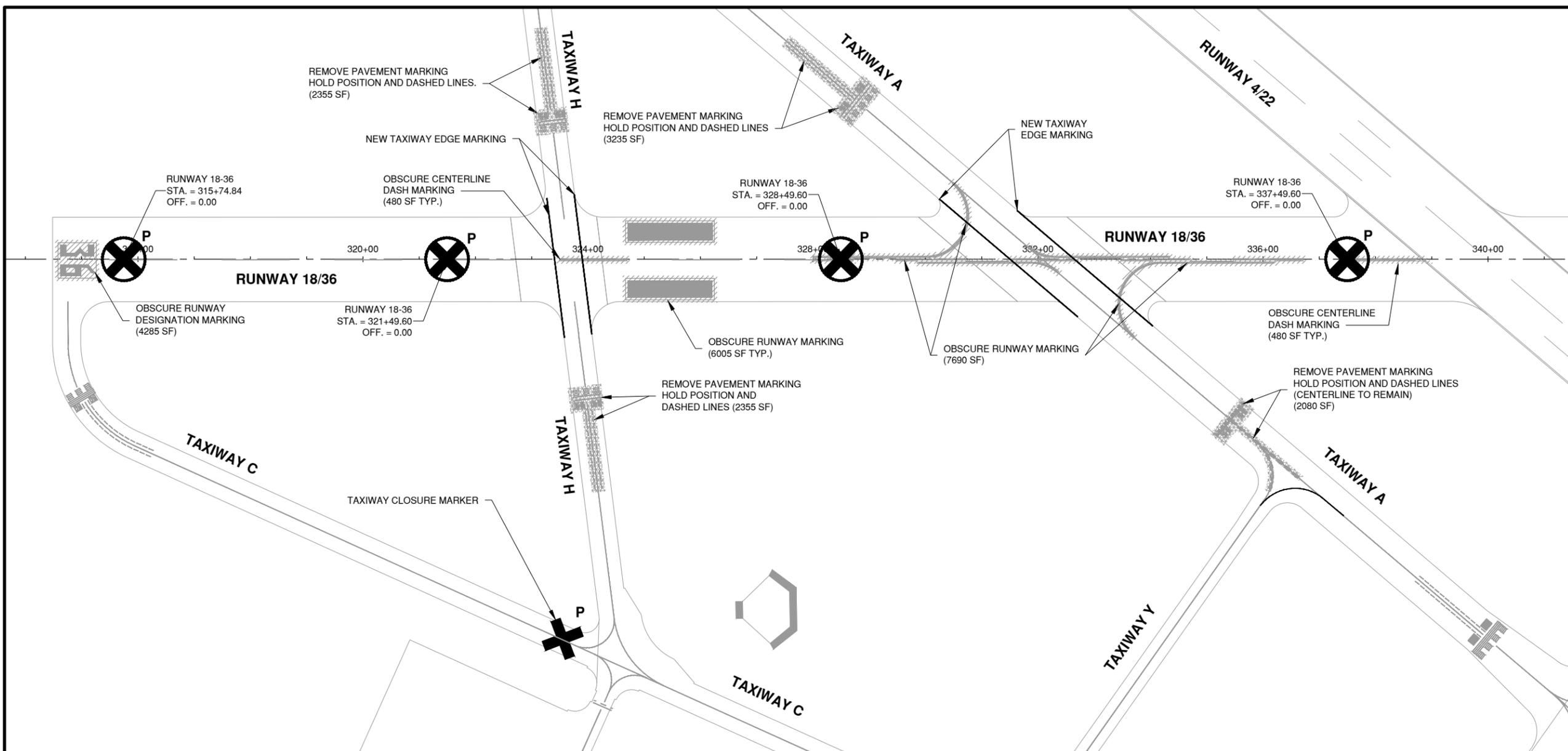


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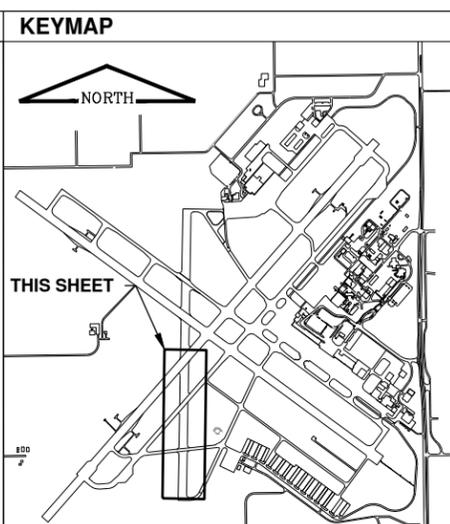
RUNWAY 18-36 CLOSURE 1

CD100
SHEET 16 OF 63

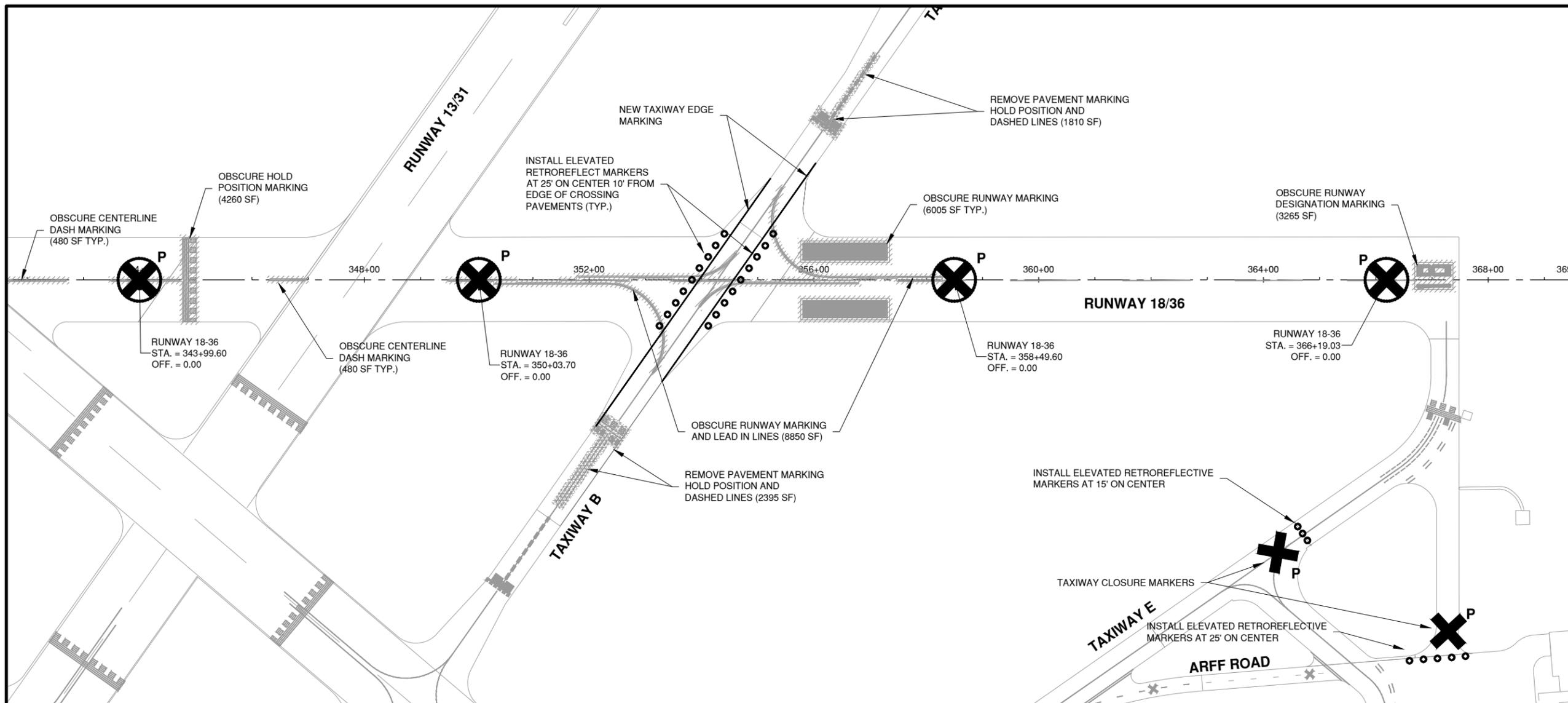
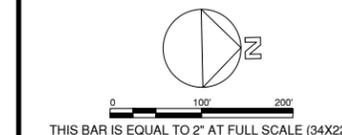


- NOTES**
- CONTRACTOR SHALL NOTIFY THE AIRPORT THROUGH THE RPR 72 HOURS PRIOR TO THE INITIATION OF ANY WORK.
 - DISCONNECT EXISTING RUNWAY 18/36 EDGE LIGHTING CIRCUIT AT VAULT.
 - WHERE INDICATED, OBSCURE INDICATED RUNWAY 18/36 MARKINGS BY APPLYING BITUMINOUS TACK COAT (ITEM AR603510) IN A QUANTITY SUFFICIENT TO COMPLETELY OBSCURE THE PAVEMENT MARKING TO THE SATISFACTION OF THE RPR AND THE AIRPORT. TACK COAT SHALL BE APPLIED IN AN AREA THAT EXTENDS 24" BEYOND THE EDGE OF THE MARKING TO BE OBSCURED.
 - WHERE INDICATED, REMOVE PAVEMENT MARKINGS IN ACCORDANCE WITH ITEM AR620900.
 - RUNWAY DESIGNATION MARKINGS SHALL BE OBSCURED AS SHOWN IN THE DETAIL ON THIS SHEET.
 - PAINT RUNWAY AND TAXIWAY CLOSURE MARKERS AT THE LOCATIONS INDICATED (ITEM AR620520).
 - PLACE ELEVATED RETROREFLECTIVE MARKERS (ITEM AR125100) AT THE LOCATIONS INDICATED.
 - REAPPLY EMULSIFIED ASPHALT TO OBSCURED MARKINGS AS REQUESTED BY THE AIRPORT OR THE RPR THROUGHOUT CONSTRUCTION.
 - INGRESS TO AND EGRESS FROM THE WORK AREA WILL REQUIRE CROSSING ACTIVE AIRFIELD PAVEMENT. CONTRACTOR SHALL REQUEST ACCESS TO THE SITE VIA ATCT RADIO CONTACT.
 - WORK ON THIS SHEET SHALL BE COMPLETE AND ACCEPTED BY THE RPR PRIOR TO BEGINNING ANY OTHER CONSTRUCTION ACTIVITIES ON THE AIRFIELD PAVEMENTS.
 - RUNWAY 18/36 CLOSURE MARKERS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.

- LEGEND**
- BEAM BARRICADES
 - ELEVATED RETROREFLECTIVE MARKER
 - OBSCURE PAVEMENT MARKINGS
 - REMOVE PAVEMENT MARKINGS
 - P RUNWAY CLOSURE MARKER (PAINTED)
 - P TAXIWAY CLOSURE MARKER (PAINTED)



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Date: Monday, September 21, 2020 9:40:14 AM



SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

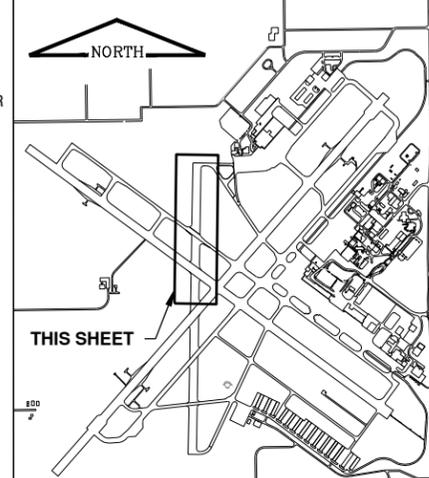
NOTES

1. CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR THROUGH THE RPR 72 HOURS PRIOR TO THE INITIATION OF ANY WORK.
2. DISCONNECT EXISTING RUNWAY 18/36 EDGE LIGHTING CIRCUIT AT VAULT.
3. WHERE INDICATED, OBSCURE INDICATED RUNWAY 18/36 MARKINGS BY APPLYING BITUMINOUS TACK COAT (ITEM AR603510) IN A QUANTITY SUFFICIENT TO COMPLETELY OBSCURE THE PAVEMENT MARKING TO THE SATISFACTION OF THE RPR AND THE AIRPORT. TACK COAT SHALL BE APPLIED IN AN AREA THAT EXTENDS 24" BEYOND THE EDGE OF THE MARKING TO BE OBSCURED.
4. WHERE INDICATED, REMOVE PAVEMENT MARKINGS IN ACCORDANCE WITH ITEM AR620900.
5. RUNWAY DESIGNATION MARKINGS SHALL BE OBSCURED AS SHOWN IN THE DETAIL ON THIS SHEET.
6. PAINT RUNWAY AND TAXIWAY CLOSURE MARKERS AT THE LOCATIONS INDICATED (ITEM AR620520).
7. PLACE ELEVATED RETROREFLECTIVE MARKERS (ITEM AR125100) AT THE LOCATIONS INDICATED.
8. REAPPLY EMULSIFIED ASPHALT TO OBSCURED MARKINGS AS REQUESTED BY THE AIRPORT OR THE RPR THROUGHOUT CONSTRUCTION.
9. INGRESS TO AND EGRESS FROM THE WORK AREA WILL REQUIRE CROSSING ACTIVE AIRFIELD PAVEMENT. CONTRACTOR SHALL REQUEST ACCESS TO THE SITE VIA ATCT RADIO CONTACT.
10. WORK ON THIS SHEET SHALL BE COMPLETE AND ACCEPTED BY THE RPR PRIOR TO BEGINNING ANY OTHER CONSTRUCTION ACTIVITIES ON THE AIRFIELD PAVEMENTS.
11. RUNWAY 18/36 CLOSURE MARKERS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.

LEGEND

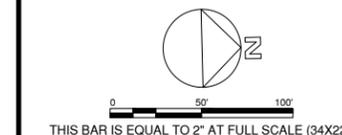
- BEAM BARRICADES
- ELEVATED RETROREFLECTIVE MARKER
- OBSCURE PAVEMENT MARKINGS
- REMOVE PAVEMENT MARKINGS
- P RUNWAY CLOSURE MARKER (PAINTED)
- P TAXIWAY CLOSURE MARKER (PAINTED)

KEYMAP



MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA021
IL PROJ. NO: SPI-4755		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH1-CD100.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
CHECKED BY: MJD		
APPROVED BY: RLV		
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SHEET TITLE
RUNWAY 18-36 CLOSURE 2



SEPTEMBER 21, 2020

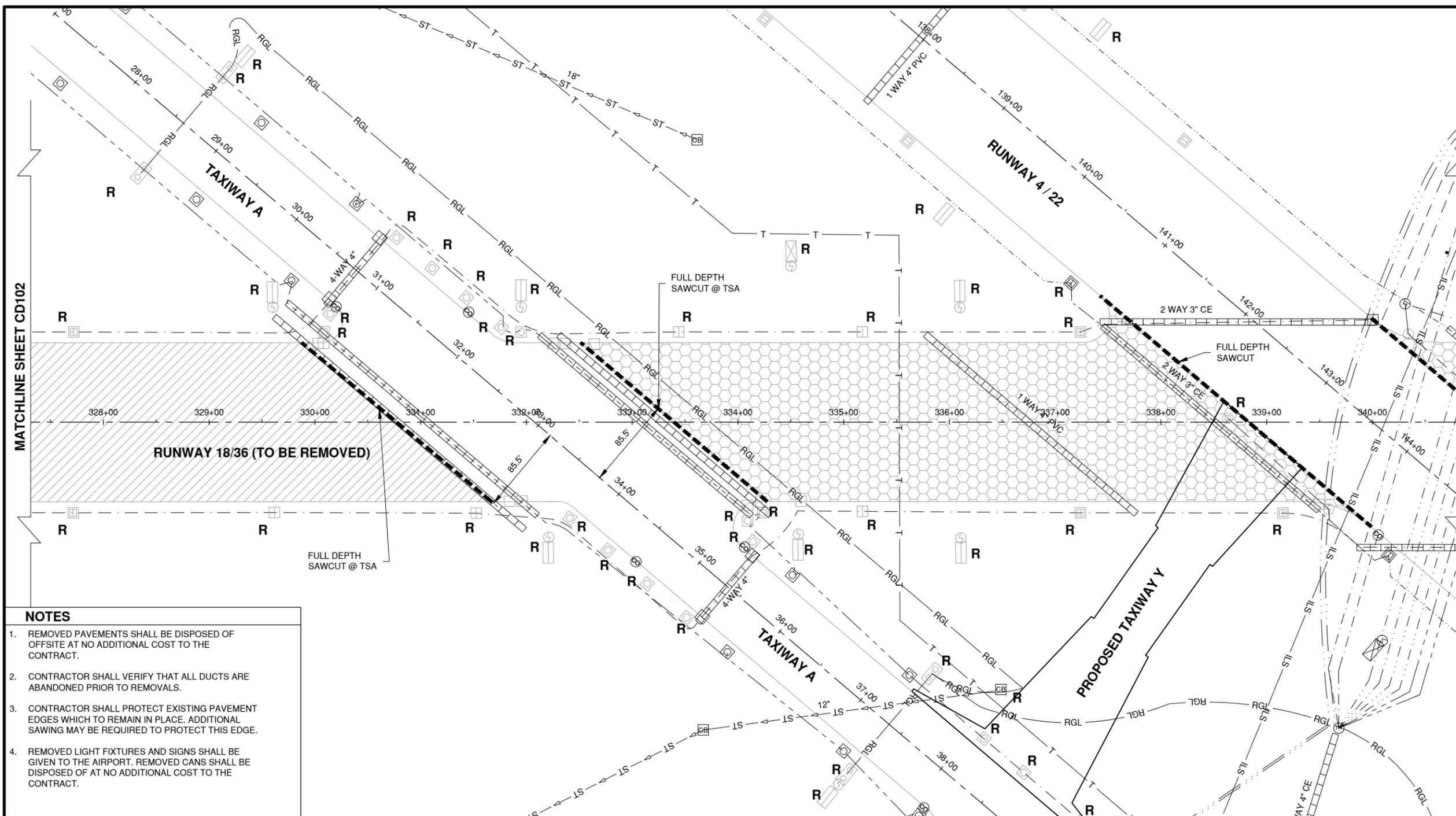
RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER


 SPRINGFIELD AIRPORT
 AUTHORITY
 ABRAHAM LINCOLN CAPITAL
 AIRPORT
 SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
AIP PROJ. NO: 3-17-0096-XX		CA021
IL PROJ. NO: SPI-4755		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH1-CD102.DWG		
DESIGNED BY: HWI		
DRAWN BY: DPA		
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EXISTING CONDITIONS & REMOVALS 2

 SHEET TITLE
CD103
 SHEET 19 OF 63


- NOTES**
1. REMOVED PAVEMENTS SHALL BE DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CONTRACT.
 2. CONTRACTOR SHALL VERIFY THAT ALL DUCTS ARE ABANDONED PRIOR TO REMOVALS.
 3. CONTRACTOR SHALL PROTECT EXISTING PAVEMENT EDGES WHICH TO REMAIN IN PLACE. ADDITIONAL SAWING MAY BE REQUIRED TO PROTECT THIS EDGE.
 4. REMOVED LIGHT FIXTURES AND SIGNS SHALL BE GIVEN TO THE AIRPORT. REMOVED CANS SHALL BE DISPOSED OF AT NO ADDITIONAL COST TO THE CONTRACT.

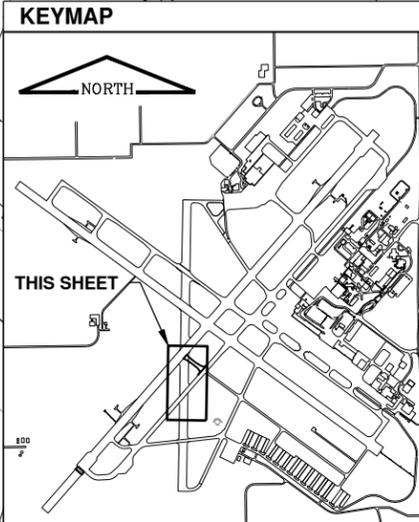
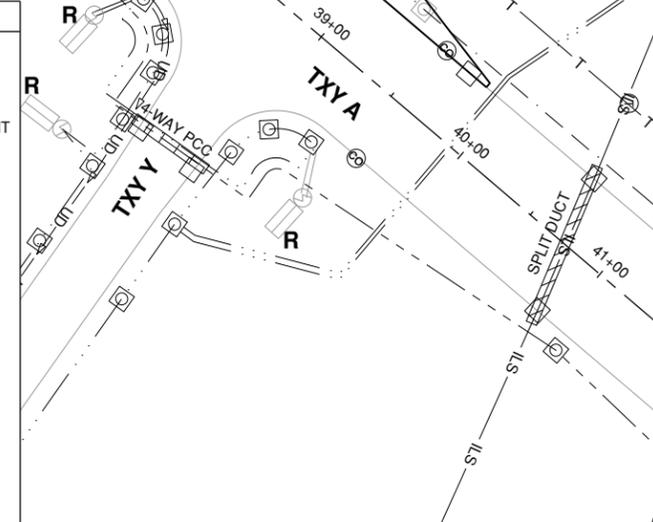
UTILITY DISCLAIMER

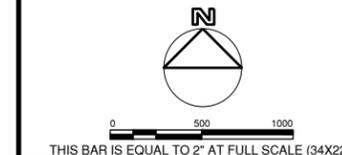
THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

LEGEND

	REM. PVMT. STRUCTURE (HMA / PCC)	R	LIGHT OR SIGN TO BE REMOVED
	REM. PVMT. STRUCTURE (HMA / PCC) ADDITIVE ALTERNATE 1	M	MODIFY SIGN PANEL
	FULL DEPTH SAWCUT		EXIST. RWY THRESHOLD STAKE-MOUNTED LIGHT
	EXIST. DUCT BANK		EXIST. RUNWAY BASE-MOUNTED LIGHT
	EXIST. STORM SEWER		EXIST. RUNWAY STAKE-MOUNTED LIGHT
	EXIST. CABLE TO BE ABN'D IN PLACE		EXIST. TAXIWAY BASE-MOUNTED LIGHT
			EXIST. TAXIWAY STAKE-MOUNTED LIGHT
			EXIST. RUNWAY GUARD LIGHT
			EXIST. SIGN
			EXIST. DISTANCE REMAINING SIGN
			EXIST. INLET / CATCH BASIN
			EXIST. CLEANOUT
			EXIST. DUCT MARKER



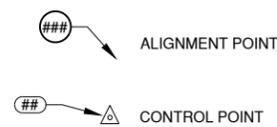


CONTROL POINT TABLE

POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
11003	103 IPIN	1159636.849'	2434842.840'	587.19'
11004	103 IPIN	1159466.767'	2434133.844'	587.66'
11005	103 IPIN	1158897.523'	2434256.542'	586.66'
11006	103 IPIN	1158619.807'	2433193.514'	588.43'
11007	103 IPIN	1158202.064'	2433751.538'	586.45'
11008	103 IPIN	1158224.510'	2432899.265'	588.36'
11009	103 IPIN	1157776.500'	2433501.190'	589.49'
11010	103 IPIN	1156239.181'	2433195.251'	588.13'
11011	103 IPIN	1156730.933'	2433693.303'	588.79'
11012	103 IPIN	1156972.836'	2433512.577'	589.43'
11013	103 IPIN	1156961.517'	2433165.713'	589.72'

ALIGNMENT POINT TABLE

POINT	DESCRIPTION	NORTHING	EASTING
101	RWY 18/36 STA 313+00	1154265.684'	2433279.034'
102	RWY 18/36 STA 369+00	1159864.931'	2433370.878'
103	RWY 4/22 STA 102+00	1153842.673'	2430578.774'
104	RWY 4/22 STA 198+00	1161053.405'	2436916.387'
105	RWY 13/31 STA 290+00	1155079.996'	2436971.296'
106	RWY 13/31 STA 205+00	1160112.085'	2430120.888'
201	TX Y STA 50+00	1161404.112'	2436266.057'
202	TX Y STA 1+00	1157723.634'	2433031.234'
203	TX Y STA 1255+00	1157576.668'	2434458.006'
204	TX Y STA 1210+00	1160240.715'	2430831.320'
205	TX Y STA 1+00	1159926.168'	2433480.525'
206	TX Y STA 22+00	1158181.938'	2434649.996'
207	TX Y STA 1+00	1157075.813'	2433041.737'
208	TX Y STA 12+00	1156424.601'	2433928.260'
209	TX Y STA 15+00	1154824.510'	2432125.248'
210	TX Y STA 43+00	1156927.715'	2433973.640'
211	TX Y STA 5+00	1155453.737'	2431886.769'
212	TX Y STA 8+67.09	1155207.697'	2432161.607'
213	TX Y STA 28+00	1155416.441'	2434083.208'
214	TX Y STA 5+00	1154439.039'	2433281.878'
215	TX Y STA 6+64.40	1154436.660'	2433428.255'
216	TX Y STA 8+75.59	1154551.247'	2433612.362'
217	TX Y STA 30+00	1156472.321'	2434519.339'



SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

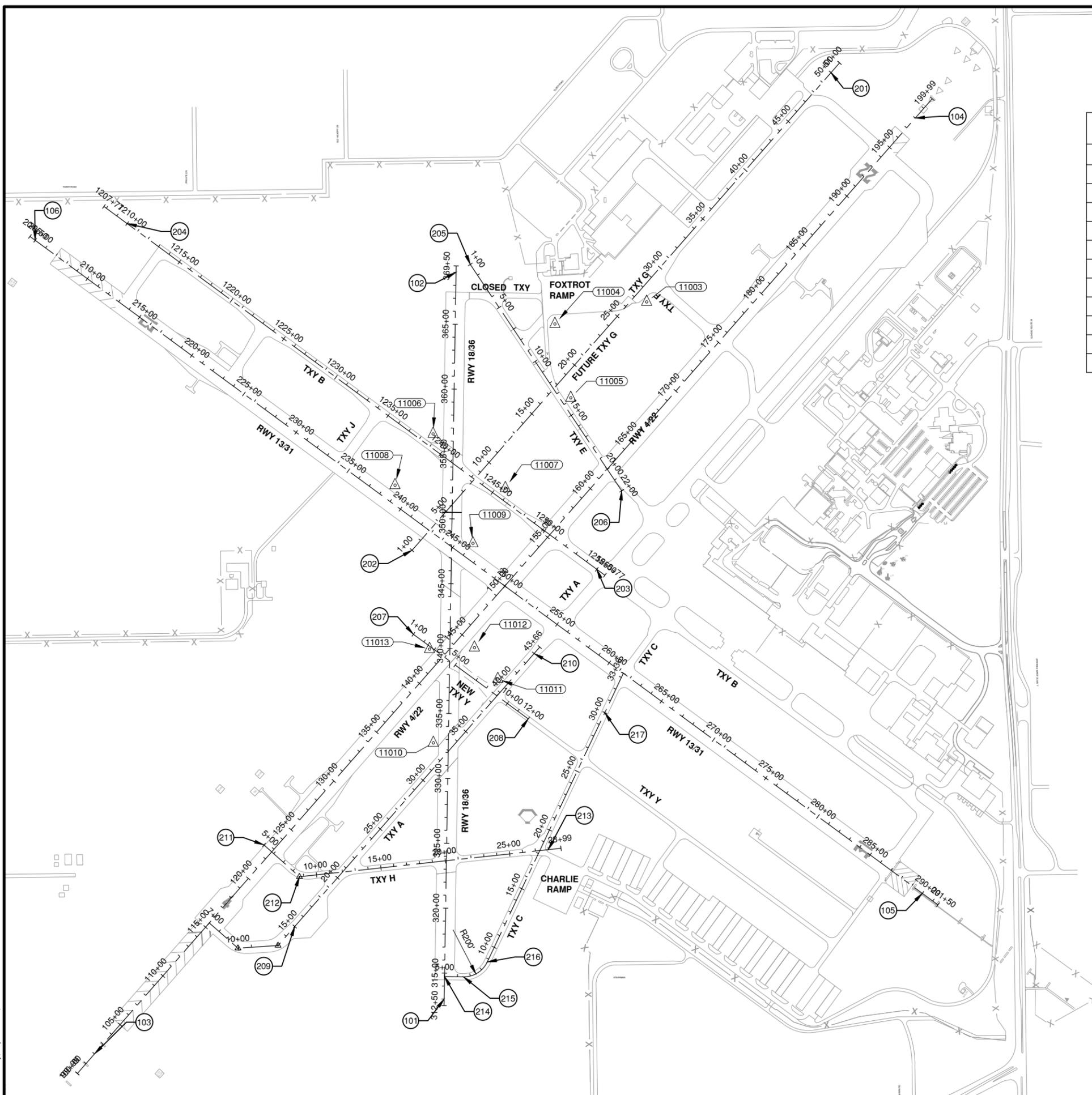


**SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL**

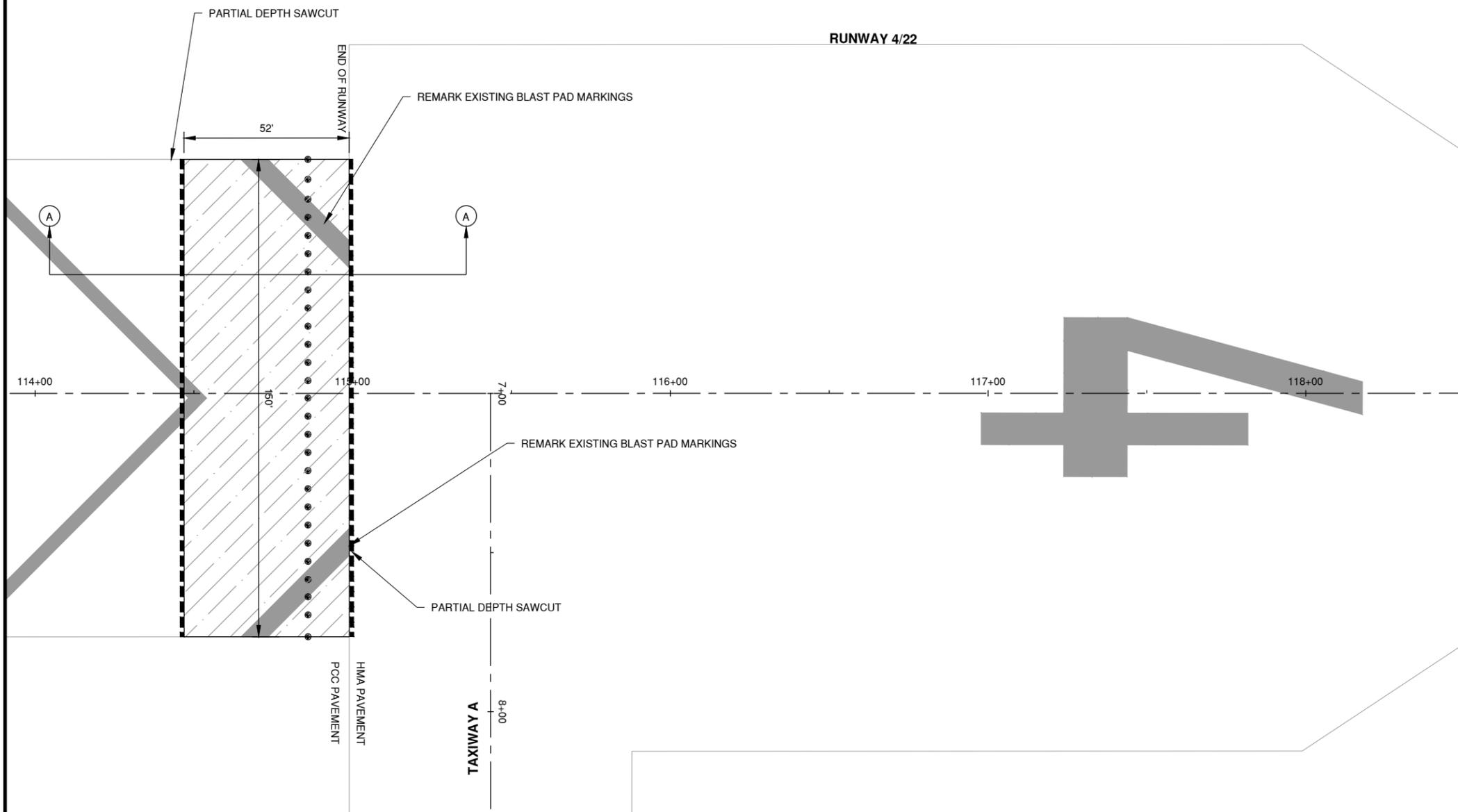
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IL PROJ. NO:	SPI-4755	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH1-CP100.DWG	
DESIGNED BY:	HWI	
DRAWN BY:	DPA	
CHECKED BY:	MJD	
APPROVED BY:	RLV	
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**SHEET TITLE
HORIZONTAL CONTROL**

CP100
SHEET **22** OF **63**



RUNWAY 4/22



LEGEND

-  PAVEMENT TO BE MILLED AND REPLACED
-  EXISTING FLUSH MOUNTED THRESHOLD LIGHT
-  PAVEMENT MARKING
-  PARTIAL DEPTH SAWCUT

- ### NOTES
1. THE IMPROVEMENTS SHOWN ON THIS SHEET ARE INCLUDED WITH THE BASE BID.
 2. CONTRACTOR SHALL REMOVE AND STORE EXISTING THRESHOLD LIGHTS AND INSTALL A TEMPORARY COVER OVER THE EXISTING LIGHT BASES TO PROTECT THE EXISTING BASES DURING CONSTRUCTION. (AR106910)
 3. PAVEMENT MARKINGS REMOVED BY MILLING SHALL BE REPAINTED IN THEIR ORIGINAL POSITION.
 4. REPLACE THE THRESHOLD LIGHTS UPON COMPLETION OF OVERLAY AND PAVEMENT MARKING. (AR106930)
 5. TACK COAT TO BE APPLIED PRIOR TO REPAVING. LIGHT BASES SHALL BE COVERED PER NOTE 2 ABOVE.



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SEPTEMBER 21, 2020

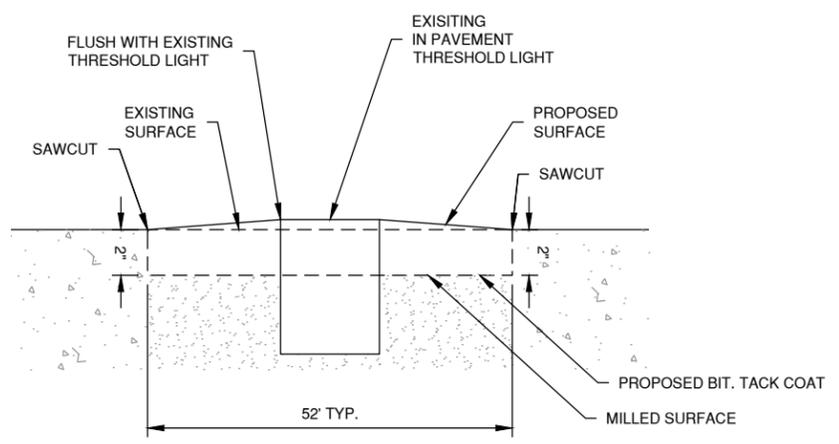
RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

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SECTION A-A
N.T.S.

KEYMAP

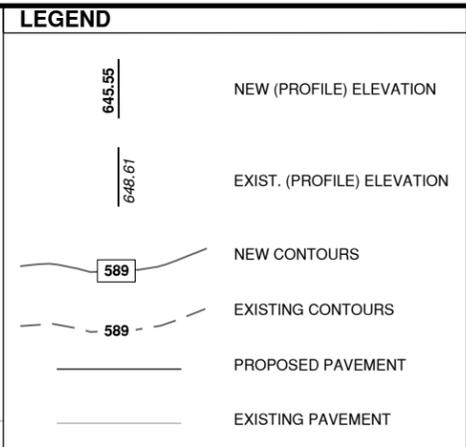
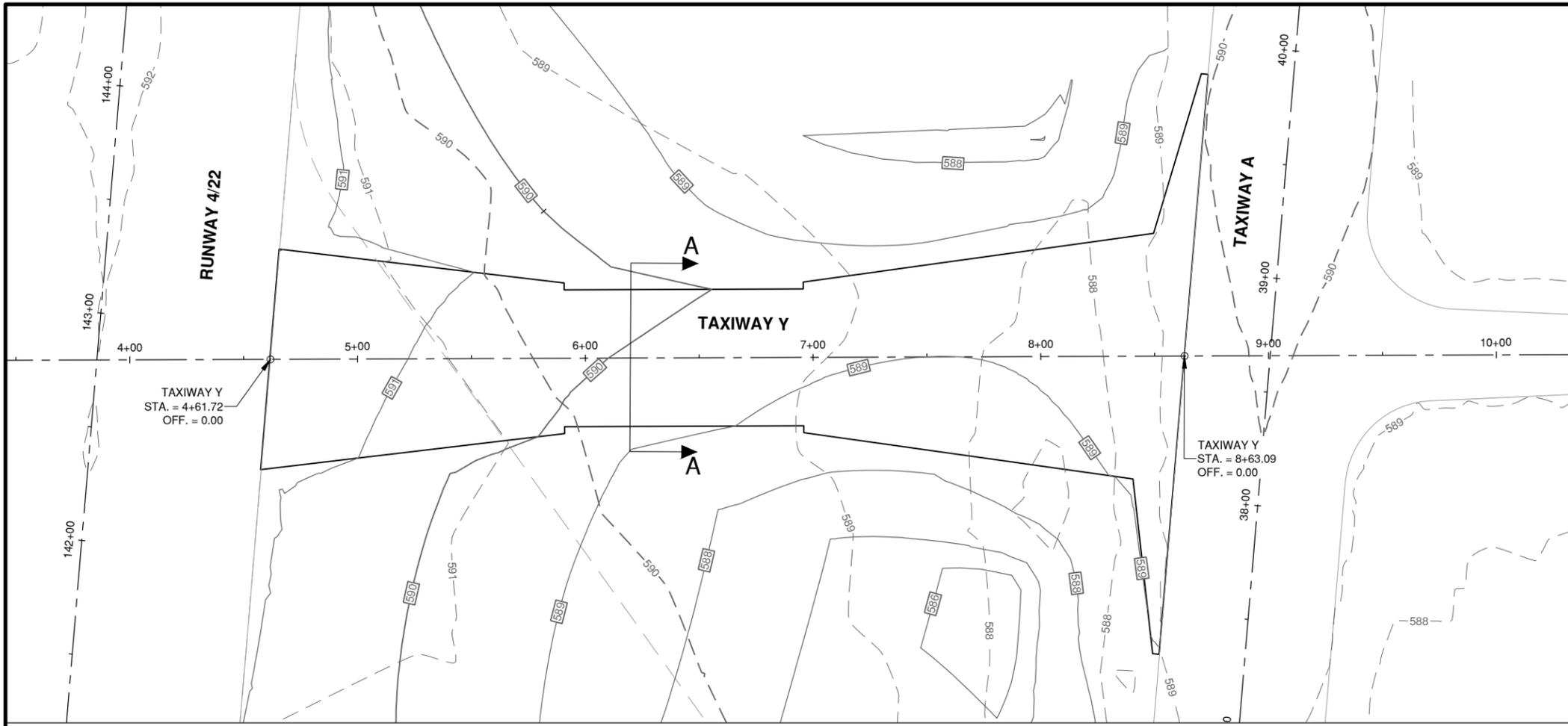
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SHEET TITLE

RUNWAY 4 THRESHOLD IMPROVEMENTS

CP102

SHEET 24 OF 63



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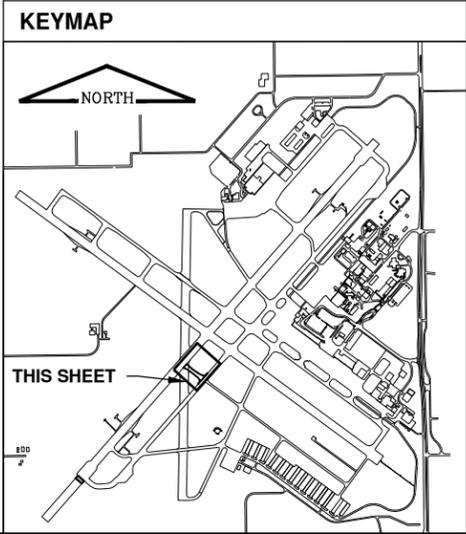
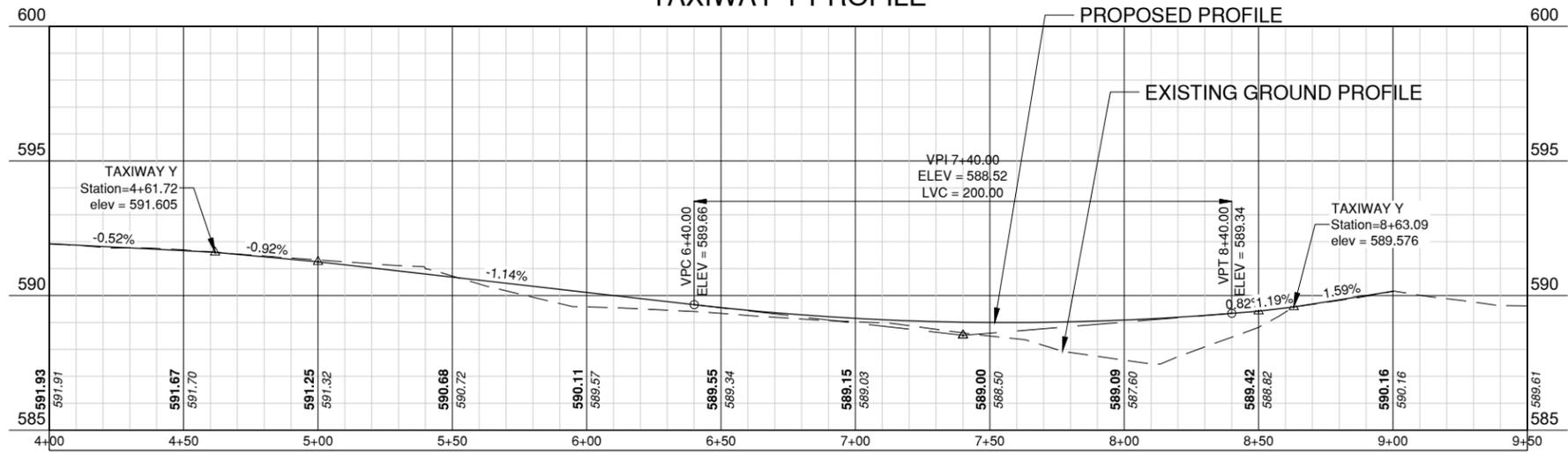
SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

TAXIWAY Y PROFILE



MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
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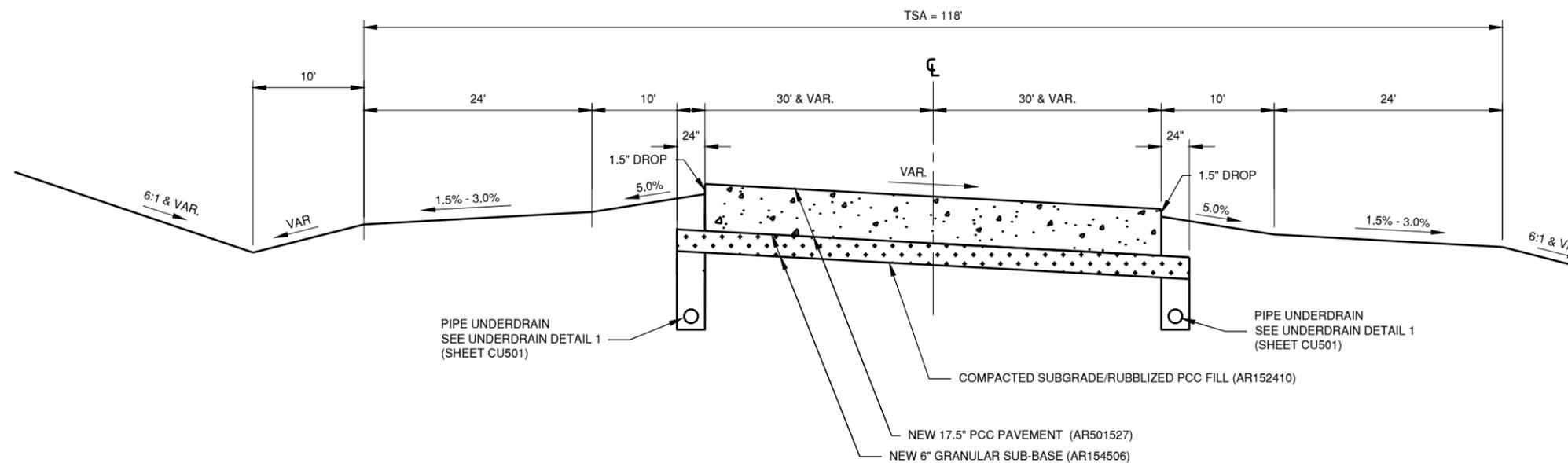
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PLAN & PROFILE - TAXIWAY Y

CP201

SHEET 25 OF 63

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Date: Monday, September 21, 2020 9:43:43 AM



TYPICAL TAXIWAY SECTION
N.T.S.

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



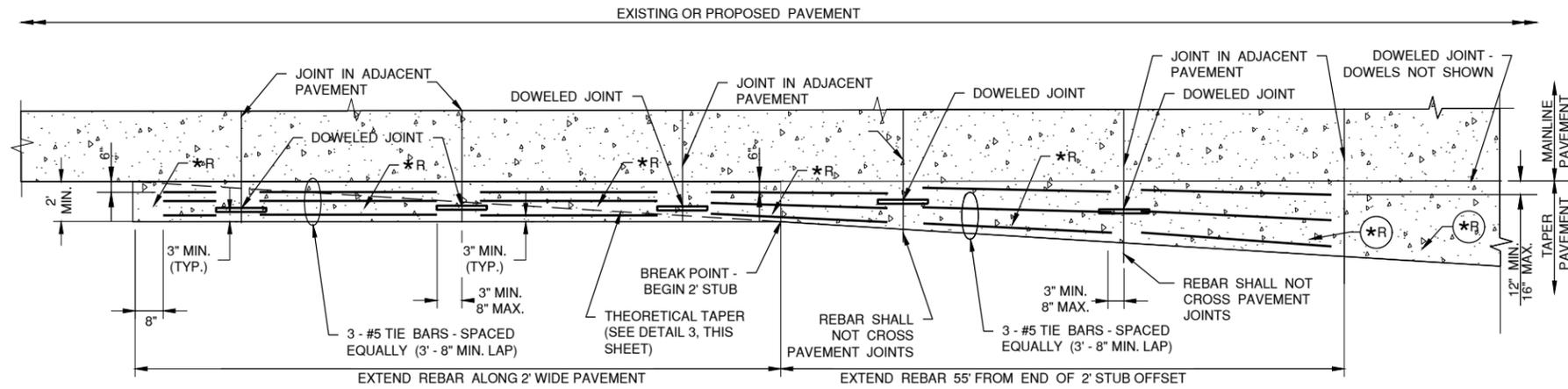
SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION

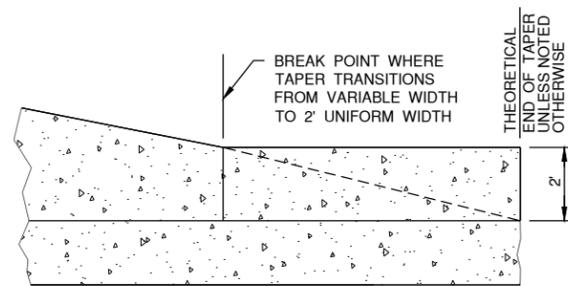
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SHEET TITLE
TYPICAL SECTION

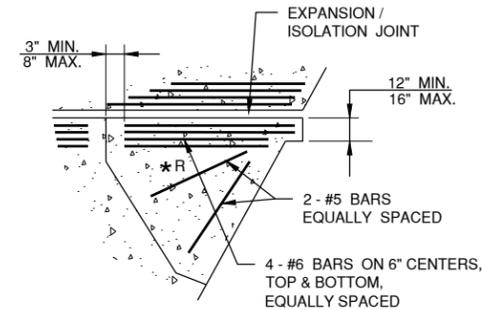
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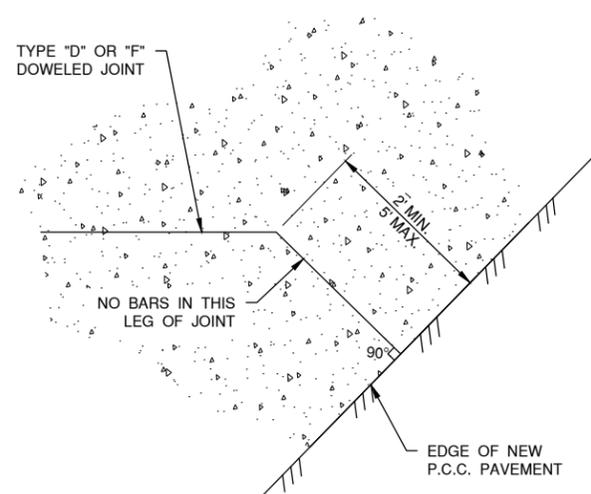
1 **FILLET TAPER DETAIL**
N.T.S. R* ODD SHAPED PANEL REINFORCEMENT



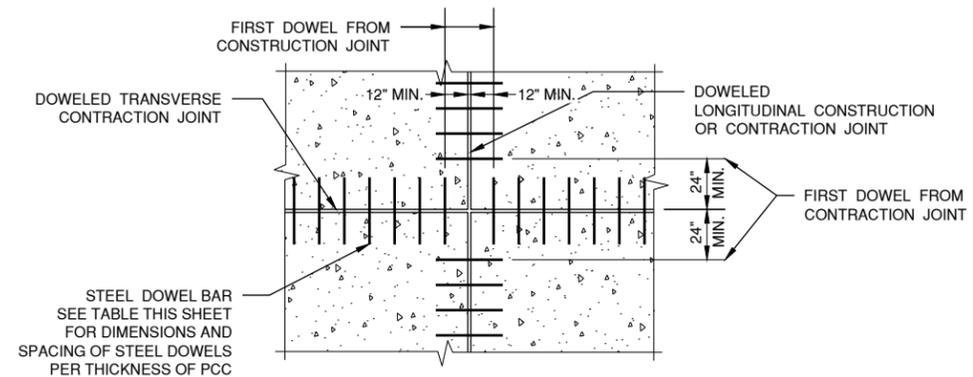
2 **STUB DETAIL - EXTEND**
N.T.S. TYPICAL AT END OF TAPER



3 **STUB DETAIL**
N.T.S. TYPICAL AT ODD-SHAPED PANEL AT EXPANSION / ISOLATION JOINT
R* ODD SHAPED PANEL REINFORCEMENT



4 **PAVING DETAIL**
N.T.S.



5 **DOWEL SPACING REQUIREMENTS AT INTERSECTING JOINTS**
N.T.S.

SEPTMBER 21, 2020
RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

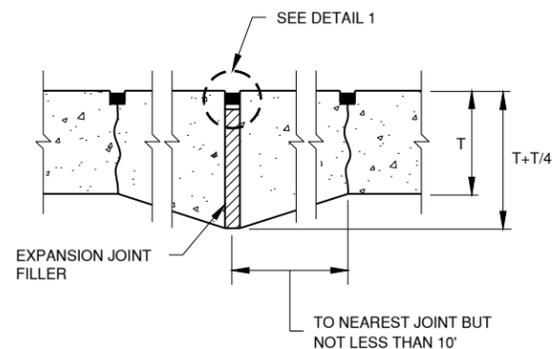


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

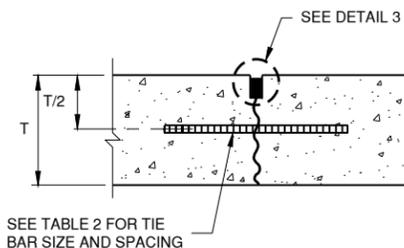
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SHEET TITLE
PAVING DETAILS 1

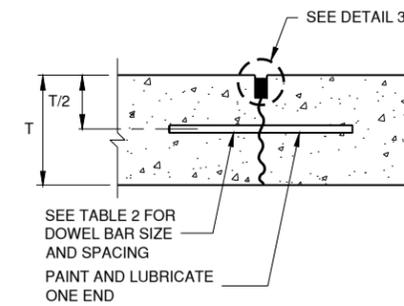
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"


TYPE A THICKENED ISOLATION

SYMBOL


TYPE B HINGED (TIED) CONTRACTION

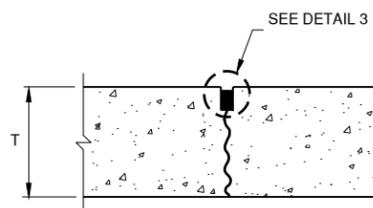
SYMBOL


TYPE C DOWELED CONTRACTION

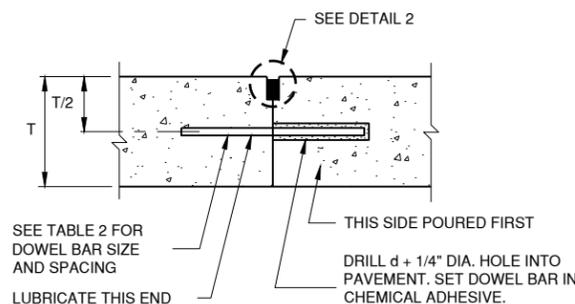
SYMBOL



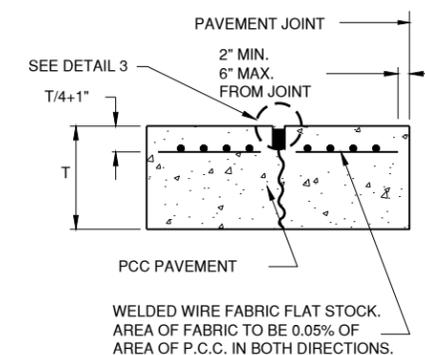
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
6	3/4"	18"	12" O.C.	#5	30"	30" O.C.
7	3/4"	18"	12" O.C.	#5	30"	30" O.C.
8	1"	19"	12" O.C.	#5	30"	30" O.C.
9	1"	19"	12" O.C.	#5	30"	30" O.C.
10	1"	19"	12" O.C.	#5	30"	30" O.C.
11	1"	19"	12" O.C.	#5	30"	30" O.C.
12	1"	19"	12" O.C.	#5	30"	30" O.C.
13	1 - 1/4"	20"	15" O.C.	#5	30"	30" O.C.
14	1 - 1/4"	20"	15" O.C.	#5	30"	30" O.C.
15	1 - 1/4"	20"	15" O.C.	#5	30"	30" O.C.
16	1 - 1/4"	20"	15" O.C.	#5	30"	30" O.C.
17	1 - 1/2"	20"	18" O.C.	#5	30"	30" O.C.
18	1 - 1/2"	20"	18" O.C.	#5	30"	30" O.C.


TYPE D DUMMY CONTRACTION

SYMBOL


TYPE E DOWELED CONSTRUCTION

SYMBOL


ODD SHAPED PANEL REINFORCEMENT

SYMBOL



NOTES:

1. ODD SHAPED PANELS INCLUDE ALL PANELS THAT EXCEED LENGTH TO WIDTH RATIO OF 1.25

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER


**SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL**

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX CA021

IL PROJ. NO: SPI-4755

CMT PROJECT NO: 180035-05

CAD DWG FILE: 18003505-PH1-CJ500.DWG

DESIGNED BY: HWI

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: RLV

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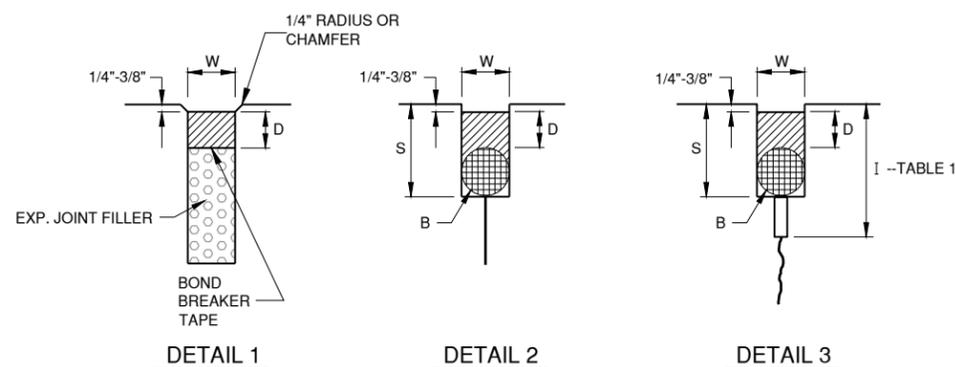
SHEET TITLE

PAVING DETAILS 2

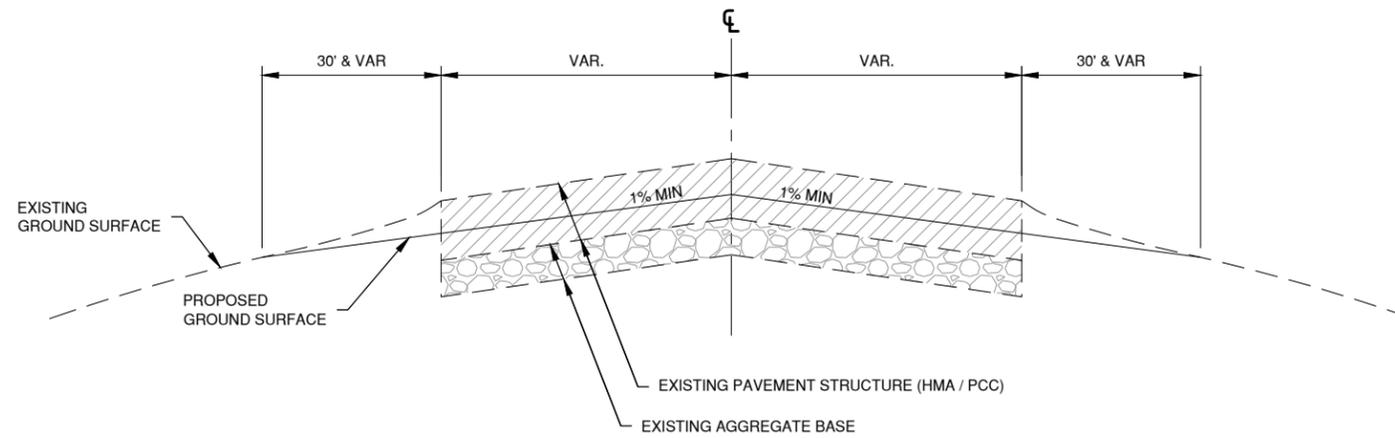
 CJ502
SHEET 31 OF 63

JOINT NOTES

- 1.) ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- 2.) THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- 3.) ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- 4.) TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- 5.) THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

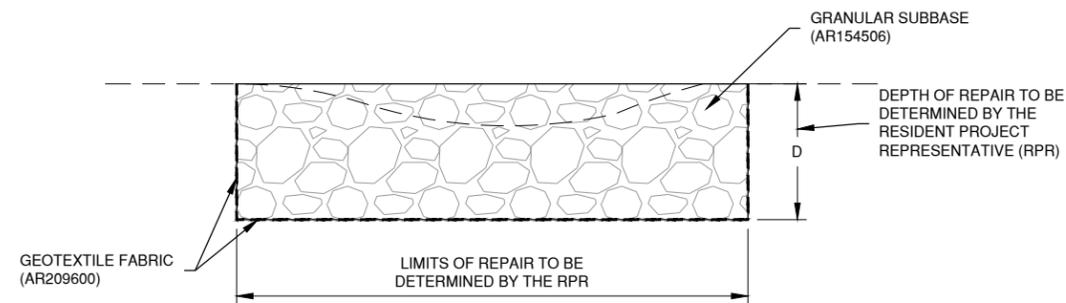

JOINT SEALING DETAILS

	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8



TYPICAL PAVEMENT REMOVAL SECTION

N.T.S.



SUBGRADE REPAIR DETAIL

N.T.S.

SUBGRADE REPAIR NOTES

1. THE REMOVAL AREA AND DEPTH SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION BY THE RPR.
2. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - A. REMOVE SOIL SUBGRADE TO THE DEPTH SPECIFIED BY THE RPR.
 - B. COMPACT THE SUBGRADE TO THE SATISFACTION OF THE RPR.
 - C. PLACE SEPARATION GEOTEXTILE FABRIC ON TOP OF THE COMPACTED SUBGRADE AND ALONG THE SIDES OF THE EXCAVATION.
 - D. PLACE AND CONSOLIDATE GRANULAR SUBBASE TO THE SATISFACTION OF THE RPR.
3. THE FOLLOWING SHALL BE CONSIDERED INCIDENTAL TO THE SUBGRADE REPAIR PAY ITEM:
 - A. SUBGRADE REMOVAL
 - B. SUBGRADE COMPACTION
 - C. SUBBASE GRANULAR
4. GEOTEXTILE FABRIC SHALL BE PAID UNDER ITEM AR209600.

SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER

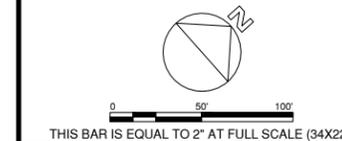


**SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL**

MARK	DATE	DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-CG301.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
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SHEET TITLE
GRADING DETAILS



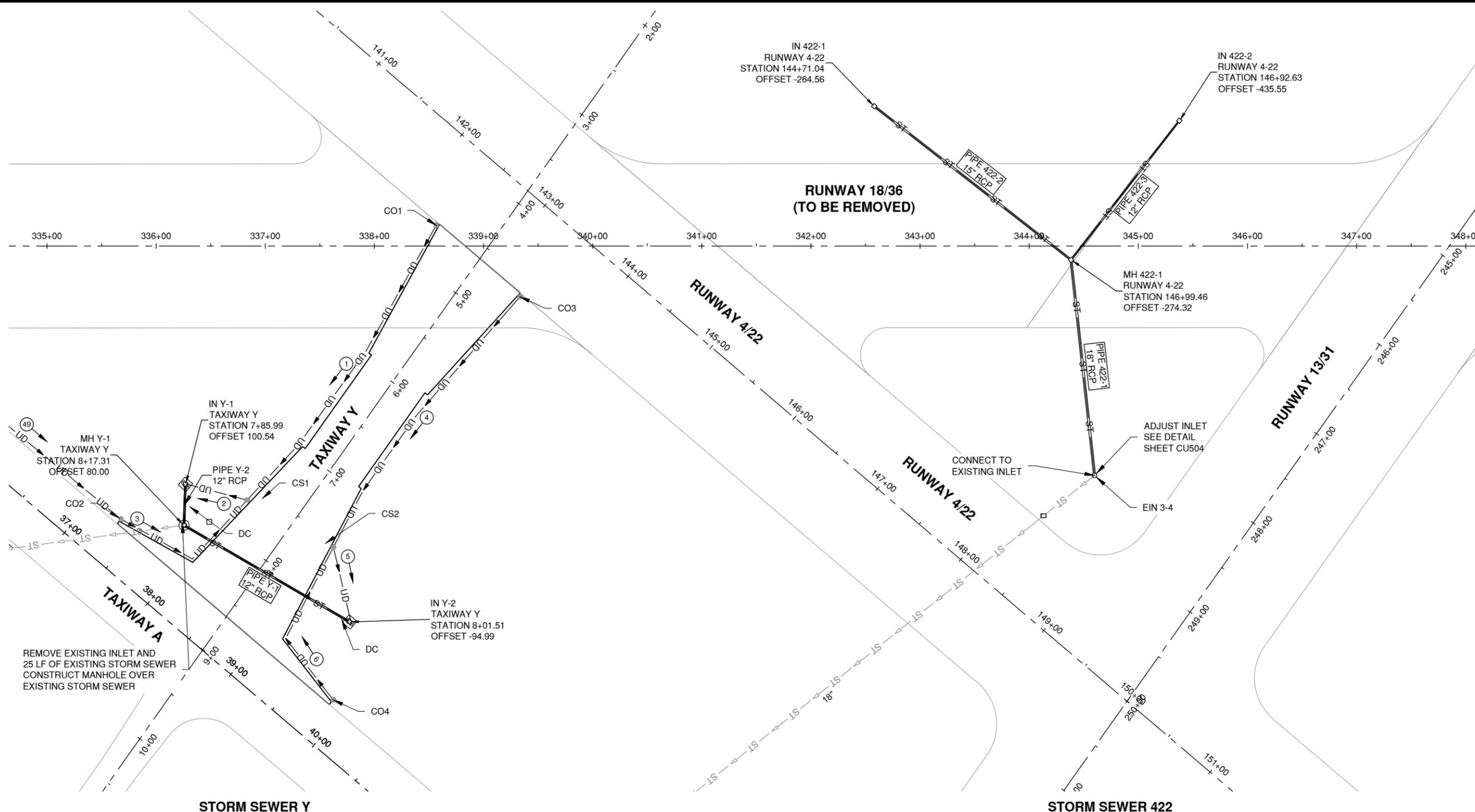
SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL



REMOVE EXISTING INLET AND 25 LF OF EXISTING STORM SEWER CONSTRUCT MANHOLE OVER EXISTING STORM SEWER

STORM SEWER Y

STORM SEWER 422

NOTES	LEGEND	KEYMAP
<p>1. THE ADJUSTMENT OF EXISTING INLET EIN 3-4 AND THE INSTALLATION OF PIPE 422-1 SHALL BE CONSTRUCTED IN PHASE 3 (NIGHT WORK).</p>	<p>— EXISTING PAVEMENT EDGE</p> <p>— ST — EXISTING STORM SEWER</p> <p>— PROPOSED STORM SEWER</p> <p>— UD — PROPOSED UNDERDRAIN</p> <p>● PROPOSED CLEANOUT/COLLECTION STRUCTURE</p> <p>① PROP. UNDERDRAIN RUN NUMBER</p> <p>□ EIN EXISTING INLET</p> <p>□ IN NEW INLET</p> <p>⊙ EXISTING MANHOLE</p>	<p>KEYMAP</p>

MARK	DATE	DESCRIPTION
AIP PROJ. NO:	3-17-0096-XX	CA021
IL PROJ. NO:	SPI-4755	
CMT PROJECT NO:	180035-05	
CAD DWG FILE:	18003505-PH1-CU100.DWG	
DESIGNED BY:	HWI	
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CHECKED BY:	MJD	
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STORM SEWER AND UNDERDRAIN PLAN

Path: K:\Springfield\Apl\180035-05_Rwy_18-36\DrawSheets\PHASE 1\18003505-PH1-CU100.dwg
Date: Monday, September 21, 2020 9:47:03 AM

EIN = EXISTING INLET
 IN = INLET - NEW
 MH = MANHOLE - NEW

STRUCTURE TABLE STORM SEWER Y		
STRUCTURE NAME AND TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL
IN Y-1 36" INLET	RIM = 585.40 SUMP = 581.67 Y-2 INV OUT = 582.67	TAXIWAY Y STA 7+85.99 OFFSET 100.54 R
IN Y-2 36" INLET	RIM = 586.94 SUMP = 583.06 Y-1 INV OUT = 584.06	TAXIWAY Y STA 8+01.51 OFFSET -94.99 L
MH Y-1 54" MANHOLE	RIM = 588.20 SUMP = 581.30 Y-2 INV IN = 582.30 Y-1 INV IN = 582.30	TAXIWAY Y STA 8+17.31 OFFSET 80.00 R

PIPE SCHEDULE STORM SEWER Y							
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
Y-1	IN Y-2	MH Y-1	584.06	582.30	164	1.00%	CONCRETE PIPE - 12"
Y-2	IN Y-1	MH Y-1	582.67	582.30	25	1.00%	CONCRETE PIPE - 12"

STRUCTURE TABLE STORM SEWER 422		
STRUCTURE NAME AND TYPE	STRUCTURE DETAILS	HORIZONTAL CONTROL
EIN 3-4 EXISTING INLET	RIM = 588.40 SUMP = 581.90 422-1 INV IN = 582.90	RUNWAY 4-22 STA 148+43.58 OFFSET -138.13 L
IN 422-1 36" INLET	RIM = 586.72 SUMP = 584.39 422-2 INV OUT = 584.39	RUNWAY 4-22 STA 144+71.04 OFFSET -264.56 L
IN 422-2 36" INLET	RIM = 587.04 SUMP = 583.18 422-3 INV OUT = 584.18	RUNWAY 4-22 STA 146+92.63 OFFSET -435.55 L
MH 422-1 54" MANHOLE	RIM = 587.19 SUMP = 583.49 422-2 INV IN = 583.70 422-3 INV IN = 583.70 422-1 INV OUT = 583.49	RUNWAY 4-22 STA 146+99.46 OFFSET -274.32 L

PIPE SCHEDULE STORM SEWER 422							
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	INVERT	INVERT	LENGTH (FT)	SLOPE	TYPE
422-3	IN 422-2	MH 422-1	584.18	583.70	149	0.30%	CONCRETE PIPE - 12"
422-1	MH 422-1	EIN 3-4	583.49	582.90	186	0.30%	CONCRETE PIPE - 18"
422-2	IN 422-1	MH 422-1	584.39	583.70	217	0.30%	CONCRETE PIPE - 15"

 SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND
 ASSOCIATED IMPROVEMENTS,
 PHASE 1

OWNER



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CAD DWG FILE: 18003505-PH1-CU100.DWG	
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SHEET TITLE
**STORM SEWER &
 UNDERDRAIN
 SCHEDULES**

CU102
 SHEET 37 OF 63

STRUCTURE SCHEDULE					
STRUCTURE	HORIZONTAL CONTROL			RIM ELEV.	INVERT ELEV.
	ALIGNMENT	STATION	OFFSET		
CO1	TAXIWAY Y	4+59.83	50.33 R	591.50	588.00
CO2	TAXIWAY Y	8+46.11	130.24 R	588.81	585.31
CO3	TAXIWAY Y	4+68.62	50.20 L	591.59	588.09
CO4	TAXIWAY Y	8+68.38	123.92 L	589.79	586.29
CS1	TAXIWAY Y	7+55.49	35.50 R	588.42	584.92
CS2	TAXIWAY Y	7+55.52	35.50 L	589.38	585.88

CSX - COLLECTION STRUCTURE
 COX - CLEAN OUT

UNDERDRAIN SCHEDULE								
STRUCTURE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	LENGTH (FT)	SLOPE (%)	TYPE	
UD-1	CO1	CS1	591.50	588.42	306.59	1.00%	PERFORATED	
UD-2	CS1	IN Y-3	0.00	581.90	56.61	-1027.91%	NON-PERFORATED	
UD-3	CO2	CS1	585.31	0.00	159.84	366.18%	PERFORATED	
UD-4	CO3	CS2	588.09	0.00	287.11	204.83%	PERFORATED	
UD-5	CS2	IN Y-2	0.00	583.06	68.59	-850.07%	NON-PERFORATED	
UD-6	CO4	CS2	586.29	0.00	165.21	354.88%	PERFORATED	
UD-49	CO29	CO2	586.06	585.31	209.90	0.36%	PERFORATED	

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

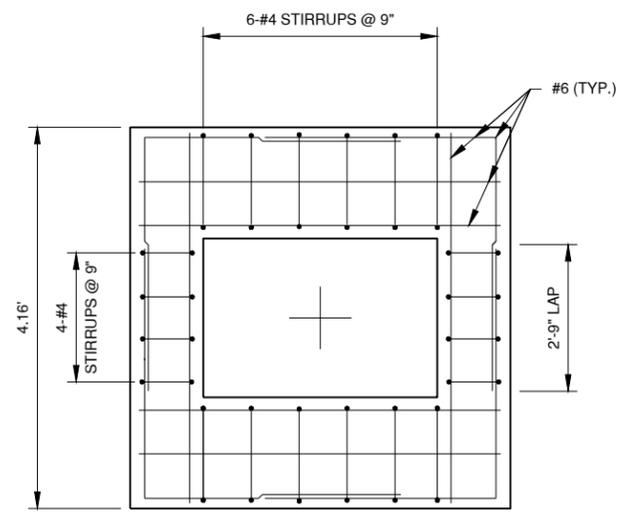
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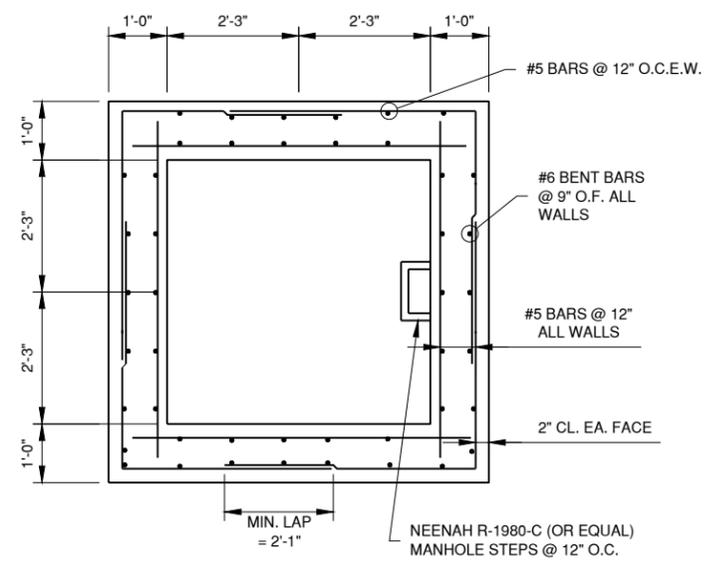
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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

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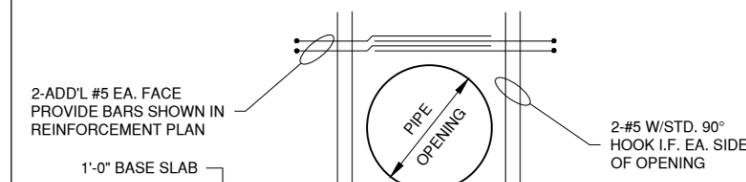
SHEET TITLE
STORM SEWER DETAILS 2



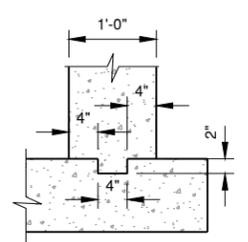
3 TOP REINFORCEMENT
N.T.S.



2 WALL REINFORCEMENT
N.T.S.



5 ADDITIONAL REINFORCEMENT AROUND PIPE PENETRATIONS
N.T.S.

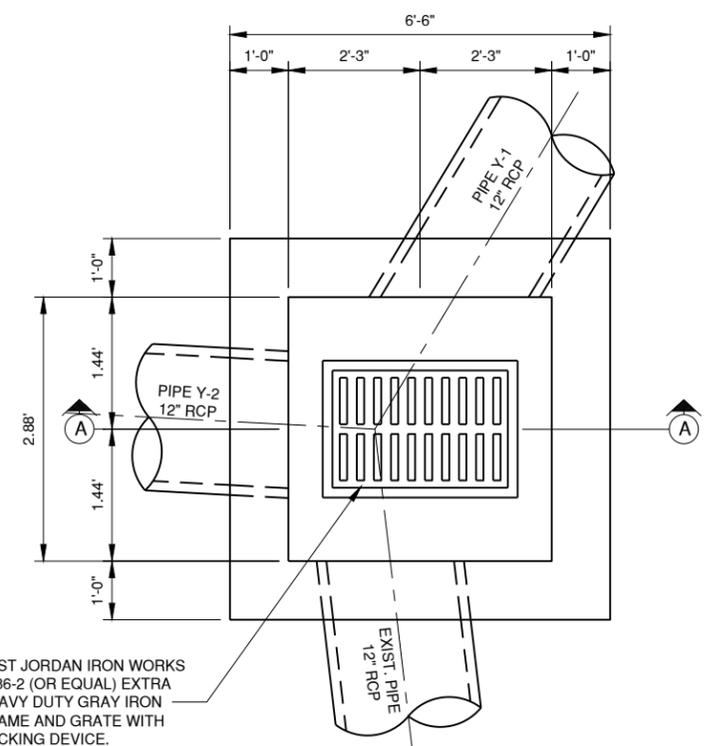


4 KEYWAY DETAIL
N.T.S.

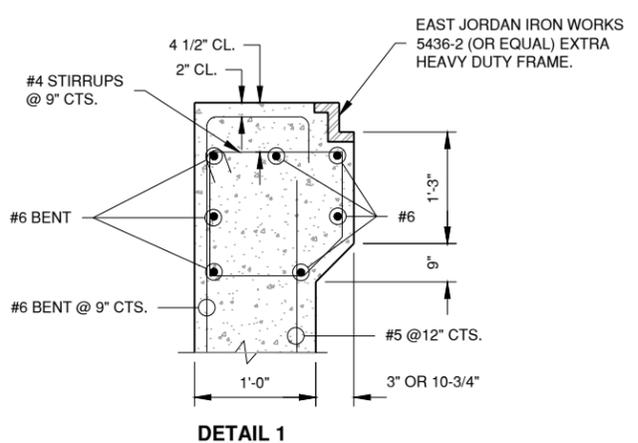
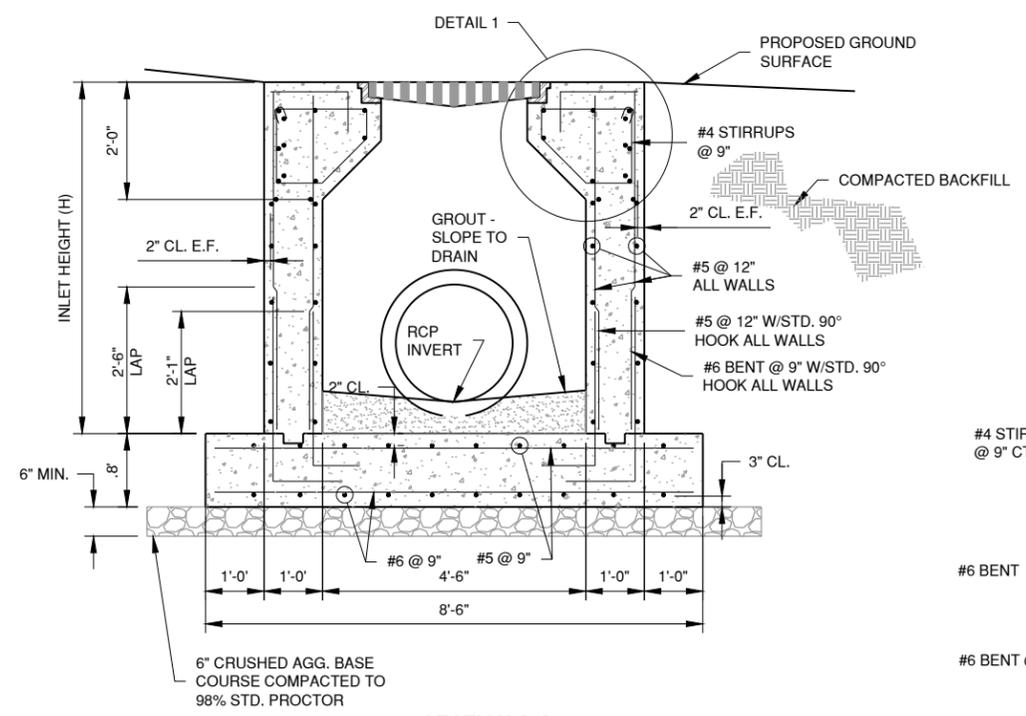
DESIGN CRITERIA
2000 PSI LOADING
WATER TABLE AT TOP OF INLET NET
ALLOWABLE SOIL BEARING PRESSURE = 2500 PSF

GENERAL NOTES

1. THE INLET GRATE SHALL BE SUPPORTED ON ALL FOUR SIDES.
2. ALL REINFORCEMENT BARS SHALL CONFORM TO ASTM A615 GRADE 60 AND SHALL BE CLEAN AND FREE OF GREASE, SCALING RUST AND OTHER FOREIGN MATERIALS.
3. INLET MAY BE CONSTRUCTED BY CAST-IN-PLACE CONCRETE OR PRECAST CONCRETE. PRECAST CONCRETE INLETS SHALL BE CONSTRUCTED TO THE LINES, DIMENSIONS AND DETAILS SHOWN ON THIS SHEET.
4. CAST-IN-PLACE CONCRETE AND PRECAST CONCRETE FOR THE INLETS SHALL BE IN ACCORDANCE WITH P-610.
5. ALL FOOTING EXCAVATIONS SHALL BE CLEAN FREE OF DEBRIS, STANDING WATER AND LOOSE SOIL AND SHALL BE INSPECTED BY THE ENGINEER PRIOR TO THE PLACEMENT OF CONCRETE OR SUBBASE.
6. CONCRETE SHALL NOT BE PLACED OVER FROZEN OR MUDDY SOIL.
7. ADDITIONAL REINFORCEMENT FOR PIPE PENETRATIONS NOT REQUIRED FOR 4" DIA. PIPES OR LESS.



1 MANHOLE DETAIL
N.T.S.



DETAIL 1

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Date: Monday, September 21, 2020 9:47:56 AM

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

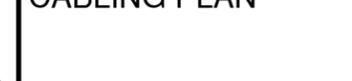
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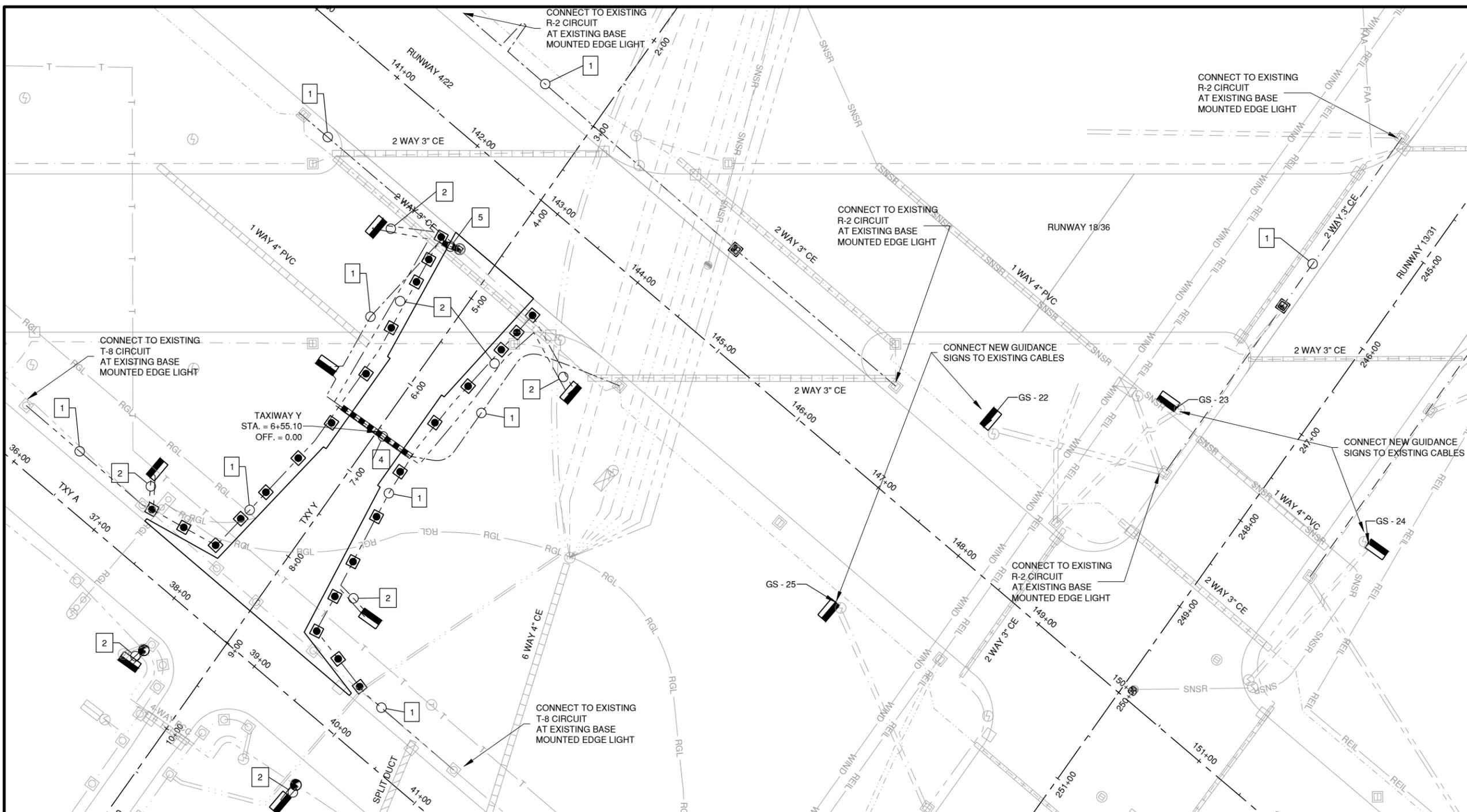
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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK	DATE	DESCRIPTION
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IL PROJ. NO: SPI-4755		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH1-EL200.DWG		
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SHEET TITLE
CABLING PLAN



EL201
SHEET 44 OF 63



GENERAL NOTES

- LIGHTS SHALL BE LOCATED NO MORE THAN 10' AND NO LESS THAN 2' FROM THE EXISTING PAVEMENT EDGE, IN A STRAIGHT LINE.
- THE CONTRACTOR SHALL VERIFY LAYOUT OF LIGHTS WITH THE R.E. PRIOR TO INSTALLING.
- LIGHTS SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE I.P., P.T., P.C., OR TAPER POINT.
- GUIDANCE SIGNS GS-22 THROUGH GS-25 AND RUNWAY EDGE LIGHT 4-22 L1 SHALL BE INSTALLED WITHIN PHASE 3.

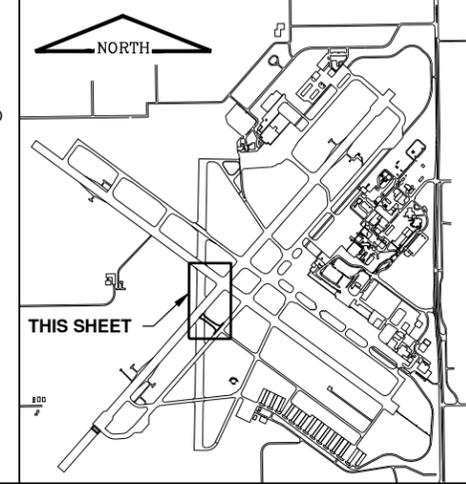
CIRCUIT LEGEND

EXISTING	NEW	CIRCUIT
---	---	R-1 (RWY 4/22)
---	---	R-2 (RWY 13/31)
---	---	T-8 (TXY A, H-WEST)
---	---	T-6 (TXY B)
---	---	T-1 (TXY C, H-EAST)
---	---	T-5 (TXY G-EAST)
---	---	T-4 (TXY G-WEST)
---	---	TXY G HOMERUN
1	○	1/C #8 5KV UG CABLE IN UD
2	○	2/C #8 5KV UG CABLE IN UD
3	○	1/C #8 5KV UG CABLE
4	○	4-WAY CONC. ENCASED DUCT
5	○	1-WAY CONC. ENCASED DUCT

LEGEND

EXISTING	NEW	DESCRIPTION
○	○	SPLICE CAN
□	■	TAXIWAY GUIDANCE SIGN
○	●	TAXIWAY EDGE LIGHT - STAKE MOUNTED
□	■	TAXIWAY EDGE LIGHT - BASE MOUNTED
□	■	RUNWAY BASE MOUNTED EDGE LIGHT
H	H	HANDHOLE
▬	▬	DUCT BANK
⊖	⊖	IN-PAVEMENT LIGHT (FMRL)

KEYMAP



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SEPTMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

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 SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-EL400.DWG	
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DRAWN BY: DPA	
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 SHEET TITLE
LIGHTING & SIGNAGE SCHEDULE

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
TWY Y - L1	MITL (LED)	TAXIWAY Y STA 4+68.97 OFFSET 57.03 R
TWY Y - L2	MITL (LED)	TAXIWAY Y STA 4+93.06 OFFSET 54.27 R
TWY Y - L3	MITL (LED)	TAXIWAY Y STA 5+17.15 OFFSET 51.51 R
TWY Y - L4	MITL (LED)	TAXIWAY Y STA 5+66.53 OFFSET 45.85 R
TWY Y - L5	MITL (LED)	TAXIWAY Y STA 6+15.90 OFFSET 40.19 R
TWY Y - L6	MITL (LED)	TAXIWAY Y STA 6+72.91 OFFSET 40.00 R
TWY Y - L7	MITL (LED)	TAXIWAY Y STA 7+18.41 OFFSET 46.36 R
TWY Y - L8	MITL (LED)	TAXIWAY Y STA 7+62.59 OFFSET 52.72 R
TWY Y - L9	MITL (LED)	TAXIWAY Y STA 7+96.89 OFFSET 57.65 R
TWY Y - L10	MITL (LED)	TAXIWAY Y STA 8+31.19 OFFSET 62.59 R
TWY Y - L11	MITL (LED)	TAXIWAY Y STA 8+34.95 OFFSET 97.07 R
TWY Y - L12	MITL (LED)	TAXIWAY Y STA 8+38.71 OFFSET 131.54 R

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
TWY Y - L13	MITL (LED)	TAXIWAY Y STA 4+79.26 OFFSET -56.73 L
TWY Y - L14	MITL (LED)	TAXIWAY Y STA 5+01.10 OFFSET -54.06 L
TWY Y - L15	MITL (LED)	TAXIWAY Y STA 5+22.94 OFFSET -51.38 L
TWY Y - L16	MITL (LED)	TAXIWAY Y STA 5+69.42 OFFSET -45.69 L
TWY Y - L17	MITL (LED)	TAXIWAY Y STA 6+15.90 OFFSET -40.00 L
TWY Y - L18	MITL (LED)	TAXIWAY Y STA 6+72.91 OFFSET -40.00 L
TWY Y - L19	MITL (LED)	TAXIWAY Y STA 7+20.65 OFFSET -46.45 L
TWY Y - L20	MITL (LED)	TAXIWAY Y STA 7+68.38 OFFSET -52.90 L
TWY Y - L21	MITL (LED)	TAXIWAY Y STA 8+05.20 OFFSET -57.87 L
TWY Y - L22	MITL (LED)	TAXIWAY Y STA 8+42.03 OFFSET -62.85 L
TWY Y - L23	MITL (LED)	TAXIWAY Y STA 8+51.69 OFFSET -94.72 L
TWY Y - L24	MITL (LED)	TAXIWAY Y STA 8+61.35 OFFSET -126.60 L

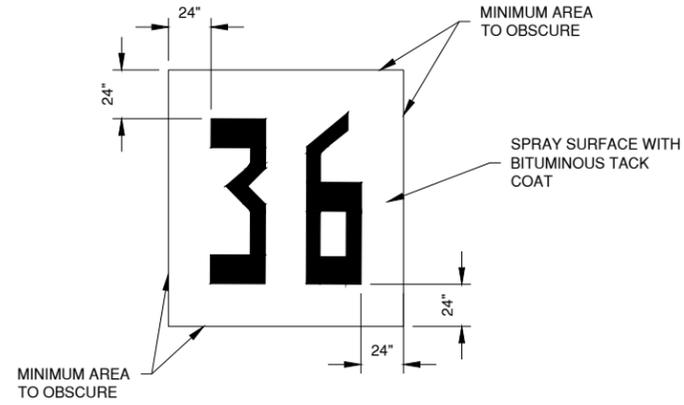
FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
RWY 4-22 - L1	HIRL	RUNWAY 4-22 STA 144+50.56 OFFSET -84.21 L
RWY 4-22 - L2	FMRL	RUNWAY 4-22 STA 142+50.64 OFFSET 85.59 R

FIXTURE #	FIXTURE TYPE	HORIZONTAL CONTROL
RWY 13-31 - L1	FMRL	RUNWAY 13-31 STA 246+10.82 OFFSET 85.14 R

GUIDANCE SIGN SCHEDULE

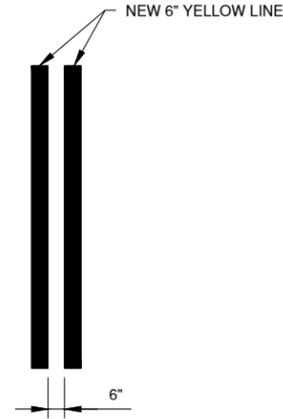
SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND (L-858R)	BLACK LEGEND ON YELLOW BACKGROUND (L-858Y)	YELLOW LEGEND ON BLACK BACKGROUND (L-858L)	NUMBER OF CHARACTERS	POWER CIRCUIT	SIGN TYPE	SIGN SIZE	SIGN STYLE	SIGN CLASS	SIGN MODE
GS- 12	NE	A			A	4	TXY A	L-858 (LED)	2	2	2	2
	SW	A ← Y →		← Y →	A		CKT T-7					
GS- 13	NW	Y			Y	4	TXY A	L-858 (LED)	2	2	2	2
	SE	Y ← A →		← A →	Y		CKT T-7					
GS- 14	NW	Y ← A →			Y	4	TXY A	L-858 (LED)	2	2	2	2
	SE						CKT T-7					
GS- 15	NE	A ← Y →			A	4	TXY A	L-858 (LED)	2	2	2	2
	SW	A			A		CKT T-7					
GS- 16	NE					2	TXY A	L-858 (LED)	2	2	2	2
	SW	Y →		Y →			CKT T-7					
GS- 17	NW					5	RWY 4-22	L-858 (LED)	2	3	2	2
	SE	Y 4 - 2 2	4 - 2 2		Y		CKT R-1					
GS- 18	NE	← Y				2	TXY A	L-858 (LED)	2	2	2	2
	SW						CKT T-7					
GS- 22	NE					5	RWY 13-31	L-858 (LED)	2	3	2	2
	SW	1 3 - 3 1	1 3 - 3 1				CKT R-2					
GS- 23	NW	2 2 - 4	2 2 - 4			4	RWY 4-22	L-858 (LED)	2	3	2	2
	SE						CKT R-1					
GS- 24	NW	2 2 - 4	2 2 - 4			4	RWY 4-22	L-858 (LED)	2	3	2	2
	SE						CKT R-1					
GS- 25	NE					5	RWY 13-31	L-858 (LED)	2	3	2	2
	SW	1 3 - 3 1	1 3 - 3 1				CKT R-2					

 NOTE:
 CONTRACTOR SHALL CONFIRM OUTPUT OF EXISTING REGULATORS (6.6A OR 20A) PRIOR TO ORDERING EQUIPMENT.



1 OBSCURE RUNWAY DESIGNATION
N.T.S.

NOTE:
NUMERALS SHALL BE COMPLETELY
COVERED AND INVISIBLE



2 TAXIWAY EDGE MARKING
N.T.S.

SEPTEMBER 21, 2020

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 1**

OWNER



**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

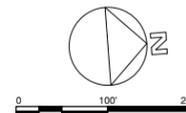
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IL PROJ. NO: SPI-4755		
CMT PROJECT NO: 180035-05		
CAD DWG FILE: 18003505-PH1-CM500.DWG		
DESIGNED BY: HWI		
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SHEET TITLE
**PAVEMENT MARKING
& SIGNAGE DETAILS**



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CONSULTANTS



SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

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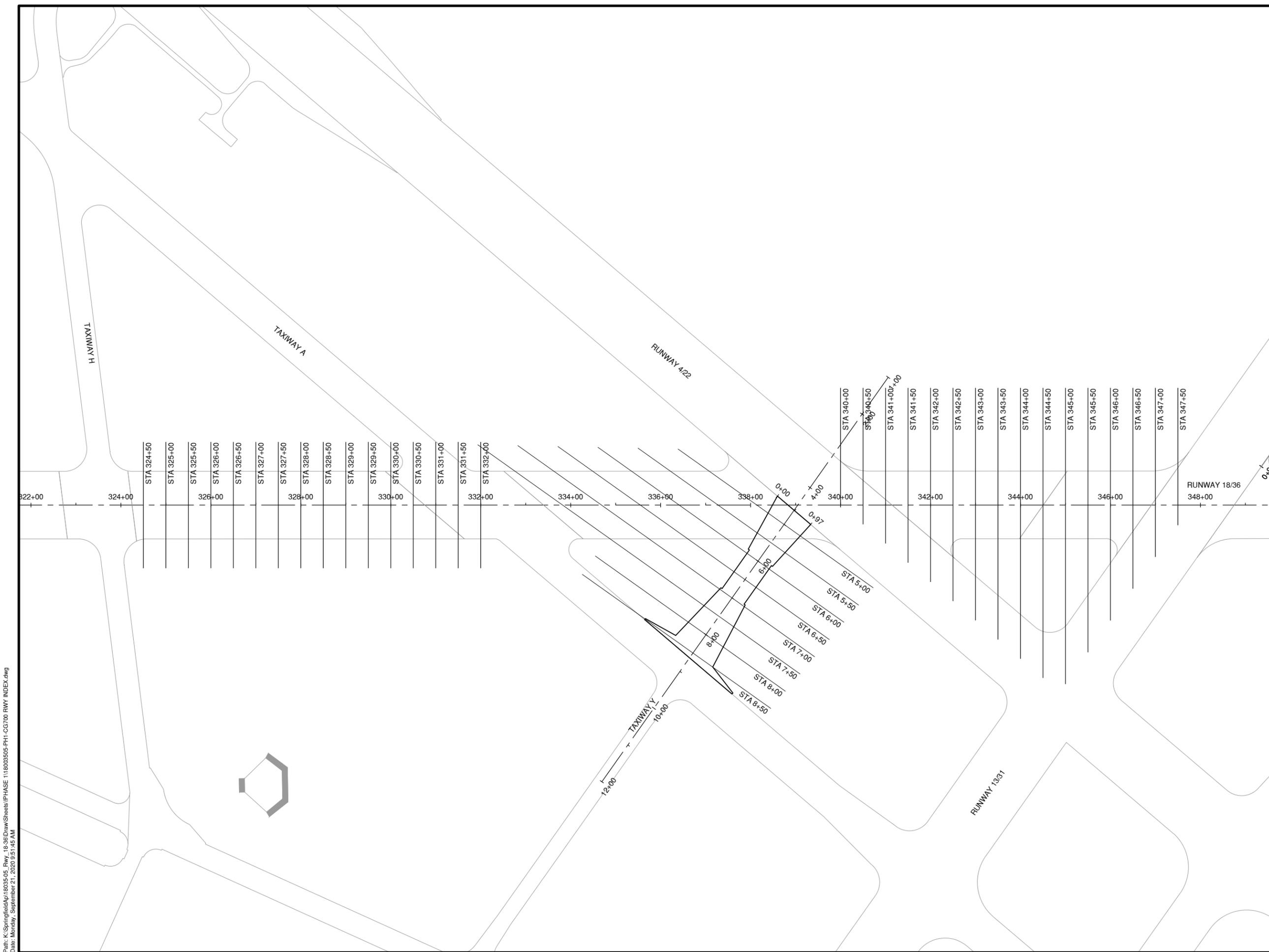
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-CG700 RWY INDEX.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
COPYRIGHT:	

CROSS SECTION INDEX

SHEET 56 OF 63



Path: K:\Springfield\AIP\180035-05_Rwy_18-36\DrawSheets\PHASE 1\18003505-PH1-CG700 RWY INDEX.dwg
Date: Monday, September 21, 2020 9:51:45 AM



License No. 184-000613

CONSULTANTS

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SEPTEMBER 21, 2020

RUNWAY 18/36 REMOVAL AND ASSOCIATED IMPROVEMENTS, PHASE 1

OWNER



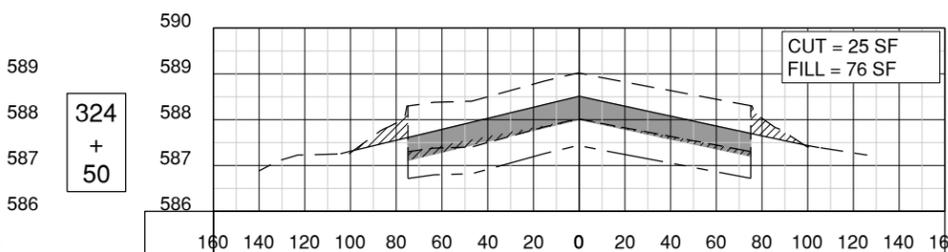
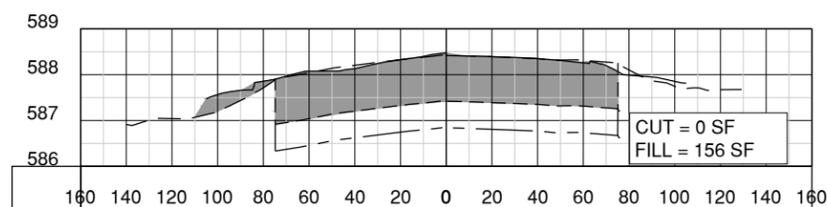
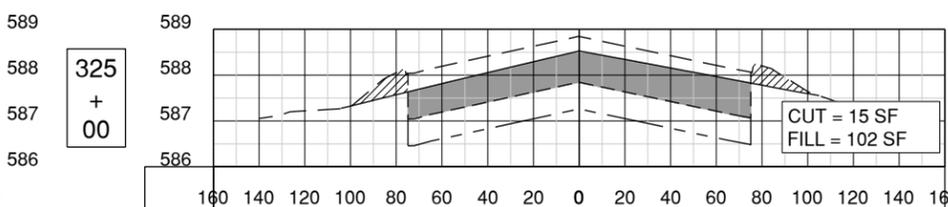
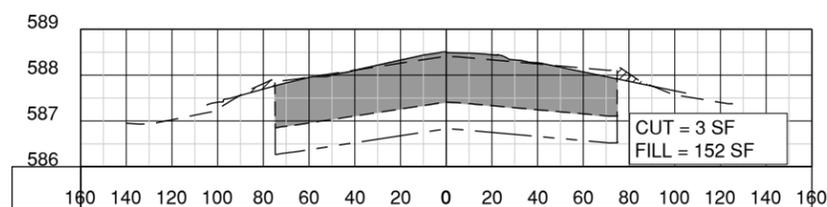
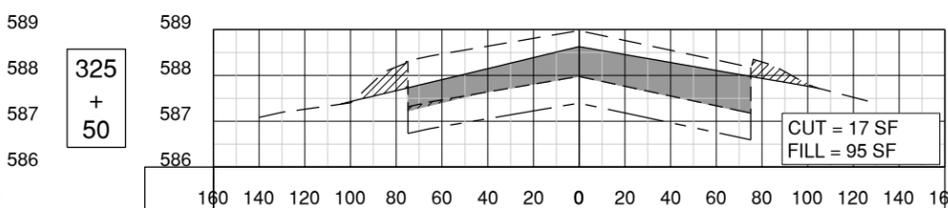
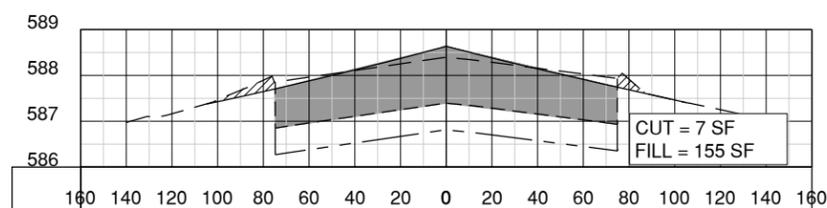
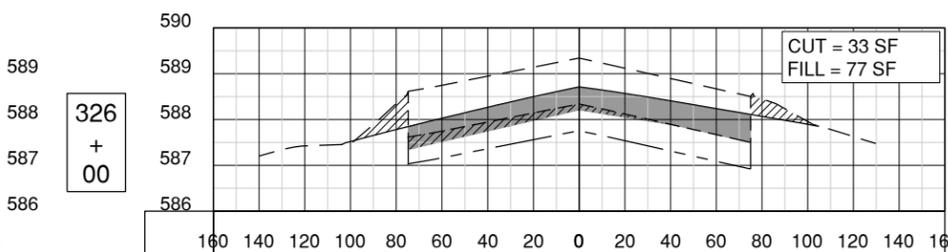
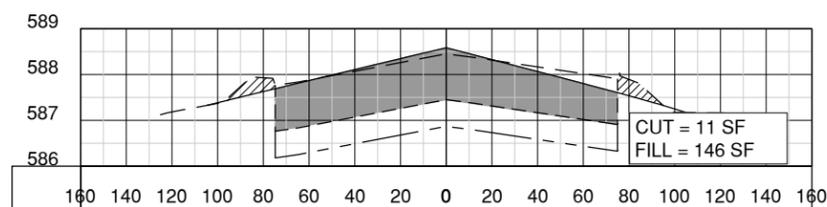
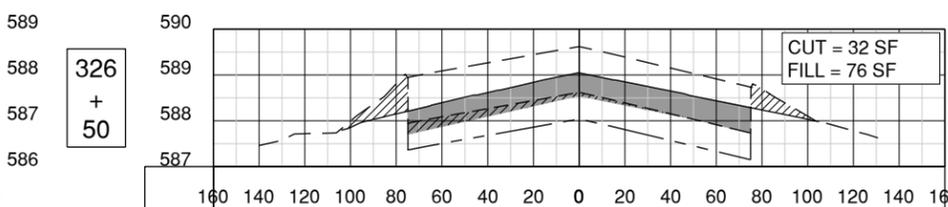
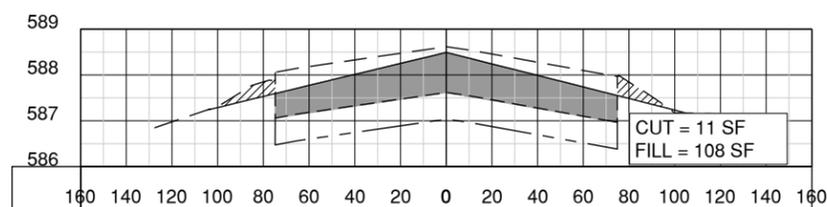
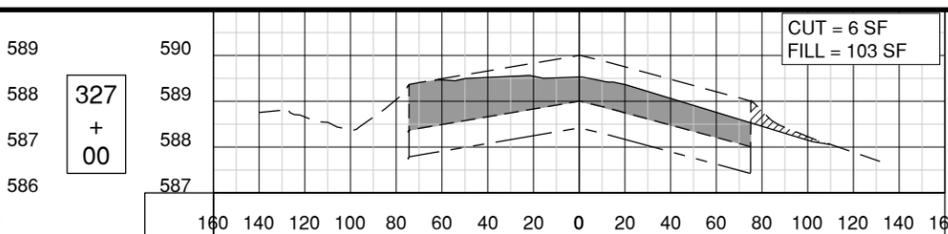
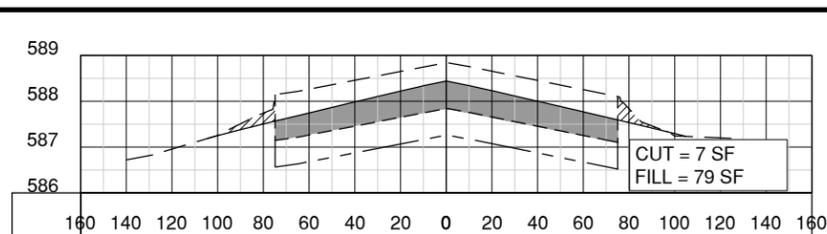
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, IL

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-CG700 RWY XSEC.DWG	
DESIGNED BY: HWI	
DRAWN BY: DPA	
CHECKED BY: MJD	
APPROVED BY: RLV	
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RUNWAY 18-36 CROSS SECTIONS 1

SHEET 57 OF 63



- EXISTING GROUND SURFACE
- PROPOSED GROUND SURFACE
- BOTTOM OF EXISTING PAVEMENT
- BOTTOM OF EXISTING BASE

- CUT AREA
- FILL AREA

Path: K:\Springfield\AIP\180035-05_Rwy_18-36\DrawSheets\PHASE 1\18003505-PH1-CG700 RWY XSEC.dwg
Date: Monday, September 21, 2020 9:52:08 AM



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CONSULTANTS

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SEPTEMBER 21, 2020

**RUNWAY 18/36 REMOVAL AND
ASSOCIATED IMPROVEMENTS,
PHASE 1**

OWNER



**SPRINGFIELD AIRPORT
AUTHORITY
ABRAHAM LINCOLN CAPITAL
AIRPORT
SPRINGFIELD, IL**

MARK | DATE | DESCRIPTION

AIP PROJ. NO: 3-17-0096-XX	CA021
IL PROJ. NO: SPI-4755	
CMT PROJECT NO: 180035-05	
CAD DWG FILE: 18003505-PH1-CG700 RWY XSEC.DWG	
DESIGNED BY: HWI	
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SHEET TITLE
**RUNWAY 18-36
CROSS SECTIONS 6**

CG706
SHEET 62 OF 63

- --- --- --- EXISTING GROUND SURFACE
- PROPOSED GROUND SURFACE
- --- --- --- BOTTOM OF EXISTING PAVEMENT
- --- --- --- BOTTOM OF EXISTING BASE
-  CUT AREA
-  FILL AREA

