

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 62J35		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN:  
VILLAGE OF CARY AND  
FOX RIVER GROVE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
**PROPOSED  
HIGHWAY PLANS**

F.A.P ROUTE 305: US ROUTE 14 (NORTHWEST HIGHWAY) BRIDGE  
OVER FOX RIVER

SECTION: 2019-058-BR  
PROJECT NO. : NHPP-38RN(839)

BRIDGE DECK OVERLAY AND JOINT REPAIR

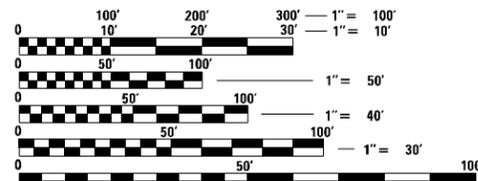
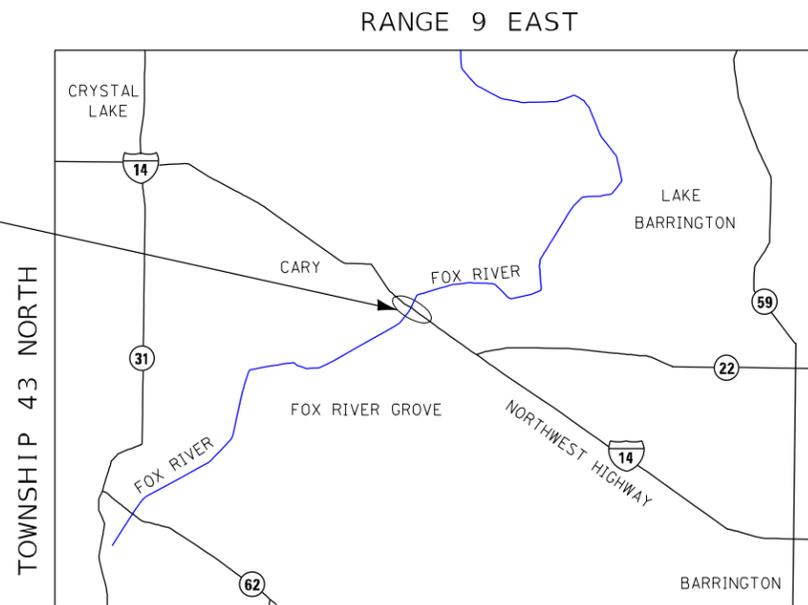
US ROUTE 14 BRIDGE  
2019 ADT = 27,600  
POSTED SPEED = 35 MPH

MCHENRY COUNTY  
C-91-419-19



LOCATION OF SECTION INDICATED THIS: - [black bar] -

PROJECT LOCATION  
US ROUTE 14  
(NORTHWEST HWY)  
BRIDGE  
STRUCTURE NO:  
056-0058



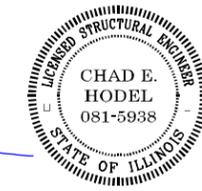
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

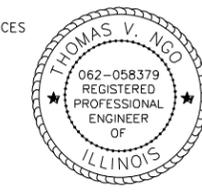
ALGONQUIN TOWNSHIP

LOCATION MAP  
NOT TO SCALE  
GROSS LENGTH OF PROJECT = 497 FT. = 0.09 MILES  
NET LENGTH OF PROJECT = 497 FT. = 0.09 MILES

WHKS & Co  
CHAD E. HODEL, P.E., S.E.  
IL. LIC. NO. 081-5938  
EXPIRES 11/30/2020  
SIGNED: [Signature]  
DATE: 08/14/2020  
SIGNATURE AND SEAL APPLIES  
TO DRWG NO TO



MILLENNIA PROFESSIONAL SERVICES  
THOMAS V. NGO  
IL. LIC. NO. 062-058379  
EXPIRES 11/30/2021  
SIGNED: [Signature]  
DATE: 08/14/2020  
SIGNATURE AND SEAL APPLIES  
TO DRWG NO 1 TO



PROJECT ENGINEER: PRAVEEN KAINI, P.E. (847) 705-4237  
PROJECT MANAGER: ALAIN MIDY, P.E. (847) 221-3056

CONTRACT NO. 62J35

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED August 11 20 20  
Chad E. Hodel  
REGIONAL ENGINEER

October 2, 2020  
Scott A. Etk  
ENGINEER OF DESIGN AND ENVIRONMENT

October 2, 2020  
James J. Gu  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

design firm  
no. 184001036

**whks**  
engineers + planners + land surveyors

**MILLENNIA**  
PROFESSIONAL SERVICES  
www.millennia.pro

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**INDEX OF SHEETS**

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**LIST OF HIGHWAY STANDARDS**

000001-07 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS  
 280001-07 TEMPORARY EROSION CONTROL SYSTEMS  
 701101-05 OFF-RD OPERATIONS, MULTILANE, 15' TO 14" FROM PAVEMENT EDGE  
 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS LESS/EQUAL TO 40 MPH  
 701602-10 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE  
 701611-01 URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN  
 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE  
 701901-08 TRAFFIC CONTROL DEVICES  
 704001-08 TEMPORARY CONCRETE BARRIER

**COMMITMENTS**

NO COMMITMENTS FOR THIS PROJECT

**HOT MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS at Ndes	QMP
<b>APPROACH PAVEMENT AND BUTT JOINT</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", 1L-9.5, N70 - 1 1/2"	4% @ 70 GYR.	QC/QA
<b>QMP DESIGNATION: Quality Control/Quality Assurance (QC/QA)</b>		

**MIXTURE NOTES:**

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN  
 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS, BY THE CONTRACTOR, SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND VILLAGE OF CARY
- THE RESIDENT ENGINEER SHALL CONTACT THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR, MEADE ELECTRIC COMPANY, AT (773) 287-7672 PRIOR TO THE START OF CONSTRUCTION TO LOCATE ALL IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER AND CONTRACTOR SHALL VERIFY ALL EXISTING PAVEMENT MARKING PRIOR TO THE PROJECT STARTING. IF A DISCREPANCY OCCURS BETWEEN EXISTING PAVEMENT MARKINGS AND THE PROPOSED PAVEMENT MARKINGS, THE RESIDENT ENGINEER SHALL CONTACT THE BUREAU OF TRAFFIC OPERATIONS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS, OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS, SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL STAGES OF CONSTRUCTION.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- POLLUTION CONTROL: THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THIS WORK IS NOT TO BE PAID SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- THE COST OF SAW CUTTING SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS REMOVAL PAY ITEMS.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
- THE CONTRACTOR SHALL OBTAIN COAST GUARD APPROVAL FOR ANY WORK THAT MAY INTERFERE WITH NAVIGATIONAL OPERATIONS OF THE NAVIGABLE WATERS. A WORK PLAN SHALL BE PREPARED BY THE CONTRACTOR, REVIEWED AND APPROVED BY THE ENGINEER AND BE SUBMITTED BY THE ENGINEER TO THE COAST GUARD AT THE ADDRESS LISTED BELOW FOR APPROVAL.  
 BRIDGE ADMINISTRATOR  
 US COAST GUARD  
 NINTH COAST GUARD DISTRICT  
 1240 E. NINTH ST  
 CLEVELAND, OH 44199-2060

REV-SEP

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 USER NAME = Millennium Professional Services



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	DRAWN - JC	REVISED -
PLOT SCALE = 2.0000 / in.	CHECKED - TVN	REVISED -
PLOT DATE = 8/13/2020	DATE - 06/26/2020	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
 INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES**

SCALE: N/A SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	2
<b>CONTRACT NO. 62J35</b>				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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 USER NAME = Millennia Professional Services

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES URBAN	CONST. TYPE CODE	
				BRIDGE SN: 056-0058 80% FED 20% STATE 0059	
28000510	INLET FILTERS	EACH	4	4	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	219	219	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	122	122	
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	183	183	
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	95.2	95.2	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	50	50	
50102400	CONCRETE REMOVAL	CU YD	25.5	25.5	
44000600	SIDEWALK REMOVAL	SQ FT	50	50	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	27.7	27.7	
50300260	BRIDGE DECK GROOVING	SQ YD	2082	2082	
50300300	PROTECTIVE COAT	SQ YD	2798	2798	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	2060	2060	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2290	2290	
50800515	BAR SPLICERS	EACH	16	16	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	132	132	
52100520	ANCHOR BOLTS, 1	EACH	5	5	
59000200	EPOXY CRACK INJECTION	FOOT	18	18	
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6	
67100100	MOBILIZATION	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	198	198	
70300900	PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS	SQ FT	36.4	36.4	
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	12412	12412	
70300906	PAVEMENT MARKING TAPE, TYPE IV 6"	FOOT	182	182	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	513	513	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	613	613	
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	36.4	36.4	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3462	3462	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	125	125	

REV-SEP



USER NAME = jcovarrubias	DESIGNED -	REVISED -
PLOT SCALE = 2.0000 / in.	DRAWN -	REVISED -
PLOT DATE = 8/13/2020	CHECKED -	REVISED -
	DATE - 06/26/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
SUMMARY OF QUANTITIES**

SCALE: N/A SHEET NO. 1 OF 2 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	3
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

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 USER NAME = Millennia Professional Services



USER NAME = jcovarrubias	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 2.0000 / in.	CHECKED -	REVISED -
PLOT DATE = 8/13/2020	DATE - 06/26/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

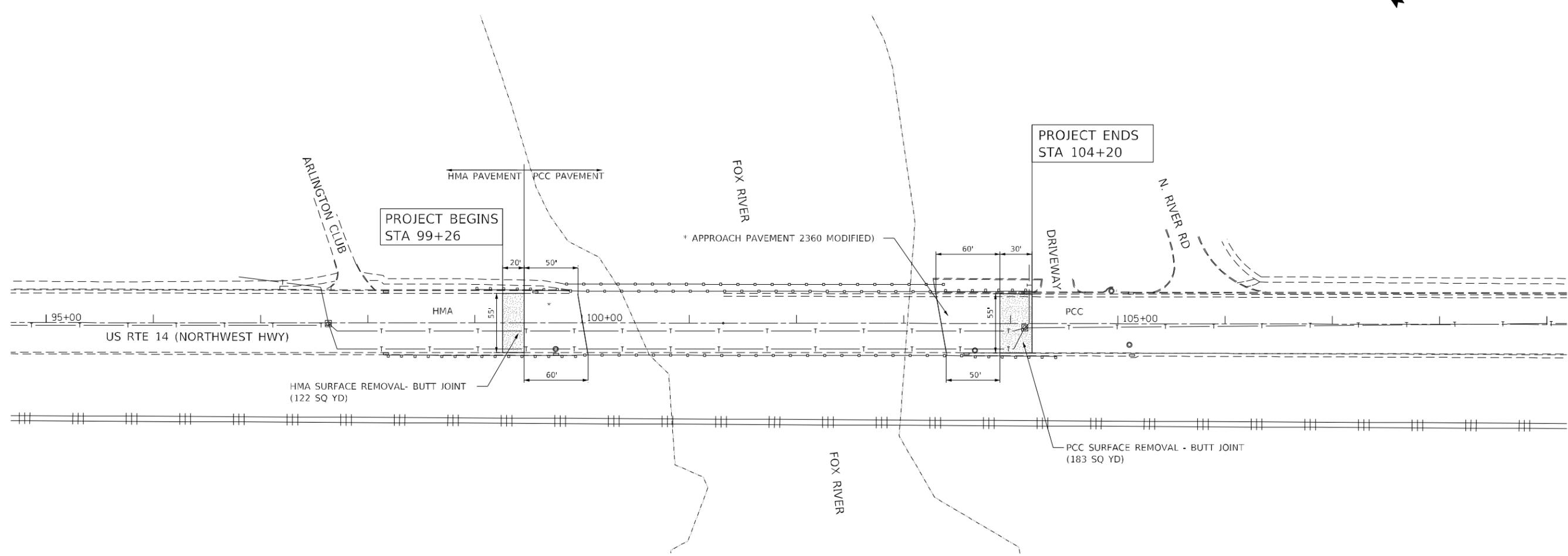
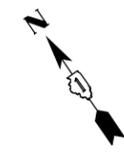
**US ROUTE 14 (NORTHWEST HIGHWAY)  
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	4
<b>CONTRACT NO. 62J35</b>				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONST. TYPE CODE	
				BRIDGE SN: 056-0058 80% FED 20% STATE 0059	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	181	181	
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	72.8	72.8	
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	5699	5699	
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	261	261	
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	234	234	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1340	1340	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	110	110	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	222	222	
78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	75	75	
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	50	50	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	270	270	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	4533	4533	
* X2700003	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	167	167	
* X2700004	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 7"	FOOT	167	167	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	120	120	
<input type="checkbox"/> X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	48	48	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	4426	4426	
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	2060	2060	
Z0001905	STRUCTURAL STEEL REPAIR	POUND	830	830	
Z0006014	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	2123	2123	
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	2123	2123	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	22	22	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	24	24	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	1	1	
Z0018051	DRAINAGE SCUPPER TO BE ADJUSTED	EACH	4	4	
<input type="checkbox"/> Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	FOOT	4	4	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500	

▪ DENOTES SPECIALTY ITEM  
 NON-PART 100% STATE

∅ 0042  
REV-SEP



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 USER NAME = Millennia Professional Services

SURF REM BUTT JT  
 SEE APPROACH SLAB DETAILS (SHEET 6)



USER NAME = jpham	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 8/13/2020	DATE - 06/26/2020	REVISED -

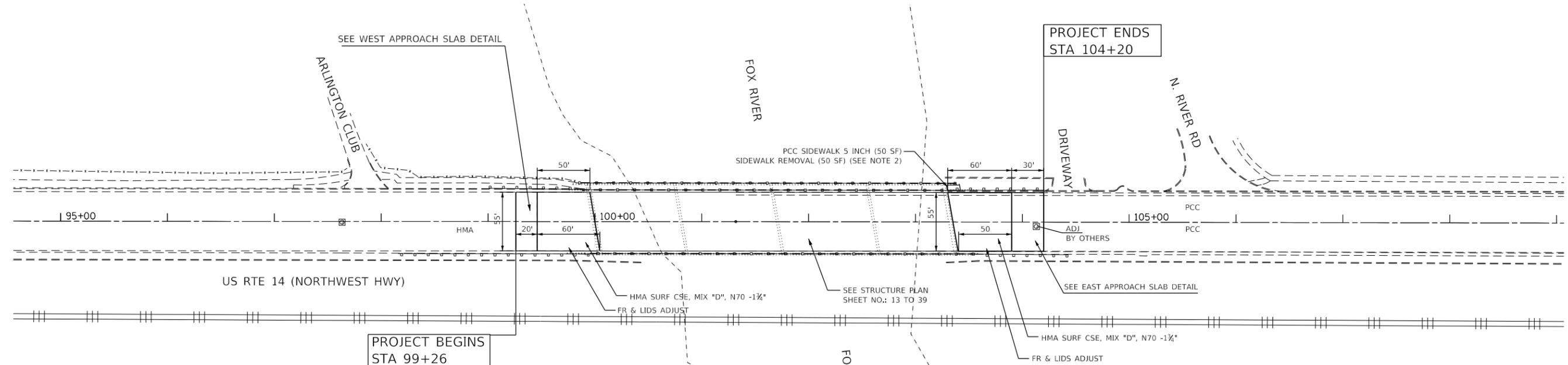
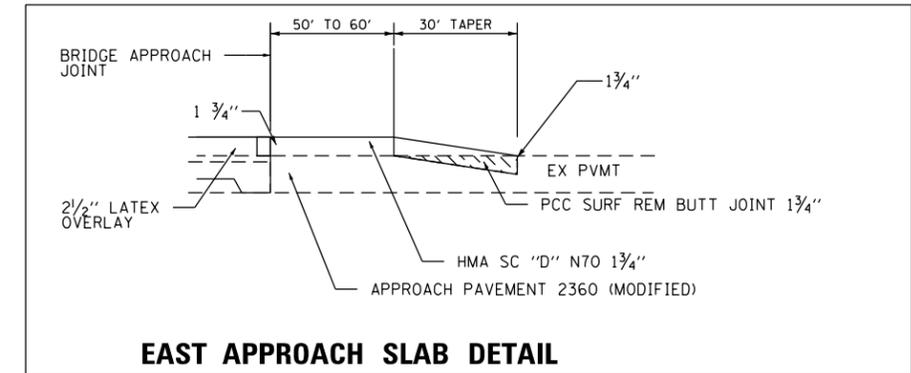
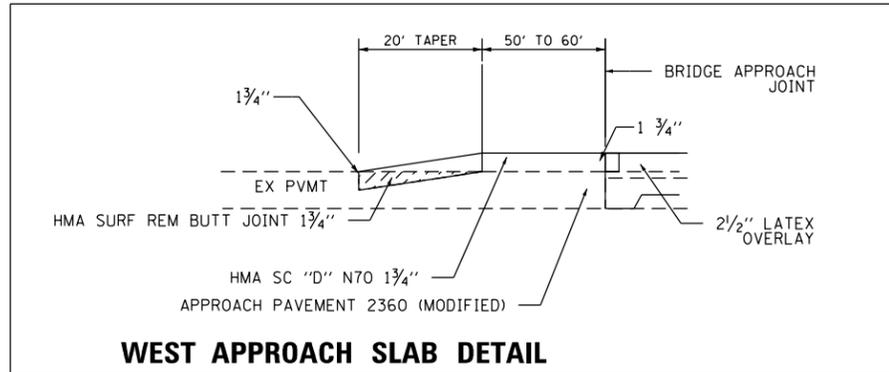
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)**  
**ROADWAY EXISTING CONDITION**

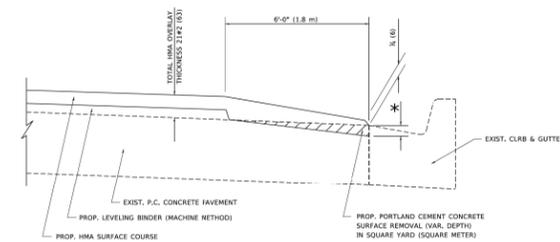
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	5
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

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**HMA TAPER AT  
EDGE OF P.C.C. PAVEMENT**



**HMA TAPER AT  
EDGE OF P.C.C. PAVEMENT**

HMA SURFACE	LEVELING BINDER	THICKNESS	THICKNESS	* MILLING AT GUTTER FLAG
MIX				
C OR D	1 1/2 (38)	1 (25)	1 1/2 (38)	
E	1 1/2 (38)	1 (25)	1 1/2 (38)	

- NOTE:**
- ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
  - COMMEMORATIVE BRICK TO BE REMOVED AND REINSTALL AT THE SAME LOCATION.

FILE NAME = \$FILE\$  
 PLOT SCALE = \$SCALE\$  
 USER NAME = \$USER\$



USER NAME = \$USERS	DESIGNED - JC	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - JC	REVISED -
PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE - 06/26/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
ROADWAY PROPOSED PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	6
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\$FILES

**MAINTENANCE OF TRAFFIC GENERAL NOTES**

1. THE TRAFFIC CONTROL DEPICTED IN THE PLANS ARE THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES, AS SPECIFIED IN THE HIGHWAY AND DISTRICT STANDARDS, AS SHOWN IN THE INDEX OF SHEETS, AND SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS, OR SPECIAL PROVISIONS.
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER.
3. ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
5. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL GENERAL NOTES, PLANS, SPECIAL PROVISIONS, APPLICABLE HIGHWAY & DISTRICT STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER 72 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
6. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, LANDSCAPING, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED. IF THEY WERE REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION ACTIVITIES, THE COST TO REPAIR ANY DAMAGES WILL BE BORNE BY THE CONTRACTOR AND NOT THE RESPONSIBILITY OF THE DEPARTMENT.
7. NO WORK SHALL COMMENCE UNTILL TRAFFIC CONTROL REQUIREMENTS ARE MET.
8. THE CONTRACTOR SHALL PLACE ONE (1) CHANGEABLE MESSAGE ALONG U.S. ROUTE 14, IN ADVANCE OF THE LANE CLOSURE. THE LOCATION OF THE CHANGEABLE MESSAGE SIGN IS TO BE DETERMINED BY THE ENGINEER. THE CHANGEABLE MESSAGE SIGN IS TO REMAIN IN PLACE FOR THE DURATION OF STAGES 1 & 2. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAY "CHANGEABLE MESSAGE SIGN".
9. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE DURING ALL PHASES OF CONSTRUCTION.

**SEQUENCE OF CONSTRUCTION**

**STAGE I**

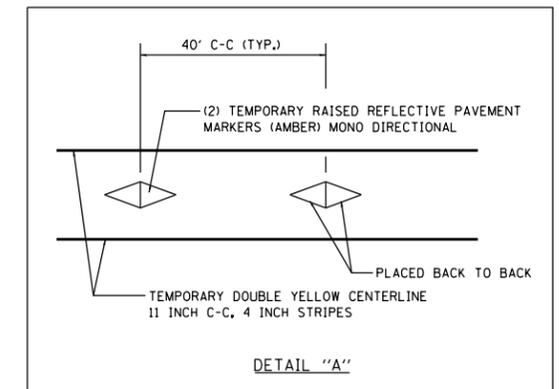
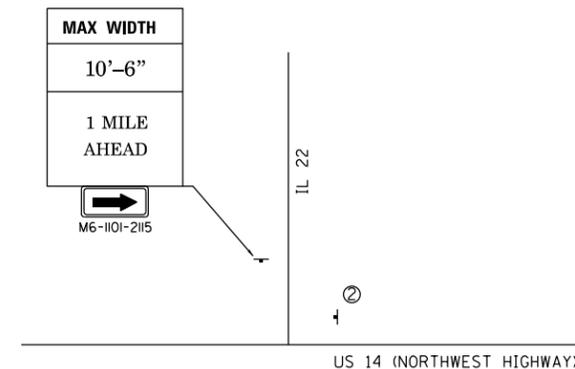
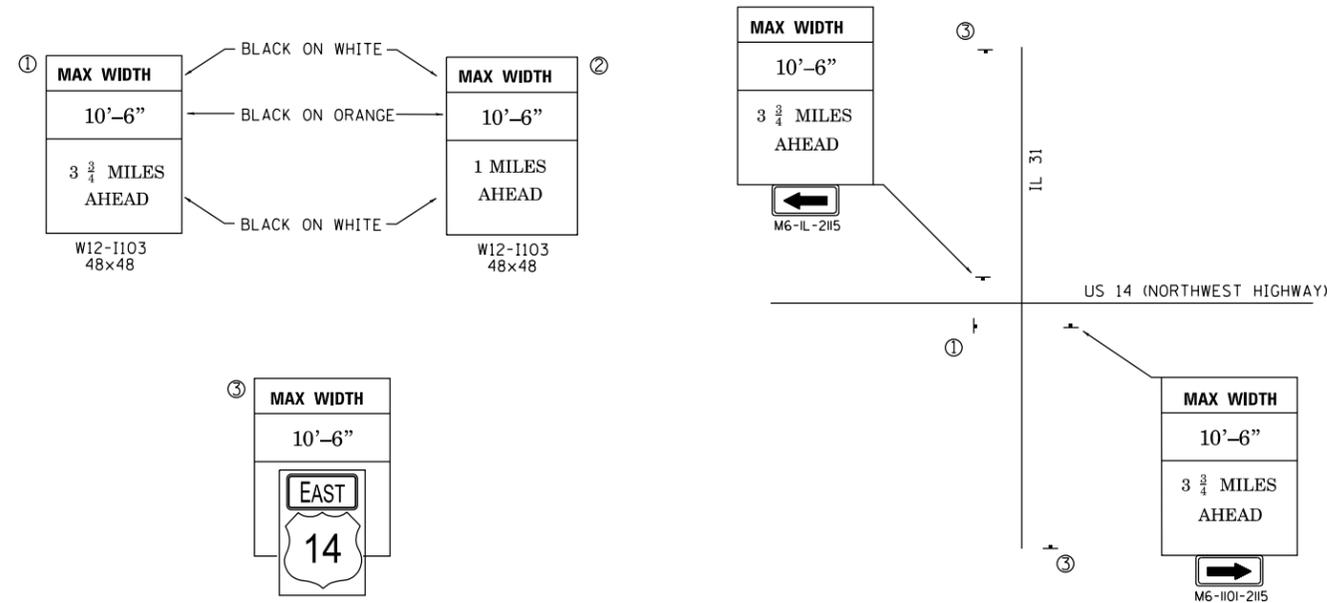
1. USING HIGHWAY STANDARDS DURING ALLOWABLE CLOSURE HOURS, INSTALL PROTECTIVE SHIELD (CONCRETE BARRIER) AS SHOWN ON THE STRUCTURAL PLANS.
2. INSTALL TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG NORTHBOUND US 14 ACCORDING TO THE STAGE I MAINTENANCE OF TRAFFIC PLAN SHEETS REDUCING TO ONE 11 1/2 LANE IN EACH DIRECTION TO SHIFT TRAFFIC TO THE SOUTH SIDE OF US 14.
3. IMPLEMENT DETOUR FOR EASTBOUND US 14 TO NORTHBOUND N. RIVER ROAD AND ARLINGTON CLUB.
4. FOLLOW STRUCTURAL SCOPE OF WORK FOR BRIDGE DECK OVERLAY AND JOINT REPAIR.
  - A. REMOVE AND REPLACE EXISTING EXPANSION JOINTS AT BOTH ABUTMENTS
  - B. SCARIFY BRIDGE DECK 3/4" AND INSTALL 2 1/2" LATEX CONCRETE OVERLAY. PERFORM DECK SLAB REPAIRS AS REQUIRED
  - C. PERFORM STRUCTURAL REPAIR OF CONCRETE AT BOTH ABUTMENTS
  - D. APPLY PROTECTIVE COAT
  - E. REPLACE DIAPHRAGMS AND REPAIR BEAM ENDS AT LOCATION SHOWN
  - F. RAISE BRIDGE SCUPPER GRATES AND CLEAN BRIDGE SCUPPER, FLOOR DRAINS, AND DOWNSPOUTS. SEE GENERAL NOTE
5. REMOVE TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG NORTHBOUND US 14, OPEN LANES ALONG NORTHBOUND US 14.

**STAGE II**

1. RELOCATE PROTECTIVE SHIELD AS SHOWN ON THE STRUCTURAL PLANS ACCORDING TO STAGE II.
2. INSTALL TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG SOUTHBOUND US 14 ACCORDING TO THE STAGE II MAINTENANCE OF TRAFFIC PLAN SHEETS REDUCING TO ONE 11 1/2 LANE IN EACH DIRECTION TO SHIFT TRAFFIC TO THE NORTH SIDE OF US 14.
3. IMPLEMENT DETOUR FOR EASTBOUND US 14 TO NORTHBOUND N. RIVER ROAD AND ARLINGTON CLUB.
4. IMPLEMENT DETOUR FOR SOUTHBOUND N. RIVER DRIVE TO EASTBOUND US 14.
5. FOLLOW STRUCTURAL SCOPE OF WORK FOR BRIDGE DECK OVERLAY AND JOINT REPAIR.
  - A. REMOVE AND REPLACE EXISTING EXPANSION JOINTS AT BOTH ABUTMENTS
  - B. SCARIFY BRIDGE DECK 3/4" AND INSTALL 2 1/2" LATEX CONCRETE OVERLAY. PERFORM DECK SLAB REPAIRS AS REQUIRED
  - C. PERFORM STRUCTURAL REPAIR OF CONCRETE AT BOTH ABUTMENTS
  - D. APPLY PROTECTIVE COAT
  - E. REPLACE DIAPHRAGMS AND REPAIR BEAM ENDS AT LOCATION SHOWN
  - F. RAISE BRIDGE SCUPPER GRATES AND CLEAN BRIDGE SCUPPER, FLOOR DRAINS, AND DOWNSPOUTS. SEE GENERAL NOTE ON THE STRUCTURAL PLANS
6. REMOVE TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG SOUTHBOUND US 14, OPEN LANES ALONG SOUTHBOUND US 14.

**POST STAGE**

1. INSTALL PERMANENT PAVEMENT MARKING ON DISTURBED PAVEMENT MARKING ALONG US 14.



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 USER NAME = \$USERNAME



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DRAWN - JC	REVISIONS -	
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

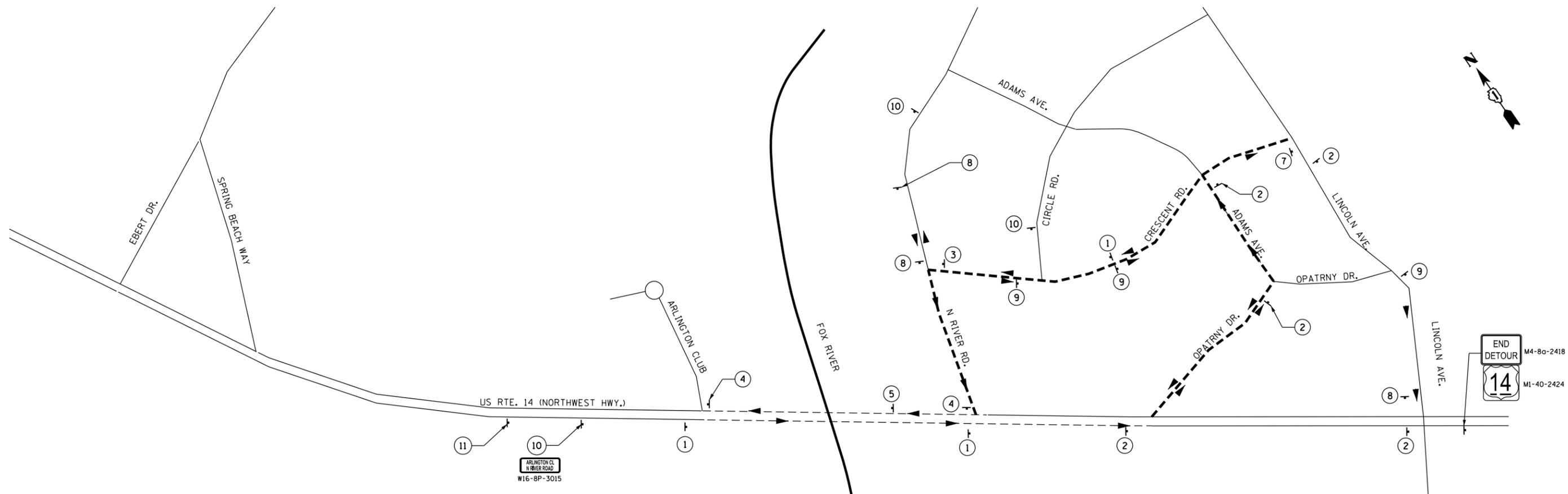
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<b>MAINTENANCE OF TRAFFIC GENERAL NOTES &amp; SEQUENCE OF CONSTRUCTION</b>	
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STA. N/A	TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	7
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\$FILES

### LEGEND

1	W16-8P-3015 M4-9S-3024	2	W16-8P-3015 M4-9L-3024	3	W16-8P-308 M4-9L-3024
4	W16-8P-308 M6-1-2115	5	W16-8P-308 M6-1-2115	6	M4-8a-2418
7	M4-8-2412 M4-5-2412 M3-2-2412 M1-40-2424 M6-1-2115	8	M4-8-2412 M4-5-2412 M3-2-2412 M1-40-2424 M6-1-2115	9	M4-8-2412 M4-5-2412 M3-2-2412 M1-40-2424 M6-1-2115
10	W20-2-48	11	NO LEFT TURN ACCESS TO ARLINGTON CT & N RIVER ROAD FOLLOW DETOUR 5" BLACK LETTER ON ORANGE BACKGROUND		- - - DETOUR ROUTE



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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

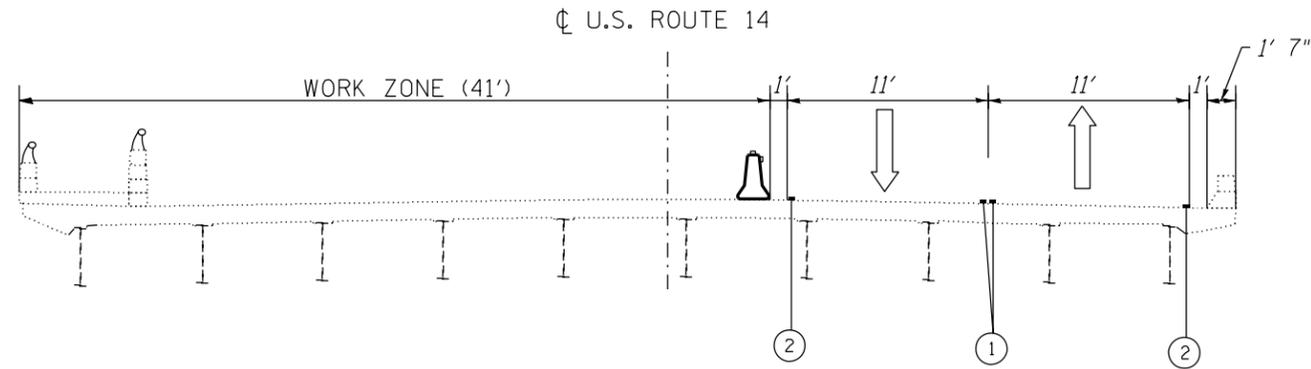
**US ROUTE 14 (NORTHWEST HIGHWAY)  
 DETOUR PLAN STAGE I AND II**

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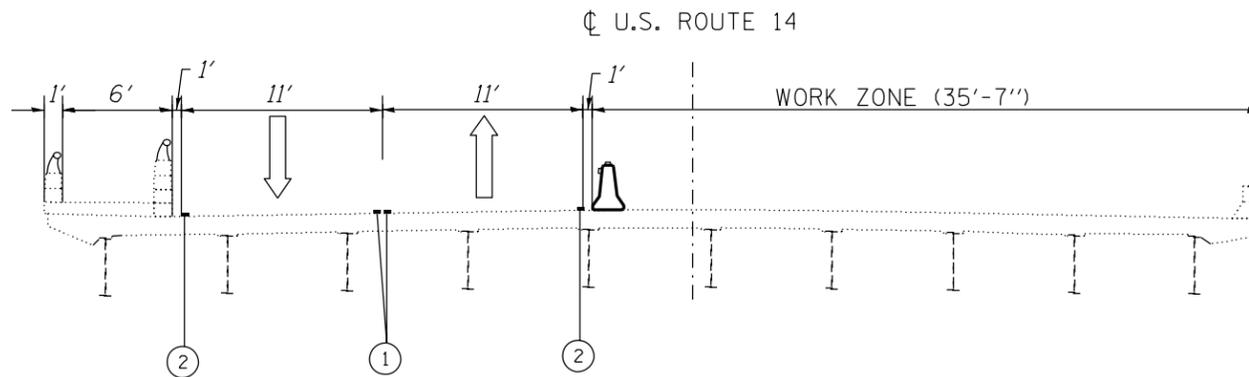
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CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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 USER NAME = Millennia Professional Services



U.S. ROUTE STAGE 1 (LOOKING EAST)



U.S. ROUTE STAGE 2 (LOOKING EAST)

**LEGEND**

-  TEMPORARY CONCRETE BARRIER WITH CRYSTAL DOUBLE SIDED TYPE C MARKERS
-  WORK ZONE
-  PAVEMENT MARKING TAPE, TYPE IV, 4" DOUBLE YELLOW @ 11" C-C WITH TEMPORARY RAISED PAVEMENT MARKINGS @ 40' C-C
-  PAVEMENT MARKING TAPE, TYPE IV, 4" SOLID WHITE
-  DIRECTION OF TRAFFIC



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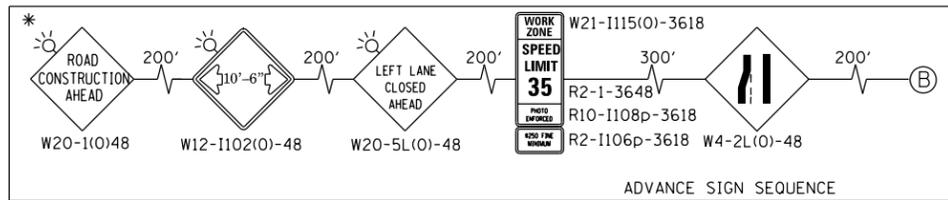
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US ROUTE 14 (NORTHWEST HIGHWAY)  
MAINTENANCE OF TRAFFIC - TYPICAL SECTIONS

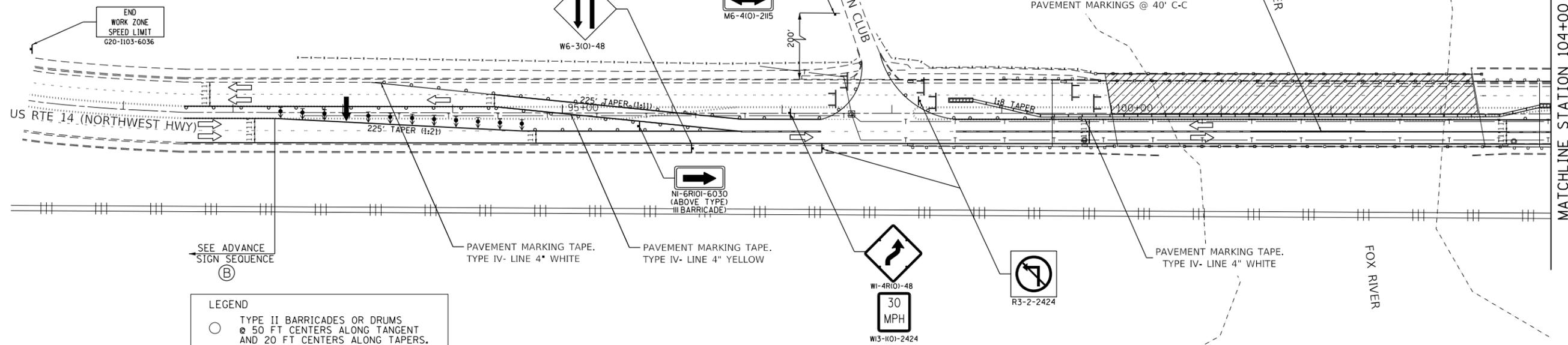
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<b>CONTRACT NO. 62J35</b>				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

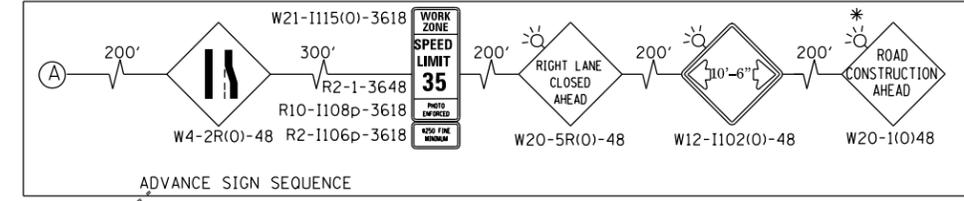
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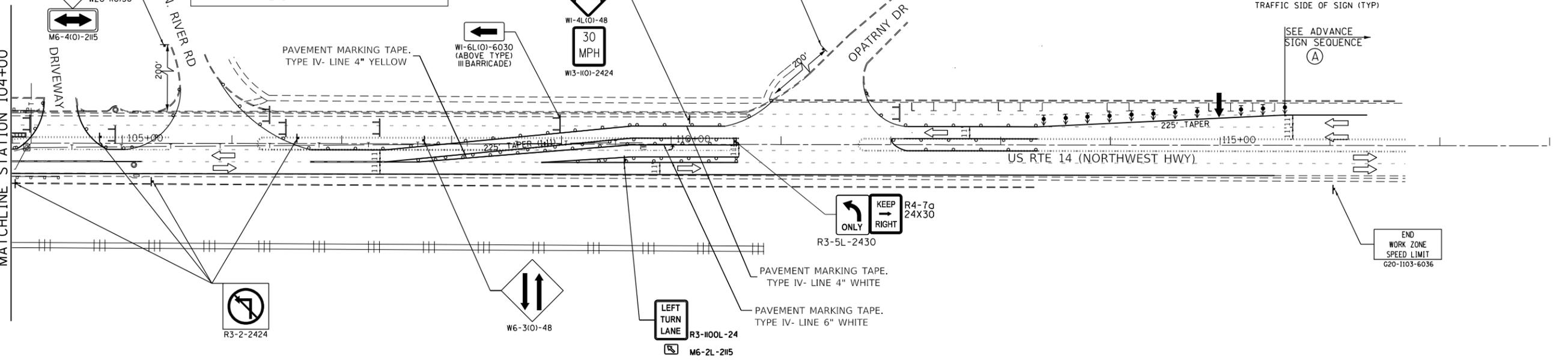
\* BEACONS SHOULD BE PLACED ON TRAFFIC SIDE OF SIGN (TYP)



- LEGEND**
- TYPE II BARRICADES OR DRUMS @ 50 FT CENTERS ALONG TANGENT AND 20 FT CENTERS ALONG TAPERS.
  - ↓ DIRECTION INDICATOR BARRICADES @ 20' C-C WITH STEADY BURN LIGHTS
  - ⊥ TYPE III BARRICADES WITH FLASHING BURNING LIGHTS
  - ▨ WORK ZONE
  - ARROW BOARD
  - ⊥ SIGN
  - ▬ IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2
  - ▬ TEMPORARY CONCRETE BARRIER WALL WITH DOUBLE SIDED TYPE C CRYSTAL MARKERS



\* BEACONS SHOULD BE PLACED ON TRAFFIC SIDE OF SIGN (TYP)



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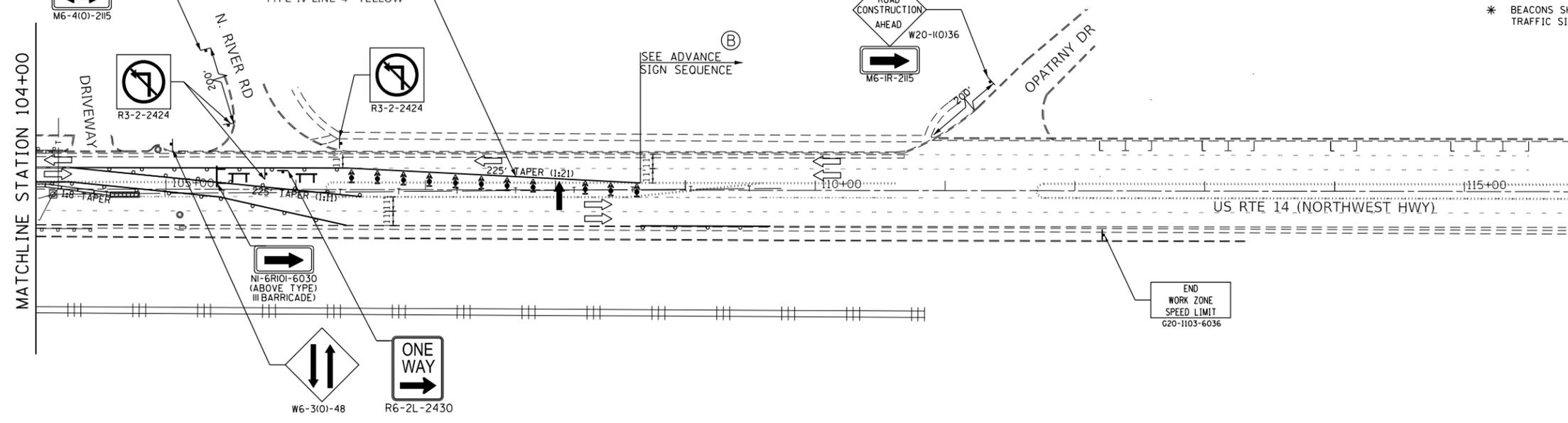
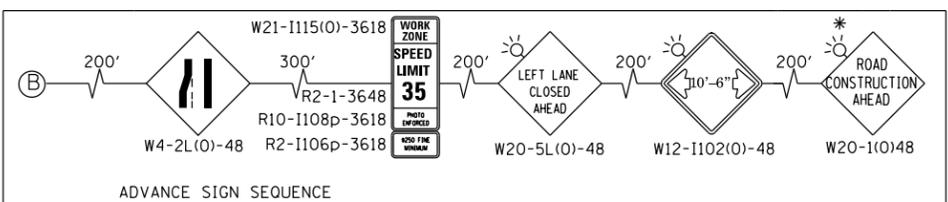
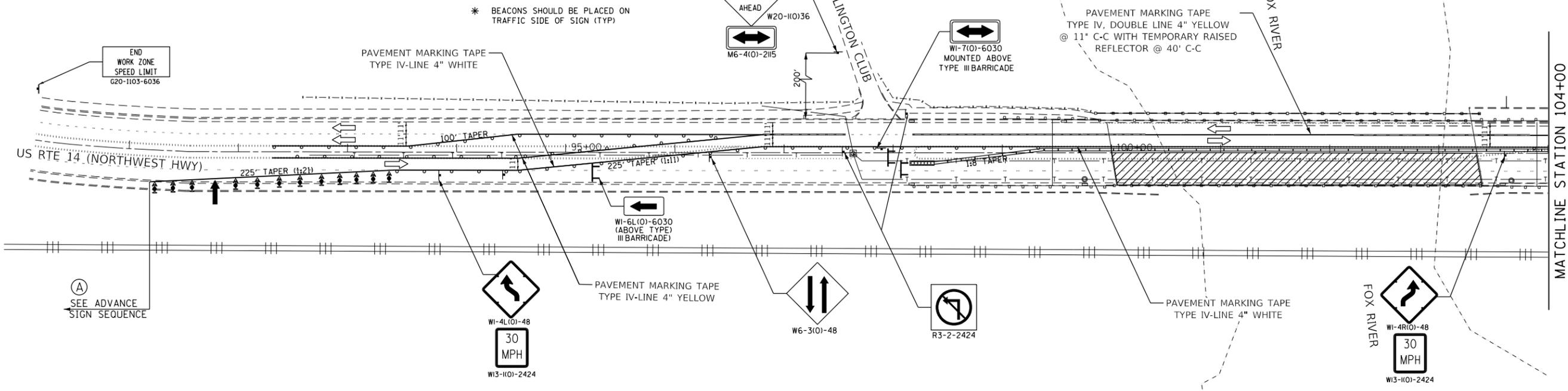
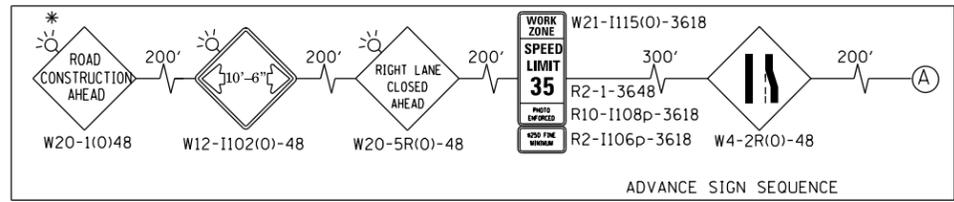
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
 MAINTENANCE OF TRAFFIC - STAGE I**

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2019-058-BR	COUNTY MCHENRY	TOTAL SHEETS 47	SHEET NO. 10
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\$FILE\$



- LEGEND**
- TYPE II BARRICADES OR DRUMS @ 50 FT CENTERS ALONG TANGENT AND 20 FT CENTERS ALONG TAPERS.
  - ⇄ DIRECTION INDICATOR BARRICADES @ 20' C-C WITH STEADY BURN LIGHTS
  - ⚡ TYPE III BARRICADES WITH FLASHING BURNING LIGHTS
  - ▨ WORK ZONE
  - ➡ ARROW BOARD
  - ⚡ SIGN
  - ▬ IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE, NARROW, TEST LEVEL 2)
  - ▬ TEMPORARY CONCRETE BARRIER WALL WITH DOUBLE SIDED TYPE C CRYSTAL MARKERS

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PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE - 06/26/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
MAINTENANCE OF TRAFFIC - STAGE II**

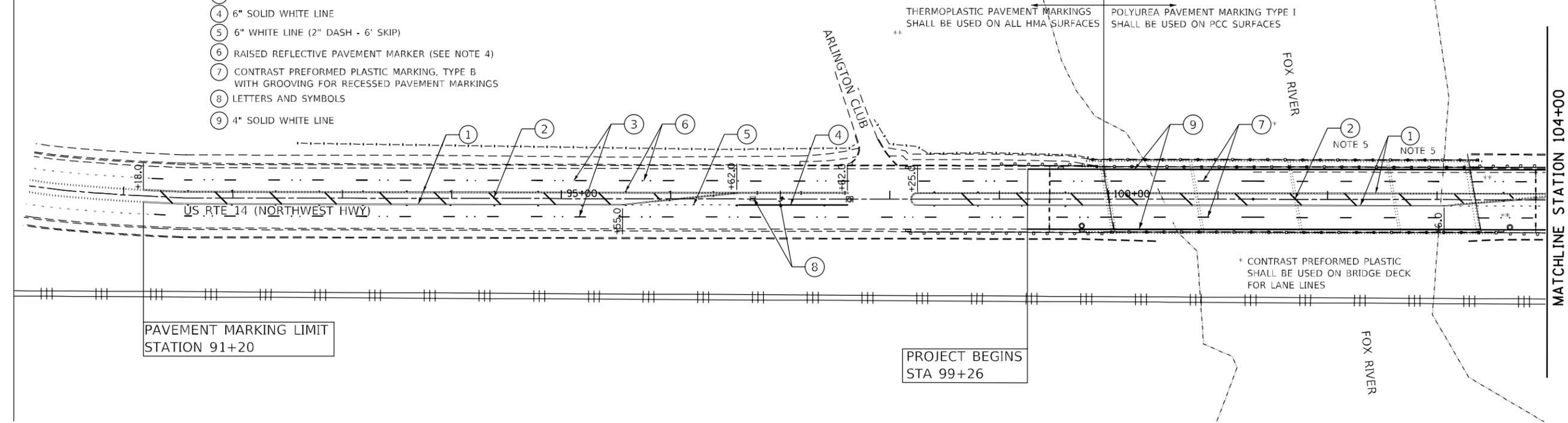
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	11
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\$FILE\$

**PAVEMENT MARKING LEGEND**

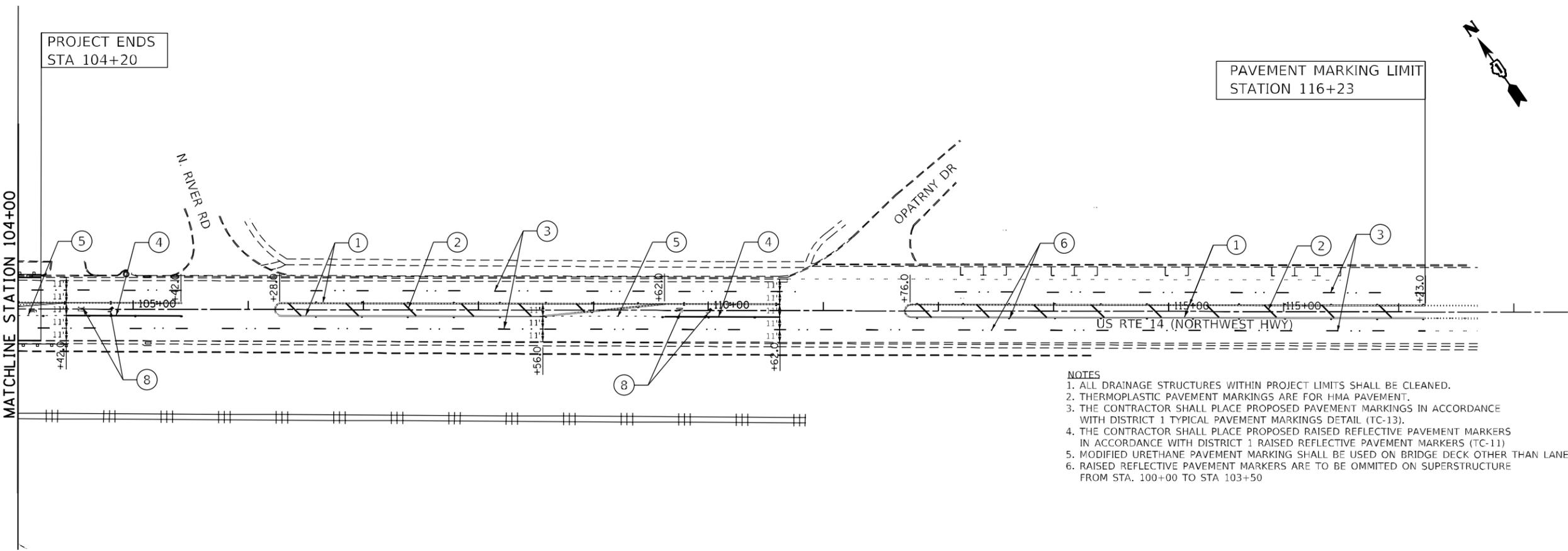
- ① 4" DOUBLE YELLOW LINE @ 11" C-C
- ② 12" SOLID YELLOW DIAGONALS
- ③ 4" WHITE LANE LINE (10' DASH - 30' SKIP)
- ④ 6" SOLID WHITE LINE
- ⑤ 6" WHITE LINE (2" DASH - 6' SKIP)
- ⑥ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)
- ⑦ CONTRAST PREFORMED PLASTIC MARKING, TYPE B WITH GROOVING FOR RECESSED PAVEMENT MARKINGS
- ⑧ LETTERS AND SYMBOLS
- ⑨ 4" SOLID WHITE LINE



PAVEMENT MARKING LIMIT  
STATION 91+20

PROJECT BEGINS  
STA 99+26

MATCHLINE STATION 104+00



MATCHLINE STATION 104+00

PROJECT ENDS  
STA 104+20

PAVEMENT MARKING LIMIT  
STATION 116+23

- NOTES**
1. ALL DRAINAGE STRUCTURES WITHIN PROJECT LIMITS SHALL BE CLEANED.
  2. THERMOPLASTIC PAVEMENT MARKINGS ARE FOR HMA PAVEMENT.
  3. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
  4. THE CONTRACTOR SHALL PLACE PROPOSED RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
  5. MODIFIED URETHANE PAVEMENT MARKING SHALL BE USED ON BRIDGE DECK OTHER THAN LANE
  6. RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE OMITTED ON SUPERSTRUCTURE FROM STA. 100+00 TO STA 103+50

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 USER NAME = Millennia Professional Services



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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US ROUTE 14 (NORTHWEST HIGHWAY)  
PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

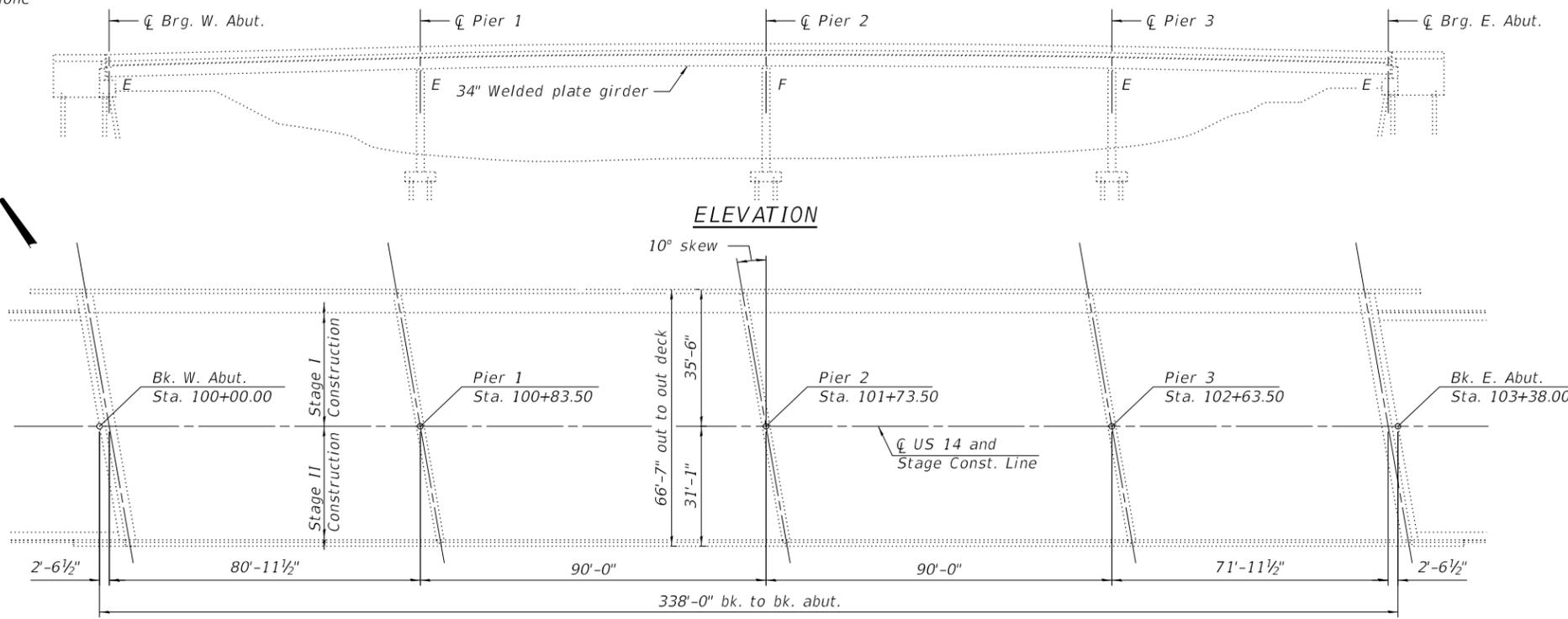
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<b>CONTRACT NO. 62J35</b>				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

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 Design firm no. 184001036  
 whks engineers + planners + land surveyors

**Existing Structure:** The existing structure was constructed in 1989 as F.A. 9, Section 25B-R (84). The structure is a four span continuous bridge with a reinforced concrete deck supported on ten 34" steel composite (positive moment region only) welded plate girders with pile bent abutments and solid wall piers. The existing structure is skewed 10 degrees right forward. The structure measures 338'-0" back to back of abutment and 66'-7" out to out of bridge deck. An additional barrier and railing was added to the sidewalk in 2004. Two lanes of traffic shall be maintained on US 14 by utilizing stage construction.

**Salvage:** None



**GENERAL NOTES**

- All structural steel shall conform to AASHTO Classification M-270 Gr. 50, unless otherwise noted.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
- Existing reinforcement extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any existing reinforcement bars intended for reuse and damaged during the concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system to the satisfaction of the Engineer. Cost included with Concrete Removal.
- Fasteners shall be ASTM F3125 Grade A325 Type I, mechanically galvanized bolts. Bolts 3/4 in. Ø, holes 13/16 in. Ø, unless otherwise noted.
- No field welding is permitted except as specified in the contract documents.
- Dirt and debris shall be cleaned from the bridge scuppers, floor drains, and drain pipes. Cost included with Bridge Deck Scarification, unless noted otherwise.
- Deck Slab Repair quantities shown are based upon a prior inspection. Resident Engineer shall determine deck slab repair areas after scarification is complete. Repair areas shall be documented in the as-built plans.
- All new structural steel shall be shop painted with an inorganic zinc rich primer per AASHTO M 300, Type 1.
- The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the drainage scuppers and floor drains.
- Dirt and debris shall be cleaned from the existing abutment seats. Cost included with Concrete Removal.
- The Contractor shall obtain Cost Guard approval for any work that may interfere with navigational operations of the navigable waters. A work plan shall be prepared by the Contractor, reviewed and approved by the Engineer and submitted by the Engineer to the Coast Guard at the address listed below for approval.

Bridge Administrator  
 US Coast Guard  
 Ninth Coast Guard District  
 1240 E. Ninth St.  
 Cleveland, OH 44199-2060

**INDEX OF SHEETS**

- General Plan and Elevation
- General Details
- Deck Repair Plan
- Deck Removal Plan
- 6. Deck Replacement Plan
- 9. Preformed Joint Strip Seal - Sidewalk
- Framing Plan
- Steel Repair Details
- Bearing Repair Details
- Abutment Repair Details
- Bar Splicer Assembly and Mechanical Splicer Details
- Adjusting Existing Scupper
- 27. Existing Structure Plans (For information only)

**SCOPE OF WORK**

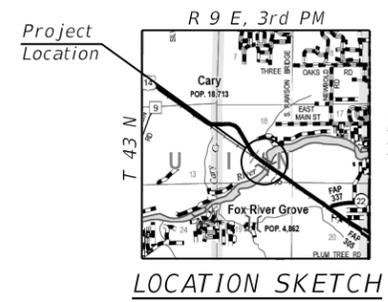
- Remove and replace existing expansion joints at both abutments.
- Scarify bridge deck 3/4" and install 2 1/2" Latex Concrete Overlay. Perform deck slab repairs as required.
- Perform Structural Repair of Concrete at both abutments.
- Apply Protective Coat.
- Replace diaphragms and repair beam ends and bearings at locations shown.
- Raise bridge scupper grates and clean bridge scupper, floor drains, and downspouts. See General Notes.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Concrete Removal	Cu. Yd.	25.5
Concrete Superstructure	Cu. Yd.	27.7
Bridge Deck Grooving	Sq. Yd.	2082
Protective Coat	Sq. Yd.	2798
Furnishing and Erecting Structural Steel	Pound	2060
Reinforcement Bars, Epoxy Coated	Pound	2290
Bar Splicers	Each	16
Preformed Joint Strip Seal	Foot	132
Anchor Bolts, 1" Ø	Each	5
Epoxy Crack Injection	Foot	18
Structural Steel Removal	Pound	2060
Structural Steel Repair	Pound	830
Bridge Deck Latex Concrete Overlay, 2 1/2"	Sq. Yd.	2123
Bridge Deck Scarification, 3/4"	Sq. Yd.	2123
Structural Repair of Concrete (Depth Equal to or Less than 5 in.)	Sq. Ft.	22
Structural Repair of Concrete (Depth Greater than 5 in.)	Sq. Ft.	24
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	1
Drainage Scupper to be Adjusted	Each	4



*CH E*  
 Expires: 11/30/2020

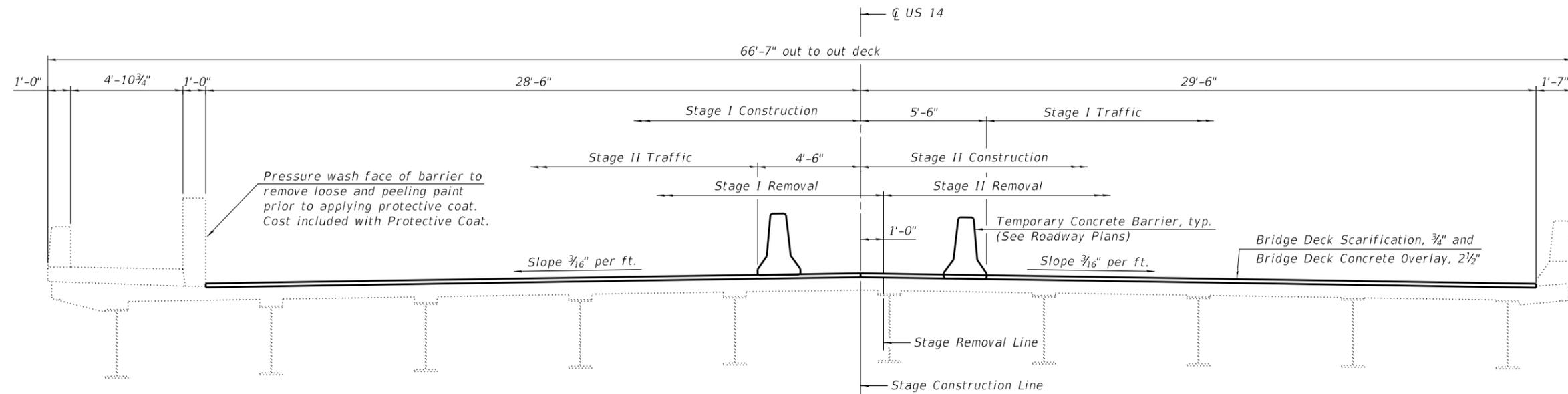


**GENERAL PLAN AND ELEVATION**  
**F.A.P. ROUTE 305 (US 14)**  
**OVER FOX RIVER**  
**PUBLIC WATERS**  
**SECTION 2019-058-BR**  
**MCHENRY COUNTY**  
**STRUCTURE NO. 056-0058**

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PLOT DATE = 8/31/2020	CHECKED - CEH	REVISED

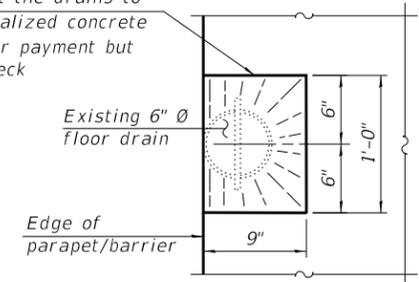
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	13
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT   NHP-38RN(839)				

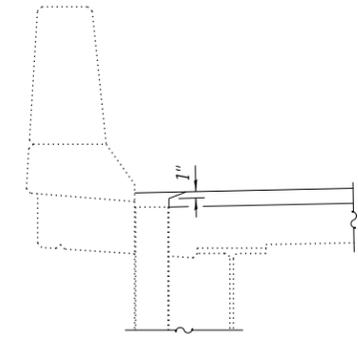


**CROSS SECTION**  
(Looking East)

Localized concrete removal may be required at the drains to allow a minimum overlay thickness of 2 1/2". Localized concrete removal at the drains will not be measured for payment but shall be included in the unit cost of Bridge Deck Scarification of the thickness specified.



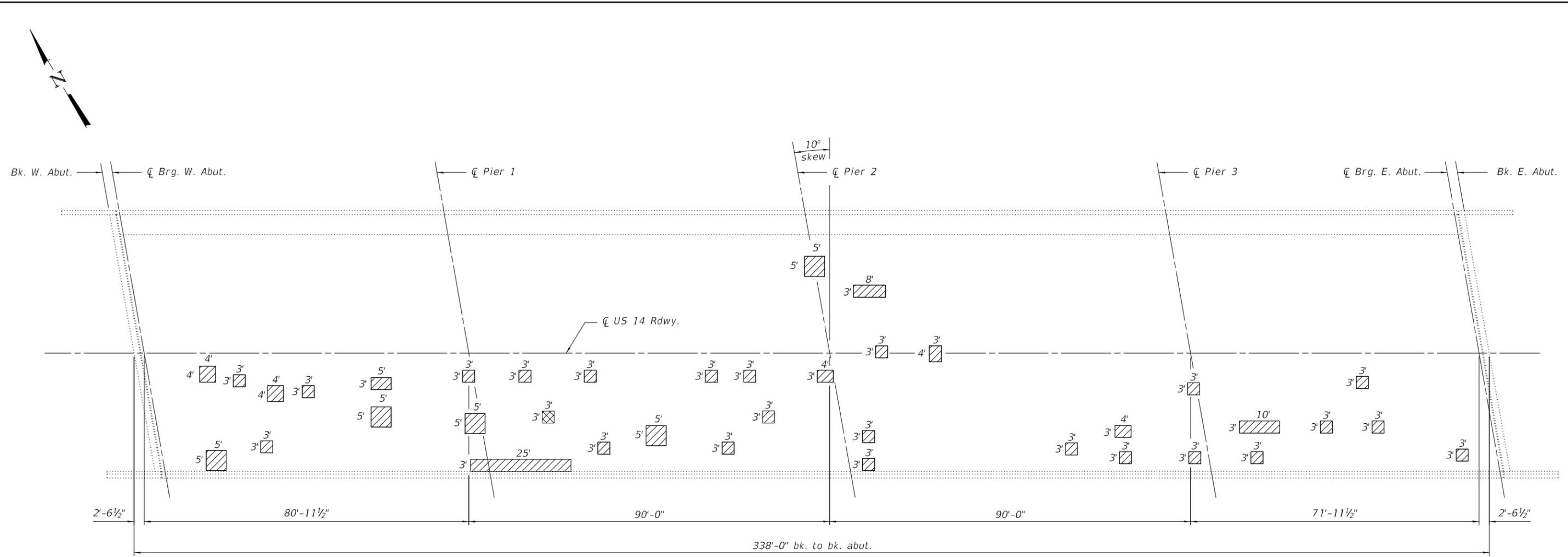
**OVERLAY TREATMENT AT DRAINS**



**SECTION AT DRAINS**

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FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1' / in.	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	14
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	NHPP-38RN(839)	



**DECK REPAIR PLAN**

**LEGEND:**

- Deck Slab Repair (Partial) (60.5 Sq. Yds., estimated. See Note 1)
- Deck Slab Repair (Full Depth, Type II)

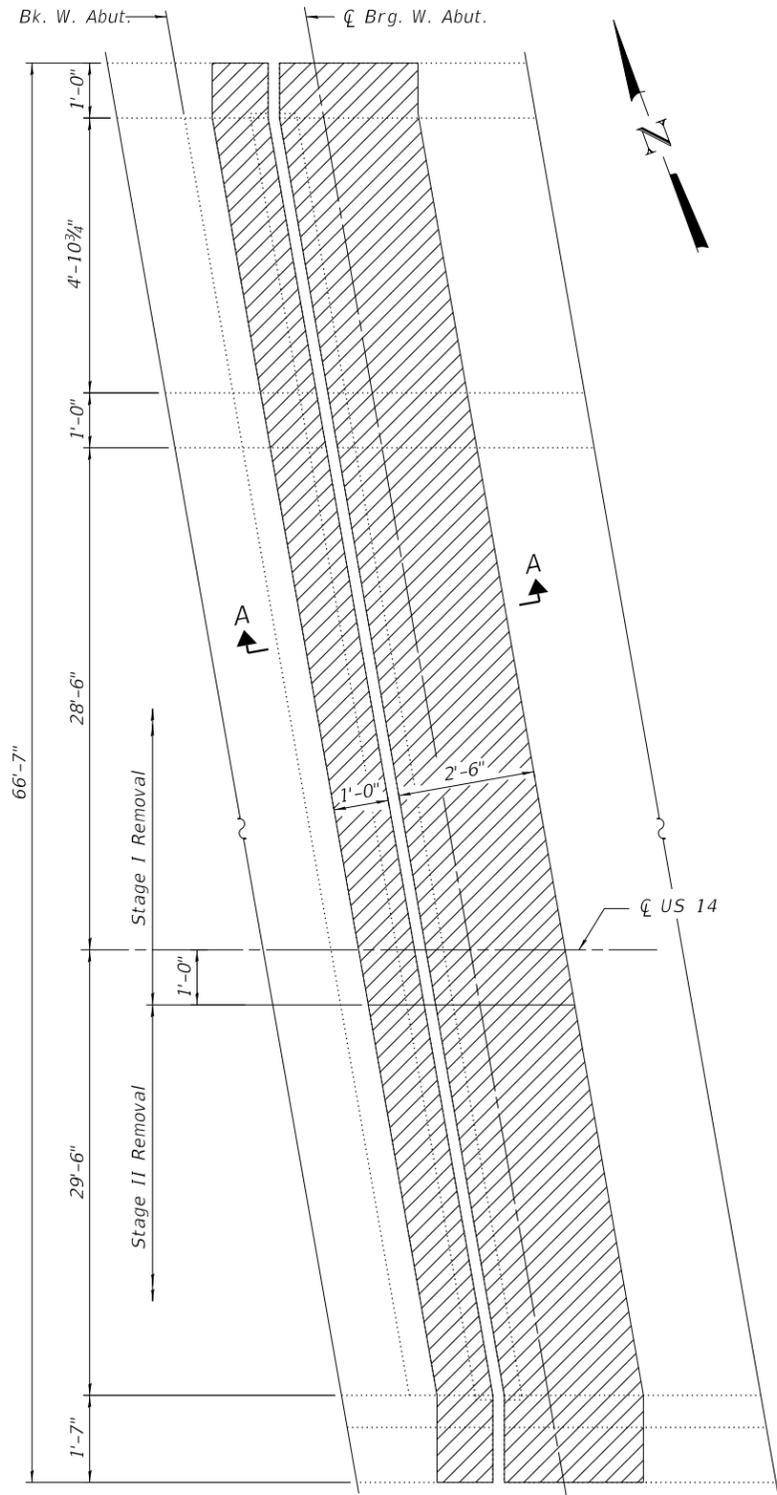
**Notes:**

1. Deck Slab Repair (Partial) locations and quantities shown are for information only and will not be measured for payment but shall be included in the cost of Bridge Deck Latex Concrete Overlay. See special provision for Bridge Deck Latex Concrete Overlay.
2. Quantities and limits of Deck Slab Repairs shown are estimated. Actual limits shall be determined in the field and noted in the As-Built Plans. See special provision for Deck Slab Repair.
3. For expansion joint reconstruction details, see Sheets 4 thru 6.
4. For Stage Construction Scheme, see Sheet 2.

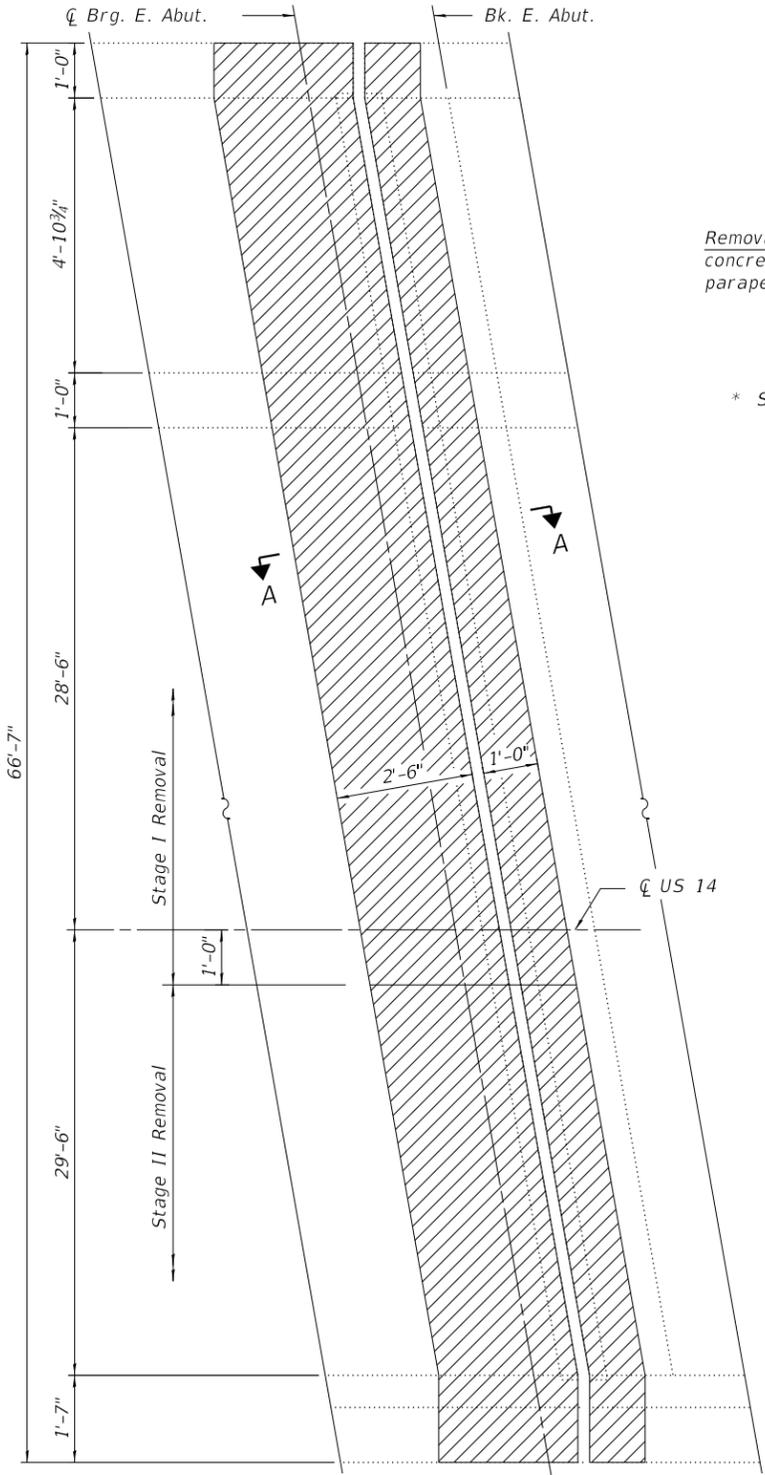
**BILL OF MATERIAL**

Item	Unit	Total
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	1.0

LAST SAVED DATE: 2/15/2020 11:14:14 AM  
 FILE NAME: L:\Vobis\DOT\_D-118793\F18\_1889-010\_VV8793\_12\_US\_14\_Over Fox River\CADD\CAD\_Sheets\056058-62J35.dgn  
 Design firm no. 184001036  
 whks engineers + planners + land surveyors

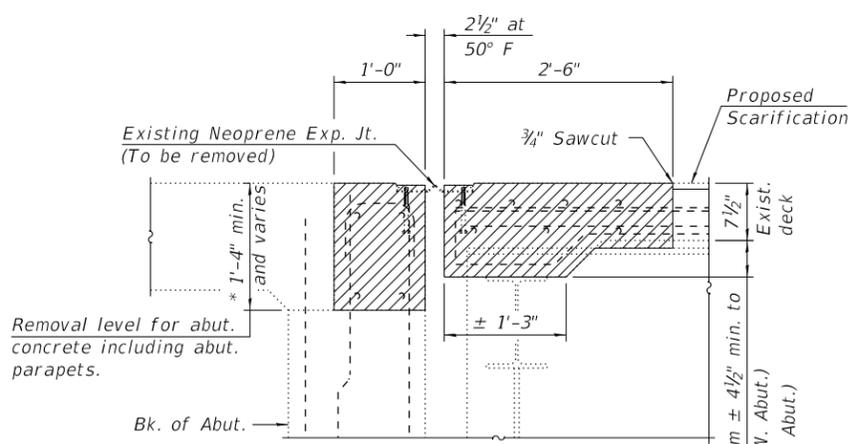


**PARTIAL PLAN - WEST ABUTMENT**



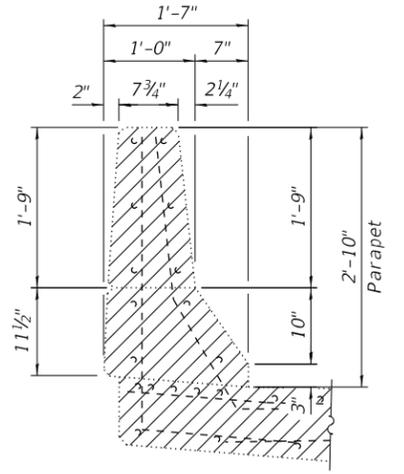
**PARTIAL PLAN - EAST ABUTMENT**

**LEGEND**

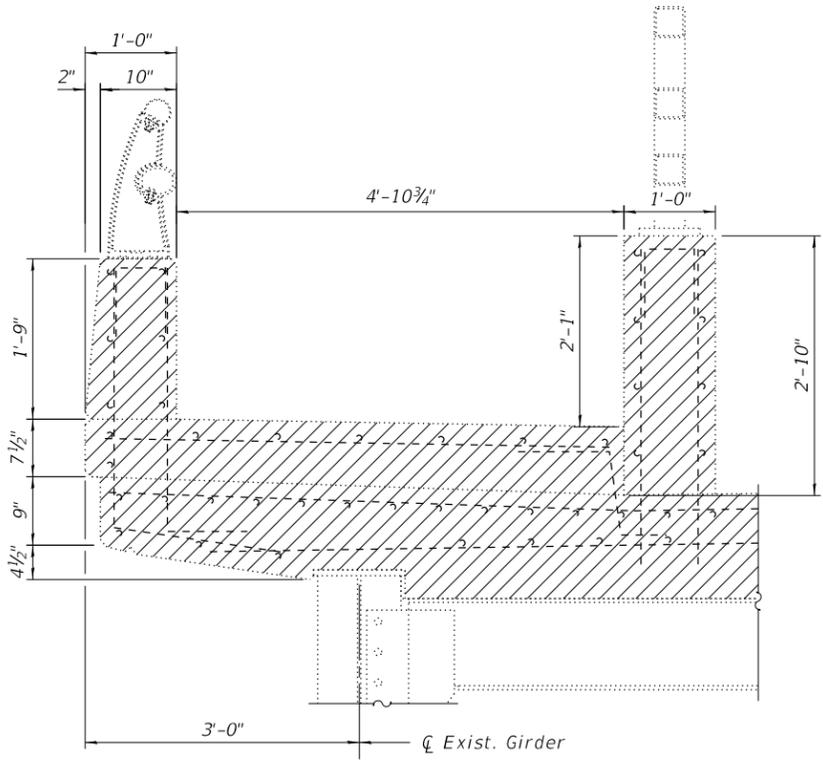


**SECTION A-A**

(Dimensions at Rt. Δ's to back of abut.)



**SECTION THRU PARAPET**



**SECTION THRU SIDEWALK**

**Notes:**

1. Removal and disposal of existing neoprene expansion joint shall be included with the cost of Concrete Removal.
2. Existing reinforcement extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any existing reinforcement bars intended for reuse and damaged during the concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system to the satisfaction of the Engineer. Cost included with Concrete Removal.
3. Portions of the existing railings may need to be temporarily removed and reinstalled to facilitate concrete removal and expansion joint replacement. Removal and reinstallation of portions of the existing railings shall be included in the cost of Concrete Removal. Existing hardware that is corroded or damaged and cannot be reused shall be replaced in-kind and paid for according to Article 109.04 of the Standard Specifications.

**STATE OF ILLINOIS  
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**DECK REMOVAL PLAN  
STRUCTURE NO. 056-0058**

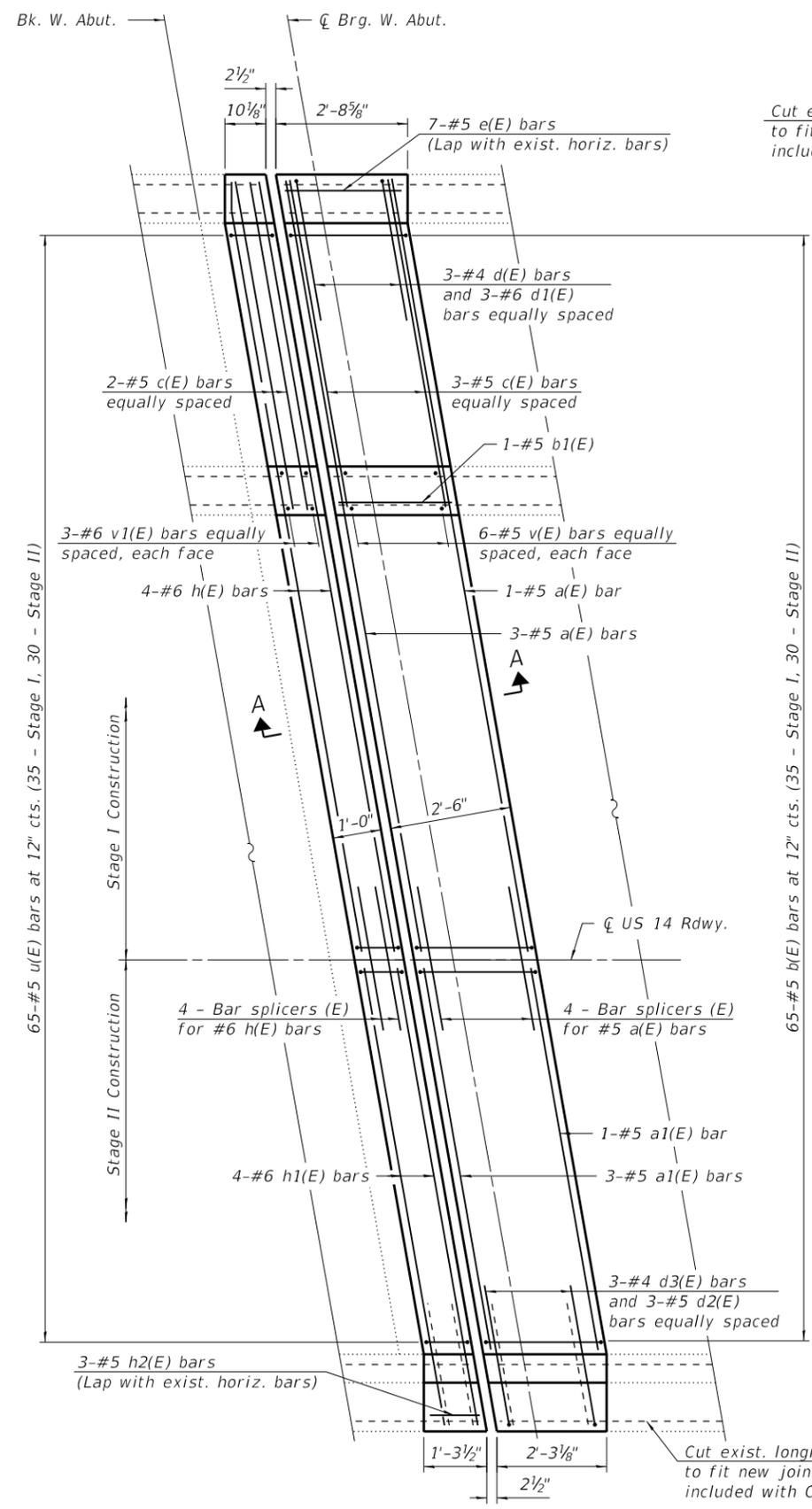
SHEET NO. 4 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	16
CONTRACT NO. 62J35				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NHP-38RW839J

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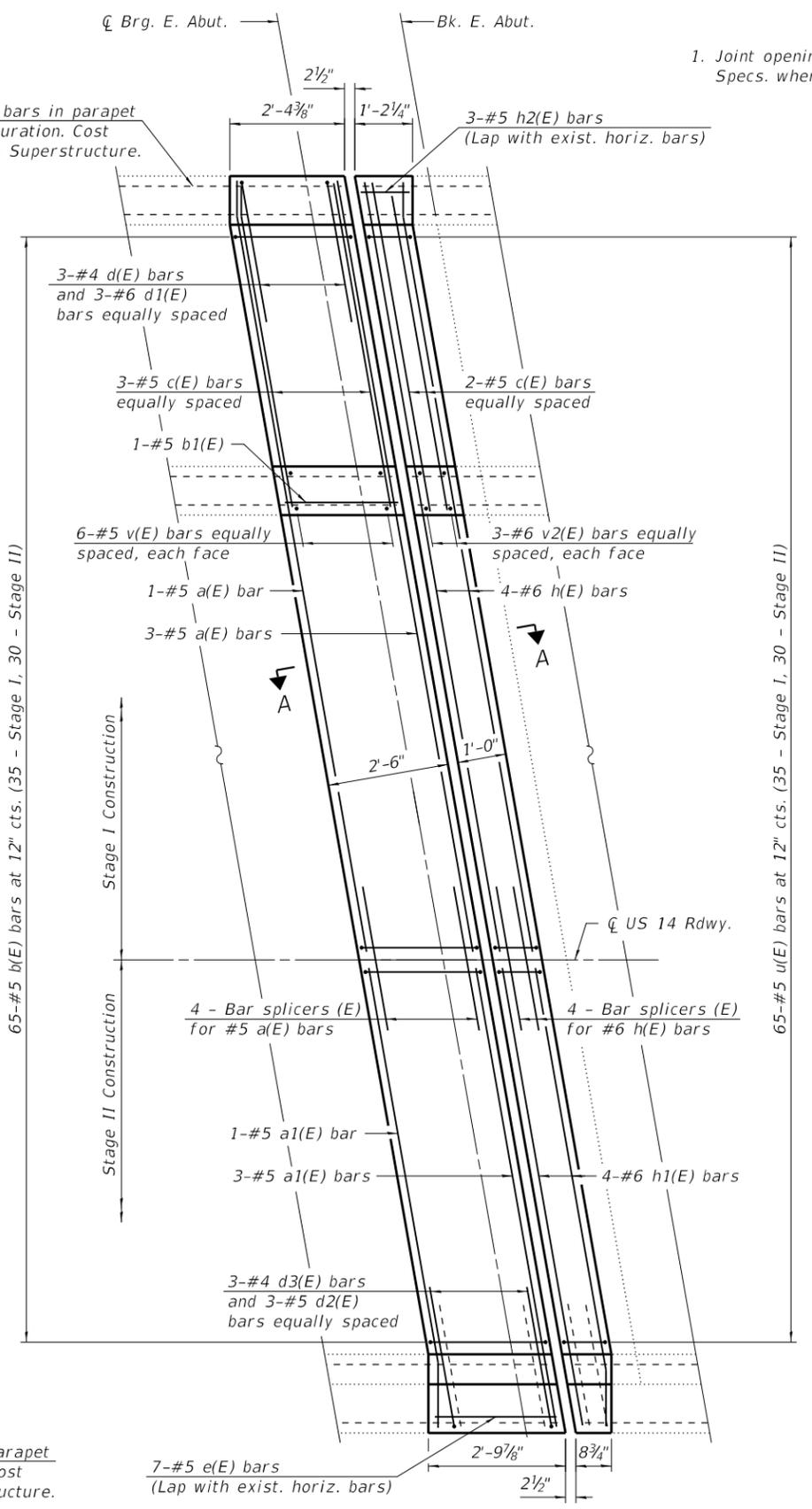
FILE NAME: L:\Jobs\DOT\_D-118793\FB\_189-010\_VV8793.12\_US\_14\_Over Fox River\CAD\CAD\_Sheets\056058-62J35.dgn



**PARTIAL PLAN - WEST ABUTMENT**

Cut exist. longitudinal bars in parapet to fit new joint configuration. Cost included with Concrete Superstructure.

Cut exist. longitudinal bars in parapet to fit new joint configuration. Cost included with Concrete Superstructure.



**PARTIAL PLAN - EAST ABUTMENT**

1. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.



DESIGN FIRM	USER NAME = dheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	CHECKED - SBC	REVISED
PLOT SCALE = 0.2"/in.	DRAWN - DLH	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	CHECKED - CEH	REVISED

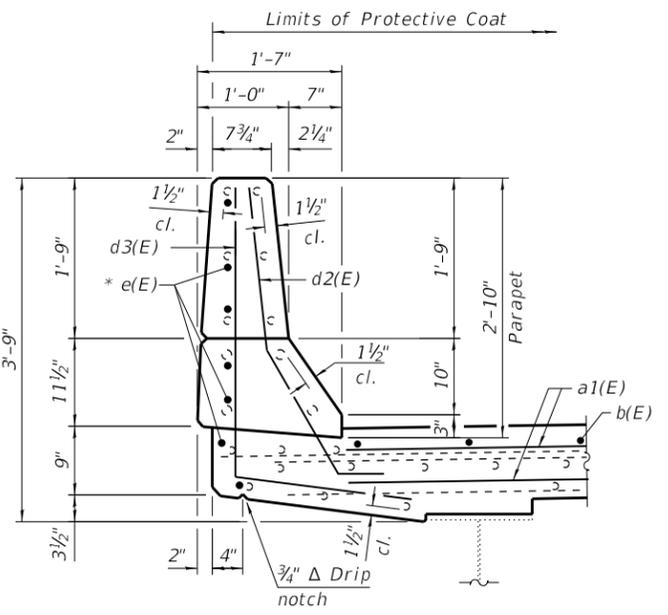
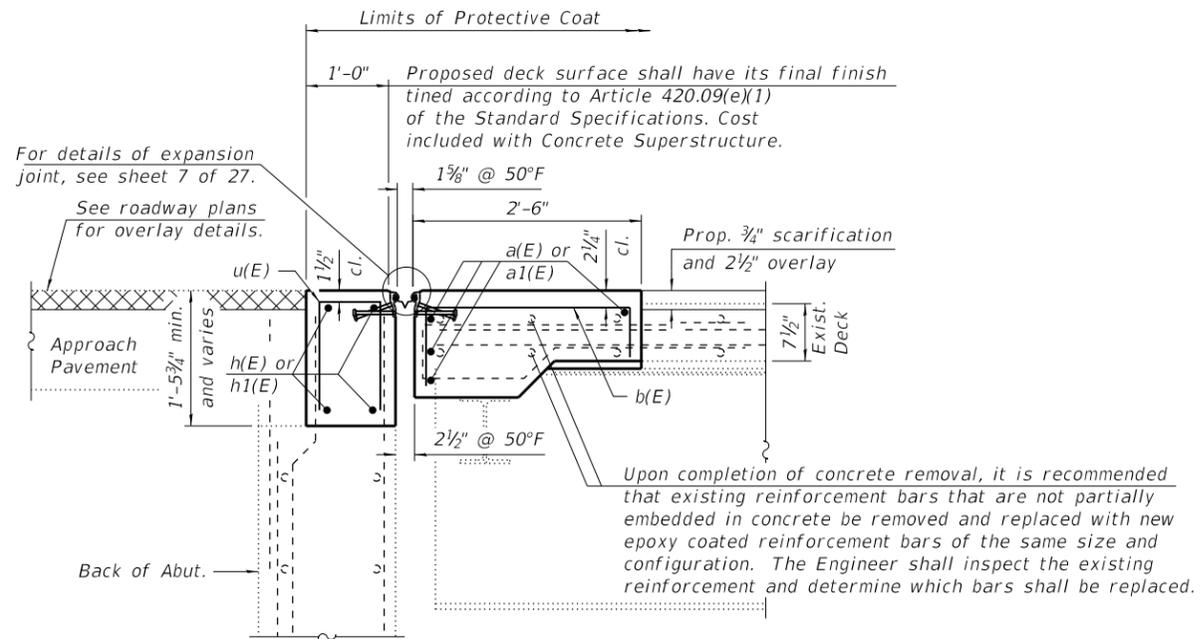
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPLACEMENT PLAN  
STRUCTURE NO. 056-0058**

SHEET NO. 5 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	17
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	NHPP-38RN(839)	

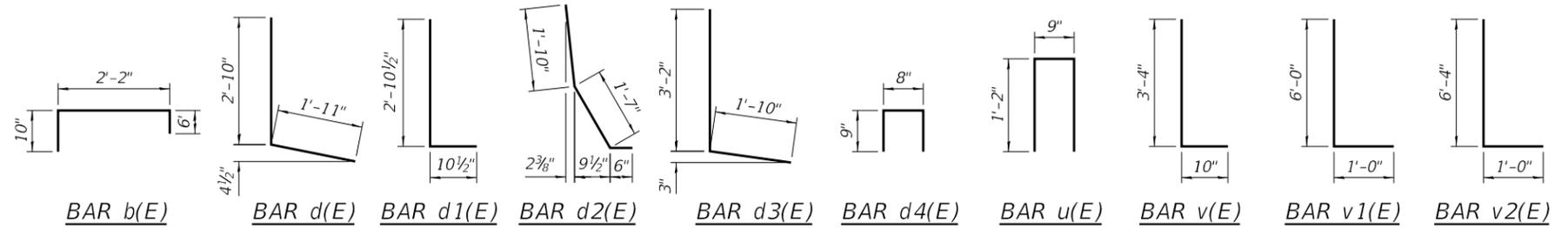
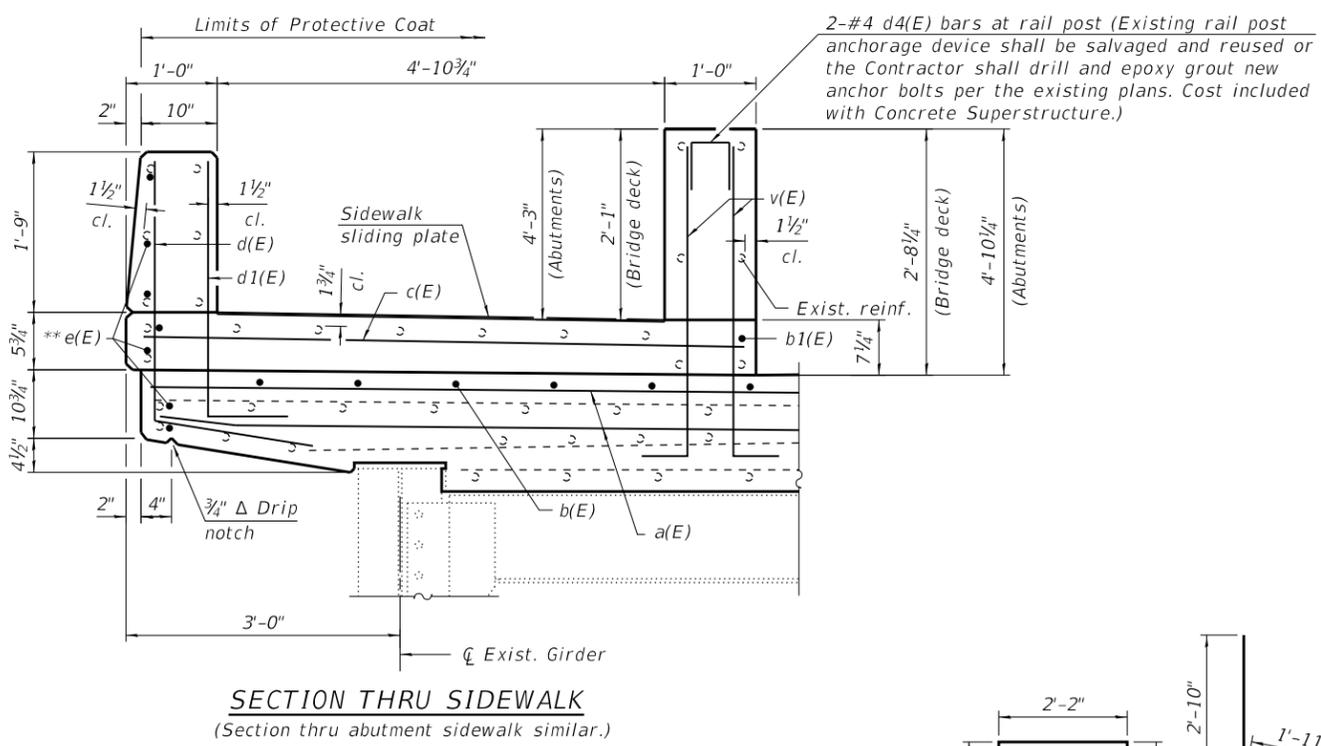
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 Design firm no. 184001036  
 whks engineers + planners + land surveyors



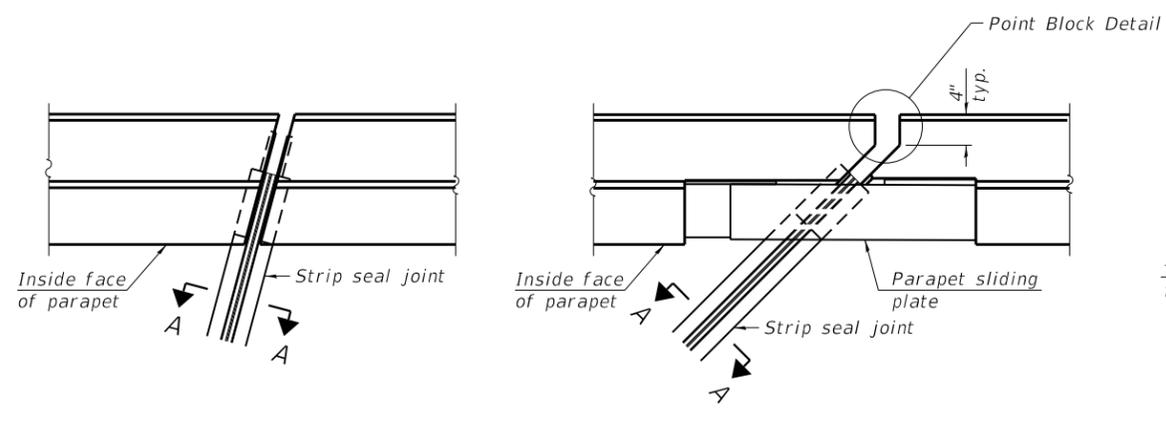
Note:  
b(E) bar bends shall utilize the radius permitted for stirrups.

**BILL OF MATERIAL**

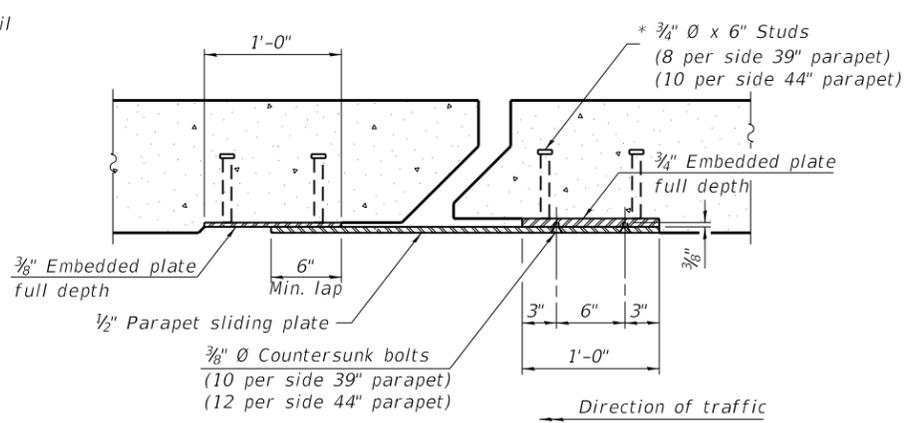
Bar	No.	Size	Length	Shape
a(E)	8	#5	35'-6"	—
a1(E)	8	#5	29'-8"	—
b(E)	130	#5	3'-6"	⌊
b1(E)	2	#5	2'-3"	—
c(E)	10	#5	6'-8"	—
d(E)	6	#4	4'-9"	└
d1(E)	6	#6	3'-9"	└
d2(E)	6	#5	3'-11"	└
d3(E)	6	#4	5'-0"	└
d4(E)	4	#4	2'-2"	└
e(E)	14	#5	2'-5"	—
h(E)	4	#6	35'-6"	—
h1(E)	4	#6	31'-0"	—
h2(E)	6	#5	1'-0"	—
u(E)	130	#5	3'-1"	⌊
v(E)	24	#5	4'-2"	└
v1(E)	6	#6	7'-0"	└
v2(E)	6	#6	7'-4"	└
Concrete Superstructure		Cu. Yd.	27.7	
Reinforcement Bars, Epoxy Coated		Pound	2290	
Bar Splicers		Each	16	



LAST SAVED DATE: 5/15/2020 11:14:14 AM  
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 Design firm: whks engineers + planners + land surveyors  
 no. 184001036

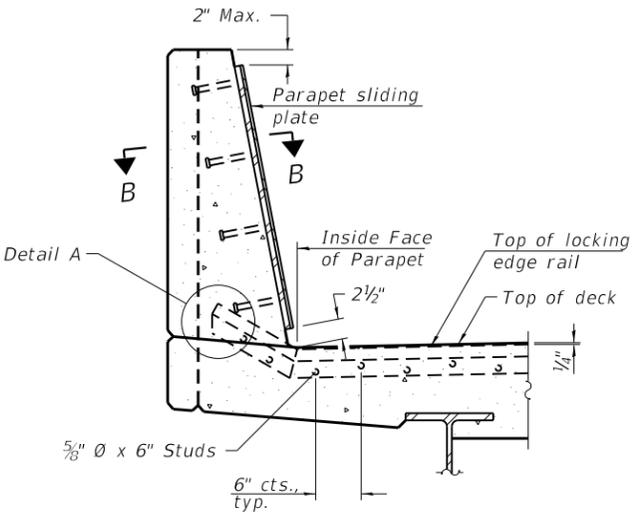


FOR SKEWS  $\leq 30^\circ$   
**PLAN AT PARAPET**

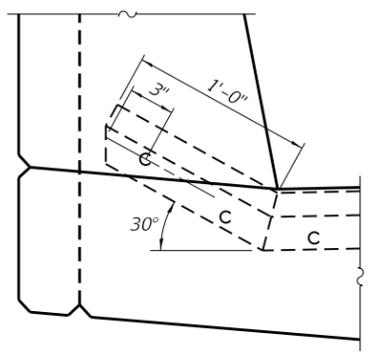


**SECTION B-B**

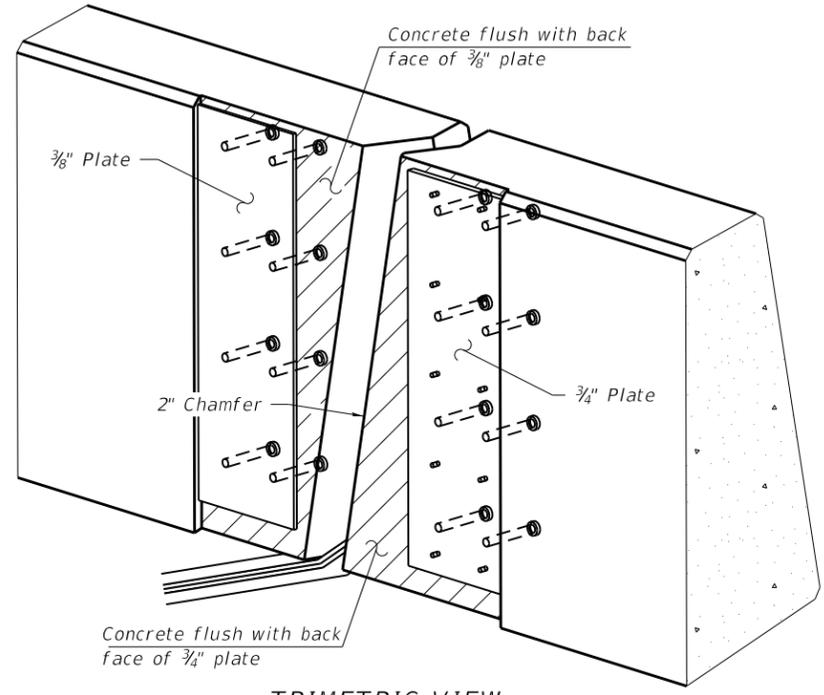
**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
 The manufacturer's recommended installation methods shall be followed.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.



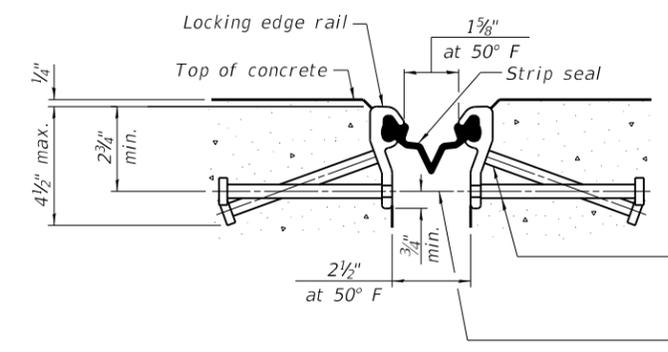
**SECTION AT PARAPET**  
 (Skews > 30° shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)



**DETAIL A**

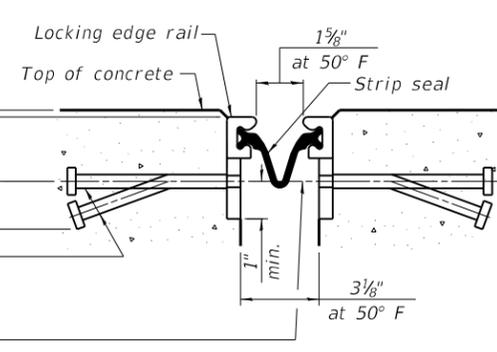


**TRIMETRIC VIEW**  
 (Showing embedded plates only)

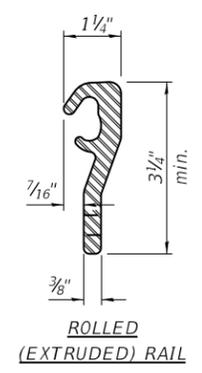


**SHOWING ROLLED RAIL JOINT**

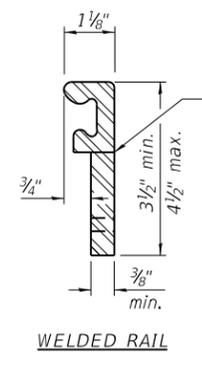
\* 5/8"  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)  
 3/8"  $\phi$  threaded rods in 7/16"  $\phi$  holes at  $\pm 4'-0"$  cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



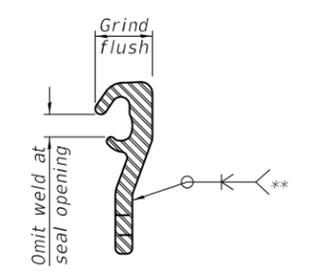
**SHOWING WELDED RAIL JOINT**



**ROLLED (EXTRUDED) RAIL**



**WELDED RAIL**



**LOCKING EDGE RAIL SPLICE**  
 The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**LOCKING EDGE RAILS**  
 \*\* Back gouge not required if complete joint penetration is verified by mock-up.

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	132

EJ-SS-S 1-1-2020



USER NAME = dheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1' / in.	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

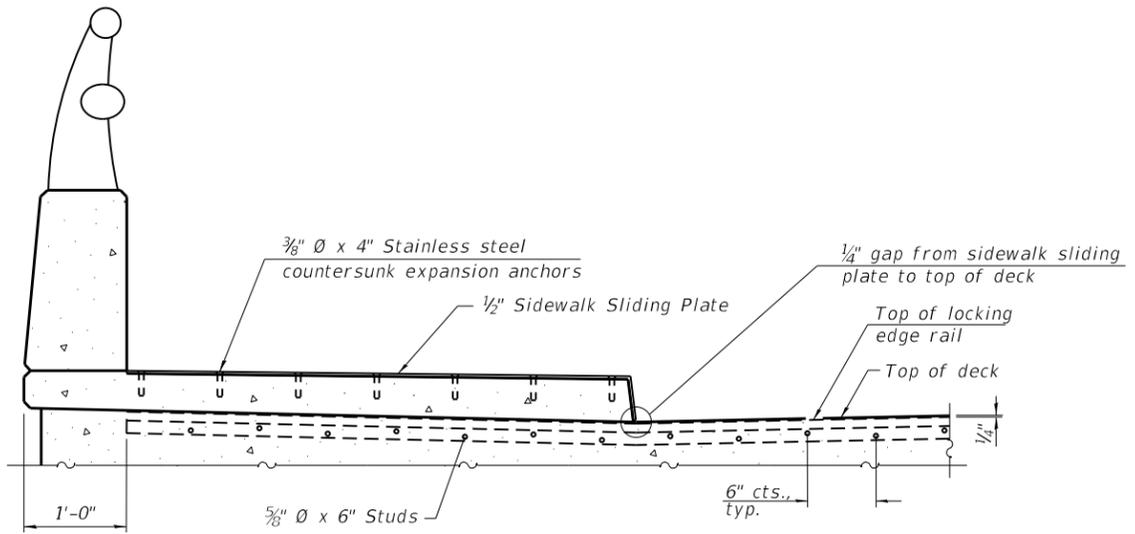
PREFORMED JOINT STRIP SEAL - SIDEWALK  
 STRUCTURE NO. 056-0058

SHEET NO. 7 OF 27 SHEETS

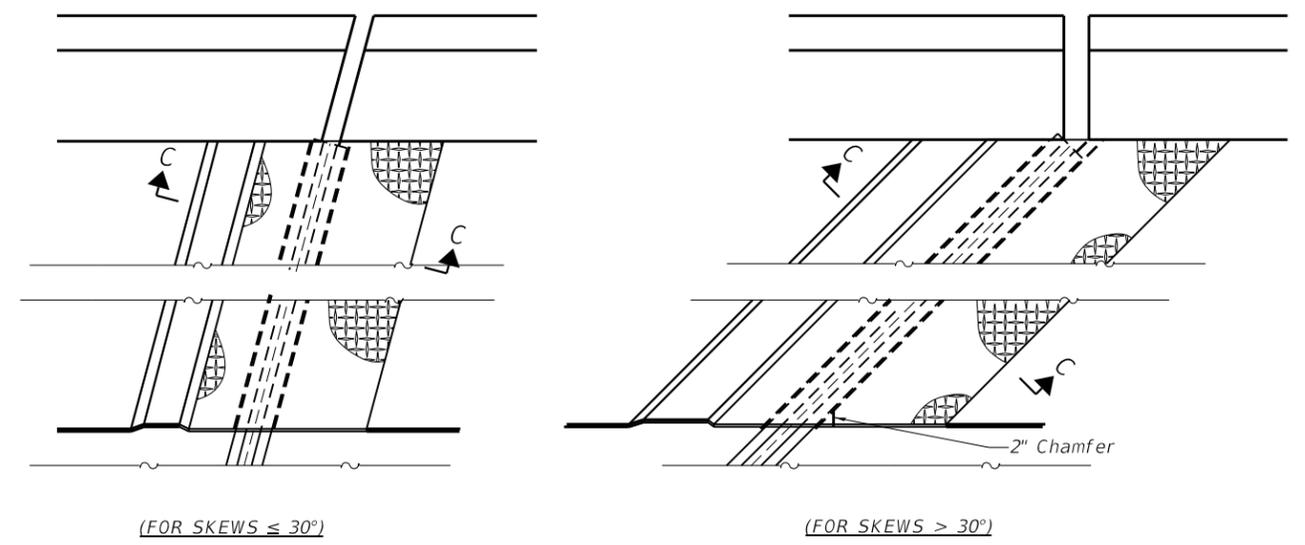
F.A.P. RTE. 305	SECTION 2019-058-BR	COUNTY MCHENRY	TOTAL SHEETS 47	SHEET NO. 19
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NHP-38RN(839)				

(Sheet 1 of 3)

LAST SAVED DATE: 21/05/2020 10:14:11 AM



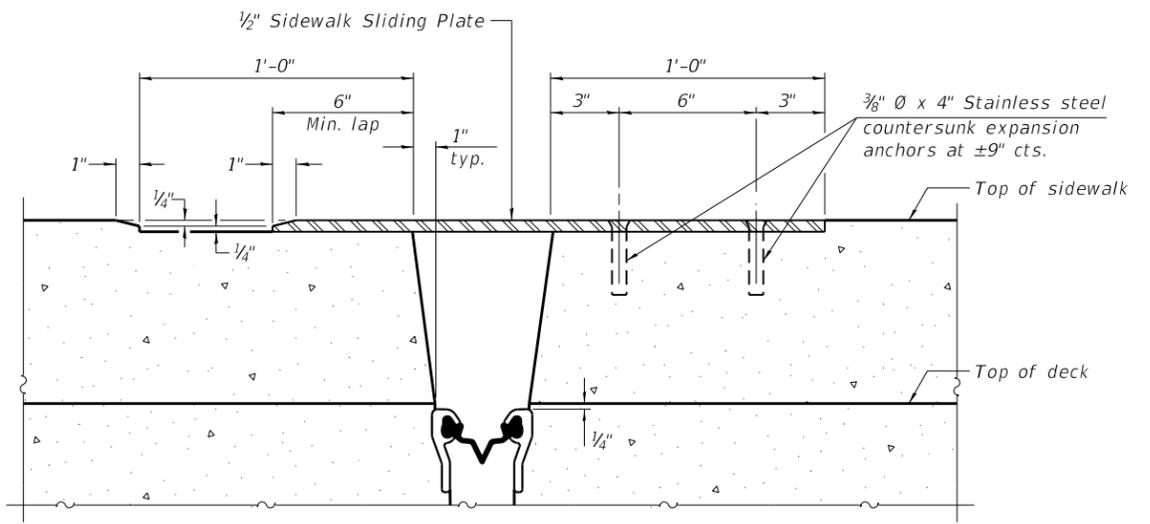
**SECTION AT RAISED SIDEWALK**



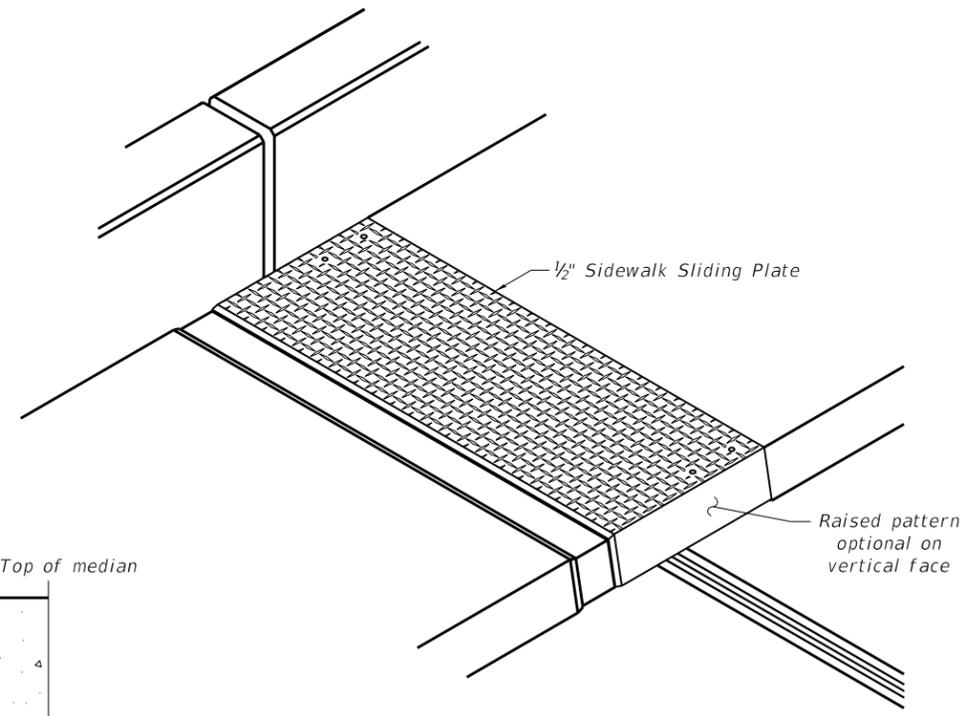
(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

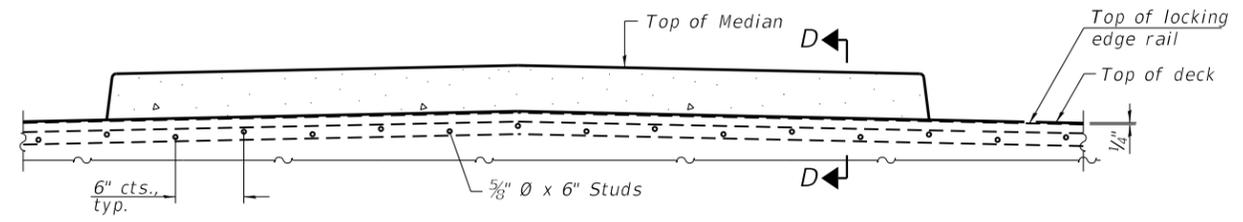
**PLAN AT RAISED SIDEWALK**



**SECTION C-C**

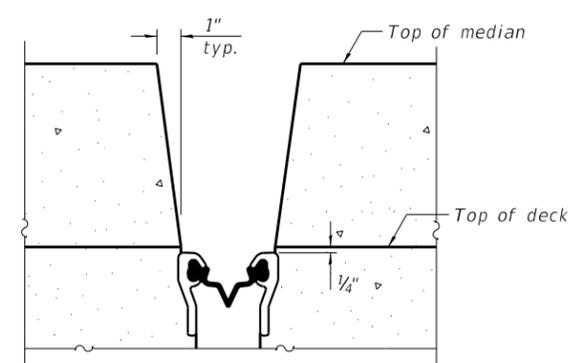


**TRIMETRIC VIEW**



**SECTION AT MEDIAN**

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



**SECTION D-D**  
(at Rt. L's)

(Sheet 2 of 3)

EJ-SS-S 1-1-2020



USER NAME = dtheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1' / in.	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

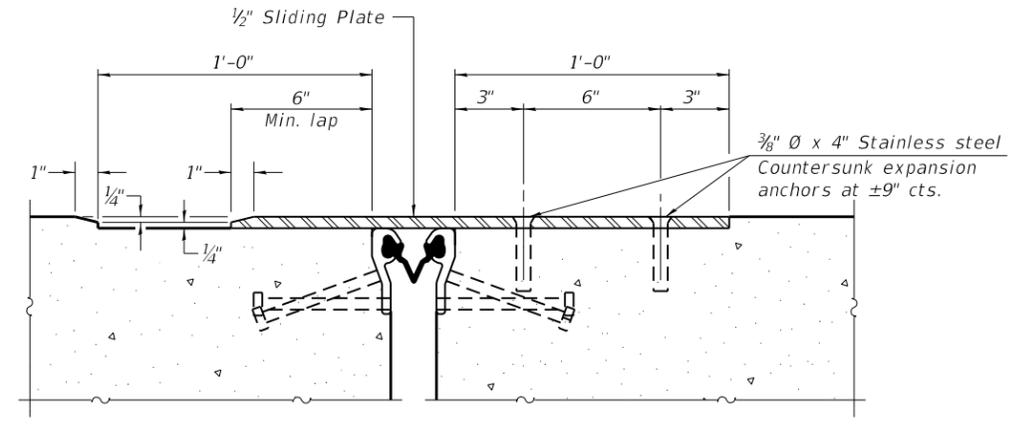
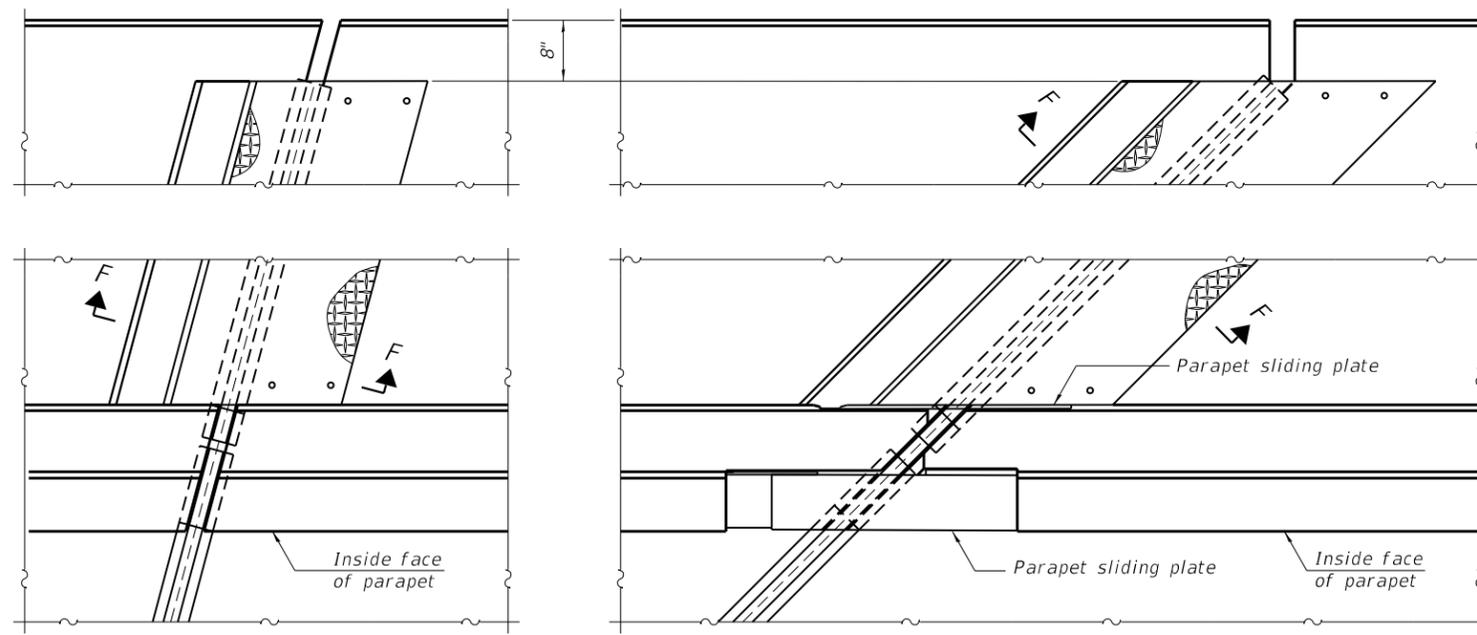
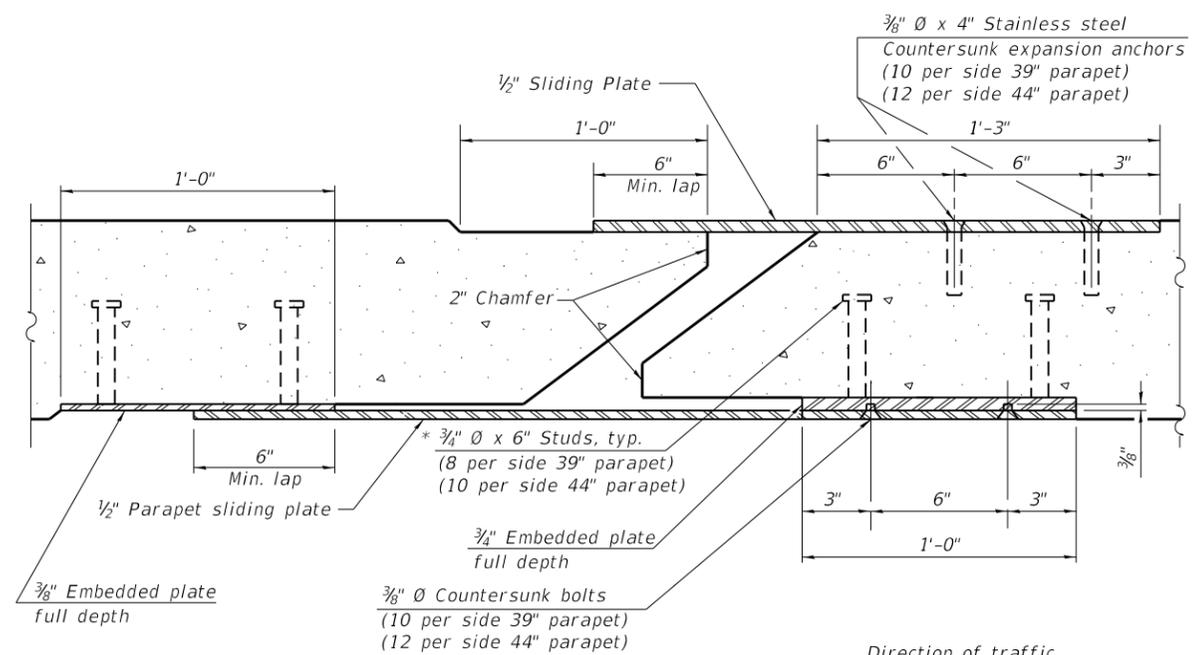
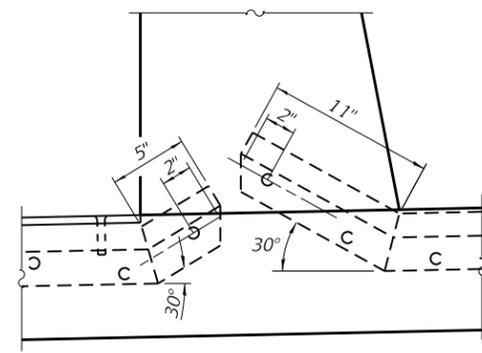
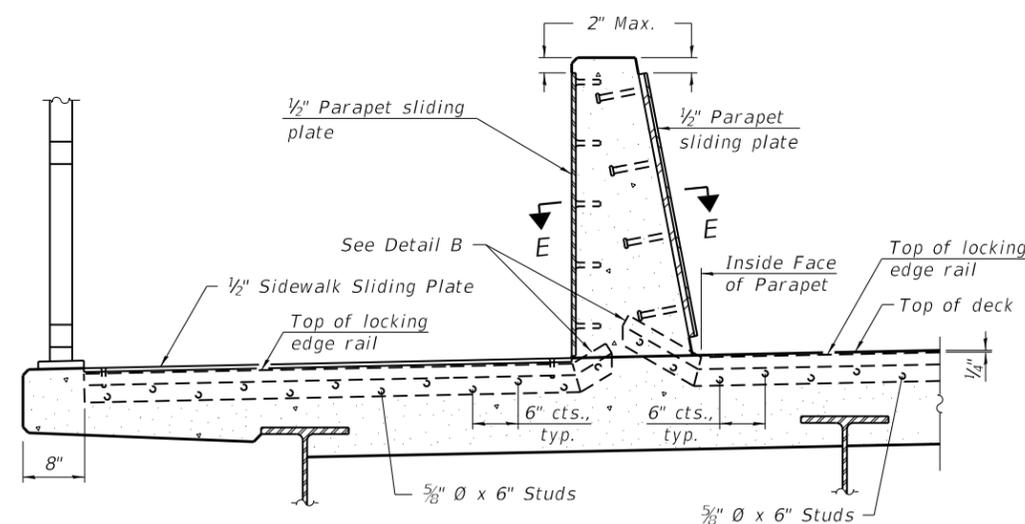
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL - SIDEWALK**  
**STRUCTURE NO. 056-0058**

SHEET NO. 8 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	20
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				NHPP-38RN(839)

LAST SAVED DATE: 5/25/2020 10:58:12 AM



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EJ-SS-S 1-1-2020

engineers + planners + land surveyors

USER NAME = dheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1' / in.	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

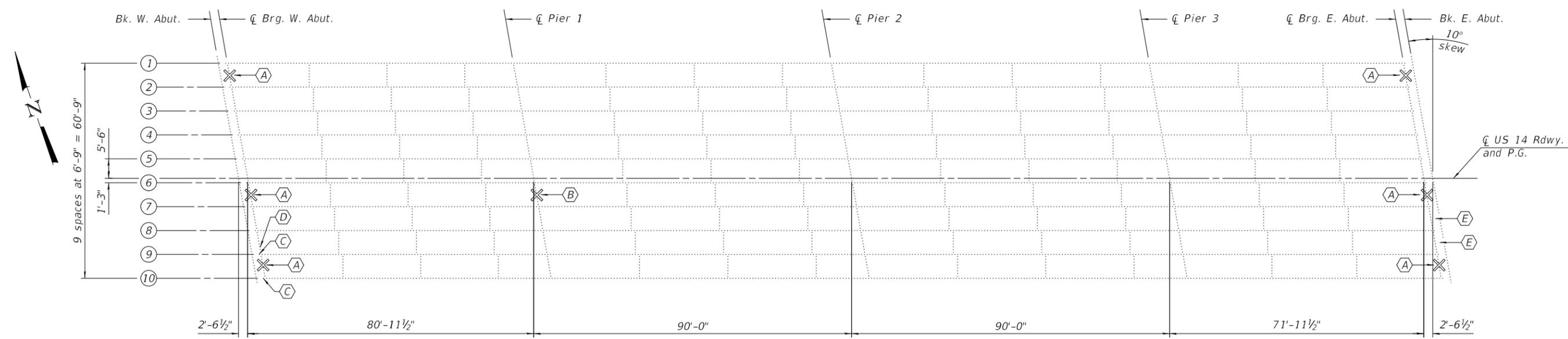
PREFORMED JOINT STRIP SEAL - SIDEWALK  
STRUCTURE NO. 056-0058

SHEET NO. 9 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	21
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NHP-38RN(839)				

(Sheet 3 of 3)

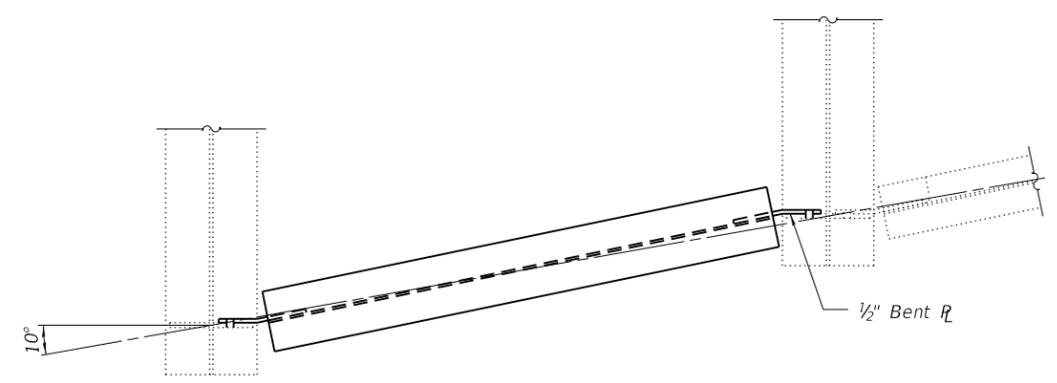
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 Design firm no. 184001036  
 whks engineers + planners + land surveyors



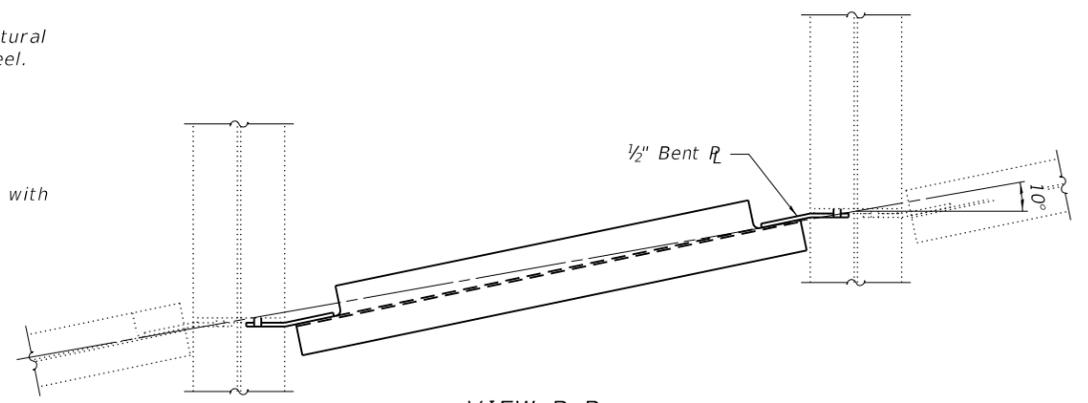
**FRAMING PLAN**

**LEGEND**

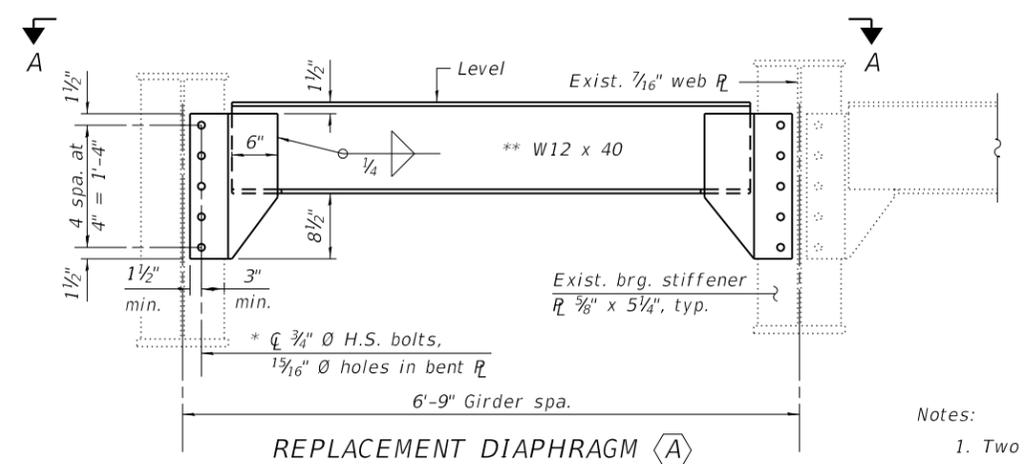
- ✕ Remove and replace diaphragms. Cost included with Structural Steel Removal and Furnishing and Erecting Structural Steel.
- (A) Replace end diaphragm. See details on this sheet.
- (B) Replace interior diaphragm. See details on this sheet.
- (C) Repair beam ends. See details on sheet 11. Cost included with Structural Steel Repair.
- (D) Temporarily disconnect diaphragm to facilitate Girder 9 beam end repairs. Reinstall existing diaphragm with new HS bolts after beam end repairs are complete. A new fill plate is required for diaphragm connection at Girder 8. Cost of new steel included with Structural Steel Repair.
- (E) Temporarily disconnect diaphragm to facilitate bearing repairs. Reinstall existing diaphragm with new HS bolts immediately after bearing repair beneath the diaphragm is complete. The work shall be sequenced so that not more than two adjacent diaphragms are removed at anytime. Cost of new steel included with Structural Steel Repair.



**VIEW A-A**



**VIEW B-B**



**REPLACEMENT DIAPHRAGM (A)**

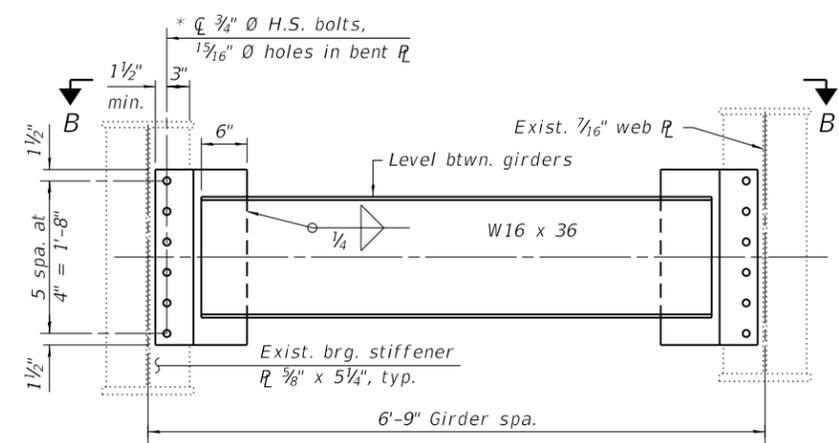
(6 required)  
 (See sheet 11 for modified diaphragm connection detail affected by Repair (C).)

**BILL OF MATERIAL**

Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	2060
Structural Steel Removal	Pound	2060
Structural Steel Repair	Pound	620

- \* Field drill holes in proposed diaphragm using holes in existing diaphragm or bearing stiffeners as a template. Cost included with the associated item of work.
- \*\* Cope bottom flange only.

- Notes:
- Two hardened washers shall be provided over all oversized holes.
  - Removal and reinstallation of existing diaphragms to facilitate adjacent work will not be measured for payment but shall be included in the cost of the associated work.



**REPLACEMENT DIAPHRAGM (B)**

(1 required)

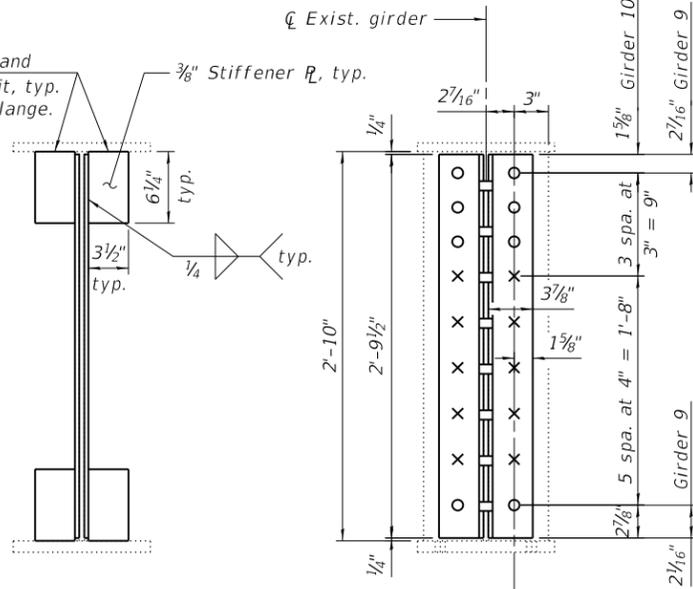
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**FRAMING PLAN  
 STRUCTURE NO. 056-0058**

SHEET NO. 10 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	22
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J35	
			NHPP-38RN(839)	

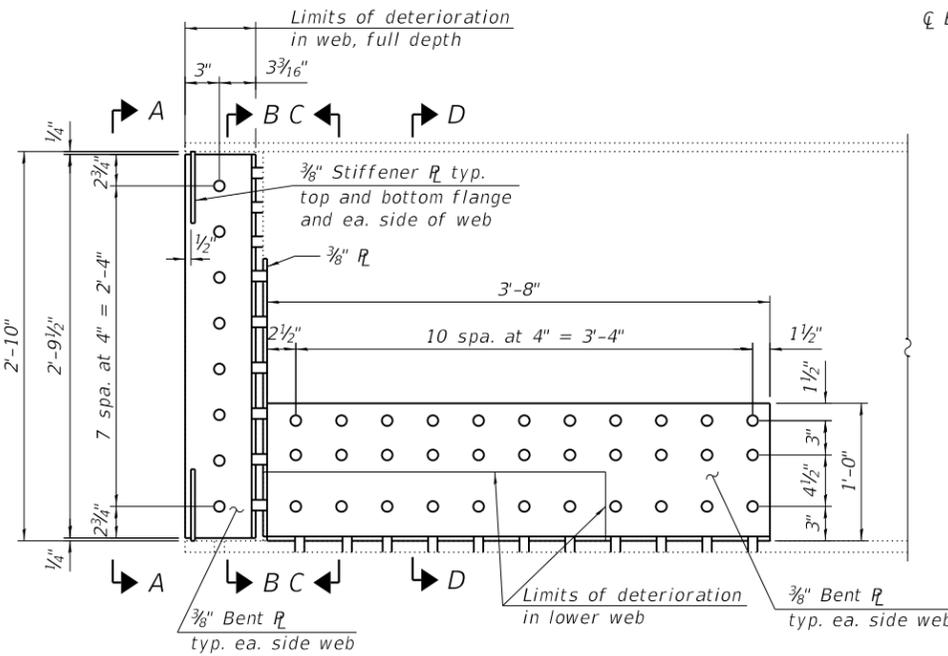
Order 1/4" longer and grind for tight fit, typ. top and bottom flange.



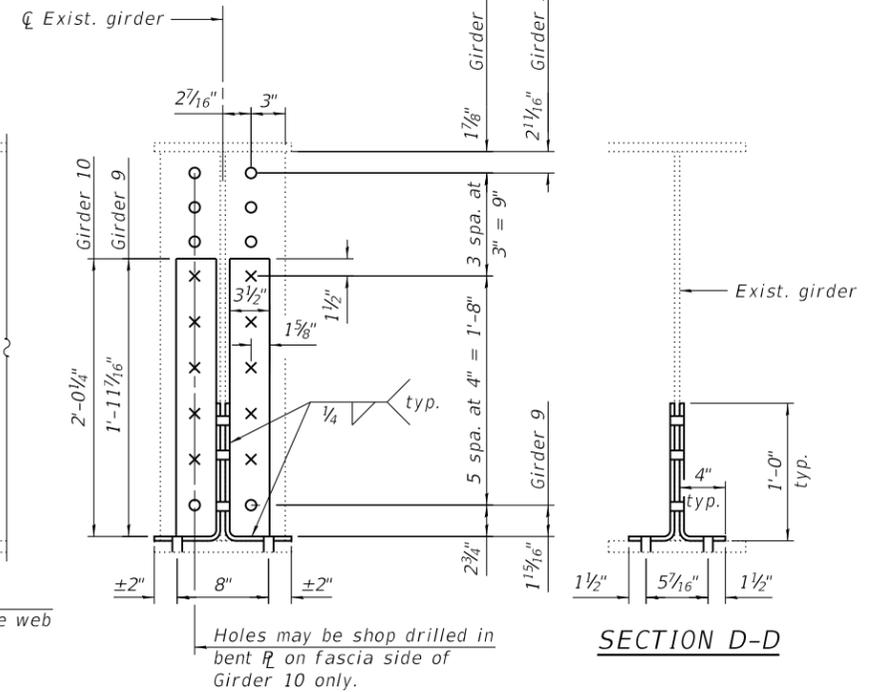
SECTION A-A

Holes may be shop drilled in bent R on fascia side of Girder 10 only.

SECTION B-B



END ELEVATION

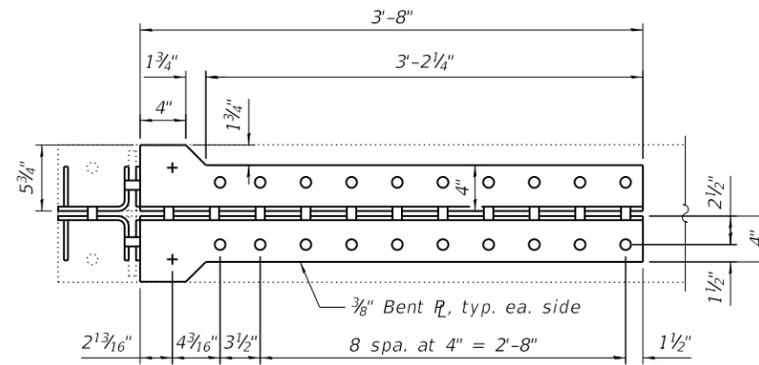


SECTION C-C

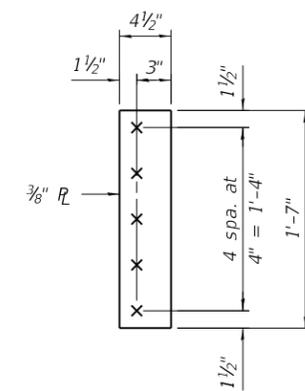
SECTION D-D

LEGEND

- Existing H.S. bolt or threaded stud to remain.
- + Existing 3/4" Ø threaded stud thru new field drilled 1 3/16" Ø holes in repair plate. Remove and reuse existing nut.
- x New 3/4" Ø H.S. bolt thru new field drilled 1 3/16" Ø holes in repair and fill plates. Use existing holes in existing bearing stiffener plates as a template.
- New 3/4" Ø H.S. bolt thru new shop drilled 1 3/16" Ø holes in repair plates and new field drilled 1 3/16" Ø holes in existing girder. Use holes in repair plates as a template.



BOTTOM PLAN



FILL PLATE

(1 Required, Girder 8)

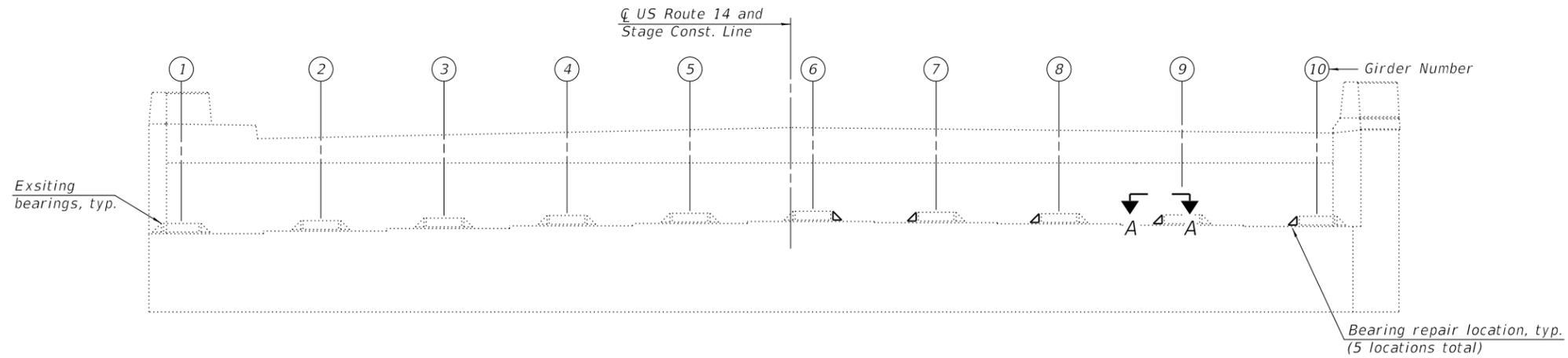
BEAM END REPAIRS (2 Locations)

Notes:

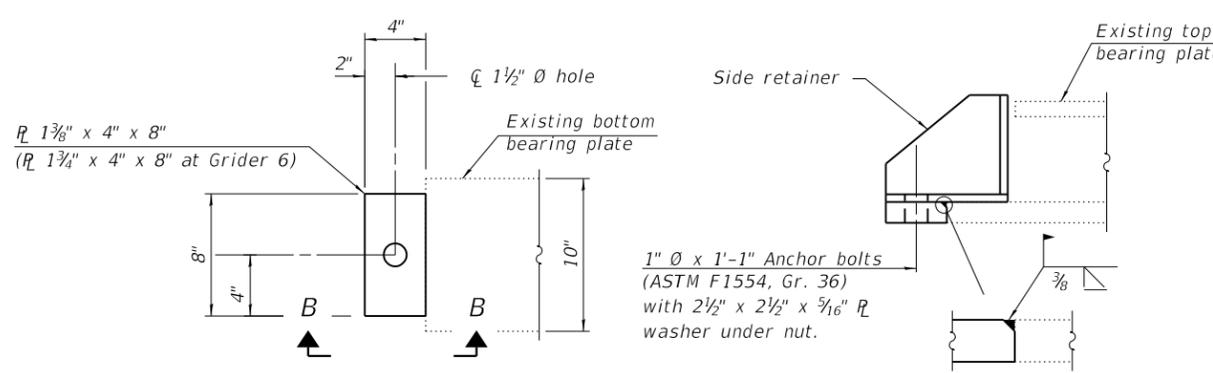
1. Existing and proposed diaphragms and their connection plates not shown for clarity.
2. The existing bolt hole pattern and dimensions are from existing plans. The Contractor shall field verify existing dimensions and make any necessary approved adjustments before ordering material.
3. The Contractor shall field verify the required bolt length and length of threads necessary to install all bolts in accordance with the Standard Specifications.
4. See sheet 10 for location of repairs, additional notes and details, and Bill of Material.

USER NAME = dheberling	DESIGNED - CEH	REVISED
FILE NAME = 056058-62J35.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1'-0"	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	23
CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT   NHP-38RN(839)				

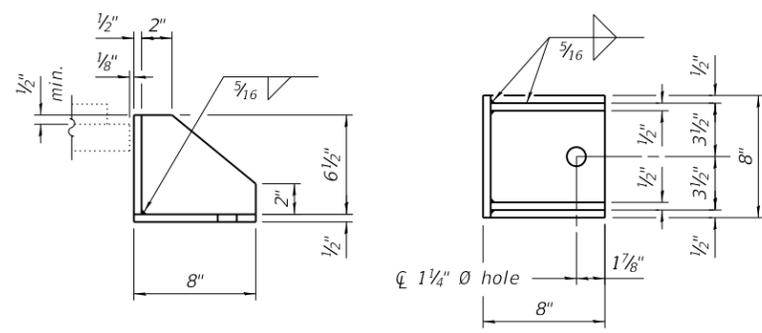


**EAST ABUTMENT**  
(Looking Southeast)  
(Showing bearing repair locations.)



**VIEW A-A**  
(Showing bottom bearing plate.)

**VIEW B-B**



**SIDE RETAINER**

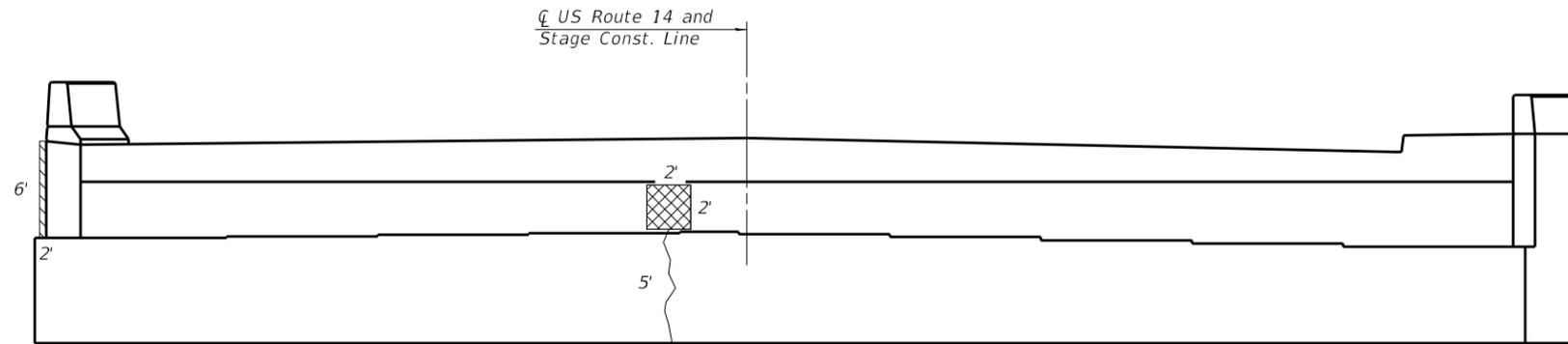
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**Notes:**

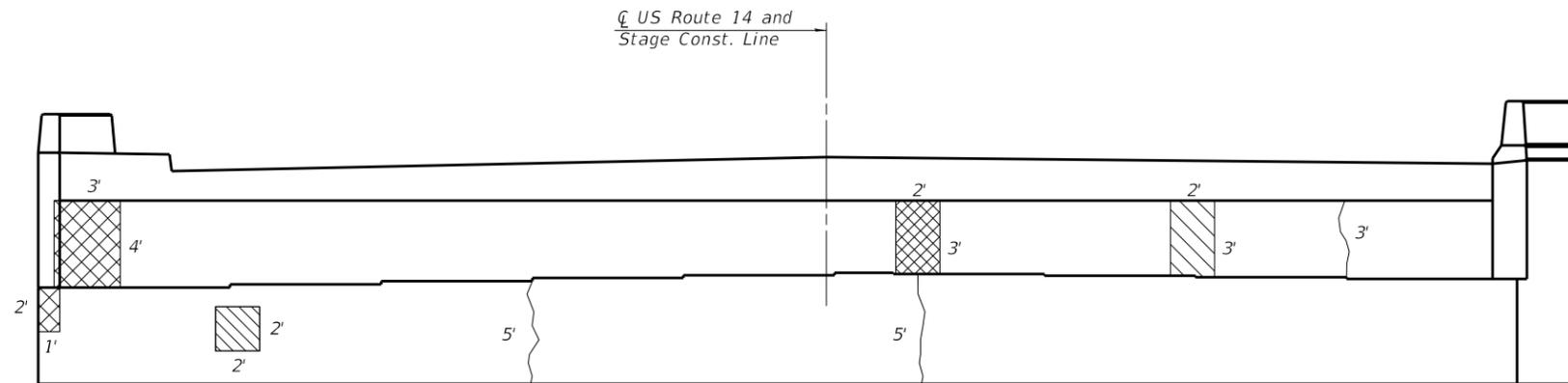
- Proposed plates, side retainers, plate washers, and all work associated with field welding is included with the cost for Structural Steel Repair.
- After field welding is complete, the weld shall be ground smooth and the welded area painted with Aluminum Epoxy Mastic. Cost included with Structural Steel Repair.
- Anchor bolts shall be furnished and installed in accordance with Article 521.06 of the Standard Specifications.
- Efforts shall be made to locate proposed anchor bolt to miss existing reinforcement. The proposed plate may be moved 1 inch in either direction along the end of the existing bearing plate if needed to adjust the proposed anchor bolt and miss existing reinforcement.
- See existing bearing plans for bearing details and dimensions not shown.

**BILL OF MATERIAL**

Item	Unit	Total
Structural Steel Repair	Pound	210
Anchor Bolts, 1" Ø	Each	5



**WEST ABUTMENT REPAIR**  
(Looking Northwest)



**EAST ABUTMENT REPAIR**  
(Looking Southeast)

**LEGEND**

-  Structural repair of concrete ≤ 5"
-  Structural repair of concrete > 5"
-  Epoxy crack injection

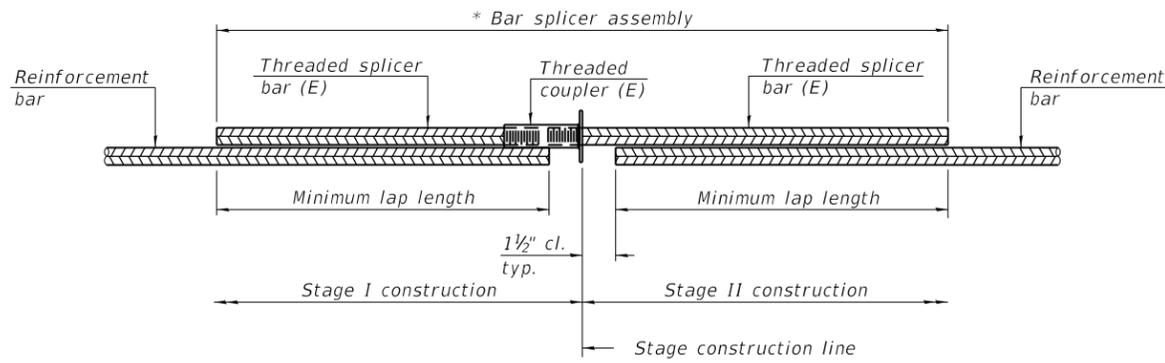
**BILL OF MATERIAL**

Item	Unit	Total
Epoxy Crack Injection	Foot	18
Structural Repair of Concrete (Depth Equal to or Less Than 5 in.)	Sq. Ft.	22
Structural Repair of Concrete (Depth Greater Than 5 in.)	Sq. Ft.	24

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FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
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PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	25
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J35	
			NHPP-38RN(839)	

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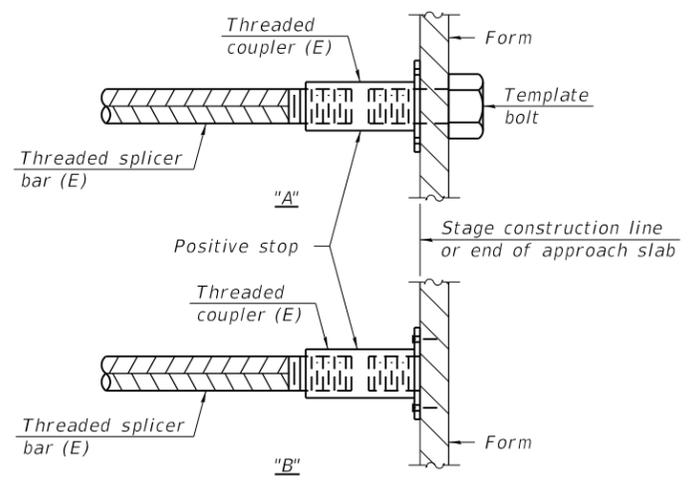


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

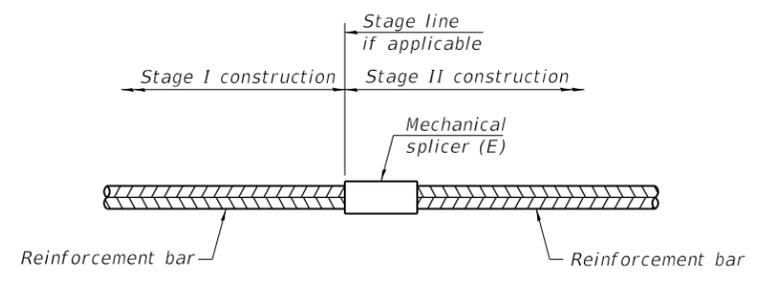
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	5	8	3'-3"
Abutments	6	8	3'-10"



**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

**Notes:**

1. Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
2. All reinforcement shall be lapped and tied to the splicer bars.
3. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
4. See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME: L:\Jobs\DOT\_D-1\8793\118-010\118-010\118-010\118-010\118-010\CAD\_Sheets\0560058-62J35.dgn

BSD-1

1-1-2020



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FILE NAME = 0560058-62J35.dgn	CHECKED - SBC	REVISED
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PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

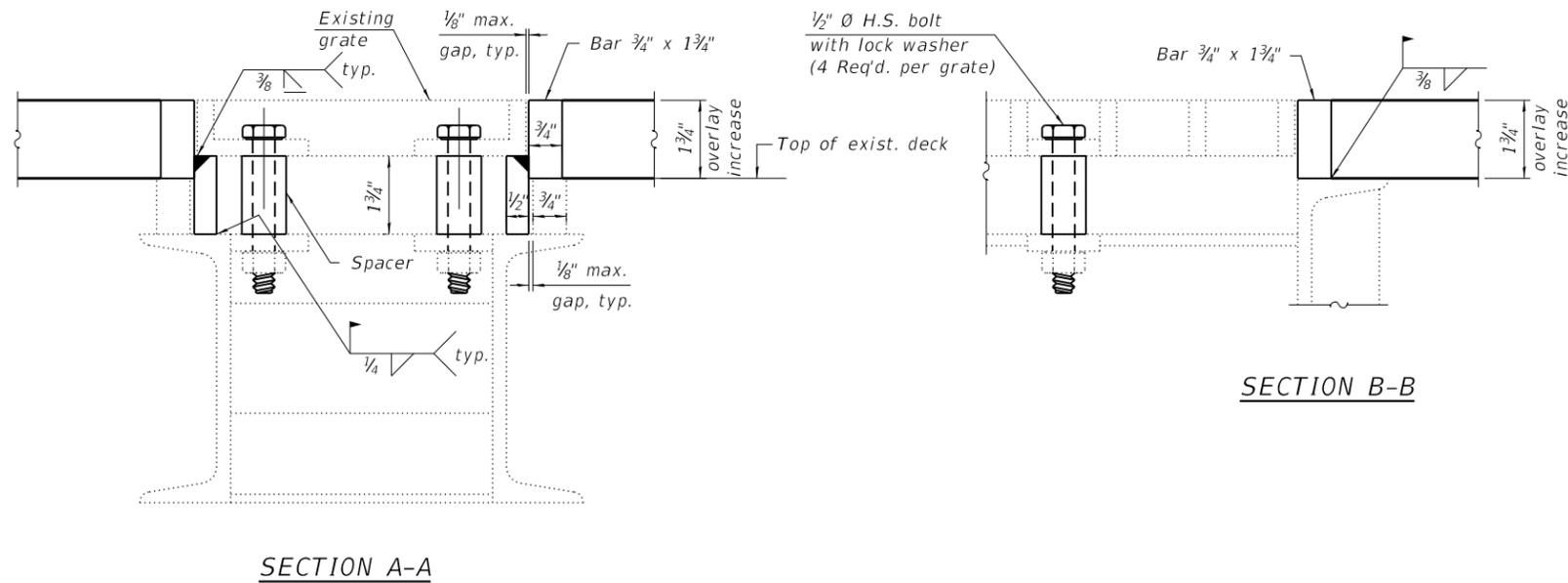
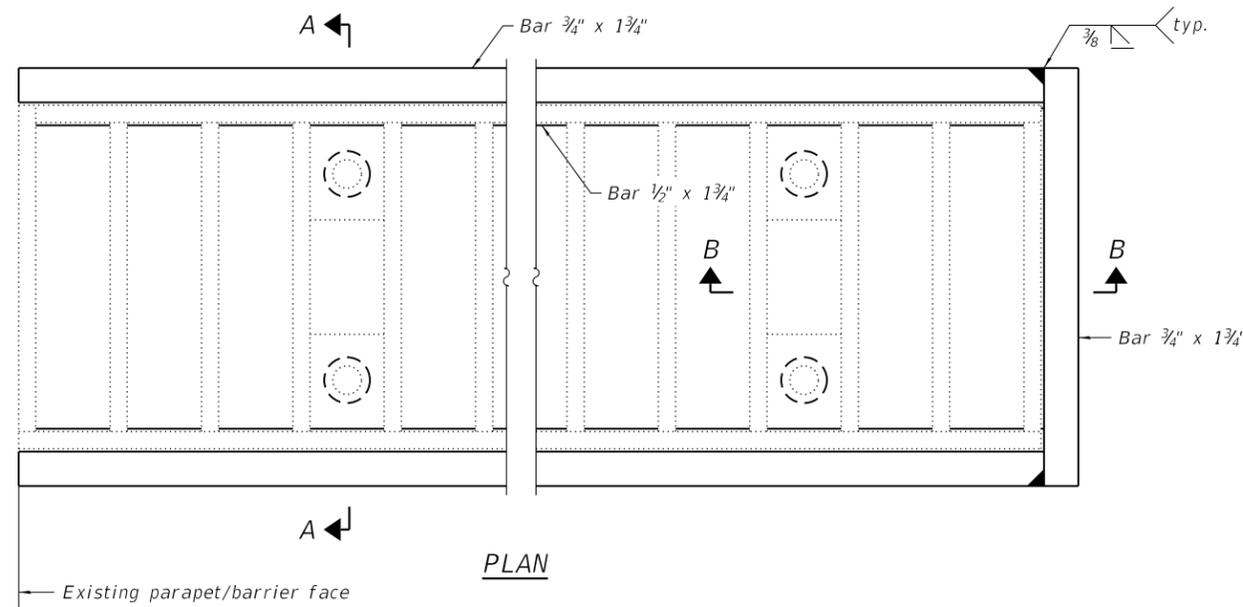
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 056-0058**

SHEET NO. 14 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	26
				<b>CONTRACT NO. 62J35</b>

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NPPP-38RN(839)



Notes:

- Existing drainage scupper grates shall be adjusted so that the top of the existing grate is flush with the top of the bridge deck overlay.
- Proposed structural steel for adjusting the scupper grates shall conform to AASHTO Classification M-270 Gr. 36. All proposed components adjusting the scupper grates shall be hot dipped galvanized.
- Bolts shall be 1/2" Ø ASTM F3125 Grade A325 Type 1, mechanically galvanized. Spacer may be fabricated from round steel pipe.
- The Contractor shall ensure that no damage is done to the existing grates to be reused.
- Shop plans for the proposed scupper adjustment ring shall be submitted to the Engineer for approval prior to fabrication.
- See existing scupper plans for details and dimensions not shown. The Contractor shall field verify the type of scupper present and the scupper dimensions.
- Galvanizing for field welded areas shall be repaired per ASTM A 780.
- Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, furnish and install scupper adjustment ring, and reinstalling the grates is included in the cost per unit each for Drainage Scupper to be Adjusted

BILL OF MATERIAL

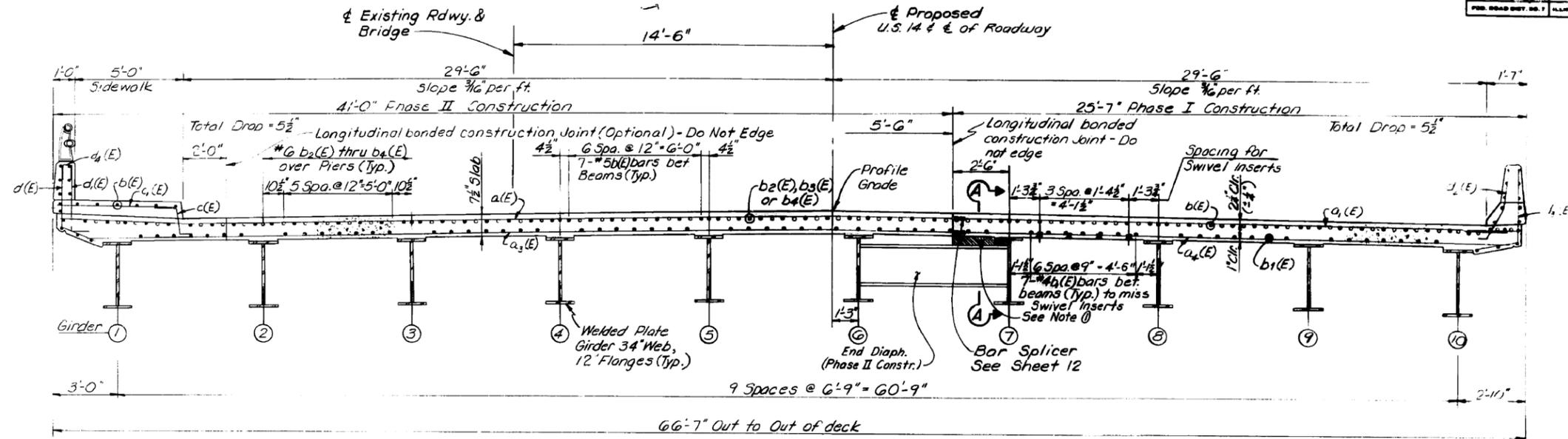
Item	Unit	Total
Drainage Scupper to be Adjusted	Each	4



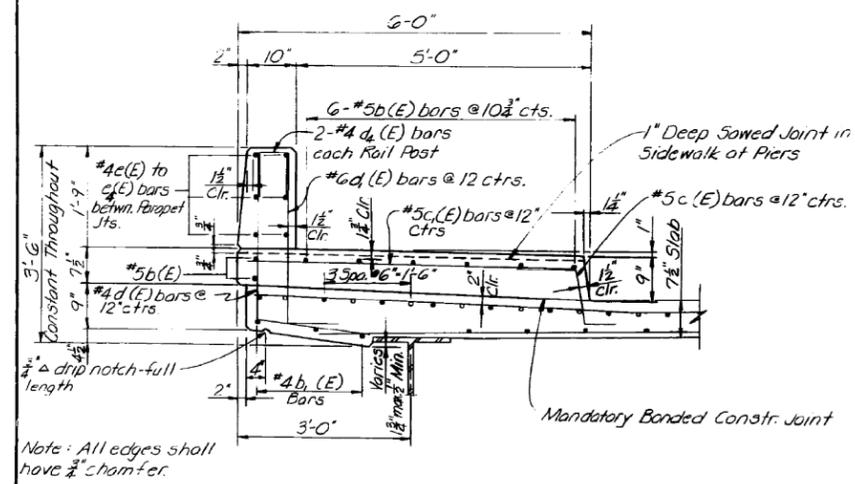
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
258-R	219	Mc HENRY	83	50
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

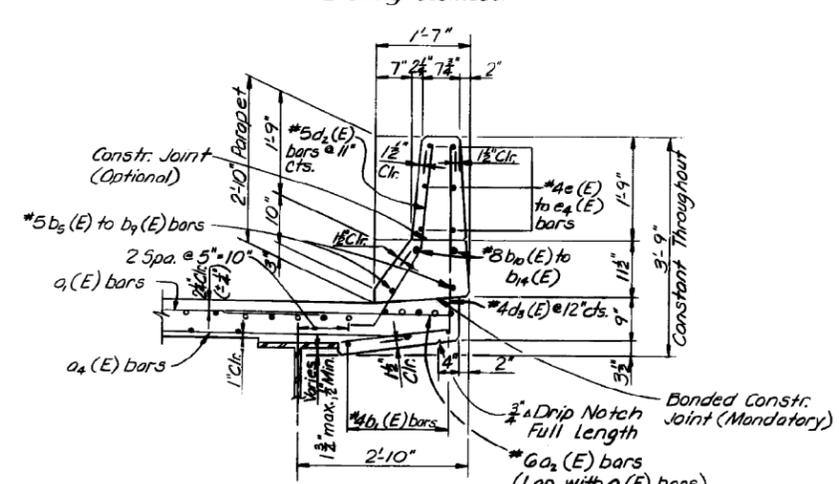
SHEET NO. 9  
SHEETS 32



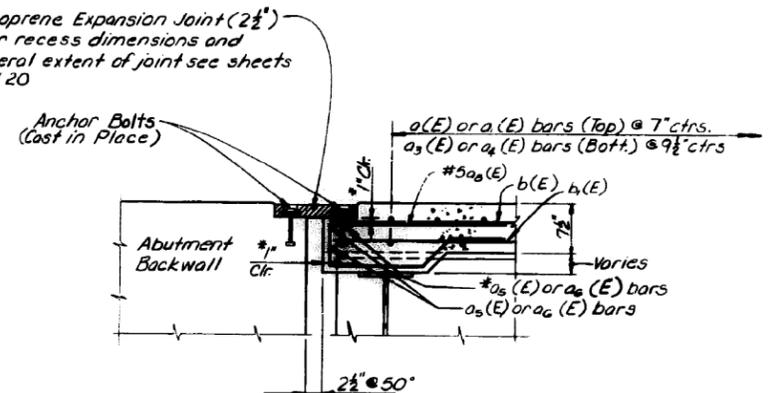
SECTION THRU ROADWAY  
Looking Southeast



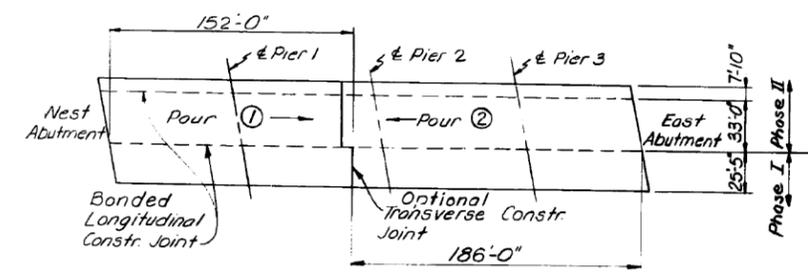
SECTION THRU PARAPET, SIDEWALK & CURB



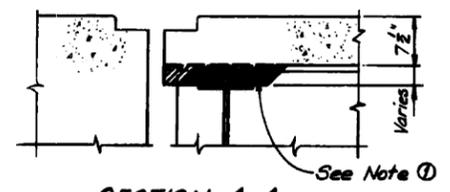
SECTION THRU PARAPET



SECTION AT ROADWAY JOINT  
Taken at right angle to joint



POUR DIAGRAM



SECTION A-A  
Taken at right angle to joint

NOTE ① Concrete slab drop at end diaphragm for Phase I construction between Beams 6 & 7 shall be poured as part of Phase II slab and slab drop. Any voids are to be epoxy grouted. If necessary 2-1" holes may be drilled in slab over diaphragm to facilitate grouting work.

\* Place #5a3(E) bars in back of anchor bolt as shown to maintain 1" Clr (±0-5"). Anchor bolts should be tied to #4(E) bars.

DECK DETAILS  
FAP ROUTE 9(U.S. 14) SECTION 25B-R(B4)  
Mc HENRY COUNTY  
STA. 219+10

HAZLET & ERDAL  
CONSULTING ENGINEERS  
FILE NO.  
DRAWN BY: ABR  
CHECKED BY: R. LIN  
APPROVED BY: KGG



USER NAME = dheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62135.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1'	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (FOR INFORMATION ONLY)  
STRUCTURE NO. 056-0058  
SHEET NO. 17 OF 27 SHEETS

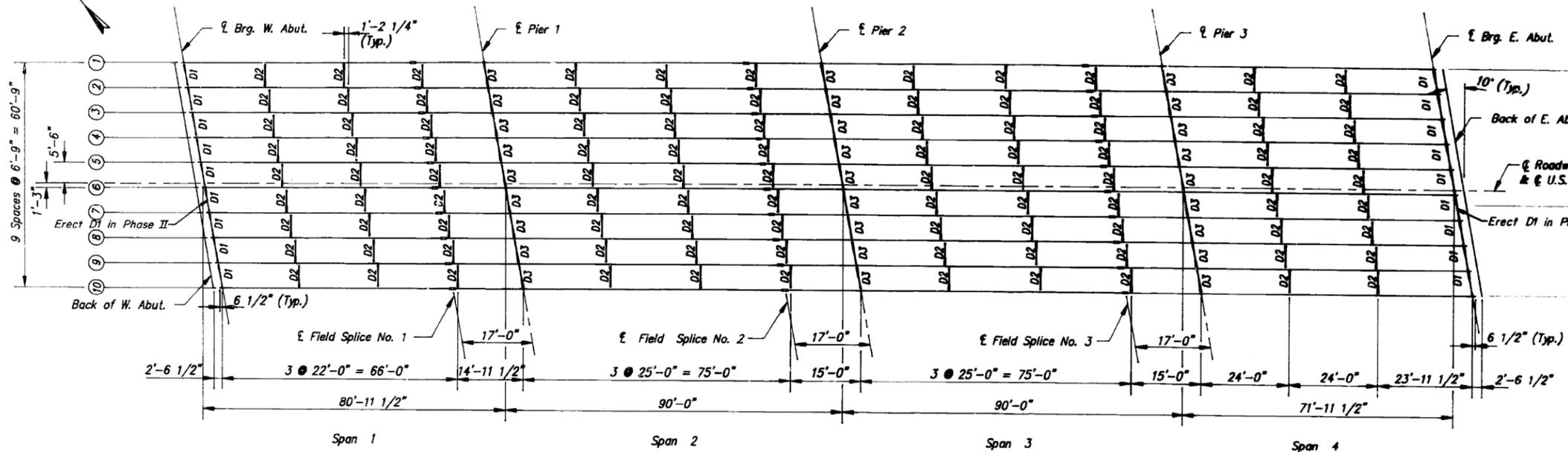
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62J35				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NHPD-38RN(839)				



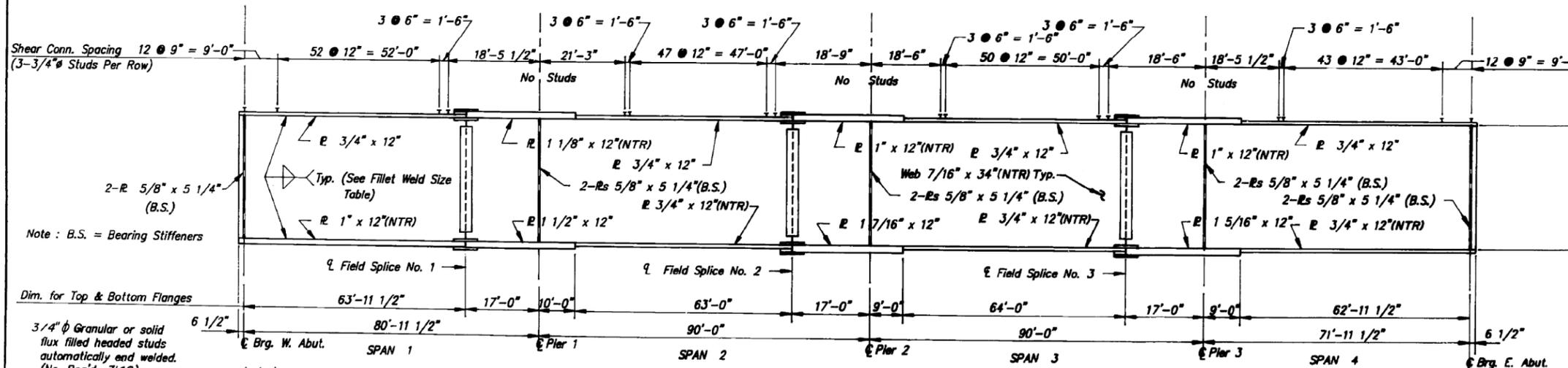
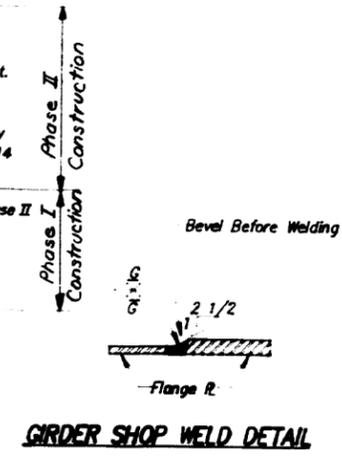
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
158- R.A.9	2(24)	McHENRY	83	55
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

SHEET NO 14  
SHEETS 32



FRAMING PLAN



SECTION AT BRG. STIFFENER

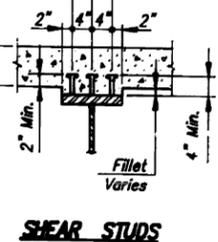
GIRDER ELEVATION

Thickness of Thicker Part Joined	Minimum Size of Fillet Weld
3/4" or less	3/16"
Over 3/4" to 1 1/2"	5/16"
Over 1 1/2" to 2 1/4"	3/8"

\*Weld size need not exceed the thickness of the thinner part joined.

STEEL FRAMING DETAILS  
F A P ROUTE 9 (US 14) SECTION 25B-R (84)  
McHENRY COUNTY  
STA. 219 + 10

HAZLET & ERDAL  
CONSULTING ENGINEERS  
FILE NO.  
DRAWN BY D.W. & D.H.  
CHECKED BY J.F.  
APPROVED BY K.G.

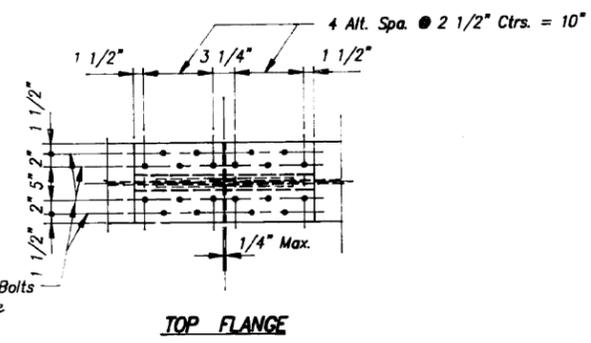


SHEAR STUDS

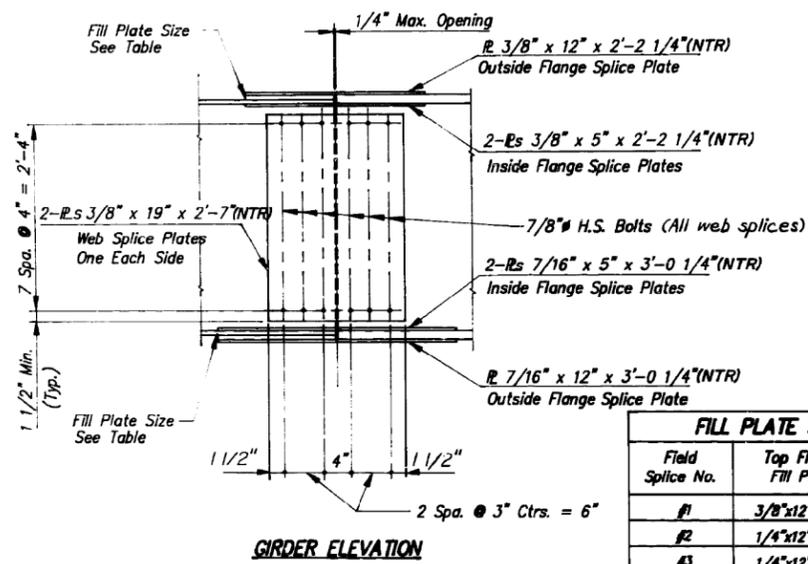
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 15 SHEETS 32
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FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT		

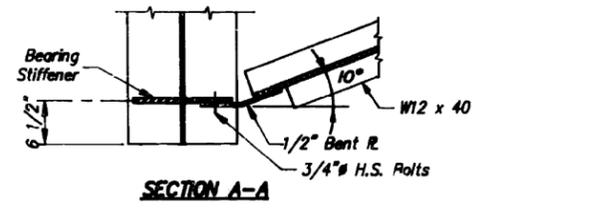


TOP FLANGE

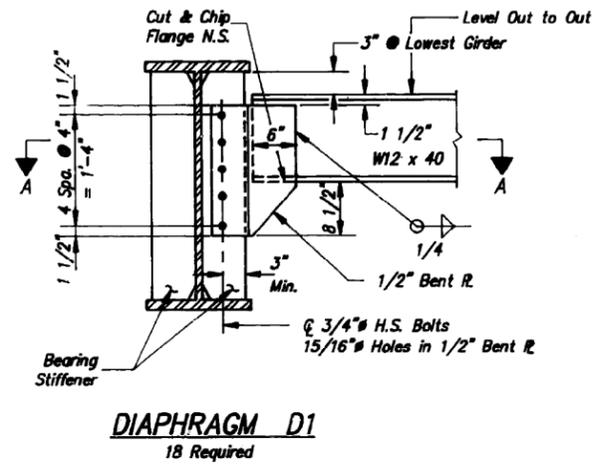


GIRDER ELEVATION

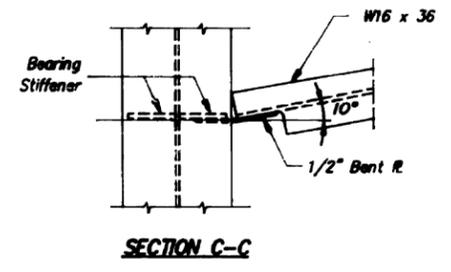
Field Splice No.	Top Flange Fill Plate	Bottom Flange Fill Plate
#1	3/8"x12"x1'-1"	1/2"x12"x1'-6"
#2	1/4"x12"x1'-1"	11/16"x12"x1'-6"
#3	1/4"x12"x1'-1"	9/16"x12"x1'-6"



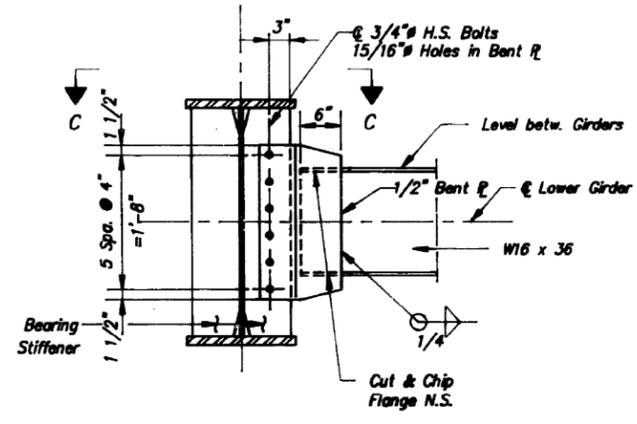
SECTION A-A



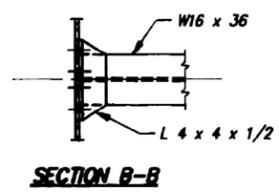
DIAPHRAGM D1  
18 Required



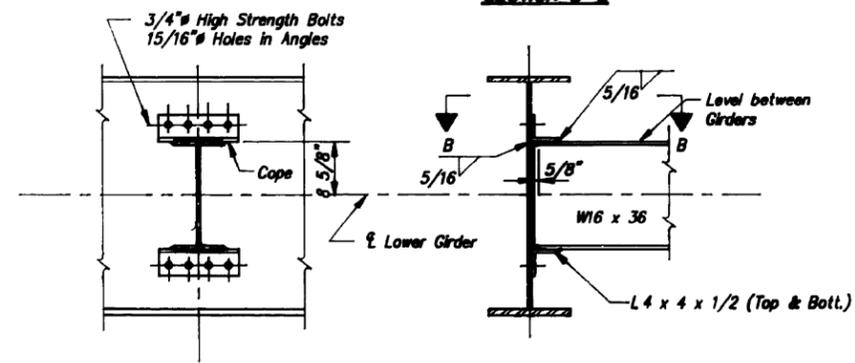
SECTION C-C



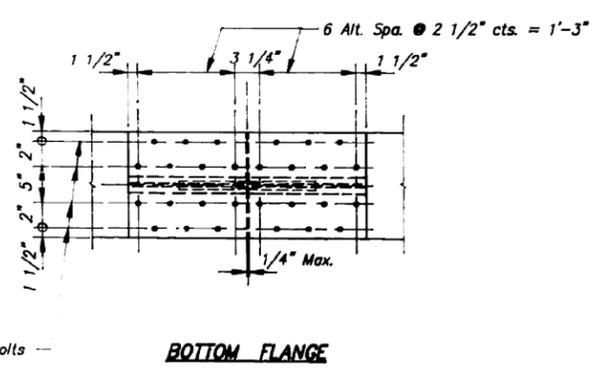
DIAPHRAGM D3  
27 Required



SECTION B-B



DIAPHRAGM D2  
99 Required



BOTTOM FLANGE

FIELD SPLICE DETAILS  
30 Required

GENERAL NOTES:

- The Structural Steel Girders and Splices shall conform to the requirements of AASHTO M223, Grade 50.
- Fasteners shall be high strength bolts, AASHTO M184. Bolts 7/8", open holes 15/16" unless otherwise noted.
- The zinc-silicate and vinyl paint system shall be used for shop and field painting of Structural Steel except where otherwise noted. Field Painting shall be in accordance with Art. 507.10
- Weight of Str. Steel AASHTO M183 = 46,520 Lbs., AASHTO M223, Grade 50 = 432,290 Lbs.
- The main load carrying member components subject to tensile stress shall conform to Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges, webs, and all splice plate material of the steel girders.

Note: Hardened Washers shall be required over all 15/16" holes.

(NTR) = Notch Toughness Requirements

STEEL FRAMING DETAILS  
F A P ROUTE 9 (US 14) SECTION 25B-R (84)  
McHENRY COUNTY  
STA. 219 + 10

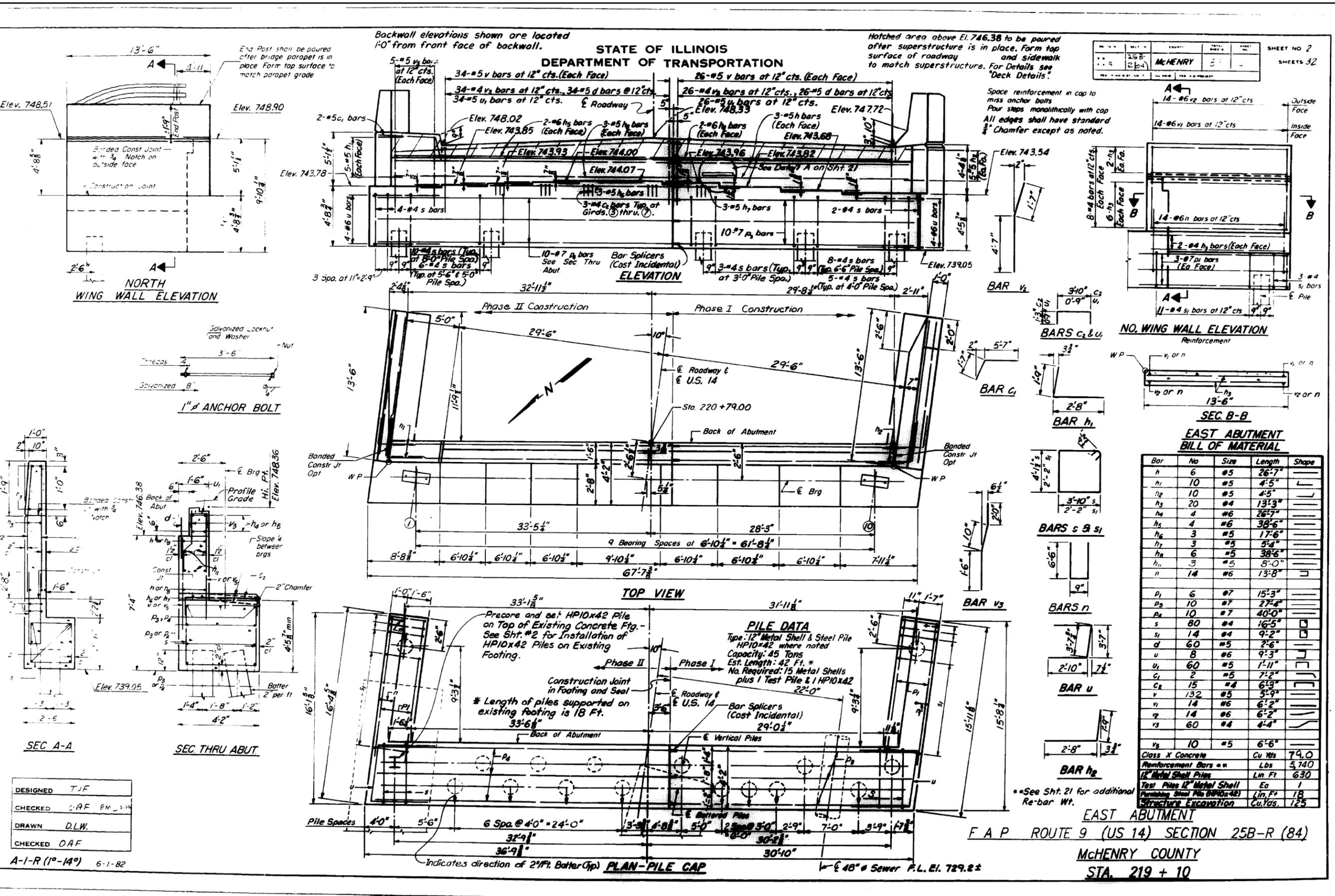
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CONSULTING ENGINEERS  
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APPROVED BY E.B.G.

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 Design Firm: whks engineers + planners + land surveyors  
 USER NAME: dneberling  
 FILE NAME: 056058-62.35.dgn  
 PLOT SCALE: 0.2" = 1'-0"  
 PLOT DATE: 8/11/2020



SHEET NO. 2		SHEETS 32	
NO.	REV.	DATE	BY
1	1	5-15-82	MC HENRY
2	1	8-24-82	

Bar	No	Size	Length	Shape	
h	6	#5	26'-7"		
h <sub>1</sub>	10	#5	4'-5"		
h <sub>2</sub>	10	#5	4'-5"		
h <sub>3</sub>	20	#4	13'-3"		
h <sub>4</sub>	4	#6	26'-7"		
h <sub>5</sub>	4	#6	38'-6"		
h <sub>6</sub>	3	#5	17'-6"		
h <sub>7</sub>	3	#5	5'-4"		
h <sub>8</sub>	6	#5	38'-6"		
h <sub>11</sub>	3	#5	8'-0"		
n	14	#6	13'-8"		
p <sub>1</sub>	6	#7	15'-3"		
p <sub>2</sub>	10	#7	27'-4"		
p <sub>4</sub>	10	#7	40'-0"		
s	80	#4	16'-5"		
s <sub>1</sub>	14	#4	9'-2"		
d	60	#5	2'-6"		
u	8	#6	9'-3"		
u <sub>1</sub>	60	#5	1'-11"		
c <sub>1</sub>	2	#5	7'-2"		
c <sub>2</sub>	15	#4	6'-3"		
v	132	#5	5'-9"		
v <sub>1</sub>	14	#6	6'-2"		
v <sub>2</sub>	14	#6	6'-2"		
v <sub>3</sub>	60	#4	4'-4"		
v <sub>5</sub>	10	#5	6'-6"		
Class X Concrete				Cu Ws	74.0
Reinforcement Bars				Lbs	5,740
12" Metal Shell Piles				Lin Ft	630
Test Piles 12" Metal Shell				Eq	1
Forming Steel Piles (HP10x42)				Lin Ft	18
Structure Excavation				Cu.Yds	125

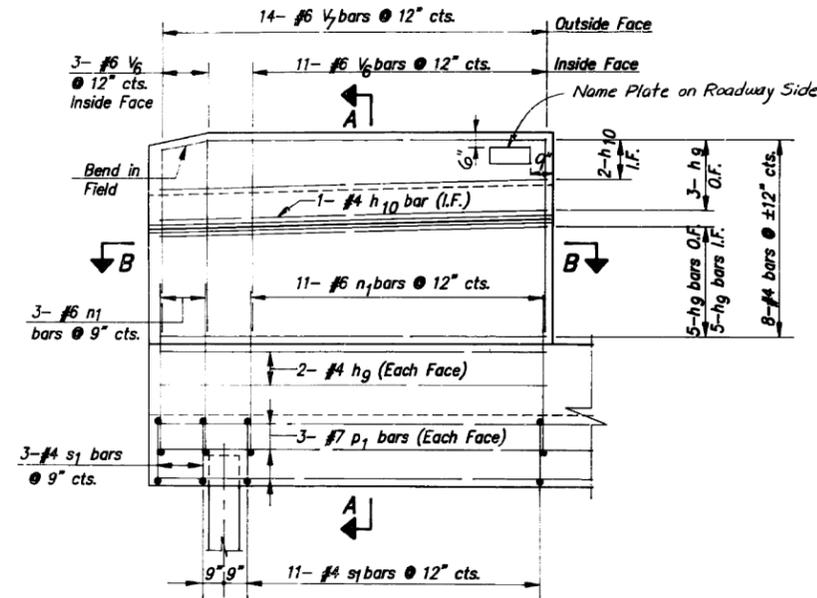
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CHECKED	CAF PM
DRAWN	DLW
CHECKED	DAF

A-1-R (1°-14°) 6-1-82

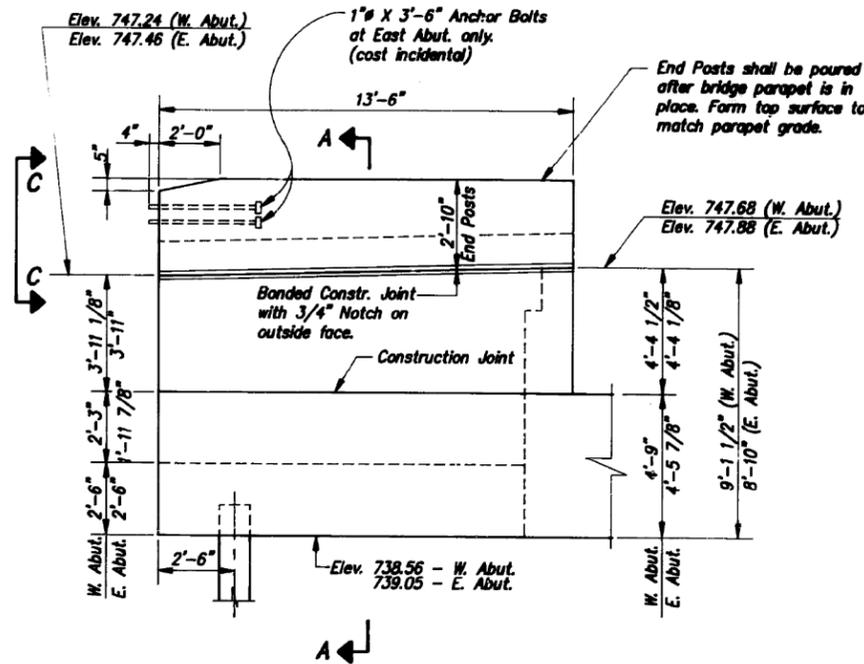
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 9	258-R(84)	McHENRY	83	62

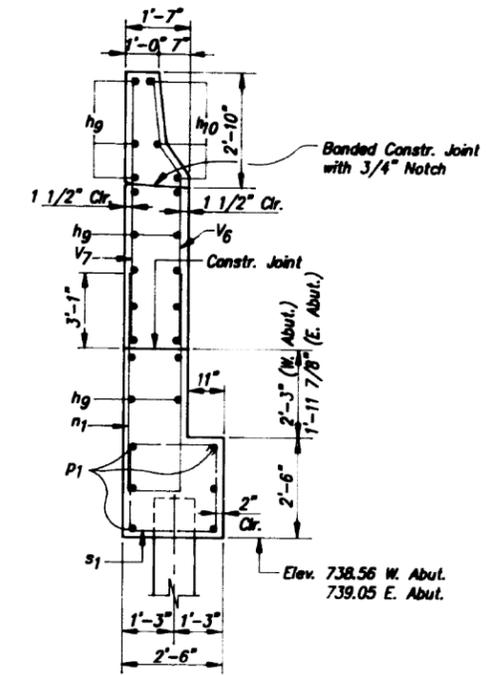
SHEET NO. 21  
SHEETS 32



**SOUTH WINGWALL ELEVATION**  
Reinforcement



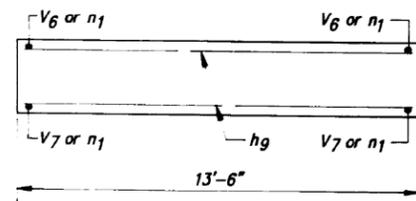
**SOUTH WINGWALL ELEVATION**  
Drawn For West Abutment  
East Abutment - Opposite Hand



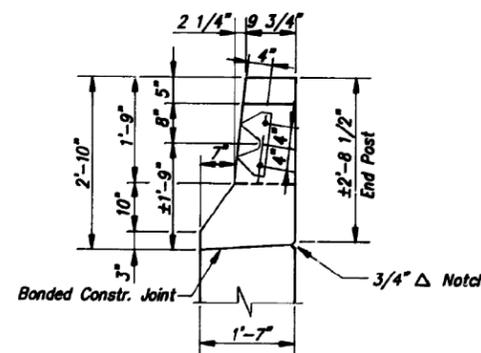
**SECTION A-A**

**BILL OF MATERIAL**

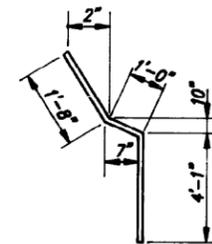
Bar	Number		Size	Length	Shape
	W. Abut.	E. Abut.			
h <sub>g</sub>	17	17	#4	13'-3"	—
h <sub>10</sub>	3	3	#4	13'-3"	—
p <sub>1</sub>	6	6	#7	15'-3"	—
n <sub>1</sub>	14	14	#6	14'-8"	U
s <sub>1</sub>	14	14	#4	9'-2"	□
v <sub>6</sub>	14	14	#6	6'-9"	—
v <sub>7</sub>	14	14	#6	6'-7"	—
<b>Reinforcement Bars</b>					
<b>West Abutment</b>					Lbs. 1,040
<b>East Abutment</b>					Lbs. 1,040
<b>Total</b>					Lbs. 2,080



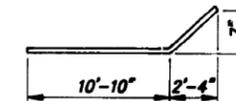
**SECTION B-B**



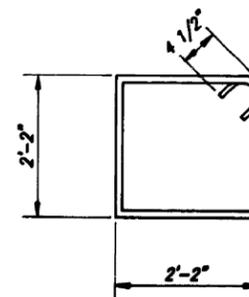
**VIEW C-C**  
(East Abut. Only)



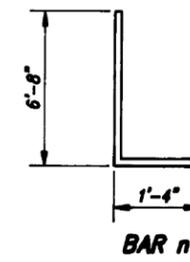
**BAR V<sub>6</sub>**



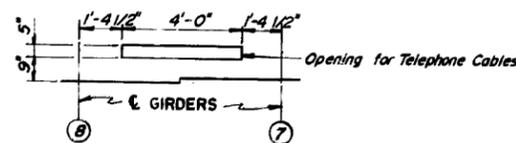
**BAR h<sub>10</sub>**



**BAR s<sub>1</sub>**



**BAR n<sub>1</sub>**



**DETAIL A**

- NOTE:**  
1. Cut V Bars in the field to clear opening by 2" top and bottom.  
2. Add 12- #4 V Bars (6 Each End) and 3 each face spaced between regular V Bars.  
3. Add 3- #5 h<sub>g</sub> Bars (See sec. thru abut. for location) centered over opening.

HAZELET + ERDAL, INC.  
CONSULTING ENGINEERS

FILE NUMBER 551  
DRAWN BY MLS  
CHECKED BY RMD  
APPROVED BY Kely

**WINGWALL DETAILS**  
**F.A.P. ROUTE 9 (US 14) SECTION 258-R (84)**  
**McHENRY COUNTY**  
**STA. 219 + 10**

design firm  
no. 184001036



USER NAME = dtheberling	DESIGNED - CEH	REVISED
FILE NAME = 0560058-62135.dgn	CHECKED - SBC	REVISED
PLOT SCALE = 0.2" = 1'	DRAWN - DLH	REVISED
PLOT DATE = 8/11/2020	CHECKED - CEH	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (FOR INFORMATION ONLY)  
STRUCTURE NO. 056-0058

SHEET NO. 24 OF 27 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	McHENRY	47	36
				CONTRACT NO. 62J35

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NHP-38RN(839)



**STRUCTURAL GENERAL NOTES**

**GENERAL:**

- PERFORM WORK IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2002, SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS ADOPTED JANUARY 1, 2002 AND INTERIM SPECIAL PROVISIONS UPDATED MAY 9, 2002.
- PLACE FILTER FABRIC ON BACK FACE OF RETAINING WALLS OVER DRAIN HOLES (INCLUDED WITH POROUS GRANULAR EMBANKMENT, SPECIAL).
- PROVIDE CONTRACTION JOINTS SPACED AT 5 FEET IN LAND BRIDGE DECK AND PORTLAND CEMENT CONCRETE SIDEWALK, SPECIAL.
- PROVIDE 3/4" PREFORMED JOINT FILLER EXPANSION JOINTS SPACED AT 100 FEET IN PORTLAND CEMENT CONCRETE SIDEWALK, SPECIAL. DISCONTINUE REINFORCEMENT BARS AND WELDED WIRE FABRIC SHEETS AT EXPANSION JOINTS.

**SUPERIMPOSED MINIMUM DESIGN LOADS:**

- LAND BRIDGE DECK: LIVE LOAD 85 P.S.F.

**CONCRETE WORK:**

- REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-31 OR M322, GRADE 60.
- PLACE REINFORCING STEEL TO PROVIDE 2" CLEAR CONCRETE COVER UNLESS OTHERWISE NOTED.
- PROVIDE 1" CHAMFER ON ALL EXPOSED EDGES.
- ALL CONSTRUCTION JOINTS SHALL BE BONDED.

**MICROPILES:**

- INSTALL PULLDOWN MICROPILES PER SPECIFICATIONS AT LOCATIONS AS SHOWN ON SHEETS 31 & 32 OF THE DRAWINGS.
- INSTALL 10/12 SOLID STEEL SHAFT HELICAL PULLDOWN MICROPILE WITH GROUT COLUMN AS SHOWN ON THE DRAWINGS. ALLOWABLE LOAD: 14 KIPS. ESTIMATED PILE LENGTH: 25 FEET. NUMBER REQUIRED: 36.

**REMODELING WORK:**

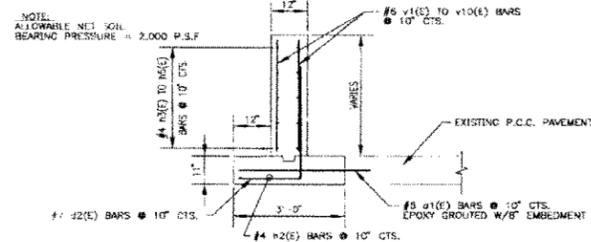
- EXISTING FACILITY DIMENSIONS AND ELEVATIONS SHOWN ON SHEETS 29 THROUGH 33 ARE FOR REFERENCE ONLY. FIELD VERIFY AS BUILT CONDITIONS PRIOR TO CONSTRUCTION.
- PROVIDE TEMPORARY SHORING OF EXISTING STRUCTURE AS REQUIRED DURING CONSTRUCTION.
- REMOVE AND DISPOSE OF EXISTING CONCRETE AND EMBEDDED ITEMS AS REQUIRED FOR PROPOSED WORK. DO NOT CUT OR DAMAGE EXISTING REINFORCING STEEL WITHOUT ENGINEERS APPROVAL.
- SANDBLAST AND CLEAN NEWLY EXPOSED CONCRETE AND REINFORCING BAR SURFACES ON STRUCTURE TO REMAIN.
- GRIND SURFACES TO REMAIN EXPOSED SMOOTH AND FLUSH WITH EXISTING ADJACENT SURFACES.
- PROVIDE 3/4 INCH DEEP SAWCUTS.

PARAPET WALL REINFORCEMENT				
BAR	NO.	SIZE	LENGTH	SHAPE
CE1	48	4	4'-0"	
CE2	68	4	2'-2"	1 F
CE3	20	5	3'-0"	
CE4	20	7	5'-4"	
CE5	48	4	8'-0"	
CE6	8	4	2'-0"	
CE7	8	4	7'-0"	
CE8	18	4	7'-0"	
CE9	6	4	8'-0"	
CE10	4	4	2'-0"	
CE11	1211	4	3'-2"	
CE12	4	8	4'-0"	
CE13	4	8	4'-0"	
CE14	4	8	4'-0"	
CE15	4	8	4'-0"	
CE16	4	8	4'-0"	
CE17	4	8	4'-0"	
CE18	4	8	4'-0"	
CE19	4	8	4'-0"	
CE20	4	8	4'-0"	

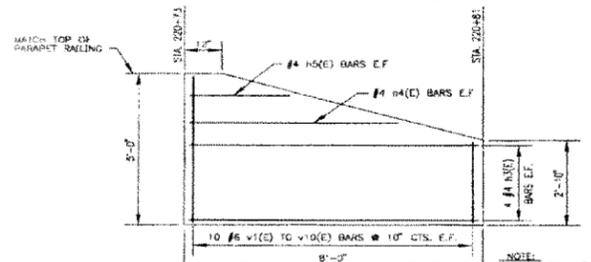
PARAPET WALL BILL OF MATERIALS			
ITEM	UNIT	QUANTITY	
REINFORCEMENT BARS, EPOXY COATED	LB	5080	
CONCRETE STRUCTURE	CU. YD.	41	
PARAPET RAILING	LT.	338	

**NOTES:**

- PROVIDE 1/2" PREFORMED JOINT FILLER EXPANSION JOINTS SPACED TO MATCH THE EXISTING NORTH SIDE PARAPET WALL.
- CUT REINFORCING AS REQUIRED AT DRAIN OPENINGS AND EXPANSION JOINTS.
- ATTACH GUARDRAIL TO PARAPET WALL WITH 5 - 3/4" ANCHOR BOLTS (A 307) WITH STANDARD WASHERS, EPOXY GROUTED WITH 9" EMBEDMENT. AFTER TIGHTENING, CUT THE ANCHOR BOLTS FLUSH WITH NUTS AND DAMAGE THE NUT TO PREVENT IT FROM LOOSENING.
- REGULATORY SPEED LIMIT: 35 MPH



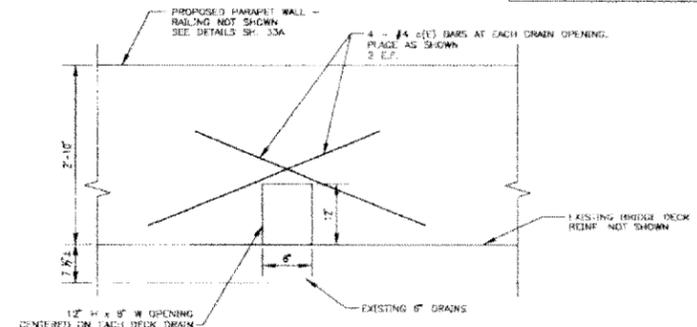
PARAPET WALL END TREATMENT SECTION



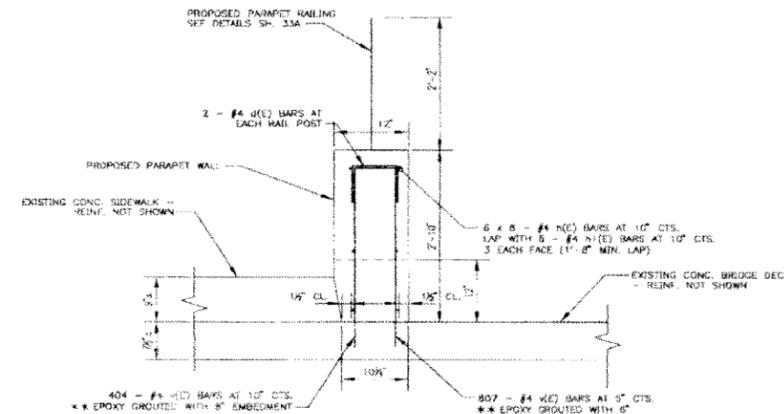
PARAPET WALL END TREATMENT B ELEVATION (LOOKING NORTH)

F.A.P. ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0305	02-02121-00-SW	MCHEMERY	48	33

ALUMINUM PROJECT: CMP-0305(022)  
CONTRACT # 83760

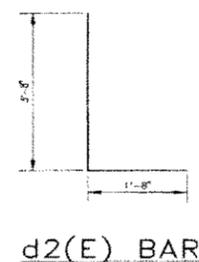


PARAPET WALL DRAIN OPENING

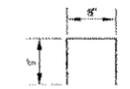


PARAPET WALL SECTION

\*\* IN ACCORDANCE WITH ART. 504 OF STANDARD SPECIFICATIONS



d2(E) BAR



d(E) BAR

**APPROVED**  
FOR STRUCTURAL ADEQUACY ONLY

*Robert E. Woodman*  
REGISTERED PROFESSIONAL ENGINEER  
IN THE STATE OF ILLINOIS



\*LICENSE EXPIRES 11-30-2004\*

CONSULTANTS	REV. NO.	DATE	DESCRIPTION
	1	8-10-04	PER IDOT REVISIONS



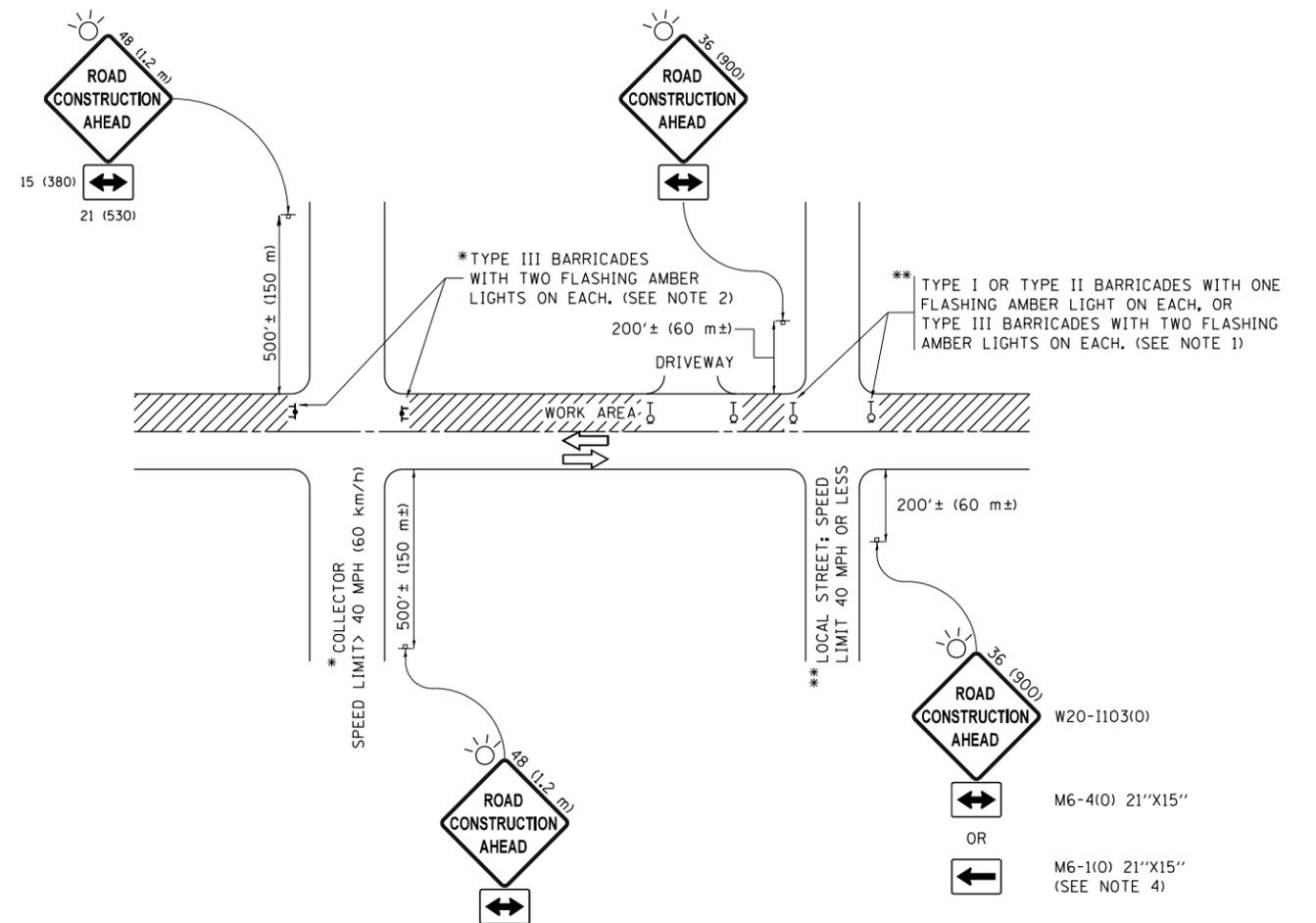
**Baxter & Woodman**  
Crystal Lake, Illinois 815.469.1200  
Burlington, Wisconsin 262.743.7834  
Mokena, Illinois 708.478.2090  
DeKalb, Illinois 815.747.3111

**ALGONQUIN TOWNSHIP**  
U.S. ROUTE 14  
PEDESTRIAN IMPROVEMENTS PROJECT

**STRUCTURAL GENERAL NOTES AND PARAPET WALL DETAILS**

DESIGNED BY	SCALE
AS NOTED	AS NOTED
CHECKED BY	PROJECT NO.
DATE	SHEET NO.
	<b>33 OF 48</b>





**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

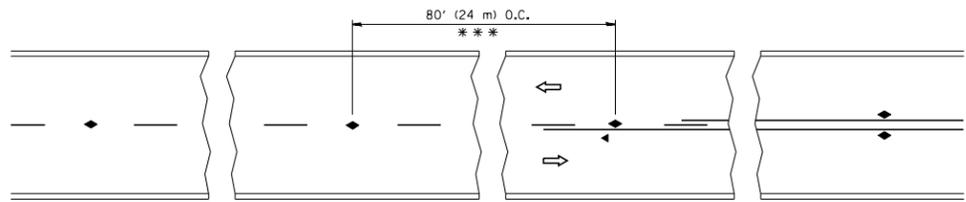
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

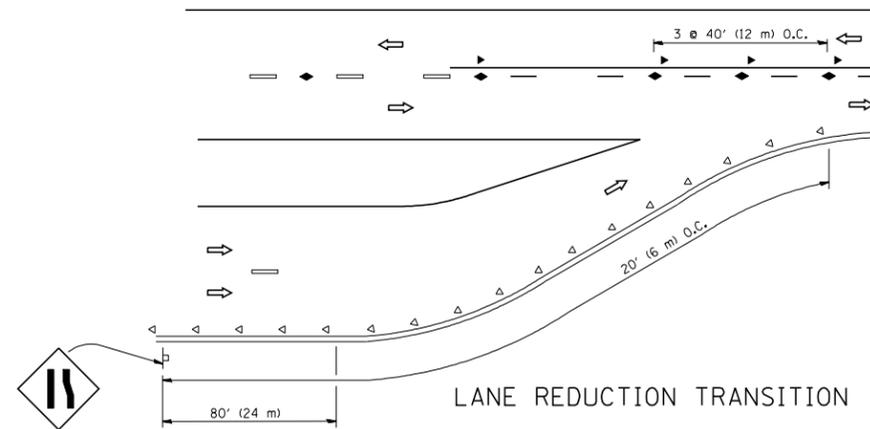
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	20019-058-BR	MCHENRY	47	40
<b>TC-10</b>			<b>CONTRACT NO. 62J35</b>	
ILLINOIS FED. AID PROJECT				

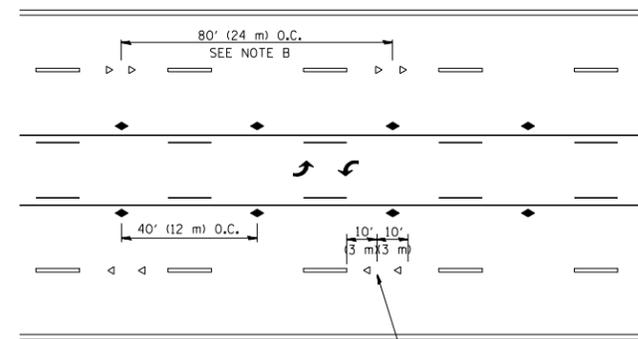


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

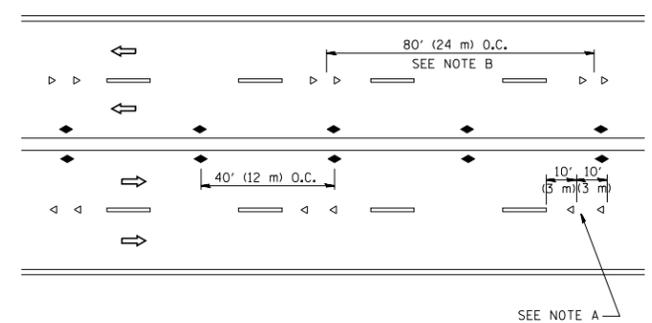
TWO-LANE/TWO-WAY



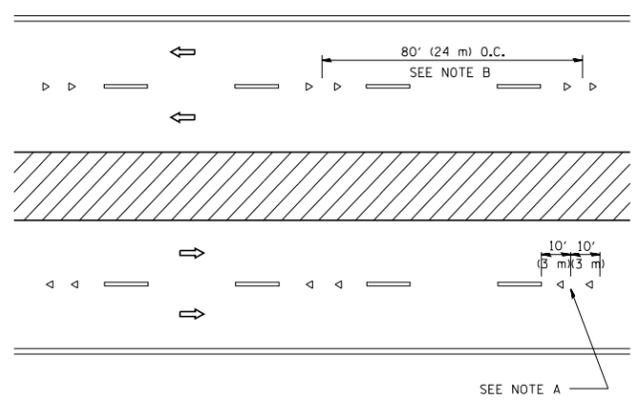
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

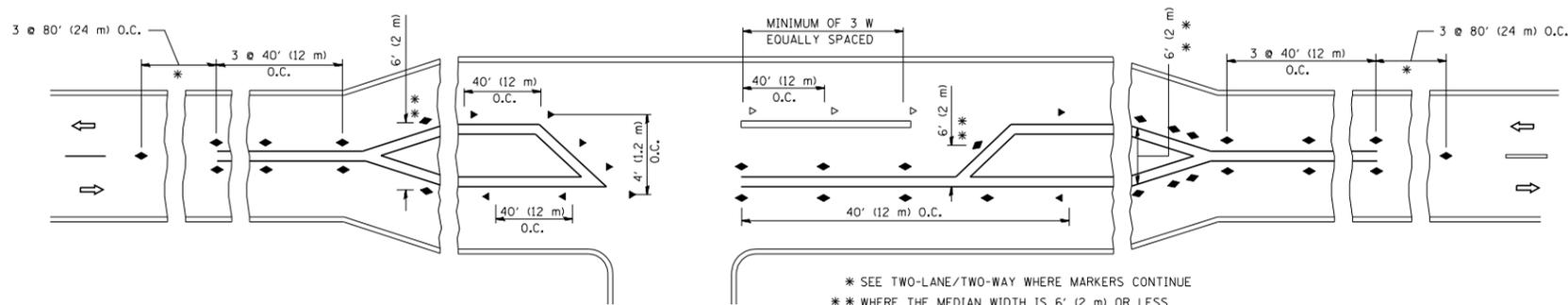
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

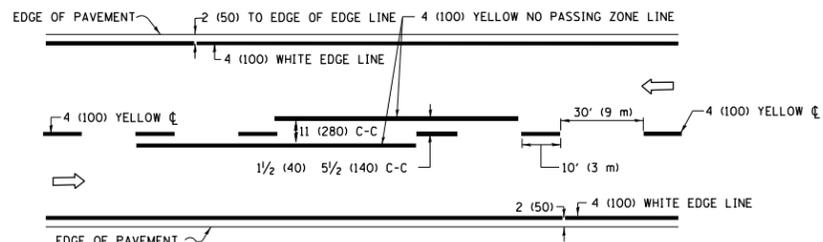
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

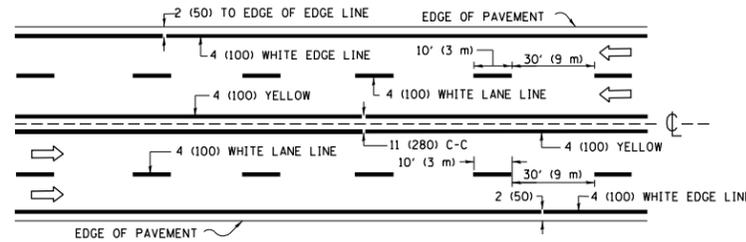
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

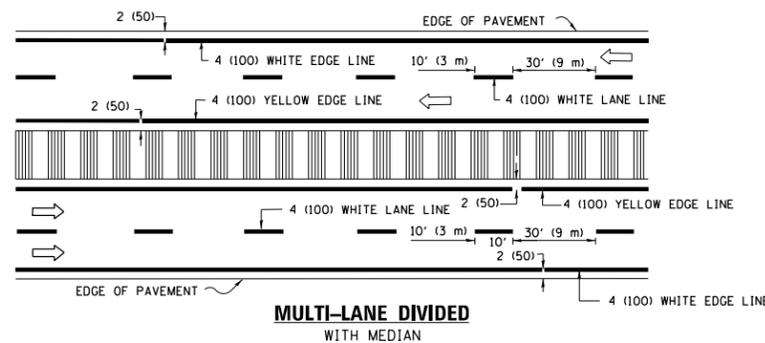
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2010-058-BR	MCHENRY	47	41
TC-11		CONTRACT NO. 62J35		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



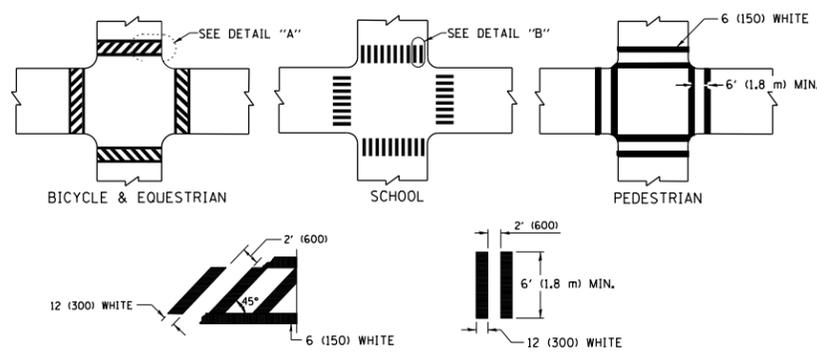
**2-LANE ROADWAY**



**MULTI-LANE UNDIVIDED**



**TYPICAL LANE AND EDGE LINE MARKING**

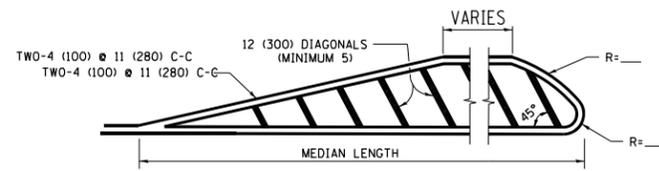
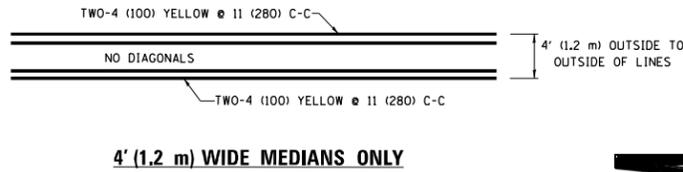


**DETAIL "A"**

**DETAIL "B"**

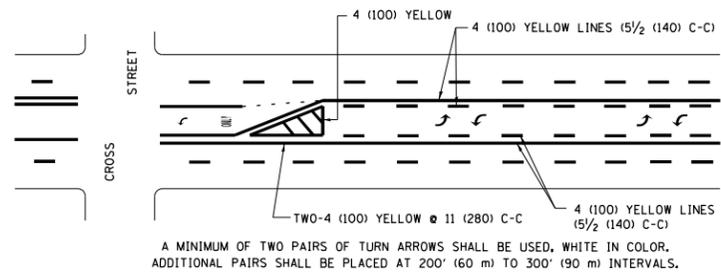
**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



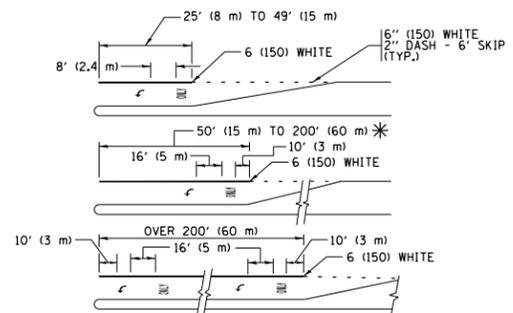
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

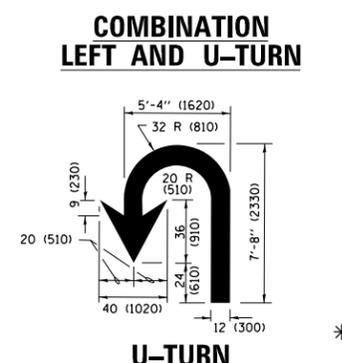
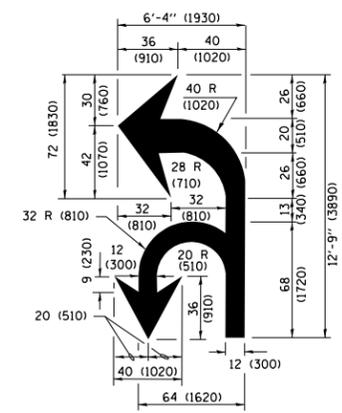
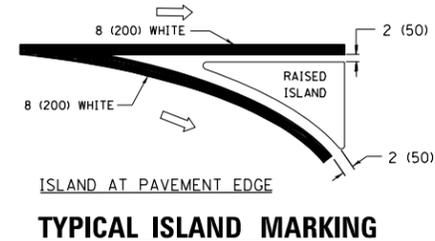
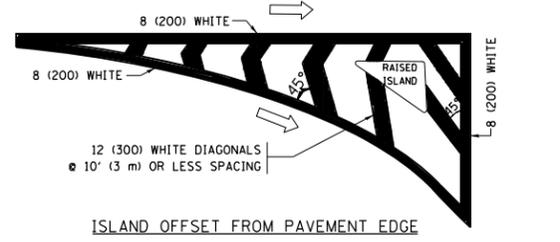


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE, FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

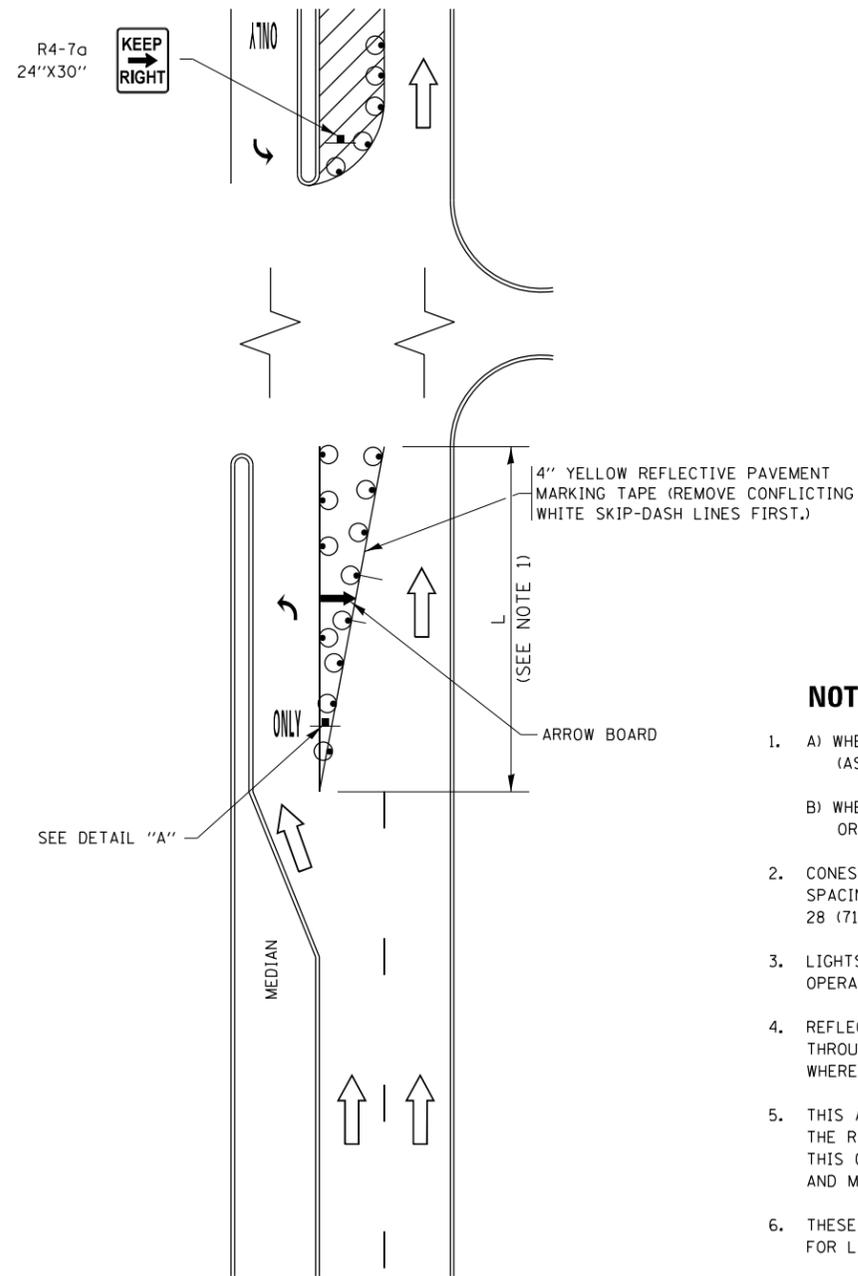
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Default	PLOT SCALE = 50.000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

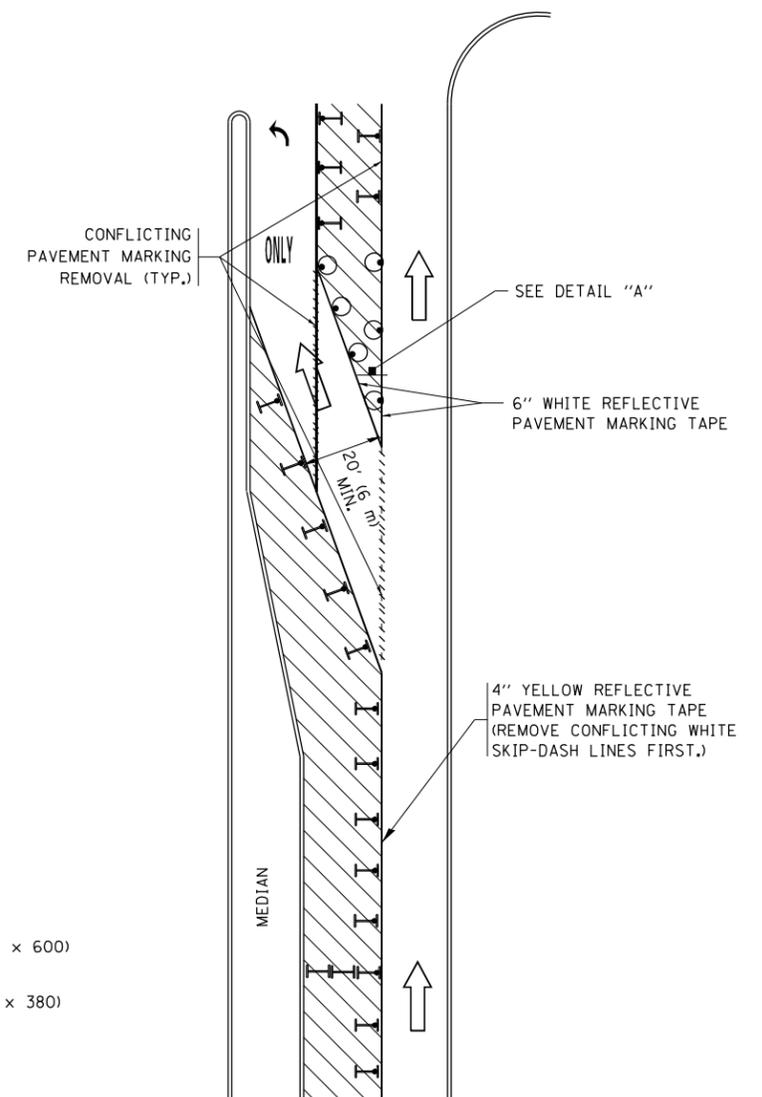
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	42
<b>TC-13</b>		<b>CONTRACT NO. 62J35</b>		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



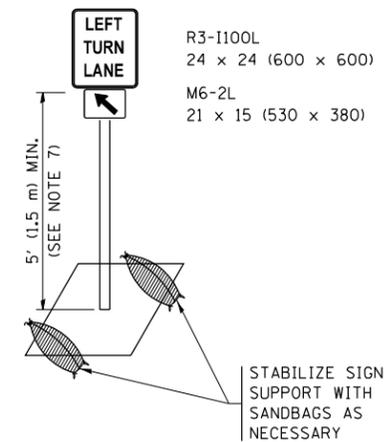
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\ADDData\CAHOUSEH1407-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		305	2019-058-BR	MCHENRY	47	43			
Default	PLOT SCALE = 50.0000' / in.	REVISED - T. RAMMACHER 01-06-00	REVISED -		<b>TC-14</b>			<b>CONTRACT NO. 62J35</b>				

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

**ROUTE MARKERS**

FOR U.S. ROUTES  
M1-40-2424

FOR ILLINOIS ROUTES  
M1-50-2424

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

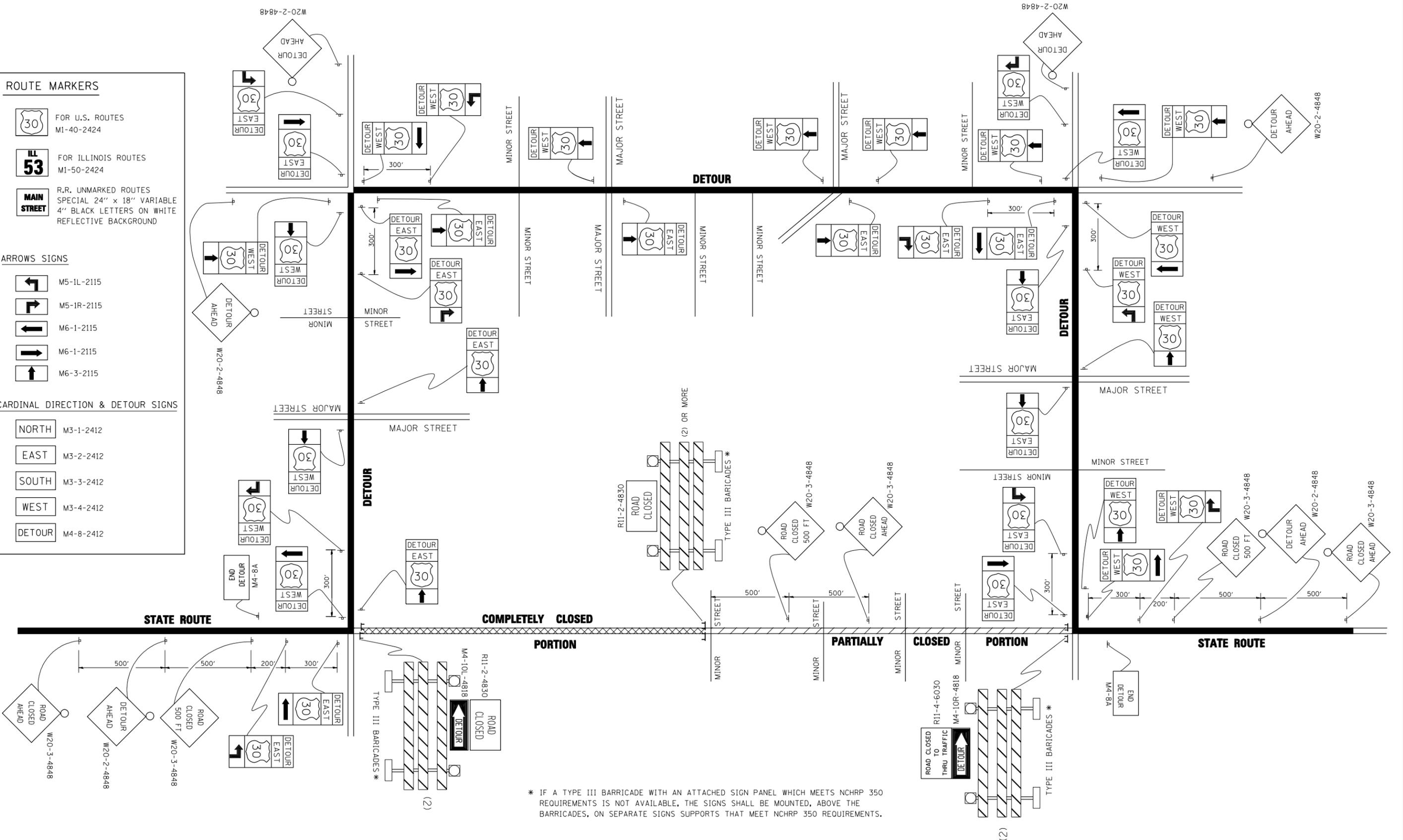
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

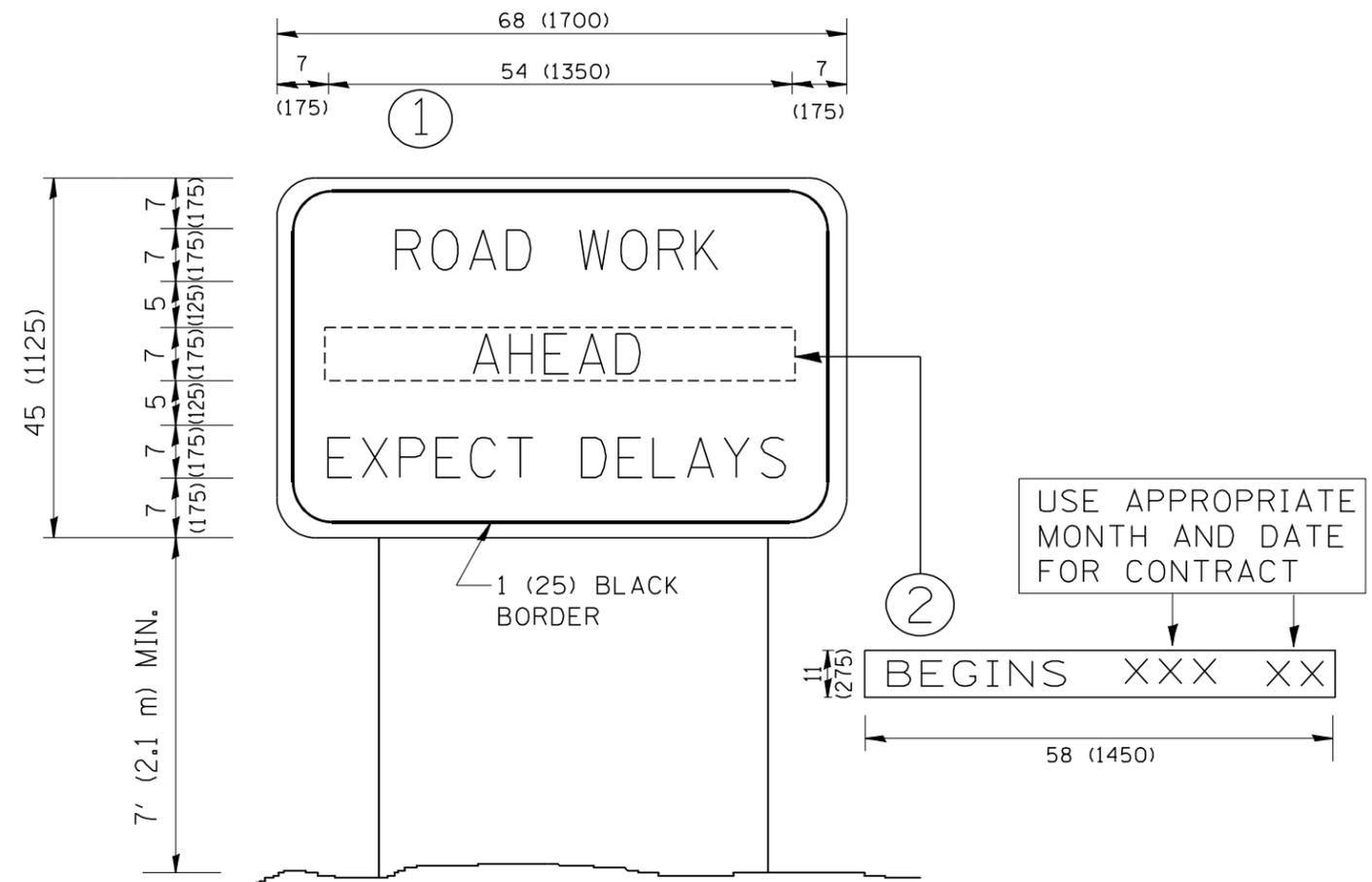
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING  
FOR CLOSING STATE HIGHWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	44
<b>TC-21</b>		CONTRACT NO. <b>62J35</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

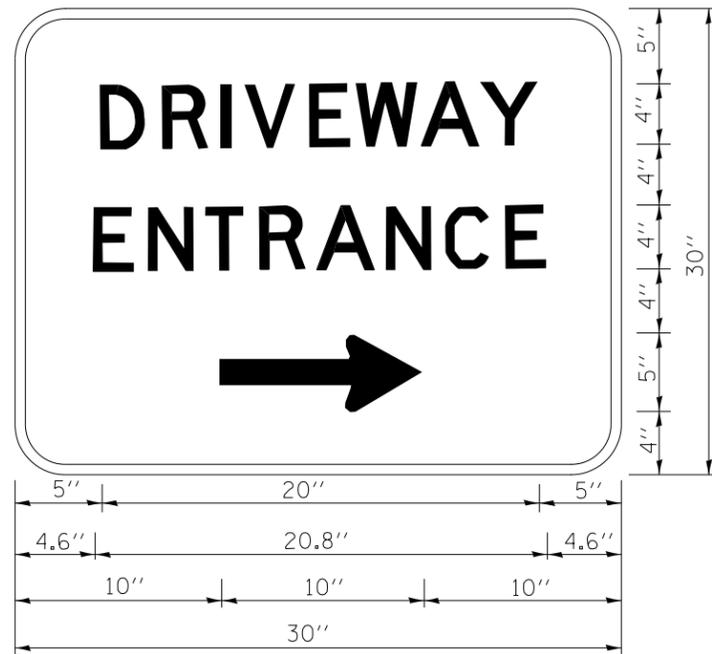
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	45
<b>TC-22</b>			<b>CONTRACT NO. 62J35</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

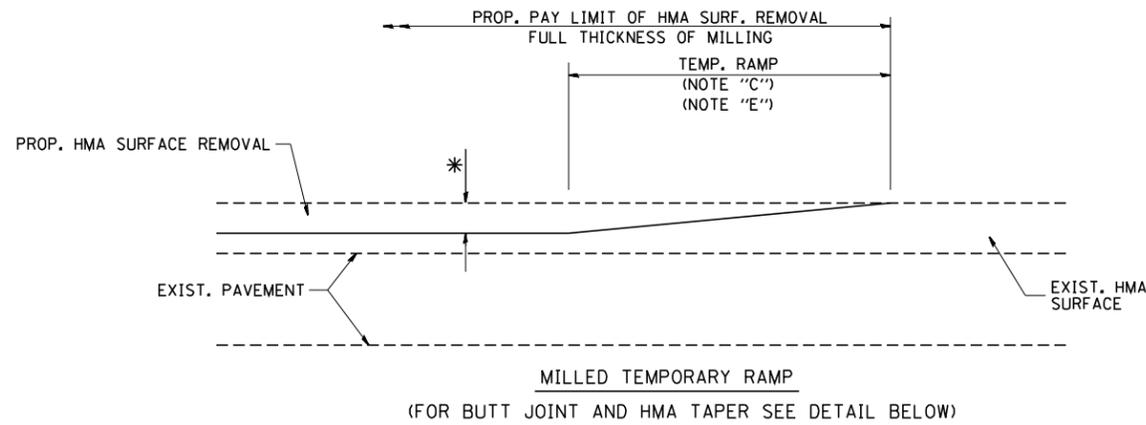
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

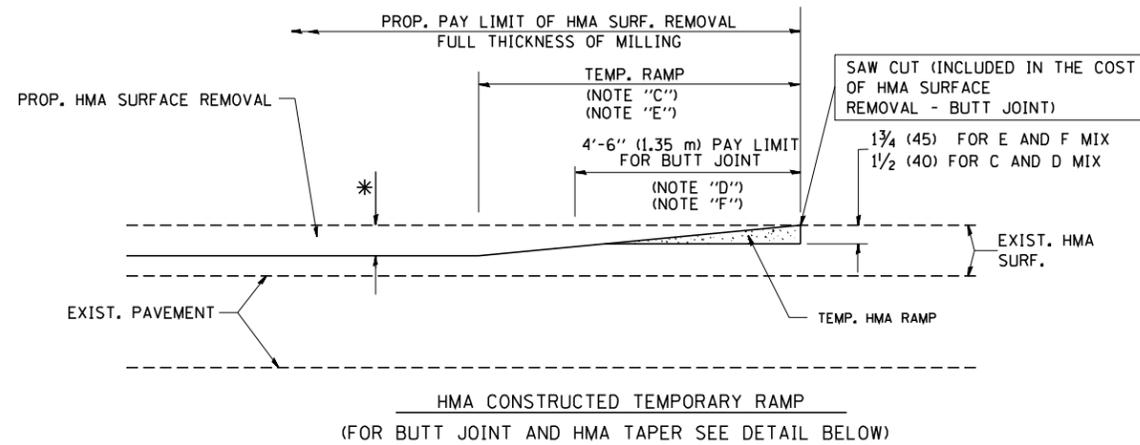
**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	46
<b>TC-26</b>			<b>CONTRACT NO. 62J35</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

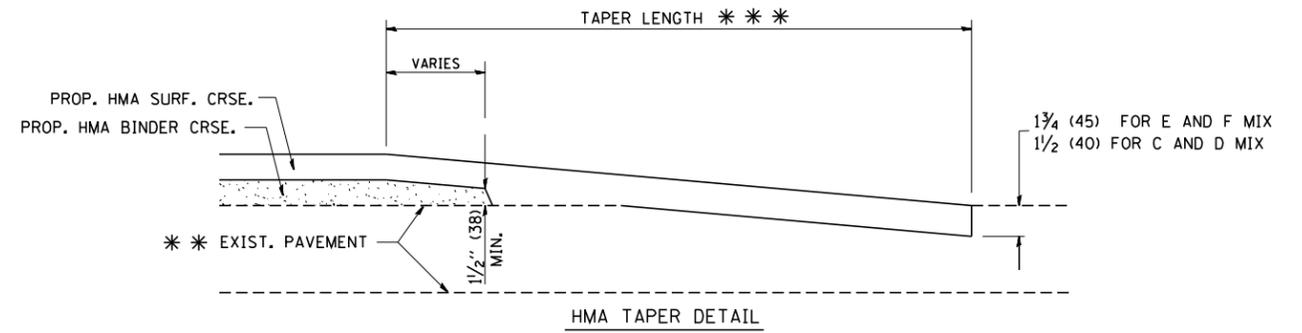
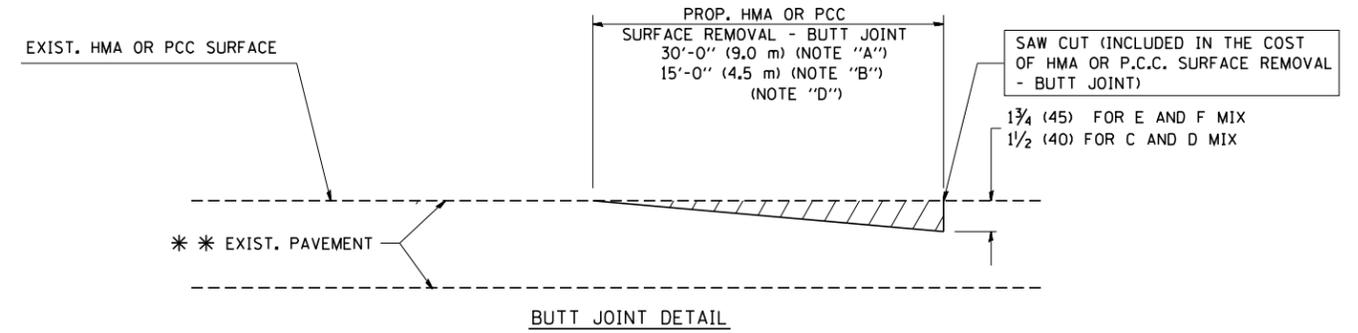


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

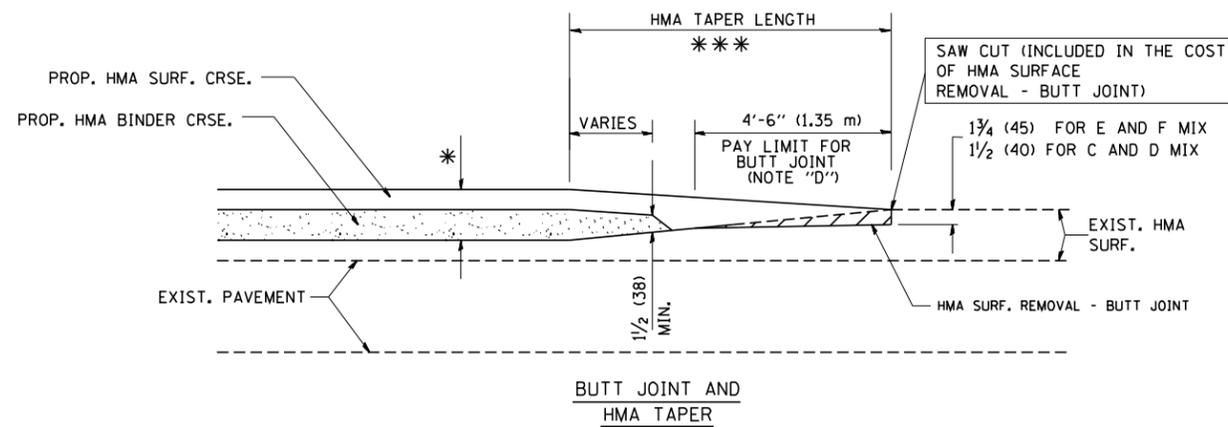
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-058-BR	MCHENRY	47	47
<b>BD400-05 BD32</b>		CONTRACT NO. 62J35		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				