

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6371	93-00295-03-PV	MCLEAN	109	57
STA.		TO STA.		
ILLINOIS F.A. PROJ. NO. M-5227046				
CONTRACT NO. 91351				

EXISTING \oslash U.S. RTE. 51 (MAIN ST.) CURVE DATA
 P.I. STA. 293+53.59
 $\Delta = 13^{\circ}40'12''$
 $D = 1^{\circ}30'00''$
 $T = 457.84'$
 $R = 3819.72'$
 $L = 911.34'$
 $E = 27.34'$
 P.C. STA. 288+95.75
 P.T. STA. 298+07.09
 S.E. = TO MATCH EXISTING

- LEGEND**
- PROPOSED PCC DRIVEWAY PAVEMENT (6" FOR P.E. - 8" FOR C.E.)
 - PROPOSED PORTLAND CEMENT CONCRETE BASE COURSE 9"
 - PROPOSED AGGREGATE SURFACE COURSE, TYPE B (8" THICK)
 - PROPOSED PORTLAND CEMENT CONCRETE SURFACE PORTAL - BUTT JOINT
 - PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 - PROPOSED SIDEWALK RAMP (SEE DETAIL)
 - CLASS B PATCH 9" - SEE REMOVAL/RELOCATION PLANS FOR LOCATIONS
 - STRUCTURE NUMBER (SEE PLAN/PROFILE SHEETS)
 - 1" TRANSVERSE EXPANSION JOINT (WITH DOWEL BARS - SEE STANDARD 420001)
 - STAGE CONSTRUCTION LIMITS

STATION EQUATION

(A) - STA. 263+02.09 (\oslash HAMILTON RD.) = STA. 298+86.14 (\oslash U.S. RTE. 51)

(B) - STA. 500+00.00 (\oslash HAMILTON RD.) = STA. 298+91.64 (\oslash U.S. RTE. 51)
 P.K. NAIL SET

NOTES

1. ON HAMILTON RD., THE P.C. CONCRETE PAVEMENT SHALL NOT BE POURED MONOLITHIC WITH THE COMBINATION CONCRETE CURB AND GUTTER EXCEPT AT THE STUB LOCATIONS SHOWN ON THE PLANS. THE COST OF ADDITIONAL GUTTER FLAG WIDTH WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR THE SPECIFIED TYPE OF P.C. CONCRETE PAVEMENT.
2. ON U.S. RTE. 51, THE P.C. CONCRETE BASE COURSE SHALL NOT BE POURED MONOLITHIC WITH THE COMBINATION CONCRETE CURB AND GUTTER EXCEPT AT THE STUB LOCATIONS SHOWN ON THE PLANS. THE COST OF ADDITIONAL GUTTER FLAG WIDTH WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR THE SPECIFIED TYPE OF P.C. CONCRETE BASE COURSE.
3. THE CROSS SLOPE FOR THE P.C. CONCRETE PAVEMENT ON HAMILTON RD. SHALL BE TRANSITIONED ON THE RIGHT SIDE FROM STA. 252+20.00 AT -2.00% TO STA. 253+40.00 AT +1.00% AND ON THE LEFT SIDE FROM STA. 261+95.00 AT -2.00% TO STA. 262+55.04 AT -0.50%. FULL SUPERELEVATION ON RT. SIDE AT +1.00% FROM STA. 253+40.00 TO STA. 262+55.04. SEE THE PROPOSED TYPICAL SECTIONS AND THE CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.
4. THE RAMPED CONCRETE MEDIAN NOSE SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. 606301, EXCEPT THAT IT SHALL BE CONSTRUCTED TO THE FULL THICKNESS OF THE BASE COURSE AND HOT-MIX ASPHALT SURFACING. THE RAMPED NOSE SHALL BE 6' LONG MEASURED FROM THE END OF THE RAMP TO THE BACK OF THE CURB. THE RAMPED NOSE SHALL BE PAID FOR AS CONCRETE MEDIAN, TYPE SM-6.12 (SPECIAL).
5. THE CONTRACTOR WILL BE REQUIRED TO SAW CUT AND REMOVE THE EXISTING PAVEMENT AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE TYPICAL SECTION TO PROVIDE A CLEAN VERTICAL EDGE IF NO LONGITUDINAL JOINT IS VISIBLE. THE SAW CUTTING OF THE PAVEMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS REMOVAL PAY ITEMS.
6. ALL ELEVATIONS REFERRED TO ARE TO THE TOP OF FINISHED (NOT BASE COURSE) CONSTRUCTION.
7. SEE DRAINAGE STRUCTURE TABLES ON PLAN/PROFILE SHEETS FOR HORIZONTAL LOCATIONS AND VERTICAL ELEVATIONS OF DRAINAGE STRUCTURES.
8. SEE THE SUPERELEVATION TRANSITION TABLES FOR ADDITIONAL STATIONS, ELEVATIONS, AND OFFSETS TO THE EDGE OF PAVEMENT FOR THE PAVEMENT WARPING ON HAMILTON RD. AND U.S. RTE. 51.
9. ALL STREET RADII ARE DIMENSIONED TO THE EDGE OF PAVEMENT.

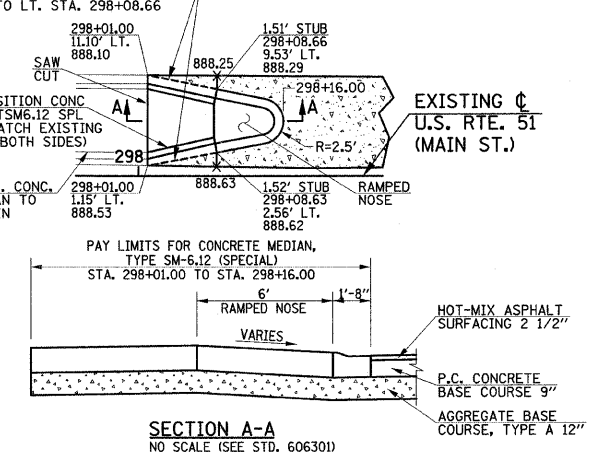
SEE SCHEDULE OF QUANTITIES FOR TYPICAL SECTION PAVEMENT PAY ITEMS AND THEIR LOCATIONS.

SEE PROPOSED TYPICAL SECTION SHEETS, HORIZONTAL ALIGNMENT AND CONTROL SHEET, SUPERELEVATION TRANSITION TABLES SHEET, REMOVALS/RELOCATIONS SHEETS, PLAN AND PROFILE SHEETS, PAVEMENT JOINTS SHEETS, MISCELLANEOUS DETAILS SHEETS, AND CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.

ILLINOIS DEPARTMENT OF TRANSPORTATION

INTERSECTION DETAILS

DATE: 6-09
 DRAWN BY: J.L.B.
 CHECKED BY: R.L.H.



EXISTING \oslash U.S. RTE. 51 (MAIN ST.)

PROPOSED \oslash HAMILTON RD. CURVE DATA
 P.I. STA. 260+37.94
 $\Delta = 11^{\circ}00'00''$
 $D = 3^{\circ}34'52''$
 $T = 154.06'$
 $R = 1600.00'$
 $L = 307.18'$
 $E = 7.40'$
 P.C. STA. 258+83.88
 P.T. STA. 261+91.06
 S.E. = TO MATCH U.S. RTE. 51 (MAIN ST.) P.G.L.
 (SEE SUPERELEVATION TRANSITION TABLES)