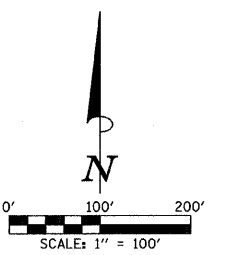
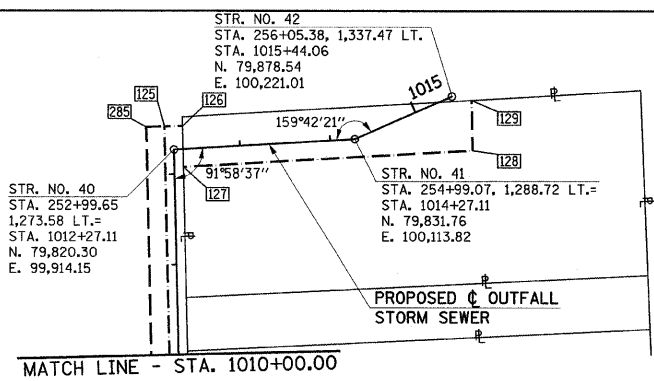


F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
6371	93-00295-03-PV	MCLEAN	109	17
STA.	TO STA.		ILLINOIS F.A. PROJ. NO. M-5227(046)	
			CONTRACT NO. 91351	



PROPOSED ζ HAMILTON RD. CURVE DATA
 P.I. STA. 242+24.41
 $\Delta = 4^{\circ}33'03''$
 $D = 1^{\circ}47'26''$
 $T = 127.15'$
 $R = 3200.00'$
 $L = 254.16'$
 $E = 2.52'$
 P.C. STA. 240+97.26
 P.T. STA. 243+51.42
 S.E. = NONE

PROPOSED ζ HAMILTON RD. CURVE DATA
 P.I. STA. 251+59.84
 $\Delta = 1^{\circ}30'00''$
 $D = 0^{\circ}34'23''$
 $T = 130.91'$
 $R = 10000.00'$
 $L = 261.80'$
 $E = 0.86'$
 P.C. STA. 250+28.93
 P.T. STA. 252+90.73
 S.E. = SEE S.E. TRANSITION TABLES

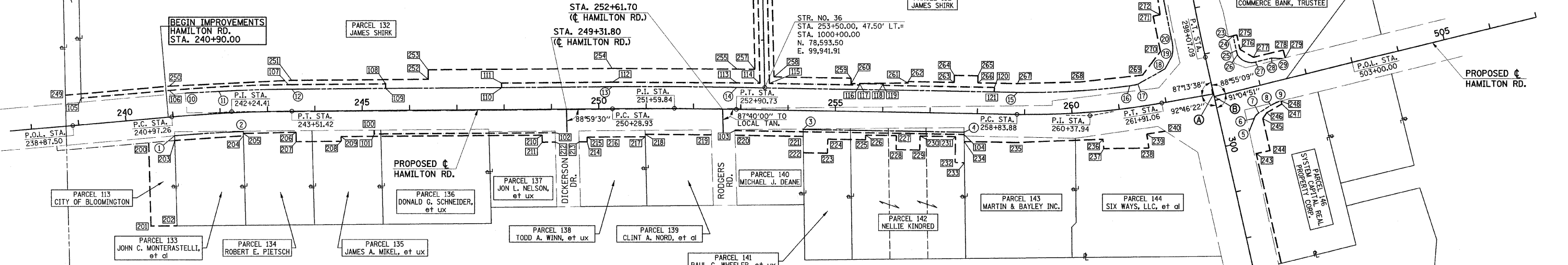


BENCHMARK NO.	DESCRIPTION	ELEVATION
408	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 246+53, 8' RT.	894.11
500	TOP N.W. FLANGE BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 251+77, 4' RT.	889.70
501	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 257+04, 10' LT.	890.74
502	TOP S.E. FLANGE BOLT ON FIRE HYDRANT ON SOUTH SIDE OF HAMILTON RD. STA. 259+84, 57' RT.	891.94
503	TOP CAP BOLT ON FIRE HYDRANT ON NORTH SIDE OF HAMILTON RD. STA. 261+99, 28' LT.	890.56
504	TOP CAP BOLT ON FIRE HYDRANT ON EAST SIDE OF MAIN STREET STA. 296+57, 45' LT.	890.67
505	TOP CAP BOLT ON FIRE HYDRANT ON EAST SIDE OF MAIN STREET STA. 293+54, 54' LT.	885.30
510	PK NAIL SET IN N.E. CORNER OF CONCRETE HEADWALL AT SALE BARN LOADING RAMP STA. 253+69, 350' LT.	882.45
511	S.W. CORNER OF CONCRETE RETAINING WALL AT S.E. AREA OF "THE CHALET SUBDIVISION"	860.27

PROPOSED ζ HAMILTON RD. CURVE DATA
 P.I. STA. 260+37.94
 $\Delta = 11^{\circ}00'00''$
 $D = 3^{\circ}34'52''$
 $T = 154.06'$
 $R = 1600.00'$
 $L = 307.18'$
 $E = 7.40'$
 P.C. STA. 258+83.88
 P.T. STA. 261+91.06
 S.E. = TO MATCH U.S. RTE. 51 (MAIN ST.) P.G.L.
 (SEE S.E. TRANSITION TABLES)

EXISTING ζ U.S. RTE. 51 (MAIN ST.) CURVE DATA
 P.I. STA. 293+53.59
 $\Delta = 13^{\circ}40'12''$
 $D = 1^{\circ}30'00''$
 $T = 457.84'$
 $R = 3819.72'$
 $L = 911.34'$
 $E = 27.34'$
 P.C. STA. 288+95.75
 P.T. STA. 298+07.09
 S.E. = TO MATCH EXISTING

(A)	STA. 263+02.09 (ζ HAMILTON RD.) = STA. 298+86.14 (ζ U.S. RTE. 51)
(B)	STA. 500+00.00 (ζ HAMILTON RD.) = STA. 298+91.64 (ζ U.S. RTE. 51)



LOCATION	DESCRIPTION	LOCAL GROUND SYSTEM	
		NORTHING	EASTING
ζ HAMILTON RD.	P.O.L. 238+87.50	78,512.922	98,479.721
	P.C. 240+97.26	78,531.187	98,688.688
	P.I. 242+24.41	78,542.258	98,815.352
	P.T. 243+51.42	78,543.244	98,942.495
	ζ - ζ INT. 249+31.80	78,547.744	99,522.855
	P.C. 250+28.93	78,548.497	99,619.986
	P.I. 251+59.84	78,549.512	99,750.890
	ζ - ζ INT. 252+61.70	78,547.593	99,852.748
	P.T. 252+90.73	78,547.100	99,881.775
	P.C. 258+83.88	78,536.172	100,474.818
	P.I. 260+37.94	78,533.333	100,628.855
	P.T. 261+91.06	78,559.938	100,780.603
	ζ - ζ INT. 263+02.09	78,579.113	100,889.968
	ζ - ζ INT. 500+00.00	78,573.748	100,891.179
	P.O.L. 503+00.00	78,645.297	101,182.522
ζ U.S. RTE. 51 (MAIN ST.)	P.C. 288+95.75	79,560.613	100,779.395
	P.I. 293+53.59	79,102.833	100,771.779
	P.T. 298+07.09	78,656.221	100,872.567
	ζ - ζ INT. 298+86.14	78,579.113	100,889.968
	ζ - ζ INT. 298+91.64	78,573.748	100,891.179
	P.O.L. 306+22.10	77,861.202	101,051.980
	P.O.L. 1000+00.00	78,594.871	99,866.843
	P.O.L. 1003+50.00	78,944.780	99,858.849
	P.O.L. 1005+39.61	79,120.480	99,930.142
	P.O.L. 1008+89.61	79,470.389	99,922.148
	P.O.L. 1012+39.61	79,820.297	99,914.153
	P.O.L. 1014+39.61	79,831.761	100,113.824
	P.O.L. 1015+56.57	79,878.545	100,221.011
	P.O.L. 1016+17.55	79,882.141	100,281.893

200	240+44.09, 50.58 RT.	224	254+85.00, 60.00 RT.	448	501+35.00, 43.05 RT.	272	297+00.00, 75.00 RT.
201	240+33.31, 227.07 RT.	225	255+65.00, 60.00 RT.	449	238+77.46, 65.00 LT.	273	295+00.00, 70.00 RT.
202	240+88.14, 231.46 RT.	226	255+65.00, 50.00 RT.	250	240+97.26, 65.00 LT.	274	295+00.00, 55.00 RT.
203	240+99.23, 50.58 RT.	227	256+30.00, 50.00 RT.	251	243+51.42, 65.00 LT.	275	297+75.00, 75.00 LT.
204	242+46.86, 50.00 RT.	228	256+30.00, 80.00 RT.	252	246+40.00, 65.00 LT.	276	298+07.09, 75.00 LT.
205	242+46.93, 45.00 RT.	229	256+80.00, 80.00 RT.	253	246+40.00, 85.00 LT.	277	501+00.00, 60.00 LT.
206	243+60.00, 45.00 RT.	230	256+80.00, 50.00 RT.	254	250+28.93, 85.00 LT.	278	501+65.00, 60.00 LT.
207	243+60.00, 65.00 RT.	231	257+55.00, 50.00 RT.	255	252+90.73, 85.00 LT.	279	501+65.00, 42.93 LT.
208	244+55.00, 65.00 RT.	232	257+55.00, 105.00 RT.	256	NOT USED	280	1006+67.54, 20.00 LT.
209	244+55.00, 55.00 RT.	233	257+74.70, 105.00 RT.	257	253+28.45, 85.00 LT.	281	1006+67.53, 20.00 RT.
210	248+80.00, 55.00 RT.	234	257+73.29, 60.00 RT.	258	253+69.10, 70.00 LT.	282	1008+14.34, 20.00 RT.
211	248+80.00, 70.00 RT.	235	258+83.88, 60.00 RT.	259	255+31.92, 70.00 LT.	283	1007+87.50, 20.00 LT.
212	249+07.86, 70.00 RT.	236	260+60.00, 60.00 RT.	260	255+32.20, 58.59 LT.	284	1007+87.50, 30.00 LT.
213	249+57.87, 70.00 RT.	237	260+60.00, 80.00 RT.	261	256+52.67, 61.87 LT.	285	1012+27.11, 39.29 RT.
214	249+75.00, 70.00 RT.	238	261+50.00, 90.00 RT.	262	256+52.57, 65.00 LT.		
215	249+75.00, 60.00 RT.	239	261+50.00, 60.00 RT.	263	257+50.00, 65.00 LT.		
216	250+28.93, 60.00 RT.	240	261+87.54, 60.00 RT.	264	257+50.00, 80.00 LT.		
217	250+98.10, 60.00 RT.	241	303+50.00, 53.67 LT.	265	258+00.00, 80.00 LT.		
218	250+97.99, 55.00 RT.	242	303+50.00, 60.00 LT.	266	258+00.00, 65.00 LT.		
219	252+38.75, 55.00 RT.	243	300+30.00, 60.00 LT.	267	258+83.88, 65.00 LT.		
220	252+89.07, 55.00 RT.	244	300+30.00, 90.00 LT.	268	260+20.00, 65.00 LT.		
221	254+32.97, 55.00 RT.	245	500+88.69, 70.05 RT.	269	261+60.00, 65.00 LT.		
222	254+34.06, 90.00 RT.	246	501+00.00, 60.00 RT.	270	297+75.00, 90.00 RT.		
223	254+85.00, 90.00 RT.	247	501+35.00, 50.00 RT.	271	297+00.00, 90.00 RT.		

100	245+25.00, 41.58 RT.	118	256+00.00, 59.00 LT.
101	245+25.00, 50.00 RT.	119	256+00.00, 56.00 LT.
102	249+07.60, 50.00 RT.	120	258+35.00, 56.00 LT.
103	252+88.63, 45.00 RT.	121	258+67.53, 46.58 LT.
104	257+72.82, 45.00 RT.	122	1006+67.53, 10.00 LT.
105	239+09.94, 56.00 LT.	123	1006+67.53, 10.00 RT.
106	240+97.26, 56.00 LT.	124	1008+13.99, 10.00 RT.
107	243+51.42, 56.00 LT.	125	1012+27.11, 27.27 LT.
108	245+50.00, 56.00 LT.	126	1012+37.98, 25.00 LT.
109	245+50.00, 46.58 LT.	127	1012+407.48, 10.00 RT.
110	247+95.00, 46.58 LT.	128	1015+41.48, 63.58 RT.
111	247+95.00, 56.00 LT.	129	1015+62.32, 12.68 RT.
112	250+28.93, 56.00 LT.		
113	252+90.73, 56.00 LT.		
114	253+39.64, 56.00 LT.		
115	253+59.66, 56.00 LT.		
116	255+50.00, 56.00 LT.		
117	255+50.00, 59.00 LT.		

1	240+99.84, 40.58 RT.	16	261+28.85, 46.58 LT.
2	242+46.99, 40.58 RT.	17	261+46.90, 47.52 LT.
3	254+32.50, 40.00 RT.	18	262+10.00, 70.00 LT.
4	257+72.66, 49.00 RT.	19	297+90.00, 68.56 RT.
5	500+71.27, 65.32 RT.	20	297+59.66, 61.58 RT.
6	500+71.27, 65.32 RT.	21	296+65.43, 61.58 RT.
7	500+83.18, 56.43 RT.	22	295+68.30, 55.00 RT.
8	501+10.90, 47.06 RT.	23	297+75.00, 57.00 LT.
9	501+30.00, 43.04 RT.	24	297+75.00, 67.00 LT.
10	241+29.52, 47.74 LT.	25	298+07.09, 67.00 LT.
11	242+00.00, 46.58 LT.	26	298+17.18, 67.00 LT.
12	243+51.42, 46.58 LT.	27	501+01.87, 49.33 LT.
13	250+28.93, 46.58 LT.	28	501+37.15, 49.33 LT.
14	252+90.73, 46.58 LT.	29	501+58.19, 42.93 LT.
15	258+83.88, 46.58 LT.		

---	SECTION LINE
---	EXISTING R.O.W. LINE
---	EXISTING PERMANENT EASEMENT LINE
---	PROPERTY LINE
---	PROPOSED R.O.W. LINE
---	PROPOSED PERMANENT EASEMENT LINE
---	PROPOSED TEMPORARY CONSTRUCTION EASEMENT LINE

ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL ALIGNMENT LAYOUT AND CONTROL
 DATE : 6-09
 DRAWN BY : J.L.B.
 CHECKED BY : D.L.M.
 SCALE : 1"=100'

SEE SHEET 18 FOR LOCATIONS OF EXISTING AND PROPOSED CENTERLINE CONTROL TIES.