09-22-2017 LETTING ITEM 051

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	1
ILLINOIS		ILLINOIS CONTRACT NO. 64E26		

D-92-078-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

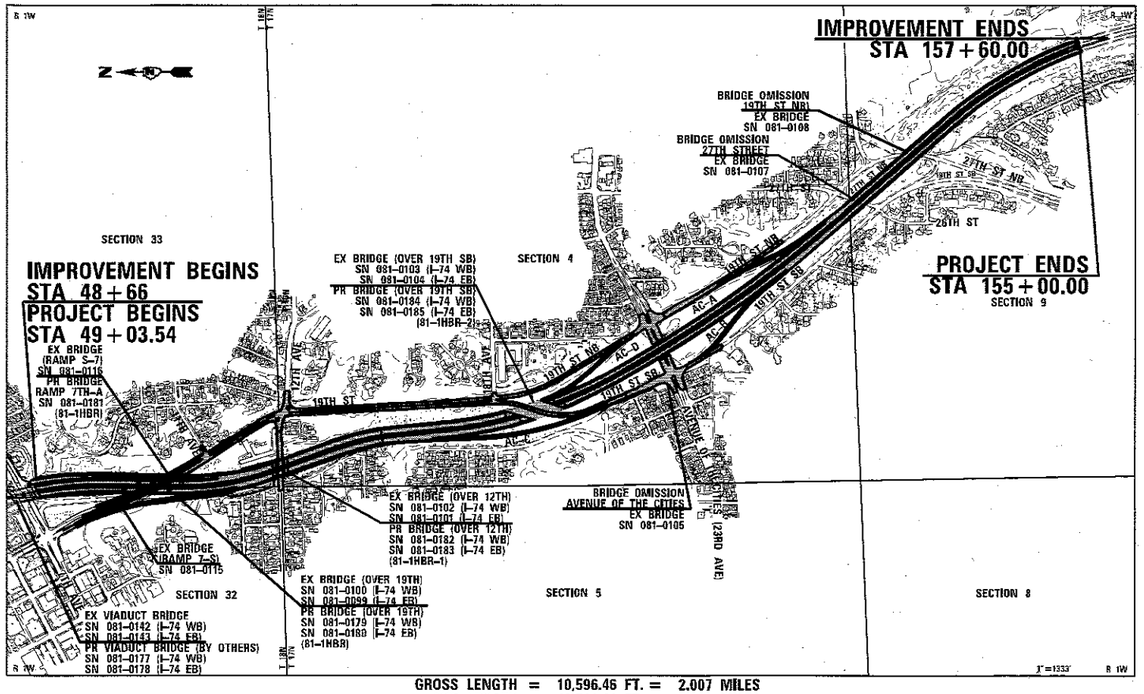
FAI ROUTE 74 (I-74)
SECTION (81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)
PROJECT NHPP-NHS-0074(326)
RECONSTRUCTION
ROCK ISLAND COUNTY
MOLINE TOWNSHIP
C-92-064-15

INDEX OF SHEETS & LIST OF STANDARDS, SEE SHEET 3

DESIGN DESIGNATION

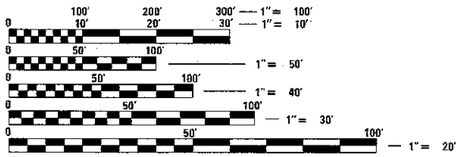
I-74: 6,950(37) INTERSTATE 22.35 (JPCCP-20)

19TH ST: 1,190(35) ARTERIAL 2.04 (JPCCP-20)
12TH AVE: 955(35) ARTERIAL 1.55 (JPCCP-20)



GROSS LENGTH = 10,596.46 FT. = 2.007 MILES
NET LENGTH = 10,596.46 FT. = 2.007 MILES

LOCATION OF SECTION INDICATED THUS: -



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 10 2017
K S Marshall
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
June 30 2017
Maureen M. Addis, P.E. for
ENGINEER OF DESIGN AND ENVIRONMENT
June 30 2017
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PROJECT MANAGER: REBECCA MARRUFFO
(815) 284-5351

CONTRACT NO. 64E26

ROCK ISLAND COUNTY SECTION (81-1)R-1 & 81-1(HBR, HBR-1, HBR-2) FAI ROUTE 74 (I-74)

REV

HANSON

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SHEETS: 1–8, 41, 43, 45–49, 54–55, 57–63, 67, 110–118, 130–137, 144–160, 175–210, 212–213, 220–223, 253–312, 316–320, 324–332, 337–378, 381–385, 391–399, 404–414, 423–426, 431–474, 566–567, 573–583, 588–609, 615–633, 640–663, 668–676, 683–685, 691–710, 743–744, 746–748, 750–751, 757–759, 766–786, 793, 1541–1564, 1568–1633, 1792–1829, 1868–2042

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DIXON, ILLINOIS

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SHEETS: 711–742

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JERILYN M. HASSARD, P.E., S.E.

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LICENSE EXP DATE 11/30/2018

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SHEETS: 1496–1507

DATE SIGNED 3/27/17

LICENSE EXP DATE 8/31/2017

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1701 RIVER DRIVE SUITE 200 MOLINE, IL 61265
309.764.7650 fax: 309.764.8616 www.shive-hattery.com

LAYOUT	CBP	4/22/2013
DRAWN	CBP	3/20/2017
REVIEWED	AAP	3/22/2017

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		DRAWN - CBP	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/27/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADDITIONAL SEALS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1IR-1 & 8)-1HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	2
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

COV-02

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LAYOUT	CBP	7/22/2011
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REVIEWED	AMP	3/22/2017

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX AND HIGHWAY STANDARDS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

IND-01

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-11R-1 & 81-10HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	3
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

GENERAL NOTES

1. THE REMOVAL OF BITUMINOUS SURFACING LESS THAN 6 INCH THICKNESS NOT ON A RIGID TYPE BASE REMOVED IN CONJUNCTION WITH THE BASE SHALL BE REMOVED AS EARTH EXCAVATION. THE REMOVAL OF BITUMINOUS SURFACING ON A RIGID TYPE BASE OR A THICKNESS OF 6 INCHES OR MORE ON A FLEXIBLE BASE REMOVED IN CONJUNCTION WITH THE BASE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT REMOVAL OF THE TYPE SPECIFIED.

2. THE FINAL TOP FOUR INCHES OF SOIL IN ANY RIGHT-OF-WAY AREA DISTURBED BY THE CONTRACTOR MUST BE CAPABLE OF SUPPORTING VEGETATION. THE SOIL MUST BE FROM THE A HORIZON (ZERO TO 2' DEEP) OF SOIL PROFILES OF LOCAL SOILS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICES BID AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

3. ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION. ENTRANCE TO BORROW/WASTE/USE SITE MUST ALSO BE APPROVED BY THE DEPARTMENT.

4. THE CONTRACTOR SHALL SEED OR SOD ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS AS SHOWN ON THE PLANS. SEEDING CLASS 4 OR 2A SHALL BE USED. CLASS 2A SHALL BE USED ON FRONT SLOPES AND DITCH BOTTOMS. CLASS 4 SHALL BE USED BEHIND TYPE A GUTTER, ON ALL BACKSLOPES AND AREAS BEHIND THE BACKSLOPE, AND BEYOND THE TOE OF FRONT SLOPE ON FILL SECTIONS WITHOUT DITCHES. SEE PLANS FOR SODDING LOCATIONS.

5. PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.

6. PLACEMENT AND COMPACTION OF THE BACKFILL FOR PROPOSED ACROSS ROAD CULVERTS AND EXISTING ACROSS ROAD CULVERTS THAT ARE REMOVED SHALL CONFORM TO ARTICLE 502.10 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT THE MATERIAL SHALL CONFORM TO ARTICLE 208.02 OF THE STANDARD SPECIFICATIONS, AND SHALL BE COMPACTED TO A MINIMUM OF 95% OF THE STANDARD LABORATORY DENSITY. ANY MATERIAL CONFORMING TO THE REQUIREMENTS OF ARTICLE 1003.04 OR 1004.05 FOR TRENCH BACKFILL WHICH HAS BEEN EXCAVATED FROM THE TRENCHES SHALL BE USED FOR BACKFILLING THE TRENCHES. THE ENTIRE EXCAVATION, WITHIN 2 FEET OUTSIDE OF EACH SHOULDER, SHALL BE BACKFILLED WITH TRENCH BACKFILL MATERIAL TO THE BOTTOM OF THE PROPOSED SUBGRADE. IMPERVIOUS MATERIAL SHALL BE USED ON THE OUTER 3 FEET AT EACH END OF THE CULVERT. THIS TRENCH BACKFILL MATERIAL WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE ITEM OF THE WORK FOR WHICH IT IS REQUIRED.

7. THE SUBGRADE ON THIS PROJECT, EXCLUSIVE OF ROCK CUT AREAS IS SCHEDULED TO BE IMPROVED TO A 12" DEPTH ACCORDING TO MECHANISTIC PAVEMENT DESIGN. THE AREAS SCHEDULED TO BE IMPROVED TO A DEPTH GREATER THAN 12" ARE ESTIMATED BASED ON THE ORIGINAL GEOTECHNICAL INVESTIGATION. THE SUBGRADE SHALL BE PROCESSED IN ACCORDANCE WITH ARTICLE 301.04 OF THE STANDARD SPECIFICATIONS BEFORE THE ENGINEER SHALL DETERMINE THE LIMITS AND THE ADDITIONAL THICKNESS OF IMPROVEMENT REQUIRED, IF ANY. ANY ADDITIONAL UNDERCUTTING REQUIRED AFTER THIS EVALUATION SHALL BE PAID FOR AS EARTH EXCAVATION.

8. ALL "AGGREGATE SUBGRADE IMPROVEMENT" (SECTION 303), SHALL BE COMPLETED IN ACCORDANCE WITH ARTICLES 311.04, 311.05, 311.05(A), 311.06 AND 311.07. ALL AGGREGATE SUBGRADE THICKNESSES EQUAL TO OR LESS THAN 12 INCHES SHALL BE CONSTRUCTED OF AGGREGATE OF CA02 GRADATION. ALL AGGREGATE SUBGRADE THICKNESSES GREATER THAN 12 INCHES SHALL BE CONSTRUCTED OF CS02.

9. ALL EMBANKMENT CONSTRUCTED OF COHESIVE SOIL SHALL BE CONSTRUCTED WITH NOT MORE THAN 110% OF OPTIMUM MOISTURE CONTENT, DETERMINED BY THE STANDARD PROCTOR TEST. COHESIVE SOIL SHALL BE DEFINED AS ANY SOIL WHICH CONTAINS GREATER THAN 10% PARTICLES BY WEIGHT PASSING THE 75 µM (#200 SIEVE). THE 110% OF OPTIMUM MOISTURE LIMIT MAY BE WAIVED IN FREE-DRAINING GRANULAR MATERIAL WHEN APPROVED BY THE ENGINEER.

10. CLOSED EXPANSION JOINTS ON JOINTED PAVEMENTS SHALL BE RE-ESTABLISHED DURING THE PATCHING OPERATIONS. CLASS B PATCHES - WHEN THE PAVEMENT REQUIRES PATCHING AT THE LOCATION OF THE EXPANSION JOINT, A NEW JOINT SHOULD BE ESTABLISHED USING A DOWELLED EXPANSION PATCH AS SHOWN ON HIGHWAY STANDARD 442101. WHEN THE JOINT IS CLOSED, BUT DOES NOT REQUIRE PATCHING, AN EXPANSION JOINT MAY BE FORMED BY SAWING THE PAVEMENT AND FILLING THE SAW CUT WITH A PREFORMED EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF SECTION 1051 OF THE STANDARD SPECIFICATIONS AS SHOWN ON STANDARD 420001. IF JOINTS ARE PAVED OVER, THE HMA SHALL BE REMOVED OVER THE JOINT AND REPLACED WITH A FILLER. THIS WORK WILL BE INCLUDED IN THE COST OF THE OVERLAY MATERIAL.

11. WHEN LAYING OUT FOR PATCHING, THE MINIMUM DISTANCE BETWEEN NEW PATCHES (SAW CUT TO SAW CUT) SHALL BE 15 FEET. WHEN PATCH SPACING IS LESS THAN 15 FEET, THE PAVEMENT BETWEEN PATCHES SHALL ALSO BE REMOVED AND REPLACED.

12. ALL MANDATORY JOINT SEALING FOR CLASS B PATCHES WILL NOT BE MEASURED FOR PAYMENT. OPTIONAL SAWING OF THE JOINT FOR THE SEALANT RESERVOIR WILL NOT BE MEASURED FOR PAYMENT.

    THE ENGINEER RESERVES THE RIGHT TO CHECK ALL PATCHES FOR SMOOTHNESS BY THE USE OF A 10' ROLLING STRAIGHT EDGE SET TO A 3/16" TOLERANCE IN THE WHEEL PATHS. ANY PATCH AREAS HIGHER THAN 3/16" MUST BE GROUND SMOOTH WITH AN APPROVED GRINDING DEVICE CONSISTING OF MULTIPLE SAWS. THE USE OF BUSHHAMMER OR OTHER IMPACT DEVICES WILL NOT BE PERMITTED. ANY PATCH WITH DEPRESSIONS GREATER THAN 3/16" SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER.

    THE MANDATORY SAW CUTS FOR PAVEMENT PATCHING ARE:
    CLASS B PATCH: CUT TWO TRANSVERSE SAW CUTS OUTLINING THE PATCH AND ONE TRANSVERSE PRESSURE RELIEF SAW CUT. THE LONGITUDINAL EDGES OF THE PATCH SHALL BE CUT FULL DEPTH. WHEN THE PATCH IS ADJACENT TO A PCC SHOULDER, TWO SAW CUTS ALONG THE SHOULDER WILL BE REQUIRED.

    THE MANDATORY SAW CUTS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR SAW CUTS.

13. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

LOCATION(S)	12TH AVE MILL/RESURFACING	
MIXTURE USE(S):	SURFACE	LEVELING BINDER
PG	SBS PG 70-22	SBS PG 70-22 (UPPER LIFT) PG 64-22 (LOWER LIFT)
DESIGN AIR VOIDS	4.0 @ N50	4.0 @ N50
MIXTURE COMPOSITION (MIXTURE GRADATION)	IL 9.5	IL 9.5 OR 9.5FG
FRICTION AGGREGATE	D	N/A
MIXTURE WEIGHT	112 LBS/SY/IN	
QUALITY MANAGEMENT PROGRAM	QC/QA	QC/QA
SUBLOT SIZE	N/A	N/A
PAY ITEM	40603535	40600825

LOCATION(S)	AOTC (23RD AVE) MILL/RESURFACING
MIXTURE USE(S):	SURFACE
PG	SBS PG 70-22
DESIGN AIR VOIDS	4.0 @ N50
MIXTURE COMPOSITION (MIXTURE GRADATION)	IL 9.5
FRICTION AGGREGATE	D
MIXTURE WEIGHT	112 LBS/SY/IN
QUALITY MANAGEMENT PROGRAM	QC/QA
SUBLOT SIZE	N/A
PAY ITEM	40603535

LOCATION(S)	MOT TEMPORARY PAVEMENT: FULL DEPTH PAVEMENT – **HMA OPTION**	
MIXTURE USE(S):	SURFACE (2")	BINDER (6 ¼" IN 2 LIFTS)
PG	SBS PG 70-28	SBS PG 70-28
DESIGN AIR VOIDS	4.0 @ N90	4.0 @ N90
MIXTURE COMPOSITION (MIXTURE GRADATION)	IL 9.5	IL 19.0
FRICTION AGGREGATE	E	N/A
MIXTURE WEIGHT	119 LBS/SY/IN	
QUALITY MANAGEMENT PROGRAM	QC/QA	
SUBLOT SIZE	N/A	
PAY ITEM	Z0062456 & X4400110 (Note: choice of material to be used for temporary pavement pay item is left to the Contractor to choose. HMA is one of the options)	

LOCATION(S)	I-74 MOT SHOULDER MILL/RESURFACE	
MIXTURE USE(S):	SURFACE	LEVELING BINDER
PG	SBS PG 70-22	SBS PG 70-22
DESIGN AIR VOIDS	4.0 @ N90	4.0 @ N90
MIXTURE COMPOSITION (MIXTURE GRADATION)	IL 9.5	IL 9.5 OR 9.5FG
FRICTION AGGREGATE	E	N/A
MIXTURE WEIGHT	119 LBS/SY/IN	
QUALITY MANAGEMENT PROGRAM	QC/QA	
SUBLOT SIZE	N/A	N/A
PAY ITEM	40603570	40600845

LAYOUT	CBP	1/22/2001
DRAWN	CBP	3/20/2017
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
SHEET 1 OF 4

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

GEN-01

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	6B1-1R-1 & B1-1GBR, HBR-1, HBR-2)	ROCK ISLAND	2042	4

CONTRACT NO. 64E26
ILLINOIS FED. AID PROJECT

GENERAL NOTES

14. THE CONTRACTOR SHALL PLACE TEMPORARY HOT-MIX ASPHALT TAPERS ALONG ALL SIDES OF THE UTILITY STRUCTURES PROTRUDING ABOVE THE MILLED SURFACE. THE TEMPORARY TAPERS SHALL EXTEND 2' OUTSIDE OF THE CASTINGS, EXCEPT FOR THE APPROACH SIDE TO TRAFFIC SHALL HAVE A 4' TAPER LENGTH. HOT-MIX ASPHALT MEETING THE APPROVAL OF THE ENGINEER SHALL BE USED, NO COLD MILLINGS WILL BE ALLOWED. THE COST OF THE MATERIAL, PLACEMENT, MAINTENANCE, REMOVAL AND DISPOSAL OF SAID WORK WILL BE INCLUDED IN THE PAY ITEM FOR HOT-MIX ASPHALT SURFACE REMOVAL.

15. THE CONTRACTOR WILL BE REQUIRED TO FURNISH 5 ½" HIGH BRASS STENCILS AS APPROVED BY THE ENGINEER AND INSTALL STATIONING AT 250' INTERVALS. STATIONING SHALL BE PLACED ON BOTH LANES OF 2-LANE HIGHWAYS AND ON THE OUTSIDE LANES IN BOTH DIRECTIONS ON 4-LANE HIGHWAYS. THE STATIONS SHALL BE PLACED 6" INSIDE THE PAVEMENT MARKING EDGE SO THEY CAN BE READ FROM THE SHOULDER. THIS WORK WILL BE INCLUDED IN THE COST OF THE FINAL PAVEMENT SURFACE.

16. THE AREA TO BE TACKED OR PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAY'S PRODUCTION, BUT NO MORE THAN FIVE DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER. AN APPLICATION RATE OF 0.05 LB/SQ FT SHALL BE USED FOR TACK COAT.

17. INSTALL RUMBLE STRIPS ON MAINLINE SHOULDERS IN ACCORDANCE WITH STATE STANDARD 642001. RUMBLE STRIPS SHALL BE PLACED ON SHOULDERS ON BOTH SIDES OF THE PAVEMENT.

18. A NATIONWIDE 404 PERMIT HAS BEEN ISSUED FOR THIS PROJECT AND THE CONDITIONS OF THAT PERMIT MUST BE ADHERED TO.

19. THE NEW NUMBER FOR WB I-74 MAINLINE STRUCTURE OVER 19TH ST WILL BE 081-0179.
THE NEW NUMBER FOR EB I-74 MAINLINE STRUCTURE OVER 19TH ST WILL BE 081-0180.
THE NEW NUMBER FOR RAMP 7TH-A STRUCTURE OVER 19TH ST WILL BE 081-0181.
THE NEW NUMBER FOR WB I-74 MAINLINE STRUCTURE OVER 12TH AVE WILL BE 081-0182.
THE NEW NUMBER FOR EB I-74 MAINLINE STRUCTURE OVER 12TH AVE WILL BE 081-0183.
THE NEW NUMBER FOR WB I-74 MAINLINE STRUCTURE OVER SB 19TH ST WILL BE 081-0184.
THE NEW NUMBER FOR EB I-74 MAINLINE STRUCTURE OVER SB 19TH ST WILL BE 081-0185.

20. THIS STRUCTURE WILL RETAIN THE SAME NUMBER: AVENUE OF THE CITIES EXISTING BRIDGE STRUCTURE 081-0105.

21. THE SOILS REPORT AND PROFILES ARE AVAILABLE AT THE DISTRICT OFFICE FOR CONTRACTOR'S REVIEW.

22. THE ADDITIONAL THICKNESS OF PROPOSED PAVEMENT REQUIRED TO MATCH THE BRIDGE APPROACH PAVEMENT, SHOWN IN STANDARD 420401, SHALL BE INCLUDED IN THE COST OF THE PROPOSED PAVEMENT AND NOT PAID FOR SEPARATELY.

23. BARRIER WALL REFLECTORS, TYPE B SHALL BE INSTALLED ON THE TOP OF BRIDGE PARAPET WALLS. THE MARKERS, THE COLOR, AND THE SPACING SHALL BE ACCORDING TO STANDARD 782006, EXCEPT THE MINIMUM IS 2 PER SIDE.

24. THE BORING LOGS INDICATE THAT GROUNDWATER LEVELS MAY ENCROACH ON THE CONSTRUCTION LIMITS OF THIS PROJECT. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTROL THE GROUND WATER DURING CONSTRUCTION IN ORDER TO KEEP THE CONSTRUCTION AREA FREE OF WATER AND NOT JEOPARDIZE ADJACENT PAVEMENT STRUCTURES. THE NEED FOR AND THE METHOD OF CONTROLLING THE WATER SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER AND THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAY ITEM BEING WORKED ON AT THE LOCATION DEWATERING IS NEEDED.

25. PRECAST GRATED INLET SPECIALS MAY BE SUBSTITUTED IN LIEU OF CAST-IN-PLACE UNITS WITH FLOORS UPON RECEIPT OF MANUFACTURER'S SHOP DRAWINGS WHICH HAVE BEEN APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING NECESSARY DIMENSIONS ON THE EXISTING DRAINAGE STRUCTURE REQUIRED FOR THE ATTACHMENT. NO ADDITIONAL COST FOR THIS SUBSTITUTION SHALL BE ALLOWED.

26. CONNECTING BANDS FOR CORRUGATED METAL PIPES SHALL BE METAL AND SHALL BE COATED WITH THE SAME MATERIAL AS THE PIPE SECTIONS. THE CONNECTING BANDS SHALL BE A MINIMUM OF 18" WIDE.

27. NOSES OF CURBED CORNER ISLANDS NOTED AS 1 & 2 ON HIGHWAY STANDARD 606301 SHALL BE RAMPED UNLESS THE CURB FUNCTION IS FOR THE PROTECTION OF PEDESTRIANS, SIGNALS, LIGHT STANDARDS OR SIGN TRUSS SUPPORTS.

28. USE M-6 CURB ON ISLANDS WHEN LOCATED ADJACENT TO A HIGHWAY WITH SPEEDS OF 45 MPH OR LESS.

29. ON INTERMEDIATE ISLANDS, THE VARIABLE CURB AND GUTTER FLAG WILL BE INCLUDED IN THE PAY ITEM FOR CONCRETE MEDIAN (SPECIAL).

30. THE CONTRACTOR SHALL INSTALL A 18" DIAMETER FORMED OPENING IN THE CONCRETE MEDIAN SURFACE OF THE ISLAND AS DIRECTED BY THE ENGINEER. ALSO, A 4" DIAMETER FORMED OPENING SHALL BE INSTALLED IN EACH CORNER OF THE ISLAND 1' BEHIND THE BACK OF CURB. ALL EXISTING PAVEMENT SURFACES OF OTHER EXISTING OBSTRUCTIONS BENEATH THESE OPENINGS SHALL BE REMOVED BY THE CONTRACTOR. AFTER THE MEDIAN IS IN PLACE THE 18" OPENING SHALL BE CORED DOWN 4' AND FILLED WITH DIRT. ALL COSTS INCURRED SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR CONCRETE MEDIAN (SPECIAL).

31. THE ISLANDS ON THIS PROJECT ARE INTERMEDIATE ISLANDS AS SHOWN ON THE ISLAND DETAIL SHEET IN THE PLANS.

32. THE CONTRACTOR SHALL INSTALL 18" DIAMETER FORMED OPENINGS IN THE CONCRETE MEDIAN SURFACE, SPACED AT INTERVALS NO GREATER THAN 250', AND/OR AS DIRECTED BY THE ENGINEER. ALL EXISTING PAVEMENT SURFACES OR OTHER EXISTING OBSTRUCTIONS BENEATH THESE OPENINGS SHALL BE REMOVED BY THE CONTRACTOR. AFTER THE MEDIAN IS IN PLACE, CORE EACH OPENING DOWN 4' AND FILL WITH DIRT. ALL COSTS INCURRED SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR CONCRETE MEDIAN SURFACE, 4 INCH OR CONCRETE MEDIAN, OF THE TYPE SPECIFIED IN THE PLANS.

33. ALL FRAMES AND GRATES OF DRAINAGE STRUCTURES TO BE REMOVED OR FILLED SHALL BE CAREFULLY SALVAGED AND SHALL REMAIN THE PROPERTY OF THE CITY OF MOLINE. CONTRACTOR SHALL DELIVER SALVAGED ITEMS TO THE CITY OF MOLINE AS DIRECTED BY THE ENGINEER. ALL COST FOR THE SALVAGE IS INCLUDED IN THE APPLICABLE REMOVAL PAY ITEMS AND NOT PAID FOR SEPARATELY.

34. THE COST OF MAKING STORM SEWER CONNECTIONS TO EXISTING DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE VARIOUS CONTRACT UNIT PRICES FOR STORM SEWER.

35. LATERAL DISTANCES FROM THE CENTERLINE ON ALL INLETS AND MANHOLES ARE TO THE FACE OF THE STRUCTURE/EDGE OF PAVEMENT/SHOULDER. LATERAL DISTANCES TO PERMANENT BARRIER ARE TO THE FACE OF BARRIER, UNLESS OTHERWISE NOTED.

36. THE NEW MANHOLE LIDS ON THIS PROJECT SHALL HAVE THE WORD "STORM", "SANITARY", OR "WATER" ON THE LID. THE WORD TO BE USED IS NOTED ON THE PLANS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE WORD TO BE USED ON OTHER LIDS NOT NOTED ON THE PLANS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.

37. ALL PROPOSED MANHOLES ON THIS PROJECT SHALL BE CAST-IN-PLACE OR PRECAST. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR MANHOLE OF THE TYPE AND SIZE SPECIFIED.

38. THE CONTRACTOR SHALL DETERMINE FLOWLINES OF EXISTING SEWER LINES WHICH ARE SHOWN ON THE PLANS AS ESTIMATED OR UNKNOWN. THIS INFORMATION IS NECESSARY BEFORE ORDERING INLETS AND MANHOLES.

39. THE CONTRACTOR SHALL SUPPLY THE RESIDENT ENGINEER WITH THE MANUFACTURER'S INSTALLATION REQUIREMENTS FOR THE TYPE OF STEEL PLATE BEAM GUARDRAIL TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT.

40. ONE 16D GALVANIZED NAIL SHALL BE USED TO TOE NAIL THE WOOD BLOCK OUT TO THE WOOD POST ON ALL TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT.

41. DELINEATORS SHALL BE INSTALLED AS SHOWN IN STANDARD 635001, EXCEPT THAT THE POST SHALL BE ROTATED 180° AND ONLY METAL-BACKED DELINEATORS SHALL BE PERMITTED. DELINEATORS SHALL BE PLACED AT THE ENDS OF APPROACH GUARDRAIL TERMINAL SECTIONS, AND AT EACH HEADWALL OR END SECTION OF AR CULVERTS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR DELINEATORS.

42. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTING AND MAINTAINING AN ELECTRONIC LOG OF ALL STAKEOUT SURVEY THAT IS PERFORMED ON THE JOB, EITHER BY HIM/HER OR ANY SUB-CONTRACTOR PERFORMING THE STAKEOUT. UPON REQUEST, ALL LOGS SHALL BE SUBMITTED TO THE DEPARTMENT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK, BUT SHALL BE CONSIDERED INCLUDED IN THE COST FOR CONSTRUCTION LAYOUT.

LAYOUT	CBP	7/22/2011
DRAWN	CBP	3/20/2017
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
SHEET 2 OF 4

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

GEN-02

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1IR-1 & 81-1(6)BR, HBR-1, HBR-2)	ROCK ISLAND	2042	5
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

GENERAL NOTES

43. PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:

    A. ALL WORDS, SUCH AS ONLY, SHALL BE 8 FEET HIGH.
    B. ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
    C. THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8 INCHES, NOT 7 INCHES, AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.
    D. CENTERLINE SKIP DASH PAVEMENT MARKING ON MULTI-LANE DIVIDED, MULTI-LANE UNDIVIDED, AND ONE-WAY ROADWAY SHALL BE ACCORDING TO DISTRICT STANDARD 41.1.

44. PERMANENT SURVEY MARKERS, TYPE II, SHALL BE SET AT INTERVALS OF 1 MILE OR AS DIRECTED BY THE ENGINEER. BRIDGE OR CULVERT PROJECTS SHALL HAVE ONE SURVEY MARKER PLACED NEAR THE STRUCTURE. ESTIMATED: 4 EACH.

45. PERMANENT SURVEY MARKERS, TYPE II PLACED IN URBAN AREAS SHOULD BE PLACED IN SIDEWALK AREAS. THE MARKER SHALL BE PLACED AS SHOWN ON DISTRICT STANDARD 66.2. THE PERMANENT SURVEY MARKER SHALL BE PLACED FLUSH WITH THE TOP OF THE SURFACE OF THE MATERIAL IN WHICH IT IS PLACED.

46. PERMANENT SURVEY MARKERS, TYPE II SHALL BE CAST-IN-PLACE AS SHOWN ON DISTRICT STANDARD 66.2 OR ANOTHER OPTION WOULD BE TO INSTALL A VAULTED STYLE MONUMENT AS DESCRIBED BY NGS AS A 3D MONUMENT (TOP SECURITY SLEEVE ROD MONUMENT), WITH INSTALLATION INSTRUCTIONS PROVIDED BY THE DISTRICT CHIEF OF SURVEYS. IF POURED IN PLACE, THE BOTTOM OF THE MARKER SHALL BE 5'-0" BELOW THE GROUND SURFACE.

47. THE PERMANENT SURVEY MARKERS, IF POSSIBLE, SHALL BE INSTALLED AT THE BEGINNING OF THE JOB AND PROTECTED THROUGHOUT.

48. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A DESCRIPTION OF LOCATION, ELEVATION, AND COORDINATES FOR EACH PERMANENT SURVEY MARKER. THE HORIZONTAL COORDINATES MUST BE DERIVED BY GPS AND THE ELEVATION DERIVED USING AN ELECTRONIC LEVEL. THE META DATA, SUCH AS THE GEOID USED, (NGS ADJUSTMENT IE: 97 HARN, 03, 07), AND THE BASE POINT(S) NAME OR NUMBER SHALL BE SUBMITTED ALONG WITH A COMPLETE COLLECTION LOG. IF COLLECTED USING RTK METHOD, IT WILL REQUIRE EITHER 3 COLLECTIONS (AVERAGED) FROM 2 DIFFERENT BASES, OR A MINIMUM OF 3 COLLECTIONS (AVERAGED), AT LEAST 2 HOURS APART, FROM THE SAME BASE. IF USING A CORS TYPE NETWORK, THE COLLECTION PROCEDURE SHALL INCLUDE LOCALIZING WITH CHECK SHOTS ON AT LEAST 2 DIFFERENT HARN MONUMENTS BOTH BEFORE AND AFTER COLLECTION. THE LEVEL CIRCUIT SHALL BE RUN FROM FURNISHED MARK TO FURNISHED MARK AND THEN ADJUSTED. THE ERROR OF CLOSURE SHALL BE SUBMITTED WITH THE ELECTRONIC LEVEL NOTES IN A RECOGNIZED FORMAT APPROVED BY THE ENGINEER AND/OR THE CHIEF OF SURVEYS. THE ENGINEER SHALL SUBMIT THIS INFORMATION TO THE DISTRICT CHIEF OF SURVEYS.

49. THE CONTRACTOR SHALL BEGIN FENCE ERECTION AS SOON AS CLEARING OPERATIONS PERMIT. THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER FOOT FOR CHAIN LINK FENCE, 4'.

50. TREE PLANTING LAYOUT SHALL BE PERFORMED BY THE DISTRICT ROADSIDE MANAGEMENT SPECIALIST. MULCH SHALL BE PLACED 4" THICK AND TO THE DIAMETER AROUND THE TREE AS SHOWN ON DISTRICT STANDARD 92.1. THE MULCH SHALL BE HARDWOOD WOOD CHIPS PLACED ON WEED BARRIER FABRIC. THIS WORK SHALL BE INCLUDED IN THE COST OF THE TREE.

51. EXCESS TREES THAT CANNOT BE PLANTED ALONG THE PROJECT LIMITS SHALL BE PLANTED AT ALTERNATIVE LOCATIONS AS DETERMINED BY THE DISTRICT ROADSIDE MANAGEMENT SPECIALIST.

52. ALL GUTTER OUTLETS SHALL BE EXTENDED TO DITCH FLOW AS DIRECTED BY THE ENGINEER.

53. RIGHT-OF-WAY MARKERS WILL BE ERECTED PER HIGHWAY STANDARD 666001 WITH THE BACK FACE OF THE MARKER ON THE RIGHT-OF-WAY LINE, UNLESS THE NEW RIGHT-OF-WAY LINE HAS BEEN SURVEYED AND PINNED, IN WHICH INSTANCE THE RIGHT-OF-WAY MARKERS WILL BE ERECTED 12 INCHES INSIDE THE NEW RIGHT-OF-WAY LINE. THE METHOD OF INSTALLATION SHALL BE APPROVED BY THE ENGINEER.

54. WORK ON THIS PROJECT WILL BE IN PROGRESS AT THE SAME TIME AS WORK ON THE FOLLOWING CONTRACTS:

Project	Type of Work
64C08	Roadway Reconstruction
IM-NHS-074-1(197)5--03-82	Bridge – WPG
IM-NHS-074-1(198)5--03-82	Bridge - WPG
IM-NHS-074-1(199)5--03-82	Bridge-Unspecified
IM-NHS-074-1(206)5--03-82	Pavement - Grade & Replace
IM-NHS-074-1(200)5--03-82	Bridge, New - Steel Girder
ITS-074-1(221)5--05-82	ITS Deployment and Integration
ITS-074-1(222)5--05-82	Fiber Optics
IMN-74-1(208)5--0E-82	Lighting
IMN-74-1(209)5--0E-82	Lighting
IMN-74-1(235)5--0E-82	Lighting

WORK ON THESE PROJECTS SHALL BE SCHEDULED TO KEEP INTERFERENCE BETWEEN ALL THE PROJECTS TO A MINIMUM. THE CONTRACTORS SHALL INFORM EACH OTHER OF PROGRESS OF THE PROJECTS AND GIVE FAIR WARNING TO THE OTHER CONTRACTORS WHEN A PROBLEM MIGHT BE ENCOUNTERED.

55. ANY SUBCONTRACTOR CHOSEN TO DO UNDERGROUND STORAGE TANK REMOVAL AND/OR SPECIAL OR HAZARDOUS WASTE MANAGEMENT MUST BE ON THE STATE FIRE MARSHALL'S CURRENTLY APPROVED LIST OF QUALIFIED CONTRACTORS TO DO SUCH WORK. PRIOR TO ANY INVOLVEMENT WITH SPECIAL OR HAZARDOUS WASTE, THE PRIME CONTRACTOR SHALL NOTIFY THE DISTRICT ENVIRONMENT UNIT HAZARDOUS WASTE COORDINATOR WHO THIS DESIGNATED SUB-CONTRACTOR IS AND FURNISH FIVE PROJECTS THIS SUB-CONTRACTOR HAS SUCCESSFULLY CONCLUDED, INCLUDING THE IEPA INCIDENT NUMBER. THE DISTRICT WILL THEN CONFIRM THE SUCCESSFUL CONCLUSION OF THESE PROJECTS BY REVIEWING THE IEPA DATA BASE. ONLY AFTER APPROVAL FROM THE DISTRICT ENVIRONMENT UNIT WILL THE SUB-CONTRACTOR BE AUTHORIZED TO PROCEED WITH ANY INVOLVEMENT WITH SPECIAL/HAZARDOUS WASTE.

56. COHESIVE SOIL USED TO BACKFILL UNDERGROUND STORAGE TANKS, OUTSIDE THE LIMITS OF THE ROADWAY, SHALL BE PLACED AT A MOISTURE CONTENT OF NO MORE THAN 110% OF OPTIMUM, AND COMPACTED TO 95% OF THE STANDARD DRY DENSITY.

57. BACKFILL PLUGS REQUIRED UNDER ARTICLE 669.09 GROUNDWATER MANAGEMENT SHALL BE CONSTRUCTED OF CONCRETE WHEN WITHIN THE FOLLOWING LIMITS: ALL TRENCHES MADE IN THE SUBGRADE OF THE PROPOSED IMPROVEMENT, AND ALL TRENCHES OUTSIDE OF THE SUBGRADE WHERE THE INNER EDGE OF THE TRENCH IS CLOSER THAN 2 FEET TO THE EDGE OF THE PROPOSED PAVEMENT, STABILIZED SHOULDER, CURB OR SIDEWALK.

58. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.39 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123. THE FOLLOWING LISTED UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS ARE MEMBERS OF JULIE:

AT&T	(309) 757-4707
CENTURYLINK	(563) 355-6402
CITY OF MOLINE	(309) 524-2368
MIDAMERICAN ENERGY COMPANY - ELECTRIC	(309) 793-3696
MIDAMERICAN ENERGY HIGH VOLTAGE	(563) 333-8186
MIDAMERICAN ENERGY COMPANY - GAS	(309) 793-3760
KONE INC	(309) 949-1108
MEDIACOM	(309) 743-4750
MCI	(972) 729-6322
WINDSTREAM	(630) 925-4751

IDOT IS NOT A MEMBER OF JULIE. IF YOU ARE NEAR ANY OVERHEAD LIGHTING, INTERSECTION LIGHTING OR TRAFFIC SIGNALS, CONTACT THE IDOT TRAFFIC OFFICE AT 815/284-5469 AT LEAST 48 HOURS PRIOR TO WORK.

59. TIE BARS SHALL BE INSTALLED TO TIE PCC APPURTENANCE TO ADJACENT EXISTING CONCRETE PAVEMENT.

TIE THE FOLLOWING TO THE EXISTING CONCRETE PAVEMENT		LENGTH, SIZE, AND SPACING OF TIE BARS
GUTTER OR CURB & GUTTER	STD. 606001	24" LONG NO. 6 @ 24" CENTERS
PCC BASE COURSE	STD. 353001	24" LONG NO. 6 @ 30" CENTERS
PCC PAVEMENT	STD. 420101	24" LONG NO. 6 @ 30" CENTERS

TIE BARS TO BE INSTALLED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 420.05(B) OF THE STANDARD SPECIFICATIONS. SEE HIGHWAY STANDARD 420001 FOR DETAIL ON LONGITUDINAL CONSTRUCTION JOINT GROUTED-IN-PLACE TIE BAR. THE COST OF THE TIE BARS TO BE INCLUDED IN THE COST OF THE PCC APPURTENANCE ADJACENT TO THE EXISTING PAVEMENT.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
SHEET 3 OF 4

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)IR-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	6

CONTRACT NO. 64E26
ILLINOIS FED. AID PROJECT

GENERAL NOTES

60. CADD DATA WILL BE AVAILABLE TO CONTRACTORS AND CONSULTANTS WORKING ON THIS PROJECT. THIS INFORMATION WILL BE PROVIDED UPON REQUEST AS MICROSTATION CADD FILES AND GEOPAK COORDINATE GEOMETRY FILES ONLY. IF DATA IS REQUIRED IN OTHER FORMATS IT WILL BE YOUR RESPONSIBILITY TO MAKE THESE CONVERSIONS. IF ANY DISCREPANCY OR INCONSISTENCY ARISES BETWEEN THE ELECTRONIC DATA AND THE INFORMATION ON THE HARD COPY, THE INFORMATION ON THE HARD COPY SHOULD BE USED. CONTACT THE DISTRICT'S PROJECT ENGINEER TO REQUEST THESE FILES.

61. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE MUNICIPALITY TO DETERMINE APPROVED METHODS OF UTILITY STRUCTURE ADJUSTMENT. UTILITY STRUCTURES MAY INCLUDE, BUT ARE NOT LIMITED TO, MANHOLES, WATER VALVES, HANDHOLES, ETC. ALL MATERIALS AND WORK NECESSARY TO COMPLETE ADJUSTMENTS PER MUNICIPALITY REQUIREMENTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED ADJUSTMENT PAY ITEM.

62. RELOCATE TEMPORARY IMPACT ATTENUATORS SHALL INCLUDE STORAGE AND TRASNPORTATION TO AND FROM STORAGE, WHEN THE DEVICE IS NOT NEEDED FOR A TIME, AS SHOWN ON THE STAGING PLANS. THIS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR IMPACT ATTENUATORS, RELOCATE OF THE TYPE SPECIFIED.

63. WHEN RELOCATE TEMPORARY CONCRETE BARRIER IS SPECIFIED, THE WALL SHALL BE REMOVED, STORAGE AND TRANSPORTATION TO AND FROM STORAGE, WHEN THE WALL IS NOT NEEDED FOR A TIME AS SHOWN ON THE STAGING PLANS, RELOCATED AND REINSTATED AT THE NEW LOCATION. THE INSTALLATION REQUIREMENTS SHALL BE THE SAME AS THOSE FOR A NEW INSTALLATION. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR RELOCATE TEMPORARY CONCRETE BARRIER.

64. CONTRACTOR COORDINATION REQUIREMENTS AT SOUTH ABUTMENT OF STRUCTURE 081-0177 AND 081-0178: CONTRACTOR (RESPONSIBLE FOR CONSTRUCTION OF SN 081-6014) SHALL COORDINATE WITH "BRIDGE CONTRACTOR" (RESPONSIBLE FOR CONSTRUCTION OF SN 081-0178). CONSTRUCTION AT THE SOUTH ABUTMENT SHALL FOLLOW THE STEPS OUTLINED BELOW:

    A. CONTRACTOR SHALL CONSTRUCT AGGREGATE COLUMN GROUND IMPROVEMENT BENEATH SOUTH ABUTMENT. AGGREGATE COLUMNS WILL BE LOCATED SUCH THAT THEY DO NOT INTERFERE WITH THE PROPOSED PILE LOCATIONS.

    B. "BRIDGE CONTRACTOR" SHALL DRIVE PILES FOR THE SOUTH ABUTMENT OF THE VIADUCT STRUCTURE.

    C. CONTRACTOR SHALL CONSTRUCT MSE WALL AND PLACE BACKFILL UP TO THE ELEVATION OF THE BOTTOM OF ABUTMENT.

    D. "BRIDGE CONTRACTOR" SHALL CONSTRUCT THE ABUTMENT, WINGWALLS AND MASKWALLS.

    E. CONTRACTOR SHALL THEN COMPLETE CONSTRUCTION OF THE MSE WALLS AND PLACEMENT OF BACKFILL. "BRIDGE CONTRACTOR IS THEN RESPONSIBLE FOR THE SOUTH APPROACH PAVEMENT OF THE VIADUCT.

    F. THESE STEPS SHALL BE REPEATED FOR CONSTRUCTION OF THE EASTBOUND PORTION OF I-74.

65. THE PROJECT SOILS REPORT RECOMMENDS THE USE OF SETTLEMENT PLATFORMS TO OBSERVE AND DETERMINE THE MAGNITUDE AND RATE OF EMBANKMENT SETTLEMENT. THE DETERMINATION OF THE TIME AT WHICH THE NECESSARY CONSOLIDATION HAS TAKEN PLACE AND WHEN THE EMBANKMENT MAY BE RELEASED FOR ADDITIONAL LIFTS OF FILL OR THE NEXT STAGES OF CONSTRUCTION WILL BE DETERMINED BY THE ENGINEER ON THE BASIS OF THE DATA OBTAINED FROM THE COMBINED SETTLEMENT MONITORING INSTRUMENTATION. SETTLEMENT PLATFORMS, IN ACCORDANCE WITH ARTICLE 204 OF THE STANDARD SPECIFICATIONS, SHALL BE INSTALLED AT THE FOLLOWING LOCATIONS:

EMBANKMENT LOCATIONS		
ALIGNMENT	STATION	OFFSET (FT)
I-74	53+50	55 LT
I-74	50+00	55 RT
I-74	60+00	90 RT
RAMP 7TH-A	627+30	35 LT

ACGI LOCATIONS		
ALIGNMENT	STATION	OFFSET (FT)
I-74	48+90	40 LT
I-74	49+20	50 RT
I-74	61+05	20 RT
I-74	62+15	40 LT
I-74	70+85	10 LT
I-74	71+80	10 LT

SETTLEMENT PLATFORMS INSTALLED WITHIN AGGREGATE COLUMN GROUND IMPROVEMENT (ACGI) TREATMENT AREAS MAY BE USED BY THE ACGI SUBCONTRACTOR TO PARTIALLY SATISFY THE VERIFICATION REQUIREMENTS OF GUIDE BRIDGE SPECIAL PROVISION 71; HOWEVER, ADDITIONAL SETTLEMENT MONITORING POINTS ON THE FACE OF THE MSE RETAINING WALLS WILL BE NEEDED.

SETTLEMENT PLATFORMS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCLUDED IN THE COST BID FOR EARTH EXCAVATION.

EMBANKMENT RESTING PERIODS ARE REQUIRED AT THE FOLLOWING LOCATIONS. CONTRACTOR SHALL SEQUENCE WORK TO ALLOW SETTLEMENT TO OCCUR PRIOR TO PAVING.

ALIGNMENT	RESTING PERIOD
I-74	9 MONTHS
RAMP 7TH-B	4 MONTHS

SEE STRUCTURE PLANS FOR ADDITIONAL SETTLEMENT TIME RESTRICTIONS IN AREAS WITH ACGI TREATMENT.

66. UTILITY NOTE: THE LOCATIONS OF THE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATION USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAIN THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

67. THE CONTRACTOR SHALL BE REQUIRED TO COMPLETE CONSTRUCTION ACTIVITIES DURING THE WINTER STAGE AS SHOWN IN THE PLANS IN ORDER TO ACHIEVE THE COMPLETION DATES SPECIFIED. THIS WORK WILL BE INCLUDED IN THE CONTRACT COST FOR THE ASSOCIATED ITEMS. NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR WORK PERFORMED DURING THE WINTER STAGE

68. CONTRACTOR SHALL BE RESPONSIBLE FOR RECONSTRUCTING THE GROUND AREAS WHERE STRUCTURE HAS BEEN REMOVED AND AREAS THAT BECOME DAMAGED DURING THE REMOVAL OPERATION. AT THE DIRECTION OF THE ENGINEER, THE GROUND AREAS SHALL BE REESTABLISHED IN KIND WITH THE AREA IMMEDIATELY ADJACENT TO THE REMOVAL AREA. COST SHALL BE INCLUDED WITH THE "REMOVAL OF EXISTING STRUCTURES" OF THE NUMBER SPECIFIED.

69. THE CITY OF MOLINE AND UTILITY COMPANIES WILL BE RELOCATING UTILITIES PRIOR TO THE START OF CONSTRUCTION OF THIS CONTRACT TO AVOID CONFLICTS WITH THE PROPOSED IMPROVEMENTS. THESE RELOCATIONS ARE UNDER A SEPARATE CONTRACT AND WERE NOT AVAILABLE AT THE TIME OF LETTING. THEREFORE, THE UTILITIES SHOWN IN THE PLANS MAY NOT ILLUSTRATE THE MOST CURRENT CONFIGURATIONS. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE CITY OF MOLINE, THE UTILITY COMPANIES, AND/OR THE ENGINEER TO DETERMINE THE LOCATION OF THE REVISED UTILITIES.

70. ALL CONCRETE FOR THE C.I.P RETAINING WALLS WITH A FORM LINER TEXTURED SURFACE SHALL BE SELF-CONSOLIDATING CONCRETE MEETING THE REQUIREMENTS OF SECTION 1020 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONCRETE USED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LAYOUT	CBP	7/22/2011
DRAWN	CBP	3/20/2017
REVIEWED	AAP	3/22/2017

GEN-04

FILE NAME = 02CONCD-H2S-sht-gennote15BM.dgn	USER NAME = perke29954	DESIGNED - MTH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES SHEET 4 OF 4				F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - CBP	REVISED -						74	(81-1)R-1 & 81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	7
	PLOT SCALE =	CHECKED - AAP	REVISED -									CONTRACT NO. 64E26	
	PLOT DATE = 5/2/2017	DATE - 3/23/2017	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.			ILLINOIS	FED. AID PROJECT	

TOPIC: BALD EAGLES

COMMITMENT: PRIOR TO CONSTRUCTION, THE AREA WILL BE SURVEYED TO ACCURATELY IDENTIFY BALD EAGLE NEST SITES.

ENVIRONMENTAL CONTACT: JILL RUDLOFF, IA DOT

TOPIC: HOME AND BUSINESS ACCESS

COMMITMENT: DURING THE FINAL DESIGN AND CONSTRUCTION OF THE PROPOSED ITEMS ON LOCAL ROADS, MINIMIZING IMPACTS ON EXISTING BUSINESSES THAT UTILIZE LOADING DOCKS, ALLEYS AND DRIVEWAYS SHALL BE EXERCISED.

TOPIC: PRIVATE PROPERTY STRUCTURE

COMMITMENT: THE CONCRETE BLOCK STRUCTURE LOCATED AT I-74 MAINLINE STATION 69+45, 106' LT, SHALL NOT BE DISTURBED DURING CONSTRUCTION. ANY DAMAGE TO THIS STRUCTURE OR ITS CONTENTS RESULTING FROM THE CONTRACTOR'S ACTIVITIES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND NO ADDITIONAL PAYMENT WILL BE MADE FOR ANY NECESSARY REPAIR OR REPLACEMENT.

TOPIC: FENCE REPAIR

COMMITMENT: THE EXISTING FENCE ALONG THE PROPERTY AT 1890 14TH AVENUE LOCATED BETWEEN STATION 79+33.34, 96.5' RT AND 81+10.41, 184.6' RT SHALL ONLY HAVE THE FENCE FABRIC REPLACED. ALL WORK SHALL BE PERFORMED WITHIN THE EXISTING ROW AT THIS LOCATION AND SHALL BE PAID FOR AS CHAIN LINK FABRIC, TYPE I, 4'-0".

TOPIC: AMERICAN RENTAL ASSOCIATION TEMPORARY ACCESS DRIVEWAY (1900 19TH ST)

COMMITMENT: DURING THE CLOSURE OF 19TH STREET, TEMPORARY ACCESS FOR THIS PROPERTY WILL BE PROVIDED OFF OF 23RD STREET AS SHOWN IN THE STAGING PLANS. ONCE CONSTRUCTION IS COMPLETE AND 19TH STREET IS OPENED TO TRAFFIC, THE TEMPORARY ACCESS TO 23RD STREET WILL BE REMOVED. THIS AREA WILL BE SODDED AND HOT-MIX ASPHALT CURB (SPECIAL) WILL BE PLACED TO MATCH THE EXISTING PARKING LOT EDGE. TREES WILL NOT BE REPLACED.

LAYOUT	CBP	7/22/2011
DRAWN	CBP	3/20/2017
REVIEWED	AAP	3/22/2017

FILE NAME = D2CONCD-HPS-sht-gennote03M.dgn	USER NAME = hehn21663	DESIGNED - MTH	REVISED -
		DRAWN - CBP	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

COMMITMENTS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

CMT-01

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-IR-1 & 8)-IOHBR, HBR-1, HBR-2)	ROCK ISLAND	2042	8
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	1274	1274																	
	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	212	212																	
	20100500	TREE REMOVAL, ACRES	ACRE	2.75	2.75																	
	20200100	EARTH EXCAVATION	CU YD	180,520	127,000	53,520																
*	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100	100																	
	20700220	POROUS GRANULAR EMBANKMENT	CU YD	142	24	118																
	20800150	TRENCH BACKFILL	CU YD	17,915	12,684	5161																70
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	115,907	74,827	41,080																
	25000210	SEEDING, CLASS 2A	ACRE	21.00	14.75	6.25																
	25000310	SEEDING, CLASS 4	ACRE	6.50	3.90	2.60																
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2485	1544	941																
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2485	1544	941																
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2485	1544	941																
**	25000750	MOWING	ACRE	21.00	14.75	6.25																

CONSTRUCTION CODE

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** NON-PARTICIPATING ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 1 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	9
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																		
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				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
25100125	MULCH, METHOD 3	ACRE	36.50	21.75	14.75																
25100630	EROSION CONTROL BLANKET	SQ YD	119,254	76,812	42,442																
25100900	TURF REINFORCEMENT MAT	SQ YD	13,875	12,754	1121																
25200100	SODDING	SQ YD	714		714																
25200200	SUPPLEMENTAL WATERING	UNIT	6.4		6.4																
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	109,500	65,390	44,110																
28000305	TEMPORARY DITCH CHECKS	FOOT	3090	1970	1120																
28000400	PERIMETER EROSION BARRIER	FOOT	13,702	10,528	3174																
28000500	INLET AND PIPE PROTECTION	EACH	24	11	13																
28000510	INLET FILTERS	EACH	234	128	106																
28100107	STONE RIPRAP, CLASS A4	SQ YD	69																69		
28200200	FILTER FABRIC	SQ YD	10,708	10,595	44														69		
28500200	PRECAST BLOCK REVETMENT MAT	SQ YD	574	531	43																
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	16		16																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 2 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	10
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																		
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				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
30300011	AGGREGATE SUBGRADE IMPROVEMENT	TON	2000		2000																
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	72,573		72,479						39	55									
31100800	SUBBASE GRANULAR MATERIAL, TYPE A 9"	SQ YD	2095	1665	430																
31200100	STABILIZED SUBBASE 4"	SQ YD	206,231	136,512	69,631						39	49									
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	13,583	6567	7016																
40600925	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	199		199																
40600845	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N90	TON	447	447																	
40603535	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1112		1112																
40603570	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90	TON	724	724																	
42000060	WELDED WIRE REINFORCEMENT	SQ YD	220		220																
42000080	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB	SQ YD	4008	3817	191																
42000406	PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)	SQ YD	36,000		36,000																
42000511	PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)	SQ YD	154,317	133,263	21,054																
42001300	PROTECTIVE COAT	SQ YD	394,692	274,161	120,531																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 3 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	11
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	635		635																
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	31,822.0										31,822.0								
42400800	DETECTABLE WARNINGS	SQ FT	351										351								
44000100	PAVEMENT REMOVAL	SQ YD	108,671	60,943	47,728																
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	10,890		10,890																
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	7519	7324	195																
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	2387		2387																
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	202		202																
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	18,126		17,849						135	142									
44000600	SIDEWALK REMOVAL	SQ FT	28,480		28,480																
44001980	CONCRETE BARRIER REMOVAL	FOOT	914										914								
44003100	MEDIAN REMOVAL	SQ FT	10,707		10,707																
44004000	PAVED DITCH REMOVAL	FOOT	3022	2995	27																
44004250	PAVED SHOULDER REMOVAL	SQ YD	40,055	34,518	5537																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 4 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	12
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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				CONSTRUCTION CODE																		
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	143		143																
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	130		130																
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	135		135																
44201294	CLASS B PATCH - EXPANSION JOINT	FOOT	232		232																
44201296	DEFORMED BARS - EXPANSION JOINT	EACH	238		238																
44201299	DOWEL BARS 1 1/2"	EACH	500		500																
44213200	SAW CUTS	FOOT	1782		1782																
44213204	TIE BARS 3/4"	EACH	50		50																
45200300	JOINT OR CRACK FILLING	POUND	10																10		
48100500	AGGREGATE SHOULDERS, TYPE A 6"	SQ YD	4757	2663	2094																
48203009	HOT-MIX ASPHALT SHOULDERS, 3"	SQ YD	2095	1665	430																
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	3			1	2														
50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1					1													
50100500	REMOVAL OF EXISTING STRUCTURES NO. 3	EACH	2						1	1											

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 5 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	13
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
				88.5% FEDERAL 11.5% STATE ROADWAY 0003 URBAN	88.5% FEDERAL 11.5% STATE ROADWAY 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0179 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0180 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0181 0011 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0182 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0183 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0184 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0185 0010 URBAN	88.5% FEDERAL 11.5% STATE SAFETY 0021 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6013 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6014 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6015 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6016 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6017 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6020 0004 URBAN	88.5% FEDERAL 11.5% STATE STRUCTURE 0004 URBAN	100% MOLINE UTILITIES 0043 URBAN
50100600	REMOVAL OF EXISTING STRUCTURES NO. 4	EACH	1								0.5	0.5									
50102400	CONCRETE REMOVAL	CU YD	606.0															606.0			
50104400	CONCRETE HEADWALL REMOVAL	EACH	15	14	1																
50104650	SLOPE WALL REMOVAL	SQ YD	910																	910	
50105220	PIPE CULVERT REMOVAL	FOOT	179	179																	
50157300	PROTECTIVE SHIELD	SQ YD	12,356			2682	5023	1302	458	616	1268	1007									
50200100	STRUCTURE EXCAVATION	CU YD	24,166	31	153	708	1494	1227			1858	1399		497	2941	1936	4766	2882	1870	2351	53
50200450	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	CU YD	1070													432	638				
50300225	CONCRETE STRUCTURES	CU YD	5222.7			510.7	697.3	669.8	228.4	233.0	896.7	778.9		379.2						792.0	36.7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	7833.5			906.0	961.1	616.9	417.5	415.3	875.5	750.7			469.3	532.8	215.5	156.1	20.4	1496.4	
50300260	BRIDGE DECK GROOVING	SQ YD	14,339			2778	2938	1737	1103	1104	2552	2127									
50300285	FORM LINER TEXTURED SURFACE	SQ FT	12,950											4755	2527			1831	2423	1414	
50300300	PROTECTIVE COAT	SQ YD	22,584			3222	3374	2093	1225	1224	2965	2526			1060	1205	491	353	45	2801	
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1			0.24	0.26	0.15	0.05	0.05	0.13	0.12									

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 6 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	14
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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				CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
50500505	STUD SHEAR CONNECTORS	EACH	67,995			10,848	11,544	5148	4104	4104	13,935	12,438		1894						3980	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2,584,330			340,680	410,110	305,860	128,150	127,780	343,320	300,100		37,660	73,680	84,580	33,090	24,440	3010	368,820	3050
50800515	BAR SPLICERS	EACH	1246			128	140	103	176	178	281	240									
50800530	MECHANICAL SPLICERS	EACH	738			240	258	240													
51100100	SLOPE WALL 4 INCH	SQ YD	6540			1282	1364	798	140	144	1173	970								669	
51200957	FURNISHING METAL SHELL PILES 12" X 0.250"	FOOT	10,232								5398	4834									
51201400	FURNISHING STEEL PILES HP10X42	FOOT	587				99	166	158	164											
51201610	FURNISHING STEEL PILES HP12X63	FOOT	3492			1974		1518													
51201700	FURNISHING STEEL PILES HP12X74	FOOT	2473				2242														231
51201800	FURNISHING STEEL PILES HP14X73	FOOT	5480						2580	2900											
51202000	FURNISHING STEEL PILES HP14X102	FOOT	6836			2316	2336	2184													
* 51202305	DRIVING PILES	FOOT	29,100			4290	4677	3868	2738	3064	5398	4834									231
51203200	TEST PILE METAL SHELLS	EACH	4								4										
51203610	TEST PILE STEEL HP12X63	EACH	2			2															

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 7 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	15
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
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				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
51203700	TEST PILE STEEL HP12X74	EACH	3				1														2
51203800	TEST PILE STEEL HP14X73	EACH	4						2	2											
51204000	TEST PILE STEEL HP14X102	EACH	1			1															
51204650	PILE SHOES	EACH	214								112	94									8
51500100	NAME PLATES	EACH	17			1	1	1	1	1	1	1		1	1	1	1	1	1	4	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	1136.0			124.0	124.0	88.0	156.0	156.0	265.0	223.0									
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	78			8	8	6	9	9	20	18									
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	41			8	8	6			10	9									
52100510	ANCHOR BOLTS, 3/4"	EACH	100								52	48									
52100515	ANCHOR BOLTS, 7/8"	EACH	24																		24
52100520	ANCHOR BOLTS, 1"	EACH	264			96	96	72													
52100530	ANCHOR BOLTS, 1 1/4"	EACH	96						36	36	12	12									
52200010	TEMPORARY SHEET PILING	SQ FT	2450			1746	341				363										
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	1980			899	118		963												

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 8 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	16
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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				CONSTRUCTION CODE																		
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	00 04	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
52200100	FURNISHING SOLDIER PILES (HP SECTION)	FOOT	8821											1219						7602	
52200105	FURNISHING SOLDIER PILES (W SECTION)	FOOT	2617											2185						432	
52200200	DRILLING AND SETTING SOLDIER PILES (IN SOIL)	CU FT	35,961											10,322						25,639	
52200250	UNTREATED TIMBER LAGGING	SQ FT	16,958											4563						12,395	
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	3	3																	
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	5	5																	
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	1	1																	
54213681	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"	EACH	1	1																	
54244805	INLET BOX, STANDARD 542501	EACH	1	1																	
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	4037	2518	1519																
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	1697	1171	526																
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	2853	2638	138																77
550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	1087	1087																	
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	1669	939	730																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 9 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	17
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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				CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
550A0420	STORM SEWERS, CLASS A, TYPE 2 27"	FOOT	529	529																	
550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	172	96	76																
550A0450	STORM SEWERS, CLASS A, TYPE 2 36"	FOOT	1264	1264																	
550A0470	STORM SEWERS, CLASS A, TYPE 2 42"	FOOT	311	311																	
550A0480	STORM SEWERS, CLASS A, TYPE 2 48"	FOOT	118	118																	
550A0640	STORM SEWERS, CLASS A, TYPE 3 12"	FOOT	26	26																	
550A0660	STORM SEWERS, CLASS A, TYPE 3 15"	FOOT	52		52																
550A0680	STORM SEWERS, CLASS A, TYPE 3 18"	FOOT	374	354	20																
550A0710	STORM SEWERS, CLASS A, TYPE 3 24"	FOOT	401	237	164																
550A0730	STORM SEWERS, CLASS A, TYPE 3 30"	FOOT	36	36																	
550A0750	STORM SEWERS, CLASS A, TYPE 3 36"	FOOT	232	232																	
550A0980	STORM SEWERS, CLASS A, TYPE 4 18"	FOOT	130	130																	
550A1010	STORM SEWERS, CLASS A, TYPE 4 24"	FOOT	161	161																	
550A1300	STORM SEWERS, CLASS A, TYPE 5 30"	FOOT	209	125	84																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 10 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	18
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE: 88.5% FEDERAL 11.5% STATE ROADWAY 0003 URBAN	88.5% FEDERAL 11.5% STATE ROADWAY 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0179 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0180 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0181 0011 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0182 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0183 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0184 0010 URBAN	88.5% FEDERAL 11.5% STATE SN 081-0185 0010 URBAN	88.5% FEDERAL 11.5% STATE SAFETY 0021 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6013 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6014 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6015 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6016 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6017 0004 URBAN	88.5% FEDERAL 11.5% STATE SN 081-6020 0004 URBAN	88.5% FEDERAL 11.5% STATE STRUCTURE 0004 URBAN	100% MOLINE UTILITIES 0043 URBAN
550A1580	STORM SEWERS, CLASS A, TYPE 6 24"	FOOT	47		47																
55100300	STORM SEWER REMOVAL 8"	FOOT	114	59	55																
55100500	STORM SEWER REMOVAL 12"	FOOT	2567	1849	718																
55100700	STORM SEWER REMOVAL 15"	FOOT	121	121																	
55100900	STORM SEWER REMOVAL 18"	FOOT	896	625	194																77
55101200	STORM SEWER REMOVAL 24"	FOOT	1414	818	596																
55101400	STORM SEWER REMOVAL 30"	FOOT	347	347																	
55101600	STORM SEWER REMOVAL 36"	FOOT	284	284																	
55101800	STORM SEWER REMOVAL 42"	FOOT	120	120																	
55102300	STORM SEWER REMOVAL 72"	FOOT	276		276																
55200900	STORM SEWERS JACKED IN PLACE, 24"	FOOT	346																		346
55201100	STORM SEWERS JACKED IN PLACE, 30"	FOOT	87	87																	
58700300	CONCRETE SEALER	SQ FT	40,015			5004	5613	5624	1827	1847	10,188	9004								908	
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	1960								206	172		354						1228	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 11 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)BR-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	19
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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5/5/2017

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	00-04	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	65	61	4																
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	1852	1793	59																
60108206	PIPE UNDERDRAINS, TYPE 2, 6"	FOOT	63,113	43,826	19,058						135	94									
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	35	21	14																
60218600	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 4 FRAME AND GRATE	EACH	9	9																	
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	3	3																	
60219510	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	28	16	12																
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	10	5	5																
60221700	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 8 GRATE	EACH	3	1	2																
60222210	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	2	1	1																
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	6	1	5																
60224035	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	4	2	2																
60224440	MANHOLES, TYPE A, 7'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	2	2																	
60224446	MANHOLES, TYPE A, 7'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	3	2																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 12 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	20
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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				CONSTRUCTION CODE																		
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
60224448	MANHOLES, TYPE A, 7'-DIAMETER, TYPE 8 GRATE	EACH	1		1																
60224464	MANHOLES, TYPE A, 8'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	1	1																	
60240301	INLETS, TYPE B, TYPE 8 GRATE	EACH	8	2	6																
60240305	INLETS, TYPE B, TYPE 10 FRAME AND GRATE	EACH	2	1	1																
60240324	INLETS, TYPE B, TYPE 20 FRAME AND GRATE	EACH	65	27	38																
60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	5	5																	
60247160	DRAINAGE STRUCTURES, TYPE 1, WITH TWO TYPE 20 FRAMES AND GRATES	EACH	21	21																	
60247170	DRAINAGE STRUCTURES, TYPE 2, WITH TWO TYPE 22 FRAMES AND GRATES	EACH	5	5																	
60255500	MANHOLES TO BE ADJUSTED	EACH	55		55																
60256910	MANHOLES TO BE ADJUSTED WITH NEW TYPE 20 FRAME AND GRATE	EACH	2		2																
60260100	INLETS TO BE ADJUSTED	EACH	42		42																
60270050	DRAINAGE STRUCTURES, TYPE 4 WITH TWO TYPE 20 FRAME AND GRATES	EACH	5	5																	
60500040	REMOVING MANHOLES	EACH	62	43	19																
60500050	REMOVING CATCH BASINS	EACH	9	9																	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 13 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	21
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
60500060	REMOVING INLETS	EACH	19	11	8																
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	38.5	5.1	33.4																
60600605	CONCRETE CURB, TYPE B	FOOT	135.0	90.0	45.0																
60602800	CONCRETE GUTTER, TYPE B	FOOT	1149.0		1149.0																
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	250.0		250.0																
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	17,759.0	142.0	17,340.0						135.0	142.0									
60607400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.24	FOOT	93.5		93.5																
60608250	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06	FOOT	2574.0	2574.0																	
60608582	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	789.0	789.0																	
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	854.0		854.0																
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	12,648		12,648																
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	2264		2264																
60620000	CONCRETE MEDIAN, TYPE SB-6.24	SQ FT	2370		2370																
60624600	CORRUGATED MEDIAN	SQ FT	7990		7990																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 14 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	22
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
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					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0084	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	6362.5										6362.5								
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	4										4								
*	63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	10										10								
*	63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	11										11								
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	5										5								
	63200310	GUARDRAIL REMOVAL	FOOT	10,577										10,577								
	63500105	DELINEATORS	EACH	117										117								
	63700275	CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT	FOOT	4661										4661								
	63700900	CONCRETE BARRIER BASE	FOOT	6160										6160								
	64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	34,185										34,185								
	64300260	IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	3										3								
	64301090	ATTENUATOR BASE	SQ YD	28										28								
*	64401100	HIGH TENSION CABLE MEDIAN BARRIER	FOOT	317										317								
*	64401300	HIGH TENSION CABLE MEDIAN BARRIER TERMINALS	EACH	2										2								

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 15 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & (81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	23
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
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					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	66400105	CHAIN LINK FENCE, 4'	FOOT	8309	6652	1657																
	66600105	FURNISHING AND ERECTING RIGHT OF WAY MARKERS	EACH	10	10																	
*	66700305	PERMANENT SURVEY MARKERS, TYPE II	EACH	4	4																	
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	31,000	31,000																	
*	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1																	
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5																	
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	45	45																	
	67100100	MOBILIZATION	L SUM	1	1																	
	70100410	TRAFFIC CONTROL AND PROTECTION, STANDARD 701416	EACH	1	1																	
	70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	9		9																
	70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1																	
	70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1		1																
	70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1		1																
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1		1																

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 16 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	24
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	00 64	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1		1																
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1		1																
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1		1																
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1		1																
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1		1																
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	366		366																
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	1125	243	882																
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1																	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3663										3663								
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1221										1221								
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	344										344								
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	250,758										250,758								
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	29,330										29,330								
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	8302										8302								

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 17 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	25
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	89.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2608										2608								
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	27										27								
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	32,525.0										32,525.0								
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	19,362.5										19,362.5								
	70600241	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	7										7								
	70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	6										6								
	70600251	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1										1								
	70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2										2								
	70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	6										6								
	70600270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	1										1								
	70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2										2								
	70600341	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	13										13								
	70600352	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4										4								
*	72000100	SIGN PANEL - TYPE 1	SQ FT	2722										2722								

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 18 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)HR-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	26
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	72000200	SIGN PANEL - TYPE 2	SQ FT	560										560							
*	72000300	SIGN PANEL - TYPE 3	SQ FT	8833										8833							
*	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	23										23							
*	72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	138										138							
*	72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	1185										1185							
*	72400320	REMOVE SIGN PANEL - TYPE 2	SQ FT	488										488							
*	72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	7045										7045							
*	72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	5										5							
*	72600100	MILE POST MARKER ASSEMBLY	EACH	18										18							
*	72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	42,274										42,274							
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	542										542							
*	73000100	WOOD SIGN SUPPORT	FOOT	3390										3390							
*	73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	1										1							
*	73300200	OVERHEAD SIGN STRUCTURE - SPAN, TYPE II-A (4'-6" X 5'-3")	FOOT	243																243	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 19 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)RR-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	27
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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					CONSTRUCTION CODE																		
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	00 04	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	73300300	OVERHEAD SIGN STRUCTURE - SPAN, TYPE III-A (5'-0" X 7'-0")	FOOT	90																	90	
*	73301600	OVERHEAD SIGN STRUCTURE - BUTTERFLY, TYPE I-B-A	FOOT	14.6										14.6								
*	73302210	OVERHEAD SIGN STRUCTURE - CANTILEVER, TYPE III-C-A (36" X 7'-0")	FOOT	33																	33	
*	73400100	CONCRETE FOUNDATIONS	CU YD	160.4										69.0							91.4	
*	73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	165										6							159	
	73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	4										2							2	
	73600200	REMOVE OVERHEAD SIGN STRUCTURE - CANTILEVER	EACH	1										1								
	73700100	REMOVE GROUND MOUNTED SIGN SUPPORT	EACH	106										106								
	73700200	REMOVE CONCRETE FOUNDATION - GROUND MOUNT	EACH	106										106								
	73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	17										8							9	
*	78008300	POLYUREA PAVEMENT MARKING TYPE II - LETTERS AND SYMBOLS	SQ FT	1001										1001								
*	78008310	POLYUREA PAVEMENT MARKING TYPE II - LINE 4"	FOOT	24,768										24,768								
*	78008320	POLYUREA PAVEMENT MARKING TYPE II - LINE 5"	FOOT	53,404										53,404								
*	78008330	POLYUREA PAVEMENT MARKING TYPE II - LINE 6"	FOOT	15,250										15,250								

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 20 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	28
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	78008340	POLYUREA PAVEMENT MARKING TYPE II - LINE 8"	FOOT	15,069										15,069							
*	78008350	POLYUREA PAVEMENT MARKING TYPE II - LINE 12"	FOOT	4402										4402							
*	78008370	POLYUREA PAVEMENT MARKING TYPE II - LINE 24"	FOOT	548										548							
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1044										1044							
	78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	27										27							
*	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	73										73							
*	78200010	BARRIER WALL REFLECTORS, TYPE B	EACH	170										170							
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	212										212							
*	80400100	ELECTRIC SERVICE INSTALLATION	EACH	2										1							1
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	33																	33
*	81028320	UNDERGROUND CONDUIT, PVC, 1" DIA.	FOOT	6																	6
*	81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	1882										1674							208
*	81028370	UNDERGROUND CONDUIT, PVC, 3" DIA.	FOOT	1092										716							376
*	81028390	UNDERGROUND CONDUIT, PVC, 4" DIA.	FOOT	241																	241

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 21 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1NR-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	29
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	00 64	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	81026750	UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 2" DIA.	FOOT	13,905										13,905								
*	81100300	CONDUIT ATTACHED TO STRUCTURE, 1" DIA., GALVANIZED STEEL	FOOT	805										805								
*	81200210	CONDUIT EMBEDDED IN STRUCTURE, 1" DIA., PVC	FOOT	16										16								
*	81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	13,523										13,523								
*	81300220	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 6" X 6" X 4"	EACH	11										11								
*	81300530	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12" X 10" X 6"	EACH	6										6								
*	81300550	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12" X 12" X 6"	EACH	6										6								
*	81300986	JUNCTION BOX, STAINLESS STEEL, EMBEDDED IN STRUCTURE, 8" X 24" X 10"	EACH	2										2								
*	81400100	HANDHOLE	EACH	30										26								4
*	81400300	DOUBLE HANDHOLE	EACH	1																		1
*	81603040	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.8 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	2304										1604								700
*	81702100	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 12	FOOT	12,070										12,070								
*	81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	4125										4125								
*	81702120	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	1935										1935								

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 22 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	30
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0049	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	81702130	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	22,101										22,101								
	81702140	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4	FOOT	51,032										51,032								
*	82500300	LIGHTING CONTROLLER, POLE MOUNTED, 240 VOLT, 30 AMP	EACH	1																		1
*	82500370	LIGHTING CONTROLLER, BASE MOUNTED, 240 VOLT, 200 AMP	EACH	1										1								
*	83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	50										50								
*	83600357	LIGHT POLE FOUNDATION, METAL, 15" BOLT CIRCLE, 8" X 8'	EACH	44										44								
*	83600505	BREAKAWAY DEVICE, COUPLING WITH ALUMINUM SKIRT	EACH	100										100								
*	84200600	REMOVAL OF LIGHTING UNIT, NO SALVAGE	EACH	128										128								
*	84200804	REMOVAL OF POLE FOUNDATION	EACH	99										99								
*	84500110	REMOVAL OF LIGHTING CONTROLLER	EACH	3										3								
*	84500120	REMOVAL OF ELECTRIC SERVICE INSTALLATION	EACH	3										3								
*	84500130	REMOVAL OF LIGHTING CONTROLLER FOUNDATION	EACH	3										3								
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2																		2
*	85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1																		1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 23 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)IR-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	31
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0009	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	86200200	UNINTERRUPTABLE POWER SUPPLY, STANDARD	EACH	1																		1
*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	942																		942
*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	962																		962
*	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2879																		2879
*	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	299																		299
*	87301405	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 16 1 PAIR	FOOT	1104																		1104
*	87301815	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 3 C	FOOT	47																		47
*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	554																		554
*	87502510	TRAFFIC SIGNAL POST, GALVANIZED STEEL 17 FT.	EACH	2																		2
*	87602000	PEDESTRIAN PUSH-BUTTON POST	EACH	1																		1
*	87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1																		1
*	87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1																		1
*	87700200	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1																		1
*	87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1																		1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 24 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)HR-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	32
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	6																		6
*	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	3																		3
*	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	20																		20
*	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	22																		22
*	87900200	DRILL EXISTING HANDHOLE	EACH	5																		5
*	88040070	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	7																		7
*	88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	7																		7
*	88040150	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1																		1
*	88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	1																		1
*	88102825	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED WITH COUNT DOWN TIMER	EACH	4																		4
*	88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	16																		16
*	88800100	PEDESTRIAN PUSH-BUTTON	EACH	4																		4
*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1																		1
	Z0007124	STEEL RAILING (SPECIAL)	FOOT	1268				381	313	78		256	240									

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 25 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	33
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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				CONSTRUCTION CODE																		
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0006	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
Z0006900	CHAIN LINK FABRIC, TYPE 1, 4'-0"	FOOT	198	198																	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	115																	115	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	12																	12	
Z0013300	CONCRETE REMOVAL (SPECIAL)	SQ YD	48																	48	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1																	
Z0018000	DRAINAGE SCUPPERS (SPECIAL)	EACH	16			4	6	2			2	2									
Z0018800	DRAINAGE SYSTEM	L SUM	1			0.20	0.30	0.50													
20029477	TUBULAR MARKER MAINTENANCE	EACH	21										21								
Z0025505	PROPERTY MARKERS	EACH	25	25																	
Z0028415	GEOTECHNICAL REINFORCEMENT	SQ YD	213,028	140,456	72,572																
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	436										436								
52200500	MECHANICALLY STABILIZED EARTH RETAINING WALL	SQ FT	72,233												25,138	22,489	12,021	7841	4744		
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	4525								356	314		854						3001	
* Z0049300	REFERENCING LAND SECTION MARKERS	EACH	1	1																	

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	PLOT DATE = 5/5/2017	CHECKED - MRC	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 26 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 &	ROCK ISLAND	2042	34
	81-1(HBR, HBR-1, HBR-2)	CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	Z0049790	RELOCATING NAME PLATES	EACH	1																	1	
	Z0054400	ROCK FILL	CU YD	3347												765	1919	663				
	Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	33																		33
	Z0062456	TEMPORARY PAVEMENT	SQ YD	20,851	12,229	8622																
	Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	6																	6	
Ø	Z0076605	TRAINEES	HOUR	5000	5000																	
	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	3																		3
Ø	Z0076609	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	5000	5000																	
	X0301423	NOISE ABATEMENT WALL, GROUND MOUNTED	SQ FT	24,127																	24,127	
	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1																	
*	X0322281	WIDE AREA VIDEO DETECTION SYSTEM COMPLETE	EACH	1										1								
	X0322352	SEEDING MOBILIZATION	EACH	3	3																	
	52005605	TEMPORARY MECHANICALLY STABILIZED EARTH RETAINING WALL	SQ FT	7224												5542	258	282	479	663		
	X0324013	NOISE ABATEMENT WALL, STRUCTURE MOUNTED	SQ FT	1478																	1478	
*	X0325482	REMOVE EXISTING ITS EQUIPMENT	EACH	19										19								
*	X0326263	EQUIPMENT CABINET	EACH	3										3								

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	PLOT DATE = 9/5/2017	CHECKED - MRC	REVISED -		

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1VR-1 & 81-1CHBR, HBR-1, HBR-2)	ROCK ISLAND	2042	35
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	X0326382	CONCRETE BARRIER BASE (SPECIAL)	FOOT	1567		1567																
	X0326649	LINEAR DELINEATOR PANELS, 6 INCH	EACH	180										180								
	X0326677	REMOVE HIGH TENSION CABLE MEDIAN BARRIER	FOOT	6781										6781								
	X0326687	REMOVE HIGH TENSION CABLE MEDIAN BARRIER TERMINAL	EACH	9										9								
*	X0327006	ROADWAY LIGHT POLE, INSTALL ONLY	EACH	81										81								
*	X0327139	AGGREGATE COLUMN GROUND IMPROVEMENT	L SUM	1												0.25	0.25		0.25	0.25		
*	X0327748	REMOVE AND REPLACE ITS EQUIPMENT	EACH	1										1								
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	94,552										94,552								
**	X2503110	MOWING (SPECIAL)	ACRE	2.00	2.00																	
	X2810106	STONE RIPRAP, CLASS A3 (SPECIAL)	SQ YD	8792	8792																	
	X4400110	TEMPORARY PAVEMENT REMOVAL	SQ YD	24,142	12,229	11,913																
	X4402805	ISLAND REMOVAL	SQ FT	1097		1097																
	X5210140	HIGH LOAD MULTI-ROTATIONAL BEARINGS, GUIDED EXPANSION, 350K	EACH	6					6													
	X5210160	HIGH LOAD MULTI-ROTATIONAL BEARINGS, GUIDED EXPANSION, 450K	EACH	16			8	8														

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 28 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	36
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
				88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
				ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
				0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
				URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
X5810325	HIGH LOAD MULTI-ROTATIONAL BEARINGS, FIXED - 350K	EACH	14				8	6													
X5210335	HIGH LOAD MULTI-ROTATIONAL BEARINGS, FIXED - 450K	EACH	8			8															
X5509900	ABANDON AND FILL EXISTING STORM SEWER	FOOT	211	211																	
X5860110	GRANULAR BACKFILL FOR STRUCTURES	CU YD	1593			209	214	127	141	142	415	345									
X6020090	MANHOLES, WITH RESTRICTOR PLATE	EACH	5		1															4	
X6029001	JUNCTION BOX, NUMBER 1	L SUM	1		1																
X6029002	JUNCTION BOX, NUMBER 2	L SUM	1		1																
X6029003	JUNCTION BOX, NUMBER 3	L SUM	1		1																
X6060500	CORRUGATED MEDIAN REMOVAL	SQ FT	9068		9068																
X6060714	CONCRETE MEDIAN (SPECIAL)	SQ FT	1262		1262																
X6061100	CONCRETE MEDIAN, TYPE SB (SPECIAL)	SQ FT	1332		1332																
X6062700	CONCRETE GUTTER, TYPE A (SPECIAL)	FOOT	535.0	535.0																	
X6370250	CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT	FOOT	1469										1469								
X6370279	CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)	FOOT	1567										1567								

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	PLOT SCALE =	DRAWN - DTS	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 29 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	37
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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					CONSTRUCTION CODE																		
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
	X6370700	CONCRETE BARRIER TRANSITION (SPECIAL)	FOOT	322										322								
	X6430120	REMOVE IMPACT ATTENUATORS, NO SALVAGE	EACH	1										1								
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1		1																
	X7010410	SPEED DISPLAY TRAILER	CAL MO	36	36																	
	X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	1686	104	1582																
*	X7260100	MILE POST MARKER ASSEMBLY (SPECIAL)	EACH	4										4								
*	X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	23,671										23,671								
*	X7830072	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	50,974										50,974								
*	X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	14,701										14,701								
*	X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	14,793										14,793								
*	X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	863										863								
*	X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	548										548								
	X8100863	INTERCEPT EXISTING CONDUIT	EACH	1																		1
*	X8110454	CONDUIT ATTACHED TO STRUCTURE, 1" DIA., STAINLESS STEEL	FOOT	10										10								

* SPECIALTY ITEM

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		CHECKED - MRC	REVISED -
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	PLOT DATE = 5/5/2017	CHECKED - MRC	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 30 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	38
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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REV

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0180	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0054	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	X8110458	CONDUIT ATTACHED TO STRUCTURE, 2" DIA., STAINLESS STEEL	FOOT	80										80								
*	X8140105	HANDHOLE (SPECIAL)	EACH	3										3								
*	X8140115	HANDHOLE TO BE ADJUSTED	EACH	1																		1
*	X8360120	LIGHT POLE FOUNDATION, SPECIAL	EACH	12										12								
*	X8360310	LIGHT POLE FOUNDATION, 30" DIAMETER, SPECIAL	FOOT	18										18								
*	X8410102	TEMPORARY LIGHTING SYSTEM	L SUM	1										1								
*	X8860400	DETECTOR LOOP, SPECIAL	FOOT	1474																		1474
	X0546101	HOT-MIX ASPHALT CURB (SPECIAL)	FOOT	33.0	33.0																	
	X0800004	AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"	SQ YD	140,456	140,456																	
*	X7900227	ROADWAY LUMINAIRE, SPECIAL (INSTALL ONLY)	EACH	131										131								
*	X7900228	UNDERPASS LUMINAIRE, (INSTALL ONLY)	EACH	18										18								
*	X7900229	MVDS COMM CABLE, INSTALL ONLY	FOOT	3030										3030								
*	X7900230	MVDS POWER CABLE, INSTALL ONLY	FOOT	1515										1515								
	X2800113	CATCH BASIN, TYPE B (SPECIAL), TYPE 7 GRATE	EACH	2																	2	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 31 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	39
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

5/5/2017 2:02:29 PM 500_CD_Plan_Turn_In.dgn

REV

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE																	
					88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	88.5% FEDERAL 11.5% STATE	100% MOLINE
					ROADWAY	ROADWAY	SN 081-0179	SN 081-0160	SN 081-0181	SN 081-0182	SN 081-0183	SN 081-0184	SN 081-0185	SAFETY	SN 081-6013	SN 081-6014	SN 081-6015	SN 081-6016	SN 081-6017	SN 081-6020	STRUCTURE	UTILITIES
					0003	0004	0010	0010	0011	0010	0010	0010	0010	0021	0004	0004	0004	0004	0004	0004	0004	0043
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
*	X1400253	UNIT DUCT, 600V, 4-1C NO.6, 1/C NO.8 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	13,044										13,044								
*	X1400250	POWER CONNECTION TO EXISTING METER	EACH	2										2								
*	X1400231	45 FT STEEL ITS POLE, BLACK PAINTED	EACH	1										1								
*	X1400251	STEEL LUMINAIRE MAST ARM ASSEMBLY 15 FT.	EACH	2										2								

* SPECIALTY ITEM

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CHECKED - MRC | REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SHEET NO. 32 OF 32 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	40
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

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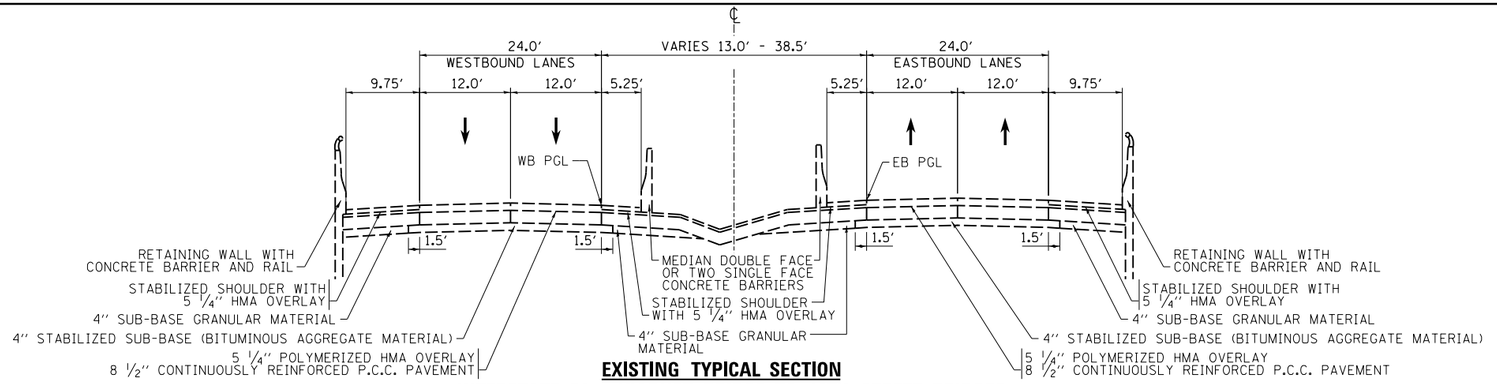
5/5/2017

REV

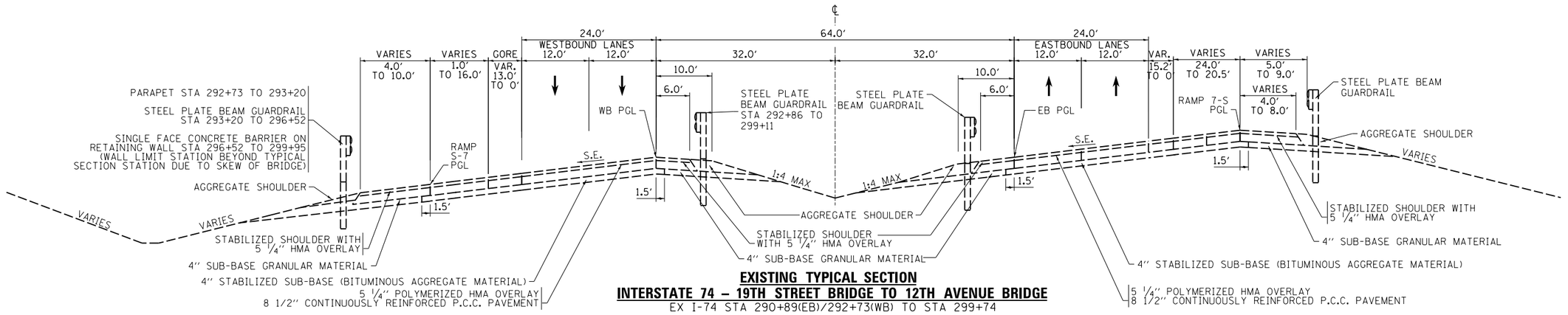


EXISTING TYPICAL SECTION

INTERSTATE 74 – VIADUCT BRIDGE TO 19TH STREET BRIDGES

EX I-74 STA 279+58 TO STA 285+53(EB)/287+01(WB)
(PR I-74 STA 50+61 TO STA 56+58(EB)/58+06(WB))

SEE BRIDGE PLANS FOR 19TH ST BRIDGE TYPICALS BETWEEN:
EB: STA 285+53 TO STA 290+89 (PR I-74 STA 56+58 TO STA 61+93)
WB: STA 287+01 TO STA 292+73 (PR I-74 STA 58+06 TO STA 63+78)

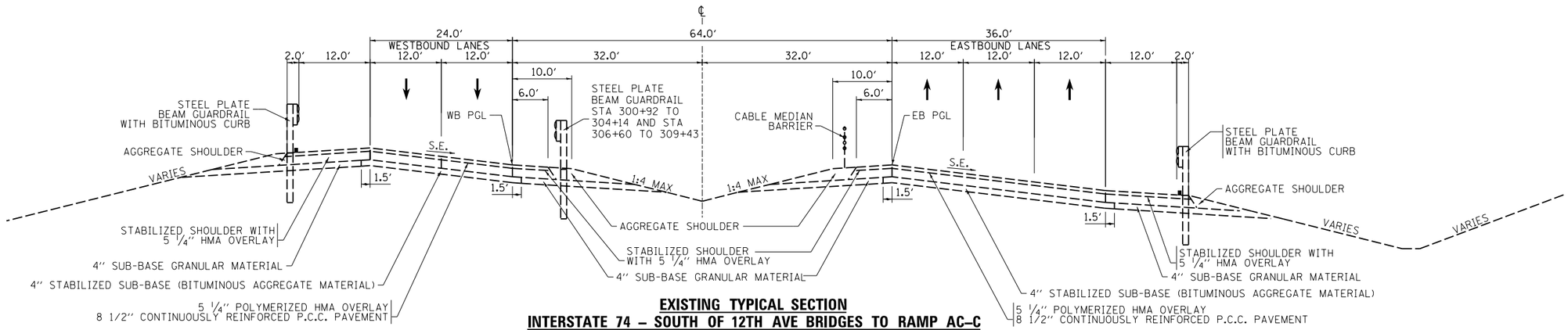


EXISTING TYPICAL SECTION

INTERSTATE 74 – 19TH STREET BRIDGE TO 12TH AVENUE BRIDGE

EX I-74 STA 290+89(EB)/292+73(WB) TO STA 299+74
(PR I-74 STA 61+93(EB)/63+78(WB) TO STA 70+81)

SEE BRIDGE PLANS FOR 12TH AVE BRIDGE TYPICALS BETWEEN:
EX I-74 STA 299+74 TO STA 300+74 (PR I-74 STA 70+81 TO STA 71+81)



EXISTING TYPICAL SECTION

INTERSTATE 74 – SOUTH OF 12TH AVE BRIDGES TO RAMP AC-C

EX I-74 STA 300+74 TO STA 309+95
(PR I-74 STA 71+81 TO STA 81+00)

TYP-01

LAYOUT		
DRAWN	CBP	7/22/2011
REVIEWED	CBP	12/22/2011
	AAP	3/22/2017

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		DRAWN - MTH	REVISED -
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	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
EXISTING I-74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	41
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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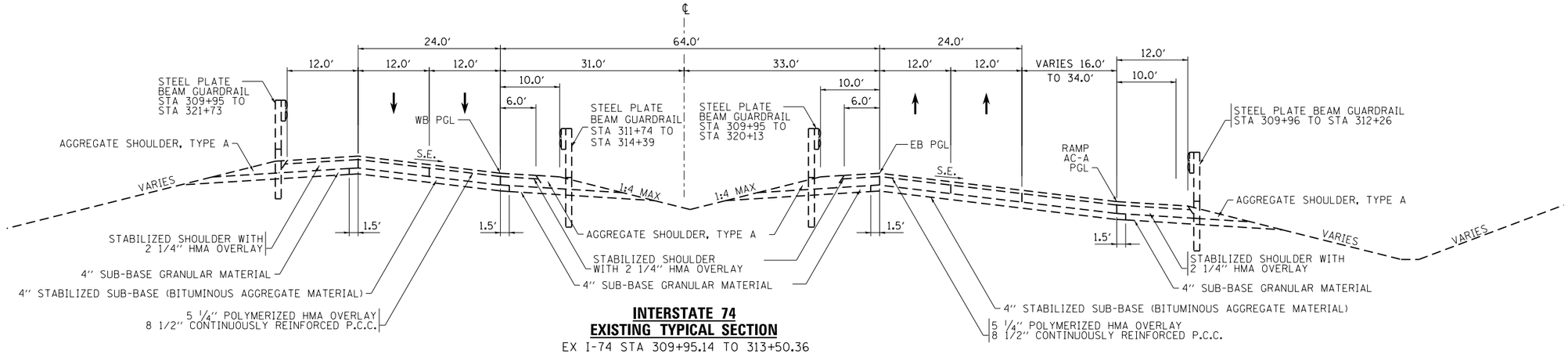
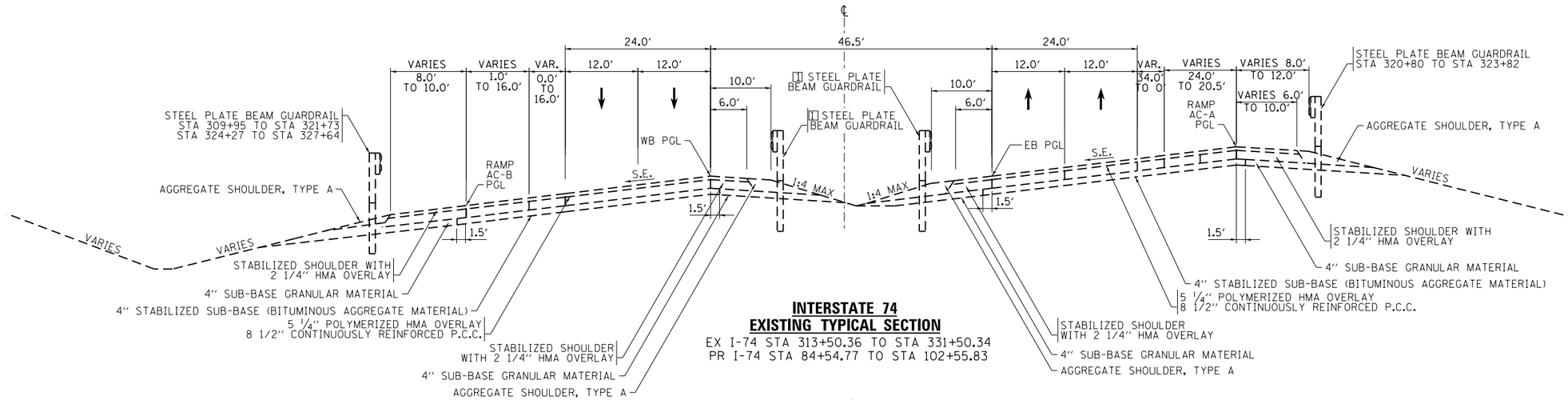
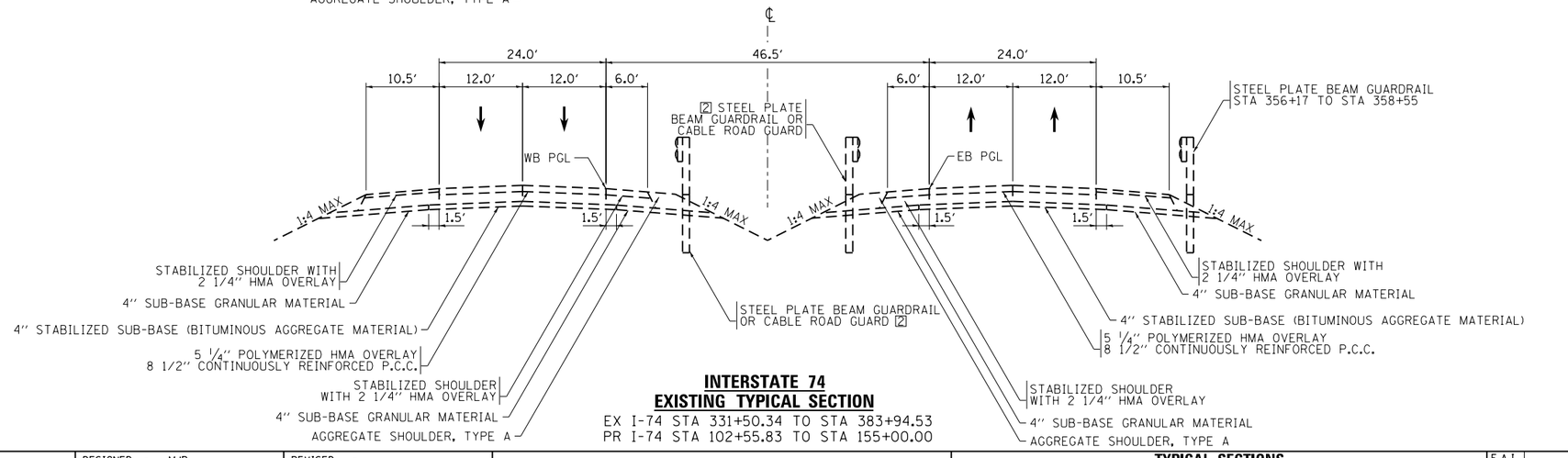


INTERSTATE 74
EXISTING TYPICAL SECTION
EX I-74 STA 309+95.14 TO 313+50.36
PR I-74 STA 81+00.00 TO STA 84+54.77



INTERSTATE 74
EXISTING TYPICAL SECTION
EX I-74 STA 313+50.36 TO STA 331+50.34
PR I-74 STA 84+54.77 TO STA 102+55.83



INTERSTATE 74
EXISTING TYPICAL SECTION
EX I-74 STA 331+50.34 TO STA 383+94.53
PR I-74 STA 102+55.83 TO STA 155+00.00

[1] STEEL PLATE BEAM GUARDRAIL
STA 320+24 TO STA 323+19 (RT)
STA 311+74 TO STA 314+39 (LT)

CABLE ROAD GUARD
STA 309+95 TO STA 320+13 (RT)

[2] STEEL PLATE BEAM GUARDRAIL
STA 334+13 TO STA 337+53 (LT)
STA 337+45 TO STA 339+56 (RT)
STA 356+62 TO STA 359+07 (LT)
STA 358+70 TO STA 361+49 (RT)
STA 363+30 TO STA 365+54 (LT)
STA 365+88 TO STA 368+06 (RT)

CABLE ROAD GUARD
STA 325+75 TO STA 333+98 (LT)
STA 337+59 TO STA 356+48 (LT)
STA 359+57 TO STA 363+16 (LT)
STA 366+25 TO STA 383+94 (LT)

TYP-02

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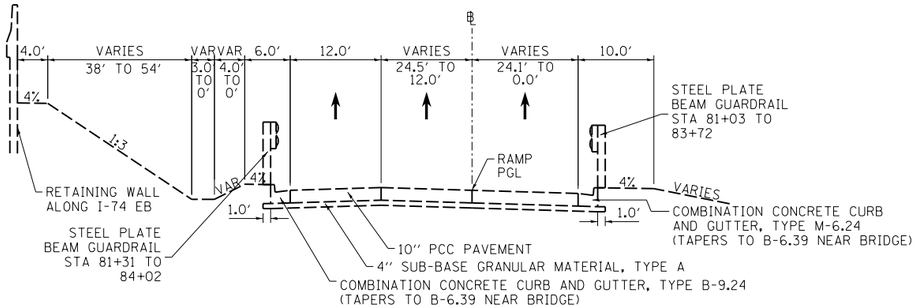
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

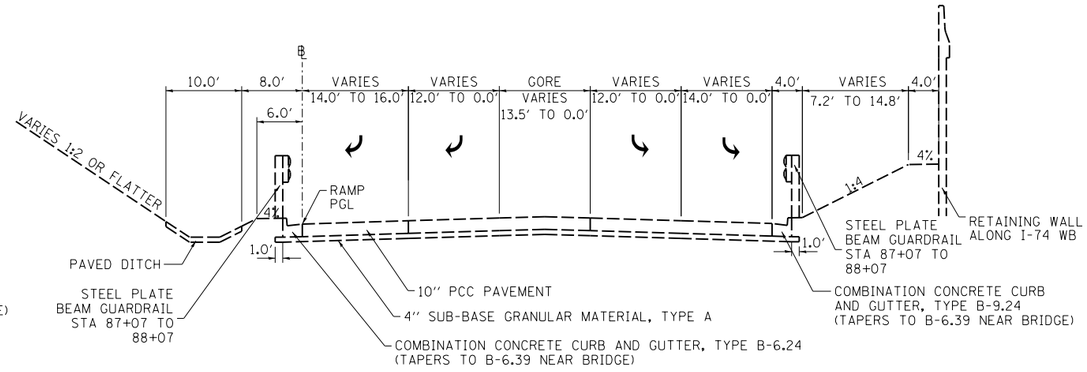
TYPICAL SECTIONS
EXISTING I-74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	42
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			



EXISTING 7TH AVE TO I–74 SB/EB ENTRANCE RAMP (7–S)
(NORTH OF 19TH STREET BRIDGES, LOOKING SOUTH)
EX RAMP 7-S STA 78+77 TO STA 84+21
EXISTING BRIDGE LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
STA 84+21 TO STA 90+84



EXISTING I–74 NB/WB TO 7TH AVE EXIT RAMP (S–7)
(NORTH OF 19TH STREET BRIDGES, LOOKING SOUTH)
EX RAMP S-7 STA 77+86 TO STA 88+22
EXISTING BRIDGE LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
STA 88+22 TO STA 92+75

TYP-03

LAYOUT	CBP	7/22/2011	
DRAWN	CBP	12/22/2011	
REVIEWED	AAP	3/22/2017	

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	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
EXISTING RAMPS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	43
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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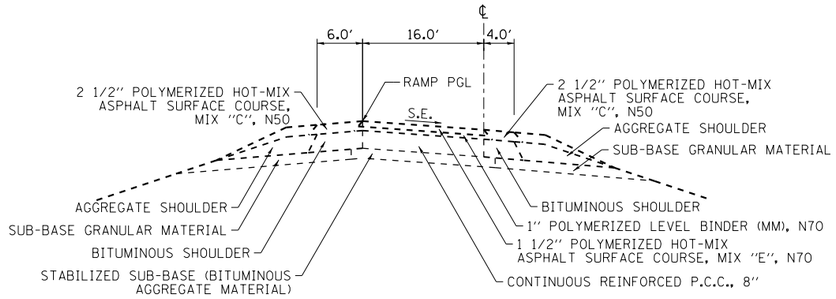
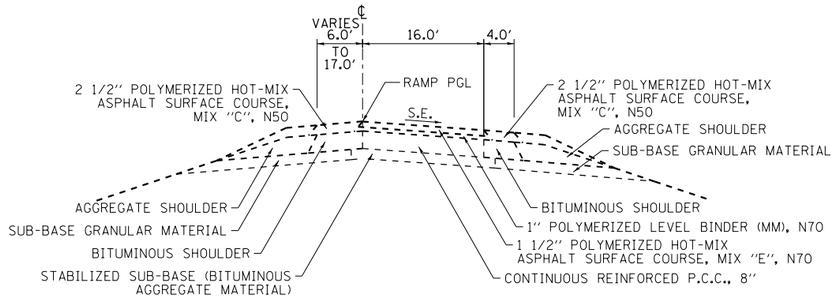


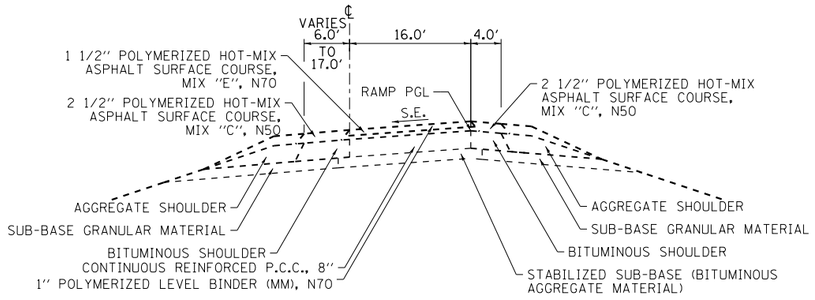
AVENUE OF THE CITIES RAMP RAC-A & RAC-D
EXISTING TYPICAL SECTION

EX RAMP RAC-A STA 43+40.00 TO STA 47+73.38
EX RAMP RAC-D STA 14+00.00 TO STA 19+26.47



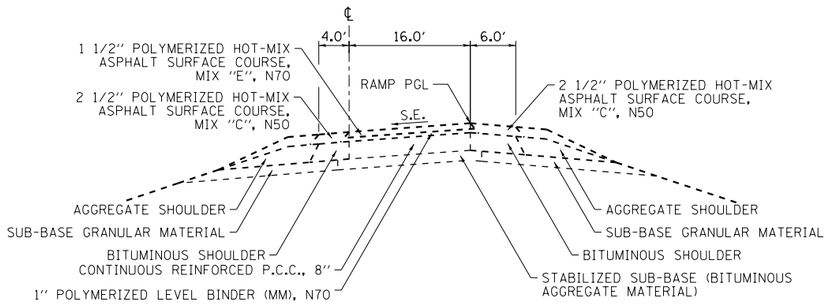
AVENUE OF THE CITIES RAMP RAC-A
EXISTING TYPICAL SECTION

EX RAMP RAC-A STA 47+73.38 TO STA 53+20.35



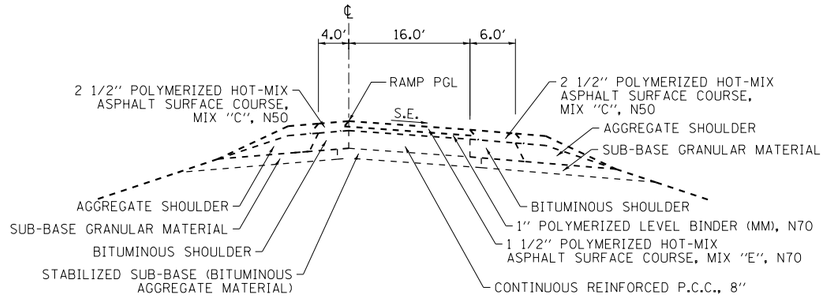
AVENUE OF THE CITIES RAMP RAC-D
EXISTING TYPICAL SECTION

EX RAMP RAC-D STA 9+41.31 TO STA 14+00.02



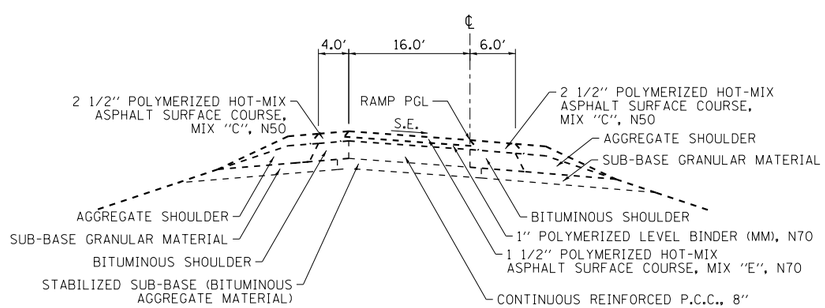
AVENUE OF THE CITIES RAMP RAC-B
EXISTING TYPICAL SECTION

EX RAMP RAC-B STA 67+16.35 TO STA 73+75.00



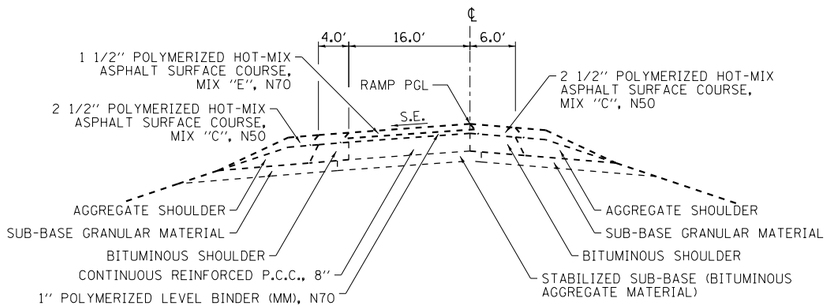
AVENUE OF THE CITIES RAMP RAC-B
EXISTING TYPICAL SECTION

EX RAMP RAC-B STA 73+75.00 TO STA 74+83.74



AVENUE OF THE CITIES RAMP RAC-B & RAC-C
EXISTING TYPICAL SECTION

EX RAMP RAC-B STA 74+83.74 TO STA 77+21.15
EX RAMP RAC-C STA 13+00.00 TO STA 18+55.21



AVENUE OF THE CITIES RAMP RAC-C
EXISTING TYPICAL SECTION

EX RAMP RAC-C STA 18+55.21 TO STA 29+17.93

TYP-04

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		DRAWN - JRM	REVISED -			74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	44
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -						CONTRACT NO. 64E26	
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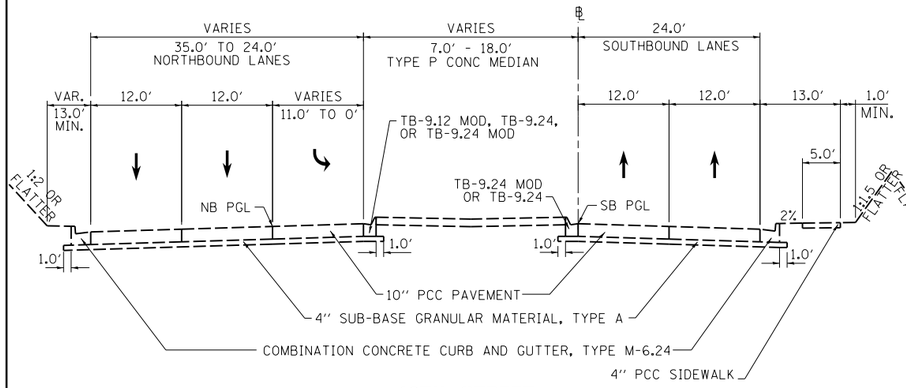
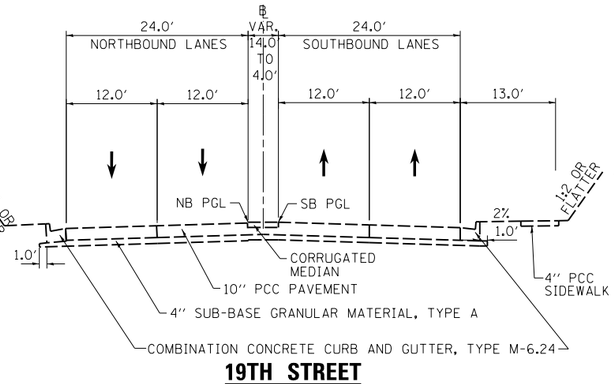


19TH STREET

(7TH AVE TO 11TH AVE)
EXISTING 19TH ST STA 39+32 TO STA 54+38

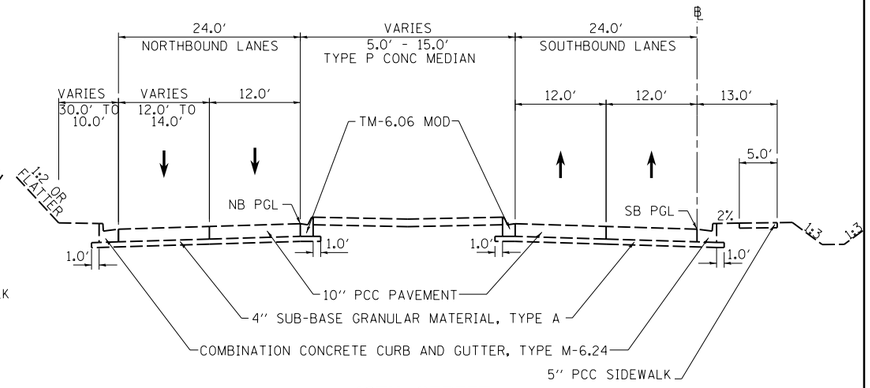
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
EXISTING 19TH ST STA 54+38 TO STA 54+97



19TH STREET

(11TH AVE TO 750' NORTH OF 18TH AVE)
EXISTING 19TH ST STA 54+97 TO STA 72+82.91
STA EQN: STA 72+82.91 (19TH ST ℄) = STA 512+55.18 (NB 19TH ST) = STA 39+94.81 (SB 19TH ST)
EXISTING NB 19TH ST FRONTAGE RD STA 512+55.18 TO STA 513+74
EXISTING SB 19TH ST FRONTAGE RD STA 39+94.81 TO STA 41+13

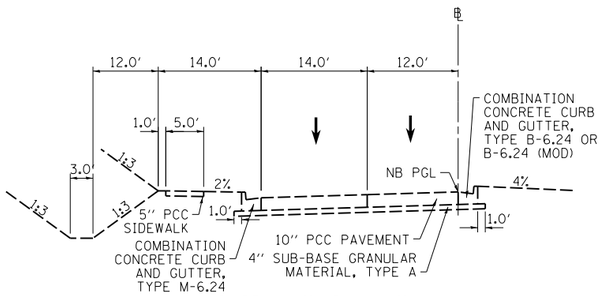
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
EXISTING 19TH ST STA 60+83 TO STA 64+10



19TH STREET

(750' NORTH OF 18TH AVE TO 18TH AVE)
EXISTING NB 19TH ST FRONTAGE RD STA 513+74 TO STA 521+10=
EXISTING SB 19TH ST FRONTAGE RD STA 41+13 TO STA 48+50

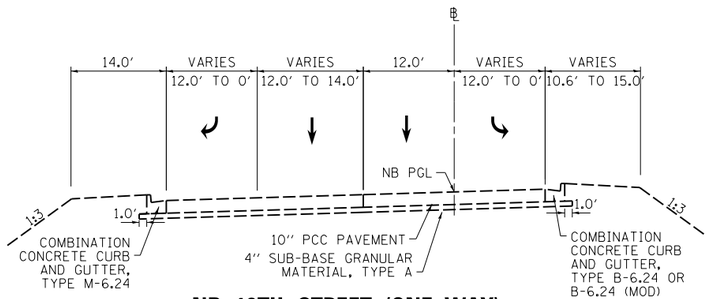
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
EXISTING NB 19TH ST FRONTAGE RD STA 521+10 TO STA 522+11=
EXISTING SB 19TH ST FRONTAGE RD STA 48+50 TO STA 49+50



NB 19TH STREET (ONE-WAY)

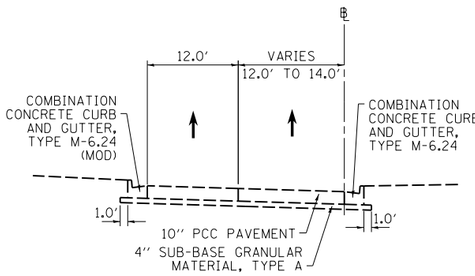
(LOOKING SOUTH)
EXISTING NB 19TH ST STA 522+11 TO STA 535+77

INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
EXISTING NB 19TH ST STA 535+77 TO STA 538+97



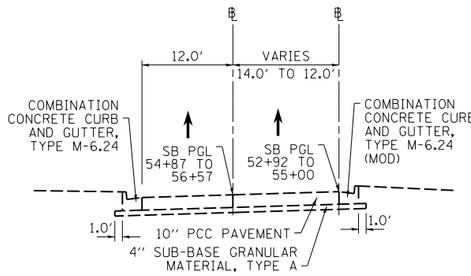
NB 19TH STREET (ONE-WAY)

(LOOKING SOUTH)
EXISTING NB 19TH ST STA 538+97 TO STA 551+59



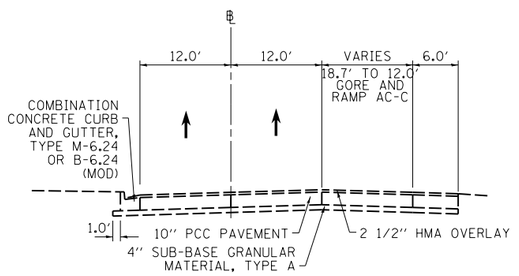
SB 19TH STREET (ONE-WAY)

(LOOKING SOUTH)
EXISTING SB 19TH ST STA 49+50 TO STA 52+92



SB 19TH STREET (ONE-WAY)

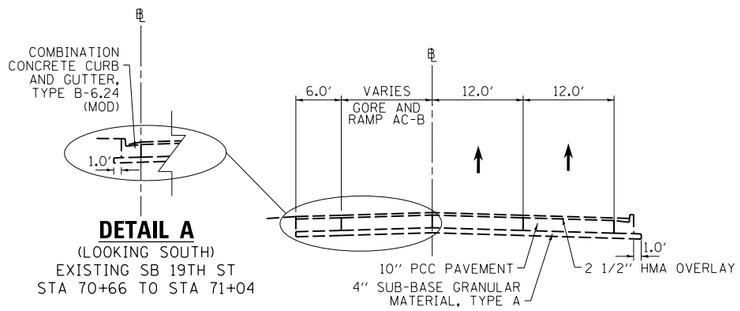
(LOOKING SOUTH)
EXISTING SB 19TH ST STA 52+92 TO STA 56+57
STA EQN: STA 55+00.00 (SB 19TH ST ℄ AT RT EOP) =
STA 54+87.07 (SB 19TH ST ℄ BETWEEN LANES)



SB 19TH STREET (ONE-WAY)

(LOOKING SOUTH)
EXISTING SB 19TH ST STA 56+57 TO STA 61+94

INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
EXISTING SB 19TH ST STA 61+94 TO STA 67+53



DETAIL A

(LOOKING SOUTH)
EXISTING SB 19TH ST
STA 70+66 TO STA 71+04

SB 19TH STREET (ONE-WAY)

(LOOKING SOUTH)
EXISTING SB 19TH ST STA 67+53 TO STA 71+04

LAYOUT	CBP	7/22/2011
DRAWN	CBP	12/22/2011
REVIEWED	AAP	3/22/2017

TYP-05

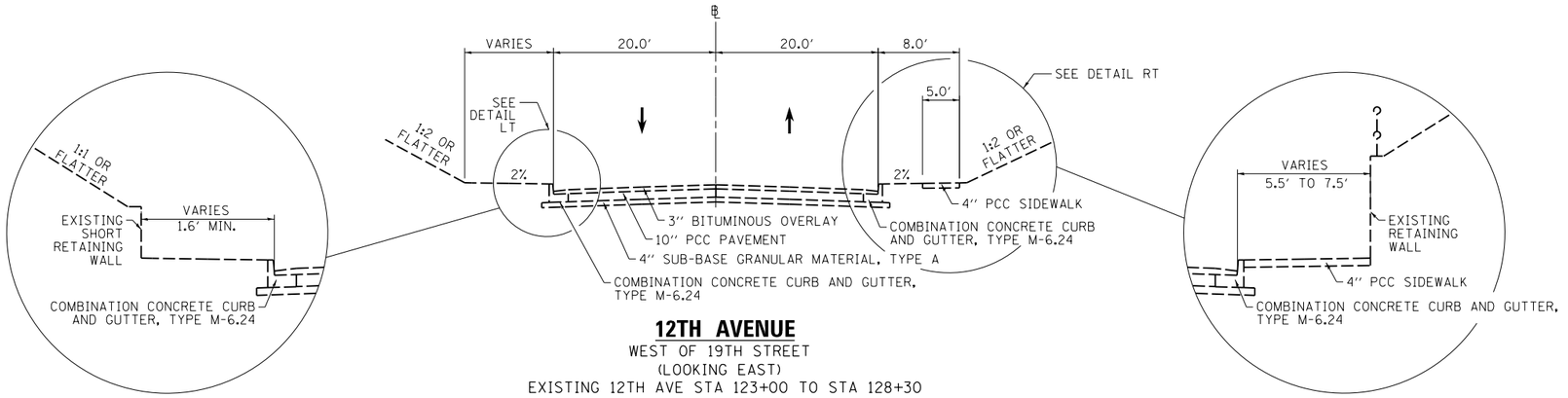
FILE NAME =	USER NAME = hehn01663	DESIGNED - CBP	REVISED -
D2CONCD-HPS-sht-typical02L.dgn		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
EXISTING LOCAL ROADS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1R-1 & 8)-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	45
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		

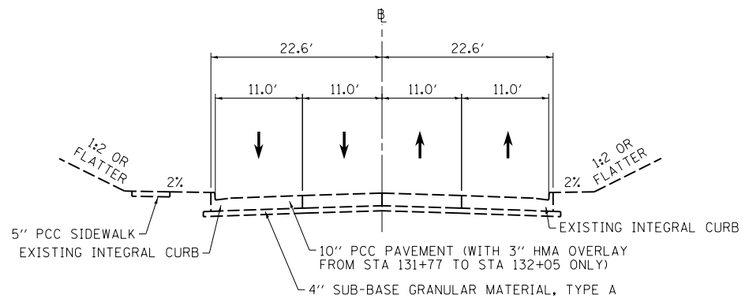


12TH AVENUE
WEST OF 19TH STREET
(LOOKING EAST)
EXISTING 12TH AVE STA 123+00 TO STA 128+30

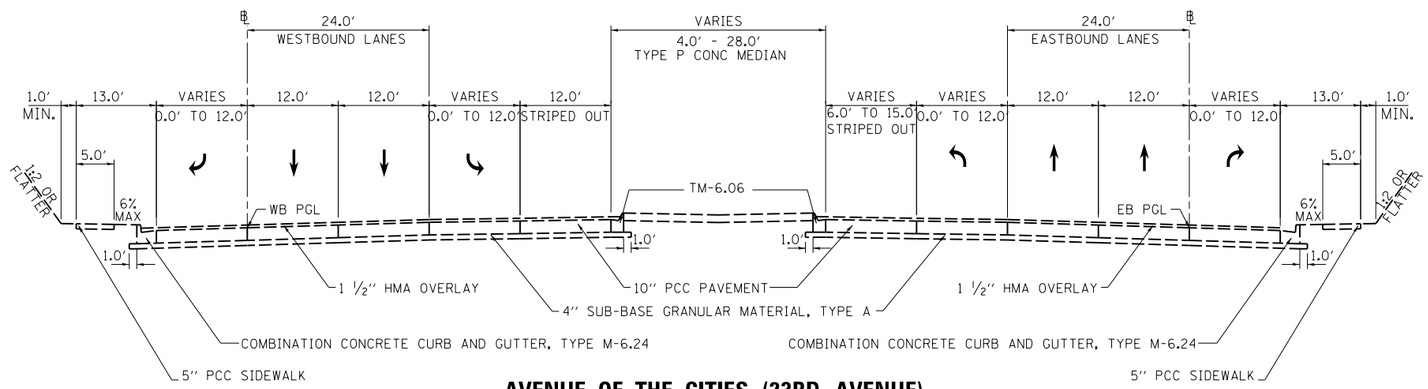
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION):
STA 128+30 TO STA 131+53

DETAIL LT
RETAINING WALL
STA 122+64 TO STA 123+54 LT

DETAIL RT
RETAINING WALL
STA 127+70 TO STA 129+48



12TH AVENUE
EAST OF 19TH STREET
(LOOKING EAST)
EXISTING 12TH AVE STA 131+53 TO STA 132+05



AVENUE OF THE CITIES (23RD AVENUE)
EXISTING WB 23RD AVE STA 119+00 TO STA 126+75
EXISTING EB 23RD AVE STA 219+00 TO STA 226+75

BRIDGE OMISSION LIMITS:
EXISTING WB 23RD AVE STA 122+23 TO STA 123+83
EXISTING EB 23RD AVE STA 222+38 TO STA 223+98

TYP-06

FILE NAME = D2CONCD-HPS-sht-typical03L.dgn	USER NAME = hehn01663	DESIGNED - CBP	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

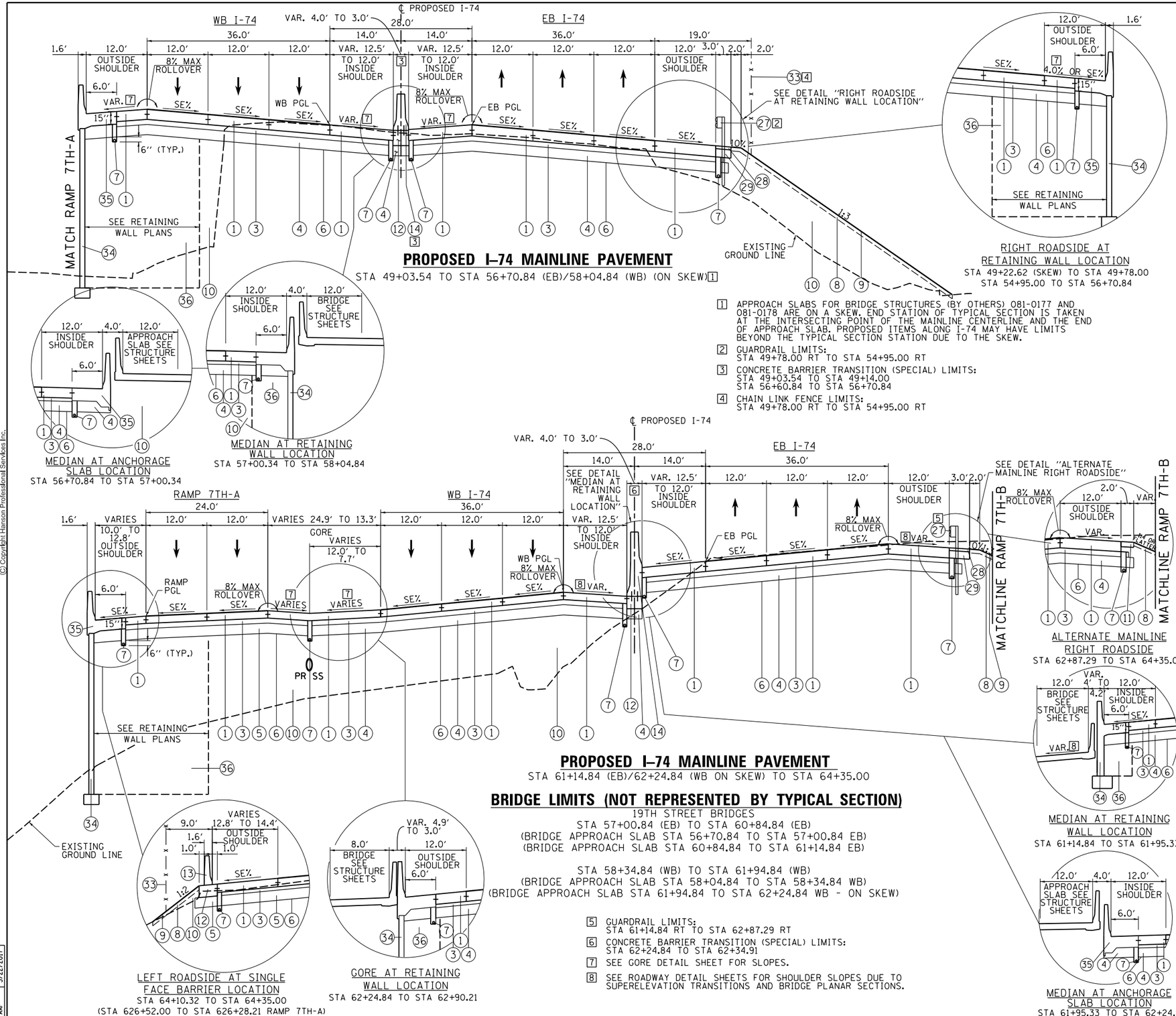
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
EXISTING LOCAL ROADS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	46
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

LAYOUT	CBP	7/22/2011
DRAWN	CBP	12/22/2011
REVIEWED	AAP	3/22/2017



BRIDGE LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

19TH STREET BRIDGES

STA 57+00.84 (EB) TO STA 60+84.84 (EB)
(BRIDGE APPROACH SLAB STA 56+70.84 TO STA 57+00.84 EB)
(BRIDGE APPROACH SLAB STA 60+84.84 TO STA 61+14.84 EB)

STA 58+34.84 (WB) TO STA 61+94.84 (WB)
(BRIDGE APPROACH SLAB STA 58+04.84 TO STA 58+34.84 WB)
(BRIDGE APPROACH SLAB STA 61+94.84 TO STA 62+24.84 WB - ON SKEW)

1 APPROACH SLABS FOR BRIDGE STRUCTURES (BY OTHERS) 081-0177 AND 081-0178 ARE ON A SKEW. END STATION OF TYPICAL SECTION IS TAKEN AT THE INTERSECTING POINT OF THE MAINLINE CENTERLINE AND THE END OF APPROACH SLAB. PROPOSED ITEMS ALONG I-74 MAY HAVE LIMITS BEYOND THE TYPICAL SECTION STATION DUE TO THE SKEW.

2 GUARDRAIL LIMITS:
STA 49+78.00 RT TO STA 54+95.00 RT

3 CONCRETE BARRIER TRANSITION (SPECIAL) LIMITS:
STA 49+03.54 TO STA 49+14.00
STA 56+60.84 TO STA 56+70.84

4 CHAIN LINK FENCE LIMITS:
STA 49+78.00 RT TO STA 54+95.00 RT

5 GUARDRAIL LIMITS:
STA 61+14.84 RT TO STA 62+87.29 RT

6 CONCRETE BARRIER TRANSITION (SPECIAL) LIMITS:
STA 62+24.84 TO STA 62+34.91

7 SEE GORE DETAIL SHEET FOR SLOPES.

8 SEE ROADWAY DETAIL SHEETS FOR SHOULDER SLOPES DUE TO SUPERELEVATION TRANSITIONS AND BRIDGE PLANAR SECTIONS.

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

INTERSTATE 74

STRUCTURAL DESIGN TRAFFIC: YEAR 2027

PV = 82,671 SU = 4,342 MU = 3,437

ROAD/STREET CLASSIFICATION: INTERSTATE CLASS: I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 8% S = 37% M = 37%

TRAFFIC FACTOR: ACTUAL TF = 22.35 AC TYPE = N/A

MINIMUM TF = 8.26

PG GRADE: BINDER = N/A SURFACE = N/A

SUBGRADE SUPPORT RATING:

SSR = IBR = 3 (POOR)

TYP-07

LAYOUT	RLT	1/22/2011
DRAWN	MTH	5/22/2013
REVIEWED	AAP	3/22/2017

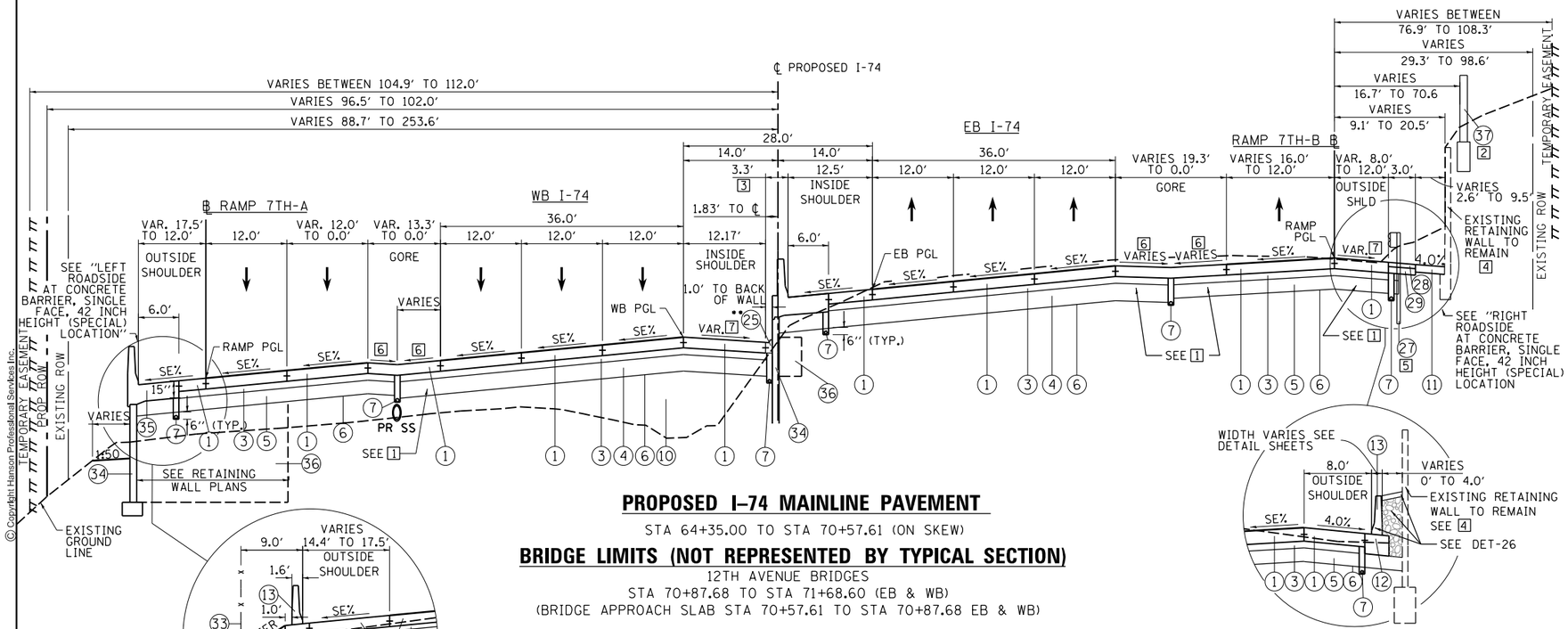
FILE NAME = D2CONCD-HPS-sht-typical07M.dgn
USER NAME = hehn01663 | DESIGNED - RLT | REVISED -
DRAWN - MTH | REVISED -
PLOT SCALE = | CHECKED - AAP | REVISED -
PLOT DATE = 3/22/2017 | DATE - 3/23/2017 | REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I-74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	47
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			



PROPOSED I-74 MAINLINE PAVEMENT

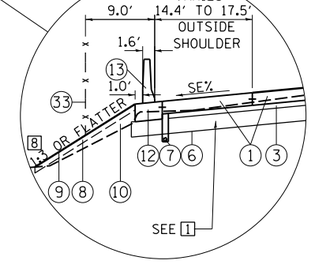
STA 64+35.00 TO STA 70+57.61 (ON SKEW)

BRIDGE LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

12TH AVENUE BRIDGES

STA 70+87.68 TO STA 71+68.60 (EB & WB)

(BRIDGE APPROACH SLAB STA 70+57.61 TO STA 70+87.68 EB & WB)



LEFT ROADSIDE AT CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) LOCATION

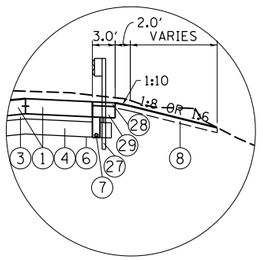
STA 64+35.00 TO STA 67+45.39

(STA 626+28.21 TO STA 623+26.84 RAMP 7TH-A)

RIGHT ROADSIDE AT CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) LOCATION

STA 64+35.00 TO STA 64+95.18

(STA 535+14.85 TO STA 535+76.66 RAMP 7TH-B)



RIGHT ROADSIDE AT DITCH LOCATION

STA 66+61.98 TO STA 70+57.61

[1] THE THICKNESS OF THE RAMP SHOULDER SUBGRADE AND THE GORE SUBGRADE TRANSITIONS IN ACCORDANCE WITH IDOT STANDARDS 420206 AND 420306.
- (5) FROM STA 64+35.00 TO STA 65+54.62 — RAMP 7TH-A
- (4) FROM STA 65+54.62 TO STA 70+57.61(ON SKEW) — RAMP 7TH-A
- (5) FROM STA 64+35.00 TO STA 67+09.69 — RAMP 7TH-B
- (4) FROM STA 67+09.69 TO STA 70+57.61(ON SKEW) — RAMP 7TH-B

[2] PROPOSED NOISE ABATEMENT WALL LIMITS
STA 64+09.31, 104.05' RT TO STA 77+19.73, 84.00' RT
(EXTENDS BEYOND TYPICAL SECTION)

[3] MEDIAN WALL BLISTER FOR OVERHEAD SIGN STRUCTURE:
STA 65+20.59 TO STA 65+89.42 AND BRIDGE STRUCTURE TIE-IN STA 70+34.28 TO STA 70+56.74.
WALL BLISTERS ALSO AT LIGHT POLE LOCATIONS.
SEE LIGHTING AND RETAINING WALL PLANS AND DETAILS.

[4] EXISTING RETAINING WALL LIMITS:
STA 64+21.08, 95.52' RT TO STA 66+61.98, 92.52' RT

[5] GUARDRAIL LIMITS:
STA 64+95.18 RT TO STA 70+34.38 RT
(STA 535+76.66 TO STA 541+31.49 RAMP 7TH-B)

[6] SEE GORE DETAIL SHEET FOR SLOPES

[7] SEE ROADWAY DETAIL SHEETS FOR SHOULDER SLOPES DUE TO SUPERELEVATION TRANSITIONS AND BRIDGE PLANAR SECTIONS.

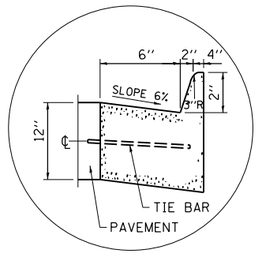
[8] EMBANKMENT GRADE IS 1:2 BETWEEN STA 63+00 AND 65+30 FOR BRIDGE CONE GRADING.

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.



**DETAIL 1

(25) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06

TYP-08

LAYOUT	RLT	7/22/2011
DRAWN	MTH	6/18/2012
REVIEWED	AAP	3/22/2017

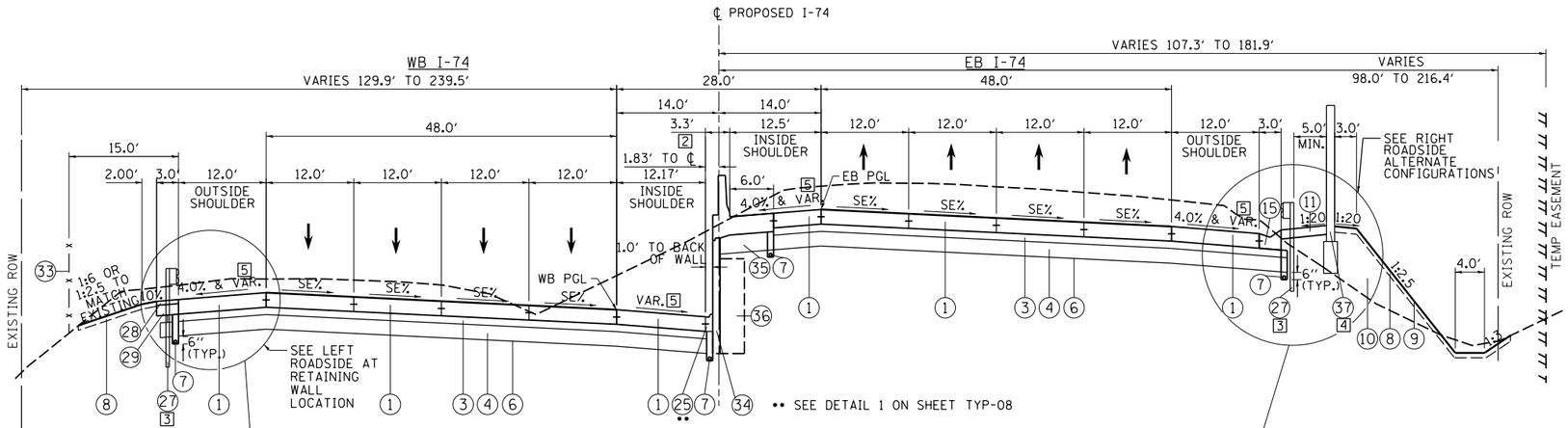
FILE NAME = D2CONCD-HPS-sht-typical08M.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
		DRAWN - MTH	REVISED -	
	PLOT SCALE =	CHECKED - AAP	REVISED -	
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -	

TYPICAL SECTIONS PROPOSED I-74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

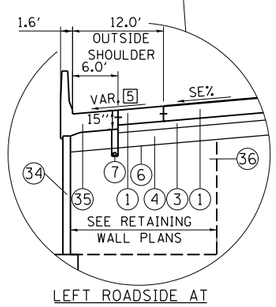
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	48
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

HANSON



PROPOSED I-74 MAINLINE PAVEMENT

STA 71+98.67 (ON SKEW) TO STA 81+00.00
(BRIDGE APPROACH SLAB STA 71+68.60 TO STA 71+98.67 EB & WB)



LEFT ROADSIDE AT
RETAINING WALL LOCATION
STA 72+22.48 (ON SKEW) TO STA 72+46.00

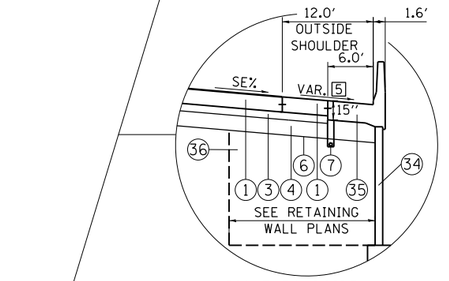
1 APPROACH SLABS FOR BRIDGE STRUCTURES 081-0182 AND 081-0183 ARE ON A SKEW. END STATION OF TYPICAL SECTION IS TAKEN AT THE INTERSECTING POINT OF THE MAINLINE CENTERLINE. PROPOSED ITEMS ALONG I-74 MAY HAVE LIMITS BEYOND THE TYPICAL SECTION STATION DUE TO SKEW.

2 MEDIAN WALL BLISTER FOR BRIDGE STRUCTURE TIE-IN:
STA 71+98.27 TO STA 72+22.36
WALL BLISTERS ALSO AT LIGHT POLE LOCATIONS.
SEE LIGHTING AND RETAINING WALL PLANS AND DETAILS.

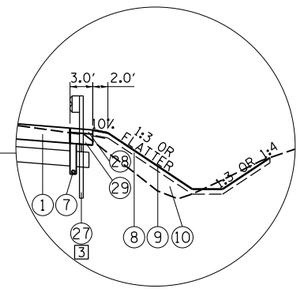
3 GUARDRAIL LIMITS:
STA 71+93.00 RT TO STA 81+00.00 RT
STA 72+46.00 LT TO STA 81+00.00 LT

4 PROPOSED NOISE ABATEMENT WALL LIMITS:
(EXTENDS BEYOND TYPICAL SECTION)
STA 64+09.31, 104.05' RT TO STA 77+19.73, 84.00' RT

5 SEE ROADWAY DETAIL SHEETS FOR SHOULDER SLOPES DUE TO SUPERELEVATION TRANSITIONS AND BRIDGE PLANAR SECTIONS.



RIGHT ROADSIDE AT
RETAINING WALL LOCATION
STA 71+77.25 (ON SKEW) TO STA 71+93.00



RIGHT ROADSIDE WITHOUT
NOISE ABATEMENT WALL LOCATION
STA 77+30.00 TO STA 81+00.00

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

LAYOUT	RLT	7/22/2011
DRAWN	MTH	5/22/2013
REVIEWED	AAP	3/22/2017

TYP-09

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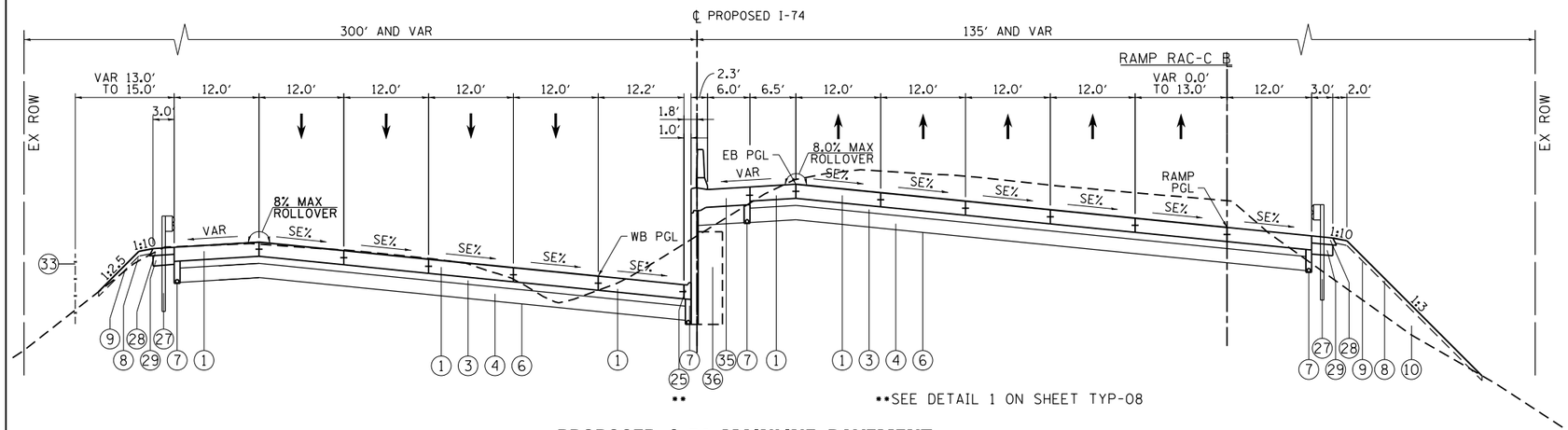
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I-74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

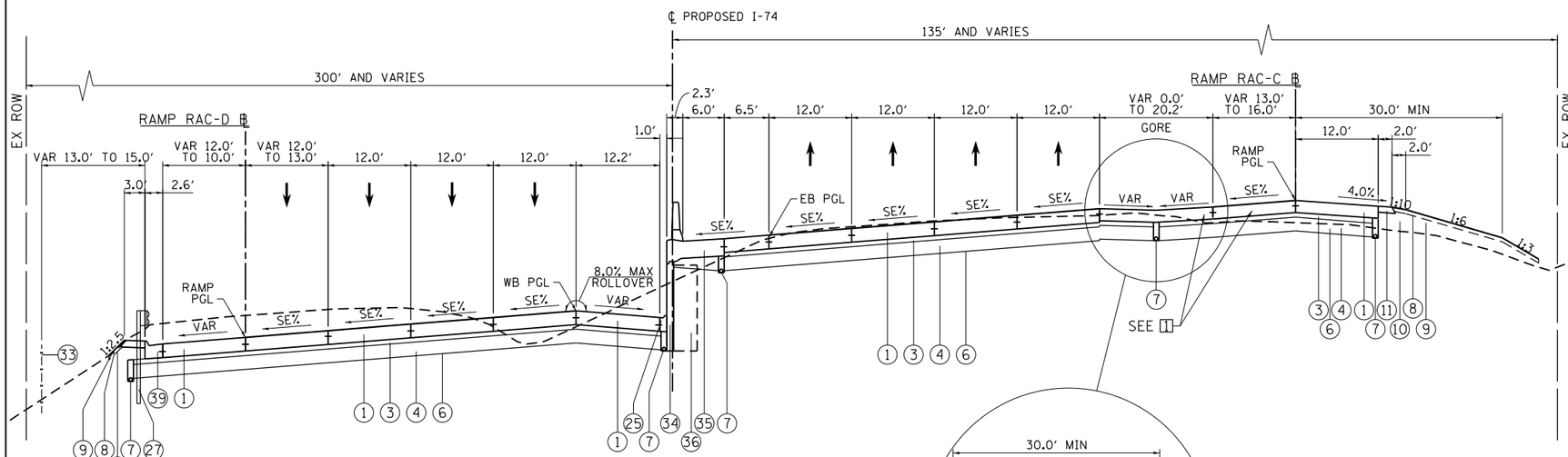
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	49
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			



••SEE DETAIL 1 ON SHEET TYP-08

PROPOSED I–74 MAINLINE PAVEMENT

STA 81+00.00 TO STA 84+71.55



PROPOSED I–74 MAINLINE PAVEMENT

STA 84+71.55 TO STA 91+49.36

RIGHT ROADSIDE OUTSIDE RAMP LIMITS

STA 88+28.44 TO STA 91+49.15

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

TYP-10

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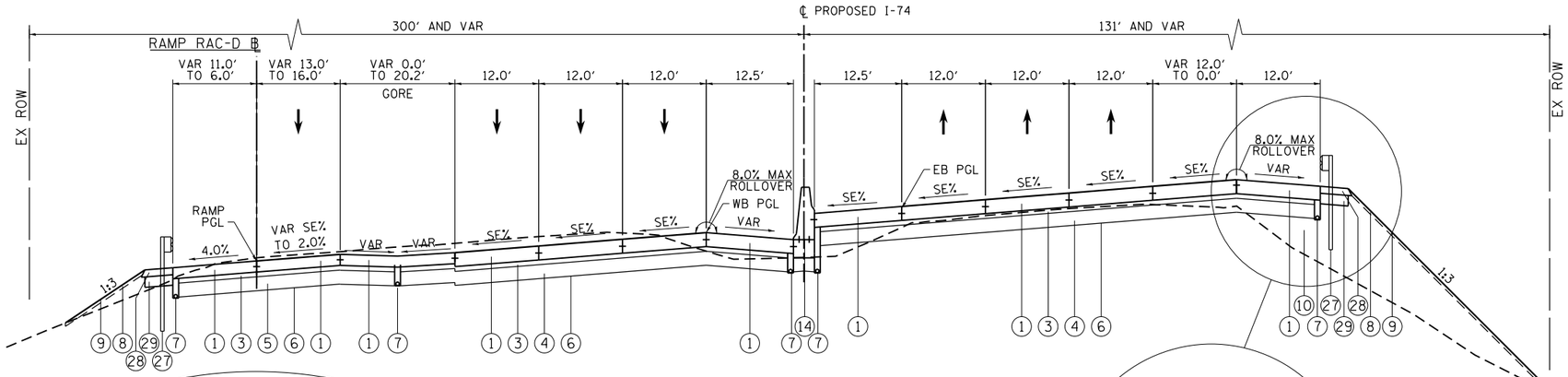
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/3/2017	DATE - 3/23/2017	REVISED -

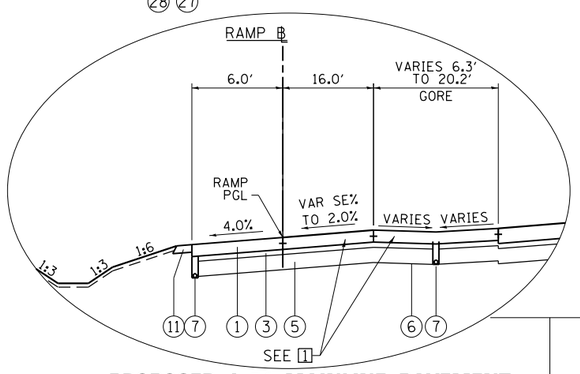
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I–74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	50
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			



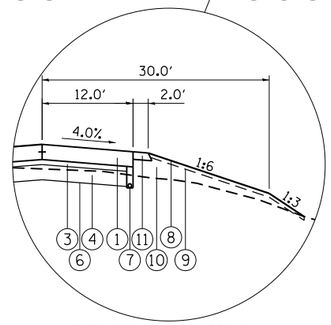


PROPOSED I–74 MAINLINE PAVEMENT

WB STA 123+37.30 TO STA 125+77.06
EB STA 117+34.97 TO STA 120+54.15

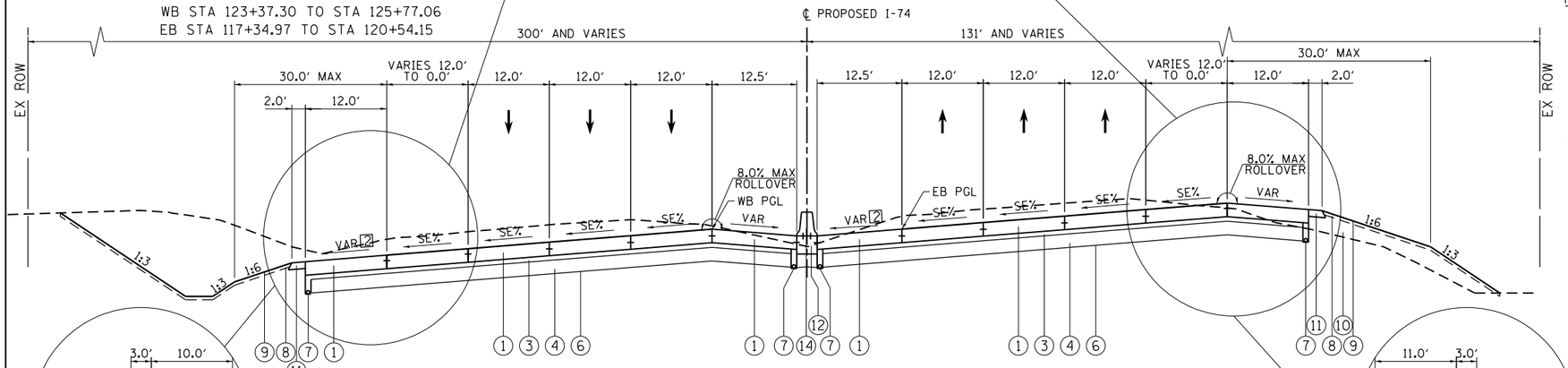
PROPOSED I–74 MAINLINE PAVEMENT

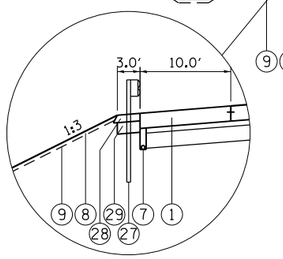
STA 91+49.36 TO STA 97+75.74
BRIDGE OMISSION
WB STA 93+52.42 TO STA 96+73.88
EB STA 93+95.10 TO STA 97+09.81

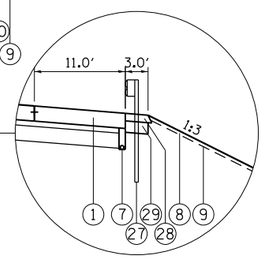


RIGHT ROADSIDE

STA 91+49.15 TO STA 92+66.47







PROPOSED I–74 MAINLINE PAVEMENT

STA 97+75.74 TO STA 129+60.00

ROADSIDE AT GUARDRAIL

STA 129+08.23 TO STA 129+60.00

ROADSIDE AT GUARDRAIL

STA 128+05.36 TO STA 129+60.00

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

[2] SHOULDER SUPERELEVATION VARIES:
STA 97+75.74 TO STA 102+60.50: MATCH MAINLINE SUPERELEVATION
STA 102+60.50 TO STA 129+60.00: 4.0% SUPERELEVATION

Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

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$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/3/2017	DATE - 3/23/2017	REVISED -

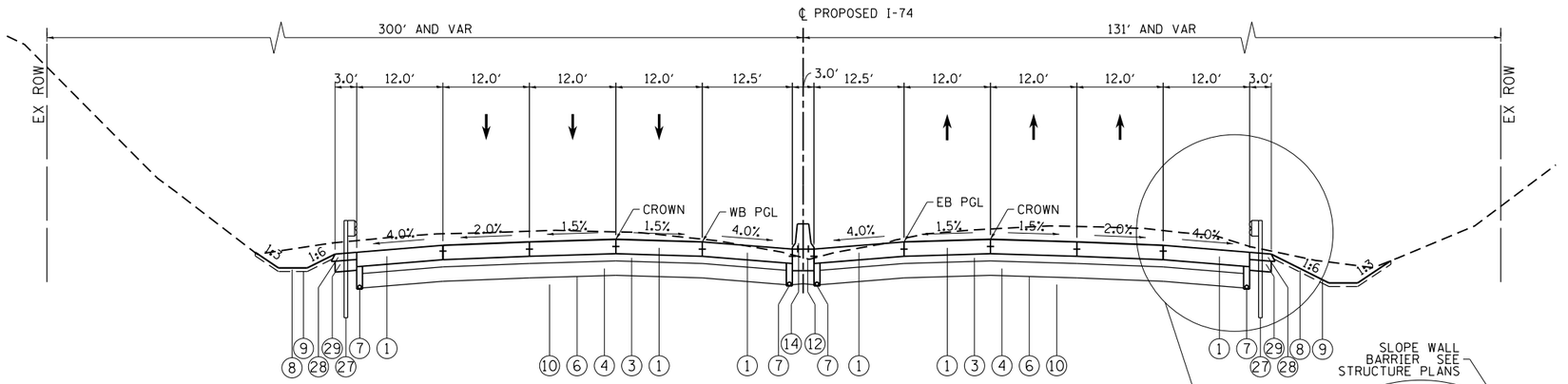
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I–74 MAINLINE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

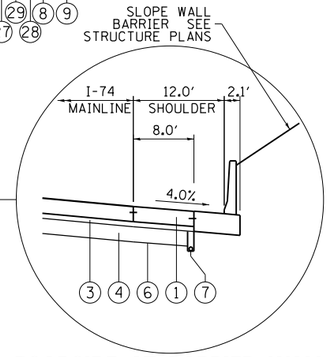
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	51
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

TYP-11



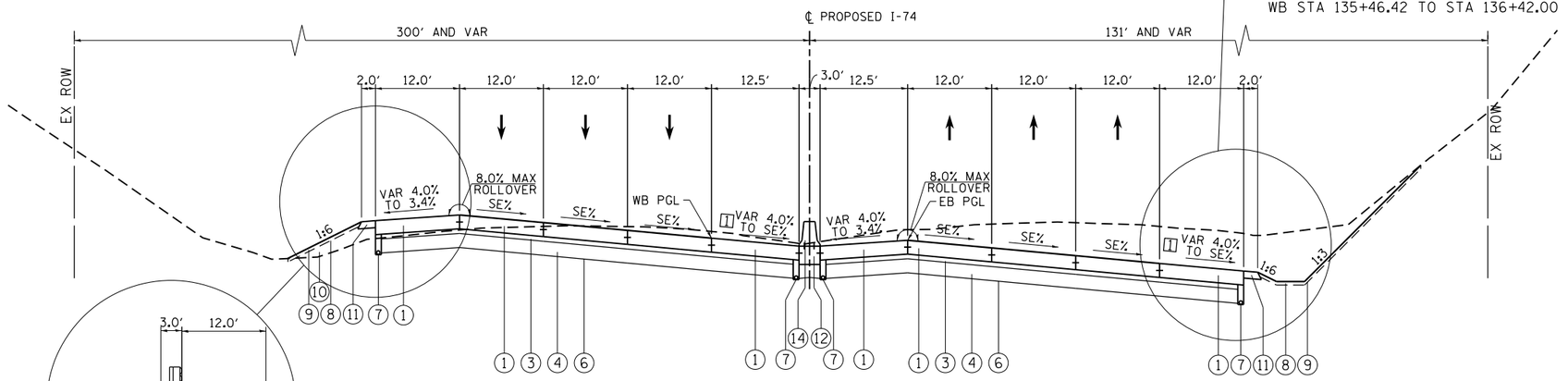
PROPOSED I–74 MAINLINE PAVEMENT

STA 129+60.00 TO STA 138+52.25 (EB)
STA 129+60.00 TO STA 138+33.00 (WB)



ROADSIDE AT BARRIER WALL

EB STA 130+36.00 TO STA 130+92.00
EB STA 136+98.42 TO STA 137+75.00
WB STA 129+59.00 TO STA 130+15.00
WB STA 135+46.42 TO STA 136+42.00



PROPOSED I–74 MAINLINE PAVEMENT

STA 138+52.25 TO STA 141+20.00 (EB)
STA 138+33.00 TO STA 145+28.00 (WB)

ROADSIDE AT GUARDRAIL

WB STA 134+34.00 TO STA 135+46.42
EB STA 134+34.00 TO STA 136+98.42
WB STA 136+42.00 TO STA 138+72.64

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

[1] SHOULDER SUPERELEVATION VARIES:
STA 138+33.00 TO STA 141+20.00 (EB): 4.0%
STA 138+33.00 TO STA 141+86.00 (WB): 4.0%
STA 141+86.00 TO STA 145+28.00 (WB): MATCH MAINLINE SUPERELEVATION

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312-565-0450

TYP-12

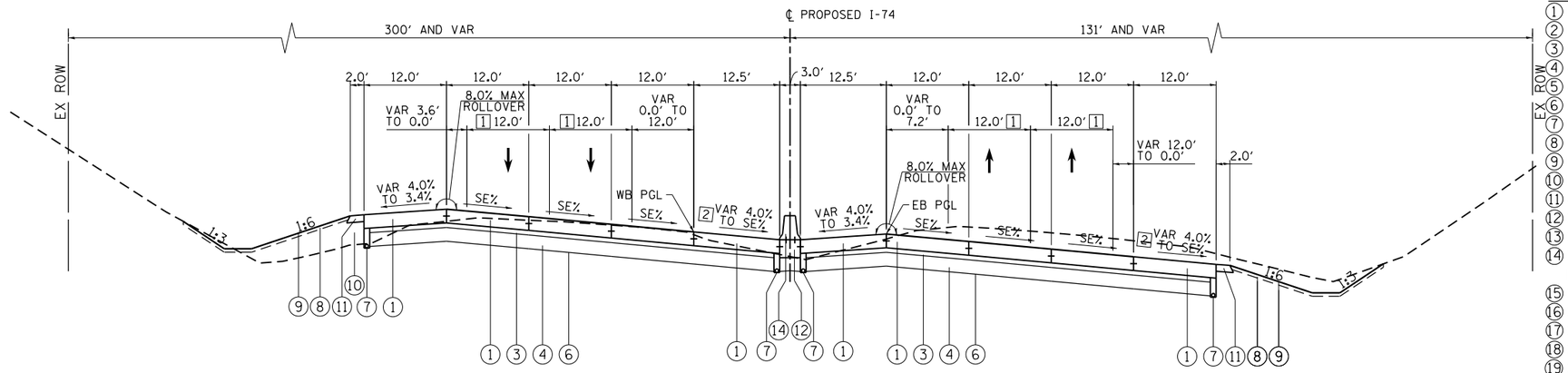
FILE NAME = ...\D2CONCD-ABC-sht-typical04.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/3/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I–74 MAINLINE

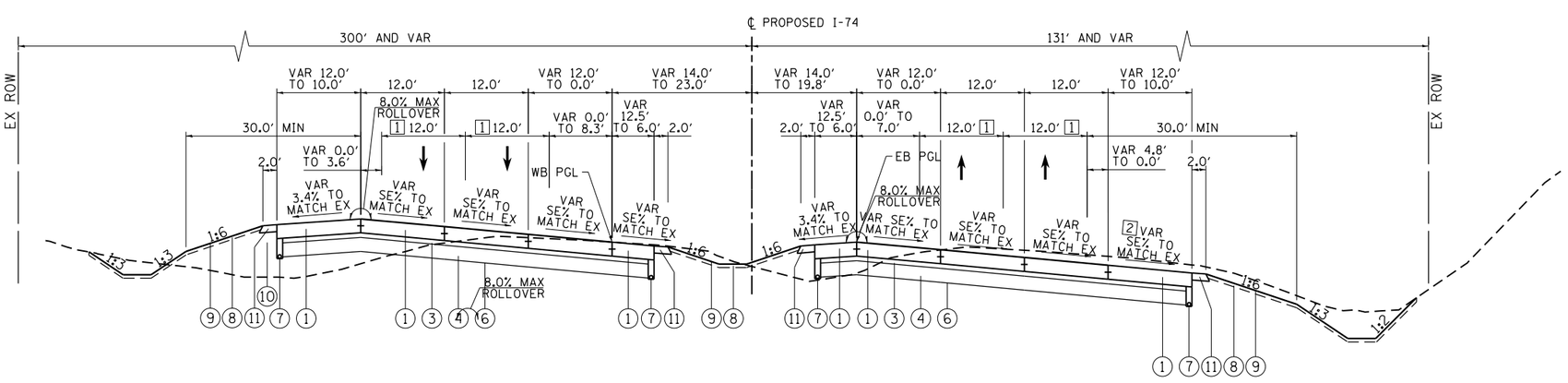
SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	52
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		



PROPOSED I–74 MAINLINE PAVEMENT

STA 141+20.00 (EB) TO STA 153+90.00
STA 145+28.00 (WB) TO STA 153+90.00



PROPOSED I–74 MAINLINE PAVEMENT

STA 153+90.00 TO STA 155+00.00

[1] SEE PAVEMENT MARKING PLANS FOR LANE TRANSITION LIMITS

[2] SHOULDER SUPERELEVATION VARIES:
STA 141+20.00 TO STA 141+50.00 (EB): 4.0%
STA 141+50.00 TO STA 153+90.00 (EB): MATCH MAINLINE SUPERELEVATION
STA 145+28.00 (WB): MATCH MAINLINE SUPERELEVATION
STA 153+90.00 TO STA 155+00.00: TRANSITION TO MATCH EX SUPERELEVATION

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH,

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205 North Michigan Avenue, Suite 2400
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312-565-0450

TYP-13

FILE NAME =	USER NAME = jtardy	DESIGNED - JRM	REVISED -
...\D2CONCD-ABC-sht-typical05.dgn		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/3/2017	DATE - 3/23/2017	REVISED -

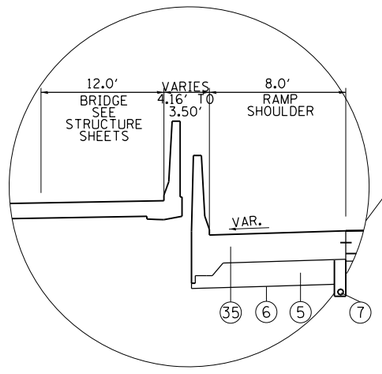
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED I–74 MAINLINE

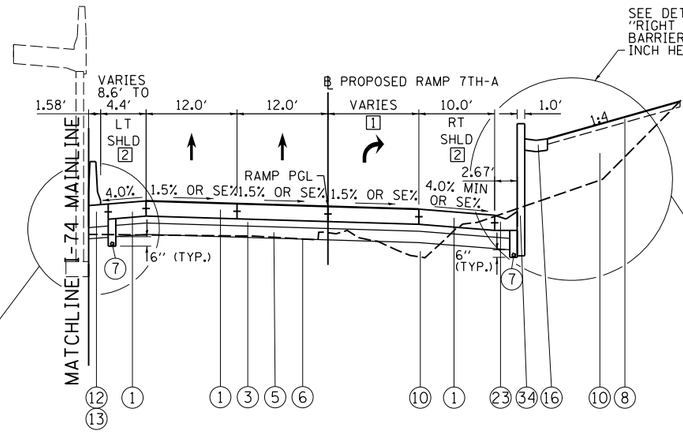
SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

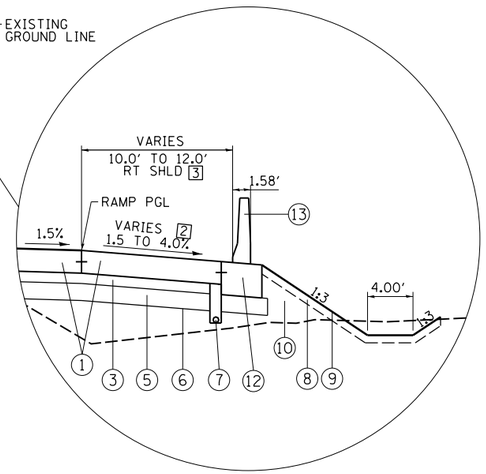
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	53
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

HANSON



LEFT ROADSIDE AT ANCHORAGE SLAB LOCATION
STA 631+12.94 TO STA 632+17.72





RIGHT ROADSIDE AT CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) LOCATION
STA STA 631+12.94 TO 632+50.00

PROPOSED RAMP 7TH-A PAVEMENT

STA 631+13.01 (ON SKEW) TO STA 642+37.34
(STA 620+00.00 TO STA 627+37.00 SEE MAINLINE TYPICALS)

BRIDGE LIMIT (NOT REPRESENTED BY TYPICAL SECTION)

STA 627+67.00 TO STA 630+83.01
(BRIDGE APPROACH STA 627+37.00 TO STA 627+67.00 ON SKEW)
(BRIDGE APPROACH STA 630+83.01 TO STA 631+13.01 ON SKEW)

INTERSECTION OMISSION LIMITS (BY OTHERS)

STA 642+37.34 TO STA 644+48.97

[1] STA 631+13.01 TO STA 636+46.88 = 0.00'
STA 636+46.88 TO STA 636+98.42 = 0.00' TO 1.00' STUB
STA 636+98.42 TO STA 638+86.88 = 1.00' STUB TO 12.00'
STA 638+86.88 TO STA 642+14.18 = 12.00'

[2] SHOULDER CROSS SLOPE TRANSITIONS TO ACCOMMODATE BRIDGE PLANAR SECTION:
RT SHOULDER: STA 631+56 -1.5%
STA 632+19 -4.0%
LT SHOULDER: STA 631+56 1.5%
STA 632+66 -4.0%

[3] SHOULDER WIDTH:
STA 631+12.94 TO 632+20.00 = 10.0'
STA 632+20.00 TO 632+50.00 = TRANSITIONS 10.0' TO 12.0'

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

RAMP 7TH-A

STRUCTURAL DESIGN TRAFFIC: YEAR 2025

PV = 8,600 SU = 226 MU = 226

ROAD/STREET CLASSIFICATION: RAMP CLASS: I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 100% S = 100% M = 100%

TRAFFIC FACTOR: ACTUAL TF = 3.82 AC TYPE = N/A

MINIMUM TF = 10.05

PG GRADE: BINDER = N/A SURFACE = N/A

SUBGRADE SUPPORT RATING:

SSR = IBR = 3 (POOR)

TYP-14

LAYOUT	RLT	7/22/2011
DRAWN	MTH	6/18/2012
REVIEWED	AAP	3/22/2017

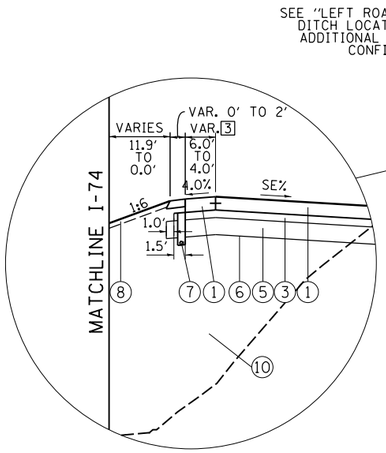
FILE NAME = D2CONCD-HPS-sht-typical13M.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED RAMPS
7TH-A

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

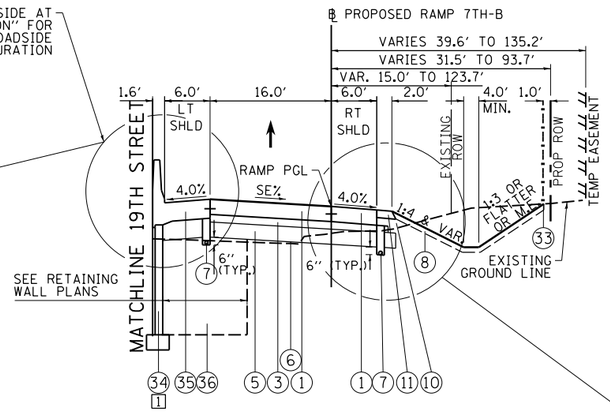
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	54
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		



LEFT ROADSIDE AT DITCH LOCATIONS

STA 531+48.06 LT TO 535+14.85 LT

SEE "LEFT ROADSIDE AT DITCH LOCATION" FOR ADDITIONAL ROADSIDE CONFIGURATION



PROPOSED RAMP 7TH–B PAVEMENT

STA 522+95.46 TO STA 535+14.85 [5]

(STA 521+59.61 TO 522+95.46 SEE 19TH STREET TYPICALS)

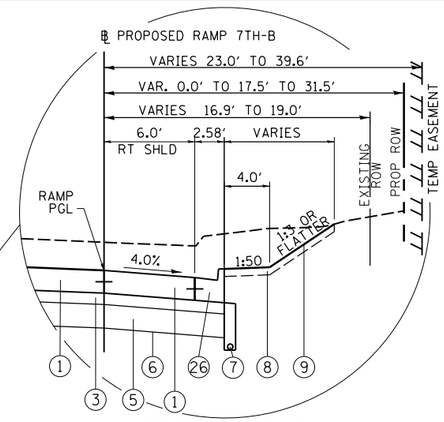
(STA 535+14.85 TO 541+31.49 SEE MAINLINE TYPICALS)

INTERSECTION OMISSION LIMITS (BY OTHERS)

STA 520+00.00 TO STA 521+12.83

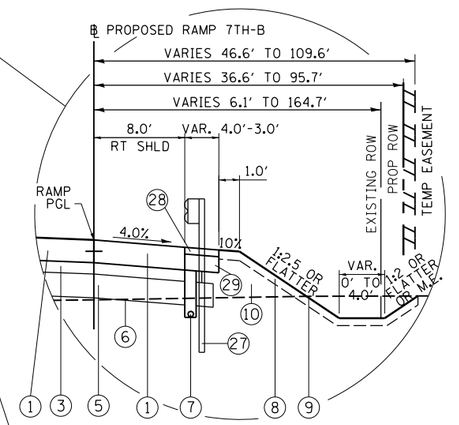
INTERSECTION LIMIT (NOT REPRESENTED BY TYPICAL SECTION)

STA 521+12.83 TO STA 521+59.61



RIGHT ROADSIDE AT GUTTER LOCATIONS

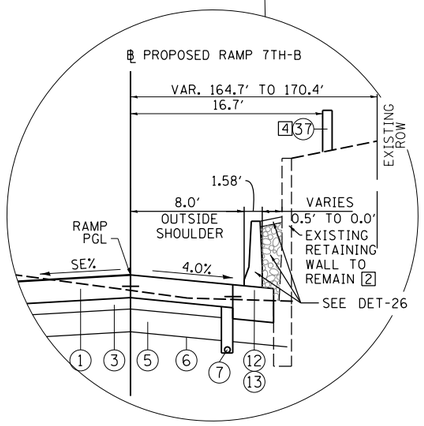
STA 522+95.01 RT TO 524+00.00 RT



RIGHT ROADSIDE AT GUARDRAIL LOCATIONS

STA 529+61.55 RT TO 530+55.00 RT

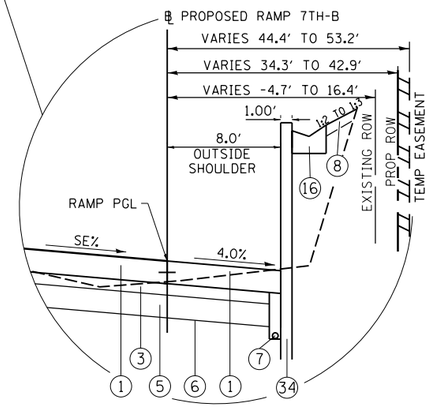
STA 531+30.00 RT TO 534+73.63 RT



RIGHT ROADSIDE AT SINGLE FACE BARRIER LOCATION

STA 534+73.63 RT TO STA 535+14.85 RT

SEE DETAIL ON SHEET DET-26



RIGHT ROADSIDE AT PROPOSED RETAINING WALL LOCATION

STA 530+55.00 RT TO STA 531+30.00 RT

[1] RETAINING WALL LIMITS: STA 522+95.01 LT TO STA 531+48.06 LT

[2] EXISTING RETAINING WALL LIMITS: STA 534+99.75, 9.10' RT TO STA 537+48.43, 20.53' RT

[3] PAVED SHOULDER TRANSITION: 6.0' TO 4.0' STA 531+48.50 TO 531+98.50

AGGREGATE SHOULDER TRANSITION: 0.0' TO 2.0' STA 531+48.50 TO 531+98.50

[4] PROPOSED NOISE ABATEMENT WALL BEGINS AT STA 534+86.96 AND ENDS BEYOND THIS TYPICAL SECTION LIMIT. SEE NOISE ABATEMENT WALL PLANS FOR DETAILS.

[5] SEE MAINLINE TYPICAL SECTIONS BEYOND STA 535+14.85 (RAMP 7TH-B) = STA 64+35.00 (I-74)

[6] M.E. = MATCH EXISTING

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
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17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
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21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

RAMP 7TH-B

STRUCTURAL DESIGN TRAFFIC: YEAR 2025

PV = 6,020 SU = 158 MU = 158

ROAD/STREET CLASSIFICATION: RAMP CLASS: I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 100% S = 100% M = 100%

TRAFFIC FACTOR: ACTUAL TF = 2.67 AC TYPE = N/A

MINIMUM TF = 10.05

PG GRADE: BINDER = N/A SURFACE = N/A

SUBGRADE SUPPORT RATING:

SSR = IBR = 3 (POOR)

TYP-15

LAYOUT	RLT	7/22/2011
DRAWN	MTH	6/18/2012
REVIEWED	AAP	3/22/2017

FILE NAME =	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
D2CONCD-HPS-sht-typical14M.dgn		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED RAMPS
7TH–B

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1)R-1 & 8)-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	55

CONTRACT NO. 64E26

ILLINOIS FED. AID PROJECT

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Alfred Benesch & Company
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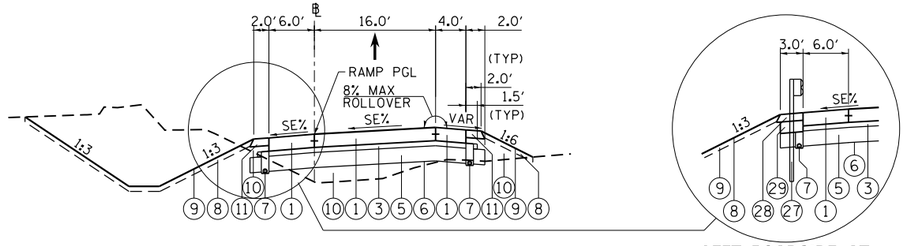
Chicago, Illinois 60601
312-565-0450

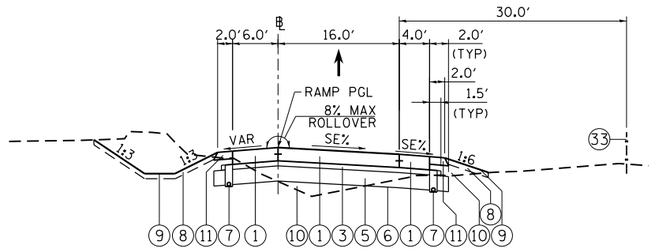


AVENUE OF THE CITIES RAMP RAC-A AND RAC-D
PROPOSED TYPICAL SECTION

PR RAMP RAC-A STA 1019+93.50 TO STA 1023+39.84
PR RAMP RAC-D STA 826+62.40 TO STA 829+74.10

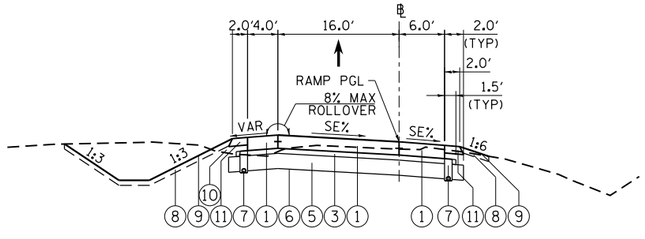
LEFT ROADSIDE AT
GUARDRAIL LOCATIONS

STA 826+62.40 TO STA 826+87.34



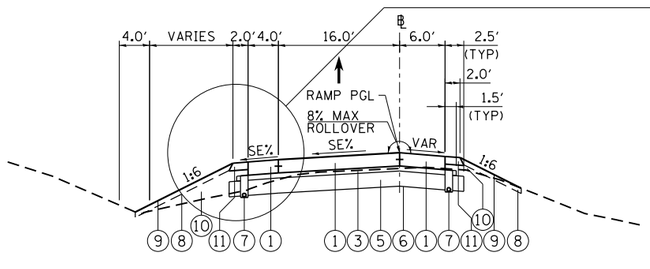
AVENUE OF THE CITIES RAMP RAC-A AND RAC-D
PROPOSED TYPICAL SECTION

PR RAMP RAC-A STA 1017+17.50 TO STA 1019+93.50
PR RAMP RAC-D STA 829+74.10 TO STA 833+63.68



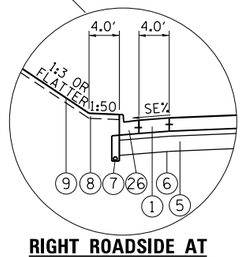
AVENUE OF THE CITIES RAMP RAC-C AND RAC-B
PROPOSED TYPICAL SECTION

PR RAMP RAC-C STA 726+00.00 TO STA 726+24.00
PR RAMP RAC-B STA 924+83.48 TO STA 928+51.77



AVENUE OF THE CITIES RAMP RAC-C AND RAC-B
PROPOSED TYPICAL SECTION

PR RAMP RAC-C STA 726+24.00 TO STA 739+88.50
PR RAMP RAC-B STA 921+95.44 TO STA 924+83.48



RIGHT ROADSIDE AT
CUT LOCATION

STA 735+30.00 TO STA 736+71.72

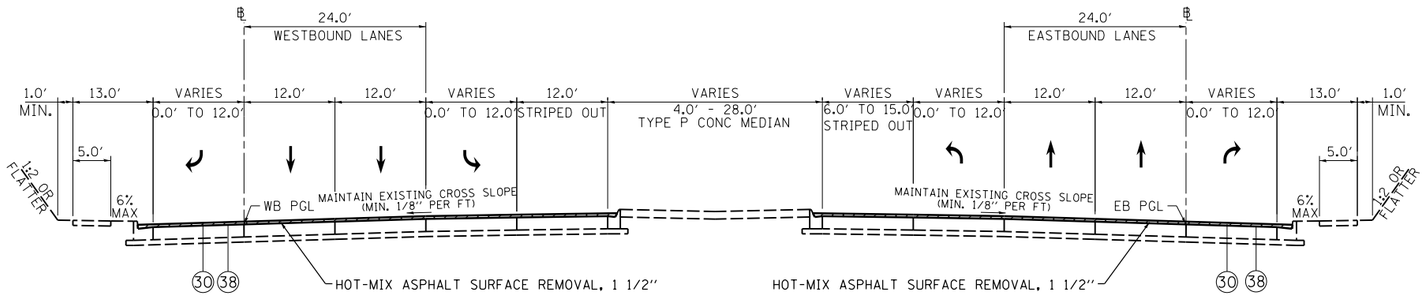
PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

FILE NAME = ...\D2CONCD-ABC-sht-typical07.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS PROPOSED RAMPS AVENUE OF THE CITIES	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRM	REVISED -			74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	56
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -					CONTRACT NO. 64E26		
	PLOT DATE = 5/3/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		



AVENUE OF THE CITIES (23RD AVENUE)

EXISTING WB 23RD AVE STA 119+00 TO STA 126+75
EXISTING EB 23RD AVE STA 219+00 TO STA 226+75

BRIDGE OMISSION LIMITS:
EXISTING WB 23RD AVE STA 122+23 TO STA 123+83
EXISTING EB 23RD AVE STA 222+38 TO STA 223+98

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2″ (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4″ (JOINTED)
3. STABILIZED SUBBASE 4″
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2″
5. AGGREGATE SUBGRADE IMPROVEMENT 12″
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6″
8. TOPSOIL FURNISH AND PLACE, 4″
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6″
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42″ HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3″ (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9″
30. 1½″ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ″D″, N50
31. 1½″ POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4′
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

LAYOUT	RLT	7/22/2011
DRAWN	MTH	6/18/2012
REVIEWED	AAP	3/22/2017

TYP-17

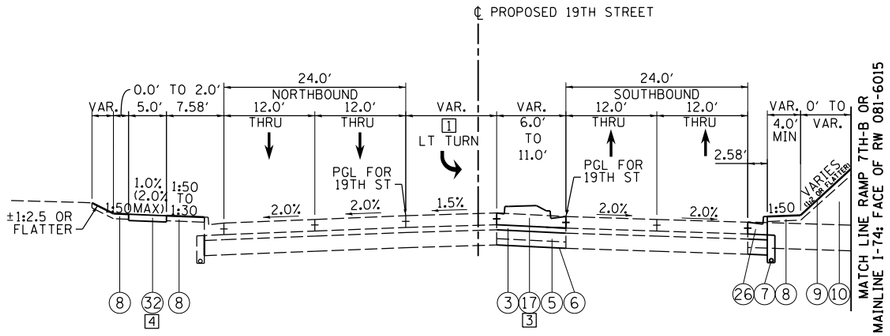
FILE NAME = D2CONCD-HPS-sht-typical14L.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED LOCAL ROADS
AVENUE OF THE CITIES (23RD AVENUE) RESURFACING

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	57
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			



PROPOSED 19TH STREET

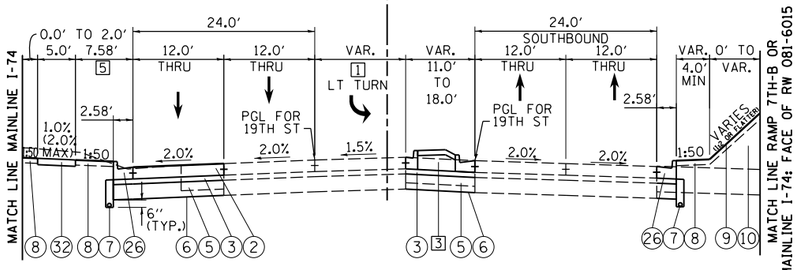
STA 1915+96.34 TO STA 1917+43.85

1 LEFT TURN LANE WIDTH
STA 1915+96.34 TO STA 1916+71.35 = 12.00′
STA 1916+71.35 TO STA 1918+46.35 = 12.00′ TO 0.00′

2 MEDIAN WIDTH
STA 1915+96.34 TO STA 1916+71.35 = 6.00′
STA 1916+71.35 TO STA 1918+46.35 = 6.00′ TO 18.00′
STA 1918+46.35 TO STA 1919+22.55(NB)/1920+29.63(SB) = 18.00′

3 MEDIAN SHALL BE PAID FOR ACCORDING TO THE FOLLOWING:
- STA 1915+96.36 TO STA 1916+71.35 = (17) WHICH SHALL INCLUDE THE CONSTRUCTION OF B-6.12 C&G FOR NB AND B-6.24 C&G FOR SB
- STA 1916+71.35 TO STA 1917+58.84 = (18)
- STA 1917+58.84 TO STA 1925+56.76 = (21) AND (26)

4 SIDEWALK RECONSTRUCTION LIMITS FOR TYPICAL SECTION:
STA 1917+00.00 TO STA 1917+43.85



PROPOSED 19TH STREET

STA 1917+43.85 TO STA 1919+22.55(NB)/1920+29.63(SB)

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2″ (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4″ (JOINTED)
3. STABILIZED SUBBASE 4″
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2″
5. AGGREGATE SUBGRADE IMPROVEMENT 12″
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6″
8. TOPSOIL FURNISH AND PLACE, 4″
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6″
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42″ HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3″ (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9″
30. 1½″ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ″D″, N50
31. 1½″ POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4′
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

19TH ST

STRUCTURAL DESIGN TRAFFIC: YEAR 2025

PV = 10,234 SU = 269 MU = 269

ROAD/STREET CLASSIFICATION: ARTERIAL CLASS: I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 32% S = 45% M = 45%

TRAFFIC FACTOR: ACTUAL TF = 2.04 AC TYPE = N/A

MINIMUM TF = N/A

PG GRADE: BINDER =N/A SURFACE =N/A

SUBGRADE SUPPORT RATING:

SSR = IBR = 3 (POOR)

TYP-18

HANSON

LAYOUT	RLT	12/13/2016
DRAWN	MTH	12/13/2016
REVIEWED	AAP	3/22/2017

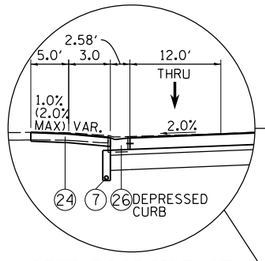
FILE NAME = D2CONCD-HPS-sht-typical15L.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED LOCAL ROADS
19TH STREET

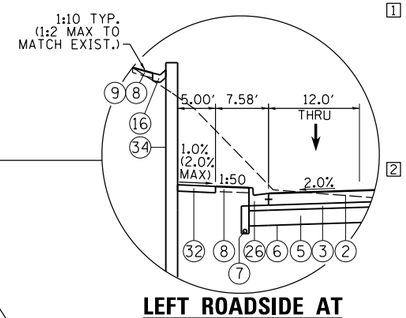
SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	58
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		



LEFT ROADSIDE AT ENTRANCE LOCATIONS

STA 1930+12.58 TO STA 1930+71.01



LEFT ROADSIDE AT RETAINING WALL LOCATION

STA 1924+75.00 TO STA 1928+22.00

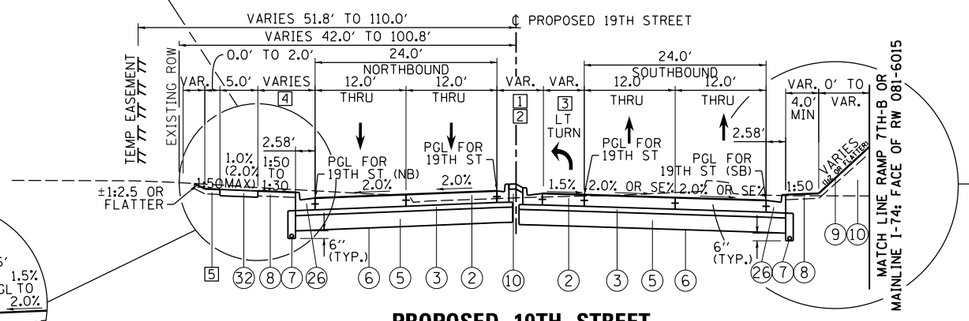
[1] MEDIAN WIDTH
STA 1919+22.55(NB)/1920+29.63(SB) TO STA 1924+84.87 = 18.00'
STA 1924+84.87 TO STA 1926+59.87 = 18.00' TO 6.00'
STA 1926+59.87 TO STA 1928+07.90 = 6.00'
STA 1928+72.95 TO STA 1933+28.51 = VARIES 16.00' TO 4.00'
STA 1933+28.51 TO STA 1935+74.23 = 4.00'
STA 1937+06.68 TO STA 1950+00.18 = 4.00'
STA 1950+00.00 (NB) TO STA 1955+48.65 (NB) & STA 2050+00.00 (SB) TO STA 2055+49.71 (SB) = VARIES 4.00' TO 15.33'

[2] MEDIAN SHALL BE PAID FOR ACCORDING TO THE FOLLOWING:
- STA 1917+58.84 TO STA 1925+56.76 = (21) AND (26)
- STA 1925+56.76 TO STA 1926+59.87= (18)
- STA 1926+59.87 TO STA 1928+07.90 = (17) WHICH SHALL INCLUDE THE CONSTRUCTION OF B-6.24 C&G FOR NB AND B-6.12 C&G FOR SB
- STA 1928+79.84 TO STA 1930+63.84 = (21) AND (26)
- STA 1930+63.84 TO STA 1931+26.00 = (18)
- STA 1931+26.00 TO STA 1935+74.23 = (22)
- STA 1937+06.68 TO STA 1951+34.01 = (22)
- STA 1951+34.01 TO STA 1954+25.07 = (19)
- STA 1954+25.07 TO STA 1955+48.65 = (21) AND (20)

[3] LEFT TURN LANE WIDTH
STA 1924+84.87 TO STA 1926+59.87 = 0.00' TO 12.00'
STA 1926+59.87 TO STA 1928+07.90 = 12.00'

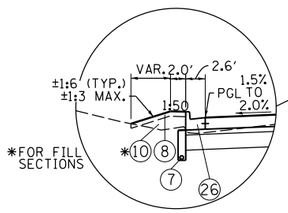
[4] SIDEWALK OFFSET FROM EOP
STA 1917+00.00 TO STA 1929+00.00 = 7.58'
STA 1929+00.00 TO STA 1929+20.00 = 7.58' TO 5.58'
STA 1929+20.00 TO STA 1931+26.00 = 5.58'

[5] ITEM TYPICALLY PAID FOR AS (8) EXCEPT BETWEEN STA 1920+41 LT TO STA 1924+67 LT AND STA 1922+28 RT TO STA 1926+31 RT WHERE IT SHALL BE PAID FOR AS STRUCTURAL ITEM "SLOPE WALL 4 INCH". SEE BRIDGE PLANS FOR DETAILS.



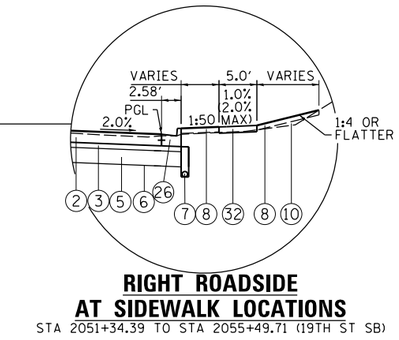
PROPOSED 19TH STREET

STA 1919+22.55(NB)/1920+29.63(SB) TO STA 1931+26.00 (19TH ST)
AND
STA 1951+34.01 TO STA 1955+48.65 (19TH ST NB) / STA 2051+34.39 TO STA 2055+49.71 (19TH ST SB)



LEFT ROADSIDE AT NO SIDEWALK LOCATIONS

STA 1951+34.01 TO STA 1955+48.65 (19TH ST NB)

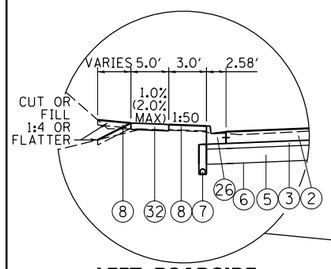


RIGHT ROADSIDE AT SIDEWALK LOCATIONS

STA 2051+34.39 TO STA 2055+49.71 (19TH ST SB)

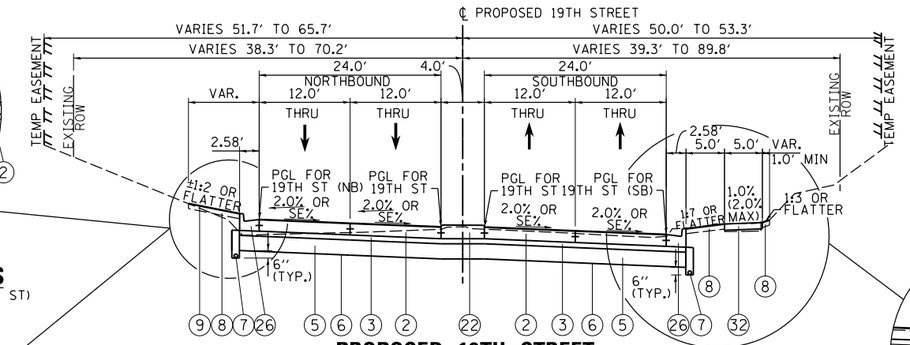
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

19TH STREET AND 11TH AVENUE INTERSECTION
STA 1928+07.90 TO STA 1928+72.95 (19TH ST)
AND
19TH STREET AND 18TH AVENUE INTERSECTION
STA 1955+48.65 TO STA 1955+95.79 (19TH ST NB) / STA 2055+49.71 TO STA 2055+95.97 (19TH ST SB)



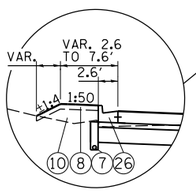
LEFT ROADSIDE AT SIDEWALK LOCATIONS

STA 1931+26.00 TO STA 1935+74.23 (19TH ST)



PROPOSED 19TH STREET

STA 1931+26.00 TO STA 1950+00.18 (19TH ST)
AND
STA 1950+00.00 TO STA 1951+34.01 (19TH ST NB) / STA 2050+00.00 TO STA 2051+34.39 (19TH ST SB)

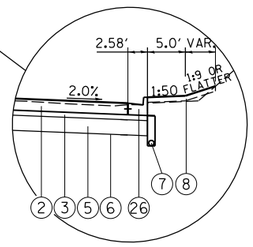


LEFT ROADSIDE

STA 1937+06.68 TO STA 1939+80.00 (19TH ST)

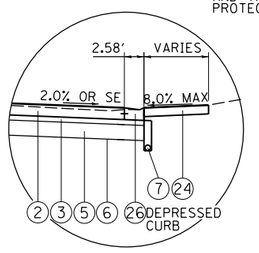
INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

19TH STREET AND 12TH AVENUE INTERSECTION
STA 1935+74.23 TO STA 1937+06.68 (19TH ST)



RIGHT ROADSIDE AT NO SIDEWALK LOCATIONS

STA 1931+26.00 TO STA 1931+64.70 (19TH ST)
AND
STA 1932+15.46 TO STA 1935+74.23 (19TH ST)



RIGHT OR LEFT ROADSIDE AT ENTRANCE LOCATION

STA 1931+64.70 RT TO STA 1932+15.46 RT (19TH ST)
STA 1933+22.00 LT TO STA 1933+54.00 LT (19TH ST)

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

HANSON

LAYOUT	RLT	7/22/2011
DRAWN	MTH	6/18/2012
REVIEWED	AAP	3/22/2017

FILE NAME = D2CONCD-HPS-sht-typical16L.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

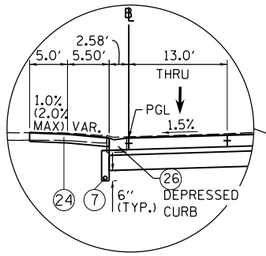
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED LOCAL ROADS
19TH STREET

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

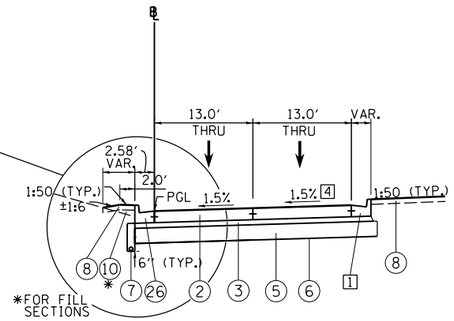
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	59
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

TYP-19



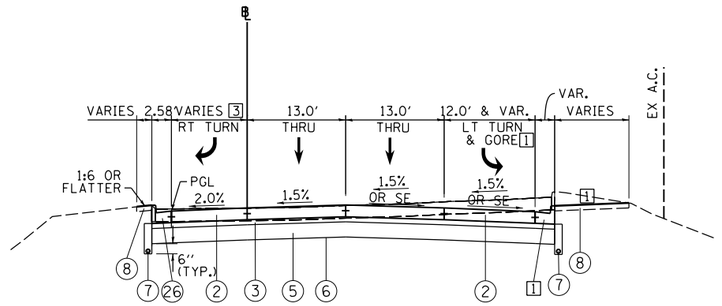
LEFT ROADSIDE AT ENTRANCE LOCATION

STA 1963+19.07 TO STA 1963+73.95



19TH STREET NB (ONE-WAY)

19TH ST NB STA 1955+95.79 TO STA 1969+80.00
AND
19TH ST NB STA 1982+20.18 TO 1985+60.00



19TH STREET NB (ONE-WAY)

19TH ST NB STA 1973+00.00 TO STA 1982+20.18

INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

19TH STREET NB AND AVENUE OF THE CITIES (23RD AVE) INTERSECTION
STA 1969+80.00 TO STA 1973+00.00
INTERSECTION TO BE MILLED AND RESURFACED = (30)

[1] RIGHT ROADSIDE ELEMENTS ACCORDING TO THE FOLLOWING:
19TH NB:
STA 1955+95.79 TO 1966+87.63 = (26)
STA 1966+87.63 TO 1968+09.25 = GORE AREA SEE SHEET RDWY-15 FOR DETAILS
STA 1968+09.25 TO 1969+80.00 = SEE RAMP TYP. AND SHEET RDWY-15 FOR DETAILS
STA 1973+00.00 TO 1974+32.00 = (26)
STA 1974+32.00 TO 1978+51.13 = 4" WIDE SHOULDER (2) AT 4.0%, 2' WIDE (6) AT 6.0% AND 1:4 TO MATCHLINE I-74 CROSS SECTIONS
STA 1978+51.13 TO 1980+83.74 = SEE RAMP TYP. AND SHEET RDWY-17 FOR DETAILS
STA 1980+83.74 TO 1982+20.18 = GORE AREA SEE SHEET RDWY-17 FOR DETAILS
STA 1982+20.18 TO 1985+60.00 = (26)

19TH SB:
STA 2055+95.97 TO 2063+43.26 = (26)
STA 2063+43.26 TO 2063+89.49 = GORE AREA SEE SHEET RDWY-15 FOR DETAILS
STA 2063+89.49 TO 2066+53.41 = SEE RAMP TYP. AND SHEET RDWY-15 FOR DETAILS
STA 2066+53.41 TO 2068+78.00 = (26)
STA 2074+30.00 TO 2078+00.00 = (26)

[2] LEFT ROADSIDE ELEMENTS ACCORDING TO THE FOLLOWING:
19TH SB:
STA 2074+30.00 TO 2074+54.08 = (26)
STA 2074+54.08 TO 2076+53.21 = SEE RAMP TYP. AND SHEET RDWY-15 FOR DETAILS
STA 2076+53.21 TO 2077+32.71 = GORE AREA SEE SHEET RDWY-17 FOR DETAILS
STA 2077+32.71 TO 2078+00.00 = (26)

[3] DIMENSION TO BASELINE AND ROADWAY JOINT
STA 1973+00.00 TO STA 1974+75.00 = 10'
STA 1974+75.00 TO STA 1976+32.50 = TAPERS 10' TO 1' STUB

[4] CROSS SLOPE
STA 1955+95.79 TO STA 1969+80.00 = 1.5%
STA 1973+00.00 TO STA 1979+33.63 = 1.5%
STA 1979+33.63 TO STA 1980+14.63 = 1.5% TO -1.5%
STA 1980+14.63 TO STA 1982+25.00 = -1.5%
STA 1982+25.00 TO STA 1982+85.00 = TRANSITION -1.5% TO 1.5%
STA 1982+85.00 TO STA 1985+60.00 = 1.5%

[5] UNDERDRAINS FOR THIS TYPICAL SECTION DO NOT SPAN THE ENTIRE STATION RANGE OF THE TYPICAL SECTION. SEE THE DRAINAGE AND UTILITY SHEETS FOR SPECIFIC LIMITS OF UNDERDRAINS.

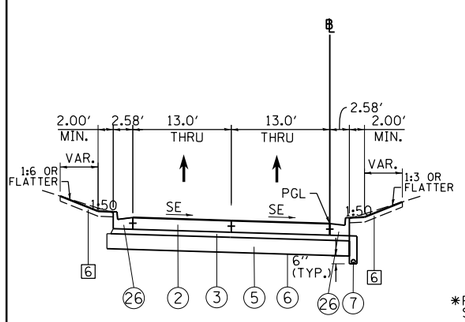
[6] ITEM TO BE PAID FOR AS EITHER (8) OR (32). SEE ROADWAY PLANS FOR LOCATIONS AND LIMITS. AREAS UNDER THE 19TH ST SB BRIDGES TO BE PAID FOR AS (32), SHALL NOT BE INSTALLED UNTIL AFTER THE ADJACENT PROPOSED PIER IS IN PLACE IN EITHER STAGE 2 OR 3. SEE SN 081-0184 AND SN 081-0185.

PROPOSED LEGEND:

(1) PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
(2) PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
(3) STABILIZED SUBBASE 4"
(4) AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
(5) AGGREGATE SUBGRADE IMPROVEMENT 12"
(6) GEOTECHNICAL REINFORCEMENT
(7) PIPE UNDERDRAINS, TYPE 2, 6"
(8) TOPSOIL FURNISH AND PLACE, 4"
(9) EROSION CONTROL BLANKET (SEE NOTE 8)
(10) EMBANKMENT
(11) AGGREGATE SHOULDERS, TYPE A 6"
(12) CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
(13) CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
(14) CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
(15) CONCRETE GUTTER, TYPE A (SPECIAL)
(16) CONCRETE GUTTER, TYPE B
(17) CONCRETE MEDIAN, TYPE SB (SPECIAL)
(18) CONCRETE MEDIAN, TYPE SB-6.24
(19) CONCRETE MEDIAN, TYPE SB-6.12
(20) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(21) CONCRETE MEDIAN SURFACE, 4 INCH
(22) CORRUGATED MEDIAN
(23) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
(24) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
(25) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
(26) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
(27) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
(28) HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
(29) SUBBASE GRANULAR MATERIAL, TYPE A 9"
(30) 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
(31) 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
(32) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
(33) CHAIN LINK FENCE, 4'
(34) RETAINING WALL (SEE RETAINING WALL PLANS)
(35) ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
(36) SELECT BACKFILL (SEE RETAINING WALL PLANS)
(37) NOISE ABATEMENT WALL
(38) POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

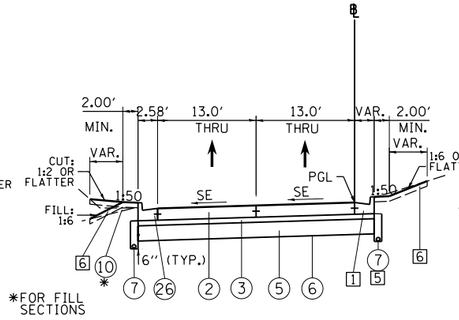
NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.



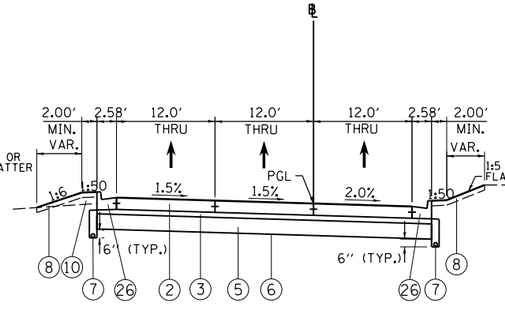
19TH STREET SB (ONE-WAY)

19TH ST SB STA 2055+95.97 TO STA 2059+09.00



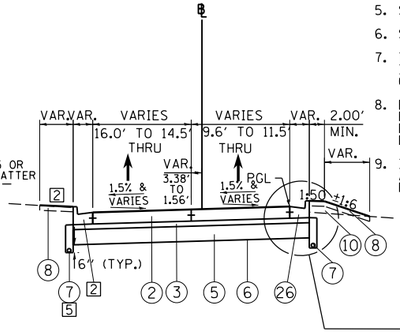
19TH STREET SB (ONE-WAY)

19TH ST SB STA 2059+09.00 TO STA 2066+53.41



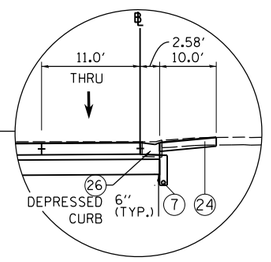
19TH STREET SB (ONE-WAY)

19TH ST SB STA 2066+53.41 TO STA 2068+20.00



19TH STREET SB (ONE-WAY)

19TH ST SB STA 2074+30.00 TO STA 2078+00.00



RIGHT ROADSIDE AT ENTRANCE LOCATION

STA 2077+38.91 TO STA 2077+87.70

INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

19TH STREET SB AND AVENUE OF THE CITIES (23RD AVE) INTERSECTION
STA 2068+20.00 TO STA 2074+30.00
INTERSECTION TO BE MILLED AND RESURFACED = (30)

LAYOUT	MGJ	7/24/2014
DRAWN	MTH	7/24/2014
REVIEWED	AAP	3/22/2017

FILE NAME = D2CONCD-HPS-sht-typical18L.dgn	USER NAME = petke00954	DESIGNED - MGJ	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 4/20/2017	DATE - 3/23/2017	REVISED -

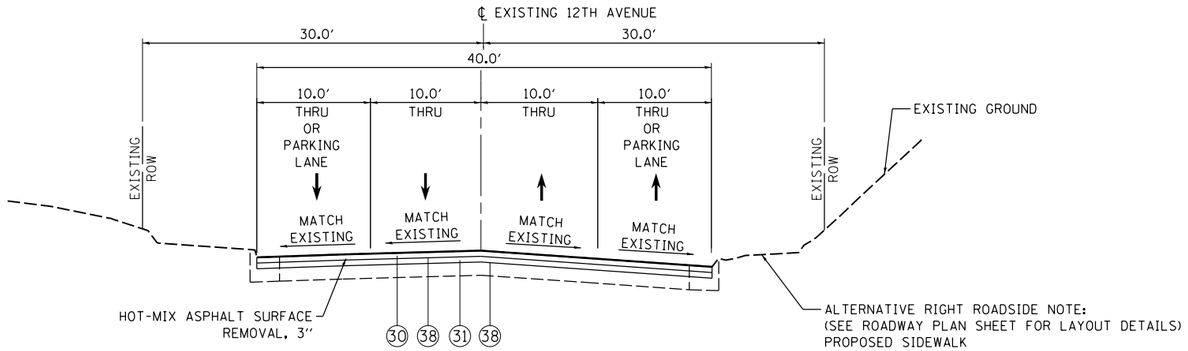
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
PROPOSED LOCAL ROADS
19TH STREET

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

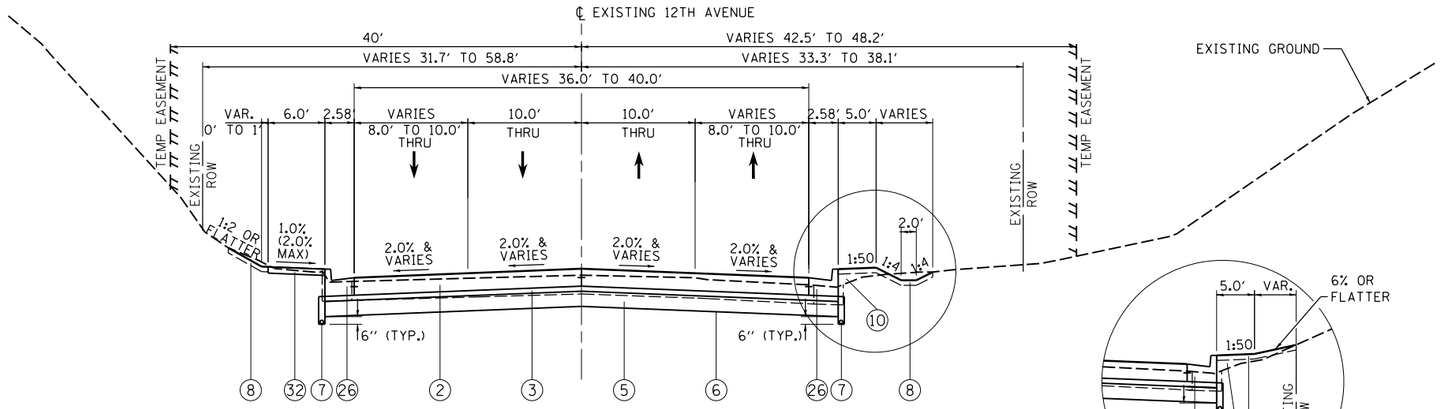
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	60
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

TYP-20



PROPOSED 12TH AVENUE

STA 123+00.00 TO STA 128+30.50



PROPOSED 12TH AVENUE

STA 130+41.69(WB)/131+53.35(EB) TO STA 132+05.00

ALTERNATE RIGHT ROADSIDE

STA 131+75.00 TO 132+05.00

INTERSECTION LIMITS (NOT REPRESENTED BY TYPICAL SECTION)

12TH AVENUE AND 19TH STREET INTERSECTION
STA 128+30.50 TO STA 130+41.69 (12TH AVE WB)
STA 128+30.50 TO STA 131+53.35 (12TH AVE EB)

PROPOSED LEGEND:

1. PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)
2. PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)
3. STABILIZED SUBBASE 4"
4. AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"
5. AGGREGATE SUBGRADE IMPROVEMENT 12"
6. GEOTECHNICAL REINFORCEMENT
7. PIPE UNDERDRAINS, TYPE 2, 6"
8. TOPSOIL FURNISH AND PLACE, 4"
9. EROSION CONTROL BLANKET (SEE NOTE 8)
10. EMBANKMENT
11. AGGREGATE SHOULDERS, TYPE A 6"
12. CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
13. CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
14. CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
15. CONCRETE GUTTER, TYPE A (SPECIAL)
16. CONCRETE GUTTER, TYPE B
17. CONCRETE MEDIAN, TYPE SB (SPECIAL)
18. CONCRETE MEDIAN, TYPE SB-6.24
19. CONCRETE MEDIAN, TYPE SB-6.12
20. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
21. CONCRETE MEDIAN SURFACE, 4 INCH
22. CORRUGATED MEDIAN
23. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
24. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
25. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
26. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
27. STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
28. HOT-MIX ASPHALT SHOULDERS, 3" (FOR STABILIZATION AT SPBGR)
29. SUBBASE GRANULAR MATERIAL, TYPE A 9"
30. 1½" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50
31. 1½" POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
32. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
33. CHAIN LINK FENCE, 4'
34. RETAINING WALL (SEE RETAINING WALL PLANS)
35. ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
36. SELECT BACKFILL (SEE RETAINING WALL PLANS)
37. NOISE ABATEMENT WALL
38. POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

NOTES:

1. SEE ROADWAY PLANS FOR PAVEMENT WIDTH TRANSITION LOCATIONS.
2. FOR SUPERELEVATION TRANSITIONS, SEE DETAIL SHEETS FOR MAINLINE AND PROFILE SHEETS FOR RAMPS AND LOCAL ROADS.
3. SEE DRAINAGE PLANS FOR LOCATIONS OF PIPE UNDERDRAINS AND DRAINAGE STRUCTURES.
4. SEE PAVEMENT GORE DETAILS FOR LAYOUT AND SLOPE INFORMATION.
5. SEE CROSS SECTIONS FOR SIDE SLOPE AND DITCH DETAILS.
6. SEE RETAINING WALL PLANS FOR LOCATION AND DETAILS.
7. THE UNIT WEIGHT TO CALCULATE ALL HOT MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN FOR MIX D. APPLICATION RATE FOR TACK COAT IS 0.05 LB/SQ FT.
8. EROSION CONTROL BLANKET TO BE PLACED AS NEEDED ON SLOPES 1:3 AND FLATTER (TURF REINFORCEMENT ON STEEPER THAN 1:3) AND SHALL NOT BE PLACED ON FURROWED SLOPES. SEE EROSION AND SEDIMENT CONTROL SHEETS FOR LOCATIONS.
9. IF CURB AND GUTTER OR PCC PAVEMENT IS CONSTRUCTED AFTER OCTOBER 15TH AND THE ROAD WILL BE OPEN TO TRAFFIC PRIOR TO THE FOLLOWING APRIL 15TH, PROTECTIVE COAT SHALL BE USED.

12TH AVENUE

STRUCTURAL DESIGN TRAFFIC: YEAR 2025

PV = 7,766 SU = 204 MU = 204

ROAD/STREET CLASSIFICATION: ARTERIAL CLASS: I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 32% S = 45% M = 45%

TRAFFIC FACTOR: ACTUAL TF = 1.55 AC TYPE = N/A

MINIMUM TF = N/A

PG GRADE: BINDER = N/A SURFACE = N/A

SUBGRADE SUPPORT RATING:

SSR = IBR = 3 (POOR)

TYP-21

LAYOUT	RLT	1/22/2011
DRAWN	MTH	12/22/2011
REVIEWED	AAP	3/22/2017

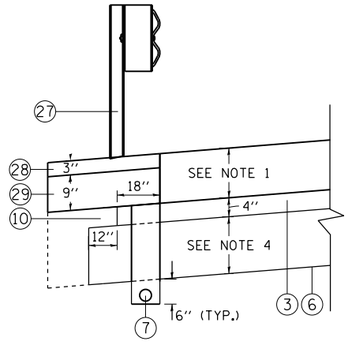
FILE NAME =	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
D2CONCD-HPS-sht-typical17L.dgn		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

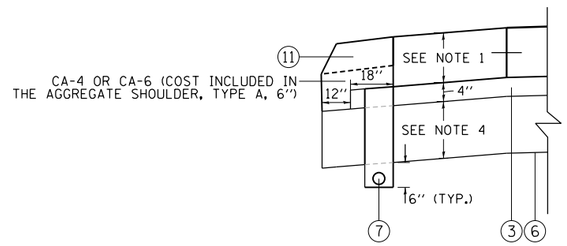
TYPICAL SECTIONS
PROPOSED LOCAL ROADS
12TH AVENUE

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

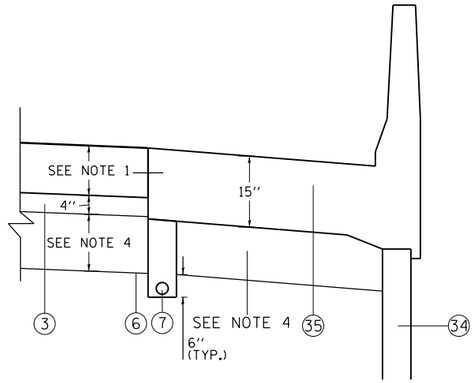
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(8)-1)R-1 & 8)-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	61
		CONTRACT NO. 64E26		
		ILLINOIS FED. AID PROJECT		



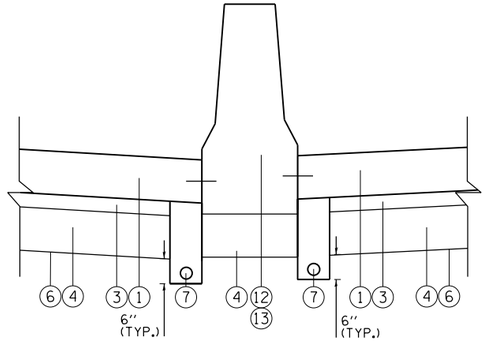
GUARDRAIL
(MAINLINE AND RAMP)



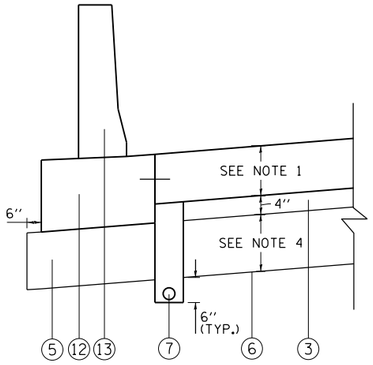
AGGREGATE SHOULDER
(MAINLINE AND RAMPS)



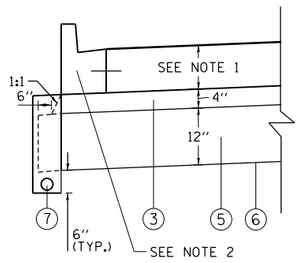
RETAINING WALL
(MAINLINE AND RAMP)



MEDIAN BARRIER
(MAINLINE)



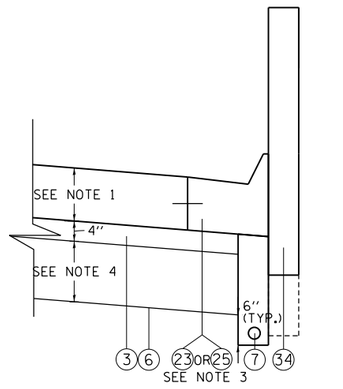
BARRIER
(MAINLINE AND RAMPS)



GUTTER OR CURB AND GUTTER
(RAMP AND LOCAL ROAD)

NOTES

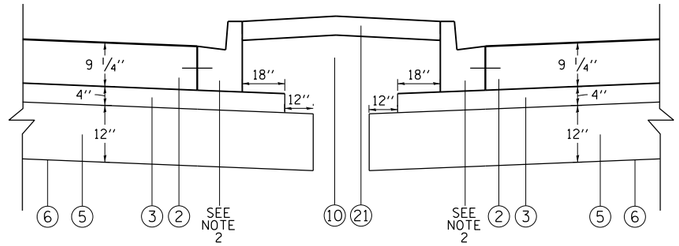
1. FOR MAINLINE AND RAMPS, PAVEMENT TO BE PAID FOR AS (1).
   FOR LOCAL ROADS, PAVEMENT TO BE PAID FOR AS (2).
2. FOR MAINLINE AND RAMPS, GUTTER TO BE PAID FOR AS (15), (23), OR (25).
   FOR LOCAL ROADS, CURB AND GUTTER TO BE PAID FOR AS ITEMS (17), (18), (19), (20), OR (26). SEE ROADWAY PLANS FOR LOCATIONS.
3. FOR MAINLINE, ITEM TO BE PAID FOR AS (25).
   FOR RAMP, ITEM TO BE PAID FOR AS (23).
   SEE ROADWAY PLANS FOR LOCATIONS.
4. FOR MAINLINE, SUBGRADE TO BE PAID FOR AS (4).
   FOR RAMPS AND LOCAL ROADS, SUBGRADE TO BE PAID FOR AS (5).



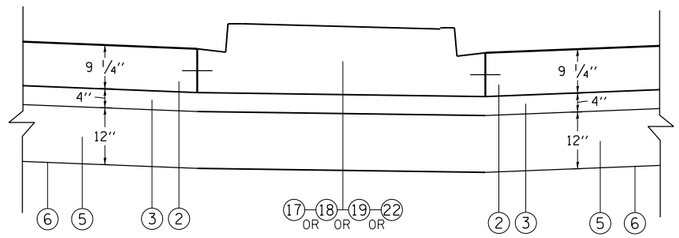
MOUNTABLE CURB AND GUTTER WITH RETAINING WALL
(MAINLINE AND RAMP)

PROPOSED LEGEND:

(1) PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2'' (JOINTED)
(2) PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4'' (JOINTED)
(3) STABILIZED SUBBASE 4''
(4) AGGREGATE SUBGRADE IMPROVEMENT 13 1/2''
(5) AGGREGATE SUBGRADE IMPROVEMENT 12''
(6) GEOTECHNICAL REINFORCEMENT
(7) PIPE UNDERDRAINS, TYPE 2, 6''
(8) TOPSOIL FURNISH AND PLACE, 4''
(9) EROSION CONTROL BLANKET
(10) EMBANKMENT
(11) AGGREGATE SHOULDERS, TYPE A 6''
(12) CONCRETE BARRIER BASE OR CONCRETE BARRIER BASE (SPECIAL)
(13) CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
(14) CONCRETE BARRIER, VARIABLE CROSS-SECTION 42'' HEIGHT OR CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT (SEE PLANS FOR LOCATIONS)
(15) CONCRETE GUTTER, TYPE A (SPECIAL)
(16) CONCRETE GUTTER, TYPE B
(17) CONCRETE MEDIAN, TYPE SB (SPECIAL)
(18) CONCRETE MEDIAN, TYPE SB-6.24
(19) CONCRETE MEDIAN, TYPE SB-6.12
(20) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(21) CONCRETE MEDIAN SURFACE, 4 INCH
(22) CORRUGATED MEDIAN
(23) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24
(24) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
(25) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06
(26) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
(27) STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
(28) HOT-MIX ASPHALT SHOULDERS, 3'' (FOR STABILIZATION AT SPBGR)
(29) SUBBASE GRANULAR MATERIAL, TYPE A 9''
(30) 1½'' POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ''D'', N50
(31) 1½'' POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50
(32) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
(33) CHAIN LINK FENCE, 4'
(34) RETAINING WALL (SEE RETAINING WALL PLANS)
(35) ANCHORAGE SLAB (SEE RETAINING WALL OR ANCHORAGE SLAB PLANS)
(36) SELECT BACKFILL (SEE RETAINING WALL PLANS)
(37) NOISE ABATEMENT WALL
(38) POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)



MEDIANS
(LOCAL ROAD)



LAYOUT	CBP	5/15/2013
DRAWN	MTH	5/18/2013
REVIEWED	AAP	3/22/2017

TYP-22

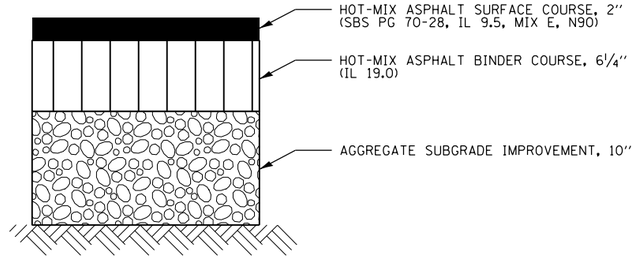
FILE NAME = D2CONCD-HPS-sht-typical19M.dgn	USER NAME = petke00954	DESIGNED - CBP	REVISED -
		DRAWN - MTH	REVISED -
	PLOT SCALE =	CHECKED - AAP	REVISED -
	PLOT DATE = 3/28/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
TYPICAL SUBBASE EXTENSIONS

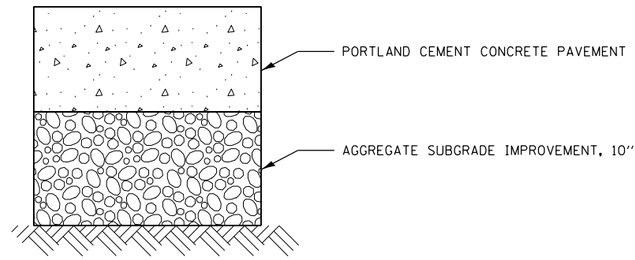
SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	62
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		



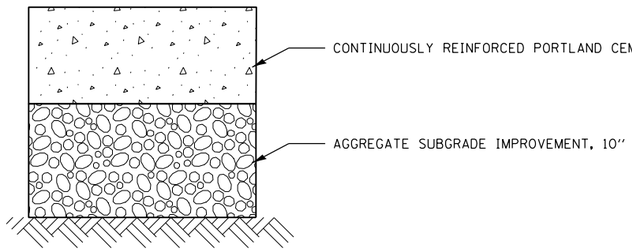
HOT-MIX ASPHALT OPTION

1. THE 6¼" BINDER THICKNESS SHOULD BE PLACED IN 2 LIFTS.
2. THIS WORK SHALL CONSIST OF DESIGNING, PRODUCING AND CONSTRUCTING A HMA SURFACE COURSE ON A PREPARED BASE, ACCORDING TO SECTIONS 311, 406, 1030 AND 1102 OF THE CURRENT STANDARD SPECIFICATIONS.



PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) OPTION

1. THE CONTRACTOR SHALL SAW TRANSVERSE JOINTS IN THE PAVEMENT ACCORDING TO THE DETAIL FOR JOINTED PCC PAVEMENT IN STANDARD 420101, EXCEPT THAT DOWEL BARES ARE NOT REQUIRED. THESE JOINTS SHALL NOT BE SEALED.
2. TIE BARS, SAWED JOINTS AND ALL OTHER REQUIRED MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR TEMPORARY PAVEMENT.



CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT OPTION

1. THE CONTRACTOR SHALL SAW TRANSVERSE JOINTS IN THE PAVEMENT, EXCEPT THAT DOWEL BARS ARE NOT REQUIRED. THESE JOINTS SHALL NOT BE SEALED.
2. TIE BARS, SAWED JOINTS AND ALL OTHER REQUIRED MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR TEMPORARY PAVEMENT.

ADDITIONAL NOTES

1. ALL WORK AND MATERIALS REQUIRED TO INSTALL TEMPORARY PAVEMENT SHALL BE INCLUDED IN THE CONTRACT UNIT COST PER SQUARE YARD FOR TEMPORARY PAVEMENT.
2. THE TEMPORARY PAVEMENT STRUCTURE SHALL BE REMOVED AFTER THE STAGE(S) IT IS NEEDED IS COMPLETED. REMOVAL SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR TEMPORARY PAVEMENT REMOVAL.

LAYOUT	CDM	7/16/2015
DRAWN	CDM	7/16/2015
REVIEWED	AAP	3/22/2017

TYP-23

FILE NAME = D2CONCD-HPS-sht-typical20M.dgn	USER NAME = petke00954	DESIGNED - CDM	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**TYPICAL SECTIONS TEMPORARY PAVEMENT OPTIONS FOR MAINTENANCE OF TRAFFIC**			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - CDM	REVISED -					74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	63
	PLOT SCALE =	CHECKED - AAP	REVISED -							CONTRACT NO. 64E26		
	PLOT DATE = 3/28/2017	DATE - 3/23/2017	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.			ILLINOIS	FED. AID PROJECT	

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

LOCATION				EARTH EXCAVATION (2020100)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	TOPSOIL FURNISH AND PLACE, 4" (21101615)
MAINLINE I-74 AND RAMPS				CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD	SQUARE YARD
MAINLINE I-74 STAGE 1-0								
STATION	114+00.00	TO	120+00.00	216.9	162.6	485.0	-322.4	1,428.3
	120+00.00	TO	125+00.00	158.3	118.8	141.6	-22.8	166.7
STATION	125+00.00	TO	130+00.00	283.4	212.6	372.5	-159.9	738.3
	130+00.00	TO	135+00.00	692.6	519.6	500.0	19.6	1,120.0
STATION	135+00.00	TO	140+00.00	834.4	625.8	455.5	170.4	1,043.3
	140+00.00	TO	145+00.00	242.8	182.1	595.5	-413.4	1,515.0
	145+00.00	TO	150+00.00	228.5	171.4	508.2	-336.9	1,723.3
	150+00.00	TO	157+92.72	317.7	238.2	729.2	-491.0	2,491.1
SUB TOTAL				2,974.8	2,231.1	3,787.5	-1,556.4	10,226.1
MAINLINE I-74 STAGE 1-1								
STATION	122+00.00	TO	125+00.00	875.3	656.5	118.7	537.8	968.3
	125+00.00	TO	130+00.00	1,453.4	1090.1	120.4	969.7	1,048.3
STATION	130+00.00	TO	135+00.00	2,071.7	1553.8	37.4	1516.3	396.7
	135+00.00	TO	140+00.00	2,332.8	1749.6	26.1	1723.5	605.0
STATION	140+00.00	TO	145+00.00	2,593.1	1944.9	3.2	1941.6	765.0
	145+00.00	TO	150+00.00	1,888.8	1416.6	16.6	1400.0	468.3
STATION	150+00.00	TO	155+00.00	1,632.2	1224.2	38.8	1185.4	506.7
SUB TOTAL				12,847.3	9,635.5	361.2	9,274.3	4,758.3
MAINLINE I-74 STAGE 1-2								
STATION	122+00.00	TO	125+00.00	661.3	496.0	124.8	371.2	1,076.7
	125+00.00	TO	130+00.00	3,189.4	2392.1	138.7	2253.4	1,768.3
STATION	130+00.00	TO	135+00.00	4,292.5	3219.4	66.5	3152.9	901.7
	135+00.00	TO	140+00.00	3,101.8	2326.3	11.4	2314.9	1,410.0
STATION	140+00.00	TO	145+00.00	3,933.0	2949.7	237.7	2712.0	1,576.7
	145+00.00	TO	150+00.00	3,160.0	2370.0	379.6	1990.4	1,236.7
STATION	150+00.00	TO	155+00.00	2,294.8	1721.1	463.8	1257.3	1,281.7
	155+00.00	TO	157+50.00	4,626.9	3470.2	16.9	3453.2	3,066.7
SUB TOTAL				25,259.7	18,944.8	1,439.4	17,505.3	12,318.3
AVENUE OF THE CITIES RAMP RAC-A STAGE 1-2								
STATION	1019+42.65	TO	1025+77.79	1,709.1	1281.8	284.0	997.9	2,799.9
SUB TOTAL				1,709.1	1,281.8	284.0	997.9	2,799.9
MAINLINE I-74 STAGE 1-3								
STATION	1023+50.00	TO	1024+00.00	154.4	115.8	0.0	115.8	0.0
SUB TOTAL				154.4	115.8	0.0	115.8	0.0
AVENUE OF THE CITIES RAMP RAC-A STAGE 1-3								
STATION	1017+67.00	TO	1019+42.65	971.4	728.6	11.5	717.1	1,010.8
SUB TOTAL				971.4	728.6	11.5	717.1	1,010.8
STAGE 1 TOTAL				43,916.8	32,937.6	5,883.6	27,054.0	31,113.5

NOTE:
MAINLINE/RAMP CROSS SECTIONS ARE SHEETED AT 50' INTERVALS.
EARTHWORK END AREA QUANTITIES BASED ON 10' INTERVAL
CROSS SECTIONS TO PROVIDE A MORE ACCURATE ESTIMATE.

• CONTRACTOR MAY BORROW EXCAVATION FOR STAGE 1-0 SHORTAGE AS NEEDED FROM DETENTION POND EXCAVATION THAT IS TO COMPLETED IN STAGE 1-2 OR OTHER LOCATIONS WITHIN THE PROJECT LIMITS WITH EXCESS. PAYMENT WILL NOT BE MADE FOR FURNISHED EXCAVATION

EARTHWORK SUMMARY			
PAY ITEM NO.	DESCRIPTION	UNIT	QUANTITY
20200100	EARTH EXCAVATION	CU YD	180,520
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	115,907

SCHQ-01

FILE NAME = ...\D2CONCD-ABC-sht-schedule01M.dgn	USER NAME = jtardy	DESIGNED - BMH	REVISED -
		DRAWN - BMH	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
EARTHWORK

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	64
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

LOCATION				EARTH EXCAVATION (2020100)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	TOPSOIL FURNISH AND PLACE, 4" (21101615)
MAINLINE I-74 AND RAMPS				CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD	SQUARE YARD
MAINLINE I-74 STAGE 2								
STATION	48+18.21	TO	53+00.00	474.5	355.9	5116.9	-4761.0	0.0
	53+00.00	TO	58+42.09	960.1	720.1	2022.9	-1302.7	0.0
STATION	61+87.58	TO	67+00.00	2428.2	1821.1	2316.5	-495.4	419.8
	67+00.00	TO	71+20.08	3132.4	2349.3	18.0	2331.4	80.6
STATION	71+32.41	TO	76+00.00	4348.4	3261.3	0.8	3260.5	568.4
	76+00.00	TO	81+00.00	4933.1	3699.8	0.4	3699.4	644.5
STATION	81+00.00	TO	85+00.00	2,637.1	1977.8	150.3	1827.6	396.7
	85+00.00	TO	90+00.00	3,261.9	2446.5	275.6	2170.9	886.7
STATION	90+00.00	TO	95+00.00	985.6	739.2	777.1	-38.0	1,523.3
	95+00.00	TO	100+00.00	1,629.4	1222.1	49.8	1172.3	211.7
STATION	100+00.00	TO	105+00.00	6,110.4	4582.8	31.9	4550.9	1,493.3
	105+00.00	TO	110+00.00	6,461.7	4846.3	4.5	4841.8	1,446.7
STATION	110+00.00	TO	115+00.00	4,027.1	3020.4	66.8	2953.6	4,273.7
	115+00.00	TO	120+00.00	2,603.9	1952.9	219.2	1733.7	4,352.3
STATION	120+00.00	TO	123+00.00	1,461.0	1095.8	19.9	1075.9	211.7
SUB TOTAL				45,455.0	34,091.2	11,070.4	23,020.8	16,509.2
RAMP 7TH A STAGE 2								
STATION	631+75.66	TO	635+00.00	921.5	691.1	796.4	-105.3	822.6
	635+00.00	TO	640+00.00	852.6	639.5	453.0	186.5	434.4
	640+00.00	TO	642+37.34	830.6	622.9	143.9	479.0	551.0
SUB TOTAL				2,604.6	1,953.5	1,393.3	560.2	1,808.1
AVENUE OF THE CITIES RAMP RAC-D STAGE 2								
STATION	825+07.00	TO	830+00.00	1,341.8	1006.3	50.6	955.7	1,405.0
STATION	830+00.00	TO	833+57.00	1,674.4	1255.8	369.5	886.3	3,090.1
SUB TOTAL				3,016.2	2,262.1	420.2	1,842.0	4,495.1
STAGE 2 TOTAL				51,075.8	38,306.9	12,883.8	25,423.0	22,812.4

NOTE:
MAINLINE/RAMP CROSS SECTIONS ARE SHEETED AT 50' INTERVALS.
EARTHWORK END AREA QUANTITIES BASED ON 10' INTERVAL
CROSS SECTIONS TO PROVIDE A MORE ACCURATE ESTIMATE.

SCHQ-02

FILE NAME = ...\D2CONCD-ABC-sht-schedule02M.dgn	USER NAME = jtardy	DESIGNED - BMH	REVISED -
		DRAWN - BMH	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
EARTHWORK

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	65
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

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Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

LOCATION				EARTH EXCAVATION (2020100)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	TOPSOIL FURNISH AND PLACE, 4" (21101615)
MAINLINE I-74 AND RAMPS				CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD	SQUARE YARD
MAINLINE I-74 STAGE 3								
STATION	48+86.26	TO	53+00.00	1,416.5	1062.3	21,893.9	-20831.6	4,731.6
	53+00.00	TO	57+08.09	5,262.6	3947.0	2,218.4	1728.6	3,448.3
STATION	60+77.59	TO	66+00.00	1,998.7	1499.0	4,465.3	-2966.3	440.5
	66+00.00	TO	70+90.46	3,117.4	2338.0	4.0	2334.0	588.1
STATION	71+35.59	TO	76+00.00	3,714.2	2785.6	2,038.1	747.5	2,532.7
	76+00.00	TO	81+00.00	4,120.3	3090.3	321.7	2768.5	1,278.7
STATION	81+00.00	TO	85+00.00	2,219.7	1664.8	5.8	1659.0	225.0
	85+00.00	TO	90+00.00	1,504.9	1128.7	71.9	1056.8	156.7
STATION	90+00.00	TO	95+00.00	1,370.7	1028.1	1,237.8	-209.7	1,555.0
	95+00.00	TO	100+00.00	1,297.2	972.9	310.5	662.4	2,537.7
STATION	100+00.00	TO	105+00.00	4,013.3	3010.0	172.8	2837.2	6,640.0
	105+00.00	TO	110+00.00	3,672.3	2754.2	14.3	2740.0	816.7
STATION	110+00.00	TO	115+00.00	4,577.3	3432.9	322.2	3110.7	5,874.7
	115+00.00	TO	122+00.00	1,288.1	966.0	235.1	730.9	191.7
SUB TOTAL				39,573.1	29,679.9	33,311.9	-3,632.0	31,017.1
RAMP 7TH B STAGE 3								
STATION	522+95.01	TO	525+00.00	53.5	40.1	472.8	-432.6	179.0
	525+00.00	TO	530+00.00	21.9	16.4	9,321.0	-9304.5	1,942.3
	530+00.00	TO	535+34.85	755.9	566.9	5,860.8	-5293.9	1,614.4
SUB TOTAL				831.3	623.4	15,654.6	-15,031.1	3,735.7
AVENUE OF THE CITIES RAMP RAC-B STAGE 3-1								
STATION	922+06.00	TO	925+00.00	288.2	216.2	194.7	21.4	483.3
STATION	925+00.00	TO	931+72.00	73.4	55.0	660.8	-605.8	555.0
SUB TOTAL				361.6	271.2	855.6	-584.4	1,038.3
AVENUE OF THE CITIES RAMP RAC-C STAGE 3-1								
STATION	724+10.00	TO	730+00.00	956.5	717.4	325.4	392.0	3,059.8
STATION	730+00.00	TO	737+18.22	2,597.0	1947.7	178.5	1769.2	5,077.4
SUB TOTAL				3,553.5	2,665.1	503.9	2,161.3	8,137.2
STAGE 3-1 SUB TOTAL				44,319.5	33,239.6	50,325.8	-17,086.2	43,928.4
AVENUE OF THE CITIES RAMP RAC-B STAGE 3-2								
STATION	922+07.00	TO	925+00.00	367.3	275.5	59.4	216.1	885.0
STATION	925+00.00	TO	931+71.93	380.3	285.3	715.4	-430.1	3,656.3
SUB TOTAL				747.6	560.7	774.7	-214.0	4,541.3
STAGE 3-2 SUB TOTAL				747.6	560.7	774.7	-214.0	4,541.3
STAGE 3 TOTAL				45,067.2	33,800.4	51,100.6	-17,300.2	48,469.7
MAINLINE AND RAMPS TOTAL				140,060	105,045	69,870	35,180	102,396

SCH0-03

FILE NAME = ...\D2CONCD-ABC-sht-EWSched-Mainline.dgn	USER NAME = jtardy	DESIGNED - BMH	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SCHEDULE OF QUANTITIES EARTHWORK**
		DRAWN - BMH	REVISED -		
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -		
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	66
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

LOCATION				EARTH EXCAVATION (2020100)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	TOPSOIL FURNISH AND PLACE, 4" (21101615)
LOCAL ROADS				CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD	SQUARE YARD
11TH AVENUE A STAGE 3-3								
STATION	1100+50.00	TO	1101+13.13	69.4	52.0	1.2	50.8	78.0
TOTAL (11TH AVENUE)				**69.4**	**52.0**	**1.2**	**50.8**	**78.0**
12TH AVENUE STAGE 1-3								
STATION	126+60.00	TO	128+30.50	26.9	20.1	2.4	17.7	15.9
STATION	128+30.50	TO	129+67.80	373.3	280.0	0.6	279.4	16.4
STATION	130+24.57	TO	132+10.77	327.3	245.5	74.5	171.0	136.9
SUB TOTAL				**727.5**	**545.6**	**77.5**	**468.1**	**169.1**
12TH AVENUE STAGE 3-1								
STATION	124+10.00	TO	126+60.00	**33.6**	**37.3**	**37.3**	**11.0**	**11.0**
TOTAL (12TH AVENUE)				**761.1**	**582.9**	**114.7**	**479.1**	**180.1**
23RD STREET TEMPORARY ENTRANCE								
STAGE 1-0				90.8	68.1	9.1	59.0	0.0
STAGE 1-4				9.1	6.8	90.8	-84.0	187.4
TOTAL (TEMPORARY ENTRANCE)				**99.9**	**74.9**	**99.9**	**-25.0**	**187.4**
19TH STREET STAGE 1-1								
STATION	1938+21.30	TO	1940+50.00	615.6	461.7	2.1	459.6	204.1
STATION	1940+50.00	TO	1945+00.00	1,377.6	1,033.2	7.7	1,025.5	498.8
STATION	1945+00.00	TO	1950+00.00	1,529.3	1,147.0	9.5	1,137.5	494.1
SUB TOTAL				**3,522.5**	**2,641.9**	**19.3**	**2,622.6**	**1,196.9**
19TH STREET STAGE 1-3								
STATION	192+885.23	TO	1935+00.00	1,473.1	1,104.9	29.7	1,075.2	283.4
STATION	1935+00.00	TO	1938+21.30	575.3	431.5	111.1	320.3	461.4
SUB TOTAL				**2,048.4**	**1,536.3**	**140.8**	**1,395.5**	**744.8**
19TH STREET STAGE 2-0								
STATION	1918+02.18	TO	1918+41.92	1.8	1.3	0.0	1.3	0.0
STATION	1918+65.00	TO	1925+00.00	3,240.8	2,430.6	5.1	2,425.5	0.0
STATION	1925+00.00	TO	1928+80.29	5,722.8	4,292.1	3.3	4,288.8	0.0
SUB TOTAL				**8,965.4**	**6,724.0**	**8.5**	**6,715.6**	**0.0**
19TH STREET STAGE 2-1								
STATION	1920+59.53	TO	1925+00.00	3,965.4	2,974.0	381.8	2,592.3	283.7
STATION	1925+00.00	TO	1928+39.43	257.5	193.1	38.9	154.3	110.3
SUB TOTAL				**4,222.9**	**3,167.2**	**420.6**	**2,746.6**	**393.9**
19TH STREET STAGE 2-3								
STATION	1923+90.00	TO	1927+80.00	**0.0**	**0.0**	**4,824.8**	**-4,824.8**	**0.0**
19TH STREET STAGE 3-0								
STATION	1917+00.00	TO	1921+50.66	3,021.8	2,266.3	9.2	2,257.1	0.0
STATION	1926+84.81	TO	1928+37.65	34.0	25.5	0.2	25.3	2.8
SUB TOTAL				**3,055.8**	**2,291.9**	**9.4**	**2,282.5**	**2.8**
19TH STREET STAGE 3-2								
STATION	1913+20.00	TO	1913+62.87	10.1	7.6	1.8	5.8	55.9
STATION	1915+11.98	TO	1920+00.00	41.9	31.4	259.8	-228.4	480.1
STATION	1920+00.00		1925+00.00	781.6	586.2	5,324.5	-4,738.4	1,640.8
STATION	1925+00.00		1929+34.57	754.9	566.2	122.7	443.5	1,180.3
SUB TOTAL				**1,588.5**	**1,191.4**	**5,708.8**	**-4,517.4**	**3,357.2**
19TH STREET STAGE 3-3								
STATION	1913+20.00	TO	1920+00.00	98.4	73.8	1.4	72.4	198.0
STATION	1920+00.00	TO	1925+00.00	1,263.9	947.9	743.5	204.5	56.7
STATION	1925+00.00	TO	1929+34.57	592.9	444.7	130.6	314.1	234.9
SUB TOTAL				**1,955.2**	**1,466.4**	**875.4**	**591.0**	**489.6**
19TH STREET STAGE 3-4								
STATION	1913+20.00	TO	1920+00.00	67.7	50.8	109.6	-58.9	0.0
STATION	1920+00.00	TO	1925+00.00	78.8	59.1	328.0	-268.9	0.0
STATION	1925+00.00	TO	1928+07.90	94.1	70.5	0.0	70.5	0.0
STATION	1928+75.90	TO	1935+74.23	130.9	98.2	97.9	0.2	615.3
SUB TOTAL				**371.5**	**278.6**	**535.6**	**-257.0**	**615.3**
TOTAL (19TH STREET)				**25,730.2**	**19,297.7**	**12,543.2**	**6,754.5**	**6,800.6**

LOCATION				EARTH EXCAVATION (2020100)	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	TOPSOIL FURNISH AND PLACE, 4" (21101615)
LOCAL ROADS				CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD	SQUARE YARD
19TH STREET NB STAGE 1-0								
STATION	1968+10.00	TO	1970+21.65	**42.2**	**31.7**	**2.9**	**0.0**	**0.0**
SUB TOTAL				**42.2**	**31.7**	**2.9**	**0.0**	**0.0**
19TH STREET NB STAGE 1-1								
STATION	1950+00.00	TO	1955+00.00	1,432.4	1,074.3	102.6	971.7	603.8
STATION	1955+00.00	TO	1960+00.00	1,232.6	924.5	20.4	904.1	507.8
STATION	1960+00.00	TO	1965+00.00	923.0	692.3	0.0	692.3	373.9
STATION	1965+00.00	TO	1969+80.00	610.2	457.6	0.0	457.6	192.8
STATION	1978+51.13	TO	1985+60.00	590.9	443.2	0.0	443.2	179.9
SUB TOTAL				**4,789.1**	**3,591.8**	**123.0**	**3,468.8**	**1,858.1**
19TH STREET NB STAGE 1-2								
STATION	1973+00.00	TO	1980+00.00	723.2	542.4	0.0	542.4	187.0
STATION	1980+00.00	TO	1985+60.00	447.2	335.4	0.0	335.4	108.2
SUB TOTAL				**1,170.4**	**877.8**	**0.0**	**877.8**	**295.2**
19TH STREET NB STAGE 1-3								
STATION	1968+06.12	TO	1970+27.40	483.4	362.6	1.9	360.7	509.9
STATION	1973+00.00	TO	1977+00.00	916.3	687.2	0.0	687.2	633.3
STATION	1977+00.00	TO	1980+81.87	1,195.9	896.9	0.0	896.9	852.8
SUB TOTAL				**2,595.6**	**1,946.7**	**1.9**	**1,944.9**	**1,996.0**
TOTAL (19TH STREET NB)				**8,597.4**	**6,448.0**	**127.8**	**6,291.5**	**4,149.3**
19TH STREET SB STAGE 1-0								
STATION	1964+40.00	TO	1968+78.00	185.6	139.2	0.7	138.4	0.0
STATION	2077+62.00	TO	2078+25.00	54.1	40.6	0.0	40.6	0.0
SUB TOTAL				**239.7**	**179.8**	**0.7**	**179.0**	**0.0**
19TH STREET SB STAGE 1-1A								
STATION	2076+10.00	TO	2078+00.00	**260.1**	**195.1**	**4.2**	**190.9**	**126.0**
19TH STREET SB STAGE 1-1								
STATION	2056+12.50	TO	2060+00.00	892.8	669.6	0.4	669.2	267.9
STATION	2060+00.00	TO	2065+00.00	1,343.2	1,007.4	23.2	984.2	560.7
STATION	2065+00.00	TO	2068+20.00	527.8	395.9	0.0	395.9	126.9
STATION	2074+30.00	TO	2076+10.00	540.9	405.7	0.0	405.7	396.3
SUB TOTAL				**3,304.8**	**2,478.6**	**23.7**	**2,454.9**	**1,351.8**
19TH STREET SB STAGE 1-2								
STATION	2064+40.00	TO	2068+78.00	408.1	306.1	111.5	194.6	481.4
STATION	2074+30.00	TO	2078+00.00	251.4	188.6	3.7	184.9	155.7
SUB TOTAL				**659.5**	**494.6**	**115.1**	**379.5**	**637.1**
TOTAL (19TH STREET SB)				**4,464.1**	**3,348.1**	**143.7**	**3,204.3**	**2,114.9**
LOCAL ROADS TOTAL				**39,725**	**29,805**	**13,035**	**16,760**	**13,511**

NOTE:
CROSS SECTIONS ARE SHEETED AT 25' INTERVALS.
EARTHWORK END AREA QUANTITIES BASED ON 10' INTERVAL
CROSS SECTIONS TO PROVIDE A MORE ACCURATE ESTIMATE.

SCHQ-04

LAYOUT	MTH	4/23/2015
DRAWN	RLT	3/21/2017
REVIEWED	MTH	3/22/2017

FILE NAME = D2CONCD-HPS-sht-EWSched-LocalRoads01.dgn	USER NAME = hehn01663	DESIGNED - RLT	REVISED -
		DRAWN - RLT	REVISED -
	PLOT SCALE =	CHECKED - MTH	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
EARTHWORK

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	67
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

20100110				
TREE REMOVAL (6 TO 15 UNITS DIAMETER)				
ALIGNMENT	STA	OFFSET	LT/RT	UNITS
I-74	50+89	125'	LT	7
I-74	51+57	128'	LT	10
I-74	51+59	125'	LT	9
I-74	58+91	132'	LT	12
I-74	58+94	145'	LT	7
I-74	62+22	126'	RT	10
I-74	62+53	138'	RT	12
I-74	63+91	165'	LT	6
I-74	63+92	165'	LT	6
I-74	63+96	154'	LT	6
I-74	64+34	155'	LT	6
I-74	64+39	156'	LT	8
I-74	66+14	113'	RT	12
I-74	66+26	117'	RT	8
I-74	66+29	114'	RT	6
I-74	66+44	97'	RT	8
I-74	66+51	123'	RT	6
I-74	66+60	122'	RT	9
I-74	66+70	123'	RT	8
I-74	66+71	122'	RT	8
I-74	66+78	131'	RT	12
I-74	66+88	109'	RT	10
I-74	66+94	130'	RT	9
I-74	66+98	133'	RT	10
I-74	67+06	126'	RT	7
I-74	67+14	129'	RT	6
I-74	67+23	125'	RT	8
I-74	67+30	134'	RT	8
I-74	67+31	124'	RT	10
I-74	67+34	135'	RT	8
I-74	67+44	135'	RT	6
I-74	67+50	132'	RT	12
I-74	67+52	132'	RT	8
I-74	67+57	132'	RT	14
I-74	67+88	126'	RT	10
I-74	67+89	130'	RT	9
I-74	67+90	130'	RT	12
I-74	68+01	131'	RT	7
I-74	68+12	134'	RT	10
I-74	68+17	132'	RT	8
I-74	68+18	130'	RT	10
I-74	68+19	133'	RT	6
I-74	68+28	131'	RT	10
I-74	68+33	135'	RT	6
I-74	68+37	132'	RT	10
I-74	68+39	132'	RT	6
I-74	68+46	136'	RT	8
I-74	68+52	128'	RT	8
I-74	68+55	131'	RT	8
I-74	68+66	125'	RT	8
I-74	68+91	124'	RT	10
I-74	68+94	129'	RT	12
I-74	68+96	109'	RT	6
I-74	68+99	123'	RT	8
I-74	69+00	129'	RT	6
I-74	69+01	129'	RT	9
I-74	69+05	110'	RT	12
I-74	69+06	123'	RT	12
I-74	69+07	137'	RT	12
I-74	69+15	130'	RT	10
I-74	69+27	132'	RT	12
I-74	69+39	135'	RT	6
I-74	69+40	130'	RT	6
I-74	69+47	134'	RT	12
I-74	69+54	121'	RT	12
I-74	69+54	126'	RT	8
I-74	69+55	126'	RT	6
I-74	69+61	135'	RT	10
I-74	69+63	132'	RT	6
I-74	69+66	128'	RT	10
I-74	69+77	129'	RT	8
I-74	69+86	135'	RT	6
I-74	69+89	136'	RT	6
I-74	69+92	135'	RT	6
I-74	70+01	131'	RT	10
I-74	70+13	128'	RT	12
I-74	70+17	117'	RT	8
I-74	70+20	108'	RT	8
I-74	70+20	111'	RT	8
I-74	70+23	97'	RT	6
I-74	70+29	102'	RT	8
I-74	70+34	98'	RT	8
I-74	71+95	108'	LT	9
I-74	71+98	115'	LT	14

20100110				
TREE REMOVAL (6 TO 15 UNITS DIAMETER)				
ALIGNMENT	STA	OFFSET	LT/RT	UNITS
I-74	71+98	115'	LT	14
I-74	72+00	85'	LT	6
I-74	72+06	100'	LT	6
I-74	72+06	141'	LT	12
I-74	72+17	100'	LT	6
I-74	72+28	80'	LT	6
I-74	72+28	105'	LT	12
I-74	72+28	107'	LT	10
I-74	72+36	139'	LT	6
I-74	72+37	123'	LT	6
I-74	72+40	167'	LT	8
I-74	72+41	95'	LT	6
I-74	72+41	166'	LT	14
I-74	72+41	169'	LT	8
I-74	72+45	136'	LT	12
I-74	72+49	124'	LT	6
I-74	81+00	113	RT	6.0
I-74	81+01	126	RT	8.0
I-74	81+08	120	RT	14.0
I-74	91+04	101	LT	10.0
I-74	91+36	128	LT	10.0
I-74	92+33	116	LT	10.0
I-74	92+76	91	LT	10.0
I-74	91+91	92	RT	12.0
I-74	92+76	101	RT	14.0
I-74	92+90	87	RT	12.0
I-74	92+86	102	RT	14.0
I-74	103+45	119	LT	12.0
I-74	127+81	111	LT	10.0
I-74	148+69	98	LT	15.0
I-74	149+15	85	LT	12.0
I-74	156+23	233	LT	6.0
I-74	156+23	233	LT	6.0
I-74	156+23	233	LT	6.0
I-74	156+23	233	LT	12.0
I-74	156+23	233	LT	9.0
I-74	156+41	201	LT	7.0
I-74	156+69	204	LT	6.0
I-74	156+81	206	LT	9.0
I-74	156+81	206	LT	9.0
I-74	156+86	221	LT	6.0
I-74	156+96	207	LT	8.0
I-74	156+95	237	LT	6.0
I-74	157+13	208	LT	14.0
I-74	157+87	237	LT	8.0
I-74	157+93	184	LT	6.0
I-74	158+01	170	LT	6.0
I-74	158+01	176	LT	8.0
I-74	158+00	184	LT	8.0
I-74	157+99	210	LT	12.0
I-74	155+44	93	LT	10.0
I-74	155+86	136	LT	12.0
I-74	155+98	159	LT	10.0
I-74	156+16	198	LT	6.0
I-74	156+39	171	LT	6.0
I-74	157+09	135	LT	9.0
NB 19TH ST	1964+37	222'	LT	12
NB 19TH ST	1964+45	213'	LT	12
NB 19TH ST	1964+58	211'	LT	10
NB 19TH ST	1964+65	201'	LT	12
			TOTAL	1274.00

20100210				
TREE REMOVAL (OVER 15 UNITS DIAMETER)				
ALIGNMENT	STA	OFFSET	LT/RT	UNITS
I-74	69+29	127	RT	18
I-74	72+36	244	LT	16
I-74	142+84.00	106	LT	18.0
I-74	144+34.00	107	LT	16.0
I-74	144+83.00	100	RT	24.0
I-74	145+58.00	108	20	20.0
I-74	149+04.00	88	LT	18.0
I-74	149+29.00	86	LT	16.0
I-74	156+56.00	200	LT	16.0
I-74	156+78.00	228	LT	16.0
I-74	157+40.00	142	LT	16.0
SB 19TH ST	2068+53	30'	RT	18
	TOTAL			212.0

20100500							
TREE REMOVAL, ACRES							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	ACRE
I-74	50+50	85'	RT	55+15	95'	RT	0.3122
I-74	71+40	115'	RT	81+00	129'	RT	0.8167
I-74	98+28	155	LT	101+77	201	LT	0.2235
I-74	108+74	81	LT	110+31	95	LT	0.0696
I-74	110+59	160	LT	110+77	138	LT	0.0400
I-74	108+99	170	RT	109+21	186	RT	0.0056
I-74	109+36	181	RT	110+30	183	RT	0.0390
I-74	140+89	90	RT	141+80	107	RT	0.0266
I-74	143+45	119	LT	143+60	108	LT	0.0065
I-74	146+58	91	LT	146+93	98	LT	0.0046
I-74	147+58	100	LT	148+42	84	LT	0.0302
I-74	151+72	86	LT	153+56	88	LT	0.0459
I-74	153+72	87	LT	154+48	83	LT	0.0287
I-74	154+69	86	LT	155+04	93	LT	0.0069
I-74	156+68	174	LT	157+13	182	LT	0.0059
I-74	157+13	219	LT	157+91	171	LT	0.1078
I-74	157+35	111	LT	157+82	91	LT	0.0155
I-74	158+02	100	LT	158+30	98	LT	0.0081
7TH-A	629+63	25'	RT	631+31	95'	RT	0.0831
7TH-A	632+42	28'	RT	632+54	29'	RT	0.0007
7TH-A	633+15	19'	RT	633+35	20'	RT	0.0021
7TH-A	633+75	21'	RT	633+94	22'	RT	0.0014
7TH-A	640+61	33'	RT	641+82	56'	RT	0.0490
7TH-B	523+85	19'	RT	527+53	61'	RT	0.1639
7TH-B	527+82	60'	RT	528+45	54'	RT	0.0139
7TH-B	528+87	48'	RT	533+05	67'	RT	0.3764
7TH-B	534+28	30'	RT	534+48	18'	RT	0.0033
7TH-B	534+70	28'	RT	535+68	18'	RT	0.0157
19TH ST	1925+06	50'	LT	1928+30	52'	LT	0.0346
19TH ST	1929+11	44'	LT	1929+46	47'	LT	0.0027
19TH ST	1935+03	37'	LT	1936+01	53'	LT	0.0079
19TH ST	1943+72	36'	LT	1944+16	34'	LT	0.0040
19TH ST SB	2066+29	30'	RT	2067+36	33'	RT	0.0228
						TOTAL	2.75

44000100			
PAVEMENT REMOVAL			
ALIGNMENT	STA FROM	STA TO	SQ YD
PR I-74	50+61	58+04	1,961
PR I-74	50+61	56+58	1,627
PR I-74	61+95	67+00	3,144
PR I-74	63+80	67+00	1,560
PR I-74	67+00	70+64	1,953
PR I-74	67+00	70+95	1,105
PR I-74	71+64	76+00	1,907
PR I-74	71+95	76+00	1,087
PR I-74	76+00	81+00	2,018
PR I-74	76+00	81+00	1,380
PR I-74	81+00.00	93+69	3,784
PR I-74	81+00.00	94+88	6,180
PR I-74	95+54.87	111+00	4,393
PR I-74	96+68.30	111+00	3,748
PR I-74	111+00	125+00	3,741
PR I-74	111+00	125+00	4,722
PR I-74	125+00	140+00.00	4,564
PR I-74	125+00	140+00.00	4,145
PR I-74	140+00	155+00.00	3,989
PR I-74	140+00	155+00.00	3,935
EX RAMP 7-S	78+78	84+21	2,111
EX RAMP S-7	77+87	87+02	3,111
EX RAMP S-7	87+02	88+22	254
EX RAC-A - PR RAC-A	44+48	1025+00	1,790
PR RAC-C	723+92	734+00.00	1,855
PR RAC-C	734+00	739+72.71	950
PR RAC-D - EX RAC-D	824+25.26	18+73.38	1,371
RAC-B	922+00	929+58	1,531
19TH ST	1920+75	1921+22	127
19TH ST	1920+75	1921+22	81
19TH ST	1921+22	1930+77	5,504
19TH ST	1930+77	1939+80	6,734
19TH ST	1939+80	1948+30	2,265
19TH ST	1939+80	1948+30	2,265
19TH ST	1948+30	1957+00 (NB) / 2057+00 (SE)	4,954
19TH ST NB	1957+00	1969+80	3,709
19TH ST NB	1973+00	1985+60	4,623
19TH ST SB	2057+00	2068+20	3,406
19TH ST SB	2074+30	2078+00	1,087
		TOTAL	108,671

44000155						
HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"						
ALIGNMENT	STA FROM		STA TO			SQ YD
AOTC WB (EB)	119+00	(219+00)	122+23	(222+38)		6,586
BRIDGE OMISSION						
AOTC WB (EB)	123+83	(223+98)	126+75	(226+75)		4,304
					TOTAL	10,890

44000158							
HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"							
ALIGNMENT	STA FROM		STA TO				SQ YD
I-74 (EB)	114+00.00		117+33.05	EASTBOUND			394
I-74 (WB)	115+00.00		123+01.38	WESTBOUND			950
I-74 (EB)	123+34.36		159+00.00	EASTBOUND			4,140
I-74 (WB)	127+00.94		142+95.89	WESTBOUND			1,840
RAC-A	1018+90.91		1023+05.19	RAMP RAC-A			195
						TOTAL	7,519

44000161							
HOT-MIX ASPHALT SURFACE REMOVAL, 3"							
ALIGNMENT	STA FROM		STA TO				SQ YD
12TH AVE	123+00.00		128+30.50				2,387
						TOTAL	2,387

44000200							
DRIVEWAY PAVEMENT REMOVAL							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
19TH ST	1930+23	45'	LT	1930+62	44'	LT	53
19TH ST	1931+76	40'	RT	1932+03	40'	RT	31
12TH AVE	127+13	28'	RT	127+31	28'	RT	21
19TH ST NB	1963+20	3'	LT	1963+73	3'	LT	49
19TH ST SB	2077+48	24'	RT	2077+88	24'	RT	48
						TOTAL	202

SCHO-05

FILE NAME = ...\D2CONCD-ABC-sht-schedule03M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	68
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

25000210							
SEEDING, CLASS 2A							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	ACRE
I-74	48+86	67'	RT	56+00	96'	RT	1.35
I-74	64+70	117'	LT	67+80	91'	LT	0.07
I-74	72+03	142'	LT	81+00	91'	LT	0.41
I-74	71+24	105'	RT	77+64	84'	RT	0.56
I-74	77+56	77'	RT	81+00	128'	RT	0.13
I-74	81+00	128'	RT	87+28	98'	RT	0.390
I-74	81+00	91'	LT	93+60	78'	LT	0.670
I-74	88+28	76'	RT	98+32	172'	RT	0.810
I-74	97+49	73'	RT	102+20	72'	RT	0.740
I-74	97+76	64'	LT	107+51	78'	LT	1.380
I-74	98+71	123'	RT	100+80	114'	RT	0.128
I-74	100+99	114'	RT	103+33	174'	RT	0.297
I-74	105+23	120'	LT	107+10	122'	LT	0.332
I-74	108+83	95'	RT	113+76	117'	RT	1.027
I-74	110+60	89'	LT	116+90	99'	LT	1.108
I-74	108+30	68'	RT	117+35	64'	RT	0.830
I-74	108+64	78'	LT	123+37	64'	LT	1.400
I-74	120+55	100'	RT	130+36	68'	RT	0.460
I-74	123+40	99'	LT	129+65	64'	LT	0.120
I-74	130+08	66'	LT	135+20	77'	LT	0.117
I-74	130+89	70'	RT	137+09	67'	RT	0.130
I-74	136+44	70'	LT	155+00	73'	LT	0.764
I-74	137+75	69'	RT	155+00	69'	RT	0.630
I-74	155+00	73'	LT	158+40	121'	LT	0.700
RAMP 7TH-B	521+00	30'	RT	526+52	8'	RT	0.15
RAMP 7TH-B	526+62	23'	LT	535+07	22'	LT	0.52
RAMP 7TH-B	526+46	26'	RT	530+55	11'	RT	0.30
RAMP 7TH-B	531+30	11'	RT	541+46	25'	RT	0.42
RAMP 7TH-A	632+50	16'	RT	629+73	19'	RT	0.13
RAMP 7TH-A	642+57	25'	RT	641+00	24'	RT	0.06
RAMP AC-C	725+00	32'	RT	737+66	28'	RT	0.760
RAMP AC-D	823+59	42'	LT	832+37	13'	LT	0.360
RAMP-AC-D	826+90	36'	LT	831+91	21'	LT	0.393
RAMP AC-B	922+82	21'	RT	931+75	29'	RT	0.490
RAMP AC-A	1019+05	8'	LT	1023+40	35'	LT	0.261
19TH ST	1913+20	56'	LT	1919+23	46'	LT	0.16
19TH ST	1913+13	42'	LT	1920+61	41'	LT	0.09
19TH ST	1915+12	42'	RT	1918+93	36'	RT	0.07
19TH ST	1919+23	46'	LT	1920+49	46'	LT	0.06
19TH ST	1924+70	41'	LT	1928+33	40'	LT	0.04
11TH AVE	1100+46	33	LT	1101+13	13	LT	0.01
19TH ST	1926+00	36'	RT	1931+75	42'	RT	0.33
19TH ST	1928+77	40	LT	1930+15	35	LT	0.01
19TH ST	1930+69	34'	LT	1933+25	32'	LT	0.02
19TH ST	1932+00	41'	RT	1934+23	39'	RT	0.05
19TH ST	1933+51	32'	LT	1936+01	29'	LT	0.02
19TH ST-12TH	1934+23	39'	RT	128+15	28'	LT	0.05
19TH ST	1936+08	29'	LT	1936+58	72'	LT	0.01
12TH-19TH ST	132+05	34'	RT	1938+59	33'	LT	0.09
19TH ST	1936+58	34'	RT	1945+16	34'	RT	0.10
12TH AVE	124+10	23'	RT	124+26	23'	RT	0.01
12TH AVE	125+86	23'	RT	127+02	22'	RT	
19TH ST-19SB	1945+16	34'	RT	2056+10	7'	RT	0.13
19TH NB	1955+98	30'	RT	1959+75	38'	RT	0.14
19TH NB	1956+17	8'	LT	1959+76	8'	LT	0.05
19TH NB	1959+75	38'	RT	1966+88	33'	RT	0.10
19TH NB	1959+76	8'	LT	1963+26	8'	LT	0.05
19TH NB	1963+66	8'	LT	1969+80	8'	LT	0.08
19TH NB	1968+10	71'	RT	1970+27	54'	RT	0.11
19TH NB	19TH NB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						0.01
19TH NB	19TH NB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						
19TH NB	19TH NB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						
19TH NB	19TH NB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						
19TH NB	1973+00	18'	LT	1985+60	5'	LT	0.09
19TH NB	1973+00	43'	RT	1980+82	72'	RT	0.33
19TH NB	1982+20	33'	RT	1985+60	34'	RT	0.04
19TH SB	2056+17	16'	RT	2057+96	10'	RT	0.03
19TH SB	2060+50	10'	RT	2063+44	6'	RT	0.05
19TH SB	2061+29	36'	LT	2068+20	33'	LT	0.20
19TH SB	19TH SB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						0.01
19TH SB	19TH SB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						
19TH SB	19TH SB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						
19TH SB	19TH SB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						
19TH SB	2074+30	24'	LT	2076+50	62'	LT	0.10
19TH SB	2077+33	20'	LT	2078+00	19'	LT	0.01
						TOTAL	21.00

25000310							
SEEDING, CLASS 4							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	ACRE
I-74	49+80	179'	RT	54+67	145'	RT	0.25
I-74	72+03	135'	RT	77+03	95'	RT	0.05
I-74	77+30	96'	RT	81+00	128'	RT	0.10
I-74	82+90	124'	RT	87+27	129'	RT	0.11
I-74	98+00	116'	RT	101+50	113'	RT	0.06
I-74	98+58	122'	LT	102+88	221'	LT	0.04
I-74	101+50	105'	RT	103+21	109'	RT	0.24
I-74	105+23	120'	LT	107+50	99'	LT	0.18
I-74	106+50	78'	RT	107+20	82'	RT	0.02
I-74	108+30	83'	RT	109+00	91'	RT	0.03
I-74	108+64	113'	LT	113+50	112'	LT	0.13
I-74	111+00	104'	RT	113+00	114'	RT	0.06
I-74	123+53	121'	LT	128+89	134'	LT	0.33
I-74	124+00	100'	RT	130+36	74'	RT	0.21
I-74	130+17	86'	LT	135+20	77'	LT	0.23
I-74	130+95	81'	RT	136+81	85'	RT	0.21
I-74	136+44	126'	LT	137+50	74'	LT	0.08
I-74	137+80	96'	RT	154+71	87'	RT	0.56
I-74	139+95	83'	LT	154+00	89'	LT	0.66
I-74	156+05	130'	LT	157+46	135'	LT	0.31
RAMP 7TH-A	634+52	24'	RT	630+70	67'	RT	0.15
RAMP 7TH-A	642+28	35'	RT	634+52	24'	RT	0.25
RAMP 7TH-B	523+72	17'	RT	526+40	44'	RT	0.10
RAMP 7TH-B	526+40	44'	RT	532+86	85'	RT	0.25
RAMP 7TH-B	536+90	24'	RT	541+44	24'	RT	0.25
RAMP AC-C	725+00	39'	RT	137+67	38'	RT	0.30
RAMP AC-B	923+50	25'	RT	931+72	48'	RT	0.40
19TH ST	1923+96	141'	LT	1928+28	52'	LT	0.15
19TH ST	1931+91	43'	LT	1934+38	39'	LT	0.02
19TH ST-12TH	1934+38	39'	LT	132+16	29'	LT	0.10
19TH ST	1936+65	40'	RT	1941+00	39'	RT	0.01
19TH ST/19TH NB	1938+59	33'	LT	1955+60	36'	LT	0.25
19TH ST	1941+10	39'	RT	1945+17	39'	RT	0.01
19TH ST/19TH SB	1945+17	39'	RT	2056+17	16'	RT	0.10
19TH NB	1955+98	34'	LT	1956+22	13'	LT	0.01
19TH SB	2065+14	49'	RT	2070+94	74'	RT	0.20
						TOTAL	6.50

25000400						
NITROGEN FERTILIZER NUTRIENT						
SEEDING	ACRE		LBS/ACRE			POUND
SODDING	0.15		60			9
SEEDING, CLASS 2A	21.00		90			1,891
SEEDING, CLASS 4	6.50		90			585
					TOTAL	2,485

25000500						
PHOSPHORUS FERTILIZER NUTRIENT						
SEEDING	ACRE		LBS/ACRE			POUND
SODDING	0.15		60			9
SEEDING, CLASS 2A	21.00		90			1,891
SEEDING, CLASS 4	6.50		90			585
					TOTAL	2,485

25000600						
POTASSIUM FERTILIZER NUTRIENT						
SEEDING	ACRE		LBS/ACRE			POUND
SODDING	0.15		60			9
SEEDING, CLASS 2A	21.00		90			1,891
SEEDING, CLASS 4	6.50		90			585
					TOTAL	2,485

25200100							
SODDING							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
11TH AVE	1100+46	23'	RT	1101+13	23'	RT	77
19TH ST	1928+84	56'	LT	1930+23	45'	LT	88
19TH ST	1930+62	44'	LT	1931+91	43'	LT	79
19TH ST	1936+46	56'	RT	1936+65	40'	RT	5
12TH AVE	126+83	36'	RT	127+13	29'	RT	13
12TH AVE	127+33	32'	RT	127+75	28'	RT	27
12TH AVE	127+38	23'	RT	127+70	23'	RT	6
19TH ST NB	1963+16	14'	LT	1963+32	18'	LT	3
19TH ST NB	1963+61	18'	LT	1963+75	14'	LT	4
19TH ST NB	1964+09	260'	LT	1964+87	220'	LT	236
19TH ST NB	1970+61	37'	LT	1970+71	48'	LT	3
19TH ST SB	2071+86	53'	RT	2071+98	70'	RT	8
19TH ST SB	2073+64	60'	RT	2073+77	46'	RT	4
19TH ST SB	2074+30	17'	RT	2077+48	24'	RT	155
19TH ST SB	2077+78	24'	RT	2078+00	17'	RT	6
						TOTAL	714

25200200						
SUPPLEMENTAL WATERING						
ITEM	SQ YD	GAL/SQ YD	APPLICATIONS	GALLONS		UNIT
SODDING AREA	714	3	3	6,426		6.4
					TOTAL	6.4

SCHQ-06

FILE NAME = ...\D2CONCD-ABC-sht-schedule04M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES SEEDING, FERTILIZER AND MULCH	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRM	REVISED -			74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	69
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -						CONTRACT NO. 64E26	
	PLOT DATE = 5/6/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS	FED. AID PROJECT	

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

25000750							
MOWING							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	ACRE
I-74	48+86	67'	RT	56+00	96'	RT	1.3500
I-74	64+70	117'	LT	67+80	91'	LT	0.0700
I-74	72+03	142'	LT	81+00	91'	LT	0.4100
I-74	71+24	105'	RT	77+64	84'	RT	0.5600
I-74	77+56	77'	RT	81+00	128'	RT	0.1300
I-74	81+00	128'	RT	87+28	98'	RT	0.390
I-74	81+00	91'	LT	93+60	78'	LT	0.670
I-74	88+28	76'	RT	98+32	172'	RT	0.810
I-74	97+49	73'	RT	102+20	72'	RT	0.740
I-74	97+76	64'	LT	107+51	78'	LT	1.380
I-74	98+71	123'	RT	100+80	114'	RT	0.128
I-74	100+99	114'	RT	103+33	174'	RT	0.297
I-74	105+23	120'	LT	107+10	122'	LT	0.332
I-74	108+83	95'	RT	113+76	117'	RT	1.027
I-74	110+60	89'	LT	116+90	99'	LT	1.108
I-74	108+30	68'	RT	117+35	64'	RT	0.830
I-74	108+64	78'	LT	123+37	64'	LT	1.400
I-74	120+55	100'	RT	130+36	68'	RT	0.460
I-74	123+40	99'	LT	129+65	64'	LT	0.120
I-74	130+08	66'	LT	135+20	77'	LT	0.117
I-74	130+89	70'	RT	137+09	67'	RT	0.130
I-74	136+44	70'	LT	155+00	73'	LT	0.764
I-74	137+75	69'	RT	155+00	69'	RT	0.630
I-74	155+00	73'	LT	158+40	121'	LT	0.700
RAMP 7TH-B	521+00	30'	RT	526+52	8'	RT	0.1500
RAMP 7TH-B	526+62	23'	LT	535+07	22'	LT	0.5200
RAMP 7TH-B	526+46	26'	RT	530+55	11'	RT	0.3000
RAMP 7TH-B	531+30	11'	RT	541+46	25'	RT	0.4200
RAMP 7TH-A	632+50	16'	RT	629+73	19'	RT	0.1300
RAMP 7TH-A	642+57	25'	RT	641+00	24'	RT	0.0600
RAMP AC-C	725+00	32'	RT	737+66	28'	RT	0.760
RAMP AC-D	823+59	42'	LT	832+37	13'	LT	0.360
RAMP-AC-D	826+90	36'	LT	831+91	21'	LT	0.390
RAMP AC-B	922+82	21'	RT	931+75	29'	RT	0.490
RAMP AC-A	1019+05	8'	LT	1023+40	35'	LT	0.261
19TH ST	1913+20	56'	LT	1919+23	46'	LT	0.1600
19TH ST	1913+13	42'	LT	1920+61	41'	LT	0.0900
19TH ST	1915+12	42'	RT	1918+93	36'	RT	0.0700
19TH ST	1919+23	46'	LT	1920+49	46'	LT	0.0600
19TH ST	1924+70	41'	LT	1928+33	40'	LT	0.0400
11TH AVE	1100+46	33	LT	1101+13	13	LT	0.0100
19TH ST	1926+00	36'	RT	1931+75	42'	RT	0.3300
19TH ST	1928+77	40	LT	1930+15	35	LT	0.0100
19TH ST	1930+69	34'	LT	1933+25	32'	LT	0.0200
19TH ST	1932+00	41'	RT	1934+23	39'	RT	0.0500
19TH ST	1933+51	32'	LT	1936+01	29'	LT	0.0200
19TH ST-12TH	1934+23	39'	RT	128+15	28'	LT	0.0500
19TH ST	1936+08	29'	LT	1936+58	72'	LT	0.0100
12TH-19TH ST	132+05	34'	RT	1938+59	33'	LT	0.0900
19TH ST	1936+58	34'	RT	1945+16	34'	RT	0.1000
12TH AVE	124+10	23'	RT	124+26	23'	RT	0.0100
12TH AVE	125+86	23'	RT	127+02	22'	RT	
19TH ST-19SB	1945+16	34'	RT	2056+10	7'	RT	0.1300
19TH NB	1955+98	30'	RT	1959+75	38'	RT	0.1400
19TH NB	1956+17	8'	LT	1959+76	8'	LT	0.0500
19TH NB	1959+75	38'	RT	1966+88	33'	RT	0.1000
19TH NB	1959+76	8'	LT	1963+26	8'	LT	0.0500
19TH NB	1963+66	8'	LT	1969+80	8'	LT	0.0800
19TH NB	1968+10	71'	RT	1970+27	54'	RT	0.1100
19TH NB	19TH NB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						0.0100
19TH NB	19TH NB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						
19TH NB	19TH NB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						
19TH NB	19TH NB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						
19TH NB	1973+00	18'	LT	1985+60	5'	LT	0.0900
19TH NB	1973+00	43'	RT	1980+82	72'	RT	0.3300
19TH NB	1982+20	33'	RT	1985+60	34'	RT	0.0400
19TH SB	2056+17	16'	RT	2057+96	10'	RT	0.0300
19TH SB	2060+50	10'	RT	2063+44	6'	RT	0.0500
19TH SB	2061+29	36'	LT	2068+20	33'	LT	0.2000
19TH SB	19TH SB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						0.0100
19TH SB	19TH SB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						
19TH SB	19TH SB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						
19TH SB	19TH SB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						
19TH SB	2074+30	24'	LT	2076+50	62'	LT	0.1000
19TH SB	2077+33	20'	LT	2078+00	19'	LT	0.0100
						TOTAL	21.00

25100125	
MULCH, METHOD 3	
	ACRE
AFTER STAGE 1	9.65
AFTER STAGE 2/3	26.85
TOTAL	36.5

SCHQ-07

FILE NAME = ...\D2CONCD-ABC-sht-schedule05M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
		DRAWN - JRM	REVISED -	
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -	
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -	

SCHEDULE OF QUANTITIES
SEEDING, FERTILIZER AND MULCH

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	70
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

25100630							
EROSION CONTROL BLANKET							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
I-74	48+86	67'	RT	56+00	96'	RT	6,515
I-74	49+80	179'	RT	54+60	147'	RT	1,109
I-74	64+70	117'	LT	67+80	91'	LT	318
I-74	71+24	105'	RT	77+64	84'	RT	1,026
I-74	72+03	142'	LT	81+00	79'	LT	1,914
I-74	77+30	96'	RT	81+00	129'	RT	1,036
I-74	81+00	145'	RT	88+04	136'	RT	2,789
I-74	88+00	76'	RT	98+32	172'	RT	297
I-74	97+49	73'	RT	107+26	81'	RT	3,444
I-74	108+37	85'	RT	117+35	64'	RT	3,285
I-74	97+76	64'	LT	107+43	102'	LT	7,082
I-74	98+71	123'	RT	100+80	114'	RT	710
I-74	100+99	114'	RT	103+33	174'	RT	1,441
I-74	105+23	120'	LT	107+10	122'	LT	1,659
I-74	108+83	95'	RT	113+76	117'	RT	5,159
I-74	110+60	89'	LT	116+90	99'	LT	5,366
I-74	108+66	112'	LT	123+37	64'	LT	7,201
I-74	118+85	126'	RT	130+11	86'	RT	3,129
I-74	124+01	117'	LT	129+06	75'	LT	1,603
I-74	131+89	69'	LT	134+40	65'	LT	92
I-74	131+51	133'	RT	132+00	88'	RT	129
I-74	131+44	70'	RT	136+61	75'	RT	558
I-74	138+27	68'	RT	155+00	69'	RT	2,112
I-74	139+00	63'	LT	155+00	67'	LT	1,848
I-74	153+92	101'	LT	155+00	122'	LT	365
RAMP 7TH-A	642+57	25'	RT	634+52	24'	RT	1,360
RAMP 7TH-A	634+52	24'	RT	630+70	67'	RT	1,129
RAMP 7TH-B	521+00	30'	RT	526+40	44'	RT	1,051
RAMP 7TH-B	522+95	23'	LT	526+65	33'	LT	317
RAMP 7TH-B	526+40	44'	RT	530+60	36'	RT	1,908
RAMP 7TH-B	526+65	33'	LT	535+07	22'	LT	2,516
RAMP 7TH-B	531+10	26'	RT	531+30	28'	RT	28
RAMP 7TH-B	531+30	11'	RT	541+46	25'	RT	2,143
RAMP 7TH-B	536+90	24'	RT	541+44	24'	RT	1,214
RAMP AC-C	725+80	40'	RT	739+73	21'	RT	5,552
RAMP AC-D	823+58	42'	LT	832+37	13'	LT	1,774
RAMP-AC-D	826+90	36'	LT	831+91	21'	LT	1,902
RAMP AC-B	922+83	15'	RT	930+00	51'	RT	3,534
RAMP AC-A	1019+05	62'	LT	1024+00	35'	LT	1,311
19TH ST	1913+13	42'	LT	1920+61	41'	LT	419
19TH ST	1913+20	56'	LT	1920+49	46'	LT	1,074
19TH ST	1924+70	41'	LT	1928+33	40'	LT	198
19TH ST	1923+65	94'	LT	1928+42	113'	LT	882
19TH ST	1926+00	36'	RT	1931+75	42'	RT	672
19TH ST	1928+77	40	LT	1930+15	35	LT	49
19TH ST	1930+69	34'	LT	1933+25	32'	LT	85
19TH ST	1931+91	43'	LT	1934+38	39'	LT	165
19TH ST	1932+00	41'	RT	1934+23	39'	RT	242
19TH ST	1933+51	32'	LT	1936+01	30'	LT	85
19TH ST-12TH	1934+23	39'	RT	128+15	28'	LT	218
19TH ST-12TH	1934+38	39'	LT	132+16	29'	LT	414
19TH ST	1936+08	29'	LT	1936+58	72'	LT	44
19TH ST	1936+58	34'	RT	1945+16	34'	RT	468
19TH ST	1936+65	40'	RT	1941+00	39'	RT	43
19TH ST/19TH NB	1945+10	35'	LT	1955+60	36'	LT	765
19TH ST	1941+10	39'	RT	1945+17	39'	RT	39
19TH ST-19SB	1945+16	34'	RT	2056+10	7'	RT	607
19TH ST/19TH SB	1945+17	39'	RT	2056+17	16'	RT	381
12TH AVE	124+10	23'	RT	124+26	23'	RT	5
12TH AVE	125+86	23'	RT	127+02	22'	RT	37
12TH-19TH ST	132+05	34'	RT	1945+10	35'	LT	822
19TH NB	1955+98	30'	RT	1959+75	38'	RT	664
19TH NB	1955+98	34'	LT	1956+22	13'	LT	33
19TH NB	1956+17	8'	LT	1959+76	8'	LT	212
19TH NB	1959+75	38'	RT	1966+88	33'	RT	480
19TH NB	1959+76	8'	LT	1963+26	8'	LT	227
19TH NB	1963+66	8'	LT	1969+80	8'	LT	363
19TH NB	1968+10	71'	RT	1970+27	54'	RT	530
19TH NB	19TH NB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						4
19TH NB	19TH NB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						6
19TH NB	19TH NB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						8
19TH NB	19TH NB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						9
19TH NB	1973+00	18'	LT	1985+60	5'	LT	440
19TH NB	1973+00	43'	RT	1980+14	70'	RT	1,427
19TH NB	1982+20	33'	RT	1985+60	34'	RT	190
19TH SB	2056+17	16'	RT	2057+97	10'	RT	150
19TH SB	2060+50	10'	RT	2063+44	6'	RT	242
19TH SB	2061+30	36'	LT	2068+20	33'	LT	967
19TH SB	2065+14	49'	RT	2070+94	74'	RT	851

25100630							
EROSION CONTROL BLANKET							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
19TH SB	19TH SB/AVE OF THE CITIES NORTHEAST QUADRANT ADA SEEDING						8
19TH SB	19TH SB/AVE OF THE CITIES NORTWEST QUADRANT ADA SEEDING						5
19TH SB	19TH SB/AVE OF THE CITIES SOUTHEAST QUADRANT ADA SEEDING						5
19TH SB	19TH SB/AVE OF THE CITIES SOUTHWEST QUADRANT ADA SEEDING						5
19TH SB	2074+30	24'	LT	2076+50	62'	LT	484
19TH SB	2077+33	20'	LT	2078+00	19'	LT	15
STAGE 1-0							
RAMP AC-C	734+18.32	37'	RT	739+73	21'	RT	1,662
I-74	114+00	167'	RT	116+99	66'	RT	1,609
I-74	115+01	93'	LT	121+08	67'	LT	1,562
I-74	124+34	72'	RT	130+14	90'	RT	1,718
I-74	127+01	79'	LT	129+08	76'	LT	169
I-74	130+36	74'	LT	134+37	69'	LT	449
I-74	131+51	76'	RT	136+63	81'	RT	904
I-74	136+80	70'	LT	157+82	95'	LT	6,699
I-74	138+29	75'	RT	159+00	59'	RT	3,517
						TOTAL	119,254

28000305						
TEMPORARY DITCH CHECKS						
ALIGNMENT	STR FR	STR TO	LT/RT	EACH	FEET/EACH	FOOT
I-74	49+80	51+80	RT	4	10	40
I-74	51+80	53+80	RT	7	10	70
I-74	53+80	54+50	RT	3	10	30
I-74	61+10	63+00	RT	11	10	110
I-74	67+10	70+00	RT	14	10	140
I-74	72+00	73+00	RT	5	10	50
I-74	73+00	76+75	RT	19	10	190
I-74	79+40	80+00	RT	4	10	40
I-74	80+00	81+00	RT	5	10	50
I-74	81+00	95+00	RT	49	10	490
I-74	95+00	114+00	RT	39	10	390
I-74	114+00	124+00	RT	12	10	120
I-74	95+00	114+00	LT	29	10	290
I-74	114+00	124+00	LT	7	10	70
I-74	114+00	124+00	RT	9	10	90
I-74	114+00	124+00	LT	4	10	40
I-74	124+00	155+00	RT	27	10	270
I-74	124+00	155+00	LT	23	10	230
RAMP 7TH-A	630+90	632+10	RT	7	10	70
RAMP 7TH-A	641+60	642+00	RT	3	10	30
RAMP 7TH-B	524+00	526+75	RT	13	10	130
RAMP 7TH-B	526+75	527+75	RT	3	10	30
RAMP 7TH-B	527+75	530+00	RT	12	10	120
TOTAL						3090

28000500				
INLET AND PIPE PROTECTION				
STR	STA	O/S	RT/LT	EACH
B34	49+44.39	179.9	RT	1
E19	66+89.06	103.06	RT	1
E19a	69+02.29	136.79	RT	1
EX INLET	73+04.68	142.83	RT	1
J10	97+84.18	112.93	RT	1
L5	128+50.00	79.00	LT	1
L7	129+50.00	75.00	RT	1
L9	130+07.99	80.50	RT	1
L17	136+00.00	75.00	RT	1
L18	136+46.00	76.00	RT	1
M10	153+82.15	84.64	LT	1
B25	642+24.13	31.76	RT	1
N38b	630+70.00	43.94	RT	1
E04	532+26.49	20.67	RT	1
E05	531+61.80	33.17	LT	1
E09	527+83.11	53.2	RT	1
E13b	524+00.00	14.15	RT	1
N33c	1921+10.00	55.53	RT	1
N38	1924+86.73	53.00	LT	1
N40c	1925+80.00	49.64	RT	1
N45	1928+30.00	50.00	LT	1
EX INLET	1928+89.42	61.19	RT	1
EX INLET	1937+62.89	58.53	LT	1
N49a	2063+10.00	43.00	LT	1
TOTAL				24

X2810106					
STONE RIPRAP, CLASS A3 (SPECIAL)					
ALIGNMENT	FROM STA	LT/RT	TO STA	LT/RT	SQ YD
I-74	102+50	LT	105+00	LT	386
I-74	111+00	RT	116+00	RT	833
I-74	127+48	LT	129+00	LT	218
I-74	127+52	RT	130+06	RT	408
I-74	130+35	LT	134+28	LT	839
I-74	131+46	RT	136+64	RT	762
I-74	136+71	LT	155+00	LT	2790
I-74	138+31	RT	155+00	RT	2556
TOTAL					8,792

25100900					
TURF REINFORCEMENT MAT					
ALIGNMENT	STA FROM	LT/RT	STA TO	LT/RT	SQ YD
I-74	71+90	RT	77+00	RT	1985
I-74	80+50	LT	81+00	LT	67
I-74	81+00	LT	93+60	LT	3368
I-74	125+20	RT	128+18	RT	452
I-74	129+07	LT	129+67	LT	228
I-74	129+93	LT	132+75	LT	358
I-74	129+99	RT	130+69	RT	183
I-74	130+86	RT	131+51	RT	290
I-74	133+75	LT	135+59	LT	413
I-74	136+23	LT	136+97	LT	150
I-74	136+57	RT	137+36	RT	209
I-74	137+69	RT	138+31	RT	116
I-74	139+70	RT	148+50	RT	782
I-74	140+00	LT	153+92	LT	2265
I-74	151+00	RT	154+26	RT	237
RAMP 7TH-B	524+25	RT	524+50	RT	39
RAMP 7TH-B	527+10	RT	527+30	RT	21
RAMP 7TH-B	529+51	RT	529+60	RT	12
RAMP 7TH-B	529+80	RT	530+00	RT	16
RAMP 7TH-B	530+60	RT	531+10	RT	106
RAMP 7TH-B	531+30	RT	531+60	RT	39
19TH ST	1926+60	RT	1927+80	RT	888
STAGE 1-0					
I-74	129+07	LT	129+43	LT	228
I-74	129+93	LT	130+32	LT	89
I-74	130+18	RT	130+68	RT	183
I-74	131+14	RT	131+51	RT	384
I-74	134+08	LT	135+39	LT	291
I-74	136+23	LT	136+43	LT	150
I-74	136+67	RT	137+36	RT	209
I-74	137+89	RT	138+31	RT	116
				TOTAL	13,875

28000250						
TEMPORARY EROSION CONTROL SEEDING						
		ACRE	LBS/ACRE	PLICATIONS		POUND
AFTER STAGE 1		9.65		100	30	28,950
AFTER STAGE 2/3		26.85		100	30	80,550
					TOTAL	109,500

28000400							
PERIMETER EROSION BARRIER							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74	64+33	190'	LT	67+79	101'	LT	379
I-74	71+27	108'	RT	81+00	131'	RT	974
I-74	72+56	190'	LT	81+00	93'	LT	958
I-74	81+00	93'	LT	92+64	134'	LT	1151
I-74	81+00	145'	RT	84+50	107'	RT	374
I-74	94+62	111'	LT	100+50	163'	LT	474
RAMP 7TH-A	642+52	43'	RT	630+70	68'	RT	1287
RAMP 7TH-B	521+00	30'	RT	534+90	18'	RT	1416
19TH ST	1923+96	141'	LT	1928+34	67'	LT	471
STAGE 1-0							
I-74	115+01	93'	LT	119+75	90'	LT	558
I-74	114+00	66'	RT	114+00	167'	RT	101
I-74	124+34	72'	RT	130+14	90'	RT	586
I-74	127+02	77'	LT	129+06	74'	LT	204
I-74	130+37	72'	LT	134+33	73'	LT	398
I-74	131+52	107'	RT	136+32	79'	RT	518
I-74	136+82	71'	LT	105+71	106'	LT	1923
I-74	138+29	73'	RT	157+92	63'	RT	1931
						TOTAL	13702

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

SCHO-08

FILE NAME = ...\D2CONCD-ABC-sht-schedule06M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES EROSION CONTROL
		DRAWN - JRM	REVISED -		
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -		
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1)HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	71
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

28000510
INLET FILTERS

STRUCTURE	STA	OFFSET	LT/RT	EACH
B04a	56+00.00	60.05	LT	1
B05	56+00.00	0.00	-	2
B06a	54+00.00	60.05	LT	1
B07	54+00.00	0.00	-	2
B08a	51+50.00	60.00	LT	1
B09	51+50.00	0.00	-	2
B10a	49+08.31	60.12	LT	1
B11	49+25.00	0.00	-	2
B11a	49+35.29	0.00	-	2
B16a	636+31.81	10.00	RT	1
B17	636+32.13	30.00	LT	1
B18a	638+81.14	21.56	RT	1
B18c	637+69.28	13.93	RT	1
B18d	638+63.32	30.10	RT	1
B18f	637+48.73	20.82	RT	1
B19	638+81.64	30.00	LT	1
B20d	641+00.00	22.00	RT	1
B20	641+34.71	23.50	RT	1
B20a	641+44.67	23.26	RT	1
B20b	641+65.00	30.00	LT	1
B26	642+28.63	28.10	LT	1
B29	54+84.97	60.50	RT	1
B30	53+50.00	65.00	RT	1
B31	51+50.00	65.00	RT	1
B32	49+45.00	60.13	RT	1
B32a	50+02.04	60.00	RT	1
B62	50+64.14	65.03	RT	1
B02a	57+94.84	60.00	LT	1
B03	57+94.84	2.00	LT	1
B13a	631+23.00	9.17	RT	1
B14a	633+81.82	10.00	RT	1
B14c	632+67.29	17.51	RT	1
B15	633+81.88	30.90	LT	1
B28	56+60.84	60.69	RT	1
E02	62+05.32	2.07	RT	1
E02a	61+24.84	2.00	RT	1
E11	64+16.15	88.77	RT	1
F01	70+35.00	1.62	LT	1
F01a	70+35.00	1.50	RT	1
F02	68+50.00	1.42	LT	1
F02a	68+50.00	1.50	RT	1
F03	66+50.00	3.50	LT	1
F03a	66+50.00	1.50	RT	1
F04	64+50.00	1.50	LT	1
F04a	64+50.00	1.50	RT	1
F05a	620+44.40	15.02	RT	1
F06a	622+24.72	17.50	RT	1
F07a	624+19.46	16.50	RT	1
F09a	626+12.85	14.27	RT	1
F22	62+36.92	62.00	LT	1
F23	62+34.90	1.50	LT	1
F24	63+00.00	1.50	LT	1
F24a	63+00.00	1.50	RT	1
F25	63+00.00	55.35	LT	1
F26	627+26.94	10.00	RT	1
F27	627+16.94	10.00	RT	1
F28	63+39.21	56.62	LT	1
F29	64+00.00	60.00	LT	1
F30	64+50.00	58.09	LT	1
F31	65+06.96	54.29	LT	1
F12	72+57.61	1.50	LT	1
F12a	72+57.61	1.50	RT	1
F13	72+57.61	74.05	LT	1
F15	72+09.26	74.03	RT	1
G01	78+50.00	1.50	RT	1
G01a	78+50.00	1.50	LT	1
G02	80+50.00	1.50	RT	1
G02a	80+50.00	1.50	LT	1
G3	82+85.00	3.83	RT	1
G3A	82+85.00	3.83	LT	1
I1E	85+25.00	3.83	RT	1
I1W	85+25.00	3.83	LT	1
I1D	85+25.00	74	LT	1
I2E	87+75.00	3.83	RT	1
I2W	87+75.00	3.83	LT	1

28000510
INLET FILTERS

STRUCTURE	STA	OFFSET	LT/RT	EACH
I2D	87+75.00	74	LT	1
I3E	90+25.00	3.83	RT	1
I3W	90+25.00	3.83	LT	1
I3D	90+25.00	74	LT	1
I6A	92+50.00	74	LT	1
I5E	92+80.00	3.83	RT	1
I5W	92+80.00	3.83	LT	1
I4E	93+50.00	3.83	RT	1
I4W	93+50.00	3.83	LT	1
I10	96+00.00	80.4	LT	1
J1E	97+00.00	3.83	RT	1
J1W	97+00.00	3.83	LT	1
J9	99+05.00	3.83	RT	1
J2	99+05.00	3.83	LT	1
J8E	101+55.00	3.83	RT	1
J8W	101+55.00	3.83	LT	1
J7E	101+80.00	3.83	RT	1
J7W	101+80.00	3.83	LT	1
J6E	102+25.00	3.83	RT	1
J6W	102+25.00	3.83	LT	1
J4E	104+50.00	3.83	RT	1
J4W	104+50.00	3.83	LT	1
J3E	107+00.00	3.83	RT	1
J3W	107+00.00	3.83	LT	1
K1E	109+50.00	3.83	RT	1
K1W	109+50.00	3.83	LT	1
K2E	111+50.00	3.83	RT	1
K2W	111+50.00	3.83	LT	1
K6E	113+38.00	3.83	RT	1
K6W	113+38.00	3.83	LT	1
K3E	115+50.00	3.83	RT	1
K3W	115+50.00	3.83	LT	1
L1E	119+75.00	3.83	RT	1
L1W	119+75.00	3.83	LT	1
L2E	122+00.00	3.83	RT	1
L2W	122+00.00	3.83	LT	1
L3E	124+25.00	3.83	RT	1
L3W	124+25.00	3.83	LT	1
L4E	126+50.00	3.83	RT	1
L4W	126+50.00	3.83	LT	1
L6E	128+50.00	3.83	RT	1
L6W	128+50.00	3.83	LT	1
L10 E	130+00.00	3.83	RT	1
L10W	130+00.00	3.83	LT	1
L11E	131+50.00	3.83	RT	1
L11W	131+50.00	3.83	LT	1
L13E	133+25.00	3.83	RT	1
L13W	133+25.00	3.83	LT	1
L15E	135+00.00	3.83	RT	1
L15W	135+00.00	3.83	LT	1
L19E	136+42.00	3.83	RT	1
L19W	136+42.00	3.83	LT	1
L20E	137+50.00	3.83	RT	1
L20W	137+50.00	3.83	LT	1
M1E	140+00.00	3.83	RT	1
M1W	140+00.00	3.83	LT	1
M2E	142+00.00	3.83	RT	1
M2W	142+00.00	3.83	LT	1
M3E	144+00.00	3.83	RT	1
M3W	144+00.00	3.83	LT	1
M4E	146+00.00	3.83	RT	1
M4W	146+00.00	3.83	LT	1
M5E	148+00.00	3.83	RT	1
M5W	148+00.00	3.83	LT	1
M6E	150+00.00	3.83	RT	1
M6W	150+00.00	3.83	LT	1
M7E	152+00.00	3.83	RT	1
M7W	152+00.00	3.83	LT	1
M9E	154+05.00	3.83	RT	1
M9W	154+05.00	3.83	LT	1
E13a	524+00.00	6.00	RT	1
E13b	524+00.00	14.15	RT	1
N26	1915+41.61	33.00	RT	1
N27	1917+00.00	33.00	RT	1
N29	1915+41.61	46.48	RT	1

28000510
INLET FILTERS

STRUCTURE	STA	OFFSET	LT/RT	EACH
N30	1917+51.03	45.18	RT	1
N31	1919+52.46	52.13	RT	1
N33	1919+92.77	33.00	RT	1
N34	1919+92.77	33.00	LT	1
N34a	1920+40.00	33.00	LT	1
N33c	1921+10.00	55.53	RT	1
N33d	1921+60.00	45.54	LT	1
N36a	1922+84.95	33.00	RT	1
N37	1922+84.95	33.00	LT	1
N37a	1922+00.00	33.00	LT	1
N38c	1924+77.62	48.41	LT	1
N39a	1925+40.00	45.58	LT	1
N40	1925+40.00	3.43	RT	1
N40b	1925+40.00	33.00	RT	1
N40c	1925+80.00	49.64	RT	1
N42	1928+90.76	32.61	LT	1
N43	1100+57.63	10.50	RT	1
N44	1100+58.23	10.50	LT	1
EX INLET	1928+89.32	32.22	RT	1
EX INLET	1931+77.76	29.92	RT	1
EX INLET	1931+78.02	29.07	LT	1
EX INLET	1935+28.07	36.90	RT	1
EX INLET	1935+41.18	56.73	RT	1
EX INLET	1936+21.55	80.48	RT	1
EX INLET	1936+34.93	59.97	RT	1
EX INLET	1936+35.66	36.66	LT	1
EX INLET	1936+50.81	56.93	LT	1
Z5a	1936+64.33	28.50	RT	1
EX INLET	1937+26.48	52.46	LT	1
W20a	1937+27.78	25.94	RT	1
W20c	1937+38.17	26.00	RT	1
EX INLET	1937+75.26	30.85	LT	1
W20e	1939+35.00	26.00	LT	1
EX INLET	1939+75.87	28.72	RT	1
EX INLET	1939+76.39	27.95	LT	1
H9a	1939+85.93	26.00	RT	1
EX INLET	1942+16.39	28.07	LT	1
EX INLET	1942+25.21	28.16	RT	1
EX INLET	1944+75.00	27.96	LT	1
EX INLET	1944+87.80	27.91	RT	1
EX INLET	1944+91.27	28.06	LT	1
EX INLET	1948+74.88	28.22	RT	1
EX INLET	1948+75.35	27.79	LT	1
H21a	1948+84.88	26.00	RT	1
EX INLET	1952+45.84	57.37	RT	1
EX INLET	1952+46.25	2.48	LT	1
EX MH	1952+46.48	27.66	RT	1
H23a	1954+00.00	0.00	RT	1
H24a	1954+00.00	60.98	RT	1
EX INLET	1955+60.09	27.79	LT	1
EX INLET	1955+98.83	26.77	LT	1
EX INLET	1956+24.39	2.40	LT	1
EX INLET	1960+05.53	1.81	LT	1
EX INLET	1962+51.18	2.13	LT	1
EX INLET	1956+00.30	1.86	LT	1
EX INLET	1967+51.01	2.04	LT	1
EX INLET	1973+19.15	11.93	LT	1
EX INLET	1977+94.03	1.95	LT	1
EX INLET	1980+65.68	1.85	LT	1
N52	1982+40.00	26.00	RT	1
EX INLET	1983+10.94	1.92	LT	1
N53	1983+50.00	26.00	RT	1
K18	1983+63.00	26.00	RT	1
EX INLET	1983+62.88	2.06	LT	1
EX INLET	1984+14.32	2.30	LT	1
EX INLET	2056+22.44	2.97	RT	1
EX INLET	2057+87.89	2.30	RT	1
N47b	2058+69.89	26.14	LT	1
N47	2059+50.00	26.00	LT	1
EX INLET	2061+15.70	24.55	LT	1
N48	2063+10.00	26.00	LT	1
EX INLET	2065+91.61	21.98	RT	1
EX INLET	2067+17.18	14.35	RT	1
EX INLET	2074+58.25	11.95	RT	1
EX INLET	2076+93.65	12.74	RT	1
EX INLET	128+32.68	21.19	RT	1
EX INLET	131+11.14	30.05	RT	1
Z3	131+75.00	20.00	LT	1
Z4a	131+75.70	20.00	RT	1
TOTAL				234

28500200
PRECAST BLOCK REVETMENT MAT

ALIGNMENT	STA	OFFSET	LT/RT	SQ YD
PR I-74	58+52	120'	LT	4
PR I-74	61+15	75'	RT	4
PR I-74	62+50	68'	RT	4
PR I-74	63+90	108'	RT	36
PR I-74	67+00	98'	RT	4
PR I-74	68+57	90'	RT	4
PR I-74	69+01	109'	RT	4
PR I-74	72+10	125'	RT	4
PR I-74	77+77	85'	RT	4
PR I-74	81+00	115'	RT	4
PR I-74	82+85	104'	RT	36
PR I-74	85+46	100'	RT	4
PR I-74	88+03	124'	RT	4
PR I-74	90+52	160'	RT	4
PR I-74	91+84	102'	RT	4
PR I-74	93+02	185'	RT	4
PR I-74	94+67	120'	RT	4
PR I-74	96+00	118'	LT	36
PR I-74	96+20	207'	RT	4
PR I-74	98+34	115'	RT	4
PR I-74	99+83	222'	RT	4
PR I-74	99+12	128'	LT	4
PR I-74	100+99	104'	RT	4
PR I-74	100+99	82'	LT	4
PR I-74	102+25	85'	LT	36
PR I-74	102+31	86'	LT	4
PR I-74	103+63	81'	RT	4
PR I-74	104+92	84'	LT	4
PR I-74	106+22	87'	RT	4
PR I-74	107+54	66'	LT	4
PR I-74	108+80	71'	RT	4
PR I-74	110+15	69'	LT	4
PR I-74	112+63	96'	RT	4
PR I-74	112+76	93'	LT	4
PR I-74	113+67	222'	RT	4
PR I-74	118+56	100'	RT	4
PR I-74	119+41	79'	LT	4
PR I-74	121+09	96'	RT	4
PR I-74	121+14	68'	LT	4
PR I-74	123+66	86'	RT	4
PR I-74	123+79	94'	LT	4
PR I-74	125+78	87'	LT	4
PR I-74	126+23	80'	RT	4
PR I-74	127+37	82'	LT	4
PR I-74	128+32	75'	LT	4
PR I-74	129+54	77'	RT	4
PR I-74	131+27	68'	LT	4
PR I-74	131+81	69'	RT	4
PR I-74	133+17	69'	LT	4
PR I-74	133+26	70'	LT	36
PR I-74	134+00	73'	RT	4
PR I-74	135+35	74'	LT	4
PR I-74	137+99	68'	LT	4
PR I-74	138+97	72'	RT	4
PR I-74	140+58	76'	LT	4
PR I-74	141+58	77'	RT	4
PR I-74	143+15	78'	LT	4
PR I-74	144+22	77'	RT	4
PR I-74	145+71	72'	LT	4
PR I-74	146+86	79'	RT	4
PR I-74	148+27	75'	LT	4
PR I-74	149+51	78'	RT	4
PR I-74	150+83	81'	LT	4
PR I-74	152+14	80'	RT	4
PR I-74	152+58	80'	LT	4
PR I-74	153+98	79'	RT	4
RAMP 7TH-A	632+42	18'	RT	4
RAMP 7TH-A	641+15	33'	RT	4
RAMP 7TH-B	526+00	12'	RT	4
RAMP 7TH-B	528+00	40'	RT	4
RAMP 7TH-B	529+52	19'	RT	4
RAMP 7TH-B	530+57	14'	RT	4
RAMP 7TH-B	531+31	14'	RT	4
PR 19th ST	1969+82	38'	RT	4
PR 19th ST	1974+54	50'	RT	4
PR 19th ST	2066+32	24'	RT	4
PR 19th ST	2074+76	28'	LT	4
STAGE 1-0				
PR I-74	156+20	134'	LT	36
PR I-74	157+40	143'	LT	36
PR I-74	158+04	87'	LT	36
TOTAL				574

28200200
FILTER FABRIC

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
PR I-74	58+52	120'	LT				4
PR I-74	61+15	75'	RT				4
PR I-74	62+50	68'	RT				4
PR I-74	63+90	108'	RT				36
PR I-74	67+00	98'	RT				4
PR I-74	68+57	90'	RT				4
PR I-74	69+01	109'	RT				4
PR I-74	72+10	125'	RT				4
PR I-74	77+77	85'	RT				4
PR I-74	81+00	115'	RT				4
PR I-74	82+85	104'	RT				36
PR I-74	85+46	100'	RT				4
PR I-74	88+03	124'	RT				4
PR I-74	90+52	160'	RT				4
PR I-74	91+84	102'	RT				4
PR I-74	93+02	185'	RT				4
PR I-74	94+67	120'	RT				4
PR I-74	96+00	118'	LT				36
PR I-74	96+20	207'	RT				4
PR I-74	98+34	115'	RT				4
PR I-74	99+83	222'	RT				4
PR I-74	99+12	128'	LT				4
PR I-74	100+99	104'	RT				4
PR I-74	100+99	82'	LT				4
PR I-74	102+25	85'	LT				36
PR I-74	102+31	86'	LT				4
PR I-74	102+50	96'	LT	105+00	96'	LT	444
PR I-74	103+63	81'	RT				4
PR I-74	104+92	84'	LT				4
PR I-74	106+22	87'	RT				4
PR I-74	107+54	66'	LT				4
PR I-74	108+80	71'	RT				4
PR I-74	110+15	69'	LT				4
PR I-74	111+00	97'	RT	116+00	80'	RT	949
PR I-74	112+63	96'	RT				4
PR I-74	112+76	93'	LT				4
PR I-74	113+67	222'	RT				4
PR I-74	118+56	100'	RT				4
PR I-74	119+41	79'	LT				4
PR I-74	121+09	96'	RT				4
PR I-74	121+14	68'	LT				4
PR I-74	123+66	86'	RT				4
PR I-74	123+79	94'	LT				4
PR I-74	125+78	87'	LT				4
PR I-74	126+23	80'	RT				4

28200200
FILTER FABRIC

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ YD
PR I-74	127+37	82'	LT				4
PR I-74	127+48	89'	LT	129+00	71'	LT	255
PR I-74	127+52	84'	RT	130+06	76'	RT	467
PR I-74	128+32	75'	LT				4
PR I-74	129+54	77'	RT				4
PR I-74	130+35	78'	LT	134+28	80'	LT	936
PR I-74	131+27	68'	LT				4
PR I-74	131+46	90'	RT	136+64	86'	RT	881
PR I-74	131+81	69'	RT				4
PR I-74	133+17	69'	LT				4
PR I-74	133+26	70'	LT				36
PR I-74	134+00	73'	RT				4
PR I-74	135+35	74'	LT				4
PR I-74	136+71	79'	LT	155+00	76'	LT	3,207
PR I-74	137+99	68'	LT				4
PR I-74	138+31	80'	RT	155+00	83'	RT	2,924
PR I-74	138+97	72'	RT				4
PR I-74	140+58	76'	LT				4
PR I-74	141+58	77'	RT				4
PR I-74	143+15	78'	LT				4
PR I-74	144+22	77'	RT				4
PR I-74	145+71	72'	LT				4
PR I-74	146+86	79'	RT				4
PR I-74	148+27	75'	LT				4
PR I-74	149+51	78'	RT				4
PR I-74	150+83	81'	LT				4
PR I-74	152+14	80'	RT				4
PR I-74	152+58	80'	LT				4
PR I-74	153+98	79'	RT				4
RAMP 7TH-A	632+42	18'	RT				4
RAMP 7TH-A	641+15	33'	RT				4
RAMP 7TH-B	526+00	12'	RT				4
RAMP 7TH-B	528+00	40'	RT				4
RAMP 7TH-B	529+52	19'	RT				4
RAMP 7TH-B	530+57	14'	RT				4
RAMP 7TH-B	531+31	14'	RT				4
PR 19th ST	1969+82	38'	RT				4
PR 19th ST	1974+54	50'	RT				4
PR 19th ST	2066+32	24'	RT				4
PR 19th ST	2074+76	28'	LT				4
STAGE 1-0							
PR I-74	156+20	134'	LT				36
PR I-74	157+40	143'	LT				36
PR I-74	158+04	87'	LT				36
TOTAL							10,639

SCHQ-09

FILE NAME = ...\D2CONCD-ABC-sht-schedule07M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
EROSION CONTROL

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	72
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

44000500

COMBINATION CURB AND GUTTER REMOVAL

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
RAMP S-7	77+73	76'	RT	87+02	20'	RT	929
RAMP S-7	77+87	2'	LT	87+02	2'	LT	917
RAMP S-7	87+02	20'	RT	88+09	20'	RT	107
RAMP S-7	87+02	2'	LT	88+07	3'	LT	105
RAMP 7-S	78+78	39'	LT	84+02	27'	LT	526
RAMP 7-S	78+78	26'	RT	83+72	3'	RT	496
12TH AVE	127+02	20'	RT	127+14	28'	RT	15
12TH AVE	127+30	28'	RT	127+38	20'	RT	12
12TH AVE	127+41	20'	RT	128+31	20'	RT	90
19TH ST	1920+75	52'	RT	1920+99	53'	RT	24
19TH ST	1920+75	17'	LT	1921+29	17'	LT	54
19TH ST	1920+99	53'	RT	1930+64	32'	RT	974
19TH ST	1921+29	17'	LT	1928+42	113'	LT	779
19TH ST	1928+68	113'	LT	1930+89	30'	LT	290
19TH ST	1930+64	32'	RT	1935+32	138'	RT	573
19TH ST	1930+89	30'	LT	1936+90	200'	LT	736
19TH ST	1935+83	151'	RT	1939+80	28'	RT	465
19TH ST	1937+23	193'	LT	1939+80	28'	LT	405
19TH ST	1939+80	28'	RT	1948+30	28'	RT	850
19TH ST	1939+80	28'	LT	1948+30	28'	LT	850
19TH ST / 19TH ST SB	1948+30	28'	RT	2057+00	1'	RT	870
19TH ST / 19TH ST NB	1948+30	28'	LT	1955+61	36'	LT	752
19TH ST / 19TH ST NB	1955+97	34'	LT	1957+00	2'	LT	126
19TH ST SB	2055+94	36'	LT	2057+00	27'	LT	222
19TH ST NB	1957+00	2'	LT	1969+80	3'	LT	1282
19TH ST NB	1957+00	28'	RT	1966+76	28'	RT	988
19TH ST SB	2057+00	27'	LT	2068+20	26'	LT	1108
19TH ST SB	2057+00	1'	RT	2063+48	5'	RT	651
19TH ST SB	2065+90	22'	RT	2068+78	14'	RT	288
19TH ST NB	1964+35	213'	LT	1964+67	195'	LT	33
19TH ST NB	1969+50	29'	RT	1969+80	29'	RT	30
19TH ST NB	1973+00	13'	LT	1985+60	3'	LT	1260
19TH ST NB	1973+00	40'	RT	1974+81	46'	RT	182
19TH ST NB	1980+78	28'	RT	1985+60	27'	RT	485
19TH ST SB	2074+30	13'	RT	2078+00	13'	RT	375
						TOTAL	17,849

44000600

SIDEWALK REMOVAL

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ FT
12TH AVE	124+10	28'	RT	127+13	28'	RT	1,436
12TH AVE	127+31	28'	RT	129+73	40'	RT	1,443
12TH AVE	129+85	81'	LT	132+14	27'	LT	1,036
19TH ST	1915+31	74'	RT	1920+94	64'	RT	2,678
19TH ST	1920+94	64'	RT	1930+59	43'	RT	8,852
19TH ST	1928+79	38'	LT	1928+84	56'	LT	94
19TH ST	1930+59	43'	RT	1931+76	40'	RT	550
19TH ST	1932+03	40'	RT	1935+48	54'	RT	1,660
19TH ST	1936+70	39'	RT	1939+80	39'	RT	1,455
19TH ST	1939+80	39'	RT	1948+30	39'	RT	4,080
19TH ST / SB	1948+30	39'	RT	2055+97	14'	RT	3,705
19TH ST NB	1956+00	13'	LT	1956+22	13'	LT	78
19TH ST NB	1963+16	13'	LT	1963+31	13'	LT	71
19TH ST NB	1963+63	13'	LT	1963+75	13'	LT	50
19TH ST NB	1970+62	36'	LT	1970+77	37'	LT	103
19TH ST NB	1970+86	8'	LT	1970+95	1'	LT	56
19TH ST NB	1970+86	42'	RT	1970+94	31'	RT	69
19TH ST NB	1972+06	59'	RT	1972+31	45'	RT	160
19TH ST NB	1972+16	0'	LT	1972+27	14'	LT	105
19TH ST NB	1972+36	61'	LT	1972+48	48'	LT	98
19TH ST SB	2071+86	51'	RT	2072+04	67'	RT	146
19TH ST SB	2072+09	32'	RT	2072+16	11'	RT	128
19TH ST SB	2072+17	42'	LT	2072+23	60'	LT	90
19TH ST SB	2073+35	11'	RT	2073+50	27'	RT	128
19TH ST SB	2073+39	44'	LT	2073+43	31'	LT	53
19TH ST SB	2073+58	56'	RT	2073+75	44'	RT	156
						TOTAL	28,480

44001980

CONCRETE BARRIER REMOVAL

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
PR I-74	50+61	7'	RT	57+46	7'	LT	685
PR I-74	53+80	9'	RT	56+09	14'	RT	229
						TOTAL	914

44003100

MEDIAN REMOVAL

ALIGNMENT	STA	OFFSET	LT/RT/C	STA	OFFSET	LT/RT/C	SQ FT
19TH ST	1920+98	14	RT	1921+15	15	RT	195
19TH ST	1921+15	15	RT	1926+00	1	RT	4,728
19TH ST	1947+90	0	C	1948+30	0	C	176
19TH ST/19NB	1948+30	0	C	1955+58	13	LT	5,608
NOTE: MEDIAN REMOVAL INCLUDES THE REMOVAL OF ADJACENT COMB CURB GUTTER REM							
						TOTAL	10,707

44004000

PAVED DITCH REMOVAL

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
PR I-74	48+19	129'	LT	58+00	99'	LT	1,007
PR I-74	51+98	104'	LT	51+99	121'	LT	17
PR I-74	59+26	197'	LT	60+05	134'	LT	108
PR I-74	62+63	19'	LT	67+00	19'	LT	435
PR I-74	63+89	106'	RT	63+95	93'	RT	15
PR I-74	67+00	19'	LT	68+17	19'	LT	116
PR I-74	72+99	143'	RT	76+00	103'	RT	305
PR I-74	76+00	103'	RT	76+76	81'	RT	79
PR I-74	79+19	80'	RT	81+00	123'	RT	181
PR I-74	97+80	106	RT	101+59	102	RT	392
PR I-74	110+60	85	RT	114+00	106	RT	340
19TH ST	1927+68	54'	RT	1927+95	52'	RT	27
						TOTAL	3,022

44004250

PAVED SHOULDER REMOVAL

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	SQ YD
I-74	50+61.00		LT	58+04.00		LT	841
I-74	50+61.00		LT	58+04.00		LT	465
I-74	50+61.00		RT	56+58.22		RT	326
I-74	50+61.00		RT	56+58.16		RT	735
I-74	61+95.00		RT	67+00.00		RT	339
I-74	61+95.00		RT	67+00.00		RT	400
I-74	63+80.00		LT	67+00.00		LT	250
I-74	63+80.00		LT	67+00.00		LT	157
I-74	67+00.00		LT	70+95.00		LT	428
I-74	67+00.00		LT	70+95.00		LT	453
I-74	67+00.00		RT	70+64.00		RT	289
I-74	67+00.00		RT	70+64.00		RT	273
I-74	71+64.00		RT	76+00.00		RT	417
I-74	71+64.00		RT	76+00.00		RT	328
I-74	71+95.00		LT	76+00.00		LT	300
I-74	71+95.00		LT	76+00.00		LT	497
I-74	76+00.00		LT	81+00.00		LT	673
I-74	76+00.00		LT	81+00.00		LT	505
I-74	76+00.00		RT	81+00.00		RT	321
I-74	76+00.00		RT	81+00.00		RT	642
I-74	81+00.00	22.92'	RT	94+52.93	25.54'	RT	757'
I-74	81+00.00	32.88'	LT	93+77.44	25.27'	LT	895'
I-74	81+00.00	63.53'	RT	86+17.58	89.71'	RT	460'
I-74	86+24.01	64.12'	RT	94+87.45	49.86'	RT	1010'
I-74	81+00.00	63.46'	LT	93+09.05	69.03'	LT	1356'
I-74	96+99.94	50.06'	RT	111+00.00	56.53'	RT	1677'
I-74	96+61.78	20.00'	RT	111+00.00	16.68'	RT	958'
I-74	96+00.00	22.60'	LT	111+00.00	17.53'	LT	1081'
I-74	97+37.29	53.35'	LT	111+00.00	47.15'	LT	1593'
I-74	118+43.03	77.00'	RT	125+00.00	64.15'	RT	746'
I-74	111+00.00	46.53'	RT	118+39.75	46.97'	RT	847'
I-74	111+00.00	16.68'	RT	125+00.00	17.06'	RT	970'
I-74	111+00.00	17.53'	LT	125+00.00	17.40'	LT	975'
I-74	111+00.00	47.15'	LT	124+86.50	49.12'	LT	1632'
I-74	125+00.00	17.40'	LT	140+00.00	16.82'	LT	1067'
I-74	125+00.00	71.30'	LT	140+00.00	47.32'	LT	1676'
I-74	125+00.00	17.06'	RT	140+00.00	17.75'	RT	1033'
I-74	125+00.00	54.39'	RT	140+00.00	47.78'	RT	1678'
I-74	129+79	60'	RT	129+94	58.00'	rt	67'
I-74	140+00.00	17.05'	LT	155+00.00	13.53'	LT	1062'
I-74	140+00.00	47.32'	LT	155+00.00	43.41'	LT	1695'
I-74	140+00.00	18.11'	RT	155+00.00	17.21'	RT	1022'
I-74	140+00.00	47.78'	RT	155+00.00	47.08'	RT	1622'
RAC-A	44+48.12			1022+88	30.28'	RT	357'
RAC-A	1017+00.00	3.79'	RT	1025+00.00	4.83'	LT	638'
RAC-B	920+07.47	16.07'	LT	928+52.12	27.25'	LT	438'
RAC-B	922+06.40	0.14'	LT	929+59	0.16'	LT	563'
RAC-C	723+86.86	6.66'	RT	734+00.00	1.71'	LT	695'
RAC-C	723+91.92	9.21'	LT	734+00.00	22.05'	LT	474'
RAC-C	734+00.00	17.73'	LT	736+23.34	23.32'	LT	94'
RAC-C	734+00.00	1.70'	LT	739+72.71	0.14'	LT	387'
RAC-D	824+91.48	2.02'	RT	833+23.62	2.25'	LT	1291'
RAC-D	827+04	19.85'	RT	833+63.68	21.74'	RT	390'
19TH ST NB	1974+24.00		RT	1978+51.00		RT	210
						TOTAL	40,055

SCHQ-10

FILE NAME = ...\D2CONCD-ABC-sht-schedule3IM.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	73
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

63200310							
GUARDRAIL REMOVAL							
ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	FOOT
I-74	62+20	90'	RT	67+00	85'	RT	496
I-74	62+61	19'	RT	67+00	16'	RT	442
I-74	63+92	30'	LT	67+00	28'	LT	305
I-74	64+26	95'	LT	67+00	81'	LT	266
I-74	67+00	81'	LT	67+57	77'	LT	55
I-74	67+00	28'	LT	70+17	32'	LT	313
I-74	67+00	16'	RT	70+62	16'	RT	365
I-74	67+00	85'	RT	70+41	72'	RT	353
I-74	71+88	70'	RT	76+00	69'	RT	412
I-74	71+99	36'	LT	75+20	33'	LT	322
I-74	72+42	76'	LT	76+00	80'	LT	358
I-74	76+00	80'	LT	81+00	77'	LT	515
I-74	76+00	69'	RT	81+00	73'	RT	487
I-74	77+67	35'	LT	80+48	33'	LT	285
I-74	81+00	74'	LT	92+79	74'	LT	1172
I-74	82+79	29'	LT	85+43	29'	LT	265
I-74	81+00	74'	RT	83+30	84'	RT	227
I-74	91+29	15'	RT	94+24	20'	RT	297
I-74	91+85	69'	RT	94+87	61'	RT	308
I-74	95+32	77'	LT	98+70	104'	LT	333
I-74	96+15	22'	LT	97+64	8'	RT	153
I-74	105+18	13'	RT	108+58	13'	RT	341
I-74	108+51	14'	LT	110+61	14'	LT	211
I-74	127+23	61'	RT	129+60	58'	RT	238
I-74	127+68	14'	RT	130+12	13'	RT	246
I-74	129+75	13'	LT	132+54	13'	LT	279
I-74	134+36	14'	RT	137+28	14'	RT	295
I-74	136+28	13'	LT	139+12	12'	LT	285
RAMP 7-S	81+03	14'	RT	83+72	5'	RT	270
RAMP 7-S	81+31	30'	LT	84+02	28'	LT	271
RAMP S-7	87+07	21'	RT	88+09	20'	RT	102
RAMP S-7	87+07	4'	LT	88+07	4'	LT	102
19TH ST SB	2068+78	35'	RT	2070+81	50'	RT	208
						TOTAL	10,577

X0326677							
REMOVE HIGH TENSION CABLE MEDIAN BARRIER							
ALIGNMENT	STA FROM	STA TO		LT/RT			FOOT
I-74	71+87	76+00		RT			385
I-74	76+00	81+00		RT			498
I-74	81+00	90+90		RT			992
I-74	97+01	104+84		RT			771
I-74	108+85	127+34		RT			1837
I-74	130+82	134+01		RT			305
I-74	137+50	157+60		RT			1993
						TOTAL	6,781

X0326687				
REMOVE HIGH TENSION CABLE MEDIAN BARRIER TERMINAL				
ALIGNMENT	STA	LT/RT		EACH
I-74	71+87	RT		1
I-74	90+90	RT		1
I-74	97+01	RT		1
I-74	104+84	RT		1
I-74	108+85	RT		1
I-74	127+34	RT		1
I-74	130+82	RT		1
I-74	134+01	RT		1
I-74	137+50	RT		1
		TOTAL		9

X4402805						
ISLAND REMOVAL						
ALIGNMENT	STA	STA	OFFSET	LT/RT	LOCATION	SQ FT
19TH ST	1935+67	1936+04	26'	RT	12TH AVE WEST LEG	561
19TH ST	1936+81	1937+11	26'	LT	12TH AVE EAST LEG	536
					TOTAL	1,097

X6060500							
CORRUGATED MEDIAN REMOVAL							
ALIGNMENT	STA			STA			SQ FT
19TH ST	1928+85			1930+77			2,343
19TH ST	1930+77			1935+74			2,273
19TH ST	1937+07			1939+80			1,034
19TH ST	1939+80			1947+90			3,418
						TOTAL	9,068

X6430120							
REMOVE IMPACT ATTENUATORS, NO SALVAGE							
ALIGNMENT	STA						EACH
I-74	62+90						1
						TOTAL	1

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

SCHQ-11

FILE NAME = ...\D2CONCD-ABC-sht-schedule32M.dgn $MODELNAME$	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	74
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

30300001

AGGREGATE SUBGRADE IMPROVEMENT

ALIGNMENT	STA FROM		STA TO		SQ YD	DEPTH (IN)	CU YD
EX 12TH AVE	123+00.00		128+30.50		95	12	32
AOTC WB (EB)	119+00	(219+00)	122+23	(222+38)	263	12	88
AOTC WB (EB)	123+83	(223+98)	126+75	(226+75)	171	12	57
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							
						TOTAL	176

30300011

AGGREGATE SUBGRADE IMPROVEMENT

	TON
AS DIRECTED BY ENGINEER	2,000
TOTAL	2,000

30300112

AGGREGATE SUBGRADE IMPROVEMENT 12"

ALIGNMENT	STA FROM	STA TO	LOCATION	SQ YD
RAMP 7TH-A	625+10.00	627+34.00	PAVEMENT, SHOULDER & GORE	1,176
RAMP 7TH-A	631+16.01	642+37.34	PAVEMENT & SHOULDER	6,160
RAMP 7TH-B	522+94.93	537+99.01	PAVEMENT, SHOULDER & GORE	5,340
19TH ST	1917+43.85	1969+80.00	(19TH NB) / 2068+20 (19TH SB) PAVEMENT	38,012
19TH ST NB	1973+00.00	1985+60.00	PAVEMENT	5,645
19TH ST SB	2074+30.00	2078+00.00	PAVEMENT	1,276
19TH ST SB	1967+70.26	1970+03.40	RAMP RAC-D	419
RAC-A	1015+36.50	1025+77.79	RAMP RAC-A	3,146
RAC-B	920+07.48	931+71.93	RAMP RAC-B	3,372
RAC-C	724+09.79	739+72.71	RAMP RAC-C	5,053
RAC-D	824+87.85	833+18.36	RAMP RAC-D	2,880
			TOTAL	72,479

31100800

SUBBASE GRANULAR MATERIAL, TYPE A 9"

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	SQ YD
I-74	49+78.00	62.00'	RT	54+95.00	62.00'	RT	178
I-74	61+14.84	62.00'	RT	62+87.29	62.00'	RT	62
I-74	72+46.00	74.00'	LT	81+00.00	74.00'	LT	306
I-74	77+54.77	74.00'	RT	81+00.00	74.00'	RT	117
I-74	81+00.00	77.00'	RT	85+49.06	77.00'	RT	147
I-74	81+00.00	77.00'	LT	84+95.43	79.00'	LT	143
I-74	92+32.71	72.13'	RT	94+93.06	71.42'	RT	89
I-74	97+56.88	67.64'	RT	98+86.44	62.77'	RT	40
I-74	127+71.36	62.00'	RT	130+36.00	65.00'	RT	91
I-74	128+90.23	62.00'	LT	129+59.00	65.00'	LT	20
I-74	130+92.00	65.00'	RT	136+98.42	65.00'	RT	203
I-74	130+15.00	65.00'	LT	135+46.42	65.00'	RT	178
I-74	136+42.00	65.00'	LT	139+06.64	62.00'	LT	91
RAMP 7TH-B	529+43.49	6.00'	RT	530+55.00	8.00'	RT	45
RAMP 7TH-B	531+30.00	8.00'	RT	534+73.63	8.00'	RT	121
RAMP 7TH-B	535+76.66	8.00'	RT	541+07.63	11.60'	RT	187
RAC-D	824+99.00	9.00'	LT	827+21.53	6.00'	LT	77
						TOTAL	2,095

#2000321

AGGREGATE SUBGRADE IMPROVEMENT 13 1/2"

ALIGNMENT	STA FROM	STA TO	LOCATION	SQ YD
I-74	49+03.54	56+67.84	EB ML PAVEMENT & SHOULDER	5,296
I-74	49+03.54	58+01.84	WB ML PAVEMENT & SHOULDER	6,343
I-74	61+17.84	70+54.61	EB ML PAVT & SHLDR/RAMP 7TH-B MERGE	6,929
I-74	62+27.84	70+54.61	WB ML PAVT & SHLDR/RAMP 7TH-A DIVERGE	6,971
I-74	72+01.67	81+00.00	EB ML PAVEMENT & SHOULDER	7,736
I-74	72+01.67	81+00.00	WB ML PAVEMENT & SHOULDER	7,505
I-74	81+00.00	93+76.97	EB ML PAVEMENT & SHOULDER	11,099
I-74	96+94.50	155+00.00	EB ML PAVEMENT & SHOULDER	39,476
I-74	81+00.00	93+70.91	WB ML PAVEMENT & SHOULDER	10,009
I-74	96+89.39	155+00.00	WB ML PAVEMENT & SHOULDER	39,092
			TOTAL	140,456

31200100

STABILIZED SUBBASE 4"

ALIGNMENT	STA FROM	STA TO	LOCATION	SQ YD
I-74	49+03.54	56+67.84	EB ML PAVEMENT & SHOULDER	4,943
I-74	49+03.54	58+01.84	WB ML PAVEMENT & SHOULDER	5,414
I-74	61+17.84	70+54.61	EB ML PAVT & SHLDR/RAMP 7TH-B MERGE	6,068
I-74	62+27.84	70+54.61	WB ML PAVT & SHLDR/RAMP 7TH-A DIVERGE	6,301
I-74	72+01.67	81+00.00	EB ML PAVEMENT & SHOULDER	6,924
I-74	72+01.67	81+00.00	WB ML PAVEMENT & SHOULDER	7,392
I-74	81+00.00	93+76.97	EB ML PAVEMENT & SHOULDER	11,163
I-74	96+94.50	155+00.00	EB ML PAVEMENT & SHOULDER	40,299
I-74	81+00.00	93+70.91	WB ML PAVEMENT & SHOULDER	10,352
I-74	96+89.39	155+00.00	WB ML PAVEMENT & SHOULDER	39,980
RAMP 7TH-A	625+10.00	627+34.00	PAVEMENT, SHOULDER & GORE	1,239
RAMP 7TH-A	631+16.01	642+37.34	PAVEMENT & SHOULDER	5,805
RAMP 7TH-B	521+59.61	537+99.01	PAVEMENT, SHOULDER & GORE	5,030
RAC-A	1017+00.00	1026+77.39	RAMP RAC-A	3,065
RAC-B	920+07.48	932+52.96	RAMP RAC-B	3,325
RAC-C	723+11.82	739+72.71	RAMP RAC-C	4,915
RAC-D	824+80.32	833+63.68	RAMP RAC-D	2,801
19TH ST	1913+20.25	1969+80.00	(19TH NB) / 2068+20 (19TH SB)	41,176
19TH ST NB	1973+00.00	1985+60.00	PAVEMENT	5,471
19TH ST SB	2074+30.00	2078+00.00	PAVEMENT	1,237
19TH ST NB	1967+68.83	1970+01.97	RAMP RAC-D	407
			TOTAL	213,307

40600295

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)

ALIGNMENT	STA FROM	STA TO		SQ FT	APPLICATIONS	RATE (LBS/SQ FT)	POUND
I-74	114+00.00	117+33.05		3,497	1	0.050	175
I-74	116+00.00	159+00.00		45,150	1	0.050	2,258
I-74	115+00.00	123+01.38		8,414	1	0.050	421
I-74	127+01.05	156+05.44		30,496	1	0.050	1,525
I-74	114+00.00	117+33.05		3,497	1	0.025	87
I-74	116+00.00	159+00.00		45,150	1	0.025	1,129
I-74	115+00.00	123+01.38		8,414	1	0.025	210
I-74	127+01.05	156+05.44		30,496	1	0.025	762
EX 12TH AVE	123+00	128+30.50		21,268	2	0.05	2,127
AOTC EB & WB	119+00	122+23		59,232	1	0.05	2,962
AOTC EB & WB	123+83	126+75		38,532	1	0.05	1,927
							-
						TOTAL	13,583

40600825

POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50

ALIGNMENT	STA FROM	STA TO	SQ YD	UNIT WT (LB/SY-IN)	THICKNESS (IN)		TON
12TH AVE	123+00.00	128+30.50	2,363	112	1 1/2		199
						TOTAL	199

40600845

POLYMERIZED LEVELING BINDER (MACHINE METHOD), N90

ALIGNMENT	STA FROM	STA TO	SQ YD	UNIT WT (LB/SY-IN)	THICKNESS (IN)	TON
I-74 (EB)	114+00.00	117+33.05	394	119	1	23
I-74 (WB)	115+00.00	123+01.38	950	119	1	57
I-74 (EB)	123+34.36	159+00.00	4,140	119	1	246
I-74 (WB)	127+00.94	142+95.89	1,840	119	1	109
RAC-A	1018+90.91	1023+05.19	195	119	1	12
					TOTAL	447

40603535

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50

ALIGNMENT	STA FROM	STA TO	SQ YD	UNIT WT (LB/SY-IN)	THICKNESS (IN)		TON
12TH AVE	123+00.00	128+30.50	2,363	112	1 1/2		199
AOTC EB & WB	119+00	122+23	6,582	112	1 1/2		553
AOTC EB & WB	123+83	126+75	4,282	112	1 1/2		360
						TOTAL	1,112

40603570

PLOYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N90

ALIGNMENT	STA FROM	STA TO	SQ YD	UNIT WT (LB/SY-IN)	THICKNESS (IN)		TON
I-74	114+00.00	117+33.05	389	119	1.25		29
I-74	116+00.00	159+00.00	5,017	119	1.25		373
I-74	115+00.00	123+01.38	935	119	1.25		70
I-74	127+01.05	156+05.44	3,388	119	1.25		252
						TOTAL	724

42000060

WELDED WIRE REINFORCEMENT

ALIGNMENT	STA FROM			STA TO			SQ YD
EX 12TH AVE	123+00.00			128+30.50			70
AOTC WB (EB)	119+00	(219+00)		122+23	(222+38)		180
AOTC WB (EB)	123+83	(223+98)		126+75	(226+75)		120
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							
						TOTAL	370

42000080

PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB

ALIGNMENT		STA	STA	SQ YD
I-74	WB	48+94	49+29	210
I-74	EB	49+13	49+36	142
I-74	EB	56+56	56+71	92
I-74	WB	57+90	58+05	81
I-74	EB	61+15	61+30	91
I-74	WB	62+26	62+47	129
I-74	EB	70+11	70+45	219
I-74	WB	70+20	70+69	372
I-74	EB	71+87	72+17	223
I-74	WB	72+10	72+46	265
I-74	WB	92+46	93+52	541
I-74	EB	93+62	94+58	491
I-74	EB	97+10	97+88	382
I-74	WB	96+26	97+04	579
RAMP 7TH-A		627+03	627+37	137
RAMP 7TH-A		631+13	631+28	54
			TOTAL	4,008

42000406

PORTLAND CEMENT CONCRETE PAVEMENT 9 1/4" (JOINTED)

ALIGNMENT	STA FROM	STA TO					SQ YD
19TH ST	1913+20.25	1969+80.00	(19TH NB) / 2068+20 (19TH SB)				34,042
19TH NB	1973+00.00	1985+60.00					4,856
19TH SB	2074+30.00	2078+00.00					1,069
						TOTAL	39,967

42000511

PORTLAND CEMENT CONCRETE PAVEMENT 10 1/2" (JOINTED)

ALIGNMENT	STA FROM	STA TO	LOCATION	SQ YD
I-74	49+29	57+90	WB ML PAVEMENT & SHOULDER	5,157
I-74	49+36	56+56	EB ML PAVEMENT & SHOULDER	4,662
I-74	61+30	70+11	EB ML PAVEMENT & SHOULDER/RAMP 7TH-B MERGE	5,653
I-74	62+47	70+20	WB ML PAVEMENT & SHOULDER/RAMP 7TH-A DIVERGE	5,774
I-74	72+17	81+00	EB ML PAVEMENT & SHOULDER	6,465
I-74	72+46	81+00	WB ML PAVEMENT & SHOULDER	6,906
I-74	81+00	92+46	WB ML PAVEMENT & SHOULDER	9,151
I-74	81+00	93+62	EB ML PAVEMENT & SHOULDER	10,201
I-74	97+04	155+00	WB ML PAVEMENT & SHOULDER	39,871
I-74	97+88	155+00	EB ML PAVEMENT & SHOULDER	39,423
RAMP 7TH-A	625+10	627+03	PAVEMENT, SHOULDER & GORE	1,114
RAMP 7TH-A	631+28	642+37	PAVEMENT & SHOULDER	5,203
RAMP 7TH-B	522+95	537+99	PAVEMENT, SHOULDER & GORES	4,322
RAC-A	1015+36	1023+40	RAMP RAC-A	2,157
RAC-B	920+07	928+52	RAMP RAC-B	2,111
RAC-C	726+00	739+73	RAMP RAC-C	3,828
RAC-D	824+88	833+66	RAMP RAC-D	2,085
19THN	1968+17	1970+03	RAMP RAC-D	234
			TOTAL	154,317

SCHQ-12

FILE NAME = ...\D2CONCD-ABC-sht-schedule33M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
	PLOT SCALE =	CHECKED - JJT	REVISED -
$MODELNAME$	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
PAVEMENT

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	75
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

42001300

PROTECTIVE COAT

ALIGNMENT	STA FROM	STA TO	DESCRIPTION	SQ YD
PAVEMENTS				
I-74	49+29	57+90	EB ML PAVEMENT & SHOULDER	10,314
I-74	49+36	56+56	WB ML PAVEMENT & SHOULDER	9,324
I-74	61+30	70+11	EB ML PAVT & SHOULDER/RAMP 7TH-B MERGE	11,306
I-74	62+47	70+20	WB ML PAVT & SHLDR/RAMP 7TH-A DIVERGE	11,548
I-74	72+17	81+00	EB ML PAVEMENT & SHOULDER	12,930
I-74	72+46	81+00	WB ML PAVEMENT & SHOULDER	13,812
I-74	81+00	92+46	EB ML PAVEMENT & SHOULDER	18,302
I-74	81+00	93+62	WB ML PAVEMENT & SHOULDER	20,402
I-74	97+04	155+00	EB ML PAVEMENT & SHOULDER	79,742
I-74	97+88	155+00	WB ML PAVEMENT & SHOULDER	78,846
RAMP 7TH-A	625+10	627+03	PAVEMENT, SHOULDER & GORE	2,228
RAMP 7TH-A	631+28	642+37	PAVEMENT & SHOULDER	10,406
RAMP 7TH-B	522+95	537+99	PAVEMENT, SHOULDER & GORE	8,644
RAC-A	1015+36	1023+40	RAMP RAC-A	4,314
RAC-B	920+07	928+52	RAMP RAC-B	4,222
RAC-C	726+00	739+73	RAMP RAC-C	7,656
RAC-D	824+88	833+66	RAMP RAC-D	4,170
19TH NB	1968+17	1970+03	RAMP RAC-D	468
19TH ST	1917+44	1969+80	(19TH NB) / 2068+20 (19TH SB)	60,150
19TH NB	1973+00	1985+60		9,712
19TH SB	2074+30	2078+00		2,138
PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB				
I-74	48+94	49+29	WB	420
I-74	49+13	49+36	EB	284
I-74	56+56	56+71	EB	184
I-74	57+90	58+05	WB	162
I-74	61+15	61+30	EB	183
I-74	62+26	62+47	WB	258
I-74	70+11	70+45	EB	437
I-74	70+20	70+69	WB	744
I-74	71+87	72+17	EB	445
I-74	72+10	72+46	WB	531
I-74	92+46	93+52	WB	1,082
I-74	93+62	94+58	EB	982
I-74	97+10	97+88	EB	764
I-74	96+26	97+04	WB	1,158
RAMP 7TH-A	627+03	627+37		274
RAMP 7TH-A	631+13	631+28		109
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12				44
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24				4,977
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.24				27
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06				169
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24				253
CONCRETE GUTTER, TYPE A (SPECIAL)				178
CONCRETE CURB, TYPE B				5
CONCRETE CURB, TYPE B				3
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24				41
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06				117
COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24				226
			TOTAL	394,692

42300300

PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH

ALIGNMENT	STA	SQ YD
19TH ST	1930+42.31	72
19TH ST	1931+87.75	42
19TH ST	1933+38.00	25
12TH AVE	127+21.67	25
19TH NB	1963+46.51	67
19TH SB	2077+63.27	44
PARKING LOT ALONG RETAINING WALL RW-11		360
	TOTAL	635

42400200

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ FT
19TH ST	1913+42	45.58'	LT	1928+38	46.07'	LT	9,510.0
19TH ST	1928+72	45.89'	LT	1930+19	40.15'	LT	784.0
19TH ST	1930+65	38.70'	LT	1933+25	36.58'	LT	1,297.5
19TH ST	1933+51	36.58'	LT	1935+92	36.58'	LT	1,242.5
12TH AVE	124+10	28.00'	RT	127+14	27.93'	RT	1,882.5
12TH AVE	127+31	27.53'	RT	129+78	48.97'	RT	1,508.0
12TH AVE	129+96	65.59'	LT	132+16	28.58'	LT	1,315.5
19TH ST	1936+81	38.36'	RT	1950+00	38.58'	RT	6,581.0
19TH ST SB	2050+00	12.58'	RT	2056+16	11.74'	RT	3,114.5
19TH ST SB	2056+08	39.04'	LT	2056+15	29.27'	LT	65.0
19TH ST SB	2057+96	7.52'	RT	2060+50	10.05'	RT	1,407.0
19TH ST SB	2058+76	44.74'	LT	2061+29	32.77'	LT	3,483.0
19TH ST NB	1956+10	13.74'	LT	1956+22	13.41'	LT	93.5
19TH ST NB	1963+16	13.12'	LT	1963+30	13.43'	LT	66.0
19TH ST NB	1963+63	12.94'	LT	1963+75	13.35'	LT	48.0
AOTC WB	119+85	23.43'	LT	120+09	27.85'	LT	143.5
AOTC WB	120+19	12.31'	LT	120+44	9.13'	LT	145.0
AOTC WB	120+97	13.40'	LT	121+15	6.88'	LT	96.0
AOTC WB	125+25	12.40'	LT	125+38	7.80'	LT	76.0
AOTC WB	125+69	15.99'	LT	125+78	14.65'	LT	51.5
AOTC WB	125+96	42.88'	LT	126+11	38.07'	LT	114.5
AOTC EB	220+07	42.15'	RT	220+26	48.47'	RT	145.5
AOTC EB	220+34	20.76'	RT	220+52	14.16'	RT	120.0
AOTC EB	220+96	13.30'	RT	221+07	8.65'	RT	57.0
AOTC EB	225+25	8.53'	RT	225+44	13.15'	RT	110.0
AOTC EB	225+86	11.45'	RT	226+04	14.52'	RT	102.0
AOTC EB	226+31	29.86'	RT	226+50	26.33'	RT	98.5
						TOTAL	33,657.5

44201296

DEFORMED BARS - EXPANSION JOINT

ALIGNMENT	STA FROM			STA TO			EACH
EX 12TH AVE	123+00.00			128+30.50			30
AOTC WB (EB)	119+00	(219+00)		122+23	(222+38)		40
AOTC WB (EB)	123+83	(223+98)		126+75	(226+75)		40
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							

44201299

DOWEL BARS 1 1/2"

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
DOWEL BARS FOR PATCH LOCATIONS:							
	ASSUME 25 12' X 10' TYPE II PATCHES						500
	ASSUME 21 12' X 12' TYPE III PATCHES						420
	ASSUME 12 12' X 20' TYPE IV PATCHES						240
						TOTAL	1,160

42400800

DETECTABLE WARNINGS

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	SQ FT
11TH AVE	1100+41	23.53	LT	1100+46	23.49	LT	10.0
11TH AVE	1100+40	23.24	RT	1100+45	23.28	RT	10.0
19TH ST	1935+78	31.93'	RT	1935+86	29.30'	RT	13.5
19TH ST	1936+02	28.58'	LT	1936+08	30.58'	LT	13.0
12TH AVE	126+99	23.00'	RT	127+13	27.96'	RT	21.5
12TH AVE	127+31	27.53'	RT	127+41	22.58'	RT	16.5
12TH AVE	129+31	24.34'	LT	129+39	22.32'	LT	14.5
12TH AVE	129+62	26.40'	RT	129+73	31.17'	RT	22.0
NB 19TH ST	1956+10	6.58'	LT	1956+16	6.34'	LT	12.0
NB 19TH ST	1956+09	30.58'	RT	1956+15	30.82'	RT	12.0
NB 19TH ST	1963+24	13.28'	LT	1963+26	13.32'	LT	11.0
NB 19TH ST	1963+67	13.02'	LT	1963+69	13.06'	LT	9.5
SB 19TH ST	2056+08	32.96'	LT	2056+14	33.23'	LT	12.0
SB 19TH ST	2056+10	4.86'	RT	2056+16	4.58'	RT	12.0
AOTC WB	120+04	25.59'	LT	120+09	27.81'	LT	10.0
AOTC WB	120+19	12.31'	LT	120+24	14.56'	LT	11.0
AOTC WB	120+41	9.17'	LT	120+43	14.14'	LT	10.0
AOTC WB	120+98	8.39'	LT	121+00	13.37'	LT	10.0
AOTC WB	125+36	7.61'	LT	125+37	13.01'	LT	10.5
AOTC WB	125+69	15.99'	LT	125+73	12.33'	LT	10.0
AOTC WB	125+73	17.70'	LT	125+77	14.04'	LT	10.0
AOTC WB	125+97	36.12'	LT	126+03	34.86'	LT	10.0
AOTC EB	220+21	38.53'	RT	220+26	40.19'	RT	10.0
AOTC EB	220+34	20.76'	RT	220+39	22.62'	RT	10.0
AOTC EB	220+50	14.31'	RT	220+52	14.16'	RT	10.0
AOTC EB	220+97	13.33'	RT	220+99	13.37'	RT	9.5
AOTC EB	225+42	13.21'	RT	225+44	13.17'	RT	10.0
AOTC EB	225+87	16.66'	RT	225+89	16.99'	RT	10.0
AOTC EB	225+97	18.43'	RT	225+99	18.75'	RT	10.0
AOTC EB	226+32	29.86'	RT	226+37	28.24'	RT	10.5
						TOTAL	351.0

44200970

CLASS B PATCHES, TYPE II, 10 INCH

ALIGNMENT	STA			STA			SQ YD
EX 12TH AVE	123+00.00			128+30.50			60
AOTC WB (EB)	119+00	(219+00)		122+23	(222+38)		160
AOTC WB (EB)	123+83	(223+98)		126+75	(226+75)		110
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							
						TOTAL	330

44200974

CLASS B PATCHES, TYPE III, 10 INCH

ALIGNMENT	STA		LT/RT	STA		LT/RT	SQ YD
EX 12TH AVE	123+00.00			128+30.50			60
AOTC WB (EB	119+00	(219+00)		122+23	(222+38)		160
AOTC WB (EB	123+83	(223+98)		126+75	(226+75)		110
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							
						TOTAL	330

44200976

CLASS B PATCHES, TYPE IV, 10 INCH

ALIGNMENT	STA		LT/RT	STA		LT/RT	SQ YD
EX 12TH AVE	123+00.00			128+30.50			20
AOTC WB (EB	119+00	(219+00)		122+23	(222+38)		70
AOTC WB (EB	123+83	(223+98)		126+75	(226+75)		40
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							
						TOTAL	130

44201294

CLASS B PATCH - EXPANSION JOINT

ALIGNMENT	STA FROM			STA TO			FOOT
EX 12TH AVE	123+00.00			128+30.50			132
AOTC WB (EB)	119+00	(219+00)		122+23	(222+38)		
AOTC WB (EB)	123+83	(223+98)		126+75	(226+75)		
NOTE: EXACT LOCATIONS TO BE DETERMINED DURING RESURFACING AND APPROVED BY THE ENGINEER							

SCHQ-13

FILE NAME = ...\D2CONCD-ABC-sht-schedule34M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/6/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
PAVEMENT

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	76
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

63000001

STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	LF
I-74	49+91.55	62.00'	RT	54+57.33	62.00'	RT	456.2'
I-74	61+28.11	62.00'	RT	62+75.09	62.00'	RT	150.0'
I-74	72+05.93	77.00'	RT	81+00.00	74.00'	RT	880.0'
I-74	72+82.90	74.00'	LT	81+00.00	74.00'	LT	831.1'
I-74	81+00.00	74.00'	RT	82+25.77	74.00'	RT	121.7'
I-74	81+00.00	74.00'	LT	92+63.96	73.24'	LT	1,156.5'
I-74	93+21.68	70.95'	RT	94+56.76	69.03'	RT	137.5'
I-74	97+69.90	64.97'	RT	98+55.97	63.21'	RT	87.5'
I-74	128+61.60	62.00'	RT	129+99.10	62.00'	RT	137.5'
I-74	129+20.73	62.00'	LT	129+45.73	62.00'	LT	25.0'
I-74	130+51.90	62.00'	LT	135+33.15	62.00'	LT	482.0'
I-74	131+05.27	62.00'	RT	136+61.52	62.00'	RT	557.0'
I-74	136+78.90	62.00'	LT	138+16.40	62.00'	LT	137.5'
7TH-B	531+43.27	8.00'	RT	534+36.82	8.00'	RT	293.8'
7TH-B	535+89.90	8.00'	RT	540+70.92	11.00'	RT	482.3
RAC-C	720+00.00	12.00'	RT	722+85.38	6.00'	RT	290.8'
RAC-D	824.92.71	6.00'	LT	826+30.83	6.00'	LT	137.5'
							6,362.5

63100045

TRAFFIC BARRIER TERMINAL, TYPE 2

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
I-74	62+75.09	62.00'	RT	62+87.29	62.00'	RT	1
I-74	85+16.29	83.53'	RT	85+28.79	84.21'	RT	1
I-74	98+55.97	63.21'	RT	98+68.47	63.03'	RT	1
I-74	129+22.73	62.00'	LT	129+10.23	62.00'	LT	1
							-
						TOTAL	4

63100070

TRAFFIC BARRIER TERMINAL, TYPE 5

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
I-74	49+78.00	62.00'	RT	49+91.55	62.00'	RT	1
I-74	61+14.84	62.00'	RT	61+28.11	62.00'	RT	1
I-74	71+93.00	74.00'	RT	72+05.93	77.00'	RT	1
I-74	92+63.96	73.24'	LT	92+77.44	73.52'	LT	1
I-74	97+56.85	65.00'	RT	97+69.90	65.00'	RT	1
I-74	129+45.73	62.00'	LT	129+59.00	62.00'	LT	1
I-74	130+92.00	62.00'	RT	131+05.27	62.00'	RT	1
I-74	135+33.15	62.00'	LT	135+46.42	62.00'	LT	1
7TH-B	531+30.00	8.00'	RT	531+43.27	8.00'	RT	1
7TH-B	535+76.66	8.00'	RT	535+89.90	8.00'	RT	1
						TOTAL	10

63100085

TRAFFIC BARRIER TERMINAL, TYPE 6

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
I-74	54+57.33	62.00'	RT	54+95.00	62.00'	RT	1
I-74	72+46.00	74.00'	LT	72+82.90	74.00'	LT	1
I-74	94+56.76	69.03'	RT	94+93.02	68.86'	RT	1
I-74	129+99.10	62.00'	RT	130+36.00	62.00'	RT	1
I-74	130+15.00	62.00'	LT	130+51.90	62.00'	LT	1
I-74	136+42.00	62.00'	LT	136+78.90	62.00'	LT	1
I-74	136+61.52	62.00'	RT	136+98.42	62.00'	RT	1
7TH-B	530+17.98	8.00'	RT	530+55.00	8.00'	RT	1
7TH-B	534+36.82	8.00'	RT	534+73.83	8.00'	RT	1
7TH-B	540+70.92	11.00'	RT	541+07.83	11.60'	RT	1
RAC-D	824+55.59	8.50'	LT	824.92.71	8.00'	RT	1
						TOTAL	11

64300260

IMPACT ATTENUATORS (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

ALIGNMENT	STA		EACH
I-74 WB	62+90		1
I-74	153+90.00		1
			-
RAMP 7TH-B	522+95		1
			-
		TOTAL	3

63100167

TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
I-74	92+72.61	72.65'	RT	93+21.68	70.95'	RT	1
I-74	128+11.61	63.00'	RT	128+61.60	62.00'	RT	1
I-74	138+16.40	62.00'	LT	138+66.39	63.00'	LT	1
RAC-D	826+30.83	6.00'	LT	826+79.59	7.00'	LT	1
7TH-B	529+67.82	9.00'	RT	530+17.98	8.00'	RT	1
						TOTAL	5

63700275

CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74	102+75.00	0.00'	C	103+70.25	0.00'	C	96.00'
I-74	103+75.75	0.00'	C	106+90.25	0.00'	C	315.00'
I-74	106+95.75	0.00'	C	107+04.76	0.00'	C	10.00'
I-74	108+71.03	0.00'	C	108+76.00	0.00'	C	5.00'
I-74	108+86.00	0.00'	C	112+22.25	0.00'	C	337.00'
I-74	112+27.75	0.00'	C	119+20.00	0.00'	C	693.00'
I-74	119+30.00	0.00'	C	122+72.25	0.00'	C	343.00'
I-74	122+77.75	0.00'	C	126+22.25	0.00'	C	345.00'
I-74	126+27.75	0.00'	C	129+42.25	0.00'	C	315.00'
I-74	129+47.75	0.00'	C	129+54.22	0.00'	C	7.00'
I-74	130+15.15	0.00'	C	130+32.93	0.00'	C	18.00'
I-74	130+78.79	0.00'	C	132+62.25	0.00'	C	184.00'
I-74	132+67.75	0.00'	C	135+82.25	0.00'	C	315.00'
I-74	135+87.75	0.00'	C	136+22.62	0.00'	C	35.00'
I-74	137+27.85	0.00'	C	142+62.25	0.00'	C	535.00'
I-74	142+67.75	0.00'	C	146+22.25	0.00'	C	355.00'
I-74	146+27.75	0.00'	C	149+82.25	0.00'	C	355.00'
I-74	149+87.75	0.00'	C	153+42.25	0.00'	C	355.00'
I-74	153+47.75	0.00'	C	153+90.00	0.00'	C	43.00'
						TOTAL	4,661

63700900

CONCRETE BARRIER BASE

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74	49+14.00	0.00'	C	56+60.84	0.00'	C	727
I-74	62+34.91	0.00'	C	64+25.00	0.00'	C	190
I-74	49+03.54	0.00'	C	49+14.00	0.00'	C	10
I-74	56+60.84	0.00'	C	56+70.84	0.00'	C	10
I-74	62+24.84	0.00'	C	62+34.91	0.00'	C	10
I-74	93+60.00	0.00'	C	93+60.90	0.00'	C	1.00'
I-74	97+04.50	0.00'	C	97+05.00	0.00'	C	1.00'
I-74	97+15.00	0.00'	C	100+34.00	0.00'	C	319.00'
I-74	100+44.00	0.00'	C	102+75.00	0.00'	C	231.00'
I-74	102+75.00	0.00'	C	103+70.25	0.00'	C	96.00'
I-74	103+75.75	0.00'	C	106+90.25	0.00'	C	315.00'
I-74	106+95.75	0.00'	C	107+04.76	0.00'	C	10.00'
I-74	108+71.03	0.00'	C	108+76.00	0.00'	C	5.00'
I-74	108+86.00	0.00'	C	112+22.25	0.00'	C	337.00'
I-74	112+27.75	0.00'	C	119+20.00	0.00'	C	693.00'
I-74	119+30.00	0.00'	C	122+72.25	0.00'	C	343.00'
I-74	122+77.75	0.00'	C	126+22.25	0.00'	C	345.00'
I-74	126+27.75	0.00'	C	129+42.25	0.00'	C	315.00'
I-74	129+47.75	0.00'	C	129+54.22	0.00'	C	7.00'
I-74	130+15.15	0.00'	C	130+32.93	0.00'	C	18.00'
I-74	130+78.79	0.00'	C	132+62.25	0.00'	C	184.00'
I-74	132+67.75	0.00'	C	135+82.25	0.00'	C	315.00'
I-74	135+87.75	0.00'	C	136+22.62	0.00'	C	35.00'
I-74	137+27.85	0.00'	C	142+62.25	0.00'	C	535.00'
I-74	142+67.75	0.00'	C	146+22.25	0.00'	C	355.00'
I-74	146+27.75	0.00'	C	149+82.25	0.00'	C	355.00'
I-74	149+87.75	0.00'	C	153+42.25	0.00'	C	355.00'
I-74	153+47.75	0.00'	C	153+90.00	0.00'	C	43.00'
						TOTAL	6,160

X0326382

CONCRETE BARRIER BASE (SPECIAL)

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	FOOT
7TH-A	623+26.84	17.50'	RT	626+52.00	12.82'	RT	323
7TH-A	631+12.94	10.00'	RT	632+50.00	12.00'	RT	138
7TH-A	632+17.72	32.00'	LT	642+25.05	29.01'	LT	1,002
7TH-B	534+73.63	8.00'	RT	535+76.66	8.00'	RT	104

64401100

HIGH TENSION CABLE MEDIAN BARRIER

ALIGNMENT	STA FROM	LT/RT	OFFSET	STA TO	LT/RT	OFFSET	FOOT
I-74	154+42.47	RT	6.86'	157+60.00	RT	15.00'	317.00'
						TOTAL	317

64401300

HIGH TENSION CABLE MEDIAN BARRIER TERMINALS

ALIGNMENT	STA FROM	STA TO	LT/RT	OFFSET			EACH
I-74	154+42.06	154+69.56	RT	15.00'			1.00
I-74	157+50.11	157+77.61	RT	15.00'	(STAGE 1-1)		1.00
						TOTAL	2

X6370250

CONCRETE BARRIER, VARIABLE CROSS-SECTION 42" HEIGHT

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74	49+14.00	0'	C	56+60.84	0'	C	747
I-74	62+34.91	0'	C	64+35.00	0'	C	200
I-74	91+49.15	000+00.00	C	93+60.90	000+00.00	C	212
I-74	97+04.50	000+00.00	C	102+70.00	000+00.00	C	566
						TOTAL	1,725

X6370279

CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
7TH-A	623+26.84	17.50'	RT	626+52.00	12.82'	RT	323
7TH-A	631+12.94	10.00'	RT	632+50.00	12.00'	RT	138
7TH-A	632+17.72	32.00'	LT	642+25.05	29.01'	LT	1,002
7TH-B	534+73.63	8.00'	RT	535+76.66	8.00'	RT	104
						TOTAL	1,567

X6370700

CONCRETE BARRIER TRANSITION (SPECIAL)

ALIGNMENT	STA FROM	STA TO		FOOT
I-74	49+03.54	49+14.00		10
I-74	56+60.84	56+70.84		10
I-74	62+24.84	62+34.91		10
I-74	93+60.90	94+05.55		45
I-74	96+58.03	97+04.50		47
I-74	107+04.76	107+34.25		30
I-74	108+44.62	108+71.03		27
I-74	129+54.22	129+69.30		16
I-74	129+80.71	130+15.15		35
I-74	130+32.93	130+52.30		20
I-74	130+63.71	130+78.79		16
I-74	136+22.62	136+50.03		28
I-74	137+00.44	137+27.85		28
				-
			TOTAL	322

72501000

TERMINAL MARKER - DIRECT APPLIED

ALIGNMENT	STA	OFFSET	LT/RT		EACH
I-74	92+65.55	72.65'	RT		1
I-74	128+04.35	63.00'	RT		1
I-74	138+24.65	62.00'	LT		1
7TH-B	529+67.82	9.00'	RT		1
RAC-D	826+37.11	6.00'	LT		1
				TOTAL	5

64301090

ATTENUATOR BASE

ALIGNMENT	STA		SQ YD
I-74 WB	62+90		14
I-74	153+90.00		14
			-
RAMP 7TH-B	522+95		14

SCHQ-14

FILE NAME = ...\D2CONCD-ABC-sht-schedule36M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
		DRAWN - JRM	REVISED -	
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -	
	PLOT DATE = 5/6/2017	DATE - 3/23/2017	REVISED -	

SCHEDULE OF QUANTITIES
BARRIER

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	77
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

44213200

SAW CUTS

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	FOOT
SAW CUTS FOR PATCH LOCATIONS:							
	ASSUME 25 12' X 10' TYPE II PATCHES						1,100
	ASSUME 21 12' X 12' TYPE III PATCHES						1,008
	ASSUME 5 12' X 20' TYPE IV PATCHES						320
						TOTAL	2,428

44213204

TIE BARS 3/4"

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH
TIE BARS FOR PATCH LOCATIONS:							
	ASSUME 5 12' X 20' TYPE IV PATCHES						50
						TOTAL	50

48100500

AGGREGATE SHOULDERS, TYPE A 6"

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	SQ YD
I-74	62+87.00	62.00'	RT	64+52.95	62.00'	RT	36
I-74	71+72.00	75.58'	RT	77+19.95	77.00'	RT	419
I-74	84+95.43	76.58'	LT	92+56.24	74.02'	LT	249
I-74	88+28.44	74.00'	RT	92+44.83	71.96'	RT	93
I-74	97+75.73	62.00'	LT	123+37.30	62.00'	LT	561
I-74	98+74.36	62.95'	RT	117+34.98	62.00'	RT	417
I-74	122+49.84	65.28'	LT	127+83.36	62.00'	RT	118
I-74	126+77.13	67.02'	LT	129+02.23	62.00'	LT	30
I-74	137+75.00	62.00'	RT	155+00.00	57.00'	RT	379
I-74	138+94.64	62.00'	LT	155+00.00	53.80'	LT	361
RAMP 7TH-B	524+00.00	6.00'	RT	529+43.49	6.00'	RT	120
RAMP 7TH-B	531+48.00	23.58'	LT	535+34.85	20.00'	LT	79
RAMP 7TH-B	535+76.66	11.00'	RT	537+48.43	11.00'	RT	123
RAMP 7TH-A	641+00.00	22.00'	RT	641+68.34	22.00'	RT	15
RAC-A	1016+79.36	20.00'	RT	1023+40.19	20.00'	RT	148
RAC-A	1019+05.41	6.00'	LT	1026+76.53	6.00'	LT	171
RAC-B	920+07.48	20.00'	LT	928+52.29	20.00'	LT	187
RAC-B	922+86.22	6.00'	RT	933+68.43	6.00'	RT	241
RAC-C	723+11.32	6.00'	RT	739+72.71	6.00'	RT	369
RAC-C	725+99.82	20.00'	LT	735+30.00	20.00'	LT	206
RAC-D	827+09.81	6.00'	LT	832+36.07	6.00'	LT	116
RAC-D	826+61.72	20.00'	RT	833+65.79	20.00'	RT	156
NB 19TH ST	1974+35.00	42.00'	RT	1978+51.00	42.00'	RT	92
19TH ST	1968+19.70	45.85'	RT	1969+95.44	31.48'	RT	39
19TH ST	1978+51.13	42.00'	RT	1979+94.46	45.42'	RT	32
						TOTAL	4,757

48203009

HOT-MIX ASPHALT SHOULDERS, 3"

ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	SQ YD
I-74	49+78.00	62.00'	RT	54+95.00	62.00'	RT	178
I-74	61+14.84	62.00'	RT	62+87.29	62.00'	RT	62
I-74	72+46.00	74.00'	LT	81+00.00	74.00'	LT	306
I-74	77+54.77	74.00'	RT	81+00.00	74.00'	RT	117
I-74	81+00.00	77.00'	RT	85+49.06	77.00'	RT	147
I-74	81+00.00	77.00'	LT	84+95.43	79.00'	LT	143
I-74	92+32.71	72.13'	RT	94+93.06	71.42'	RT	89
I-74	97+56.88	67.64'	RT	98+86.44	62.77'	RT	40
I-74	127+71.36	62.00'	RT	130+36.00	65.00'	RT	91
I-74	128+90.23	62.00'	LT	129+59.00	65.00'	LT	20
I-74	130+92.00	65.00'	RT	136+98.42	65.00'	RT	203
I-74	130+15.00	65.00'	LT	135+46.42	65.00'	RT	178
I-74	136+42.00	65.00'	LT	139+06.64	62.00'	LT	91
RAMP 7TH-B	529+43.49	6.00'	RT	530+55.00	8.00'	RT	45
RAMP 7TH-B	531+30.00	8.00'	RT	534+73.63	8.00'	RT	121
RAMP 7TH-B	535+76.66	8.00'	RT	541+07.63	11.60'	RT	187
RAC-D	824+99.00	9.00'	LT	827+21.53	6.00'	LT	77
						TOTAL	2,095

X6062700

CONCRETE GUTTER, TYPE A (SPECIAL)

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74 ML EB	71+93.00	75.50'	RT	77+30.00	75.50'	RT	535.0
						TOTAL	535.0

60602800

CONCRETE GUTTER, TYPE B

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
7TH-B	530+79.11	10.50'	RT	531+05.89	10.50'	RT	27.0
7TH-A	632+74.00	14.67'	RT	640+75.29	26.67'	RT	805.5
19TH STREET	1924+75.00	47.66'	LT	1927+94.38	47.66	LT	316.5
						TOTAL	1,149.0

60603800

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
19TH ST NB	1954+25.07	25.00'	RT	1955+47.65	28.78'	RT	124.0
19TH ST SB	2054+26.24	25.00'	LT	2055+48.70	35.90'	LT	126.0
						TOTAL	250.0

60605000

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
19TH ST	1915+11.98	35.00'	RT	1935+52.33	76.63'	RT	2,064.0
19TH ST	1917+43.85	35.00'	LT	1928+42.20	113.14'	LT	1,159.5
19TH ST	1917+58.84	0.99'	LT	1925+56.76	7.00'	LT	797.0
19TH ST	1917+58.84	7.00'	RT	1925+56.76	1.00'	RT	799.5
19TH ST	1928+67.79	113.22'	LT	1936+48.43	51.17'	LT	868.5
19TH ST	1928+74.96	5.77'	LT	1930+63.84	2.14'	LT	189.5
19TH ST	1928+74.96	5.77'	LT	1930+63.84	5.85'	RT	192.0
19TH ST	1936+43.39	46.42'	RT	1950+00.18	28.00'	RT	1,358.0
19TH ST	1937+43.36	39.18'	LT	1950+00.18	28.00'	LT	1,258.0
19TH ST NB	1950+00.00	2.00'	LT	1955+60.98	35.88'	LT	584.5
19TH ST NB	1955+96.92	33.79'	LT	1969+80.00	2.00'	LT	1,404.5
19TH ST NB	1955+98.59	31.60'	RT	1966+87.63	28.00'	RT	1,105.5
19TH ST NB	1973+00.00	40.00'	RT	1974+08.00	40.00'	RT	108.0
19TH ST NB	1973+00.00	12.00'	LT	1985+60.00	2.00'	LT	1,260.0
19TH ST NB	1982+20.18	28.00'	RT	1985+60.00	28.00'	RT	341.5
19TH ST SB	2050+00.00	2.00'	RT	2063+43.26	2.00'	RT	1,344.5
19TH ST SB	2055+98.72	35.31'	LT	2068+20.00	26.00'	LT	1,212.5
19TH ST SB	2066+25.00	14.47'	RT	2068+78.00	13.72'	RT	253.0
19TH ST SB	2074+30.00	12.58'	RT	2078+00.00	13.44'	RT	371.0
19TH ST SB	2077+32.71	17.18'	LT	2078+00.00	16.56'	LT	67.0
12TH AVE	127+40.60	20.00'	RT	128+55.00	21.02'	RT	113.0
12TH AVE	128+34.04	20.00'	LT	128+88.34	31.60'	LT	60.0
12TH AVE	130+69.49	76.33'	RT	132+05.00	22.00'	RT	166.5
12TH AVE	130+41.69	22.00'	LT	132+05.00	22.00'	LT	157.5
7TH-B	522+94.93	8.00'	RT	524+00.00	8.00'	RT	105.0
RAC-C	735+30.00	20.00'	LT	736+71.72	20.00'	LT	142.0'
						TOTAL	17,482.0

60607400

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-9.24

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
12TH AVE	128+55.00	21.02'	RT	129+54.52	22.60'	RT	93.5
						TOTAL	93.5

60608250

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.06

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74 WB	64+35.00	2.00'	LT	70+56.74	2.00'	LT	623.0
I-74 WB	71+99.31	2.00'	LT	81+00.00	1.33'	LT	901.0
I-74 WB	81+00.00	1.83'	LT	91+49.15	1.83'	LT	1050.0
						TOTAL	2,574.0

60608582

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74 WB	84+95.42	74.00'	LT	92+60.70	71.32'	LT	766.0
I-74 WB	95+84.92	81.11'	LT	96+07.19	81.11'	LT	23.0
						TOTAL	789.0

60610400

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
7TH-A	632+50.00	12.00'	RT	641+00.00	24.00'	RT	854.0
						TOTAL	854.0

X6060714

CONCRETE MEDIAN

ALIGNMENT	STA FROM	STA TO
12TH AVE	129+16.52	129+52.00
12TH AVE	130+35.50	130+72.74

60600605

CONCRETE CURB, TYPE B

ALIGNMENT	STA FROM	STA TO	FOOT
I-74 (EB)	54+79.68	54+95.00	15.0
I-74 (WB)	72+46.00	72+61.00	15.0
I-74 (EB)	130+20.98	130+36.00	15.0
I-74 (WB)	130+15.00	130+30.02	15.0
I-74 (WB)	136+42.00	136+57.00	15.0
I-74 (EB)	136+83.40	136+98.42	15.0
7TH-B	530+39.95	530+55.00	15.0
7TH-B	534+58.67	534+73.63	15.0
7TH-B	540+92.70	541+07.63	15.0
		TOTAL	135.0

60618300

CONCRETE MEDIAN SURFACE, 4 INCH

ALIGNMENT	STA FROM	STA TO	SQ FT
19TH ST	1917+58.84	1925+56.76	9,757
19TH ST	1928+79.84	1930+63.84	1,633
19TH ST NB	1954+25.07	1955+48.65	1,258
		TOTAL	12,648

60619600

CONCRETE MEDIAN, TYPE SB-6.12

ALIGNMENT	STA FROM	STA TO	SQ FT
19TH NB	1951+34.01	1954+25.07	2,264
		TOTAL	2,264

60620000

CONCRETE MEDIAN, TYPE SB-6.24

ALIGNMENT	STA FROM	STA TO	SQ FT
19TH ST	1916+71.35	1917+58.84	788
19TH ST	1925+56.76	1926+59.87	901
19TH ST	1930+63.84	1931+26.00	681

60624600

CORRUGATED MEDIAN

ALIGNMENT	STA FROM	STA TO	SQ FT
19TH ST	1931+26.00	1935+74.23	2,220
19TH ST	1937+06.68	1950+00.18	5,173
19TH ST NB	1950+00.00	1951+34.01	597
		TOTAL	7,990

X6061100

CONCRETE MEDIAN, TYPE SB (SPECIAL)

ALIGNMENT	STA FROM	STA TO	SQ FT
19TH ST	1913+20.25	1916+71.35	2,098
19TH ST	1926+59.87	1928+07.90	884
		TOTAL	2,982

Z0028415

GEOTECHNICAL REINFORCEMENT

ALIGNMENT	STA FROM	STA TO	LOCATION	SQ YD
I-74	49+03.54	56+67.84	EB ML PAVEMENT & SHOULDER	5,296
I-74	49+03.54	58+01.84	WB ML PAVEMENT & SHOULDER	6,343
I-74	61+17.84	70+54.61	EB ML PAVT & SHLDR/RAMP 7TH-B MERGE	6,929
I-74	62+27.84	70+54.61	WB ML PAVT & SHLDR/RAMP 7TH-A DIVERGE	6,971
I-74	72+01.67	81+00.00	EB ML PAVEMENT & SHOULDER	7,736
I-74	72+01.67	81+00.00	WB ML PAVEMENT & SHOULDER	7,505
I-74	81+00.00	93+76.97	EB ML PAVEMENT & SHOULDER	11,099
I-74	96+94.50	155+00.00	WB ML PAVEMENT & SHOULDER	39,476
I-74	81+00.00	93+70.91	EB ML PAVEMENT & SHOULDER	10,009
I-74	96+89.39	155+00.00	WB ML PAVEMENT & SHOULDER	39,092
RAMP 7TH-A	625+10.00	627+37.00	PAVEMENT, SHOULDER & GORE	1,176
RAMP 7TH-A	631+13.01	642+37.34	PAVEMENT & SHOULDER	6,160
RAMP 7TH-B	522+94.93	537+99.01	PAVEMENT, SHOULDER & GORE	5,340
RAC-A	1015+36.50	1025+77.79	RAMP RAC-A	3,146
RAC-B	920+07.48	931+71.93	RAMP RAC-B	3,372
RAC-C	724+09.79	739+72.71	RAMP RAC-C	5,053
RAC-D	824+87.85	833+18.36	RAMP RAC-D	2,880
19TH ST	1917+43.85	1969+80.00	(19TH NB) / 2068+20 (19TH SB) PAVEMENT	38,012
19TH ST NB	1973+00.00	1985+60.00	PAVEMENT	5,645
19TH ST SB	1967+70.26	1970+03.40	RAMP RAC-D	419
19TH ST SB	2074+30.00	2078+00.00	PAVEMENT	1,276
AOTC WB (EB	119+00	(219+00)	122+23 (222+38) PATCHES	40
AOTC WB (EB	123+83	(223+98)	126+75 (226+75) PATCHES	26
EX 12TH AVE	123+00.00	128+30.50	PATCHES	27
			TOTAL	213,028

#2000318

HOT-MIX ASPHALT CURB (SPECIAL)

ALIGNMENT	STA FROM	STA TO	FOOT
NB 19TH ST	1964+34.00	1964+67.00	33.0
		(TEMPORARY DRIVEWAY)	
		TOTAL	33.0

SCHQ-15

FILE NAME = ...\D2CONCD-ABC-sht-schedule35M.dgn $MODELNAME$	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES MISCELLANEOUS PAVEMENT	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRM	REVISED -			74	(8)-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	78
	PLOT SCALE =	CHECKED - JJT	REVISED -					CONTRACT NO. 64E26		
	PLOT DATE = 5/6/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

64200116					
SHOULDER RUMBLE STRIPS, 16 INCH					
ALIGNMENT	STA FROM	STA TO	LT/RT	OFFSET	FOOT
I-74 ML (WB)	48+88.35	58+04.84	LT	51.67	927
I-74 ML (WB)	48+99.87	58+04.84	LT	12.33	908
I-74 ML (EB)	49+07.25	56+70.84	RT	12.33	761
I-74 ML (EB)	49+19.28	56+70.84	RT	51.67	741
I-74 ML (EB)	61+14.84	70+53.69	RT	12.33	943
I-74 ML (EB)	61+14.84	64+52.95	RT	51.67	344
I-74 ML (WB)	62+25.20	70+61.54	LT	12.33	832
I-74 ML (WB)	62+26.35	62+90.30	LT	51.67	63
I-74 ML (EB)	71+78.39	81+00.00	RT	63.67	910
I-74 ML (EB)	71+94.74	81+00.00	RT	12.33	903
I-74 ML (WB)	72+02.60	81+00.00	LT	12.33	900
I-74 ML (WB)	72+18.95	81+00.00	LT	63.67	893
I-74 ML (EB)	81+00.00	82+49.40	RT	63.00'	150
I-74 ML (EB)	81+00.00	93+90.97	RT	13.00'	1,291
I-74 ML (WB)	81+00.00	93+53.42	LT	13.00'	1,254
I-74 ML (WB)	81+00.00	90+99.53	LT	63.00'	1,000
I-74 ML (EB)	88+28.44	94+57.98	RT	63.00'	630
I-74 ML (EB)	97+60.88	117+34.97	RT	51.00'	1,975
I-74 ML (EB)	97+09.38	153+90.00	RT	13.00'	5,681
I-74 ML (WB)	96+77.85	153+90.00	LT	13.00'	5,713
I-74 ML (WB)	97+75.83	123+37.30	LT	51.00'	2,562
I-74 ML (EB)	129+98.00	153+90.00	RT	51.00'	2,392
I-74 ML (WB)	129+78.00	153+90.00	LT	51.00'	2,412
					-
				TOTAL	34,185

66400105							
CHAIN LINK FENCE 4'							
ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	FOOT
I-74	49+78.00	69.0'	RT	54+95.00	69.0'	RT	518
I-74	71+93.00	84.0'	RT	79+33.34	96.5'	RT	791
I-74	72+46.00	89.0'	LT	81+00.00	89.0'	LT	884
I-74	81+00.00	89.00'	LT	92+56.26	74.00'	LT	1,152
I-74	82+63.45	158.65'	RT	85+19.82	130.69'	RT	260
I-74	97+74.54	79.35'	RT	99+48.61	141.71'	RT	210
I-74	100+70.92	144.03'	RT	101+09.41	146.61'	RT	40
I-74	101+03.22	103.59'	LT	107+12.70	223.16'	LT	610
I-74	108+94.60	229.85'	RT	114+12.68	124.03'	RT	540
I-74	109+73.69	213.38'	LT	119+15.26	119.52'	LT	930
I-74	129+41.36	145.81'	RT	130+68.86	106.87'	RT	170
I-74	131+07.92	105.48'	RT	133+36.07	157.04'	RT	240
I-74	120+43.96	163.02'	LT	122+56.57	151.73'	LT	210
I-74	154+63.21	113.25'	LT	155+54.67	131.57'	LT	97
RAMP 7TH-A	623+26.84	26.5'	RT	626+52.05	21.8'	RT	336
RAMP 7TH-A	639+27.10	42.4'	RT	639+28.56	25.7'	RT	17
RAMP 7TH-A	641+00.00	25.7'	RT	642+75.93	53.5'	RT	200
RAMP 7TH-B	523+72.88	18.2'	RT	532+54.76	86.5'	RT	969
19TH ST	1923+66.22	97.2'	LT	1924+75.00	46.7'	LT	118
19TH ST	1924+10.91	74.5'	LT	1924+19.16	89.7'	LT	17
						TOTAL	8,309

66600105							
FURNISHING AND ERECTING RIGHT OF WAY MARKERS							
ALIGNMENT	STA	OFFSET	LT/RT				EACH
I-74	67+41	102'	LT				1
I-74	69+20	96'	LT				1
RAMP 7TH-B	523+47	17'	RT				1
RAMP 7TH-B	524+24	37'	RT				1
RAMP 7TH-B	525+76	37'	RT				1
RAMP 7TH-B	527+45	94'	RT				1
RAMP 7TH-B	530+91	34'	RT				1
RAMP 7TH-B	531+57	38'	RT				1
RAMP 7TH-B	532+53	96'	RT				1
RAMP 7TH-B	532+59	88'	RT				1
						TOTAL	10

66700305							
PERMANENT SURVEY MARKERS, TYPE II							
LOCATION							EACH
I-74 ML STA. 50+00, 66' RT							1
I-74 ML STA. 81+00, 78' RT							1
19TH STREET NB STA. 1970+25, 35' RT							1
I-74 ML STA. 154+00, 70' RT							1
						TOTAL	4

67000400							
ENGINEER'S FIELD OFFICE, TYPE A							
							CAL MO
10/1/2017	TO	11/30/2020					39
POST STAGE 3 PUNCHLIST							6
						TOTAL	45

78200005								
GUARDRAIL REFLECTORS, TYPE A								
ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH	COLOR
I-74	49+78.00	62.00'	RT	54+95.00	62.00'	RT	4	CRYSTAL
I-74	61+14.84	62.00'	RT	62+87.29	62.00'	RT	4	CRYSTAL
I-74	71+93.00	74.00'	RT	81+00.00	74.00'	RT	6	CRYSTAL
I-74	72+46.00	74.00'	LT	81+00.00	74.00'	LT	6	CRYSTAL
I-74	81+00.00	74.00'	RT	82+25.77	74.00'	RT	1	CRYSTAL
I-74	81+00.00	74.00'	LT	82+25.77	74.00'	LT	1	CRYSTAL
I-74	82+25.77	74.00'	RT	85+28.77	84.21'	RT	2	CRYSTAL
I-74	82+25.77	74.00'	LT	86+63.98	74.00'	LT	1	CRYSTAL
I-74	86+63.98	74.00'	LT	92+77.48	73.52'	LT	4	CRYSTAL
I-74	92+66.47	72.65'	RT	94+93.02	68.86'	RT	4	CRYSTAL
I-74	97+56.85	65.08'	RT	98+68.27	63.03'	RT	4	CRYSTAL
I-74	129+10.23	62.00'	LT	129+61.00	62.00'	LT	4	CRYSTAL
I-74	128+04.36	63.00'	RT	130+35.00	62.00'	RT	4	CRYSTAL
I-74	130+17.00	62.00'	LT	135+48.42	62.00'	LT	4	CRYSTAL
I-74	130+91.00	62.00'	RT	136+97.42	62.00'	RT	4	CRYSTAL
I-74	136+44.00	62.00'	LT	138+74.64	62.00'	LT	4	CRYSTAL
7TH-B	529+67.82	9.00'	RT	530+55.00	8.00'	RT	4	CRYSTAL
7TH-B	531+30.00	8.00'	RT	534+73.63	8.00'	RT	4	CRYSTAL
7TH-B	535+76.66	8.00'	RT	541+31.49	12.00'	RT	4	CRYSTAL
RAC-D	824+55.59	8.50'	LT	826+87.34	7.00'	LT	4	CRYSTAL
						TOTAL	73	

78200010								
BARRIER WALL REFLECTORS, TYPE B								
ALIGNMENT	STA	OFFSET	LT/RT	STA	OFFSET	LT/RT	EACH	COLOR
I-74	49+03.54	1.5'	RT	55+23.55	1.5'	RT	5	AMBER
I-74	49+03.54	1.5'	LT	55+23.55	1.5'	LT	5	BI-DIRECTIONAL
I-74	55+23.55	1.5'	RT	61+53.55	1.5'	RT	2	AMBER
I-74	55+23.55	1.5'	LT	61+53.55	1.5'	LT	2	BI-DIRECTIONAL
I-74	61+53.55	1.5'	RT	70+48.03	1.5'	RT	6	AMBER
I-74	61+53.55	1.5'	LT	70+48.03	1.5'	LT	6	BI-DIRECTIONAL
I-74	70+48.03	1.5'	RT	76+63.77	1.5'	RT	2	AMBER
I-74	70+48.03	1.5'	LT	76+63.77	1.5'	LT	2	BI-DIRECTIONAL
I-74	76+63.77	1.5'	RT	81+00.00	1.5'	RT	3	AMBER
I-74	76+63.77	1.5'	LT	81+00.00	1.5'	LT	3	BI-DIRECTIONAL
I-74	49+22.62	62.0'	RT	49+78.00	62.0'	RT	1	CRYSTAL
I-74	54+95.00	62.0'	RT	61+14.84	62.0'	RT	2	CRYSTAL
I-74	70+34.38	74.3'	RT	71+93.00	74.0'	RT	1	CRYSTAL
I-74	48+85.50	62.0'	LT	62+90.30	61.7'	LT	10	CRYSTAL
I-74	70+79.33	74.1'	LT	72+46.00	74.0'	LT	1	CRYSTAL
I-74	81+00.00	1.5'	LT/RT	82+25.77	1.5'	LT/RT	2	AMBER
I-74	82+25.77	1.5'	LT/RT	86+63.98	1.5'	LT/RT	2	AMBER
I-74	86+63.98	1.5'	LT/RT	91+00.00	1.5'	LT/RT	6	AMBER
I-74	91+00.00	1.5'	LT/RT/TOP	102+54.51	1.5'	LT/RT/TOP	21	AMBER
I-74	102+54.51	1.5'	LT/RT/TOP	153+90.00	1.5'	LT/RT/TOP	39	AMBER
RAMP 7TH-A	620+00.00	12.1'	RT	641+00.00	24.7'	RT	29	AMBER
RAMP 7TH-A	627+65.28	32.0'	LT	633+25.55	32.0'	LT	7	AMBER
RAMP 7TH-B	522+95.00	22.0'	LT	531+48.06	22.0'	LT	10	AMBER
RAMP 7TH-B	530+55.00	8.0'	RT	531+30.00	8.0'	RT	1	CRYSTAL
RAMP 7TH-B	534+73.63	8.0'	RT	535+76.66	8.0'	RT	2	CRYSTAL
						TOTAL:	170	

Z0009900							
CHAIN LINK FABRIC, TYPE 1, 4'-0"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
I-74	79+33.34	96.5'	RT	81+10.41	184.6'	RT	198
						TOTAL	198

SCHQ-16

FILE NAME =	USER NAME = jtardy	DESIGNED - JRM	REVISED -
...\D2CONCD-ABC-sht-schedule37M.dgn		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
MISCELLANEOUS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	79
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

50105220

PIPE CULVERT REMOVAL

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	109+56.18	5.7	RT	109+56.18	71.56	RT	54
PR I-74	121+05.85	78.69	RT	121+06.13	12.18	RT	67
PR I-74	146+04.91	8.99	RT	146+05.37	66.95	RT	58
						TOTAL	179

55100300

STORM SEWER REMOVAL 8"

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR 19TH ST	1921+59.65	26.04	LT	1921+89.26	24.49	LT	10
PR 19TH ST	1919+74.91	25.98	LT	1919+95.24	26.92	LT	20
PR I-74	58+90.81	24.17	LT	58+90.63	35.89	LT	12
PR I-74	59+77.37	83.69	LT	59+78.13	69.85	LT	14
PR I-74	59+87.75	40.98	LT	59+74.70	49.14	LT	15
PR I-74	59+28.53	87.45	RT	59+20.63	91.40	RT	9
PR 7TH-B	530+98.00	34.00	LT	531+06.23	33.26	RT	8
PR 7TH-B	532+04.74	78.61	LT	531+96.14	80.78	LT	9
PR 7TH-B	532+00.37	36.06	LT	531+93.13	37.90	LT	8
PR I-74	62+41.59	56.78	LT	62+50.87	57.30	LT	9
						TOTAL	114

55100500

STORM SEWER REMOVAL 12"

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	49+12.32	6.68	LT	49+11.19	28.44	LT	22
PR I-74	53+07.63	95.94	LT	52+78.50	85.24	LT	32
PR I-74	53+09.93	116.32	LT	53+07.63	95.94	LT	21
PR I-74	53+01.66	54.57	LT	52+78.50	85.24	LT	39
PR I-74	50+77.39	32.50	LT	50+78.59	4.47	RT	37
PR I-74	50+78.59	4.47	RT	50+78.42	8.68	RT	4
PR I-74	50+78.42	8.68	RT	50+79.58	44.40	RT	36
PR I-74	53+94.58	49.33	RT	53+95.90	11.91	RT	37
PR I-74	53+95.90	11.91	RT	54+20.86	6.00	RT	26
PR I-74	54+19.47	32.55	LT	54+20.86	6.00	RT	39
PR I-74	54+20.86	6.00	RT	54+45.61	8.89	RT	25
PR I-74	54+45.61	8.89	RT	56+78.50	0.85	RT	233
PR I-74	56+78.50	0.85	RT	56+97.60	25.79	RT	31
PR I-74	56+97.60	25.79	RT	57+15.07	82.63	RT	59
PR I-74	57+84.45	114.70	LT	57+85.53	107.37	LT	7
PR I-74	58+50.08	39.22	LT	58+90.81	24.17	LT	43
PR I-74	58+90.81	24.17	LT	59+44.26	7.90	LT	56
PR I-74	60+52.59	82.91	LT	59+78.13	69.85	LT	76
PR I-74	59+78.13	69.85	LT	59+74.70	49.14	LT	21
PR I-74	59+74.70	49.14	LT	59+44.26	7.90	LT	51
PR I-74	59+44.26	7.90	LT	59+64.77	28.49	RT	42
PR I-74	59+64.77	28.49	RT	59+67.89	49.36	RT	21
PR I 74	59+28.53	87.45	RT	59+77.06	63.48	RT	54
PR I-74	62+71.17	20.53	LT	62+75.19	73.19	LT	53
PR I-74	62+50.87	57.30	LT	62+75.19	73.19	LT	29
PR I-74	63+22.14	65.78	LT	62+75.19	73.19	LT	46
PR I-74	61+90.78	151.35	RT	62+26.62	93.44	RT	70
PR I-74	62+26.62	93.44	RT	62+26.04	88.60	RT	5
PR I-74	64+49.08	93.48	LT	64+26.92	174.10	LT	83
PR I-74	67+49.18	77.53	LT	67+21.54	98.27	LT	34
PR I-74	71+95.71	8.64	RT	71+93.60	66.17	RT	58
PR I-74	71+93.60	66.17	RT	71+96.89	121.60	RT	53
PR I-74	72+40.27	30.34	LT	72+43.69	98.32	LT	68
PR I-74	72+45.80	80.89	LT	71+88.27	139.99	LT	82
PR I-74	72+45.80	80.89	LT	72+44.65	185.30	LT	104
PR I-74	82+84.30	20.44	LT	83+00.49	72.32	LT	54
PR I-74	117+57.20	52.71	RT	117+65.26	107.63	RT	56
PR I-74	123+52.56	99.20	LT	123+53.83	57.65	LT	42
PR 7TH-B	526+66.50	120.99	LT	526+42.51	58.01	LT	68
PR 7TH-B	530+98.00	34.00	LT	530+83.67	27.11	LT	16
PR 7TH-B	532+00.37	36.06	LT	531+95.37	4.92	RT	41
PR 7TH-B	532+04.74	78.61	LT	532+00.37	36.06	LT	43
PR 19TH ST	1919+94.44	80.77	RT	1919+92.99	46.82	RT	34
PR 19TH ST	1919+95.24	26.92	LT	1920+41.00	28.38	LT	45
PR 19TH ST	1920+41.00	28.38	LT	1920+42.09	23.49	RT	52
PR 19TH ST	1921+59.65	26.04	LT	1920+41.00	28.38	LT	118
PR 19TH ST	1925+12.31	46.33	RT	1925+59.08	42.32	RT	48
PR 19TH ST	1925+59.08	42.32	RT	1925+39.30	24.70	RT	23
PR 19TH ST	1925+71.52	59.51	RT	1925+59.08	42.32	RT	21
PR 19TH ST	1936+34.50	59.75	RT	1936+57.50	33.92	RT	32
PR 11TH AVE	1100+35.33	35.49	RT	1100+24.26	34.47	RT	11
PR 11TH AVE	1100+69.77	12.94	LT	1100+71.61	12.97	RT	26
PR 11TH AVE	1100+71.61	12.97	RT	1100+35.33	35.49	RT	43
TEMPORARY PIPES							
PR 19TH ST	1921+10.00	55.53	RT	1920+70.00	43.85	RT	42
PR 19TH ST	1921+60.00	45.59	LT	1921+59.65	26.04	LT	20
PR 19TH ST	1925+40.00	45.58	LT	1925+40.00	33.00	LT	13
PR 19TH ST	1925+80.00	49.64	RT	1925+59.09	42.32	RT	22
						TOTAL	2567

55100700

STORM SEWER REMOVAL 15"

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	50+79.58	44.40	RT	50+98.25	93.66	RT	53
PR I-74	50+97.74	103.30	RT	50+94.88	144.93	RT	42
PR I-74	50+98.25	93.66	RT	50+97.74	103.30	RT	10
PR I-74	136+06.48	0.12	RT	136+06.26	16.05	RT	16
						TOTAL	121

55100900

STORM SEWER REMOVAL 18"

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	49+11.19	28.44	LT	48+86.77	18.09	LT	27
PR I-74	50+94.88	144.93	RT	49+37.87	161.17	RT	150
PR I-74	52+78.50	85.24	LT	52+46.29	62.38	LT	40
PR I-74	52+46.29	62.38	LT	49+11.20	28.44	LT	343
PR I-74	62+75.19	73.19	LT	62+78.29	137.81	LT	65
19TH ST	1923+93.91	105.95	LT	1925+36.70	32.30	LT	158
19TH ST	1925+36.70	32.30	LT	1925+39.56	3.16	RT	36
19TH ST	2065+86.78	1.87	RT	2065+86.58	74.75	LT	77
						TOTAL	896

X5509900

ABANDON AND FILL EXISTING STORM SEWER

ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	67+21.54	98.27	LT	65+37.22	220.71	LT	211
						TOTAL	211

SCHQ-17

FILE NAME = ...\D2CONCD-ABC-sht-drain16M.dgn	USER NAME = jtardy	DESIGNED -	REVISED -
		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
STORM SEWER REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	80
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

55101200							
STORM SEWER REMOVAL 24"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	59+77.06	63.48	RT	59+67.89	49.36	RT	17
PR I-74	60+13.60	105.35	RT	59+77.06	63.48	RT	56
PR I-74	92+44.19	72.59	LT	92+13.80	141.59	LT	75
PR I-74	92+74.46	10.57	LT	92+44.19	72.59	LT	66
PR I-74	94+71.57	181.00	RT	95+03.89	108.20	RT	80
PR I-74	101+55.00	0.24	RT	101+55.22	89.22	RT	89
PR I-74	101+55.39	66.04	LT	101+55.00	0.24	RT	65
PR I-74	128+80.35	79.28	LT	129+58.22	1.32	RT	112
PR I-74	129+58.22	1.32	RT	130+07.52	15.73	RT	49
PR I-74	130+08.02	80.54	RT	130+07.52	15.73	RT	65
PR I-74	133+53.74	69.95	LT	133+55.24	13.82	RT	84
PR I-74	136+40.52	78.10	RT	136+40.46	16.20	RT	60
PR 7TH B	532+14.28	10.33	RT	530+83.66	27.11	LT	136
PR 7TH-B	532+26.49	20.67	RT	532+14.28	10.33	RT	16
PR 7TH-B	534+72.68	5.68	RT	537+78.27	32.40	RT	290
PR 19TH ST	1922+66.64	44.07	RT	1922+84.81	44.65	RT	18
PR 19TH ST	2062+57.07	33.35	LT	2061+15.66	24.55	LT	136
						TOTAL	1,414

55101400							
STORM SEWER REMOVAL 30"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	130+07.52	15.73	RT	133+55.24	13.82	RT	347
						TOTAL	347

55101600							
STORM SEWER REMOVAL 36"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	133+54.78	13.82	RT	136+06.26	16.05	RT	251
PR I-74	136+06.26	16.05	RT	136+40.46	10.00	RT	33
						TOTAL	284

55101800							
STORM SEWER REMOVAL 42"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR I-74	136+40.46	16.20	RT	137+60.20	15.16	RT	120
						TOTAL	120

55102300							
STORM SEWER REMOVAL 72"							
ALIGNMENT	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	LENGTH
PR 19TH ST	1922+66.64	44.07	RT	1922+84.81	44.65	RT	18
PR 19TH ST	1922+84.81	44.65	RT	1925+39.38	24.70	RT	258
				TOTAL			276

SCHQ-18

FILE NAME = ...\D2CONCD-ABC-sht-drain16AM.dgn	USER NAME = jtardy	DESIGNED -	REVISED -
		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
STORM SEWER REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	81
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE STRUCTURE SCHEDULE

LOCATION							PAY ITEM # -->	54213657	54213669	54213675	54213681	54244805	60218400	60218600	60219000	60219510	60221100	60221700	60222210	60223800	60224035	60224440	60224446	60224448	60224464	60240301	60240305	60240324	60240328	60247160	60247170	60270050	X6020090	#2000331
								PRC FLAR END SEC 12	PRC FLAR END SEC 24	PRC FLAR END SEC 30	PRC FLAR END SEC 36	INLET BOX 542501	MAN TA 4 DIA T1F CL	MAN TA 4 DIA T4F&G	MAN TA 4 DIA T8G	MAN TA4 DIA T20F&G	MAN TA 5 DIA T1F CL	MAN TA 5 DIA T8G	MAN TA 5 DIA T20F&G	MAN TA 6 DIA T1F CL	MAN TA 6 DIA T20F&G	MAN TA 7 DIA T20F&G	MAN TA 7 DIA T1F CL	MAN TA 7 DIA T8G	MAN TA 8 DIA T20F&G	INLETS TB T8G	INLETS TB T10F&G	INLETS TB T20F&G	INLETS TB T24F&G	DR STR T1 W/2 T20F&G	DR STR T2 W/2 T22F&G	DR STR T4 W/2 T20F&G	MANOLE W/RESTRICT PLT	CB TB (SP) T7G
SHEET	ROADWAY	STR	STA	OFFSET LT	OFFSET RT	RIM ELEV		EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
DRUM-01	PR I-74	B4	56+00.00	51.00		629.38							1																					
DRUM-01	PR I-74	B4a	56+00.00	60.05		628.95																						1						
DRUM-01	PR I-74	B6	54+00.00	51.00		626.15							1																					
DRUM-01	PR I-74	B6a	54+00.00	60.05		625.81																						1						
DRUM-01	PR I-74	B8	51+50.00	51.00		621.96							1																					
DRUM-01	PR I-74	B8a	51+50.00	60.00		621.63																						1						
DRUM-01	PR I-74	B10	49+25.00	51.00		618.20							1																					
DRUM-01	PR I-74	B10a	49+08.31	60.12		617.67																						1						
DRUM-01	RAMP 7TH-A	B16	636+31.87		1.00	610.64							1																					
DRUM-01	RAMP 7TH-A	B16a	636+31.81		10.00	610.27																						1						
DRUM-01	RAMP 7TH-A	B17	636+32.13	30.00		610.15																						1						
DRUM-01	RAMP 7TH-A	B18	638+81.98		12.57	598.75																	1											
DRUM-01	RAMP 7TH-A	B18a	638+81.14		21.56	598.43																						1						
DRUM-01	RAMP 7TH-A	B18b	637+69.67		4.80	604.05							1																					
DRUM-01	RAMP 7TH-A	B18c	637+69.28		13.93	603.70																						1						
DRUM-01	RAMP 7TH-A	B18d	638+63.32		30.10	604.58																						1						
DRUM-01	RAMP 7TH-A	B18f	637+48.73		20.82	612.33										1																		
DRUM-01	RAMP 7TH-A	B19	638+81.64	30.00		598.02																						1						
DRUM-01	RAMP 7TH-A	B20	641+34.71		23.50	586.14															1													
DRUM-01	RAMP 7TH-A	B20a	641+44.67		23.26	585.72															1													
DRUM-01	RAMP 7TH-A	B20b	641+65.00	30.00		584.93																						1						
DRUM-01	RAMP 7TH-A	B20c	641+00.00		13.00	588.23														1														
DRUM-01	RAMP 7TH-A	B20d	641+00.00		22.00	587.87																						1						
DRUM-01	RAMP 7TH-A	B25	642+24.13		31.76	580.99																			1									
DRUM-01	RAMP 7TH-A	B26	642+28.63	28.10		584.03																						1						
DRUM-01	RAMP 7TH-A	B26a	642+37.54	33.27		584.71														1														
DRUM-01	RAMP 7TH-A	B63a	640+00.00		13.00	593.06														1														
DRUM-02	PR I-74	B5	56+00.00	0.00		629.30 WB 628.01 EB																										1		
DRUM-02	PR I-74	B7	54+00.00	0.00		624.22 EB 624.10 WB																										1		
DRUM-02	PR I-74	B9	51+50.00	0.00		620.02 EB 619.91 EB																										1		
DRUM-02	PR I-74	B11	49+25.00	0.00		616.45 EB 616.17 WB																										1		
DRUM-02	PR I-74	B11a	49+35.29	0.00		616.54 EB 616.33 WB																										1		
DRUM-02	PR I-74	B29	54+84.97		60.50	624.49										1																		
DRUM-02	PR I-74	B30	53+50.00		65.00	621.65										1																		
DRUM-02	PR I-74	B31	51+50.00		65.00	618.28										1																		
DRUM-02	PR I-74	B32	49+45.00		60.13	615.06																						1						
DRUM-02	PR I-74	B32a	50+02.04		60.00	616.01										1																		
DRUM-02	PR I-74	B33	49+45.15		125.88	588.64											1																	
DRUM-02	PR I-74	B34	49+44.39		179.90	585.76																					1							
DRUM-02	PR I-74	B61	50+05.96		95.67	606.11							1																					
DRUM-02	PR I-74	B62	50+64.14		65.03	616.85										1																		
DRUM-05	PR I-74	B2	57+94.84	51.00		634.12							1																					
DRUM-05	PR I-74	B2a	57+94.84	60.00		634.15																						1						
DRUM-05	RAMP 7TH-A	B13	631+23.00		1.00	635.21							1																					
DRUM-05	RAMP 7TH-A	B13a	631+23.00		9.17	635.08																						1						
DRUM-05	RAMP 7TH-A	B14	633+81.88		1.00	622.69							1																					
DRUM-05	RAMP 7TH-A	B14a	633+81.82		10.00	622.34																						1						
DRUM-05	RAMP 7TH-A	B14b	632+67.29		1.00	628.22							1																					
DRUM-05	RAMP 7TH-A	B14c	632+67.29		17.51	631.19										1																		
DRUM-05	RAMP 7TH-A	B15	633+81.88	30.90		622.87																						1						
DRUM-05	PR I-74	F1	70+35.00	1.62		676.88								1																				
DRUM-05	PR I-74	F2	68+50.00	1.42		670.87								1																				
DRUM-05	PR I-74	F3	66+50.00	3.50		663.85								1																				
DRUM-05	PR I-74	F4	64+50.00	1.50		656.96								1																				
DRUM-05	RAMP 7TH-A	F5	620+44.40		9.50	674.17							1																					
DRUM-05	RAMP 7TH-A	F5a	620+44.40		15.02	673.93																						1						
DRUM-05	RAMP 7TH-A	F6	622+24.72		9.50	668.02							1																					
DRUM-05	RAMP 7TH-A	F6a	622+24.72		17.50	667.66																						1						
DRUM-05	RAMP 7TH-A	F7	624+19.46		9.50	660.55							1																					
DRUM-05	RAMP 7TH-A	F7a	624+19.46		16.50	660.07																						1						
DRUM-05	RAMP 7TH-A	F9	626+12.85		10.03	653.25														1														
DRUM-05	RAMP 7TH-A	F9a	626+12.85		14.27	653.02																						1						
DRUM-05	RAMP 7TH-A	F10	626+22.84		9.49	652.91																										1		
DRUM-05	RAMP 7TH-A	F11	626+41.19		85.97	619.26							1																					
DRUM-05	RAMP 7TH-A	F11a	626+35.37		62.38	629.17							1																					
DRUM-05	RAMP 7TH-A	F11b	626+30.41		41.78	639.86							1																					
DRUM-05	RAMP 7TH-A	F11c	626+25.41		20.58	650.70							1																					
DRUM-05	PR I-74	F22	62+36.92	62.00		647.28																						1						
DRUM-05	PR I-74	F23	62+34.90	1.50		651.81								1																				
DRUM-05	PR I-74	F24	63+00.00	1.50		652.22								1																				
DRUM-05	PR I-74	F25	63+00.00	55.35		649.84															1														
DRUM-05	RAMP 7TH-A	F26	627+26.94		10.00	649.71																						1						
DRUM-05	RAMP 7TH-A	F27	627+16.94		10.00	650.02										1																		
DRUM-05	PR I-74	F28	63+39.21	56.62		651.20															1														
DRUM-05	PR I-74	F29	64+00.00	60.00		653.25															1														
DRUM-05	PR I-74	F30	64+50.00	58.09		655.11																1													
DRUM-05	PR I-74	F31	65+06.96	54.29		657.24																						1						
DRUM-05	RAMP 7TH-A	N38b	630+70.00		43.94	619.60																					1							
SHEET 1 SUBTOTAL								0	0	0	0	0	18	6	0	8	1	0	1	4	4	1	1	1	0	2	0	25	0	0	0	5	1	0

LAYOUT	CBP	7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	JJW	8/21/2015

SCHQ-19

FILE NAME = D2CONCD-HPS-sht-drain10M.dgn	USER NAME = hehn01663	DESIGNED - MGJ	REVISED -
		DRAWN - MGJ	REVISED -
	PLOT SCALE =	CHECKED - JJW	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	82
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE STRUCTURE SCHEDULE

LOCATION							PAY ITEM # -->	54213657	54213669	54213675	54213681	54244805	60218400	60218600	60219000	60219510	60221100	60221700	60222210	60223800	60224035	60224440	60224446	60224448	60224464	60240301	60240305	60240324	60240328	60247160	60247170	60270050	X6020090	#2000331
								PRC FLAR END SEC 12	PRC FLAR END SEC 24	PRC FLAR END SEC 30	PRC FLAR END SEC 36	INLET BOX 542501	MAN TA 4 DIA T1F CL	MAN TA 4 DIA T4F&G	MAN TA 4 DIA T8G	MAN TA4 DIA T20F&G	MAN TA 5 DIA T1F CL	MAN TA 5 DIA T8G	MAN TA 5 DIA T20F&G	MAN TA 6 DIA T1F CL	MAN TA 6 DIA T20F&G	MAN TA 7 DIA T20F&G	MAN TA 7 DIA T1F CL	MAN TA 7 DIA T8G	MAN TA 8 DIA T20F&G	INLETS TB T8G	INLETS TB T10F&G	INLETS TB T20F&G	INLETS TB T24F&G	DR STR T1 W/2 T20F&G	DR STR T2 W/2 T22F&G	DR STR T4 W/2 T20F&G	MANOLE W/RESTRICT PLT	CB TB (SP) T7G
SHEET	ROADWAY	STR	STA	OFFSET LT	OFFSET RT	RIM ELEV		EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
DRUM-06	PR I-74	B3	57+94.84	2.00		634.24																					1						
DRUM-06	PR I-74	B28	56+60.84		60.69	630.05																					1						
DRUM-06	PR I-74	E2	62+05.32		2.07	650.71										1																	
DRUM-06	PR I-74	E2a	61+24.84		2.00	647.74																					1						
DRUM-06	RAMP 7TH-B	E7	530+40.24	55.37		619.00											1																
DRUM-06	RAMP 7TH-B	E9	527+83.11		53.20	606.00																				1							
DRUM-06	RAMP 7TH-B	E10	527+69.30		4.07	619.87							1																				
DRUM-06	PR I-74	E11	64+16.15		88.77	661.48										1																	
DRUM-06	PR I-74	E12	63+89.90		107.59	--				1																							
DRUM-06	PR I-74	E18	66+62.51		73.95	670.91							1																				
DRUM-06	PR I-74	E19	66+89.06		103.06	669.63									1																		
DRUM-06	PR I-74	E19a	69+02.29		136.79	688.50																				1							
DRUM-06	PR I-74	E19b	69+00.72		108.53	--		1																									
DRUM-06	PR I-74	F1a	70+35.00		1.50	681.83																					1						
DRUM-06	PR I-74	F2a	68+50.00		1.50	675.40																					1						
DRUM-06	PR I-74	F3a	66+50.00		1.50	667.79																					1						
DRUM-06	PR I-74	F4a	64+50.00		1.50	660.12																					1						
DRUM-06	PR I-74	F24a	63+00.00		1.50	654.28																					1						
DRUM-09	PR I-74	F12	72+57.61	1.50		680.61								1																			
DRUM-09	PR I-74	F13	72+57.61	74.05		679.82										1																	
DRUM-09	PR I-74	F14	72+50.00	97.03		673.69							1																				
DRUM-09	PR I-74	F18	72+33.55	108.40		667.45							1																				
DRUM-09	PR I-74	F19	72+17.10	119.78		656.89							1																				
DRUM-09	PR I-74	F20	72+00.66	131.16		647.96							1																				
DRUM-09	PR I-74	F21	71+88.27	139.99		646.75							1																				
DRUM-09	PR I-74	G1a	78+50.00	1.50		680.80								1																			
DRUM-09	PR I-74	G1b	78+50.00	72.00		683.60							1																				
DRUM-09	PR I-74	G2a	80+50.00	1.50		680.10								1																			
DRUM-09	PR I-74	G2b	80+50.00	72.00		682.90							1																				
DRUM-10	PR I-74	F12a	72+57.61		1.50	686.17																					1						
DRUM-10	PR I-74	F15	72+09.26		74.03	686.68																						1						
DRUM-10	PR I-74	F16	72+09.40		94.35	684.50							1																				
DRUM-10	PR I-74	F16a	72+09.46		109.12	678.59							1																				
DRUM-10	PR I-74	F17	72+09.52		124.53	--		1																									
DRUM-10	PR I-74	G1	78+50.00		1.50	688.05																					1						
DRUM-10	PR I-74	G2	80+50.00		1.50	687.07																					1						
DRUL-03	RAMP 7TH-B	E13b	524+00.00		14.15	596.50																				1							
DRUL-03	19TH ST.	N29	1915+41.61		46.48	593.62										1																	
DRUL-03	19TH ST.	N30	1917+51.03		45.18	605.77										1																	
DRUL-03	19TH ST.	N31	1919+52.46		52.13	617.77																						1					
DRUL-03	19TH ST.	N34	1919+92.77	33.00		601.62										1																	
SHEET 2 SUBTOTAL								2	0	1	0	0	11	3	1	6	1	0	0	0	0	0	0	0	0	3	1	12	0	0	0	0	0	0

LAYOUT	CBP	7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	JJW	8/21/2015

SCHO-20

FILE NAME = D2CONCD-HPS-sht-drainIIM.dgn	USER NAME = hehn01663	DESIGNED - MGJ	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SCHEDULE OF QUANTITIES DRAINAGE SCHEDULES**
		DRAWN - MGJ	REVISED -		
	PLOT SCALE =	CHECKED - JJW	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -		

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	83
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE STRUCTURE SCHEDULE

LOCATION						PAY ITEM # -->	54213657	54213669	54213675	54213681	54244805	60218400	60216600	60219000	60219510	60221100	60221700	60222210	60223800	60224035	60224440	60224446	60224448	60224464	60240301	60240305	60240324	60240328	60247160	60247170	60270050	X6020090	#2000331
							PRC FLAR END SEC 12	PRC FLAR END SEC 24	PRC FLAR END SEC 30	PRC FLAR END SEC 36	INLET BOX 542501	MAN TA 4 DIA T1F CL	MAN TA 4 DIA T4F&G	MAN TA 4 DIA T8G	MAN TA4 DIA T20F&G	MAN TA 5 DIA T1F CL	MAN TA 5 DIA T8G	MAN TA 5 DIA T20F&G	MAN TA 6 DIA T1F CL	MAN TA 6 DIA T20F&G	MAN TA 7 DIA T20F&G	MAN TA 7 DIA T1F CL	MAN TA 7 DIA T8G	MAN TA 8 DIA T20F&G	INLETS TB T8G	INLETS TB T10F&G	INLETS TB T20F&G	INLETS TB T24F&G	DR STR T1 W/2 T20F&G	DR STR T2 W/2 T22F&G	DR STR T4 W/2 T20F&G	MANOLE W/RESTRICT PLT	CB TB (SP) T7G
SHEET	ROADWAY	STR	STA	OFFSET LT	OFFSET RT	RIM ELEV	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
DRUL-05	19TH ST.	N34a	1920+40.00	33.00		602.50																					1						
DRUL-05	19TH ST.	N36a	1922+84.95		33.00	607.08																					1						
DRUL-05	19TH ST.	N37	1922+84.95	33.00		607.08									1																		
DRUL-05	19TH ST.	N37a	1922+00.00	33.00		605.49																					1						
DRUL-05	19TH ST.	N38	1924+86.73	53.00		616.00											1																
DRUL-05	19TH ST.	N38a	1924+86.73	33.00		610.67										1																	
DRUL-05	19TH ST.	N38c	1924+77.62	48.41		613.89																				1							
DRUL-05	19TH ST.	N39	1925+40.00	33.00		611.85										1																	
DRUL-05	19TH ST.	N40b	1925+40.00		33.00	611.85																					1						
DRUL-07	19TH ST.	F11d	1927+79.97		31.05	615.77																1											
DRUL-07	19TH ST.	N42	1928+90.76	32.61		617.20												1															
DRUL-07	11TH AVE.	N43	1100+57.63		10.50	616.31									1																		
DRUL-07	11TH AVE.	N44	1100+58.23	10.50		616.14									1																		
DRUL-07	19TH ST.	N45	1928+30.00	50.00		616.63																			1								
DRUL-09	19TH ST.	W20a	1937+27.78		25.94	628.09																					1						
DRUL-09	19TH ST.	W20c	1937+38.17		26.00	628.30																					1						
DRUL-09	19TH ST.	Z5a	1936+64.33		28.50	627.35																					1						
DRUL-11	19TH ST.	H9a	1939+85.93		26.00	629.74																					1						
DRUL-11	19TH ST.	W20e	1938+35.00	26.00		629.02																					1						
DRUL-13	19TH ST.	H21a	1948+84.88		26.00	639.53																					1						
DRUL-15	19TH ST.	H23a	1954+50.00		0.00	648.61																					1						
DRUL-15	19TH ST.	H24a	1954+50.00		60.98	648.44									1																		
DRUL-15	19TH ST.	H24b	1954+49.71		75.98	648.55													1														
DRUL-21	19TH ST.	K18	1983+63.03		26.00	684.82									1																		
DRUL-21	19TH ST.	N52	1982+40.00		26.00	685.04																					1						
DRUL-21	19TH ST.	N53	1983+50.00		26.00	684.81									1																		
DRUL-22	19TH ST.	N46	2059+50.00	4.50		654.17										1																	
DRUL-22	19TH ST.	N47	2059+50.00	26.00		653.87																					1						
DRUL-22	19TH ST.	N47a	2058+11.90	25.86		653.50										1																	
DRUL-22	19TH ST.	N47b	2058+69.89	26.14		653.30																					1						
DRUL-23	19TH ST.	N48	2063+10.00	26.00		664.49																					1						
DRUL-23	19TH ST.	N49a	2063+10.00	43.00		662.40											1																
DRUL-27	12TH AVE.	Z3	131+75.00	20.00		629.44																					1						
DRUL-27	12TH AVE.	Z4	131+75.70		12.52	629.59						1																					
DRUL-27	12TH AVE.	Z4a	131+75.70		20.00	629.44																					1						
SHEET 3 SUBTOTAL							0	0	0	0	0	1	0	0	6	4	2	1	1	0	0	1	0	0	1	1	17	0	0	0	0	0	0

TEMPORARY DRAINAGE STRUCTURE SCHEDULE

LOCATION					PAY ITEM # -->	6C240301	60240324
						INLETS TB T8G	INLETS TB T20F&G
ROADWAY	STR	STA	OFFSET LT	OFFSET RT		EA	EA
19TH ST.	N23	1913+48.85	20.29				1
19TH ST.	N33c	1921+10.00		55.53		1	
19TH ST.	N33d	1921+60.00	45.54				1
19TH ST.	N39a	1925+40.00	45.58				1
19TH ST.	N40c	1925+80.00		49.64		1	
TOTAL						2	3

LAYOUT	CBP	7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	JJW	8/21/2015

SCHQ-21

FILE NAME = D2CONCD-HPS-sht-drain12M.dgn	USER NAME = hehn01663	DESIGNED - MGJ	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SCHEDULE OF QUANTITIES DRAINAGE SCHEDULES**
		DRAWN - MGJ	REVISED -		
	PLOT SCALE =	CHECKED - JJW	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -		

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	84
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE STRUCTURE SCHEDULE

LOCATION				OFFSET			PAY ITEM # -->	54213657	54213669	54213675	54213681	54244805	60218400	60218600	60219000	60219510	60221100	60221700	60222210	60223800	60224035	60224440	60224446	60224448	60224464	60240301	60240305	60240324	60240328	60247160	60247170	60270050	X6020090	#2000331
								PRC FLAR END SEC 12	PRC FLAR END SEC 24	PRC FLAR END SEC 30	PRC FLAR END SEC 36	INLET BOX 542501	MAN TA 4 DIA T1F CL	MAN TA 4 DIA T4F&G	MAN TA 4 DIA T8G	MAN TA4 DIA T20F&G	MAN TA 5 DIA T1F CL	MAN TA 5 DIA T8G	MAN TA 5 DIA T20F&G	MAN TA 6 DIA T1F CL	MAN TA 6 DIA T20F&G	MAN TA 7 DIA T20F&G	MAN TA 7 DIA T1F CL	MAN TA 7 DIA T8G	MAN TA 8 DIA T20F&G	INLETS TB T8G	INLETS TB T10F&G	INLETS TB T20F&G	INLETS TB T24F&G	DR STR T1 W/2 T20F&G	DR STR T2 W/2 T22F&G	DR STR T4 W/2 T20 F&G	MANOLE W/RESTRICT PLT	CB TB SPL T7G
SHEET	ROADWAY	STR	STA	LT	RT	RIM ELEV		EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
DRUM-13	PR I-74	G3	82+85.00		3.83	685.81																											1	
DRUM-13	PR I-74	G3A	82+85.00	3.83		679.51										1																		
DRUM-13	PR I-74	G3B	82+85.00	71.00		680.81											1																	
DRUM-13	PR I-74	G4	82+85.00		74.00	684.52							1																					
DRUM-13	PR I-74	G5	82+85.00		118.00				1																									
DRUM-13	PR I-74	I1C	85+25.00	68.00		678.44							1																					
DRUM-13	PR I-74	I1D	85+25.00	74.00		678.21																							1					
DRUM-13	PR I-74	I1EB	85+25.00		3.83	684.63																						1						
DRUM-13	PR I-74	I1WB	85+25.00	3.83		678.67										1																		
DRUM-13	PR I-74	I2C	87+75.00	68.00		676.02											1																	
DRUM-13	PR I-74	I2D	87+75.00	74.00		675.77																							1					
DRUM-13	PR I-74	I2EB	87+75.00		3.83	683.20																						1						
DRUM-13	PR I-74	I2WB	87+75.00	3.83		677.81										1																		
DRUM-13	PR I-74	I3C	90+25.00	68.00		675.12											1																	
DRUM-13	PR I-74	I3D	90+25.00	74.00		674.88																							1					
DRUM-13	PR I-74	I3EB	90+25.00		3.83	680.87																						1						
DRUM-13	PR I-74	I3WB	90+25.00	3.83		676.93										1																		
DRUM-13	PR I-74	I9	92+13.80	141.59		652.00							1																					
DRUM-13	PR I-74	I8	92+30.00	105.80		663.30							1																					
DRUM-13	PR I-74	I7	92+41.00	80.56		672.50							1																					
DRUM-13	PR I-74	I6	92+45.00	69.00		674.29																											1	
DRUM-13	PR I-74	I6A	92+50.00	74.00		674.10																							1					
DRUM-13	PR I-74	I5EB	92+80.00		3.83	678.16																						1						
DRUM-13	PR I-74	I5WB	92+80.00	3.83		676.78										1																		
DRUM-13	PR I-74	I4EB	93+50.00		3.83	677.42																						1						
DRUM-13	PR I-74	I4WB	93+50.00	3.83		676.43										1																		
DRUM-13	PR I-74	I10	96+00.00	80.40		671.89																												
DRUM-13	PR I-74	I11	96+00.00	122.00			1																						1					
DRUM-14	PR I-74	J1EB	97+00.00		3.83	673.24																						1						
DRUM-14	PR I-74	J1WB	97+00.00	3.83		673.79										1																		
DRUM-14	PR I-74	J2	99+05.00	3.83		671.27																1												
DRUM-14	PR I-74	J9	99+05.00		3.83	671.51																											1	
DRUM-14	PR I-74	J8	101+55.00		0.00	WB 669.91 EB 669.93																								1				
DRUM-14	PR I-74	J7	101+80.00		0.00	WB 669.90 EB 669.92																								1				
DRUM-14	PR I-74	J5	102+25.00	90.25				1																										
DRUM-14	PR I-74	J6	102+25.00		0.00	WB 669.95 EB 669.95																									1			
DRUM-14	PR I-74	J4	104+50.00		0.00	WB 670.69 EB 670.69																								1				
DRUM-14	PR I-74	J3	107+00.00		0.00	WB 671.79 EB 671.79																								1				
DRUM-14	PR I-74	K1	109+50.00		0.00	WB 674.39 EB 674.39																								1				
DRUM-15	PR I-74	K2	111+50.00		0.00	WB 677.03 EB 677.03																								1				
DRUM-15	PR I-74	K6EB	113+38.00		3.83	679.51																						1						
DRUM-15	PR I-74	K6WB	113+38.00	3.83		679.51										1																		
DRUM-15	PR I-74	K6	113+45.20	6.30		679.70																	1											
DRUM-15	PR I-74	K10	114+98.71		173.33	677.37														1														
DRUM-15	PR I-74	K3	115+50.00		0.00	WB 681.97 EB 681.97																								1				
DRUM-15	PR I-74	K9	117+20.11		1.98	682.56											1																	
DRUM-15	PR I-74	L1	119+75.00		0.00	WB 681.32 EB 681.32																								1				
DRUM-15	PR I-74	L2	122+00.00		0.00	WB 679.34 EB 679.34																								1				
DRUM-15	PR I-74	L3	124+25.00		0.00	WB 677.36 EB 677.36																								1				
DRUM-16	PR I-74	L4	126+50.00		0.00	WB 675.38 EB 675.38																								1				
DRUM-16	PR I-74	L5	128+50.00	79.00		671.20																											1	
DRUM-16	PR I-74	L6	128+50.00		0.00	WB 673.62 EB 673.62																									1			
DRUM-16	PR I-74	L7	129+50.00		75.00	671.10																											1	
DRUM-16	PR I-74	L8EB	129+50.00		3.83	672.74																	1											
DRUM-16	PR I-74	L10	130+00.00		0.00	WB 672.30 EB 672.30																									1			
DRUM-16	PR I-74	L9	130+08.00		80.50	670.45									1																			
DRUM-16	PR I-74	L11	131+50.00		0.00	WB 670.98 EB 670.98																								1				
DRUM-16	PR I-74	L12	133+25.00	73.26				1																										
DRUM-16	PR I-74	L13	133+25.00	0.00		WB 669.44 EB 669.44																									1			
DRUM-16	PR I-74	L15	135+00.00		0.00	WB 668.00 EB 668.00																								1				
DRUM-16	PR I-74	L17	136+00.00		78.00	664.15									1																			
DRUM-16	PR I-74	L19	136+42.00		0.00	WB 667.00 EB 667.00																									1			
DRUM-16	PR I-74	L18	136+46.00		76.00	664.55												1																
DRUM-16	PR I-74	L20EB	137+50.00		3.83	666.30																			1									
DRUM-16	PR I-74	L20WB	137+50.00	3.83		666.30																							1					
SHEET SUBTOTAL							1	3	0	0	0	5	0	2	8	4	1	0	1	0	1	2	0	1	0	0	8	5	13	5	0	3	2

SCHQ-22

Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

benesch

FILE NAME = ...\D2CONCD-ABC-sht-drainl0M.dgn	USER NAME = jtardy	DESIGNED - AAF	REVISED -	
		DRAWN - AAF	REVISED -	
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -	
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	85
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

DRAINAGE STRUCTURE SCHEDULE

						PAY ITEM # -->	54213657	54213669	54213675	54213681	54244805	60218400	60218600	60219000	60219510	60221100	60221700	60222210	60223800	60224035	60224440	60224446	60224448	60224464	60240301	60240305	60240324	60240328	60247160	60247170	60270050	X6020090	#2000331
LOCATION							PRC FLAR END SEC 12	PRC FLAR END SEC 24	PRC FLAR END SEC 30	PRC FLAR END SEC 36	INLET BOX 542501	MAN TA 4 DIA T1F CL	MAN TA 4 DIA T4F&G	MAN TA 4 DIA T8G	MAN TA4 DIA T20F&G	MAN TA 5 DIA T1F CL	MAN TA 5 DIA T8G	MAN TA 5 DIA T20F&G	MAN TA 6 DIA T1F CL	MAN TA 6 DIA T20F&G	MAN TA 7 DIA T20F&G	MAN TA 7 DIA T1F CL	MAN TA 7 DIA T8G	MAN TA 8 DIA T20F&G	INLETS TB T8G	INLETS TB T10F&G	INLETS TB T20F&G	INLETS TB T24F&G	DR STR T1 W/2 T20F&G	DR STR T2 W/2 T22F&G	DR STR T4 W/2 T20 F&G	MANOLE W/RESTRICT PLT	CB TB SPL T7G
SHEET	ROADWAY	STR	STA	OFFSET LT	OFFSET RT	RIM ELEV	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
DRUM-17	PR I-74	M1	140+00.00		0.00	WB 664.70 EB 664.70																							1				
DRUM-17	PR I-74	M2	142+00.00		0.00	WB 663.27 EB 663.48																							1				
DRUM-17	PR I-74	M3	144+00.00		0.00	WB 662.01 EB 662.13																							1				
DRUM-17	PR I-74	M4	146+00.00		0.00	WB 660.32 EB 660.44																							1				
DRUM-17	PR I-74	M5	148+00.00		0.00	WB 658.39 EB 658.52																							1				
DRUM-17	PR I-74	M6	150+00.00		0.00	WB 656.48 EB 656.60																							1				
DRUM-17	PR I-74	M7	152+00.00		0.00	WB 654.56 EB 654.68																							1				
DRUM-17	PR I-74	M10	153+82.15	84.64		651.15					1																						
DRUM-17	PR I-74	M11	154+05.00	93.85		653.35																1											
DRUM-17	PR I-74	M9	154+05.00		0.00	WB 652.59 EB 652.71																							1				
DRUM-18	PR I-74	M12	156+17.00	136.03						1																							
DRUM-18	PR I-74	M13	157+40.85	140.00				1																									
DRUM-18	PR I-74	M14	157+71.66	111.09		652.00																										1	
DRUM-18	PR I-74	M15	158+04.08	86.31				1																									
SHEET SUBTOTAL							0	2	0	1	1	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	8	0	0	1	0
TEMPORARY SUBTOTAL																									2		3						
TOTAL							3	5	1	1	1	35	9	3	28	10	3	2	6	4	2	5	1	1	8	2	65	5	21	5	5	5	2

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

SCHQ-23

FILE NAME = ...\D2CONCD-ABC-sht-drainlM.dgn	USER NAME = jtardy	DESIGNED - AAF	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SCHEDULE OF QUANTITIES DRAINAGE SCHEDULES**
		DRAWN - AAF	REVISED -		
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -		
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	86
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE PIPE SCHEDULE

			TRENCH BACKFILL	STORM SEWER, CLASS A TYPE 2										STORM SEWER, CLASS A TYPE 3						SS CL A T4		SS CL A T5	SS CL A T6	STORM SEWER, JKD		SS WM REQ
SHEET	STRUCTURE		20800150	550A0340	550A0360	550A0380	550A0400	550A0410	550A0420	550A0430	550A0450	550A0470	550A0480	550A0640	550A0660	550A0680	550A0710	550A0730	550A0750	550A0980	550A1010	550A1300	550A1580	55200900	55201100	Z0056608
	FROM	TO	CU YD	12"	15"	18"	21"	24"	27"	30"	36"	42"	48"	12"	15"	18"	24"	30"	36"	18"	24"	30"	24"	24"	30"	12"
DRUM-01	B4a	B4	2	9																						
DRUM-01	B5	B4	17	51																						
DRUM-01	B6a	B6	2	9																						
DRUM-01	B7	B6	19	51																						
DRUM-01	B8a	B8	2	9																						
DRUM-01	B9	B8	19	51																						
DRUM-01	B10	B63	140	142																						
DRUM-01	B10a	B10	4	19																						
DRUM-01	B11	B10	19	51																						
DRUM-01	B16a	B16	2	9																						
DRUM-01	B17	B16	7	31																						
DRUM-01	B18a	B18	3	9																						
DRUM-01	B18c	B18b	2	9																						
DRUM-01	B19	B18	11	43																						
DRUM-01	B20b	B20a	18	57																						
DRUM-01	B20d	B20c	2	9																						
DRUM-01	B26	B26a	2	10																						
DRUM-01	B14	B16	100		250																					
DRUM-01	B4	B6	146			202																				
DRUM-01	B6	B8	193			254																				
DRUM-01	B8	B63	117			87																				
DRUM-01	B16	B18b	53			138																				
DRUM-01	B18	B63a	198					119																		
DRUM-01	B18b	B18	188														113									
DRUM-01	B20	B20a	12								10															
DRUM-01	B20c	B20	35								36															
DRUM-01	B20a	B25	58								80															
DRUM-01	B25	B26a	67								66															
DRUM-01	B63a	B20c	122								100															
DRUM-01	B18d	B18	55												26											
DRUM-01	B18f	B18b	51												26											
DRUM-01	B26a	B27	425																167							
DRUM-01	B63	B63a	239																		58					
DRUM-02	B11a	B11	3	10																						
DRUM-02	B28	B29	49	175																						
DRUM-02	B32	B32a	15	56																						
DRUM-02	B29	B30	32		132																					
DRUM-02	B30	B31	47		196																					
DRUM-02	B31	B62	22		84																					
DRUM-02	B32a	B62	42		61																					
DRUM-02	B34	B33	0			54																				
DRUM-02	B33	B27	0																65							
DRUM-02	B61	B33	0																	66						
DRUM-02	B62	B61	0																	64						
DRUM-05	B2a	B2	3	9																						
DRUM-05	B13a	B13	6	8																						
DRUM-05	B14a	B14	2	9																						
DRUM-05	B15	B14	7	32																						
DRUM-05	F1	F5	27	70																						
DRUM-05	F2	F6	57	75																						
DRUM-05	F3	F7	27	83																						
DRUM-05	F4	F30	37	57																						
DRUM-05	F5a	F5	2	5																						
DRUM-05	F6a	F6	8	8																						
DRUM-05	F7a	F7	2	7																						
DRUM-05	F9a	F9	1	4																						
DRUM-05	F22	F25	20	62																						
DRUM-05	F23	F24	75	65																						
DRUM-05	F26	F27	2	10																						
DRUM-05	F27	F28	15	47																						
DRUM-05	F31	F30	10	56																						
DRUM-05	B2	B4	69		195																					
DRUM-05	B3	B2	17		51																					
DRUM-05	B13	B14b	167		144																					
DRUM-05	B14b	B14	46		115																					
DRUM-05	B14c	B14b	18		17																					
DRUM-05	F5	F6	55		194																					
DRUM-05	F6	F7	154			194																				
DRUM-05	F24	F25	65			54																				
DRUM-05	F11	F11d	28					28																		
DRUM-05	F25	F28	50					38																		
DRUM-05	F28	F29	106								60															
DRUM-05	F29	F30	113								49															
DRUM-05	F30	F9	88								38															
DRUM-05	F10	F11c	0													11										
DRUM-05	F11c	F11b	0													22										
DRUM-05	F11b	F11a	0													25										
DRUM-05	F11a	F11	0													24										
DRUM-05	F7	F9	194														192									
DRUM-05	F9	F10	19														10									
DRUM-05	N38b	N38	0			120																				
SHEET 1 SUBTOTAL			4,030	1,417	1,439	1,103	0	185	0	0	439	0	0	0	52	82	315	0	232	130	58	0	0	0	0	0

LAYOUT		7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	CBP	8/21/2015
	JJW	

SCHQ-24

FILE NAME = D2CONCD-HPS-sht-drain13M.dgn	USER NAME = hehn01663	DESIGNED - MGJ	REVISED -
		DRAWN - MGJ	REVISED -
	PLOT SCALE =	CHECKED - JJW	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	87
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE PIPE SCHEDULE

			TRENCH BACKFILL	STORM SEWER, CLASS A TYPE 2										STORM SEWER, CLASS A TYPE 3						SS CL A T4		SS CL A T5	SS CL A T6	STORM SEWER, JKD		SS WM REQ
SHEET	STRUCTURE		20800150	550A0340	550A0360	550A0380	550A0400	550A0410	550A0420	550A0430	550A0450	550A0470	550A0480	550A0640	550A0660	550A0680	550A0710	550A0730	550A0750	550A0980	550A1010	550A1300	550A1580	55200900	55201100	Z0056608
	FROM	TO	CU YD	12"	15"	18"	21"	24"	27"	30"	36"	42"	48"	12"	15"	18"	24"	30"	36"	18"	24"	30"	24"	24"	30"	12"
DRUM-06	E2	F23	31	30																						
DRUM-06	E2a	E2	51	80																						
DRUM-06	E19a	E19b	0	28																						
DRUM-06	F1a	F1	4	3																						
DRUM-06	F2a	F2	3	3																						
DRUM-06	F3a	F3	3	3																						
DRUM-06	F4a	F4	3	3																						
DRUM-06	F24a	F24	4	3																						
DRUM-06	E18	E11	278					255																		
DRUM-06	E19	E18	51					40																		
DRUM-06	E11	E12	22							33																
DRUM-06	E9	E10	0														51									
DRUM-06	E4	E5	475																			84				
DRUM-06	E7	E8	63															36								
DRUM-06	E5	E7	0																			125				
DRUM-06	E10	N33	109																				47			
DRUM-09	G1a	G1b	26	71																						
DRUM-09	G2a	G2b	27	74																						
DRUM-09	F12	F13	23			73																				
DRUM-09	F20	F21	2			14																				
DRUM-09	G1b	G2b	186			206																				
DRUM-09	F13	F14	0													24										
DRUM-09	F14	F18	0													20										
DRUM-09	F18	F19	0													20										
DRUM-09	F19	F20	0													20										
DRUM-10	F15	F16	0	20																						
DRUM-10	F16a	F17	0	15																						
DRUM-10	F12a	F12	3											3												
DRUM-10	F16	F16a	0											15												
DRUM-10	G1	G1a	3											3												
DRUM-10	G2	G2a	3											3												
DRUL-03	E13b	E13a	3	8																						
DRUL-03	N29	N26	3	13																						
DRUL-03	N30	N29	168	203																						
DRUL-03	N31	N30	167	202																						
DRUL-03	N34	N33	20	66																						
DRUL-05	N34a	N34	12	47																						
DRUL-05	N36a	N36	4	15																						
DRUL-05	N37	N36	41	51																						
DRUL-05	N37a	N37	27	84																						
DRUL-05	N38c	N38	0	10																						
DRUL-05	N40b	N40a	2	8																						
DRUL-05	N38a	N39	39					53																		
DRUL-05	N39	N40	37					36																		
DRUL-05	N40	N40a	23					21																		
DRUL-05	N38	N38a	28													20										
SHEET 2 SUBTOTAL			**1,945**	**1,040**	**0**	**293**	**0**	**405**	**0**	**33**	**0**	**0**	**0**	**24**	**0**	**104**	**51**	**36**	**0**	**0**	**0**	**209**	**47**	**0**	**0**	**0**

LAYOUT	CBP	7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	JJW	8/21/2015

SCHQ-25

FILE NAME = D2CONCD-HPS-sht-drainl4M.dgn	USER NAME = hehn01663	DESIGNED - KMS	REVISED -
		DRAWN - KMS	REVISED -
	PLOT SCALE =	CHECKED - JJW	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	88
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

DRAINAGE PIPE SCHEDULE

			TRENCH BACKFILL	STORM SEWER, CLASS A TYPE 2										STORM SEWER, CLASS A TYPE 3						SS CL A T4		SS CL A T5	SS CL A T6	STORM SEWER, JKD		SS WM REQ
SHEET	STRUCTURE		20800150	550A0340	550A0360	550A0380	550A0400	550A0410	550A0420	550A0430	550A0450	550A0470	550A0480	550A0640	550A0660	550A0680	550A0710	550A0730	550A0750	550A0980	550A1010	550A1300	550A1580	55200900	55201100	Z0056608
	FROM	TO	CU YD	12"	15"	18"	21"	24"	27"	30"	36"	42"	48"	12"	15"	18"	24"	30"	36"	18"	24"	30"	24"	24"	30"	12"
DRUL-07	N45	N44	0	17																						
DRUL-07	N42	N41	10							8																
DRUL-07	N43	N42	36							35																
DRUL-07	N44	N43	17					21																		
DRUL-09	W20a	W20b	2	7																						
DRUL-09	W20c	W20a	2	10																						
DRUL-09	Z5a	Z5b	6	42																						
DRUL-11	H9a	Ex	2	10																						
DRUL-11	W20e	W20d	10	60																						
DRUL-13	H21a	Ex	3	10																						
DRUL-15	H23a	H24a	12	61																						
DRUL-15	H24a	H24b	0	15																						
DRUL-21	K18	K17	4	28																						
DRUL-21	N52	N53	15	110																						
DRUL-21	N53	K18	2	10																						
DRUL-22	N47	N46	5	22																						
DRUL-22	N47b	N47a	11	59																						
DRUL-23	N48	N49a	0	20																						
DRUL-23	N49a	21531	0					185																		
DRUL-23	Z2	Z1	70			77																				
DRUL-27	Z3	Z4	8																							33
DRUL-27	Z4a	Z4	2	8																						
SHEET 3 SUBTOTAL			**217**	**489**	**0**	**77**	**0**	**206**	**0**	**43**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**33**
PIPE REMOVAL & TEMP SUBTOTAL			**5,112**	**97**	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--

TEMPORARY DRAINAGE PIPE SCHEDULE

			TRENCH BACKFILL	STORM SEWER, CLASS A TYPE 2
SHEET	STRUCTURE		20800150	550A0340
	FROM	TO	CU YD	12"
DRUL-05	N33c	N33b	11	42
DRUL-05	N33d	Ex. 9499	3	20
DRUL-05	N39a	N39	3	13
DRUL-05	N40c	Ex 9214	3	22
	TOTAL		**20**	**97**

LAYOUT	CBP	7/22/2011
DRAWN	CBP	8/20/2015
REVIEWED	JJW	8/21/2015

SCHO-26

FILE NAME = D2CONCD-HPS-sht-drain15M.dgn	USER NAME = hehn01663	DESIGNED - KMS	REVISED -
		DRAWN - KMS	REVISED -
	PLOT SCALE =	CHECKED - JJW	REVISED -
	PLOT DATE = 3/22/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	89
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

benesch

DRAINAGE PIPE SCHEDULE

			TRENCH BACKFILL	STORM SEWER, CLASS A TYPE 2										STORM SEWER, CLASS A TYPE 3						SS CL A T4		SS CL A T5	SS CL A T6	STORM SEWER, JKD		SS WM REQ
SHEET	STRUCTURE		20800150	550A0340	550A0360	550A0380	550A0400	550A0410	550A0420	550A0430	550A0450	550A0470	550A0480	550A0640	550A0660	550A0680	550A0710	550A0730	550A0750	550A0980	550A1010	550A1300	550A1580	55200900	55201100	Z0056608
	FROM	TO	CU YD	12"	15"	18"	21"	24"	27"	30"	36"	42"	48"	12"	15"	18"	24"	30"	36"	18"	24"	30"	24"	24"	30"	12"
DRUM-13	COLLAR	G3B	108				184																			
DRUM-13	G3A	G3	3				3																			
DRUM-13	G3B	G3A	33				65																			
DRUM-13	I10	I11	10	22																						
DRUM-13	I1C	I2C	228			244																				
DRUM-13	I1D	I1C	1	2																						
DRUM-13	I1EB	I1WB	1											2												
DRUM-13	I1WB	I1C	41	62																						
DRUM-13	I2C	I3C	210				242																			
DRUM-13	I2D	I2C	1	2																						
DRUM-13	I2EB	I2WB	1	3																						
DRUM-13	I2WB	I2C	22		62																					
DRUM-13	I3C	I6	247					212																		
DRUM-13	I3D	I3C	1	2																						
DRUM-13	I3EB	I3WB	1	3																						
DRUM-13	I3WB	I3C	23			62																				
DRUM-13	I4EB	I4WB	1	3																						
DRUM-13	I4WB	I5WB	75	66																						
DRUM-13	I5EB	I5WB	1	3																						
DRUM-13	I5WB	I6	98					71																		
DRUM-13	I6	I7	8					8																		
DRUM-13	I6A	I6	8	3																						
DRUM-13	I7	I8	0																		23					
DRUM-13	I8	I9	0														35									
DRUM-13	EX2	EX3	236					80													80					
DRUM-14	J1EB	J1WB	1	3																						
DRUM-14	J1WB	J2	305					201																		
DRUM-14	J2	J9	2								3															
DRUM-14	J3	J4	220						246																	
DRUM-14	J4	J6	171						221																	
DRUM-14	J5	J6	37					78																		
DRUM-14	J6	J7	30						41																	
DRUM-14	J7	J8	16						21																	
DRUM-14	J8	J2	326								246															
DRUM-14	J9	N49a	70					135																		
DRUM-14	K1	K2	135	196																						
DRUM-15	K2	K6WB	265													184										
DRUM-15	K3	K6	77	201																						
DRUM-15	K6EB	K6WB	1	3																						
DRUM-15	K6WB	K6	5													4										
DRUM-15	L1	L2	86	221																						
DRUM-15	L2	L3	99			222																				
DRUM-15	L3	L4	99			222																				
DRUM-16	L4	L6	107			196																				
DRUM-16	L5	L6	0																					73		
DRUM-16	L6	L8EB	97							96																
DRUM-16	L7	L8EB	0																					67		
DRUM-16	L8EB	L10	55								46															
DRUM-16	L9	L10	0																					73		
DRUM-16	L10	L11	194								146															
DRUM-16	L11	L13	226								172															
DRUM-16	L12	L13	0																					67		
DRUM-16	L13	L15	221									172														
DRUM-16	L15	L19	143									139														
DRUM-16	L17	L18	0			42																				
DRUM-16	L18	L19	0																					66		
DRUM-16	L19	L20EB	90										105													
DRUM-16	L20EB	EX	11										13													
DRUM-16	L20WB	L20EB	1	3																						
DRUM-17	M1	M2	78	196																						
DRUM-17	M2	M3	85		196																					
DRUM-17	M3	M4	93			196																				
DRUM-17	M4	M5	95			196																				
DRUM-17	M5	M6	99				196																			
DRUM-17	M6	M7	99				196																			
DRUM-17	M7	M9	105				201																			
DRUM-17	M9	M11	34																						87	
DRUM-17	M10	M11	0					21																		
DRUM-17	M11	M12	0								212															
DRUM-18	M13	M14	0					34																		
DRUM-18	M14	M15	0					33																		
SHEET SUBTOTAL			5,136	994	258	1,380	1,087	873	529	96	825	311	118	2	0	188	35	0	0	0	103	0	0	346	87	
PIPE REMOVAL SUBTOTAL			1,423	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--
TOTAL			**17,915**	**4,037**	**1,697**	**2,853**	**1,087**	**1,669**	**529**	**172**	**1,264**	**311**	**118**	**26**	**52**	**374**	**401**	**36**	**232**	**130**	**161**	**209**	**47**	**346**	**87**	**33**

SCHQ-27

FILE NAME = ...\D2CONCD-ABC-sht-drainl3M.dgn	USER NAME = jtardy	DESIGNED - AAF	REVISED -
		DRAWN - AAF	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	90
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

60108206

PIPE UNDERDRAINS, TYPE 2, 6"

SHEET #	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
DRUM-01/02	48+87.22	56.00	LT	49+08.31	56.00	LT	21
DRUM-01/02	49+08.31	56.00	LT	51+50.00	56.00	LT	242
DRUM-01/02	51+50.00	56.00	LT	54+00.00	56.00	LT	250
DRUM-01/02	54+00.00	56.00	LT	56+00.00	56.00	LT	200
DRUM-01/02	49+02.94	1.50	LT	49+25.00	1.50	LT	22
DRUM-01/02	49+25.00	1.50	LT	49+35.29	1.50	LT	10
DRUM-01/02	49+35.29	1.50	LT	51+50.00	1.50	LT	215
DRUM-01/02	51+50.00	1.50	LT	54+00.00	1.50	LT	250
DRUM-01/02	54+00.00	1.50	LT	56+00.00	1.50	LT	200
DRUM-01/02	49+04.14	1.50	RT	49+25.00	1.50	RT	21
DRUM-01/02	49+25.00	1.50	RT	49+35.29	1.50	RT	10
DRUM-01/02	49+35.29	1.50	RT	51+50.00	1.50	RT	215
DRUM-01/02	51+50.00	1.50	RT	54+00.00	1.50	RT	250
DRUM-01/02	54+00.00	1.50	RT	56+00.00	1.50	RT	200
DRUM-01/02	49+20.75	56.00	RT	50+02.04	56.00	RT	81
DRUM-01/02	50+02.04	62.00	RT	50+64.14	62.00	RT	62
DRUM-01/02	50+64.14	62.00	RT	51+50.00	62.00	RT	86
DRUM-01/02	51+50.00	62.00	RT	53+00.00	62.00	RT	150
DRUM-01/02	53+00.00	62.00	RT	54+84.97	62.00	RT	185
DRUM-01/02	54+84.97	56.00	RT	56+00.00	56.00	RT	115
DRUM-01/02	522+95.46	16.00	LT	523+23.30	16.00	LT	28
DRUM-01/02	523+23.30	16.00	LT	525+25.00	16.00	LT	202
DRUM-01/02	525+25.00	16.00	LT	526+59.47	16.00	LT	134
DRUM-01/02	522+94.93	6.00	RT	526+00.00	6.00	RT	305
DRUM-01/02	526+00.00	6.00	RT	526+52.29	6.00	RT	52
DRUM-01/02	642+25.15	28.02	LT	641+65.00	30.21	LT	60
DRUM-01/02	641+65.00	30.00	LT	638+81.64	30.00	LT	283
DRUM-01/02	638+81.64	30.00	LT	636+32.13	30.00	LT	250
DRUM-01/02	636+32.13	30.00	LT	634+52.04	30.21	LT	180
DRUM-01/02	641+62.19	24.00	RT	641+00.00	12.67	RT	62
DRUM-01/02	641+00.00	23.26	RT	638+81.14	21.56	RT	219
DRUM-01/02	638+81.14	21.56	RT	637+69.28	13.93	RT	112
DRUM-01/02	637+69.28	13.93	RT	636+31.81	12.67	RT	137
DRUM-01/02	636+31.81	12.67	RT	634+51.77	12.67	RT	180
DRUM-05/06	56+00.00	56.00	LT	58+04.61	56.00	LT	205
DRUM-05/06	56+00.00	1.50	LT	56+70.61	1.50	LT	71
DRUM-05/06	62+24.91	56.00	LT	63+39.21	53.00	LT	114
DRUM-05/06	63+39.21	56.00	LT	65+54.62	53.00	LT	215
DRUM-05/06	62+24.91	1.50	LT	63+00.00	1.50	LT	75
DRUM-05/06	63+00.00	1.50	LT	64+50.00	1.50	LT	150
DRUM-05/06	64+50.00	1.50	LT	66+50.00	1.50	LT	200
DRUM-05/06	66+50.00	1.50	LT	68+50.00	1.50	LT	200
DRUM-05/06	68+50.00	1.50	LT	71+00.00	1.50	LT	250
DRUM-05/06	56+00.00	56.00	RT	56+70.61	56.00	RT	71
DRUM-05/06	56+00.00	1.50	RT	56+70.61	1.50	RT	71
DRUM-05/06	61+14.84	62.00	RT	62+50.00	52.00	RT	135
DRUM-05/06	62+50.00	52.00	RT	67+09.69	52.00	RT	460
DRUM-05/06	62+24.91	1.50	RT	63+00.00	1.50	RT	75
DRUM-05/06	63+00.00	1.50	RT	64+50.00	1.50	RT	150
DRUM-05/06	64+50.00	1.50	RT	66+50.00	1.50	RT	200
DRUM-05/06	66+50.00	1.50	RT	68+50.00	1.50	RT	200
DRUM-05/06	68+50.00	1.50	RT	71+00.00	1.50	RT	250
DRUM-05/06	526+59.47	16.00	LT	527+25.00	20.00	LT	66
DRUM-05/06	527+25.00	16.00	LT	531+61.80	20.00	LT	437
DRUM-05/06	531+61.80	20.00	LT	532+50.00	20.00	LT	88
DRUM-05/06	532+50.00	20.00	LT	535+34.85	20.00	LT	285
DRUM-05/06	526+52.29	6.00	RT	528+00.00	8.00	RT	148
DRUM-05/06	528+00.00	6.00	RT	529+50.00	8.00	RT	150
DRUM-05/06	529+50.00	6.00	RT	532+26.49	8.00	RT	276
DRUM-05/06	532+26.49	8.00	RT	534+95.15	8.00	RT	269
DRUM-05/06	534+95.15	7.00	RT	537+87.35	12.00	RT	292
DRUM-05/06	537+87.35	12.00	RT	539+49.45	12.00	RT	162
DRUM-05/06	539+49.45	12.00	RT	541+31.49	12.00	RT	182
DRUM-05/06	634+52.04	30.21	LT	633+81.88	32.00	LT	70
DRUM-05/06	633+81.88	30.90	LT	631+13.21	32.00	LT	269
DRUM-05/06	634+51.77	12.67	RT	633+81.82	9.00	RT	70
DRUM-05/06	633+81.82	12.17	RT	631+12.95	9.00	RT	269
DRUM-05/06	626+51.96	11.82	RT	624+19.46	12.50	RT	233
DRUM-05/06	624+19.46	12.50	RT	623+26.84	16.50	RT	93
DRUM-05/06	623+26.84	11.50	RT	622+24.72	9.50	RT	102
DRUM-05/06	622+24.72	9.50	RT	620+44.40	9.50	RT	180
DRUM-05/06	620+44.40	9.50	RT	620+00.00	6.13	RT	44
DRUM-09/10	72+45.93	74.00	LT	72+57.61	74.00	LT	12
DRUM-09/10	72+57.61	74.00	LT	75+50.00	74.00	LT	292
DRUM-09/10	75+50.00	74.00	LT	78+50.00	74.00	LT	300
DRUM-09/10	78+50.00	74.00	LT	80+50.00	74.00	LT	200
DRUM-09/10	80+50.00	74.00	LT	81+00.00	74.00	LT	50
DRUM-09/10	71+99.30	1.50	LT	72+57.61	1.50	LT	58
DRUM-09/10	72+57.61	1.50	LT	75+50.00	1.50	LT	292
DRUM-09/10	75+50.00	1.50	LT	78+50.00	1.50	LT	300
DRUM-09/10	78+50.00	1.50	LT	80+50.00	1.50	LT	200
DRUM-09/10	80+50.00	1.50	LT	81+00.00	1.50	LT	50
DRUM-09/10	71+98.03	1.50	RT	72+57.61	1.50	RT	60
DRUM-09/10	72+57.61	1.50	RT	75+50.00	1.50	RT	292
DRUM-09/10	75+50.00	1.50	RT	78+50.00	1.50	RT	300
DRUM-09/10	78+50.00	1.50	RT	80+50.00	1.50	RT	200

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PIPE UNDERDRAINS, TYPE 2, 6"

SHEET #	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
DRUM-09/10	80+50.00	1.50	RT	81+00.00	1.50	RT	50
DRUM-09/10	71+93.00	77.00	RT	72+09.26	77.00	RT	16
DRUM-09/10	72+09.26	77.00	RT	75+50.00	77.00	RT	341
DRUM-09/10	75+50.00	77.00	RT	81+00.00	77.00	RT	550
DRUM-13	81+00	73	RT	720+54	12.8	RT	184
DRUM-13	81+00	8	RT	82+85	2.5	RT	186
DRUM-13	81+00	2.5	LT	82+85	2.5	LT	185
DRUM-13	81+00	2.5	LT	82+85	71	LT	190
DRUM-13	82+95	8	RT	82+25	2.5	RT	233
DRUM-13	82+95	2.5	LT	85+25	2.5	LT	231
DRUM-13	82+95	73	LT	85+25	73	LT	229
DRUM-13	85+35	8	RT	87+75	2.5	RT	243
DRUM-13	85+35	2.5	LT	87+75	2.5	LT	240
DRUM-13	85+35	73	LT	87+75	73	LT	239
DRUM-13	85+60	63	RT	88+10	72	RT	252
DRUM-13	87+85	8	RT	90+25	2.5	RT	243
DRUM-13	87+85	2.5	LT	90+25	2.5	LT	240
DRUM-13	87+85	73	LT	90+25	73	LT	233
DRUM-13	88+19	72	RT	91+85	72	RT	373
DRUM-13	90+35	8	RT	92+80	2.5	RT	247
DRUM-13	90+35	2.5	LT	92+80	2.5	LT	247
DRUM-13	90+35	73	LT	92+45	69	LT	207
DRUM-13	91+95	72	RT	94+70	68	RT	280
DRUM-13	92+90	2.5	RT	93+50	2.5	RT	61
DRUM-13	92+90	2.5	LT	93+50	2.5	LT	60
DRUM-13	720+64	7.5	RT	723+14	5	RT	250
DRUM-13	723+25	5	RT	725+78	5	RT	254
DRUM-13	723+48	5	RT	730+98	5	RT	250
DRUM-13	725+88	5	RT	728+38	5	RT	250
DRUM-13	731+08	5	RT	734+29	5	RT	322
DRUM-13	88+23	79	RT	90+90	112	RT	270
DRUM-13	91+06	114	RT	93+65	143	RT	269
DRUM-13	93+75	144	RT	96+15	164	RT	251
DRUM-14	97+04	2.5	LT	99+00	2.5	LT	195
DRUM-14	97+12	2.5	RT	99+00	2.5	RT	188
DRUM-14	97+13	59	LT	100+99	61	LT	380
DRUM-14	97+71	63	RT	97+84	113	RT	129
DRUM-14	98+50	63	RT	100+99	61	RT	253
DRUM-14	98+55	176	RT	96+16	164	RT	248
DRUM-14	99+03	86	LT	100+98	130	LT	194
DRUM-14	99+10	2.5	RT	101+55	2.5	RT	245
DRUM-14	99+10	2.5	LT	101+55	2.5	LT	245
DRUM-14	101+03	132	LT	103+57	192	LT	254
DRUM-14	101+70	2.5	RT	101+55	2.5	RT	15
DRUM-14	101+70	2.5	LT	101+55	2.5	LT	15
DRUM-14	102+15	2.5	RT	101+80	2.5	RT	35
DRUM-14	102+15	2.5	LT	101+80	2.5	LT	35
DRUM-14	102+20	61	LT	101+00	61	LT	118
DRUM-14	103+52	61	RT	101+00	61	RT	255
DRUM-14	104+40	2.5	RT	102+25	2.5	RT	215
DRUM-14	104+40	2.5	LT	102+25	2.5	LT	215
DRUM-14	104+83	61	LT	102+30	61	LT	251
DRUM-14	106+11	61	RT	103+61	61	RT	250
DRUM-14	106+90	2.5	RT	104+50	2.5	RT	240
DRUM-14	106+90	2.5	LT	104+50	2.5	LT	240
DRUM-14	107+45	61	LT	104+93	61	LT	250
DRUM-14	108+70	61	RT	106+21	61	RT	250
DRUM-14	109+40	2.5	RT	107+00	2.5	RT	241
DRUM-14	109+40	2.5	LT	107+00	2.5	LT	241
DRUM-14	109+65	243	RT	112+45	201	RT	290
DRUM-14	110+06	61	LT	107+55	61	LT	250
DRUM-14	738+00	5	RT	734+30	5	RT	371
DRUM-14	739+73	5	RT	738+10	5	RT	163
DRUM-14	824+98	5	LT	828+05	5	LT	306
DRUM-14	832+36	5	LT	828+06	5	LT	430
DRUM-14	1967+05	62.3	RT	1969+96	30.5	RT	292
DRUM-14	1973+00	39	RT	1975+71	41	RT	271
DRUM-15	1975+82	41	RT	1978+43	41	RT	261
DRUM-15	111+40	2.5	RT	109+50	2.5	RT	190
DRUM-15	111+40	2.5	LT	109+50	2.5	LT	190
DRUM-15	112+53	61	RT	108+80	61	RT	375
DRUM-15	112+55	198	RT	114+88	123	RT	248
DRUM-15	112+67	61	LT	110+16	61	LT	250
DRUM-15	113+23	2.5	RT	111+50	2.5	RT	178
DRUM-15	113+28	2.5	LT	111+50	2.5	LT	178
DRUM-15	114+97	121	RT	117+35	67	RT	247
DRUM-15	115+01	61	RT	112+63	61	RT	239
DRUM-15	115+35	197	LT	118+02	168	LT	265
DRUM-15	115+40	2.5	RT	113+38	2.5	RT	203
DRUM-15	115+40	2.5	LT	113+38	2.5	LT	202
DRUM-15	116+70	2.5	RT	115+50	2.5	RT	121
DRUM-15	116+70	61	RT	115+11	61	RT	160
DRUM-15	116+70	2.5	LT	115+50	2.5	LT	121
DRUM-15	116+70	61	LT	112+77	61	LT	390
DRUM-15	116+80	2.5	RT	117+20	2.5	RT	41
DRUM-15	116+80	61	RT	118+54	57	RT	175

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PIPE UNDERDRAINS, TYPE 2, 6"

SHEET #	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
DRUM-15	116+80	2.5	LT	117+20	2.5	LT	42
DRUM-15	116+80	61	LT	119+42	61	LT	260
DRUM-15	117+30	2.5	RT	119+75	2.5	RT	245
DRUM-15	117+30	2.5	LT	119+75	2.5	LT	245
DRUM-15	118+09	167	LT	120+71	111	LT	265
DRUM-15	118+64	57	RT	121+08	51	RT	245
DRUM-15	119+52	61	LT	121+15	61	LT	163
DRUM-15	119+85	2.5	RT	122+00	2.5	RT	216
DRUM-15	119+85	2.5	LT	122+00	2.5	LT	215
DRUM-15	120+83	109	LT	122+51	77	LT	170
DRUM-15	121+25	61	LT	123+78	61	LT	252
DRUM-15	122+10	2.5	RT	124+25	2.5	RT	216
DRUM-15	122+10	2.5	LT	124+25	2.5	LT	215
DRUM-15	123+88	61	LT	125+77	53	LT	189
DRUM-15	124+35	2.5	RT	126+50	2.5	RT	216
DRUM-15	124+35	2.5	LT	126+50	2.5	LT	215
DRUM-15	922+86	5	RT	924+53	5	RT	167
DRUM-15	927+10	5	RT	924+54	5	RT	256
DRUM-15	927+20	5	RT	929+70	5	RT	249
DRUM-15	929+80	5	RT	932+25	5	RT	245
DRUM-15	932+35	5	RT	934+85	5	RT	250
DRUM-15	934+95	5	RT	937+43	5	RT	249
DRUM-15	1019+05	5	LT	1023+79	5	LT	473
DRUM-15	1023+90	5	LT	1025+77	5	LT	186
DRUM-16	126+60	2.5	RT	128+50	2.5	RT	190
DRUM-16	126+60	2.5	LT	128+50	2.5	LT	190
DRUM-16	128+60	2.5	RT	129+55	2.5	RT	71
DRUM-16	128+60	2.5	LT	130+00	2.5	LT	140
DRUM-16	129+65	2.5	RT	130+00	2.5	RT	87
DRUM-16	130+10	2.5	RT	131+50	2.5	RT	141
DRUM-16	130+10	2.5	LT	131+50	2.5	LT	138
DRUM-16	131+38	61	LT	133+17	61	LT	180
DRUM-16	131+50	61	RT	134+00	61	RT	250
DRUM-16	131+60	2.5	RT	133+25	2.5	RT	167
DRUM-16	131+60	2.5	LT	133+25	2.5	LT	166
DRUM-16	133+32	61	LT	135+35	61	LT	202
DRUM-16	133+35	2.5	RT	135+00	2.5	RT	166
DRUM-16	133+35	2.5	LT	135+00	2.5	LT	165
DRUM-16	134+10	61	RT	136+46	76	RT	244
DRUM-16	135+10	2.5	RT	136+42	2.5	RT	133
DRUM-16	135+10	2.5	LT	136+42	2.5	LT	132
DRUM-16	135+48	57	LT	138+00	61	LT	252
DRUM-16	136+50	61	RT	138+97	61	RT	249
DRUM-16	136+52	2.5	RT	137+50	2.5	RT	99
DRUM-16	136+52	2.5	LT	137+50	2.5	LT	98
DRUM-16	137+60	2.5	RT	140+00	2.5	RT	241
DRUM-16	137+60	2.5	LT	140+00	2.5	LT	240
DRUM-16	138+10	61	LT	140+59	61	LT	250
DRUM-16	139+07	61	RT	141+58	61	RT	250
DRUM-16	937+53	5	RT	940+71	9	RT	318
DRUM-16	940+81	9	RT	941+21	35	RT	60
DRUM-16	941+27	10	RT	131+40	61	RT	136
DRUM-16	1025+87	5	LT	1027+37	5	LT	150
DRUM-16	1027+46	5	LT	1028+30	5	LT	84
DRUM-16	1028+65	5	LT	131+27	61	LT	262
DRUM-17	140+10	2.5	RT	142+00	2.5	RT	191
DRUM-17	140+10	2.5	LT	142+00	2.5	LT	191
DRUM-17	140+69	61	LT	143+15	61	LT	250
DRUM-17	141+68	61	RT	144+22	61	RT	250
DRUM-17	142+10	2.5	RT	144+00	2.5	RT	190
DRUM-17	142+10	2.5	LT	144+00	2.5	LT	190
DRUM-17	143+25	61	LT	145+71	61	LT	250
DRUM-17	144+10	2.5	RT	146+00	2.5	RT	191
DRUM-17	144+10	2.5	LT	146+00	2.5	LT	191
DRUM-17	144+32	61	RT	146+86	61	RT	250
DRUM-17	145+81	61	LT	148+27	61	LT	250
DRUM-17	146+10	2.5	RT	148+00	2.5	RT	191
DRUM-17	146+10	2.5	LT	148+00	2.5	LT	191
DRUM-17	146+96	61	RT	149+49	61	RT	249
DRUM-17	148+10	2.5	RT	150+00	2.5	RT	191
DRUM-17	148+10	2.5	LT	150+00	2.5	LT	191
DRUM-17	148+37	61	LT	150+83	61	LT	250
DRUM-17	149+60	61	RT	152+14	61	RT	250
DRUM-17	150+10	2.5	RT	152+00	2.5	RT	191
DRUM-17	150+10	2.5	LT	152+00	2.5	LT	191
DRUM-17	150+93	61	LT	152+57	61	LT	167
DRUM-17	152+10	2.5	RT	154+05	2.5	RT	196
DRUM-17	152+10	2.5	LT	154+05	2.5	LT	196
DRUM-17	152+24	61	RT	153+99	61	RT	172
DRUM-17	152+67	61	LT	154+03	79	LT	148
DRUL-03	1917+43.85	35.58	LT	1919+92.77	35.58	LT	249
DRUL-03	1919+92.77	35.58	LT	1920+00.00	35.58	LT	7
DRUL-03	1915+11.98	35.58	RT	1917+00.00	35.58	RT	188
DRUL-03	1917+00.00	35.58	RT	1920+00.00	35.58	RT	300
DRUL-05	1920+00.00	35.58	LT	1922+84.95	35.58	LT	285

60108206

PIPE UNDERDRAINS, TYPE 2, 6"

SHEET #	STA FROM	OFFSET	LT/RT	STA TO	OFFSET	LT/RT	FOOT
DRUL-05	1922+84.95	35.58	LT	1925+40.00	35.58	LT	255
DRUL-05	1925+40.00	35.58	LT	1926+00.00	35.58	LT	60
DRUL-05	1920+00.00	35.58	RT	1922+84.95	35.58	RT	285
DRUL-05	1922+84.95	35.58	RT	1925+40.00	35.58	RT	255
DRUL-05	1925+40.00	35.58	RT	1926+00.00	35.58	RT	60
DRUL-07	1926+00.00	35.58	LT	1928+19.35	35.58	LT	219
DRUL-07	1928+90.76	35.19	LT	1932+00.00	28.58	LT	309
DRUL-07	1931+78.02	29.07	LT	1932+00.00	28.58	LT	22
DRUL-07	1926+00.00	35.58	RT	1928+89.66	32.70	RT	290
DRUL-07	1928+89.66	32.70	RT	1931+77.77	29.92	RT	288
DRUL-07	1928+89.66	32.70	RT	1932+00.00	31.27	RT	310
DRUL-09	1932+00.00	28.58	LT	1936+11.44	28.58	LT	411
DRUL-09	1937+28.15	50.86	LT	1938+00.00	28.59	LT	72
DRUL-09	1932+00.00	31.27	RT	1935+13.18	32.22	RT	313
DRUL-09	1936+57.30	34.35	RT	1938+00.00	28.59	RT	143
DRUL-11	1938+00.00	28.59	LT	1939+76.39	27.95	LT	176
DRUL-11	1939+76.39	27.95	LT	1942+16.32	28.02	LT	240
DRUL-11	1942+16.32	28.02	LT	1944+00.00	28.58	LT	184
DRUL-11	1938+00.00	28.59	RT	1939+75.93	28.65	RT	176
DRUL-11	1939+75.93	28.65	RT	1942+25.49	28.30	RT	250
DRUL-11	1942+25.49	28.30	RT	1944+00.00	28.58	RT	175
DRUL-13	1944+00.00	28.58	LT	1944+75.00	27.96	LT	75
DRUL-13	1944+75.00	27.96	LT	1948+75.35	27.79	LT	400
DRUL-13	1948+75.35	27.79	LT	1950+00.00	28.58	LT	125
DRUL-13	1944+00.00	28.58	RT	1944+87.49	28.13	RT	87
DRUL-13	1944+87.49	28.13	RT	1948+74.88	28.22	RT	387
DRUL-13	1948+74.88	28.22	RT	1950+00.00	28.58	RT	125
DRUL-15	1950+00.00	2.58	LT	1952+46.25	2.48	LT	246
DRUL-15	1952+46.25	2.48	LT	1955+37.31	2.58	LT	291
DRUL-15	2050+00.00	2.58	RT	2052+47.47	2.72	RT	247
DRUL-15	2052+47.47	2.72	RT	2056+00.00	2.58	RT	353
DRUL-17	1956+24.32	2.58	LT	1960+05.80	1.39	LT	381
DRUL-17	1960+05.80	1.39	LT	1962+00.00	2.58	LT	194
DRUL-18	1962+00.00	2.58	LT	1962+51.61	2.58	LT	52
DRUL-18	1962+51.61	2.58	LT	1965+01.42	2.18	LT	250
DRUL-18	1965+01.42	2.18	LT	1967+51.01	1.82	LT	250
DRUL-18	1967+51.01	1.82	LT	1968+00.00	2.58	LT	49
DRUL-19	1968+00.00	2.58	LT	1969+80.00	2.58	LT	180
DRUL-19	1973+00.00	12.58	LT	1974+00.00	12.58	LT	100
DRUL-20	1974+00.00	12.58	LT	1977+94.03	2.09	LT	394
DRUL-20	1977+94.03	2.09	LT	1980+00.00	2.58	LT	206
DRUL-21	1982+20.18	26.00	RT	1983+50.00	26.00	RT	130
DRUL-21	1983+50.00	26.00	RT	1983+63.03	26.00	RT	13
DRUL-21	1983+63.03	26.00	RT	1985+50.00	26.00	RT	187
DRUL-21	1980+00.00	2.58	LT	1980+65.67	1.75	LT	66
DRUL-21	1980+65.67	1.75	LT	1983+62.99	2.62	LT	297
DRUL-21	1983+62.99	2.62	LT	1984+14.32	2.30	LT	51
DRUL-21	1984+14.32	2.30	LT	1985+50.00	2.58	LT	136
DRUL-22	2056+00.00	2.58	RT	2056+22.44	2.97	RT	22
DRUL-22	2056+22.44	2.97	RT	2057+87.89	2.30	RT	165
DRUL-22	2057+87.89	2.30	RT	2060+00.00	2.58	RT	212
DRUL-22	2059+00.00	28.58	LT	2061+15.66	24.55	LT	216
DRUL-22	2061+15.66	24.55	LT	2062+00.00	28.58	LT	84
DRUL-23	2066+43.76	14.58	RT	2067+17.18	14.58	RT	73
DRUL-23	2067+17.18	14.58	RT	2068+00.00	14.58	RT	83
DRUL-23	2062+00.00	28.58	LT	2063+50.00	28.58	LT	150
DRUL-23	2063+50.00	28.58	LT	2068+00.00	28.58	LT	450
DRUL-24	2068+00.00	14.58	RT	2068+20.00	14.58	RT	20
DRUL-24	2068+00.00	28.58	LT	2068+20.00	28.58	LT	20
DRUL-25	2074+30.00	12.58	RT	2075+93.80	12.58	RT	164
DRUL-25	2075+93.80	12.58	RT	2078+00.00	12.58	RT	206
DRUL-25	2077+32.69	17.14	LT	2078+00.00	17.14	LT	67
DRUL-27	128+34.04	20.59	LT	129+06.88	71.50	LT	73
DRUL-27	128+30.50	20.79	RT	129+13.14	22.75	RT	83
DRUL-27	130+48.39	22.58	LT	132+05.00	22.67	LT	157
DRUL-27	130+76.58	55.10	RT	132+05.00	22.67	RT	128
						TOTAL	62,884

SCHO-28

FILE NAME = ...\D2CONCD-ABC-sht-drain17M.dgn	USER NAME = jtardy	DESIGNED -	REVISED -
		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	91
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

60100060				
CONCRETE HEADWALLS FOR PIPE DRAINS				
ALIGNMENT	STA	OFFSET	LT/RT	EACH
PR I-74	58+51.71	119.62	LT	1
PR I-74	61+14.84	74.68	RT	1
PR I-74	62+50.00	67.98	RT	1
PR I-74	67+00.00	97.64	RT	1
PR I-74	68+57.14	89.97	RT	1
PR I-74	81+00.00	114.51	RT	1
PR I-74	101+00.23	102.55	RT	1
PR I-74	101+00.51	82.43	LT	1
PR I-74	102+30.30	86.04	LT	1
PR I-74	103+63.21	81.59	RT	1
PR I-74	104+92.15	84.04	LT	1
PR I-74	106+22.32	87.20	RT	1
PR I-74	107+54.43	66.32	LT	1
PR I-74	108+79.83	70.88	RT	1
PR I-74	110+15.79	68.72	LT	1
PR I-74	112+62.34	96.22	RT	1
PR I-74	112+75.95	93.15	LT	1
PR I-74	115+10.49	88.86	RT	1
PR I-74	118+54.88	100.25	RT	1
PR I-74	121+08.88	95.61	RT	1
PR I-74	119+41.34	79.61	LT	1
PR I-74	121+14.50	67.68	LT	1
PR I-74	123+65.76	85.62	RT	1
PR I-74	123+79.58	94.28	LT	1
PR I-74	125+78.17	87.04	LT	1
PR I-74	126+23.18	80.32	RT	1
PR I-74	127+37.33	81.67	LT	1
PR I-74	128+32.13	73.87	LT	1
PR I-74	129+35.12	73.33	RT	1
PR I-74	131+27.37	67.60	LT	1
PR I-74	131+80.35	69.42	RT	1
PR I-74	133+17.07	69.32	LT	1
PR I-74	134+00.73	72.73	RT	1
PR I-74	135+34.76	74.16	LT	1
PR I-74	137+99.57	67.57	LT	1
PR I-74	138+96.87	71.42	RT	1
PR I-74	140+58.07	76.21	LT	1
PR I-74	141+58.10	76.59	RT	1
PR I-74	143+15.41	77.90	LT	1
PR I-74	144+22.37	76.67	RT	1
PR I-74	145+71.38	72.25	LT	1
PR I-74	146+86.39	79.15	RT	1
PR I-74	148+26.93	75.16	LT	1
PR I-74	149+51.09	77.60	RT	1
PR I-74	150+82.73	80.89	LT	1
PR I-74	152+14.41	80.15	RT	1
PR I-74	152+58.22	80.05	LT	1
PR I-74	153+98.33	79.55	RT	1
RAMP 7TH-B	526+00.00	12.00	RT	1
RAMP 7TH-B	528+00.00	39.97	RT	1
RAMP 7TH-B	529+51.69	19.20	RT	1
RAC-B	924+54.00	38.44	RT	1
RAC-C	723+14.97	21.25	RT	1
RAC-C	725+78.84	28.39	RT	1
RAC-C	728+37.79	33.67	RT	1
RAC-C	729+66.43	40.92	LT	1
RAC-C	730+98.10	28.88	RT	1
RAC-C	732+63.02	51.38	LT	1
RAC-C	734+30.04	23.55	RT	1
RAC-C	738+09.92	23.97	RT	1
RAC-D	828+03.79	19.75	LT	1
19TH ST	2078+00.00	29.53	LT	1
19TH ST	1967+27.00	163.50	RT	1
19TH ST	1975+71	149.00	RT	1
19TH ST	1978+45	173.00	RT	1
TOTAL				65

60108200			
PIPE UNDERDRAINS 6" (SPECIAL)			
ALIGNMENT	STA	LT/RT	FOOT
PR I-74	58+51.71	LT	14
PR I-74	61+14.84	RT	13
PR I-74	62+50.00	RT	7
PR I-74	67+00.00	RT	14
PR I-74	68+57.14	RT	14
PR I-74	81+00.00	RT	37
PR I-74	88+10	RT	51
PR I-74	91+85	RT	30
PR I-74	94+70	RT	52
PR I-74	100+99	RT	43
PR I-74	101+00	RT	43
PR I-74	103+61	RT	19
PR I-74	106+21	RT	26
PR I-74	108+80	RT	9
PR I-74	112+63	RT	34
PR I-74	115+11	RT	26
PR I-74	118+54	RT	43
PR I-74	131+40	RT	41
PR I-74	134+00	RT	12
PR I-74	138+97	RT	11
PR I-74	141+58	RT	16
PR I-74	144+22	RT	16
PR I-74	146+86	RT	19
PR I-74	149+49	RT	18
PR I-74	152+14	RT	20
PR I-74	153+99	RT	21
PR I-74	100+99	LT	23
PR I-74	101+00	LT	23
PR I-74	102+30	LT	26
PR I-74	104+93	LT	24
PR I-74	107+55	LT	20
PR I-74	110+16	LT	9
PR I-74	112+77	LT	34
PR I-74	119+42	LT	20
PR I-74	121+15	LT	7
PR I-74	123+78	LT	40
PR I-74	125+77	LT	35
PR I-74	131+27	LT	9
PR I-74	133+17	LT	9
PR I-74	135+35	LT	15
PR I-74	138+00	LT	9
PR I-74	140+59	LT	18
PR I-74	143+15	LT	19
PR I-74	145+71	LT	16
PR I-74	148+27	LT	17
PR I-74	150+83	LT	22
PR I-74	152+57	LT	20
RAMP 7TH-B	526+00.00	RT	5
RAMP 7TH-B	528+00.00	RT	33
RAMP 7TH-B	529+51.69	RT	9
RAMP AC-A	1023+79	LT	6
RAMP AC-A	1025+77	LT	11
RAMP AC-A	1027+37	LT	14
RAMP AC-A	1028+67	LT	10
RAMP AC-B	924+53	RT	33
RAMP AC-B	924+54	RT	33
RAMP AC-B	929+70	RT	19
RAMP AC-B	932+25	RT	22
RAMP AC-B	932+26	RT	44
RAMP AC-B	934+85	RT	26
RAMP AC-B	937+43	RT	14
RAMP AC-B	940+71	RT	17
RAMP AC-C	723+14	RT	16
RAMP AC-C	725+78	RT	21
RAMP AC-C	728+38	RT	29
RAMP AC-C	730+98	RT	25
RAMP AC-C	734+29	RT	19
RAMP AC-C	734+30	RT	19
RAMP AC-C	738+10	RT	15
RAMP AC-D	828+05	LT	16
RAMP AC-D	828+06	LT	16
19TH ST	2078+00	LT	12
19TH ST	1967+05	RT	104
19TH ST	1975+71	RT	108
19TH ST	1978+43	RT	92
TOTAL			1852

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		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE SCHEDULES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	92
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

50104400				
CONCRETE HEADWALL REMOVAL				
ALIGNMENT	STA	OFFSET	LT/RT	EACH
PR I-74	66+88.94	103.12	RT	1
PR I-74	71+95.71	8.64	RT	1
PR I-74	72+40.27	30.34	LT	1
PR I-74	72+43.69	98.32	LT	1
PR I-74	101+55.17	95.93	RT	1
PR I-74	101+55.48	70.96	LT	1
PR I-74	109+56.04	71.45	RT	1
PR I-74	109+56.18	5.70	RT	1
PR I-74	117+65.87	109.94	RT	1
PR I-74	121+05.52	83.59	RT	1
PR I-74	121+06.11	6.25	RT	1
PR I-74	123+52.26	101.57	LT	1
PR I-74	146+05.22	5.67	RT	1
PR I-74	146+05.43	72.57	RT	1
19TH ST	1927+78.46	59.38	RT	1
			TOTAL	15

60500040				
REMOVING MANHOLES				
ALIGNMENT	STA	OFFSET	LT/RT	EACH
PR I-74	49+11.19	28.44	LT	1
PR I-74	49+12.32	6.68	LT	1
PR I-74	49+63.80	38.59	LT	1
PR I-74	50+78.42	8.68	RT	1
PR I-74	50+78.59	4.47	RT	1
PR I-74	50+79.58	44.40	RT	1
PR I-74	50+94.82	144.92	RT	1
PR I-74	50+97.74	103.30	RT	1
PR I-74	50+98.25	93.77	RT	1
PR I-74	52+78.50	85.24	LT	1
PR I-74	53+07.63	95.94	LT	1
PR I-74	53+95.96	11.91	RT	1
PR I-74	54+20.81	6.00	RT	1
PR I-74	54+45.61	8.89	RT	1
PR I-74	55+74.54	111.28	RT	1
PR I-74	56+78.50	0.85	RT	1
PR I-74	56+97.60	25.79	RT	1
PR I-74	58+50.07	39.22	LT	1
PR I-74	58+90.81	24.17	LT	1
PR I-74	60+13.60	105.35	RT	1
PR I-74	60+23.66	72.53	RT	1
PR I-74	60+26.99	96.48	RT	1
PR I-74	61+26.63	37.62	RT	1
PR I-74	61+28.33	80.55	RT	1
PR I-74	61+48.30	124.41	RT	1
PR I-74	62+26.62	93.44	RT	1
PR I-74	62+50.87	57.30	LT	1
PR I-74	62+75.19	73.19	LT	1
PR I-74	63+22.14	65.78	LT	1
PR I-74	63+94.78	94.12	RT	1
PR I-74	66+74.68	89.55	RT	1
PR I-74	71+93.54	66.17	RT	1
PR I-74	72+45.80	80.89	LT	1
PR I-74	82+84.29	20.30	LT	1
PR I-74	92+13.80	141.59	LT	1
PR I-74	114+98.70	173.32	RT	1
PR I-74	117+20.14	1.95	RT	1
PR I-74	117+57.20	52.71	RT	1
PR I-74	123+53.83	57.65	LT	1
PR I-74	130+07.52	15.73	RT	1
PR I-74	133+55.24	13.82	RT	1
PR I-74	136+06.26	16.05	RT	1
PR I-74	136+40.46	16.20	RT	1
19TH ST	1919+92.99	46.82	RT	1
19TH ST	1919+95.24	26.92	LT	1
19TH ST	1920+40.99	28.39	LT	1
19TH ST	1920+42.09	23.49	RT	1
19TH ST	1921+59.65	26.04	LT	1
19TH ST	1922+33.61	61.90	RT	1
19TH ST	1922+84.81	44.65	RT	1
19TH ST	1922+86.80	61.36	RT	1
19TH ST	1922+88.83	17.16	LT	1
19TH ST	1922+91.16	24.54	RT	1
19TH ST	1923+34.88	40.44	LT	1
19TH ST	1923+47.53	57.33	LT	1
19TH ST	1924+20.46	34.21	LT	1
19TH ST	1925+12.31	46.33	RT	1
19TH ST	1925+36.61	32.75	LT	1
19TH ST	1925+59.09	42.32	RT	1
19TH ST	1928+68.28	71.64	LT	1
19TH ST	1928+91.04	35.33	LT	1
19TH ST	2062+57.07	33.35	LT	1
		TOTAL		62

60500050				
REMOVING CATCH BASINS				
ALIGNMENT	STA	OFFSET	LT/RT	EACH
PR I-74	83+00.50	71.55	LT	1
PR I-74	92+74.97	10.85	LT	1
PR I-74	101+55.35	0.24	RT	1
PR I-74	128+80.35	79.28	LT	1
PR I-74	129+58.22	1.32	RT	1
PR I-74	130+08.02	80.54	RT	1
PR I-74	133+53.74	69.95	LT	1
PR I-74	136+06.48	0.12	RT	1
PR I-74	136+40.52	78.10	RT	1
			TOTAL	9

60500060				
REMOVING INLETS				
ALIGNMENT	STA	OFFSET	LT/RT	EACH
PR I-74	50+77.37	32.50	LT	1
PR I-74	53+01.66	54.57	LT	1
PR I-74	53+94.53	49.33	RT	1
PR I-74	54+19.17	32.55	LT	1
PR I-74	59+96.39	134.95	LT	1
PR I-74	62+26.04	88.60	RT	1
PR I-74	62+70.82	18.31	LT	1
PR I-74	64+49.18	93.48	LT	1
PR I-74	67+49.28	77.53	LT	1
PR I-74	72+46.66	73.73	LT	1
PR I-74	92+44.22	72.65	LT	1
19TH ST	1919+93.93	79.99	RT	1
19TH ST	1928+42.01	69.72	LT	1
19TH ST	1936+57.50	33.92	RT	1
Temporary Inlets				
19TH ST	1913+48.85	20.29	LT	1
19TH ST	1921+10.00	55.53	RT	1
19TH ST	1921+60.00	45.54	LT	1
19TH ST	1925+40.00	45.58	LT	1
19TH ST	1925+80.00	49.64	RT	1
		TOTAL		19

Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

SCHQ-30

FILE NAME = ...\D2CONCD-ABC-sht-drainl8M.dgn	USER NAME = jtardy	DESIGNED -	REVISED -
		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE STRUCTURE REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	93
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

60255500

MANHOLES TO BE ADJUSTED

ALIGNMENT	STA	OFFSET	LT/RT	SHEET	TYPE		EACH
19TH ST.	1913+27.34	56.00	RT	DRUL-01	STM	E14	1
19TH ST.	1917+00.00	5.02	RT	DRUL-03	STM	N28	1
19TH ST.	1917+71.41	7.00	LT	DRUL-03	SAN	--	1
19TH ST.	1918+72.42	20.46	LT	DRUL-03	SAN	--	1
19TH ST.	1919+07.19	9.94	RT	DRUL-03	SAN	--	1
19TH ST.	1919+40.00	30.47	RT	DRUL-03	STM	N33a	1
19TH ST.	1919+92.77	33.00	RT	DRUL-03	STM	N33	1
19TH ST.	1920+70.00	43.85	RT	DRUL-05	STM	N33b	1
19TH ST.	1922+93.76	7.35	LT	DRUL-05	SAN	--	1
19TH ST.	1924+96.10	21.01	LT	DRUL-05	SAN	--	1
19TH ST.	1925+39.38	24.70	RT	DRUL-05	STM	N40a	1
19TH ST.	1926+39.01	25.63	RT	DRUL-07	STM	--	1
19TH ST.	1926+54.96	14.82	RT	DRUL-07	STM	--	1
19TH ST.	1926+86.61	28.30	LT	DRUL-07	STM	--	1
19TH ST.	1928+55.44	19.58	LT	DRUL-07	SAN	--	1
19TH ST.	1928+89.03	36.05	RT	DRUL-07	STM	--	1
19TH ST.	1928+89.91	24.24	LT	DRUL-07	STM	N41	1
19TH ST.	1930+22.57	11.39	RT	DRUL-07	TEL	--	1
19TH ST.	1930+96.48	22.15	RT	DRUL-07	STM	--	1
19TH ST.	1931+51.52	12.47	LT	DRUL-07	STM	--	1
19TH ST.	1935+16.72	22.00	RT	DRUL-09	STM	--	1
19TH ST.	1935+68.08	34.07	RT	DRUL-09	STM	--	1
19TH ST.	1935+90.46	34.84	LT	DRUL-09	STM	--	1
19TH ST.	1936+01.42	20.43	RT	DRUL-09	SAN	--	1
19TH ST.	1936+04.55	21.73	LT	DRUL-09	STM	--	1
19TH ST.	1936+16.60	41.77	RT	DRUL-09	STM	--	1
19TH ST.	1936+28.31	1.81	LT	DRUL-09	STM	--	1
19TH ST.	1936+44.78	20.17	RT	DRUL-09	STM	--	1
19TH ST.	1936+98.68	59.06	LT	DRUL-09	STM	--	1
19TH ST.	1937+27.78	32.42	RT	DRUL-09	STM	W20b	1
19TH ST.	1937+29.29	4.72	LT	DRUL-09	SAN	--	1
19TH ST.	1939+77.11	5.47	RT	DRUL-11	STM	--	1
19TH ST.	1940+33.74	23.83	RT	DRUL-11	SAN	--	1
19TH ST.	1942+15.60	9.68	LT	DRUL-11	STM	--	1
19TH ST.	1942+78.65	17.28	RT	DRUL-11	SAN	--	1
19TH ST.	1944+92.04	35.41	RT	DRUL-13	STM	--	1
19TH ST.	1945+47.00	6.24	RT	DRUL-13	SAN	--	1
19TH ST.	1945+83.38	31.05	LT	DRUL-13	STM	--	1
19TH ST.	1948+67.34	28.22	RT	DRUL-13	STM	--	1
19TH ST.	1949+27.55	9.94	LT	DRUL-13	SAN	--	1
19TH ST.	1952+46.48	27.66	RT	DRUL-15	STM	--	1
19TH ST.	1955+68.35	56.43	RT	DRUL-15	SAN	--	1
19TH ST.	1955+71.11	34.88	RT	DRUL-15	STM	--	1
19TH ST.	1975+17.16	0.87	RT	DRUL-20	STM	--	1
19TH ST.	2057+76.59	12.57	LT	DRUL-22	SAN	--	1
19TH ST.	2058+04.87	2.86	LT	DRUL-22	STM	--	1
19TH ST.	2059+10.74	13.39	LT	DRUL-22	SAN	--	1
19TH ST.	2060+64.13	2.70	LT	DRUL-22	STM	--	1
19TH ST.	2061+21.72	18.40	LT	DRUL-22	SAN	--	1
19TH ST.	2062+59.42	24.39	LT	DRUL-23	SAN	--	1
19TH ST.	2064+03.69	27.64	LT	DRUL-23	SAN	--	1
19TH ST.	2064+27.37	31.75	LT	DRUL-23	FO	--	1
19TH ST.	2065+92.16	1.87	RT	DRUL-23	STM	Z2	1
19TH ST.	2067+16.47	3.65	RT	DRUL-23	STM	--	1
12TH AVE.	131+11.28	1.55	RT	DRUL-27	SAN	--	1
						TOTAL	55

60256910

MANHOLES TO BE ADJUSTED WITH NEW TYPE 20 FRAME AND GRATE

ALIGNMENT	STA	OFFSET	LT/RT	SHEET	TYPE	STR	EACH
19TH ST.	1924+90.11	33.00	LT	DRUL-05	STM	N38a	1
19TH ST.	1925+40.00	33.00	LT	DRUL-05	STM	N39	1
						TOTAL	2

60260100

INLETS TO BE ADJUSTED

ALIGNMENT	STA	OFFSET	LT/RT	SHEET	--	--	EACH
19TH AVE.	1928+89.63	32.75	RT	DRUL-07	--	--	1
19TH AVE.	1931+77.77	29.92	RT	DRUL-07	--	--	1
19TH AVE.	1931+78.02	29.07	LT	DRUL-07	--	--	1
19TH AVE.	1935+28.07	36.90	RT	DRUL-09	--	--	1
19TH AVE.	1935+48.18	56.73	RT	DRUL-09	--	--	1
19TH AVE.	1936+21.56	80.48	RT	DRUL-09	--	--	1
19TH AVE.	1936+34.93	59.97	RT	DRUL-09	--	--	1
19TH AVE.	1936+35.66	36.66	LT	DRUL-09	--	--	1
19TH AVE.	1936+50.81	56.93	LT	DRUL-09	--	--	1
19TH AVE.	1937+26.48	52.46	LT	DRUL-09	--	--	1
19TH AVE.	1937+75.30	30.84	LT	DRUL-09	--	--	1
19TH AVE.	1939+76.39	27.95	LT	DRUL-11	--	--	1
19TH AVE.	1939+75.93	28.65	RT	DRUL-11	--	--	1
19TH AVE.	1942+16.32	28.02	LT	DRUL-11	--	--	1
19TH AVE.	1942+25.49	28.30	RT	DRUL-11	--	--	1
19TH AVE.	1944+75.24	27.96	LT	DRUL-13	--	--	1
19TH AVE.	1944+91.28	28.06	LT	DRUL-13	--	--	1
19TH AVE.	1944+87.49	28.13	RT	DRUL-13	--	--	1
19TH AVE.	1948+75.35	27.79	LT	DRUL-13	--	--	1
19TH AVE.	1948+74.88	28.22	RT	DRUL-13	--	--	1
19TH AVE.	1952+46.25	2.48	LT	DRUL-15	--	--	1
19TH AVE.	1952+45.84	57.37	RT	DRUL-15	--	--	1
19TH AVE.	1955+60.09	27.79	LT	DRUL-15	--	--	1
19TH AVE.	1955+96.83	26.77	LT	DRUL-15	--	--	1
19TH AVE.	1956+24.32	2.34	LT	DRUL-17	--	--	1
19TH AVE.	1960+05.80	1.39	LT	DRUL-17	--	--	1
19TH AVE.	1962+51.61	2.13	LT	DRUL-18	--	--	1
19TH AVE.	1965+01.42	2.18	LT	DRUL-18	--	--	1
19TH AVE.	1967+51.01	1.82	LT	DRUL-18	--	--	1
19TH AVE.	1973+19.03	11.99	LT	DRUL-19	--	--	1
19TH AVE.	1977+94.03	2.09	LT	DRUL-20	--	--	1
19TH AVE.	1980+65.67	1.76	LT	DRUL-21	--	--	1
19TH AVE.	1983+10.94	1.92	LT	DRUL-21	--	--	1
19TH AVE.	1983+62.90	2.23	LT	DRUL-21	--	--	1
19TH AVE.	1984+14.32	2.30	LT	DRUL-21	--	--	1
19TH AVE.	2056+22.44	2.97	RT	DRUL-22	--	--	1
19TH AVE.	2057+87.89	2.30	RT	DRUL-22	--	--	1
19TH AVE.	2061+15.66	24.55	LT	DRUL-22	--	--	1
19TH AVE.	2065+91.66	21.98	RT	DRUL-23	--	--	1
19TH AVE.	2067+16.95	14.36	RT	DRUL-23	--	--	1
19TH AVE.	2074+58.25	11.95	RT	DRUL-25	--	--	1
19TH AVE.	2076+93.65	12.74	RT	DRUL-25	--	--	1
						TOTAL	42

60600095

CLASS SI CONCRETE (OUTLET)

ALIGNMENT	STA FROM	STA TO	LT/RT		STANDARD		CU YD
I-74	77+30	77+78	RT		606101		5.1
RAMP 7TH-A	632+42	632+74	RT		606201		2.2
RAMP 7TH-A	640+75	641+25	RT		606201		3.5
RAMP 7TH-A	641+68	642+14	RT		606006		3.8
RAMP 7TH-A	642+25	642+37	LT		D2 21.2		1.3
RAMP 7TH-B	530+47	530+79	RT		606201		2.2
RAMP 7TH-B	531+06	531+40	RT		606201		2.5
19TH STREET	1927+94	1928+28	LT		606201		2.5
NB 19TH ST	1969+81	1970+27	RT		606006		3.8
NB 19TH ST	1974+08	1974+54	RT		606006		3.8
SB 19TH ST	2065+80	2066+25	RT		606006		3.8
SB 19TH ST	2074+30	2074+76	LT		606006		3.8
						TOTAL	38.5

SCHQ-31

FILE NAME = ...\D2CONCD-ABC-sht-drain18AM.dgn	USER NAME = jtardy	DESIGNED -	REVISED -
		DRAWN -	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED -	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
DRAINAGE STRUCTURE REMOVALS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	94
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

Z0025505							
PROPERTY MARKERS							
							EACH
Assumed based on conflicts between old property corners and new ROW/easements							25
						TOTAL	25

Z0049300							
REFERENCING LAND SECTION MARKERS							
ALIGNMENT	STATION	OFFSET	LT/RT				EACH
12TH AVE	123+32.77	1'	RT				1
						TOTAL	1

benesch
Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

SCHQ-32

FILE NAME = ...\D2CONCD-ABC-sht-schedule09M.dgn $MODELNAME$	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES MARKERS			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRM	REVISED -					74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	95
	PLOT SCALE =	CHECKED - JJT	REVISED -							CONTRACT NO. 64E26		
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.		ILLINOIS	FED. AID PROJECT		

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312-565-0450

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70300220

TEMPORARY PAVEMENT MARKING - LINE 4"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
RAC-A	1017+07	RT	1026+70	RT		WHITE	964	STAGE 1-0
RAC-A	1016+82	RT	1026+48	RT		YELLOW	969	STAGE 1-0
EX RAMP AC-C	23+20	RT	29+18	RT		WHITE	696	STAGE 1-0
RAC-B	925+16	RT	930+09	LT		WHITE	493	STAGE 1-0A
RAC-B	925+16	LT	930+08	LT		YELLOW	492	STAGE 1-0A
19TH ST NB	1964+40	RT	1980+21	RT		WHITE	839	STAGE 1-0
19TH ST NB	1964+66	RT	1992+10	RT		YELLOW	3,071	STAGE 1-0
19TH ST NB	1967+93	RT	1975+45	RT		DOT-DASH WHITE	86	STAGE 1-0
19TH ST NB	1974+53	RT	1977+21	RT		SKIP-DASH WHITE	67	STAGE 1-0
19TH ST SB	2056+32	LT	2078+27	LT		YELLOW	2,241	STAGE 1-0
19TH ST SB	2059+49	RT	2078+25	LT		WHITE	903	STAGE 1-0
19TH ST SB	2064+53	LT	2069+09	LT		SKIP-DASH WHITE	114	STAGE 1-0
19TH ST SB	2069+09	LT	2075+35	LT		DOT-DASH WHITE	60	STAGE 1-0
PR I-74	116+62	RT	160+56	RT		YELLOW	4,871	STAGE 1-1
PR I-74	122+81	RT	160+56	RT		WHITE	3,746	STAGE 1-1
PR I-74	113+10	LT	159+00	LT		YELLOW	4,603	STAGE 1-1
PR I-74	113+49	LT	121+08	LT		WHITE	756	STAGE 1-1
PR I-74	125+12	LT	159+00	LT		WHITE	3,443	STAGE 1-1
RAC-A	1013+86	RT	1020+48	RT		YELLOW	662	STAGE 1-1
RAC-A	1013+86	RT	1025+12	RT		WHITE	1,129	STAGE 1-1
RAC-B	925+39	LT	927+79	LT		YELLOW	265	STAGE 1-1
RAC-B	925+40	LT	933+99	LT		WHITE	884	STAGE 1-1
EX RAMP AC-C	22+80	LT	28+80	RT		YELLOW	638	STAGE 1-1
EX RAMP AC-C	22+80	LT	29+18	RT		WHITE	641	STAGE 1-1
RAC-C	731+51	RT	741+54	RT		WHITE	1,003	STAGE 1-1
RAC-C	731+51	LT	741+54	LT		YELLOW	1,003	STAGE 1-1
19TH ST	1932+81	RT	1935+95	RT		WHITE	486	STAGE 1-1
19TH ST	1932+81	RT	1935+95	RT		YELLOW	375	STAGE 1-1
19TH ST NB	1964+40	RT	1978+42	RT		WHITE	1,888	STAGE 1-1
19TH ST NB	1964+66	RT	1992+10	RT		YELLOW	2,121	STAGE 1-1
19TH ST NB	1973+30	RT	1975+97	RT		DOT-DASH WHITE	42	STAGE 1-1
19TH ST NB	1974+29	RT	1975+97	RT		SKIP-DASH WHITE	42	STAGE 1-1
19TH ST SB	2059+04	RT	2079+58	LT		YELLOW	1,209	STAGE 1-1
19TH ST SB	2059+10	RT	2079+62	LT		WHITE	1,016	STAGE 1-1
19TH ST SB	2068+25	RT	2070+03	RT		DOT-DASH WHITE	74	STAGE 1-1
19TH ST SB	2068+25	RT	2079+25	LT		YELLOW	1,209	STAGE 1-1A
19TH ST SB	2075+11	LT	2078+40	LT		WHITE	341	STAGE 1-1A
PR I-74	114+00	RT	117+04	RT		WHITE	304	STAGE 1-2
PR I-74	114+00	RT	154+99	RT		YELLOW	4,099	STAGE 1-2
PR I-74	121+50	RT	154+99	RT		WHITE	3,340	STAGE 1-2
PR I-74	157+57	RT	160+56	RT		WHITE	296	STAGE 1-2
PR I-74	157+57	RT	160+56	RT		YELLOW	304	STAGE 1-2
PR I-74	114+00	LT	116+50	LT		YELLOW	250	STAGE 1-2
PR I-74	114+00	LT	116+50	LT		WHITE	250	STAGE 1-2
PR I-74	121+39	LT	136+23	LT		YELLOW	1,484	STAGE 1-2
PR I-74	124+04	LT	136+23	LT		WHITE	1,219	STAGE 1-2
PR I-74	140+14	LT	155+00	LT		YELLOW	1,494	STAGE 1-2
PR I-74	140+14	LT	155+00	LT		WHITE	1,486	STAGE 1-2
PR I-74	159+74	LT	161+29	LT		YELLOW	156	STAGE 1-2
PR I-74	159+94	LT	161+30	LT		WHITE	138	STAGE 1-2
RAC-B	922+93	LT	929+23	LT		YELLOW	638	STAGE 1-2
RAC-B	923+18	RT	932+68	LT		WHITE	953	STAGE 1-2

70300220

TEMPORARY PAVEMENT MARKING - LINE 4"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
RAC-A	1015+98	RT	1024+07	RT		WHITE	818	STAGE 1-2
RAC-A	1016+71	RT	1022+51	RT		YELLOW	583	STAGE 1-2
PR RAMP AC-C	732+71	LT	737+86	LT		WHITE	513	STAGE 1-2
PR RAMP AC-C	734+71	LT	737+84	LT		YELLOW	311	STAGE 1-2
19TH ST NB	1966+89	RT	1991+91	LT		WHITE	2,744	STAGE 1-2
19TH ST NB	1973+60	RT	1974+50	RT		DOT-DASH WHITE	22	STAGE 1-2
19TH ST NB	1974+50	RT	1979+13	RT		SKIP-DASH WHITE	116	STAGE 1-2
19TH ST SB	2060+12	RT	2078+25	LT		WHITE	1,805	STAGE 1-2
19TH ST SB	2061+61	RT	2065+83	LT		YELLOW	459	STAGE 1-2
19TH ST SB	2068+20	LT	2070+74	LT		DOT-DASH WHITE	112	STAGE 1-2
PR I-74	122+40	LT	124+04	LT		WHITE	164	STAGE 1-3
PR I-74	130+36	LT	131+40	LT		WHITE	104	STAGE 1-3
RAC-A	1017+71	LT	1026+09	RT		YELLOW	844	STAGE 1-3
RAC-A	1018+54	LT	1030+36	LT		WHITE	1,186	STAGE 1-3
RAC-C	733+61	RT	736+71	RT		WHITE	310	STAGE 1-3
RAC-C	733+61	LT	736+71	LT		YELLOW	310	STAGE 1-3
19TH ST	1913+20	RT	1928+22	RT		YELLOW	1,567	STAGE 1-3
19TH ST	1913+22	RT	1914+83	RT		SKIP-DASH WHITE	40	STAGE 1-3
19TH ST NB	1965+19	RT	1982+20	RT		WHITE	584	STAGE 1-3
19TH ST NB	1965+04	RT	1992+10	RT		YELLOW	2,527	STAGE 1-3
19TH ST NB	1972+28	RT	1972+75	RT		DOT-DASH WHITE	12	STAGE 1-3
19TH ST SB	2062+00	LT	2072+03	LT		SKIP-DASH WHITE	400	STAGE 1-3
PR I-74	110+13	RT	117+48	RT		WHITE	737	STAGE 1-4
PR I-74	110+13	RT	157+57	RT		YELLOW	4,738	STAGE 1-4
PR I-74	125+18	RT	157+57	RT		WHITE	3,219	STAGE 1-4
PR I-74	114+00	LT	125+26	LT		WHITE	1,122	STAGE 1-4
PR I-74	114+00	LT	159+94	LT		YELLOW	4,603	STAGE 1-4
PR I-74	129+78	LT	159+94	LT		WHITE	3,040	STAGE 1-4
RAC-A	1015+37	RT	1025+27	RT		YELLOW	993	STAGE 1-4
RAC-A	1017+67	LT	1029+77	LT		WHITE	1,210	STAGE 1-4
RAC-B	925+40	RT	936+39	LT		WHITE	1,100	STAGE 1-4
RAC-B	925+40	LT	928+66	LT		YELLOW	329	STAGE 1-4
19TH ST	1913+20	RT	1935+73	RT		YELLOW	2,400	STAGE 1-4
19TH ST	1913+26	RT	1950+00	RT		SKIP-DASH WHITE	1,208	STAGE 1-4
19TH ST NB	1972+28	RT	1992+10	RT		SKIP-DASH WHITE	492	STAGE 1-4
19TH ST SB	2050+00	LT	2072+03	LT		SKIP-DASH WHITE	283	STAGE 1-4
I-74	64+05	RT	73+85	RT		WHITE	1,040	STAGE 2
PR I-74	129+92	LT	136+36	LT		YELLOW	645	STAGE 2
RAMP 7-S	82+25	LT	93+00	LT		WHITE	1,152	STAGE 2
RAC-A	1017+30	LT	1029+92	LT		WHITE	1,261	STAGE 2
RAC-A	1017+30	RT	1029+92	RT		WHITE	1,263	STAGE 2
RAC-A	1017+30	RT	1029+92	RT		YELLOW	1,265	STAGE 2
19TH ST	1913+20	RT	1930+49	RT		WHITE	1,765	STAGE 2-0
19TH ST	1913+23	RT	1926+06	LT		YELLOW	1,400	STAGE 2-0
19TH ST	1913+23	RT	1914+83	RT		SKIP-DASH WHITE	40	STAGE 2-0
				70300220 SUB-TOTAL (SHEET 1)			110,688	

SCHQ-33

FILE NAME = ...\D2CONCD-ABC-sht-scheduleIIM.dgn $MODELNAME$	USER NAME = jtardy	DESIGNED - JRM	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
		DRAWN - JRM	REVISED -	
	PLOT SCALE =	CHECKED - JJT	REVISED -	
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -	

SCHEDULE OF QUANTITIES
TEMPORARY PAVEMENT MARKING

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	96
		CONTRACT NO. 64E26		
	ILLINOIS	FED. AID PROJECT		

Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

benesch

70300220

TEMPORARY PAVEMENT MARKING - LINE 4"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
19TH ST	1913+20	RT	1935+75	RT		WHITE	5,913	STAGE 2-1
19TH ST	1913+20	RT	1950+00	RT		YELLOW	5,515	STAGE 2-1
19TH ST NB	1978+01	RT	1981+17	RT		WHITE	316	STAGE 2-1
19TH ST NB	1981+17	RT	1986+95	RT		YELLOW	860	STAGE 2-1
19TH ST SB	2050+00	LT	2061+30	LT		YELLOW	966	STAGE 2-1
19TH ST SB	2054+23	LT	2055+23	LT		WHITE	100	STAGE 2-1
12TH AVE	122+46	RT	127+10	LT		WHITE	470	STAGE 2-1
12TH AVE	123+00	LT	126+16	LT		DOUBLE YELLOW	632	STAGE 2-1
12TH AVE	126+16	LT	128+33	LT		YELLOW	436	STAGE 2-1
12TH AVE	127+10	RT	128+31	RT		SKIP-DASH WHITE	30	STAGE 2-1
19TH ST	1914+56	LT	1950+00	LT		WHITE	2,930	STAGE 2-2
19TH ST	1916+27	LT	1950+00	RT		YELLOW	1,536	STAGE 2-2
19TH ST NB	1978+36	RT	1992+10	RT		YELLOW	1,360	STAGE 2-2
19TH ST SB	2050+00	LT	2052+86	LT		YELLOW	286	STAGE 2-2
19TH ST SB	2050+00	LT	2061+31	LT		WHITE	1,133	STAGE 2-2
12TH AVE	123+00	RT	128+33	RT		DOUBLE YELLOW	1,057	STAGE 2-2
12TH AVE	123+23	LT	127+50	LT		WHITE	427	STAGE 2-2
19TH ST	1919+14	RT	1928+15	RT		WHITE	1,017	STAGE 2-3
19TH ST	1919+25	RT	1934+59	RT		YELLOW	1,025	STAGE 2-3
19TH ST	1929+03	RT	1950+00	RT		SKIP-DASH WHITE	491	STAGE 2-3
19TH ST SB	2050+00	LT	2061+31	LT		SKIP-DASH WHITE	266	STAGE 2-3
12TH AVE	123+23	LT	125+73	LT		WHITE	250	STAGE 2-3
12TH AVE	123+00	RT	128+33	RT		DOUBLE YELLOW	1,057	STAGE 2-3
12TH AVE	125+73	LT	128+31	LT		SKIP-DASH WHITE	129	STAGE 2-3
PR I-74	48+99	LT	81+00	LT		WHITE	2,425	WINTER STAGE
PR I-74	48+99	LT	81+00	LT		YELLOW	3,201	WINTER STAGE
PR I-74	81+00	LT	91+00	LT		WHITE	997	WINTER 2019-2020
PR I-74	81+00	LT	140+94	LT		YELLOW	5,987	WINTER 2019-2020
PR I-74	97+76	LT	123+37	LT		WHITE	2,561	WINTER 2019-2020
RAMP 7TH-A	620+00	LT	627+03	LT		WHITE	703	WINTER STAGE
RAMP 7TH-A	627+83	LT	638+87	LT		SKIP-DASH WHITE	269	WINTER STAGE
							-	
RAC-D	820+00		832+37			WHITE	1,237	WINTER 2019-2020
RAC-D	826+64	RT	833+71	RT		YELLOW	711	WINTER 2019-2020
RAC-A	1015+37	LT	1024+02	LT		YELLOW	867	WINTER 2019-2020
RAC-A	1019+07		1029+78			WHITE	1,071	WINTER 2019-2020
							-	
19TH ST	1915+07	LT	1928+44	LT		WHITE	1,290	STAGE 3-0
19TH ST	1915+07	LT	1916+42	LT		DOT-DASH WHITE	34	STAGE 3-0
19TH ST	1916+42	LT	1921+27	RT		YELLOW	487	STAGE 3-0
PR I-74	48+86	LT	81+00	LT		WHITE	5,709	STAGE 3
PR I-74	48+92	LT	81+00	LT		YELLOW	6,414	STAGE 3
PR I-74	81+00	LT	91+48	LT		WHITE	1,043	STAGE 3-1
PR I-74	81+00	LT	134+09	LT		YELLOW	5,280	STAGE 3-1
PR I-74	81+00	LT	120+13	LT		WHITE	3,911	STAGE 3-1
PR I-74	81+00	LT	120+13	LT		YELLOW	3,900	STAGE 3-1
PR I-74	97+76	LT	123+37	LT		WHITE	2,561	STAGE 3-1
PR I-74	127+59	LT	134+09	LT		WHITE	650	STAGE 3-1
PR I-74	131+02	RT	141+41	RT		YELLOW	1,039	STAGE 3-1
PR I-74	137+50	RT	141+41	RT		WHITE	391	STAGE 3-1

70300220

TEMPORARY PAVEMENT MARKING - LINE 4"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
RAMP 7TH-A	620+84	LT	627+03	LT		WHITE	620	STAGE 3
RAMP 7TH-A	625+10	LT	627+83	LT		SKIP-DASH WHITE	69	STAGE 3
							-	
RAC-A	1020+87	RT	1023+40	RT		YELLOW	253	STAGE 3-1
RAC-A	1024+02	-	1027+59	-		WHITE	357	STAGE 3-1
							-	
RAC-B	921+84	LT	933+18	LT		YELLOW	1,134	STAGE 3-1
RAC-B	922+42	RT	933+18	RT		WHITE	1,076	STAGE 3-1
							-	
RAC-D	820+47	RT	826+62	RT		WHITE	615	STAGE 3-1
RAC-D	825+83	RT	826+62	RT		WHITE	80	STAGE 3-1
19TH ST	1913+24	LT	1935+75	RT		WHITE	2,664	STAGE 3-1
19TH ST	1914+56	RT	1950+00	RT		YELLOW	5,896	STAGE 3-1
19TH ST	1914+83	RT	1920+01	LT		SKIP-DASH WHITE	172	STAGE 3-1
19TH ST SB	2050+00	LT	2077+95	LT		YELLOW	2,642	STAGE 3-1
19TH ST SB	2054+23	LT	2078+00	LT		WHITE	782	STAGE 3-1
19TH ST SB	2068+48	RT	2070+03	RT		DOT-DASH WHITE	68	STAGE 3-1
12TH AVE	121+60	RT	127+09	RT		WHITE	709	STAGE 3-1
12TH AVE	122+18	LT	126+16	LT		DOUBLE YELLOW	796	STAGE 3-1
12TH AVE	126+16	LT	128+33	LT		YELLOW	440	STAGE 3-1
RAC-B	921+04	LT	942+24	LT		YELLOW	2,120	STAGE 3-2
RAC-B	921+41	LT	942+24	LT		WHITE	2,083	STAGE 3-2
19TH ST	1913+20	RT	1950+00	RT		WHITE	3,061	STAGE 3-2
19TH ST	1913+20	RT	1913+30	RT		SKIP-DASH WHITE	6	STAGE 3-2
19TH ST	1913+23	RT	1950+00	RT		YELLOW	4,729	STAGE 3-2
19TH ST	1916+71	LT	1917+38	LT		DOT-DASH WHITE	25	STAGE 3-2
19TH ST	1919+93	LT	1929+61	LT		DOUBLE YELLOW	1,925	STAGE 3-2
19TH ST SB	2050+00	LT	2075+36	LT		YELLOW	1,034	STAGE 3-2
19TH ST SB	2050+00	LT	2077+92	LT		WHITE	1,992	STAGE 3-2
12TH AVE	123+00	RT	128+33	RT		DOUBLE YELLOW	1,066	STAGE 3-2
12TH AVE	123+23	LT	126+80	LT		WHITE	427	STAGE 3-2
12TH AVE	125+73	RT	128+31	LT		SKIP-DASH WHITE	20	STAGE 3-2
19TH ST	1913+20	RT	1933+98	LT		WHITE	2,787	STAGE 3-3
19TH ST	1913+30	RT	1935+75	RT		SKIP-DASH WHITE	74	STAGE 3-3
19TH ST	1914+15	RT	1933+99	RT		YELLOW	2,180	STAGE 3-3
19TH ST	1915+55	LT	1917+35	LT		DOT-DASH WHITE	45	STAGE 3-3
19TH ST	1919+04	RT	1929+69	RT		DOUBLE YELLOW	2,141	STAGE 3-3
PR I-74	49+11	RT	81+00	RT	EB	YELLOW	3,188	STAGE 3-4
PR I-74	81+00	RT	137+51	RT		YELLOW	5,651	STAGE 3-4
PR I-74	133+78	RT	137+51	RT		WHITE	372	STAGE 3-4
PR I-74	133+78	RT	137+51	RT		WHITE	372	STAGE 3-4
PR I-74	121+00	LT	133+78	LT		YELLOW	1,278	STAGE 3-4
19TH ST	1914+71	LT	1915+72	LT		DOT-DASH WHITE	25	STAGE 3-4
19TH ST	1914+71	RT	1935+68	RT		YELLOW	3,962	STAGE 3-4
19TH ST	1926+63	RT	1928+76	RT		WHITE	194	STAGE 3-4
						70300220 SUB-TOTAL (SHEET 2)	140,070	
						TOTAL	250,758	
						COLOR SUBTOTALS		
						YELLOW:	138,179	
						WHITE:	112,579	

SCHQ-34

FILE NAME = ...\D2CONCD-ABC-sht-schedule12M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**SCHEDULE OF QUANTITIES TEMPORARY PAVEMENT MARKING**
		DRAWN - JRM	REVISED -		
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	97
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

benesch
Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

70300210

TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS

ALIGNMENT	STA	OFFSET	LT/RT	DESCRIPTION	-	-	SQ FT	NOTES:
19TH ST	1933+15	9	RT	TURN ARROW			15.6	STAGE 1-1 TO 1-2
19TH ST	1933+15	19	RT	TURN ARROW			15.6	STAGE 1-1 TO 1-2
19TH ST	1934+58	10	RT	TURN ARROW			15.6	STAGE 1-1 TO 1-2
19TH ST	1934+58	20	RT	TURN ARROW			15.6	STAGE 1-1 TO 1-2
NB 19TH ST	1972+41	17	RT	TURN ARROW			15.6	STAGE 1-3
19TH ST	1927+48	29	RT	TURN ARROW			15.6	STAGE 2-1 TO 2-2
19TH ST	1928+23	18	RT	TURN ARROW			15.6	STAGE 2-1 TO 2-2
19TH ST	1935+54	10	RT	TURN ARROW			15.6	STAGE 2-1 TO 2-2
SB 19TH ST	2054+47	18	LT	TURN ARROW			15.6	STAGE 2-1
SB 19TH ST	2055+07	18	LT	TURN ARROW			15.6	STAGE 2-1
19TH ST	1915+51	6	LT	TURN ARROW			15.6	STAGE 2-2 TO W.S.
19TH ST	1927+09	8	RT	TURN ARROW			15.6	STAGE 2-3 TO 3-1
19TH ST	1927+93	8	RT	TURN ARROW			15.6	STAGE 2-3 TO 3-1
19TH ST	1915+51	6	LT	TURN ARROW			15.6	STAGE 3-1 TO 3-3
19TH ST	1916+61	6	LT	TURN ARROW			15.6	STAGE 3-1 TO 3-2
19TH ST	1935+54	10	RT	TURN ARROW			15.6	STAGE 3-1 TO 3-2
SB 19TH ST	2054+45	16	LT	TURN ARROW			15.6	STAGE 3-1
SB 19TH ST	2055+04	16	LT	TURN ARROW			15.6	STAGE 3-1
19TH ST	1937+20	9	LT	TURN ARROW			15.6	STAGE 3-2 TO 3-4
19TH ST	1937+93	10	LT	TURN ARROW			15.6	STAGE 3-2 TO 3-4
19TH ST	1926+76	17	RT	TURN ARROW			15.6	STAGE 3-4
19TH ST	1928+00	16	RT	TURN ARROW			15.6	STAGE 3-4
						TOTAL	344	

COLOR SUBTOTALS

YELLOW: 0

WHITE: 344

70300280

TEMPORARY PAVEMENT MARKING - LINE 24"

ALIGNMENT	STA FR	LT/RT	DESCRIPTION	FOOT	NOTES
NB 19TH ST	1973+30	LT	WHITE STOP BAR	16	STAGE 1-2
11TH AVE	1100+57	RT	WHITE STOP BAR	11	STAGE 3-4
			TOTAL	27	

COLOR SUBTOTALS

YELLOW: 0

WHITE: 27

70300240

TEMPORARY PAVEMENT MARKING - LINE 6"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
PR I-74	111+22	RT	116+62	RT		YELLOW	540	STAGE 1-1
PR I-74	111+22	RT	116+61	RT		WHITE	539	STAGE 1-1
PR I-74	111+22	RT	116+61	RT		WHITE	539	STAGE 1-1
PR I-74	117+61	RT	160+56	RT		SKIP DASH LANE LINE	1074	STAGE 1-1
PR I-74	113+29	LT	159+00	LT		SKIP DASH LANE LINE	1143	STAGE 1-1
PR I-74	109+45	RT	114+00	RT		YELLOW	455	STAGE 1-2
PR I-74	109+45	RT	114+00	RT		WHITE	455	STAGE 1-2
PR I-74	109+45	RT	114+00	RT		WHITE	455	STAGE 1-2
PR I-74	114+00	RT	155+00	RT		SKIP DASH LANE LINE	1025	STAGE 1-2
PR I-74	155+00	RT	157+57	RT		YELLOW	257	STAGE 1-2
PR I-74	155+00	RT	157+57	RT		WHITE	257	STAGE 1-2
PR I-74	155+00	RT	157+57	RT		WHITE	257	STAGE 1-2
PR I-74	157+57	RT	160+56	RT		SKIP DASH LANE LINE	75	STAGE 1-2
PR I-74	114+00	LT	116+50	LT		SKIP DASH LANE LINE	63	STAGE 1-2
PR I-74	116+50	LT	121+39	LT		YELLOW	489	STAGE 1-2
PR I-74	116+50	LT	121+39	LT		WHITE	489	STAGE 1-2
PR I-74	116+50	LT	121+39	LT		WHITE	489	STAGE 1-2
PR I-74	121+39	LT	136+23	LT		SKIP DASH LANE LINE	371	STAGE 1-2
PR I-74	136+23	LT	140+13	LT		YELLOW	130	STAGE 1-2
PR I-74	136+23	LT	140+13	LT		WHITE	130	STAGE 1-2
PR I-74	136+23	LT	140+13	LT		WHITE	130	STAGE 1-2
PR I-74	140+13	LT	155+00	LT		SKIP DASH LANE LINE	372	STAGE 1-2
PR I-74	155+00	LT	159+84	LT		YELLOW	484	STAGE 1-2
PR I-74	155+00	LT	159+84	LT		WHITE	484	STAGE 1-2
PR I-74	155+00	LT	159+84	LT		WHITE	484	STAGE 1-2
PR I-74	159+84	LT	161+30	LT		SKIP DASH LANE LINE	36	STAGE 1-2
PR I-74	110+13	RT	157+57	RT		SKIP DASH LANE LINE	1186	STAGE 1-4
PR I-74	114+00	LT	159+94	LT		SKIP DASH LANE LINE	1149	STAGE 1-4
PR I-74	129+92	LT	136+36	LT		SKIP DASH LANE LINE	161	STAGE 2
PR I-74	48+92	LT	81+00	LT	WB	SKIP DASH LANE LINE	802	WINTER STAGE
PR I-74	48+96	LT	81+00	LT	WB	SKIP DASH LANE LINE	801	WINTER STAGE
PR I-74	81+00	LT	124+00	LT		SKIP DASH LANE LINE	1075	WINTER STAGE
PR I-74	81+00	LT	140+94	LT		SKIP DASH LANE LINE	1499	WINTER STAGE
RAC-A	1015+37		1017+67			SKIP DASH LANE LINE	58	WINTER STAGE
PR I-74	48+98	LT	81+00	LT	EB	SKIP DASH LANE LINE	801	STAGE 3
PR I-74	48+89	LT	81+00	LT	WB	SKIP DASH LANE LINE	803	STAGE 3
PR I-74	81+00	LT	134+09	LT		SKIP DASH LANE LINE	1327	STAGE 3-1
PR I-74	81+00	LT	120+13	LT		SKIP DASH LANE LINE	978	STAGE 3-1
PR I-74	120+13	LT	131+02	RT		YELLOW	1089	STAGE 3-1
PR I-74	120+13	LT	131+02	RT		WHITE	1089	STAGE 3-1
PR I-74	120+13	LT	133+18	RT		WHITE	1305	STAGE 3-1
PR I-74	131+02	RT	141+41	RT		SKIP DASH LANE LINE	260	STAGE 3-1
PR I-74	134+09	LT	137+59	LT		YELLOW	350	STAGE 3-1
PR I-74	134+09	LT	137+59	LT		WHITE	350	STAGE 3-1
PR I-74	134+09	LT	137+59	LT		WHITE	350	STAGE 3-1
RAC-B	933+18	LT	944+40	LT		YELLOW	1122	STAGE 3-1
RAC-B	933+18	RT	948+72	RT		WHITE	1554	STAGE 3-1
						TOTAL	29,330	
						COLOR SUBTOTALS		
						YELLOW:	4,916	
						WHITE:	24,414	

70300250

TEMPORARY PAVEMENT MARKING - LINE 8"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
PR I-74	116+61	RT	117+61	RT		WHITE GORE	100	STAGE 1-1
PR I-74	117+61	RT	122+81	RT		WHITE SKIP DASH	104	STAGE 1-1
PR I-74	121+08	LT	122+61	LT		WHITE GORE	153	STAGE 1-1
PR I-74	122+61	LT	125+12	LT		WHITE SKIP DASH	50	STAGE 1-1
RAC-A	1020+48	LT	1022+64	LT		WHITE GORE	216	STAGE 1-1
RAC-B	927+79	LT	928+78	LT		WHITE GORE	100	STAGE 1-1
PR I-74	117+04	RT	118+04	RT		WHITE GORE	100	STAGE 1-2
PR I-74	118+04	RT	121+50	RT		WHITE SKIP DASH	69	STAGE 1-2
PR I-74	121+40	RT	122+40	RT		WHITE GORE	100	STAGE 1-2
PR I-74	122+40	LT	124+04	LT		WHITE SKIP DASH	33	STAGE 1-2
RAC-A	1021+55	LT	1022+50	LT		WHITE GORE	100	STAGE 1-2
PR I-74	125+81	LT	128+48	LT		WHITE GORE	267	STAGE 1-3
RAC-A	1026+09	LT	1028+48	LT		WHITE GORE	239	STAGE 1-3
PR I-74	117+48	RT	118+97	RT		WHITE GORE	149	STAGE 1-4
RAC-B	928+66	LT	930+14	LT		WHITE GORE	148	STAGE 1-4
PR I-74	118+97	RT	125+18	RT		WHITE SKIP DASH	124	STAGE 1-4
PR I-74	125+26	LT	126+77	LT		WHITE GORE	151	STAGE 1-4
RAC-A	1025+27	LT	1026+77	LT		WHITE GORE	150	STAGE 1-4
PR I-74	62+93	LT	66+85	LT	EB	WHITE GORE	384	WINTER STAGE
PR I-74	62+73	LT	66+85	LT	EB	WHITE GORE	403	WINTER STAGE
PR I-74	62+73	LT	73+79	LT		WHITE SOLID	1,087	WINTER STAGE
PR I-74	73+79	LT	81+00	LT	EB	WHITE DOT DASH	180	WINTER STAGE
PR I-74	81+00	LT	93+04	LT		WHITE SKIP DASH	241	WINTER STAGE
PR I-74	93+04	LT	97+76	LT		WHITE GORE	472	WINTER STAGE
PR I-74	123+37	LT	126+77	LT		WHITE GORE	340	WINTER STAGE
RAC-D	822+00	RT	826+64	RT		WHITE GORE	464	WINTER STAGE
RAC-D	832+37	LT	833+66	LT		WHITE GORE	129	WINTER STAGE
RAC-A	1024+02	RT	1026+77	RT		WHITE GORE	275	WINTER STAGE
RAC-A	1017+67	LT	1019+05	LT		WHITE GORE	138	WINTER STAGE
RAC-A	1017+67	LT	1019+07	LT		WHITE GORE	140	WINTER STAGE
PR I-74	62+90	LT	65+46	LT	WB	WHITE GORE	512	STAGE 3
PR I-74	91+48	LT	96+95	LT		WHITE SKIP DASH	109	STAGE 3-1
PR I-74	96+95	LT	97+76	LT		WHITE GORE	81	STAGE 3-1
RAC-D	825+83	LT	826+62	LT		WHITE GORE	79	STAGE 3-1
PR I-74	125+10	RT	127+59	RT		WHITE SKIP DASH	50	STAGE 3-1
PR I-74	123+37	LT	125+10	LT		WHITE GORE	173	STAGE 3-1
RAC-A	1023+40	LT	1025+11	LT		WHITE GORE	171	STAGE 3-1
PR I-74	131+02	RT	133+18	RT		WHITE GORE	216	STAGE 3-1
PR I-74	131+02	RT	133+18	RT		WHITE GORE	216	STAGE 3-1
PR I-74	133+18	RT	137+50	RT		WHITE SKIP DASH	86	STAGE 3-1
						TOTAL	8,302	
						COLOR SUBTOTALS		
						YELLOW:		
						WHITE:	8,302	

70300260

TEMPORARY PAVEMENT MARKING - LINE 12"

ALIGNMENT	STA FR	LT/RT	STA TO	LT/RT	-	DESCRIPTION	FOOT	NOTES:
PR I-74	125+81	LT	128+48	LT		WHITE GORE	100	STAGE 1-3
PR I-74	112+00	RT	136+00	RT		MEDIAN YELLOW	483	STAGE 1-4
PR I-74	116+50	LT	136+00	LT		MEDIAN YELLOW	407	STAGE 1-4
PR I-74	117+48	RT	118+97	RT		WHITE GORE	65	STAGE 1-4
PR I-74	125+26	LT	126+77	LT		WHITE GORE	53	STAGE 1-4
PR I-74	137+50	RT	154+00	RT		MEDIAN YELLOW	407	WINTER STAGE 2018
PR I-74	137+50	LT	154+00	LT		MEDIAN YELLOW	417	WINTER STAGE 2018
PR I-74	93+04	LT	97+76	LT		WHITE GORE	147	WINTER STAGE
PR I-74	123+37	LT	126+77	LT		WHITE GORE	168	WINTER STAGE
RAC-D	832+37	RT	833+66	RT		WHITE GORE	63	WINTER STAGE
RAC-A	1017+67	RT	1019+07	RT		WHITE GORE	50	WINTER STAGE
RAMP 7TH-A	623+84	LT	627+62	LT	-	WHITE GORE	152	WINTER STAGE
RAMP 7TH-A	625+51	LT	627+30	LT	-	WHITE GORE	95	STAGE 3
						TOTAL	2,608	
						COLOR SUBTOTALS		
						YELLOW:	1,714	
						WHITE:	894	

SCH0-35

FILE NAME = ...\D2CONCD-ABC-sht-schedule13M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
TEMPORARY PAVEMENT MARKING

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	98
		CONTRACT NO. 64E26		
	ILLINOIS FED. AID PROJECT			

Alfred Benesch & Company
200 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

benesch

X0327980								
PAVEMENT MARKING REMOVAL - WATER BLASTING								NOTES:
ALIGNMENT	STA FR	OFFSET LT/RT	STA TO	OFFSET LT/RT	LINE WIDTH (IN)	DESCRIPTION	SQ FT	
NB 19TH ST	1967+39	RT	1970+91	RT	4	4" PAVEMENT MARKING	29	REMOVE STAGE 1-0
NB 19TH ST	1972+29	RT	1992+10	RT	4	4" PAVEMENT MARKING	320	REMOVE STAGE 1-0
NB 19TH ST	1974+34	RT	1974+43	LT		TURN ARROW	16	REMOVE STAGE 1-0
SB 19TH ST	2056+32	LT	2078+27	LT	4	4" PAVEMENT MARKING	312	REMOVE STAGE 1-0
I-74	109+45	RT	117+32	RT	4	WHITE	263	STAGE 1-1
I-74	109+45	RT	120+00	RT	4	YELLOW	352	STAGE 1-1
I-74	109+45	RT	120+00	RT	6	SKIP DASH LANE LINE	132	STAGE 1-1
I-74	113+10	LT	122+98	LT	4	WHITE	329	STAGE 1-1
I-74	113+10	LT	122+00	LT	4	YELLOW	297	STAGE 1-1
I-74	113+10	LT	122+00	LT	6	SKIP DASH LANE LINE	111	STAGE 1-1
I-74	117+32	RT	118+93	RT	8	WHITE	107	STAGE 1-1
I-74	118+93	RT	160+55	RT	4	WHITE	1387	STAGE 1-1
I-74	122+98	LT	126+47	LT	8	WHITE	232	STAGE 1-1
I-74	126+47	LT	159+00	LT	4	WHITE	1084	STAGE 1-1
I-74	140+13	LT	159+00	LT	6	SKIP DASH LANE LINE	236	STAGE 1-1
I-74	155+00	LT	159+00	LT	4	YELLOW	133	STAGE 1-1
I-74	155+00	RT	160+55	RT	4	YELLOW	185	STAGE 1-1
I-74	155+00	RT	160+55	RT	6	SKIP DASH LANE LINE	69	STAGE 1-1
RAC-A	1016+82	RT	1026+48	RT	4	YELLOW	322	STAGE 1-1
RAC-A	1017+07	RT	1026+70	RT	4	WHITE	321	STAGE 1-1
RAC-A	1017+07	LT	1023+05	LT	4	YELLOW	199	STAGE 1-1
RAC-A	1017+07	RT	1026+48	RT	4	WHITE	314	STAGE 1-1
RAC-A	1023+05	LT	1026+48	LT	8	WHITE	228	STAGE 1-1
19TH ST	1932+81	RT	1935+72	RT	4	4" PAVEMENT MARKING	48	REMOVE STAGE 1-1
19TH ST	1935+17	RT	1935+70	RT		THROUGH RIGHT ARROW	52	REMOVE STAGE 1-1
NB 19TH ST	1964+40	RT	1970+89	RT	4	4" PAVEMENT MARKING	202	REMOVE STAGE 1-1
NB 19TH ST	1972+30	RT	1992+10	LT	4	4" PAVEMENT MARKING	822	REMOVE STAGE 1-1
NB 19TH ST	1972+78	RT	1973+93	LT	4	4" PAVEMENT MARKING	38	REMOVE STAGE 1-1
NB 19TH ST	1974+34	RT	1974+45	RT		THROUGH ARROW	23	REMOVE STAGE 1-1
SB 19TH ST	2059+49	RT	2078+27	LT	4	4" PAVEMENT MARKING	615	REMOVE STAGE 1-1
SB 19TH ST	2063+48	LT	2063+84	LT	4	4" PAVEMENT MARKING	12	REMOVE STAGE 1-1
SB 19TH ST	2070+03	RT	2078+40	LT	4	4" PAVEMENT MARKING	220	REMOVE STAGE 1-1A
SB 19TH ST	2078+21	LT	2078+85	LT	4	4" PAVEMENT MARKING	7	REMOVE STAGE 1-1A
PR I-74	111+22	RT	116+62	RT	6	YELLOW	270	STAGE 1-2
PR I-74	111+22	RT	116+62	RT	6	WHITE	270	STAGE 1-2
PR I-74	111+22	RT	117+62	RT	6	WHITE	320	STAGE 1-2
PR I-74	113+29	LT	159+00	LT	6	SKIP DASH LANE LINE	571	STAGE 1-2
PR I-74	116+62	RT	160+56	RT	6	SKIP DASH LANE LINE	549	STAGE 1-2
PR I-74	117+61	RT	122+81	RT	8	WHITE SKIP DASH	87	STAGE 1-2
PR I-74	122+61	LT	125+12	LT	8	WHITE SKIP DASH	42	STAGE 1-2
RAC-A	1013+86	RT	1022+66	RT	4	YELLOW	293	STAGE 1-2
RAC-A	1013+86	RT	1025+12	RT	4	WHITE	375	STAGE 1-2
RAC-B	925+39	LT	928+78	LT	4	YELLOW	113	STAGE 1-2
RAC-B	925+40	LT	933+99	LT	4	WHITE	286	STAGE 1-2

X0327980								
PAVEMENT MARKING REMOVAL - WATER BLASTING								NOTES:
ALIGNMENT	STA FR	OFFSET LT/RT	STA TO	OFFSET LT/RT	LINE WIDTH (IN)	DESCRIPTION	SQ FT	
NB 19TH ST	1964+40	RT	1992+10	RT	4	4" PAVEMENT MARKING	875	REMOVE STAGE 1-2
NB 19TH ST	1972+14	LT	1972+67	RT	4	4" PAVEMENT MARKING	35	REMOVE STAGE 1-2
NB 19TH ST	1972+14	LT	1972+67	RT	12	12" PAVEMENT MARKING	76	REMOVE STAGE 1-2
SB 19TH ST	2059+04	RT	2079+62	RT	4	4" PAVEMENT MARKING	1183	REMOVE STAGE 1-2
SB 19TH ST	2078+00	LT	2079+25	LT	4	4" PAVEMENT MARKING	9	REMOVE STAGE 1-2
PR I-74	122+40	LT	124+04	LT	4	WHITE	55	STAGE 1-3
PR I-74	130+36	LT	131+40	LT	4	WHITE	35	STAGE 1-3
19TH ST	1920+75	RT	1929+35	LT	4	4" PAVEMENT MARKING	74	REMOVE STAGE 1-3
19TH ST	1913+23	RT	1920+75	LT	4	4" PAVEMENT MARKING	120	REMOVE STAGE 1-3
NB 19TH ST	1966+89	RT	1986+99	LT	4	4" PAVEMENT MARKING	658	REMOVE STAGE 1-3
NB 19TH ST	1973+30	LT			24	WHITE STOP BAR	32	REMOVE STAGE 1-3
SB 19TH ST	2060+12	RT	2078+25	LT	4	4" PAVEMENT MARKING	1015	REMOVE STAGE 1-3
PR I-74	109+45	RT	114+00	RT	6	YELLOW	228	STAGE 1-4
PR I-74	109+45	RT	114+00	RT	6	WHITE	228	STAGE 1-4
PR I-74	109+45	RT	114+00	RT	6	WHITE	228	STAGE 1-4
PR I-74	114+00	LT	116+50	LT	4	YELLOW	83	STAGE 1-4
PR I-74	114+00	LT	116+50	LT	4	WHITE	83	STAGE 1-4
PR I-74	114+00	LT	116+50	LT	6	SKIP DASH LANE LINE	31	STAGE 1-4
PR I-74	114+00	RT	155+00	RT	6	SKIP DASH LANE LINE	513	STAGE 1-4
PR I-74	114+00	RT	118+04	RT	4	WHITE	135	STAGE 1-4
PR I-74	114+00	RT	154+99	RT	4	YELLOW	1366	STAGE 1-4
PR I-74	116+50	LT	121+39	LT	6	YELLOW	245	STAGE 1-4
PR I-74	116+50	LT	121+39	LT	6	WHITE	245	STAGE 1-4
PR I-74	116+50	LT	121+39	LT	6	WHITE	245	STAGE 1-4
PR I-74	118+04	RT	121+50	RT	8	WHITE SKIP DASH	58	STAGE 1-4
PR I-74	121+39	LT	136+23	LT	4	YELLOW	495	STAGE 1-4
PR I-74	121+39	LT	122+40	LT	4	WHITE	34	STAGE 1-4
PR I-74	121+39	LT	136+23	LT	6	SKIP DASH LANE LINE	185	STAGE 1-4
PR I-74	121+50	RT	154+99	RT	4	WHITE	1116	STAGE 1-4
PR I-74	122+40	LT	124+04	LT	8	WHITE SKIP DASH	27	STAGE 1-4
PR I-74	124+04	LT	136+23	LT	4	WHITE	406	STAGE 1-4
PR I-74	125+81	LT	128+48	LT	8	WHITE GORE	178	STAGE 1-4
PR I-74	136+23	LT	140+13	LT	6	YELLOW	195	STAGE 1-4
PR I-74	136+23	LT	140+13	LT	6	WHITE	195	STAGE 1-4
PR I-74	136+23	LT	140+13	LT	6	WHITE	195	STAGE 1-4
PR I-74	140+13	LT	155+00	LT	6	SKIP DASH LANE LINE	186	STAGE 1-4
PR I-74	140+14	LT	155+00	LT	4	YELLOW	495	STAGE 1-4
PR I-74	140+14	LT	155+00	LT	4	WHITE	495	STAGE 1-4
PR I-74	155+00	RT	157+57	RT	6	WHITE	129	STAGE 1-4
PR I-74	155+00	RT	157+57	RT	6	WHITE	129	STAGE 1-4
PR I-74	155+00	LT	159+84	LT	6	WHITE	242	STAGE 1-4
PR I-74	155+00	LT	159+84	LT	6	WHITE	242	STAGE 1-4
PR I-74	157+57	RT	160+56	RT	4	WHITE	100	STAGE 1-4
PR I-74	157+57	RT	160+56	RT	4	YELLOW	100	STAGE 1-4
PR I-74	157+57	RT	160+56	RT	6	SKIP DASH LANE LINE	37	STAGE 1-4
PR I-74	159+74	LT	161+29	LT	4	YELLOW	52	STAGE 1-4
PR I-74	159+84	LT	161+30	LT	6	SKIP DASH LANE LINE	18	STAGE 1-4
PR I-74	159+94	LT	161+30	LT	4	WHITE	45	STAGE 1-4
RAC-A	1017+71	LT	1026+09	RT	4	YELLOW	279	STAGE 1-4
RAC-A	1018+54	LT	1030+36	LT	4	WHITE	394	STAGE 1-4
RAC-A	1026+09	LT	1028+48	LT	8	WHITE GORE	159	STAGE 1-4
						X0327980 SUB-TOTAL (SHEET 1)	26,177	

SCHO-36

FILE NAME = ...\D2CONCD-ABC-sht-schedule14M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES TEMPORARY PAVEMENT MARKING	F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JRM	REVISED -			74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	99
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -						CONTRACT NO. 64E26	
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			ILLINOIS	FED. AID PROJECT	

Alfred Benesch & Company
205 North Michigan Avenue, Suite 2400
Chicago, Illinois 60601
312-565-0450

benesch

X0327980

PAVEMENT MARKING REMOVAL - WATER BLASTING

ALIGNMENT	STA FR	OFFSET LT/RT	STA TO	OFFSET LT/RT	LINE WIDTH (IN)	DESCRIPTION	SQ FT	NOTES:
RAC-B	922+93	LT	929+23	LT	4	YELLOW	210	STAGE 1-4
RAC-B	923+18	RT	932+68	LT	4	WHITE	317	STAGE 1-4
19TH ST	1913+23	RT	1928+34	RT	4	4" PAVEMENT MARKING	526	REMOVE STAGE 1-4
NB 19TH ST	1965+04	RT	1992+10	LT	4	4" PAVEMENT MARKING	937	REMOVE STAGE 1-4
NB 19TH ST	1972+41	RT				TURN ARROW	16	REMOVE STAGE 1-4
19TH ST	1913+20	RT	1929+35	RT	4	4" PAVEMENT MARKING	1856	REMOVE STAGE 2-0
PR I-74	114+00	LT	125+26	LT	4	WHITE	375	STAGE 2
PR I-74	114+00	LT	159+94	LT	4	YELLOW	1531	STAGE 2
PR I-74	117+48	RT	118+97	RT	8	WHITE GORE	100	STAGE 2
PR I-74	118+97	RT	125+18	RT	8	WHITE SKIP DASH	104	STAGE 2
PR I-74	125+26	LT	126+77	LT	8	WHITE GORE	100	STAGE 2
PR I-74	129+78	LT	159+94	LT	4	WHITE	1005	STAGE 2
RAC-A	1015+37	RT	1025+27	RT	4	YELLOW	330	STAGE 2
RAC-A	1017+67	LT	1029+77	LT	4	WHITE	403	STAGE 2
RAC-A	1025+27	LT	1026+77	LT	8	WHITE GORE	100	STAGE 2
RAC-B	928+66	LT	930+14	LT	8	WHITE GORE	99	STAGE 2
19TH ST	1911+55	RT	1950+00	RT	4	4" PAVEMENT MARKING	1795	REMOVE STAGE 2-1
19TH ST	1915+50	LT	1927+91	RT		TURN ARROW	47	REMOVE STAGE 2-1
NB 19TH ST	1978+01	RT	1992+10	LT	4	4" PAVEMENT MARKING	75	REMOVE STAGE 2-1
SB 19TH ST	2050+00	LT	2061+30	LT	4	4" PAVEMENT MARKING	89	REMOVE STAGE 2-1
12TH AVE	123+00	LT	128+30	LT	4	4" PAVEMENT MARKING	559	REMOVE STAGE 2-1
19TH ST	1914+56	LT	1950+00	RT	4	4" PAVEMENT MARKING	2129	REMOVE STAGE 2-2
19TH ST	1920+75	RT	1927+82	LT	4	4" PAVEMENT MARKING	24	REMOVE STAGE 2-2
NB 19TH ST	1978+01	RT	1992+10	LT	4	4" PAVEMENT MARKING	193	REMOVE STAGE 2-2
SB 19TH ST	2050+00	LT	2061+30	LT	4	4" PAVEMENT MARKING	322	REMOVE STAGE 2-2
SB 19TH ST	2054+45	LT	2055+04	LT	4	4" PAVEMENT MARKING	31	REMOVE STAGE 2-2
12TH AVE	122+46	RT	128+33	LT	4	4" PAVEMENT MARKING	462	REMOVE STAGE 2-2
19TH ST	1918+89	RT	1950+00	RT	4	4" PAVEMENT MARKING	1179	REMOVE STAGE 2-3
19TH ST	1927+48	RT	1935+54	RT		TURN ARROW	47	REMOVE STAGE 2-3
SB 19TH ST	2050+00	LT	2061+31	LT	4	4" PAVEMENT MARKING	473	REMOVE STAGE 2-3
12TH AVE	123+23	LT	127+50	LT	4	4" PAVEMENT MARKING	142	REMOVE STAGE 2-3
NB 19TH ST	1978+01	RT	1986+95	RT	4	4" PAVEMENT MARKING	392	REMOVE WINTER STAGE
RAC-A	1017+30	LT	1029+92	LT	4	WHITE	421	WINTER 2019-2020
RAC-A	1017+30	RT	1029+92	RT	4	WHITE	421	WINTER 2019-2020
RAC-A	1017+30	RT	1029+92	RT	4	YELLOW	421	WINTER 2019-2020
PR I-74	129+92	LT	136+36	LT	6	SKIP DASH LANE LINE	81	WINTER 2019-2020
19TH ST	1915+07	LT	1928+44	LT	4	4" PAVEMENT MARKING	509	REMOVE STAGE 3-0
19TH ST	1915+51	LT				TURN ARROW	16	REMOVE STAGE 3-0

X0327980

PAVEMENT MARKING REMOVAL - WATER BLASTING

ALIGNMENT	STA FR	OFFSET LT/RT	STA TO	OFFSET LT/RT	LINE WIDTH (IN)	DESCRIPTION	SQ FT	NOTES:
RAMP 7-S	82+25	LT	93+00	LT	4	4" PAVEMENT MARKING	384	REMOVE STAGE 3
PR I-74	48+99	LT	81+00	LT	4	4" PAVEMENT MARKING	2222	REMOVE STAGE 3
PR I-74	48+92	LT	81+00	LT	6	6" PAVEMENT MARKING	800	REMOVE STAGE 3
PR I-74	62+73	LT	81+00	LT	8	8" PAVEMENT MARKING	1370	REMOVE STAGE 3
RAMP 7TH-A	620+00	LT	627+03	LT	4	4" PAVEMENT MARKING	324	REMOVE STAGE 3
RAMP 7TH-A	623+84	LT	627+62	LT	12	12" PAVEMENT MARKING	152	REMOVE STAGE 3
PR I-74	81+00	LT	93+04	LT	8	WHITE SKIP DASH	201	STAGE 3-1
PR I-74	93+04	LT	97+76	LT	8	WHITE GORE	315	STAGE 3-1
PR I-74	123+37	LT	126+77	LT	8	WHITE GORE	227	STAGE 3-1
PR I-74	81+00	LT	91+00	LT	4	WHITE	333	STAGE 3-1
PR I-74	81+00	LT	140+94	LT	4	YELLOW	1998	STAGE 3-1
PR I-74	81+00	LT	124+00	LT	6	SKIP DASH LANE LINE	538	STAGE 3-1
PR I-74	81+00	LT	140+94	LT	6	SKIP DASH LANE LINE	749	STAGE 3-1
PR I-74	97+76	LT	123+37	LT	4	WHITE	854	STAGE 3-1
PR I-74	129+92	LT	136+36	LT	4	YELLOW	215	STAGE 3-1
RAC-A	1015+37		1017+67		6	SKIP DASH LANE LINE	29	STAGE 3-1
RAC-A	1017+67	LT	1019+05	LT	8	WHITE GORE	92	STAGE 3-1
RAC-A	1017+67	LT	1019+07	LT	8	WHITE GORE	93	STAGE 3-1
RAC-A	1024+02	RT	1026+77	RT	8	WHITE GORE	184	STAGE 3-1
RAC-B	925+16	LT	928+48	LT	4	YELLOW	111	STAGE 3-1
RAC-B	925+16	RT	930+09	RT	4	WHITE	164	STAGE 3-1
RAC-B	925+40	RT	936+39	LT	4	WHITE	366	STAGE 3-1
RAC-B	928+48	LT	930+09	LT	8	WHITE	108	STAGE 3-1
RAC-D	832+37	LT	833+66	LT	8	WHITE GORE	86	STAGE 3-1
19TH ST	1914+56	RT	1950+00	RT	4	4" PAVEMENT MARKING	1776	REMOVE STAGE 3-1
SB 19TH ST	2050+00	LT	2072+03	LT	4	4" PAVEMENT MARKING	263	REMOVE STAGE 3-1
12TH AVE	123+23	LT	128+31	LT	4	4" PAVEMENT MARKING	482	REMOVE STAGE 3-1
RAC-A	1020+87	RT	1025+10	RT	4	YELLOW	141	STAGE 3-2
RAC-A	1024+02	-	1027+59	-	4	WHITE	119	STAGE 3-2
19TH ST	1913+20	RT	1950+00	RT	4	4" PAVEMENT MARKING	2440	REMOVE STAGE 3-2
SB 19TH ST	2050+00	LT	2077+95	LT	4	4" PAVEMENT MARKING	648	REMOVE STAGE 3-2
SB 19TH ST	2054+45	LT	2055+04	LT		TURN ARROW	31	REMOVE STAGE 3-2
12TH AVE	121+60	RT	128+33	LT	4	4" PAVEMENT MARKING	647	REMOVE STAGE 3-2
19TH ST	1913+20	RT	1950+00	RT	4	4" PAVEMENT MARKING	1867	REMOVE STAGE 3-3
19TH ST	1916+61	LT				TURN ARROW	16	REMOVE STAGE 3-3
SB 19TH ST	2050+00	LT	2078+00	LT	4	4" PAVEMENT MARKING	1450	REMOVE STAGE 3-3
PR I-74	48+95	LT	81+00	LT	4	4" PAVEMENT MARKING	2137	REMOVE STAGE 3-4
PR I-74	48+98	LT	81+00	LT	6	6" PAVEMENT MARKING	401	REMOVE STAGE 3-4
PR I-74	131+02	RT	141+41	RT	4	YELLOW	346	STAGE 3-4
PR I-74	137+50	RT	141+41	RT	4	WHITE	130	STAGE 3-4
RAC-B	921+04	LT	942+24	LT	4	YELLOW	707	STAGE 3-4
RAC-B	921+41	LT	942+24	LT	4	WHITE	694	STAGE 3-4
						X0327980 SUB-TOTAL (SHEET 2)	44,045	

SCHQ-37

FILE NAME = ...\D2CONCD-ABC-sht-schedule15M.dgn	USER NAME = jtardy	DESIGNED - JRM	REVISED -
		DRAWN - JRM	REVISED -
$MODELNAME$	PLOT SCALE =	CHECKED - JJT	REVISED -
	PLOT DATE = 5/5/2017	DATE - 3/23/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
TEMPORARY PAVEMENT MARKING

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(81-1)R-1 & 81-1(HBR, HBR-1, HBR-2)	ROCK ISLAND	2042	100
			CONTRACT NO. 64E26	
		ILLINOIS	FED. AID PROJECT	