

**FAI 24 (I-24)**  
**Contract No. 78972**  
**Massac County**  
**Fed. Proj. No. BR-DIKS(922)**  
Item [Click here to enter text.](#)

**IDOT PROJECT LABOR AGREEMENT DETERMINATION**

To: Omer Osman, Secretary

From: Kirk Brown, Region 5 Engineer

Date: 6/26/2024

Re: I-24, Contract Number 78972, Massac County

This project consists of bridge painting, repairs, and bridge deck overlay on the I-24 bridge (SN 064-0035) over the Ohio River in both Massac County, Illinois and McCracken County, Kentucky. September 20, 2024 Letting

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In accordance with the Project Labor Agreement Act (30 ILCS 571), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances.
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time.
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

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8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern.

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project.

10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).

12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:


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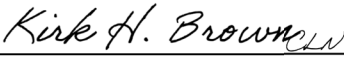
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Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed:  DocuSigned by:  
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\_\_\_\_\_  
{Division Chief} 9/9/2024 | 11:01 AM CDT  
(Date)

Agreed:  DocuSigned by:  
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\_\_\_\_\_  
{Bureau of Design & Environment} 9/6/2024 | 1:08 PM CDT  
(Date)

Agreed:  June 26, 2024  
\_\_\_\_\_  
{Regional Engineer} (Date)

Approved:  DocuSigned by:  
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\_\_\_\_\_  
Omer Osman, Secretary 9/10/2024 | 12:51 PM CDT  
(Date)

FHWA concurrence in the PLA for the above mentioned contract  
**MICHAEL W**  
**SMART**  
\_\_\_\_\_  
FHWA (Date)

Digitally signed by MICHAEL W SMART  
Date: 2024.08.20 14:24:30 -05'00'

**ATTACHMENT A:**

This project is for the rehabilitation of SN 064-0035. The work will be accomplished by stage construction consisting of single lane closures and crossovers utilizing Temporary Concrete Barrier, Alternate Route Signing, Temporary Rumble Strips, and Smart Traffic Monitoring System. The pre-stage construction roadway work includes Paved Shoulder Removal, Concrete Barrier Removal, Paved Shoulder Removal, Portland Cement Concrete Pavement (Jointed), Temporary Concrete Barrier, and all other collateral work to complete the pre-stage construction work. Stage construction bridge work includes Modular, Expansion Joints, HMA Surface Removal (Deck), Bridge Deck Scarification, Bridge Deck Patching, Bridge Deck Microsilica Concrete Overlay, Structure Repair of Concrete, Parapet Rail Removal, Concrete Parapet Cap, Bridge Painting, Structural Steel Repairs, and all other collateral work to complete the bridge stage construction work. Stage construction for roadway work include Steel Plate Beam Guardrail, Concrete Barrier Wall Transition, Concrete Barrier Wall (Special), and all other collateral work to complete the roadway-stage construction work. The project is location on FAI 24 (I-24) over the Ohio River near Paducah, KY with work in both Illinois and Kentucky.

Programmed amount is \$42,000,000

#3: This project is an interstate bridge project at a major river crossing involving multiple stages and construction seasons. Work related to this project will be accelerated to minimize conflicts to the travelling public.

#4 – This project is expected to exceed one construction season.

#5 – Project has 2 interim completion dates and a final completion date as follows:

Interim Completion Date 2025:

The Contractor shall not begin work which requires lane closures prior to March 1, 2025. Due to the Fort Massac Encampment which will occur October 17-19, 2025 the Contractor shall complete all work which requires 2025 lane closures, have all lanes of I-24 open to traffic, and have all traffic control including temporary concrete barrier removed by the interim completion date of Tuesday, October 14, 2025.

Interim Completion Date 2026:

The Contractor shall not begin work which requires lane closures prior to March 1, 2026. Due to the Fort Massac Encampment which will occur October 16-18, 2026 the Contractor shall complete all work which requires 2026 lane closures, have all lanes of I-24 open to traffic, and have all traffic control including temporary concrete barrier removed by the interim completion date of Tuesday, October 13, 2026.

Completion Date:

The Contractor shall not begin work which requires lane closures prior to March 1, 2027. Due to Labor Day weekend which begins Friday, September 3, 2027 the Contractor shall complete all remaining work which requires lane closures, have all lanes of I-24 open to traffic, and have all traffic control including temporary concrete barrier removed by the completion date of Wednesday, September 1, 2027.

#6 - Laborers 773 exp 3-31-25  
 Operating Engineers 318 exp 3-31-25

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Finishers 143 exp 4-30-25  
Ironworkers 782 7-31-25  
Teamsters 50 exp 12-31-26  
Carpenters 640 4-30-28


#8 – In order to maintain traffic during construction, lane closures, lane shifts, temporary concrete barrier, and crossovers will all be required. Due to the high traffic volumes these temporary conditions, while necessary during construction, pose a safety hazard to the traveling public. This hazard can be mitigated by minimizing the time that these temporary conditions are in place.

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Execution Page

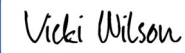
**Illinois Department of Transportation**

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Director of Highways Project Implementation

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Director of Finance & Administration

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Michael S. Prater, Chief Counsel

DocuSigned by:

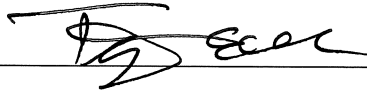
  
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Omer Osman, Secretary

9/10/2024 | 12:51 PM CDT

(Date)

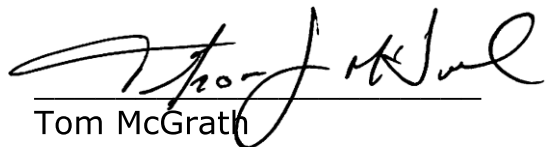
**Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the  
Unions listed below:**



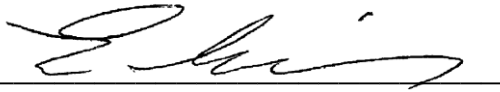
09/04/2024  
(Date)



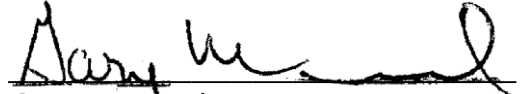
Mike Volpentesta  
Bricklayers



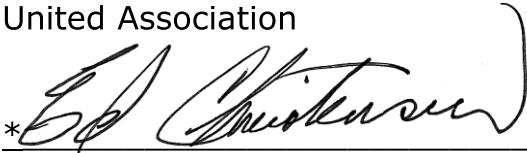
Tom McGrath  
Heat & Frost Insulators & Allied  
Workers




Evan Wooding  
United Association



Gary Menzel  
Roofers & Waterproofers



\*Ed Christensen  
Elevator Constructors



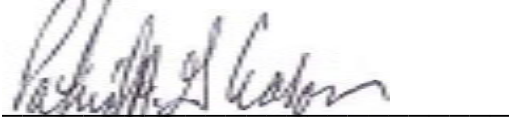
Shad Etchason  
IBEW



Ryan Anderson  
IUPAT



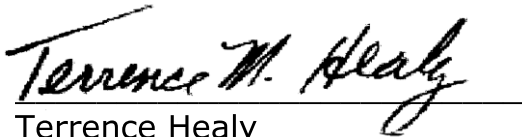
Michael R Kresge  
IUOE



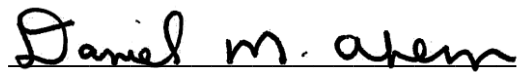
Pat Gleason  
Teamsters



Anthony Guida  
Carpenters



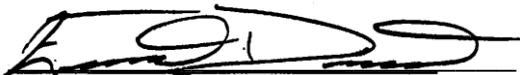
Terrence Healy  
LIUNA



Daniel M. Ahern  
Sheet Metal Workers



David Beard  
Iron Workers



Eric S. Davis  
Boilermakers



Kevin J. Farley  
OPCMIA

\*Elevator Constructors master agreement language  
must be attached to PLA