

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

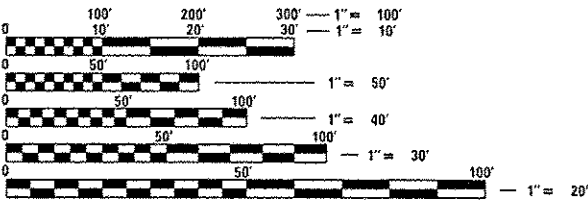
PROPOSED HIGHWAY PLANS

FAU ROUTE 3887 (IL-31)
AT SILVER GLEN ROAD
SECTION: H-N

PROJECT: ACM-3887 (010)
TRAFFIC SIGNAL INSTALLATION & CHANNELIZATION
KANE COUNTY
C-91-060-12

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED
IN UNINCORPORATED
KANE COUNTY



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

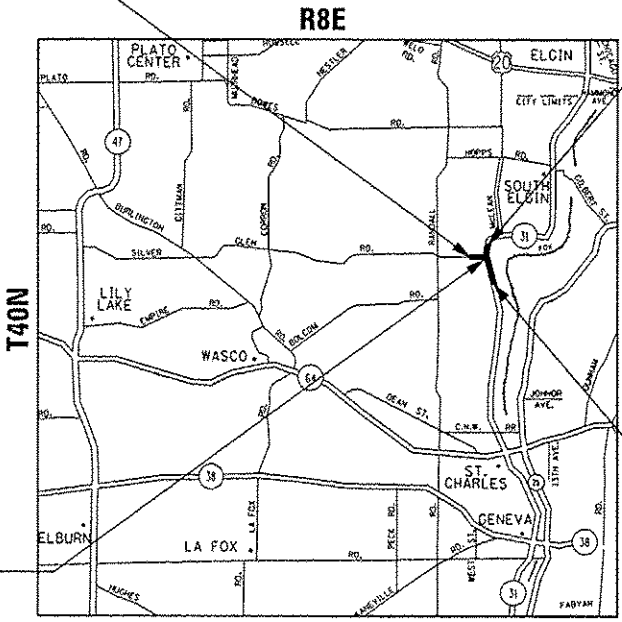
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60R28

PROJECT LIMIT
SILVER GLEN RD
STA 82+70

PROJECT LIMIT
SILVER GLEN RD
STA 85+00



PROJECT ENDS
IL ROUTE 31
STA 41+75

PROJECT BEGINS
IL ROUTE 31
STA 26+00

ST. CHARLES TOWNSHIP

TOTAL GROSS AND NET LENGTH = 1805 FT = 0.34 MILE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	1
ILLINOIS CONTRACT NO. 60R28				



TRAFFIC DATA
2013 ADT - 12,400
SPEED LIMIT - 45 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED July 7 2015
John F. Farnham as
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
Aug 14 2015
John D. Baranowski P.E. as
ENGINEER OF DESIGN AND ENVIRONMENT
Aug 14 2015
Omer Osman P.E. as
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.

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LIST OF STATE STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEM
406201-01	MAILBOX TURNOUTS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-02	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-02	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD. STRIPS / SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001-02	CATCH BASIN TYPE A
602401-03	MANHOLE TYPE A
604001-04	FRAME AND LIDS TYPE 1
604086-03	FRAME AND GRATE TYPE 23
604036-03	GRATE TYPE 8
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRTE CURB AND GUTTER
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	STEEL PLATE BEAM, PCC/HMA STABILIZATION
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINAL
631031-13	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701006-05	OFF-RO OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701011-04	OFF-RO MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS \geq 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS \geq 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-04	TRAFFIC CONTOL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
720016-03	MAST ARM MOUNTED STREET NAME SIGNS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16 FEET THROUGH 55 FEET
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOPS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF SAINT CHARLES.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER, WITH THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAMES AND LIDS UNLESS OTHERWISE SPECIFIED IN THE PLANS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

SIDEWALK REMOVAL AND P.C.C. SIDEWALK, 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT (847) 741-9857 OR DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGHTS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

GENERAL NOTES:

PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED IN ACCORDANCE WITH ARTICLE 205.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT (CUYD) WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 (01/01/2012) AND THE 100T SUBGRADE STABILITY MANUAL (05/01/2005). IF UNSTABLE AND/OR UNSUITABLE SOILS IS NOT ENCOUNTERED, DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

404 PERMIT NOTE:

THE PROJECT REQUIRES A U.S. ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN (INCLUDING WORK IN WETLANDS) TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM AND WETLAND WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK (WHICH INCLUDES WORK WITHIN WETLANDS). THE COST OFF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPELMENT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

FILE NAME :	USER NAME : Bilgramia	DESIGNED :	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES IL RTE 31 AT SILVER GLEN RD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
path\1084EBID\INTEG\Illinois.gov\PIDOT\Documents\1007 Offices\District 1\Projects\P1447\BRAND\0\Design\P144709-shr-details.dgn	BRAND	REVISED :	3887			H-N	KANE	86	2	
PLOT SCALE : 100.0000' / 1" =	CHECKED :	REVISED :	CONTRACT NO. 60R28							
PLOT DATE : 7/15/2015	DATE :	REVISED :	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
SCALE:				SHEET NO. OF SHEETS STA. TO STA.						

REV.

URBAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	3
		CONTRACT NO. 60R28		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				URBAN						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE						
				80% FED 20% STATE ROADWAY 0004	80% FED 13.3% STATE 6.7% KANE CO. T.S. INST. 0021	100% FOX RIVER & COUNTRYSIDE FIRE DIST. EVP 0021				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	8	8						
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	380	380						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	53	53						
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	12	12						
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	86	86						
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	903	903						
42001300	PROTECTIVE COAT	SQ YD	181	181						
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	54	54						
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	157	157						
42400800	DETECTABLE WARNINGS	SQ FT	40	40						
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	447	447						
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	4826	4826						

SUMMARY OF QUANTITIES				TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	80% FED. 20% STATE		80% FED. 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.				
			ROADWAY 0004		T.S. INST. 0021	EVP 0021				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	657	657						
44000300	CURB REMOVAL	FOOT	25	25						
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	455	455						
44001980	CONCRETE BARRIER REMOVAL	FOOT	10	10						
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2	SQ YD	80	80						
	1/2"									
44004250	PAVED SHOULDER REMOVAL	SQ YD	144	144						
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	80	80						
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	36	36						
48203019	HOT-MIX ASPHALT SHOULDERS, 5 1/2"	SQ YD	2052	2052						
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1						
50102400	CONCRETE REMOVAL	CU YD	2	2						
50105220	PIPE CULVERT REMOVAL	FOOT	101	101						
50200100	STRUCTURE EXCAVATION	CU YD	190	190						
50200400	ROCK EXCAVATION FOR STRUCTURES	CU YD	10.6	10.6						
50300225	CONCRETE STRUCTURES	CU YD	8.1	8.1						

*SPECIALTY ITEMS
15

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED. 20% STATE	80% FED 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.			
				ROADWAY 0004	T.S. INST. 0021	EVP 0021			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2600	2600					
54002010	EXPANSION BOLTS 1/2 INCH	EACH	32	32					
54213669	PRECAST REINFORCED CONCRETE FLARED END	EACH	2	2					
	SECTIONS 24"								
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	57	57					
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	36	36					
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	72	72					
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	619	619					
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	87	87					
55100200	STORM SEWER REMOVAL 6"	FOOT	585	585					
55100700	STORM SEWER REMOVAL 15"	FOOT	74	74					
55101200	STORM SEWER REMOVAL 24"	FOOT	52	52					
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	5	5					
60107600	PIPE UNDERDRAINS 4"	FOOT	510	510					
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	61	61					
60200805	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE	EACH	3	3					
	8 GRATE								

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED. 20% STATE	80% FED 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.			
				ROADWAY 0004	T.S. INST. 0021	EVP 0021			
60201330	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE	EACH	4	4					
	23 FRAME AND GRATE								
60204505	CATCH BASINS, TYPE A, 5' -DIAMETER, TYPE	EACH	1	1					
	8 GRATE								
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	3	3					
60500040	REMOVING MANHOLES	EACH	1	1					
60500050	REMOVING CATCH BASINS	EACH	3	3					
60500060	REMOVING INLETS	EACH	1	1					
60600605	CONCRETE CURB, TYPE B	FOOT	57	57					
60603800	COMBINATION CONCRETE CURB AND GUTTER,	FOOT	300	300					
	TYPE B-6.12								
63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6	FOOT	331.25	331.25					
	FOOT POSTS								
63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2					
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1	EACH	4	4					
	(SPECIAL) TANGENT								
63200310	GUARDRAIL REMOVAL	FOOT	418	418					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	110	110					

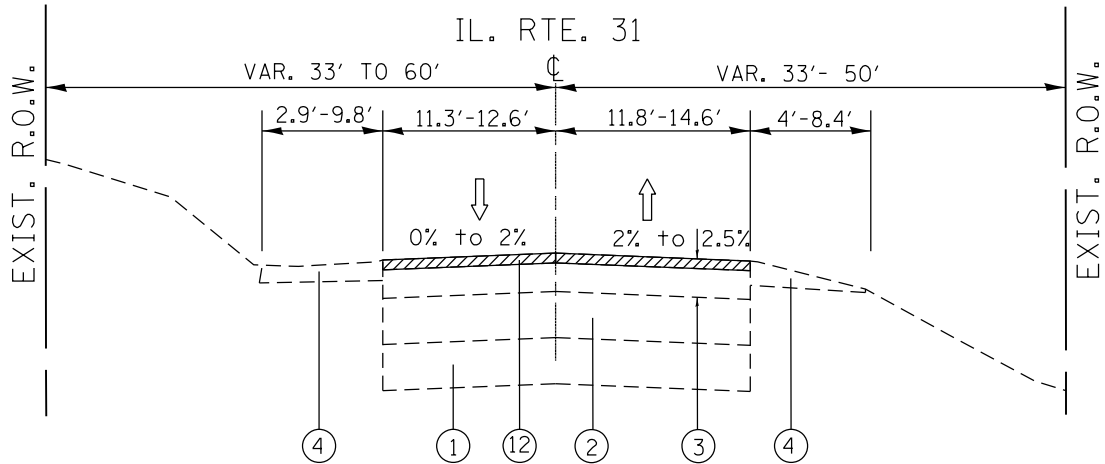
* SPECIALTY ITEMS

13

FILE NAME : p:\N00848\B00NTEC\Hindsgor\PH007\Documents\100	USER NAME : B1gromsd Office\B1gromsd\100\Projects\PH007\CH000\Design\PH007\B00NTEC\Hindsgor\PH007\Documents\100	DESIGNED : DESIGNED BY : CHECKED : DATE : 7/15/2005	REVISED : REVISED : REVISED : REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. ROUTE 31 AT SILVER GLEN RD. SUMMARY OF QUANTITIES	SCALE: SHEET NO. OF SHEETS STA. TO STA.	F.A.U. RTE. 3887 SECTION H-N COUNTY KANE TOTAL SHEETS 86 SHEET NO. 5 CONTRACT NO. 60R28 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
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URBAN				URBAN															
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED. 20% STATE	80% FED. 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.				CODE NO	ITEM	UNIT		80% FED. 20% STATE	80% FED. 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.			
				ROADWAY 0004	T.S. INST. 0021	EVP 0021								ROADWAY 0004	T.S. INST. 0021	EVP 0021			
66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1						70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	887.5	887.5					
66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1						70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	18	18					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6															
67100100	MOBILIZATION	L SUM	1	1						* 72000100	SIGN PANEL - TYPE 1	SQ FT	192.15	183.9	8.25				
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1	1						* 72000200	SIGN PANEL - TYPE 2	SQ FT	40.8	21.3	19.5				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1733	1733						* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	15	15					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	163.7	163.7						* 72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	5	5					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	7963	7963						* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	384	384					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1028	1028						* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	163.7	163.7					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	449	449						* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7963	7963					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	91	91						* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1028	1028					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	10016	10016						* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	449	449					
70300540	PAVEMENT MARKING TAPE, TYPE III 6"	FOOT	305	305						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	91	91					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3684	3684						* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	111	111					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1075	1075						13									
* SPECIALTY ITEMS																			
FILE NAME : p:\mldghebid\nteg\hinds\p\mldot\Documents\1007 OFFICE OF DISTRICT ENGINEERING PROJECT: PM44109-CADOT-Design-PM44109-ROADWAY DESIGN PLANNING																			
DESIGNED - REVISOR -																			
CHECKED - REVISOR -																			
DATE - REVISOR -																			
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION																			
IL. ROUTE 31 AT SILVER GLEN RD. SUMMARY OF QUANTITIES																			
SCALE: SHEET NO. OF SHEETS STA. TO STA.																			
F.A.U. 3887 SECTION H-N COUNTY KANE TOTAL SHEETS 86 SHEET NO. 6 CONTRACT NO. 60R28																			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT																			

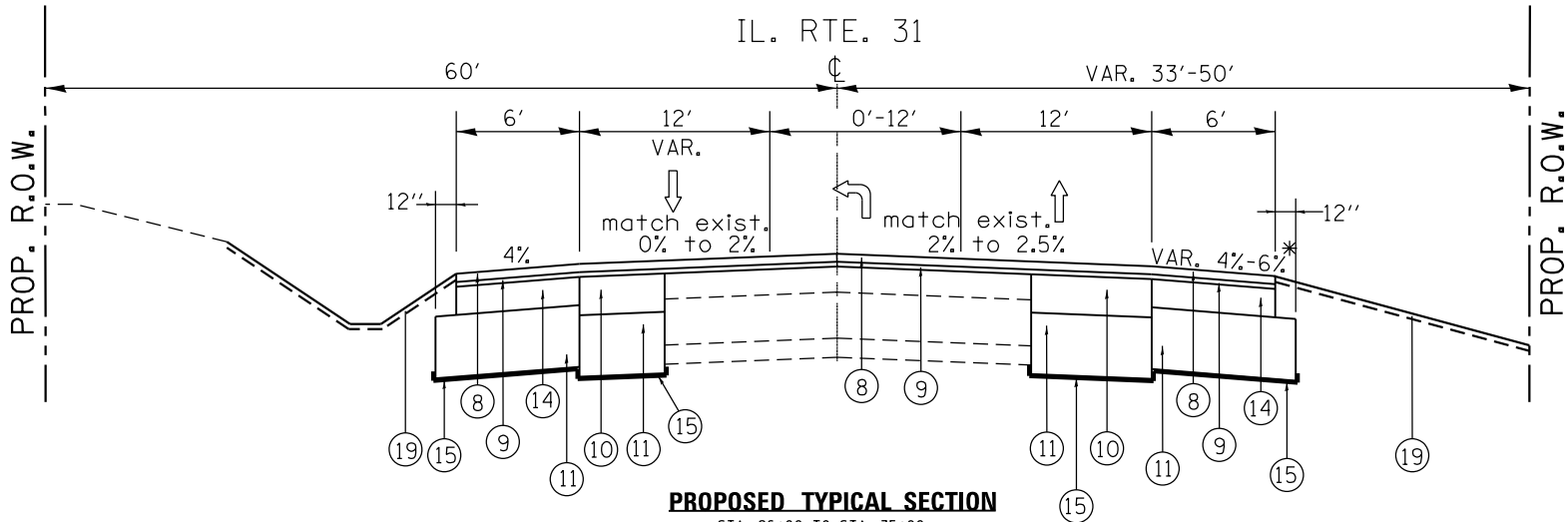
URBAN										URBAN																							
SUMMARY OF QUANTITIES				TOTAL QUANTITIES	CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				TOTAL QUANTITIES	CONSTRUCTION TYPE CODE																	
CODE NO	ITEM	UNIT	80% FED. 20% STATE		80% FED. 13.3% STATE 6.7% KANE CO.	100% FOX RIVER & COUNTRYSIDE FIRE DIST.				CODE NO	ITEM	UNIT	80% FED. 20% STATE	80% FED. 13.3% STATE 6.7% KANE CO.		100% FOX RIVER & COUNTRYSIDE FIRE DIST.																	
* 78200420	GUARDRAIL MARKERS, TYPE B	EACH	12	12						* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1754		1754																		
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4						* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	460		460																		
78300200	RAISED REFLECTIVE PAVEMENT MARKER	EACH	66	66						* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1150		1150																		
	REMOVAL									* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	310		310																		
* 80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1		1					* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	850		850																		
* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	802		802					* 87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2		2																		
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	57		57					* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	1		1																		
* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	156		156					* 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1		1																		
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	198		198					* 87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1		1																		
* 81400100	HANDHOLE	EACH	2		2					* 87700240	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1		1																		
* 81400200	HEAVY-DUTY HANDHOLE	EACH	4		4					* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12		12																		
* 81400300	DOUBLE HANDHOLE	EACH	1		1					* SPECIALTY ITEMS																							
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	170		170																												
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	680		680																												
FILE NAME : USER NAME : DESIGNED - REVISED - PLOT SCALE : 1000000' = 1" CHECKED - REVISED - PLOT DATE : 7/15/2015 DATE - REVISED -										STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION										IL. ROUTE 31 AT SILVER GLEN RD. SUMMARY OF QUANTITIES SCALE: SHEET NO. OF SHEETS STA. TO STA.										F.A.U. RTE. 3887 SECTION H-N COUNTY KANE TOTAL SHEETS 86 SHEET NO. 7 CONTRACT NO. 60R2B			



EXISTING TYPICAL SECTION

STA. 26+00 TO STA. 35+00
LOOKING NORTH
CROWN SECTION

- LEGEND
- ① EXISTING AGGREGATE SUBGRADE
 - ② EXISTING P.C.C COURSE ± 7 1/2"
 - ③ EXISTING HMA PAVEMENT ±9"
 - ④ EXISTING AGGREGATE SHOULDERS
 - ⑤ EXISTING HMA SHOULDERS
 - ⑥ EXISTING CURB AND GUTTER
 - ⑦ EXISTING HMA PAVEMENT ±5" TO 6.5"
 - ⑧ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
 - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
 - ⑩ PROPOSED HMA BASE COURSE, 7 1/2"
 - ⑪ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ⑫ PROPOSED HMA SURFACE REMOVAL 2 1/2"
 - ⑬ PROPOSED HMA SURFACE REMOVAL 1 1/2"
 - ⑭ PROPOSED HMA SHOULDER, 5 1/2" (HMA BINDER IL-19 mm)
 - ⑮ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ⑯ PROPOSED COMBINATION CURB AND GUTTER, TYPE B6.12
 - ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2" (BIKE PATH)
 - ⑱ PROPOSED AGGREGATE BASE COURSE, TYPE B 6" (BIKE PATH)
 - ⑲ PROPOSED TOPSOIL 8"



PROPOSED TYPICAL SECTION

STA. 26+00 TO STA. 35+00
LOOKING NORTH
CROWN SECTION

NOTE: CONTRACTOR SHALL PATCH FIRST.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOID @ N _{DES}	QUALITY MANAGEMENT PROGRAM (QMP)
RESURFACING		
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 1 3/4"	4% @ 90 GYR	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 GYR	QC/QA
PAVEMENT WIDENING		
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 1 3/4"	4% @ 90 GYR	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 GYR	QC/QA
HMA BASE COURSE (HMA BINDER IL-19 mm), 7 1/2"	4% @ 90 GYR	QC/QA
DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/QA
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm); PE 6"	4% @ 50 GYR	QC/QA
SHOULDERS		
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 1 3/4"	4% @ 90 GYR	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 GYR	QC/QA
HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19 mm), 5 1/2"	4% @ 70 GYR	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	QC/QA
BIKE PATH		
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50 (IL 9.5 mm); 2"	4% @ 50 GYR	QC/QA
QMP Designation: Quality Control/ Quality Assurance (QC/QA); Quality Control for Performance (OCP); Pay for Performance (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/ SQ YD/IN.

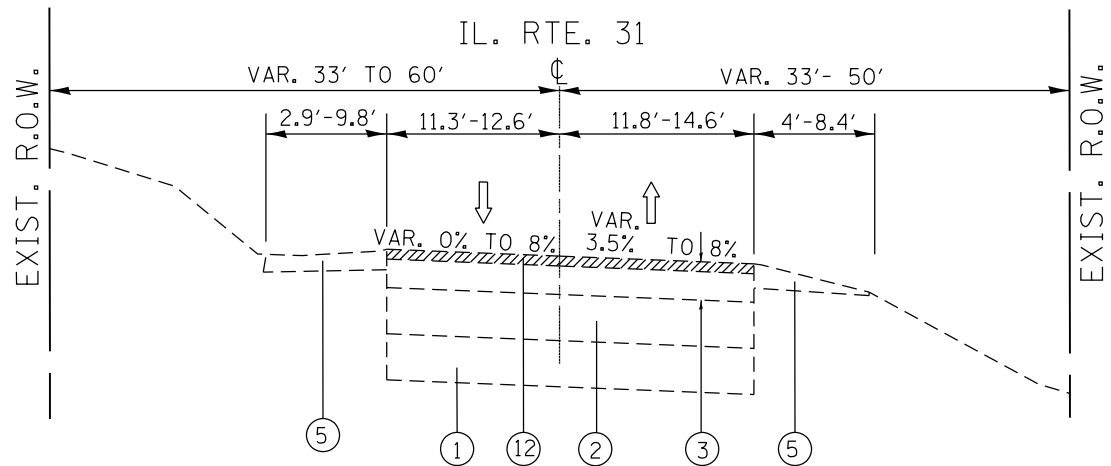
THE "AC TYPE" FOR POLYMERIZED MIXTURES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISION.

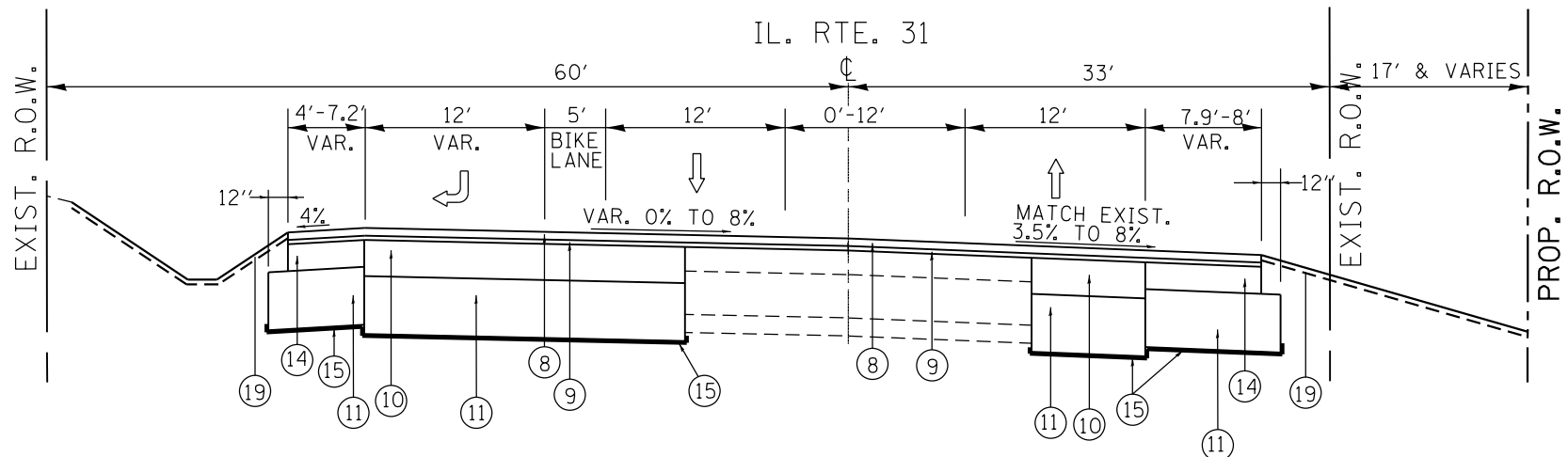
QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

*SEE CROSS SECTION FOR STEEPER SHOULDER CROSS SLOPES AT SOME DRIVEWAYS.

FILE NAME =	USER NAME = Bilgramise	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL 31 RTE AT SILVER GLEN RD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\P144709\Drawings\Design\P144709-shr-details.dgn		DRAWN -	REVISED -					3887	H-N	KANE	86	10
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 60R28				
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
				SCALE:			SHEET	OF	SHEETS	STA.	TO STA.	



EXISTING TYPICAL SECTION
STA. 36+50 TO STA. 40+50
LOOKING NORTH
SUPER ELEVATION SECTION

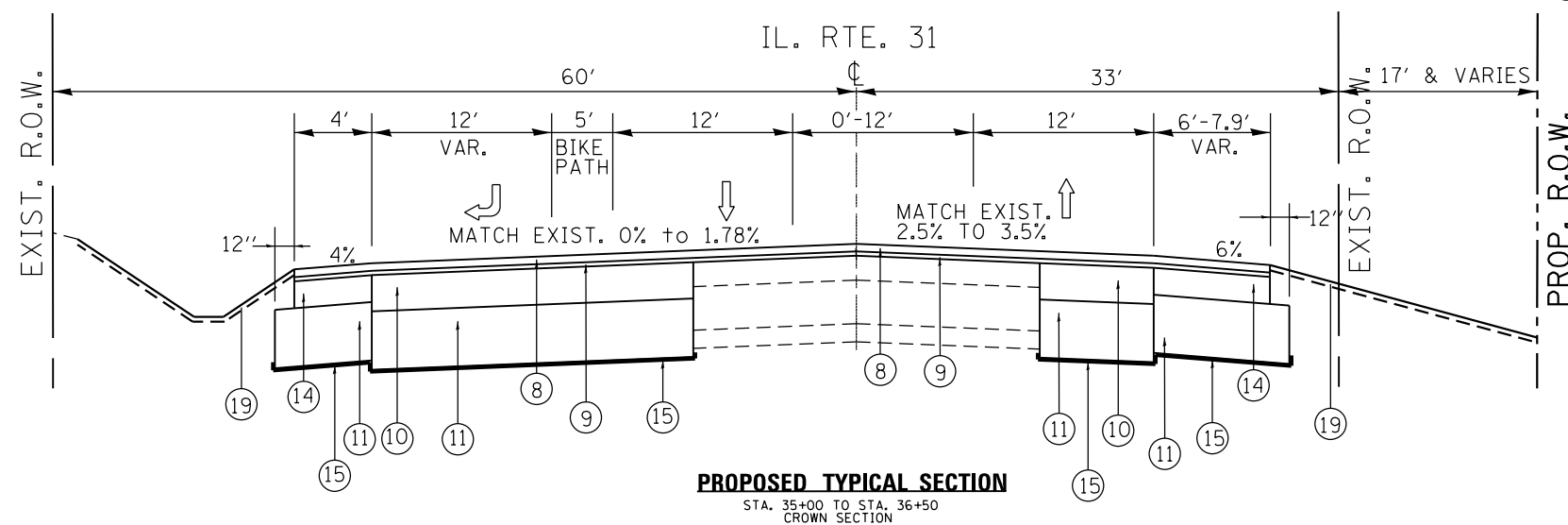
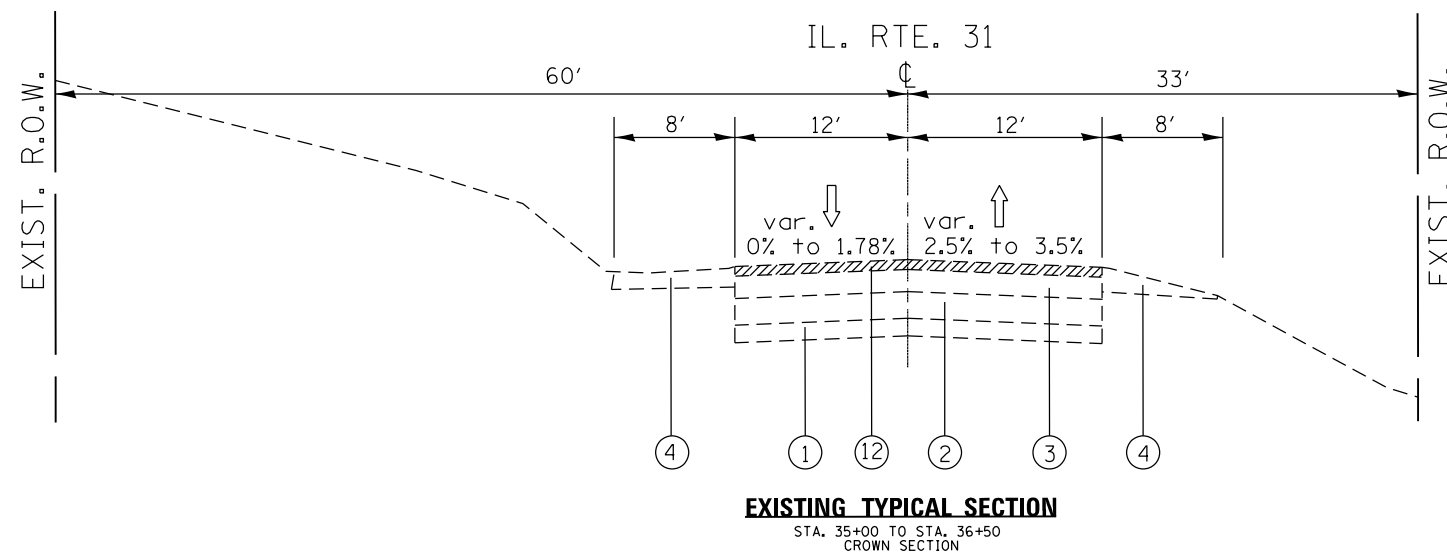


PROPOSED TYPICAL SECTION
STA. 36+50 TO STA. 40+50
LOOKING NORTH
SUPER ELEVATION SECTION

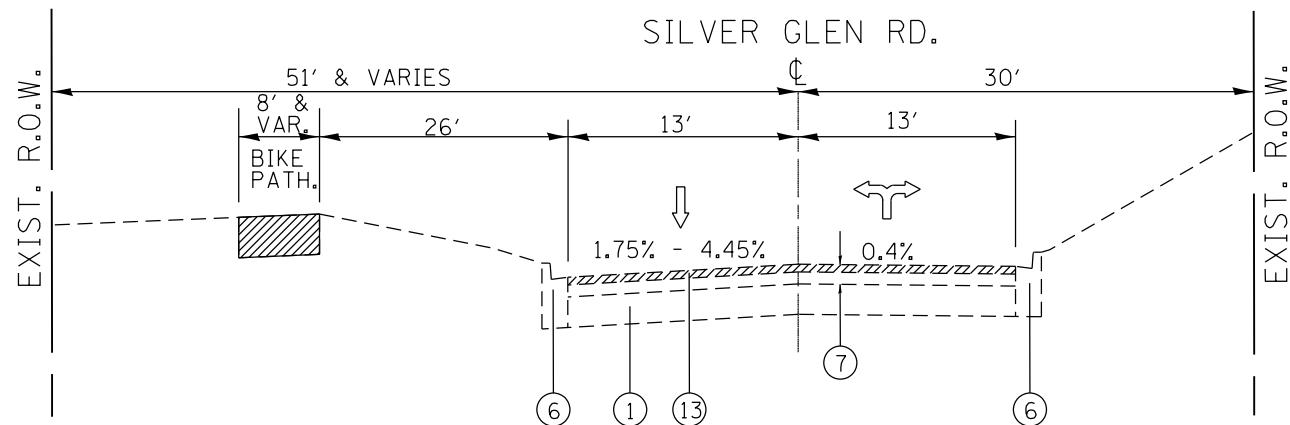
- LEGEND
- ① EXISTING AGGREGATE SUBGRADE
 - ② EXISTING P.C.C COURSE ± 7 1/2"
 - ③ EXISTING HMA PAVEMENT ±9"
 - ④ EXISTING AGGREGATE SHOULDERS
 - ⑤ EXISTING HMA SHOULDERS
 - ⑥ EXISTING CURB AND GUTTER
 - ⑦ EXISTING HMA PAVEMENT ±5" TO 6.5"
 - ⑧ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
 - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
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 - ⑮ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ⑯ PROPOSED COMBINATION CURB AND GUTTER, TYPE B6.12
 - ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 2" (BIKE PATH)
 - ⑱ PROPOSED AGGREGATE BASE COURSE, TYPE B 6" (BIKE PATH)
 - ⑲ PROPOSED TOPSOIL 8"

LEGEND

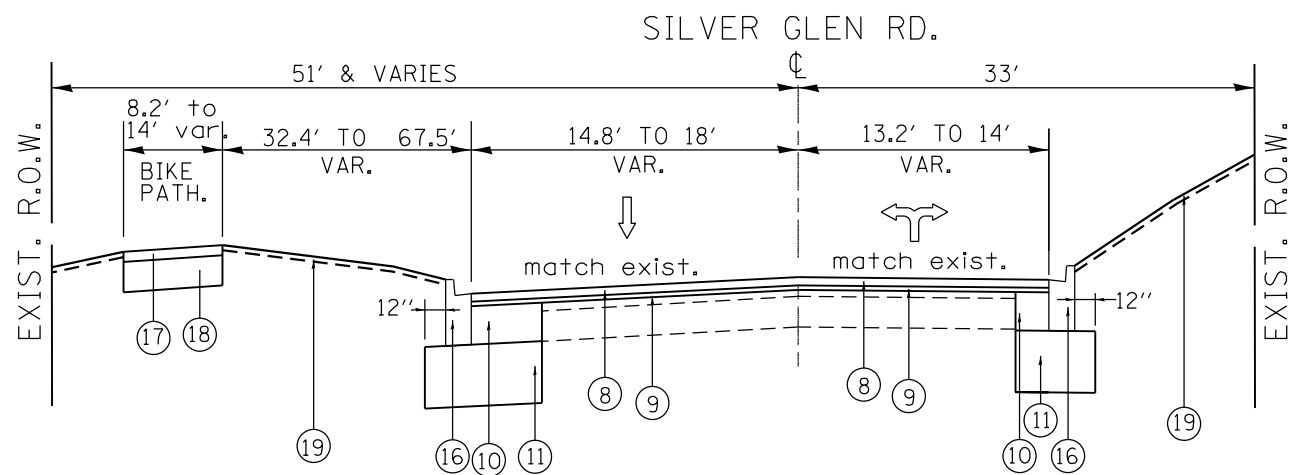
- ① EXISTING AGGREGATE SUBGRADE
- ② EXISTING P.C.C COURSE $\pm 7 \frac{1}{2}$ "
- ③ EXISTING HMA PAVEMENT ± 9 "
- ④ EXISTING AGGREGATE SHOULDERS
- ⑤ EXISTING HMA SHOULDERS
- ⑥ EXISTING CURB AND GUTTER
- ⑦ EXISTING HMA PAVEMENT ± 5 " TO 6.5"
- ⑧ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 $\frac{3}{4}$ "
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, $\frac{3}{4}$ "
- ⑩ PROPOSED HMA BASE COURSE, 7 $\frac{1}{2}$ "
- ⑪ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑫ PROPOSED HMA SURFACE REMOVAL 2 $\frac{1}{2}$ "
- ⑬ PROPOSED HMA SURFACE REMOVAL 1 $\frac{1}{2}$ "
- ⑭ PROPOSED HMA SHOULDER, 5 $\frac{1}{2}$ " (HMA BINDER IL-19 mm)
- ⑮ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
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FILE NAME =	USER NAME = Bilgramise	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	EXISTING AND PROPOSED TYPICAL SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL084EBID\INTE\Illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\PI44709\DATA\Design\PI44709-sht-details.dgn		CHANGED -	REVISED -		IL RTE 31 AT SILVER GLEN RD				3887	H-N	KANE	86	12
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -						CONTRACT NO. 60R28				
Default	PLOT DATE = 7/14/2015	DATE	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
STA. 82+70 TO STA. 84+13.7



PROPOSED TYPICAL SECTION
STA. 82+70 TO STA. 84+13.7
LOOKING EAST

NOTE: 1" GUTTER OVERLAY WILL BE NEEDED
OVER EXISTING GUTTER

LEGEND

- ① EXISTING AGGREGATE SUBGRADE
- ② EXISTING P.C.C COURSE ± 7 1/2"
- ③ EXISTING HMA PAVEMENT ±9"
- ④ EXISTING AGGREGATE SHOULDERS
- ⑤ EXISTING HMA SHOULDERS
- ⑥ EXISTING CURB AND GUTTER
- ⑦ EXISTING HMA PAVEMENT ±5" TO 6.5"
- ⑧ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑩ PROPOSED HMA BASE COURSE, 7 1/2"
- ⑪ PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑫ PROPOSED HMA SURFACE REMOVAL 2 1/2"
- ⑬ PROPOSED HMA SURFACE REMOVAL 1 1/2"
- ⑭ PROPOSED HMA SHOULDER, 5 1/2" (HMA BINDER IL-19 mm)
- ⑮ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑯ PROPOSED COMBINATION CURB AND GUTTER, TYPE B6.12
- ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 2" (BIKE PATH)
- ⑱ PROPOSED AGGREGATE BASE COURSE, TYPE B 6" (BIKE PATH)
- ⑲ PROPOSED TOPSOIL 8"

FILE NAME = p:\11\084EBID\INTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\P144709\Drawings\Design\P144709-shd-details.dgn Default	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL RTE 31 AT SILVER GLEN RD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED -	REVISED -			3887	H-N	KANE	86	13
	PLOT SCALE = 100.0000' / in.		REVIS			CONTRACT NO. 60R28				
	PLOT DATE = 7/14/2015	DATE -	ILLINOIS FED. AID PROJECT							
				SCALE:		SHEET	OF	SHEETS	STA.	TO STA.

EARTHWORK												
1	2		3		4		5		6		7	
ILL 31 AT SILVER GLEN ROAD	CUT EARTH EXCAVATION		UNSUITABLE MATERIAL		FILL EMBANKMENT		ADJUSTMENT FOR SHRINKAGE		EARTHWORK BALANCE WASTE (+) SHORTAGE (-)		TOPSOIL FURNISH & PLACE	
	(CU YD)		(CU YD)		(CU YD)		(CU YD)		(CU YD)		(SQ YD)	
	LEFT OFFSET	RIGHT OFFSET	LEFT OFFSET	RIGHT OFFSET	LEFT OFFSET	RIGHT OFFSET	LEFT OFFSET	RIGHT OFFSET	LEFT OFFSET	RIGHT OFFSET	LEFT OFFSET	RIGHT STAGE II
ILL 31 (STA. 26+00 TO STA. 41+75)	2106	166	1926	944	726	1008	1790.1	141.1	1064.1	-866.9		
Silver Glen (STA. 82+70 TO STA. 84+17)	374	0	357	85	72	39	317.9	0.0	245.9	-39.0		
TOTAL	2480	166	2283	1029	798	1047	2108.0	141.1	1310.0	-905.9	4845.5	1614.5

COLUMN 1: LOCATION FROM PLANS

COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL

COLUMN 3: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT, ASSUME 12" OF UNSUITABLE MATERIAL

COLUMN 4: QUANTITIES FROM CROSS SECTIONS (FILL)

COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%

COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGATIVE QUANTITY= FURNISHED EXCAVATION NEEDED

COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SEEDING

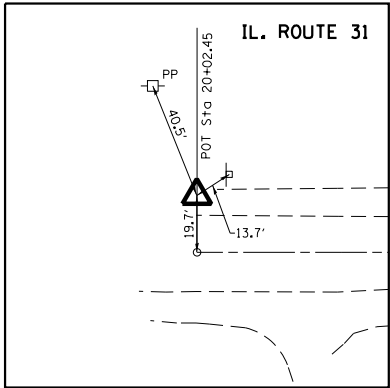
TREE REMOVAL (6 TO 15 UNITS DIAMETER)			
STATION	OFFSET (FT)		UNITS
ILL 31 AT SILVER GLEN			
27+67	56.9	LT	7
27+68	58.4	LT	8
27+68	57.3	LT	10
27+81	56.9	LT	9
28+02	55.3	LT	12
28+06	57.1	LT	15
28+37	55.5	LT	10
29+56	39.4	LT	MS (6, 6)
29+62	37.7	LT	7
30+09	39.5	LT	8
32+22	25.5	LT	15
32+45	29.8	LT	7.5
32+51	30.6	LT	8.5
32+56	30.3	LT	9.5
32+69	28.7	LT	13
32+77	29.5	LT	9.5
32+80	27.4	LT	12
32+85	29.1	LT	9.5
33+04	26.7	LT	10
36+93	29.5	LT	8.5
36+95	30.9	LT	11
37+22	35.5	LT	10
37+24	44.5	LT	6
37+24	42.9	LT	6
37+25	47.1	LT	12.5
37+49	52.3	LT	11
37+73	50.1	LT	14
37+88	56.4	LT	12
38+57	37.0	LT	7
39+13	34.8	LT	6
39+27	35.2	LT	12
39+79	37.2	LT	9
40+25	30.2	LT	7
40+26	33.5	LT	8
40+64	42.1	LT	9
SUB-TOTAL			335.5

TREE REMOVAL (6 TO 15 UNITS DIAMETER)			
	OFFSET (FT)		UNITS
ILL 31 AT SILVER GLEN			
28+27	41.3	RT	MS (11, 3, 2, 1.5)
29+84	22.7	RT	MS (7, 1.5, 1,1.5, 0.5, 0.5)
29+87	25.9	RT	11
30+03	34.3	RT	15
30+34	38.9	RT	10
30+37	35.8	RT	MS (5, 5)
30+39	32.8	RT	10
30+60	38.1	RT	MS (9, 3)
31+81	36.7	RT	6
31+83	35.9	RT	6.5
31+95	27.8	RT	13
32+39	34.5	RT	6
30+29	32.9	RT	8.5
34+88	41.5	RT	6
34+97	37.0	RT	9
35+63	29.1	RT	MS (5.5, 5.5)
35+63	28.4	RT	MS (5.5, 5.5)
SUB-TOTAL			128

TREE REMOVAL (OVER 15 UNITS DIAMETER)			
STATION	OFFSET (FT)		UNITS
ILL 31 AT SILVER GLEN			
84+35	28.3	RT	20
34+58	28.8	RT	21
35+03	33.9	RT	17
35+19	45.7	RT	16
32+32	29.7	LT	22
34+40	37.4	LT	26
34+47	46.6	LT	20
34+68	45.3	LT	23
37+15	44.8	LT	21
37+05	32.9	LT	17
37+24	38.9	LT	18
83+86	39.8	LT	26
39+03	33.0	LT	MS (16, 8, 7)
39+21	35.2	LT	18
SUB-TOTAL			281

(MS) MULTI-STEMMED TREE
NOTE: TREES ON PRIVATE PROPERTY SHALL NOT BE REMOVED.

ROAD	EXISTING SIGNS		PROPOSED SIGNS		SIGN DESCRIPTION	MUTCD CODE	HEIGHT	WIDTH	AREA	PANEL	PROP. ACTION	REMOVE SIGN	REMOVE SIGN	TELESCOPING	SIGN PANEL	SIGN PANEL
	STATION	OFFSET	STATION	OFFSET								PANEL ASSY T A	PANEL ASSY TB	STEEL SIGN SUP.	TYPE 1	TYPE 2
		FT		FT								72400100	72400200	72800100	72000100	72000200
							FT	FT	SQFT	TYPE		EACH	EACH	FT	SQFT	SQFT
IL RTE. 31	26+23	28.4 RT			SIDE ROAD	W2-2L	3.00	3.00	9.00	1	REMOVE & PLACE NEW SIGN		1	17	9.0	
					ADVANCE STREET NAME PLAQUE "Silver Glen Rd"	W16-8	0.67	1.50	1.00	1	REMOVE & PLACE NEW SIGN				1.0	
IL RTE. 31	26+28	27.7 LT			SPEED LIMIT 45	R2-1	3.00	2.50	7.50	1	REMOVE & PLACE NEW SIGN	1		15	7.5	
IL RTE. 31	27+02	21.2 RT			ADDRESS SIGN (6N705)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	27+44	23.6 RT			ADDRESS SIGN (6N715)						RELOCATE BY HOME OWNER			N/A		
IL RTE. 31	28+10	22.4 RT			BICYCLE	W11-1	3.00	3.00	9.00	1	REMOVE & PLACE NEW SIGN	1		17	9.0	
IL RTE. 31	28+26	32.2 LT			ADDRESS SIGN (6N766)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	29+27	25.6 RT			ADDRESS SIGN (6N779)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	29+81	21.2 RT			CURVE	W1-2(R)	3.00	3.00	9.00	1	REMOVE & PLACE NEW SIGN	1		17	9.0	
IL RTE. 31	30+31	22.2 RT			ADDRESS SIGN (6N?)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	30+39	18.9 RT			WOODEN ADDRESS POST						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	31+26	22.9 RT			ADDRESS SIGN (6N?)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	32+03	27.4 RT			ADDRESS SIGN (6N8?)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	32+50	22.9 LT			UNLAWFUL TO PASS STOPPED SCHOOL BUS	S4-I105	2.50	2.50	6.25	1	REMOVE & PLACE NEW SIGN	1		15	6.3	
IL RTE. 31	34+54	25.1 LT			CARDINAL DIRECTION SOUTH	M3-3	1.00	2.00	2.00	1	REMOVE & PLACE NEW SIGN	1		15	2.0	
					ILLINOIS ROUTE 31 (white/black)	M1-I100	2.00	2.00	4.00	1	REMOVE & PLACE NEW SIGN				4.0	
IL RTE. 31	34+70	23.1 RT			BICYCLE	W11-1	3.00	3.00	9.00	1	REMOVE & PLACE NEW SIGN		1	18	9.0	
					DIRECTION ARROW AUXILIARY (black on FL. Yellow)	M6-2 (L)	1.25	1.75	2.19	1	REMOVE & PLACE NEW SIGN				2.2	
IL RTE. 31	34+94	24.2 RT			TWO DIRECTIONAL ARROW	W1-7	2.00	4.00	8.00	1	REMOVE & PLACE NEW SIGN	1		14	8.0	
IL RTE. 31	35+56	23.2 RT			ADDRESS SIGN (6N900)						RELOCATE BY HOMEOWNER			N/A		
IL RTE. 31	35+73	23.8 RT			CARDINAL DIRECTION NORTH	M3-1	1.00	2.00	2.00	1	REMOVE & PLACE NEW SIGN	1		15	2.0	
					ILLINOIS ROUTE 31 (white/black)	M1-I100	2.00	2.00	4.00	1	REMOVE & PLACE NEW SIGN				4.0	
IL RTE. 31	36+04	27.0 LT			BICYCLE	W11-1	3.00	3.00	9.00	1	REMOVE & PLACE NEW SIGN		1	18	9.0	
					DIRECTION ARROW AUXILIARY	M6-2 (L)	1.25	1.75	2.19	1	REMOVE & PLACE NEW SIGN				2.2	
IL RTE. 31	36+43	RT			LOVE THE LAND OF LINCOLN	I-I107A	3.00	2.50	7.50	1	REMOVE & PLACE NEW SIGN		1	17	7.5	
					ADOPT A HIGHWAY	I-I107B	1.50	2.50	3.75	1	REMOVE & PLACE NEW SIGN				3.8	
IL RTE. 31	36+74	22.8 LT			CHEVRON ALIGNMENT	W1-8R	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN	1		14	3.0	
					CHEVRON ALIGNMENT	W1-8L	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN				3.0	
IL RTE. 31	37+59	24.8 LT			CHEVRON ALIGNMENT	W1-8R	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN	1		14	3.0	
					CHEVRON ALIGNMENT	W1-8L	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN				3.0	
IL RTE. 31	38+50	25.1 LT			CHEVRON ALIGNMENT	W1-8R	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN	1		14	3.0	
					CHEVRON ALIGNMENT	W1-8L	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN				3.0	
IL RTE. 31	39+31	23.2 LT			CHEVRON ALIGNMENT	W1-8R	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN	1		14	3.0	
					CHEVRON ALIGNMENT	W1-8L	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN				3.0	
IL RTE. 31	40+21	23.1 LT			CHEVRON ALIGNMENT	W1-8R	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN	1		14	3.0	
					CHEVRON ALIGNMENT	W1-8L	2.00	1.50	3.00	1	REMOVE & PLACE NEW SIGN				3.0	
IL RTE. 31	40+71	27.5 LT			ADDRESS SIGN						RELOCATE BY HOMEOWNER			N/A	0.0	
SILVER GLEN RD.	82+82	22.1 LT			SPEED LIMIT 45	R2-1	3.00	2.50	7.50	1	REMOVE & PLACE NEW SIGN	1		14	7.5	
SILVER GLEN RD.	83+06	22.9 LT			NO PASSING ZONE	W14-3	4.00	5.33	21.33	2	REMOVE & PLACE NEW SIGN		1	32		21.3
SILVER GLEN RD.	83+57	20.5 LT			KANE CO. ADOPT A HWY/NEXT 1.25 MILES		1.50	2.50	3.75	1	REMOVE & PLACE NEW SIGN	1		15	3.8	
					KANE COUNTY ADOPT A HIGHWAY		1.50	2.50	3.75	1					3.8	
SILVER GLEN RD.	84+60	22.7 RT			STOP SIGN	R1-1	2.50	2.50	6.25	1	REMOVE	1		N/A	N/A	
IL RTE. 31			29+00	RT	SIGNAL AHEAD	W3-3	3.00	3.00	9.00	1	NEW SIGN			15	9.0	
IL RTE. 31			41+00	RT	SIGNAL AHEAD	W3-3	3.00	3.00	9.00	1	NEW SIGN			15	9.0	
SILVER GLEN RD.			80+00	RT	SIGNAL AHEAD	W3-3	3.00	3.00	9.00	1	NEW SIGN			15	9.0	
IL RTE. 31			40+90	LT	BEGIN, RIGHT TURN LANE, YIELD TO BIKES	R4-4	2.5	3	7.5	1	NEW SIGN			15	7.5	
IL RTE. 31			38+34	LT	RIGHT LANE, MUST, TURN RIGHT	R3-7R	3	3	9	1	NEW SIGN			15	9.0	
TOTAL												15	5	384.0	183.9	21.3

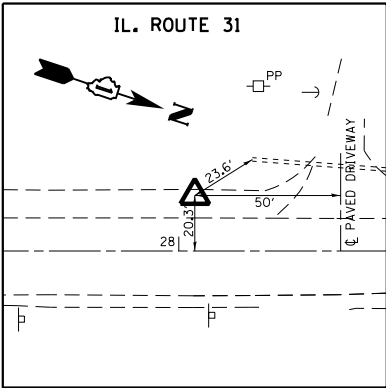


CONTROL POINT 100

SET REBAR
STA. 20+02.39, 19.74' LT
ELEV. 740.59

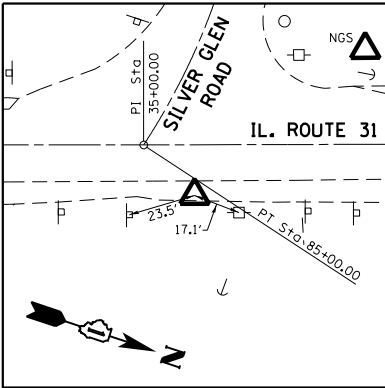
BENCHMARK #2

ELEV. 743.05
RR SPIKE IN EAST FACE OF POWER POLE
ON NORTH SIDE OF DRIVEWAY TO
6N622 IL. ROUTE 31



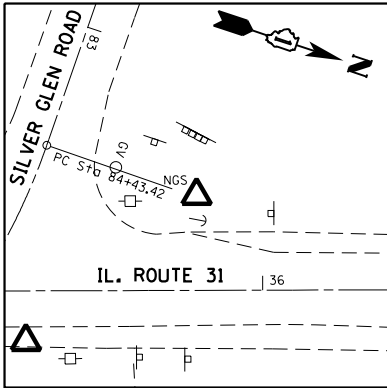
CONTROL POINT 101

SET REBAR
STA. 28+05.52, 19.29' LT
ELEV. 741.20



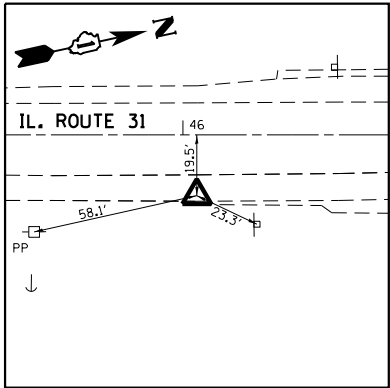
CONTROL POINT 102

SET MAGNAIL IN PAVED SHOULDER
STA. 35+17.68, 17.59' RT
ELEV. 743.27



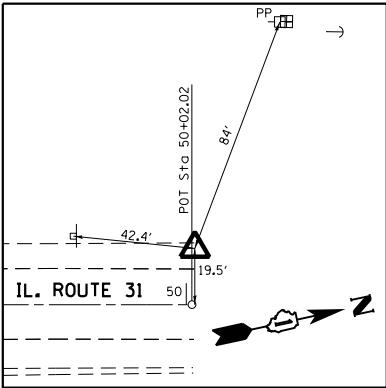
NGS MONUMENT/CONTROL POINT 3426

DISK
ELEV. 744.13



CONTROL POINT 104

SET MAG NAIL INSIDE PAVED SHOULDER
STA. 46+04.85, 21.16' RL
ELEV. 7731.96

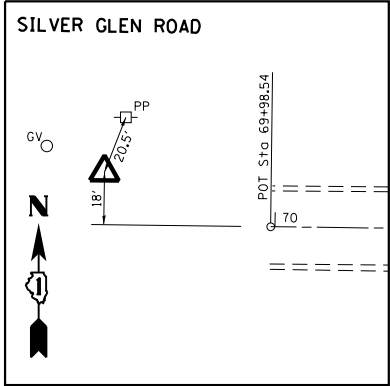


CONTROL POINT 105

SET MAG NAIL 2' INSIDE PAVED SHOULDER
STA. 50+02.98, 19.52' LT
ELEV. 731.74

BENCHMARK #1

ELEV. 728.39
RR SPIKE IN WEST FACE OF POWER POLE ON
EAST SIDE IL. ROUTE 31 @ ± STA. 50+50

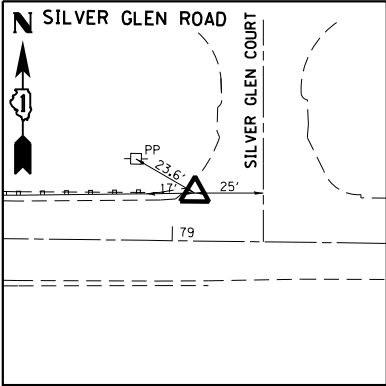


CONTROL POINT 106

SET MAG NAIL
STA. 69+40.56, 18.27' LT
ELEV. 799.59

BENCHMARK #3

ELEV. 801.92
RR SPIKE IN SOUTH FACE OF POWER POLE
NORTHEAST CORNER OF SILVER GLEN ROAD
& WHISPERING TRAIL



CONTROL POINT 107

SET MAG NAIL
STA. 79+07.65, 16.59' LT
ELEV. 776.62

ALIGNMENT COORDINATES

		N	E	STATION
IL. ROUTE 31	PT* A10	1,929,864.0260	987,571.3670	20+02.45
	PT* A11	1,931,284.9960	987,098.6210	35+00.00
	PC CURVE 1	1,931,429.9820	987,049.9150	36+52.94
	PI CURVE 1	1,931,575.8502	986,998.8838	38+07.48
	PT CURVE 1	1,931,729.2580	987,017.5320	39+56.61
	PC CURVE 2	1,931,729.2580	987,017.5320	39+56.61
	PI CURVE 2	1,931,803.8587	987,026.3738	40+31.73
	PT CURVE 2	1,931,876.9390	987,043.7720	41+06.69
SILVER GLEN ROAD	PT* A12	1,932,751.5030	987,235.5110	50+02.02
	PT* A20	1,931,310.6110	985,597.9430	69+98.54
	PC CURVE 1	1,931,292.7150	987,042.7100	84+43.42
	PI CURVE 1	1,931,292.2992	987,071.1409	84+71.85
	PT CURVE 1	1,931,284.9960	987,098.6210	85+00.00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES, AND BENCHMARKS PLAN
IL. ROUTE 31 AT SILVER GLEN ROAD

SCALE: 1"= 200' SHEET OF SHEETS STA. TO STA.

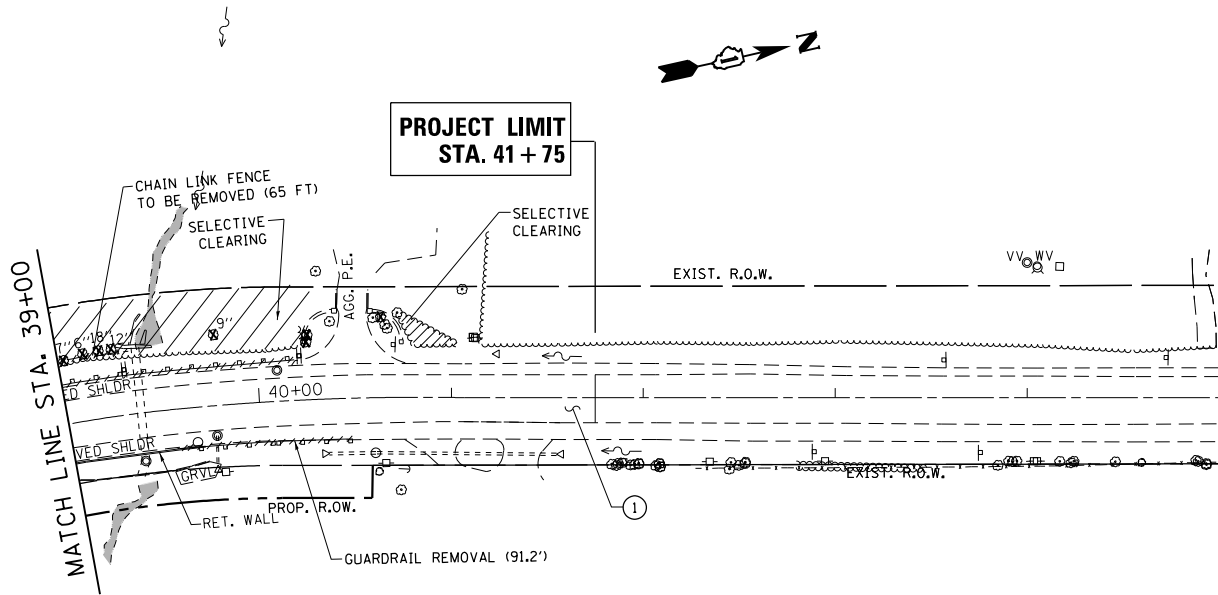
EXIST. CURVE E_IL31-1
PI STA. = 38+07.48
Δ = 26° 12' 47" (RT)
D = 8° 37' 56"
R = 663.74'
T = 154.54'
L = 303.66'
E = 17.75'
e = ----
T.R. = ----
S.E. RUN = ----
P.C. STA. = 36+52.94
P.T. STA. = 39+56.61

EXIST. CURVE E_IL31-2
PI STA. = 40+31.73
Δ = 6° 37' 55" (RT)
D = 4° 25' 08"
R = 1,296.60'
T = 75.12'
L = 150.08'
E = 2.17'
e = ----
T.R. = ----
S.E. RUN = ----
P.C. STA. = 39+56.61
P.T. STA. = 41+06.69

EXIST. CURVE E_SILVER-1
PI STA. = 84+71.85
Δ = 14° 02' 42" (RT)
D = 24° 49' 20"
R = 230.83'
T = 28.43'
L = 56.58'
E = 1.74'
e = ----
T.R. = ----
S.E. RUN = ----
P.C. STA. = 84+43.42
P.T. STA. = 85+00.00

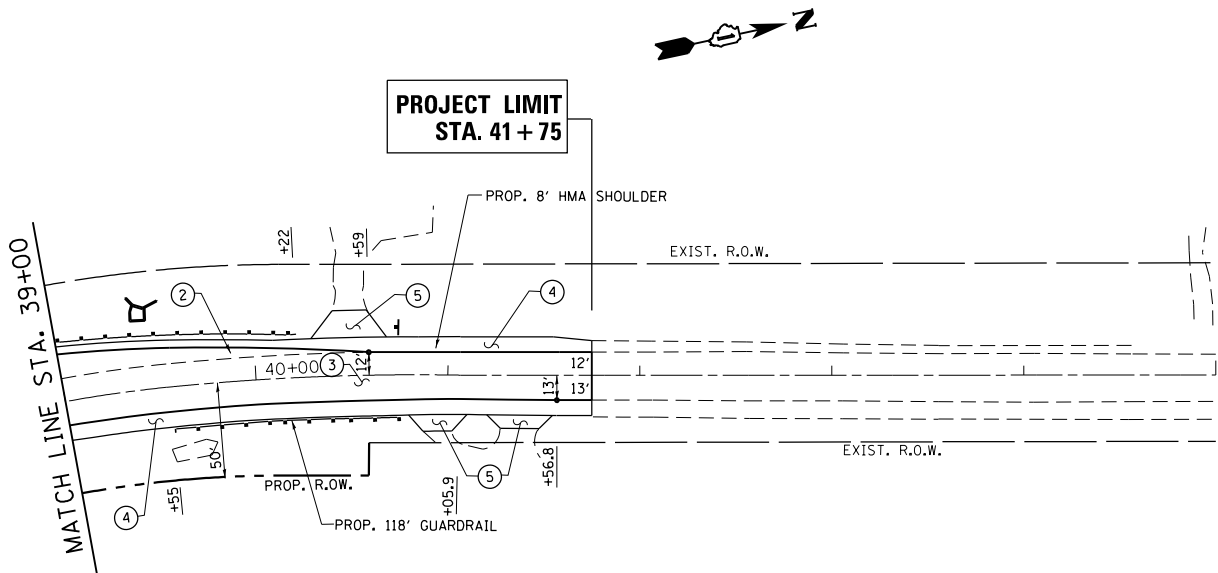
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PLOT DATE = 7/14/2015		DATE -	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	17
CONTRACT NO. 60R28				
ILLINOIS FED. AID PROJECT				



- LEGEND:**
- ① MILLING
 - PROPOSED HOT MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 - ② WIDENING
 - PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm), 1 3/4"
 - PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
 - PROPOSED HMA BASE COURSE, 7 1/2"
 - PROPOSED AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - ③ RESURFACING
 - PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm), 1 3/4"
 - PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
 - ④ HMA SHOULDER
 - PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm), 1 3/4"
 - PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"
 - PROPOSED HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19 mm), 5 1/2"
 - ⑤ HMA PE
 - PROPOSED HOT-MIX ASPHALT BASE COURSE, 6"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
 - ⑥ PCC DRIVEWAY PAVEMENT 6"
 - ⑦ HMA BIKE PATH
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5 mm), 2"
 - PROPOSED AGGREGATE BASE COURSE, TYPE B 6"
 - ⑧ PORTLAND CEMENT CONCRETE SIDEWALK 5"

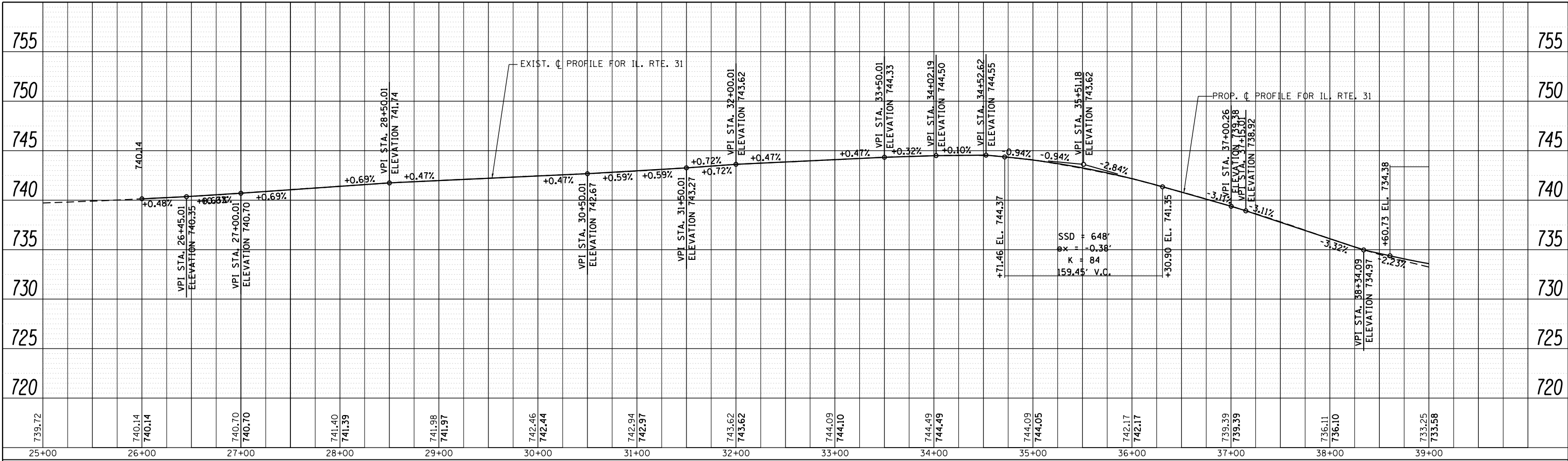
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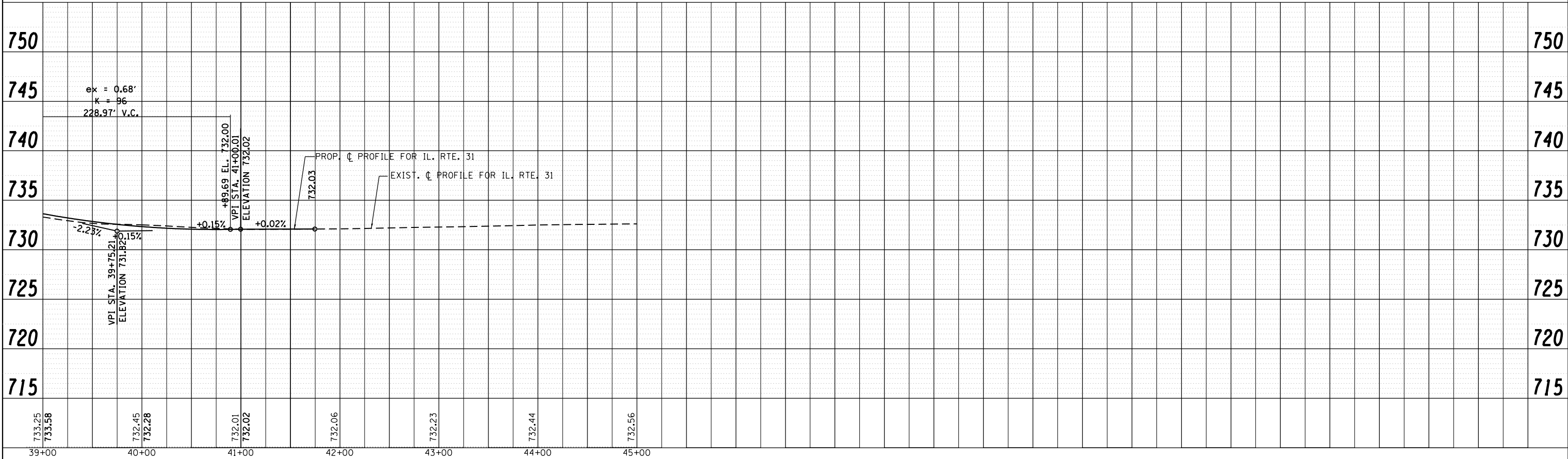
PROPOSED

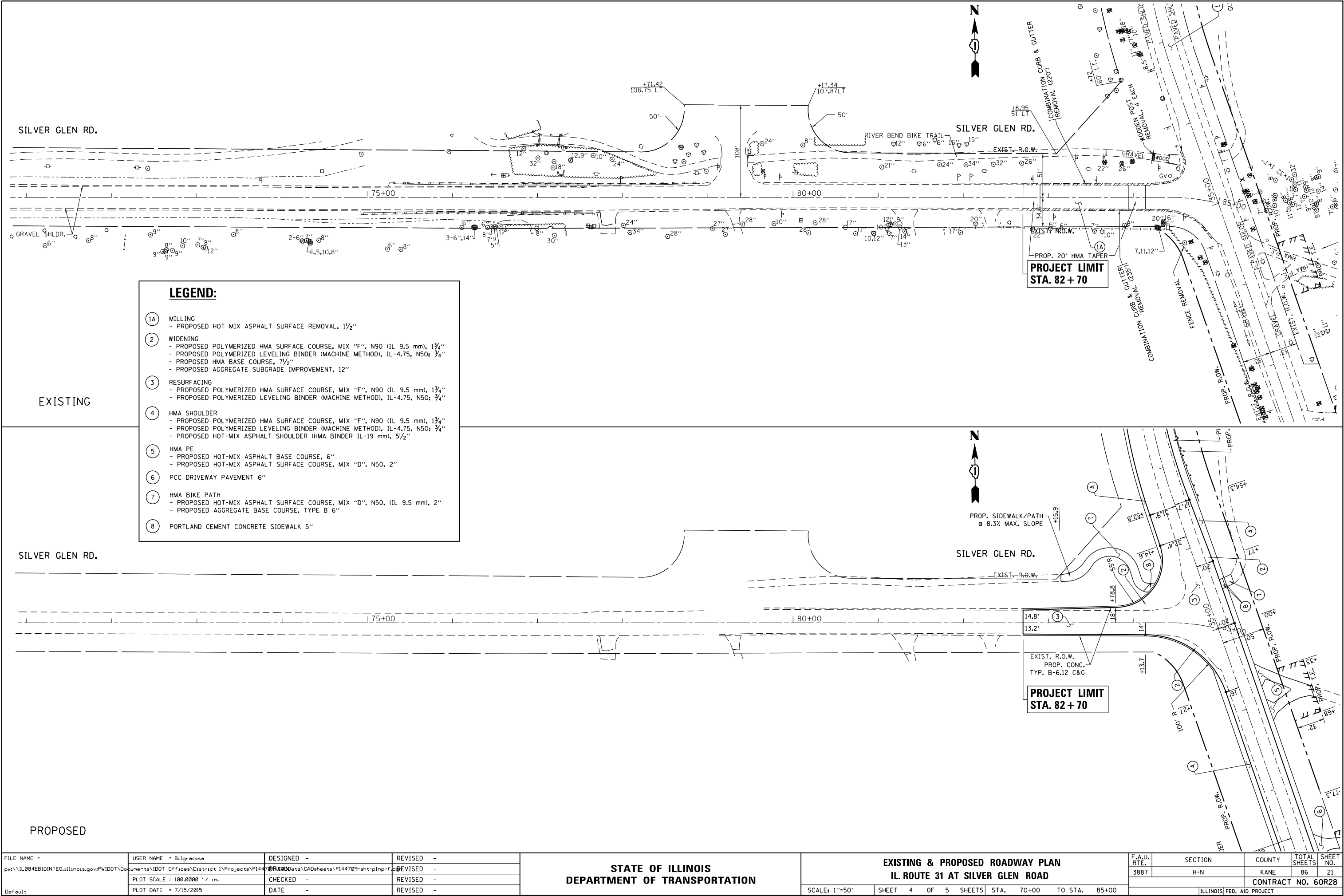
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		CHECKED -	REVISED -				3887	H-N	KANE	86	19
		DATE -	REVISED -				CONTRACT NO. 60R28				
Default	PLOT DATE = 7/15/2015				SCALE: 1"=50'	SHEET 2 OF 5 SHEETS	STA. 39+00 TO STA. 45+00	ILLINOIS FED. AID PROJECT			

PLAN	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK NO.	CHECKED		
	FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		

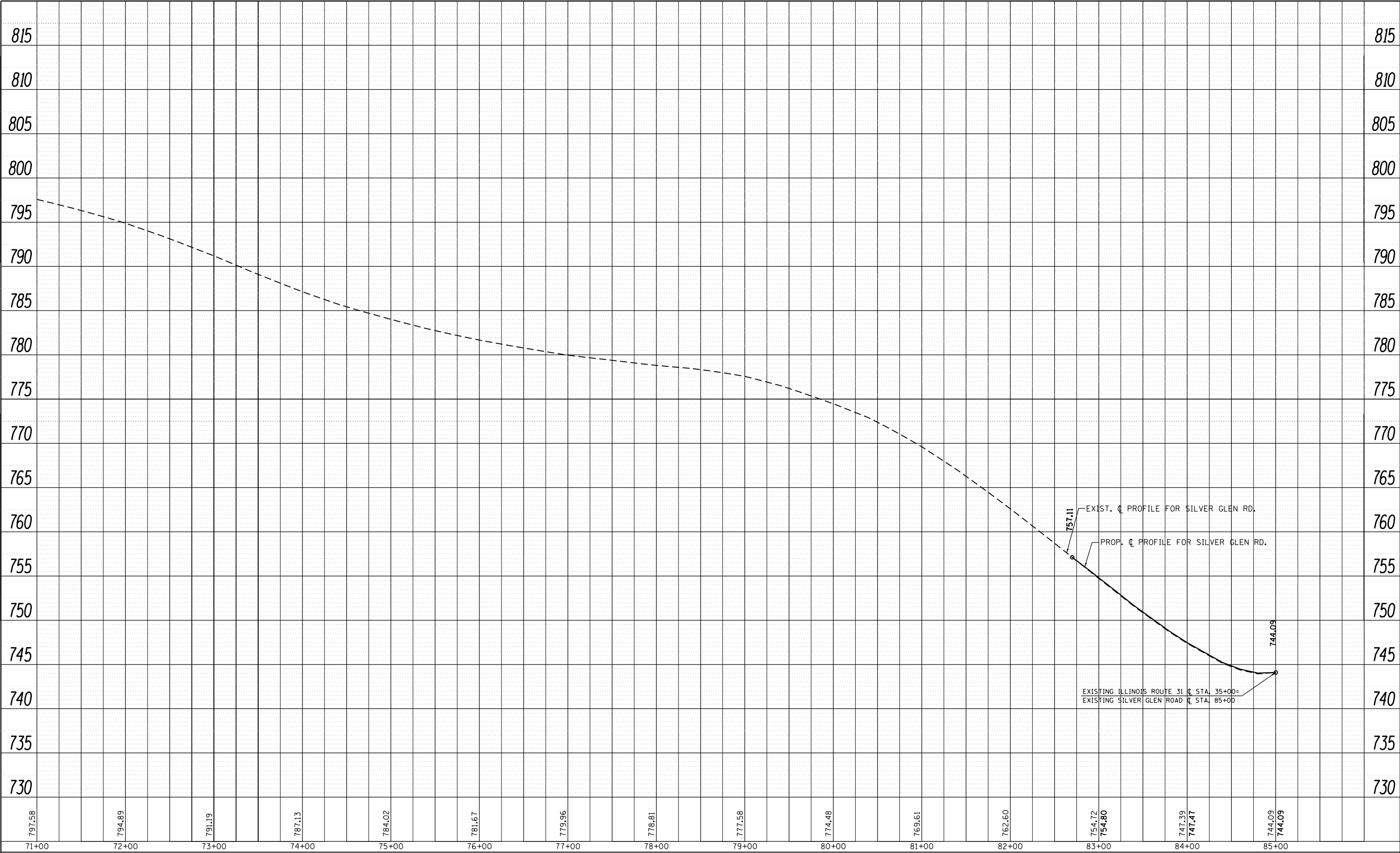




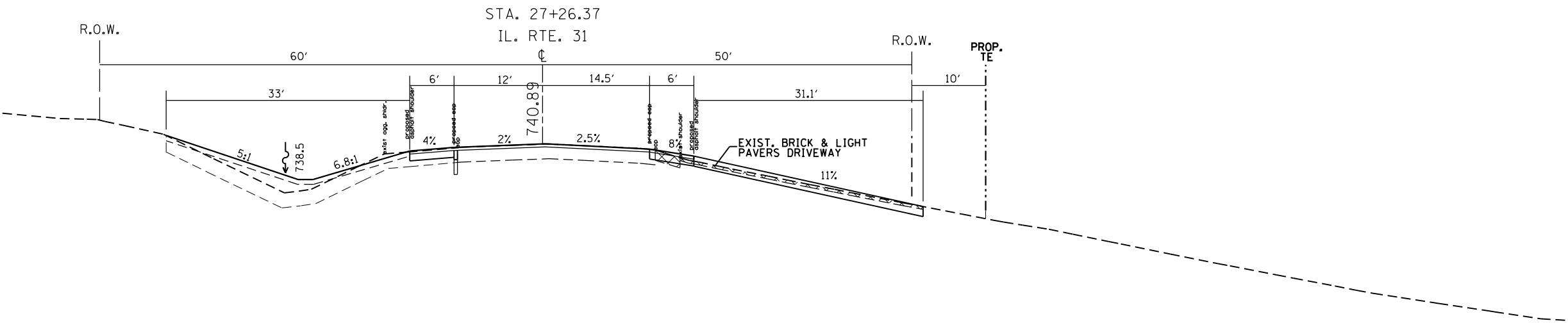
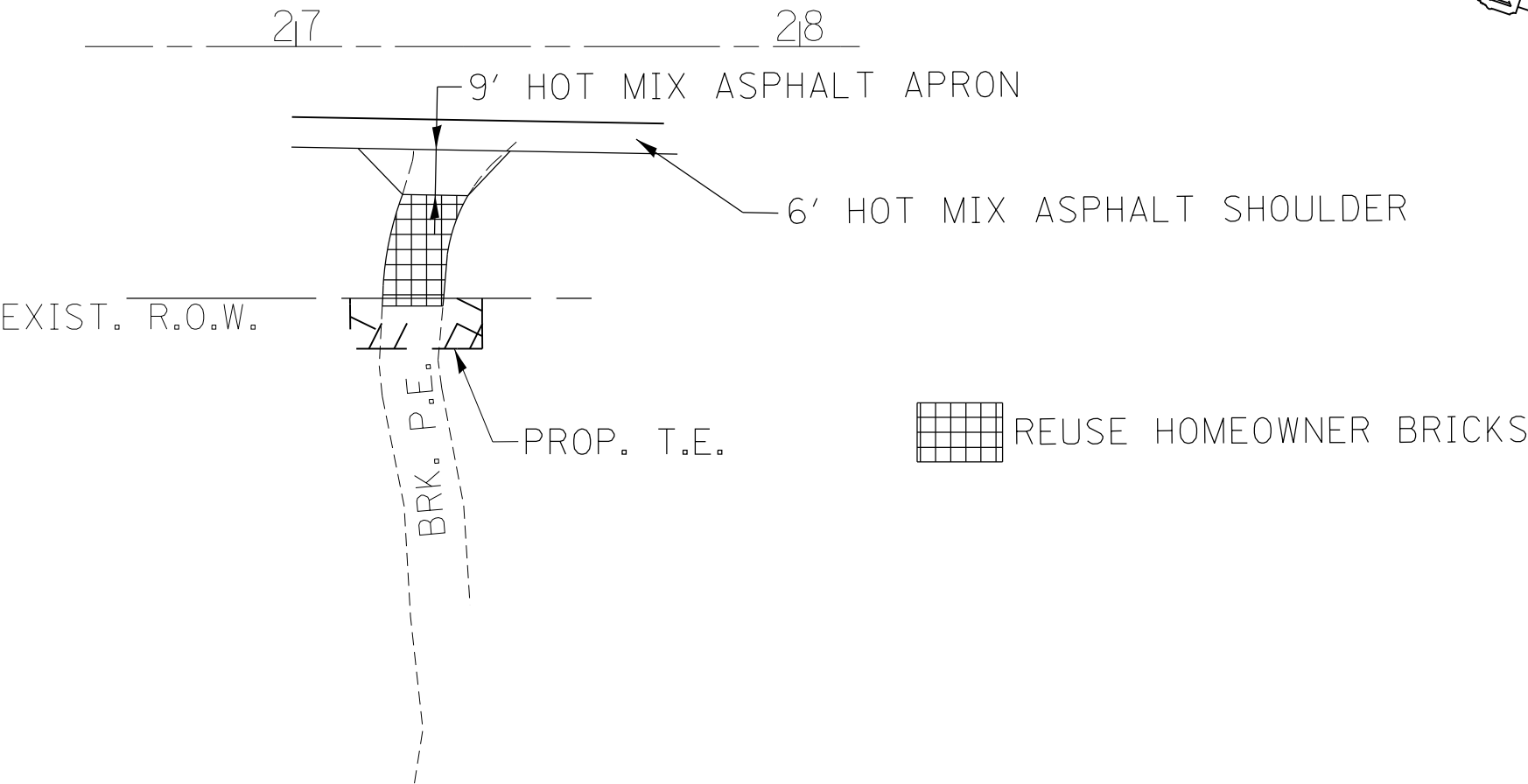
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28								
Default	PLOT DATE = 7/15/2015	DATE -	REVISED -		SCALE: 1"=50'	SHEET 4 OF 5 SHEETS	STA. 70+00 TO STA. 85+00	ILLINOIS FED. AID PROJECT					

PLAN	SURVEYED	BY	DATE
	PLOTTED		
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	ALIGNMENT		
	CADD FILE NAME		

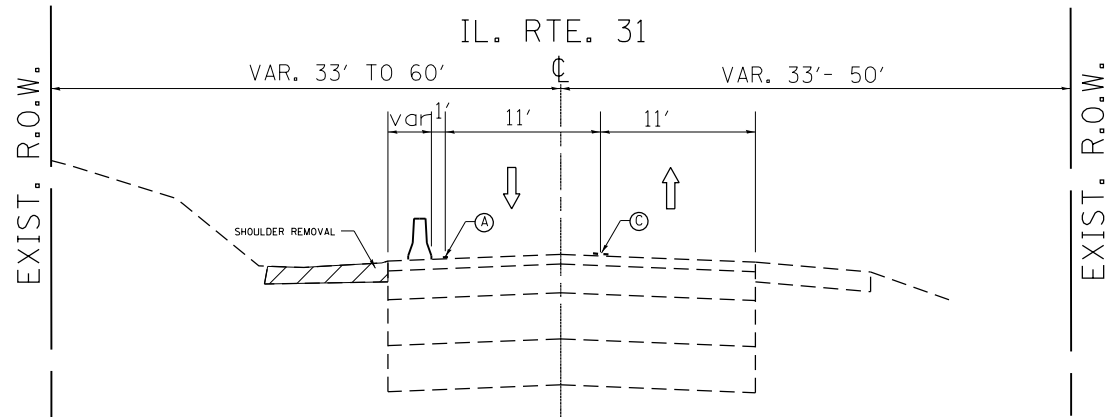
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	STRUCTURE NOTATIONS CHKD		



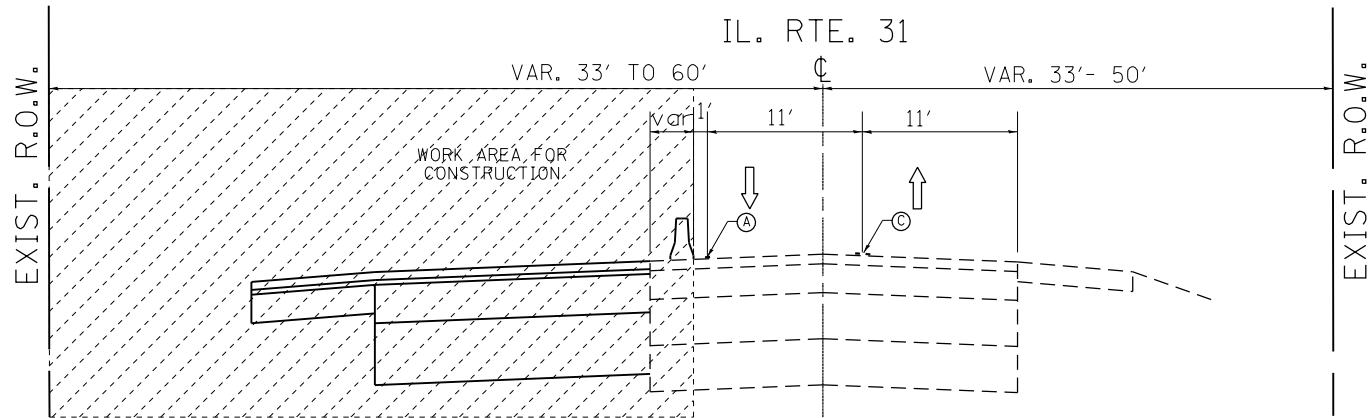
IL. ROUTE 31



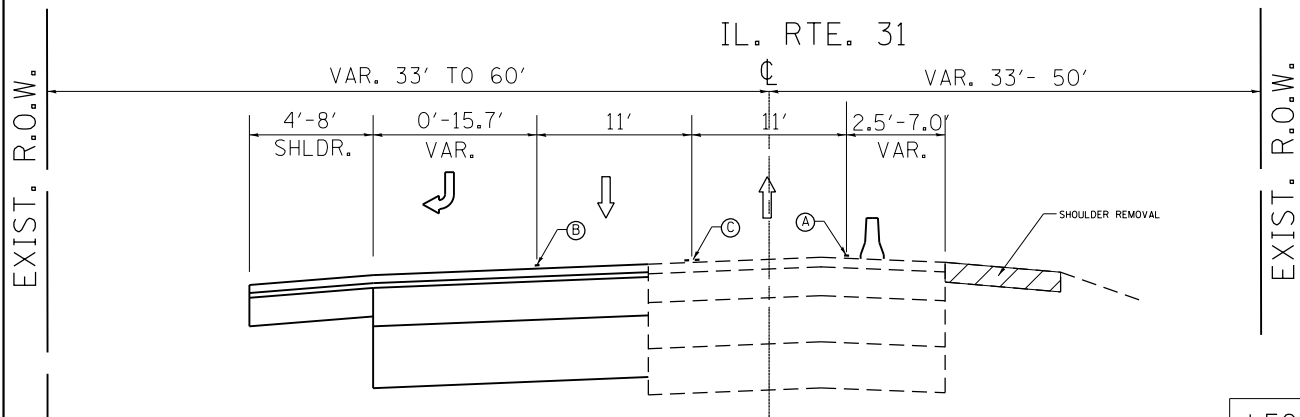
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		CHECKED -	REVISED -				3887	H-N	KANE	86	23
		PLOT DATE = 7/14/2015	DATE -				CONTRACT NO. 60R28				
Default					SCALE: NTS	SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



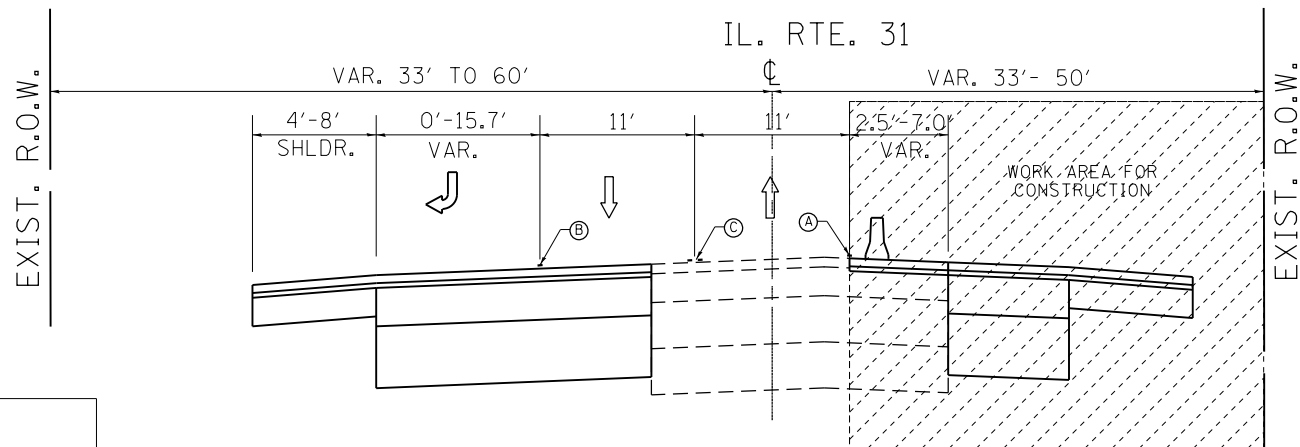
EXISTING TYPICAL SECTION
STAGE 1 REMOVALS
STA. 26+00 TO STA. 41+75
LOOKING NORTH



EXISTING TYPICAL SECTION
STAGE 1 CONSTRUCTION
STA. 26+00 TO STA. 41+75
LOOKING NORTH




EXISTING TYPICAL SECTION
STAGE 2 REMOVALS
STA. 26+00 TO STA. 41+75
LOOKING NORTH




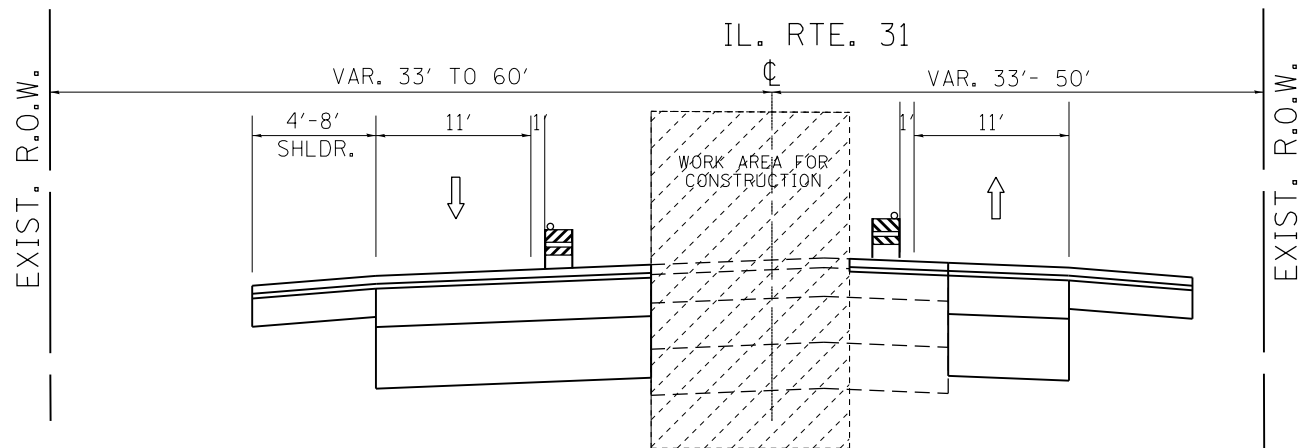
EXISTING TYPICAL SECTION
STAGE 2 CONSTRUCTION
STA. 26+00 TO STA. 41+75
LOOKING NORTH

LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE TURN LANE LINE
- (C) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW

 WORK ZONE

 SHOULDER REMOVAL



EXISTING TYPICAL SECTION
STAGE 3 CONSTRUCTION
STA. 26+00 TO STA. 41+75
LOOKING NORTH

STAGE 1 REMOVALS:

- PROVIDE 11' THRU LANES
- PLACE TEMPORARY CONCRETE BARRIER SOUTHBOUND SIDE OF IL 31
- REMOVE SHOULDER ON THE SOUTHBOUND SIDE OF IL 31

STAGE 1 CONSTRUCTION:

- CONSTRUCT NEW SB PAVEMENT, SHOULDER AND CURB AND GUTTER
- REGRADE DITCH AND PERFORM CULVERT WORK

STAGE 2 REMOVALS:

- MOVE TRAFFIC OVER TO NEWLY CONSTRUCTED PAVEMENT, PROVIDING 11' THRU LANES AND A S/B TURN LANE
- REMOVE SHOULDER ON THE NORTHBOUND SIDE

STAGE 2 CONSTRUCTION

- CONSTRUCT NEW N/B PAVEMENT, SHOULDER
- REGRADE DITCH AND PERFORM CULVERT WORK

STAGE 3 CONSTRUCTION

- RESURFACE MEDIAN LANE

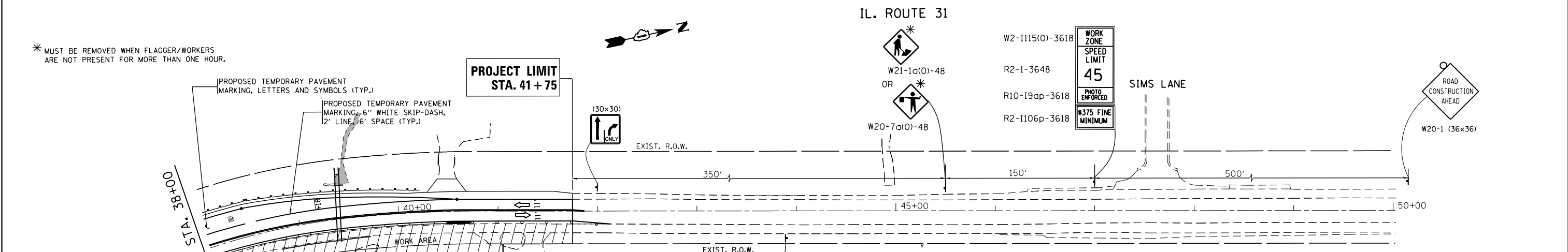
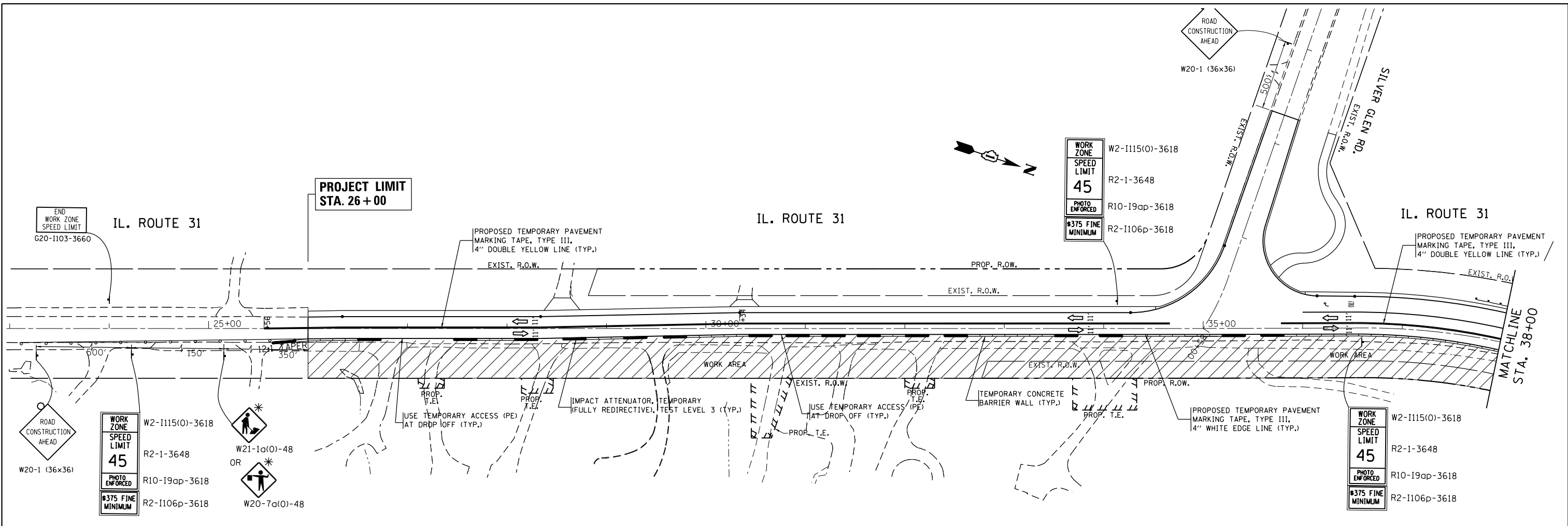
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Default	PLOT SCALE = 75.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 7/14/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGING TYPICALS
IL RTE 31 AT SILVER GLEN RD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	24
CONTRACT NO. 60R28				
ILLINOIS FED. AID PROJECT				



NOTES

REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE SHALL BE PAID FOR UNDER WORK ZONE PAVEMENT MARKING REMOVAL.

THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL) LUMP SUM.

IN ADDITION TO PINNING THE TEMPORARY BARRIER WALL END SECTIONS ON BOTH SIDES OF THE WALL, A 3'-6" DEFLECTION AREA IS REQUIRED FROM THE BACK SIDE OF THE TEMPORARY BARRIER WALL TO ANY OBSTRUCTION OR DROP OFF IN THE WORK ZONE. IF THIS 3'-6" DEFLECTION AREA CANNOT BE MAINTAINED, THE TEMPORARY CONCRETE BARRIER WALL SHALL BE ANCHORED TO THE PAVEMENT THROUGH THE 3 ANCHORING HOLES ON THE TRAFFIC SIDE OF THE TEMPORARY CONCRETE BARRIER WALL. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE TEMPORARY CONCRETE BARRIER WALL PAY ITEM.

LEGEND

- WORK AREA
- TEMPORARY CONCRETE BARRIER WALL
- TYPE II BARRICADES OR DRUMS WITH STEADY BURN BIDIRECTIONAL LIGHT PER HIGHWAY STANDARD 701326
- SIGN
- IMPACT ATTENUATOR, TEMPORARY (FULLY REDIRECTIVE), TEST LEVEL 3 (TYP.)

FILE NAME =	USER NAME = Bilgrami	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE II IL. ROUTE 31 AT SILVER GLEN ROAD				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBID\TEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\P144709\Drawings\Design\P144709-shr-Stage2.dgn			REVISED -						3887	H-N	KANE	88	26
Default		CHECKED -	REVISED -						CONTRACT NO. 60R28				
	PLOT DATE = 7/14/2015	DATE -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	

EROSION CONTROL GENERAL NOTES

1. THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN EROSION CONTROL MEASURES IMMEDIATELY AFTER STRIPPING OF EXISTING VEGETATION.
2. NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE SITE OTHER THAN THROUGH A SERIES OF TEMP DITCH CHECKS. THE CONTRACTOR WILL ADJUST HIS OPERATIONS AND IMPLEMENT EROSION CONTROL MEASURES ACCORDINGLY.
3. THE QUANTITIES SHOWN FOR TEMPORARY DITCH CHECKS ARE MEASURED PER FOOT, REGARDLESS OF TYPE OR CONFIGURATION USED.
4. THE CONTRACTOR SHALL SURROUND ALL EARTH STOCKPILES WITH SILT FENCE, THIS SHALL BE PAID FOR AS PERIMETER EROSION BARRIER. EROSION CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR AND ENGINEER AFTER ANY STORM EXCEEDING 0.5 INCH OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED AFTER EACH SIGNIFICANT SNOWMELT.
5. STOCKPILES OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN THREE (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (I.E, PERIMETER SILT FENCE). STOCKPILES TO REMAIN IN PLACE FOR 14 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING WITHIN 1 DAY.
6. ALL CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMIT.
7. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PREVENT POLLUTION OF STORM WATER AND SHALL FOLLOW IEPA & IDOT CONSTRUCTION MEMORANDUM NO. 06-60.
8. EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH SEQUENCE OF STAGE CONSTRUCTION.
9. STABILIZATION MEASURES SHALL BE INTIATED AS SOON AS PRACTICAL, BUT IN NO CASE EXCEED 1 DAY AFTER CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY WILL NOT OCCUR FOR A PERIOD OF 14 OR MORE CALENDER DAYS.
- 10.THE CONTRACTOR SHALL APPLY TEMPORARY EROSION CONTROL SEEDING TO ALL ERODIBLE BARE EARTH AREAS WITHIN THE CONTRACT LIMITS EACH WEEK, REGARDLESS OF WEATHER CONDITIONS OR PROGRESS OF THE WORK. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, ERODIBLE EMBANKMENT AND EXCAVATION AREAS WHERE WORK IS IN PROGRESS SHALL BE INCLUDED ON THE AREAS TO BE SEEDED. SEE SPECIAL PROVISION FOR TEMPORARY EROSION CONTROL SEEDING.
11. ALL PERIMETER EROSION BARRIER SHALL BE PLACED IN STAGE I, IF REQUIRED IN STAGE II IT SHALL BE LEFT IN PLACE. IT SHALL ONLY BR REPLACED IF DAMAGED, AT THE DIRECTION OF THE ENGINEER.
- 12.REFER TO LANDSCAPING PLAN FOR AREAS TO BE PERMANENTLY SEEDED
- 13.UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL REVISED JUNE 2013.
- 14.A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
- 15.DEWATERING SHALL BE CONDUCTED IN ACCORDANCE WITH IUM STANDARD 813 (UPDATED 06/2010). DEWATERING INTO DRAIN TILES IS STRICTLY PROHIBITED. COMPROMISED DRAIN TILES SHOULD BE IMMEDIATELY REPAIRED OR INCORPORATED INTO STORM WATER FACILITIES.
- 16.ALL DROP INLETS ON AND ADJACENT TO THE SITE MUST HAVE A SEDIMENT TRAPPING OR CONTAINMENT DEVICE INSTALLED DURING CONSTRUCTION ACTIVITIES.
- 17.ALL ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY AND CLEANED WHEN NECESSARY.

18. EROSION CONTROL BLANKET AND/OR STRAW MULCH WITH NETTING (DEPENDING ON SLOPE, SLOPE LENGTH, AND FLOW RATES) SHALL BE INSTALLED ON ALL SLOPES AND IN CRITICAL AREAS IMMEDIATELY UPON FINAL GRADING.
19. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
20. IN AREAS WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN 1 DAY OF COMPLETION, AND IN AREAS WHERE WORK HAS TEMPORARILY CEASED FOR 14 DAYS OR MORE, TEMPORARY STABILIZATION SHALL OCCUR BY THE 1ST DAY AFTER WORK HAS CEASED.
21. COMPLETED SLOPES SHALL BE SEEDED AND MULCHED (OR BLANKETED, IF APPLICABLE) AS THE EXCAVATION PROCEEDS TO THE EXTENT CONSIDERED DESIRABLE AND PRACTICAL. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WATER IN AND NEAR THE CRITICAL AREAS SHOULD BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOW. THE STREAM BANKS SHOULD BE STABILIZED AT THE END OF EACH DAY. ONCE WORK IN THIS AREA BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS.
22. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS, PRIOR TO WORKING IN BWU AREAS. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
23. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE.
24. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION.
25. THE CONTRACTOR SHALL CHECK ALL ESC MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.
26. THE CONTRACTOR SHOULD PROVIDE TO THE RE A PLAN TO ENSURE THAT A STABILIZED FLOW LINE WILL BE PROVIDED DURING STORM SEWER CONSTRUCTION. THE USE OF A STABILIZED FLOW LINE BETWEEN INSTALLED STORM SEWER AND OPEN DISTURBANCE WILL REDUCE THE POTENTIAL FOR THE OFFSITE DISCHARGE OF SEDIMENT-BEARING WATERS, ESPECIALLY WHEN RAIN IS FORECASTED, SO THAT FLOW WILL NOT ERODE. LACK OF APPROVED PLAN OR FAILURE TO COMPLY WILL RESULT IN AN ESC DEFICIENCY DEDUCTION.
27. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
28. EROSION CONTROL ITEMS ARE CONSIDERED TO BE A HIGH PRIORITY ON THIS CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE RE.
29. "WETLANDS NO INTRUSION" SIGNAGE SHOULD ALSO BE PROVIDED AT THE BOUNDARY OF ALL UN-IMPACTED WETLANDS AND/OR WOUS. THE CONTRACTOR CAN BORROW THE SIGNS FROM THE BUREAU OF MAINTENANCE. INCLUDE TEMPORARY FENCING AND WETLAND SIGNAGE WITHIN THE EROSION AND SEDIMENT CONTROL STRATEGY.

FILE NAME = pww\1L084EBIDINTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI44709\Drawings\Design\PI44709-shr-details.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. ROUTE 31 AT SILVER GLEN RD. EROSION CONTROL NOTES			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			REVISED -					3887	H-N	KANE	86	27
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -					CONTRACT NO. 60R28				
	PLOT DATE = 7/14/2015	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
SCALE:		SHEET NO. OF SHEETS		STA.		TO STA.						

TEMPORARY EROSION CONTROL
SEQUENCE OF CONSTRUCTION

1. ESTABLISH TEMPORARY EROSION CONTROL MEASURES AND ERECT SILT FENCE ALONG SITE BOUNDARIES AS SHOWN ON PLAN PRIOR TO MASS EARTHWORK.
2. INSTALL DITCH CHECKS IMMEDIATELY AFTER DITCH DISTURBANCE STARTS.
3. INSTALL PERMANENT SEEDING BEFORE BEGINNING NEXT CONSTRUCTION STAGE.

NOTE:

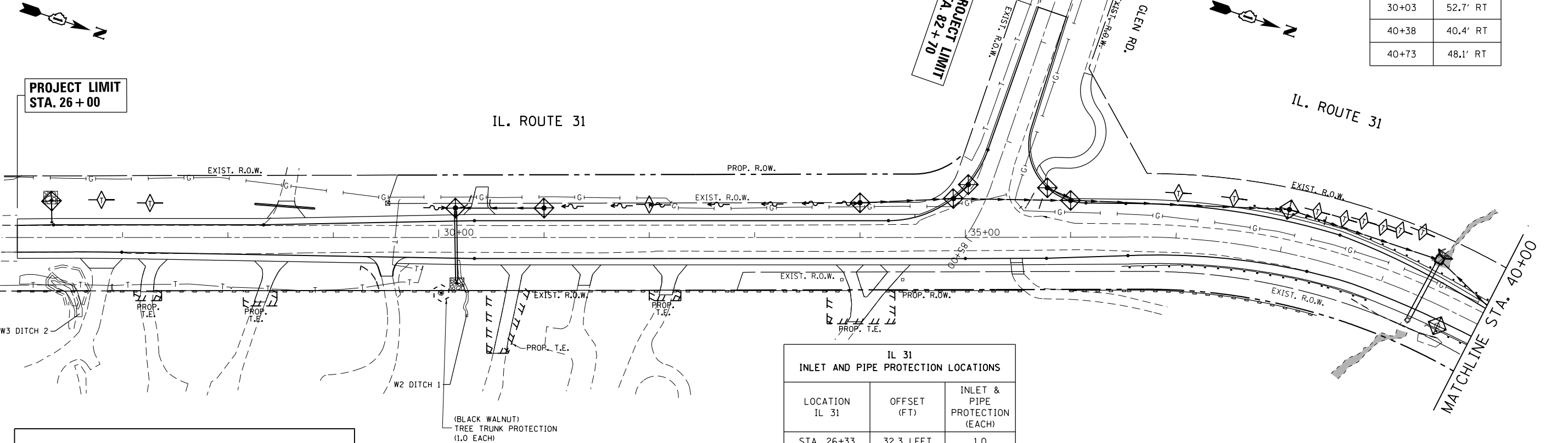
- 1) THIS PROJECT REQUIRES A U.S. ARMY COPRPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT.
- 2) INSTALL PERIMETER EROSION BARRIER ALONG R.O.W. & TEMPORARY EASEMENTS, AS SHOWN ON PLANS. PERIMETER EROSION CONTROL SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENTS.
- 3) EXCAVATION FOR THE INSTALLATION OF RIP RAP WILL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL" PER CU. YD.
- * 4) INSTALL INLET FILTERS ON ALL OPEN STRUCTURES. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE INVOLVED AREA HAS BEEN DISTURBED.
- 5) TEMPORARY EROSION CONTROL SEEDING IS TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS.
- 6) AT ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND COVER SHALL REMAIN.
- 7) THE EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH A TIME AS DIRECTED BY THE ENGINEER.

PERIMETER EROSION BARRIER LOCATIONS

LOCATION	OFFSET	FROM STATION	TO STATION
IL 31	50' RT.	26+00	38+50

TREE TRUNK PROTECTION

STATION	OFFSET
30+03	52.7' RT
40+38	40.4' RT
40+73	48.1' RT



EROSION CONTROL LEGEND

SYMBOL

DESCRIPTION



INLET FILTER *



TEMPORARY DITCH CHECK



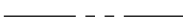
PROP. RIP RAP



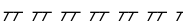
PERIMETER EROSION BARRIER
(SILT FENCE, SEE STANDARD 280001)



EXIST. R.O.W.



PROP. R.O.W.



PROP. TEMPORARY EASEMENT



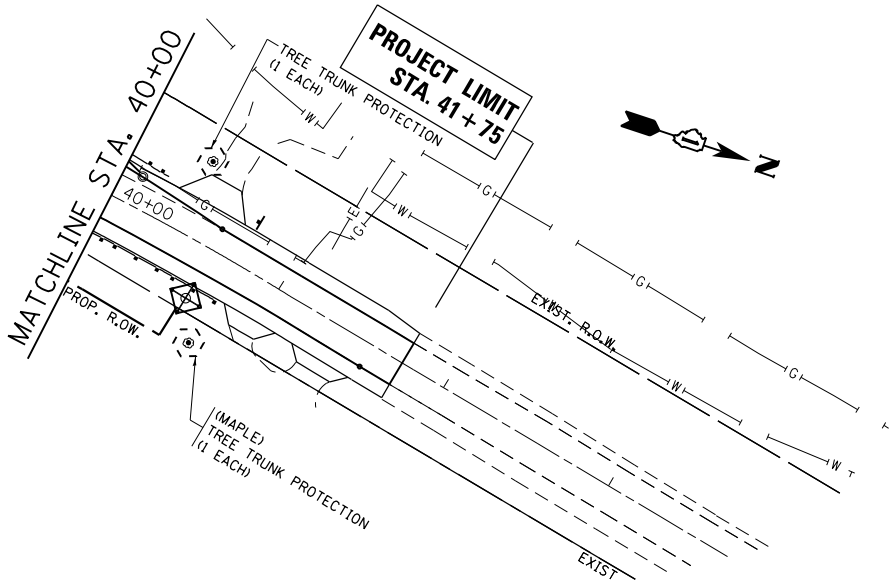
ITEMS TO BE INSTALLED BEFORE
THE AREA IS DISTURBED.

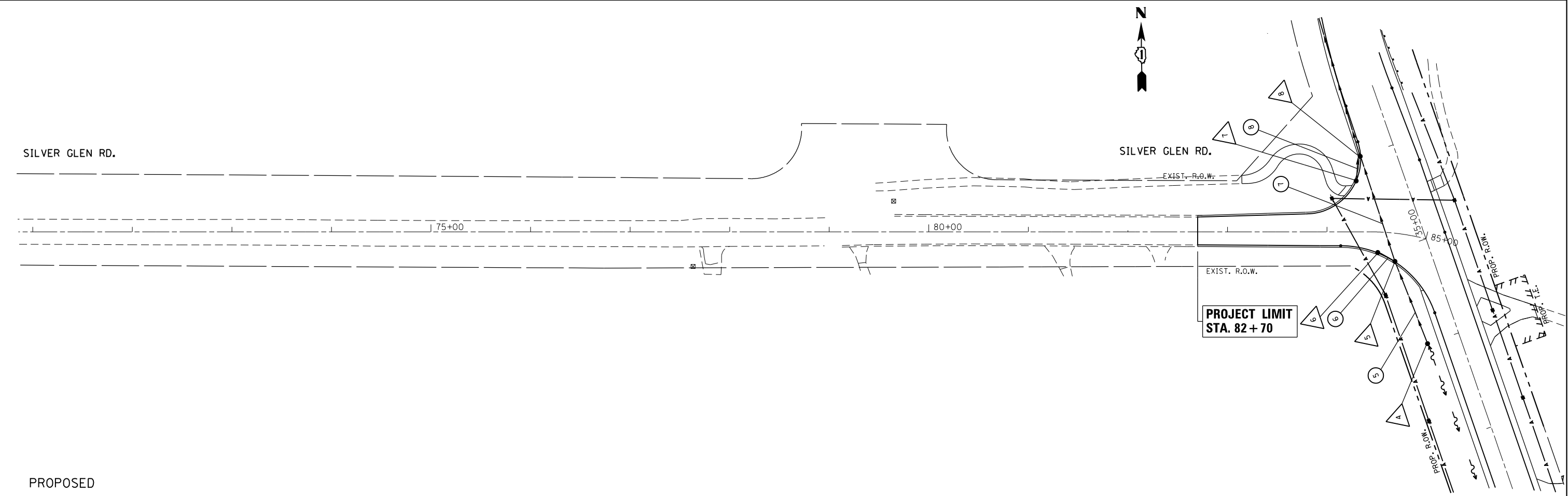
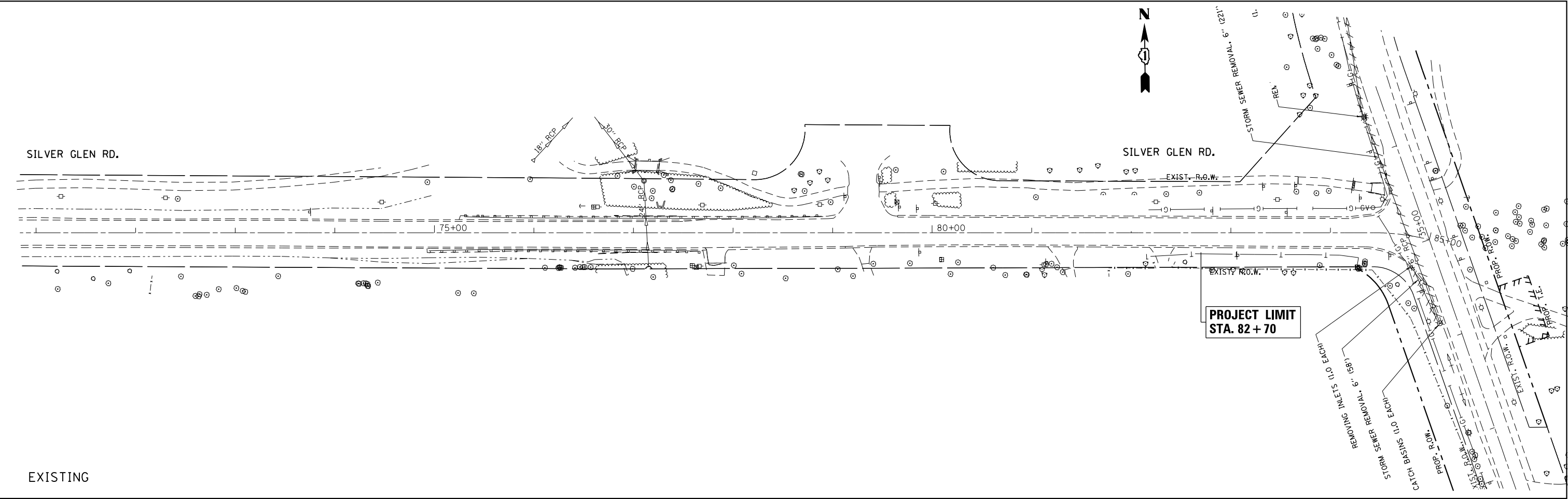
IL 31
TEMPORARY DITCH CHECK LOCATIONS

STATION	OFFSET	STATION	OFFSET
26+80	35.0' LEFT	38+50	44.5' LEFT
27+26	32.0' LEFT	38+67	47.7' LEFT
32+00	30.6' LEFT	38+85	47.7' LEFT
37+00	42.0' LEFT	39+00	50.8' LEFT
37+50	42.0' LEFT	39+19	56.8' LEFT
38+25	44.5' LEFT		

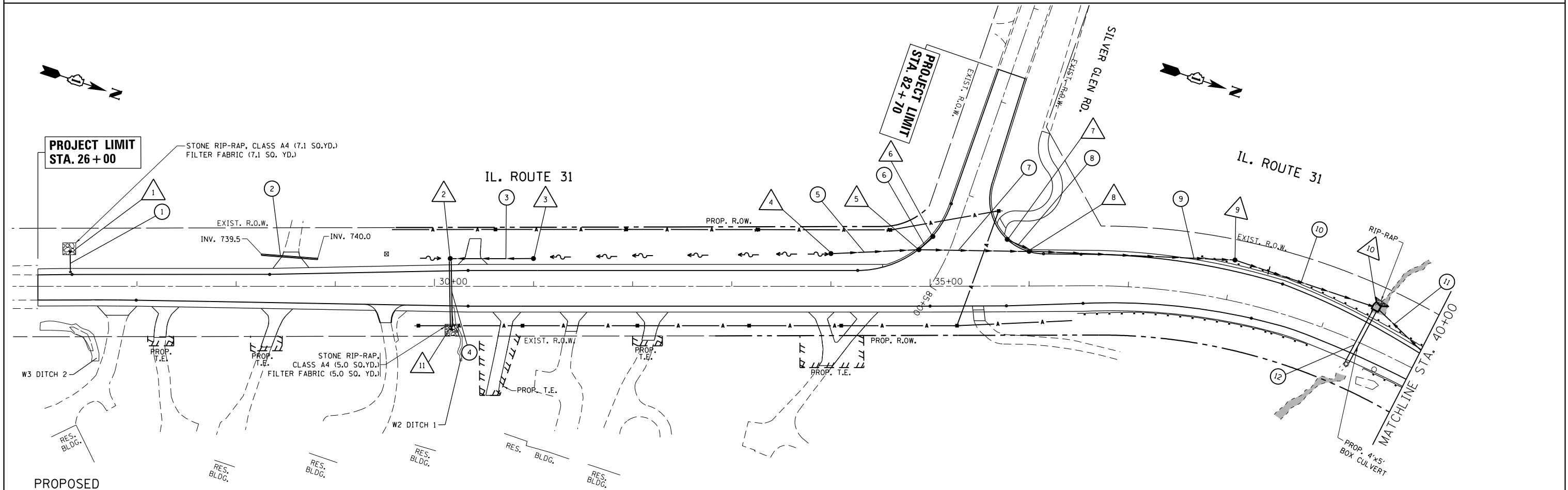
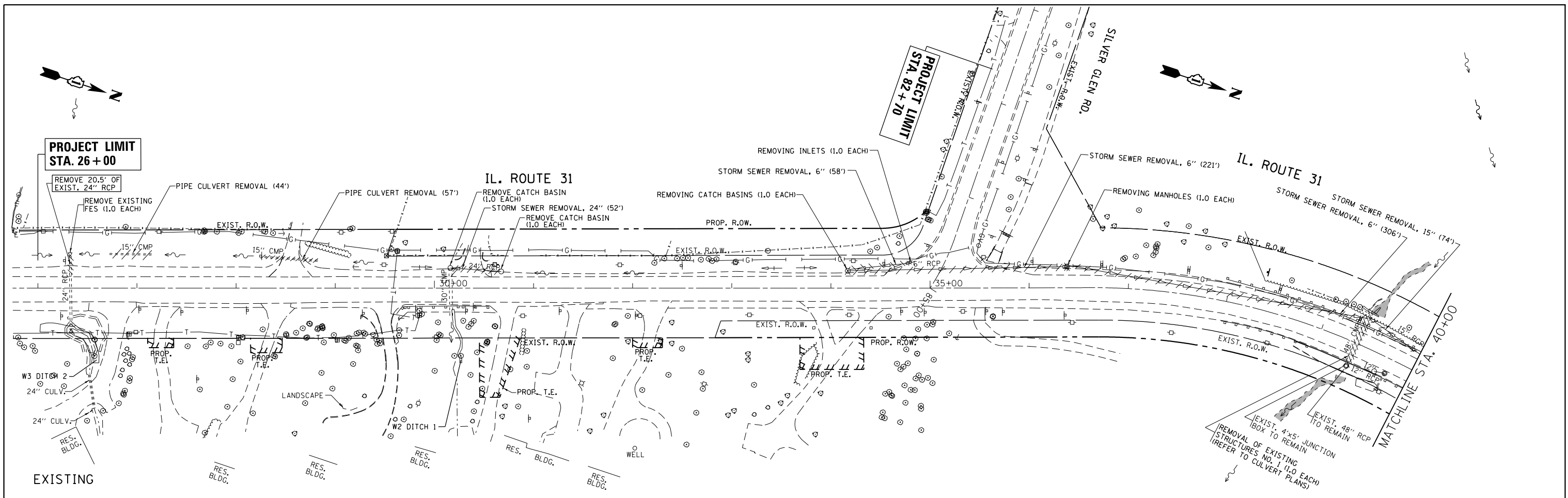
IL 31
INLET AND PIPE PROTECTION LOCATIONS

LOCATION IL 31	OFFSET (FT)	INLET & PIPE PROTECTION (EACH)
STA. 26+33	32.3 LEFT	1.0
STA. 30+16	28.0 LEFT	1.0
STA. 30+16	40.9 RIGHT	1.0
STA. 31+00	28.0 LEFT	1.0
STA. 34+00	32.0 LEFT	1.0
STA. 34+90	38.4 LEFT	1.0
STA. 35+02	50.3 LEFT	1.0
STA. 35+78	47.0 LEFT	1.0
STA. 36+00	35.0 LEFT	1.0
STA. 38+00	40.8 LEFT	1.0
STA. 39+43	44.7 LEFT	1.0
STA. 39+38	25.1 RIGHT	1.0
STA. 39+66	18.2 RIGHT	1.0
STA. 40+36	28.3 RIGHT	1.0
STA. 40+60	28.7 RIGHT	1.0
STA. 41+21	22.8 LEFT	1.0
STA. 41+55	28.9 RIGHT	1.0
TOTAL		17



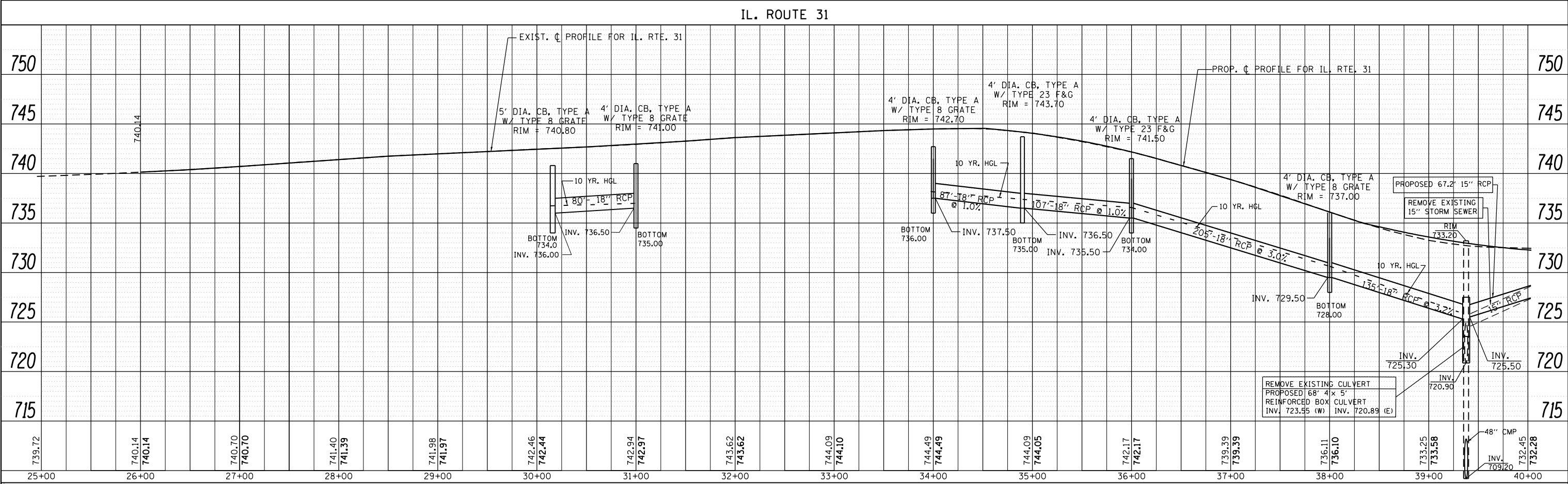


FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING & PROPOSED DRAINAGE AND UTILITY PLAN SILVER GLEN RD. AT IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\P144709\Drawings\GADsheets\P144709-shd-drainage		REVISED -	REVISED -		3887	H-N	KANE	86	29				
Default	PLOT SCALE = 100.0000' / 1 in.	CHECKED -	REVISED -		CONTRACT NO. 60R28								
	PLOT DATE = 7/15/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO STA.			

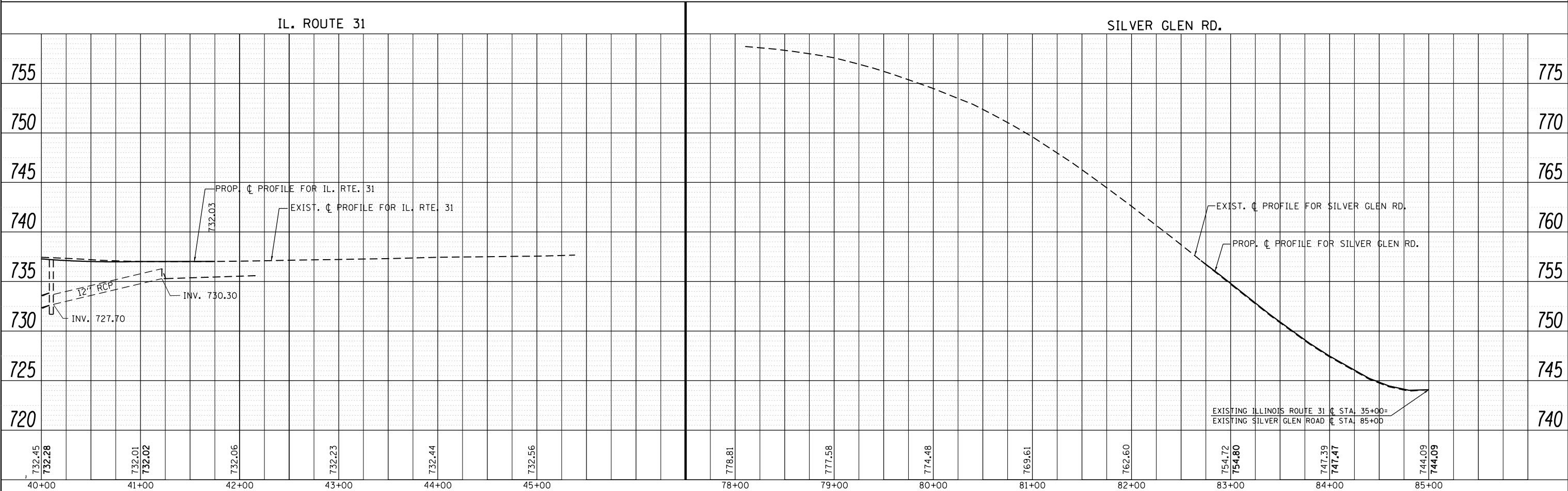


FILE NAME =	USER NAME = Bilgrimes	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>EXISTING & PROPOSED DRAINAGE AND UTILITY PLAN IL. ROUTE 31 AT SILVER GLEN RD.</div>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\P144709\Drawings\GAD\Sheets\P144709-sht-drainage		ORAWN	REVISED -						3887	H-N	KANE	86	30
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		<div>SCALE: 1"=50' SHEET OF SHEETS STA. 26+00 TO STA. 40+00</div>				<div>CONTRACT NO. 60R28</div>				
	PLOT DATE = 7/15/2015	DATE -	REVISED -										

PLAN	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK NO.	CHECKED		
	FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		



NOTE: ALL OFFSETS ARE GIVEN TO CENTER OF STRUCTURE.



REINFORCED CONC. END SECTION
STANDARD 542001
WITH 18" SUMP & GRATE
A= 7' B=6' H=6"
INV. =723.5



CAST IN PLACE
CONCRETE JUNCTION BOX
(REFER TO CULVERT PLANS)

10

18" DIA., STORM SEWER CLASS A, TY-2, 140 FT,
TBF = N/A



CB, TY-A, 5 FT. DIA., W/ TY-8 GRATE
STA. 30+16, 28 FT. (LT)
T.G. 740.8
INV 735.5 (E)
INV 736.0 (N)



CONCRETE FLARED END SECTION
INV 735.0

11

15" DIA., STORM SEWER CLASS A, TY-2, 71.4 FT,
TBF = 10.3 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-8 GRATE
STA. 31+00, 28 FT. (LT)
T.G. 741.0
INV. 736.5 (S)



24" DIA., STORM SEWER CLASS A, TY-2, 20.5 FT.
TRENCH BACK FILL = 1.9 CU YD

12

PRECAST CONCRETE BOX CULVERT 4' X 5' (SPECIAL)
(REFER TO CULVERT PLANS)



CB, TY-A, 4 FT. DIA., W/ TY-8 GRATE
STA. 34+00, 32 FT. (LT)
T.G. 742.7
INV. 737.5 (S)



15" DIA., PIPE CULVERT CLASS A, TY-1, 57 FT.
TBF = 4.5 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-23 FR & GR
STA. 34+90, 38.4 FT. (LT)
T.G. 743.7
INV. 736.5 (S.N.W)



18" DIA., STORM SEWER CLASS A, TY-2, 80 FT.
TBF = 26.2 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-23 FR & GR
STA. 35+02, 50.3 FT. (LT)
T.G. 744.0
INV. 736.8



18" DIA., STORM SEWER CLASS A, TY-2, 87 FT.
TBF = 5.6 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-23 FR & GR
STA. 35+78, 47 FT. (LF)
T.G. 742.0
INV. 736.0



12" DIA., STORM SEWER CLASS A, TY-2, 15 FT.
TBF = 8.5 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-23 FR & GR
STA. 36+00, 35 FT. (LT)
T.G. 741.5
INV. 735.5 (S.&N.)



18" DIA., STORM SEWER CLASS A, TY-2, 107 FT.
TBF = 70.9 CU YD



CB, TY-A, 4 FT. DIA., W/ TY-8 GRATE
STA. 38+00, 40.8 FT. (LT)
T.G. 737.0
INV. 729.5 (S.&N.)



18" DIA., STORM SEWER CLASS A, TY-2, 205 FT.
TBF = 177.3 CU YD

LONGITUDINAL PIPE UNDERDRAIN TABLE						
LOCATION	STATION - STATION	O/S (FT)	DIAMETER (IN)	LENGTH (FT)	NORTH OUTLET	SOUTH OUTLET
IL 31	26+00 - 28+00	RT	4	222	DITCH	DITCH
IL 31	26+00 - 28+00	LT	4	227	DITCH	DITCH
IL 31	39+00 - 40+10	LT	4	122	EXIST. MH	DITCH

PIPE UNDERDRAIN NOTES:

LONGITUDINAL UNDERDRAINS MUST BE PLACED ALONG THE OUTSIDE EDGES OF THE PROPOSED WIDENING AT APPROXIMATELY THESE LOCATIONS.

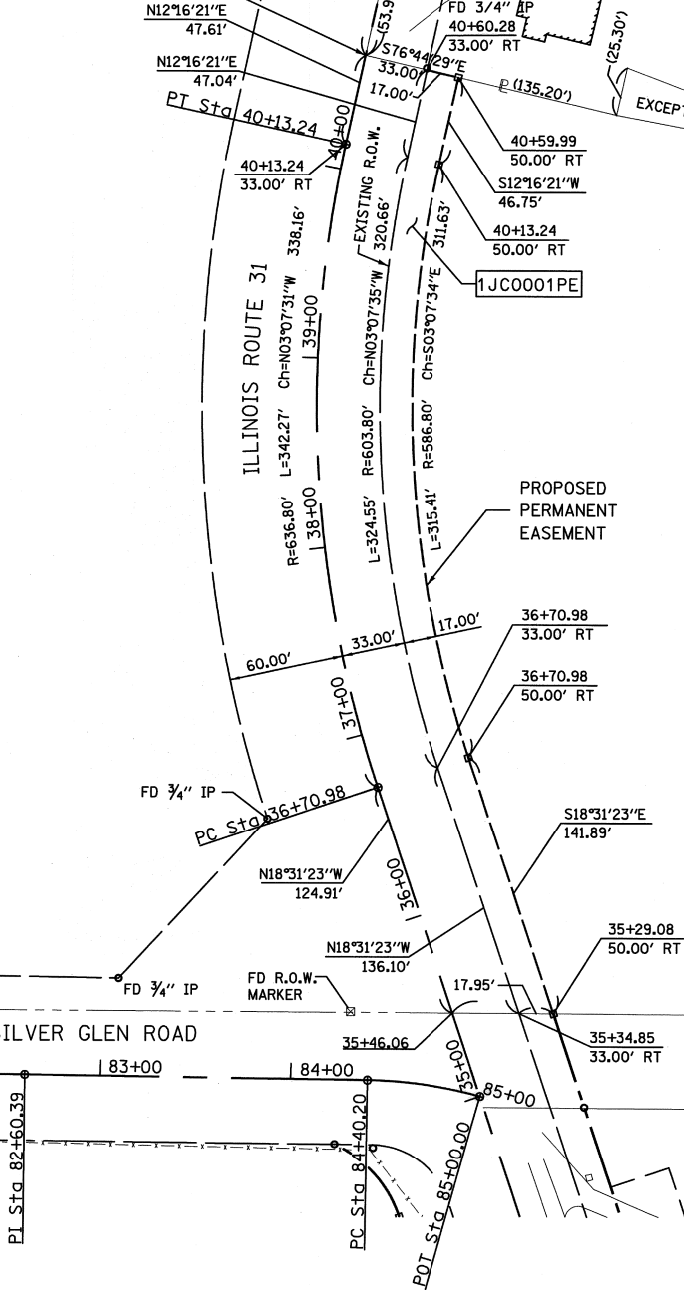
ALL PIPE UNDERDRAINS SHALL BE PLACED AT A DEPTH OF 30" BELOW THE TOP OF PROPOSED PAVEMENT OR AS DEEP AS POSSIBLE AND IN ACCORDANCE WITH CHECK SHEET #19 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS. THE COST OF MAKING PIPE UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF PIPE UNDERDRAINS ITEM.

FILE NAME = p:\w\IL\084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\P144709\Drawings\Design\P144709-sht-details.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PIPE AND STRUCTURES TABLE IL RTE 31 AT SILVER GLEN RD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					3887	H-N	KANE	86	33
	PLOT DATE = 7/14/2015	DATE -	REVISED -		SCALE:			CONTRACT NO. 60R28				
					SHEET NO. 1 OF 1 SHEETS			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PART OF THE NE 1/4 OF SECTION 9 AND PART OF THE NW 1/4 OF SECTION 10, TWP. 40 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

SEE SHEET 6 FOR P.O.C.
1JC0001PE
1JC0002
1JC0003
1JC0003TE
1JC0008
1JC0009

SW COR.
GERALD E. COX TRACT
REC. 7/25/1960
DOC. NO. 925240



PIN: 09-09-226-002

EXIST. CURVE EX-31-1
PI STA. = 38+46.35
 $\Delta = 30^\circ 47' 44''$ (RT)
D = 8° 59' 51''
R = 636.80'
T = 175.38'
L = 342.27'
E = 23.71'
P.C. STA. = 36+70.98
P.T. STA. = 40+13.24

PROPOSED
PERMANENT
EASEMENT

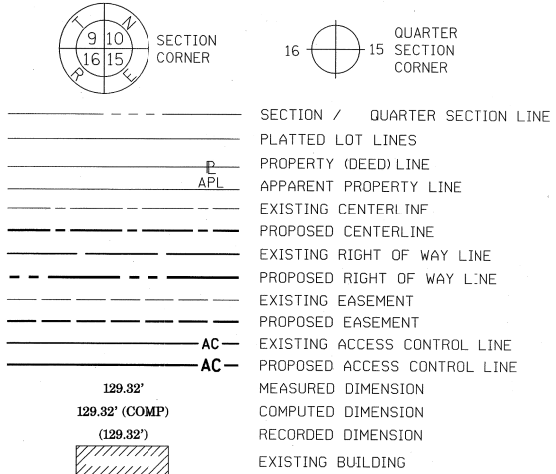
FOUND CONC. POST
PER MONUMENT RECORD
2003K125738
N 1931991.084
E 987592.136

BLACKHAWK FOREST PRESERVE

FOX RIVER

NE COR.
JOANN N. COLLINS TRACT
DOC. NO. 1179489

LEGEND



- IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- *MAG. NAIL SET
- 5 / 8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
COUNTY OF KANE)

THIS IS TO CERTIFY THAT I, REJENA LYON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 9 & 10, TOWNSHIP 40 NORTH, RANGE 8 EAST OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT Elgin, ILLINOIS THIS 16 DAY OF JULY 20 14 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3302
LICENSE EXPIRATION DATE: NOV. 30, 2014

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY, FOR THAT PART SHOWN AS PROPOSED RIGHT-OF-WAY

HAMPTON, LENZINI AND RENWICK, INC.
CIVIL ENGINEERS • STRUCTURAL ENGINEERS • LAND SURVEYORS
380 SHEPARD DRIVE
ELGIN, ILLINOIS 60123
847.697.6700 www.hlrengineering.com

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31

LIMITS: AT SILVER GLEN ROAD COUNTY: KANE
SECTION: JOB NO.: R-91-001-11
STA. 35+00 TO STA. 41+75
SCALE: 1" = 50' SHEET 2 OF 6 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
82+60.39	0.00	1931294.967	986859.652
84+40.20	0.00	1931292.595	987039.445
85+00.00	0.00	1931284.113	987098.472
35+29.08	50.00' RT	1931327.566	987136.645
36+70.98	0.00	1931446.231	987044.156
36+70.98	50.00' RT	1931462.102	987091.570
40+13.24	0.00	1931783.892	987025.719
40+13.24	50.00' RT	1931773.264	987074.576
40.59.99	50.00' RT	1931818.941	987084.513
41+75.00	0.00	1931941.951	987060.102

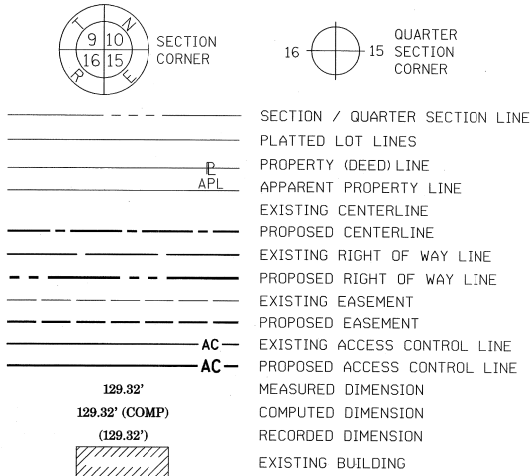
PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	SQUARE FEET	PARCEL INDEX NUMBER
1JC0001PE	8.172*	NA	NA	NA	0.198 Ac±	NA	09-09-226-004 (PT)

* AREA SHOWN IS AS DESCRIBED IN WHEATLAND TITLE COMMITMENT 2010KN-1191.0. ACTUAL TOTAL HOLDING IS MUCH LARGER.
ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED.
BEARINGS AND DISTANCES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (2011 ADJUSTMENT) GRID.
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AREAS SHOWN ON THIS PLAT ARE GROUND.
COORDINATES ARE ILLINOIS STATE PLANE COORDINATES, EAST ZONE NAD 83 (2011 ADJUSTMENT).

REVISION DATE: 07/14/2014 REVISION MADE BY: RHL

PART OF THE NE 1/4 OF SECTION 9 AND PART OF THE NW 1/4 OF SECTION 10, TWP. 40 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

LEGEND



- IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- *MAG. NAIL SET
- 5 / 8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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- T3
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- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
COUNTY OF KANE)

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DATED AT Elgin, ILLINOIS THIS 16 DAY OF JULY 2014 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3302
LICENSE EXPIRATION DATE: NOV. 30, 2014

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380 SHEPARD DRIVE
ELGIN, ILLINOIS 60123
847.697.6700 www.hlrengineering.com

184.000959
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORPORATION
JOB: 13.0245.230

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31

LIMITS: AT SILVER GLEN ROAD COUNTY: KANE
SECTION: JOB NO.: R-91-001-11
STA. 27+00 TO STA. 35+00
SCALE: 1" = 50' SHEET 3 OF 4 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

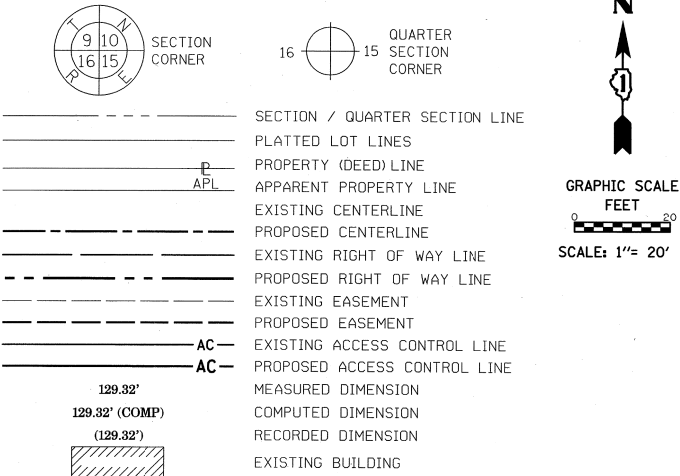
IDOT USE ONLY

RECEIVED
JUL 21 2014
PLATS & LEGALS

REVISION DATE: / / REVISION MADE BY:

PART OF THE NE 1/4 OF SECTION 9 AND PART OF THE NW 1/4 OF SECTION 10, TWP. 40 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

LEGEND



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PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31

LIMITS: AT SILVER GLEN ROAD COUNTY: KANE
SECTION: JOB NO.: R-91-001-11
STA. 27+00 TO STA. 35+00
SCALE: 1" = 20' SHEET 4 OF 6 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

IDOT USE ONLY

RECEIVED
JUL 21 2014
PLATS & LEGALS

REVISION DATE: / / REVISION MADE BY:

PART OF THE NE 1/4 OF SECTION 9 AND PART OF THE NW 1/4 OF SECTION 10, TWP. 40 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

LEGEND



SECTION CORNER



QUARTER SECTION CORNER



GRAPHIC SCALE

FEET

SCALE: 1"= 40'

- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING

BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83 (XXXX ADJUSTMENT), EAST ZONE.

- IRON PIPE OR ROD FOUND
- CUT CROSS FOUND OR SET
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PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31

LIMITS: AT SILVER GLEN ROAD COUNTY: KANE
SECTION: JOB NO.: R-91-001-11
STA. 30+00 TO STA. 36+00
SCALE: 1" = 40' SHEET 5 OF 6 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

IDOT USE ONLY

RECEIVED

JUL 21 2014

PLATS & LEGALS

REVISION DATE: / / REVISION MADE BY:

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
82+60.39	0.00	1931294.967	986859.652
84+40.20	0.00	1931292.595	987039.445
85+00.00	0.00	1931284.113	987098.472
29+61.94	60.00' LT	1930754.868	987212.515
32+44.84	60.00' LT	1931023.112	987122.641
34+53.00	60.00' LT	1931220.483	987056.513
34+98.32	78.59' LT	1931258.696	987022.451

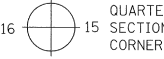
PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		PARCEL INDEX NUMBER
					ACRES	SQUARE FEET	
1JC0008	2.500 Ac±	0.853 Ac±	0.698 Ac±	1.647 Ac±	N/A	N/A	09-09-227-003
1JC0009	5.008 Ac±	0.390 Ac±	0.214 Ac±	4.618 Ac±	N/A	N/A	09-09-227-004

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PART OF THE NE 1/4 OF SECTION 9 AND PART OF THE NW 1/4 OF SECTION 10, TWP. 40 N., R. 8 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

TOTAL HOLDING DRAWING

LEGEND



GRAPHIC SCALE
FEET
0 40
SCALE: 1"= 40'

- SECTION / QUARTER SECTION LINE
PLATTED LOT LINES
PROPERTY (DEED) LINE
APL
APPARENT PROPERTY LINE
EXISTING CENTERLINE
PROPOSED CENTERLINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
EXISTING EASEMENT
PROPOSED EASEMENT
AC
EXISTING ACCESS CONTROL LINE
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JOB: 13.0245.230

IDOT USE ONLY

RECEIVED
JUL 21 2014
PLATS & LEGALS

PLAT OF HIGHWAYS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31

LIMITS: AT SILVER GLEN ROAD COUNTY: KANE
SECTION: 16 TO STA. 45+00 JOB NO.: R-91-001-11
STA. 29+00 TO STA. 45+00
SCALE: 1"= 100' SHEET 6 OF 6 SHEETS

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

STATE OF ILLINOIS)
COUNTY OF KANE)

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DATED AT Elgin, ILLINOIS THIS 16 DAY OF JULY 2014 A.D.

Rejena H. Lyon
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3302
LICENSE EXPIRATION DATE: NOV. 30, 2014

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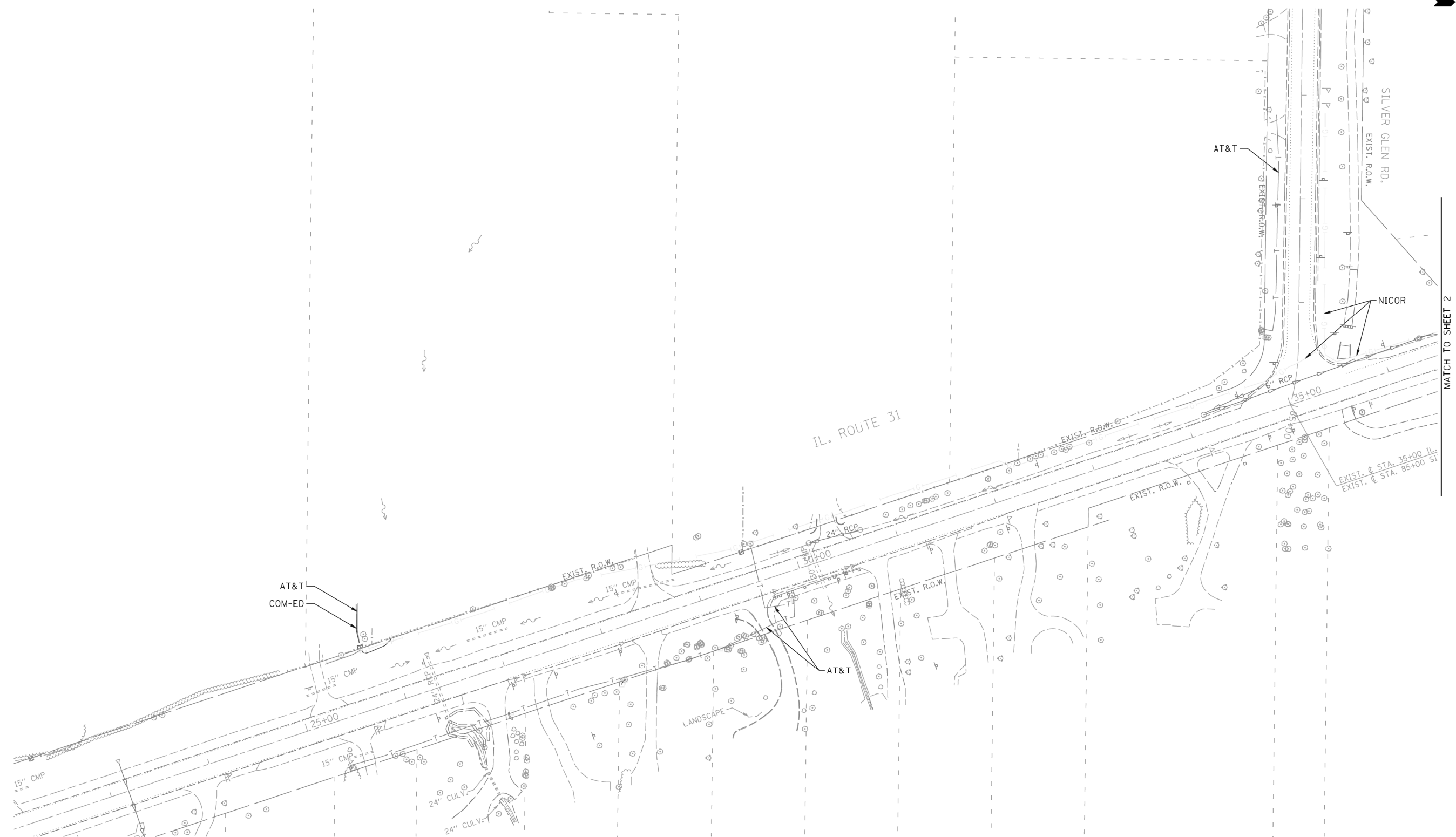


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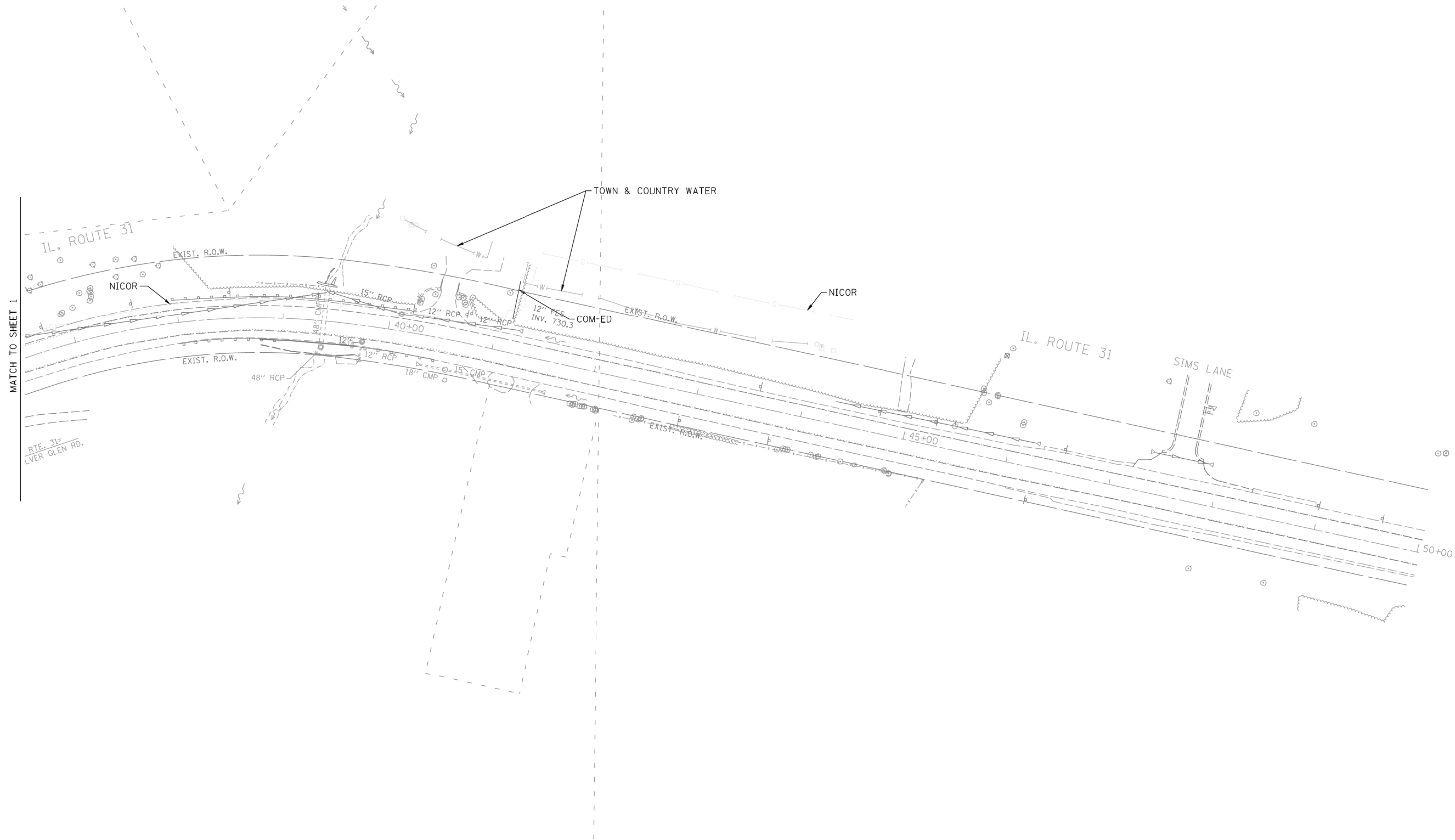
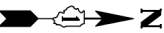
REVISION DATE: 07/14/2014 REVISION MADE BY: RHL



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<div><div><div><div><div></div><div>A</div></div><div><div>A</div><div></div></div></div><div><div><div></div><div>---</div></div><div><div>---</div><div></div></div></div><div><div><div></div><div>CTV</div></div><div><div>CTV</div><div></div></div></div><div><div><div></div><div>T</div></div><div><div>T</div><div></div></div></div><div><div><div></div><div>G</div></div><div><div>G</div><div></div></div></div><div><div><div></div><div>E</div></div><div><div>E</div><div></div></div></div><div><div><div></div><div>W</div></div><div><div>W</div><div></div></div></div><div><div><div></div><div>FO</div></div><div><div>FO</div><div></div></div></div><div><div><div></div><div></div></div><div><div></div><div></div></div></div></div><div><div>AERIAL</div><div>UNKNOWN</div><div>CABLE TV</div><div>TELEPHONE</div><div>GAS</div><div>ELECTRIC</div><div>WATER</div><div>FIBER OPTIC</div><div>TBE TEST HOLE</div></div></div>	<div>UTILITY OWNERS</div> <div>AT&T = TELEPHONE COM-ED = ELECTRIC NICOR = GAS TOWN & COUNTRY WATER = WATER</div> <div>Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's SUE field investigation was performed 10/14/13 through 10/25/13. Changes to utilities after 10/25/13 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.</div>	<div>ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.</div> <div><div><div>20</div><div>0</div><div>20</div><div>40</div></div><div><div>50</div><div>0</div><div>50</div><div>100</div></div><div>METERS</div><div>FEET</div></div>	<div><div><div><div></div><div></div></div><div><div></div><div></div></div></div><div>Cardno TBE</div></div> <div><div><div><div></div><div></div></div><div><div></div><div></div></div></div><div>Dynasty Group Engineers & Surveyors</div></div> <div><div>TBE Job No. IL09510544 SUE Plan Page: 1 of 2</div></div>
<div>Utility Quality Level "A" : Visually Verified Test Hole Utility Quality Level "B" : Designating/non Visually Verified Test Hole Utility Quality Level "C" : Research with Survey Utility Quality Level "D" : Records Research</div>	<div><div>DESIGNEDMS DRAWNSRK CHECKEDKFS DATE11/04/13</div><div>REVISED REVISED REVISED REVISED</div></div>	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div><div>IL RT. 31 at Silver Glen Rd. St. Charles, IL</div><div><div>F.A. RTE.</div><div>SECTION</div><div>COUNTY</div><div>TOTAL SHEETS</div><div>SHEET NO.</div><div>Contract No. 60R28</div><div>FED. ROAD DIST. NO.</div><div>ILLINOIS</div><div>DOT Project No.</div></div></div>

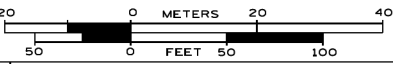


- | | | |
|---------|---------|---------------|
| — A — | A | AERIAL |
| - - - | UNKNOWN | UNKNOWN |
| — CTV — | CTV | CABLE TV |
| — T — | T | TELEPHONE |
| — G — | G | GAS |
| — E — | E | ELECTRIC |
| — W — | W | WATER |
| — FO — | FO | FIBER OPTIC |
| + | | TBE TEST HOLE |

UTILITY OWNERS		
AT&T = TELEPHONE		
COM-ED = ELECTRIC		
NICOR = GAS		
TOWN & COUNTRY WATER = WATER		

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's SUE field investigation was performed 10/14/13 through 10/25/13. Changes to utilities after 10/25/13 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B"
UNLESS NOTED OTHERWISE.



TBE Job No. IL09510544
SUE Plan Page: 2 of 2

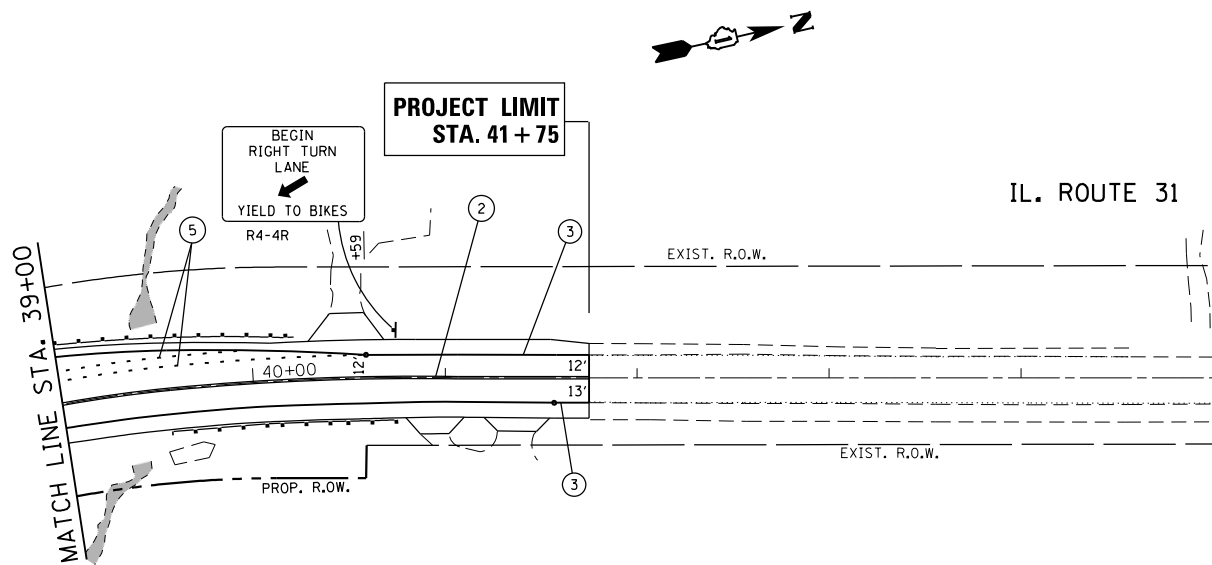
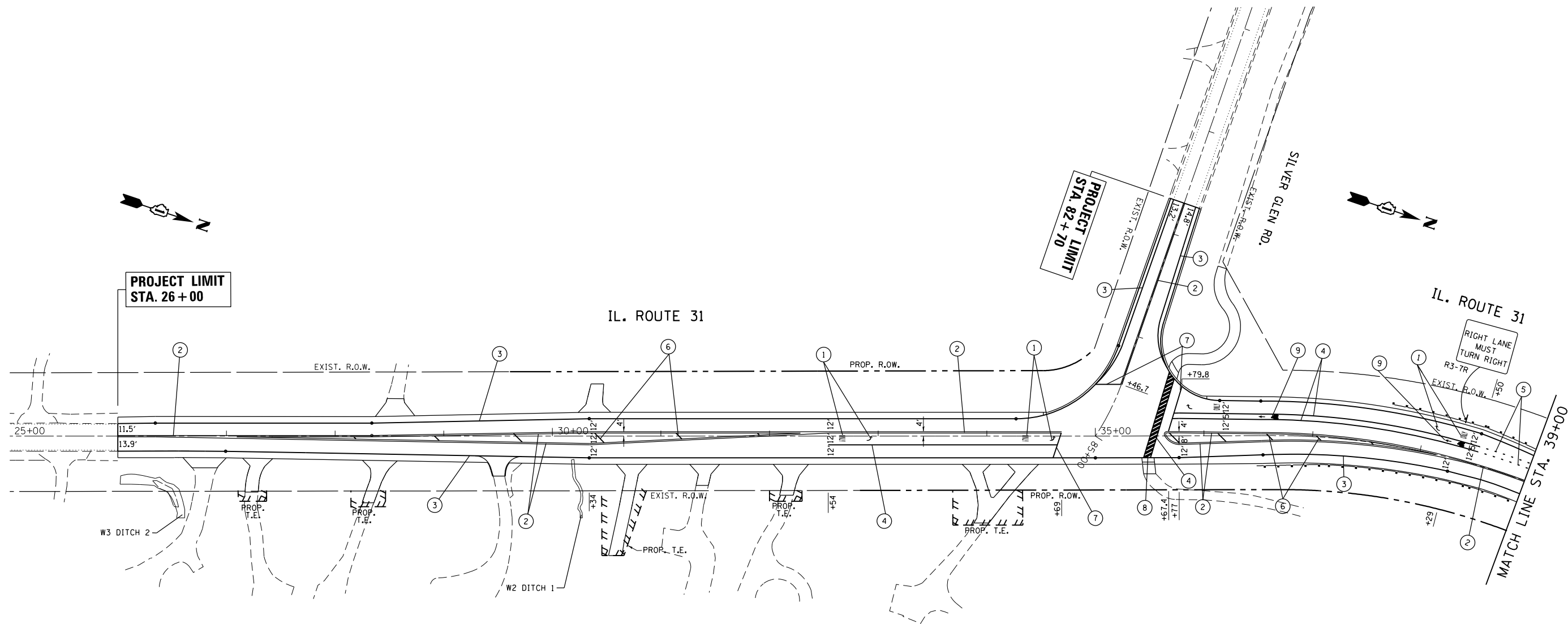
Utility Quality Level "A" : Visually Verified Test Hole
Utility Quality Level "B" : Designating/non Visually Verified Test Hole
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

DESIGNED	MS	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	11/04/13	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

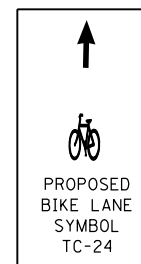
IL RT. 31 at Silver Glen Rd.
St. Charles, IL

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	H-N	Kane	86	41
Contract No. 60R28				
FED. ROAD DIST. NO. - ILLINOIS IDOT Project No.				



NOTES:

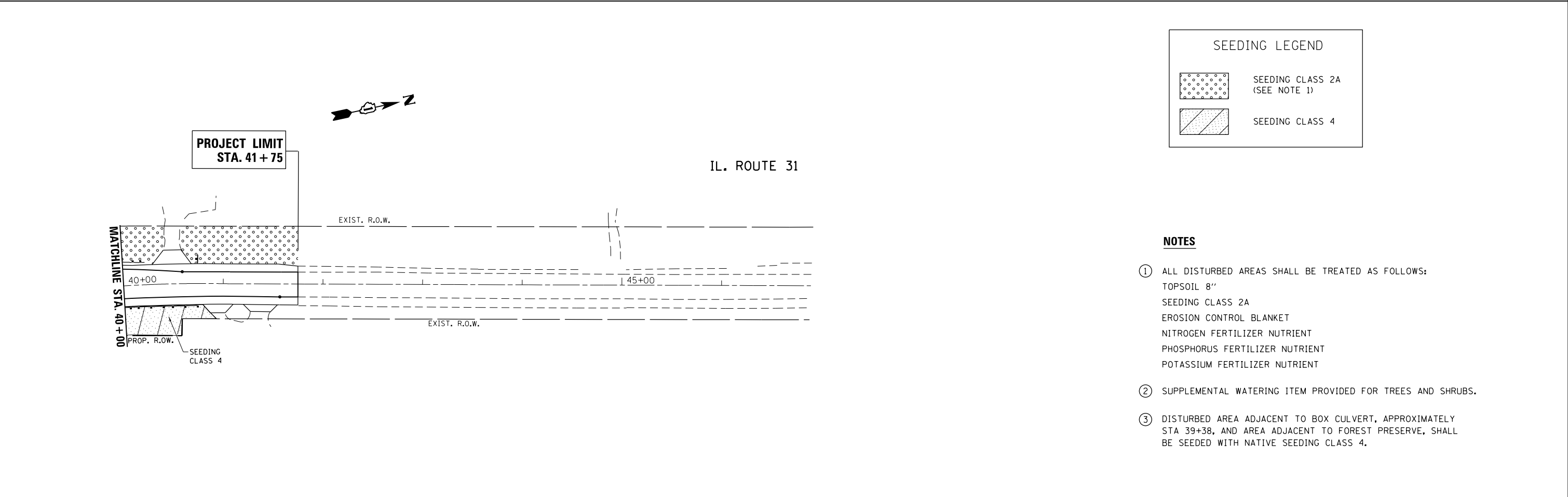
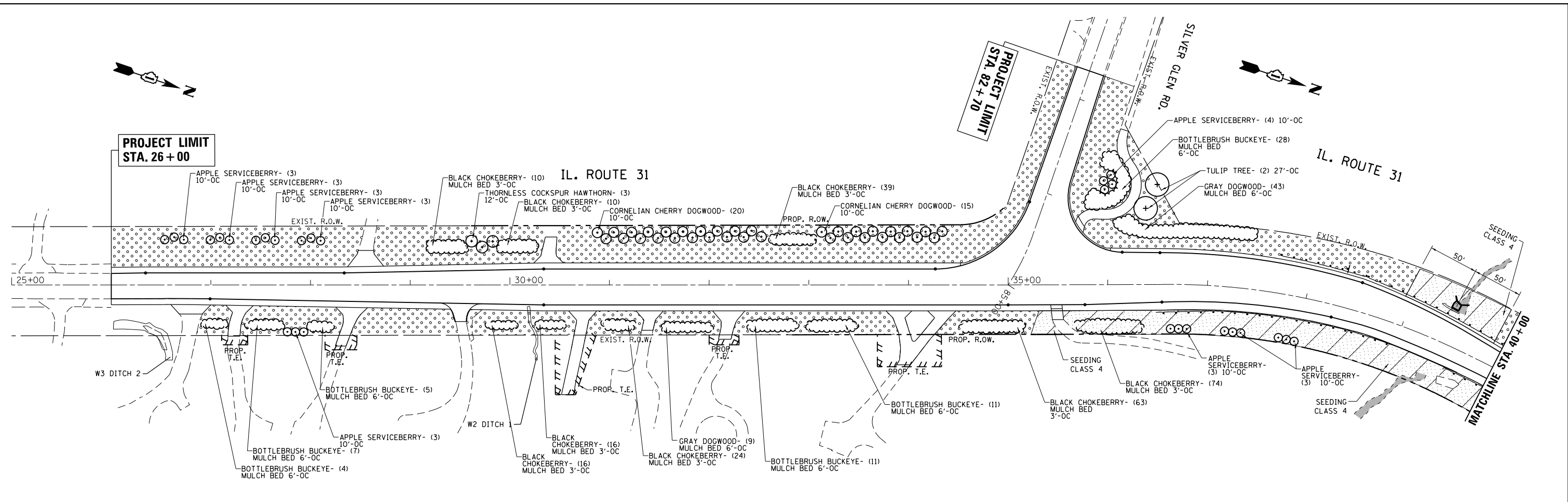
1. ALL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13) DETAIL.
2. ALL FINAL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE RAISED REFLECTIVE MARKERS (TC-11) DETAIL.



PAVEMENT MARKING LEGEND

- 1 PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, (TYP.)
- 3 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4" WHITE EDGELINE (TYP.)
- 4 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" TURN LANE LINE, SOLID WHITE (TYP.) OR CROSSWALK LINE
- 5 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 6" DOTTED LINE, WHITE (6' SKIP 2' DASH) (TYP.)
- 6 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" PAINTED MEDIAN LINE DIAGONAL, YELLOW SOLID @ 45° (TYP.)
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 24" STOP LINE, WHITE (TYP.)
- 8 PROPOSED THERMOPLASTIC PAVEMENT MARKING, 12" CROSSWALK WHITE DIAGONALS
- 9 PROPOSED THERMOPLASTIC PAVEMENT MARKING BIKE LANE SYMBOLS, WHITE (TYP.)

FILE NAME = p:\11\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI44709\DATA\GAD\Sheets\PI44709-sht-pmk.dgn Default	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN IL. ROUTE 31 AT SILVER GLEN RD.					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
											3887	H-N	KANE	86
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28									
	PLOT DATE = 7/15/2015	DATE -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT		



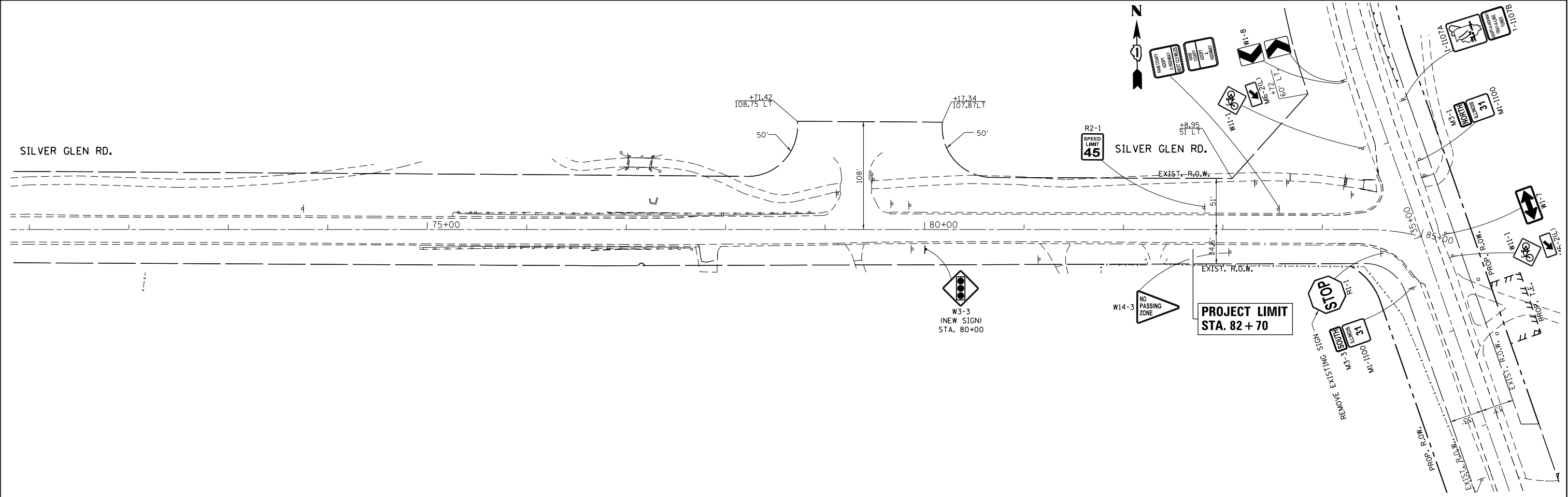
SEEDING LEGEND

SEEDING CLASS 2A
(SEE NOTE 1)

SEEDING CLASS 4

NOTES

- ① ALL DISTURBED AREAS SHALL BE TREATED AS FOLLOWS:
TOPSOIL 8"
SEEDING CLASS 2A
EROSION CONTROL BLANKET
NITROGEN FERTILIZER NUTRIENT
PHOSPHORUS FERTILIZER NUTRIENT
POTASSIUM FERTILIZER NUTRIENT
- ② SUPPLEMENTAL WATERING ITEM PROVIDED FOR TREES AND SHRUBS.
- ③ DISTURBED AREA ADJACENT TO BOX CULVERT, APPROXIMATELY STA 39+38, AND AREA ADJACENT TO FOREST PRESERVE, SHALL BE SEEDED WITH NATIVE SEEDING CLASS 4.



FILE NAME = p:\w\11084EBID\INTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI44709\DATA\GAD\Sheets\PI44709-sht-sign.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED SIGNAGE IL. ROUTE 31 AT SILVER GLEN ROAD			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		PLANNED	REVISED -					3887	H-N	KANE	86	45
		PLOT SCALE = 100.0000' / in.	CHECKED -		SCALE: 1"=50'			CONTRACT NO. 60R28				
		PLOT DATE = 7/15/2015	DATE -		SHEET 4 OF 5 SHEETS STA. 70+00 TO STA. 85+00			ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

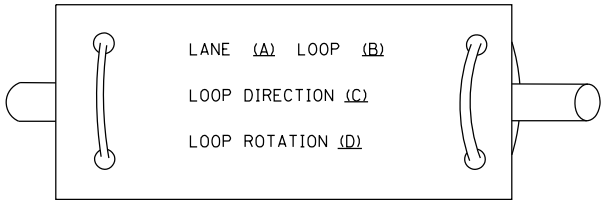
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																								
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																											
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																											
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																											
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																											
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																											
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																											
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																											
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																											
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																											
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																											
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S		STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																											
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I		ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																											
SIGNAL POST				REMOVE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																											
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																											
GUY WIRE				ABANDON ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																											
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR																											
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				QUEUE DETECTOR																											
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED QUEUE DETECTOR																											
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																											
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED SAMPLING (SYSTEM) DETECTOR																											
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				<div>RAILROAD SYMBOLS</div> <table><thead><tr><th>EXISTING</th><th>PROPOSED</th></tr></thead><tbody><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr><tr><td></td><td></td></tr></tbody></table>				EXISTING	PROPOSED																						
EXISTING	PROPOSED																																		
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED																															
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID																															
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																															
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT																															
DETECTOR LOOP, TYPE I				RADIO REPEATER																															
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																															
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																															
VIDEO DETECTION CAMERA																																			
VIDEO DETECTION ZONE																																			
PAN, TILT, ZOOM CAMERA																																			
WIRELESS DETECTOR SENSOR																																			
WIRELESS ACCESS POINT																																			

FILE NAME =	USER NAME = Bilgramise	DESIGNED - DAG/BCK	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PROJECT =	PROJECT =	CHECKED - DAD	REVISED -			3887	H-N	KANE	86	46	
PLOT SCALE = 100.0000' / 1in.	PLOT DATE = 7/14/2015	DATE - 10-28-09	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
CONTRACT NO. 60R28											

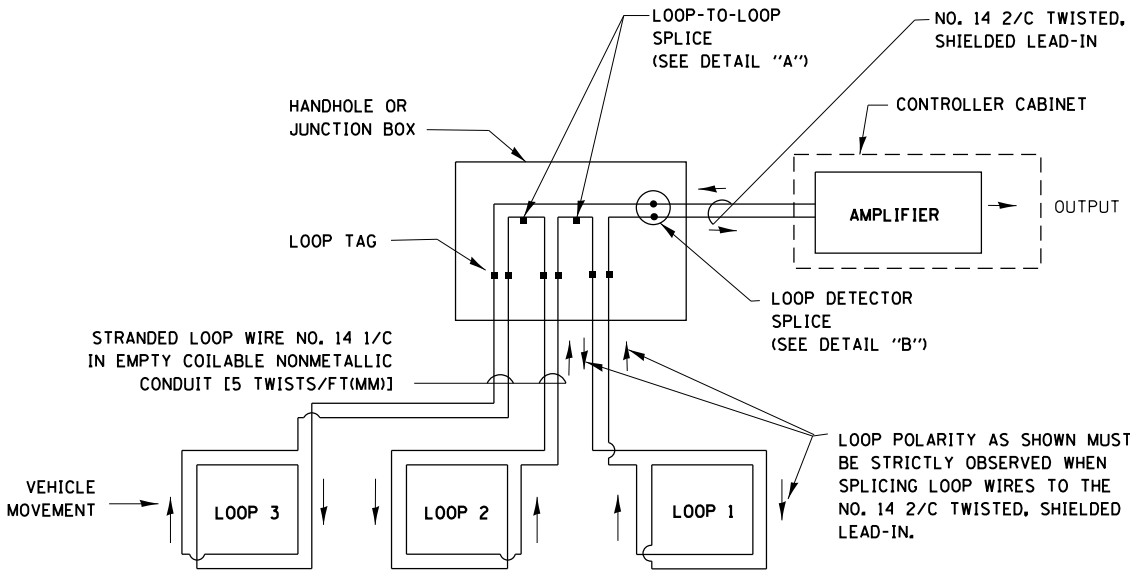
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

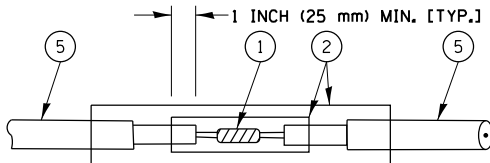


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

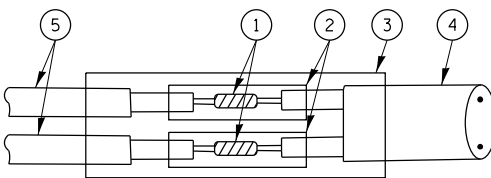


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

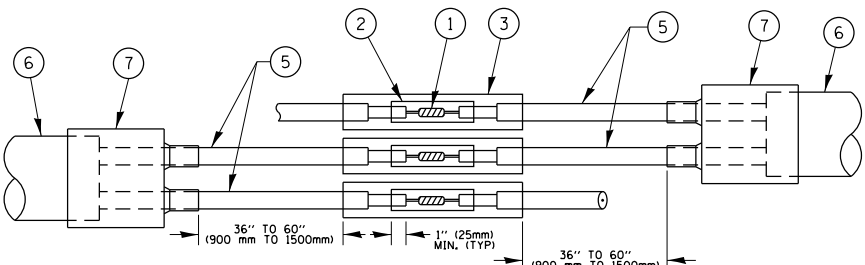


DETAIL "A"
LOOP-TO-LOOP SPLICE

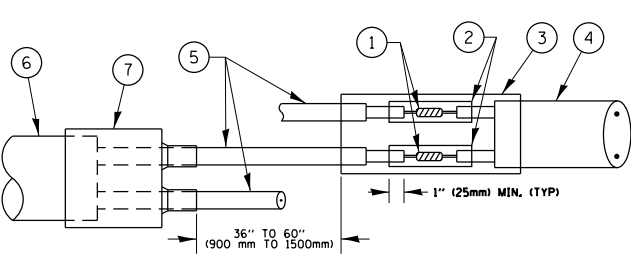


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

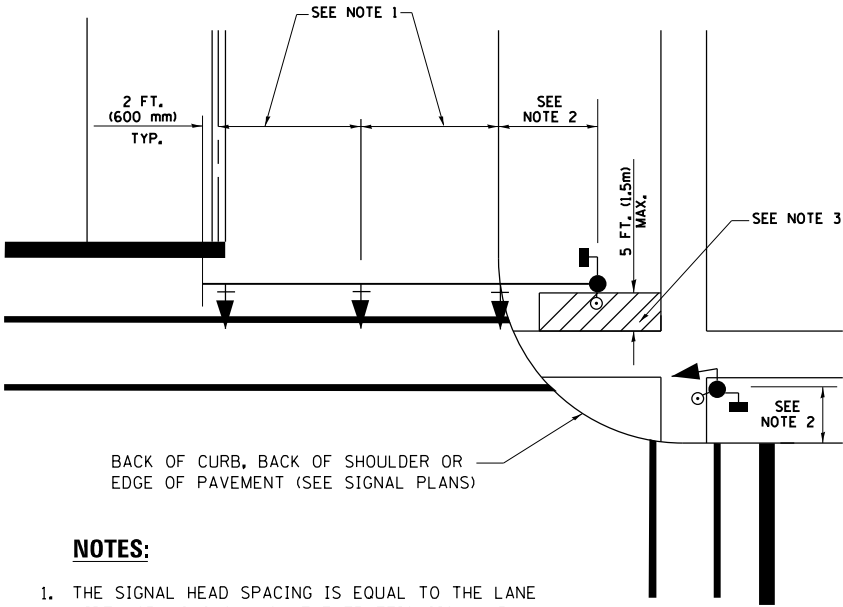
PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = Bilgramis	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\1084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\PI447\Drawings\Design\B06\Std.dgn		CHECKED - DAD	REVISED -		STANDARD TRAFFIC SIGNAL DESIGN DETAILS			3887	H-N	KANE	86	47
	PLOT SCALE = 100.0000' / in.	DATE - 10-28-09	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7 SHEETS	STA.	TO STA.	TS-05	CONTRACT NO. 60R28		
	PLOT DATE = 7/14/2015								FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

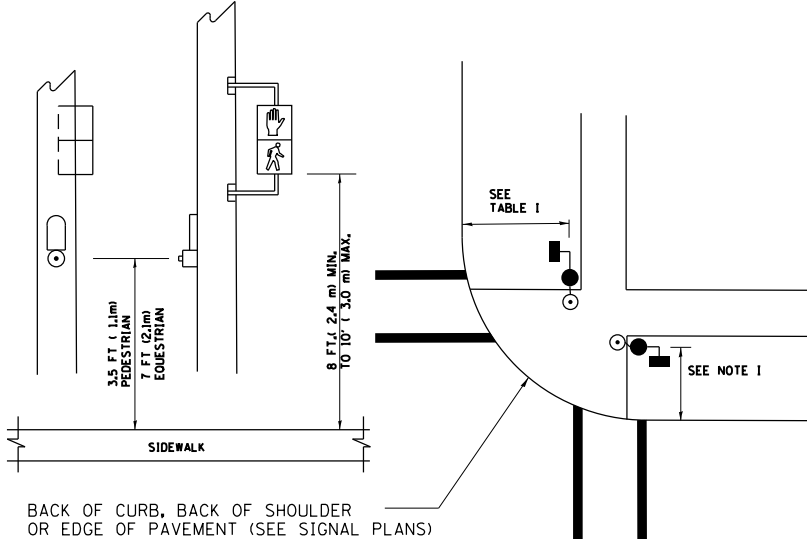
TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

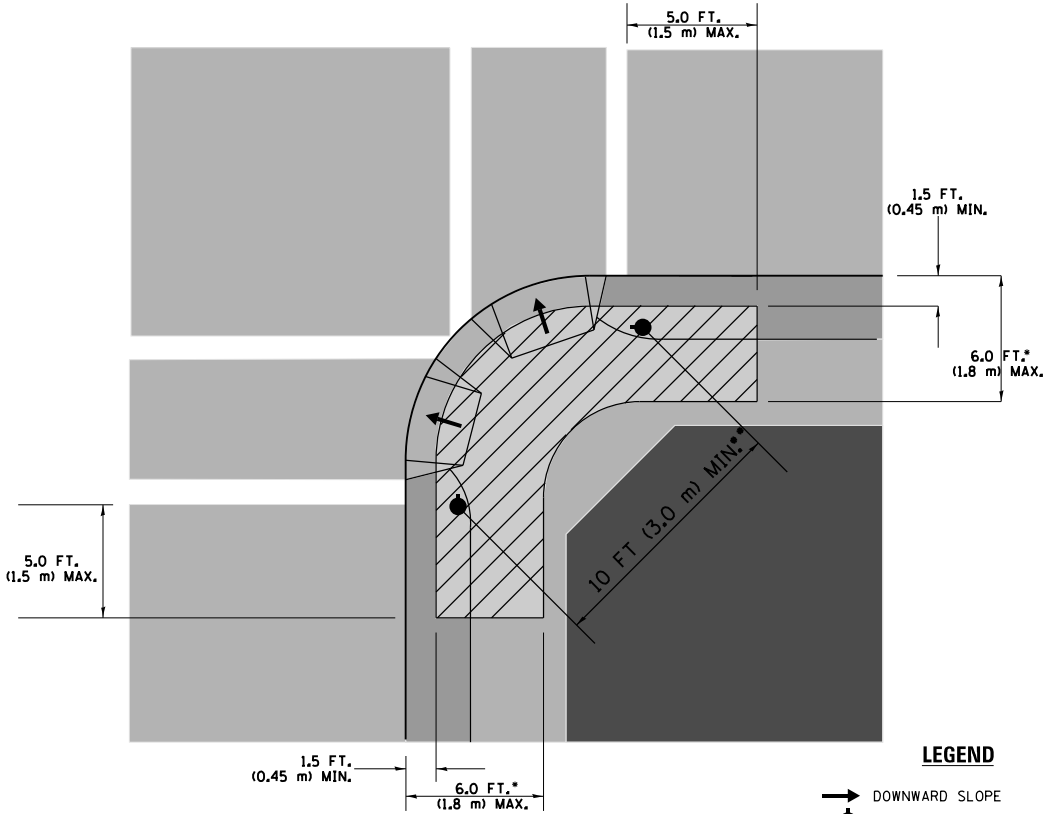
PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

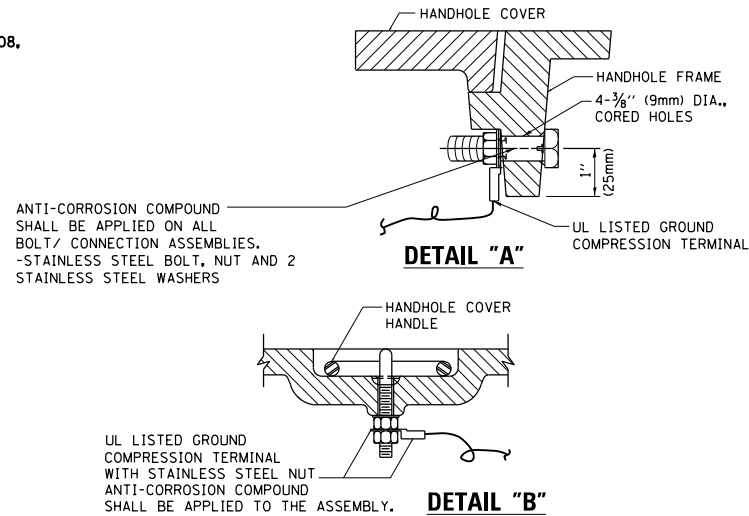
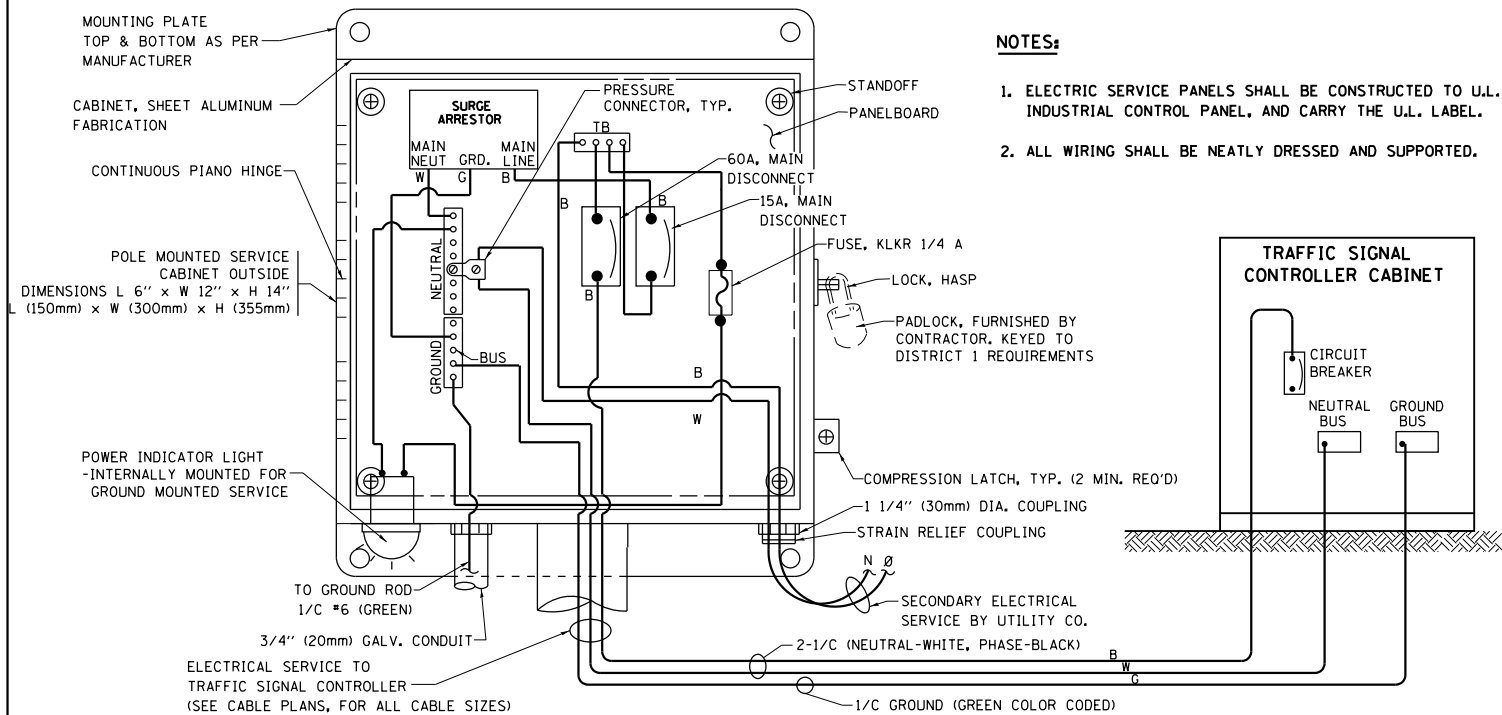
TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

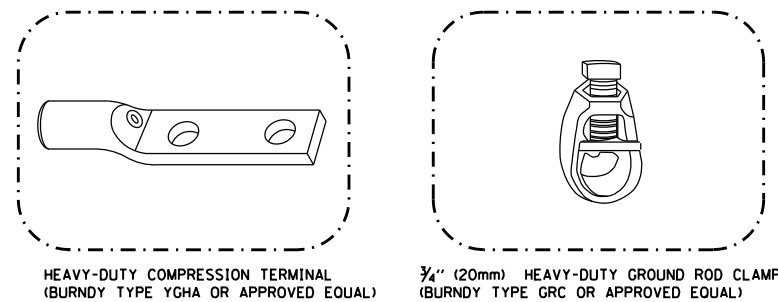
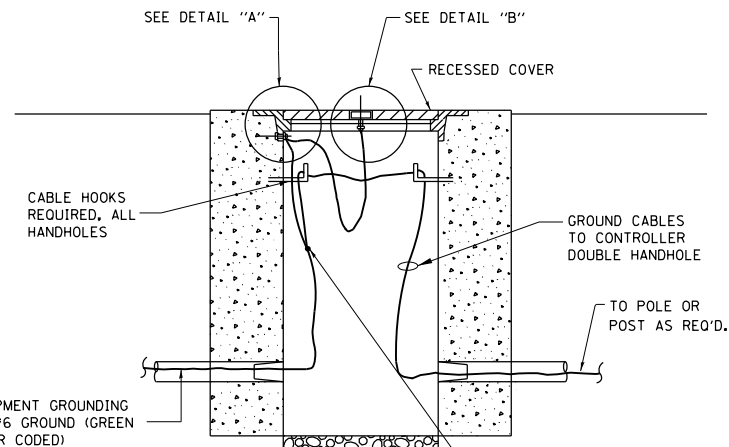
FILE NAME =	USER NAME = Bilgramiso	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\w\1L084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\PI447\Drawings\Design\B06\Std.dgn		CHECKED - DAD	REVISED -					3887	H-N	KANE	86	48
	PLOT SCALE = 100.0000 ' / in.	DATE - 10-28-09	REVISED -					TS-05		CONTRACT NO. 60R28		
	PLOT DATE = 7/14/2015				SCALE: NONE	SHEET NO. 3	OF 7 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



NOTES:

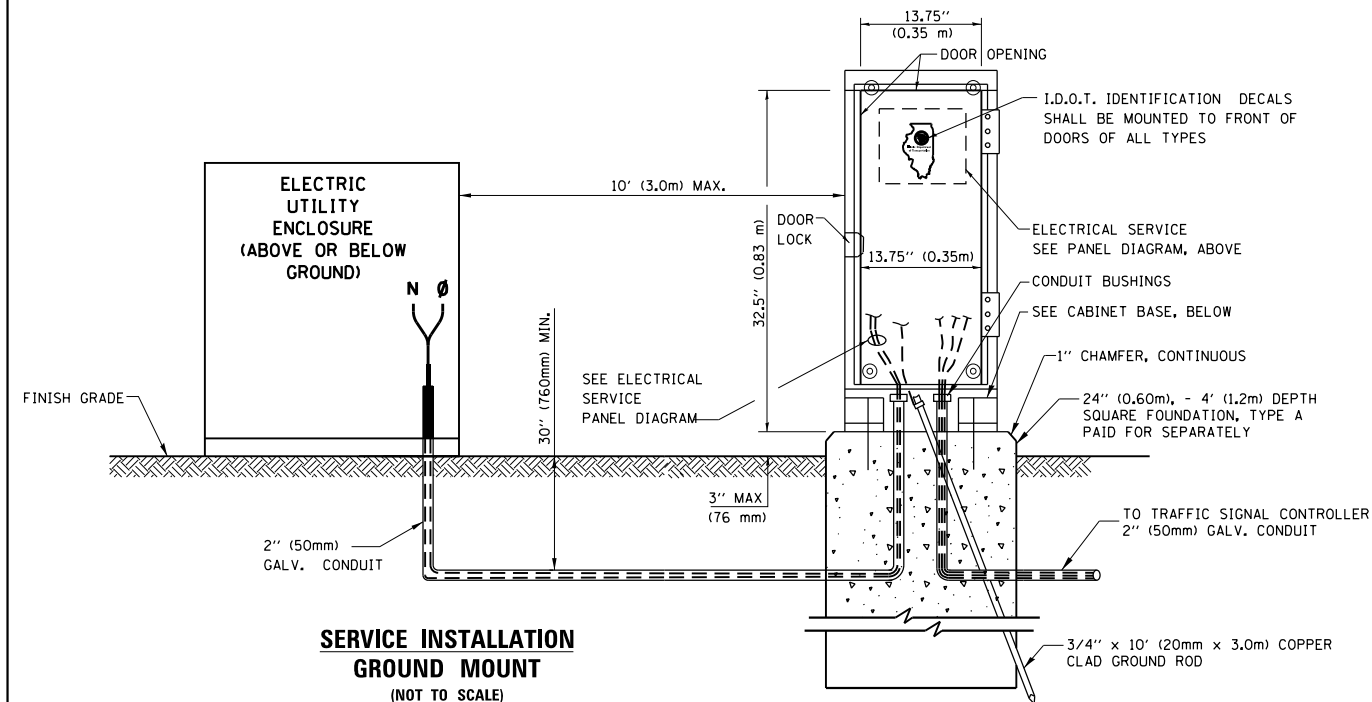
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



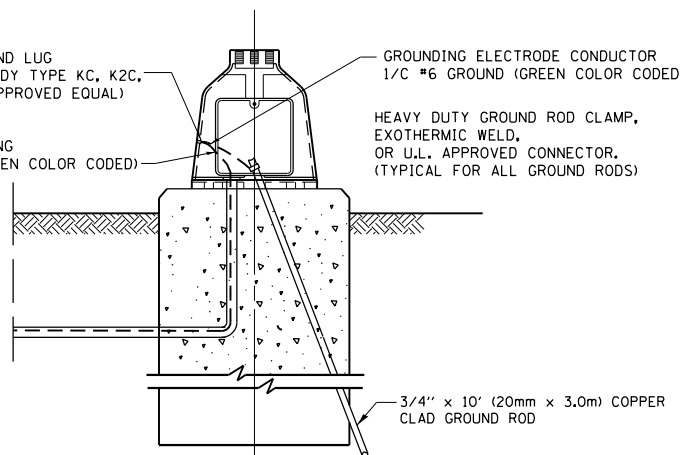
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



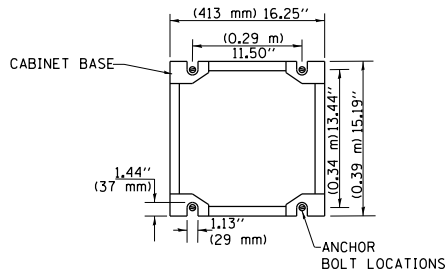
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL

(NOT TO SCALE)

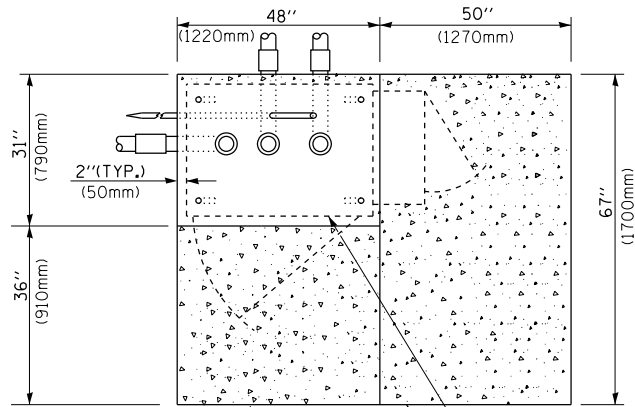


CABINET - BASE BOLT PATTERN

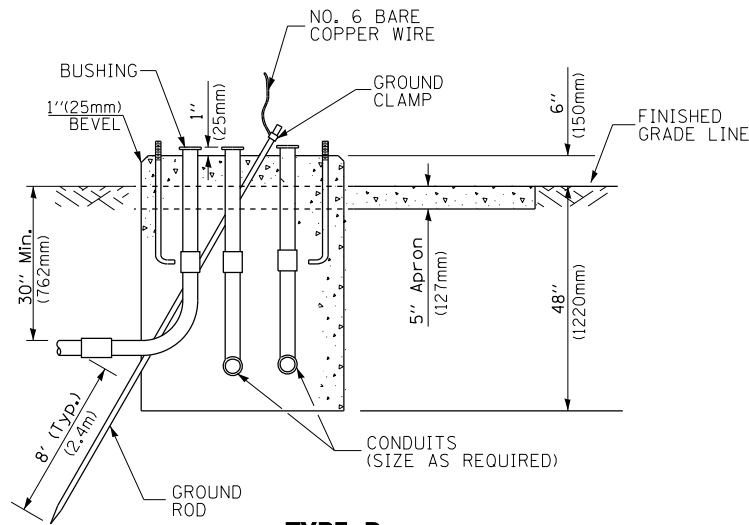
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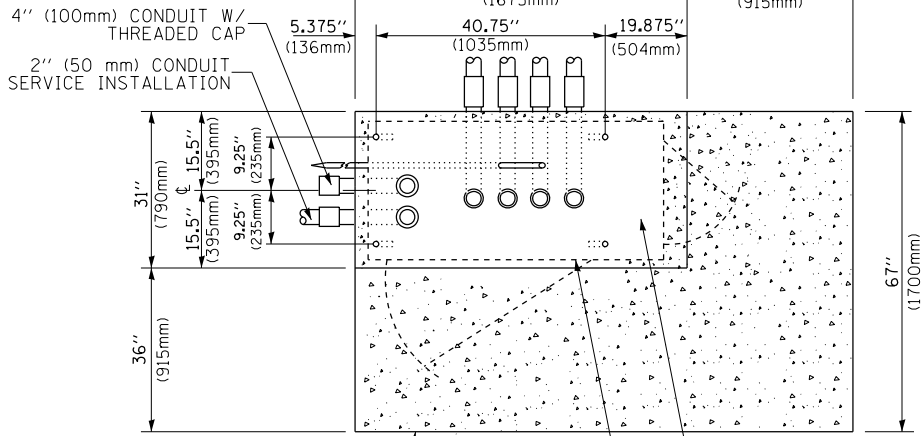
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			REVISED -					3887	H-N	KANE	86	49
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					SCALE: NONE	SHEET NO. 4	OF 7 SHEETS	STA.	TO STA.			



TOP VIEW



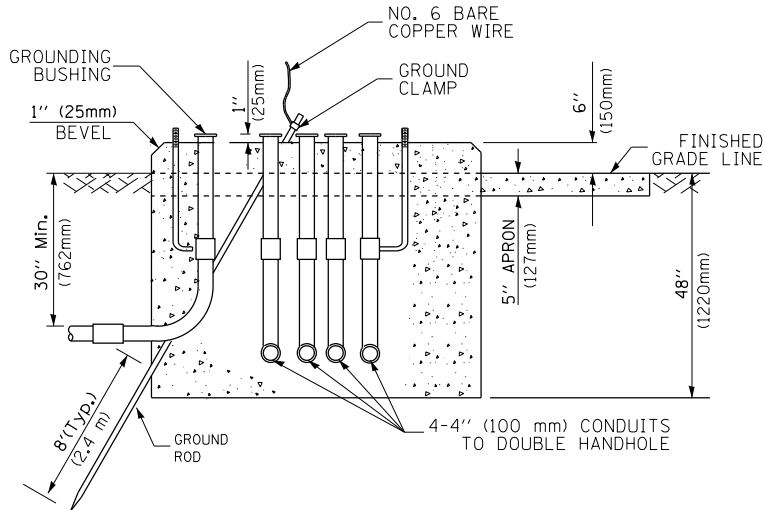
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



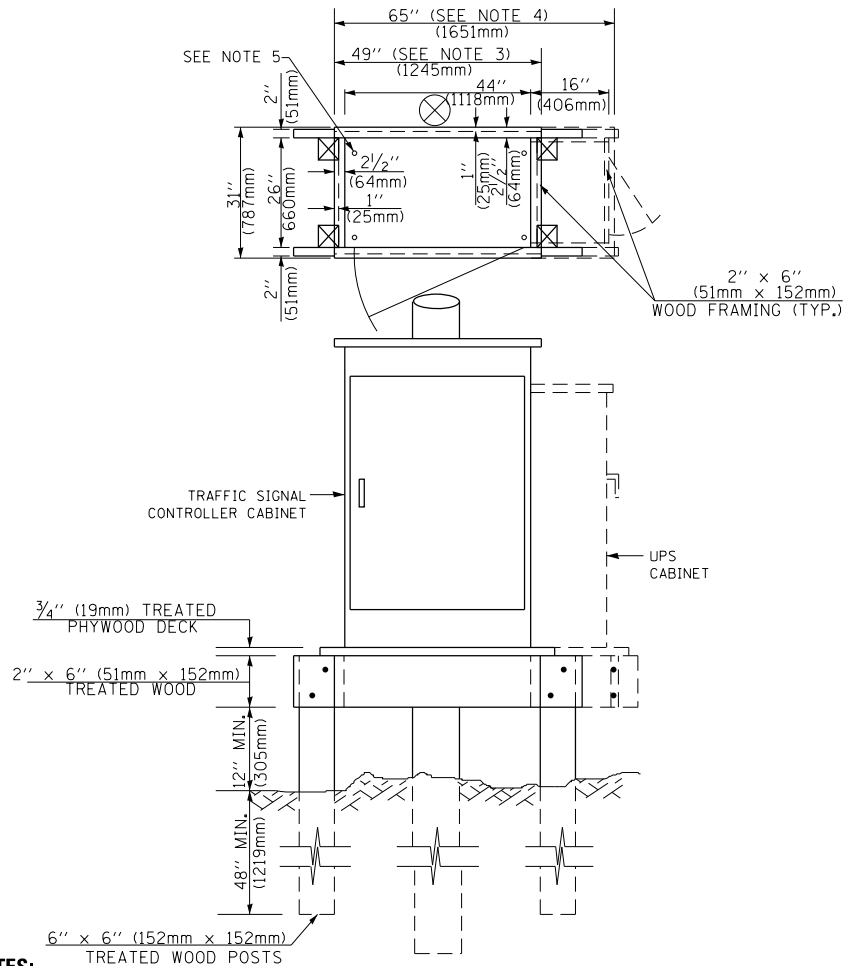
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL
BE HIGHER THAN TOP OF
DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

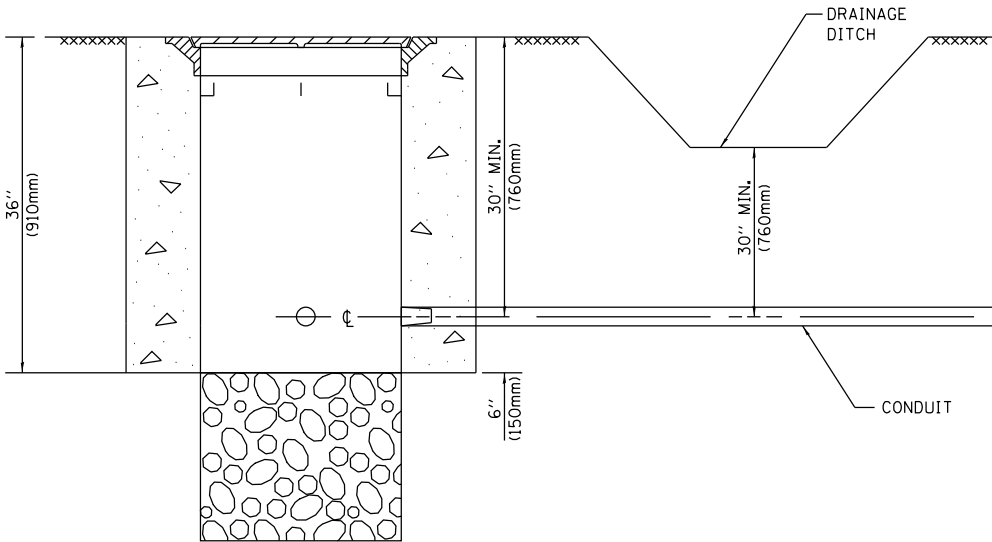
Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

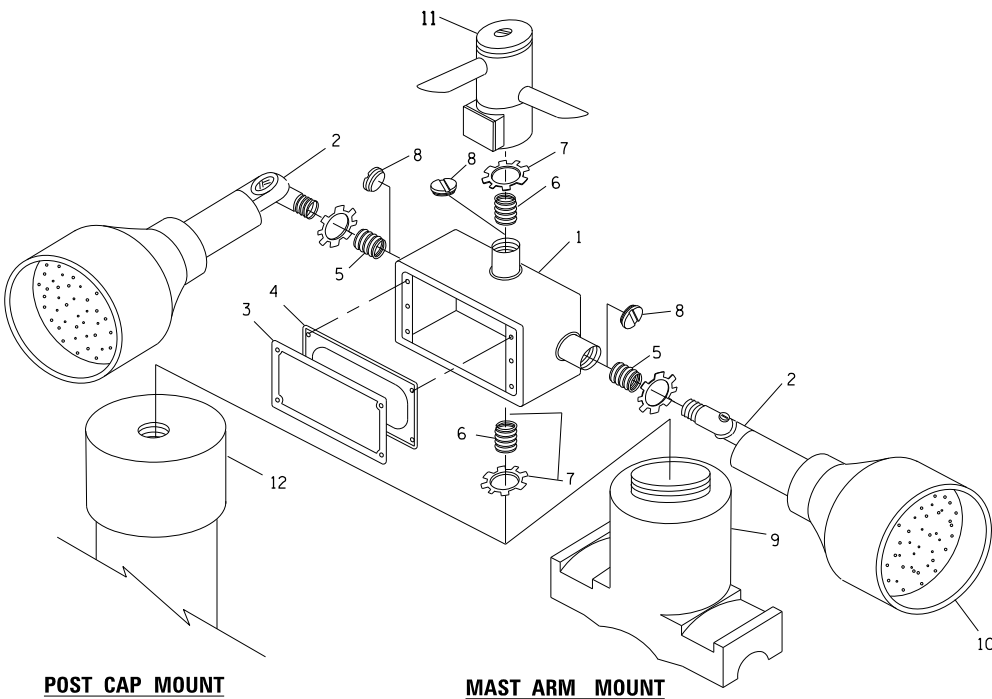
FILE NAME =	USER NAME = Bilgrami	DESIGNED - DAG	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE. 3887	SECTION H-N	COUNTY KANE	TOTAL SHEETS 86	SHEET NO. 50				
pw:\IL\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\PI447\084EBID\Design\B06\Std.dgn	CHECKED - DAD	REVISED -	SCALE: NONE								SHEET NO. 5 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS	FED. AID PROJECT
PLOT SCALE = 100.0000' / in.	DATE - 10-28-09	REVISED -												
PLOT DATE = 7/14/2015														



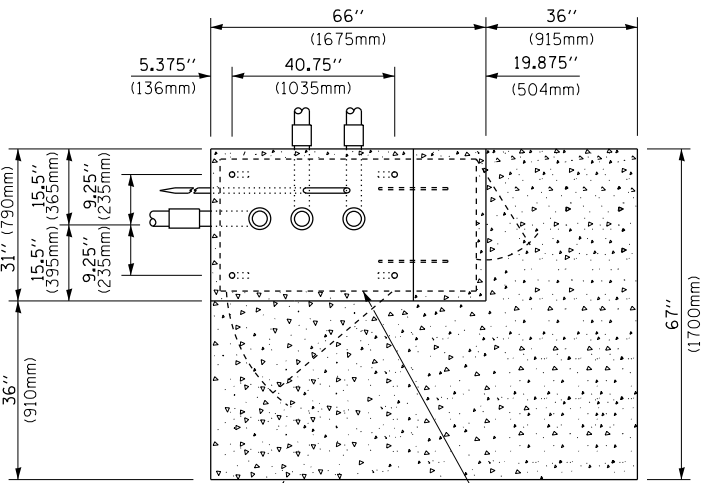
NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

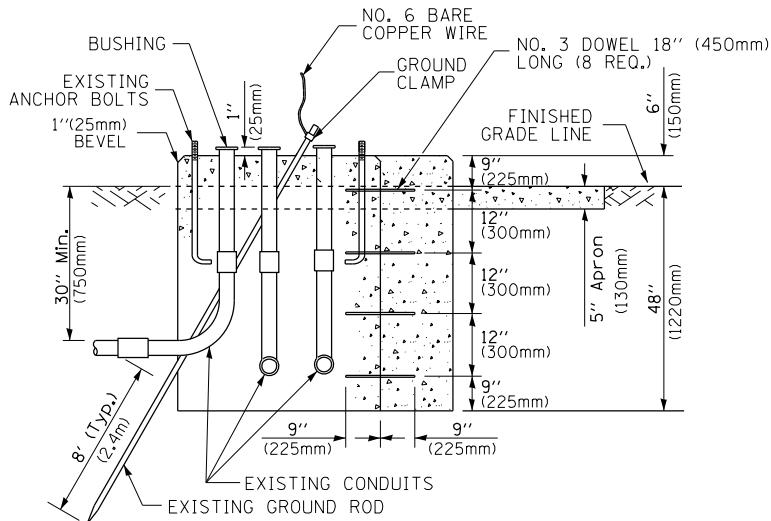
HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



TOP VIEW
(NOT TO SCALE)

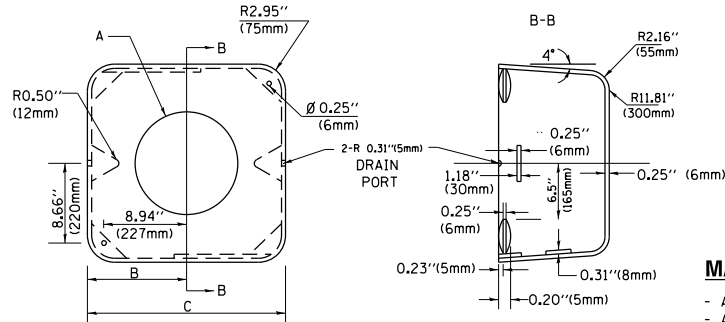


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

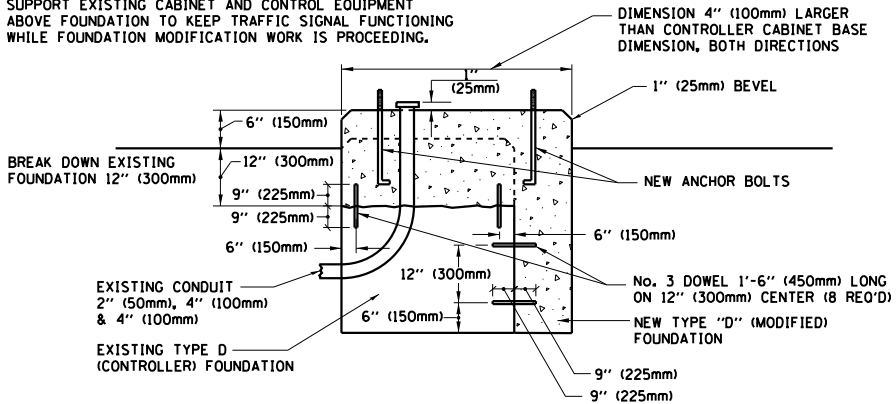
SHROUD

NOTES:

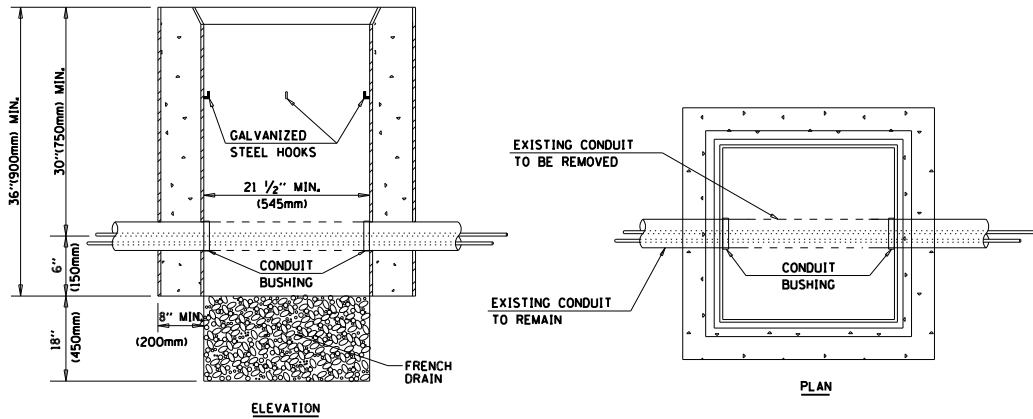
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

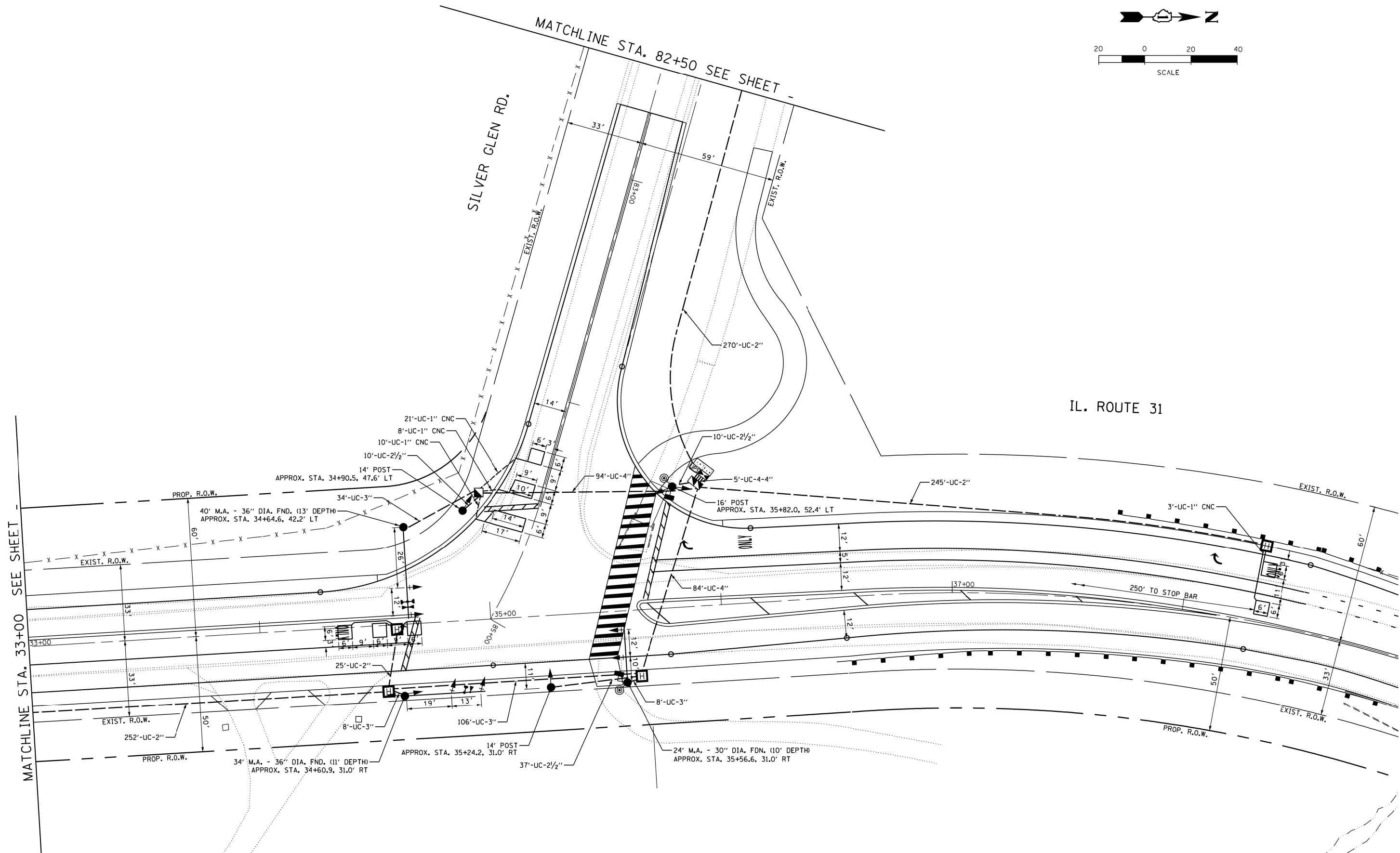
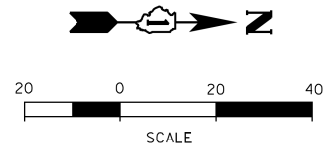


NOTES:

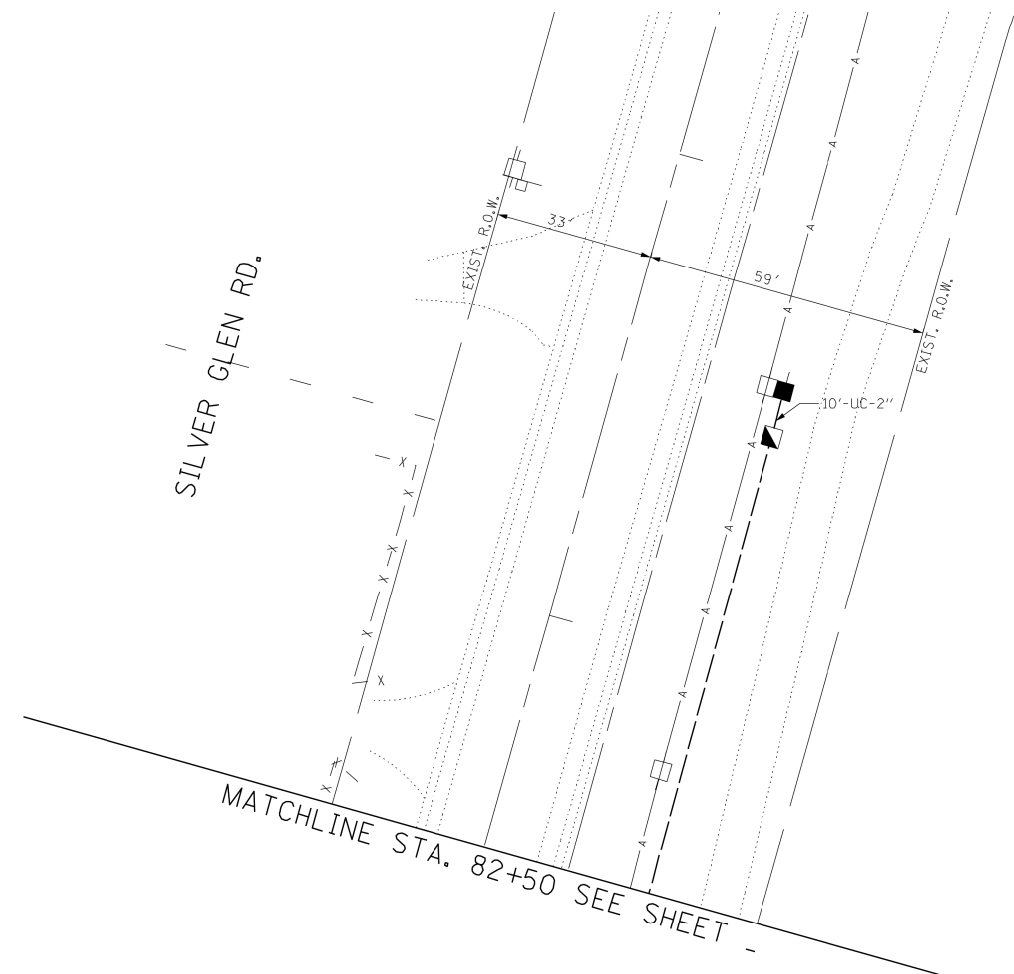
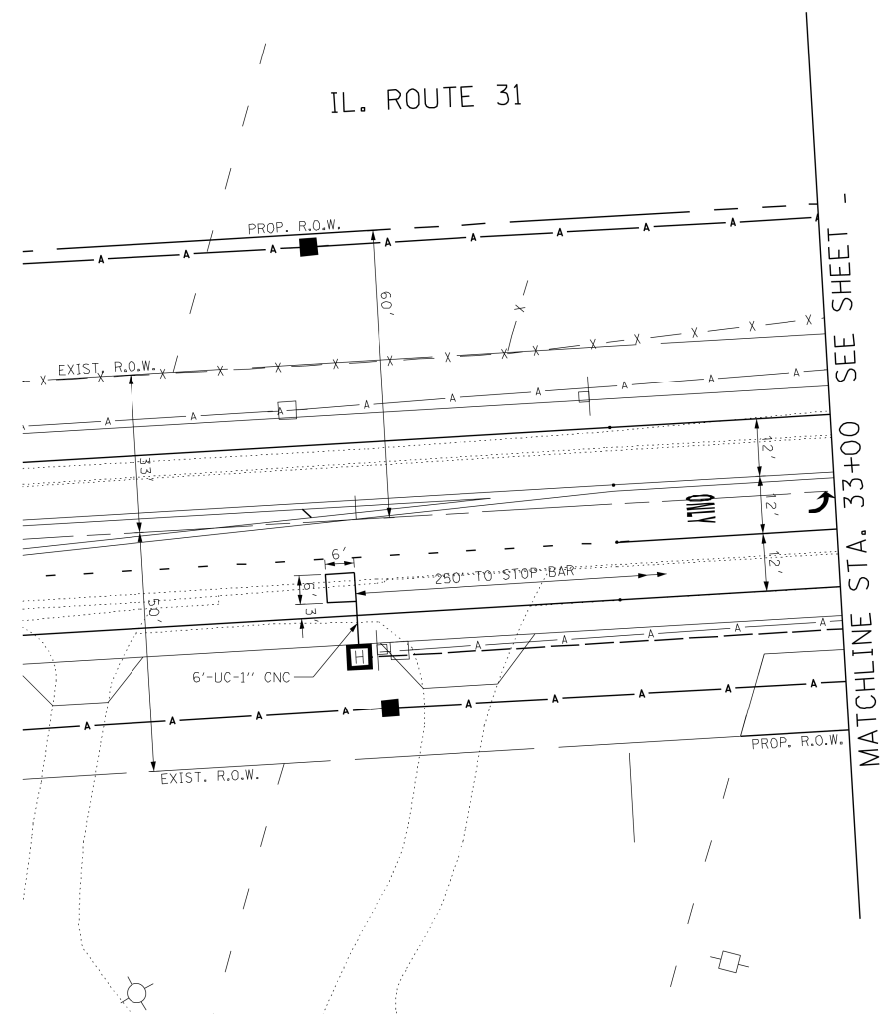
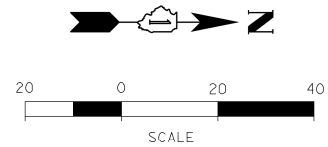
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

FILE NAME =	USER NAME = Bilgramiso	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.P. RTE.					SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\084EBIDINTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI4470\Drawings\Design\BOL\Std.dgn			REVISED -			3887	H-N		KANE	86	51			
	PLOT SCALE = 100.0000' / in.	CHECKED - DAD	REVISED -			TS-05					CONTRACT NO. 60R28			
	PLOT DATE = 7/14/2015	DATE - 10-28-09	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
				SCALE: NONE	SHEET NO. 6 OF 7 SHEETS	STA.	TO STA.							

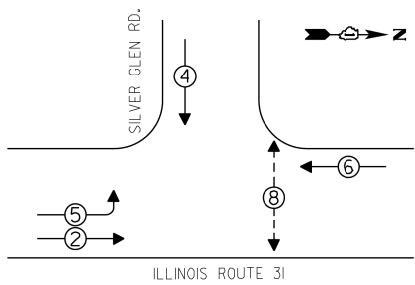


FILE NAME = S:\WP\Design\SVS\Consultant Design Let	USER NAME = smithsv Jobs\60R28-IL 31 @ Silver Glen Road (SSE)\Fire	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL INSTALLATION PLAN ILLINOIS ROUTE 31 AT SILVER GLEN ROAD		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN Spec\DI60R28-ShtSig-TS.dgn	REVISED -				3887	H-N	KANE	86	53
		PLOT SCALE = 40.0000' / in.	CHECKED -				CONTRACT NO. 60R28				
		PLOT DATE = 6/12/2015	DATE -				ILLINOIS FED. AID PROJECT				
Default					SCALE:		SHEET 1	OF 2	SHEETS	STA.	TO STA.



FILE NAME =	USER NAME = tngo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL INSTALLATION PLAN ILLINOIS ROUTE 31 AT SILVER GLEN ROAD				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60R28										
\$MODELNAME\$	PLOT DATE = 6/10/2015	DATE -	REVISED -		SCALE:	SHEET 2 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT				

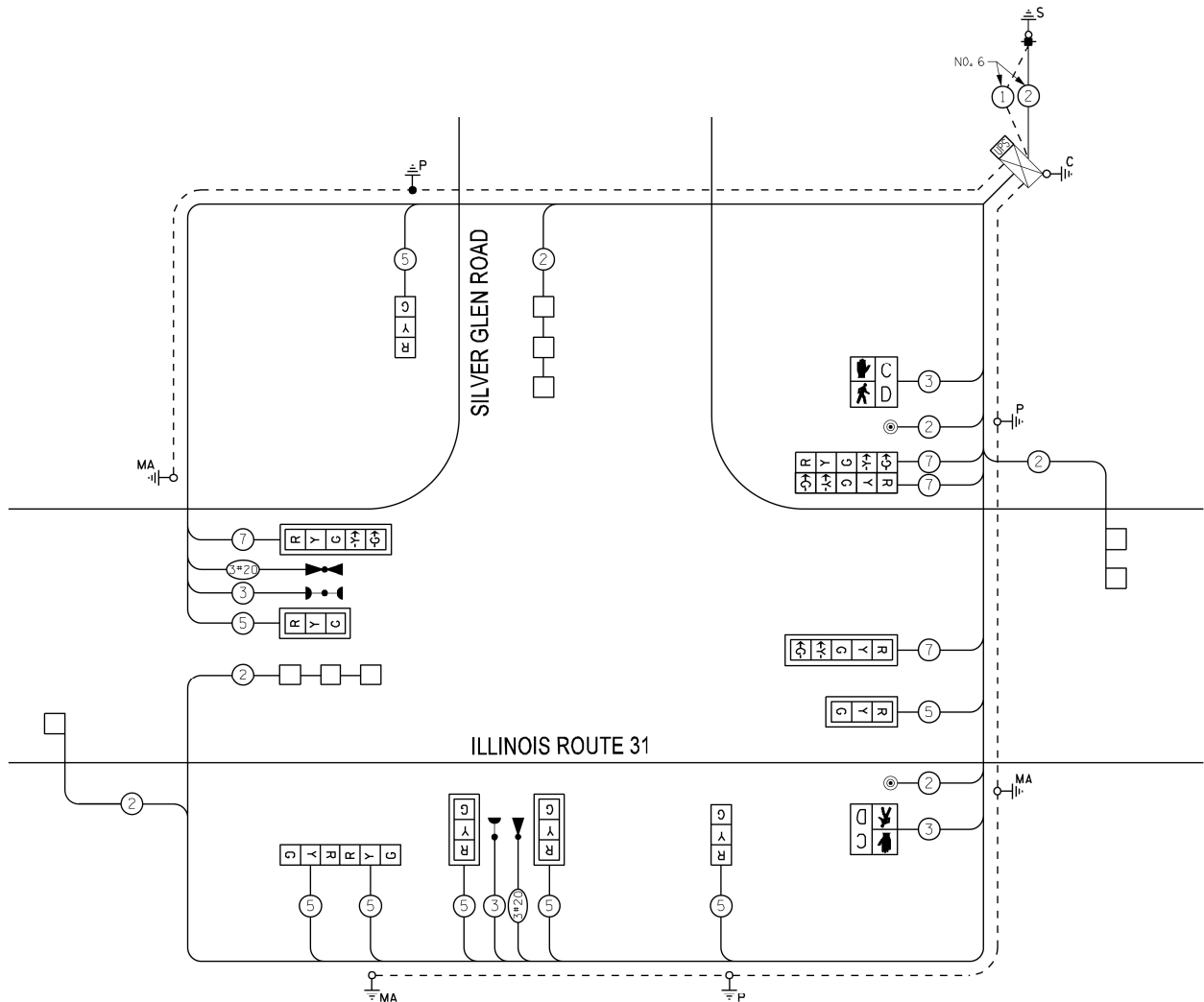
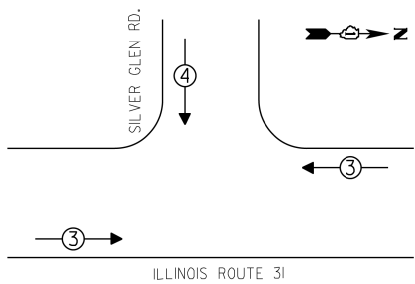
CONTROLLER SEQUENCE



- LEGEND
- SINGLE ENTRY PHASE
 - DUAL ENTRY PHASE
 - OVERLAP
 - PEDESTRIAN PHASE
 - NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE

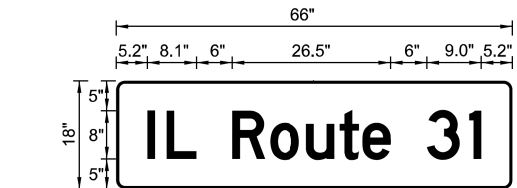


CABLE PLAN

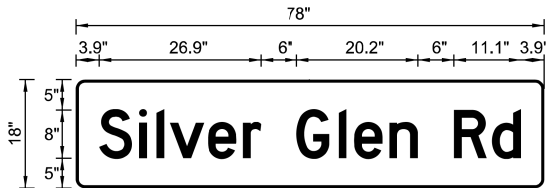
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	INCAND	LED	% OPERATIONS	
SIGNAL (RED)	12		17	0.50	102.00
(YELLOW)	12		25	0.25	75.00
(GREEN)	12		15	0.25	45.00
ARROW	8		12	0.10	9.60
PED. SIGNAL	2		25	1.00	50.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN			25	0.05	
LUMINAIRE		250		0.50	
VIDEO SYSTEM		150		1.00	
FLASHER				0.50	
ENERGY COSTS TO:					TOTAL = 381.60
ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAUMBURG, ILLINOIS 60196					
ENERGY SUPPLY - CONTACT: JOE STACHO					
PHONE: (630) 424-5704					
COMPANY: COMED					

FILE NAME =	USER NAME = tngo	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		CABLE PLAN, PHASE DESIGNATION AND EMERGENCY VEHICLE PREEMPTION SEQUENCE ILLINOIS ROUTE 31 AT SILVER GLEN ROAD		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P:\2012\ME12014-PTB162-18-Schwartz\CADD\99-60R28-IL 31-ct-Silver Glen\Bose\DI60R28-Sht 01.dwg		DRAWN -	REVISED -					3887	H-N	KANE	86	55
MODELNAME	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -					CONTRACT NO. 60R28				
	PLOT DATE = 6/10/2015	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

SIGN PANEL – TYPE 1 AND TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	8.25	1	ZZ	1



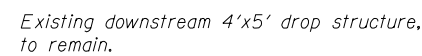
DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	9.75	2	ZZ	2

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QUANTITY
SIGN PANEL - TYPE 1	SQ FT	8.25
SIGN PANEL - TYPE 2	SQ FT	19.50
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	802
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	57
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	156
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	198
HANDHOLE	EACH	2
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	170
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	680
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1754
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	460
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1150
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	310
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	850
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	12
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	24
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNT DOWN TIMER	EACH	2
TRAFFIC SIGNAL BACKPLATE, FORMED PLASTIC	EACH	6
INDUCTIVE LOOP DETECTOR	EACH	4
DETECTOR LOOP, TYPE I	FOOT	297
* LIGHT DETECTOR	EACH	1
* LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	2
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
* ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	490
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET, SPECIAL	EACH	1

* INDICATES 100% MUNICIPALITY COST

Existing Structures: See Note 1 below.



Range 8E, 3rd P.M.

Twp. 40N

McLean Blvd

PROJECT LOCATION

Silver Glen Rd.

33

10

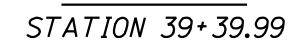
4

9

3

N

LOCATION SKETCH



GENERAL NOTES

1.

Precast concrete culverts, 4'x5' shall conform to the requirements of Article 540.06 of the Standard Specifications, the applicable requirements of AASHTO M 259 and ASTM C1577-14.

The minimum precast concrete strength shall be 5,000 psi.

Lifting holes shall be filled with concrete plugs and mastic after box sections are in place.

Fill varies from 2'-0" to 5'-0" within limits of roadway.
2.

Contractor shall maintain streamflow in accordance with the Standard Specifications Article 502.
3.

Reinforcement bars designated (E) shall be epoxy coated
4.

Diversion and Construction activities shall not be permitted to cause water levels upstream to rise more than the amount allowed per the drainage study.
5.

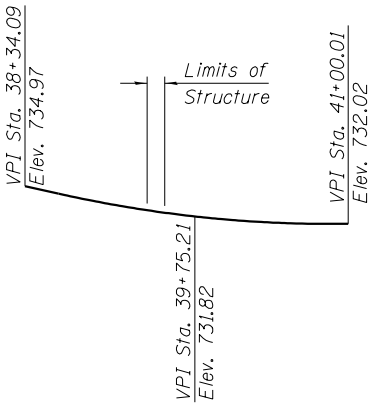
Cover from the face of Cast-in-Place Concrete to Reinforcement bars shall be 3" from surfaces formed against earth and 2" for all other surfaces unless otherwise shown. Cover from face of Precast Concrete to Reinforcement bars shall be 1" for all surfaces.
6.

Concrete fillets shall consist of unreinforced Class SI concrete. Construct after apron and walls. Use bonded construction joint (503.09 (b)) between fillet and apron wall surfaces.
8.

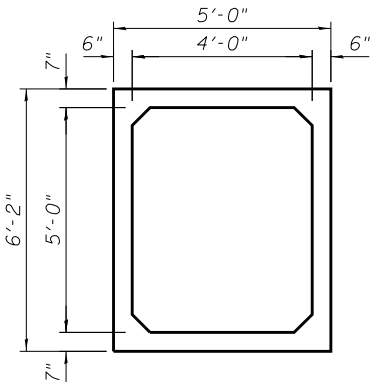
Chloride containing admixtures shall not be used for Class SI concrete.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal and Disposal of Unsuitable Material	Cu. Yd.	10.2
Porous Granular Embankment	Cu. Yd.	10.2
Concrete Barrier Removal	Foot	10
Removal of Existing Structures	Each	1
Concrete Removal	Cu. Yd.	2
Structure Excavation	Cu. Yd.	190
Rock Excavation for Structures	Cu. Yd.	10.6
Concrete Structures	Cu. Yd.	8.1
Reinforcement Bars, Epoxy Coated	Pound	2600
Expansion Bolts 1/2 Inch	Each	32
Precast Concrete Box Culverts 4'x5' (Special)	Foot	62
Concrete Barrier, Special	Foot	10
Temporary Soil Retention System	Sq. Ft.	443

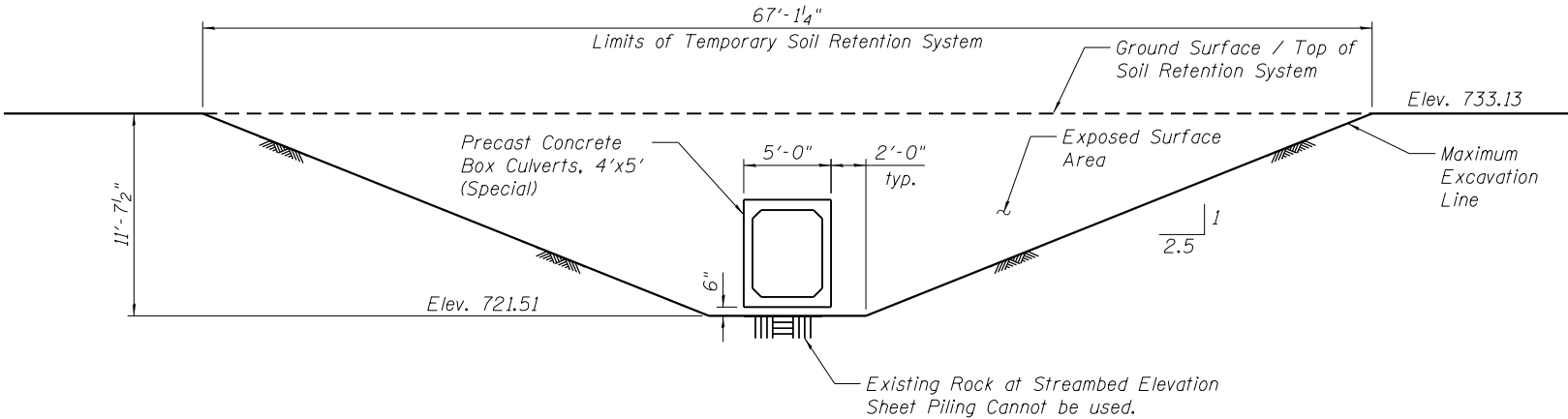
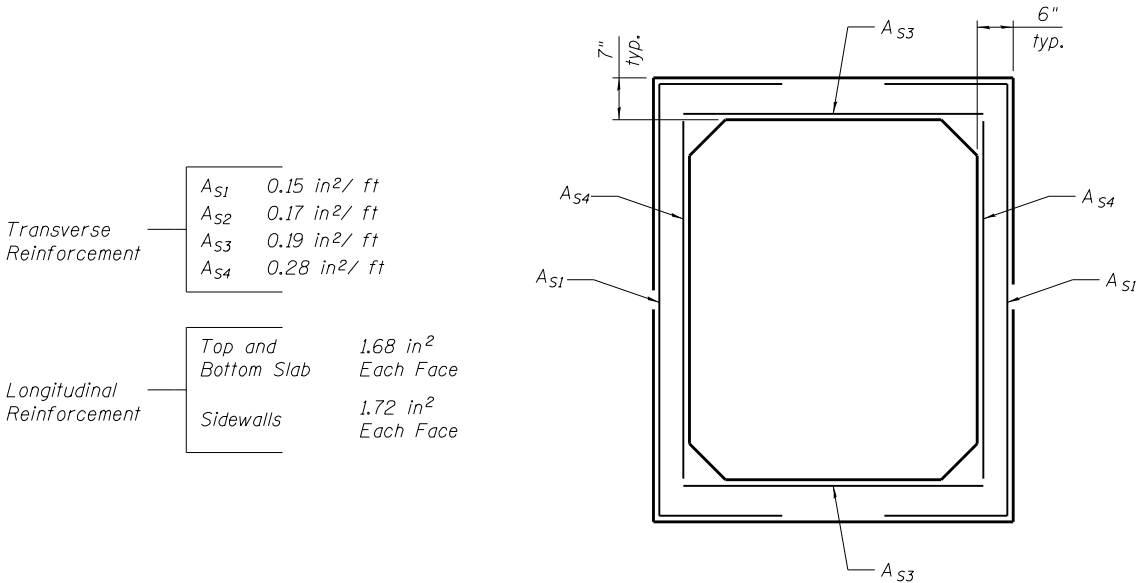


PROFILE GRADE
Along C of Road



SECTION THRU PRECAST BARREL

PRECAST BARREL REINFORCEMENT



TEMPORARY SOIL RETENTION SYSTEM DETAIL

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
CLIENT: HRGreen.com
DATE PLOTTED: 8/1/2015 9:46:54 AM
FILE NAME: P14709-shr-gemote.dgn
PLOT DRIVER: pdfnqlayerbw.dwt
PLOT LABEL: 1b1



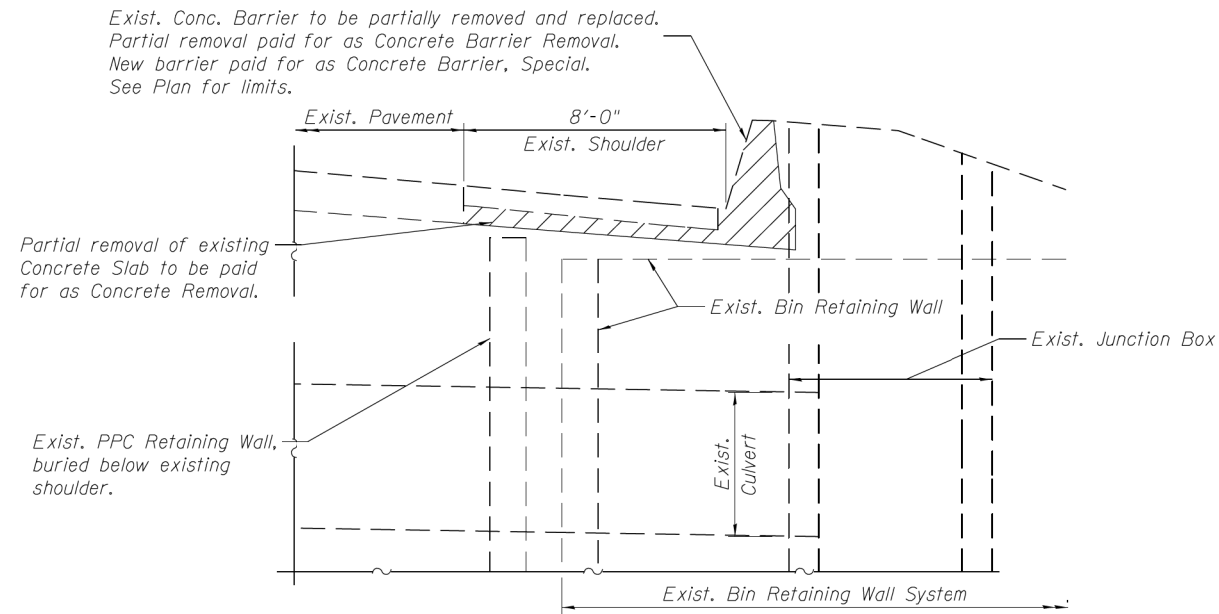
USER NAME =	DESIGNED - TOR	REVISED
	CHECKED - JMW	REVISED
PLOT SCALE =	DRAWN - WJH	REVISED
PLOT DATE =	CHECKED - RGD	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

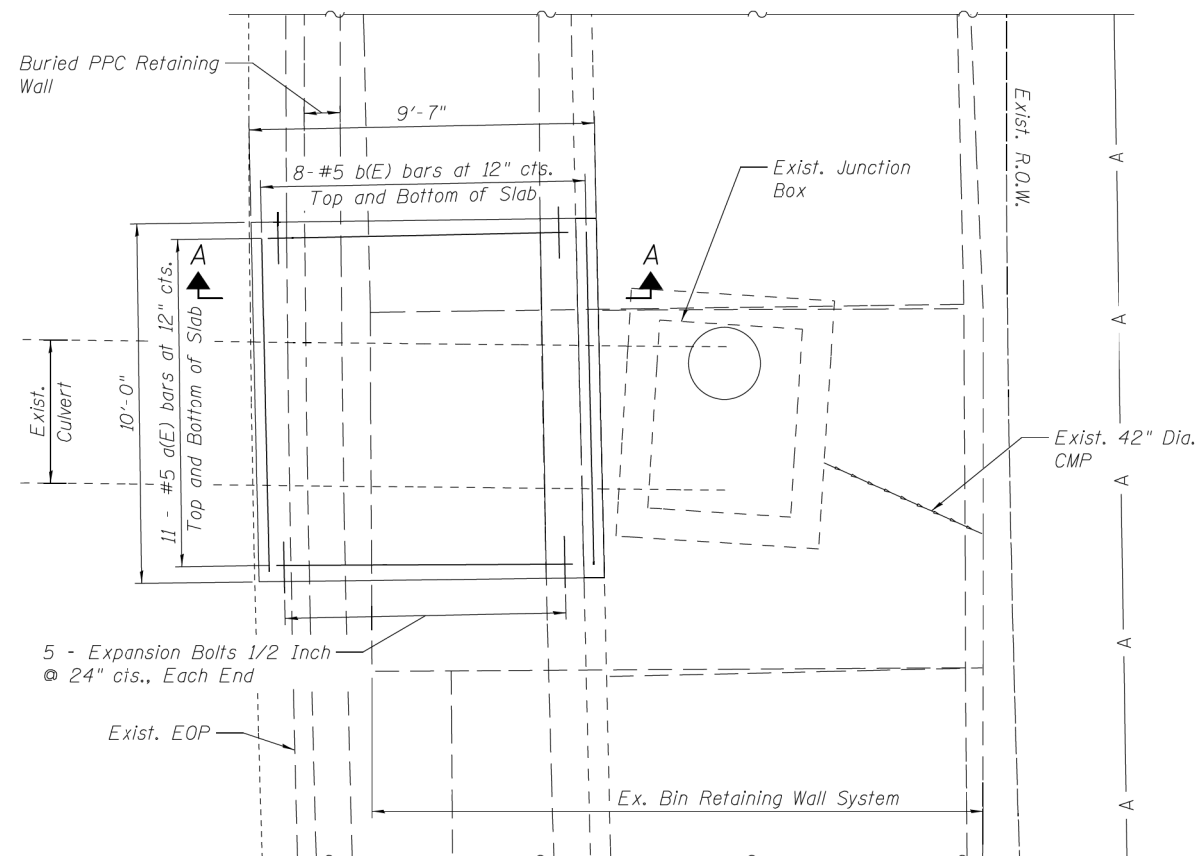
GENERAL NOTES AND BILL OF MATERIAL

SHEET NO. 2 OF 6 SHEETS

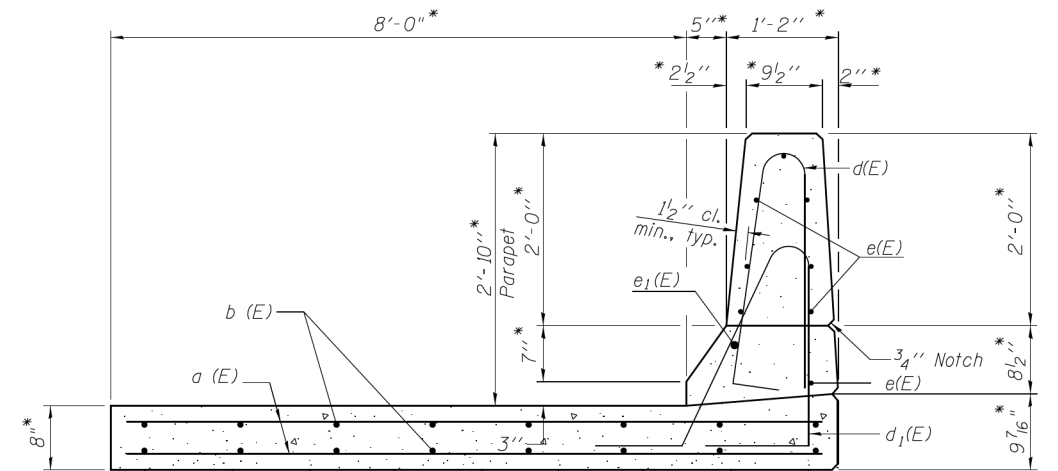
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	58
CONTRACT NO. 60R28				
ILLINOIS FED. AID PROJECT				



ELEVATION

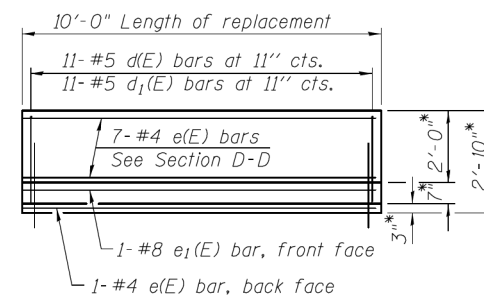
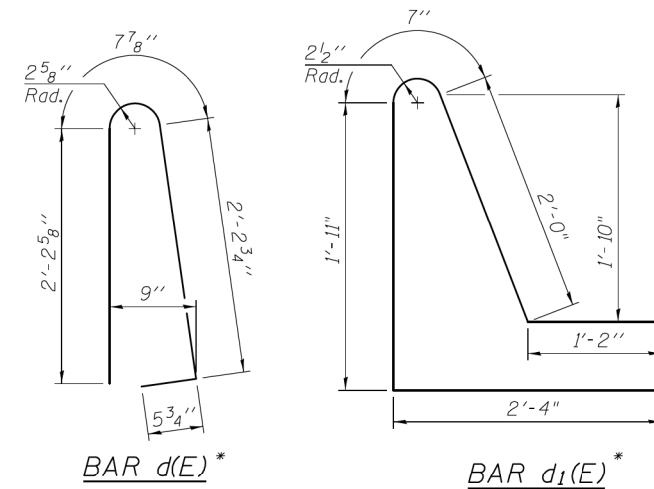


PLAN



SECTION A-A

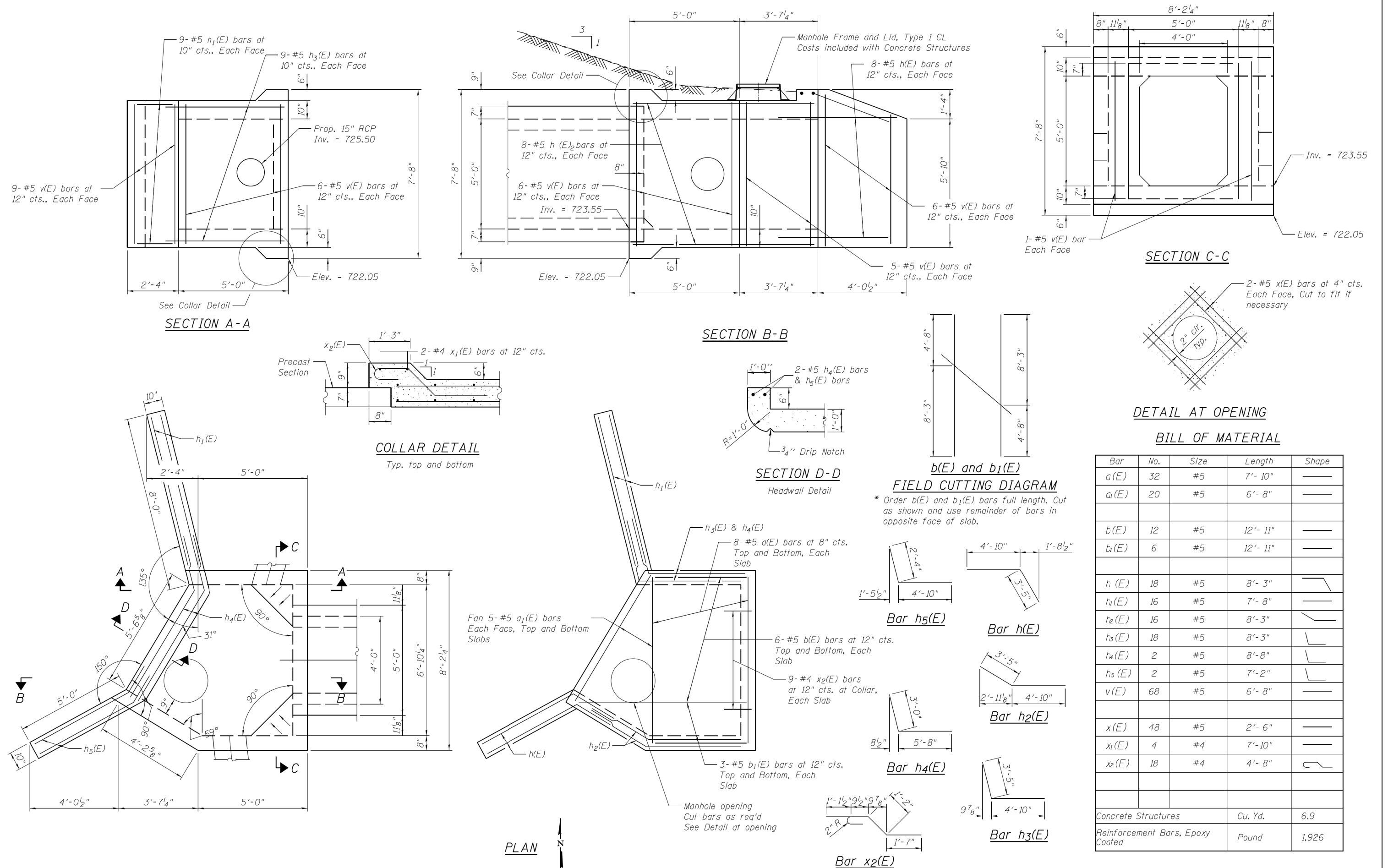
* Contractor shall field verify and adjust barrier and slab dimensions, bar lengths and bar bends as necessary to match exist. concrete barrier and slab dimensions.



**INSIDE FACE OF
PARAPET ELEVATION**

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	22	#5	9'- 3" *	—
b(E)	18	#5	9'- 8" *	—
d(E)	11	#5	5'- 7" *	U
d ₁ (E)	11	#5	8'- 0" *	⌋
e(E)	8	#4	9'- 8" *	—
e ₁ (E)	1	#8	9'- 8" *	—
Concrete Barrier, Special			Foot	10
Expansion Bolts 1/2 Inch			Each	10
Reinforcement Bars, Epoxy Coated			Pound	630 *



COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
CLIENT: HRGreen.com
DATE PLOTTED: 12/14/2014 2:52:39 PM
FILE NAME: HRGreen.com
PLOT DRIVER: HRGreen.com
PEN TABLE: HRGreen.com



USER NAME =	DESIGNED - TOR	REVISED
CHECKED - JMW	REVISOR	
PLOT SCALE =	DRAWN - WJH	REVISED
PLOT DATE =	CHECKED - RGD	REVISED





STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

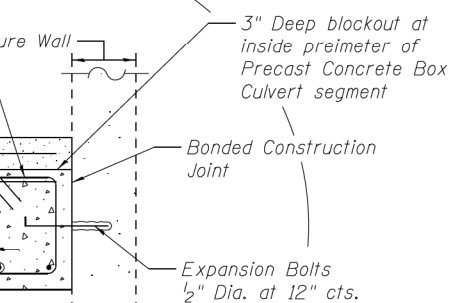
CONCRETE JUNCTION CHAMBER DETAILS

SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	60
CONTRACT NO. 60R28				
ILLINOIS FED. AID PROJECT				



LEGEND

-  = Partial concrete removal of west wall of existing drop structure. Cost shall be included in unit price for Removal of Existing Structures. Match opening of Proposed Precast Concrete Box Culvert.
- * = Precast culvert slab and wall thickness to be verified with precast supplier.
-  = Overlap between Proposed Precast Concrete Box Culvert and Proposed C.I.P. Collar.
-  = Proposed Precast Concrete
-  = Proposed C.I.P. Concrete

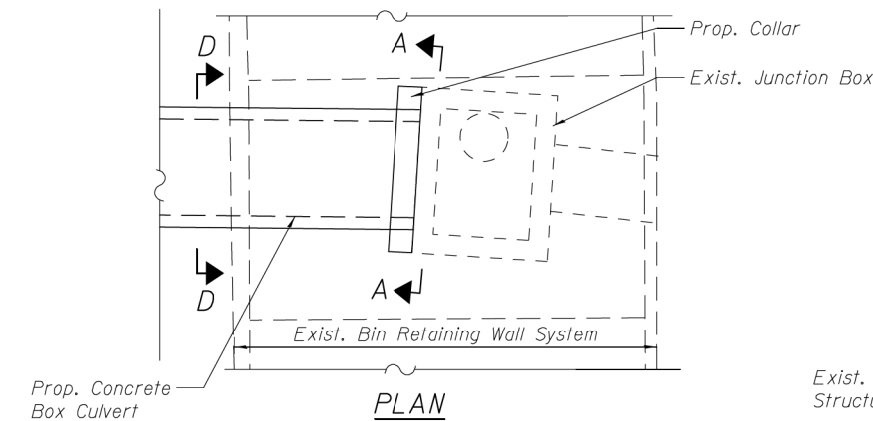


TYPICAL COLLAR
REINFORCEMENT DETAIL

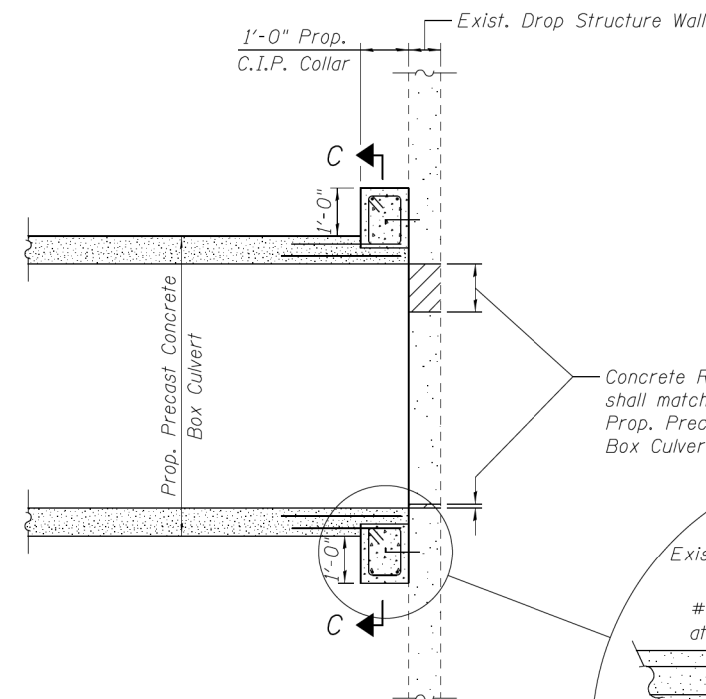
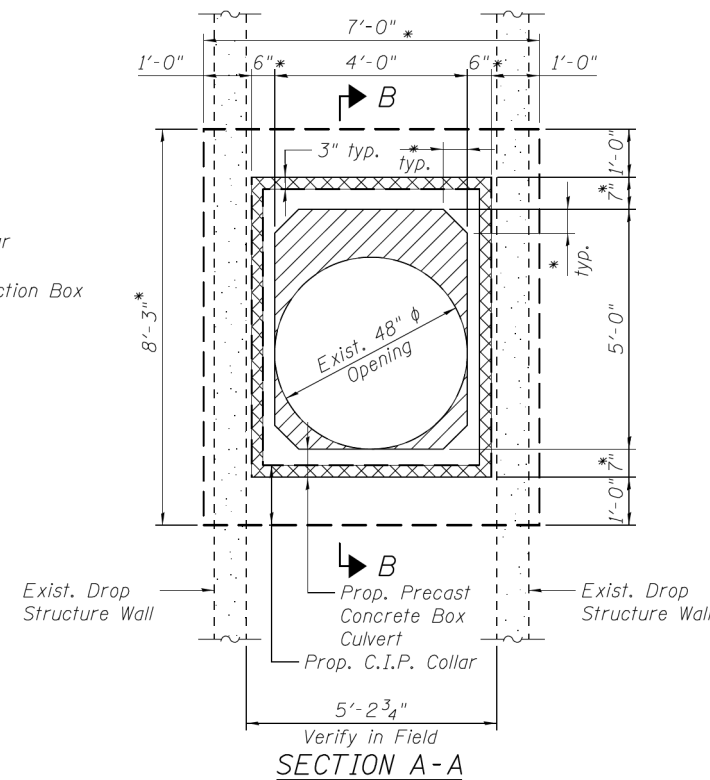
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
s(E)	26	#3	3'- 10"	
u (E)	2	#4	16'- 5"	
Concrete Structures			Cu. Yd.	1.2
Expansion Bolts 1/2 Inch			Each	22
Reinforcement Bars, Epoxy Coated			Pound	60

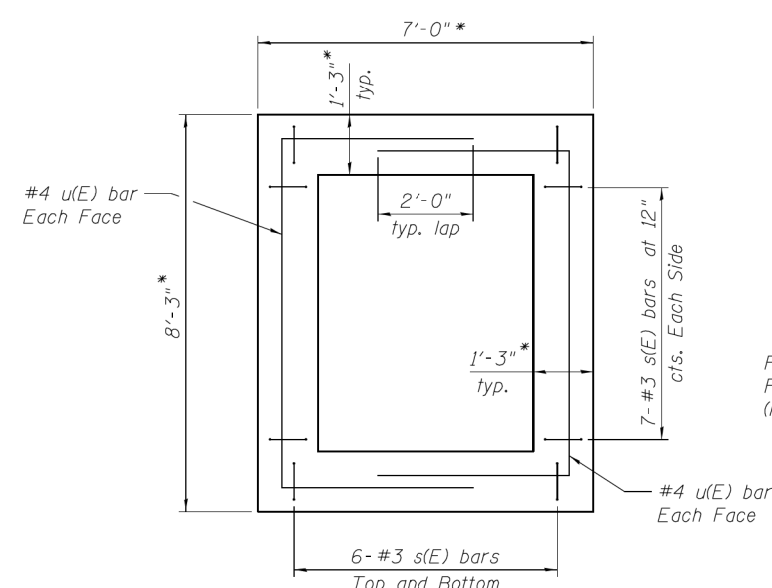
- Notes:
- Contractor shall protect the existing bin retaining wall system from damage and backfill portions of bin wall excavated during construction only after either temporary bracing or permanent structure elements capable of bracing cut sections of wall stringers have been installed.
 - Cost shall be included in the unit price for Removal of Existing Structures.



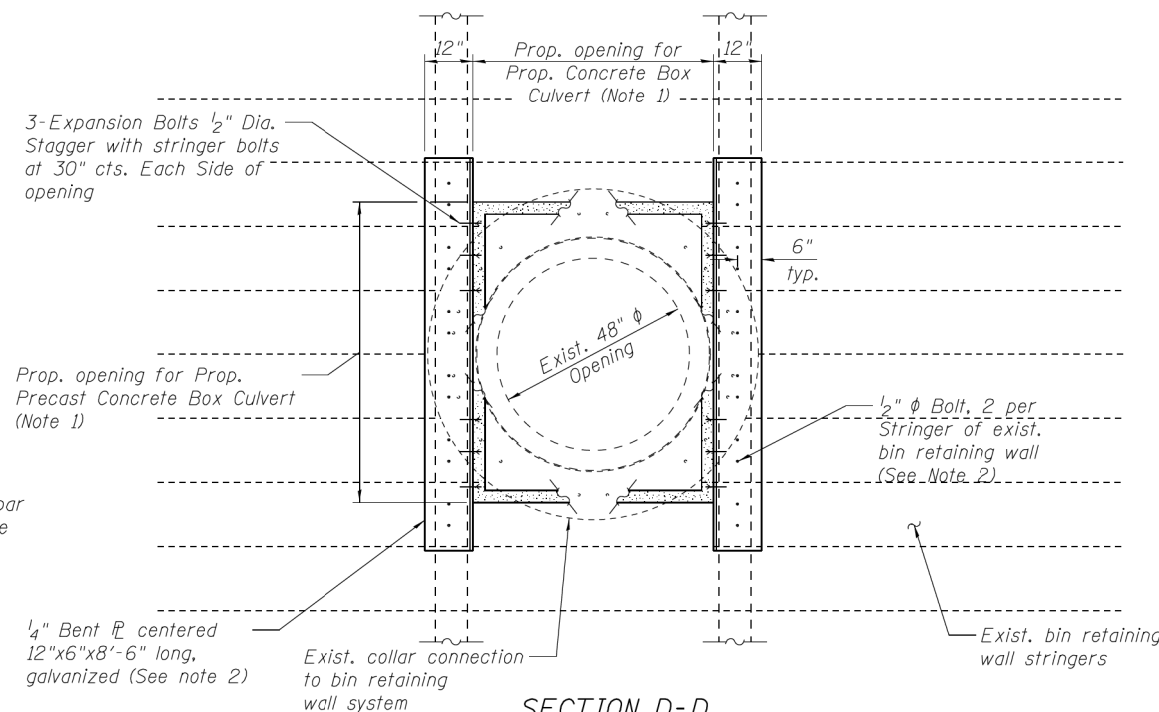
PLAN
DOWNSTREAM OUTLET
This Plan View has been reduced in scale 50%



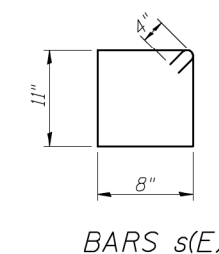
SECTION B-B



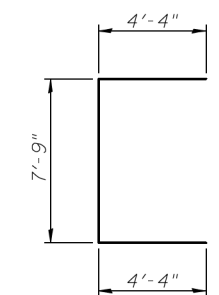
SECTION C-C



SECTION D-D



BARS s(E)



BAR u(E)

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
CLIENT: HRGreen.com
DATE PLOTTED: HRGreen.com
FILE NAME: HRGreen.com
PLOT DRIVER: HRGreen.com
PEN TABLE: HRGreen.com



USER NAME =	DESIGNED - TOR	REVISED
CHECKED - JMW	CHECKED - JMW	REVISED
PLOT SCALE =	DRAWN - WJH	REVISED
PLOT DATE =	CHECKED - RGD	REVISED

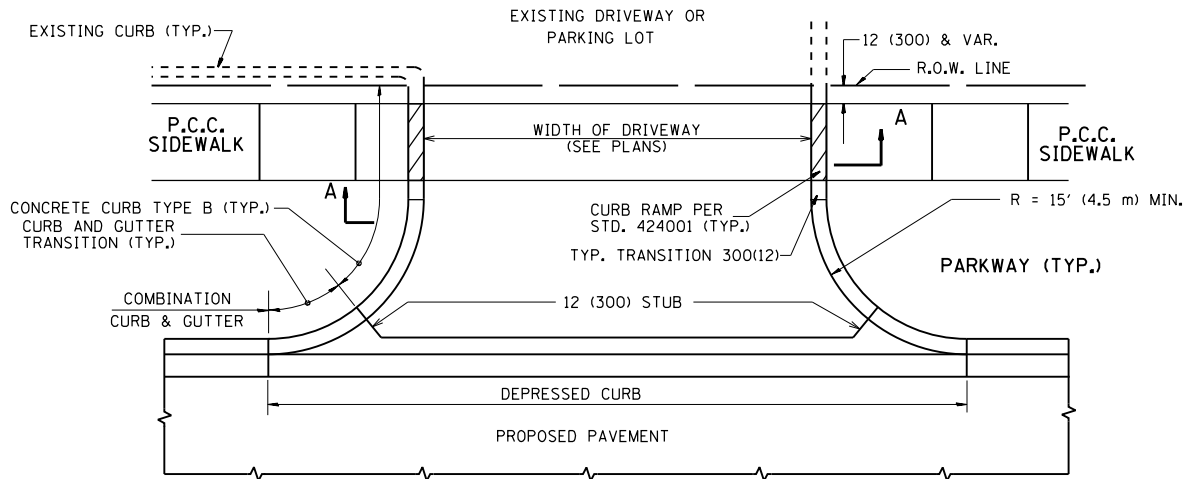
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONNECTION DETAILS

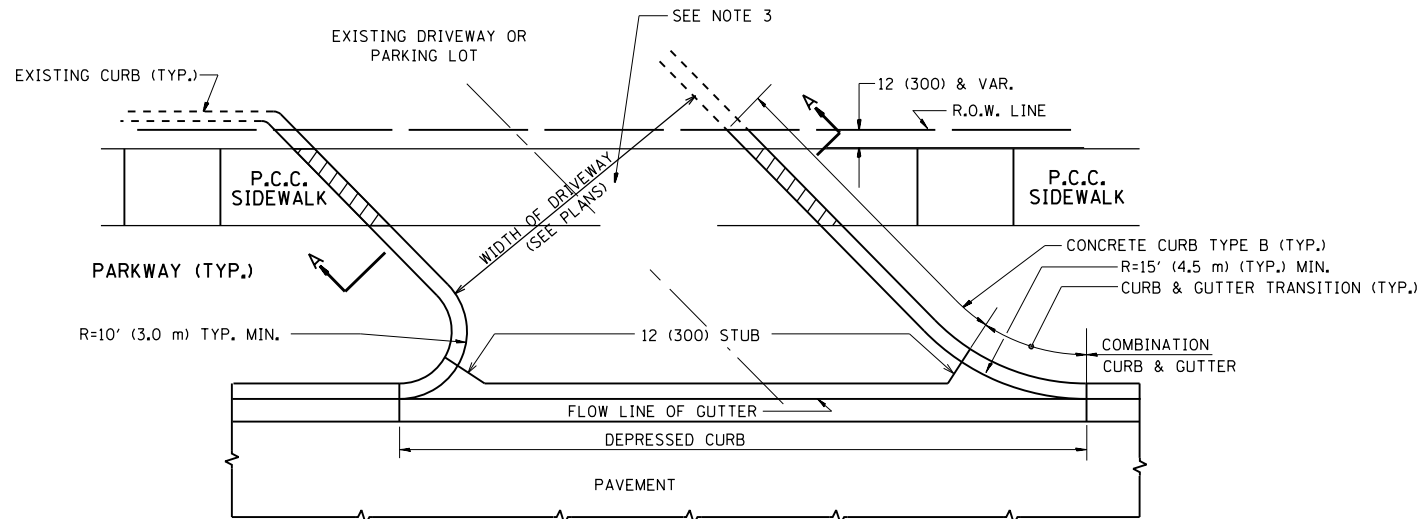
SHEET NO. OF SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	61
CONTRACT NO. 60R28				

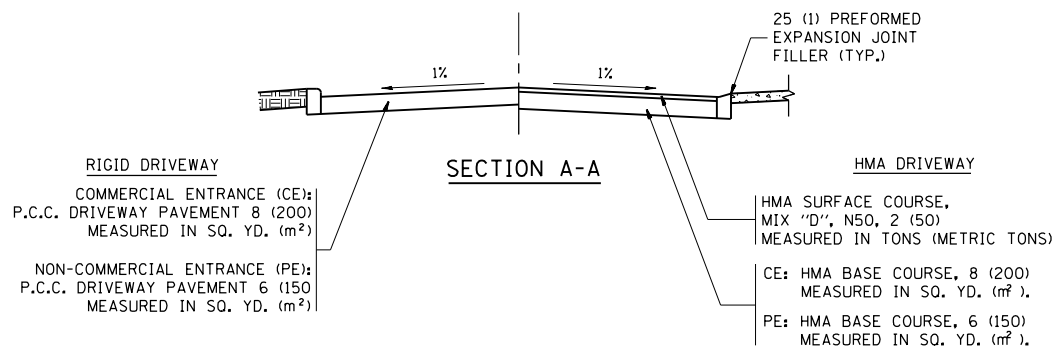
ILLINOIS FED. AID PROJECT



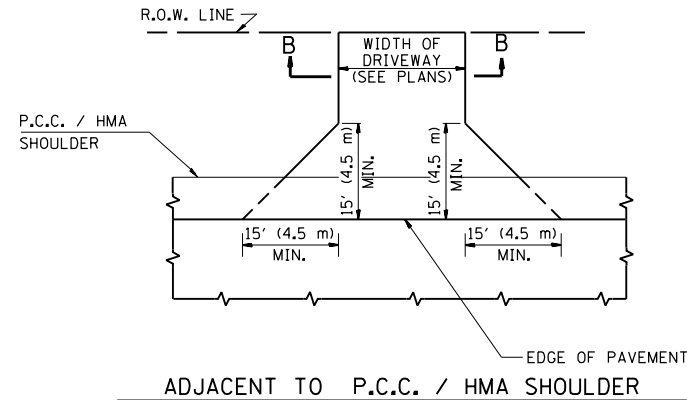
WITH CONCRETE CURB, TYPE B



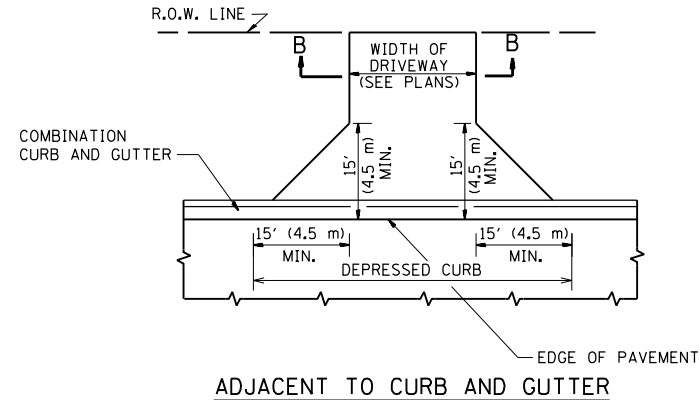
WITH CONCRETE CURB, TYPE B



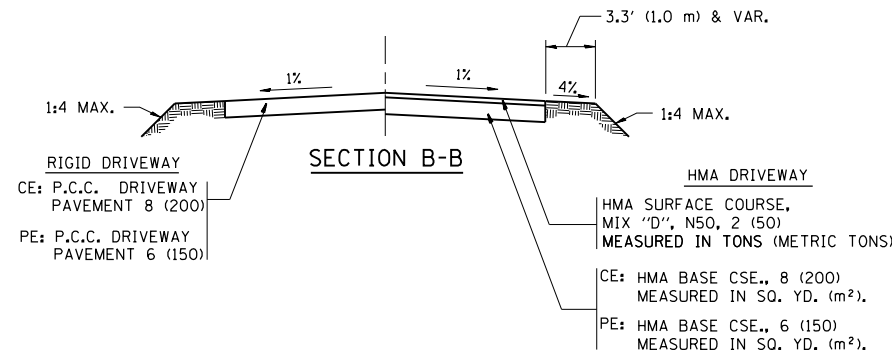
SECTION A-A



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

CE: HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (m²),
PE: HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (m²).

FILE NAME =	USER NAME = Bilgramiso	DESIGNED - R. SHAH	REVISED - P. LoFLUER 04-15-03
p:\11\084EBID\INTEG\Illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\PI4470\Drawings\Design\DistStd.dgn		REVISOR - R. BORO 01-01-07	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0002' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 7/14/2015	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N		KANE	86	62
BD0156-07 (BD-01)			CONTRACT NO. 60R28		
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT		



EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL
REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

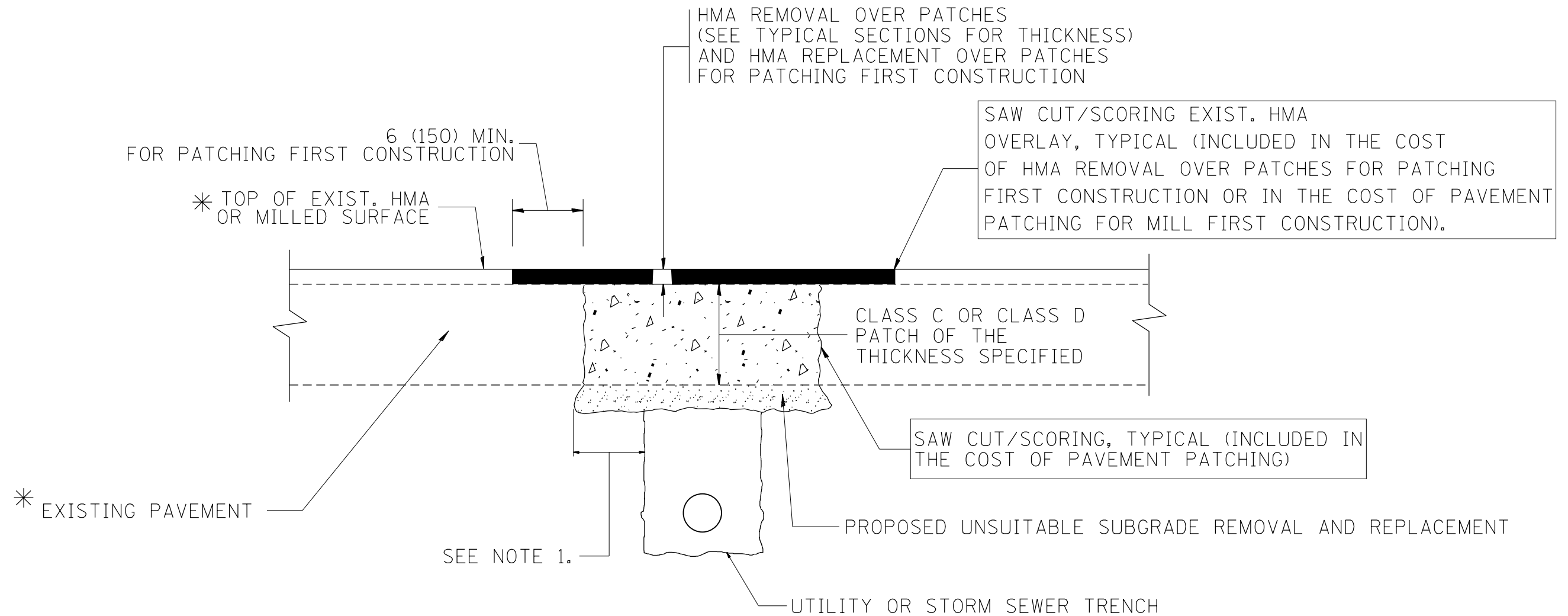
REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY
STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE
RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = Bilgramise	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</div>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pwt\NL084EBID\INTE\Gullinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\P1470\FrameData\Design\DistStd.dgn		7/14/2015	01-01-07					3887	H-N	KANE	86	63
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11			BD600-03 (BD-8)			CONTRACT NO. 60R28				
PLOT DATE = 7/14/2015	DATE = 10-25-94	REVISED - R. BORO 12-06-11			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

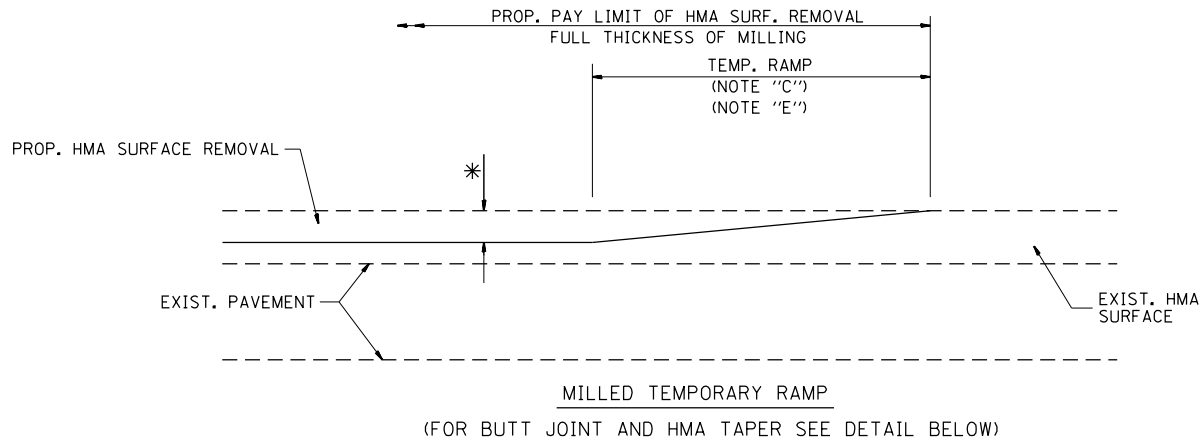
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

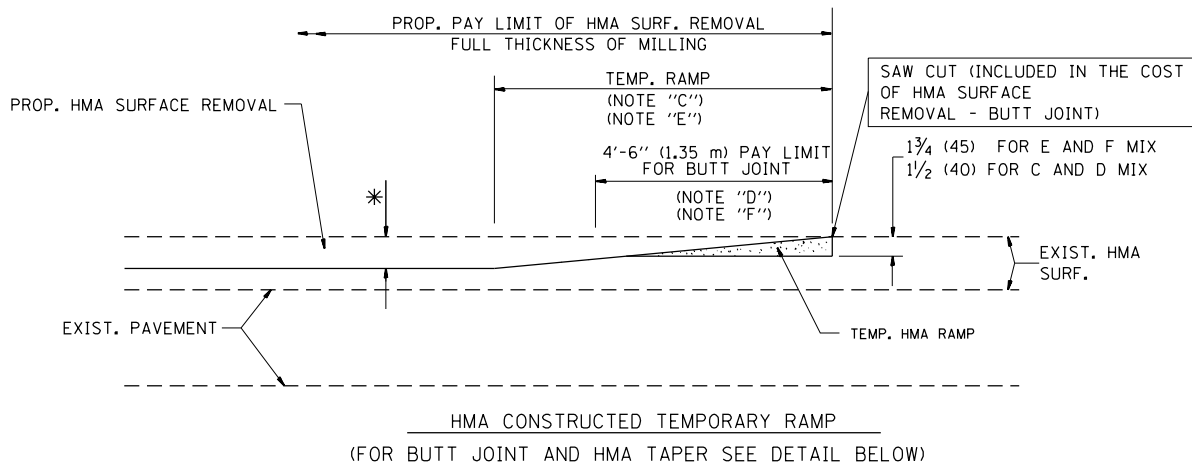
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bilgramiso	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI4470\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07					3887	H-N	KANE	86	64
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07					BD400-04 (BD-22)		CONTRACT NO. 60R28		
	PLOT DATE = 7/14/2015	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

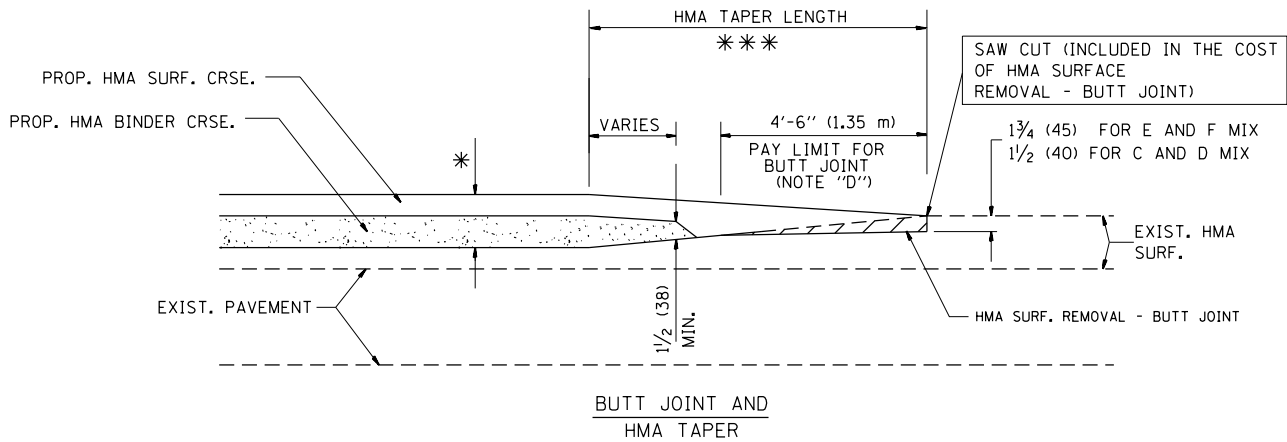


OPTION 1

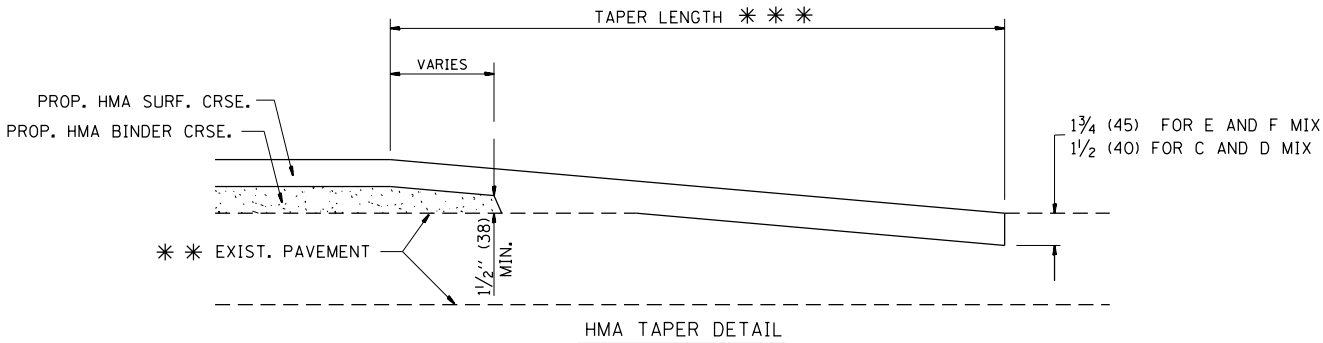
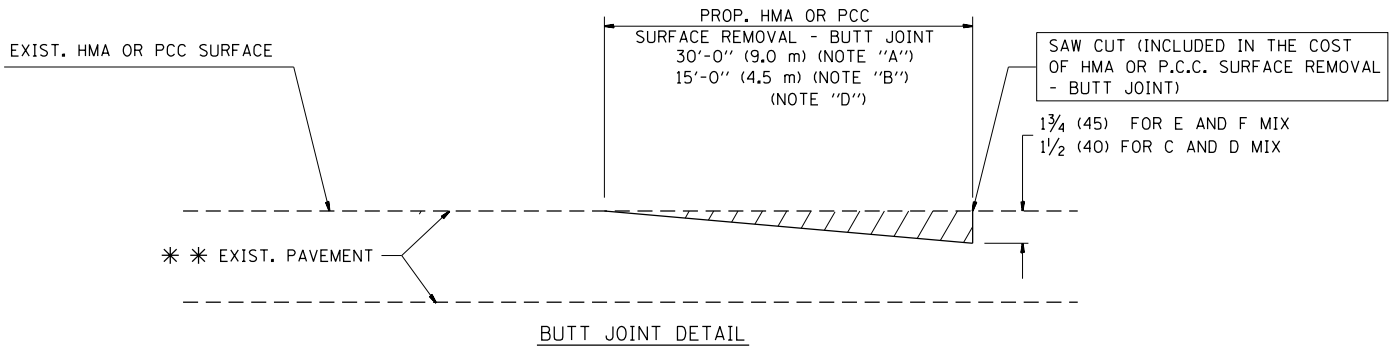


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

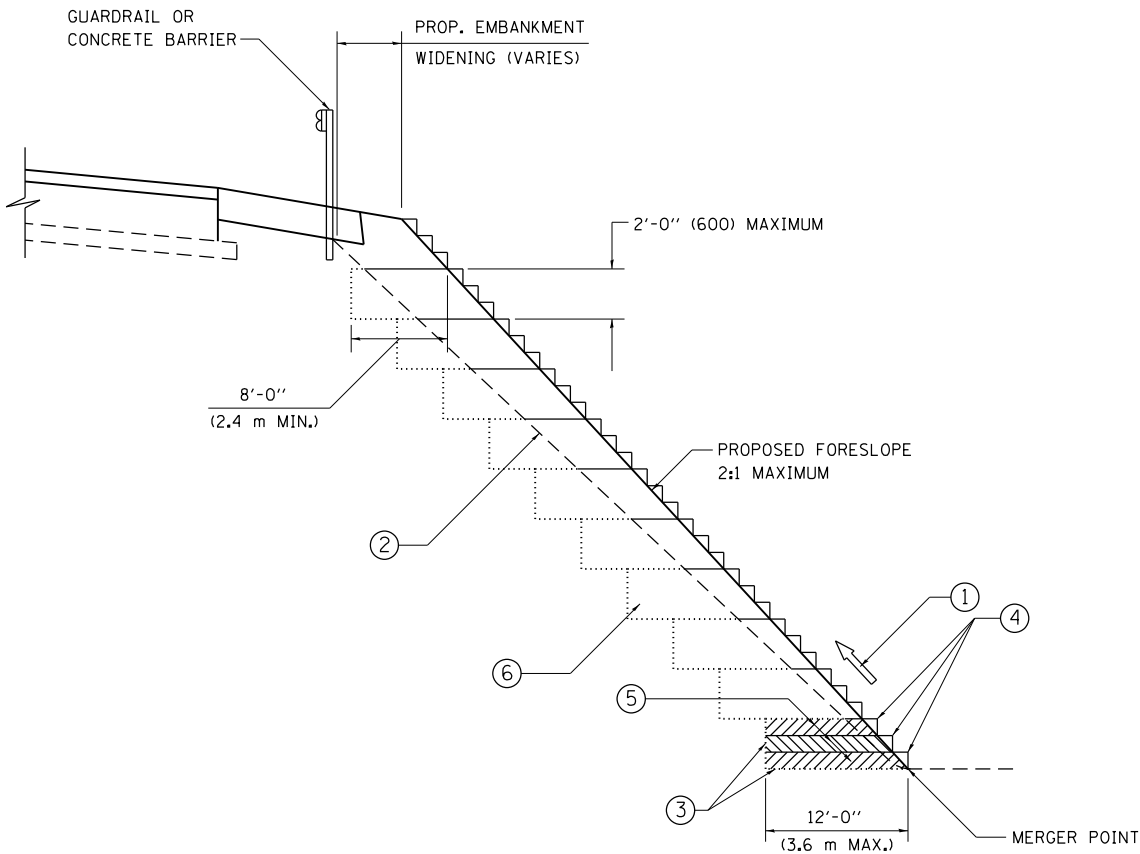
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bilgramiso	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS	F.A.P. RTE.						
p:\1\1084EBIDINTEG\Illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\PI447\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97			3887	SECTION H-N		COUNTY KANE	TOTAL SHEETS 86	SHEET NO. 65	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01			BD400-05 BD32						
	PLOT DATE = 7/14/2015	DATE - 06-13-90	REVISED - R. BORO 01-01-07			CONTRACT NO. 60R28						
SCALE: NONE					SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



TYPICAL BENCHING DETAIL
FOR EMBANKMENT

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	BENCHING DETAIL FOR EMBANKMENT WIDENING				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\1084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI447\061604\DATA\Design\CA06Std.dgn		CHECKED -	REVISED -						3887	H-N	KANE	86	66
Default	PLOT SCALE = 100.0000' / in.	CHECKED - S.E.B.	REVISED -		BD-51				CONTRACT NO. 60R28				
	PLOT DATE = 7/14/2015	DATE - 06-16-04	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	



TWO-LANE/TWO-WAY



GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

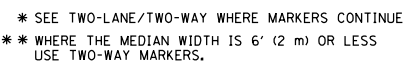
- YELLOW STRIPE
- == WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

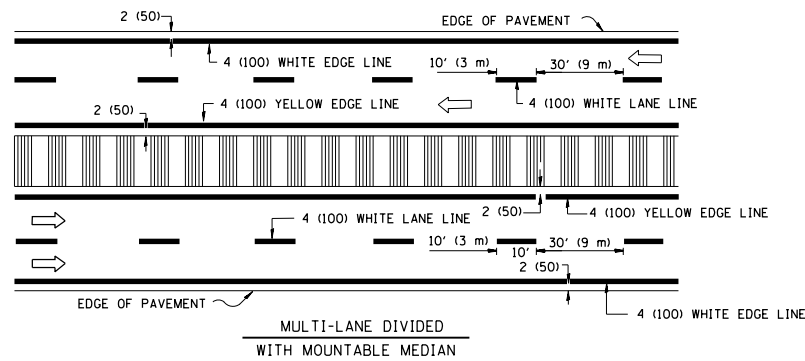
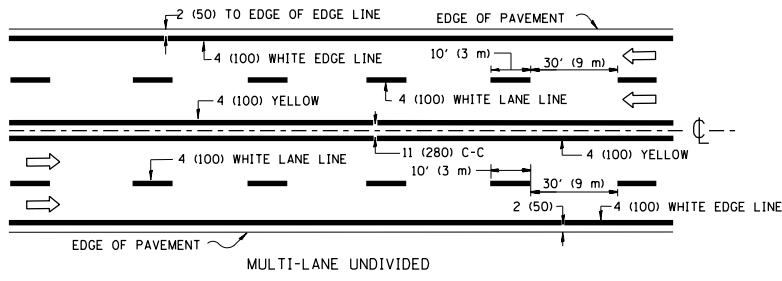
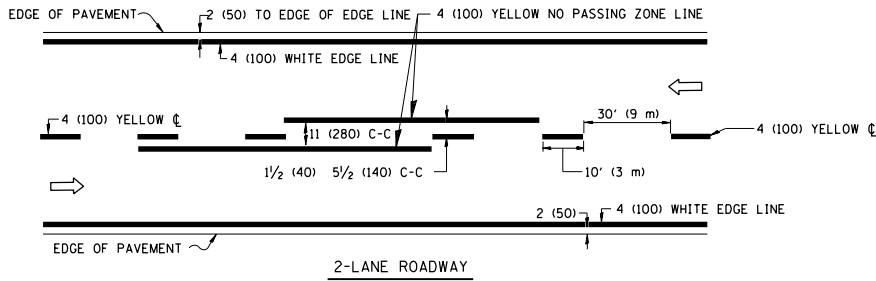
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

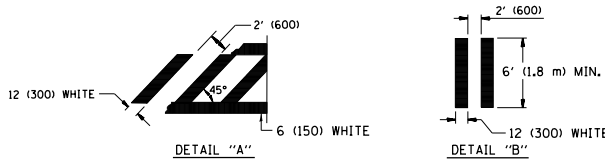
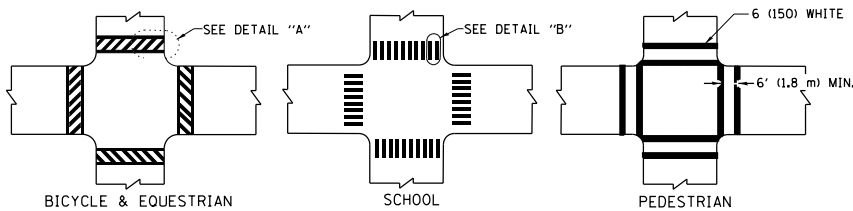
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Bilgramis	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</div>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL0846BID\INTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\PI44\TRAFFIC Data\Design\DistStd.dgn		03-12-99	REVISED - T. RAMMACHER 03-12-99						3887	H-N	KANE	86	68
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		<div>TC-11 CONTRACT NO. 60R2</div>								
	PLOT DATE = 7/14/2015	DATE	REVISED - C. JUCIUS 09-09-09						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT

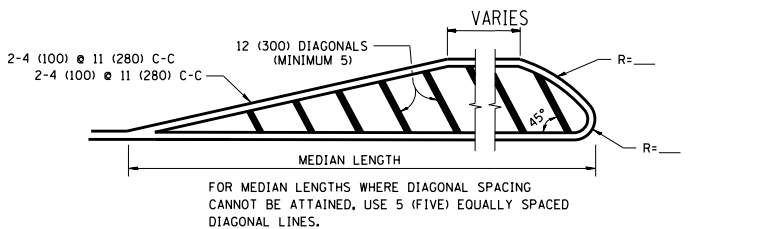
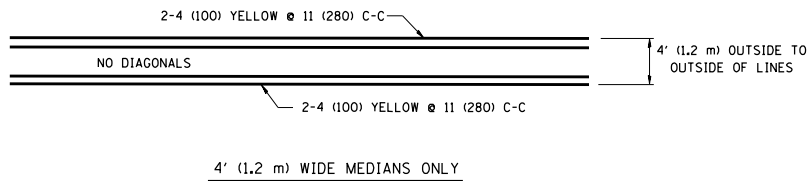


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

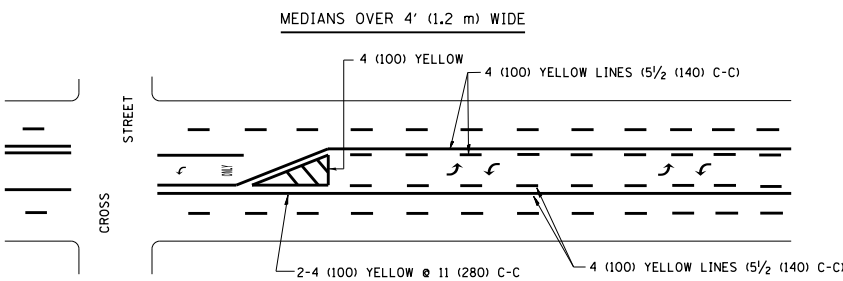
TYPICAL LANE AND EDGE LINE MARKING



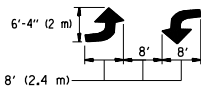
TYPICAL CROSSWALK MARKING



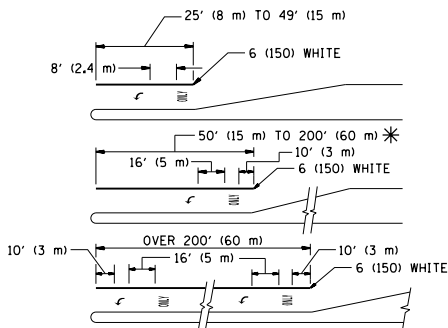
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR.
ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING

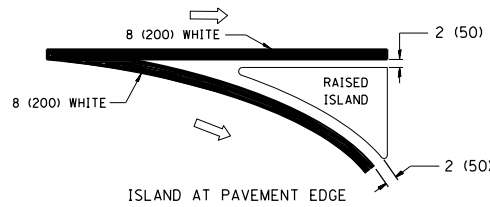
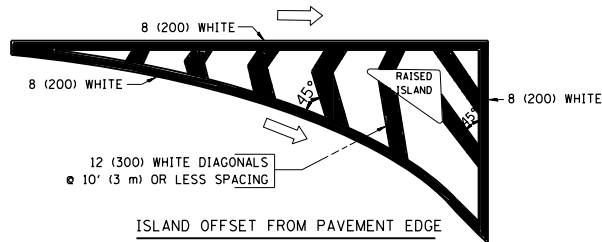


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

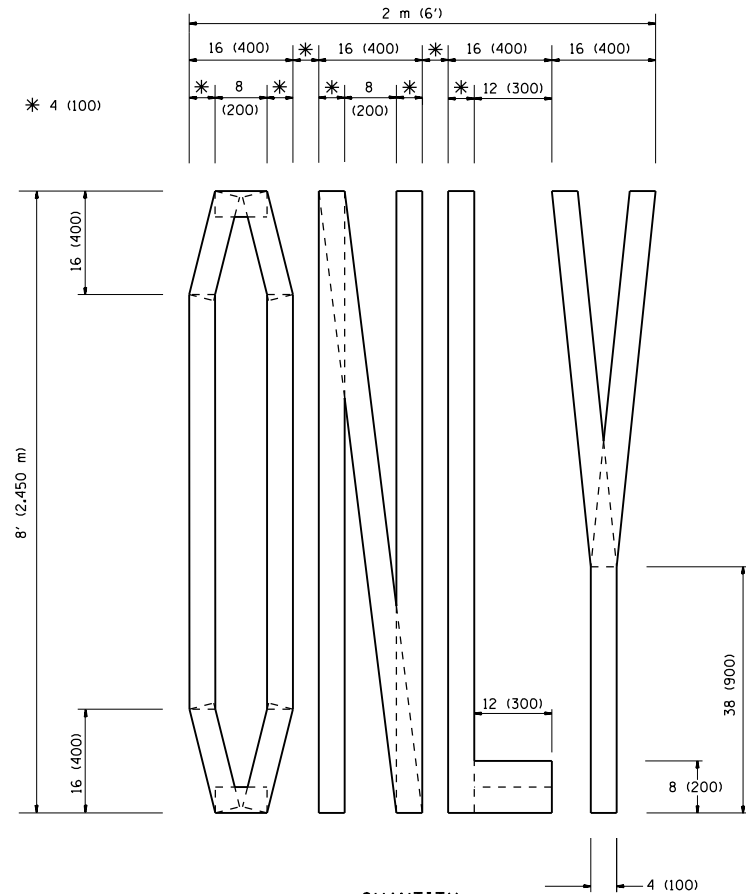


TYPICAL ISLAND MARKING

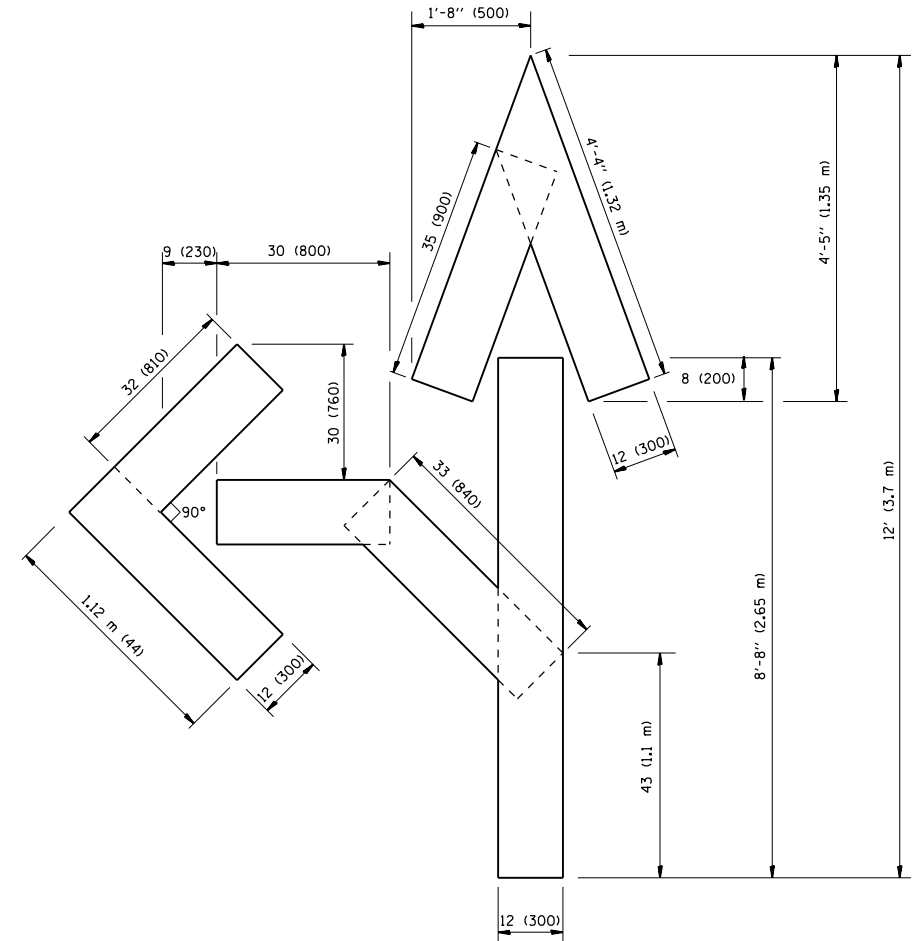
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINE; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

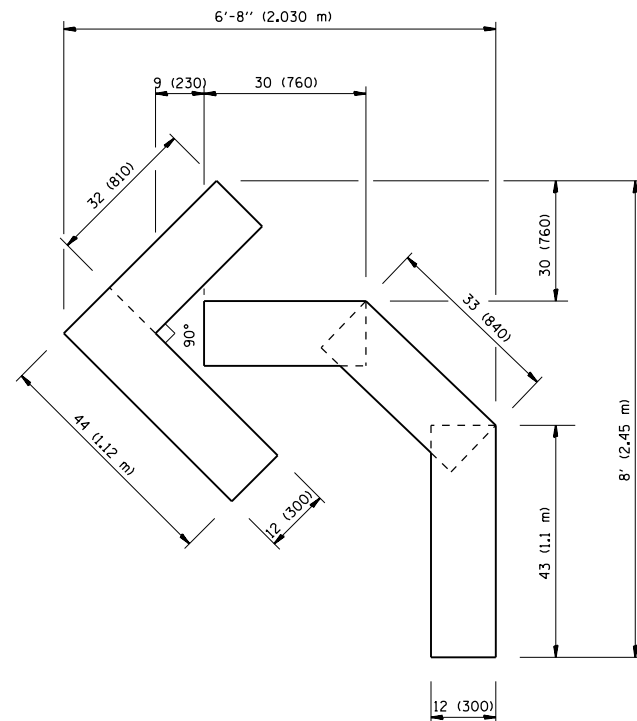
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



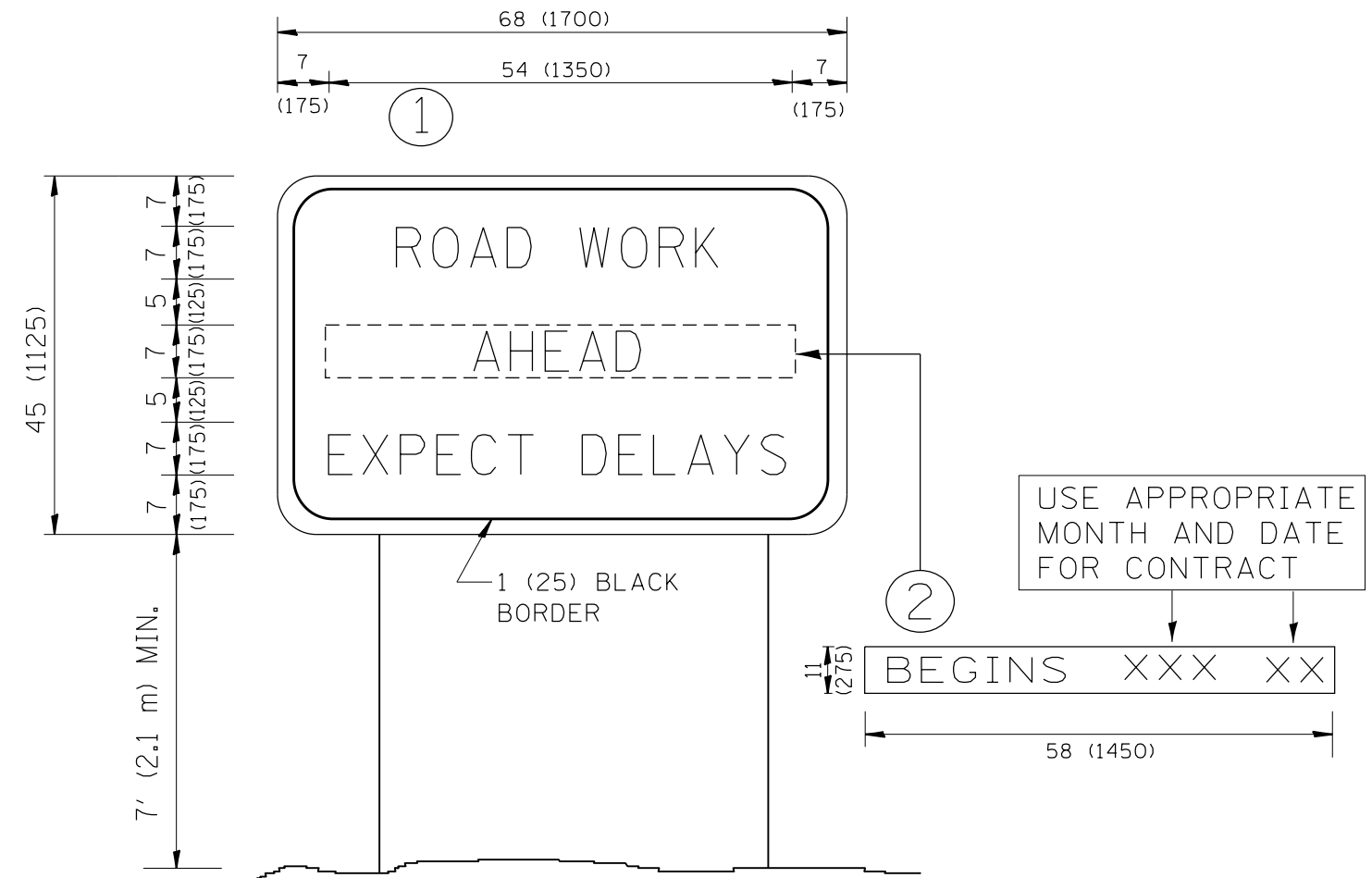
QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

FILE NAME = pwt\11084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI4470\Drawings\Design\DistStd.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
			REVISED -T. RAMMACHER 11-04-97					3887	H-N	KANE	86	70		
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98		SCALE: NONE			SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	TC-16		CONTRACT NO. 60R28
	PLOT DATE = 7/14/2015	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

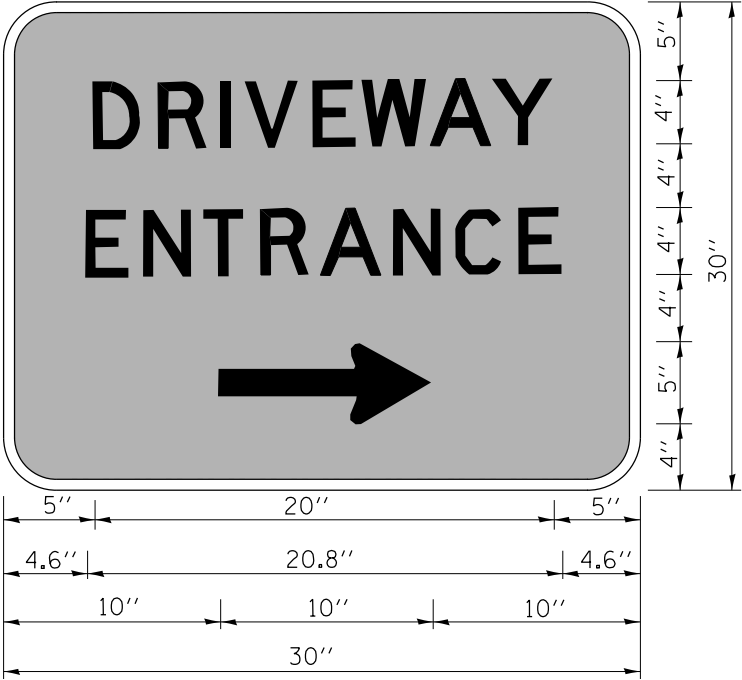


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = p:\11\084EBID\INTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI447\084EBID\Design\DistStd.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			REVISED - R. MIRS 12-11-97						3887	H-N	KANE	86	71
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22		CONTRACT NO. 60R28		
	PLOT DATE = 7/14/2015	DATE -	REVISED - C. JUCIUS 01-31-07						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.			



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

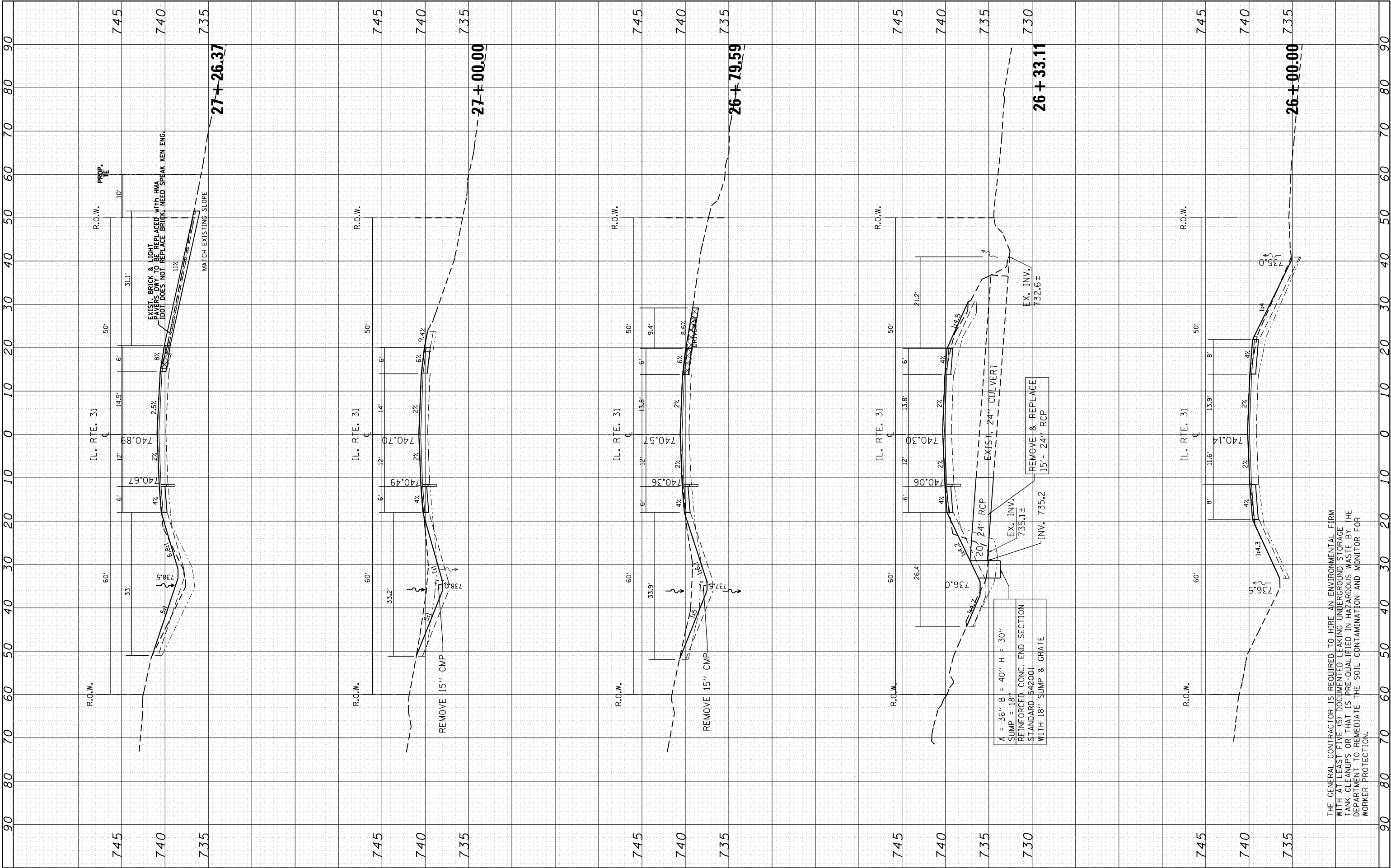
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = p:\11\084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\PI447\084EBID\Design\DistStd.dgn	USER NAME = Bilgramiso	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
									3887	H-N	KANE	86
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	PLOT DATE = 7/14/2015	DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

FINL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.	NO.	NO.

ORIGINAL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.	NO.	NO.



FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -
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PLOT SCALE = 20.0000' / in.		DATE -	REVISED -
Default			

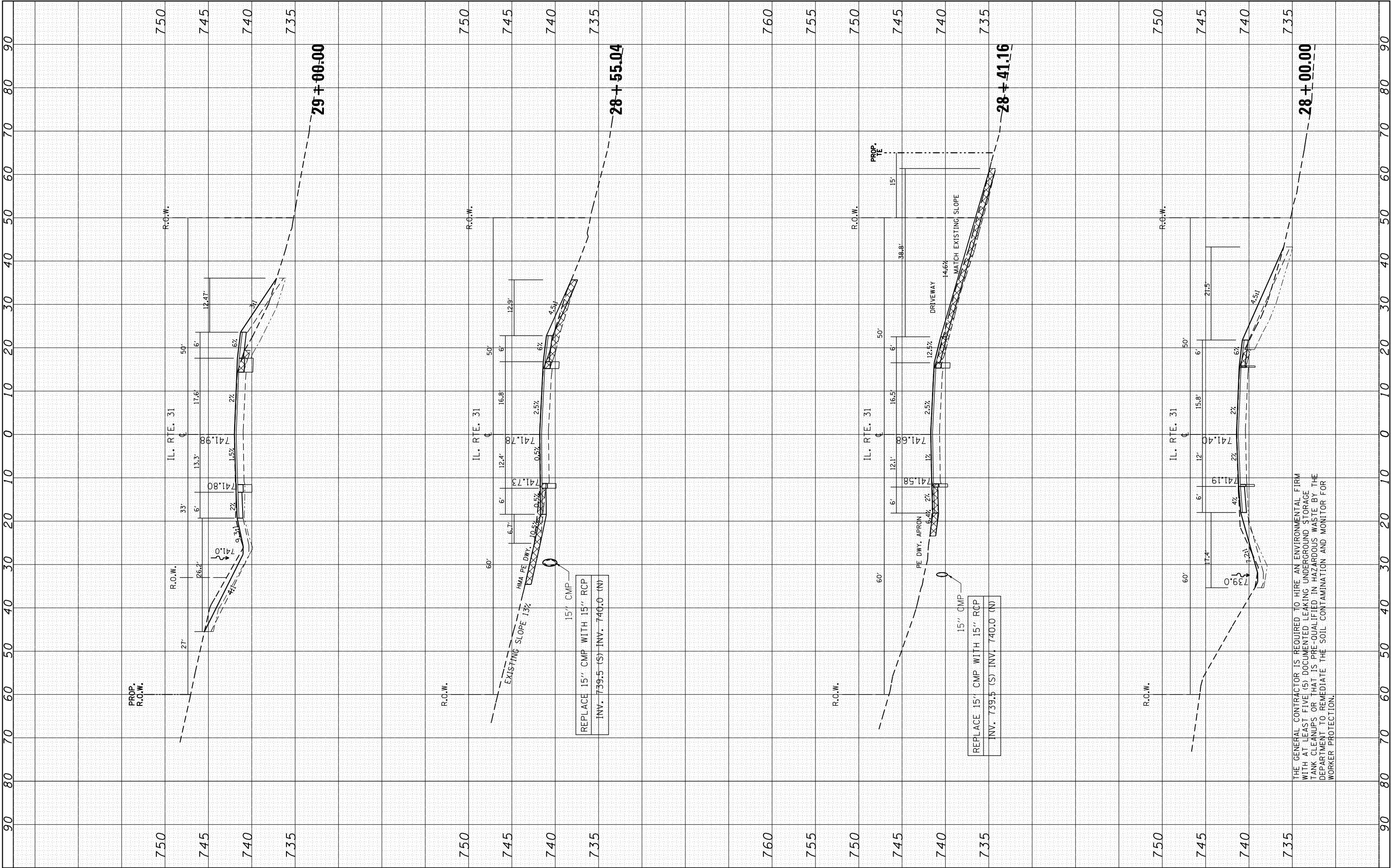
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			
CROSS SECTIONS IL. ROUTE 31			
SCALE:	SHEET	OF	SHEETS
STA. 26+00.00		TO STA. 27+26.37	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	H-N	KANE	86	73
CONTRACT NO. 60R28				
ILLINOIS FED. AID PROJECT				

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FINAL SURVEY NO.	SURVEYED PLOTTED NOTE BOOK AREAS CHECKED	BY	DATE

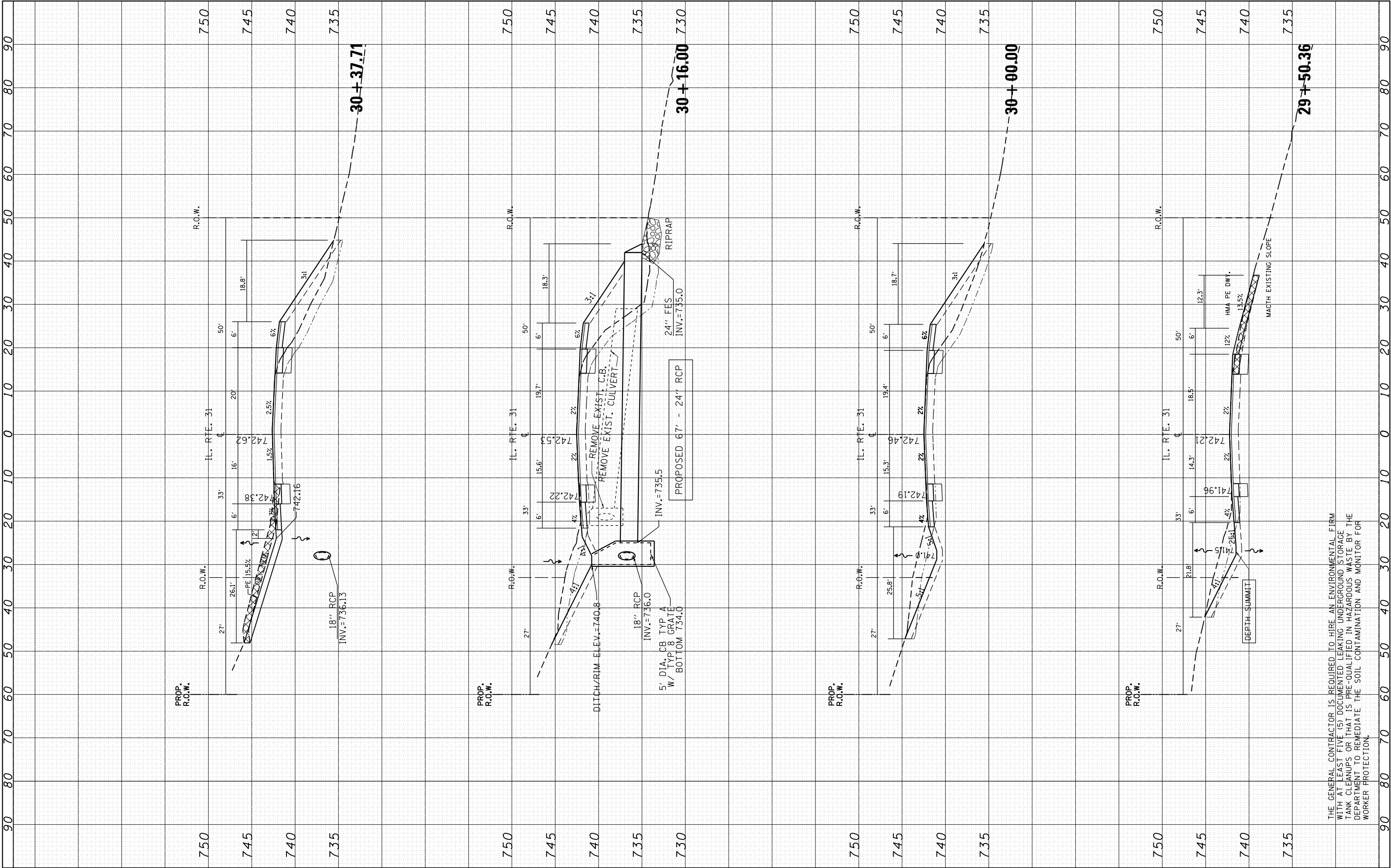
ORIGINAL SURVEY NO.	SURVEYED PLOTTED NOTE BOOK AREAS CHECKED	BY	DATE



FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\IL084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CADD\Drawings\PI44709-shr-xssht-1131.dgn					3887	H-N	KANE	86	74					
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28									
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -											
				SCALE:	SHEET	OF	SHEETS	STA. 28+00.00	TO STA. 29+00.00	ILLINOIS FED. AID PROJECT				

FINL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

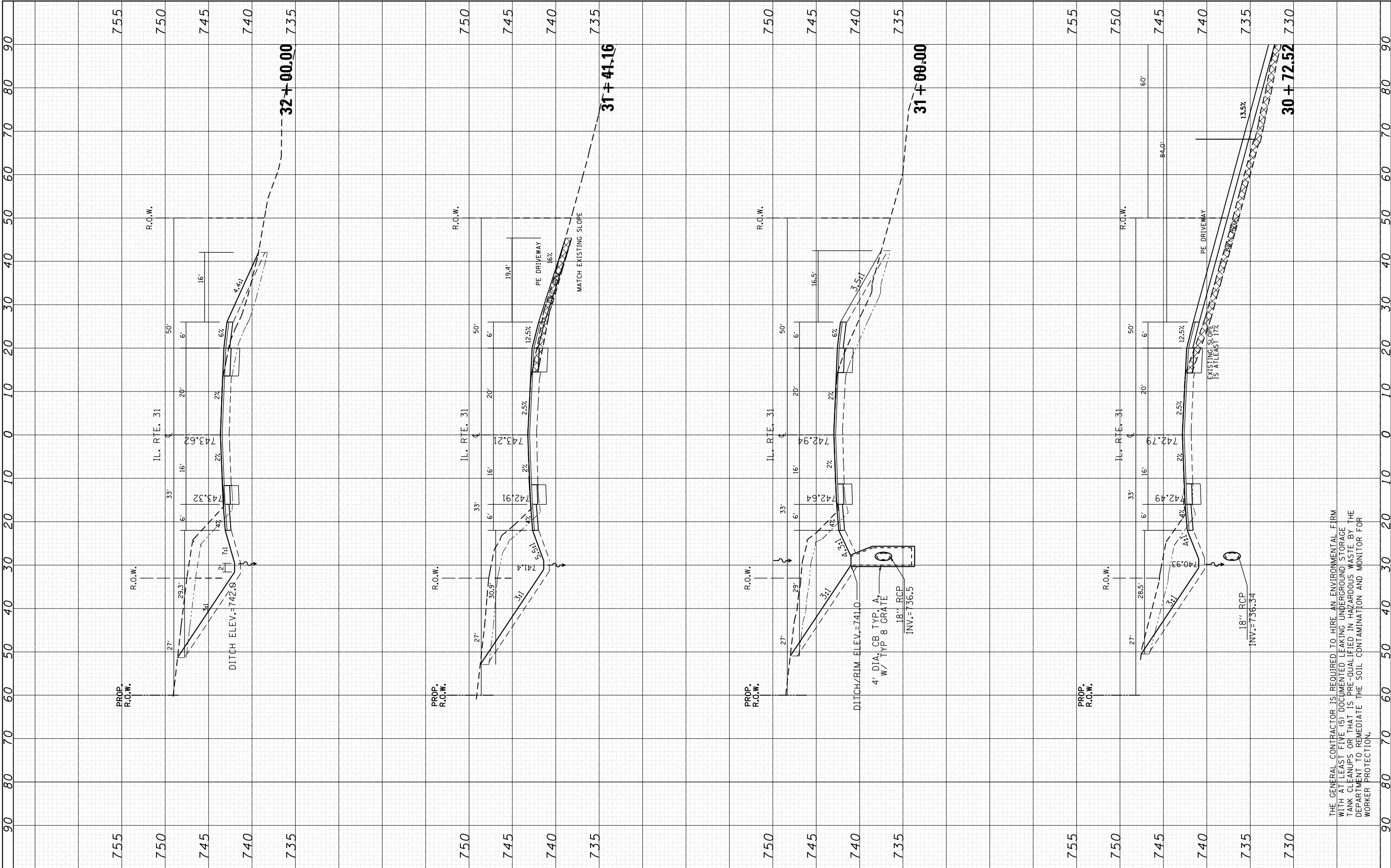


FILE NAME =	USER NAME = Bilgromiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 20.0000' / in.		DATE -	REVISED -											CONTRACT NO. 60R28		
Default	PLOT DATE = 7/14/2015													ILLINOIS FED. AID PROJECT		
				SCALE:				SHEET OF SHEETS				STA. 29+50.36 TO STA. 30+37.71				

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FINL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

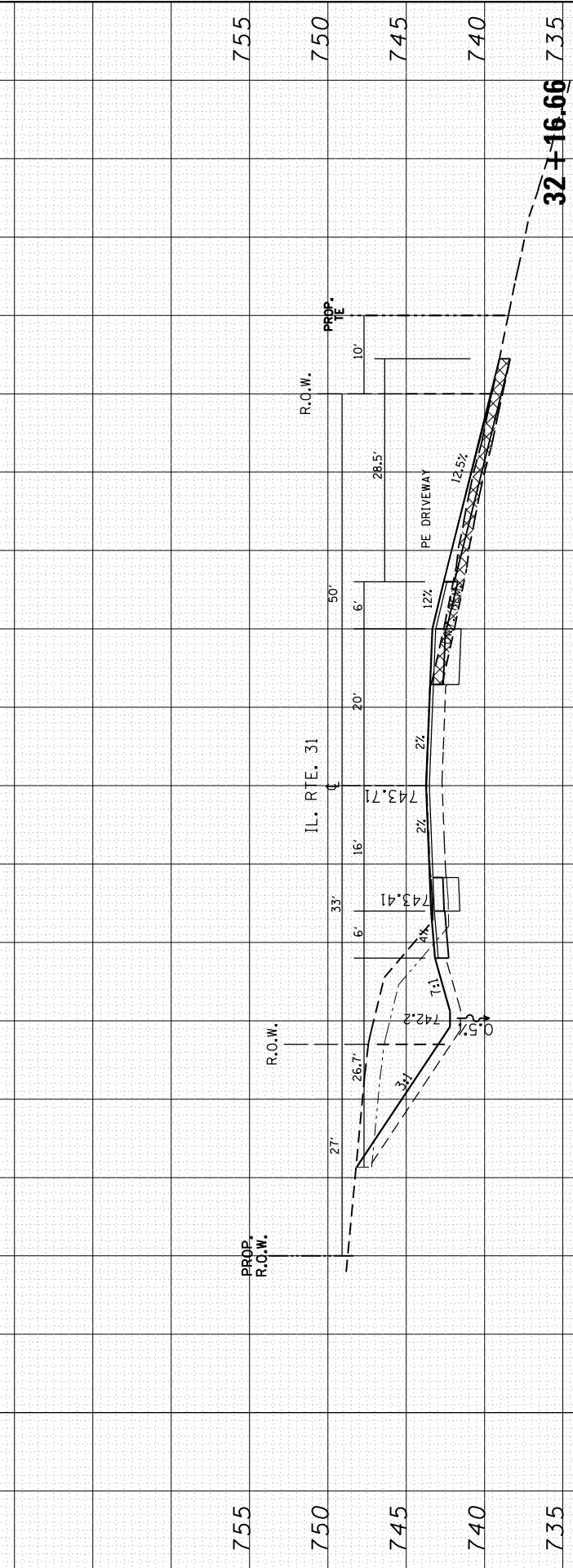
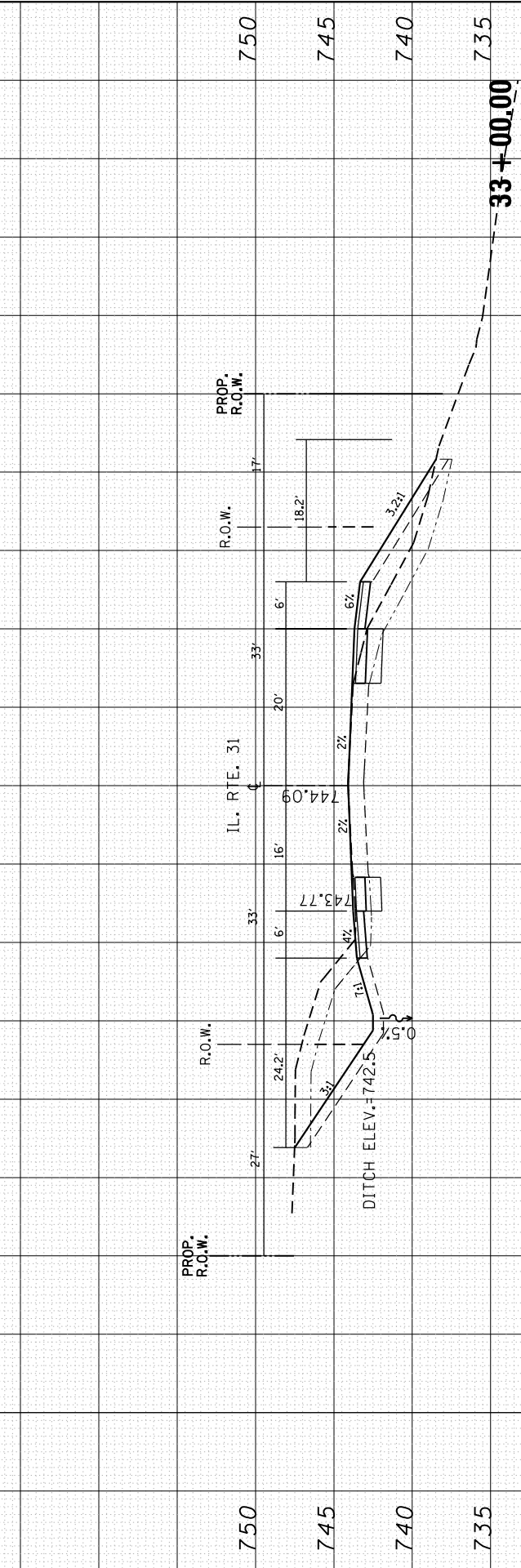
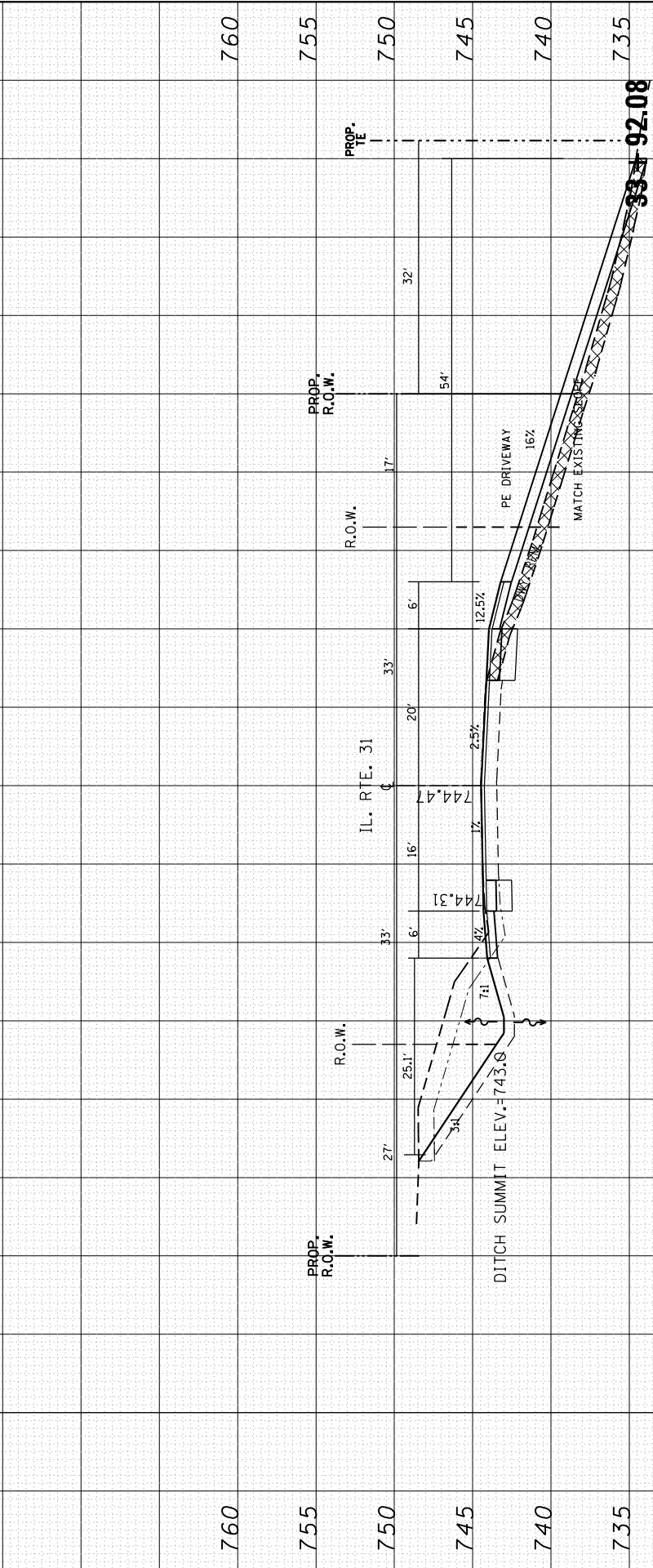
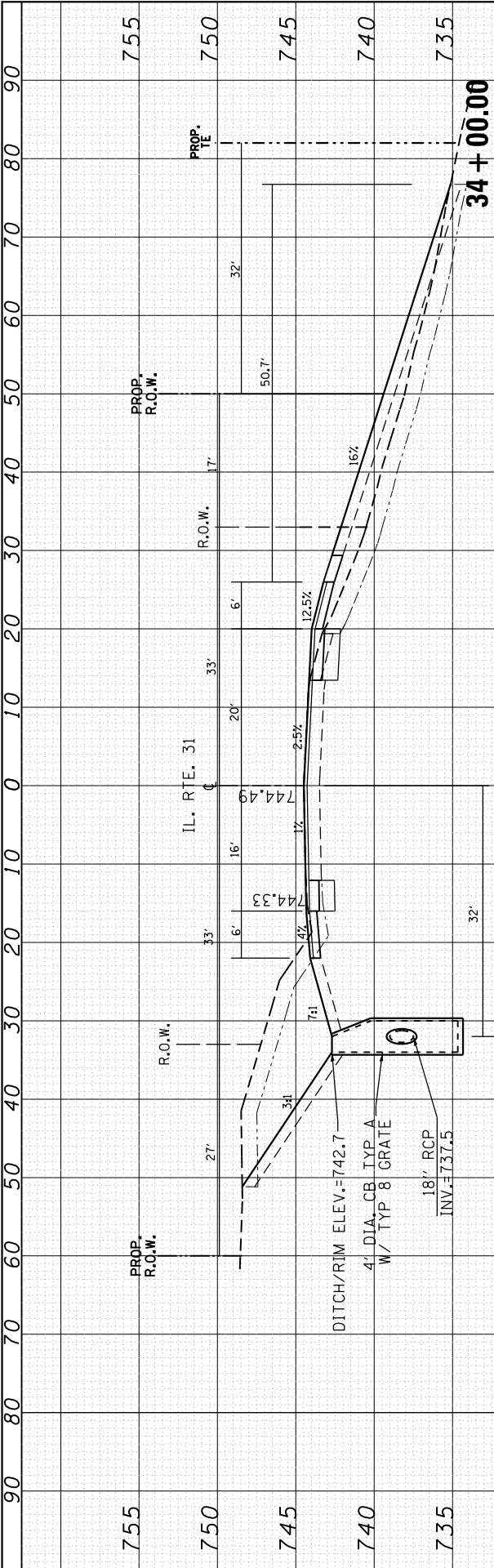


FILE NAME =	USER NAME = Bilgromiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CA000000\Design\RI44709-shr-xssht-1131.dgn		REVISED -	REVISED -		3887	H-N	KANE	86	76				
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28								
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE:	SHEET	OF	SHEETS	STA. 30+72.52	TO STA. 32+00.00			

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FINL SURVEY NO.	NO.	NOTE BOOK	NO.	BY	DATE

ORIGINAL SURVEY NO.	NO.	NOTE BOOK	NO.	BY	DATE

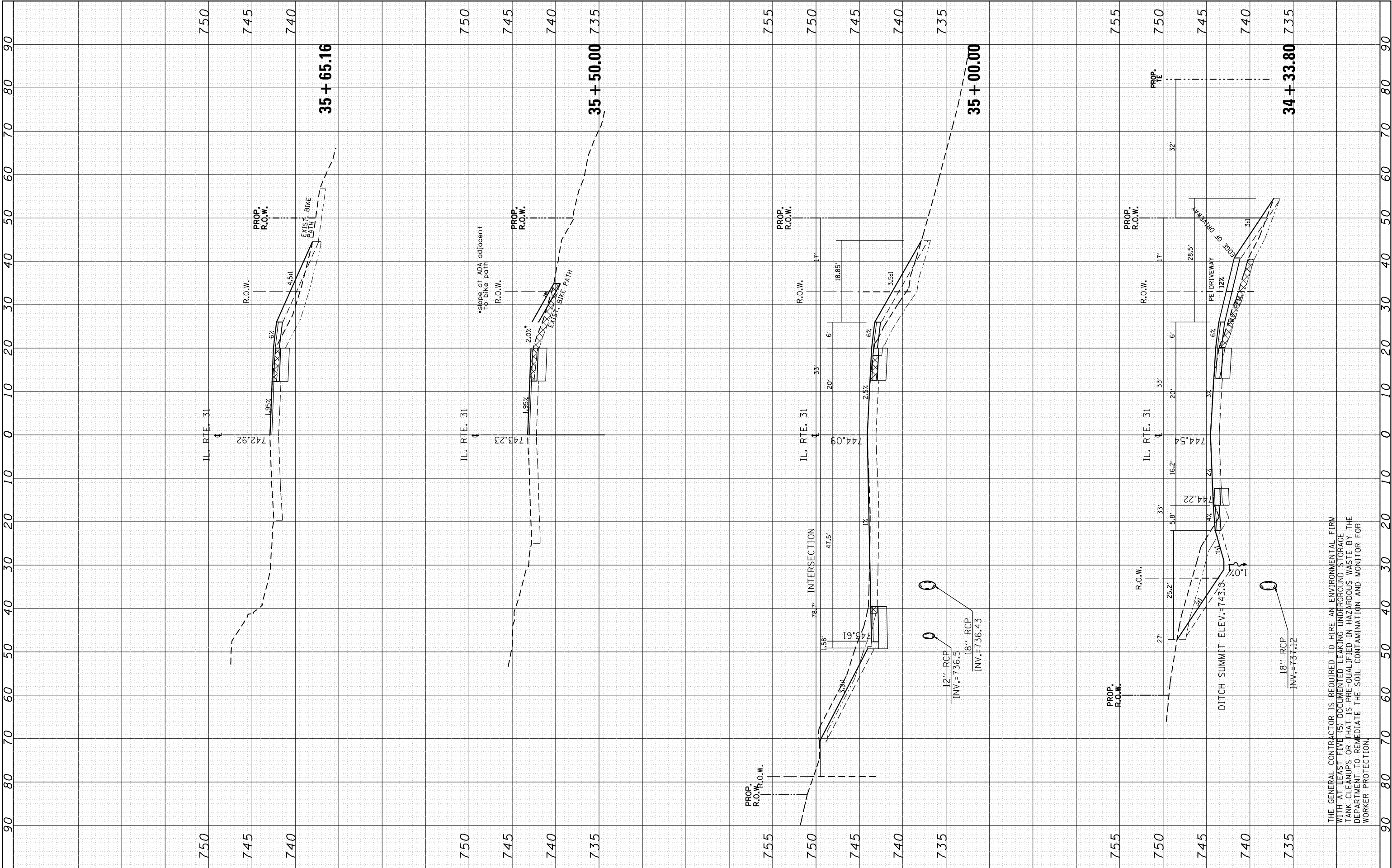


THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FILE NAME =		USER NAME = Bilgromiso		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\1\084EBIDINTEG\Illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\P144709\CAPO Design\PI44709-shr-xssht-1131.dgn		PLOT SCALE = 20.0000' / in.		CHECKED -		REVISED -			3887	H-N	KANE	86	77						
Default		PLOT DATE = 7/14/2015		DATE -		REVISED -			SCALE:				SHEET	OF	SHEETS	STA. 32+16.66	TO STA. 34+00.00	CONTRACT NO. 60R28	
															ILLINOIS		FED. AID PROJECT		

FINL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

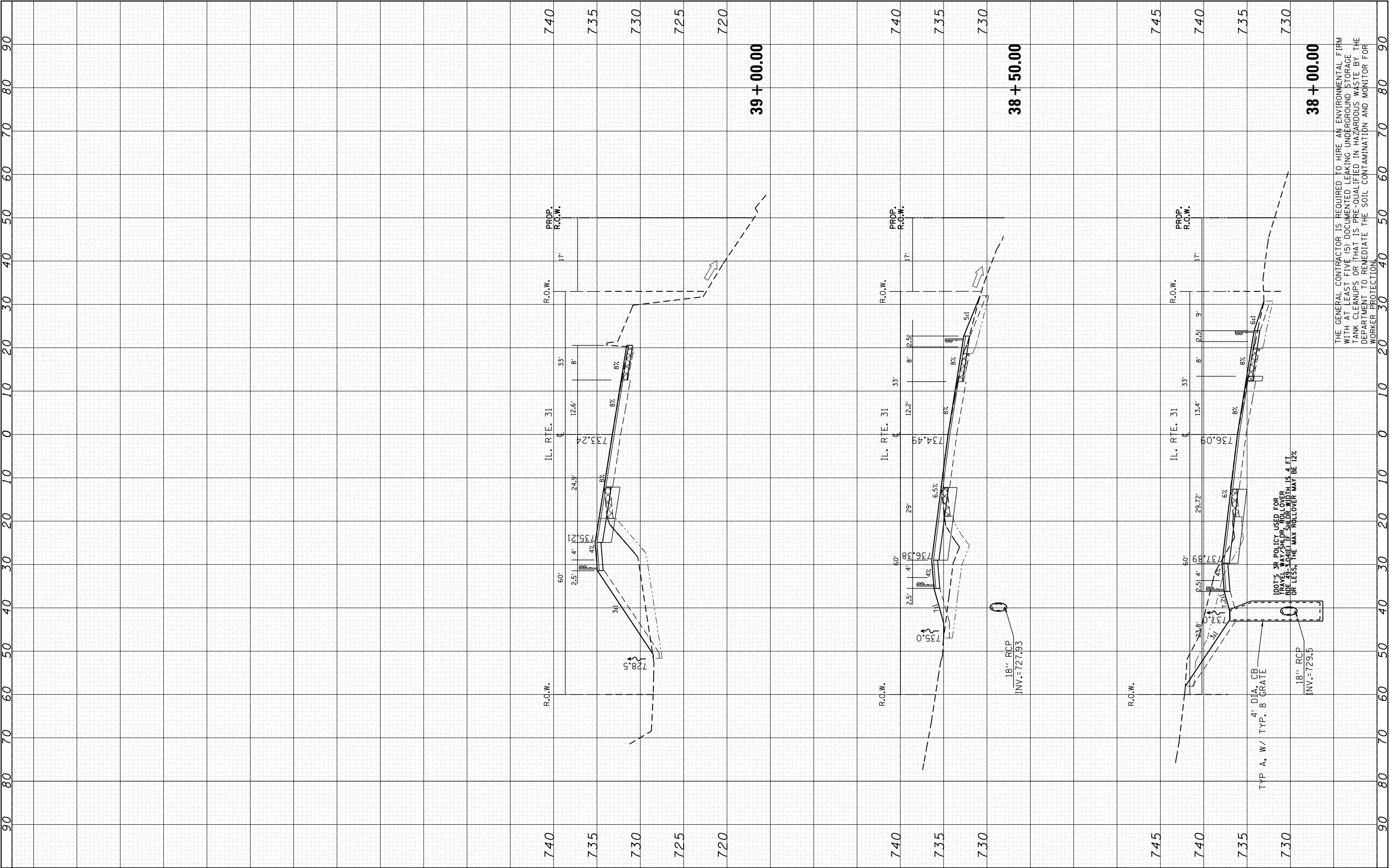


FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11\084EBIDINTEG\Illinois.gov\PIDOT\Documents	DOT Offices\District 1\Projects\P144709\CA000000\Design\PI44709-shr-xssht-1131.dgn	DESIGNED -	REVISED -		3887	H-N	KANE	86	78					
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28									
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 34+33.80	TO STA. 35+65.16	ILLINOIS FED. AID PROJECT			

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION

FINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

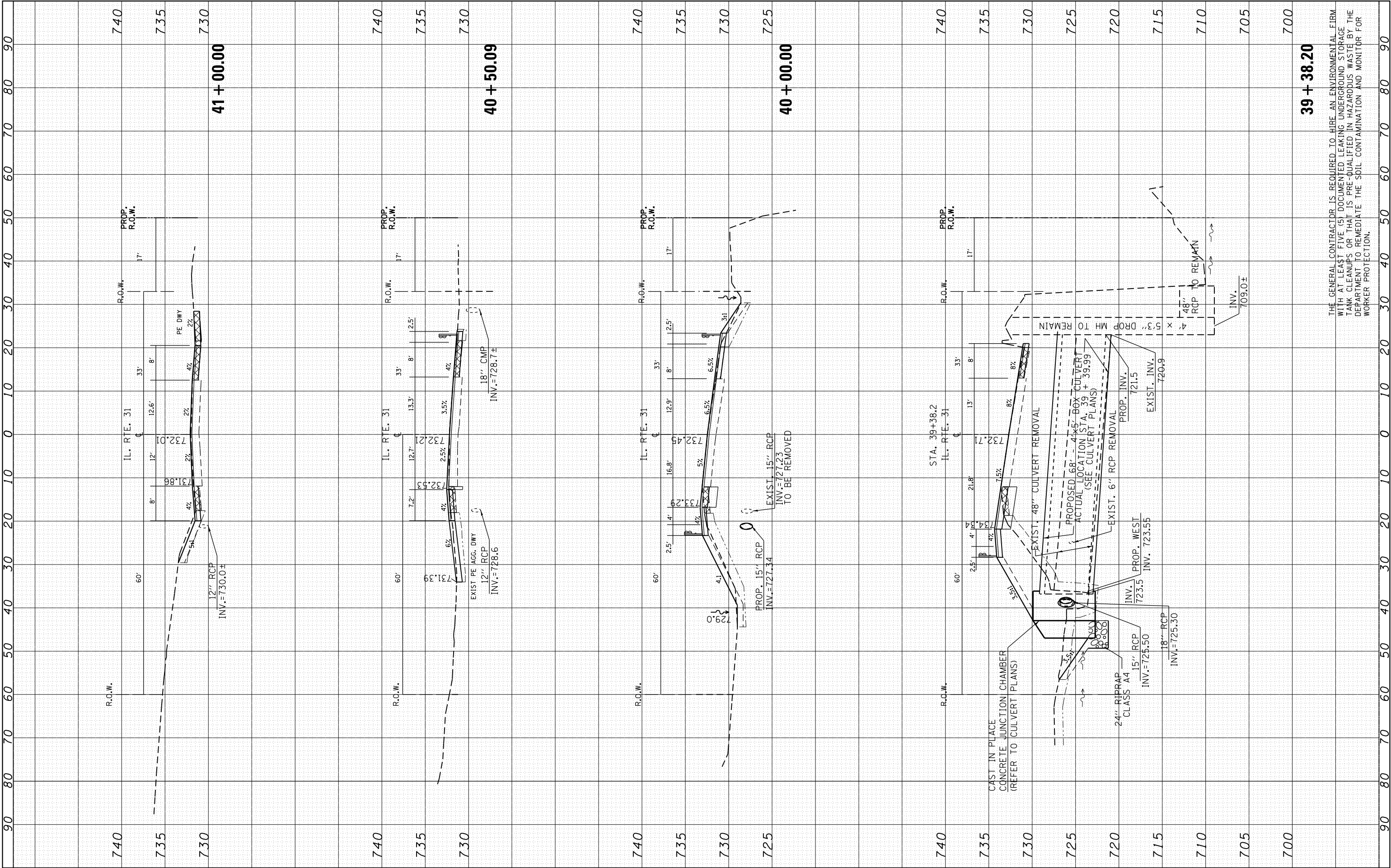


FILE NAME =		USER NAME = Bilgramiso		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.		SECTION		COUNTY		TOTAL SHEETS		SHEET NO.	
p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CADD\Drawings\P144709-shr-xssht-1131.dgn				DRAWN		REVISED -			3887		H-N		KANE		86		80					
PLOT SCALE = 20.0000' / in.				CHECKED -		REVISED -											CONTRACT NO. 60R28					
Default		PLOT DATE = 7/14/2015		DATE -		REVISED -			SCALE:		SHEET		OF		SHEETS		STA. 38+00.00		TO STA. 39+00.00			

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION

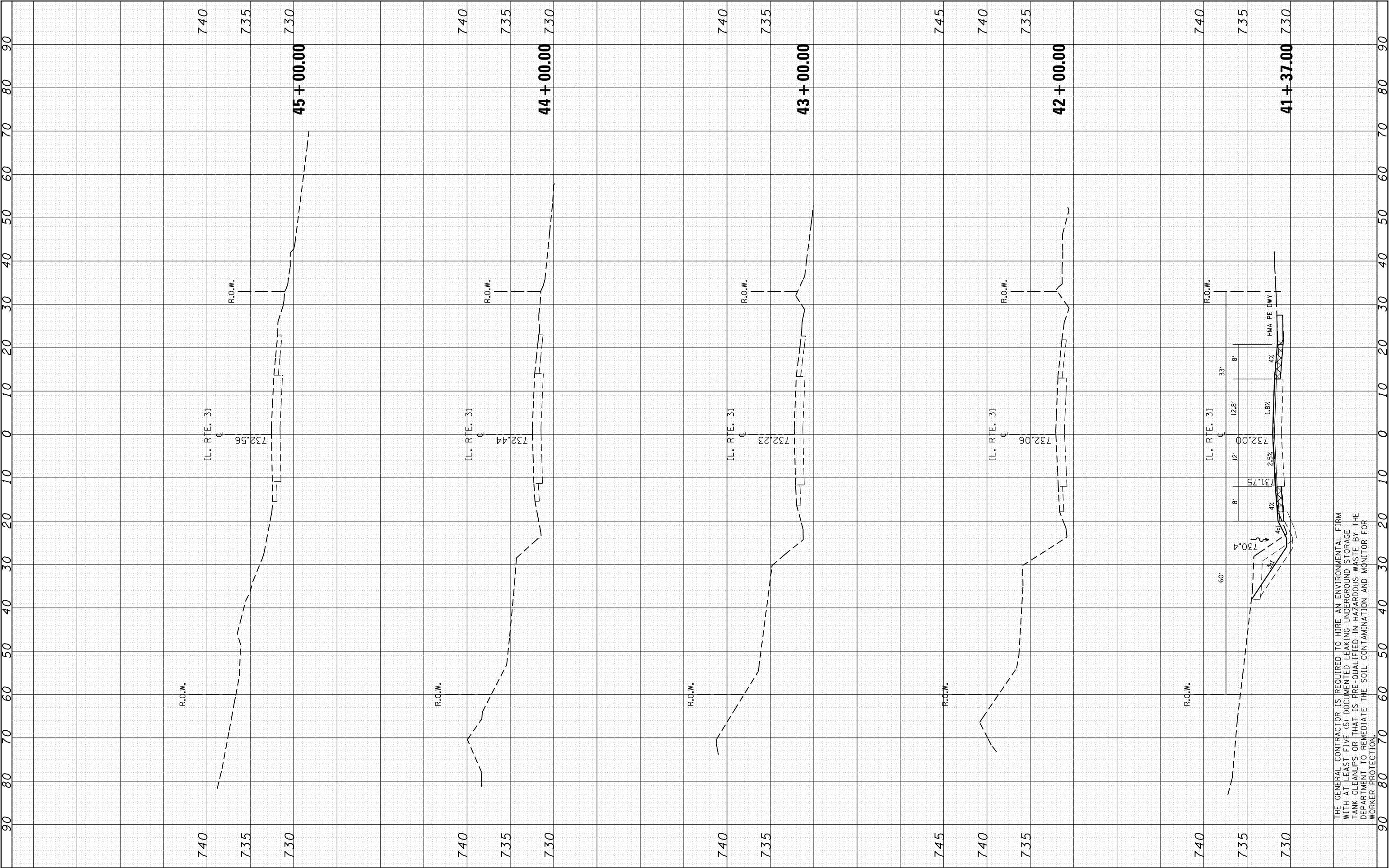
FINL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FINL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

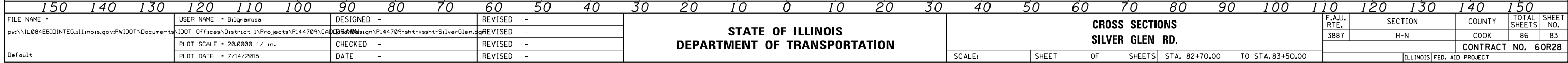
ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FILE NAME =	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS IL. ROUTE 31				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CADD\Drawn\144709-shr-xssht-1131.dgn		REVISED -	REVISED -		3887	H-N	KANE	86	82				
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28								
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 41+37.00	TO STA. 45+00.00	ILLINOIS FED. AID PROJECT		

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

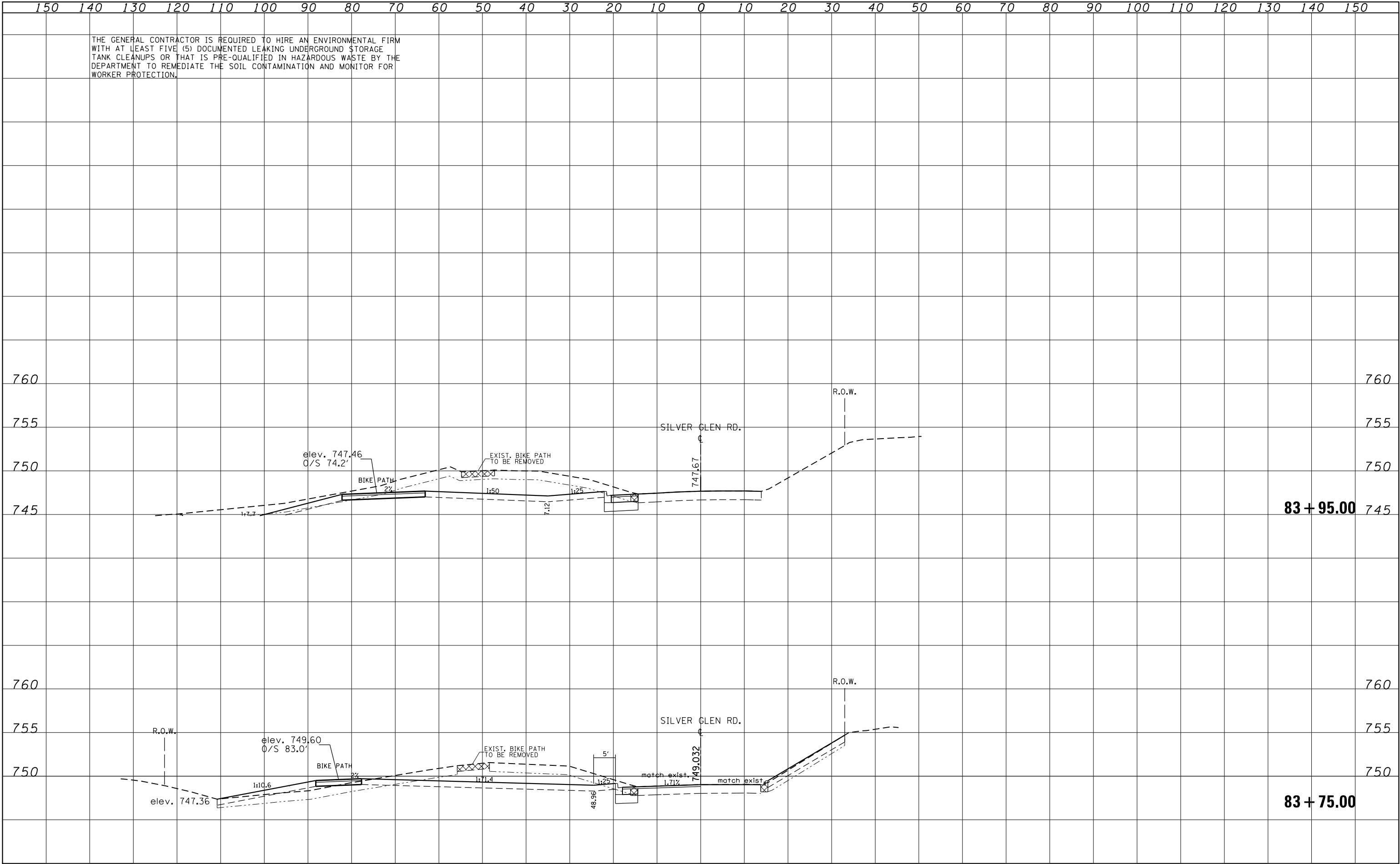
ORIGINAL		BY	DATE
SURVEY	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

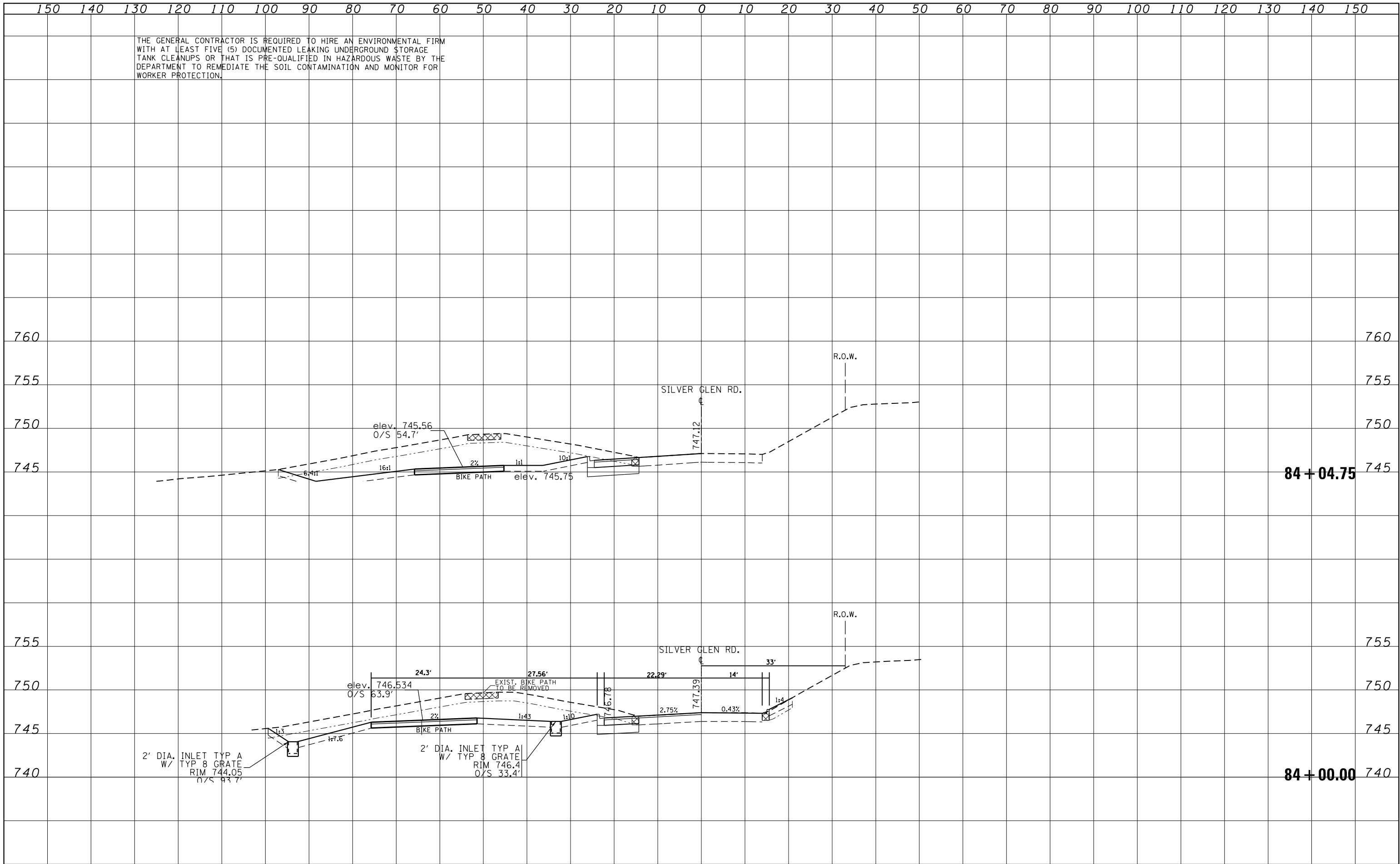


FILE NAME =	USER NAME = Bilgrami	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				CROSS SECTIONS SILVER GLEN RD.				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11084EBIDINTEG\Illinois.gov\PIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CADD\Drawings\144709-shr-xssht-SilverGlen.dgn		CHECKED -	REVISED -									3887	H-N	COOK	86	84
Default	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -												CONTRACT NO. 60R28	
	PLOT DATE = 7/14/2015	DATE -	REVISED -									SCALE:	SHEET	OF	SHEETS	STA. 83+75.00 TO STA. 83+95.00

ILLINOIS FED. AID PROJECT

FINAL SURVEY		BY	DATE
NOTE BOOK			
	SURVEYED _____		
	PLOTTED _____		
	TEMPLATE _____		
	AREAS _____		
NO. _____	AREAS CHECKED _____		

ORIGINAL SURVEY				BY		DATE
NOTE BOOK						
SURVEYED _____						
PLOTTED _____						
TEMPLATE _____						
AREAS _____						
NO. _____						
AREAS CHECKED _____						

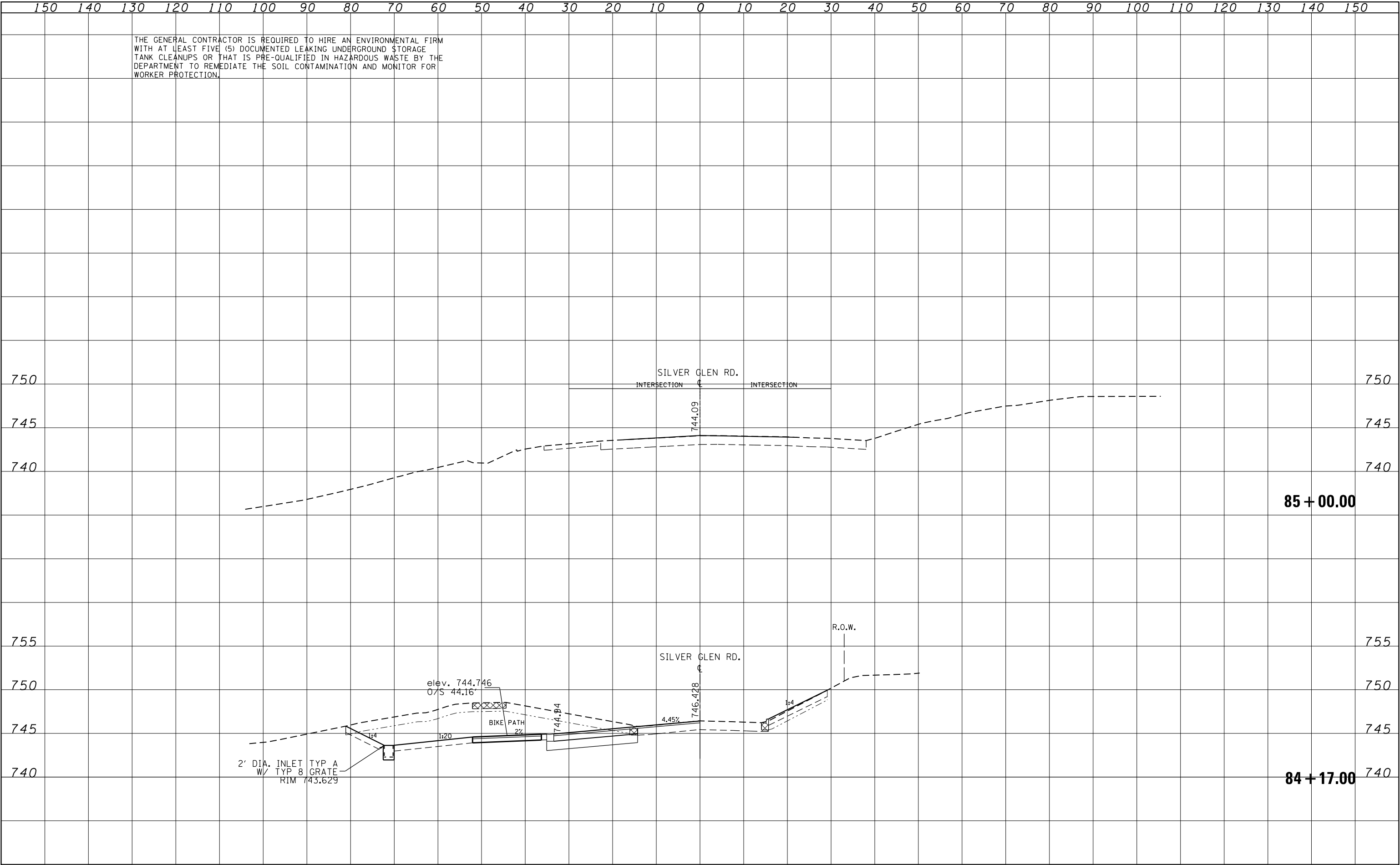


FILE NAME =	USER NAME = Bilgramise	DESIGNED -	REVISED -	<div> <div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div> </div>	CROSS SECTIONS					F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
px\\11084EB\ID\INTEG\Illinois.gov\PIDOT\Documents\PIDOT Offices\District 1\Projects\PI44709\CADD\Drawings\PI44709-sht-xxsht-Silver Glen.dwg					SILVER GLEN RD.					3887	H-N	COOK	86	85
PLOT SCALE = 20.0000' / in.		CHECKED -	REVISED -							CONTRACT NO. 60R28				
Default	PLOT DATE = 7/14/2015	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. 84+00.00 TO STA. 84+04.75					ILLINOIS FED. AID PROJECT				

THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

FINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NOTE BOOK NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FILE NAME = p:\11\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\NIDOT Offices\District 1\Projects\P144709\CADD\Drawings\144709-shr-xssht-SilverGlen.dwg Default	USER NAME = Bilgramiso	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS SILVER GLEN RD.				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	NIDOT Offices\District 1\Projects\P144709\CADD\Drawings\144709-shr-xssht-SilverGlen.dwg	DESIGNED -	REVISED -		3887	H-N	COOK	86	86				
	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60R28								
	PLOT DATE = 7/14/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
	SCALE: SHEET OF SHEETS STA. 84+17.00 TO STA. 85+00.00												