

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	1
		ILLINOIS	CONTRACT NO. 62L67	

D-91-488-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2

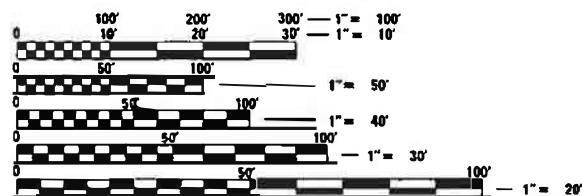
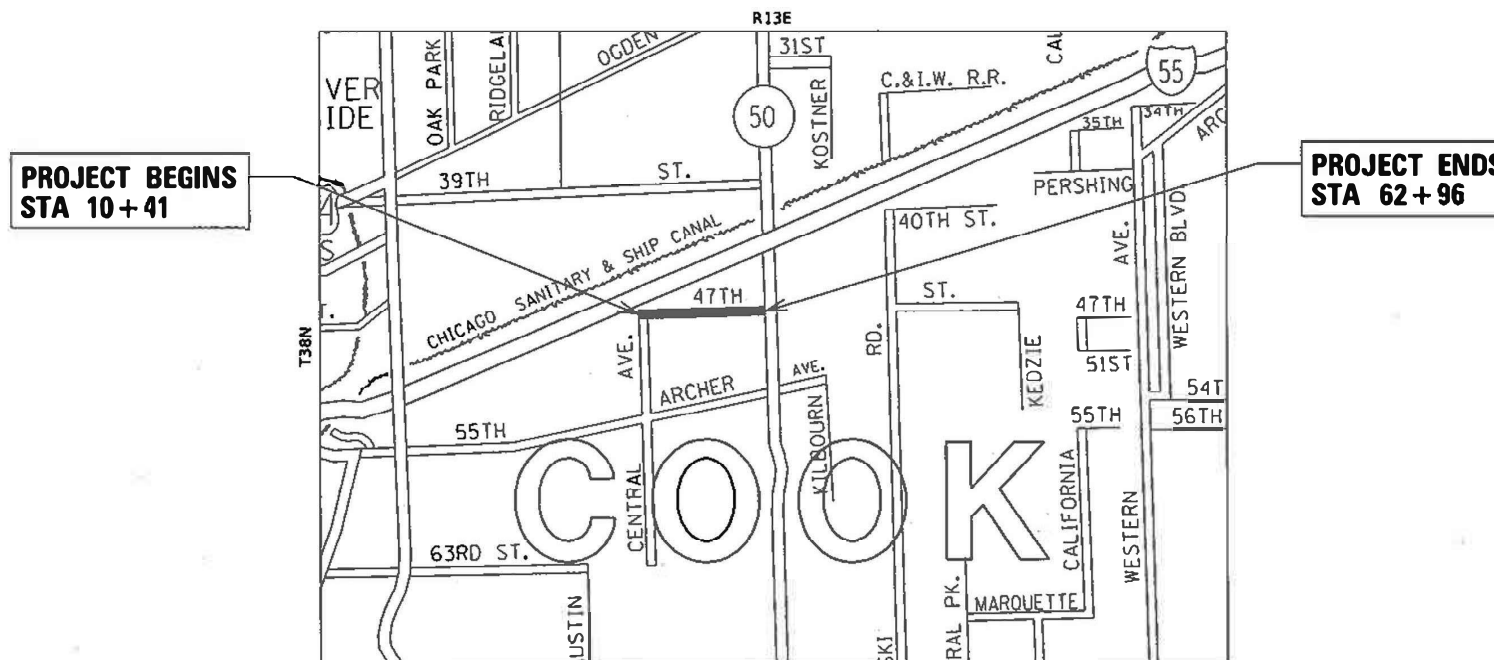
THIS IMPROVEMENT IS LOCATED
IN THE CITY OF CHICAGO & VILLAGE OF STICKNEY

TRAFFIC DATA
EXISTING ADT = 12600 (2018)
POSTED SPEED LIMIT = 30 MPH

PROPOSED HIGHWAY PLANS

O.R. ROUTE 162 - 47TH STREET
FROM CENTRAL AVE TO IL-50 (CICERO AVE)
SECTION 2020-089-RS&SW
STANDARD OVERLAY, ADA IMPROVEMENTS
COOK COUNTY

C-91-286-20



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N. - CHICAGO UTILITY ALERT NETWORK
312-744-7000
J.U.L.I.E. - JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123 OR 811

PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 62L67

STICKNEY & LAKE TOWNSHIPS

GROSS LENGTH = 5255.00 FT. = 0.995 MILE
NET LENGTH = 5255.00 FT. = 0.995 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED June 23 20 21
Joe Pires REGIONAL ENGINEER

August 13, 2021
Joe Pires ENGINEER OF DESIGN AND ENVIRONMENT

August 13, 2021
Joe Pires DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

STATE STANDARDS

INDEX OF SHEETS

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24	PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-01)
25	PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS (PD-03)
26	PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH TURNING SPACE (PD-04)

STANDARD NO.

DESCRIPTION

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PREPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701427-05	LANE CLOSURES, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS <40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." AT (312) 744-7000, J.U.L.I.E. AT 1-800-89-0123, OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE ENGINEER SHALL CONTACT AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.
- THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

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REV-SEP

USER NAME = midyja DESIGNED - DRAWN - CHECKED - DATE -	REVISIONS REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES				O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.	162	2020-089-R5&SW	COOK
							ILLINOIS FED. AID PROJECT CONTRACT NO. 62L67				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE				
X4400501	COMBINATION CURB AND GUTTER	FOOT	300	300				
	REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET							
20200100	EARTH EXCAVATION	CU YD	57	57				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	231	231				
25200110	SODDING, SALT TOLERANT	SO YD	231	231				
25200200	SUPPLEMENTAL WATERING	UNIT	3	3				
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	23	23				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17160	17160				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	41	41				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	330	330				
40600990	TEMPORARY RAMP	SO YD	160	160				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	60	60				
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-4.75, N50	TON	1424	1424				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE				
40604060	HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N50	TON	3	3				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	2492	2492				
42001300	PROTECTIVE COAT	SO YD	1182	1182				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	7882	7882				
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SO YD	25422	25422				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	23	23				
44000600	SIDEWALK REMOVAL	SO FT	7882	7882				
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/2"	SO YD	613	613				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	248	248				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	230	230				
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	135	135				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	3	3				
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1				

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
47TH STREET - CENTRAL AVE TO IL-50 (CICERO AVE)**

SCALE: SHEET NO. 1 OF 3 SHEETS STA. TO STA.

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-R5&SW	COOK	26	3
CONTRACT NO. 62L67				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

* = SPECIALTY ITEMS Δ = NON-PARTICIPATING ITEMS

REV-SEP

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE				
60262700	INLETS TO BE RECONSTRUCTED	EACH	1	1				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	13	13				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	8	8				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 100% STATE				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	11469	11469				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	1275	1275				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	205	205				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6960	6960				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1826	1826				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1067	1067				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3238	3238				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	205	205				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6960	6960				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1826	1826				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1067	1067				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	238	238				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	238	238				

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

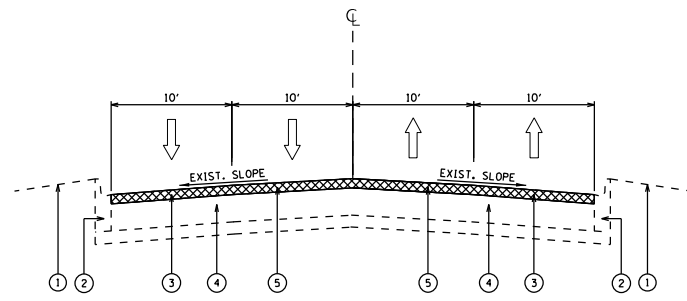
**SUMMARY OF QUANTITIES
47TH STREET - CENTRAL AVE TO IL-50 (CICERO AVE)**

SCALE: SHEET NO. 2 OF 3 SHEETS STA. TO STA.

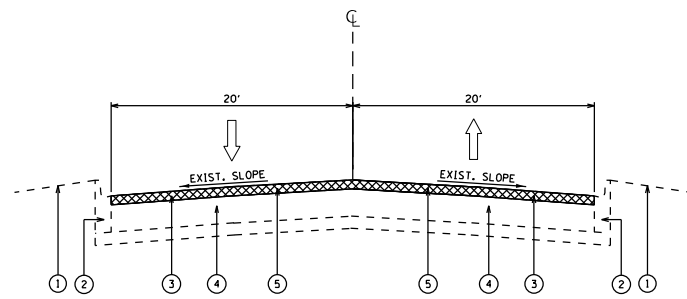
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

REV-SEP

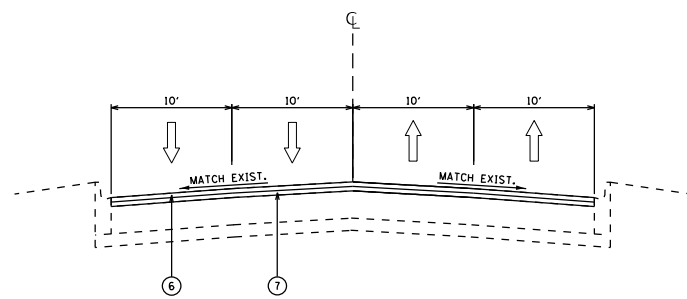
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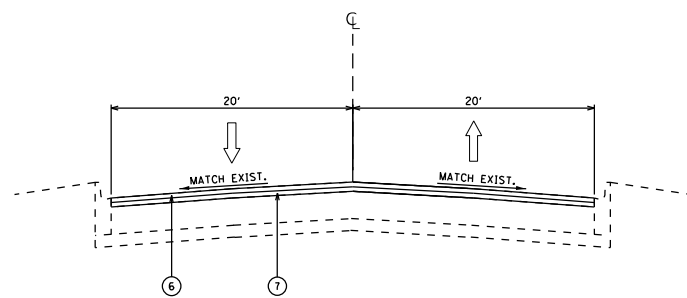
**47TH STREET – CENTRAL AVE TO LARAMIE AVE
STA 12+18 TO STA 36+40
EXISTING TYPICAL SECTION**



**47TH STREET – LARAMIE AVE TO LACROSSE AVE
STA 36+40 TO STA 60+35
EXISTING TYPICAL SECTION**



**47TH STREET – CENTRAL AVE TO LARAMIE AVE
STA 12+18 TO STA 36+40
PROPOSED TYPICAL SECTION**



**47TH STREET – LARAMIE AVE TO LACROSSE AVE
STA 36+40 TO STA 60+35
PROPOSED TYPICAL SECTION**

LEGEND

- ① EXISTING SIDEWALK
- ② EXISTING COMB. CONC. CURB AND GUTTER, B-6.12
- ③ PROPOSED HMA SURFACE REMOVAL, 2 3/4"
- ④ EXISTING P.C.C. BASE COURSE, ±10"
- ⑤ EXISTING H.M.A. SURFACE COURSE, ±2 3/4"
- ⑥ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1 3/4"
- ⑦ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; THICKNESS VARIES 3/4" TO 1"

NOTES

1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
2. THE COST OF MILLING OVER THE GUTTER IS INCLUDED IN THE COST OF SURFACE REMOVAL.
3. THE COST OF C&G REMOVAL SHALL INCLUDE THE COST OF HMA OVERLAY REMOVAL OVER THE GUTTER.
4. THE COST OF TEMPORARY RAMPS SHALL INCLUDE THE REMOVAL OF TEMPORARY RAMPS.
5. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER COURSE IL-4.75, N50.
6. CITY OF CHICAGO LIMIT BEGINS AT STA 36+40 AND STRUCTURES ADJUSTMENT (SPECIAL) ARE NOT ALLOWED.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	3.5% @ 80 GYR	OCP
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	3.5% @ 50 GYR	QC/OA
HMA DRIVEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 2"	4.0% @ 50 GYR	QC/OA
	HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19.0mm) 6"	4.0% @ 50 GYR	QC/OA
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm)	4% @ 70 GYR	QC/OA
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19.0 mm)	4% @ 70 GYR	QC/OA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP); PAY FOR PERFORMANCE (PFP)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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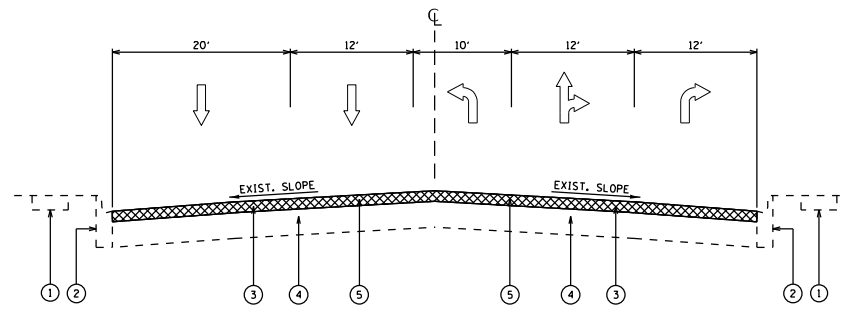
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PLOT DATE = 6/25/2021		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

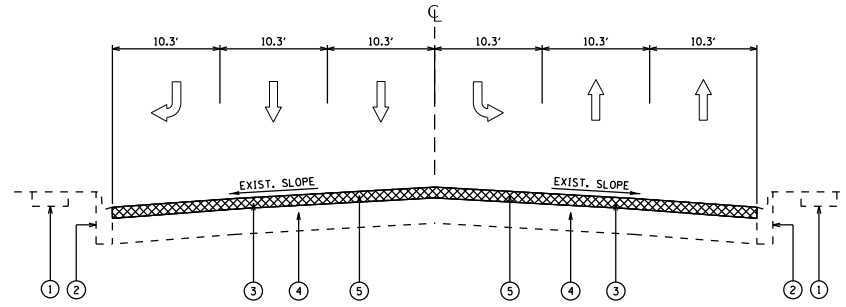
**EXISTING AND PROPOSED TYPICAL SECTIONS
47TH STREET – CENTRAL AVE TO IL-50 (CICERO AVE)**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

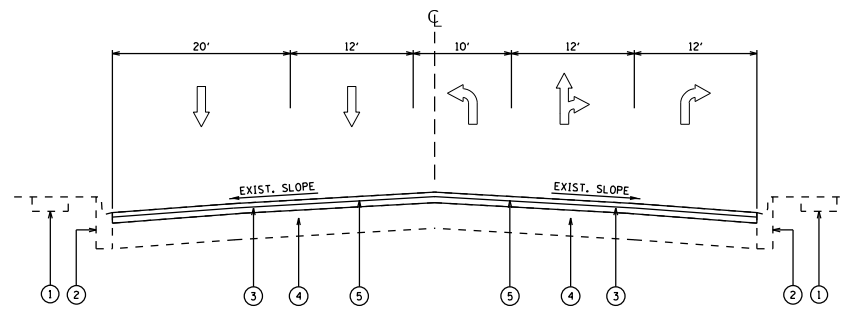
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CONTRACT NO. 62L67				
ILLINOIS		FED. AID PROJECT		



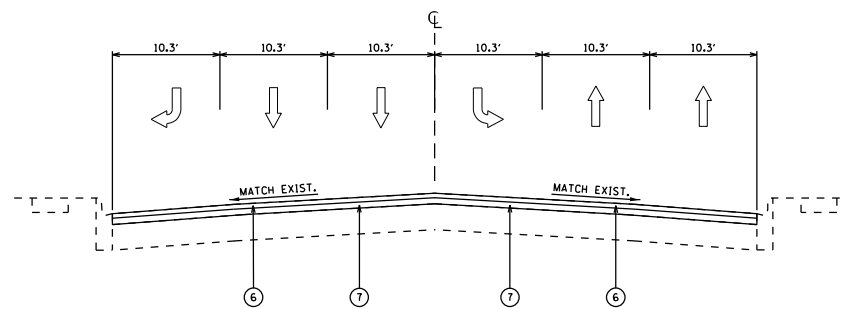
**47TH STREET AT CENTRAL AVE
STA 10+41 TO STA 13+60
EXISTING TYPICAL SECTION**



**47TH STREET AT IL-50 (CICERO AVE)
STA 60+35 TO STA 62+96
EXISTING TYPICAL SECTION**



**47TH STREET AT CENTRAL AVE
STA 10+41 TO STA 13+60
PROPOSED TYPICAL SECTION**



**47TH STREET AT IL-50 (CICERO AVE)
STA 60+35 TO STA 62+96
PROPOSED TYPICAL SECTION**

LEGEND

- ① EXISTING SIDEWALK
- ② EXISTING COMB. CONC. CURB AND GUTTER, B-6.12
- ③ PROPOSED HMA SURFACE REMOVAL, 2 3/4"
- ④ EXISTING P.C.C. BASE COURSE, ±10"
- ⑤ EXISTING H.M.A. SURFACE COURSE, ±2 3/4"
- ⑥ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80; 1 3/4"
- ⑦ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50; THICKNESS VARIES 3/4" TO 1"

NOTES

1. THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
2. THE COST OF MILLING OVER THE GUTTER IS INCLUDED IN THE COST OF SURFACE REMOVAL.
3. THE COST OF C&G REMOVAL SHALL INCLUDE THE COST OF HMA OVERLAY REMOVAL OVER THE GUTTER.
4. THE COST OF TEMPORARY RAMPS SHALL INCLUDE THE REMOVAL OF TEMPORARY RAMPS.
5. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HMA BINDER COURSE IL-4.75, N50.
6. CITY OF CHICAGO LIMIT BEGINS AT STA 36+40 AND STRUCTURES ADJUSTMENT (SPECIAL) ARE NOT ALLOWED.

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	DATE -	REVISED -

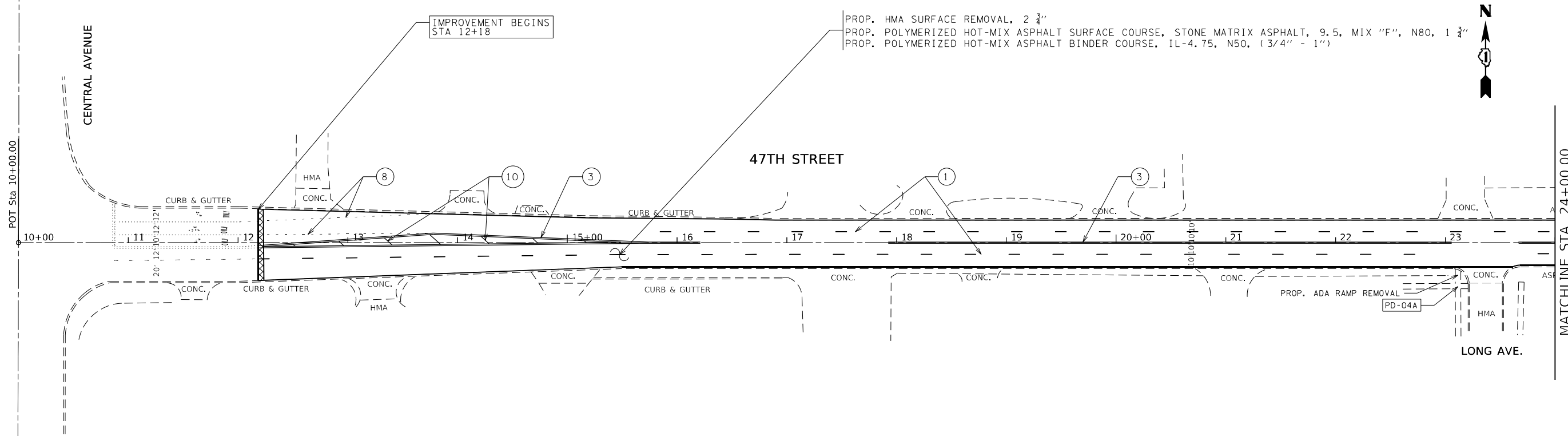
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS
47TH STREET - CENTRAL AVE TO IL-50 (CICERO AVE)**

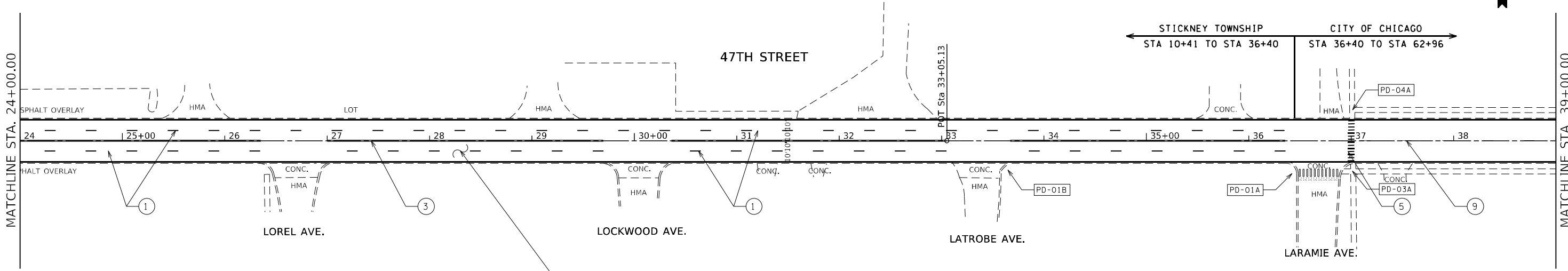
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O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-R5&SW	COOK	26	7
CONTRACT NO. 62L67				
ILLINOIS FED. AID PROJECT				

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PAVEMENT MARKING LEGEND	
①	PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6' DASH 18" SKIP, WHITE (TYP.)
②	PROP. THERMOPLASTIC PVMT MARKING LINE 6", TURN LANE, WHITE (TYP.)
③	PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 11" C-C (TYP.)
④	PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.)
⑤	PROP. THERMOPLASTIC PVMT MARKING LINE 24", CROSSWALK, WHITE (TYP.)
⑦	PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)
⑧	PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2' DASH 6" SKIP, WHITE (TYP.)
⑨	PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6' DASH 18" SKIP, YELLOW (TYP.)
⑩	PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.)



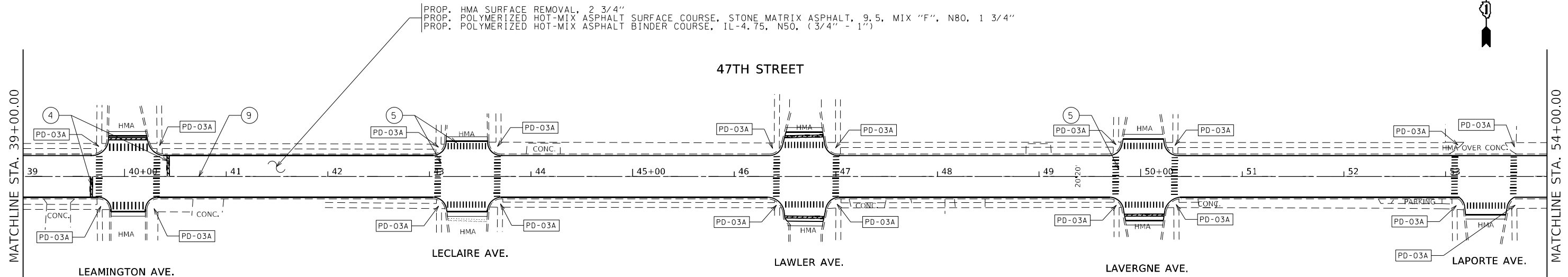
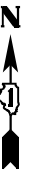
NOTE: AFTER MILLING, 2FT WIDE TEMPORARY RAMPS SHALL BE PLACES AROUND STRUCTURES WITHIN THE CITY OF CHICAGO.

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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

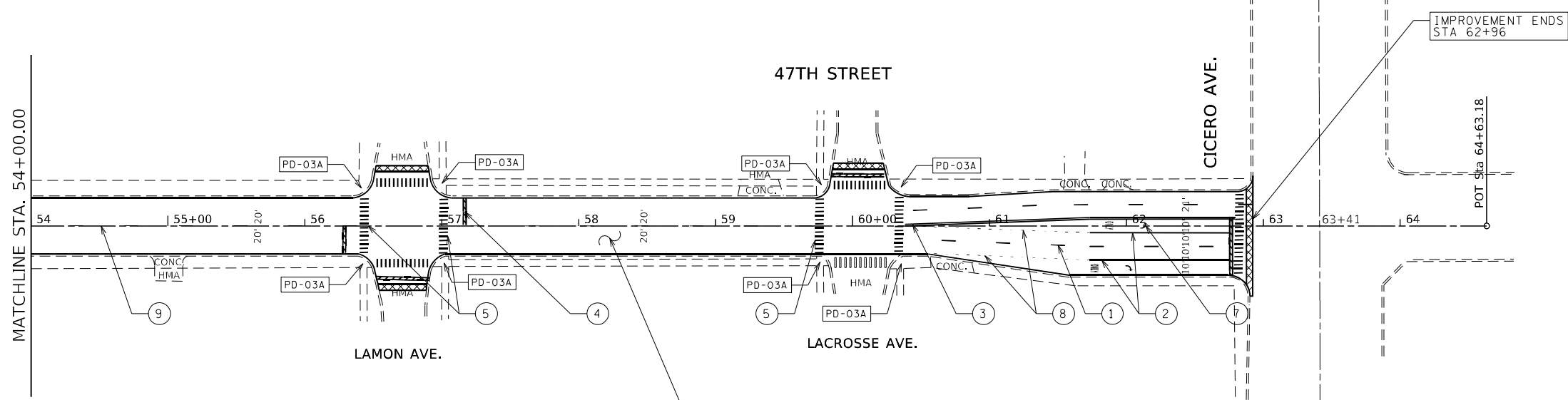
ROADWAY PLAN
47TH STREET - CENTRAL AVE TO IL-50 (CICERO AVE)
 SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. TO STA.

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-R5&SW	COOK	26	8
CONTRACT NO. 62L67				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

① PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6' DASH 18" SKIP, WHITE (TYP.)	⑦ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS, WHITE (TYP.)
② PROP. THERMOPLASTIC PVMT MARKING LINE 6", TURN LANE, WHITE (TYP.)	⑧ PROP. THERMOPLASTIC PVMT MARKING LINE 6", 2' DASH 6" SKIP, WHITE (TYP.)
③ PROP. THERMOPLASTIC PVMT MARKING LINE 4", DOUBLE YELLOW @ 11" C-C (TYP.)	⑨ PROP. THERMOPLASTIC PVMT MARKING LINE 4", 6' DASH 18" SKIP, YELLOW (TYP.)
④ PROP. THERMOPLASTIC PVMT MARKING LINE 24", STOPBAR, WHITE (TYP.)	⑩ PROP. THERMOPLASTIC PVMT MARKING LINE 12", DIAGONALS, YELLOW (TYP.)
⑤ PROP. THERMOPLASTIC PVMT MARKING LINE 24", CROSSWALK, WHITE (TYP.)	



NOTE: AFTER MILLING, 2FT WIDE TEMPORARY RAMPS SHALL BE PLACES AROUND STRUCTURES WITHIN THE CITY OF CHICAGO.

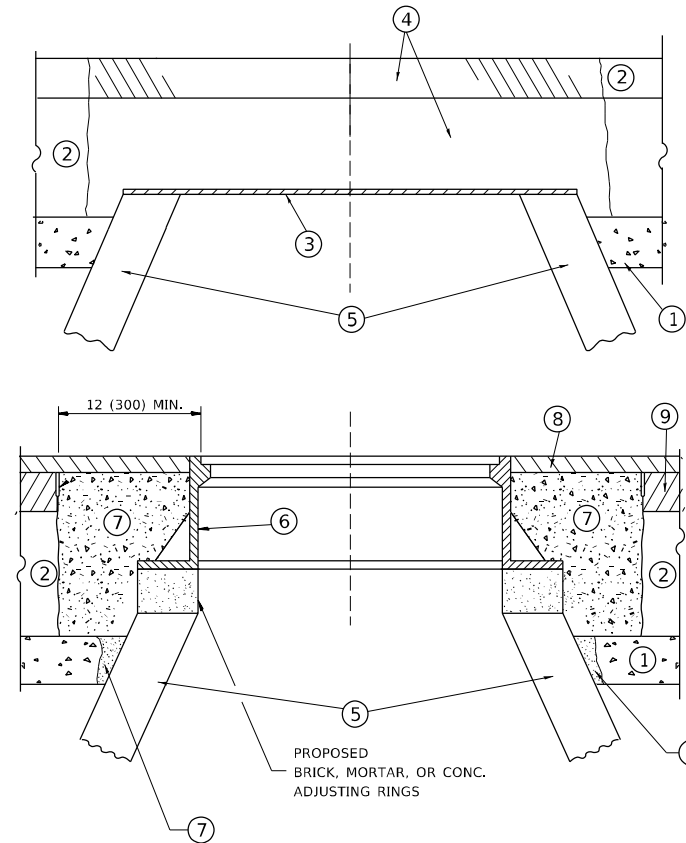
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DRAWN -	REVISOR -	REVISOR -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 7/2/2021	DATE -	REVISOR -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN	
47TH STREET - CENTRAL AVE TO IL-50 (CICERO AVE)	
SCALE: 1"=50'	SHEET 2 OF 2 SHEETS STA. TO STA.

O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-R5&SW	COOK	26	9
CONTRACT NO. 62L67				
ILLINOIS FED. AID PROJECT				



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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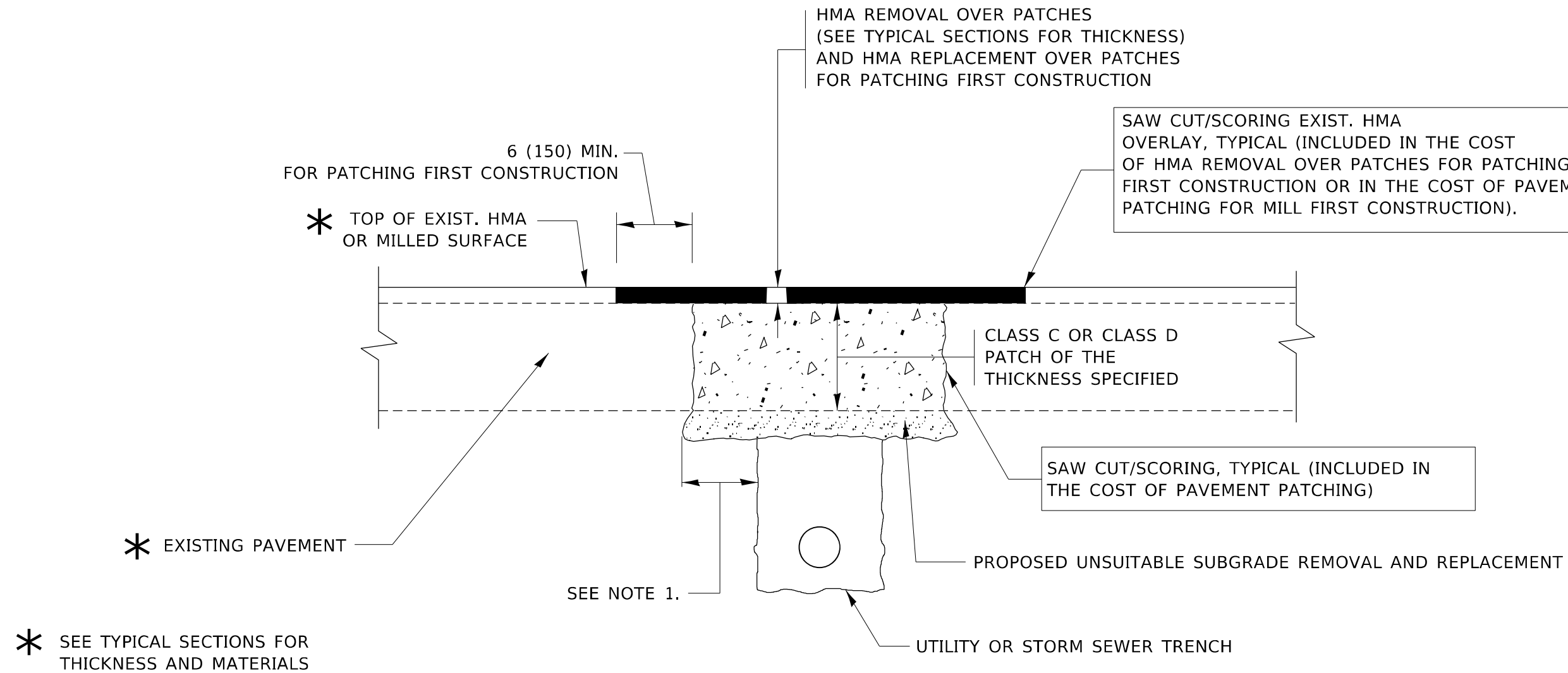
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PLOT DATE = 6/25/2021	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	10
BD600-03 (BD-8)		CONTRACT NO.	62L67	
ILLINOIS		FED. AID PROJECT		



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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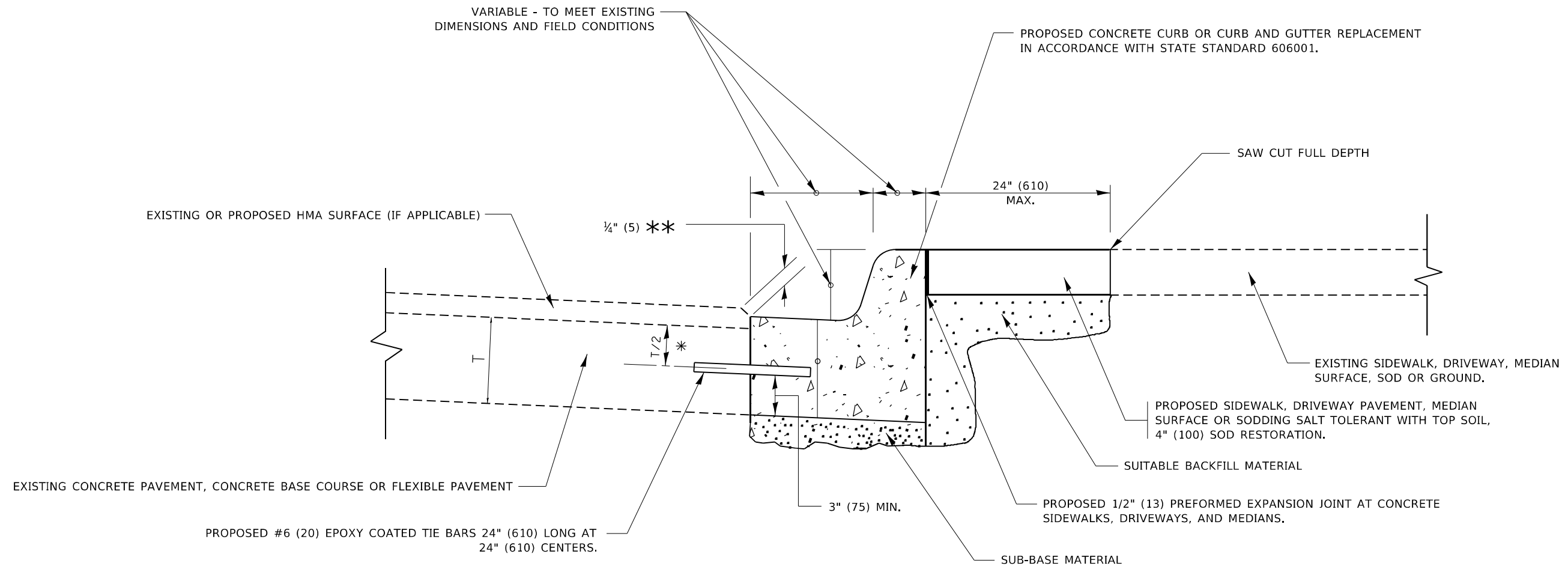
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PLOT DATE = 6/25/2021	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	11
BD400-04 (BD-22)		CONTRACT NO.		62L67
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

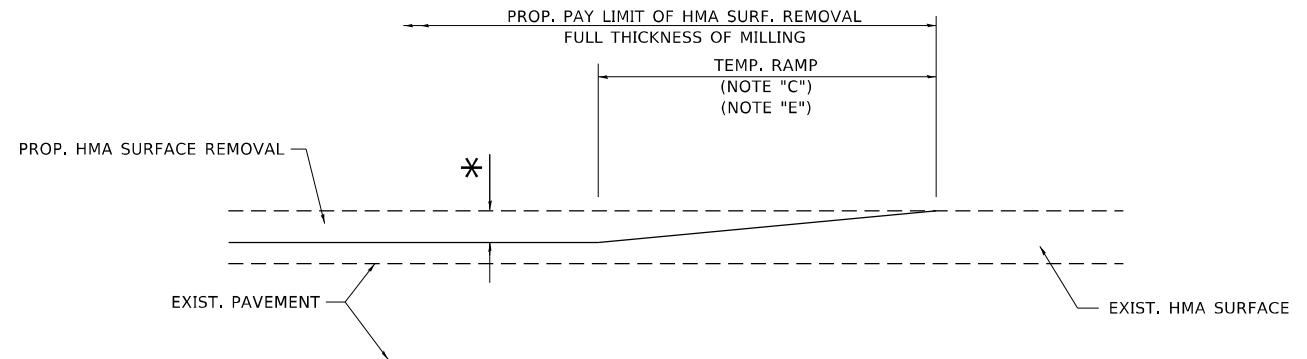
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PLOT DATE = 6/25/2021	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

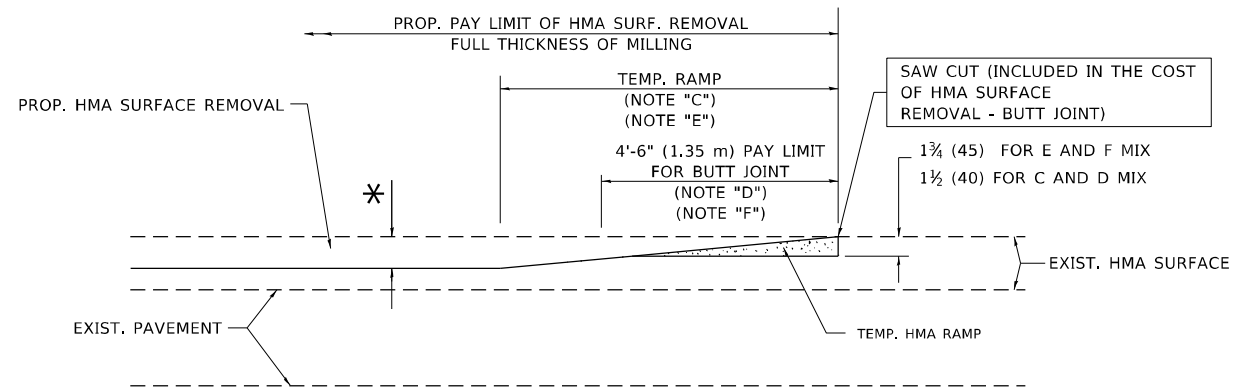
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BD600-06 (BD-24)		CONTRACT NO.	62L67	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

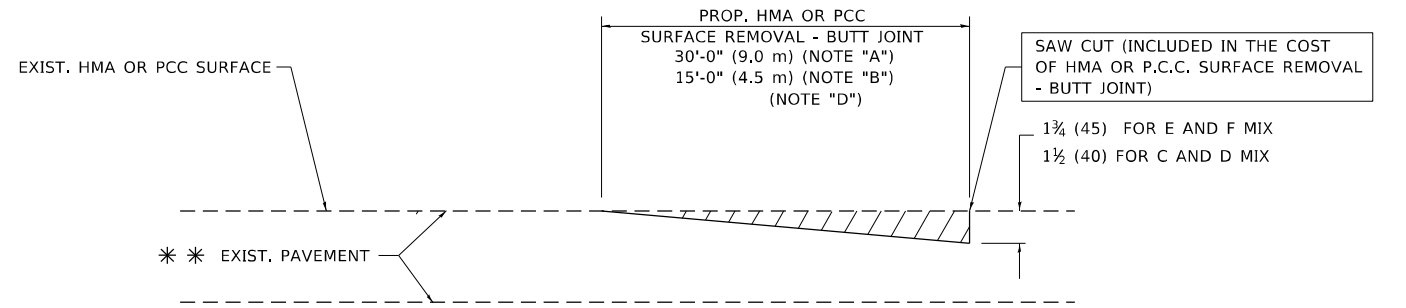


HMA CONSTRUCTED TEMPORARY RAMP

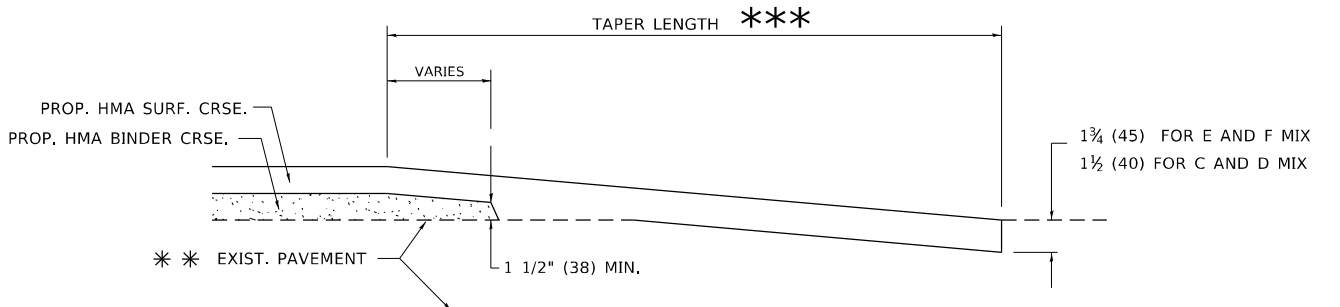
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

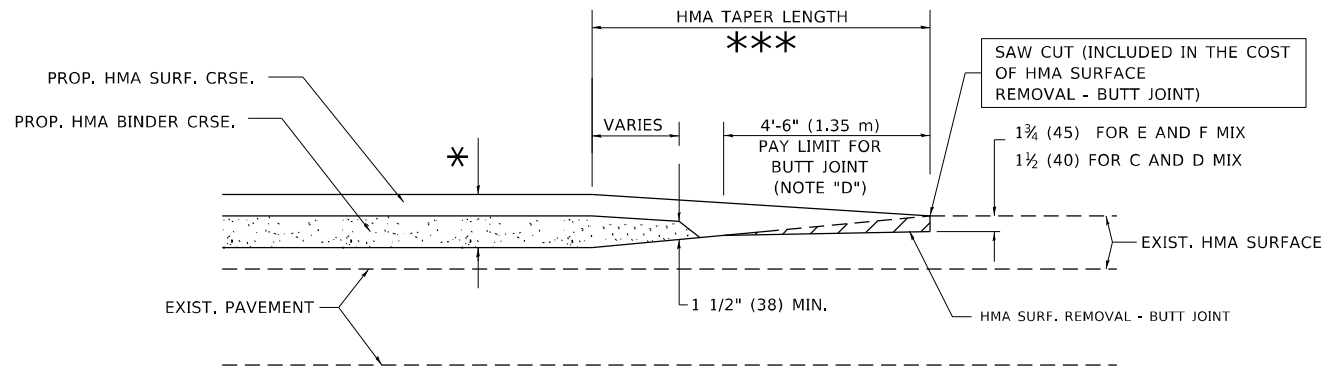
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

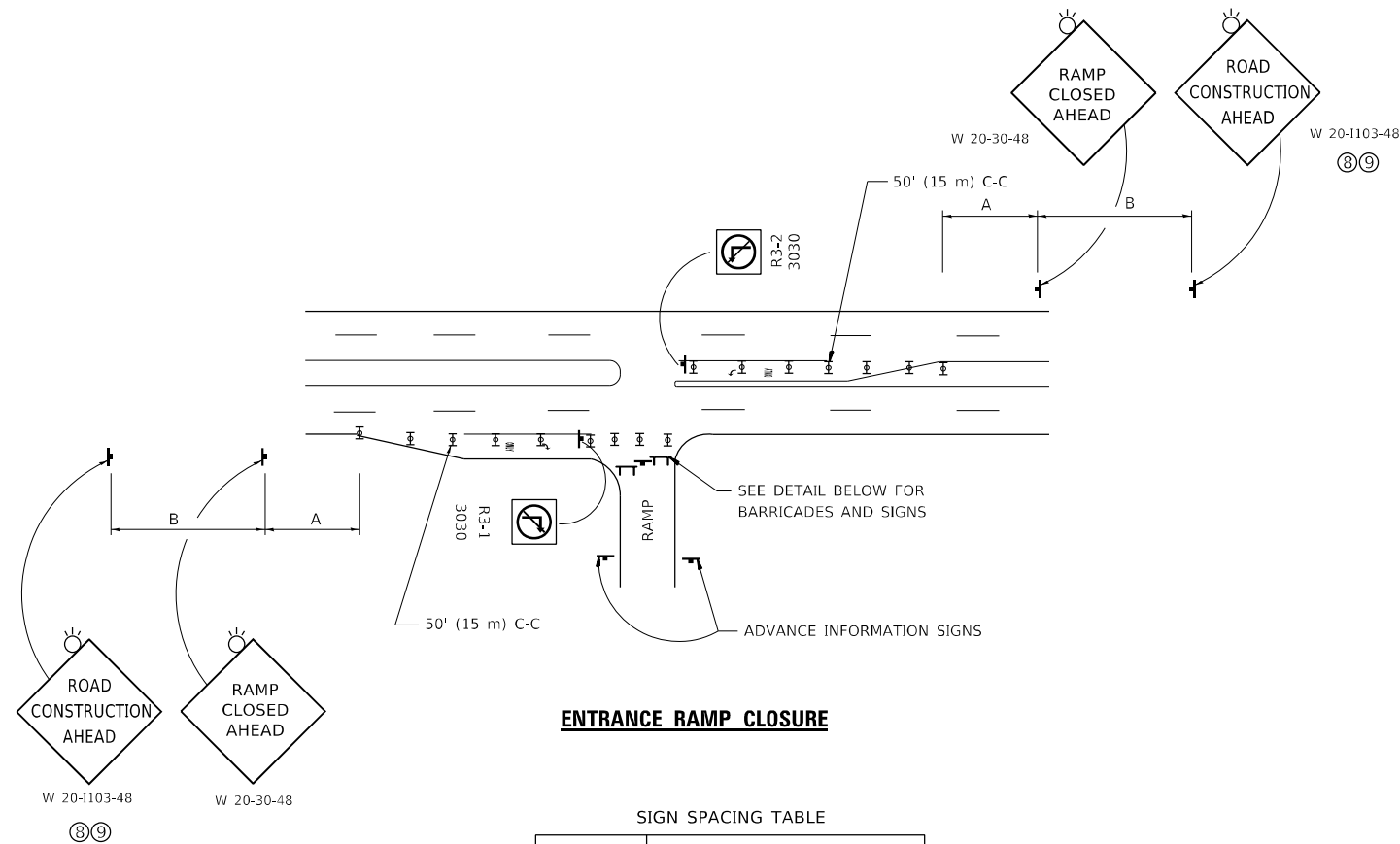
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	DATE - 06-13-90	REVISED - R.BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD32		CONTRACT NO. 62L67		
ILLINOIS FED. AID PROJECT				

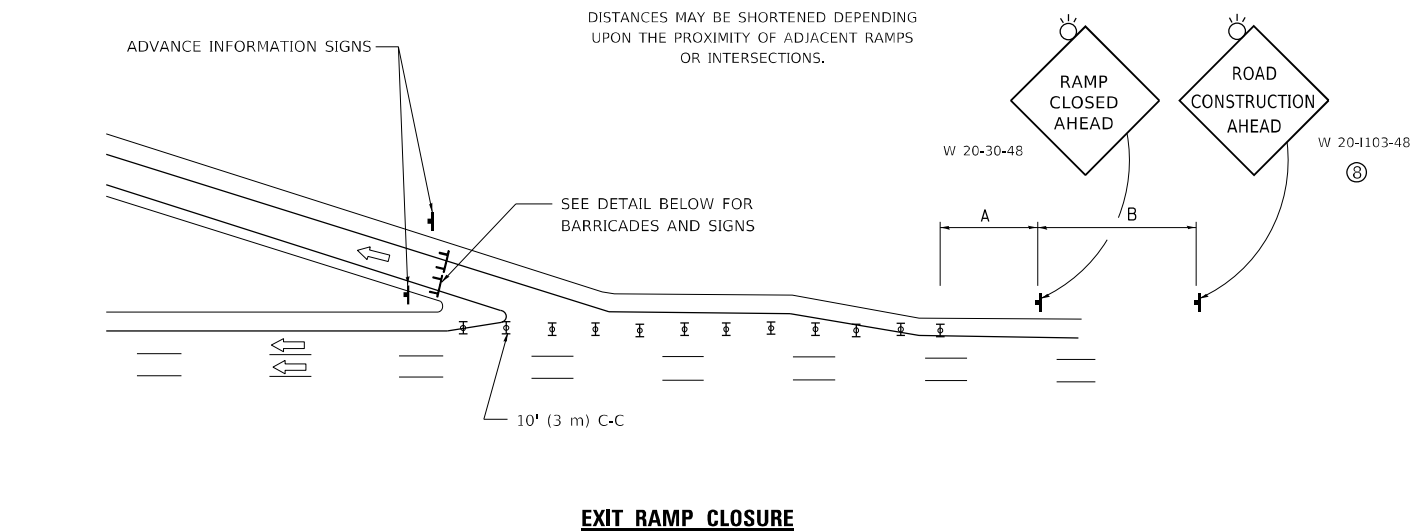


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

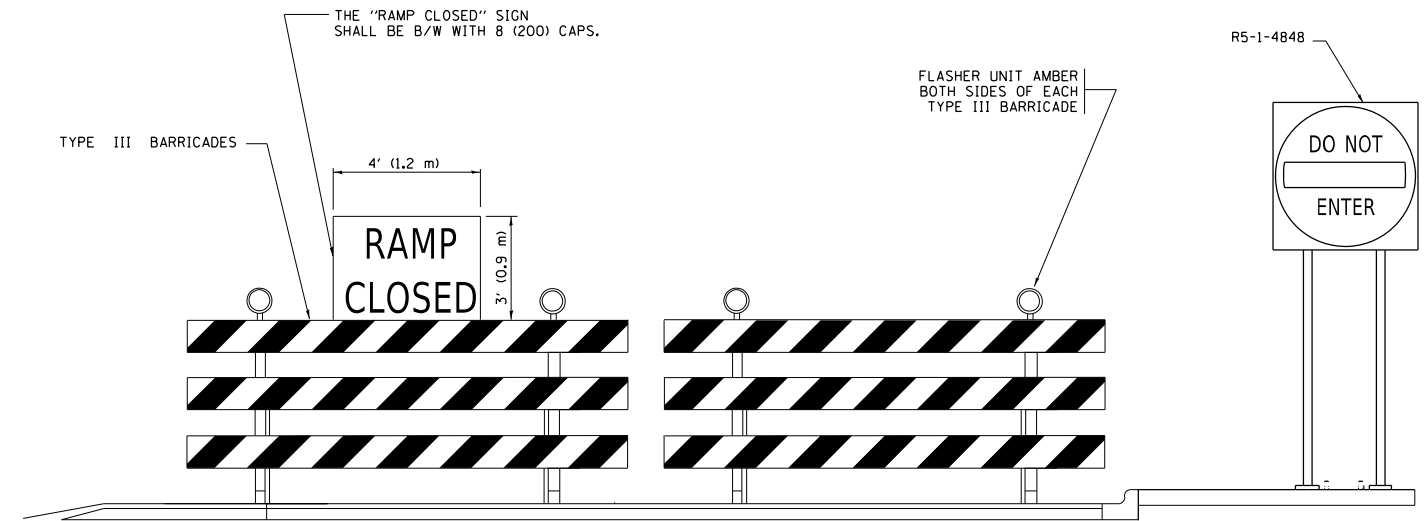
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

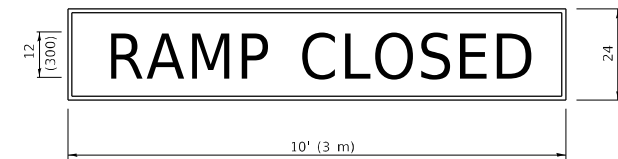
SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



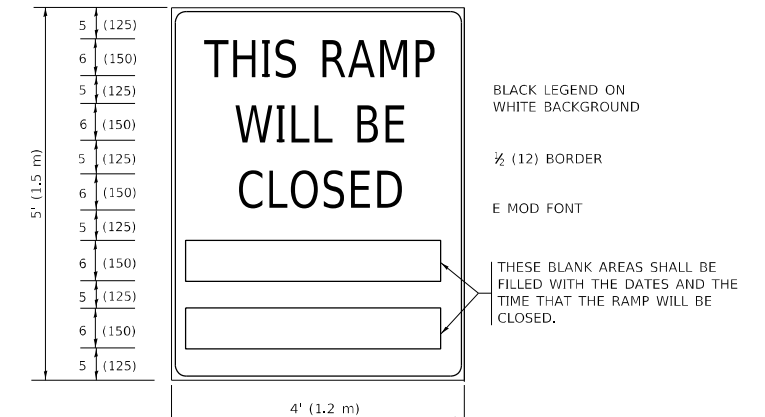
BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

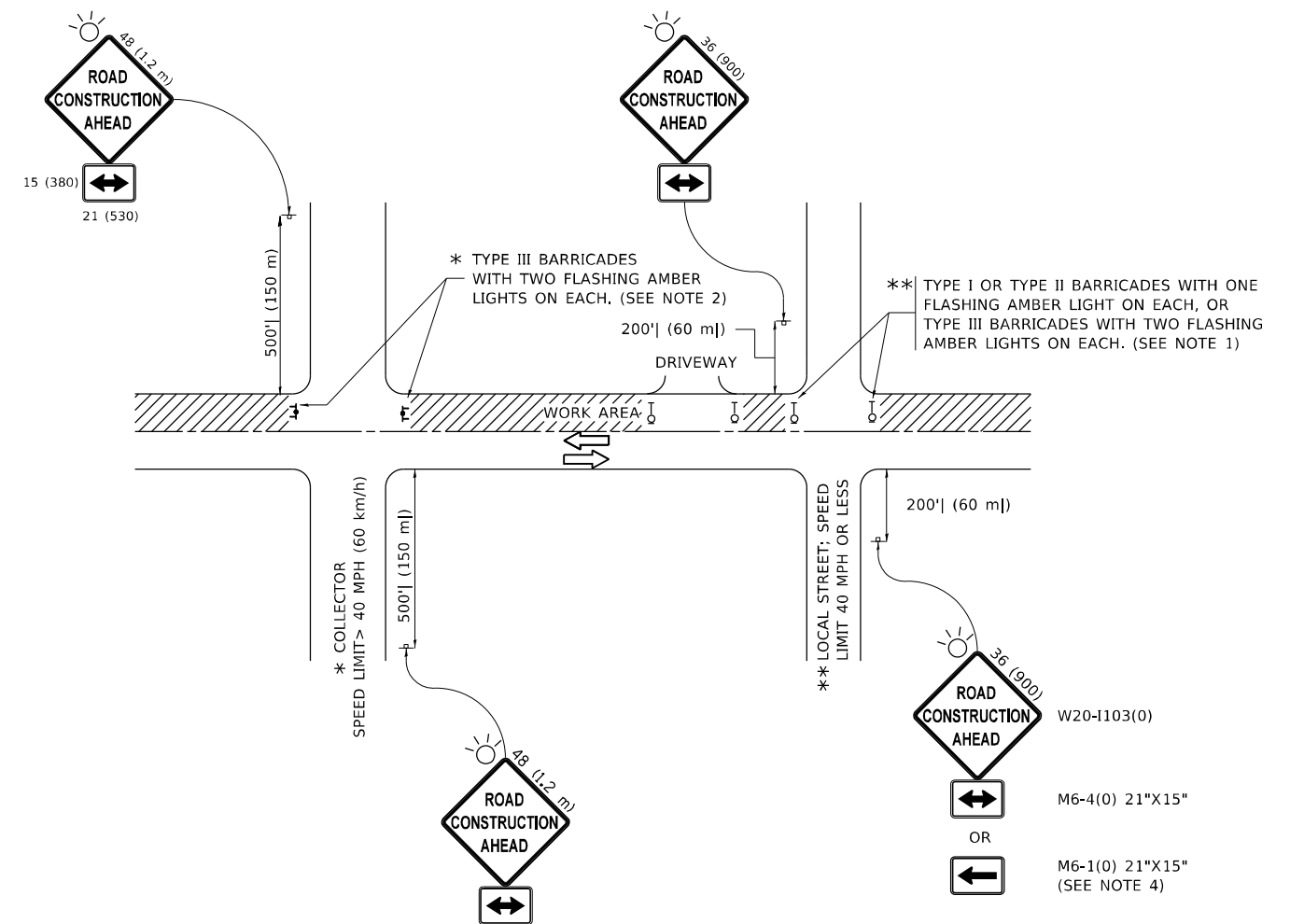
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE_AND_EXIT_RAMP
CLOSURE_DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	14
TC-08			CONTRACT NO.	62L67
		ILLINOIS	FED. AID PROJECT	

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NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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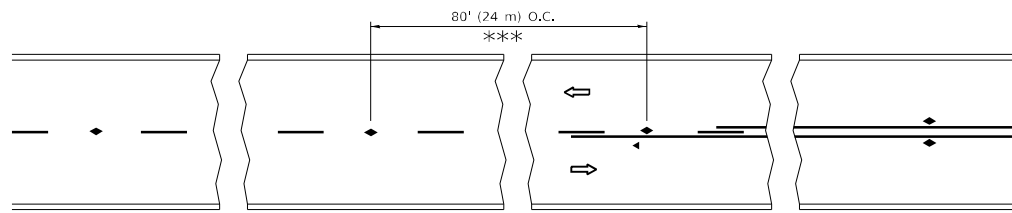
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PLOT DATE = 6/25/2021	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

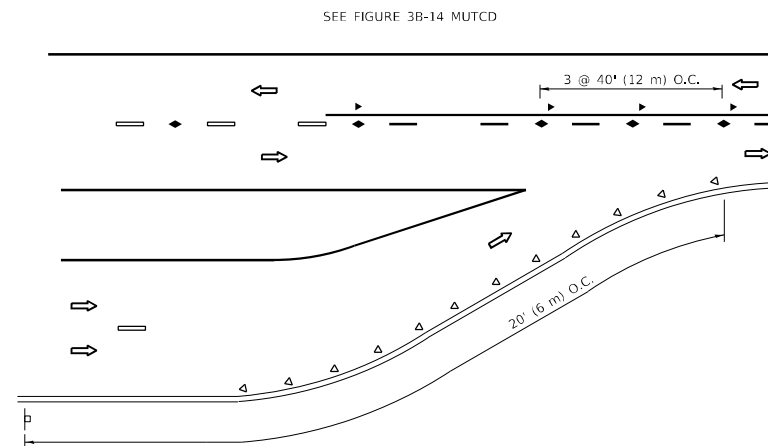
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO.	62L67
ILLINOIS FED. AID PROJECT				

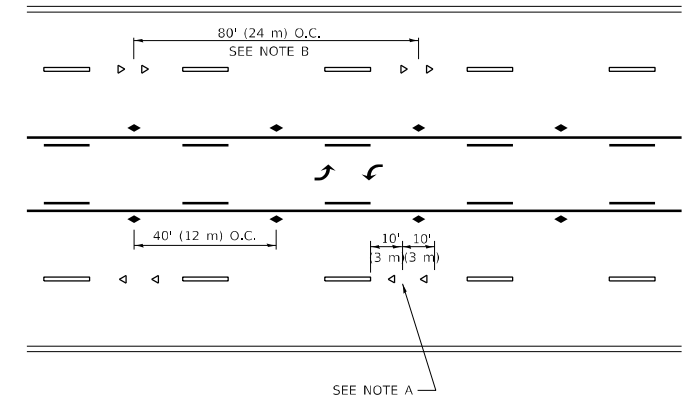


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

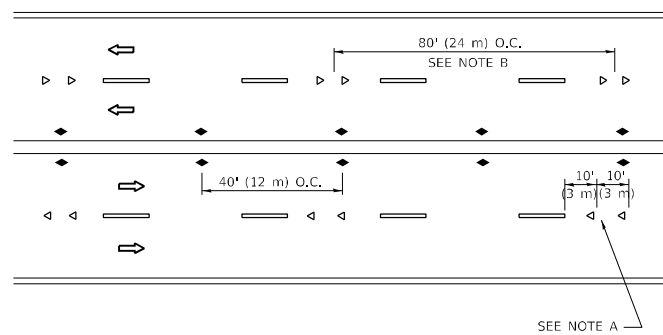
TWO-LANE/TWO-WAY



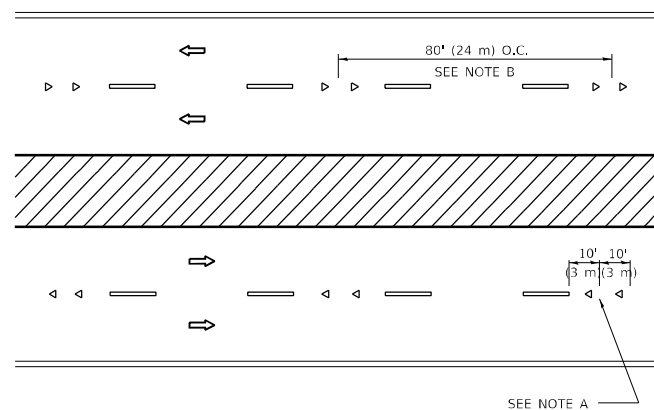
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

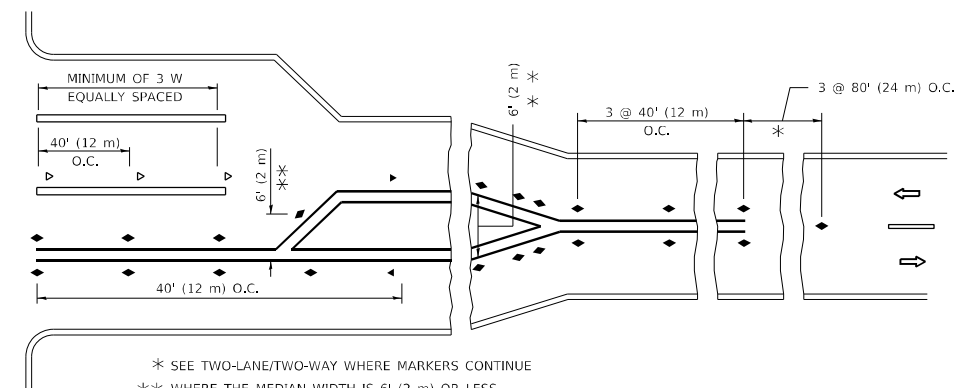
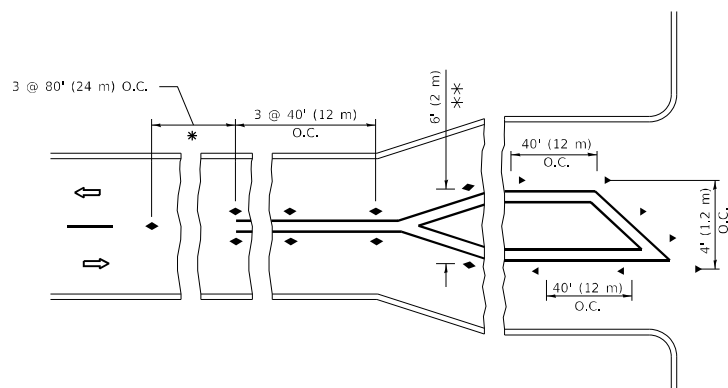
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 6/25/2021	CHECKED -	REVISED - C. JUCIUS 09-09-09
	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	16
TC-11		CONTRACT NO.		62L67
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

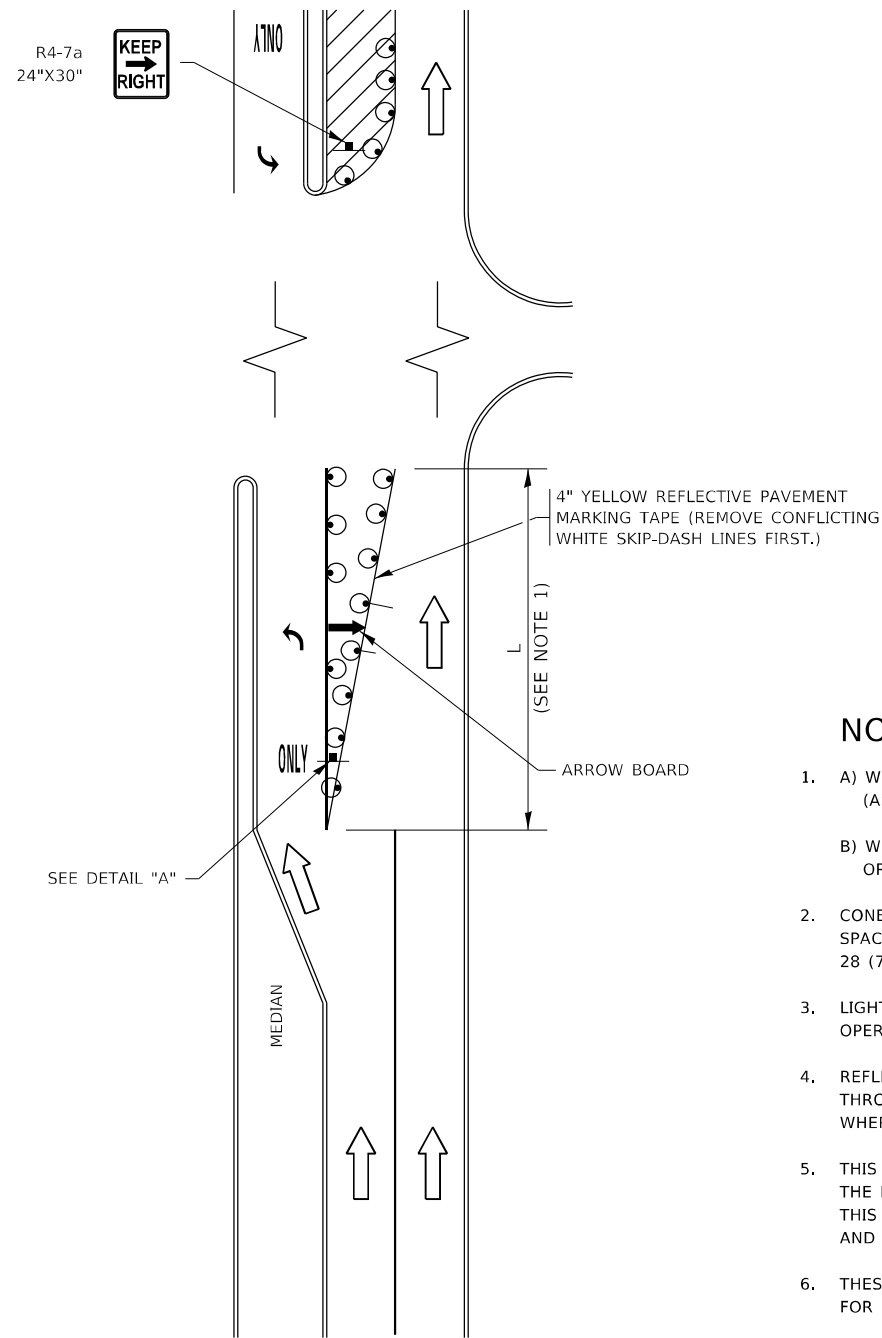


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

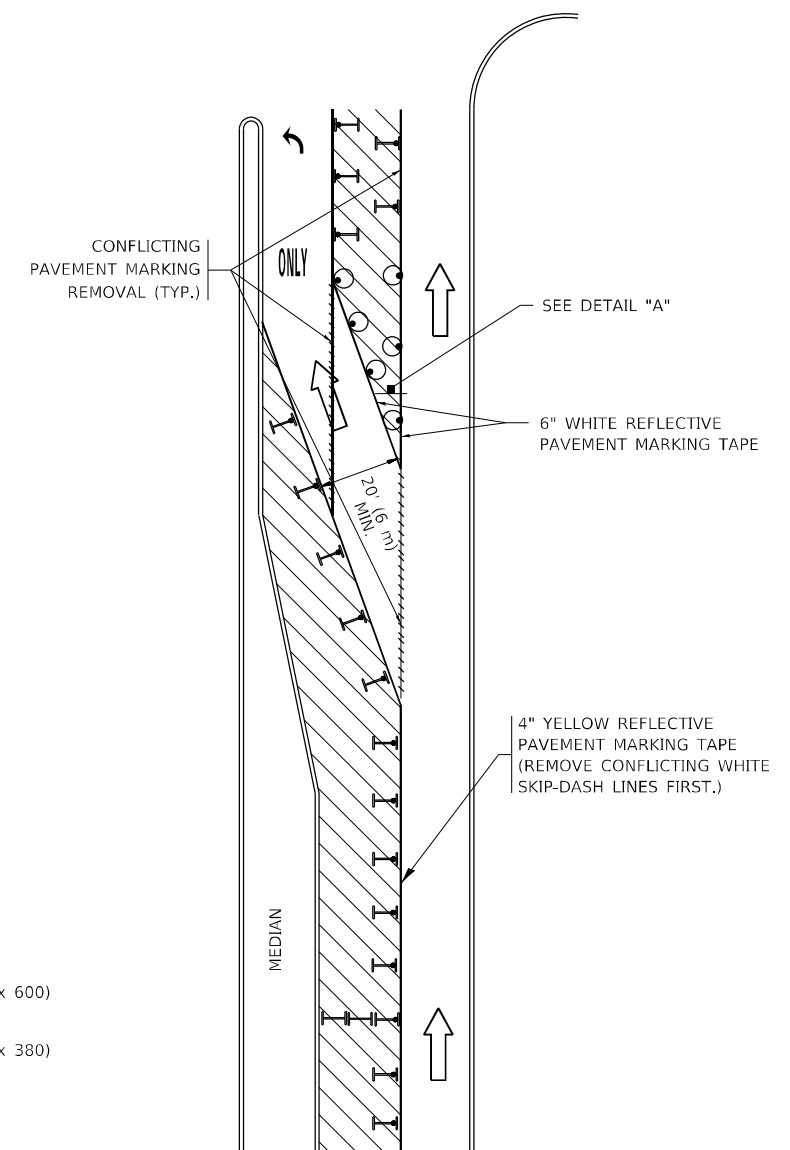


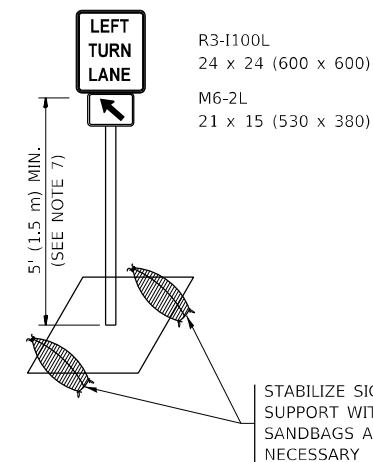
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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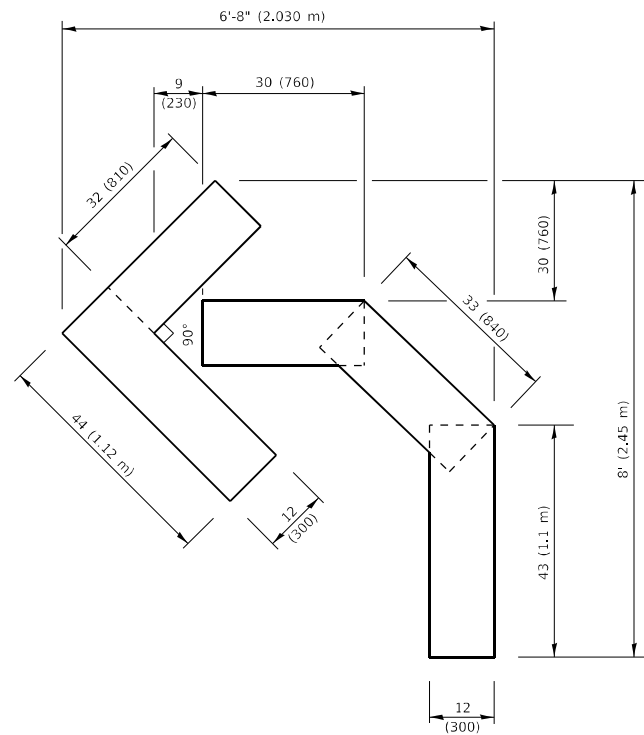
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PLOT DATE = 6/25/2021	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

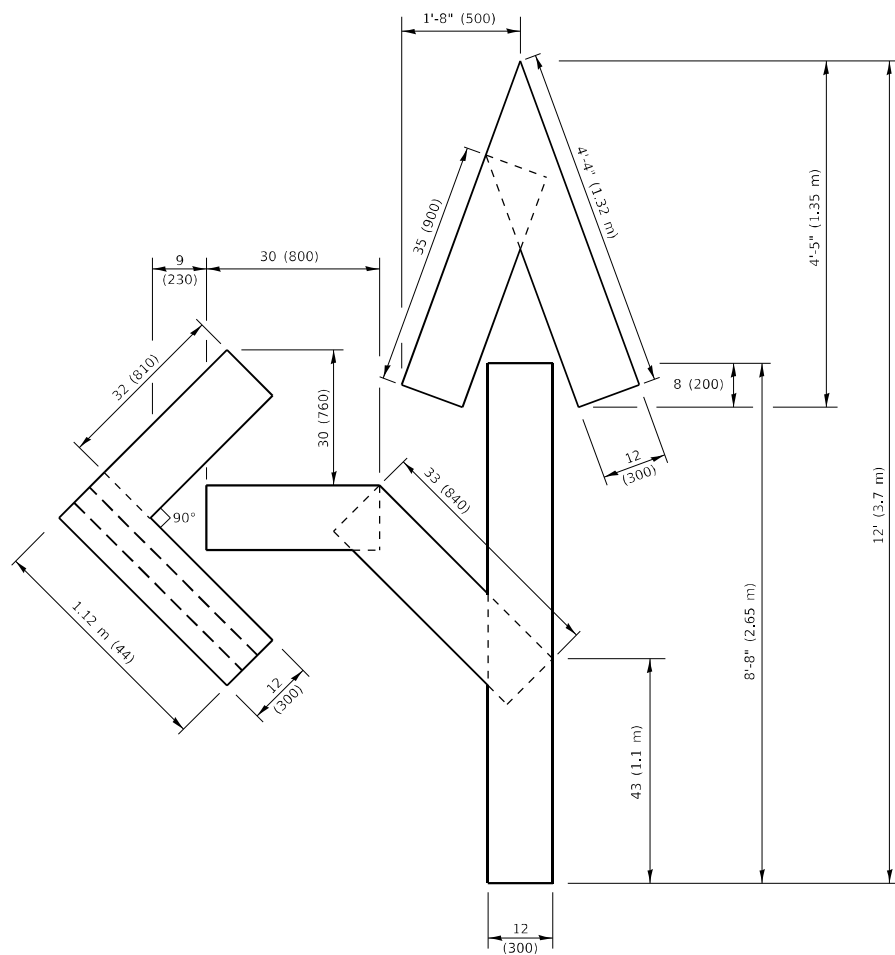
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TC-14		CONTRACT NO.		62L67
ILLINOIS FED. AID PROJECT				



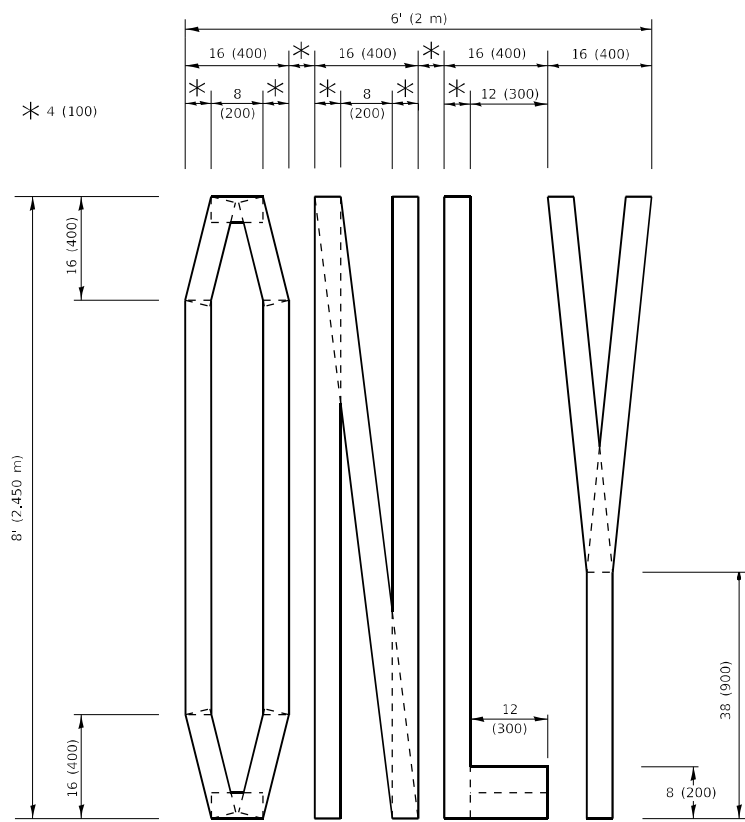
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4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



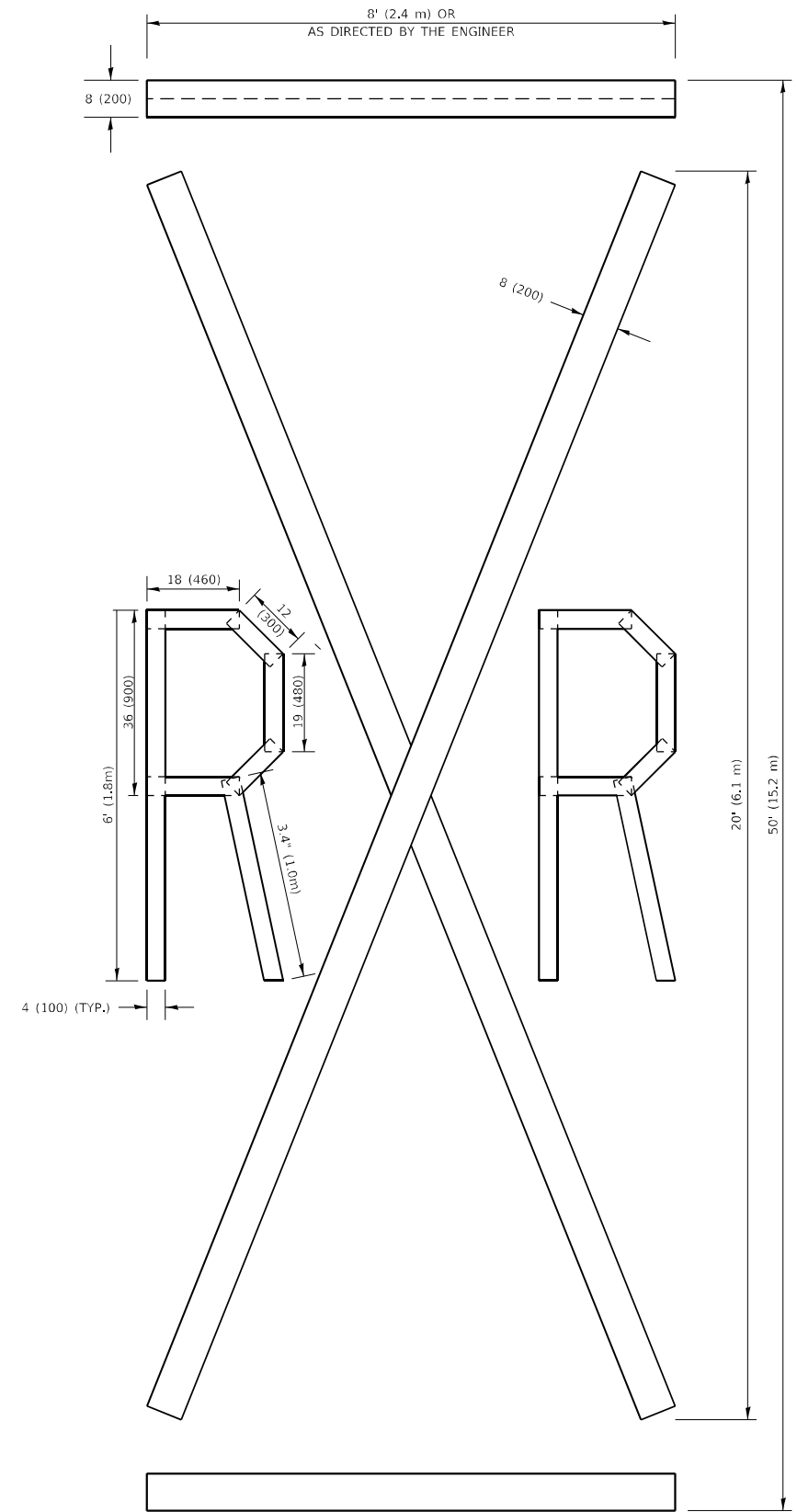
QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

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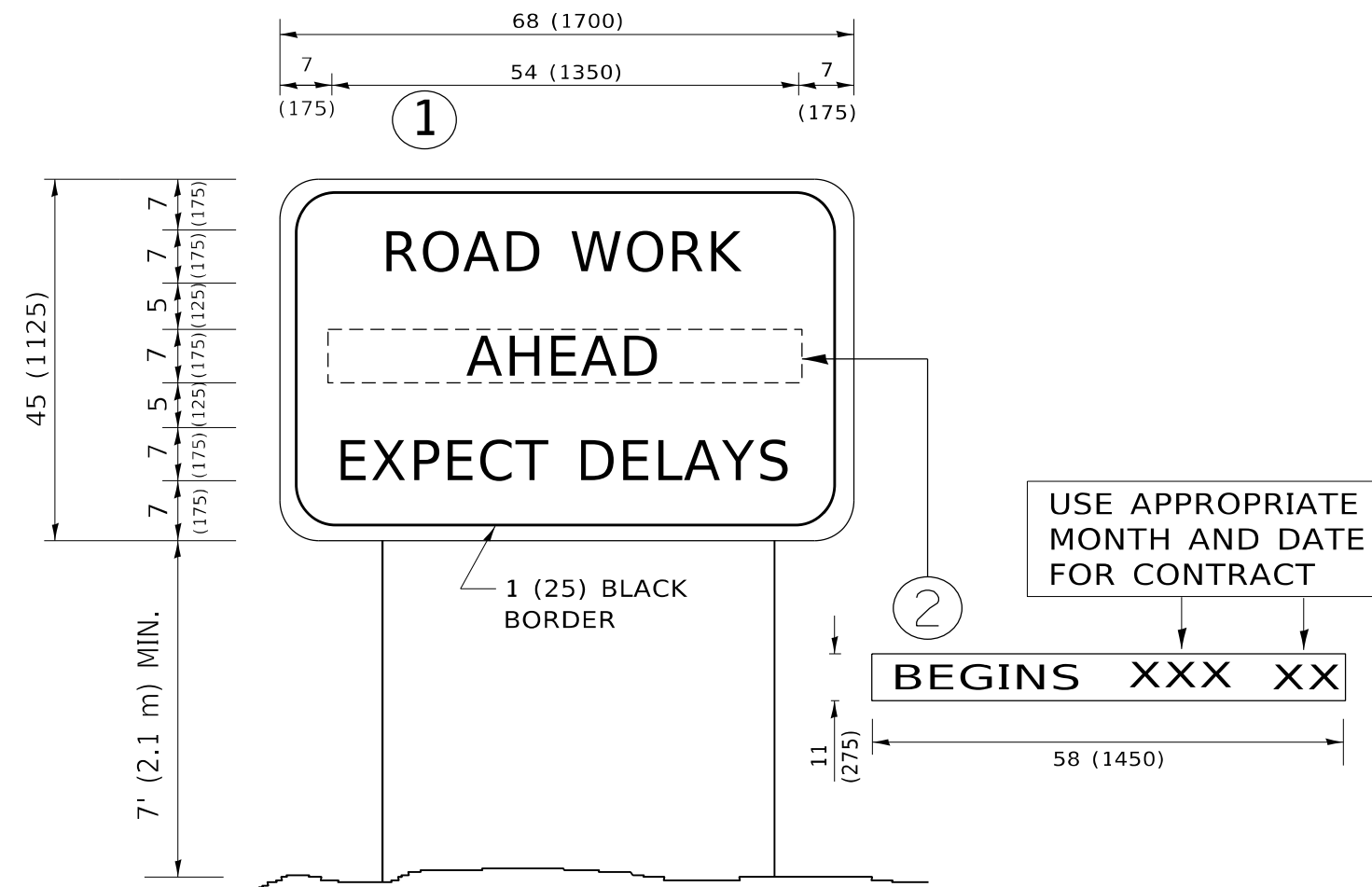
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	18
TC-16		CONTRACT NO.	62L67	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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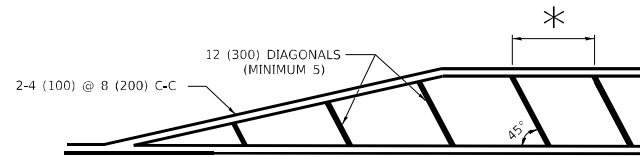
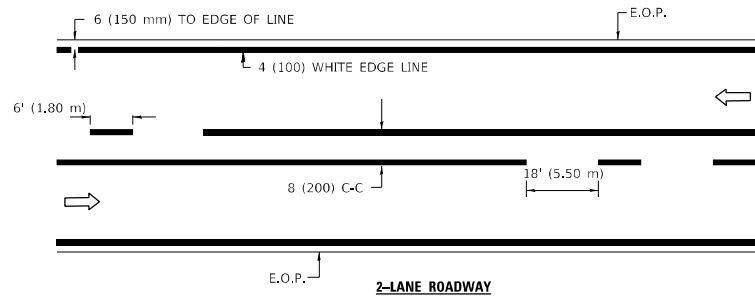
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PLOT DATE = 6/25/2021	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

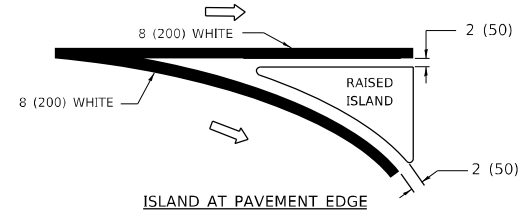
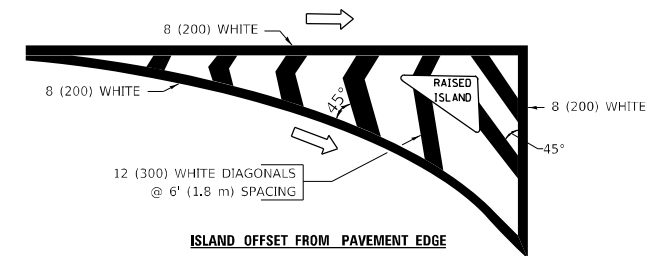
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ILLINOIS FED. AID PROJECT				

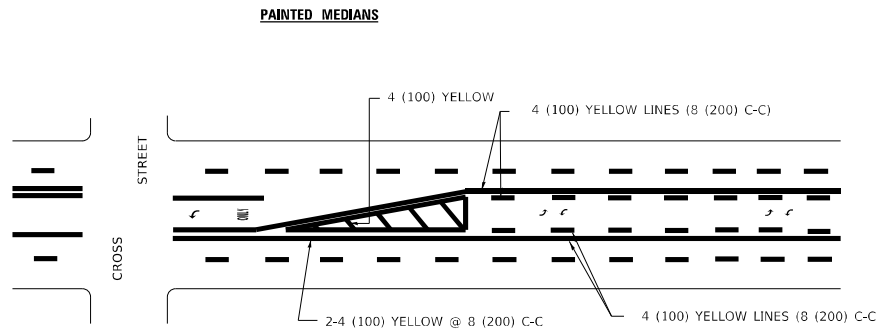
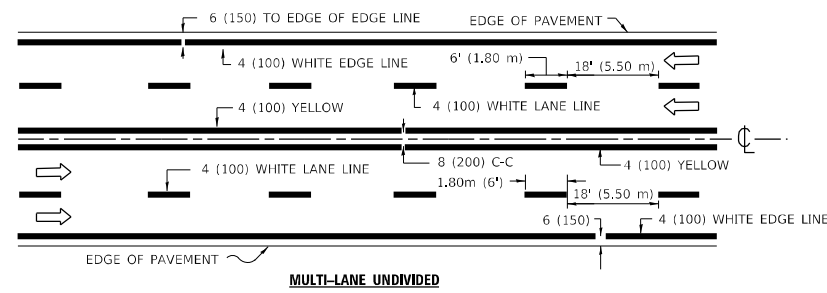


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

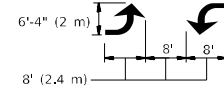
* DIAGONAL LINE SPACING: 20' (6.1 m) C-C



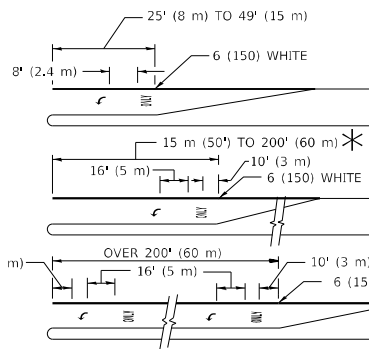
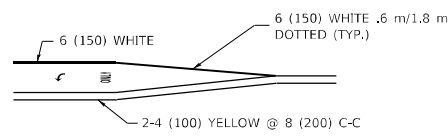
TYPICAL ISLAND MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



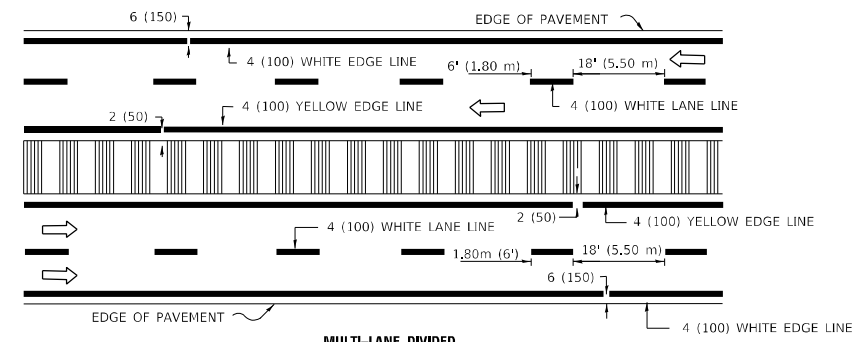
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

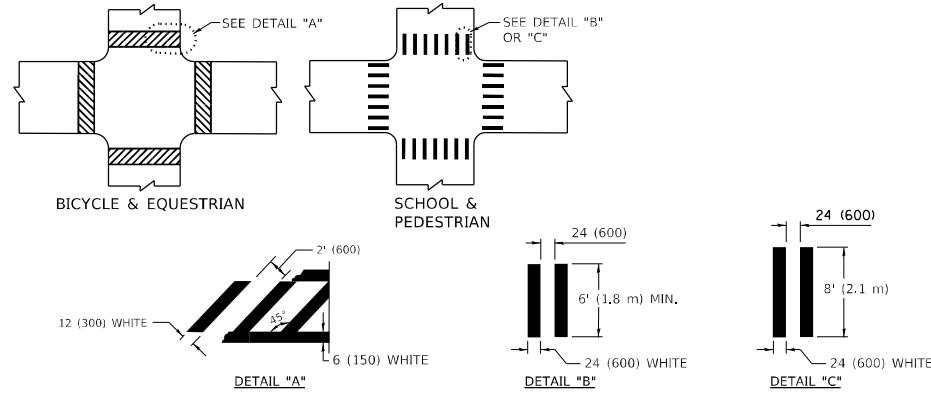
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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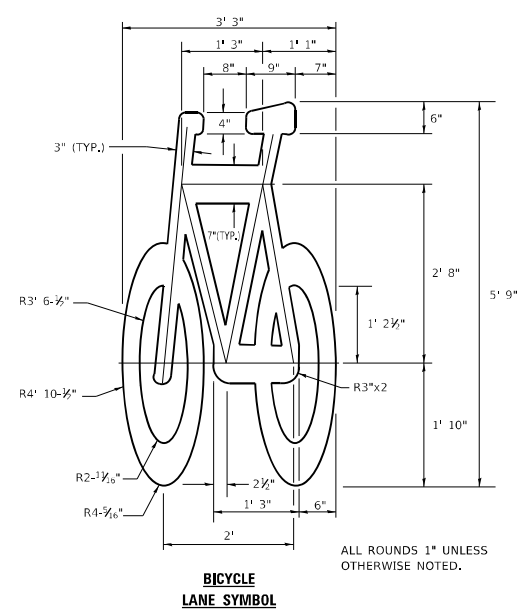
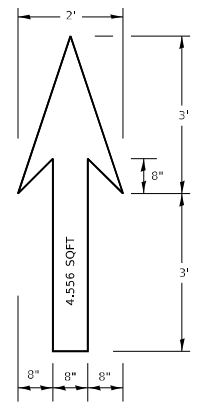
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PLOT DATE = 6/25/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

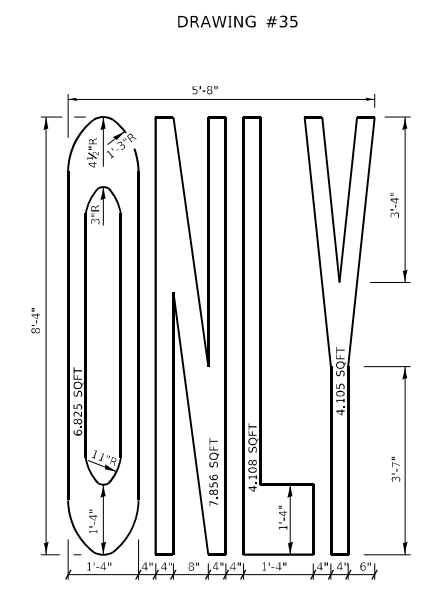
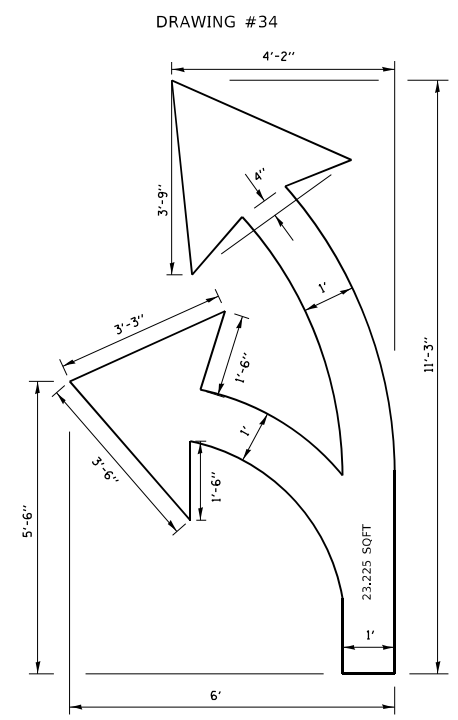
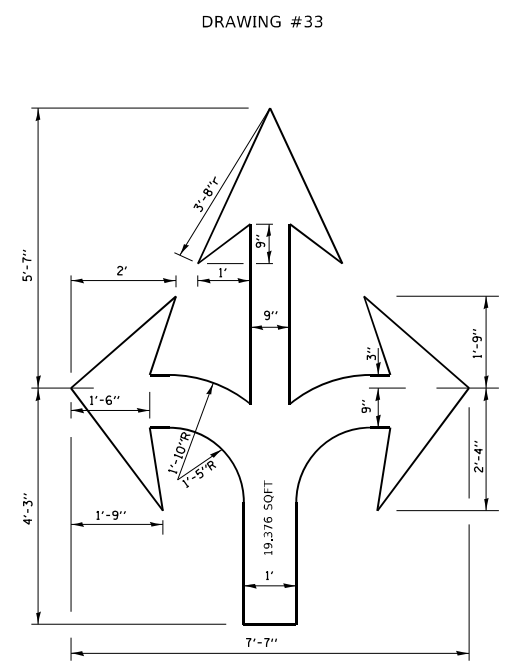
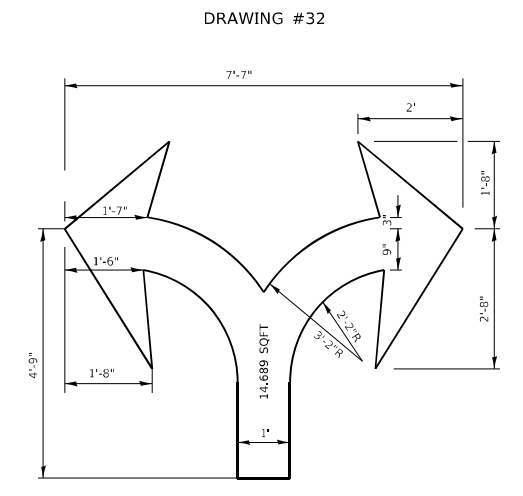
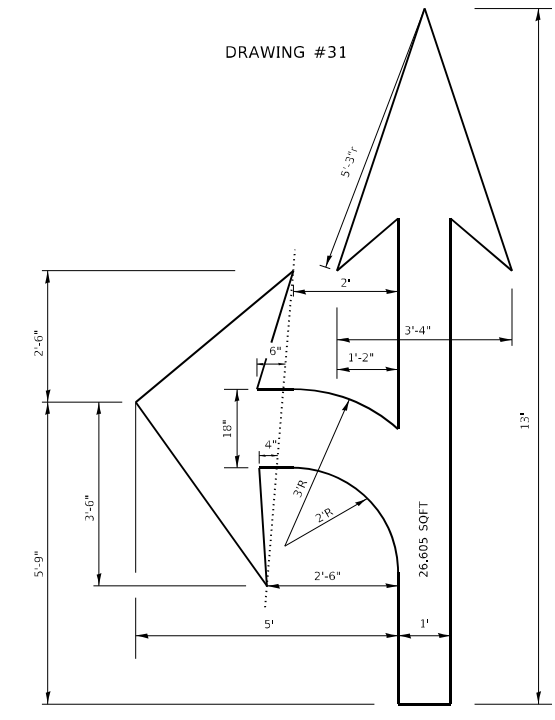
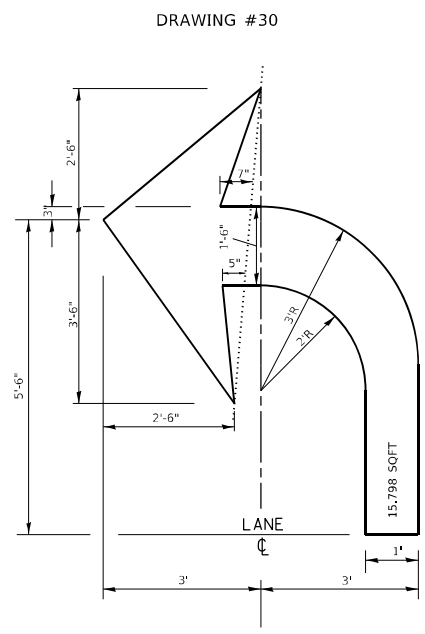
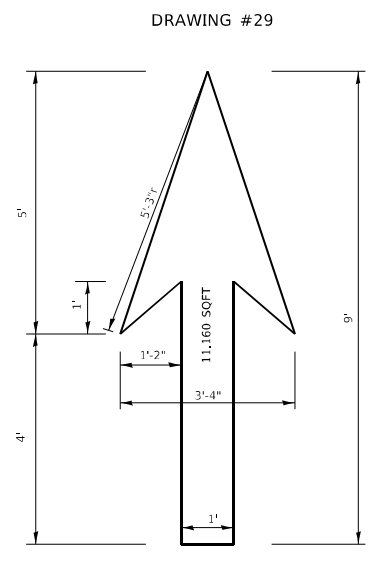
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	20
TC-24		CONTRACT NO.	62L67	
ILLINOIS		FED. AID PROJECT		



**BICYCLE
LANE SYMBOL**

- NOTE:**
1. FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2. THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS
DRAWING #28**



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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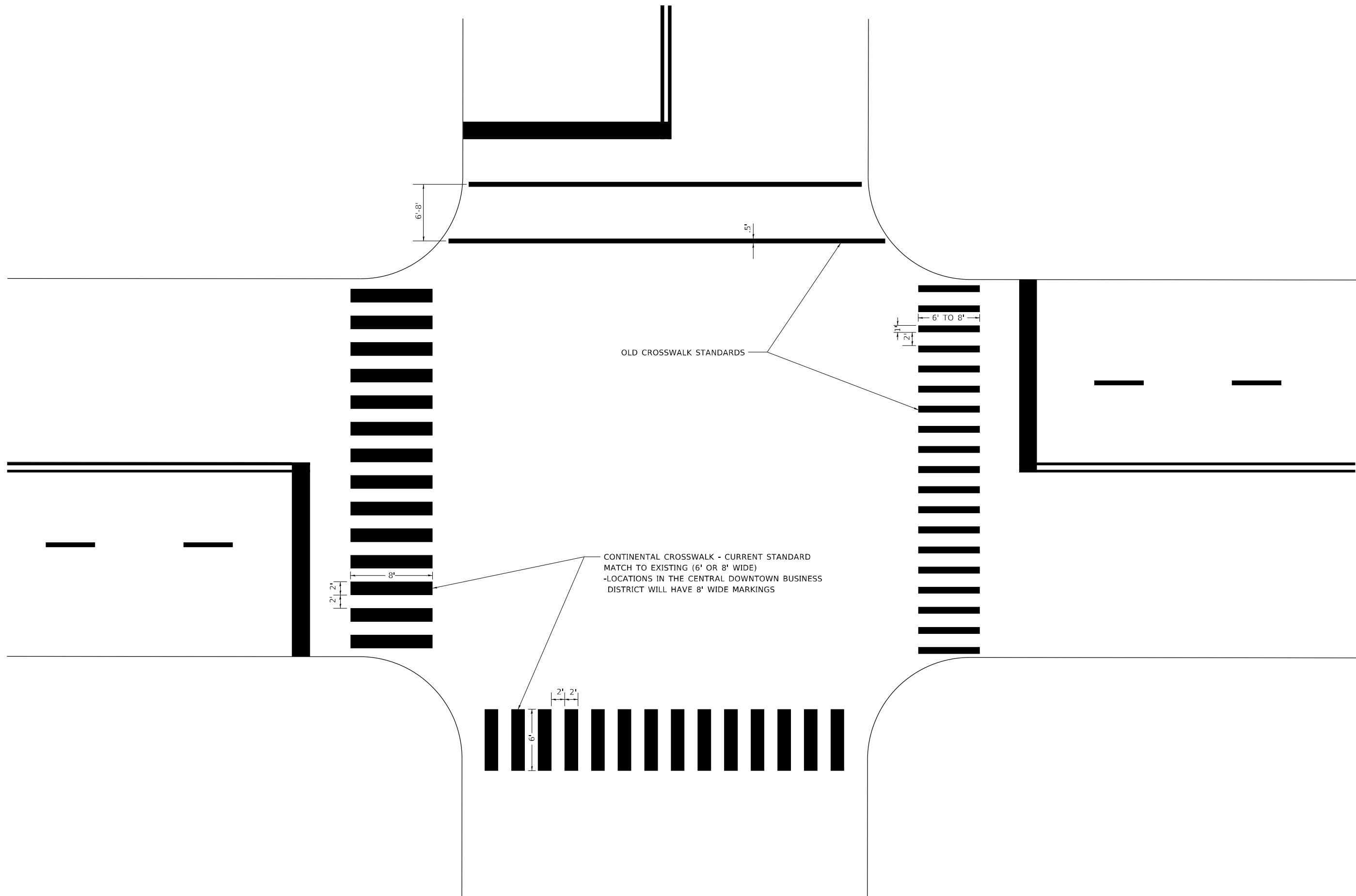
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 2	OF 3 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	21
TC-24		CONTRACT NO. 62L67		
ILLINOIS		FED. AID PROJECT		

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CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

OLD CROSSWALK STANDARDS

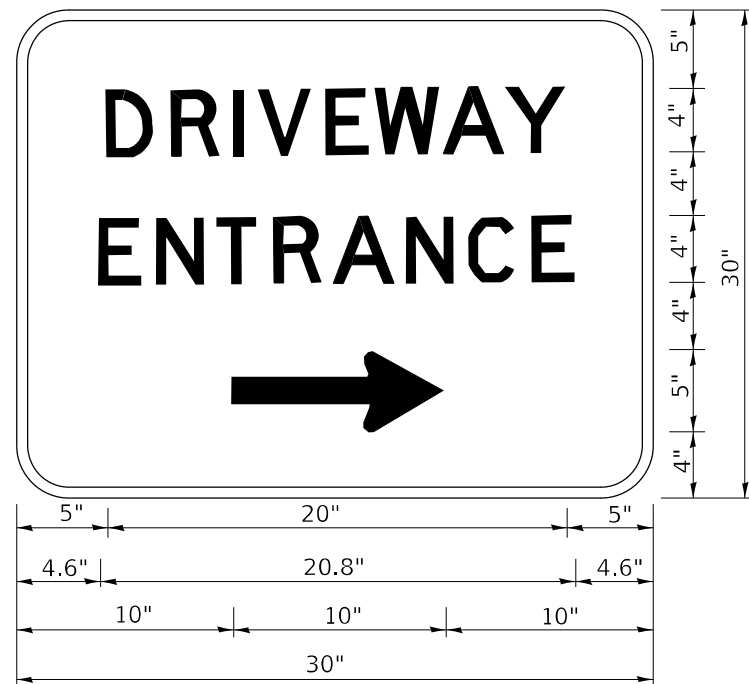
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

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PLOT SCALE = 100,0001' / in.	DRAWN -	REVISED -
PLOT DATE = 6/25/2021	CHECKED -	REVISED -
	DATE -	REVISED -

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-R5&SW	COOK	26	22
TC-24			CONTRACT NO. 62L67	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/25/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

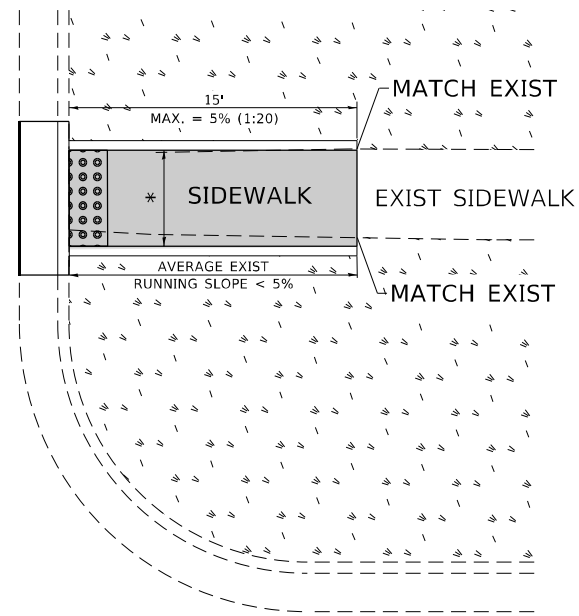
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

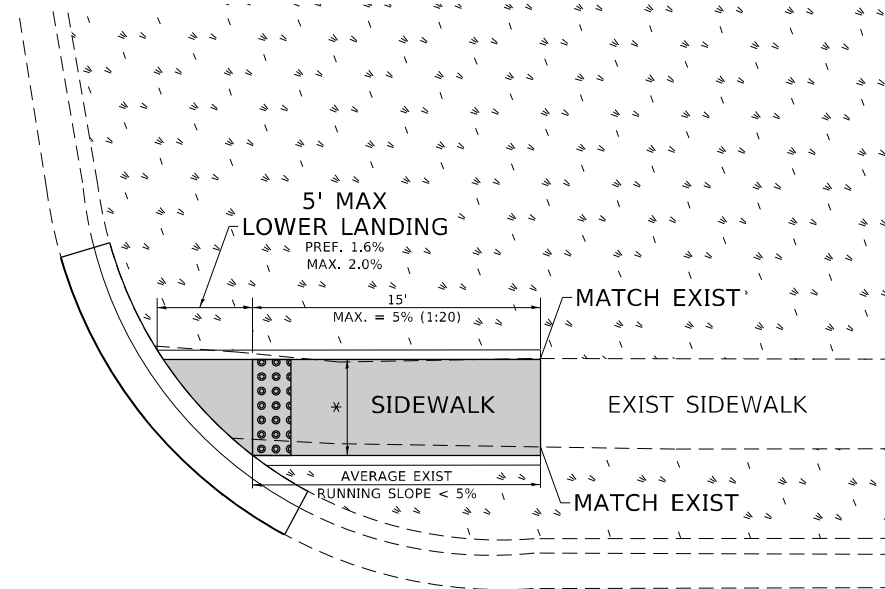
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162	2020-089-R5&SW	COOK	26	23
TC-26			CONTRACT NO.	62L67
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

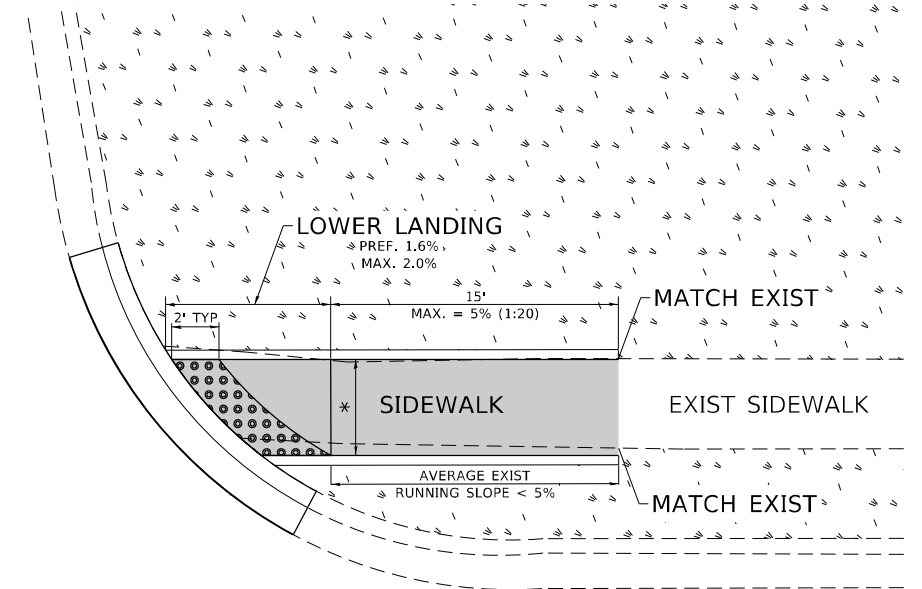
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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 PROJECT DATE: 6/25/2021
 PROJECT LOCATION: ILLINOIS
 PROJECT NUMBER: 2020-089-RS&SW
 PROJECT SHEET: PD-01
 PROJECT CONTRACT NO.: 62L67

USER NAME = dumachia	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/25/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

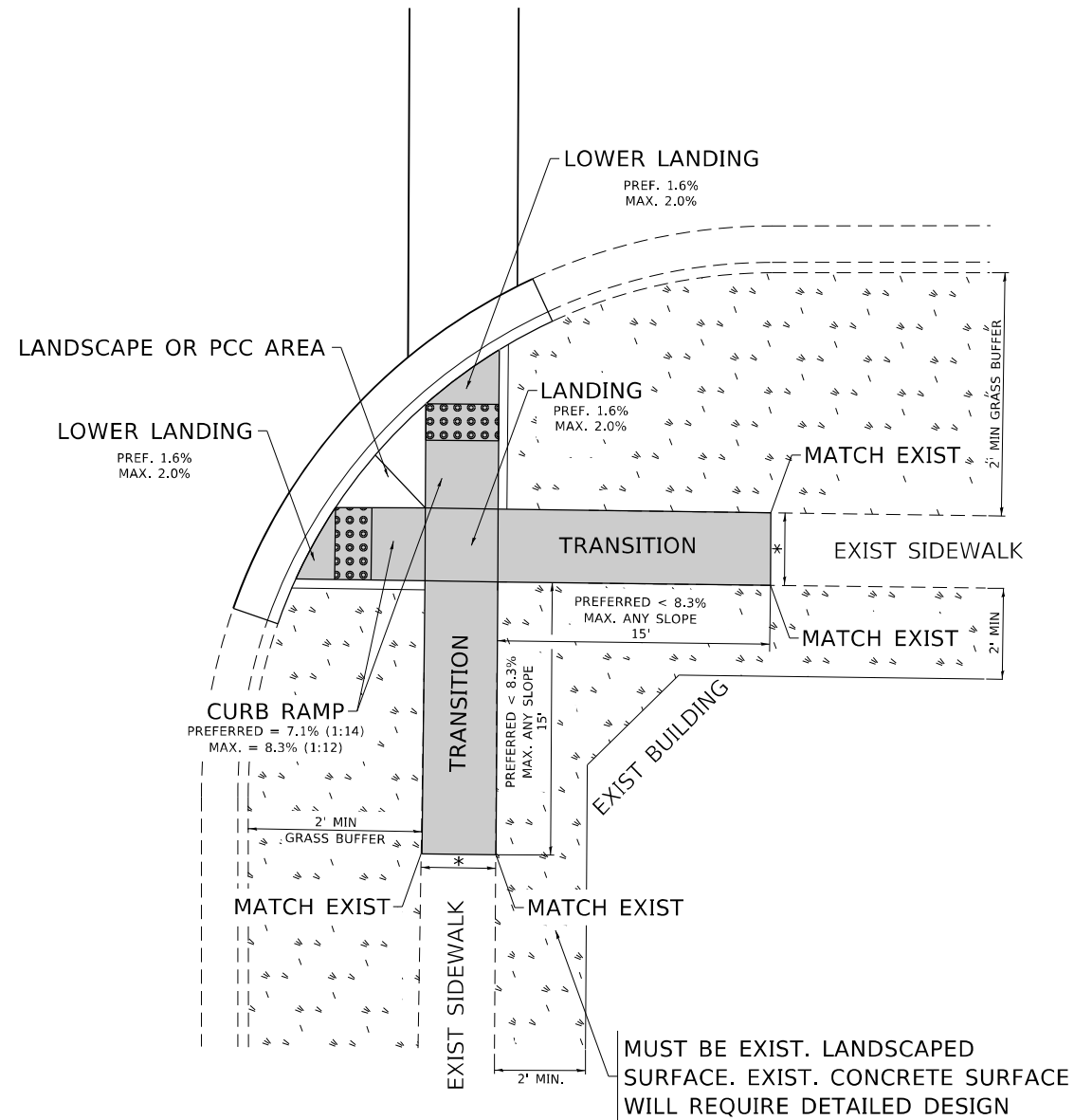
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

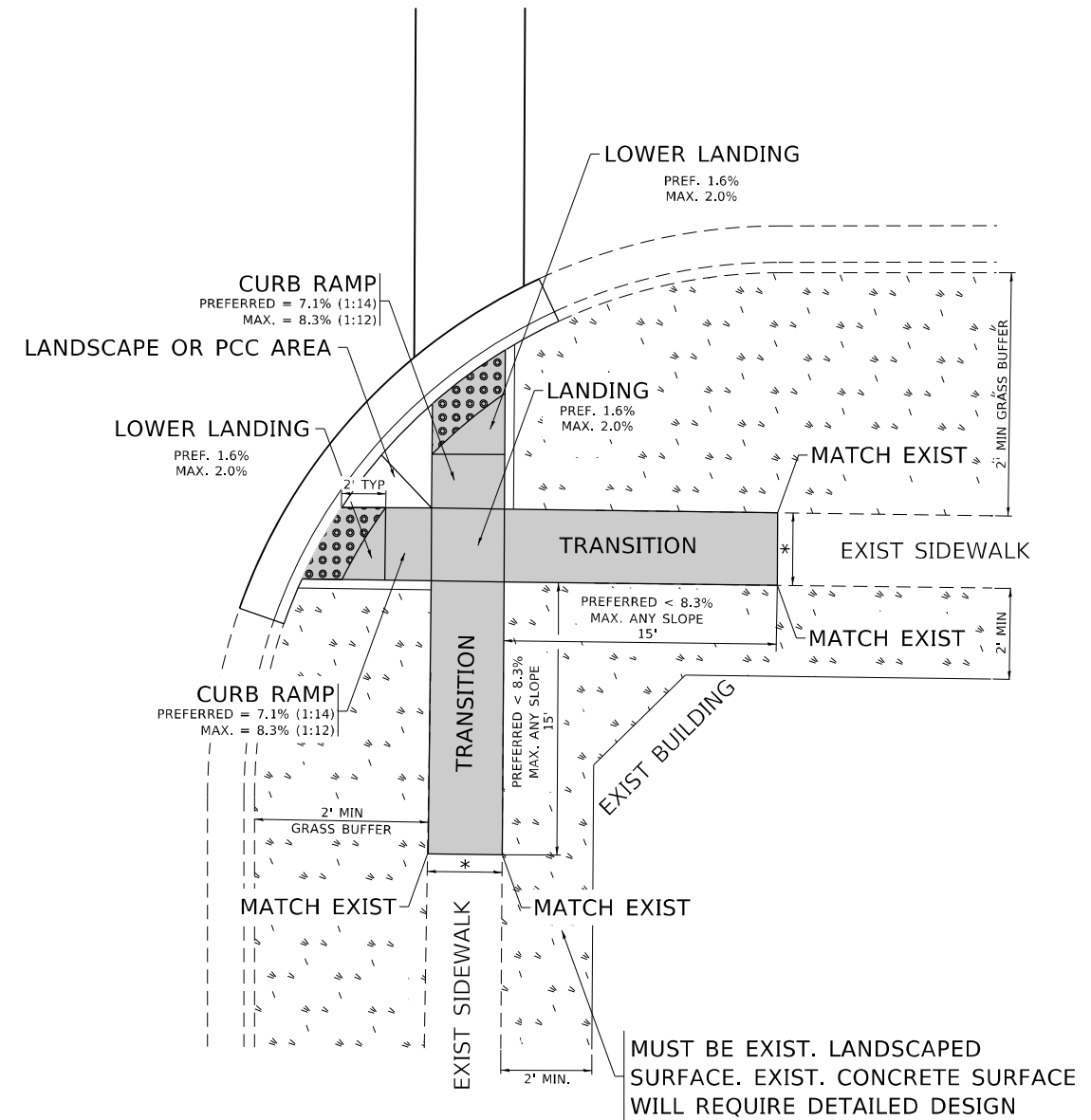
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162	2020-089-RS&SW	COOK	26	24
PD-01		CONTRACT NO. 62L67		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/25/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

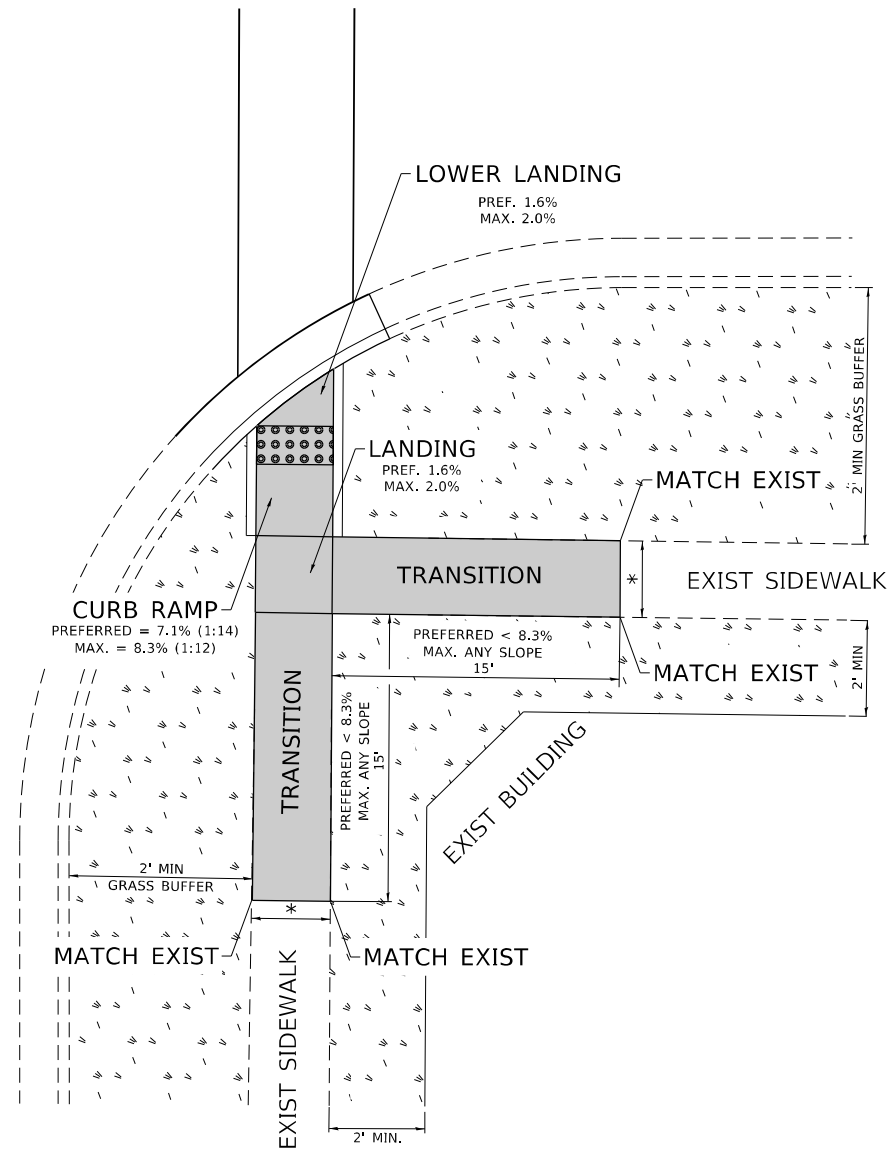
**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

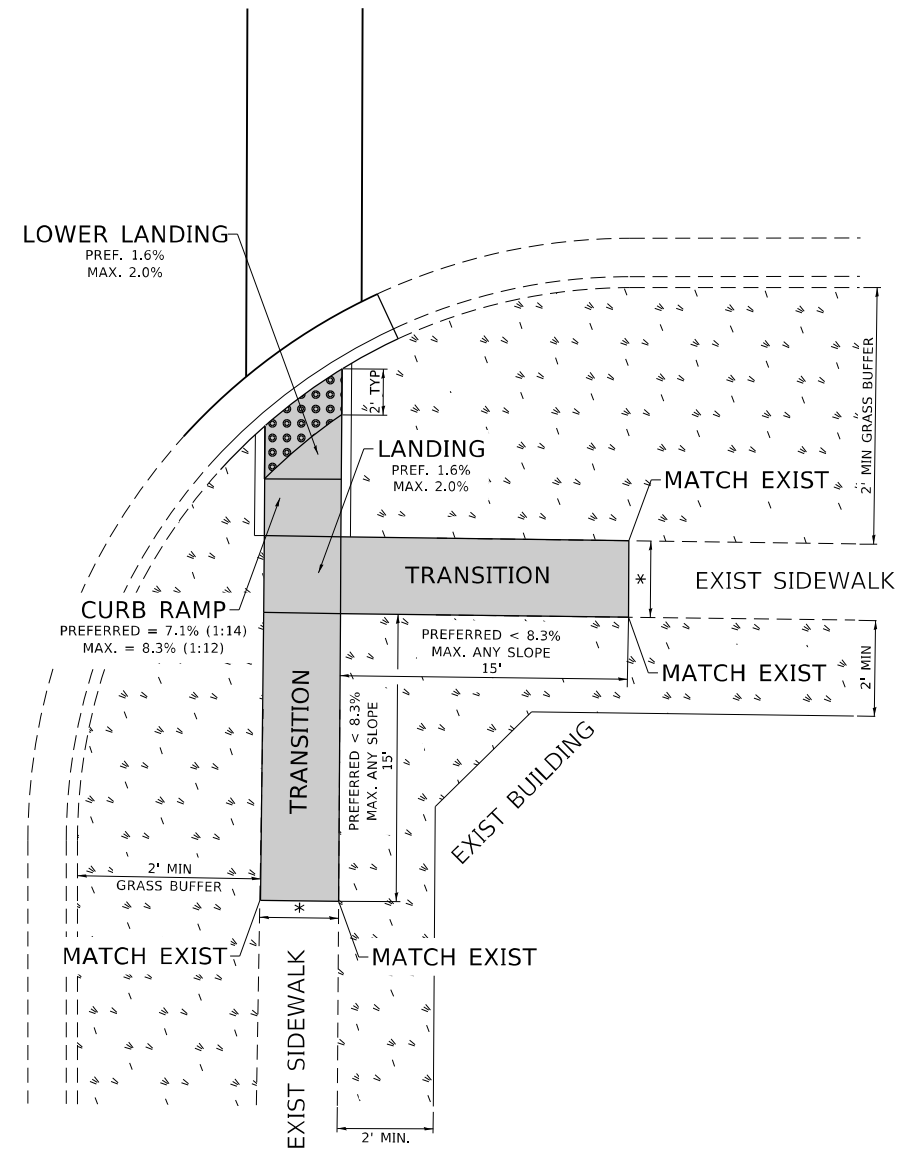
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162	2020-089-RS&SW	COOK	26	25
PD-03		CONTRACT NO. 62L67		
ILLINOIS FED. AID PROJECT				

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
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 PROJECT DATE: 6/25/2021
 USER: dumachla
 DATE: 10/02/2019

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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 6/25/2021	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
162	2020-089-RS&SW	COOK	26	26
PD-04		CONTRACT NO. 62L67		
ILLINOIS FED. AID PROJECT				