

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

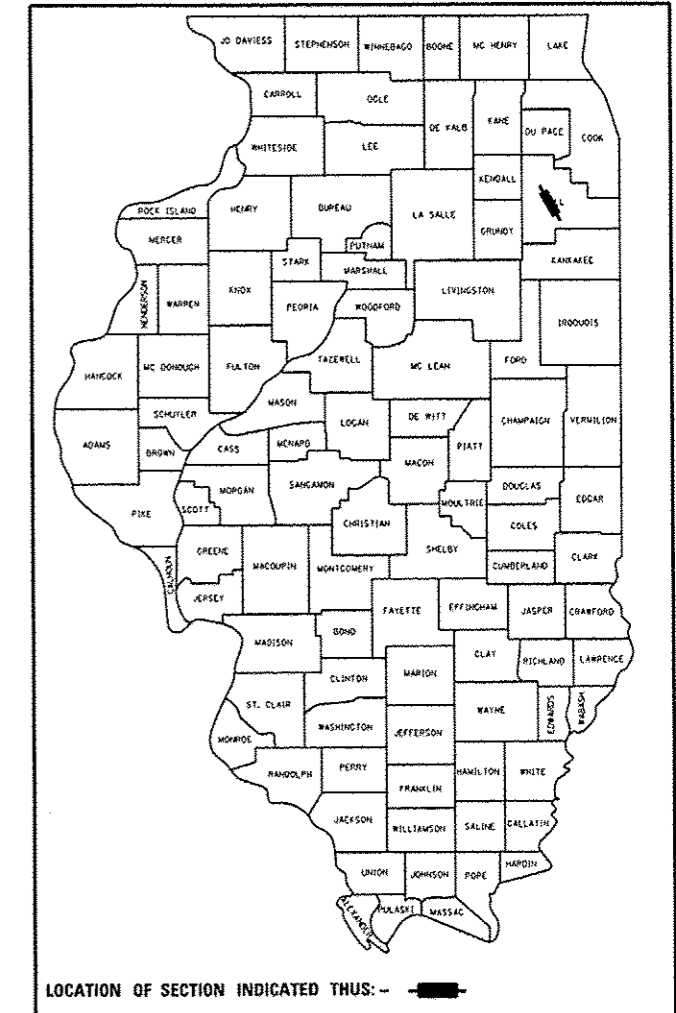
F.A.P. 852 U.S. ROUTE 52
SECTION (17&18) RS-6
2ND STREET TO U.S. ROUTE 45
RESURFACING (3P)
WILL COUNTY
C-91-183-13

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60W12	

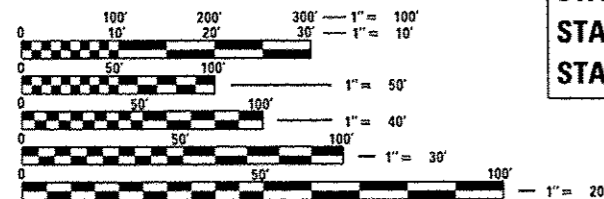
D-91-183-13

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED WITHIN THE VILLAGE OF MANHATTAN AND THE UNINCORPORATED AREA.



TRAFFIC DATA
U.S. ROUTE 52
2013 ADT = 3650
SPEED LIMIT = 35 TO 55 MPH

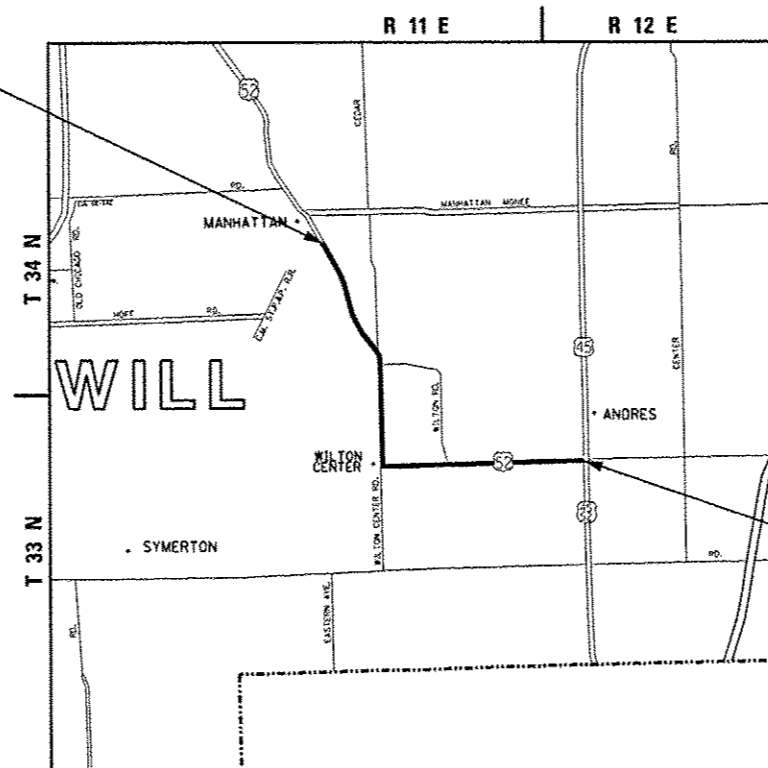


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

IMPROVEMENT BEGINS
STA. 12 + 73

OMISSION
STA. 98 + 70 TO STA. 100 + 38
STA. 270 + 24 TO STA. 272 + 12
STA. 424 + 93 TO STA. 425 + 14



IMPROVEMENT ENDS
STA. 494 + 45

MANHATTAN, WILTON, AND PEOTONE TOWNSHIPS
GROSS LENGTH = 48,172 FT. = 9.12 MILES
NET LENGTH = 47,795 FT. = 9.05 MILES

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60W12

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED May 29 2014

John Fortman, Jr.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Omur Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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LIST OF ILLINOIS DOT HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 442201-03 CLASS C AND D PATCHES
- 482011-03 HMA SHLD. STRIPS/SHLDS, WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
- 606001-05 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 635006-03 REFLECTOR AND TERMINAL MARKER PLACEMENT
- 635011-02 REFLECTOR MARKER AND MOUNTING DETAILS
- 701011-04 OFF-ROAD MOVING OPERATION, 2L, 2W, DAY ONLY
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-03 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701336-06 LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS >= 45 MPH
- 701901-03 TRAFFIC CONTROL DEVICES
- 780001-04 TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 or 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES. 48-HOUR NOTIFICATION IS REQUIRED.

TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF MANHATTAN.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC OPERATIONS ENGINEER AT (847) 705-4411 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT IN THE BUREAU OF MAINTENANCE AT (847) 705-4171 AT LEAST 2 WEEKS IN ADVANCE OF TREE REMOVAL WORK TO ASSIST WITH FIELD LAYOUT.

WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

WHEN CONSTRUCTION OPERATIONS ON TWO-LANE ROADS OPEN TO TRAFFIC RESULT IN THE REMOVAL OR COVERING OF ANY PAVEMENT STRIPING INDICATING PASSING RESTRICTIONS, "NO PASSING ZONES NOT STRIPED NEXT MILES" SIGNS SHALL BE USED. THE CONTRACTOR SHALL PLACE THE SIGNS AT THE BEGINNING OF THE UNSTRIPED AREA, JUST BEYOND EACH MAJOR INTERSECTION WITHIN THE UNSTRIPED AREA, AND AT SUCH OTHER LOCATIONS AS THE ENGINEER MAY DIRECT TO ENSURE A MINIMUM SPACING OF FIVE MILES.

THE SIGNS SHALL BE PLACED JUST PRIOR TO REMOVAL OR COVERING OF THE STRIPE AND SHALL REMAIN IN PLACE UNTIL FULL NO PASSING ZONE STRIPING HAS BEEN RESTORED. THIS WORK WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO EXTRA COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.'S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS:

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

FILE NAME	USER NAME - bskaram	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) INDEX OF SHEETS, LIST OF STATE STANDARDS & GENERAL NOTES	F.A. RFE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DATE	3/22/2014	DRAWN	REVISED		852	(17&18) RS-6	WILL	33	2	
PLLOT SCALE	1:100,000	CHECKED	REVISED		CONTRACT NO. 60W12					
PLLOT DATE	3/22/2014	DATE	REVISED		SCALE:	SHEET	OF	SHEETS	STA.	TO

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE				
				0005				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	20	20				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	180	180				
20101400	NITROGEN FERTILIZER NUTRIENT	POUND	3	3				
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3	3				
20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	3	3				
*21101645	TOPSOIL FURNISH AND PLACE, 12"	SQ YD	135	135				
*25000210	SEEDING, CLASS 2A	ACRE	0.03	0.03				
*25200110	SODDING, SALT TOLERANT	SQ YD	135	135				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), [L-4, 75, N50	TON	7413	7413				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	100	100				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	65	65				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	11,135	11,135				
44002208	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2"	SQ YD	635	635				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE				
				0005				
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	400	400				
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SQ YD	50	50				
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SQ YD	125	125				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1728	1728				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1				
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	30458	30458				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	78	78				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	132193	132193				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	284	284				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	261	261				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	470	470				

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Rev.

FILE NAME :	USER NAME :	DESIGNED :	REVISED :
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PLOT SCALE :	CHECKED :	DATE :	REVISED :
1/4" = 100'-0"	-	6/2/2014	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W12	

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE			
				0005			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	107	107			
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	78	78			
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	132193	132193			
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	284	284			
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	261	261			
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	470	470			
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	107	107			
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	925	925			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	555	555			
X0322052	ASPHALT REJUVENATING AGENT	GALLON	13,248	13,248			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	851	851			
X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	88507	88507			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE			
				0005			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	300	300			
Z0010619	HOT IN-PLACE RECYCLING	SQ YD	132,478	132,478			
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	1	1			
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4			
	SPECIALTY ITEMS						

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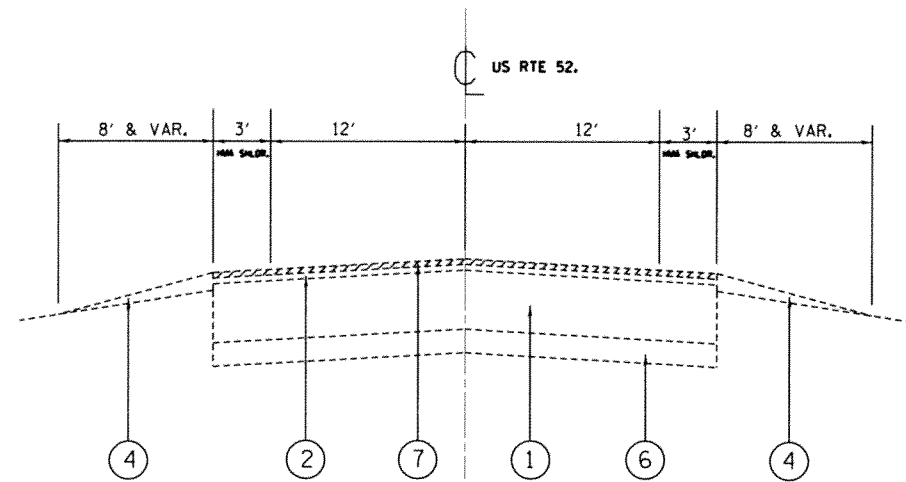
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.
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F.A.P. RATE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) R5-6	WILL	33	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W12	

Rex.

12

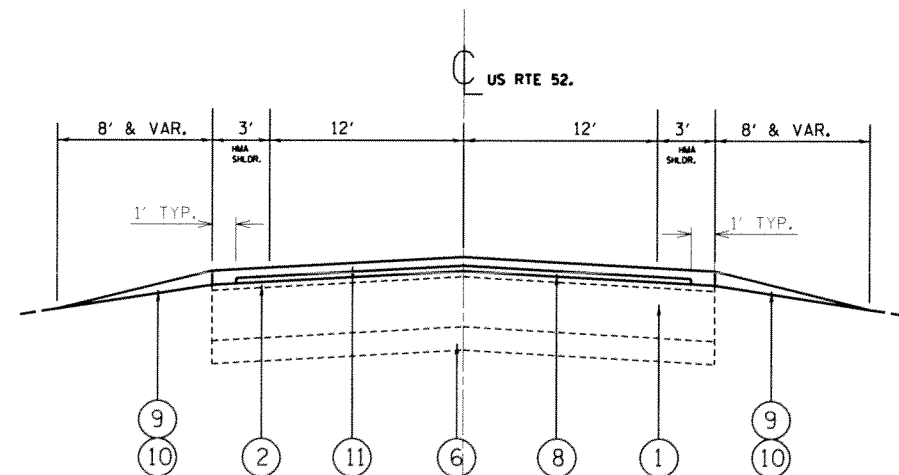


EXISTING TYPICAL SECTION
 STA 12+73 TO STA 162+07
 STA 167+76 TO STA 267+91
 STA 315+37 TO STA 489+37

LEGEND

- ① EXISTING PCC PAVEMENT +/- 8"
- ② EXISTING HMA RESURFACING TO REMAIN +/- 9" (AFTER HEATING AND SCARIFICATION)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING STABILIZED SUB-BASE
- ⑦ PROPOSED HOT IN-PLACE RECYCLING, 1"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑩ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-1/2"

R CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)



PROPOSED TYPICAL SECTION
 STA 12+73 TO STA 162+07
 STA 167+76 TO STA 267+91
 STA 315+37 TO STA 489+37

PATCH FIRST

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS AT N _{DES}	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)	4.0% @ 70 GYR	PPF
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	3.5% @ 50 GYR	QCP
CLASS D PATCHES, (HMA BINDER IL-19.0mm)	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PPF)		

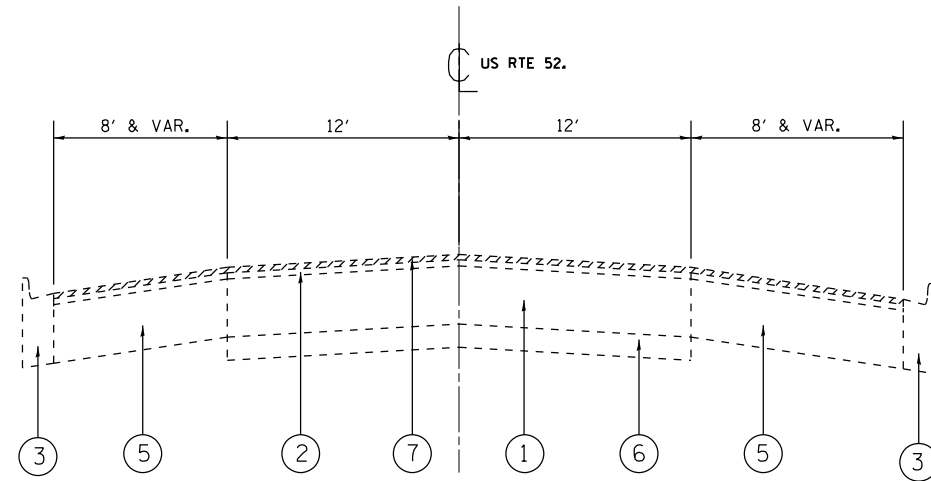
NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS./ SQ YD./IN.

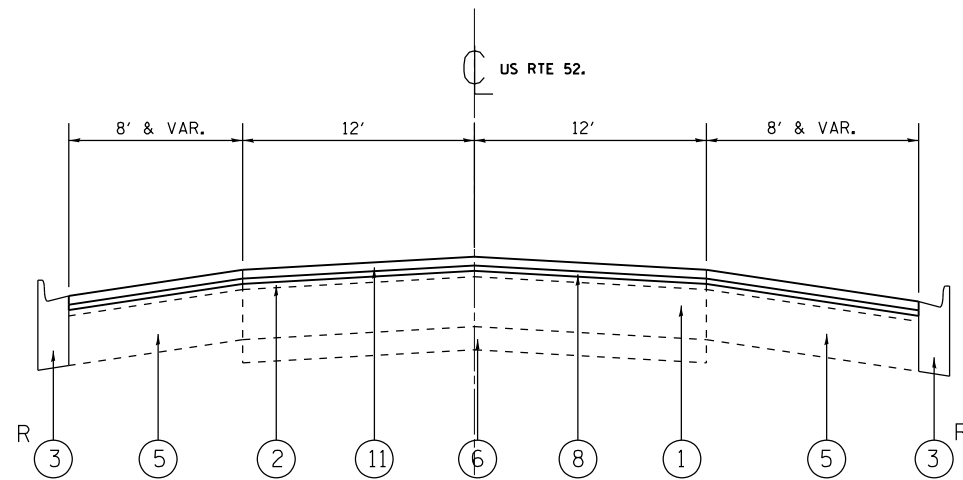
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



EXISTING TYPICAL SECTION
 STA 274+91 TO STA 315+37
 STA 286+52 TO STA 289+37
 STA 302+37 TO STA 315+37



PROPOSED TYPICAL SECTION
 STA 274+91 TO STA 315+37
 STA 286+52 TO STA 289+37
 STA 302+37 TO STA 315+37

LEGEND

- ① EXISTING PCC PAVEMENT +/- 8"
- ② EXISTING HMA RESURFACING TO REMAIN +/- 9" (AFTER HEATING AND SCARIFICATION)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING STABILIZED SUB-BASE
- ⑦ PROPOSED HOT IN-PLACE RECYCLING, 1"
- ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑩ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-1/2"

R CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)

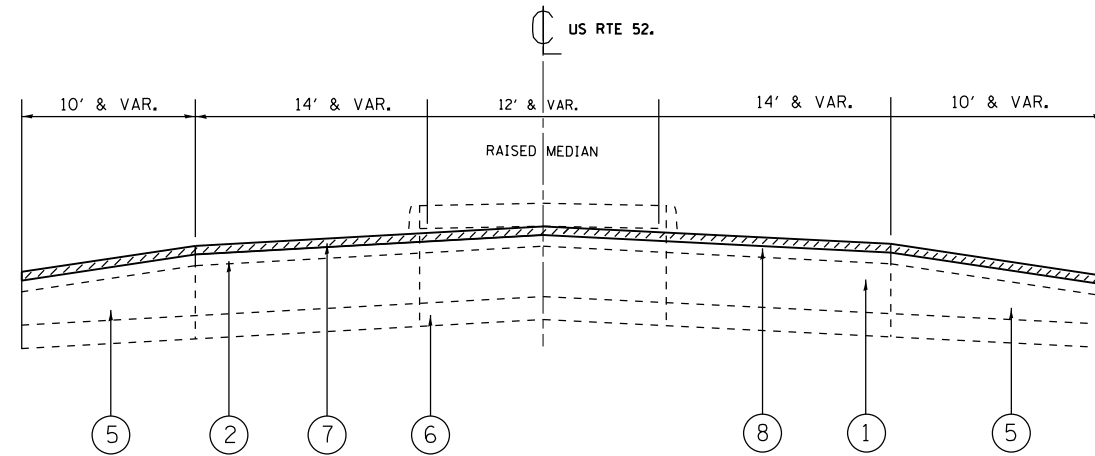
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	PLOT DATE = 6/2/2014	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 52 (2ND STREET TO US 45)
 EXISTING AND PROPOSED TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

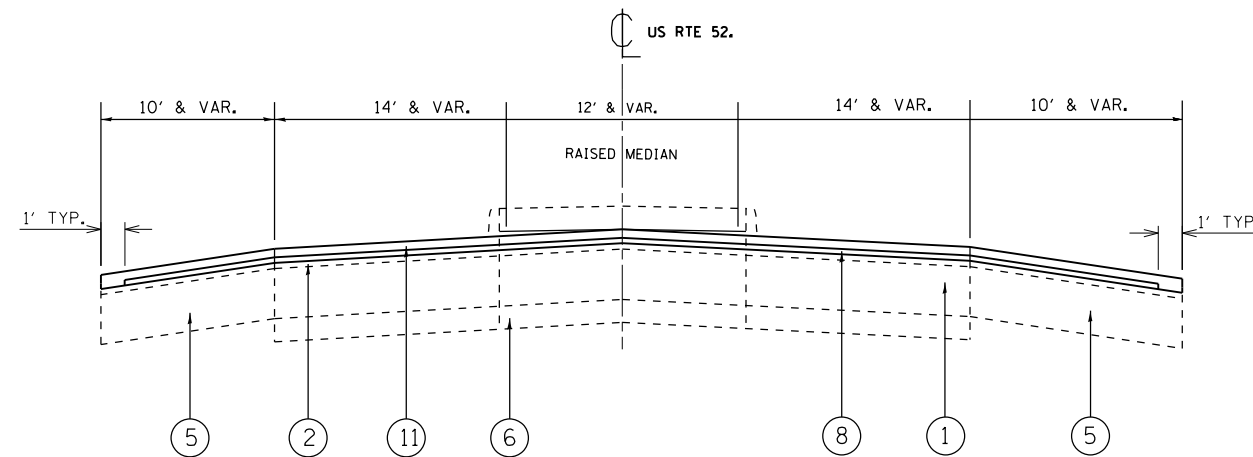
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W12	



EXISTING TYPICAL SECTION
STA 489+37 TO STA 494+45

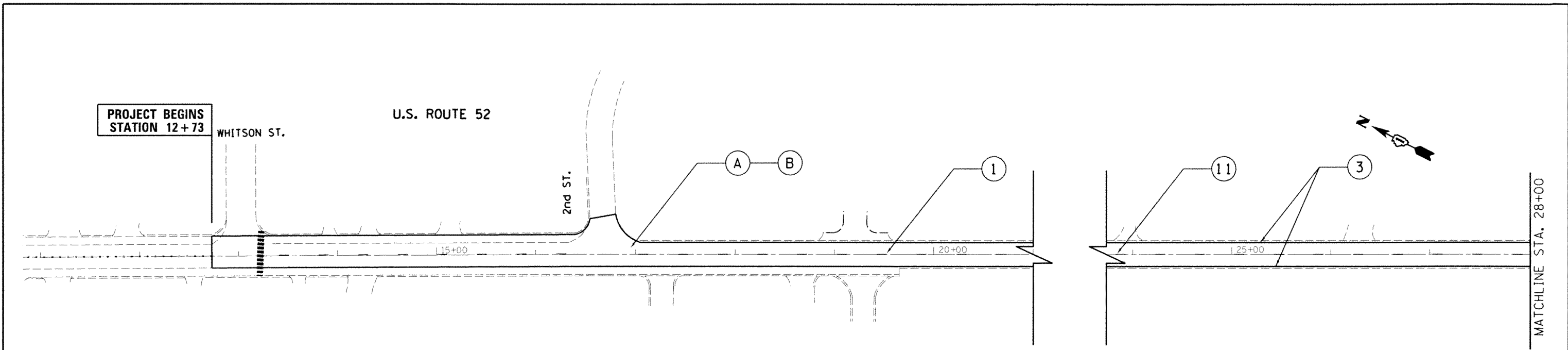
- LEGEND**
- ① EXISTING PCC PAVEMENT +/- 8"
 - ② EXISTING HMA RESURFACING TO REMAIN +/- 9" (AFTER HEATING AND SCARIFICATION)
 - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ④ EXISTING AGGREGATE SHOULDER
 - ⑤ EXISTING HMA SHOULDER
 - ⑥ EXISTING STABILIZED SUB-BASE
 - ⑦ PROPOSED HOT IN-PLACE RECYCLING, 1"
 - ⑧ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
 - ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑩ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
 - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-1/2"

R CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)



PROPOSED TYPICAL SECTION
STA 489+37 TO STA 494+45

FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	13-sh1-plan.dgn	DRAWN -	REVISED -			852	(17&18) RS-6	WILL	33	7	
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -			CONTRACT NO. 60W12					
	PLOT DATE = 6/2/2014	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO
ILLINOIS FED. AID PROJECT											



RESURFACING LEGEND

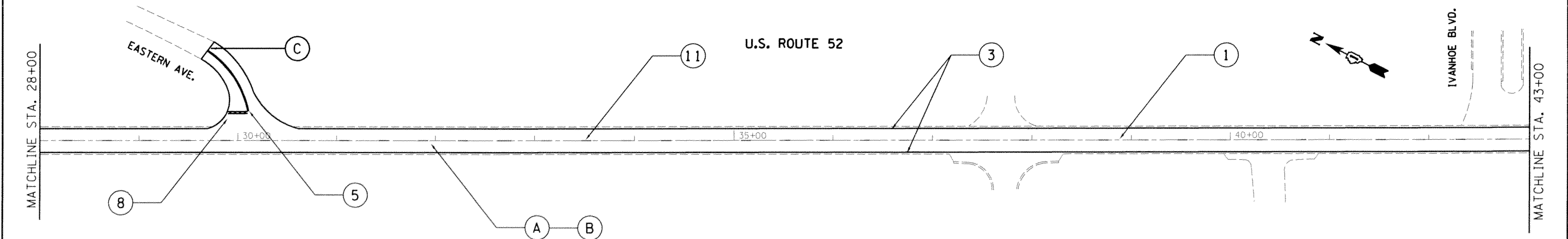
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (B032)

NOTES

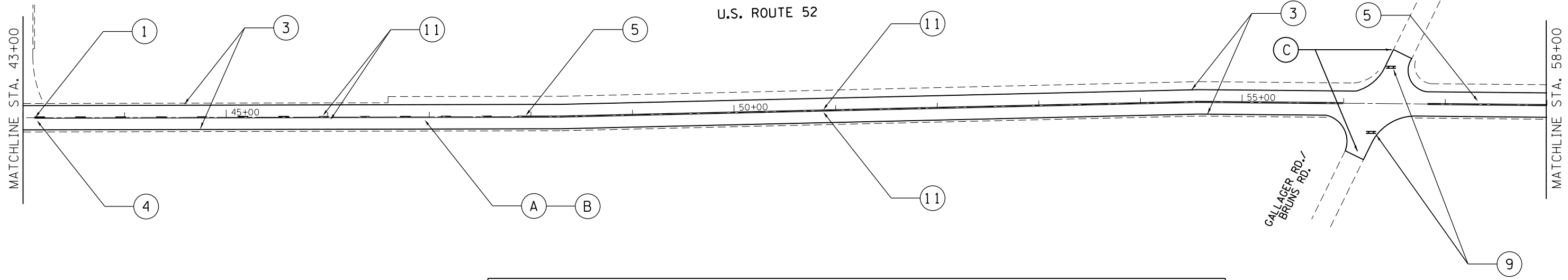
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.

PAVEMENT MARKING LEGEND

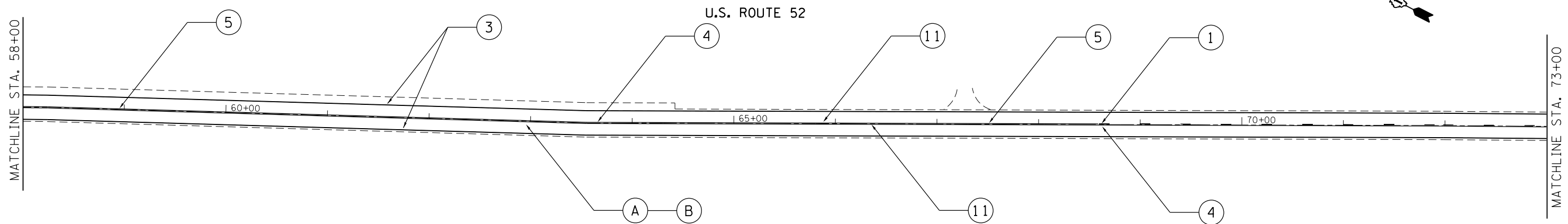
- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
- (3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
- (4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
- (5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
- (6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
- (7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
- (8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
- (9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)



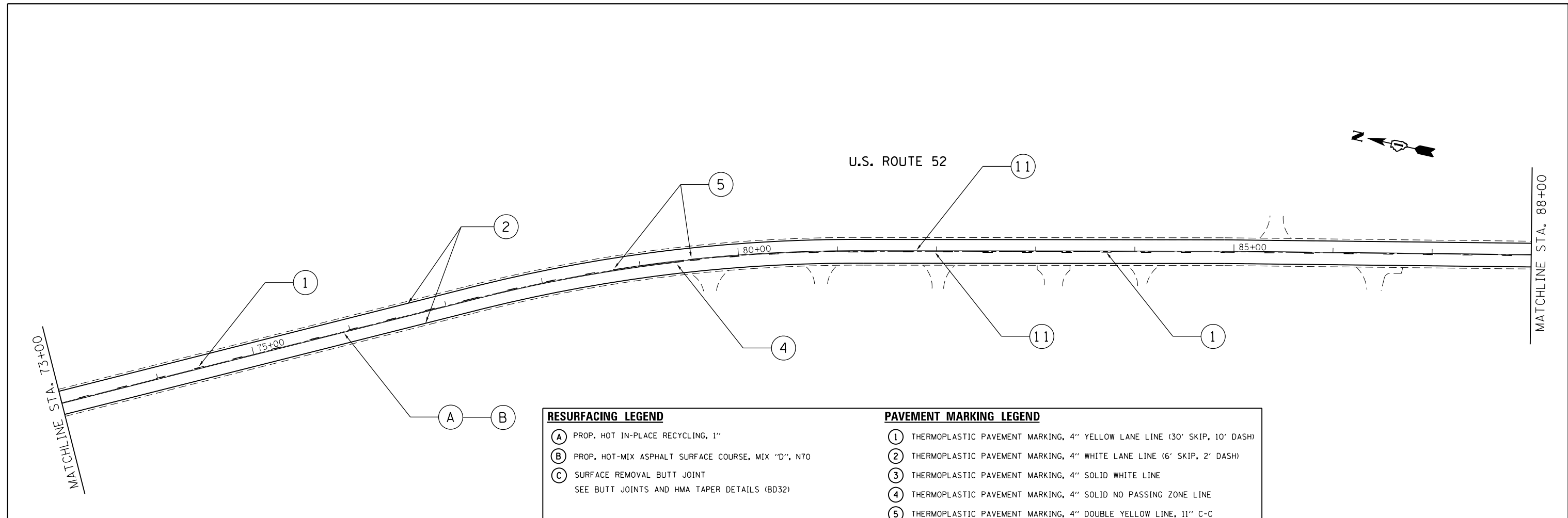
FILE NAME =	USER NAME = changj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN		F.A.P. RTE. 852	SECTION (17&18) RS-6	COUNTY WILL	TOTAL SHEETS 33	SHEET NO. 8	
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
	PLOT DATE = 7/29/2014	DATE -	REVISED -									



RESURFACING LEGEND	PAVEMENT MARKING LEGEND
(A) PROP. HOT IN-PLACE RECYCLING, 1"	(1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
(B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	(2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
(C) SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)	(3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
	(4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
	(5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
	(6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
NOTES	(7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.	(8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.	(9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
	(10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
	(11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			852	(17&18) RS-6	WILL	33	9
	PLOT DATE = 6/2/2014	CHECKED -	REVISED -			CONTRACT NO. 60W12				
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE: 1"=50'	SHEET OF SHEETS		STA. TO STA.		

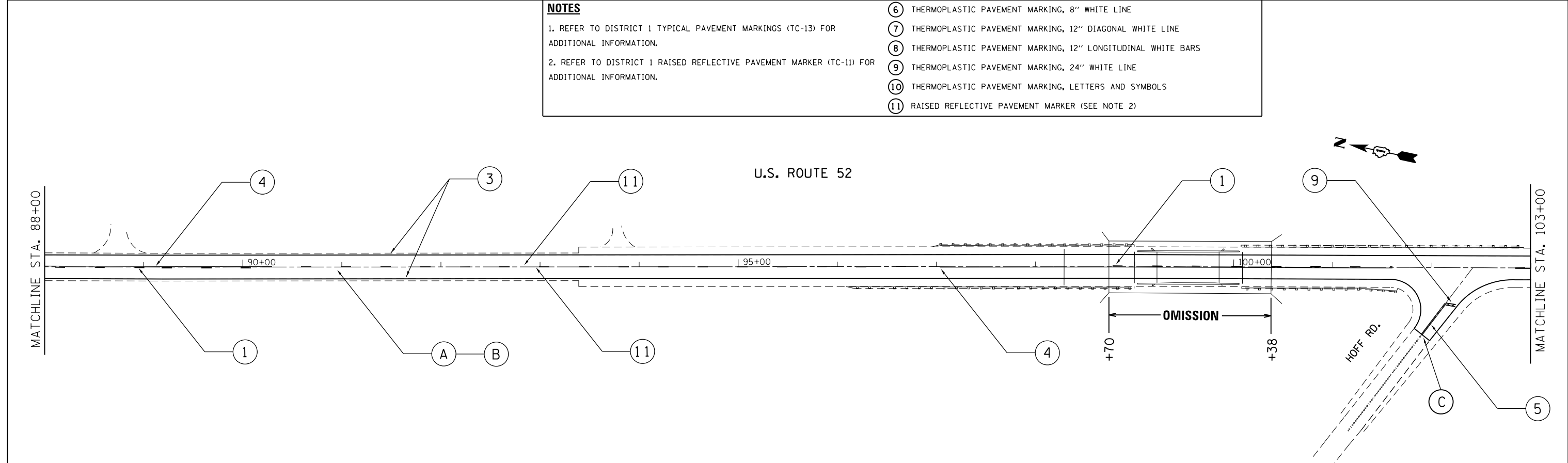


RESURFACING LEGEND	PAVEMENT MARKING LEGEND
(A) PROP. HOT IN-PLACE RECYCLING, 1"	(1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
(B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	(2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
(C) SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)	(3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
	(4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
	(5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
	(6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
	(7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
	(8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
	(9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
	(10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
	(11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

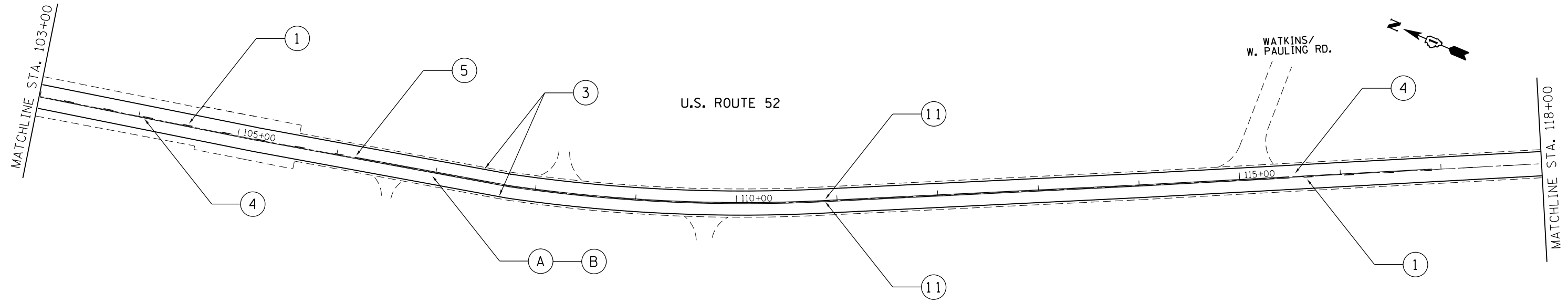
NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.

2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			852	(17&18) RS-6	WILL	33	10
	PLOT DATE = 6/2/2014	CHECKED -	REVISED -			CONTRACT NO. 60W12				
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



RESURFACING LEGEND

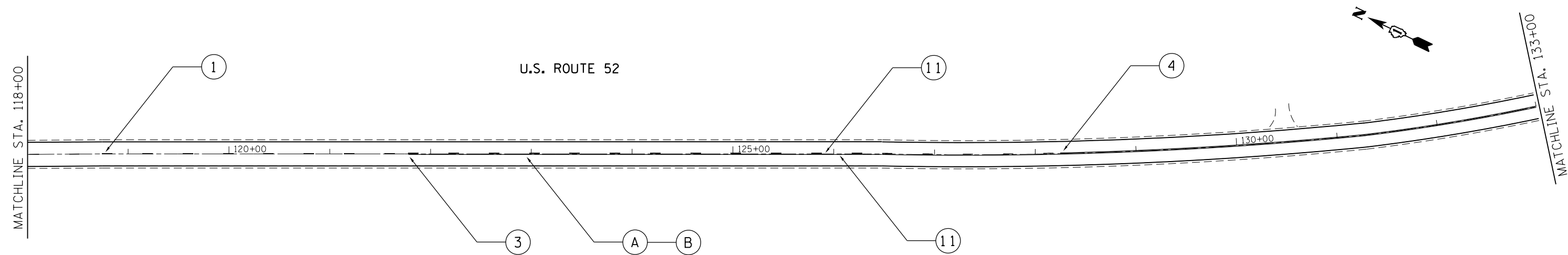
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

PAVEMENT MARKING LEGEND

- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
- (3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
- (4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
- (5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
- (6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
- (7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
- (8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
- (9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.



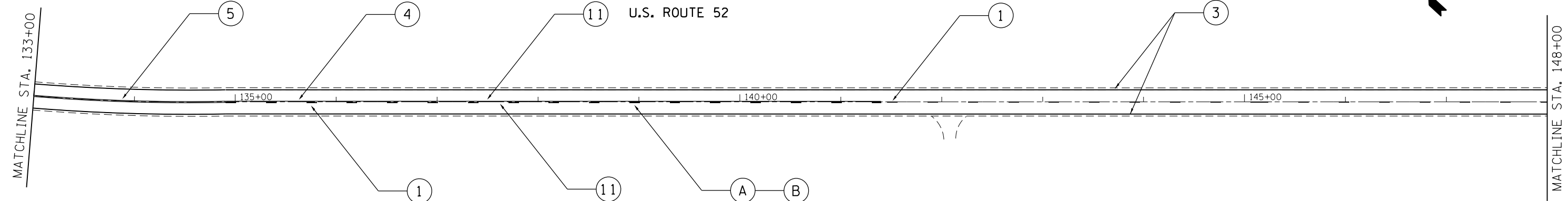
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	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	11
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				

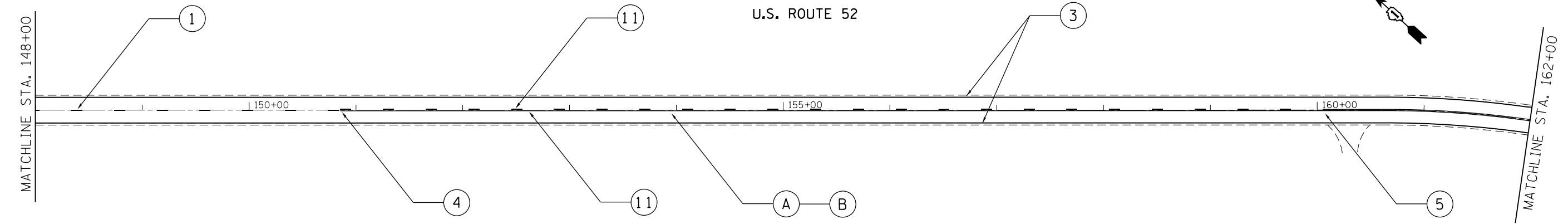


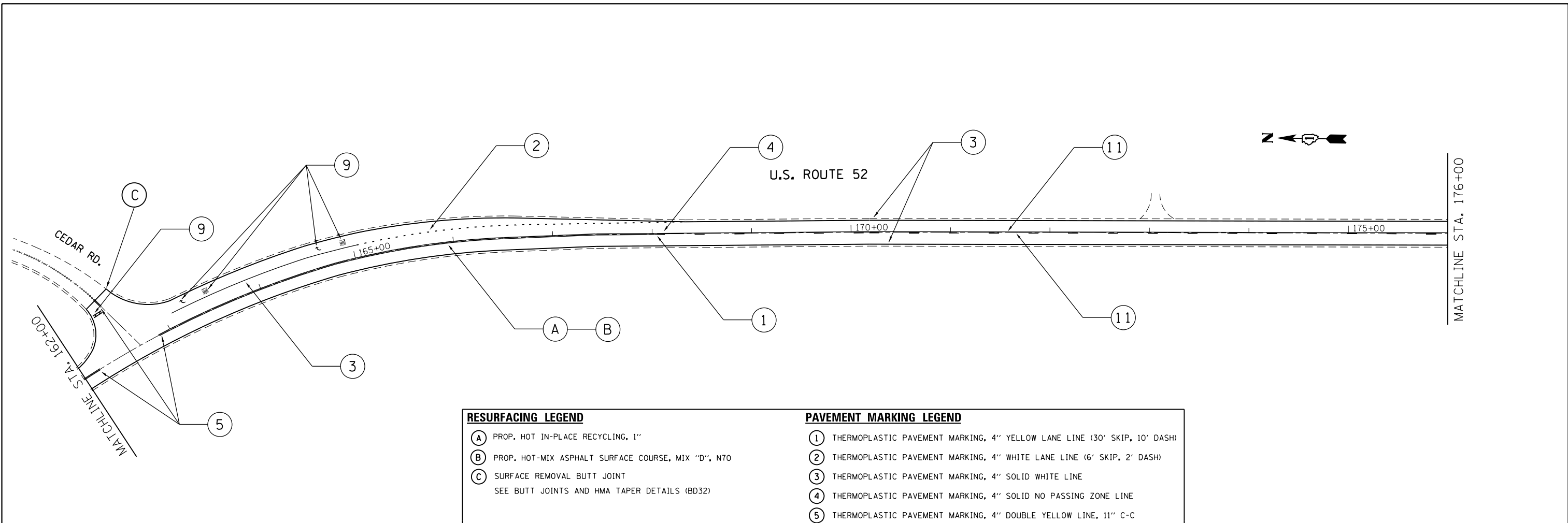
RESURFACING LEGEND	PAVEMENT MARKING LEGEND
(A) PROP. HOT IN-PLACE RECYCLING, 1"	(1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
(B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	(2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
(C) SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)	(3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
	(4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
	(5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
	(6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
	(7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
	(8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
	(9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
	(10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
	(11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.

2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.





RESURFACING LEGEND

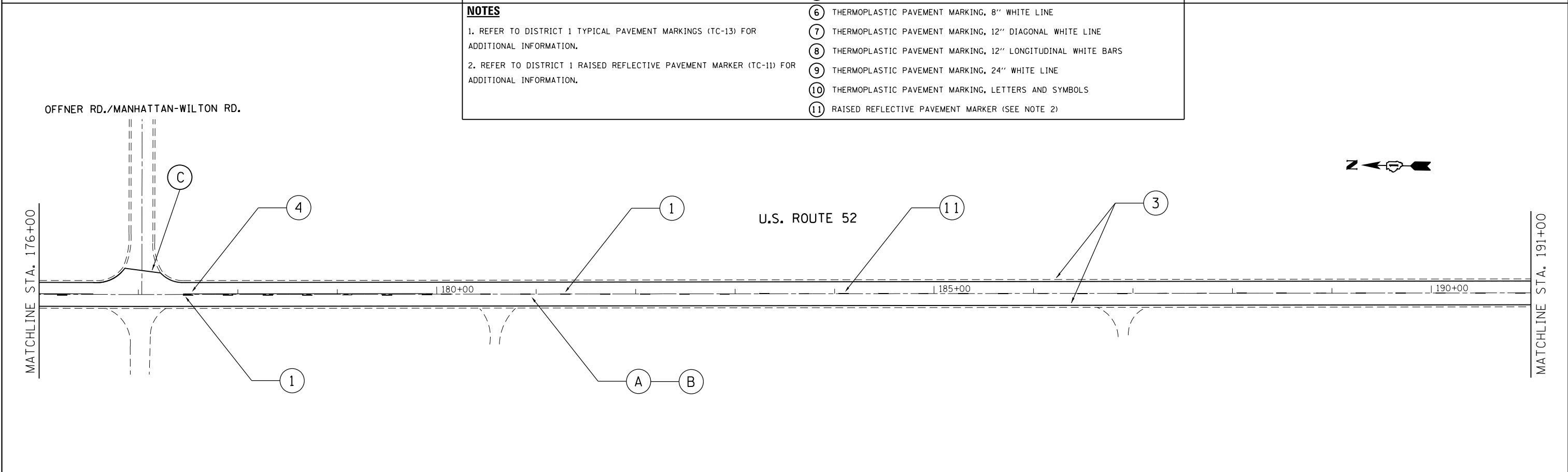
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

PAVEMENT MARKING LEGEND

- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
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- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES

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2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

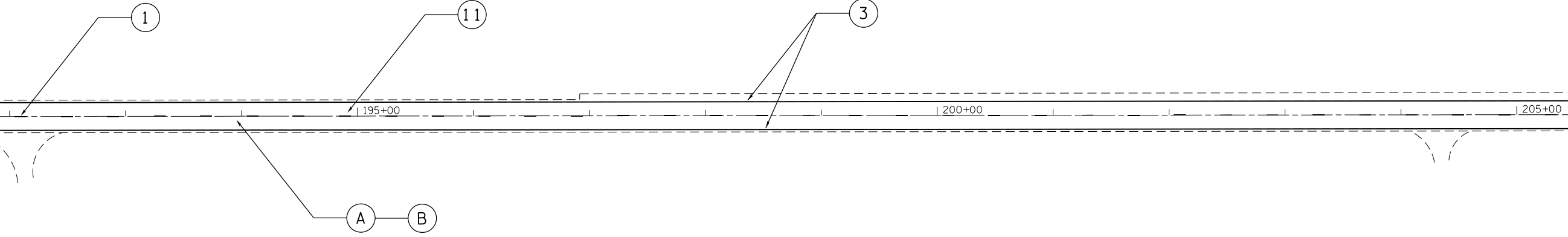
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	13
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				

U.S. ROUTE 52



MATCHLINE STA. 191+00

MATCHLINE STA. 206+00



RESURFACING LEGEND

- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

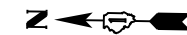
NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.

PAVEMENT MARKING LEGEND

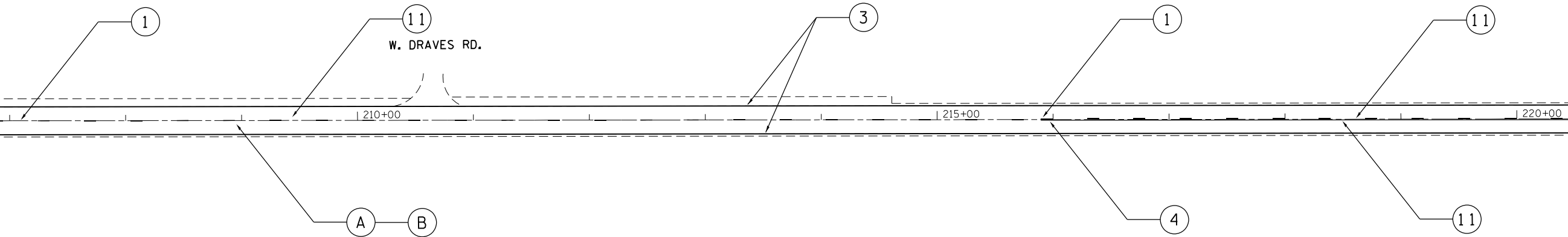
- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
- (3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
- (4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
- (5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
- (6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
- (7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
- (8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
- (9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

U.S. ROUTE 52



MATCHLINE STA. 206+00

MATCHLINE STA. 221+00



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

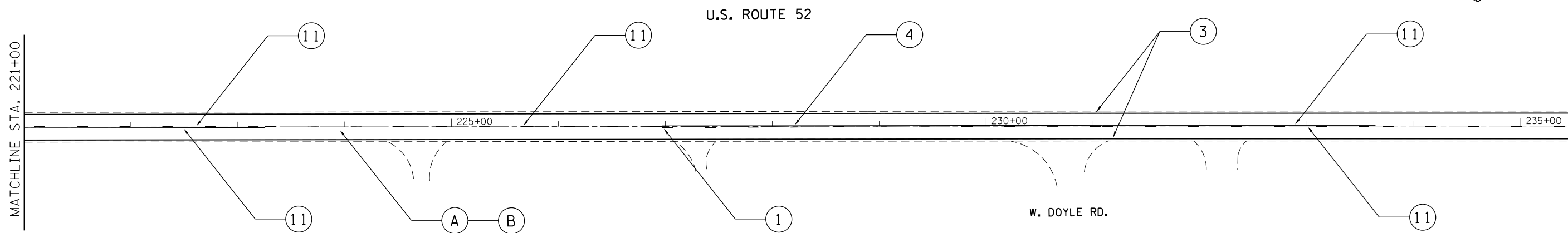
**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	14
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 221+00

MATCHLINE STA. 236+00



RESURFACING LEGEND

- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

NOTES

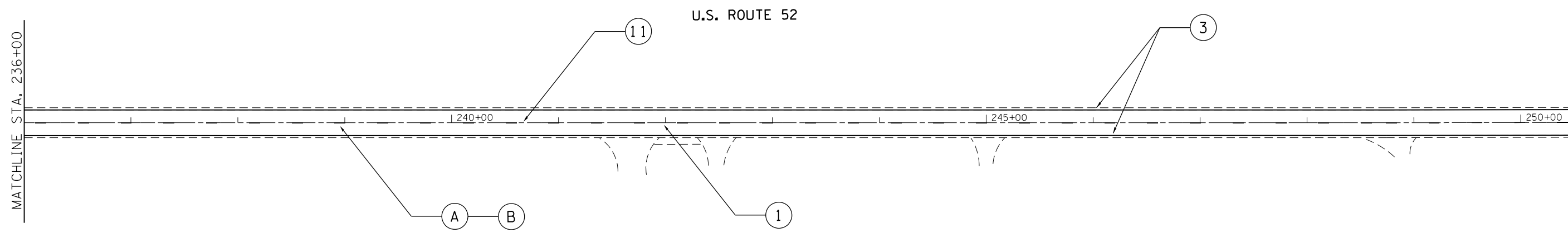
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.

PAVEMENT MARKING LEGEND

- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
- (3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
- (4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
- (5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
- (6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
- (7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
- (8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
- (9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

MATCHLINE STA. 236+00

MATCHLINE STA. 251+00



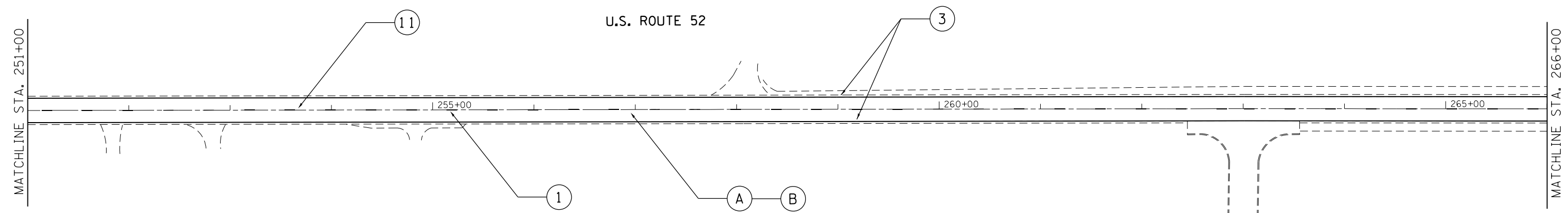
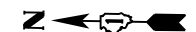
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/2/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

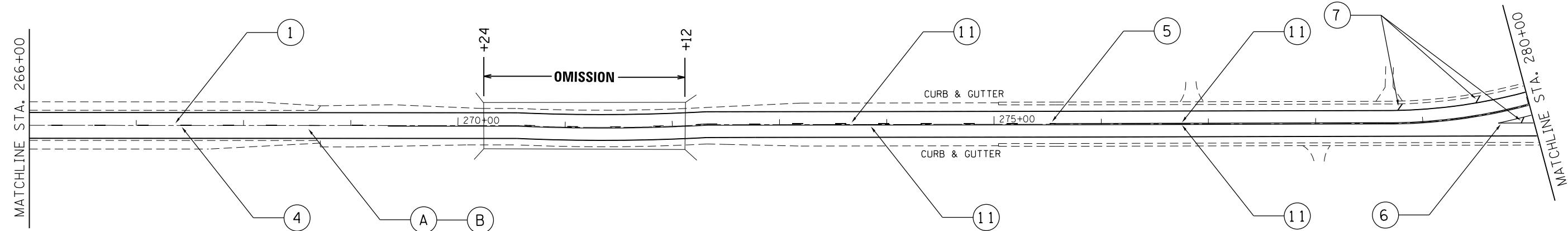
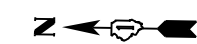
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	15
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				



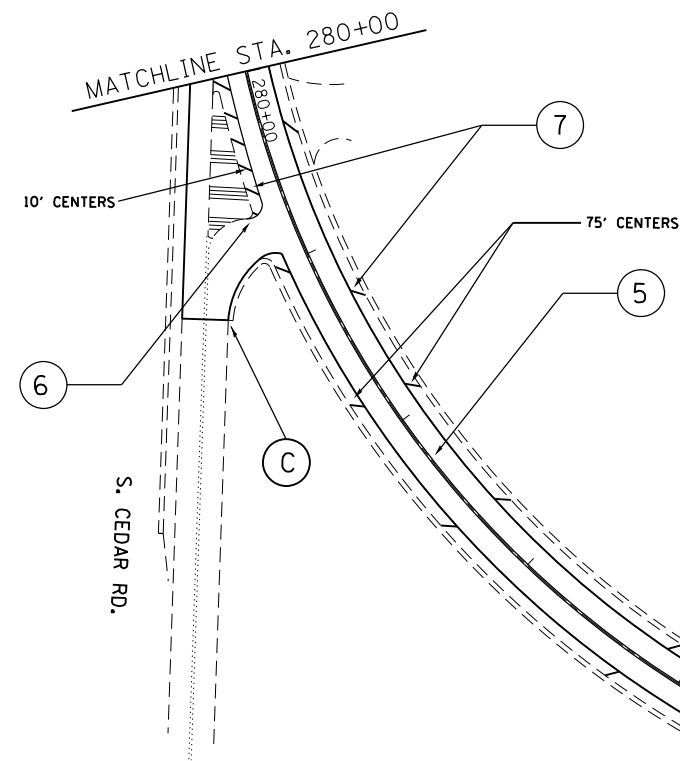
RESURFACING LEGEND	PAVEMENT MARKING LEGEND
(A) PROP. HOT IN-PLACE RECYCLING, 1"	(1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
(B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	(2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
(C) SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)	(3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
	(4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
	(5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
	(6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
	(7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
	(8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
	(9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
	(10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
	(11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES
 1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.

U.S. ROUTE 52



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -			852	(17&18) RS-6	WILL	33	16	
	PLOT DATE = 6/2/2014	CHECKED -	REVISED -			CONTRACT NO. 60W12					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



RESURFACING LEGEND

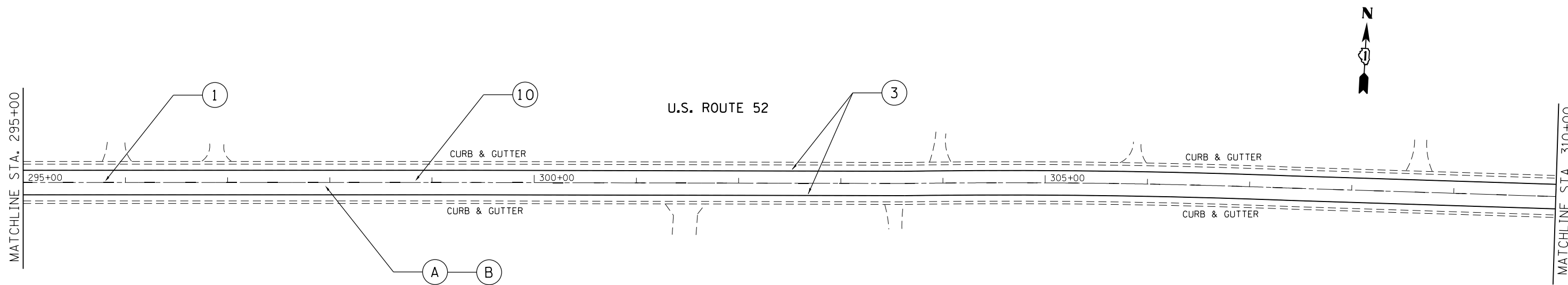
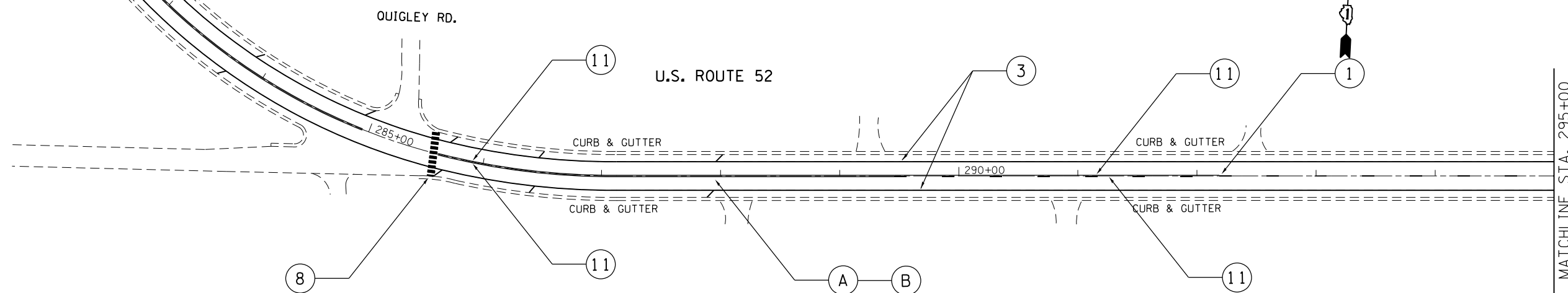
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.

PAVEMENT MARKING LEGEND

- (1) THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
- (2) THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
- (3) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
- (4) THERMOPLASTIC PAVEMENT MARKING, 4" SOLID NO PASSING ZONE LINE
- (5) THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE, 11" C-C
- (6) THERMOPLASTIC PAVEMENT MARKING, 8" WHITE LINE
- (7) THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL WHITE LINE
- (8) THERMOPLASTIC PAVEMENT MARKING, 12" LONGITUDINAL WHITE BARS
- (9) THERMOPLASTIC PAVEMENT MARKING, 24" WHITE LINE
- (10) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (11) RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)



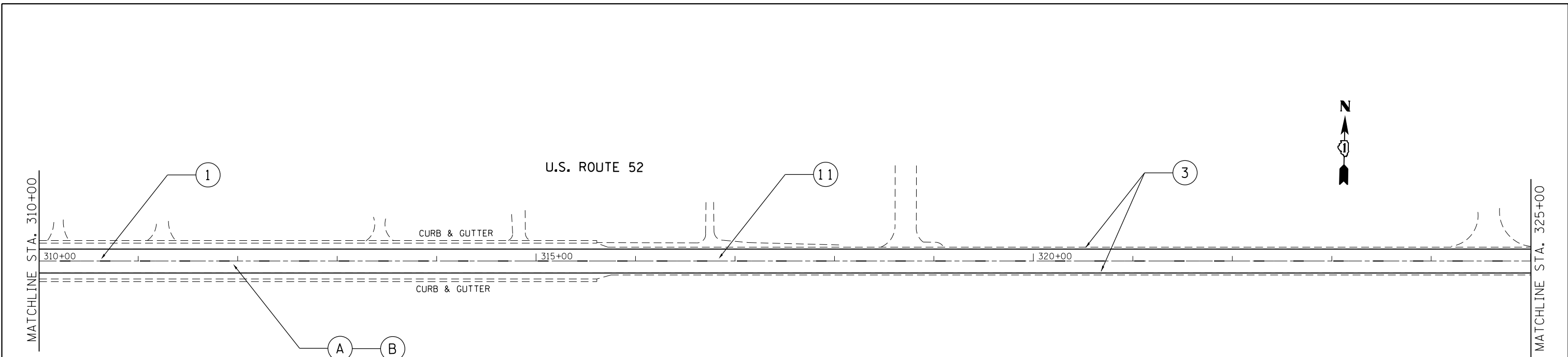
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	17
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				



RESURFACING LEGEND

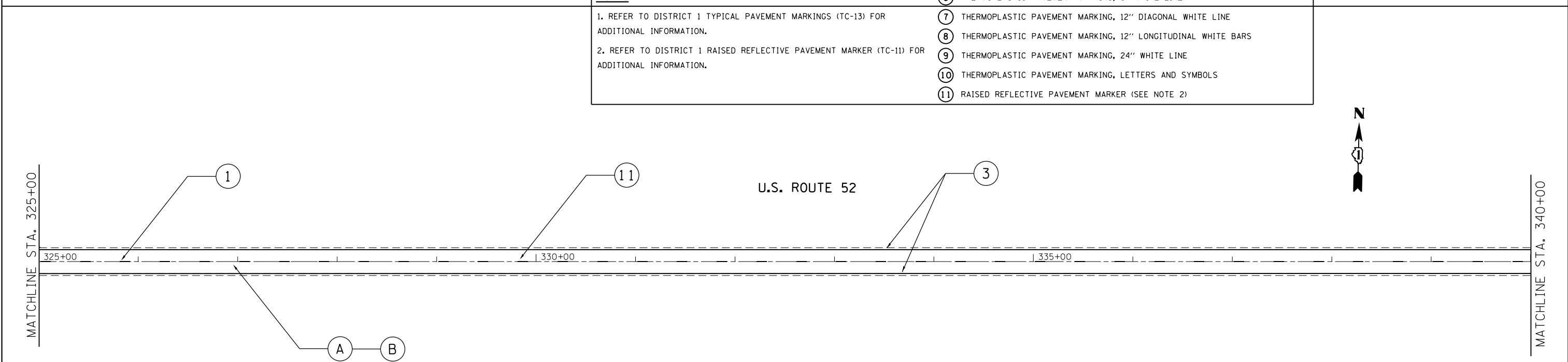
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
- (B) PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (C) SURFACE REMOVAL BUTT JOINT
SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

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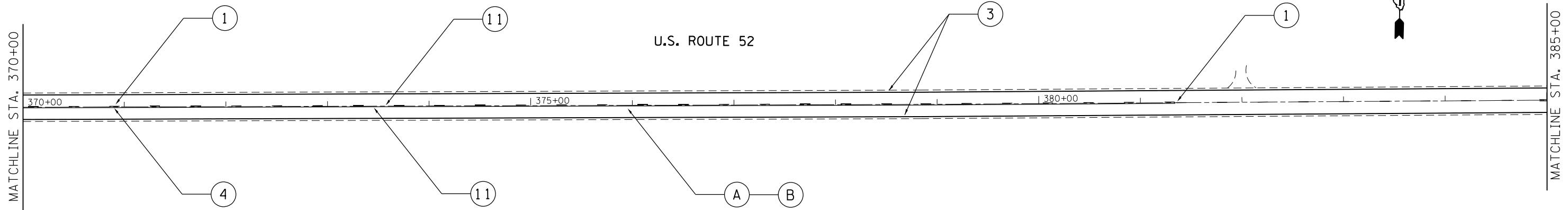
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	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	18
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				

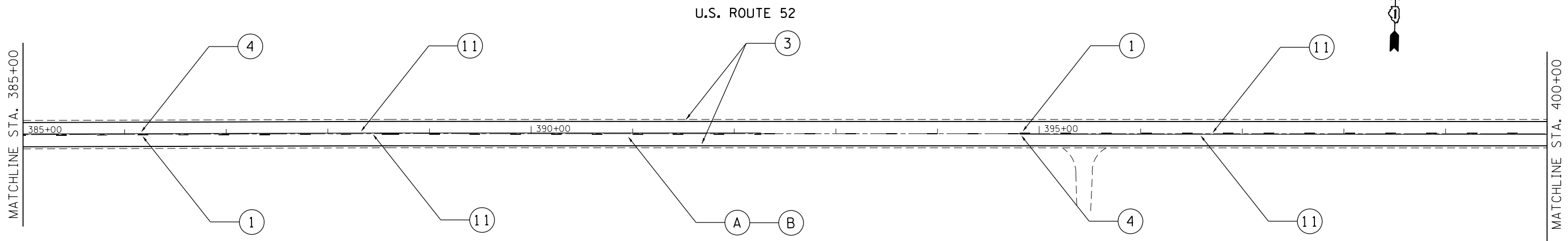


RESURFACING LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. HOT IN-PLACE RECYCLING, 1"	(1)	THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW LANE LINE (30' SKIP, 10' DASH)
(B)	PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	(2)	THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' SKIP, 2' DASH)
(C)	SURFACE REMOVAL BUTT JOINT SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)	(3)	THERMOPLASTIC PAVEMENT MARKING, 4" SOLID WHITE LINE
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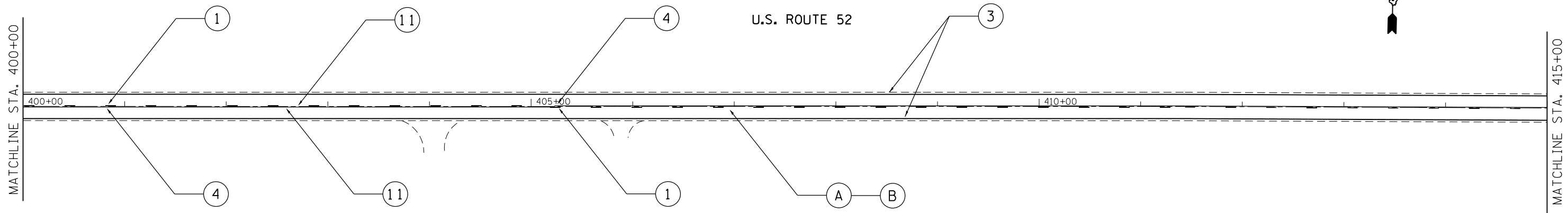
NOTES

1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.

2. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKER (TC-11) FOR ADDITIONAL INFORMATION.



FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.				
Default	et:\pw\work\p\idot\bakermo\d0331242\0118313-sh1-plan.dgn	DRAWN -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	852	(17&18) RS-6	WILL	33	20
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -									CONTRACT NO. 60W12				
	PLOT DATE = 6/2/2014	DATE -	REVISED -									ILLINOIS FED. AID PROJECT				



RESURFACING LEGEND

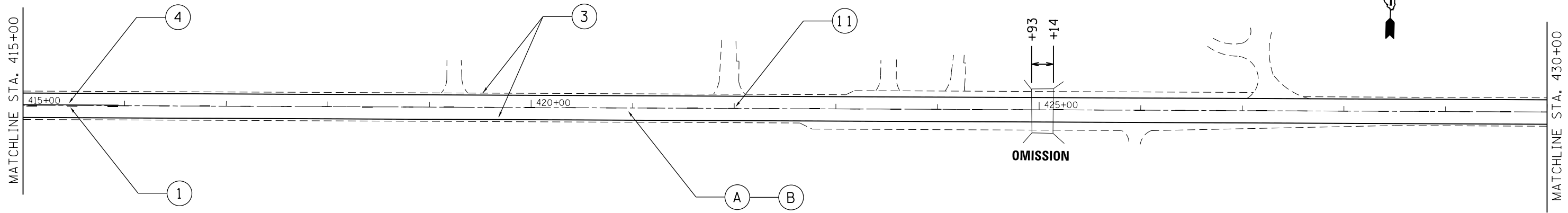
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
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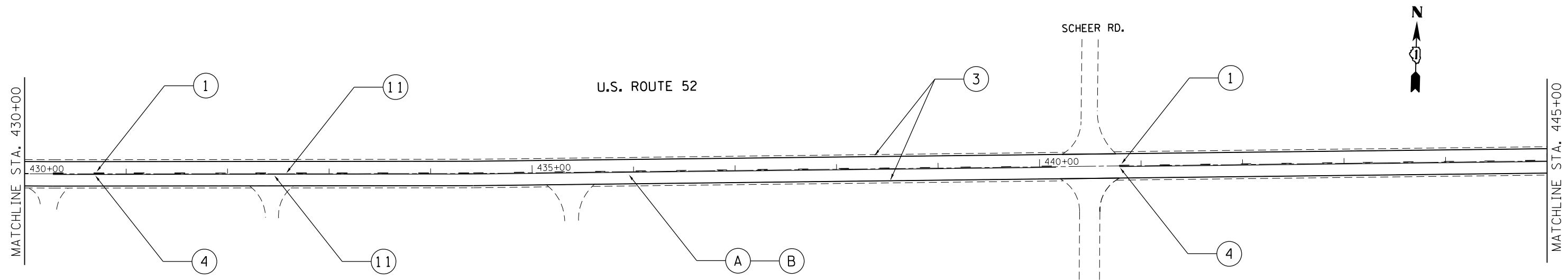
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	21
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				



RESURFACING LEGEND

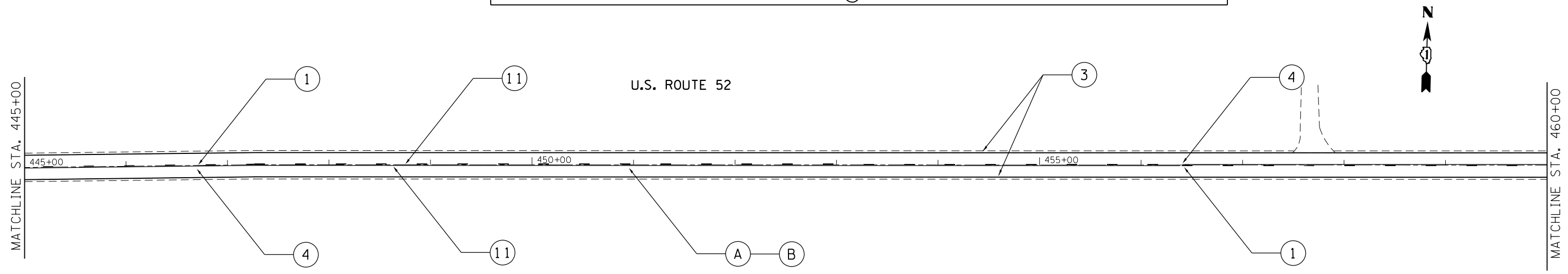
- (A) PROP. HOT IN-PLACE RECYCLING, 1"
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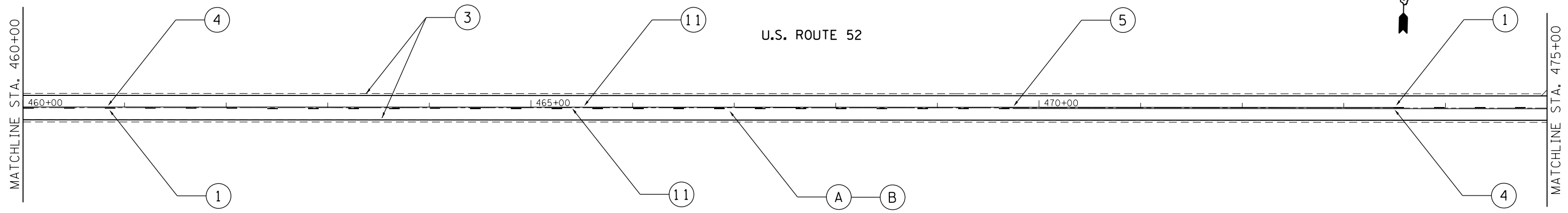
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	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	22
CONTRACT NO. 60W12				
ILLINOIS FED. AID PROJECT				

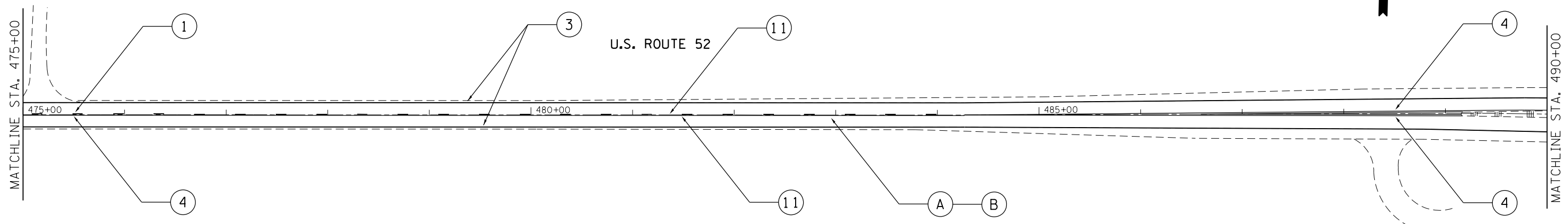


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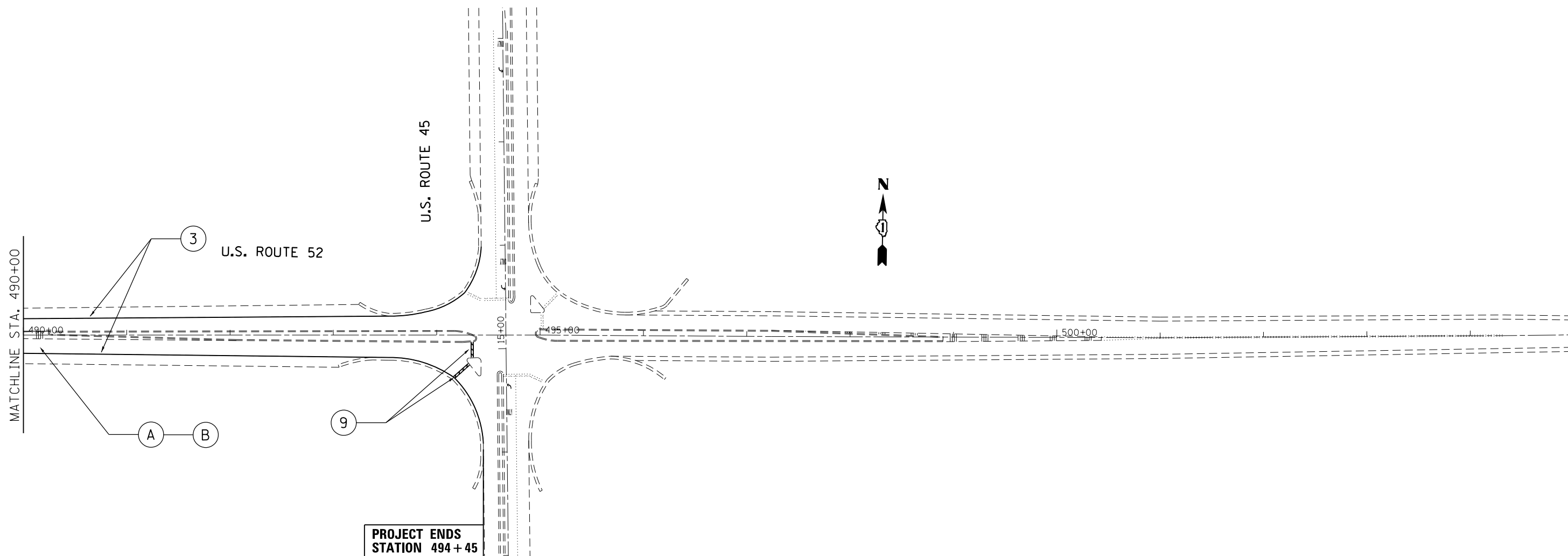
NOTES

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FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 52 (2ND STREET TO US 45) ROADWAY & PAVEMENT MARKING PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60W12							
	PLOT DATE = 6/2/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



RESURFACING LEGEND

- (A) PROP. HOT IN-PLACE RECYCLING, 1"
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SEE BUTT JOINTS AND HMA TAPER DETAILS (BD32)

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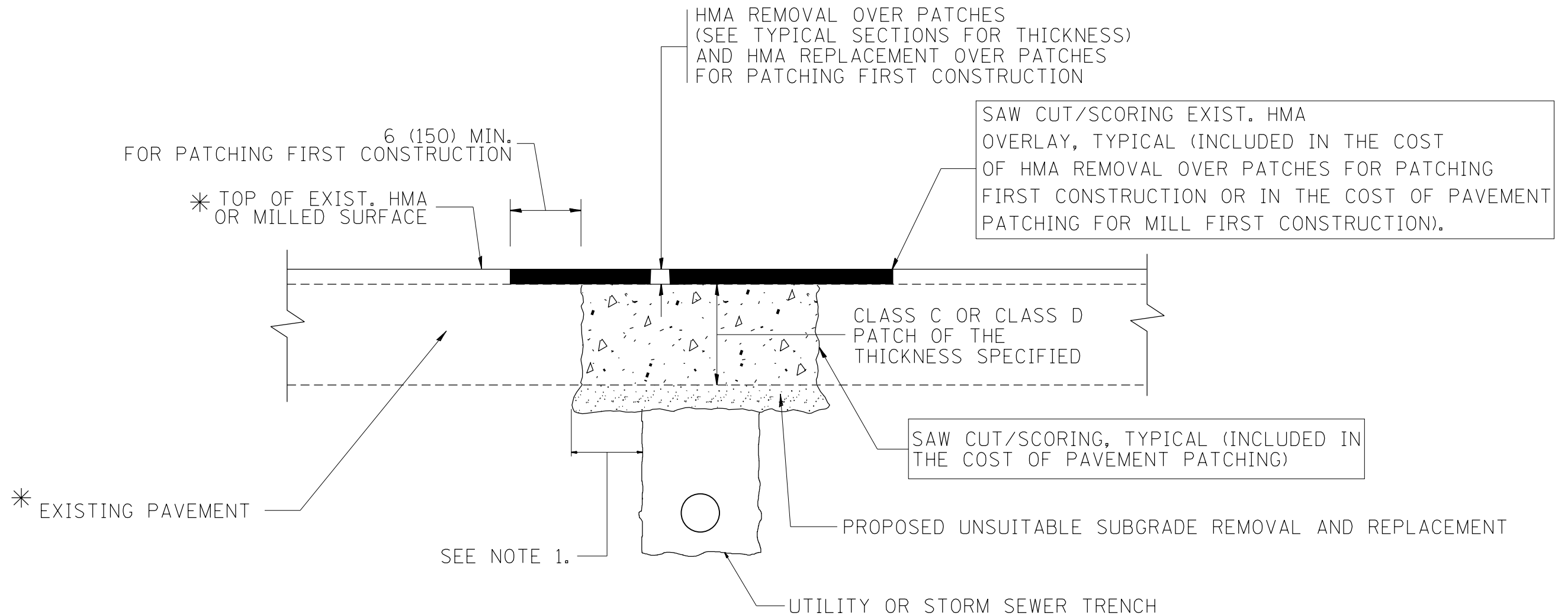
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/2/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 52 (2ND STREET TO US 45)
ROADWAY & PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W12	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bakerom	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
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	PLOT DATE = 6/2/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	25
BD400-04 (BD-22)			CONTRACT NO. 60W12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

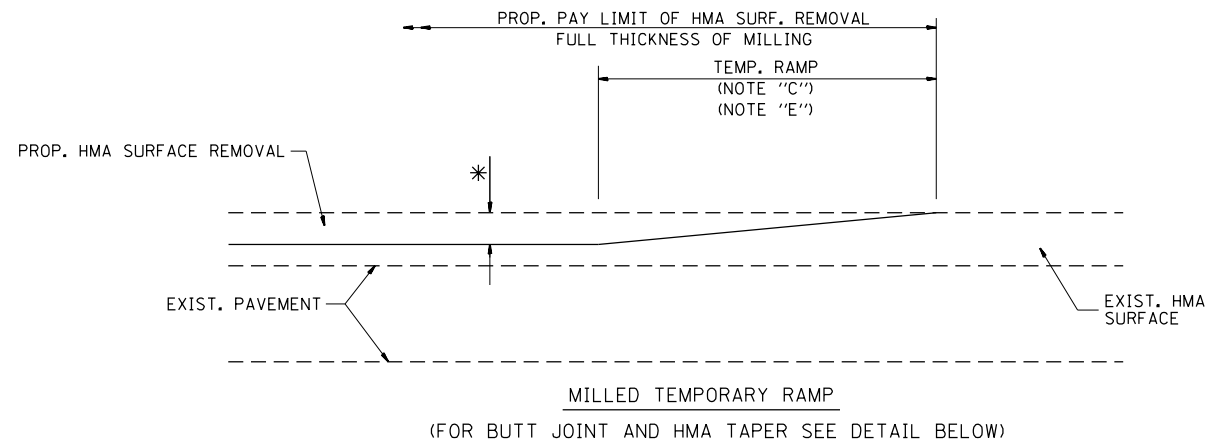
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

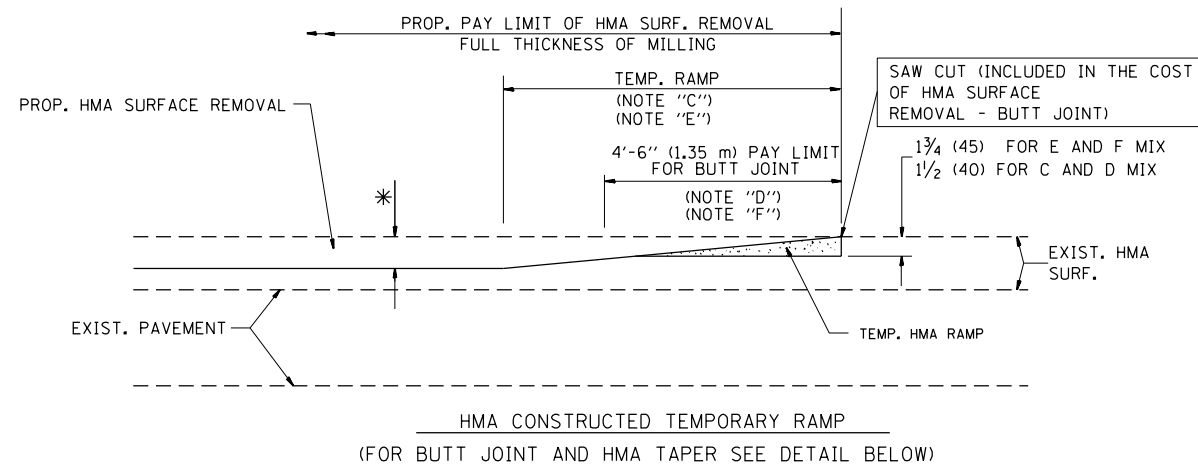
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bakerom	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p1dot\bakerom\d0331242\011813-shit-plan.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			852	(17&18) RS-6	WILL	33	26
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 60W12		
PLOT DATE = 6/2/2014	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

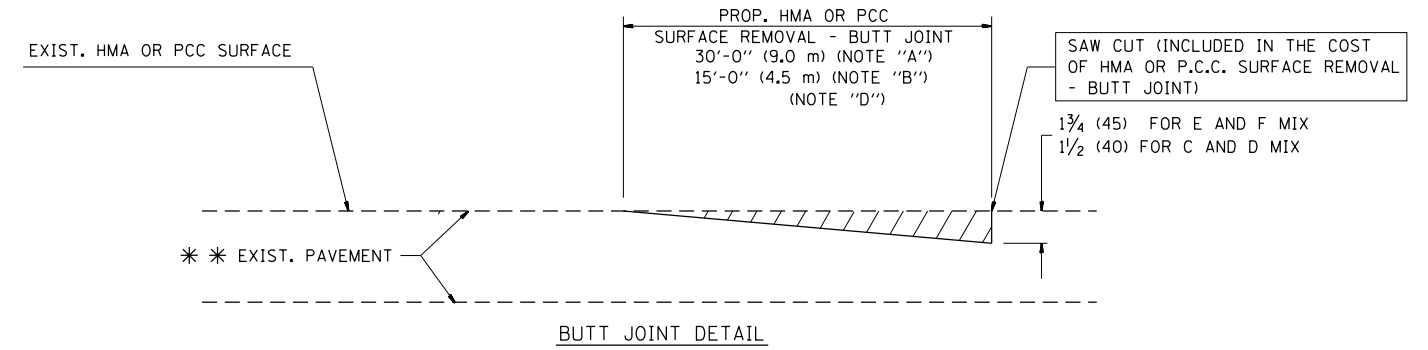


OPTION 1

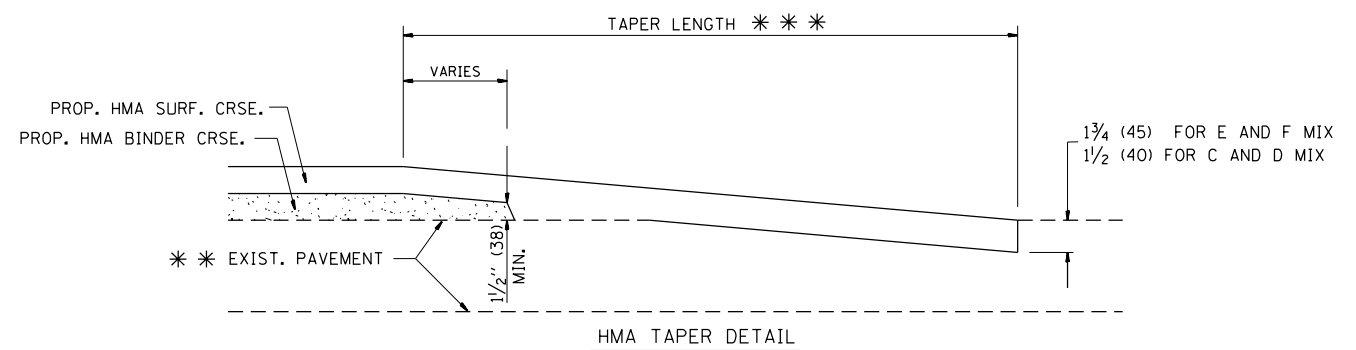


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

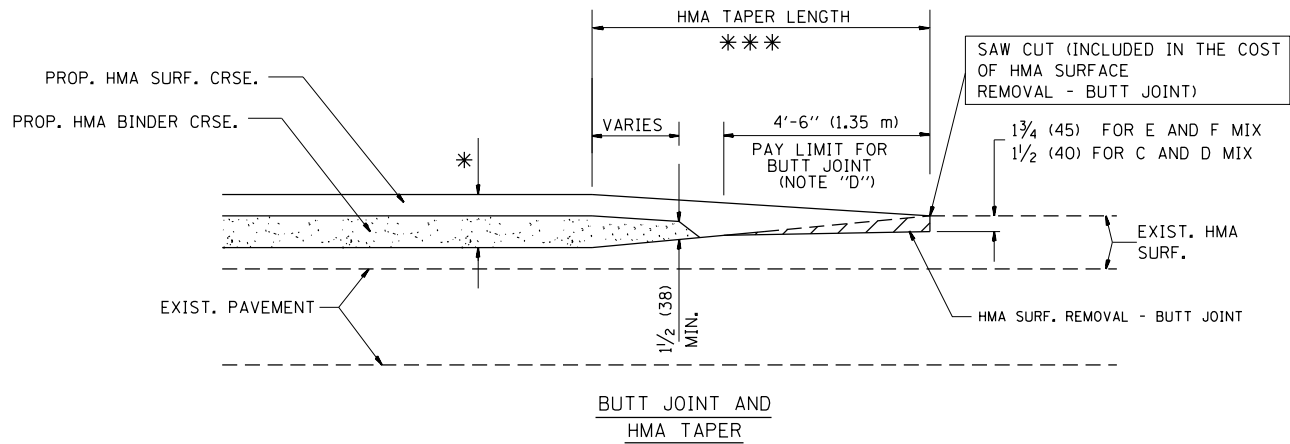
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

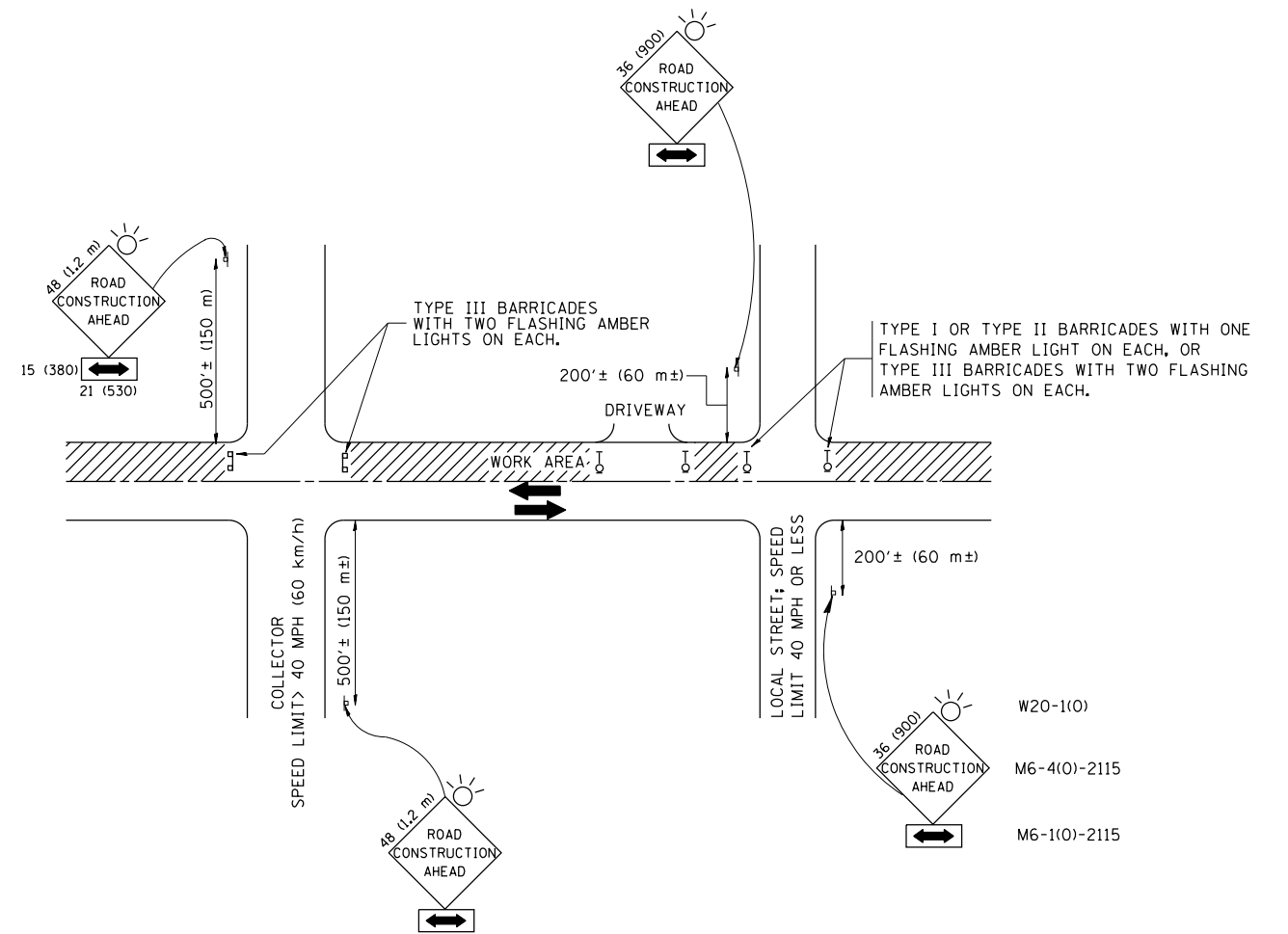
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = Bakerom	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\bakerom\d0331242\0118313-shit-plan.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			852	(17&18) RS-6	WILL	33	27	
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - M. GOMEZ 04-06-01			BD400-05 BD32		CONTRACT NO. 60W12			
PLOT DATE = 6/2/2014		DATE - 06-13-90	REVISED - R. BORO 01-01-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

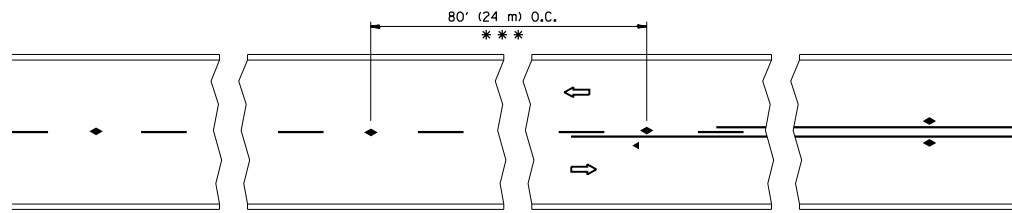
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 6/2/2014	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

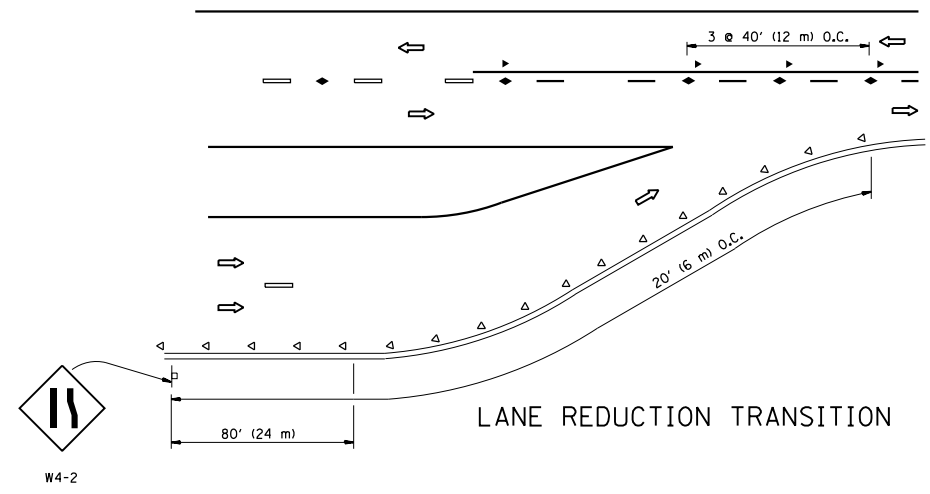
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60W12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

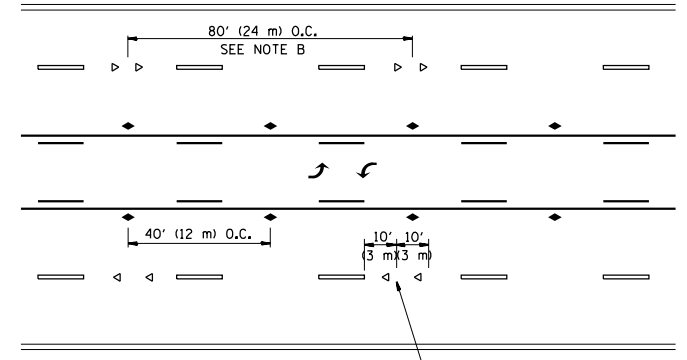


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

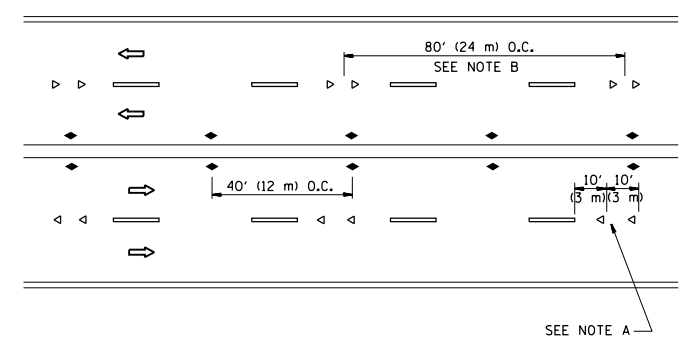
TWO-LANE/TWO-WAY



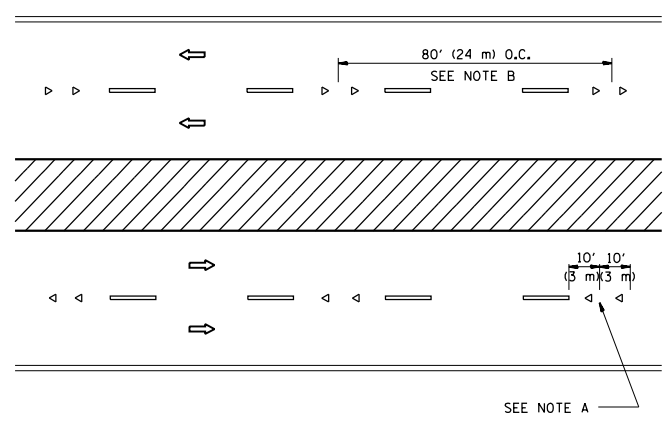
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

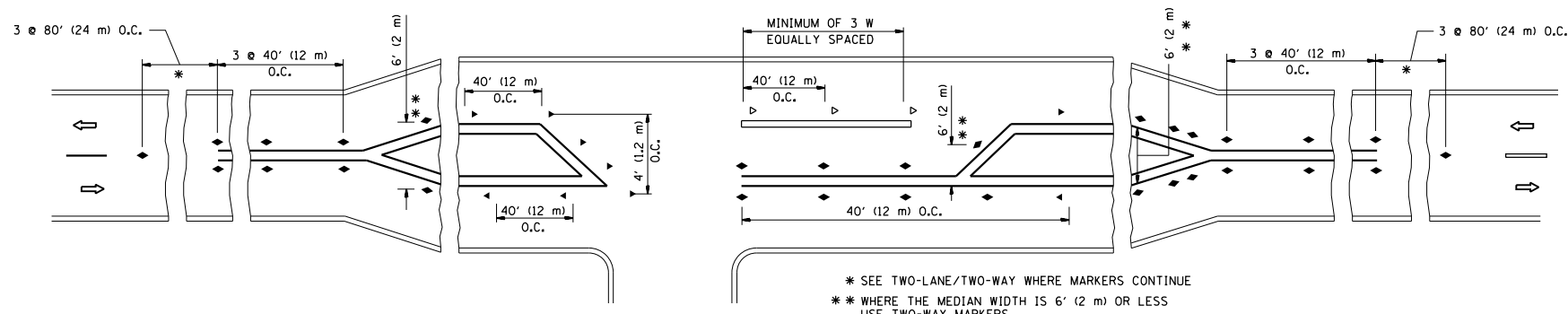
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

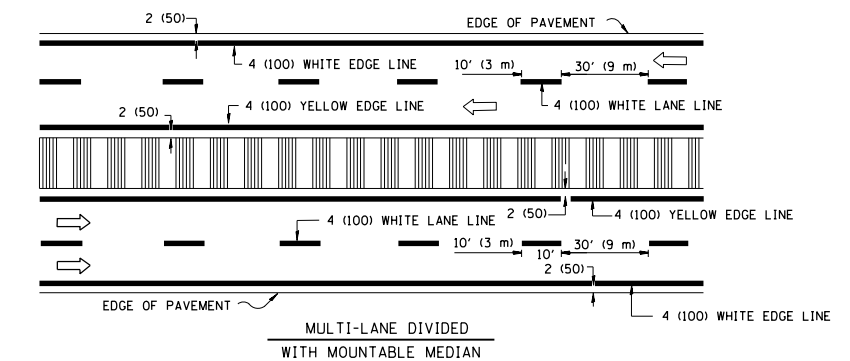
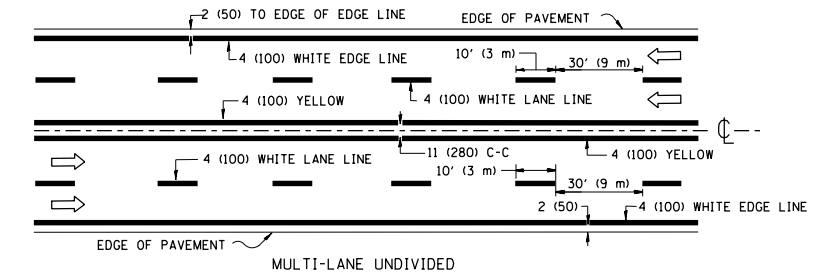
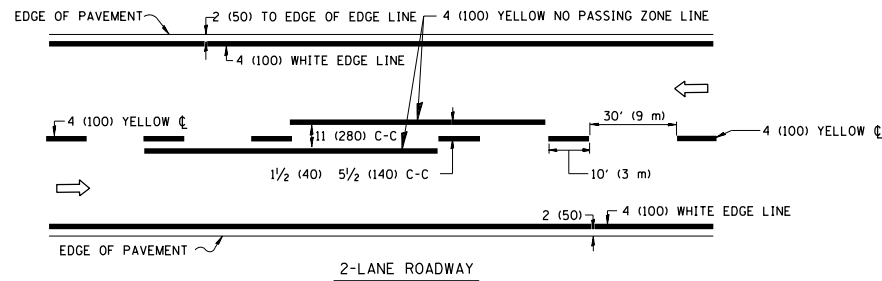
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
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	PLOT DATE = 6/2/2014	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

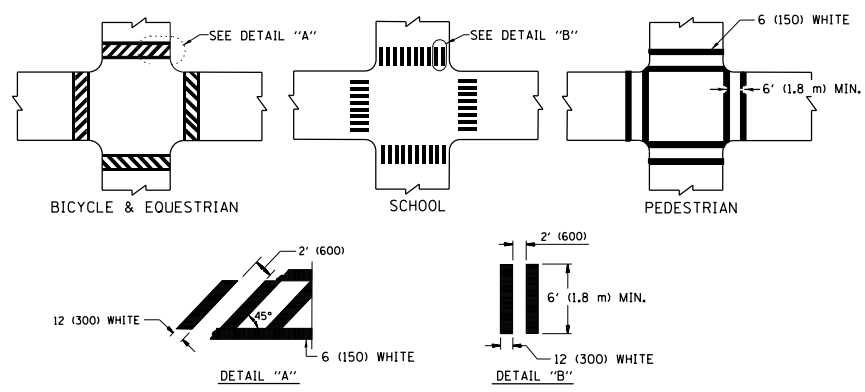
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	29
TC-11			CONTRACT NO. 60W12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

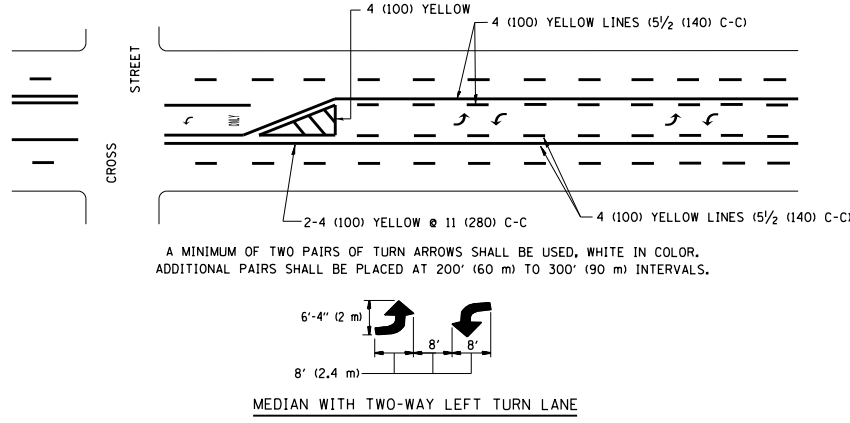
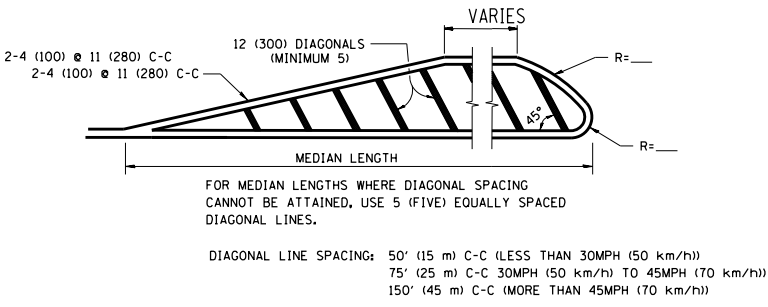
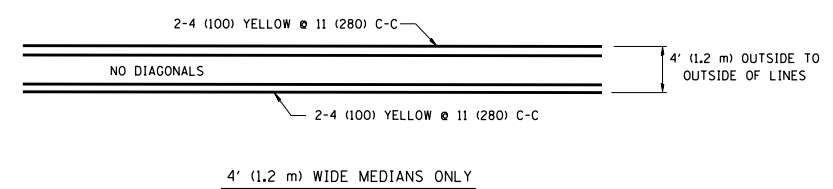


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

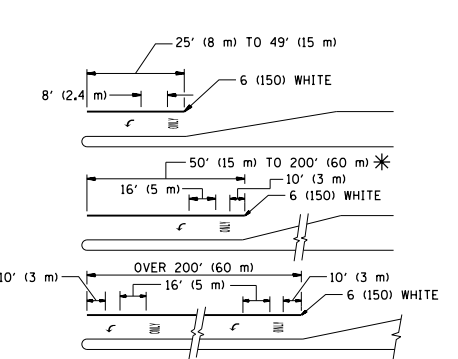


TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING

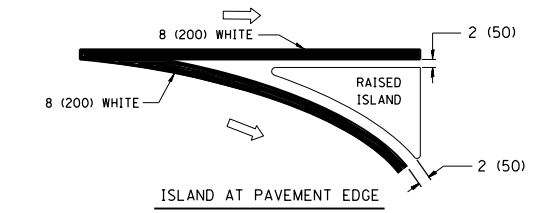
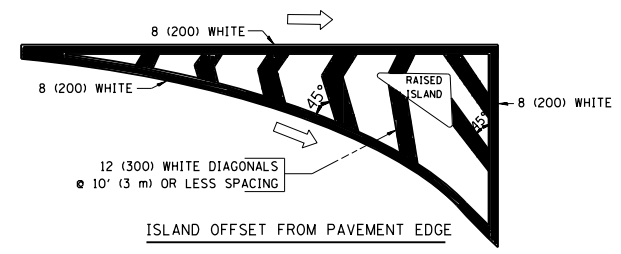
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

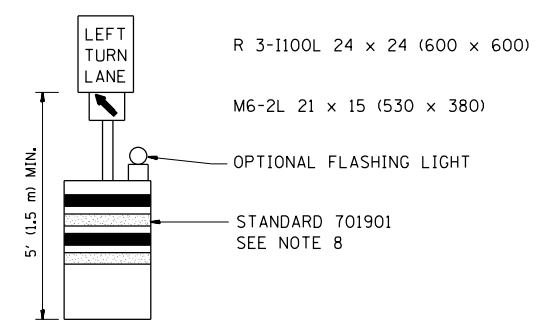
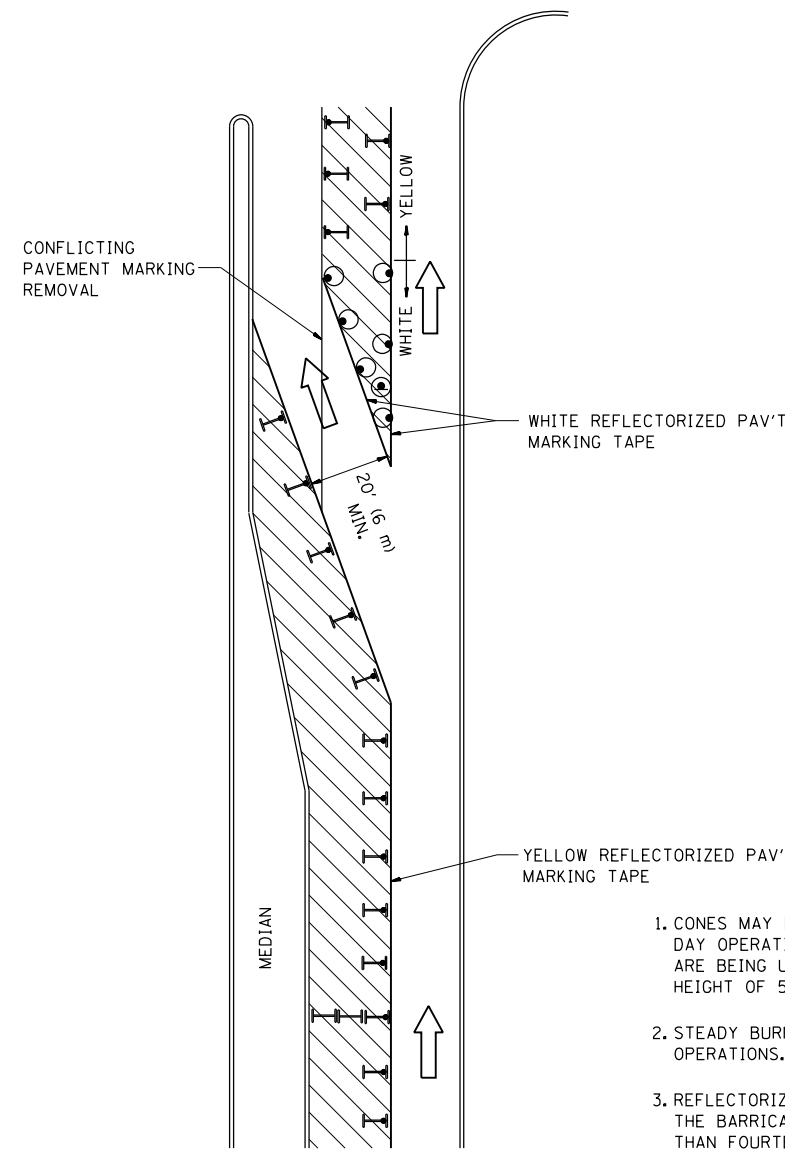
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Bakerom	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
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	PLOT DATE = 6/2/2014	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	30
TC-13		CONTRACT NO. 60W12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

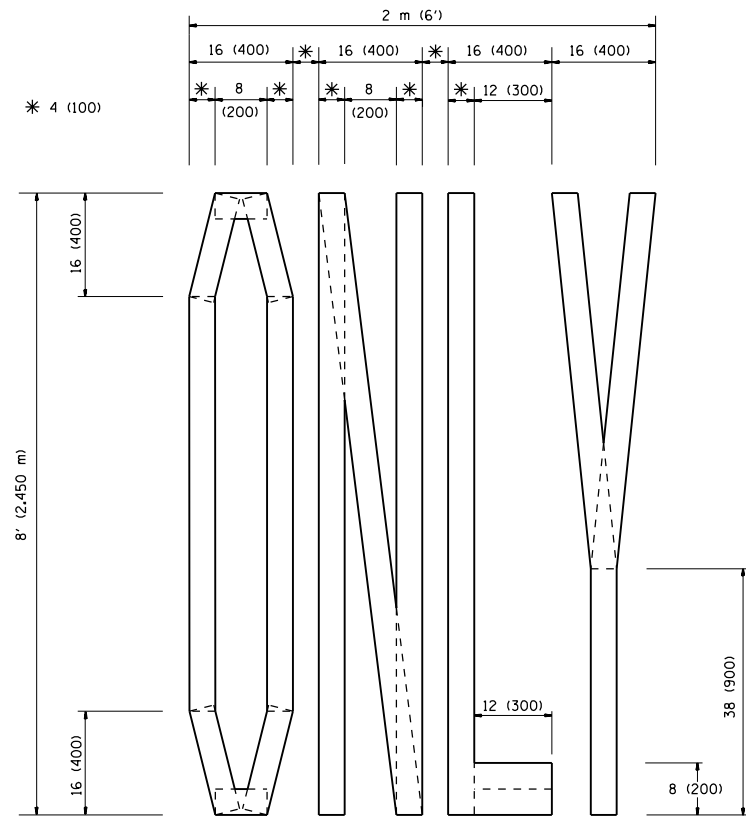
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = Bakerom	REVISED -T, RAMMACHER 09-08-94	REVISED - R, BORO 09-14-09
et:\pw\work\p1dot\bakermo\d0331242\0118313-shit-plan.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 6/2/2014	REVISED -T, RAMMACHER 01-06-00	REVISED -

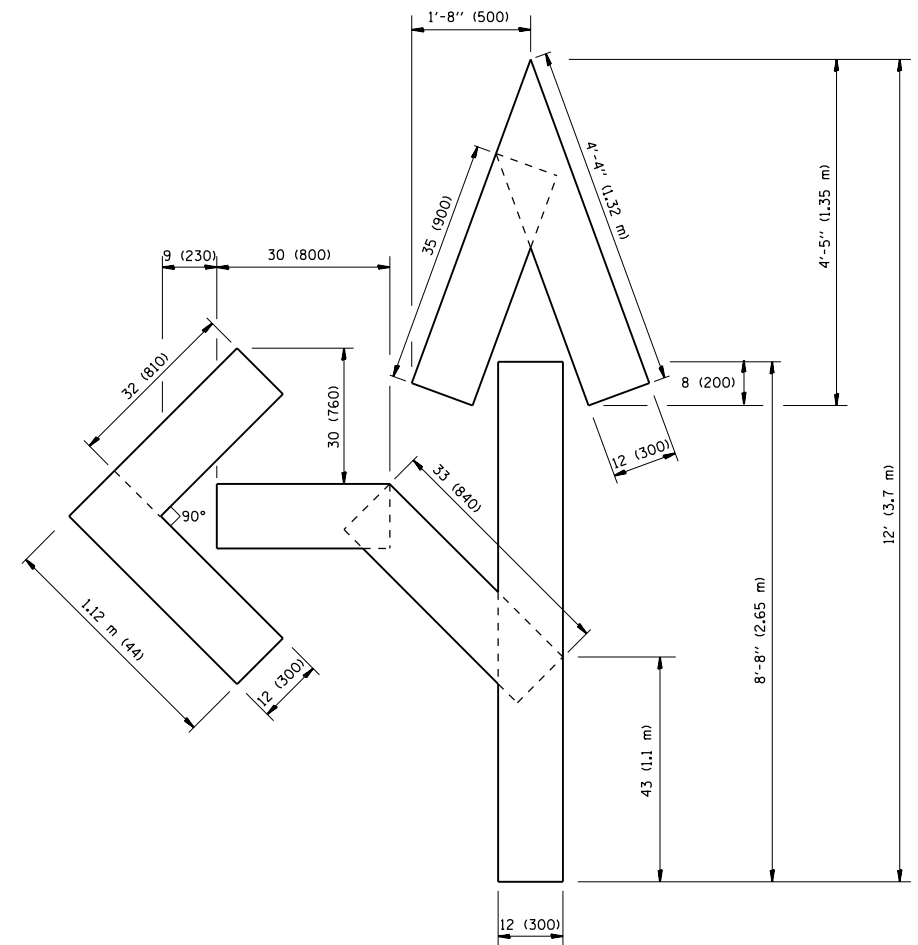
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

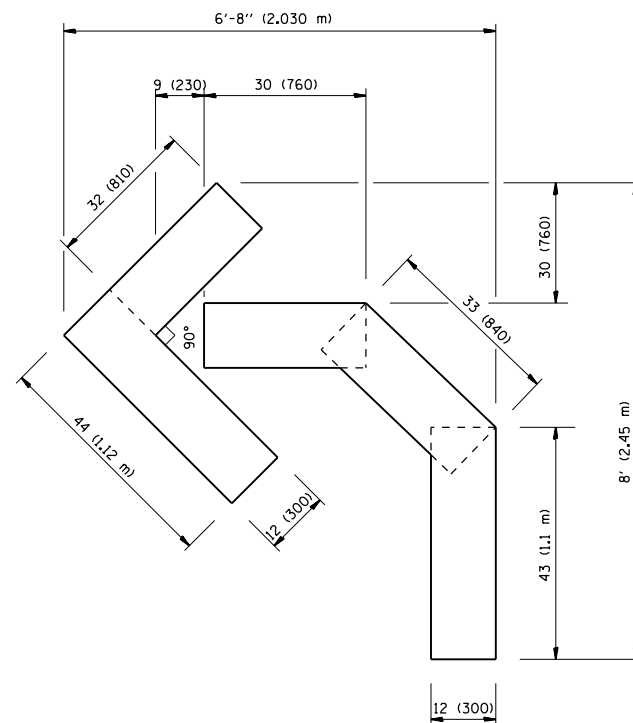
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	31
TC-14			CONTRACT NO. 60W12	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\dot\bakermo\d0331242\0118313-shit-plan.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	32
TC-16		CONTRACT NO. 60W12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bakerom	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pw\work\p1dot\bakermo\d0331242\0118313-shit-plan.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 6/2/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
852	(17&18) RS-6	WILL	33	33
TC-22		CONTRACT NO. 60W12		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				