

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.P. 344: (ILLINOIS ROUTE 83)
SECTION: 543R-3-RS
KNOLLWOOD ROAD TO ILLINOIS ROUTE 56
RESURFACING (3P)

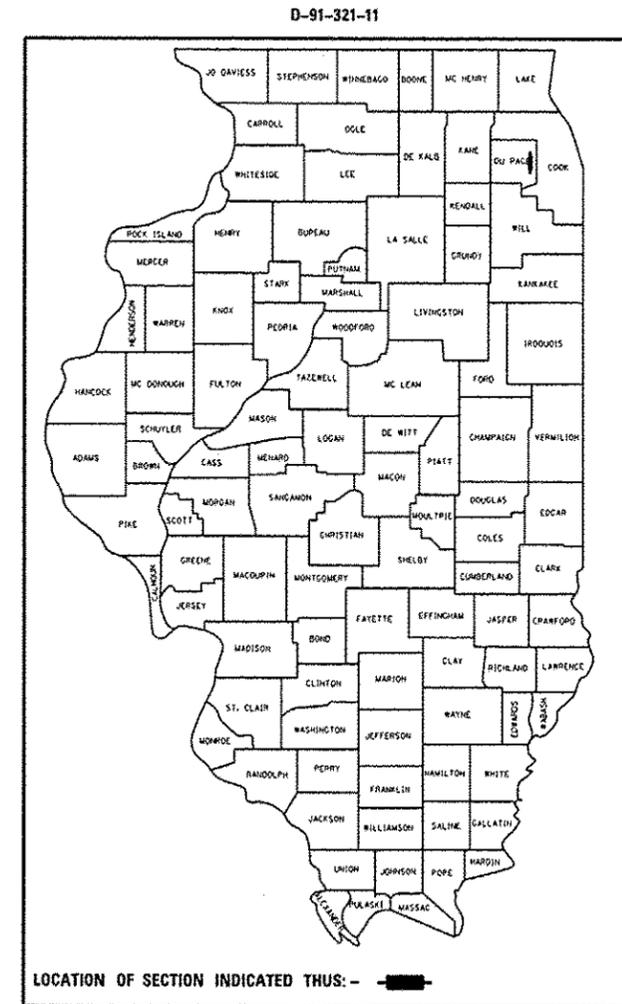
DUPAGE COUNTY

C-91-321-11

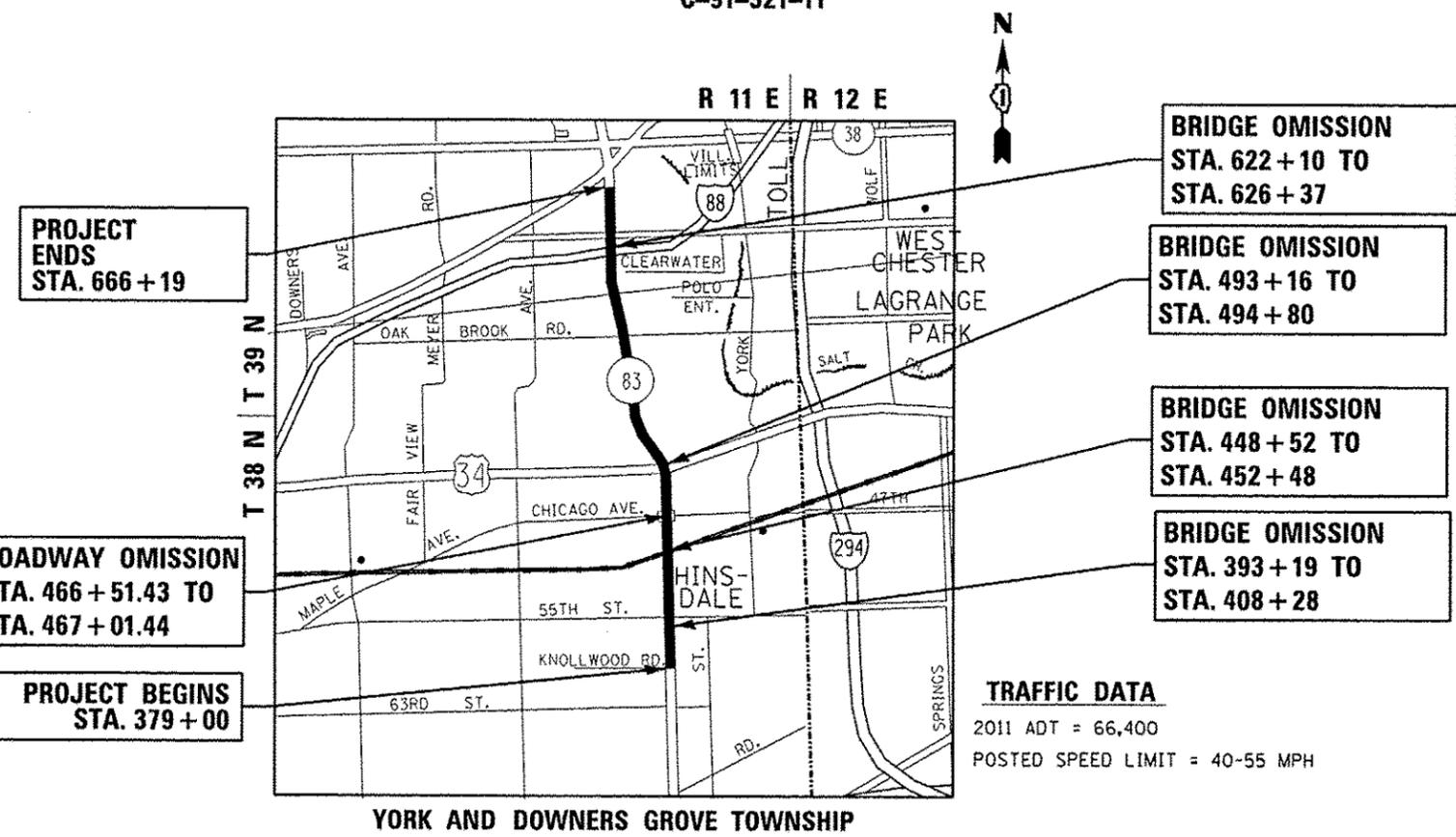
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-R	DUPAGE	51	1
		ILLINOIS	CONTRACT NO. 60N51	

FOR INDEX OF SHEETS, SEE SHEET NO.

PROJECT IS LOCATED IN THE CITY OF OAKBROOK TERRACE, THE VILLAGE OF OAKBROOK, THE VILLAGE OF WESTMONT, THE VILLAGE OF HINSDALE, THE VILLAGE OF WILLOWBROOK AND THE VILLAGE OF CLARENDON HILLS

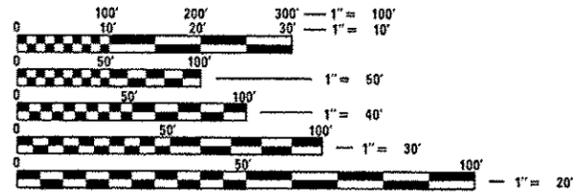


LOCATION OF SECTION INDICATED THUS: - [black bar] -



YORK AND DOWNERS GROVE TOWNSHIP

GROSS LENGTH OF PROJECT = 28,719 FT = 5.44 MILES
NET LENGTH OF PROJECT = 26,223 FT = 4.97 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: KARI SMITH (847) 705-4437
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60N51

TRAFFIC DATA
2011 ADT = 66,400
POSTED SPEED LIMIT = 40-55 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED June 9 20 14

John Fortmann Jr.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

John D. Baranowski, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

July 18 20 14

Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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LIST OF ILLINOIS DOT HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 442201-03 CLASS C AND D PATCHES
- 482011-03 HMA SHOULDER STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
- 606001-05 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND CUTTER
- 635011-02 REFLECTOR MARKER AND MOUNTING DETAILS
- 637001-05 CONCRETE BARRIER, DOUBLE FACE, 32 IN. (815 MM) HEIGHT
- 701400-07 APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701406-08 LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
- 701411-08 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEED >= 45 MPH
- 701428 TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
- 701446-05 TWO LANE CLOSURE FREEWAY/EXPRESSWAY
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701701-09 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-03 TRAFFIC CONTROL DEVICES

MAINTENANCE OF TRAFFIC GENERAL NOTES

- 1 STANDARDS 701601 AND 701701 SHALL ONLY BE APPLIED TO TRAFFIC CONTROL BETWEEN IL 56 (BUTTERFIELD RD) AND INTERSTATE 88.
- 2 STANDARDS 701400, 701428, 701406 AND 701446 SHALL ONLY BE APPLIED TO TRAFFIC CONTROL BETWEEN INTERSTATE 88 AND KNOLLWOOD RD.

COMMITMENTS

THE CONTRACTOR SHALL CONTACT TIM HALIK, VILLAGE ADMINISTRATOR OF THE VILLAGE OF WILLOWBROOK AT (630) 920-2261 TWO WEEKS BEFORE THE START OF CONSTRUCTION.

GENERAL NOTES

- 1 BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- 2 THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, CITY OF OAKBROOK TERRACE, VILLAGE OF OAKBROOK, VILLAGE OF WESTMONT, VILLAGE OF HINSDALE, VILLAGE OF WILLOWBROOK AND VILLAGE OF CLARENDON HILLS.
- 3 THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- 4 ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- 5 ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE, NO ADDITIONAL COST TO THE DEPARTMENT.
- 6 BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- 7 ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 8 LOCATIONS OF COMBINATION CONCRETE CURB AND CUTTER REMOVAL AND REPLACEMENT AS WELL AS MEDIAN REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER UNLESS NOTED ON THE PLANS.
- 9 DRAINAGE ADJUSTMENT, RECONSTRUCTION OR CLEANING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 10 THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.
- 11 THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 12 THE ENGINEER SHALL CONTACT DON CHIARUCI, THE TRAFFIC FIELD TECHNICIAN AT (847)741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 13 THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 14 DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- 15 WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- 16 BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
- 17 PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND IT'S REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- 18 THE CONTRACTOR WILL ONLY BE ALLOWED TO PERFORM WORK ON IL 83 BETWEEN THE HOURS OF 8:00 PM AND 5:00 AM FOR EVERY DAY OF THE WEEK.
- 19 WHEN WORK IS BEING PERFORMED IN LANE 2 OF A 3 LANE SECTION OF ROAD, THE CONTRACTOR MUST CLOSE LANES FROM THE LEFT WHEN ADJACENT TO ANY RAMPS.
- 20 NIGHTTIME FLAGGERS OR WORKERS SHALL BE EQUIPPED WITH A FLUORESCENT ORANGE OR FLUORESCENT YELLOW/GREEN VEST MEETING THE REQUIREMENTS OF ANSI/ISEA 107-2004 FOR CONSPICUITY CLASS 3 GARMENTS.
- 21 THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF CLARENDON HILLS. ALL ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p-work\p\idot\main\ad0316286\01	2111-shr-plan.dgn	DRAWN :	REVISED :		344	543R-3-RS	DUPAGE	51	2			
	PLOT SCALE : 100.0000 / in.	CHECKED :	REVISED :		CONTRACT NO. 60NS1							
Default	PLOT DATE : 6/13/2014	DATE :	REVISED :		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT

GENERAL NOTES

- 22 BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.
- 23 WHEN EDGE OF BARRIER IS WITHIN 3 FT OF THE TRAFFIC LANE EDGE, REDUCE LANE TO 11 FT LANE WITH TYPE III TEMPORARY TAPE AND TAPER IN 45:1. COST OF THE TAPE AND ITS REMOVAL IS INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION".
- 24 FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, CUTTER, COMBINATION CURB & GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.
- 25 THE CONTRACTOR SHALL REQUEST AN UNDERGROUND CABLE LOCATE FOR STATE OWNED ELECTRICAL FACILITIES WITH IN THEIR PROJECT LIMITS PRIOR TO START OF WORK.
- 26 THERE IS A CONDUIT(S) AND AN EMBEDDED JUNCTION BOX IN THE BARRIER WALL. CARE MUST BE TAKEN TO PREVENT DAMAGE TO ANY CONDUIT(S) AND JUNCTION BOX. IF IT IS DAMAGED, THE CONDUIT AND/OR JUNCTION BOX SHALL BE REPAIRED OR REPLACED. IF ELECTRICAL OR COPPER COMMUNICATIONS CABLE IS DAMAGED, IT SHALL BE REPLACED FROM JUNCTION BOX TO JUNCTION BOX (1500 FT. MIN.). IF FIBER OPTIC COMMUNICATIONS CABLE IS DAMAGED, IT SHALL BE REPLACED FROM EXISTING SPLICE ENCLOSURE TO EXISTING SPLICE ENCLOSURE OR TERMINATION POINT. FOR THE LOCATION OF THE CONDUITS, CONTACT ROY LAWSON AT THE ELECTRICAL FIELD OFFICE AT (708) 524-2145.
- 27 ANY CONDUIT RUSTED THRU, EXPOSED CABLES, PULL BOXES OR HANDHOLES FOR BRIDGE LIGHTING, TRAFFIC SIGNALS, COUNTERS, SURVEILLANCE EQUIPMENT FOUND IN DISREPAIR THE ENGINEER SHALL TAKE THE APPROPRIATE ACTION:

NO SAW CUTTING OR REMOVAL WILL BE MADE OR CONCRETE POURED UNTIL THE APPROPRIATE ACTION IS TAKEN TO RESOLVE EACH ISSUE;

ALL WORK REQUIRED TO REPAIR/REPLACE SIGNAL SURVEILLANCE OR LIGHTING ITEMS WILL BE DOCUMENTED BY THE ENGINEER AND THE BUREAU OF TRAFFIC WILL BE NOTIFIED.

FILE NAME :	USER NAME : mairanada	DESIGNED -	REVISED -	STATE OF ILLINOIS	INDEX OF SHEETS, LIST OF IDOT HIGHWAY STANDARDS, GENERAL NOTES AND COMMENTS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\mairanada\0316286\0316286-1\1111-shs-plan.dgn	DRAWN -	REVISED -	344			543R-3-RS	DUPAGE	51	3	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 60NS1							
Default	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
					SHEET 1 OF 1 SHEETS		STA. TO STA.			

URBAN

URBAN

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005						
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	50	50						
25200110	SODDING, SALT TOLERANT	SQ YD	50	50						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	3519	3519						
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), LL-4, 75, N50	TON	12183	12183						
40600895	CONSTRUCTING TEST STRIP	EACH	3	3						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	228	228						
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	6346	6346						
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	639	639						
40603153	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	24809	24809						
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	12815	12815						
42001300	PROTECTIVE COAT	SQ YD	178	178						
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	47802	47802						

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005						
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	20795	20795						
44002211	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 3/4"	SQ YD	4146	4146						
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	2742	2742						
44201298	DOWEL BARS 1 1/4"	EACH	6	6						
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	2650	2650						
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	240	240						
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	540	540						
44213200	SAW CUTS	FOOT	6	6						
44213202	TIE BARS 1"	EACH	8	8						
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	106954	106954						
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1339	1339						
50102400	CONCRETE REMOVAL	CU YD	4	4						
50300225	CONCRETE STRUCTURES	CU YD	4	4						
60250200	CATCH BASINS TO BE ADJUSTED	EACH	155	155						
60262700	INLETS TO BE RECONSTRUCTED	EACH	10	10						

FILE NAME =	USER NAME = mtr@state	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>				<p align="center">SUMMARY OF QUANTITIES IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)</p>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
12		DRAWN -	REVISED -					344	543R-3-RS	DUPAGE	51	4				
		CHECKED -	REVISED -					<p align="center">SCALE: SHEET NO. OF SHEETS STA. TO STA.</p>				<p align="center">CONTRACT NO. 60N51</p>				
		DATE -	REVISED -					<p align="center">FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</p>								

*SPECIALTY ITEMS

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	55	55
60404950	FRAMES AND GRATES, TYPE 24	EACH	6	6
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	6	6
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	245	245
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005
70300100	SHORT TERM PAVEMENT MARKING	FOOT	40031	40031
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	1311	1311
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	111405	111405
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	5905	5905
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	29470	29470
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	10083	10083
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	620	620
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	72341	72341
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1311	1311
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	111405	111405
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5905	5905
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	29470	29470
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	10083	10083
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	620	620
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	3290	3290

FILE NAME :	USER NAME = mll/roads	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	5
CONTRACT NO. 60N51				
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

*SPECIALTY ITEMS

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005			
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	1340	1340			
* 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2950	2950			
* 88600100	DETECTOR LOOP, TYPE I	FOOT	318	318			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2726	2726			
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	90	90			
K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	L SUM	1	1			
X0301242	PIEZO AXLE SENSOR, CLASS II	FOOT	44	44			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	345	345			
40800025	BITUMINOUS MATERIALS (PRIME COAT)	POUND	149309	149309			
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	24812	24812			
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	31918	31918			
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	245	245			
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	4500	4500			
XZ043900	PREFORMED JOINT FILLER REMOVAL	FOOT	24500	24500			

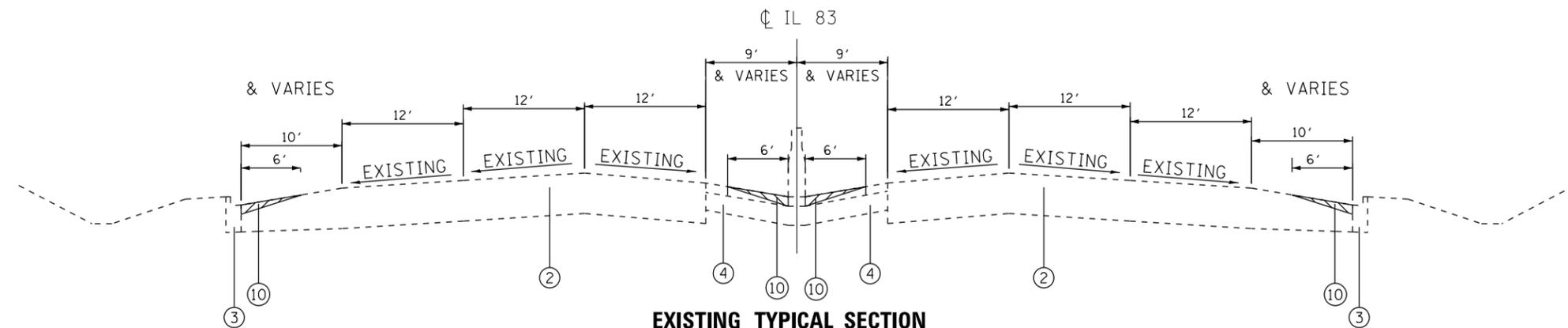
SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE 0005			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	450	450			
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	336	336			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4			
Z0034105	MATERIAL TRANSFER DEVICE	TON	24809	24809			
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500			

FILE NAME :	USER NAME : mdrand	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)			F.A.P. RATE:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE : 1/8" = 100.0000' / 1"		DRAWN -	REVISED -		344	543R-3-RS	DUPAGE	51	6			
PLOT DATE : 6/13/2014		CHECKED -	REVISED -		CONTRACT NO. 60N51			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
DATE		REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.					

*SPECIALTY ITEMS

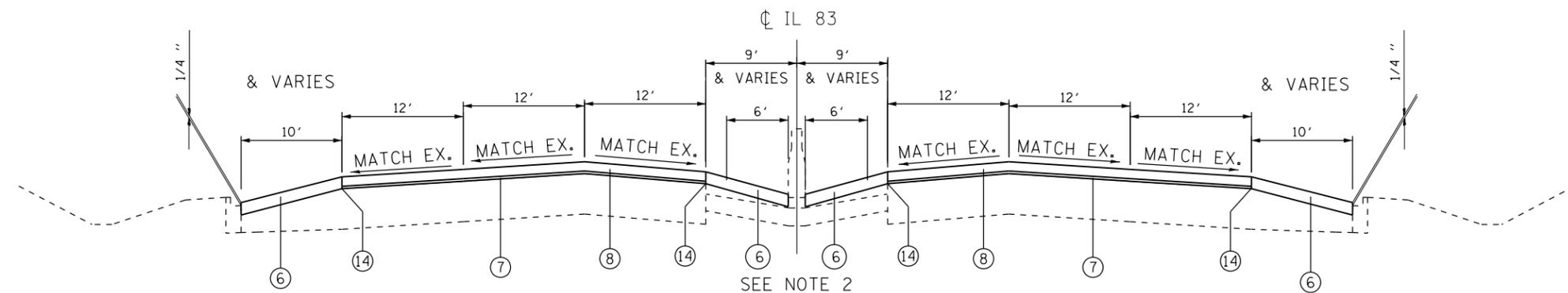
Ø 0042

REV.



EXISTING TYPICAL SECTION

STA. 379+00 TO STA. 393+19
STA. 408+28 TO STA. 411+15



PROPOSED TYPICAL SECTION

SEE NOTE 2
STA. 379+00 TO STA. 393+19
STA. 408+28 TO STA. 411+15

LEGEND

- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -3"
- ⑦ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
- ⑧ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑨ HMA SURFACE REMOVAL - 2"
- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE HMA TAPER AT EDGE OF P.C.C. PAVEMENT DETAIL)
- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑫ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
- ⑬ HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT

NOTES

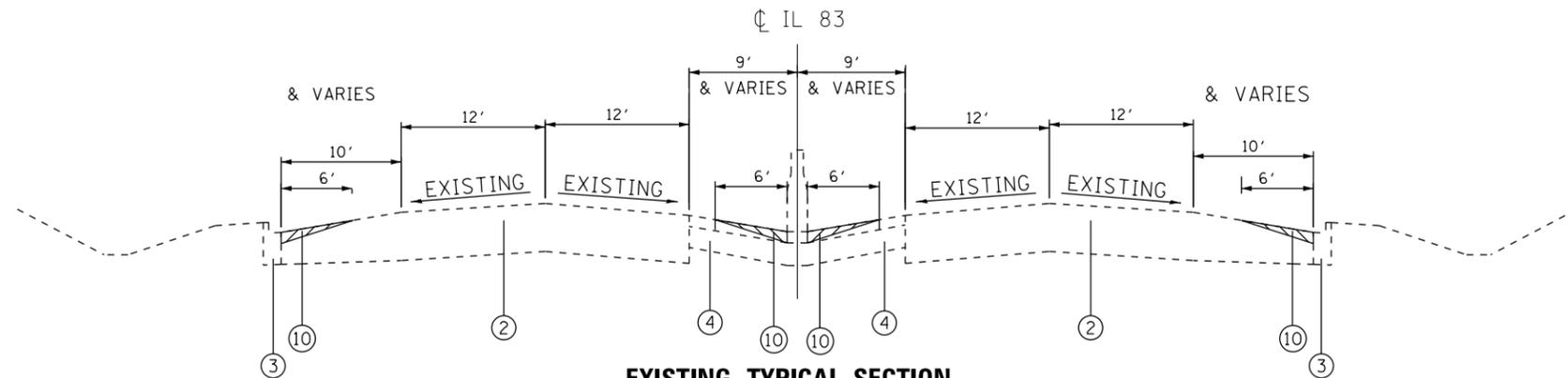
1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
2. NO ADDITIONAL COMPENSATION WILL BE GIVEN TO THE CONTRACTOR FOR ANY EXTRA WORK REQUIRED TO REMOVE THE HMA SHOULDER ADJACENT TO THE EXISTING BARRIER WALL. THIS WORK WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR HMA SURFACE REMOVAL - 2" AND HMA SURFACE REMOVAL VARIABLE DEPTH.

BRIDGE OMISSION
STA. 393+19 TO STA. 408+28
STA. 448+52 TO STA. 452+48
STA. 493+16 TO STA. 494+80

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS	Ndes	
PAVEMENT RESURFACING			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 (IL-12.5 mm)	3.5%	@ 80 GYR.	PFP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5%	@ 50 GYR.	PFP
HMA SHOULDER RESURFACING			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm)	4%	@ 70 GYR.	OCP
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4%	@ 70 GYR.	QC/OA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4%	@ 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP); PAY FOR PERFORMANCE (PFP)			

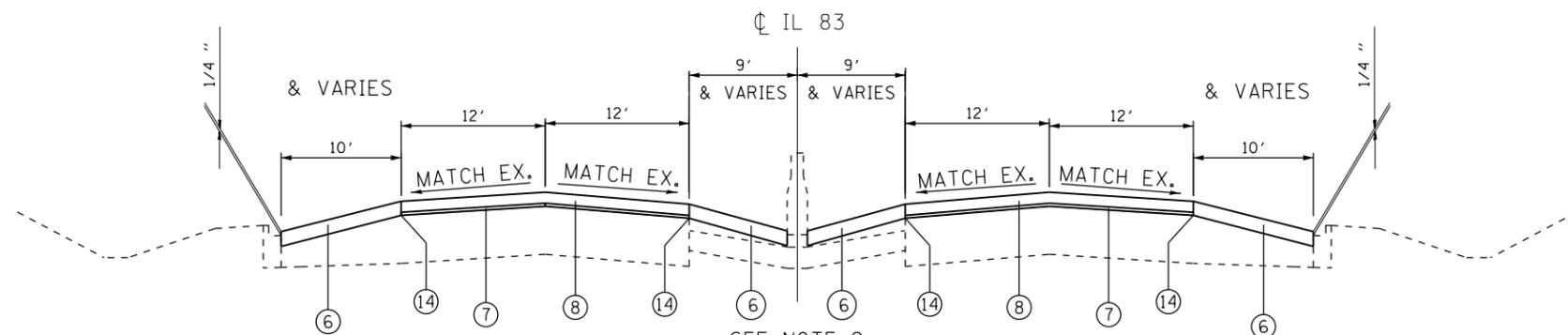
MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
3. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



EXISTING TYPICAL SECTION

STA. 411+15 TO STA. 444+54
 STA. 454+49 TO STA. 491+09



PROPOSED TYPICAL SECTION

SEE NOTE 2
 STA. 411+15 TO STA. 444+54
 STA. 454+49 TO STA. 491+09

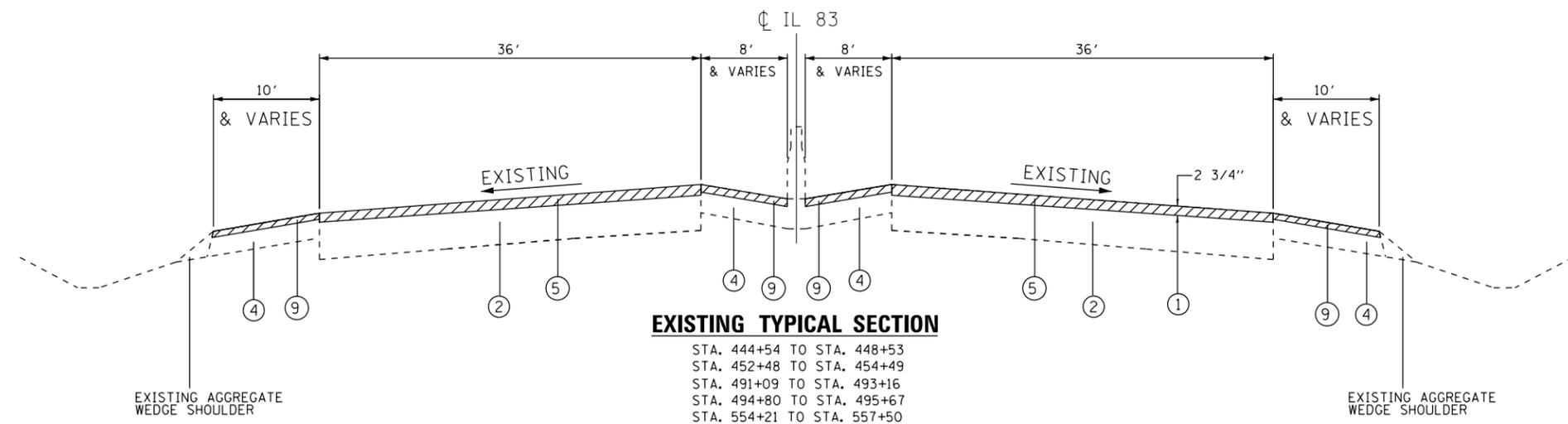
LEGEND

- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
- ⑥ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -3"
- ⑦ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
- ⑧ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"
- ⑨ HMA SURFACE REMOVAL - 2"
- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE HMA TAPER AT EDGE OF P.C.C. PAVEMENT DETAIL)
- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑫ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
- ⑬ HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT

NOTES

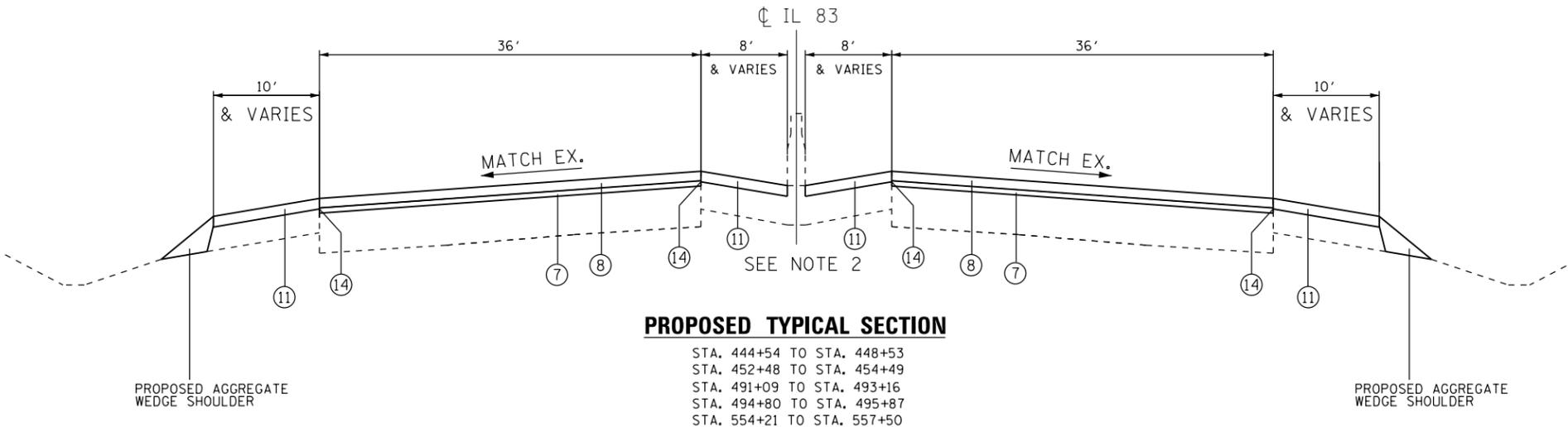
1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
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FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\pidot\marenade\d0316286\013	2111-sh-t-plen.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	8	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N51					
	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



EXISTING TYPICAL SECTION

STA. 444+54 TO STA. 448+53
 STA. 452+48 TO STA. 454+49
 STA. 491+09 TO STA. 493+16
 STA. 494+80 TO STA. 495+67
 STA. 554+21 TO STA. 557+50



PROPOSED TYPICAL SECTION

STA. 444+54 TO STA. 448+53
 STA. 452+48 TO STA. 454+49
 STA. 491+09 TO STA. 493+16
 STA. 494+80 TO STA. 495+87
 STA. 554+21 TO STA. 557+50

LEGEND

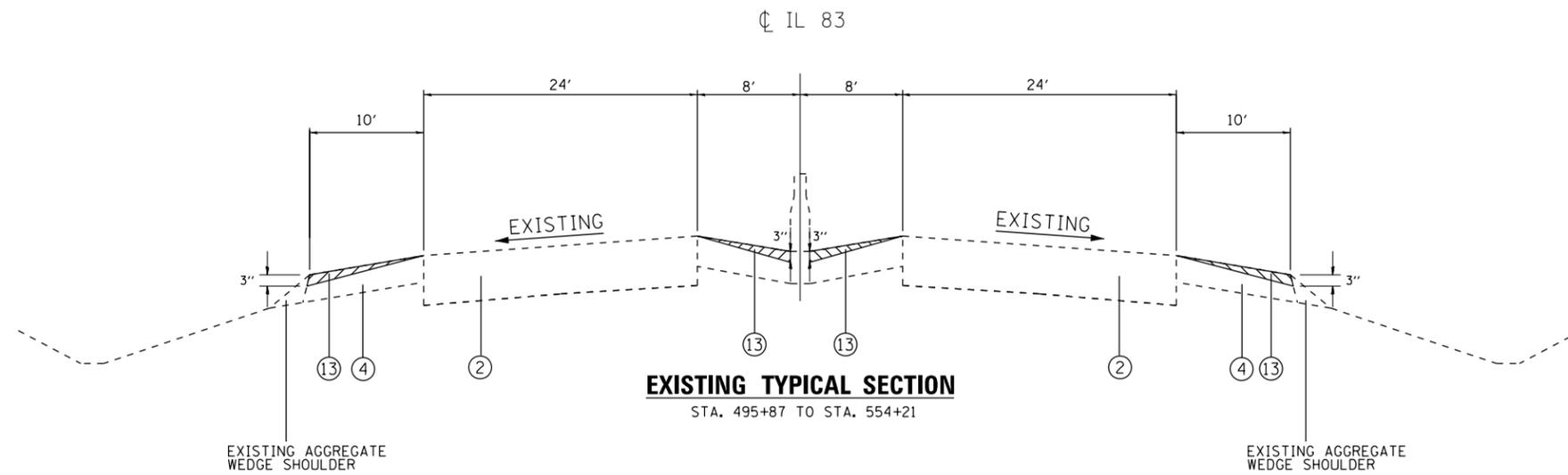
- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
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- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑫ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
- ⑬ HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT

NOTES

- 1. THE CONTRACTOR SHALL PERFORM THE PAVEMENT PATCHING OPERATIONS PRIOR TO THE HMA SURFACE REMOVAL OPERATION. SEE IDOT DISTRICT 1 DETAIL PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT BD-400-04 (BD-22) FOR ADDITIONAL INFORMATION.
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BRIDGE OMISSION
 STA. 448+52 TO STA. 452+48
 STA. 493+16 TO STA. 494+80

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\idot\marenade\d0316286\0132111-sht-plan.dgn	2111-sht-plan.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	9	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			SHEET 2 OF 4 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT	

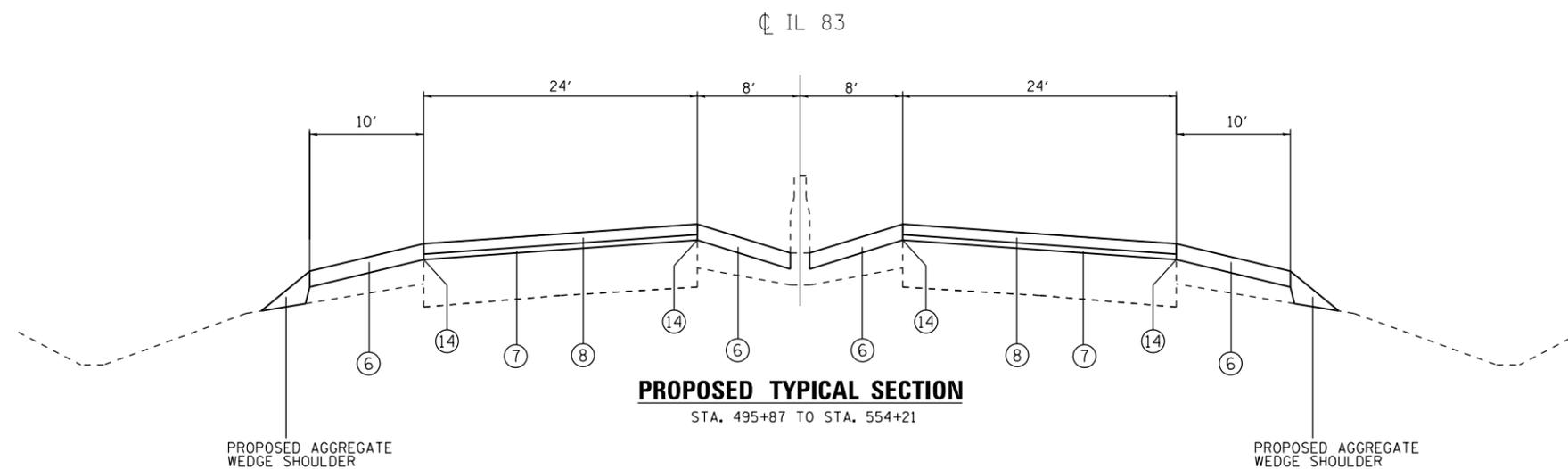


LEGEND

- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
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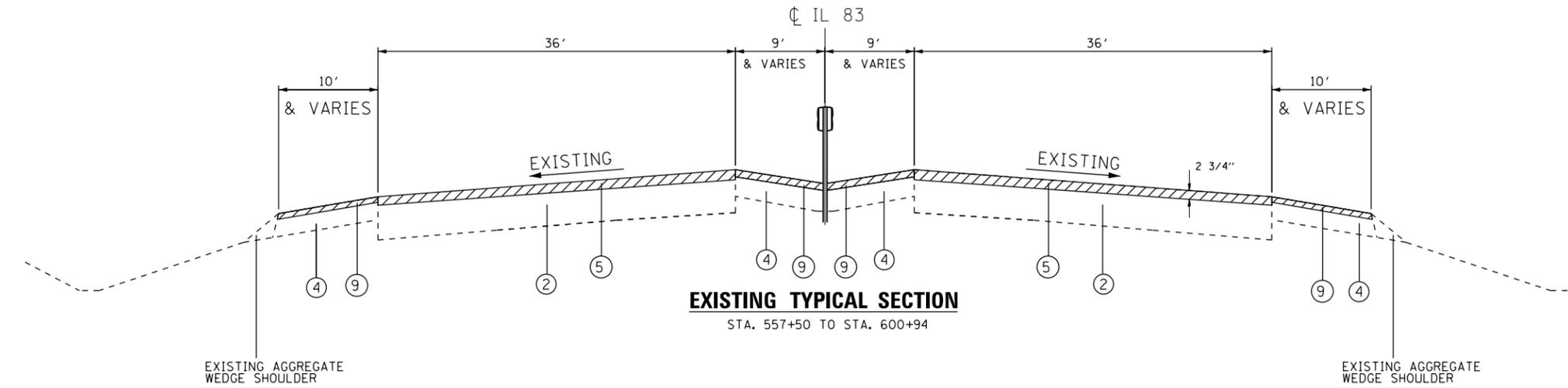
FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

LEGEND

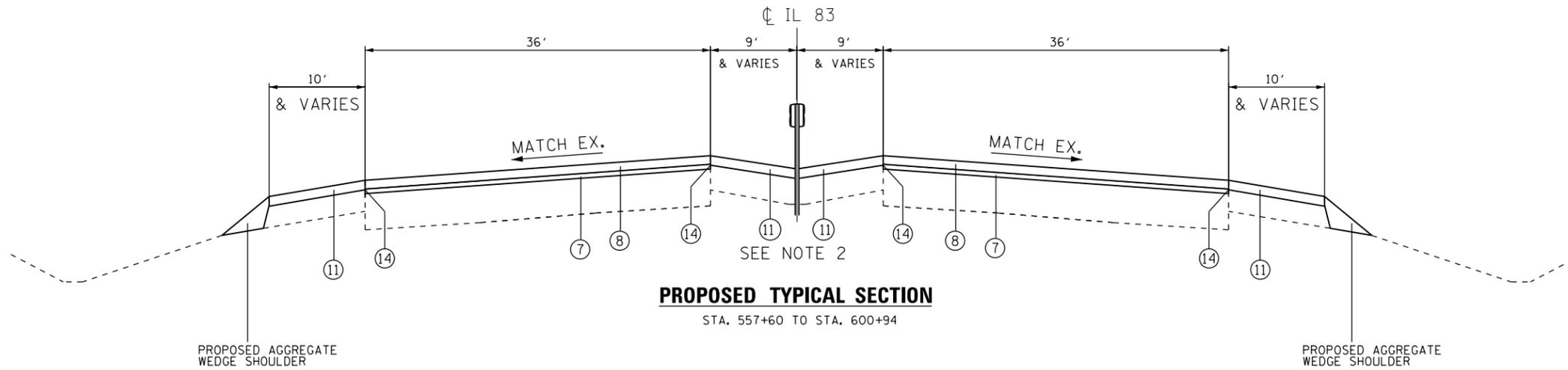
- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
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EXISTING TYPICAL SECTION
STA. 557+50 TO STA. 600+94



PROPOSED TYPICAL SECTION
STA. 557+60 TO STA. 600+94

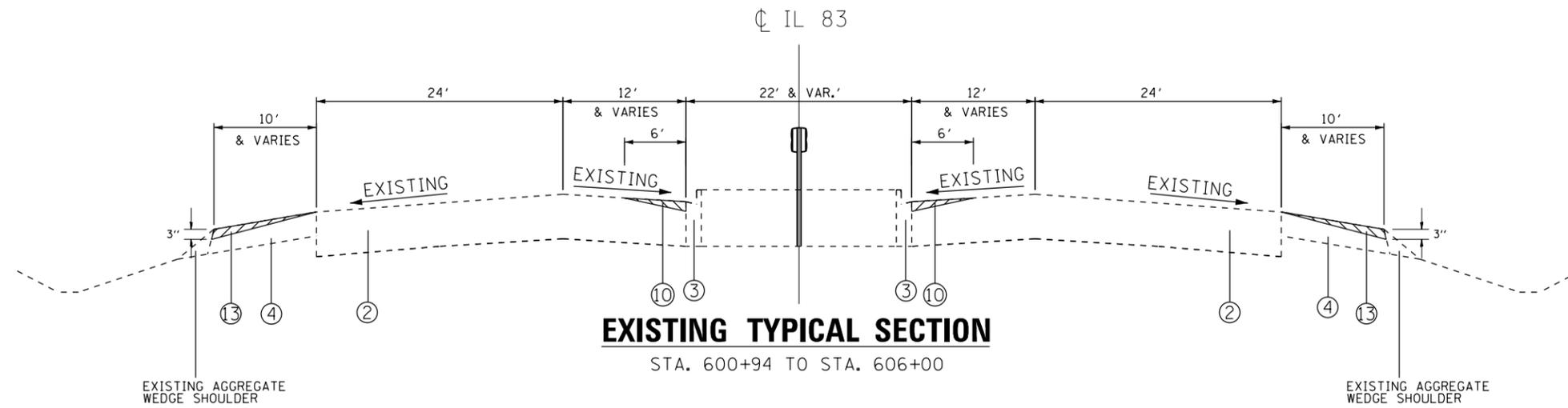
FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2111-sh-t-pln.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	11	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

LEGEND

- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
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- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
- ⑫ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -3/4"
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- ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT

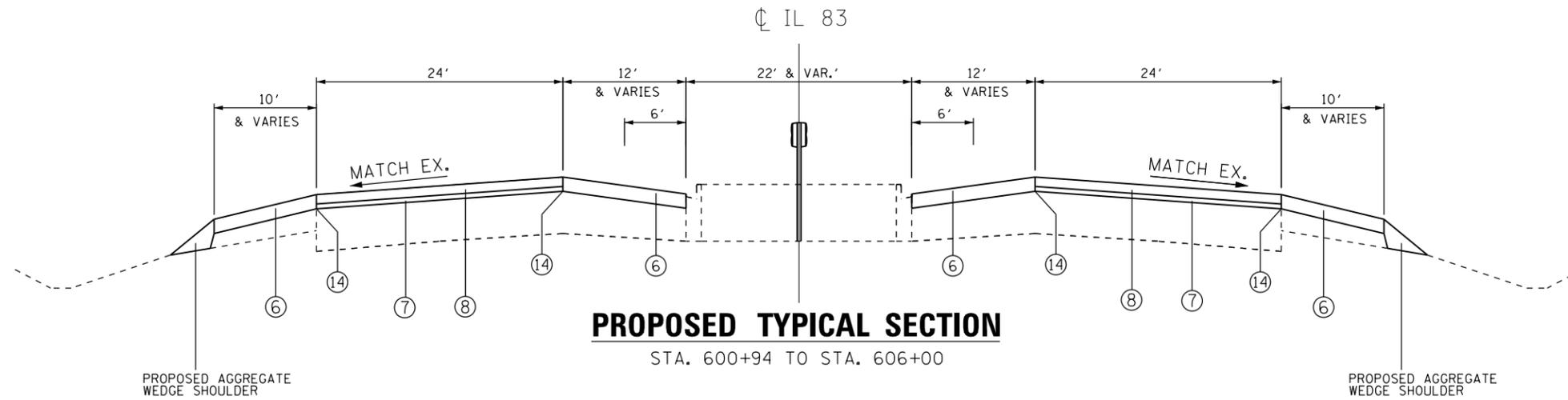
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EXISTING TYPICAL SECTION

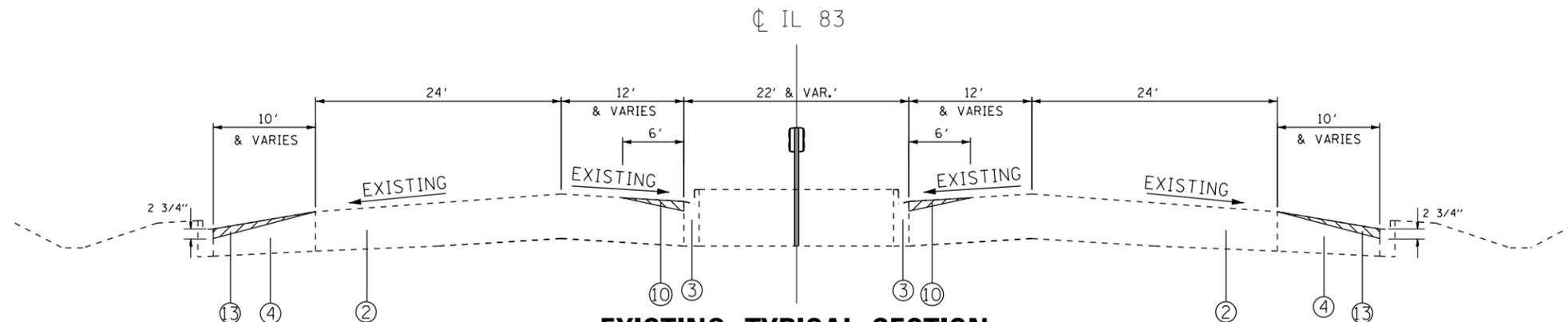
STA. 600+94 TO STA. 606+00



PROPOSED TYPICAL SECTION

STA. 600+94 TO STA. 606+00

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS	TYPICAL SECTIONS IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2111-sht-plan.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	12	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			SHEET 2 OF 4 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION

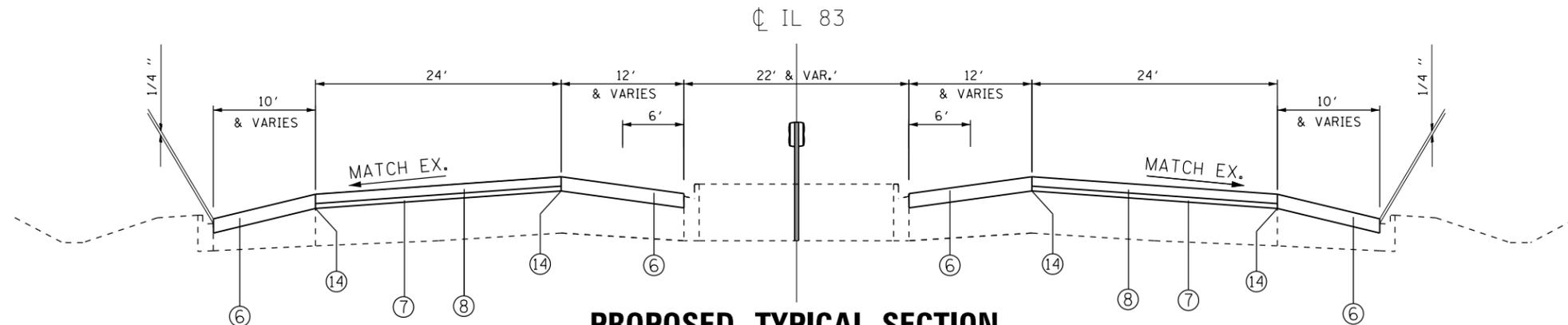
STA. 606+00 TO STA. 622+10
 STA. 626+37 TO STA. 666+19

LEGEND

- ① EXISTING +/- 2 3/4" HMA
- ② EXISTING +/-10" P.C.C. BASE
- ③ EXISTING B-6.24 CURB AND GUTTER
- ④ EXISTING SHOULDER +/-8"
- ⑤ HMA SURFACE REMOVAL - 2 3/4" (SEE NOTE 1)
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- ⑩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL VARIABLE DEPTH (SEE HMA TAPER AT EDGE OF P.C.C. PAVEMENT DETAIL)
- ⑪ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) -2"
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- ⑭ STRIP REFLECTIVE CRACK CONTROL TREATMENT

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PROPOSED TYPICAL SECTION

STA. 606+00 TO STA. 622+10
 STA. 626+37 TO STA. 666+19

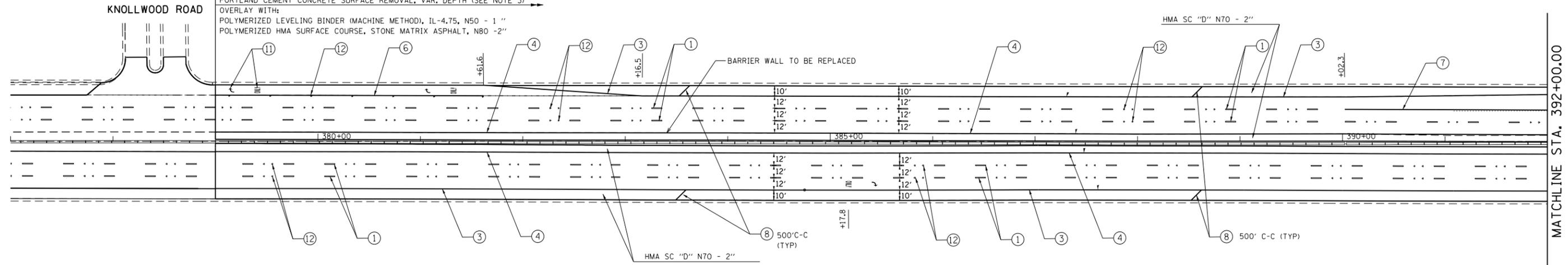
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	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	13
Default	PLOT DATE = 6/13/2014	CHECKED -	REVISED -	SHEET 2 OF 4 SHEETS STA. TO STA.		CONTRACT NO. 60N51				
						ILLINOIS FED. AID PROJECT				



IL RTE 83
(KINGERY HIGHWAY)

**PROJECT BEGINS
STA. 379+00**

PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE |
| ② THERMOPLASTIC PAVEMENT MARKING 8" WHITE DOTTED LINE (9' SKIP 3' DASH) | ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE |
| ③ THERMOPLASTIC PAVEMENT MARKING 4" SOLID WHITE LINE | ⑨ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL YELLOW LINE |
| ④ THERMOPLASTIC PAVEMENT MARKING 4" SOLID YELLOW LINE | ⑩ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR |
| ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C | ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE | ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2) |

NOTES:

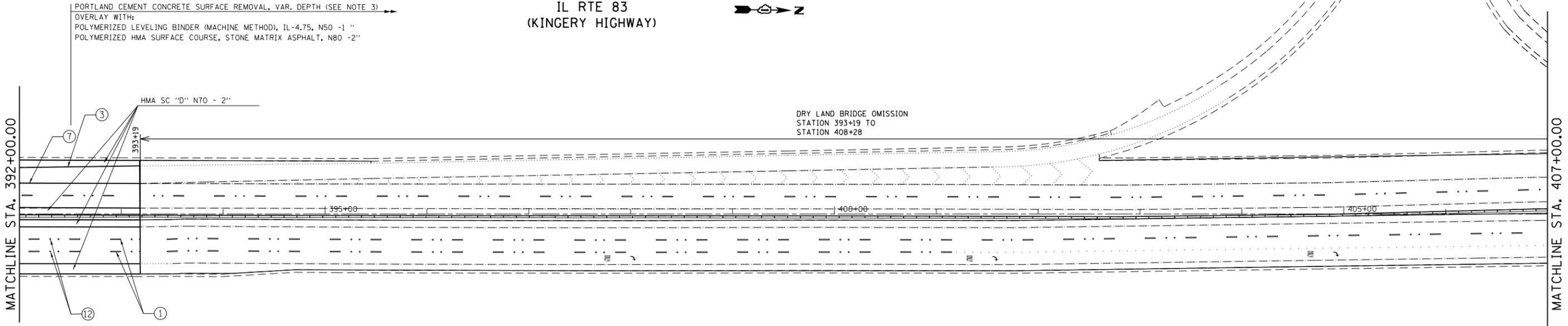
- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
- REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
- REFER TO TYPICAL SECTIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL FOR ADDITIONAL INFORMATION.
- REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

MEDIAN REMOVAL PARTIAL DEPTH



IL RTE 83
(KINGERY HIGHWAY)

PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 - 2"



FILE NAME = c:\pwwork\pwwork\marenade\d0316286\012111-sht-plan.dgn	USER NAME = marenade	DESIGNED -	REVISED -
Default	PLOT DATE = 6/13/2014	DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

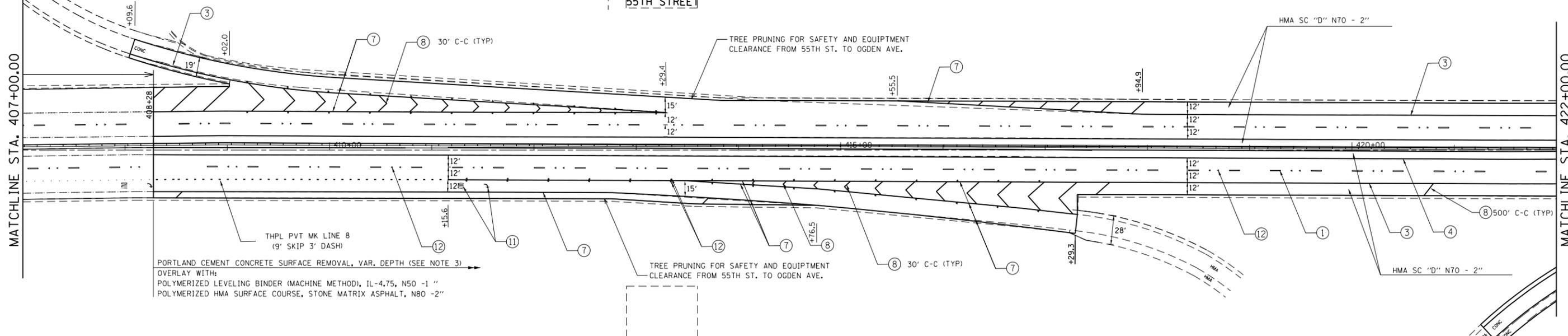
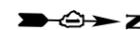
EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL. ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	543R-3-RS	DUPAGE	51	14
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50' SHEET 1 OF 10 SHEETS STA. TO STA.

IL RTE 83
(KINGERY HIGHWAY)

55TH STREET



PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"

TREE PRUNING FOR SAFETY AND EQUIPMENT
CLEARANCE FROM 55TH ST. TO OGDEN AVE.

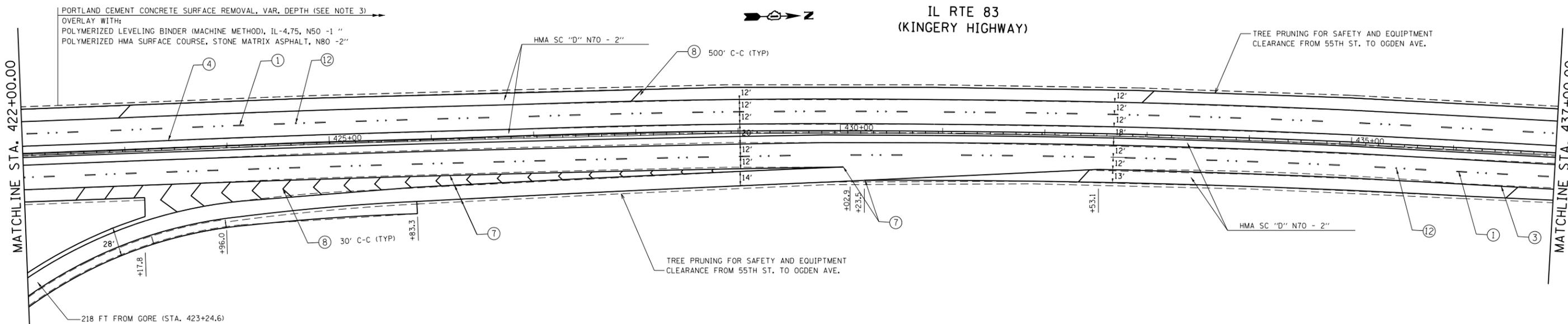
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE |
| ② THERMOPLASTIC PAVEMENT MARKING 8" WHITE DOTTED LINE (9' SKIP 3' DASH) | ⑧ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL WHITE LINE |
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| ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C | ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE | ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2) |

NOTES:

- REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
- REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.
- REFER TO TYPICAL SECTIONS AND HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL FOR ADDITIONAL INFORMATION.
- REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

MEDIAN REMOVAL PARTIAL DEPTH



PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VAR. DEPTH (SEE NOTE 3)
OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"

TREE PRUNING FOR SAFETY AND EQUIPMENT
CLEARANCE FROM 55TH ST. TO OGDEN AVE.

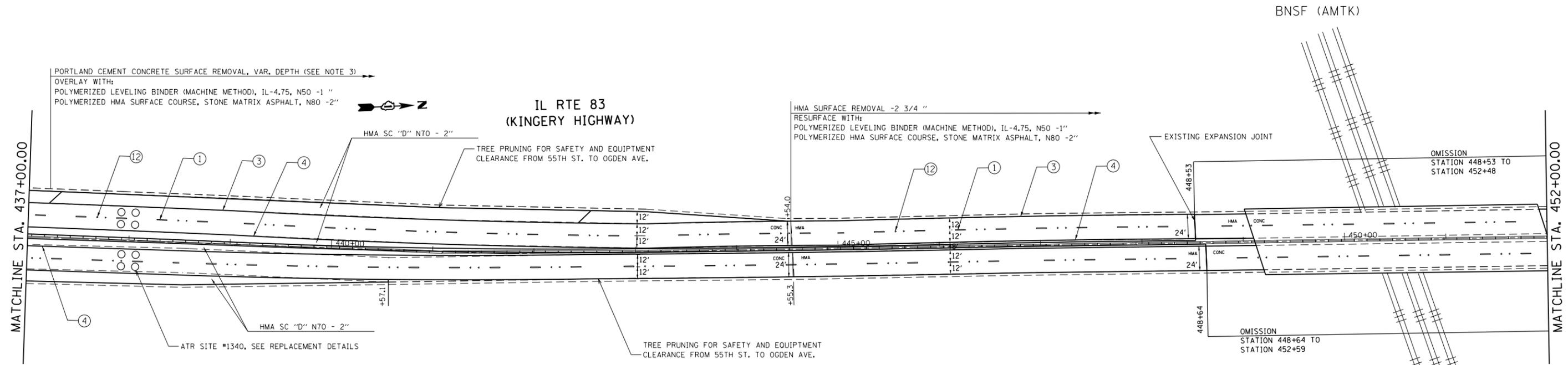
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	PLOT DATE = 6/13/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)

SCALE: 1" = 50' SHEET 2 OF 10 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	543R-3-RS	DUPAGE	51	15
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				



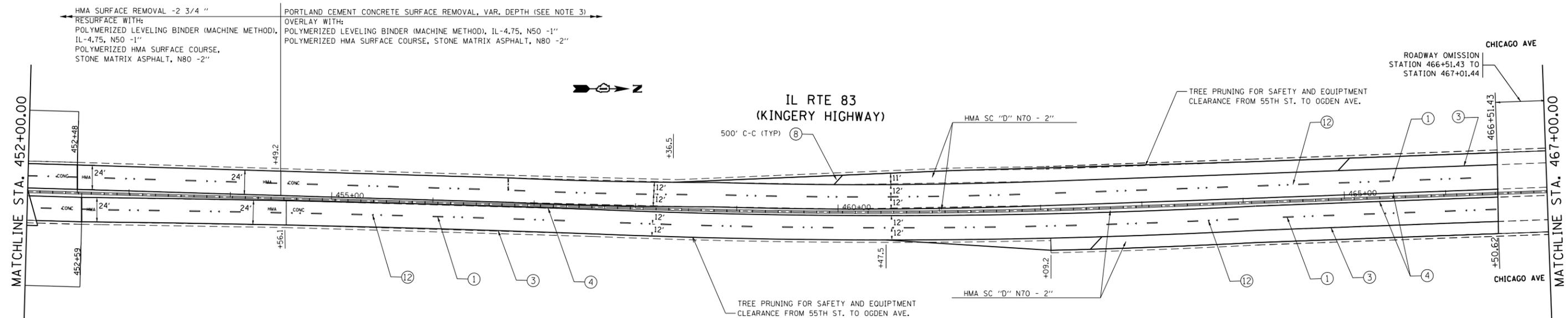
PAVEMENT MARKING LEGEND (SEE NOTE 1)

① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)	⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
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⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

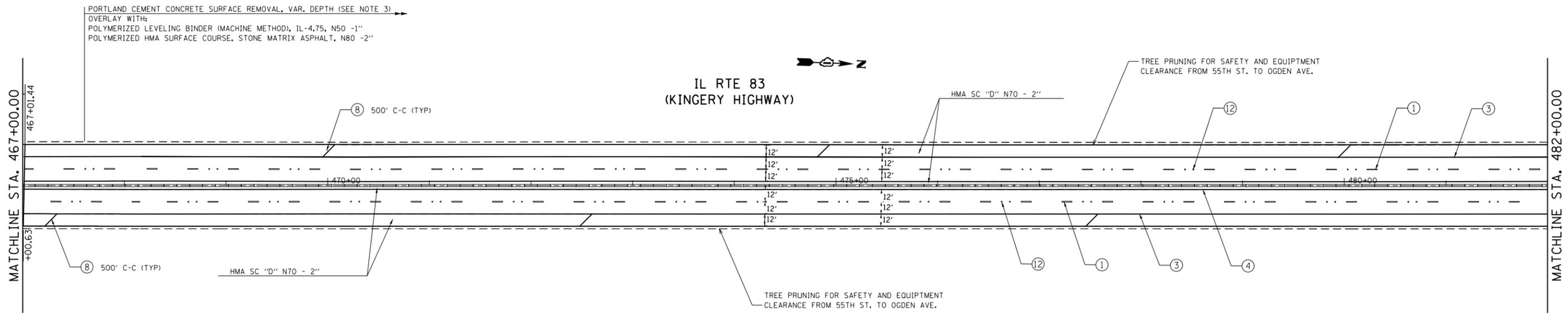
NOTES:

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- REFER TO TYPICAL SECTIONS FOR STRIP REFLECTIVE CRACK CONTROL TREATMENT LOCATIONS.

(R) MEDIAN REMOVAL PARTIAL DEPTH



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2111-shr-plan.dgn	DRAWN -	REVISED -			334	543R-3-RS	DUPAGE	51	16	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N51					
	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



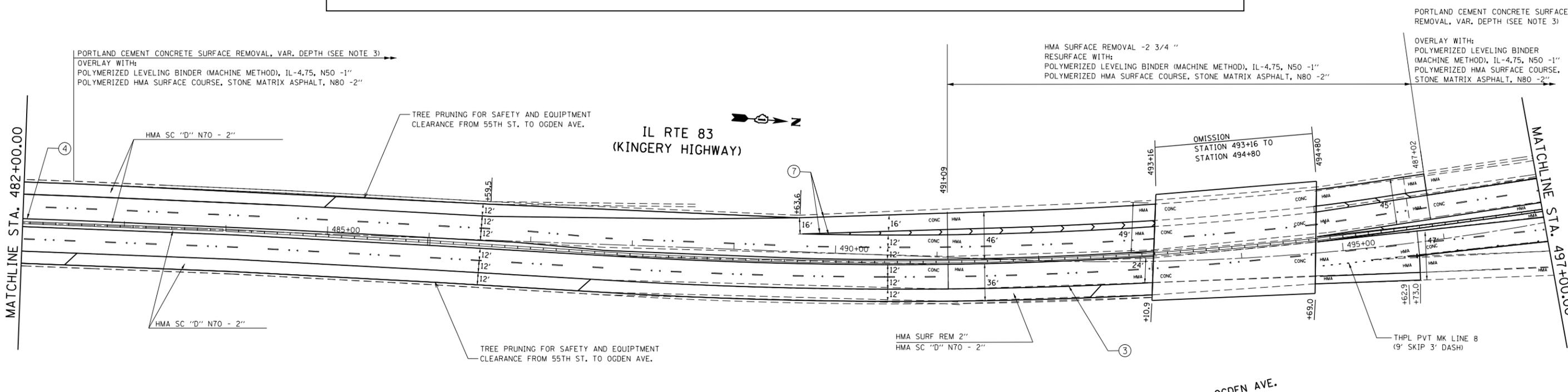
PAVEMENT MARKING LEGEND (SEE NOTE 1)

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⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C	⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES:

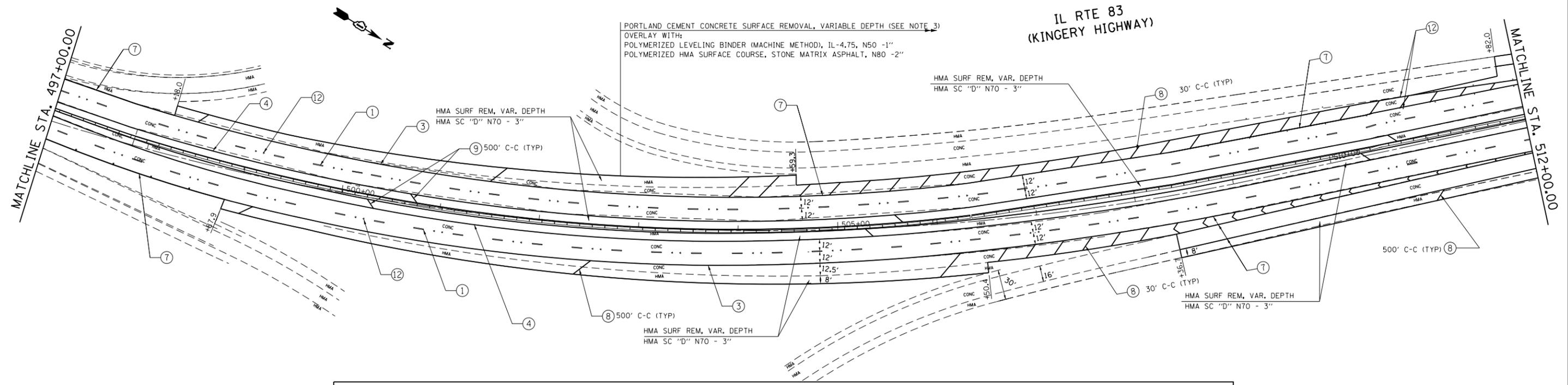
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⑫ MEDIAN REMOVAL PARTIAL DEPTH



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

IL RTE 83
(KINGERY HIGHWAY)



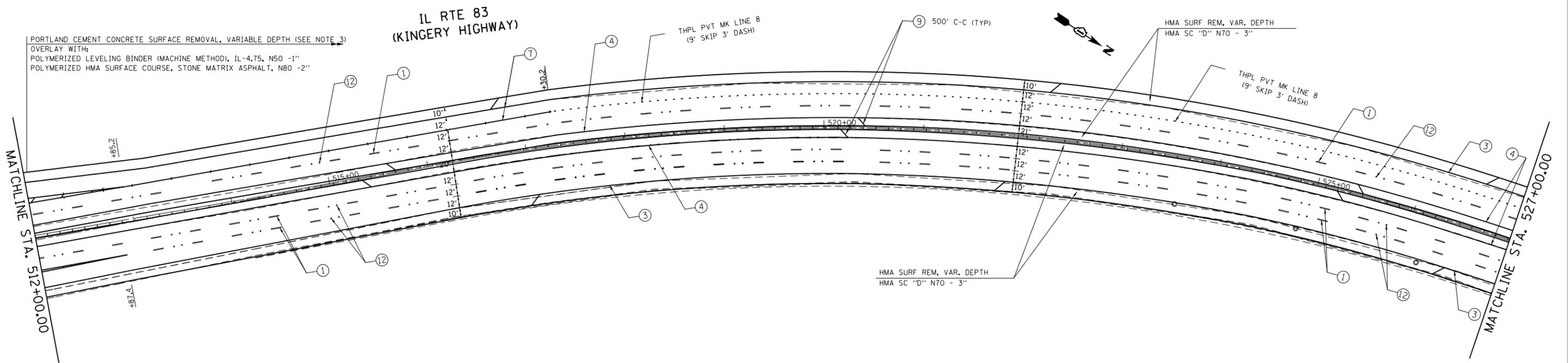
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE |
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(R) MEDIAN REMOVAL PARTIAL DEPTH



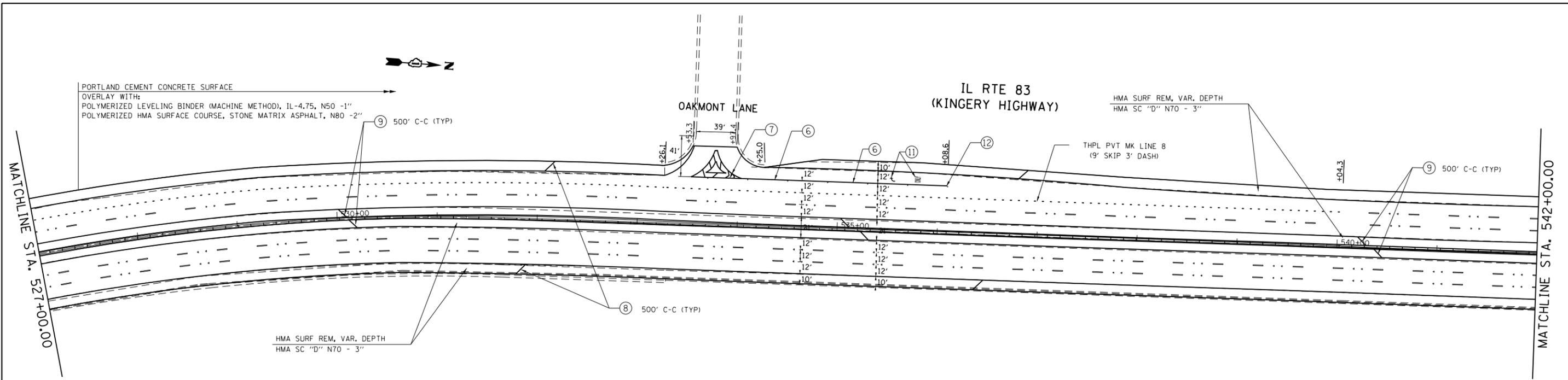
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	PLOT DATE = 6/13/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)
SCALE: 1" = 50'

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	543R-3-RS	DUPAGE	51	18
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

SHEET 5 OF 10 SHEETS STA. TO STA.



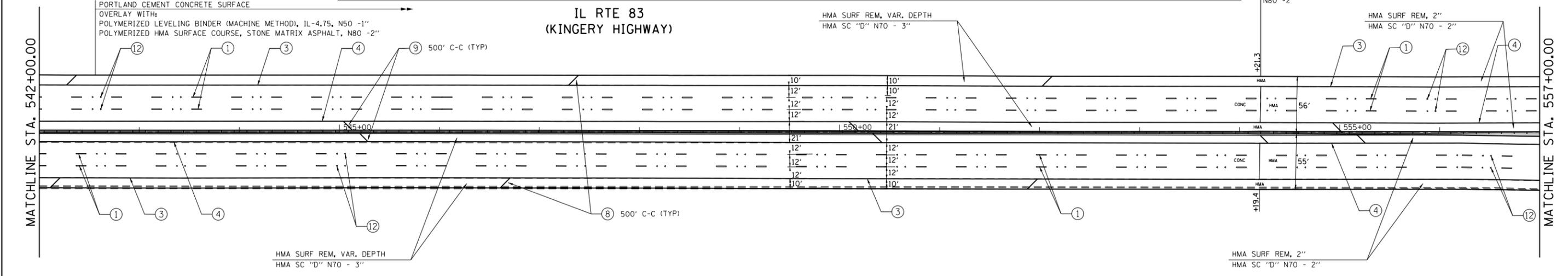
PAVEMENT MARKING LEGEND (SEE NOTE 1)

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⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE	⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

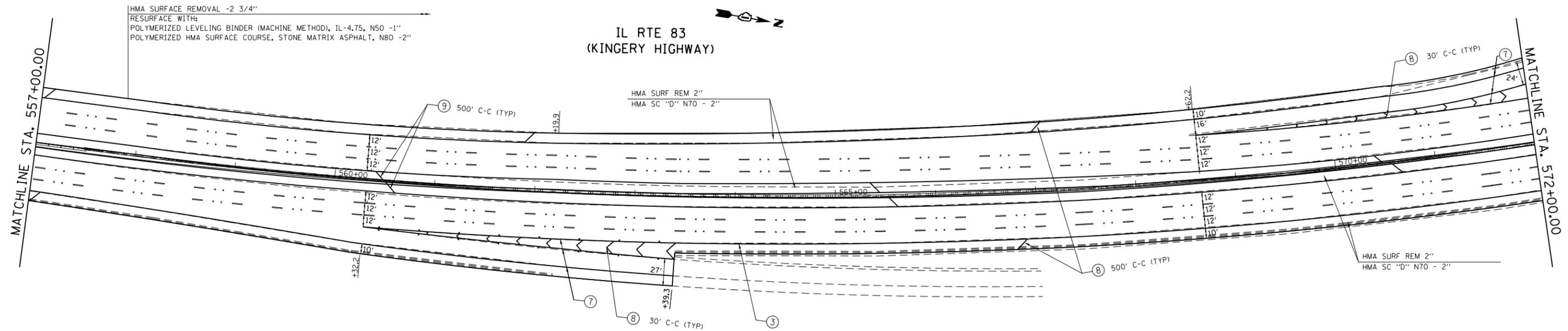
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(R) MEDIAN REMOVAL PARTIAL DEPTH



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



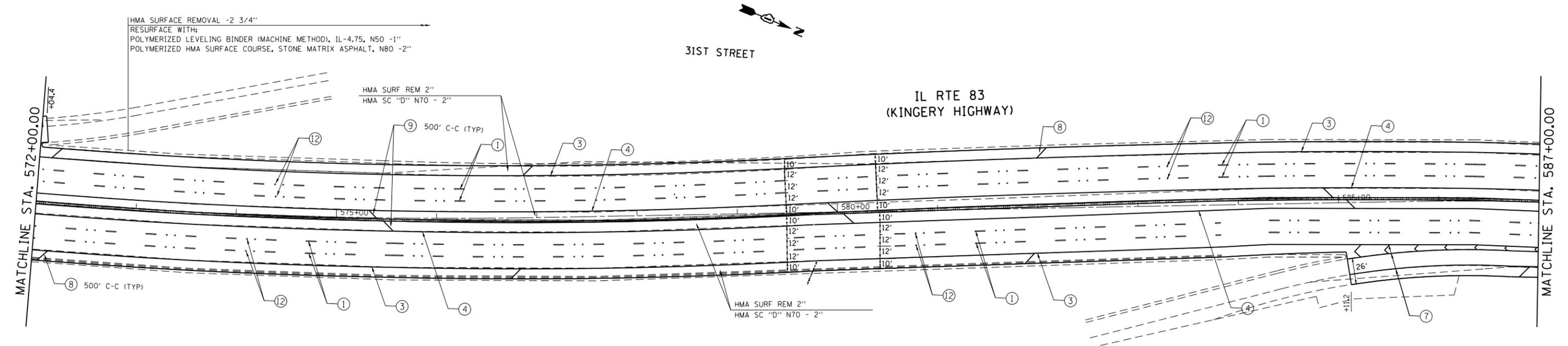
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH) | ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE |
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(R) MEDIAN REMOVAL PARTIAL DEPTH



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	PLOT DATE = 6/13/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL. ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)

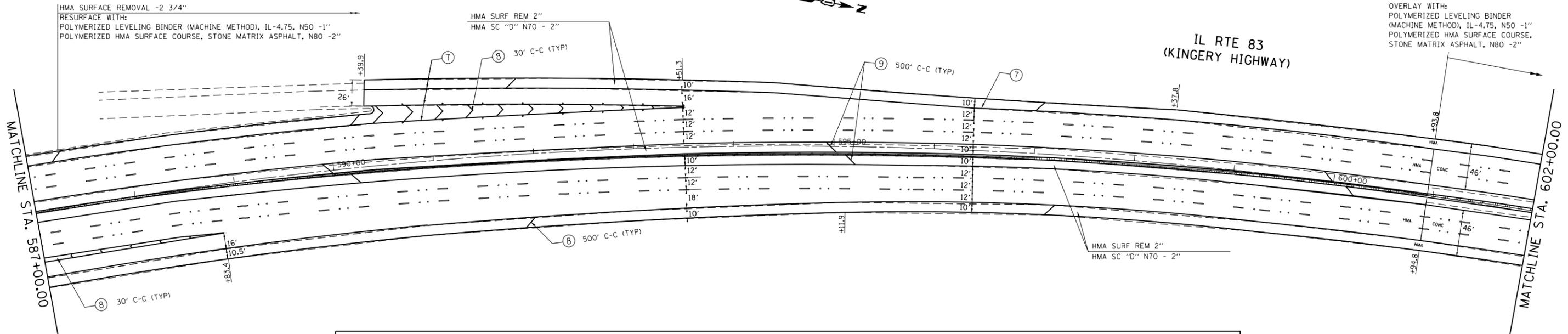
SCALE: 1" = 50' SHEET 7 OF 10 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	543R-3-RS	DUPAGE	51	20
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

PORTLAND CEMENT CONCRETE SURFACE

OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"

IL RTE 83
(KINGERY HIGHWAY)



PAVEMENT MARKING LEGEND (SEE NOTE 1)

- ① THERMOPLASTIC PAVEMENT MARKING 4" WHITE LANE LINE (30' SKIP 10' DASH)
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- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" DOUBLE YELLOW LINE 11" C-C
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 8" WHITE LINE
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- ⑪ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS (TYP)
- ⑫ RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 2)

NOTES:

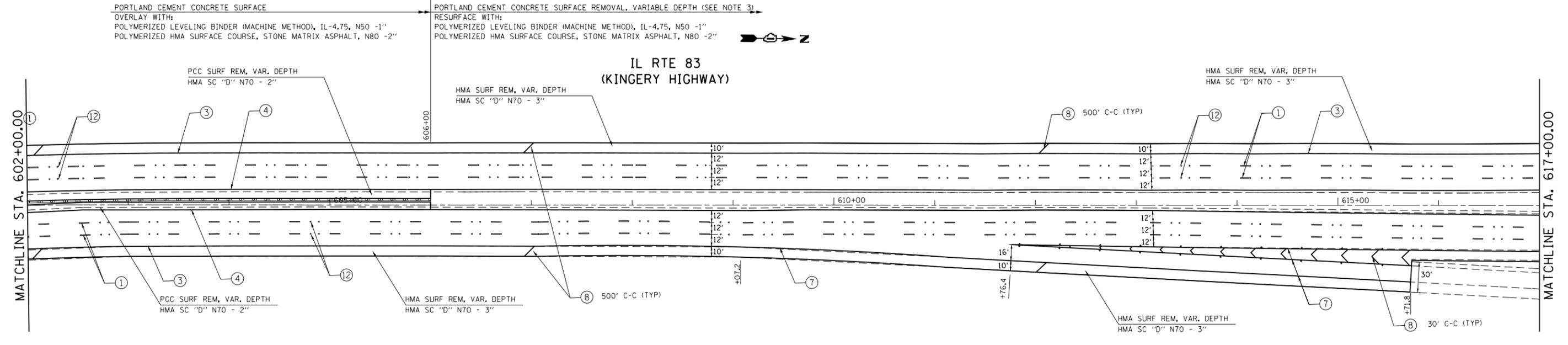
1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
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Ⓜ MEDIAN REMOVAL PARTIAL DEPTH

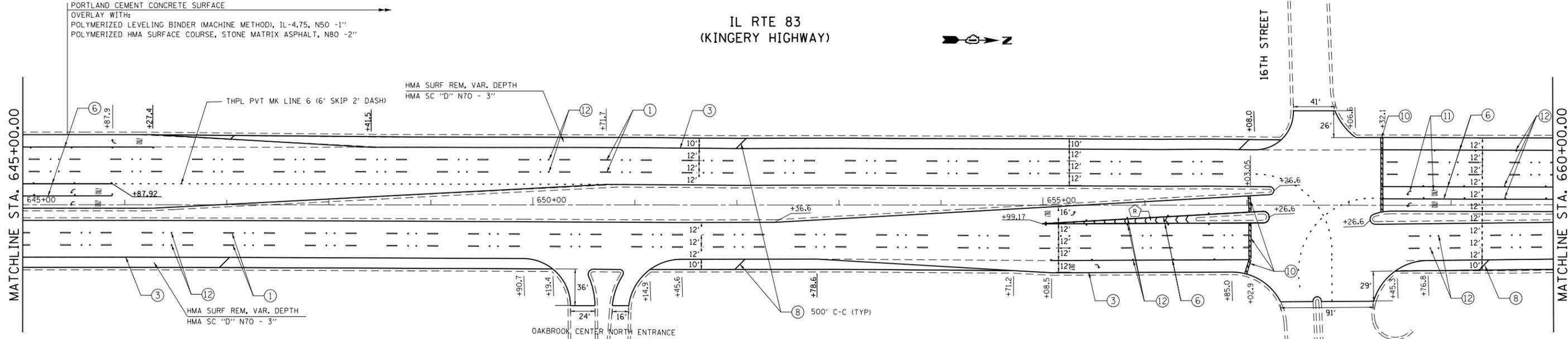
PORTLAND CEMENT CONCRETE SURFACE
OVERLAY WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"

PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VARIABLE DEPTH (SEE NOTE 3)
RESURFACE WITH:
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 -1"
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, N80 -2"

IL RTE 83
(KINGERY HIGHWAY)



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2111-sht-plan.dgn	DRAWN -	REVISED -			334	543R-3-RS	DUPAGE	51	21	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N51					
	PLOT DATE = 6/13/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



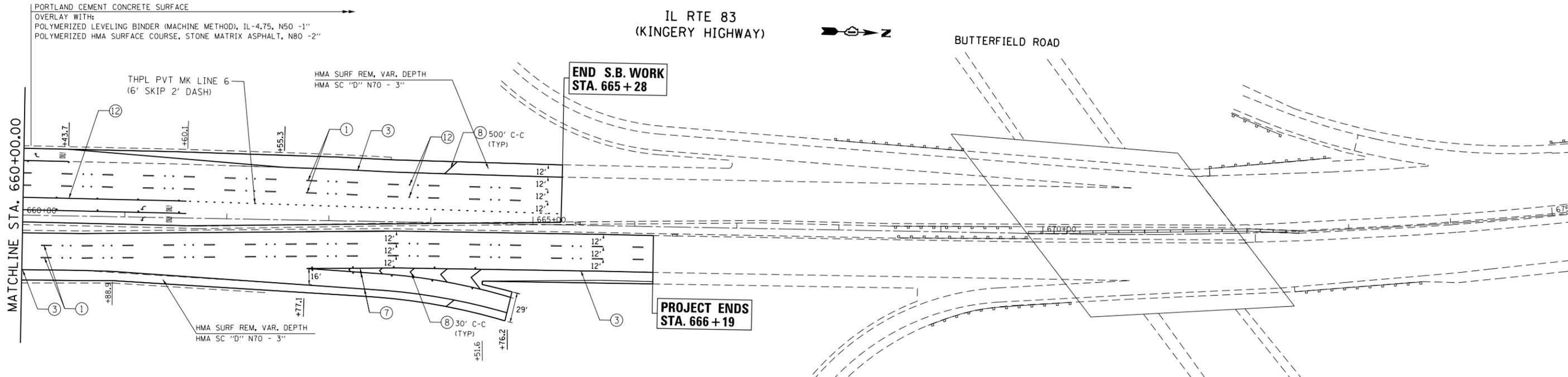
PAVEMENT MARKING LEGEND (SEE NOTE 1)

- | | |
|---|--|
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MEDIAN REMOVAL PARTIAL DEPTH



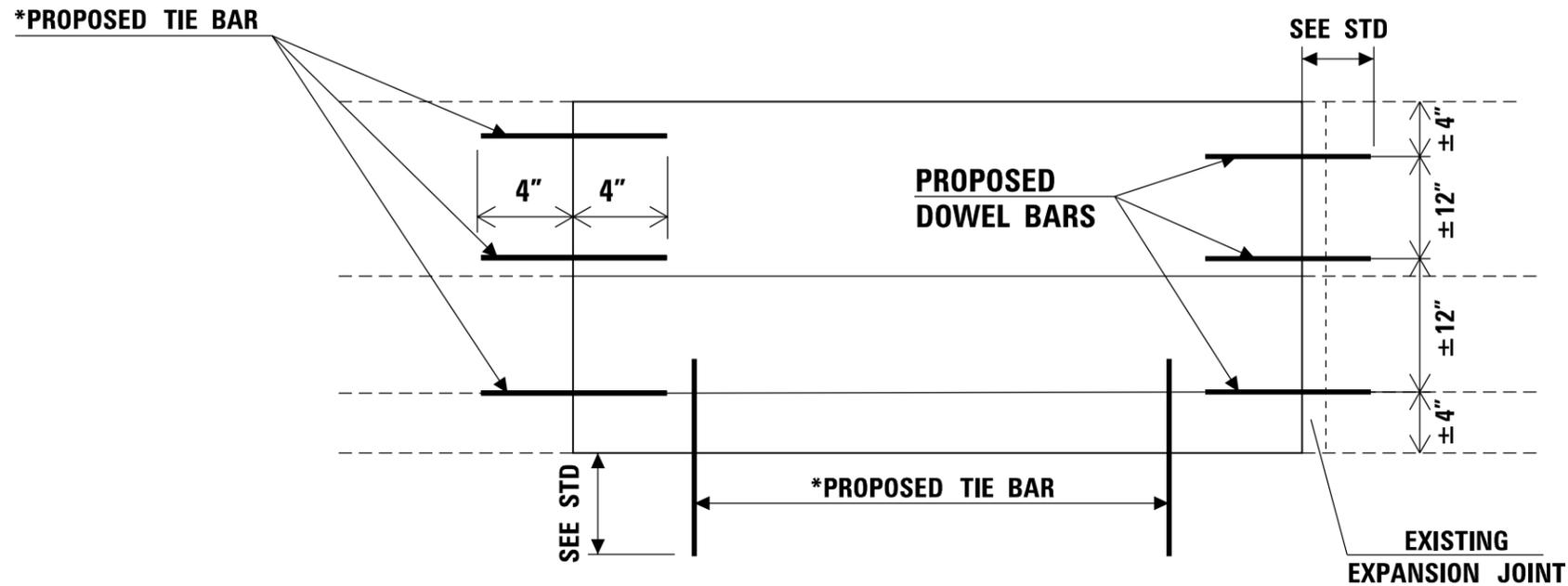
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY/PAVEMENT MARKING PLAN
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
334	543R-3-RS	DUPAGE	51	23
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50' SHEET 10 OF 10 SHEETS STA. TO STA.



FULL DEPTH REPAIR

NOTE:

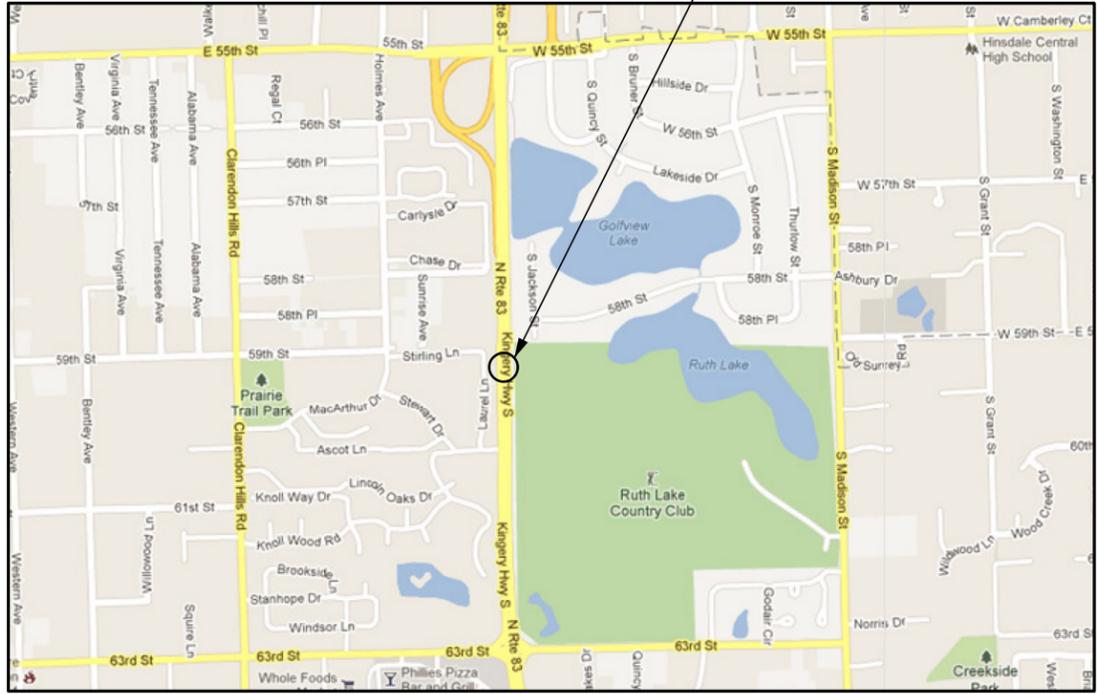
ALL BARRIER WALL REPAIR SHALL BE COMPLETED IN ACCORDANCE WITH STATE STANDARDS 637001 AND 637006 AND SECTION 637 OF THE STANDARD SPECIFICATIONS AND AS SHOWN ON THE PLANS. ALL PROPOSED DOWEL BARS AND TIE BARS SHALL BE DRILLED AND GROUTED IN PLACE. REINFORCEMENT SHALL BE INCLUDED AS SHOWN IN STANDARDS 637001 AND 637006 AND DESCRIBED IN SECTION 637 OF THE STANDARD SPECIFICATIONS EXCEPT WHEN A REINFORCEMENT CAGE IS ENCOUNTERED. IN SUCH A CASE, THE CONTRACTOR SHALL REMOVE THE DAMAGED PORTIONS OF THE CAGE AND SHALL REPLACE IT WITH BARS OF THE SAME SIZE AND SPACING. THE SCOPE OF REPAIR AT EACH LOCATION SHALL BE AS SHOWN ON THE PLANS AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

*USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS.

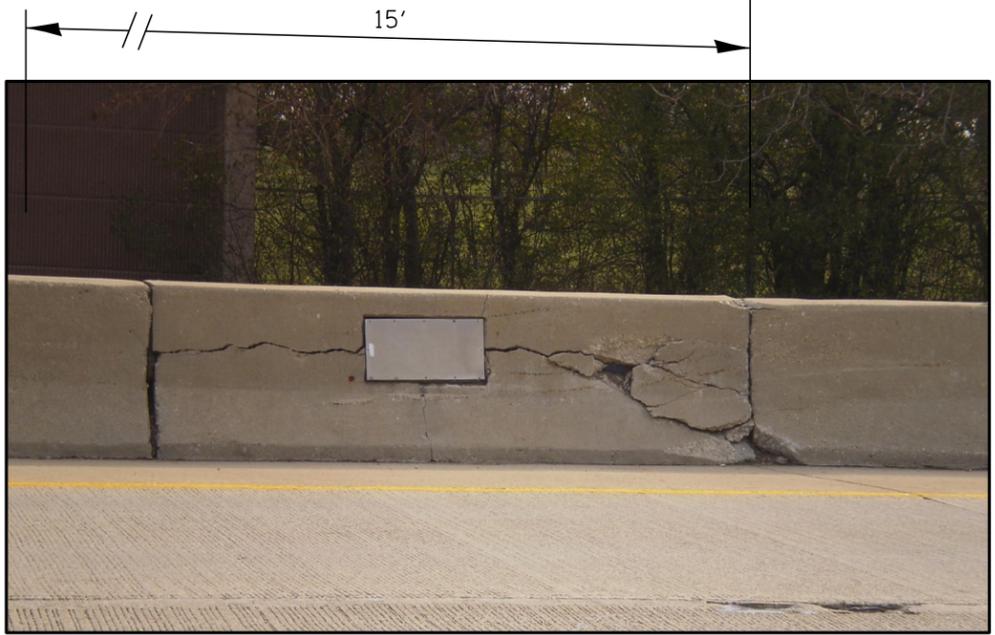
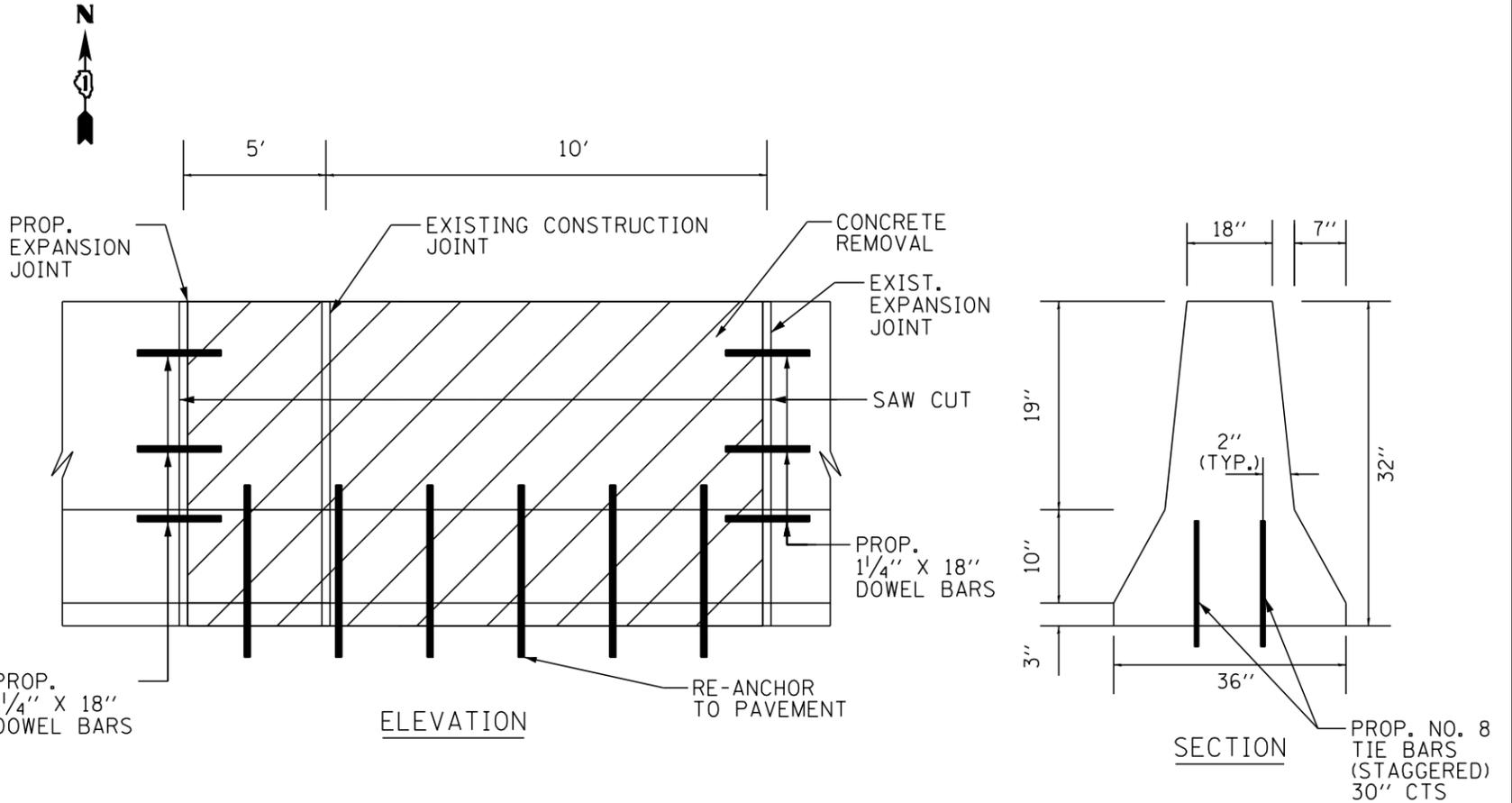
FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL REPAIR FOR FULL DEPTH (BARRIER WALL) IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\dot\marenade\d0316286\013	21111-sh-t-plen.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	24	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N51					
	PLOT DATE = 6/13/2014	DATE -	REVISED -			SCALE: N.T.S.		SHEET OF SHEETS		STA. TO STA.	

IL 83 (KINGERY HWY), SB
NORTH OF KNOLLWOOD RD, WEST FACE

R11E



VILLAGE OF WILLOWBROOK
DOWNERS GROVE TOWNSHIP
DUPAGE COUNTY



FULL DEPTH REPAIR
REPLACE 15' OF BARRIER WALL
FOR MORE DETAILS SEE SHEET 24

BILL OF MATERIALS

ITEM	UNIT	QUANTITY
CONCRETE REMOVAL	CU YD	4
CONCRETE STRUCTURE	CU YD	4
DOWEL BARS	EACH	6
TIE BARS	EACH	8
SAW CUTS	FOOT	6
PROTECTIVE COAT	SQ YD	16

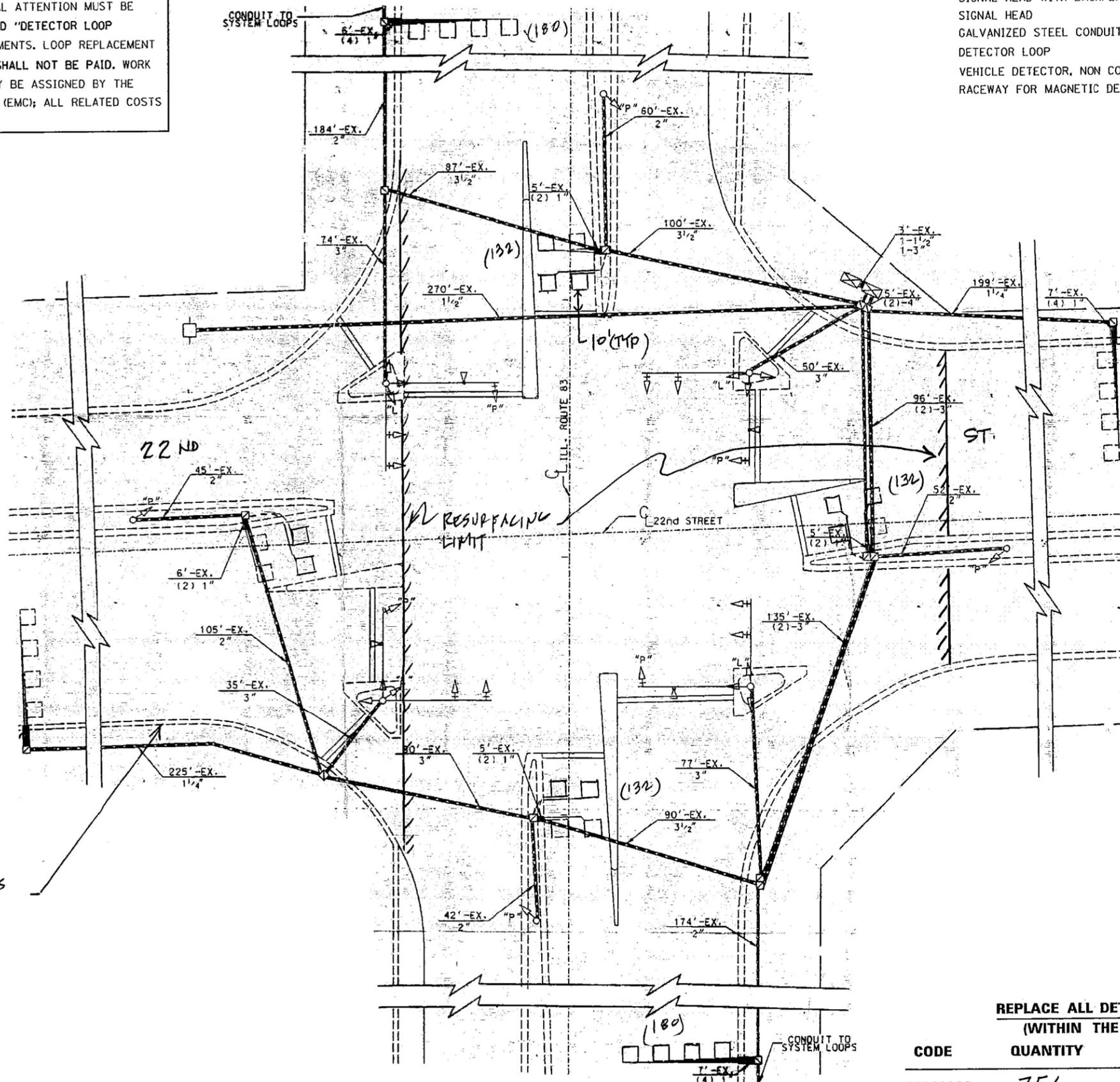
NOTES:
SAW CUTS SHALL MEET EXISTING PCC PAVEMENT TRANSVERSE JOINTS.
CONCRETE REMOVAL SHALL EXTEND A MINIMUM OF 6" PAST DAMAGE OR TO JOINT IF WITHIN 2'-0".
EXISTING BARS TO BE CLEANED, STRAIGHTENED, AND INCORPORATED IN NEW CONSTRUCTION.
BARS TO BE EXPOXY GROUTED ACCORDING TO ARTICLE 584 OF THE STANDARD SPECIFICATIONS.

TRAFFIC DATA:
2011 ADT = 66,400
SPEED LIMIT = 40-55 MPH

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		



SEE CHANGES TO GEOMETRICS THIS LEG OF INTERSECT. CONTRACT 60D12

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	756	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -
et:\pw_work\pidot\marenade\d0316286\012111-sht-pln.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/31/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)**

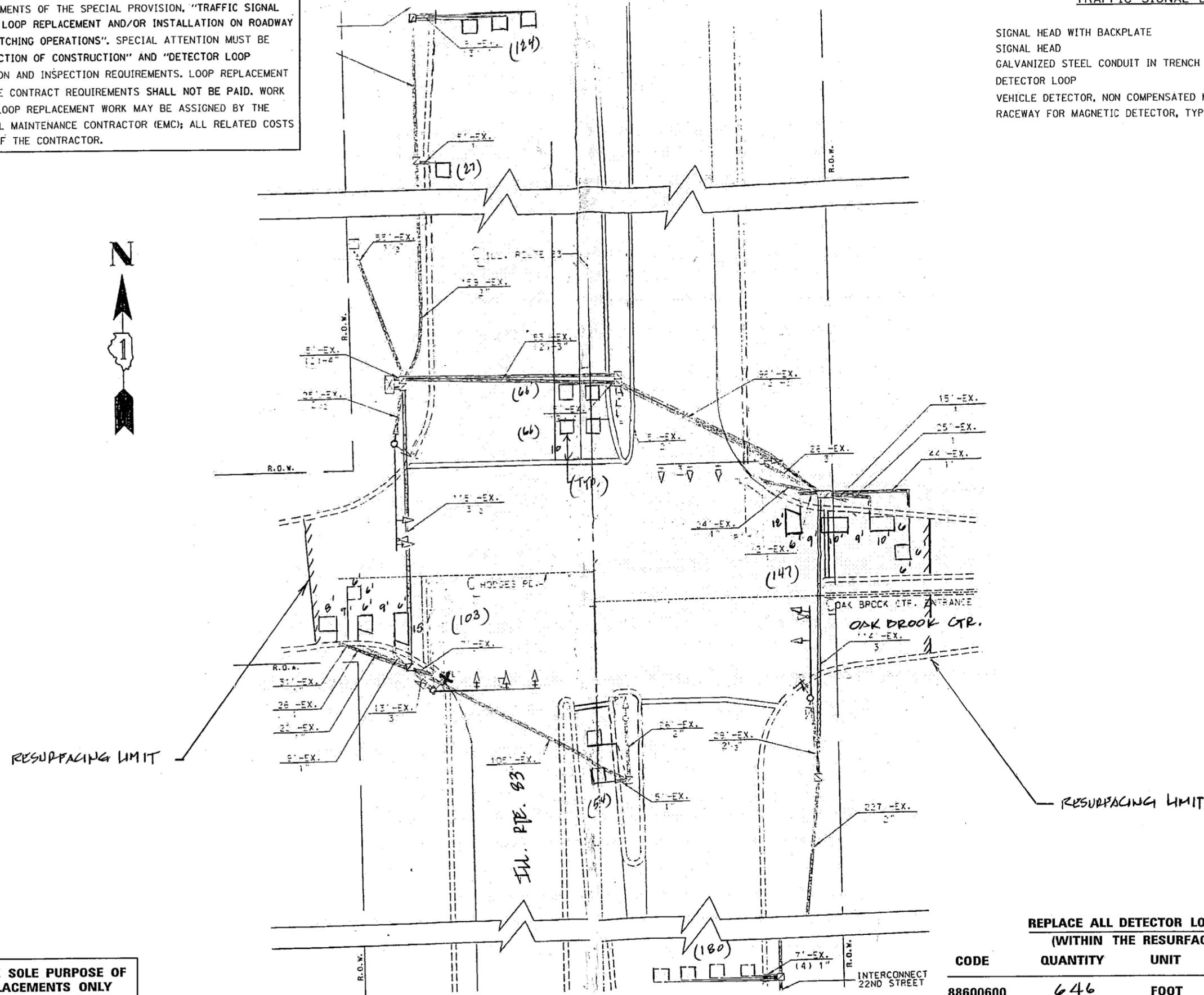
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	27
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	□	□
SIGNAL HEAD	□	□
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	□	□
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	□	□



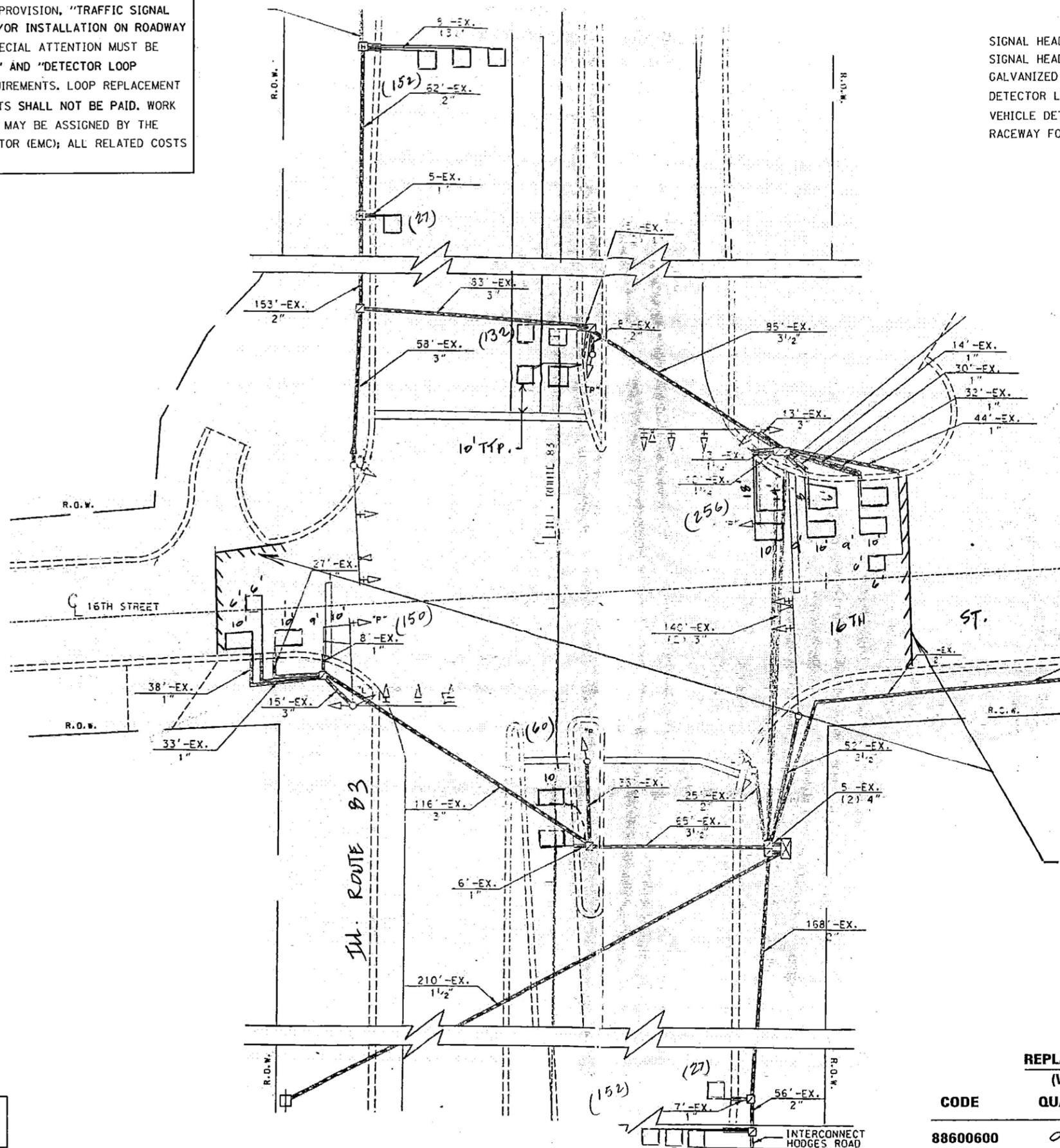
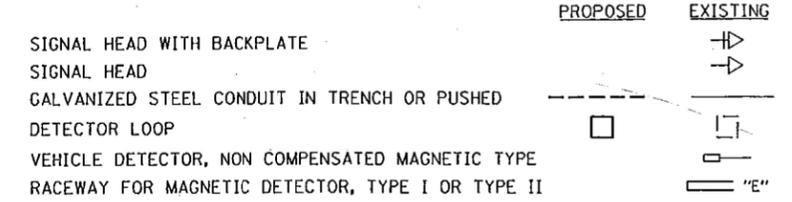
THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	646	FOOT	DETECTOR LOOP, REPLACEMENT

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

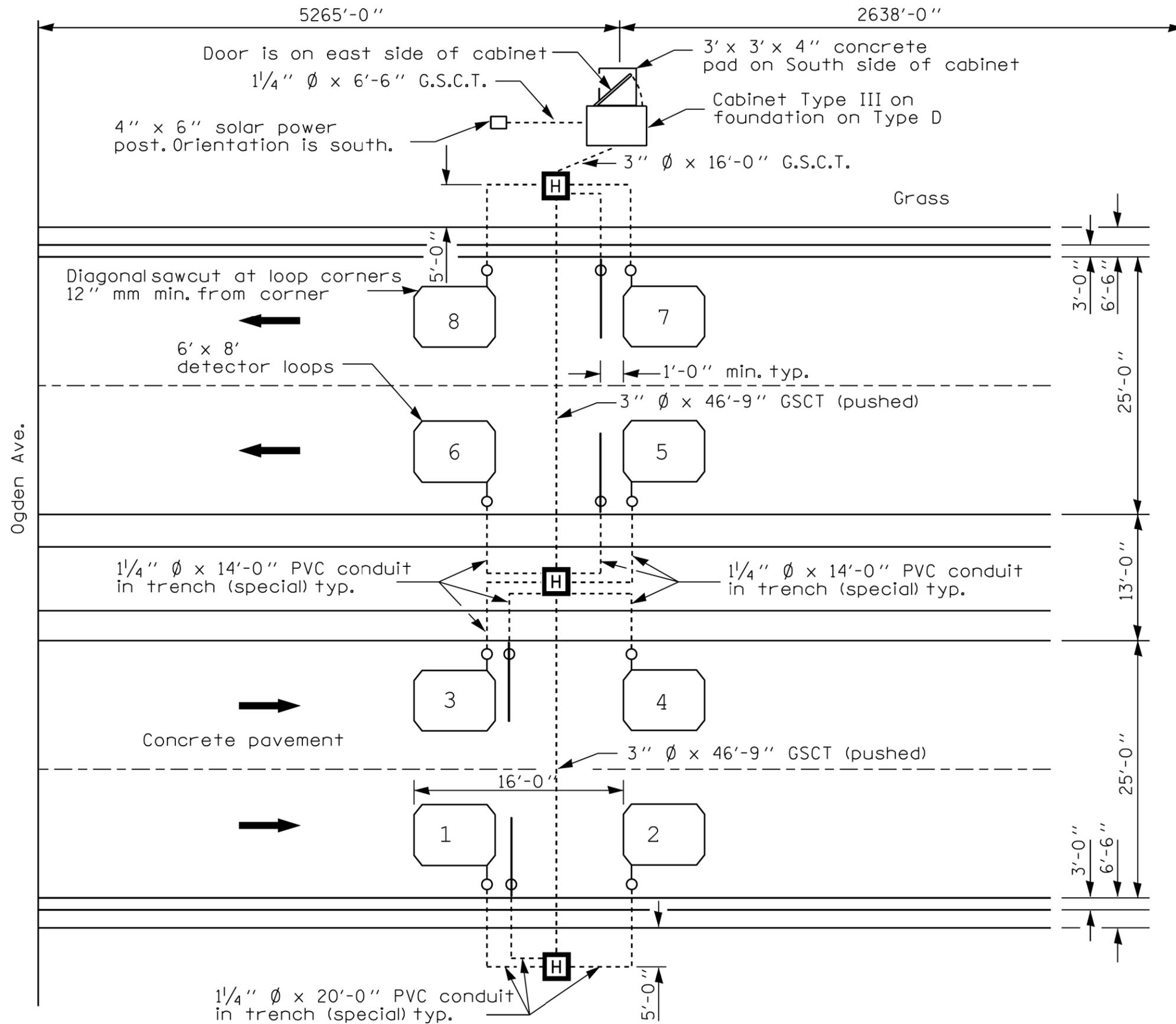
TRAFFIC SIGNAL LEGEND



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
88600600	956	FOOT	DETECTOR LOOP, REPLACEMENT

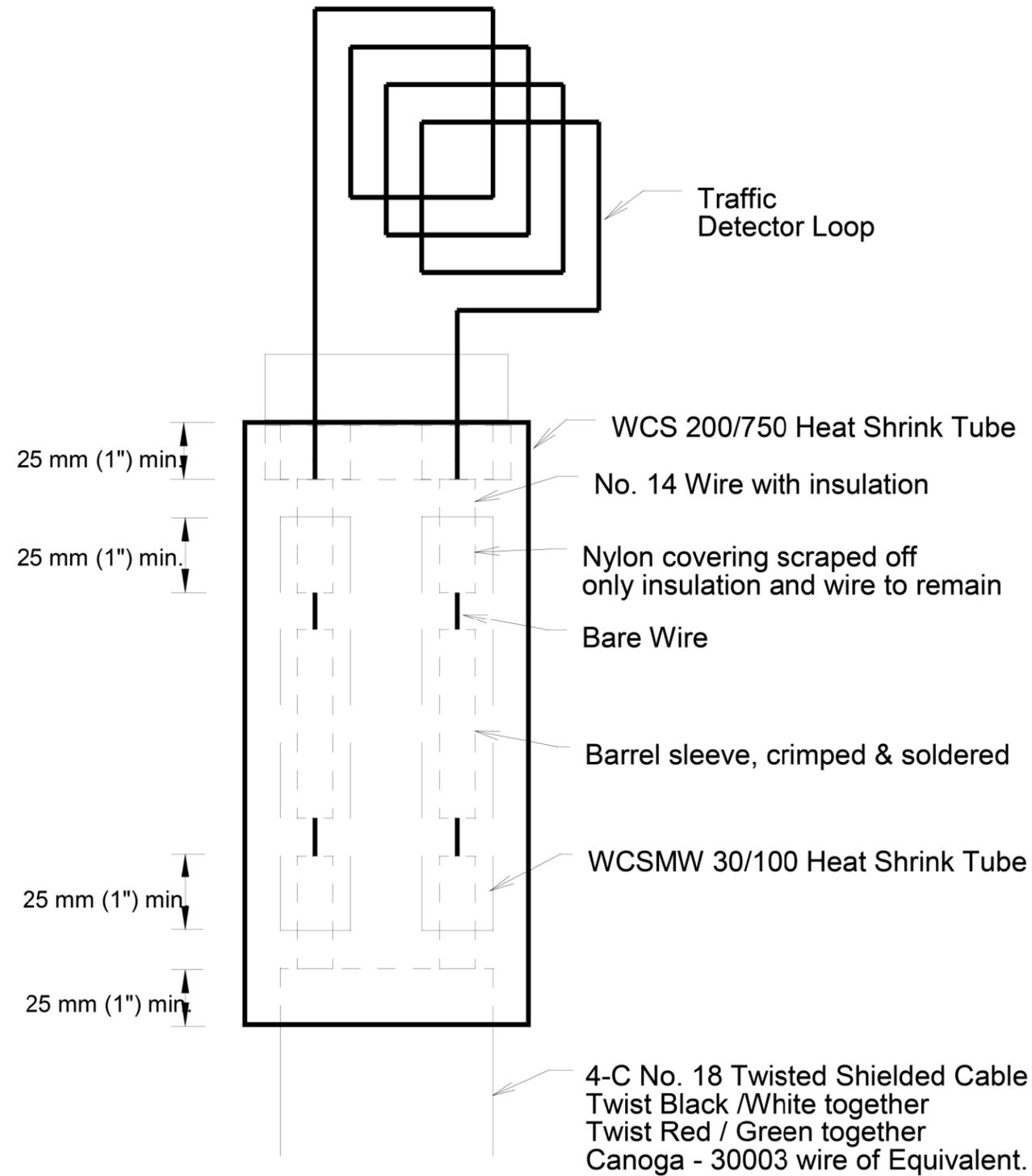


55 th St. Overpass

Notes:
 Loops must be centered in the traffic lanes.
 Not to scale.
 Classification

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ATR SITE #1340 IL. ROUTE 83 (KINGERY HIGHWAY) KNOLLWOOD ROAD TO IL. ROUTE 56 (BUTTERFIELD ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	2111-sht-plan.dgn	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	30	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N51					
	PLOT DATE = 5/31/2014	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

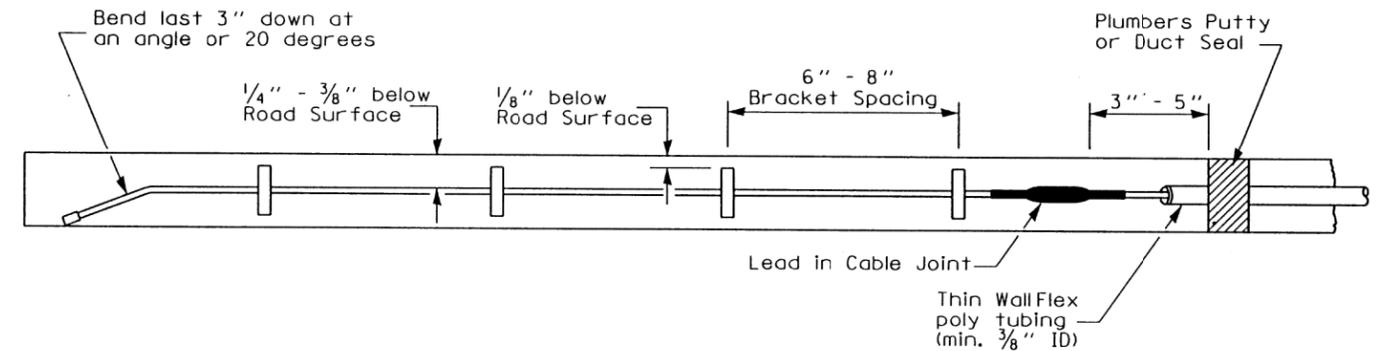
LOOP SPLICING REQUIREMENTS



Minimum 25 mm (1") Heat Shrink Tube overlap on wire.
PVC & Shielded cable to form water tight seal.

NOTE: NOT TO SCALE

PIEZO AXLE SENSORS DETAIL



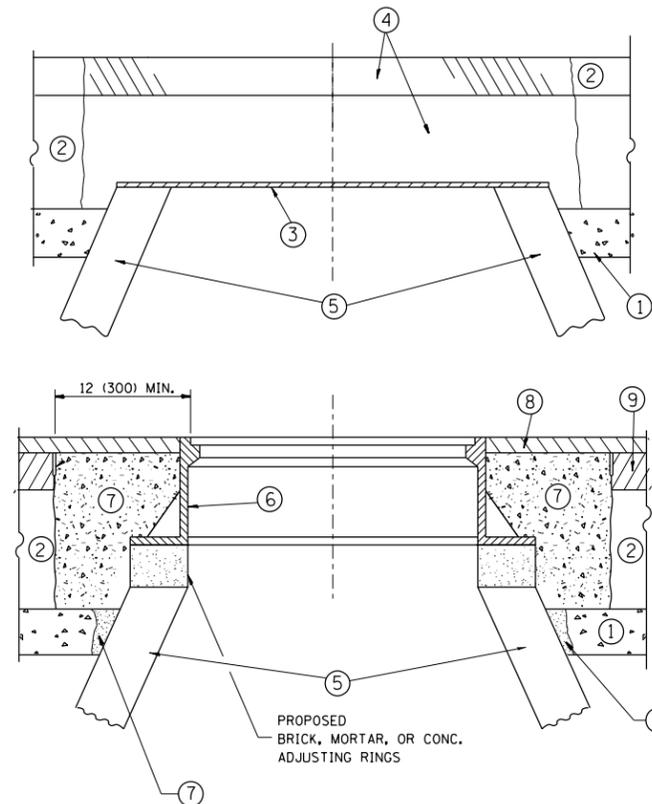
FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -
c:\pw_work\pwidot\marenade\d0316286\012111-sht-pln.dgn		DRAWN -	REVISED -
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/31/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ATR SITE #1340 DETAILS
IL ROUTE 83 (KINGERY HIGHWAY)
KNOLLWOOD ROAD TO IL ROUTE 56 (BUTTERFIELD ROAD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	31
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

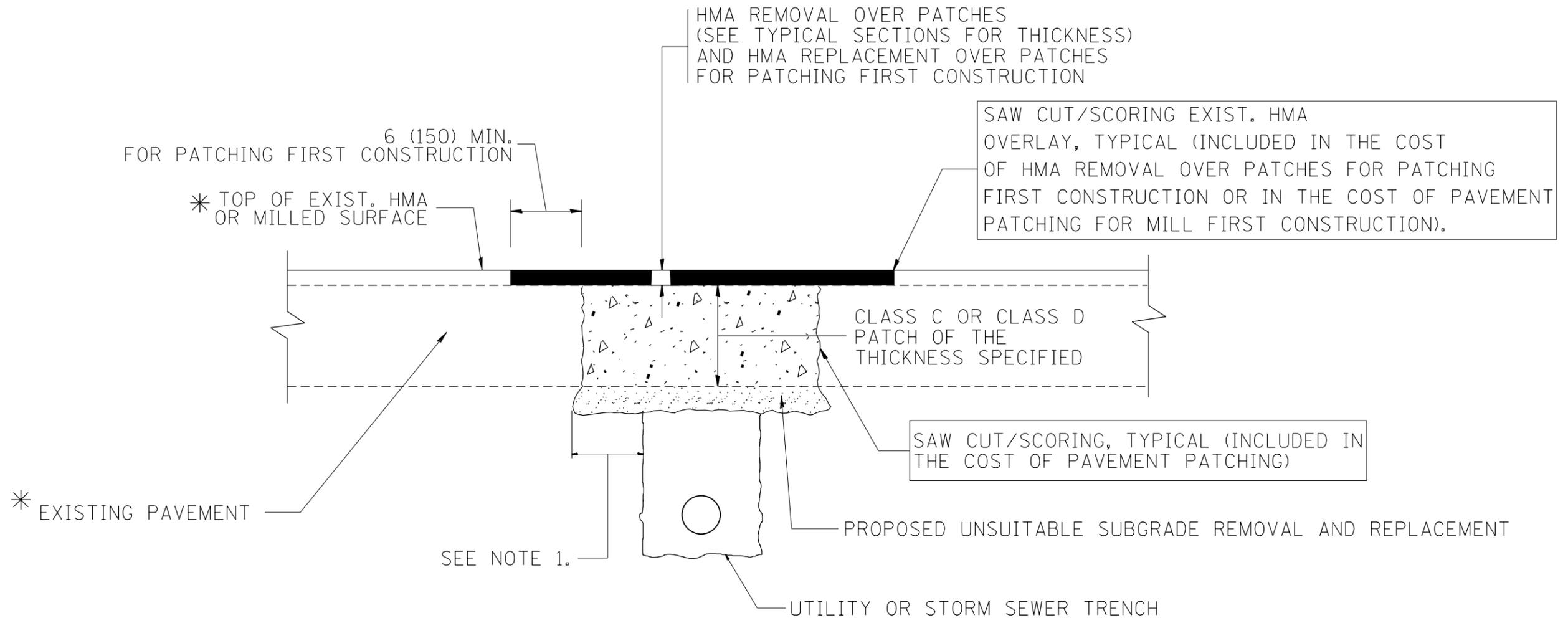
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = marenade	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
et:\pw\work\p\idot\marenade\d0316286\01321111-sh-t-plen.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 6/13/2014	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	32
BD600-03 (BD-8)		CONTRACT NO. 60N51		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
et:\pw\work\p\dot\marenade\d0316286\0132111-sh-t-pln.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 6/13/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	33
BD400-04 (BD-22)			CONTRACT NO. 60N51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

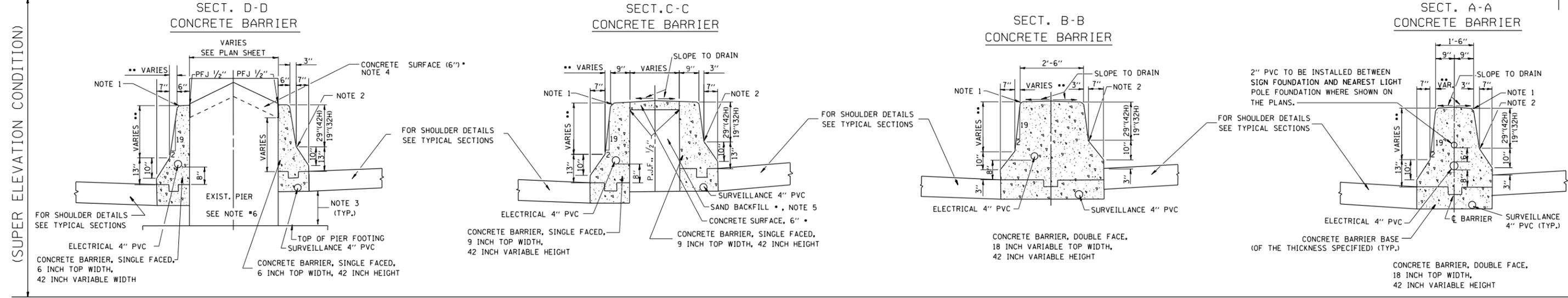
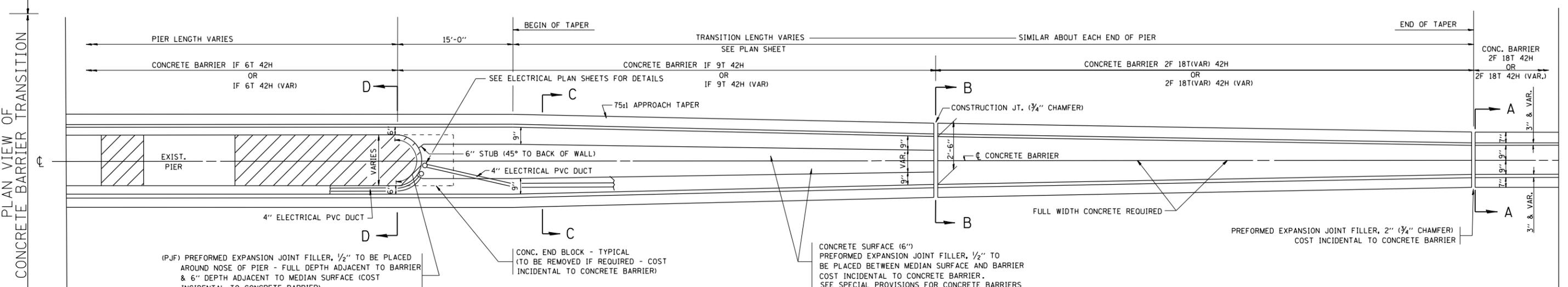
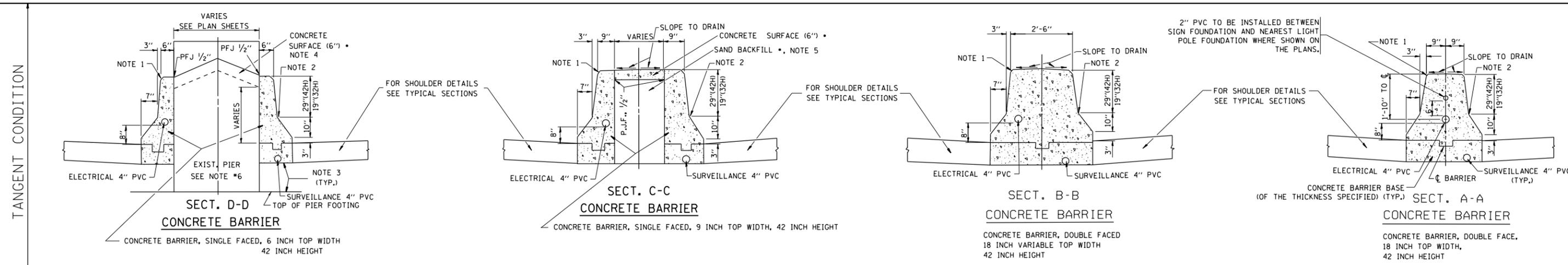
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\pwidot\marenade\d0316286\012111-sh-t-plan.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			344	543R-3-RS	DUPAGE	51	34
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 60N51		
PLOT DATE = 6/13/2014		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



GENERAL NOTES - FOR UNDERDRAIN DETAILS SEE TYPICAL SECTIONS

- PREFORMED JOINT FILLER SHALL BE INCIDENTAL TO THE CONCRETE BARRIER OF THE TYPE INVOLVED.
- FOR KEYWAY (F) DIMENSIONS, SEE TYPICAL SECTIONS
- CONCRETE BARRIER BASE PAY ITEM IS TO BE INCLUDED IF THE BARRIER IS CONSTRUCTED MONOLITHIC OR JOINTED TO BASE. IF JOINTED CONTRACTORS WILL HAVE THE OPTION OF USING A KEYWAY OR TIE BARS AT 0.C.

NOTE 1 - 3/4" CHAMFER OR 1" RADIUS (OPTIONAL)

NOTE 2 - 10" RADIUS (OPTIONAL)

NOTE 3 - EXTEND BOTTOM OF BARRIER TO FOOTING ONLY WHEN DEPTH IS 6" OR LESS, OTHERWISE MAINTAIN SAME DEPTH AS BOTTOM OF SHOULDER

NOTE 4 - PIER FILLER MATERIAL TO BE CONCRETE IF MINIMUM 6" THICKNESS WILL BE MAINTAINED. IF 6" THICKNESS CANNOT BE MAINTAINED USE ASPHALT FILLER MATERIAL AS DIRECTED BY THE ENGINEER.

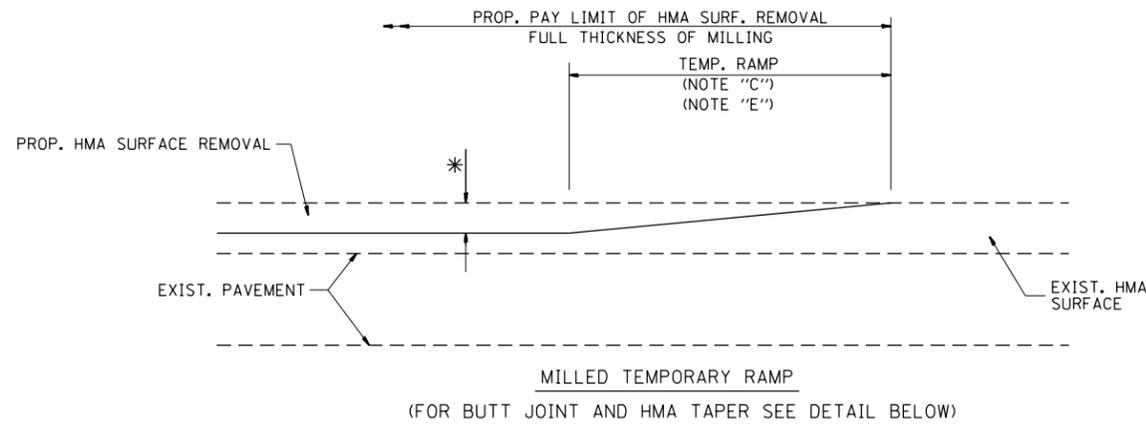
NOTE 5 - SAND BACKFILL AND CONCRETE SURFACE WILL BE REQUIRED. FILLING WITH CONCRETE WILL NOT BE ALLOWED.

NOTE 6 - IF PIER IS NEW CONSTRUCTION BARRIER WALL MAY BE MONOLITHIC

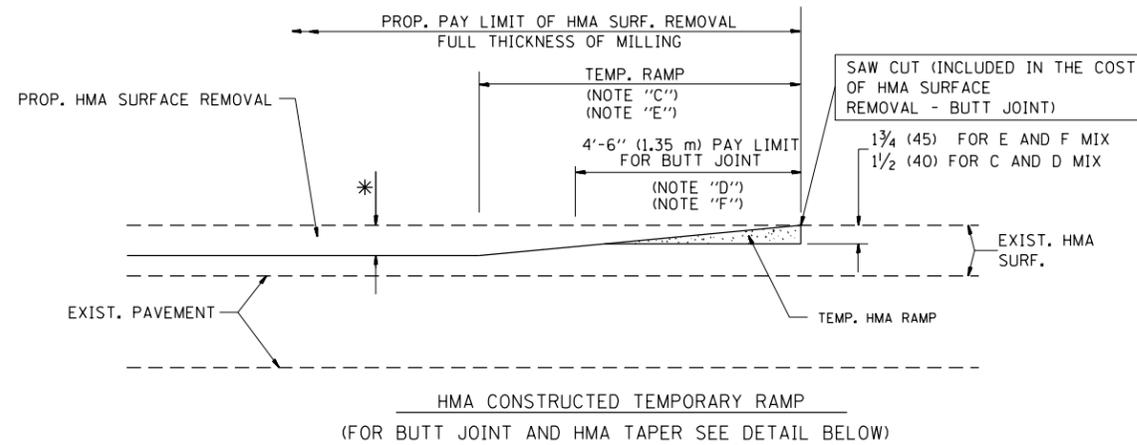
•• MAINTAIN SLOPE OF FACE AS SHOWN ON DETAIL. HEIGHT AND WIDTH OF BARRIER INCREASE WHERE A DIFFERENCE IN MEDIAN EDGE-OF-PAVEMENT GRADE ELEVATION EXISTS.

• COST OF SAND BACKFILL, CONCRETE SURFACE (6"), AND PIER FILLER MATERIAL WILL NOT BE INCIDENTAL.

FILE NAME =	USER NAME = marenade	DESIGNED - FORD	REVISED - FORD 12-06-88	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONCRETE BARRIER TRANSITION & GENERAL DETAILS, CONCRETE BARRIER BASE			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\p\dot\marenade\d0316286\DI	2111-shr-plan.dgn	DRAWN -	REVISED -		344	543R-3-RS	DUPAGE	51	35			
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -	REVISED -		BD-27			CONTRACT NO. 60N51				
PLOT DATE = 6/13/2014	DATE - 09-09-88	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

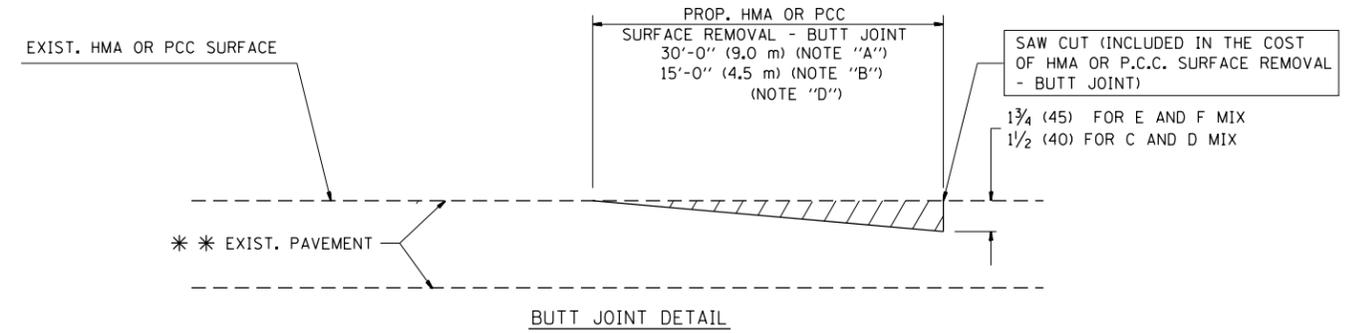


OPTION 1

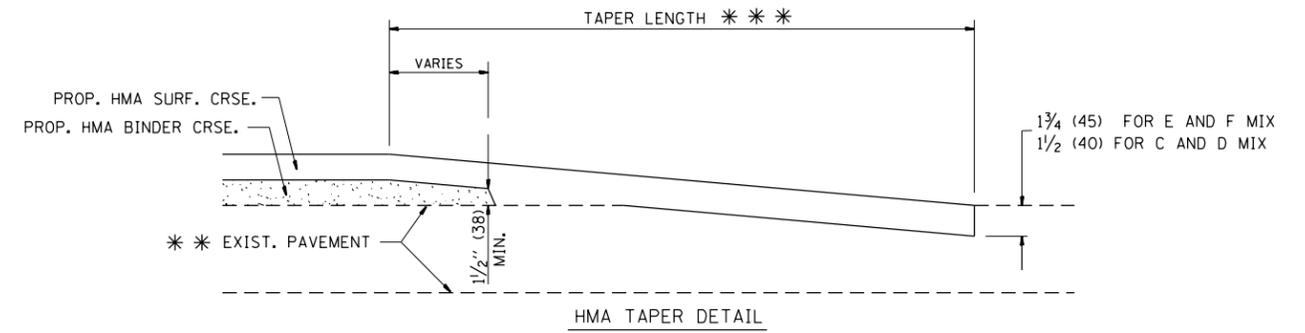


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

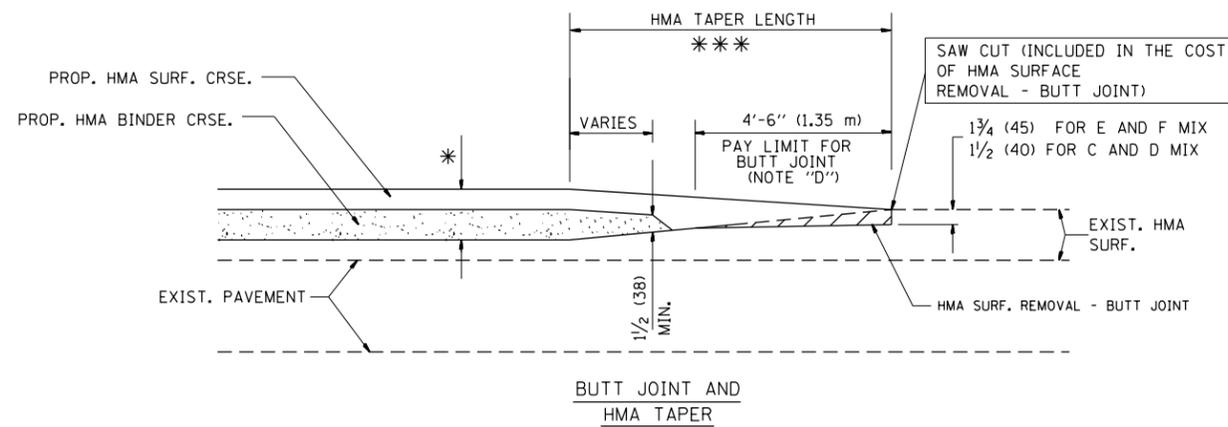
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



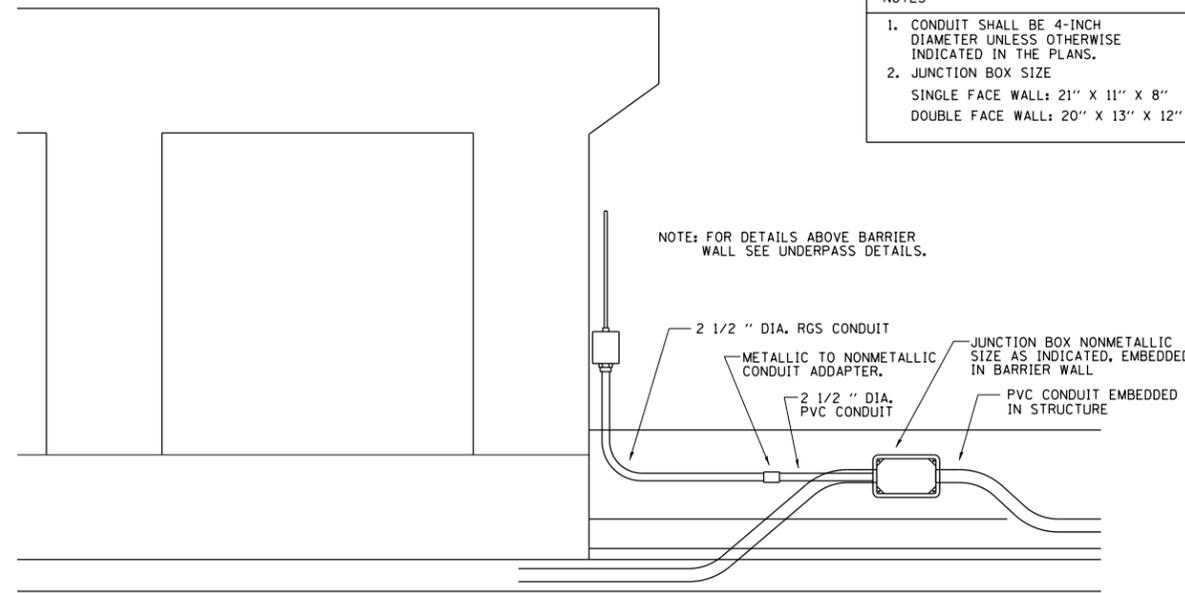
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = marenade	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
et:\pw\work\p\idot\marenade\d0316286\012111-sh-t-plen.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 6/13/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	36
BD400-05 BD32		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

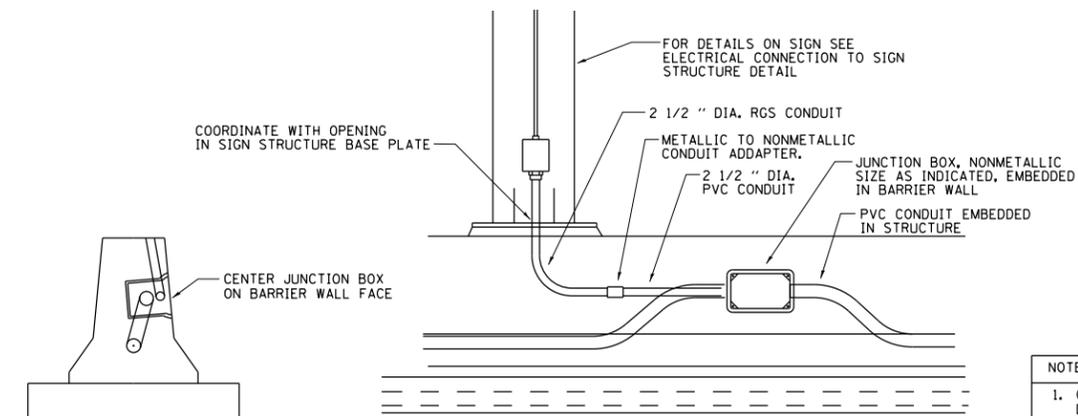


NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

NOTE: FOR DETAILS ABOVE BARRIER WALL SEE UNDERPASS DETAILS.

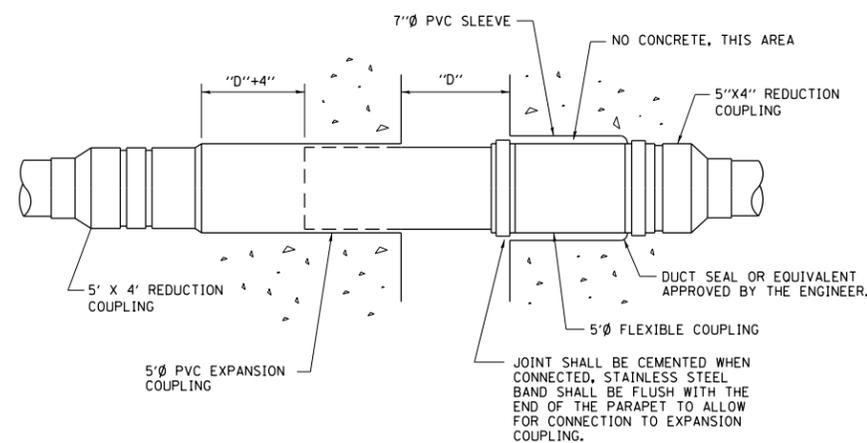
ED - BWD
ELECTRIC CONNECTION TO UNDERPASS LIGHTING



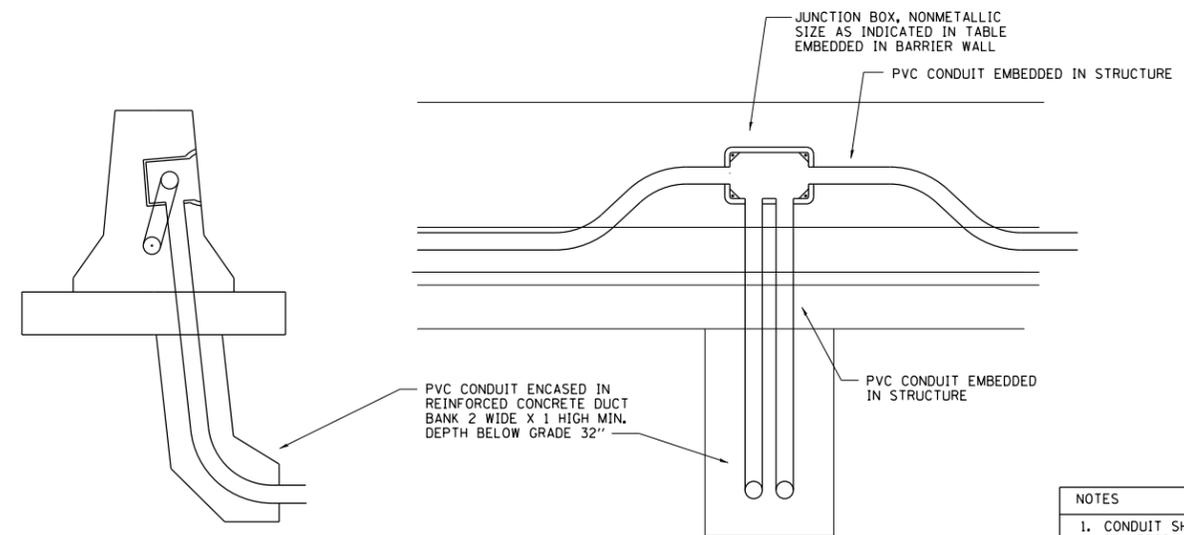
NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

ED - SGN
JUNCTION BOX EMBEDDED IN BARRIER WALL FOR SIGN LIGHTING



INSTALLATION OF CONDUIT
IN BRIDGE PARAPET EXPANSION JOINT
(N.T.S.)



NOTES

1. CONDUIT SHALL BE 4-INCH DIAMETER UNLESS OTHERWISE INDICATED IN THE PLANS.
2. JUNCTION BOX SIZE
SINGLE FACE WALL: 21" X 11" X 8"
DOUBLE FACE WALL: 20" X 13" X 12"

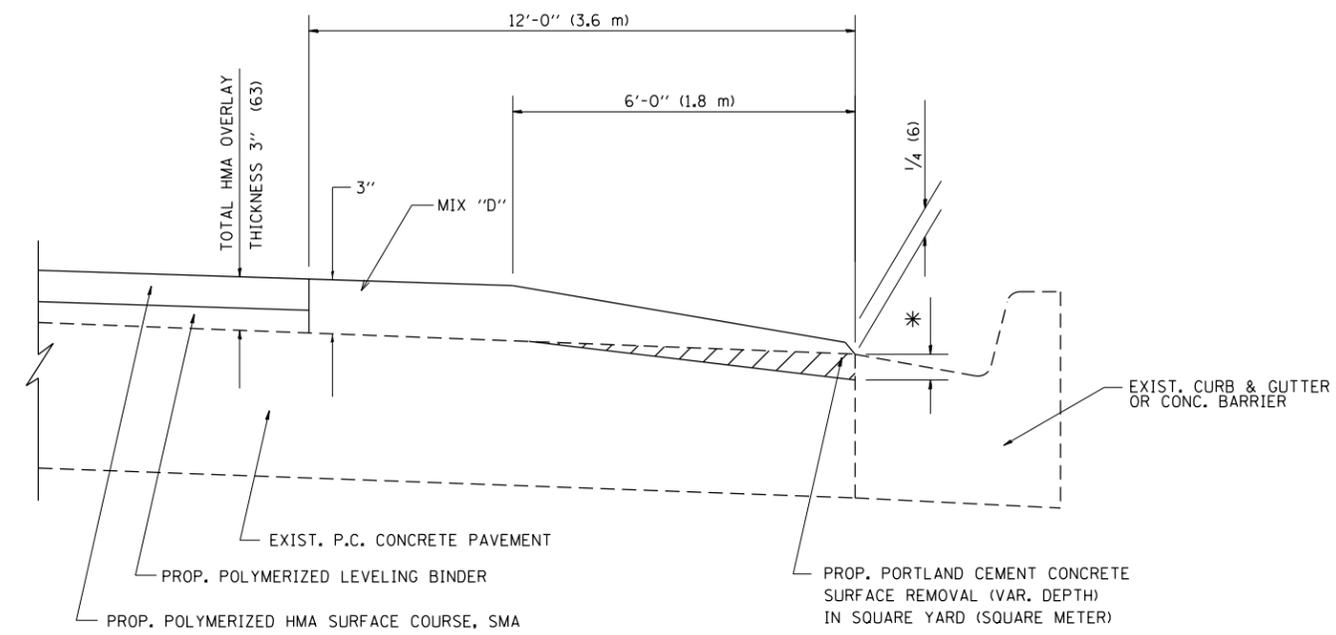
ED - BW
JUNCTION BOX EMBEDDED IN BARRIER WALL

FILE NAME = D132111-sh1-plandgn	USER NAME = masrenade	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE - 01-20-2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS ELECTRICAL DETAILS, SHEET B			
J BOX EMBEDDED IN BARRIER WALL - INSTALLATION OF CONDUIT IN BRIDGE PARAPET EXPANSION JOINT - ELECTRIC CONNECTION TO UNDERPASS LIGHTING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	37
BE-703		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE		
MIX	THICKNESS	* MILLING AT GUTTER FLAG
D	3"	2 3/4"

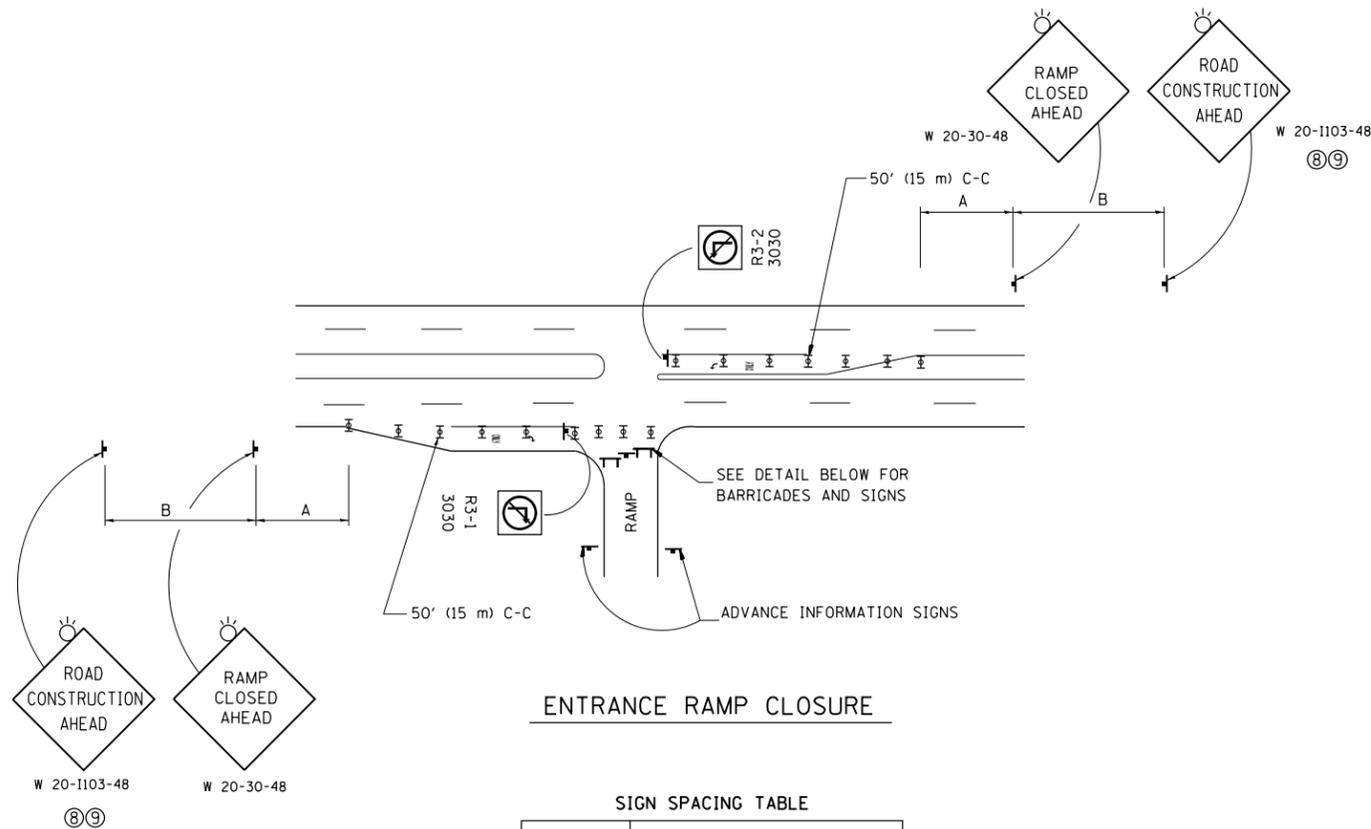
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C PAVEMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	38
CONTRACT NO. 60N51				
ILLINOIS FED. AID PROJECT				

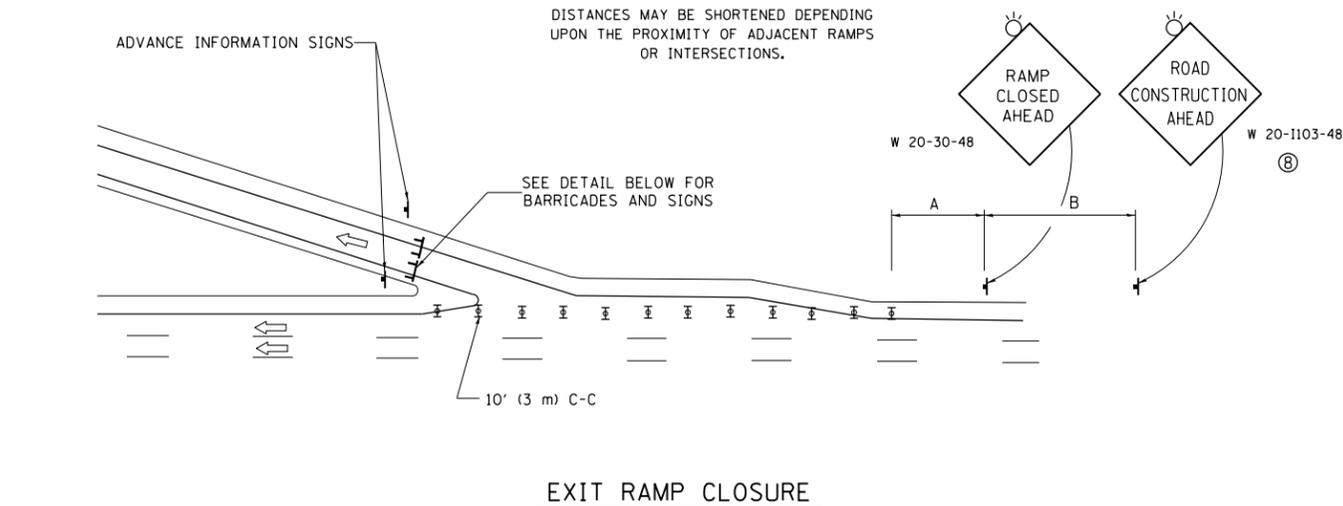


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

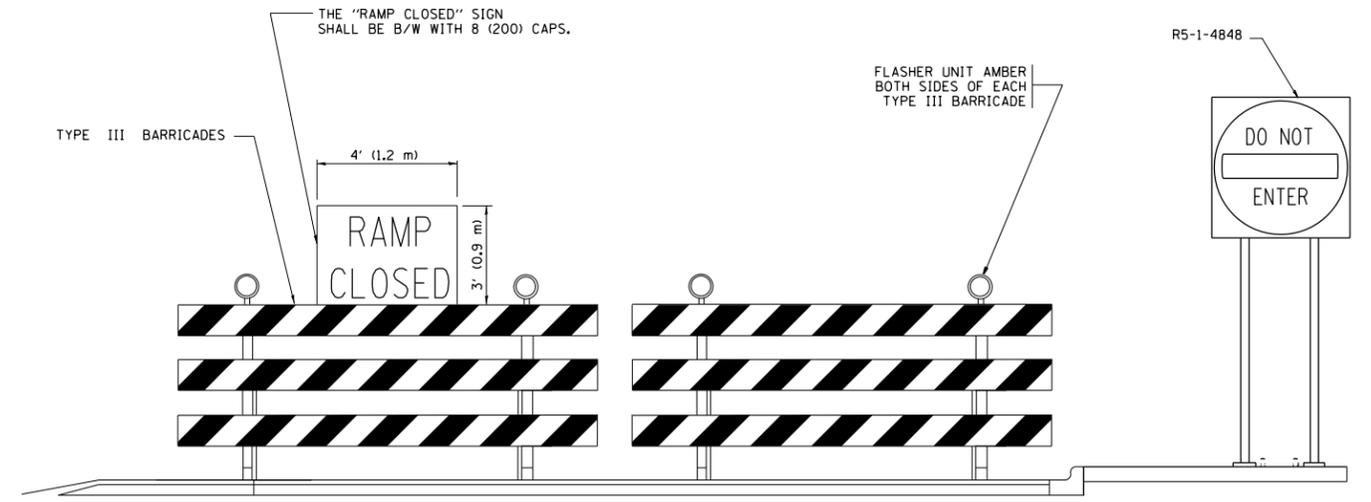
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

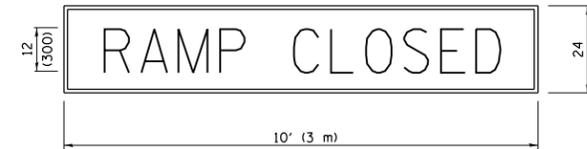
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

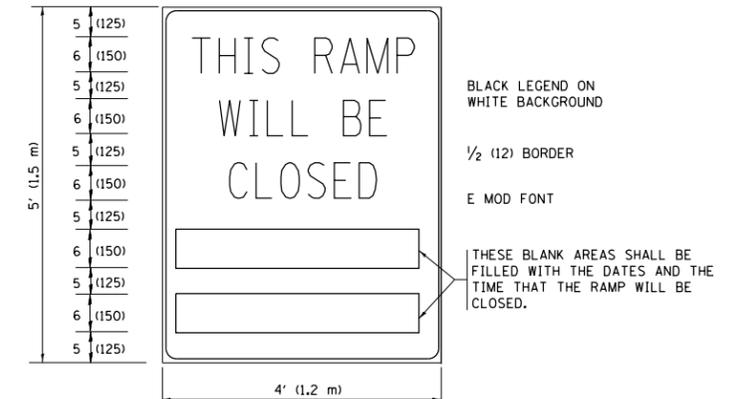
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

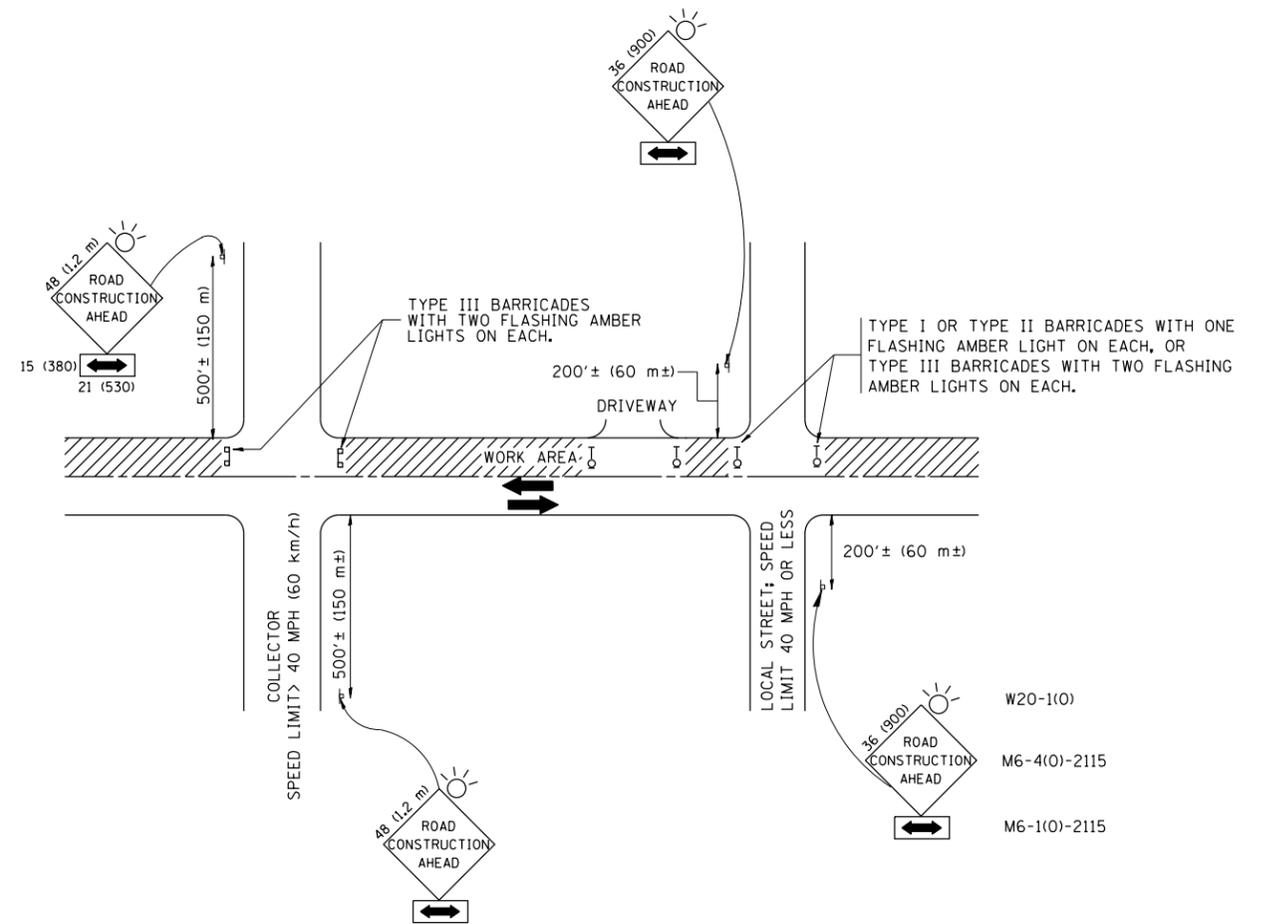
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - SPB 12-09
	PLOT DATE = 6/13/2014	DATE - 02-83	REVISED - MD 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	39
TC-08		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

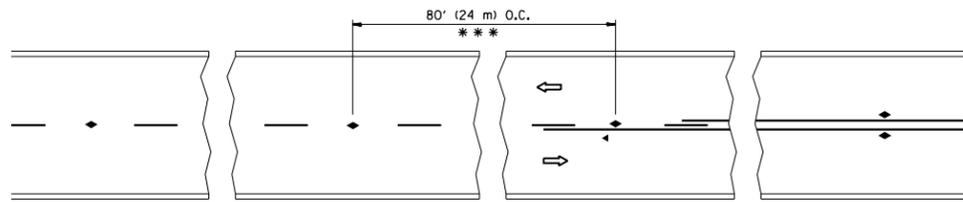
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 6/13/2014	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

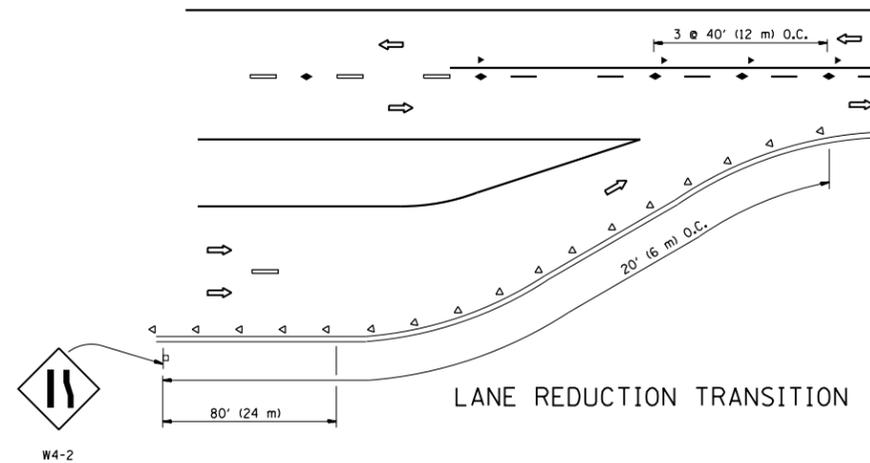
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60N51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

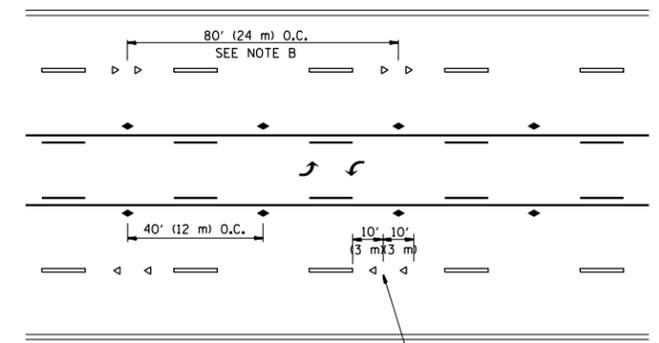


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

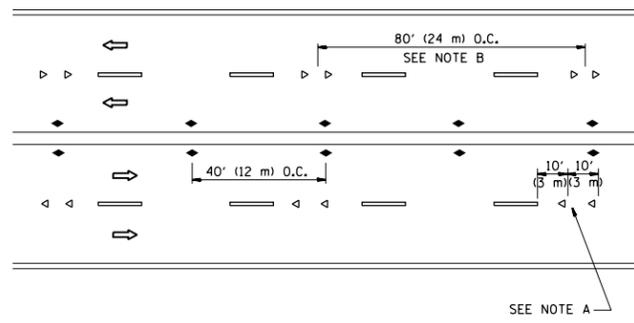
TWO-LANE/TWO-WAY



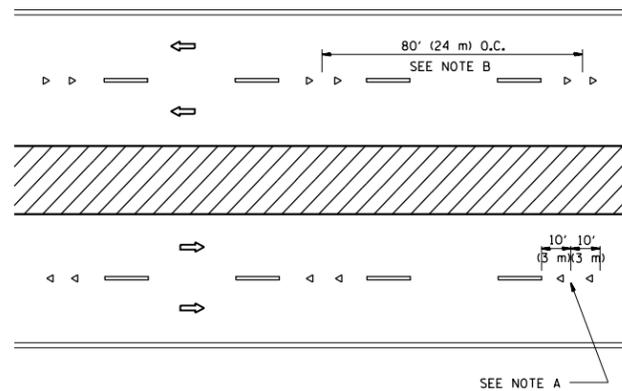
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

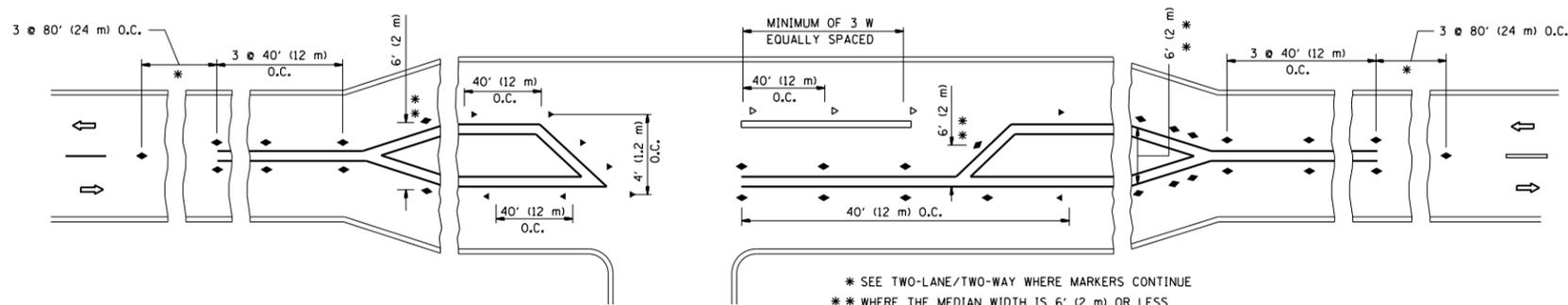
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

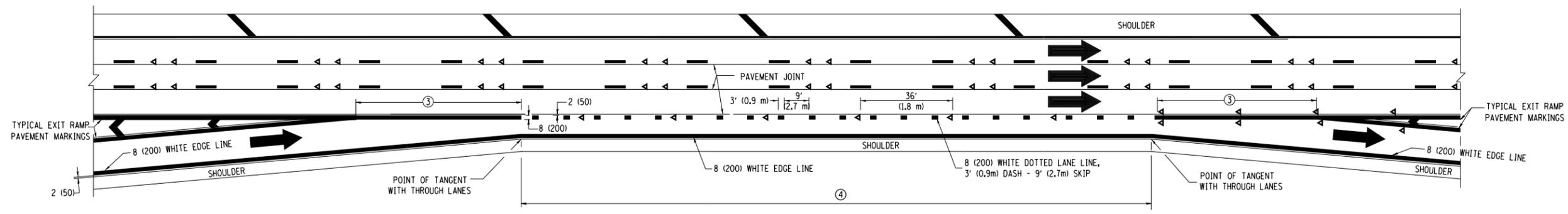
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	PLOT DATE = 6/13/2014	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

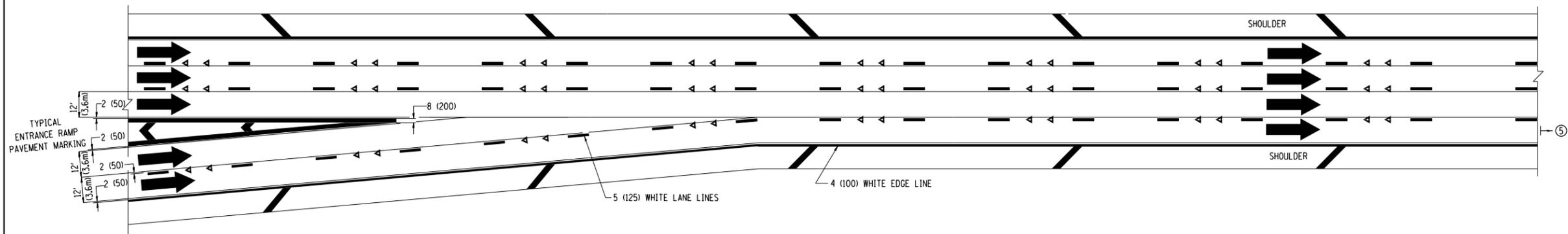
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

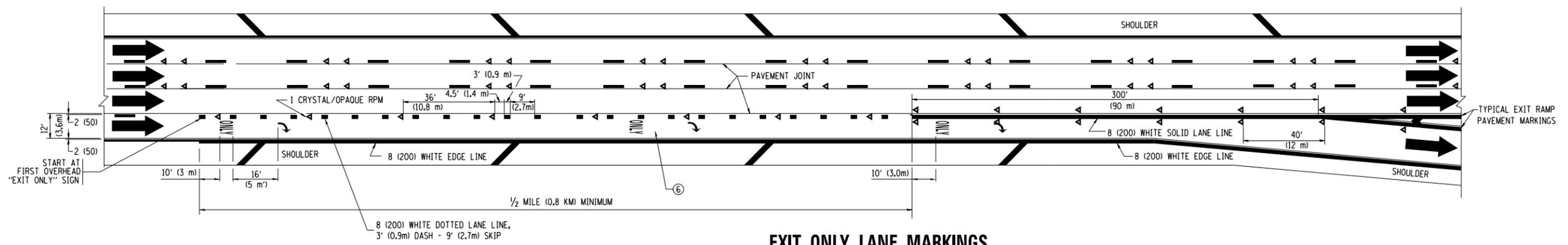
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



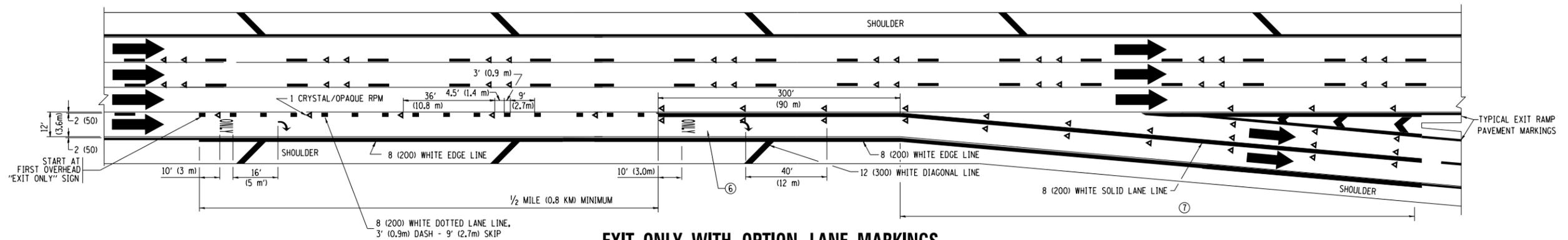
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

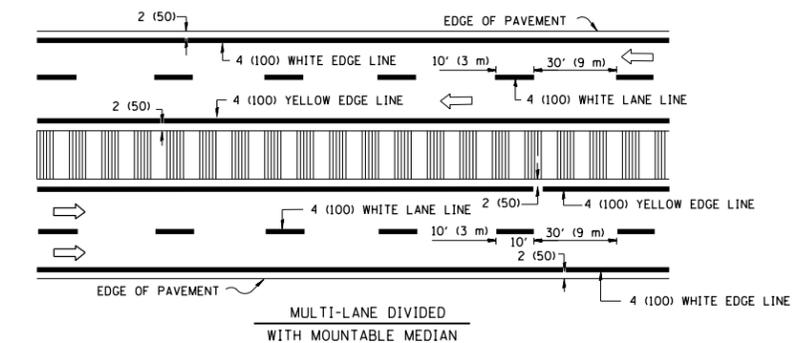
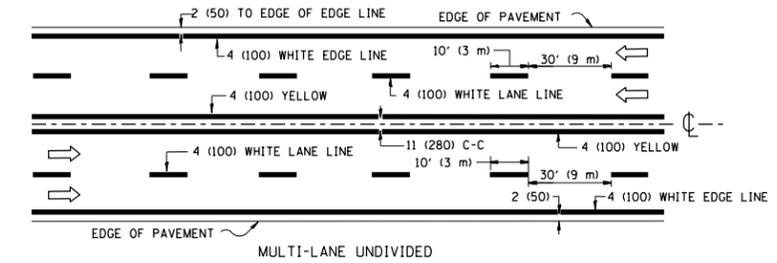
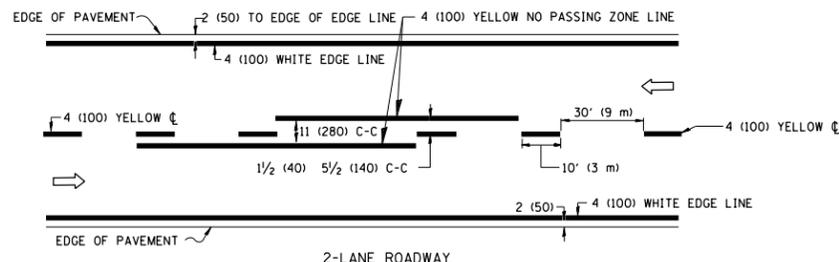
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = marenade	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
et:\pw\work\p1dot\marenade\d0316286\012111-sh-t-plan.dgn		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 6/13/2014	DATE - 01-90	REVISED - S.P.B. 01-10

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

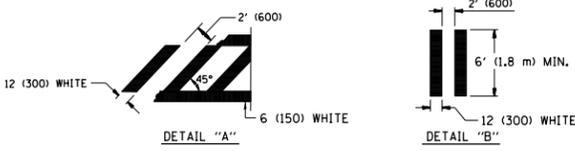
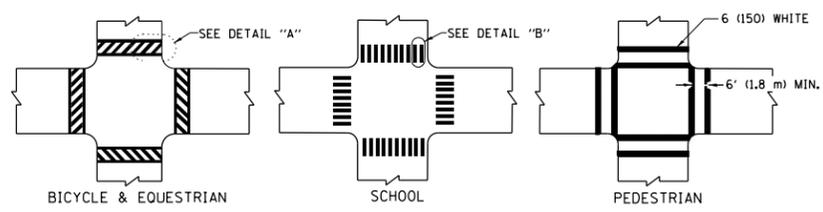
MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	43
TC-12		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

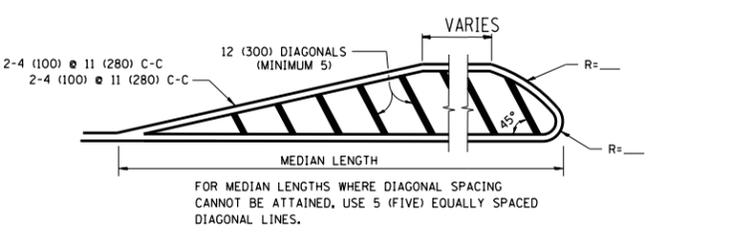
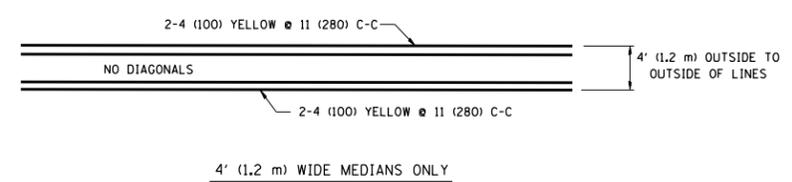


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

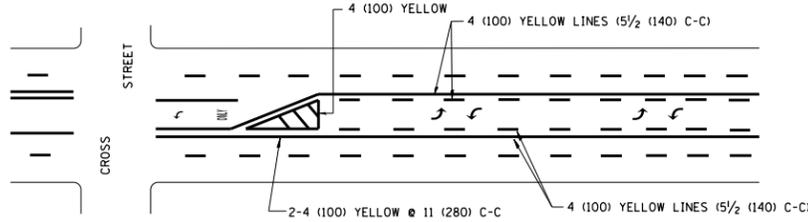


TYPICAL CROSSWALK MARKING

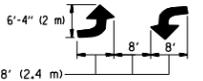


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

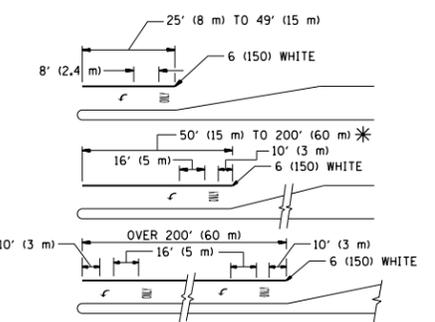


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

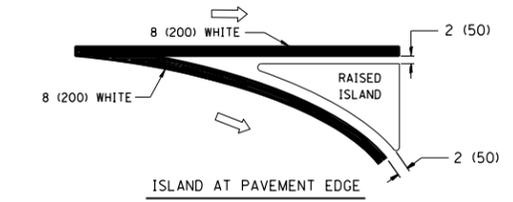
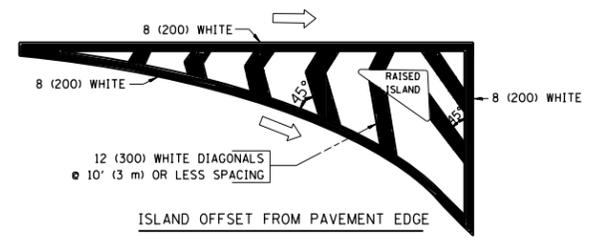


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

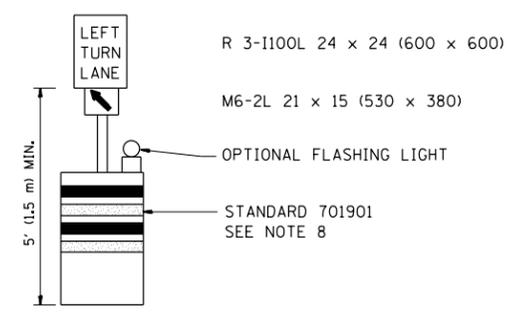
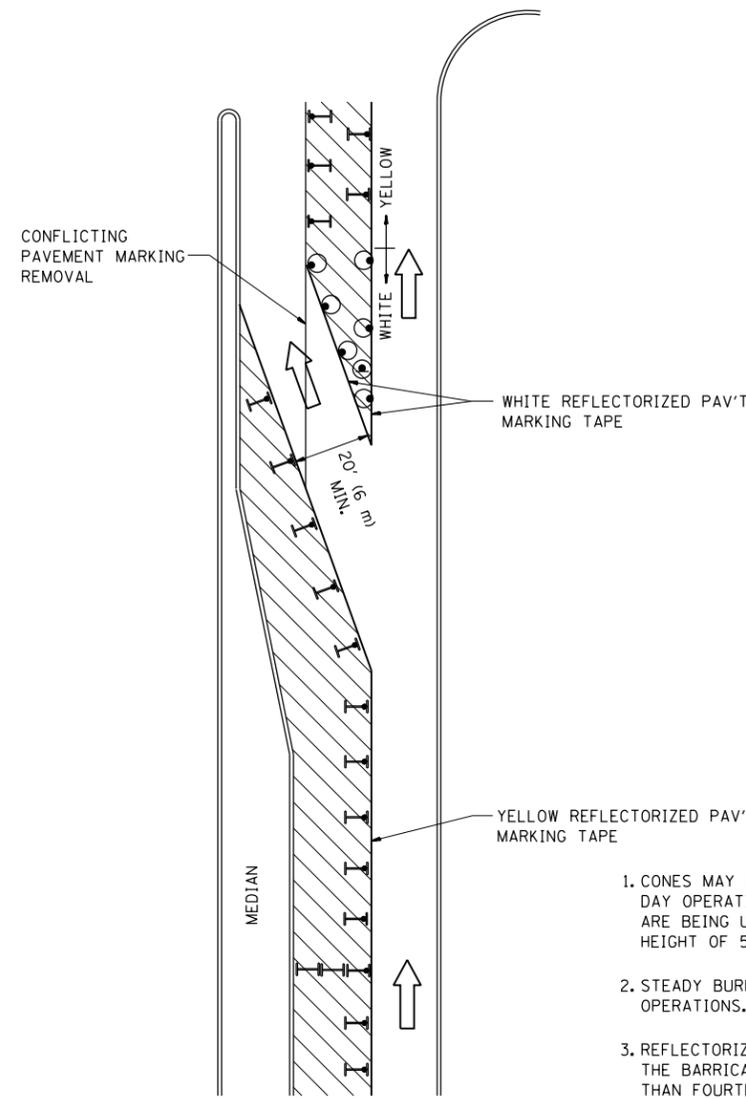


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

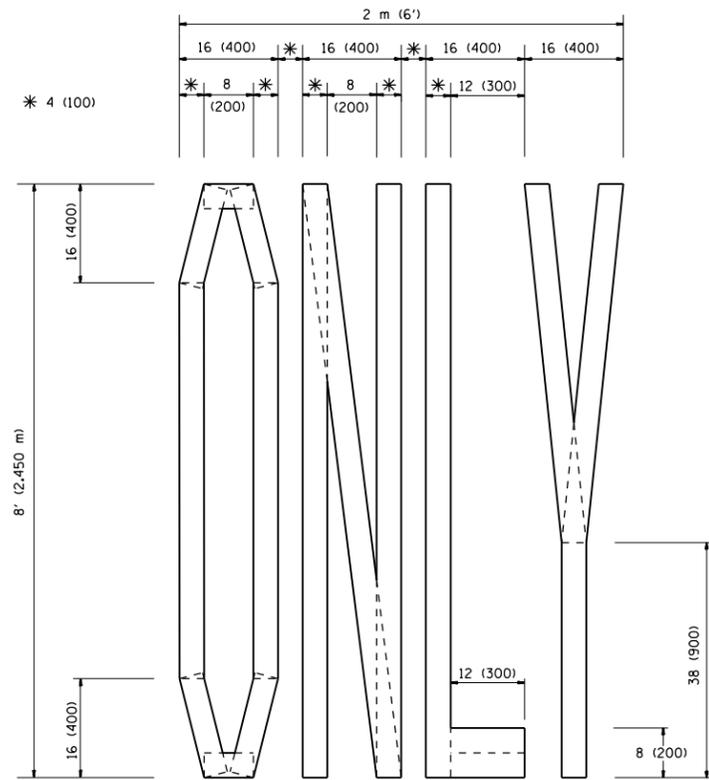
- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = marenade	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
et:\pw\work\pwidot\marenade\d0316286\013	21111-sh-t-plen.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 6/13/2014	REVISED -T. RAMMACHER 01-06-00	REVISED -

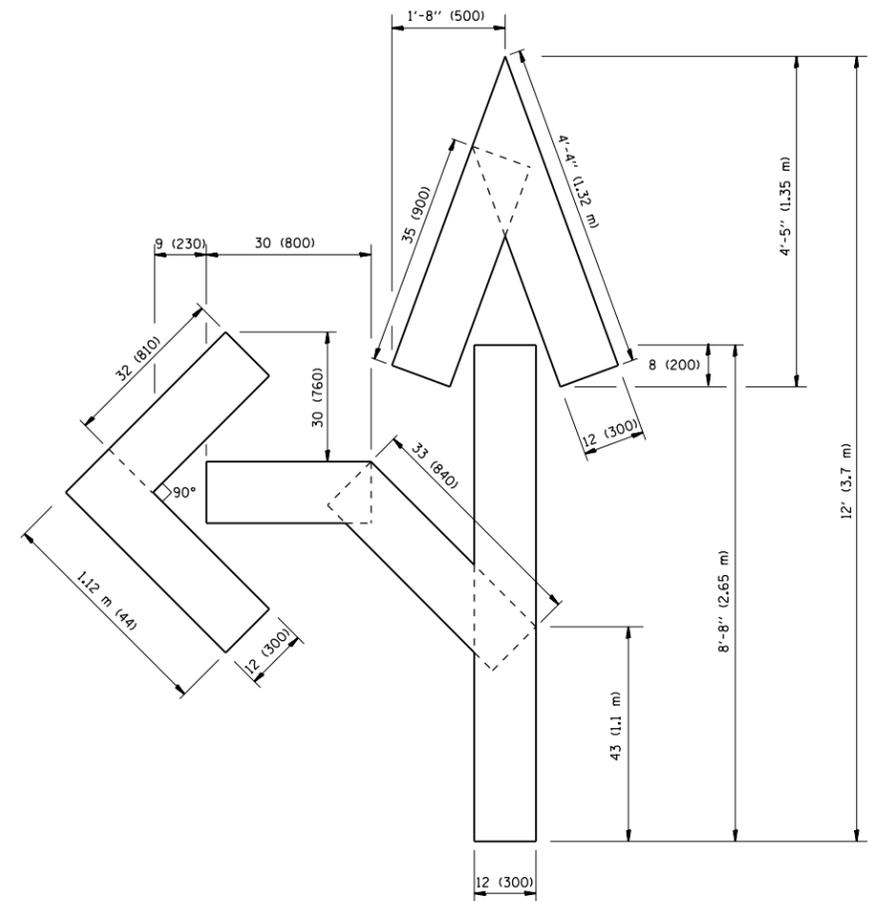
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

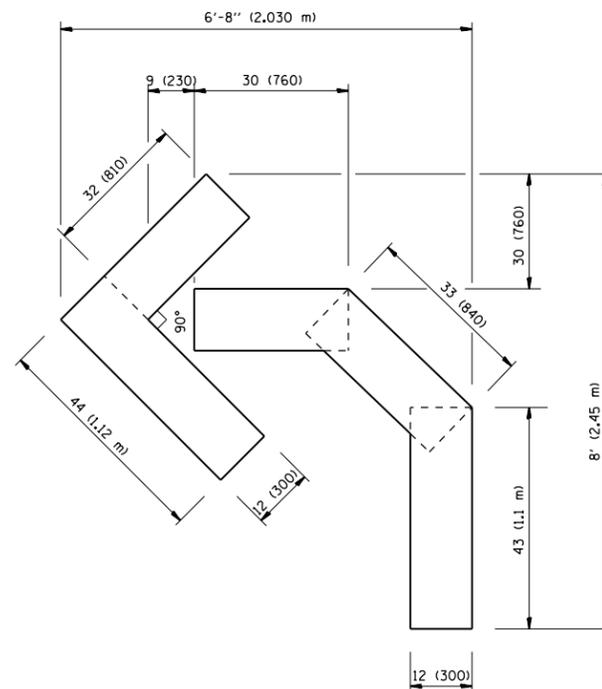
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	45
TC-14			CONTRACT NO. 60N51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\dot\marenade\d0316286\01	2111-sht-plen.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 6/13/2014	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

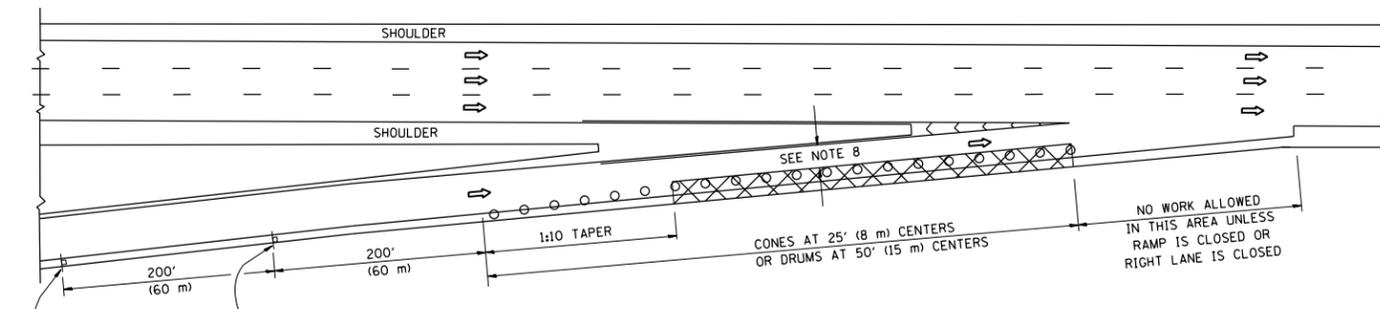
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

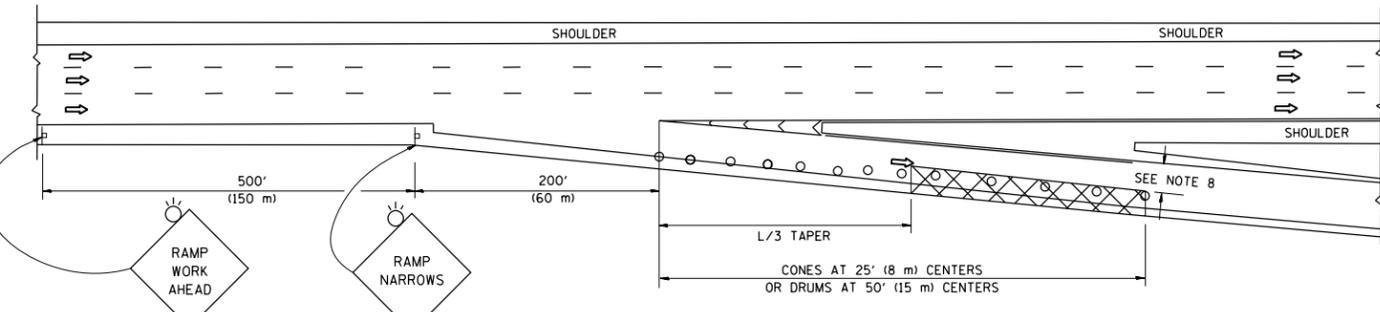
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	46
TC-16			CONTRACT NO. 60N51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

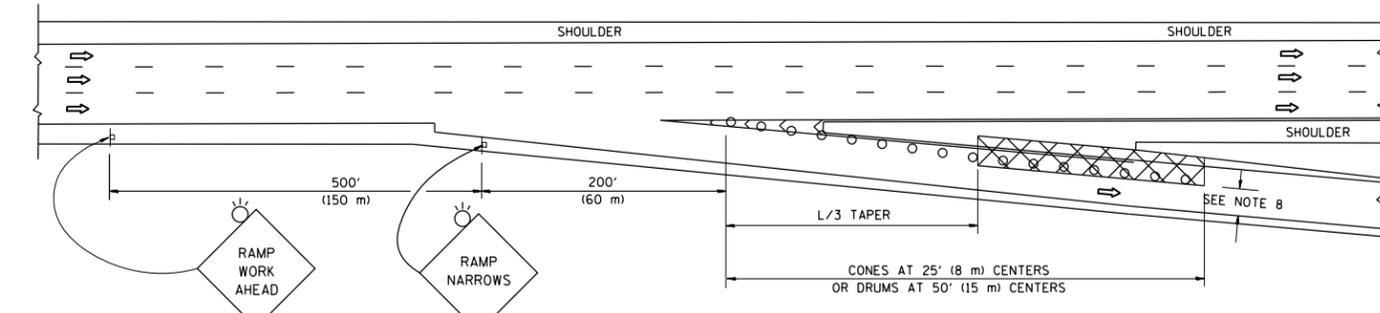
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

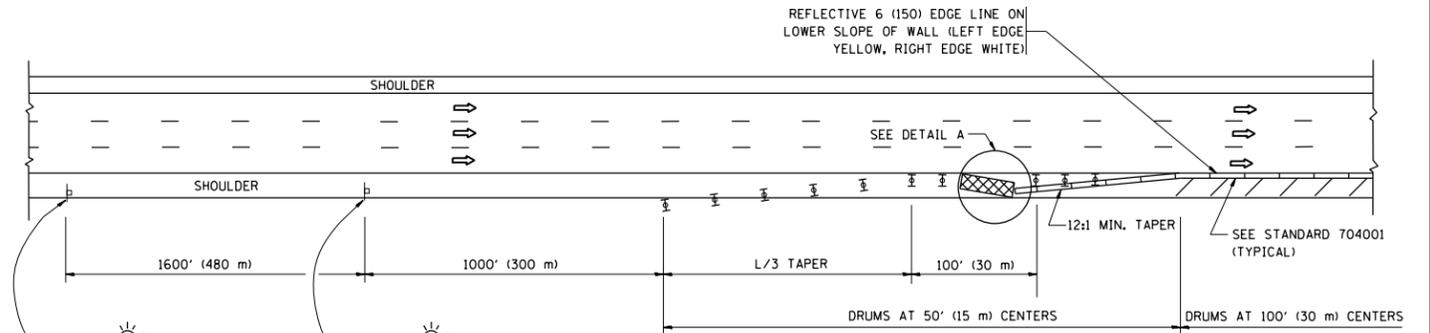
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

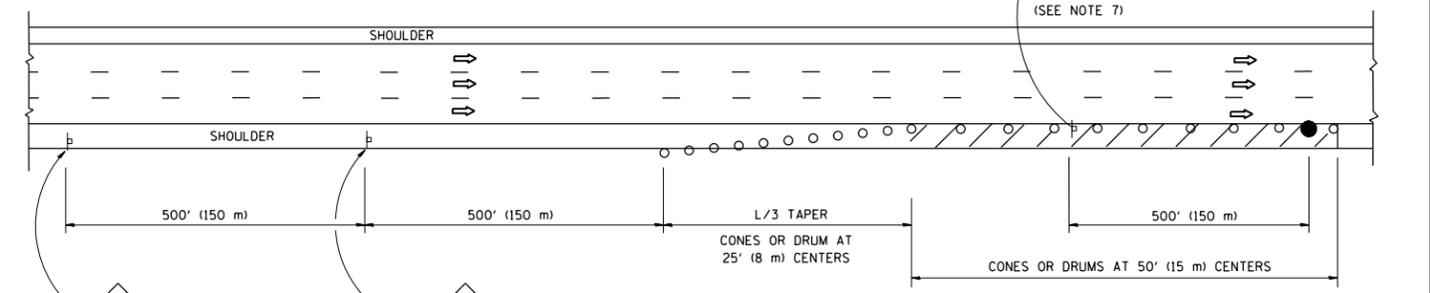
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC: $L=0.65(W)(S)$ ENGLISH: $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

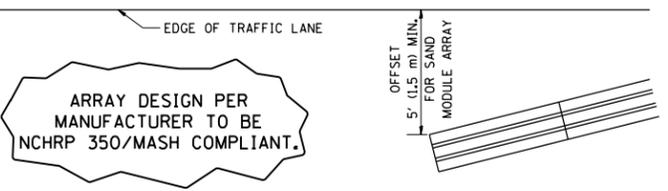
SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

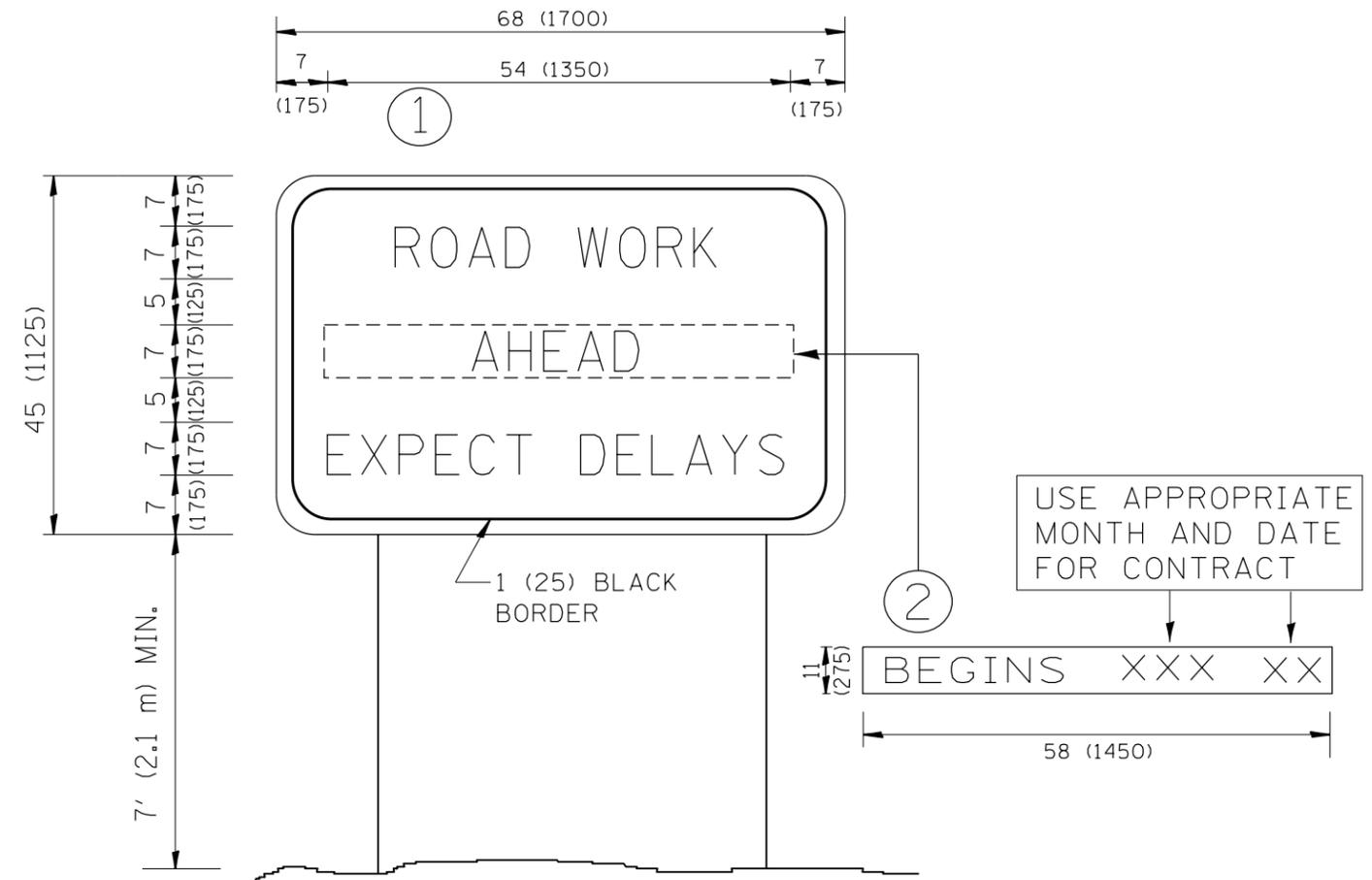


DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCRANCHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\dot\marenade\0316286\012111-sh-t-plen.dgn	2111-sh-t-plen.dgn	DRAWN - D.W.S.	REVISED - S.P.B. 01-07					344	543R-3-RS	DUPAGE	51	47
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 12-09		TC-17			CONTRACT NO. 60N51				
	PLOT DATE = 6/13/2014	DATE - 11-96	REVISED - M.D. 06-13		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - R. MIRS 09-15-97
et:\pw_work\pwidot\marenade\d0316286\01321111-sht-plen.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 6/13/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	48
TC-22		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

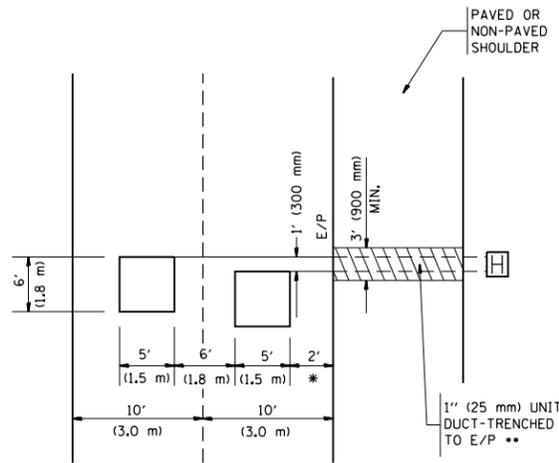
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				QUEUE DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED QUEUE DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL							
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED							
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID							
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT							
DETECTOR LOOP, TYPE I				RADIO REPEATER							
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED							
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



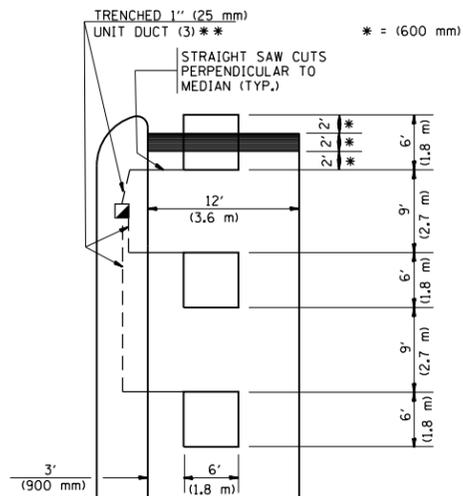
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

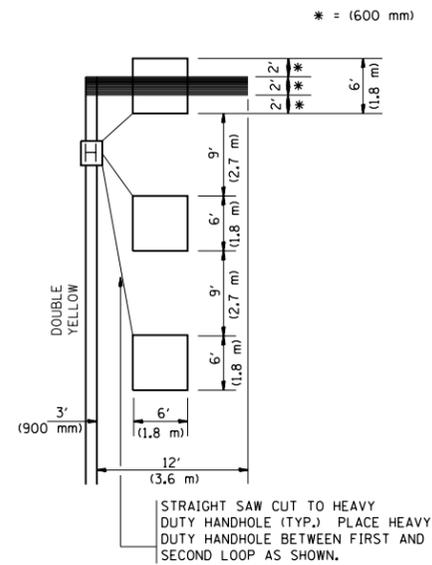


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

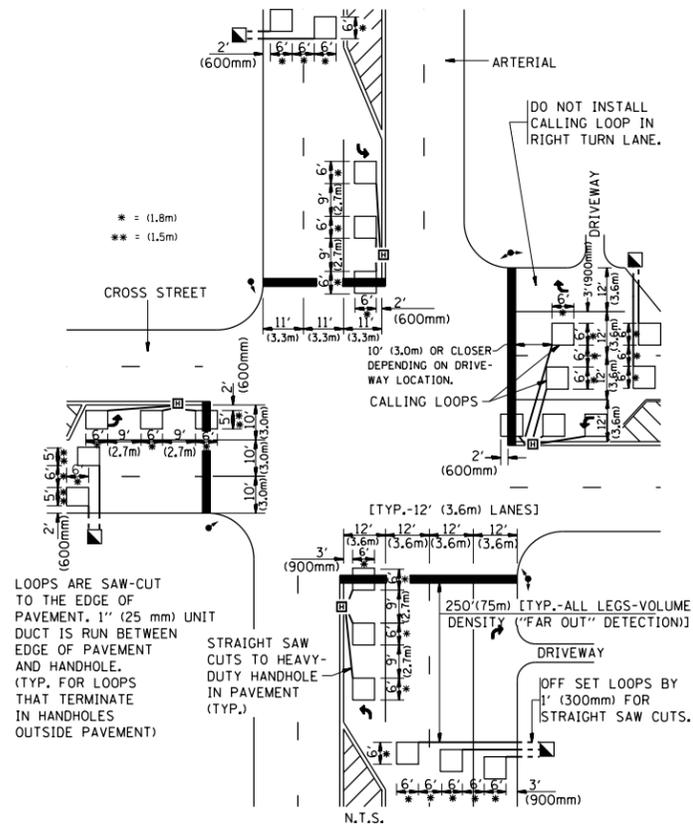
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



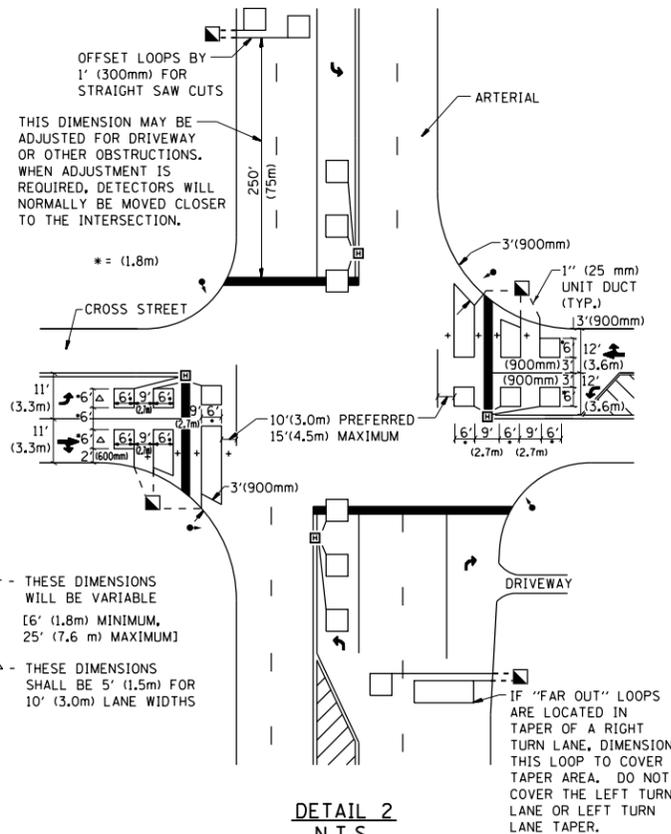
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

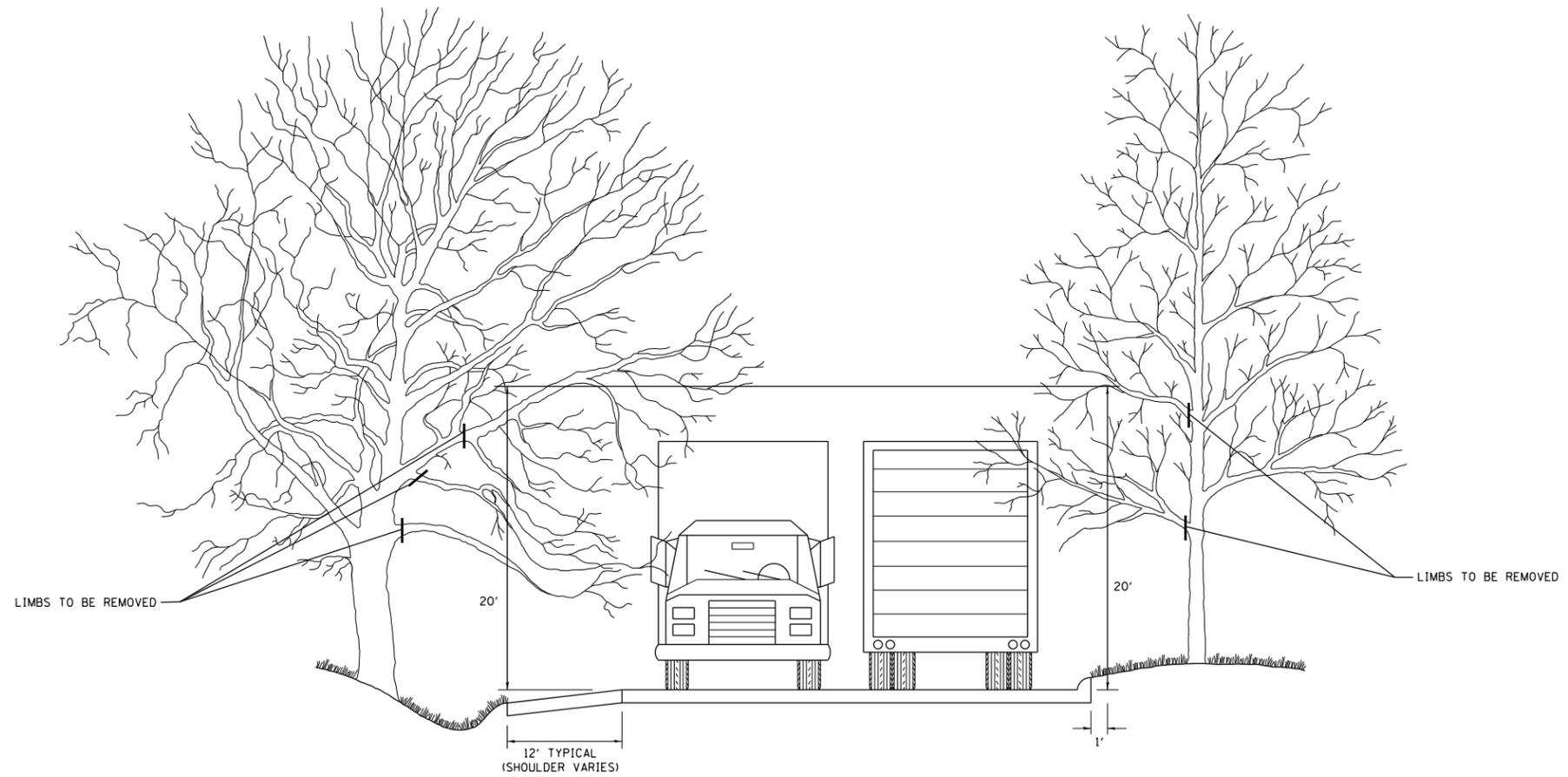
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw_work\p\dot\marenade\d0316286\012111-sh-t-plan.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			344	543R-3-RS	DUPAGE	51	50
	PLOT DATE = 6/13/2014	CHECKED - R.K.F.	REVISED -			TS-07		CONTRACT NO. 60N51		
		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



FILE NAME =	USER NAME = marenade	DESIGNED -	REVISED - R. BORO 10-31-06
et:\pw\work\p\idot\marenade\d0316286\013	2111-sht-pln.dgn	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 6/13/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PRUNING FOR SAFETY AND
EQUIPMENT CLEARANCE**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	543R-3-RS	DUPAGE	51	51
BM-20		CONTRACT NO. 60N51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				