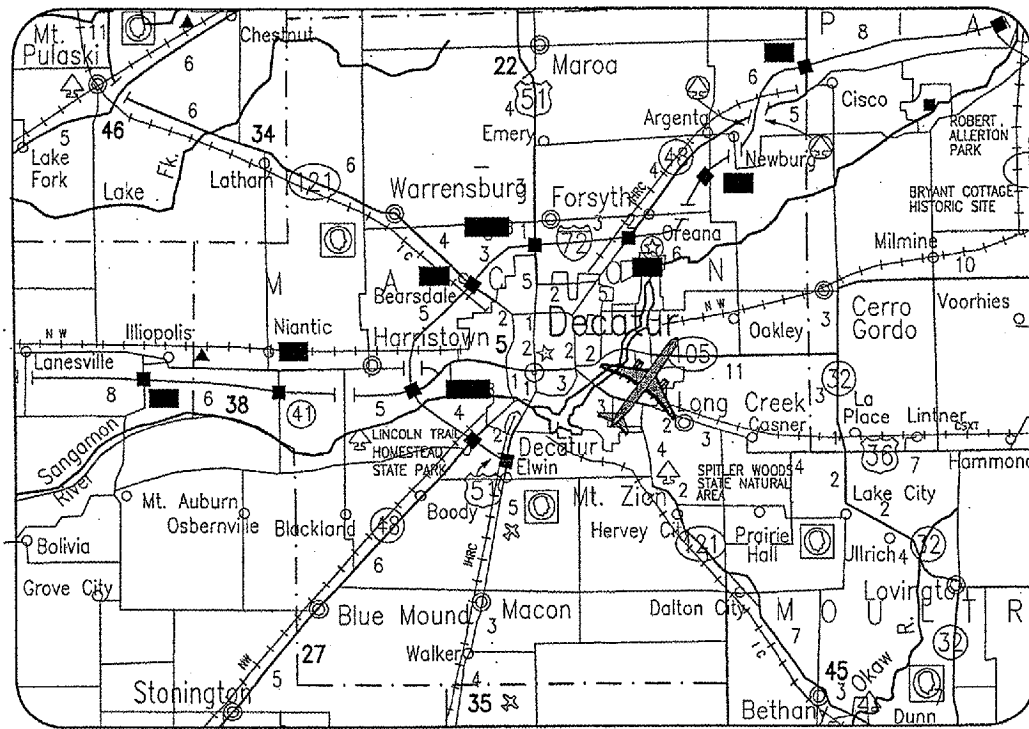


CONSTRUCTION PLANS FOR DECATUR AIRPORT DECATUR, MACON COUNTY, ILLINOIS TAXIWAY G WIDENING - PHASE II

SCOPE OF WORK

THIS PROJECT CONSISTS OF TWO CONTRACTS: THE WIDENING OF PARALLEL TAXIWAY "G" FROM RUNWAY 12-30 TO RUNWAY END 24, INCLUDING THE WIDENING OF CROSS-OVER TAXIWAYS "G3" & "G4" AND RECONSTRUCTION OF 800' OF BITUMINOUS PAVEMENT ON RUNWAY 6-24, CONVERTING TAXIWAYS "A" & "D" INTO AN EMERGENCY SERVICE ROAD, AND RELACEMENT OF TAXIWAY GUIDANCE SIGNS.



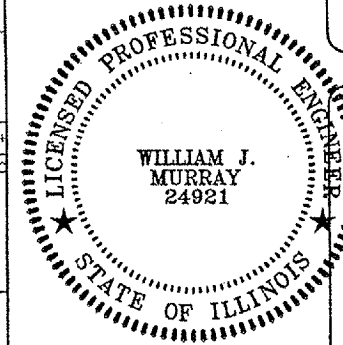
LOCATION

ILL. PROJ.: DEC-3414
A.I.P. PROJ.: 3-17-0033-28

LATITUDE: 39° 50' 05"
LONGITUDE: 88° 51' 59"
ELEVATION: 682.0' M.S.L.
DATE: JUNE 7, 2005



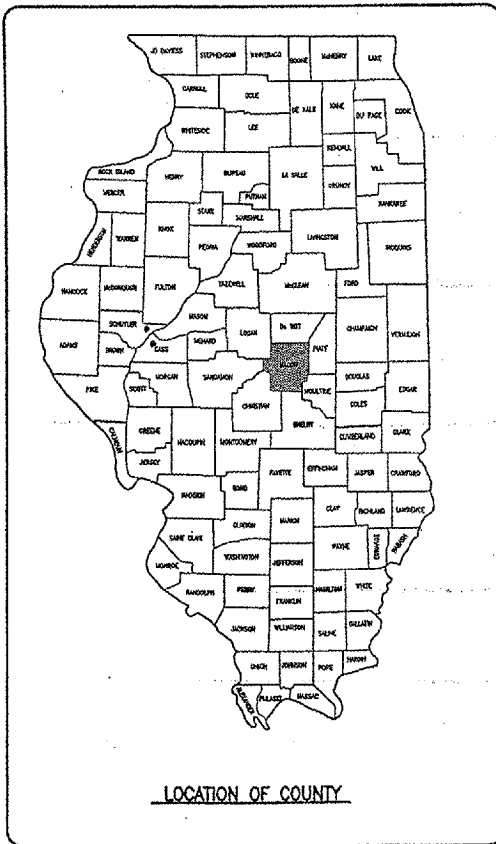
FOR ELECTRICAL
DESIGN



PLANS PREPARED BY:
HANSON
Hanson Professional Services Inc.
ELECTRICAL ENGINEER
Submitted by: *Kevin N. Lightfoot* ENG'R
Date Submitted: 6/10/2005
Lic. Exp. Date: NOVEMBER 30, 2005

PLANS PREPARED BY:
HANSON
Hanson Professional Services Inc.
DESIGN ENGINEER
Submitted by: *William J. Murray* ENG'R
Date Submitted: 6/5/05
Lic. Exp. Date: NOVEMBER 30, 2005

DECATUR AIRPORT
DECATUR PARK DISTRICT
DECATUR, ILLINOIS
Approved: 5/27/05 AIRPORT DIRECTOR
Date: Gene Marchant



JUN 09, 2005 6:48 PM CCC
I:\AIRPORTS\DECATUR\B02-04TXWD\AIRPORT_SHEETS\R-001COV.DWG - SHEET 1

DATE	REVISION																
 DECATUR, ILLINOIS													A.I.P. PROJ.: 3-17-0033-28 I.L. PROJ.: DEC-3414				
ILL. PROJ. No. <u>802-04TXD</u> Plans: <u>R-001COV.DWG</u> Scale: <u>N/A</u> Date: <u>5/10/05</u>													LAYOUT: <u>CCC</u> 5/10/05 DRAWN: <u>CCC</u> 5/10/05 REVIEWED: <u>WJM</u> 06/07/05				
 Hanson Professional Services Inc. Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62703-2088 Chicago, Wisconsin													TAXIWAY G WIDENING PHASE II COVER SHEET				
DECATUR AIRPORT DECATUR PARK DISTRICT DECATUR, ILLINOIS													1 1 of 110 sheets				

DATE	REVISION	BY



DECATUR, ILLINOIS
A.L.P. PROJ.: 3-17-0033-28
ILL. PROJ.: DEC-3414

HEI Project No. 802-041XDG	LAYOUT	CCC	05/10/05
Estimate R-0021DX.DWG	DRAWN	CCC	05/10/05
Scale N/A	REVIEWED	WJM	06/07/05
Date 05/10/05			



HANSON
Engineers, Architects, Scientists
1525 South Sixth Street
Springfield, Illinois 62703-2886
Offices Nationwide

**TAXIWAY G WIDENING
PHASE II**

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SUMMARY OF QUANTITIES

ITEM No.	DESCRIPTION	UNIT	CONTRACT No. 1 QUANTITIES	CONTRACT No. 1 AS-BUILT QTYS.	CONTRACT No. 2 QUANTITIES	CONTRACT No. 2 AS BUILT QTYS.
AR107408	L-806 WIND CONE-8' LIGHTED	EACH			6	
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	13,231		978	
AR108656	3/C #6 600 V UG CABLE IN UD	L.F.			12,060	
AR109200	INSTALL ELECTRICAL EQUIPMENT	L.S.			1	
AR110014	4" DIRECTIONAL BORE	L.F.			675	
AR110213	3" STEEL DUCT, DIRECT BURY	L.F.	155			
AR110552	EXTEND 2-WAY DUCT	L.F.	186			
AR125100	ELEVATED RETROREFLECTIVE MARKER	EACH			40	
AR125410	MIL-STAKE MOUNTED	EACH	63			
AR125415	MIL-BASE MOUNTED	EACH	6			
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2			
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	3			
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	4			
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	4			
AR125452	TAXI GUIDANCE SIGN, 12 CHARACTER	EACH			1	
AR125470	MODIFY EXISTING SIGN PANEL	EACH			12	
AR125515	HIRL, BASE MOUNTED	EACH	3			
AR125525	HIRL, INPAVEMENT	EACH	2			
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	91		53	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	12		6	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	7		4	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	1			
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1		1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1		1	
AR150540	HAUL ROUTE	L.S.	1		1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	10,488			
AR155540	BY-PRODUCT LIME	TON	708			
AR155616	SOIL PROCESSING - 16"	S.Y.	16,429			
AR156511	DITCH CHECK	EACH	16			
AR156520	INLET PROTECTION	EACH	9			
AR156521	HEADWALL PROTECTION	EACH	2			
AR156540	RIPRAP	S.Y.	74			
AR201610	BITUMINOUS BASE COURSE	TON	6,948			
AR201630	BITUMINOUS BASE TEST SECTION	EACH	1			
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	1,834			
AR208540	OVERSIZE AGGREGATE	TON	9,592			
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	4,312			
AR401610	BITUMINOUS SURFACE COURSE	TON	4,747		3,079	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1		1	
AR401640	BITUMINOUS PAVEMENT GROOVING	S.Y.			11,602	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	4,354		13,387	
AR501514	14.5" PCC PAVEMENT	S.Y.	8,735			
AR501530	PCC TEST BATCH	EACH	1			
AR501540	PCC PAVEMENT GROOVING	S.Y.			1,625	
AR501900	REMOVE PCC PAVEMENT	S.Y.	295			
AR602510	BITUMINOUS PRIME COAT	GAL.	2,864			
AR603510	BITUMINOUS TACK COAT	GAL.	4,653		2,677	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	14,106		14,845	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	868		4,322	
AR701524	24" RCP, CLASS IV	L.F.	38			
AR701536	36" RCP, CLASS IV	L.F.	46			
AR705411	POROUS BACKFILL No. 1	C.Y.	1,327			
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	7,628			
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	19			
AR705900	REMOVE UNDERDRAIN	L.F.	7,845			
AR751540	MANHOLE 4'	EACH	2			
AR751943	ADJUST MANHOLE	EACH	2			
AR752624	CONCRETE HEADWALL 24"	EACH	1			
AR752636	CONCRETE HEADWALL 36"	EACH	1			
AR752903	REMOVE HEADWALL	EACH	2			
AR800501	OFF PEAK CONSTRUCTION	L.S.	1		1	
AR800579	ELECTRICAL SERVICE FOR RWY 12 AND RWY 18 WIND CONES	L.S.			1	
AR800580	ELECTRICAL SERVICE FOR RWY 24 AND RWY 30 WIND CONES	L.S.			1	
AR800581	ELECTRICAL POWER FOR RWY 6 AND RWY 36 WIND CONES	L.S.			1	
AR901510	SEEDING	ACRE	12			
AR908510	MULCHING	ACRE	12			
AR910200	ROADWAY SIGNAGE	EACH			2	


* THE CONTRACTOR IS BIDDING ON 14.5" PCC PAVEMENT, EVEN THOUGH THE SCHEDULE OF PRICES IN THE BID DOCUMENTS STATE 14" PCC PAVEMENT.

REVISION	DATE	BY



DECATUR, ILLINOIS
A.I.P. PROJ.: 3-17-0033-28
I.L. PROJ.: DEC-3414

FILE NO. 802-04TXD	DATE 05/10/05
FILENAME R-003QTY.DWG	SCALE N/A
DATE 05/10/05	SCALE N/A
LAYOUT CCC	05/10/05
DRAWN CCC	05/10/05
REVIEWED WJM	06/07/05



HANSON
Engineers, Architects, Scientists
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Springfield, Illinois 62703-2888
Offices Nationwide

**TAXIWAY G WIDENING
PHASE II**

SUMMARY OF QUANTITIES

JUN 27, 2005 1:08 PM CCC
I:\AIRPORTS\DECATUR\802-04TXD\AIRPORT\SHEETS\CONTRACT-1\R-003QTY.DWG - Quantity Summary

SCOPE OF WORK

CONTRACT 1 INCLUDES THE FOLLOWING:

THE WIDENING OF TAXIWAY "G" FROM RUNWAY 12-30 TO RUNWAY END 24 FROM 50 FEET TO 75 FEET. THE TAXIWAY CROSSOVERS "G3" AND "G4" WILL BE WIDENED FROM 67 FEET TO 130 FEET. THE TAXIWAY WIDENING WILL INCLUDE TAXIWAY TAPERS AT THE INTERSECTIONS. THE EXISTING TAXIWAY "G" FROM RUNWAY 12-30 TO AND INCLUDING TAXIWAY CROSSOVER "G3" WILL RECEIVE A BITUMINOUS OVERLAY.

CONTRACT 2 INCLUDES THE FOLLOWING:

THE RECONSTRUCTION OF A PORTION OF RUNWAY 6-24 PAVEMENT APPROXIMATELY 800 FEET BY 150 FEET.

THE CONVERSION OF A PORTION OF TAXIWAY "A" AND "D" TO AN EMERGENCY ACCESS ROAD.

MODIFICATION OF TAXIWAY SIGNS ALONG TAXIWAY "B" AND "C."

INSTALLATION OF LIGHTED WIND CONES AT RUNWAY ENDS.

RUNWAY AND TAXIWAY CLOSING SCHEDULE - CONTRACT 1 & 2

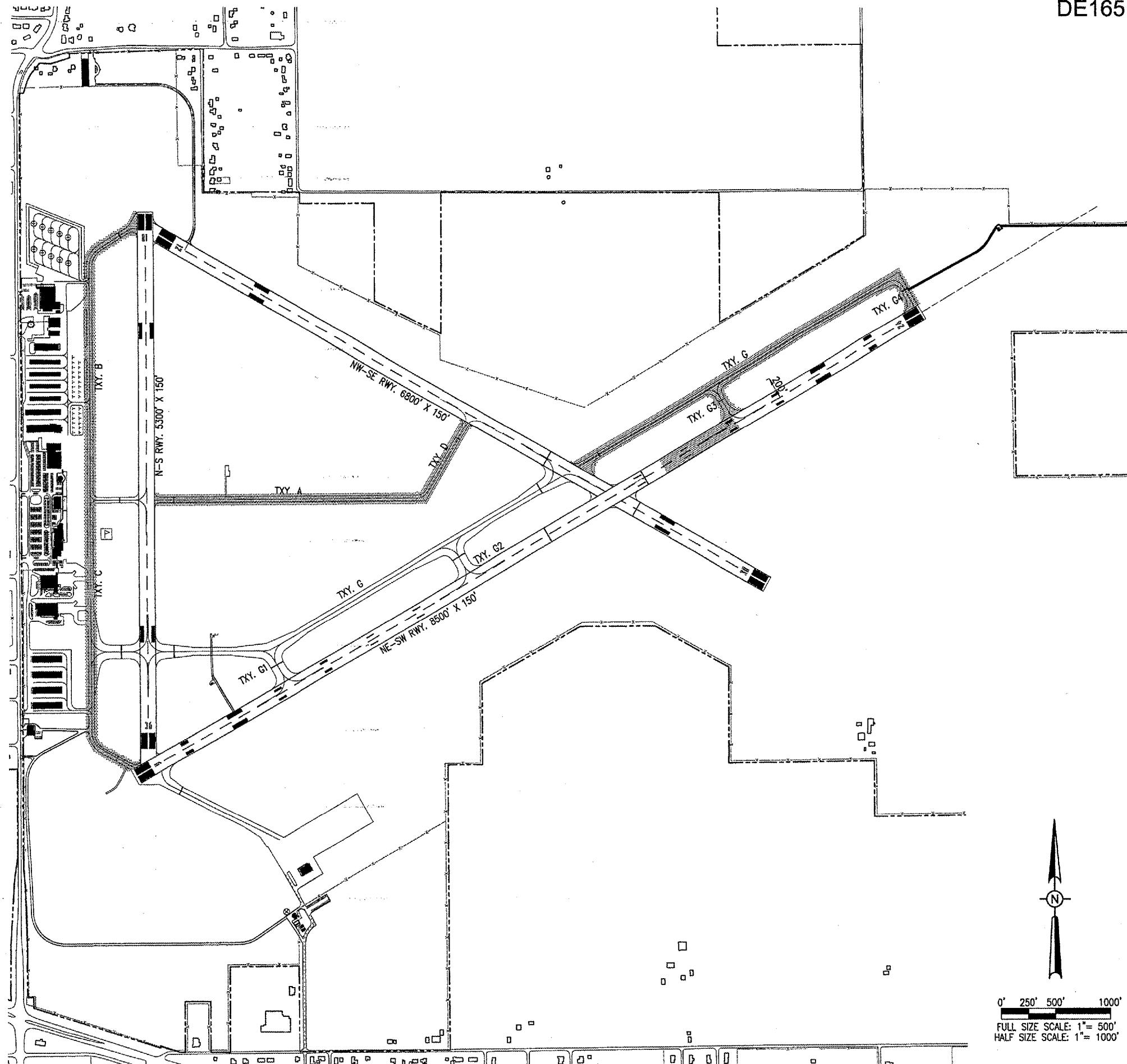
1. ANY TIME CONSTRUCTION WORK IS BEING DONE WITHIN 200 FEET OF A RUNWAY CENTERLINE OR 85 FEET OF A TAXIWAY CENTERLINE, THE RUNWAY/TAXIWAY WILL BE CLOSED FOR AIR OPERATIONS. CONSTRUCTION WORK INCLUDES THE PRESENCE OF VEHICLES, PERSONNEL, EXCAVATED HOLES, EARTH MOUNDS, PARKED EQUIPMENT, PAVEMENT DROP-OFFS EXCEEDING 3 INCHES, OR HAZARDS TO AIR TRAFFIC.
2. PRIOR NOTICE AND APPROVAL SCHEDULES BY THE AIRPORT DIRECTOR WILL BE REQUIRED PRIOR TO CLOSING A RUNWAY AND/OR TAXIWAY. RUNWAYS WILL BE CLOSED BY CROSSES PLACED OVER THE RUNWAY NUMERALS, AND TAXIWAY WILL BE CLOSED WITH LIGHTED AND FLAGGED BARRICADES.
3. RUNWAY 12-30 AND 6-24 CAN NOT BE CLOSED FOR AIRCRAFT OPERATIONS DURING THE SAME TIME PERIODS.
4. RUNWAY 6-24 CAN NOT BE CLOSED DURING PERIODS OF INSTRUMENTS WEATHER CONDITIONS, VISIBILITY OF LESS THAN THREE MILES, OR WHEN THE ATC PERSONNEL DETERMINE CONDITIONS TO BE INSTRUMENT CONDITIONS.
5. THE TIME OF CLOSURE FOR RUNWAYS 6-24 AND 12-30 WILL BE KEPT TO A MINIMUM. THE ACTION REQUIRED TO OPEN A RUNWAY IS AS STATED ON THIS SHEET.
6. RUNWAY 6-24 CAN BE CLOSED WITH PRIOR SCHEDULE APPROVAL DURING THE FOLLOWING PERIODS:

DAY	CLOSURE TIME
MONDAY	MIDNIGHT - 9:00PM
TUESDAY	6:00AM - 9:00PM
WEDNESDAY	6:00AM - 9:00PM
THURSDAY	6:00AM - 9:00PM
FRIDAY	6:00AM - MIDNIGHT
SATURDAY	ALL DAY
SUNDAY	ALL DAY

7. THE CONTRACTOR IS REQUIRED TO SUBMIT AN APPLICATION FOR AIRSPACE APPROVAL FOR THE ERECTION OF A CONCRETE MIXING PLANT, IF THE CONTRACTOR INTENDS TO LOCATE A CENTRAL MIXING PLANT ON AIRPORT PROPERTY. THE APPLICATION IS TO BE SENT TO THE DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS 60 DAYS PRIOR TO THE COMMENCEMENT OF THE START OF ERECTION OF THE PLANT.

8. OPENING A RUNWAY OR TAXIWAY WILL REQUIRE THE FOLLOWING ACTIONS:

- A) THE PAVEMENT WILL BE SWEEPED BY A POWER BROOM TO REMOVE ANY AND ALL DEBRIS FROM THE PAVEMENT.
- B) THE AREA WITHIN 200 FEET OF A RUNWAY CENTERLINE AND 85 FEET OF A TAXIWAY CENTERLINE MUST BE SMOOTH GRADED WITH A 1% - 5% SLOPE AWAY FROM THE PAVEMENT EDGE. THE PAVEMENT EDGE DROP-OFF CAN NOT EXCEED 3 INCHES. NO HOLES OR MOUNDS WILL BE PERMITTED WITHIN THE AREA.
- C) ALL PERSONNEL AND EQUIPMENT WILL BE OUT OF THE 200 FEET AND 85 FEET LIMITS.
- D) ALL RUNWAY AND TAXIWAY LIGHTING CIRCUITS WILL BE OPERATIONAL.
- E) RUNWAY AND TAXIWAY CENTERLINE MARKING, IF OBLITERATED, WILL BE REMARKED.
- F) THE CLOSED PAVEMENTS WILL BE VISUALLY INSPECTED BY AIRPORT PERSONNEL PRIOR TO OPENING. THE CONTRACTOR WILL MAKE ANY CORRECTIONS REQUIRED AS A RESULT OF THE INSPECTION.



DE165

JUN 10, 2005 9:14 AM CCC
I:\AIRPORTS\DECATUR\802-04TXD\AIRPORT\PROJECTS\CONTRACT-1\8-004STG.DWG - SHEET 4

REVISION	DATE	BY

DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.L.P. PROJ.: 3-17-0033-28

HR Project No. 802-04TXD	DPE	04/22/05
Filename: R-004STG.DWG	DRAWN	04/22/05
Scale: 1" = 500'	REVIEWED	06/07/05
Date: 04/25/05		

HANSON
 Engineers, Architects, Scientists
 1825 South Sixth Street
 Springfield, Illinois 62703-2886
 Offices Nationwide

**TAXIWAY G WIDENING
 PHASE II**
**PROPOSED OPENING
 AND CLOSING SCHEDULE**

SCOPE OF WORK

CONTRACT 1 INCLUDES THE FOLLOWING:

THE WIDENING OF TAXIWAY "G" FROM RUNWAY 12-30 TO RUNWAY END 24 FROM 50 FEET TO 75 FEET. THE TAXIWAY CROSSOVERS "G3" AND "G4" WILL BE WIDENED FROM 67 FEET TO 130 FEET. THE TAXIWAY WIDENING WILL INCLUDE TAXIWAY TAPERS AT THE INTERSECTIONS. THE EXISTING TAXIWAY "G" FROM RUNWAY 12-30 TO AND INCLUDING TAXIWAY CROSSOVER "G3" WILL RECEIVE A BITUMINOUS OVERLAY.

CONTRACT 2 INCLUDES THE FOLLOWING:

- THE RECONSTRUCTION OF A PORTION OF RUNWAY 6-24 PAVEMENT APPROXIMATELY 800 FEET BY 150 FEET.
- THE CONVERSION OF A PORTION OF TAXIWAY "A" AND "D" TO AN EMERGENCY ACCESS ROAD.
- MODIFICATION OF TAXIWAY SIGNS ALONG TAXIWAY "B" AND "C."
- INSTALLATION OF LIGHTED WIND CONES AT RUNWAY ENDS.

PROPOSED SAFETY PLAN

ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE, THE RUNWAY WILL BE CLOSED. THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO REOPENING THE RUNWAY. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE DECATUR AIRPORT IS TOWER CONTROLLED FROM 6 A.M. TO 10 P.M. AND IS COMPRISED OF THREE (3) RUNWAYS. THE PROPOSED CONSTRUCTION WILL EFFECT RUNWAY 6-24, 12/30 AND 18/36. THE SAFETY PLAN AS OUTLINED HERE AND IN THE SPECIAL PROVISIONS WILL MAXIMIZE SAFETY AND ALLOW MINIMUM RUNWAY CLOSURE TIME.

THE CLOSURE OF A RUNWAY WILL REQUIRE THE FOLLOWING:

- 1) PROVIDING THE SPECIFIED ADVANCE NOTIFICATION TO THE AIRPORT DIRECTOR.
- 2) CONFIRMING THE ISSUANCE OF THE NOTAM WITH AIRPORT DIRECTOR AND CONTROL TOWER PRIOR TO STARTING THE ACTUAL CLOSURE.
- 3) PLACING THE 10 FT. X 60 FT. CROSSES AT THE RUNWAY END OR ON THE NUMBERS AS THE SITUATIONS DICTATES.
- 4) MAINTENANCE OF THE CROSSES DURING THE CLOSING PERIOD.

RADIO CONTROL:

THE CONTRACTOR WILL BE REQUIRED TO HAVE A TWO-WAY RADIO, CAPABLE OF BEING IN CONTACT WITH THE CONTROL TOWER, GROUND CONTROL CHANNEL (121.75 MHZ). THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE CONTROL TOWER, THUS KEEPING THE CONTROL TOWER INFORMED ON HIS CONSTRUCTION ACTIVITIES AND ENABLE THE CONTROL TOWER TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE SOME ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

BARRICADES AND BARRIERS

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND BARRIERS AT THE LOCATIONS SHOWN ON THIS SHEET AND THE TEMPORARY BARRIER SHEET IN THE PLANS.

THE BARRICADES WILL BE I.D.O.T. TYPE 1, EQUIPPED WITH RED CONTINUOUS OR FLASHING LIGHTS AND EXTENDED 18 INCH SQUARE ORANGE FLAG.

THE BARRIER WILL BE CONSTRUCTED AS DETAILED ON THE TEMPORARY BARRIER DETAIL SHEET.

PAYMENT FOR BARRICADES AND BARRIERS, THEIR PLACEMENT AND REMOVALS AS RUNWAYS AND TAXIWAYS ARE OPENED AND CLOSED, MAINTENANCE, AND REPAIRS WILL BE MADE BY ITEM: AR150530 "TRAFFIC MAINTENANCE" PER LUMP SUM.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR IS RESPONSIBLE FOR THE CONSTRUCTION OF THE ACCESS ROAD AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRACTOR AND TRAILER.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA AS SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREAS. ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR WILL FURNISH ALL OF HIS EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.

WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE (3) FOOT SQUARE FLAG CONSISTING OF A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE (1) FOOT ON EACH SIDE DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY WORK BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE.

EROSION CONTROL

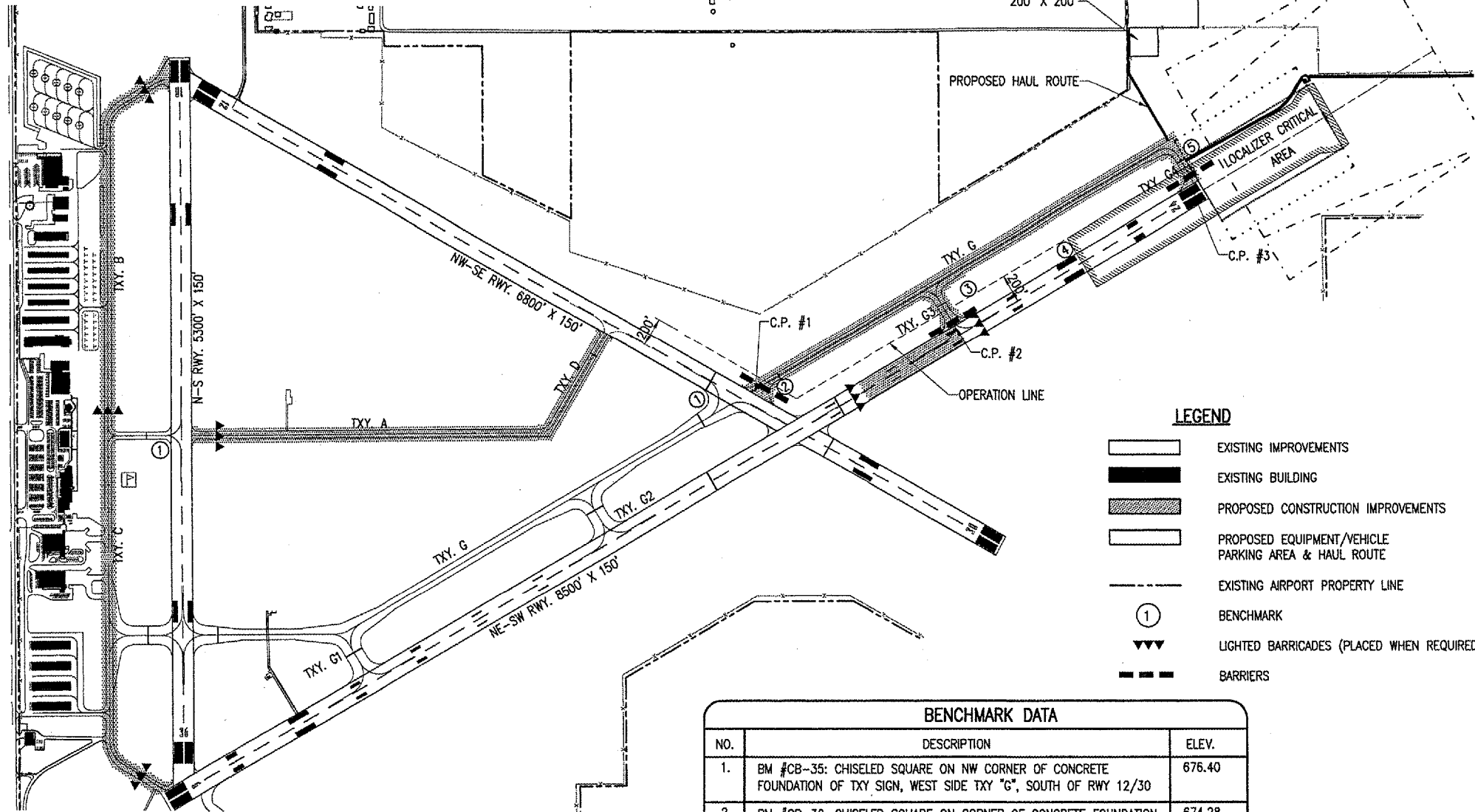
THE PROPOSED CONSTRUCTION WILL DISTURB MORE THAN 1 ACRES OF LAND, THEREFORE AN EROSION CONTROL PLAN IS PROVIDED.

ENGINEER'S FIELD OFFICE

THE EXACT LOCATION OF THE PROPOSED CONSTRUCTION TRAILER WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATHE EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATHE LINE FOR RUNWAYS.



LEGEND

- EXISTING IMPROVEMENTS
- EXISTING BUILDING
- PROPOSED CONSTRUCTION IMPROVEMENTS
- PROPOSED EQUIPMENT/VEHICLE PARKING AREA & HAUL ROUTE
- EXISTING AIRPORT PROPERTY LINE
- BENCHMARK
- LIGHTED BARRICADES (PLACED WHEN REQUIRED)
- BARRIERS

BENCHMARK DATA

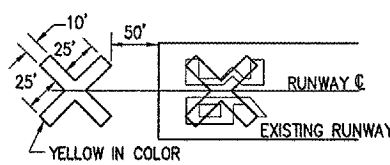
NO.	DESCRIPTION	ELEV.
1.	BM #CB-35: CHISELED SQUARE ON NW CORNER OF CONCRETE FOUNDATION OF TYX SIGN, WEST SIDE TYX "G", SOUTH OF RWY 12/30	676.40
2.	BM #CB-36: CHISELED SQUARE ON CORNER OF CONCRETE FOUNDATION OF TAXIWAY SIGN, STA. 370+91, 40.5' RT.	674.28
3.	BM #CB-40: CHISELED SQUARE ON SOUTH SIDE OF DROP INLET BETWEEN TYX "G" AND RWY 6/24, STA. 386+54, 151.5' RT.	672.89
4.	BM #CB-42: CHISELED SQUARE ON NORTH SIDE OF CONCRETE FOUNDATION OF RUNWAY LIGHT, STA. 392+22, 314' RT.	677.16
5.	BM #CB-45: CHISELED SQUARE NE CORNER OF CONC. FOUNDATION OF TYX "G" SIGN & RWY 24 HOLD LINE, STA. 403+81, 144' RT.	680.54

CRITICAL POINT DATA

	C.P. #1	C.P. #2	C.P. #3
LATITUDE	39° 50' 06.3604"	39° 50' 10.8918"	39° 50' 20.9356"
LONGITUDE	88° 51' 35.7380"	88° 51' 17.7943"	88° 50' 55.9809"
ELEVATION	676.96	675.23	680.82

CRITICAL AIRCRAFT DATA

RUNWAY	EXISTING	FUTURE	WING SPAN	APPROACH SPEEDS
18/36	C-II (FALCON 50)	C-II (FALCON 50)	49' - 79'	
12/30	C-III (727)	C-IV (757)		121 - 141 KNOTS
6/24	C-III (727)	C-IV (757)	125'	



DETAIL OF CROSS FOR CLOSED RUNWAY

"NOT TO SCALE"

CROSS NOTE

THE COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AS SHOWN ON THIS SHEET AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY WILL BE CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION TO THE CONTRACT WILL BE ALLOWED.

J.U.L.I.E. INFORMATION

COUNTY.....MACON
 CITY.....DECATUR
 TOWNSHIP.....LONG CREEK
 SECTION NO.....20 & 21
 ADDRESS.....DECATUR AIRPORT AIRPORT ROAD DECATUR, ILLINOIS 62524

DE165

DATE	REVISION

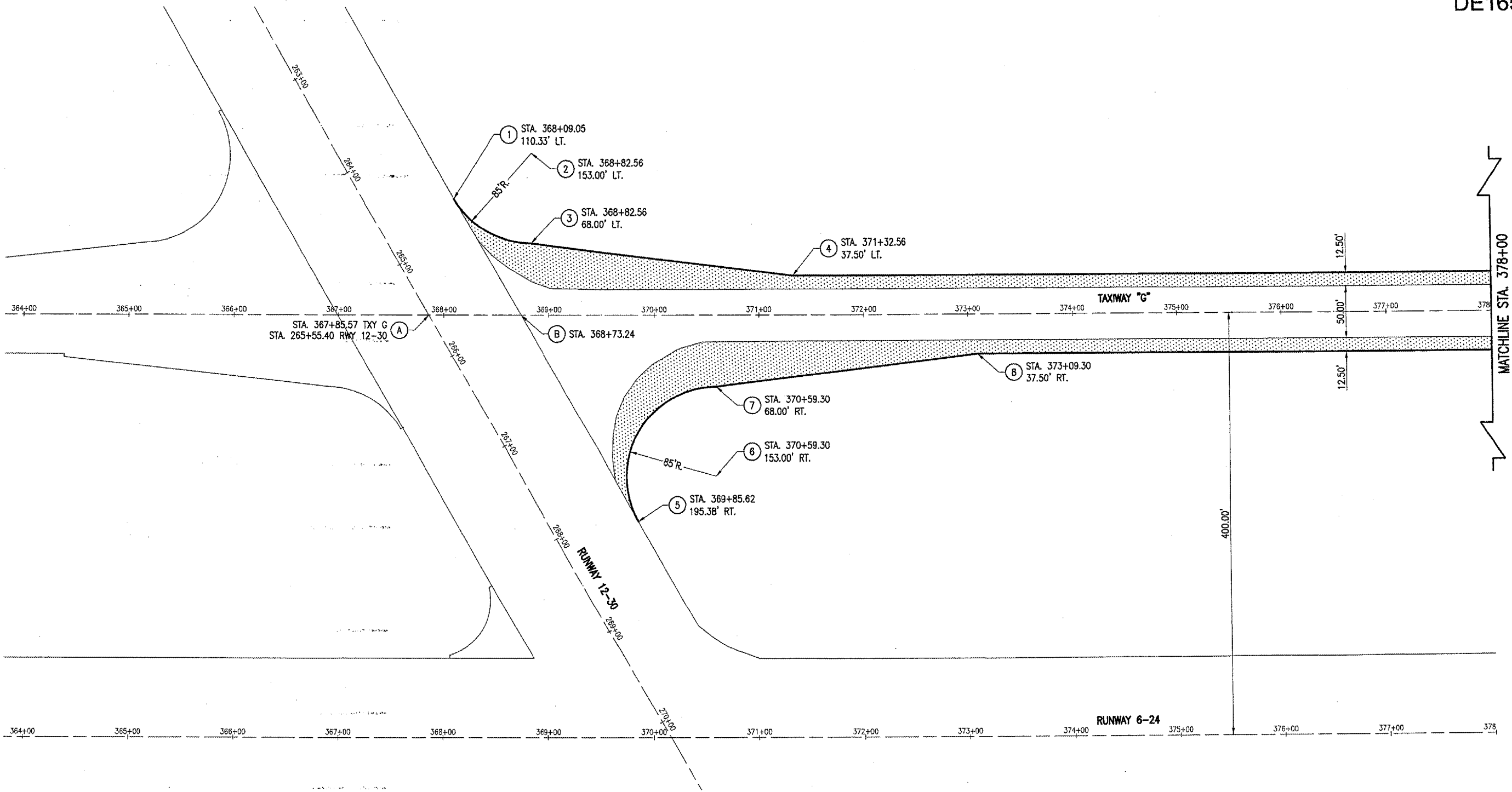
DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

REVISION	DATE	BY

HANSON
 Engineers, Architects, Scientists
 1525 South Sixth Street
 Springfield, Illinois 62705-2886
 Offices Nationwide

TAXIWAY G WIDENING PHASE II
PROPOSED SAFETY PLAN

JUN 10, 2005 9:15 AM OCC
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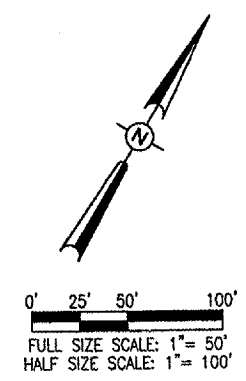
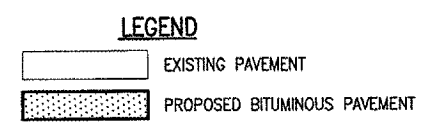


COORDINATE DATA - TXY "G" CENTERLINE

ID.	STATION	OFFSET	NORTHING	EASTING
A	367+85.57	0	1154231.78	836291.40
B	368+73.24	0	1154276.31	836366.92

COORDINATE DATA - TAXIWAY "G"

NO.	STATION	OFFSET	NORTHING	EASTING
1	368+09.05	110.33' LT.	1154338.74	836255.58
2	368+82.56	153.00' LT.	1154412.84	836297.23
3	368+82.56	68.00' LT.	1154339.62	836340.41
4	371+32.56	37.50' LT.	1154440.34	836571.24
5	369+85.62	195.38' RT.	1154165.10	836562.97
6	370+59.30	153.00' RT.	1154239.04	836604.91
7	370+59.30	68.00' RT.	1154312.25	836561.73
8	373+09.30	37.50' RT.	1154465.52	836761.58



DATE	REVISION	BY

DECATUR AIRPORT
DECATUR, ILLINOIS
 ILL. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

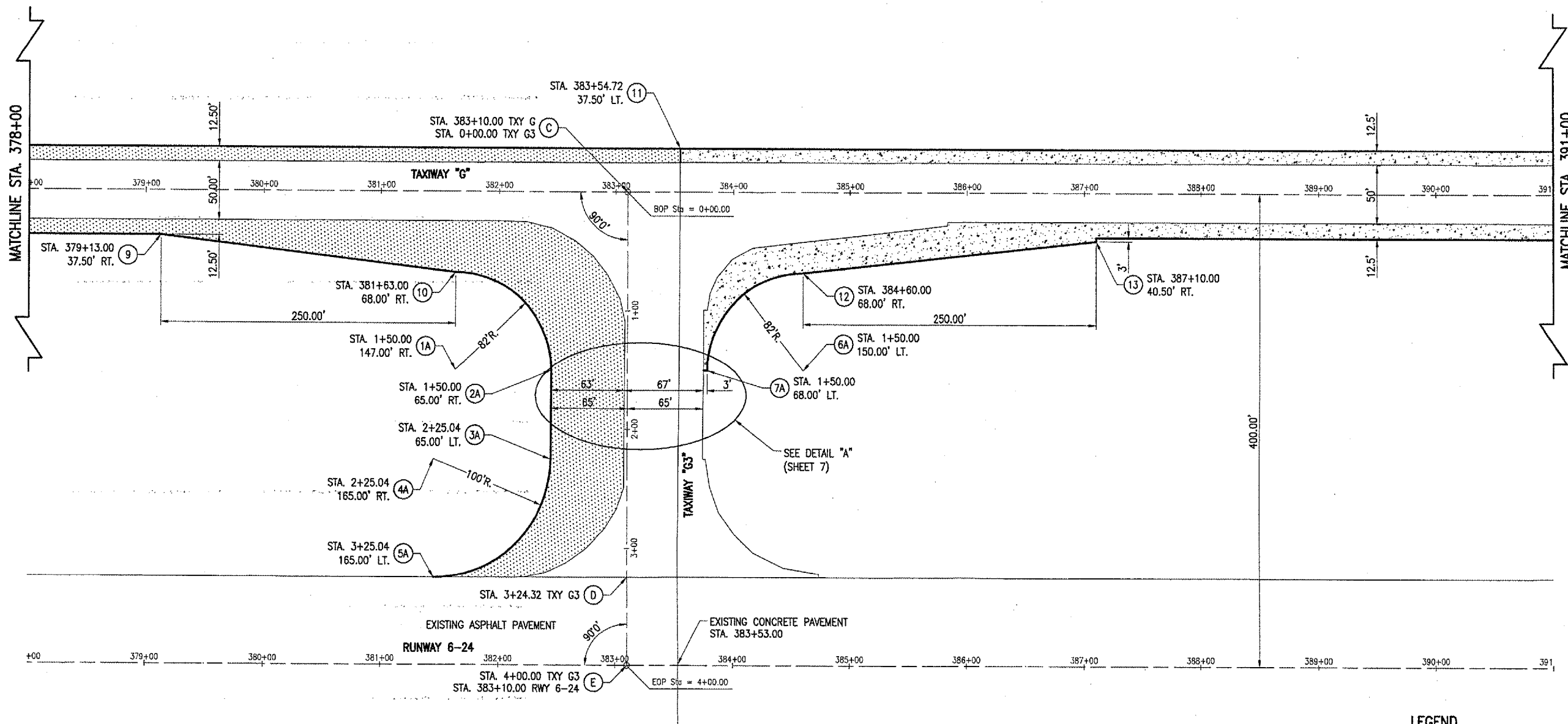
HEL PROJECT No. 802-04TXD
 Elevation 121 GEO.DWG
 Scale 1"=50'
 Date 10/28/04

LAYOUT	DATE
DRAWN	10/28/04
REVIEWED	05/07/05

HANSON
 Engineers, Architects, Scientists
 1525 South Sixth Street
 Springfield, Illinois 62703-2888
 Offices Nationwide

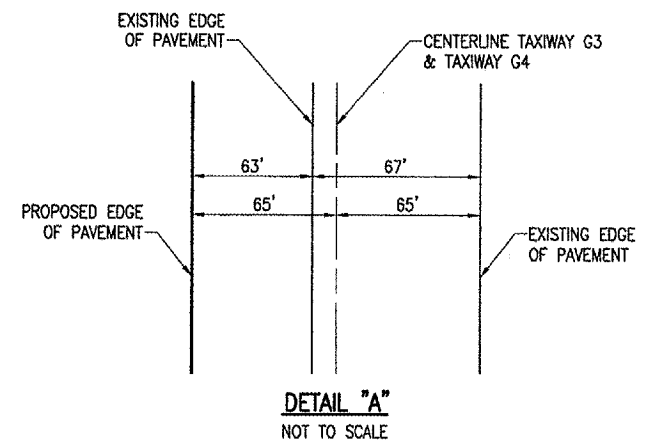
**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED
 GEOMETRIC PLAN
 STA. 367+85.57 TO 378+00

JUN 10, 2005 9:19 AM CCC
 F:\AIRPORTS\DECATUR\802-04TXD\AIRPORT SHEETS\CONTRACT-1\R-121GEO.DWG - STA. 367+85.57 TO 378+00



LEGEND

- EXISTING PAVEMENT
- PROPOSED BITUMINOUS PAVEMENT
- PROPOSED CONCRETE PAVEMENT



COORDINATE DATA - TXY "G3" CENTERLINE

ID.	STATION	OFFSET	NORTHING	EASTING
C	0+00.00	☉	1155006.16	837604.50
D	3+24.32	☉	1154726.62	837769.36
E	4+00.00	☉	1154661.62	837807.69

COORDINATE DATA - TXY "G" CENTERLINE

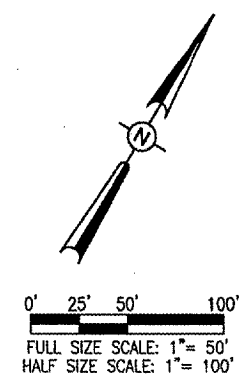
ID.	STATION	OFFSET	NORTHING	EASTING
C	383+10.00	☉	1155006.16	837604.50

COORDINATE DATA - TAXIWAY "G3"

NO.	STATION	OFFSET	NORTHING	EASTING
1A	1+50.00	147.00' RT.	1154802.29	837554.07
2A	1+50.00	65.00' RT.	1154843.94	837624.71
3A	2+25.04	65.00' RT.	1154779.30	837662.83
4A	2+25.04	165.00' RT.	1154728.51	837576.69
5A	3+25.04	165.00' RT.	1154642.37	837627.49
6A	1+50.00	150.00' LT.	1154953.16	837809.90
7A	1+50.00	68.00' LT.	1154911.50	837739.27

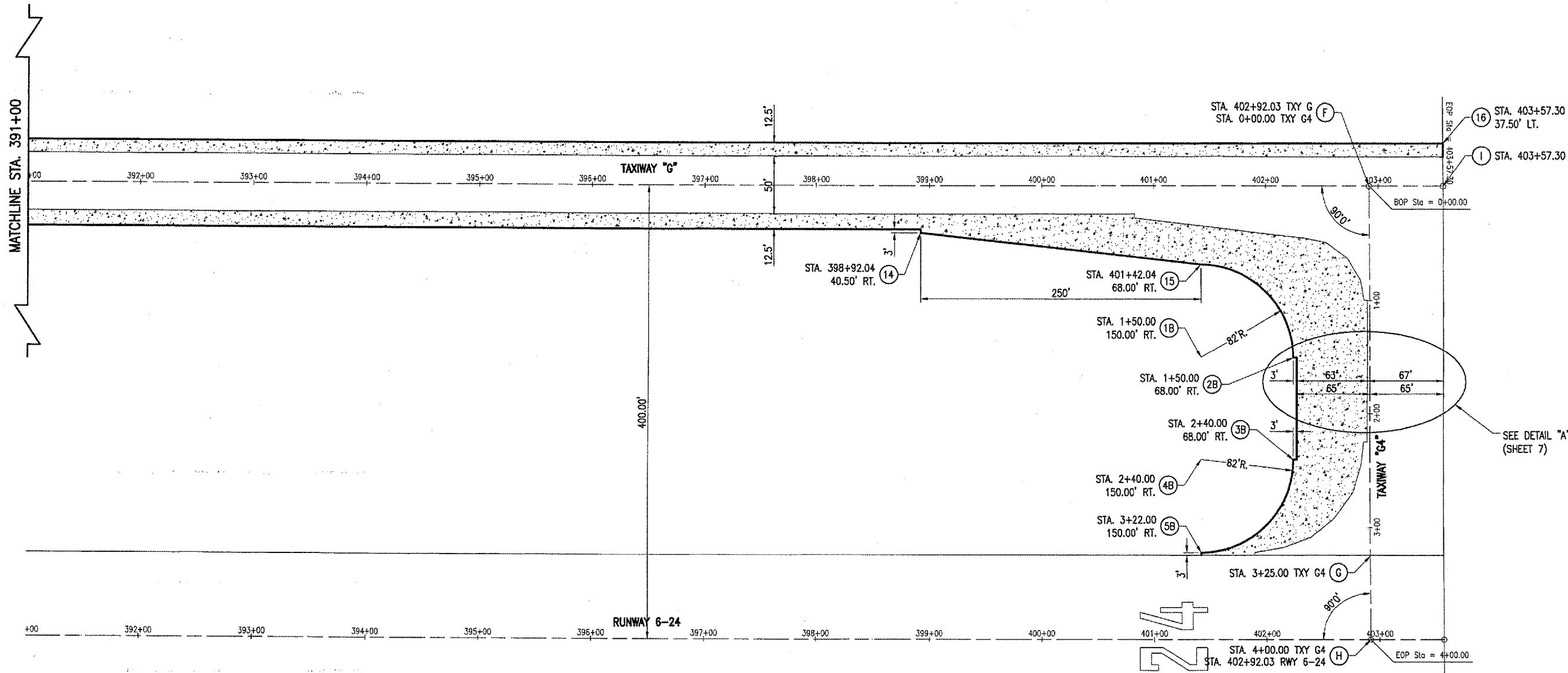
COORDINATE DATA - TAXIWAY "G"

NO.	STATION	OFFSET	NORTHING	EASTING
9	379+13.00	37.50' RT.	1154772.20	837281.60
10	381+63.00	68.00' RT.	1154872.93	837512.43
11	383+54.72	37.50' LT.	1155061.18	837623.97
12	384+60.00	68.00' RT.	1155023.79	837768.25
13	387+10.00	40.50' RT.	1155174.47	837969.62



DATE	REVISION								
HANSON Engineers, Architects, Scientists 525 S. State Street Springfield, Illinois 62768-2886 Offices Nationwide									
IRI Project No. 802-041XXD File No. 121GEO.DWG Scale 1" = 50' Date 10/28/04					LAYOUT WJM 10/28/04 DRAWN CCC 10/28/04 REVIEWED WJM 06/07/05				
DECATUR, ILLINOIS A.I.P. PROJ.: 3-17-0033-2B I.L. PROJ.: DEC-3414					TAXIWAY G WIDENING PHASE II PROPOSED GEOMETRIC PLAN STA. 378+00 TO 391+00				
7									
7 of 110 sheets									

JUN 10, 2005 9:19 AM CCC
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LEGEND
 EXISTING PAVEMENT
 PROPOSED CONCRETE PAVEMENT

COORDINATE DATA - TXY "G4" CENTERLINE

ID.	STATION	OFFSET	NORTHING	EASTING
F	0+00.00	Ⓞ	1156012.99	839311.73
G	3+25.00	Ⓞ	1155733.05	839476.82
H	4+00.00	Ⓞ	1155668.45	839514.92

COORDINATE DATA - TXY "G" CENTERLINE

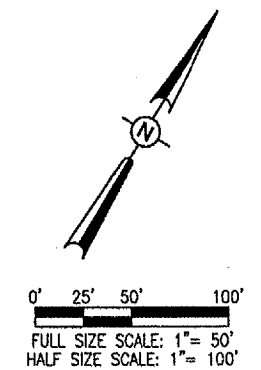
ID.	STATION	OFFSET	NORTHING	EASTING
F	402+92.03	Ⓞ	1156012.99	839311.73
I	403+57.30	Ⓞ	1156046.16	839367.97

COORDINATE DATA - TAXIWAY "G4"

NO.	STATION	OFFSET	NORTHING	EASTING
1B	1+50.00	150.00' RT.	1155807.64	839258.75
2B	1+50.00	68.00' RT.	1155849.26	839329.37
3B	2+40.00	68.00' RT.	1155771.73	839375.09
4B	2+40.00	150.00' RT.	1155730.08	839304.46
5B	3+22.00	150.00' RT.	1155659.45	839346.12

COORDINATE DATA - TAXIWAY "G"

NO.	STATION	OFFSET	NORTHING	EASTING
14	398+92.04	40.50' RT.	1155774.93	838987.79
15	401+42.04	68.00' RT.	1155878.24	839217.10
16	403+57.30	37.50' LT.	1156078.46	839348.92



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DATE	REVISION				
DECATUR, ILLINOIS					
I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28					
I.L. Project No.: 802-04TXY File No.: 1216CO.DWG Scale: 1"=50' Date: 10/28/04		W/M 10/28/04 C/C 10/28/04 W/M 06/07/05			
HANSON Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62760-2886 Chicago, Nationwide					
TAXIWAY G WIDENING PHASE II		PROPOSED GEOMETRIC PLAN STA. 391+00 TO 405+00			
8					
8 of 110 sheets					

NOTES:

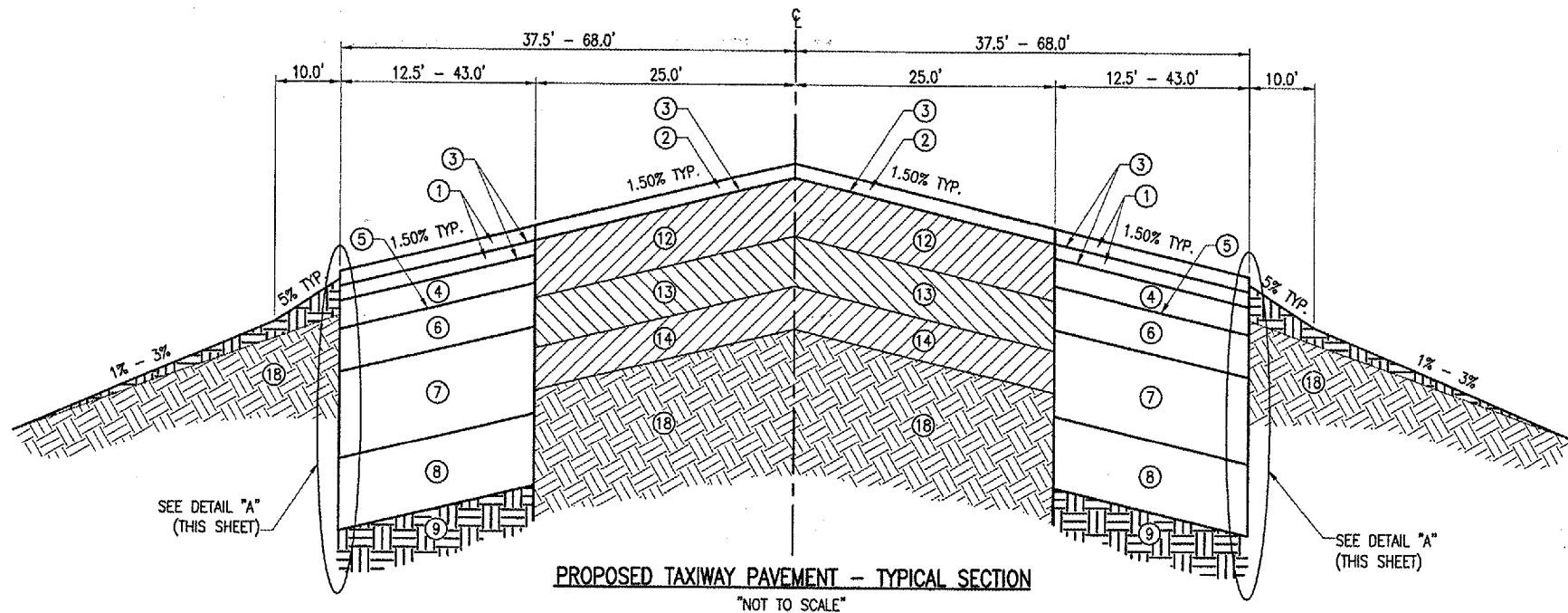
AR201 AND AR401 MARSHALL DESIGN CRITERIA SHALL BE FOR 60,000 LBS. AND GREATER.

AR209 CRUSHED AGGREGATE BASE COURSE, COMPACTION REQUIREMENTS SHALL BE BASED ON MODIFIED PROCTOR, ASTM 1557, 60,000 LBS. OR GREATER.

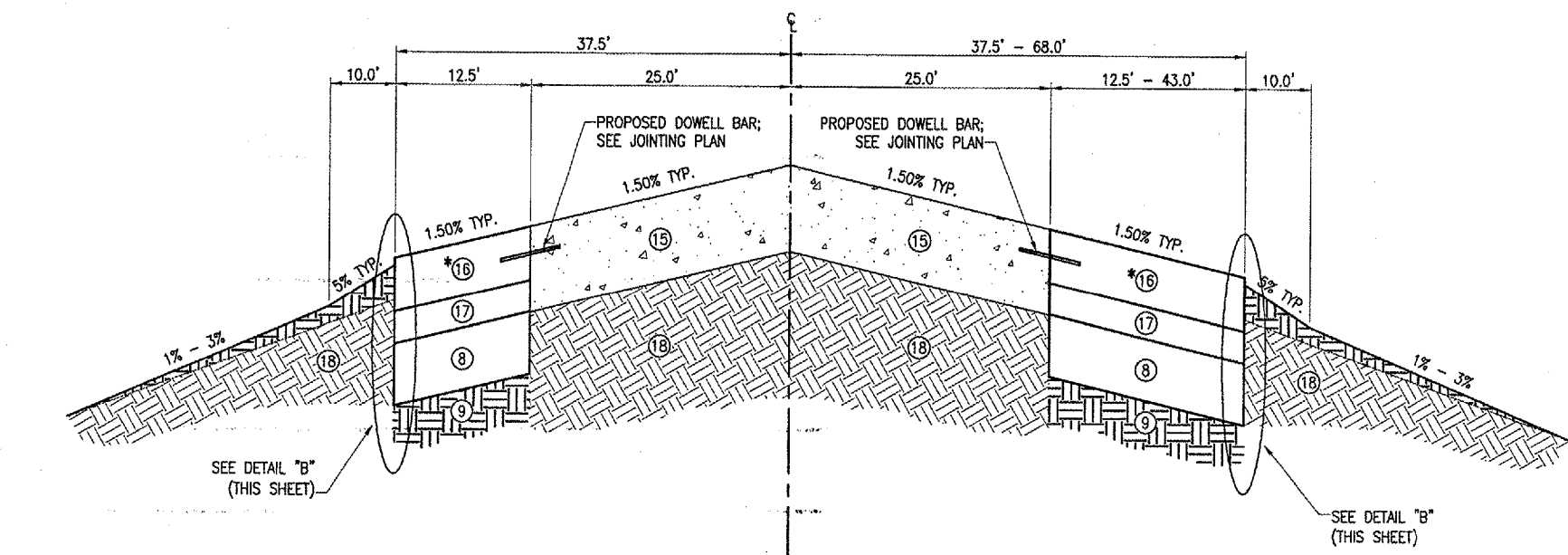
AR501 - THE PAVEMENT DESIGN IS FOR AIRCRAFT WEIGHTS OF 60,000 LBS. AND GREATER. PROPORTIONING SHALL BE ACCORDING TO SUPPLEMENTAL SPECIFICATION 501-3.6(B).

LEGEND FOR TYPICAL SECTION

- ① PROPOSED AR401: BITUMINOUS SURFACE COURSE - 2"
- ② PROPOSED AR401: VARIABLE - 2" MAX. DEPTH PER LIFT BITUMINOUS SURFACE COURSE
- ③ PROPOSED AR603: BITUMINOUS TACK COAT (RATE PER SPECIFICATION)
- ④ PROPOSED AR201: 10" BITUMINOUS BASE COURSE
- ⑤ PROPOSED AR602: BITUMINOUS PRIME COAT (RATE PER SPECIFICATION)
- ⑥ PROPOSED AR209: 8" CRUSHED AGGREGATE BASE COURSE
- ⑦ PROPOSED AR208: 20" OVERSIZED AGGREGATE
- ⑧ PROPOSED AR155: SOIL PROCESSING - 16"
- ⑨ PROPOSED AR152: COMPACTED SUBGRADE
- ⑩ PROPOSED AR705: POROUS BACKFILL No. 1
- ⑪ PROPOSED AR705: 6" PERFORATED UNDERDRAIN W/SOCK
- ⑫ EXISTING AR201: BITUMINOUS BASE COURSE & EXISTING AR401: BITUMINOUS SURFACE COURSE
- ⑬ EXISTING AR501: 9" PCC PAVEMENT
- ⑭ EXISTING AR208: 8" AGGREGATE SUBBASE COURSE
- ⑮ EXISTING AR501: 18 1/2" PCC PAVEMENT
- *⑯ PROPOSED AR501: 14 1/2" PCC PAVEMENT
- ⑰ PROPOSED AR201: 4" BITUMINOUS BASE COURSE
- ⑱ EXISTING GROUND



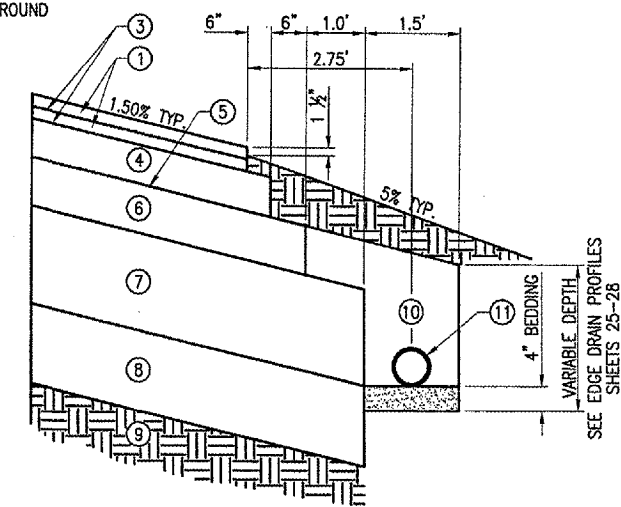
TAXIWAY "G" STA. 368+73.24 TO STA. 383+54.72



PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

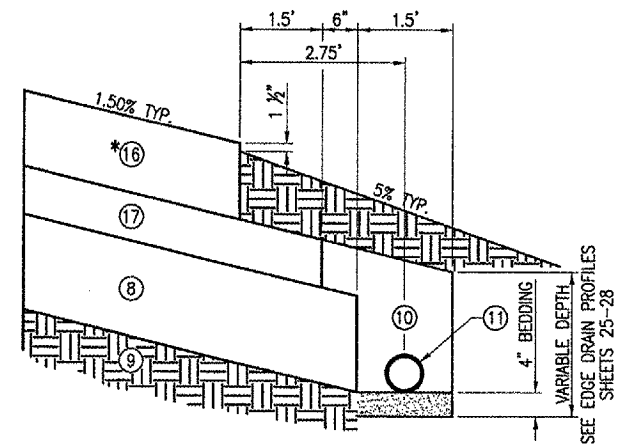
TAXIWAY "G" STA. 383+54.72 TO STA. 403+57.30

* THE CONTRACTOR IS BIDDING ON 14.5" PCC PAVEMENT, EVEN THOUGH THE SCHEDULE OF PRICES IN THE BID DOCUMENTS STATE 14" PCC PAVEMENT.



PROPOSED TAXIWAY PAVEMENT - DETAIL "A"

NOT TO SCALE

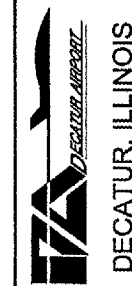


PROPOSED TAXIWAY PAVEMENT - DETAIL "B"

NOT TO SCALE

DE165

DATE	REVISION	BY



DECATUR, ILLINOIS

HEI Project No. 802-041XYD	06/07/05
Filename R-521TYP.DWG	
Scale N/A	
Date 11/12/04	
LAYOUT	
DRAWN	
REVIEWED	



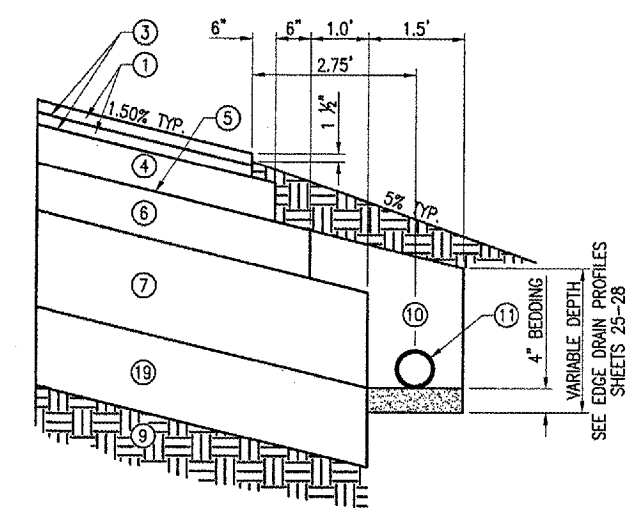
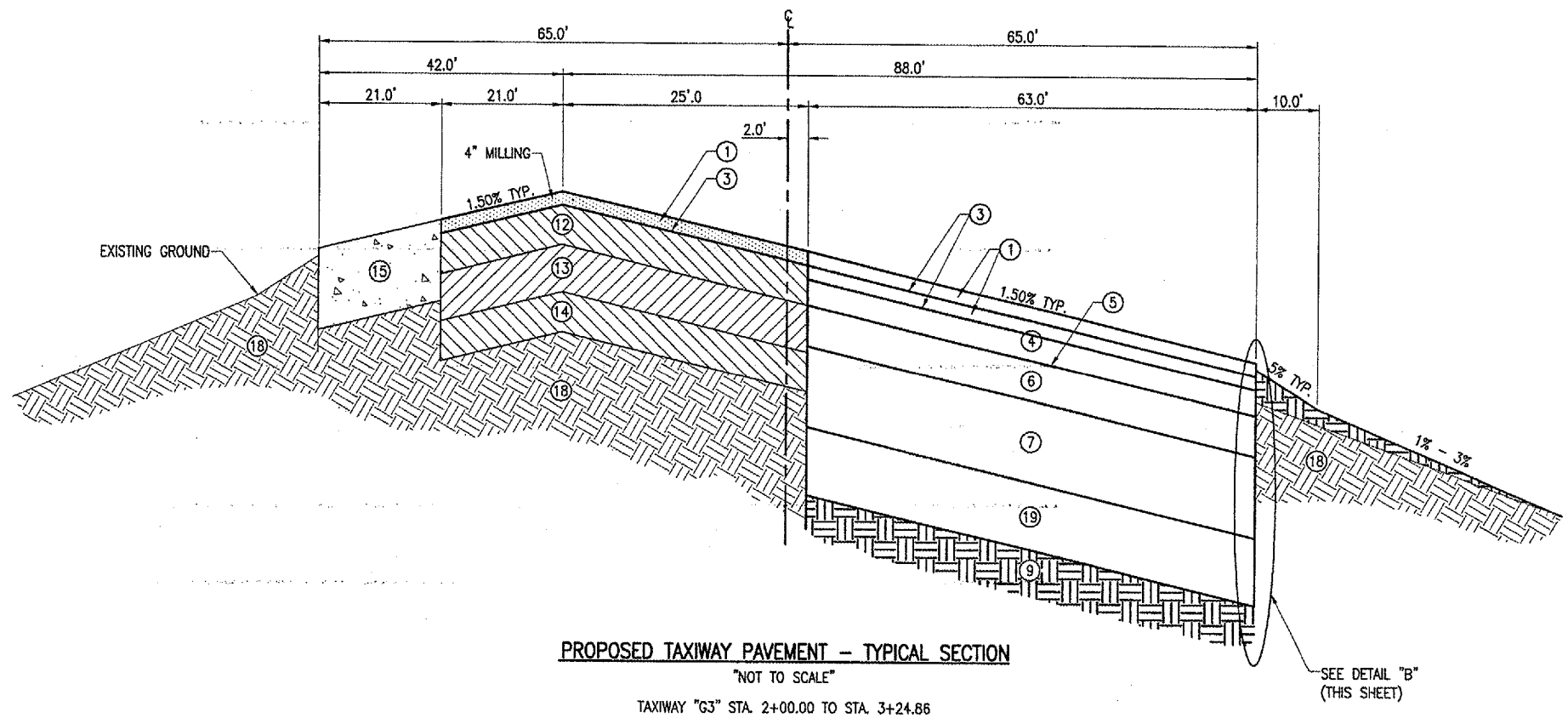
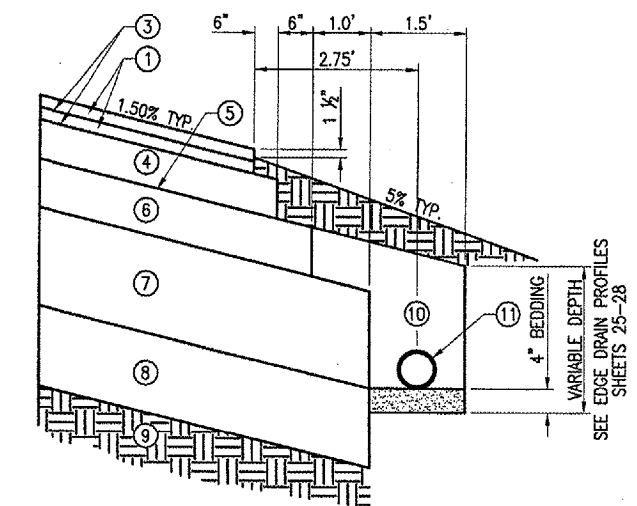
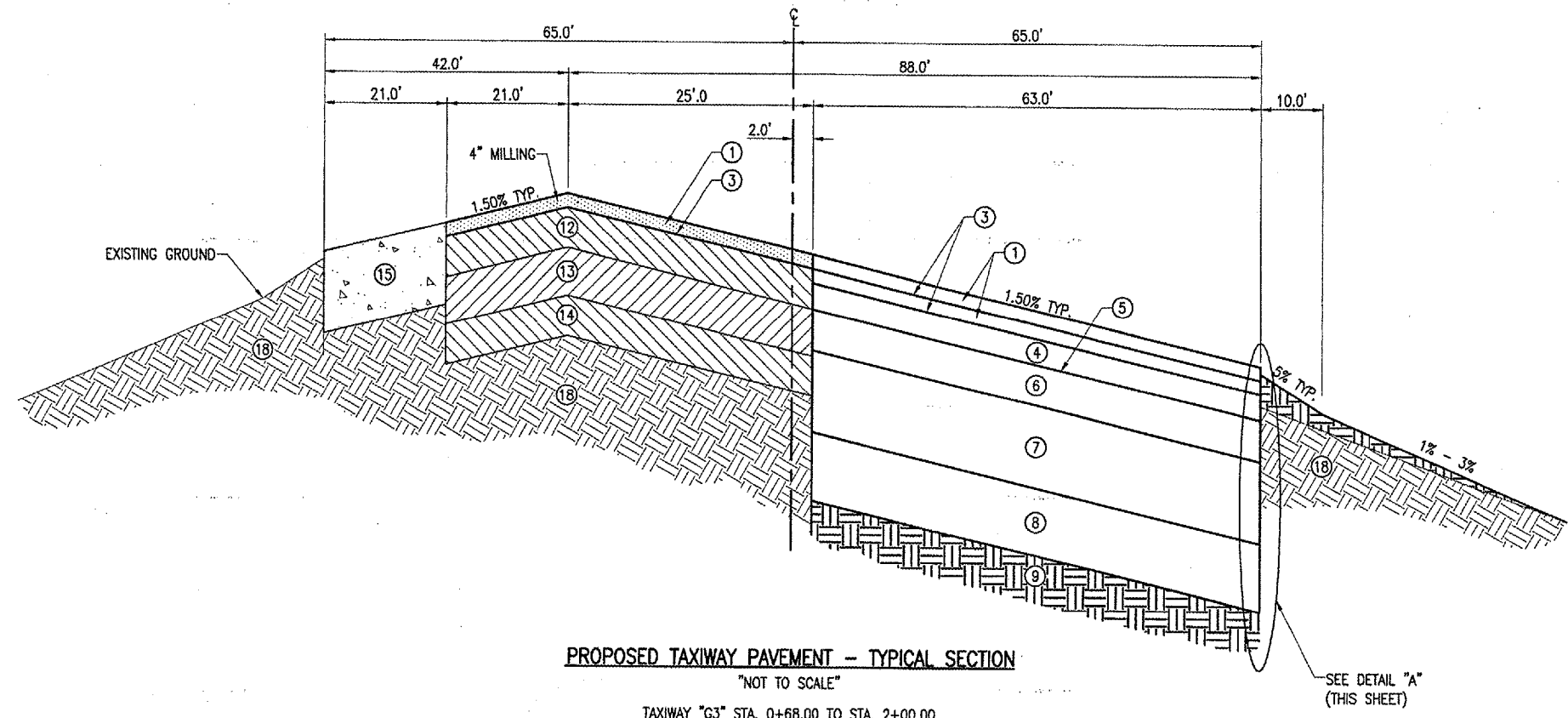
HANSON
Engineers, Architects, Scientists
1625 South Sixth Street
Springfield, Illinois 62703-2886
Offices Nationwide

TAXIWAY G WIDENING PHASE II	PROPOSED TYPICAL SECTIONS TAXIWAY "G"
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JUN 10, 2005 9:42 AM CCC
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LEGEND FOR TYPICAL SECTION

- ① PROPOSED AR401: BITUMINOUS SURFACE COURSE - 2"
- ③ PROPOSED AR603: BITUMINOUS TACK COAT (RATE PER SPECIFICATION)
- ④ PROPOSED AR201: 10" BITUMINOUS BASE COURSE
- ⑤ PROPOSED AR602: BITUMINOUS PRIME COAT (RATE PER SPECIFICATION)
- ⑥ PROPOSED AR209: 8" CRUSHED AGGREGATE BASE COURSE
- ⑦ PROPOSED AR208: 20" OVERSIZED AGGREGATE
- ⑧ PROPOSED AR155: SOIL PROCESSING - 16"
- ⑨ PROPOSED AR152: COMPACTED SUBGRADE
- ⑩ PROPOSED AR705: POROUS BACKFILL No. 1
- ⑪ PROPOSED AR705: 6" PERFORATED UNDERDRAIN W/SOCK
- ⑫ EXISTING AR201: BITUMINOUS BASE COURSE & EXISTING AR401: BITUMINOUS SURFACE COURSE
- ⑬ EXISTING AR501: 9" PCC PAVEMENT
- ⑭ EXISTING AR208: 8" AGGREGATE SUBBASE COURSE
- ⑮ EXISTING AR501: 18 1/2" PCC PAVEMENT
- ⑯ EXISTING GROUND
- ⑰ PROPOSED AR208: 8" OVERSIZED AGGREGATE



REVISION	DATE	BY

DECATUR, ILLINOIS

IL PROJ.: DEC-3414

A.I.P. PROJ.: 3-17-0033-28

FILE PROJECT No. 802-041XXD	DATE 06/07/05
FILENAME: R-521TYP.DWG	
SCALE: N/A	
DATE: 11/12/04	
LAYOUT	
DRAWN	
REVIEWED	

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Engineers, Architects, Scientists
1825 South Sixth Street
Springfield, Illinois 62703-2886
Offices Nationwide

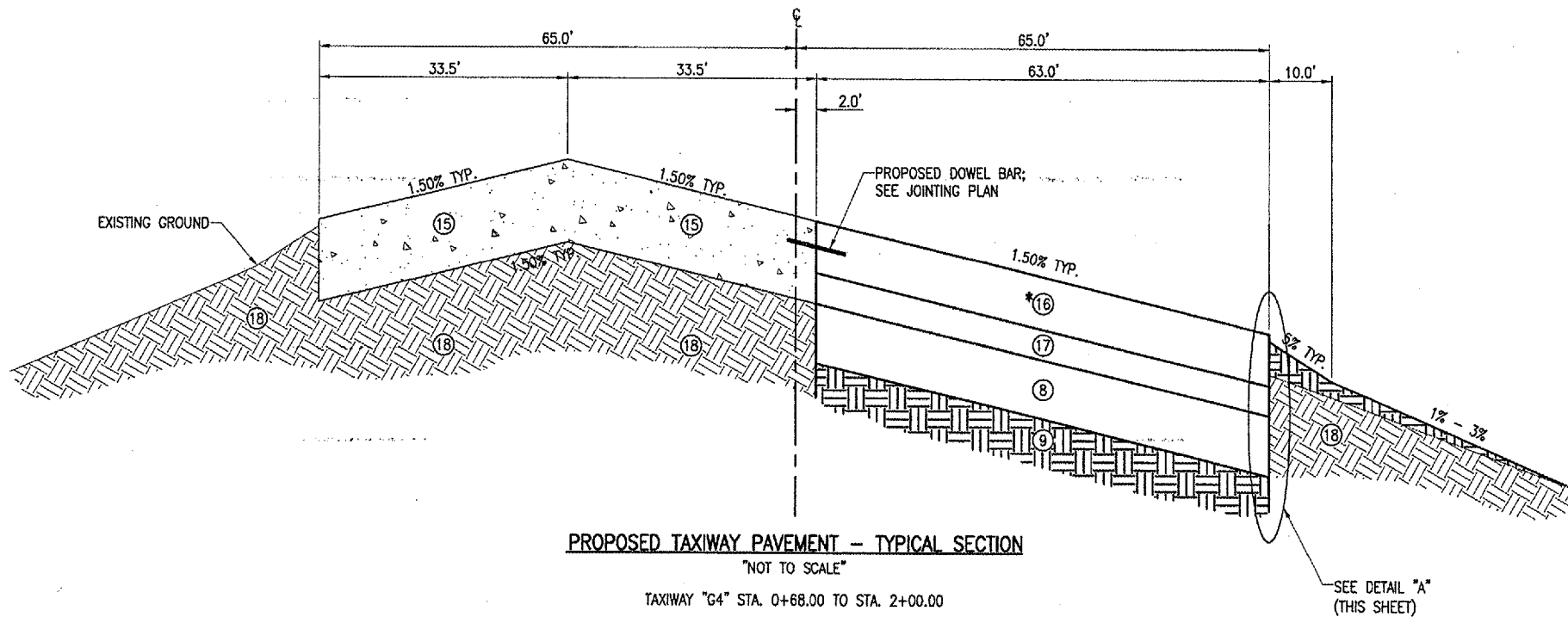
TAXIWAY G WIDENING
PHASE II

PROPOSED
TYPICAL SECTIONS
TAXIWAY "G3"

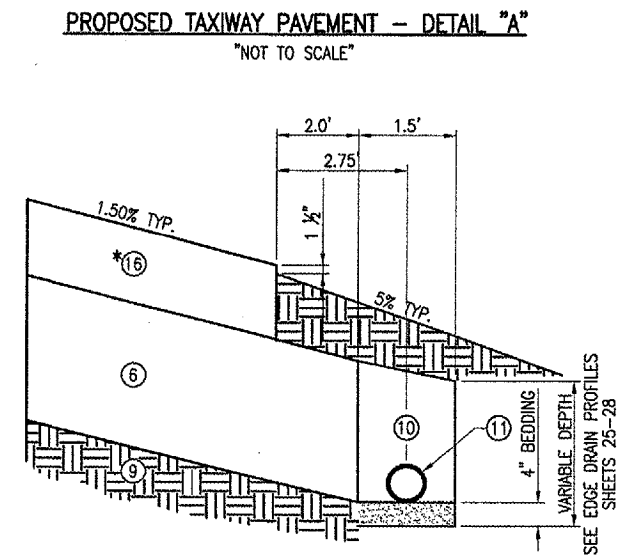
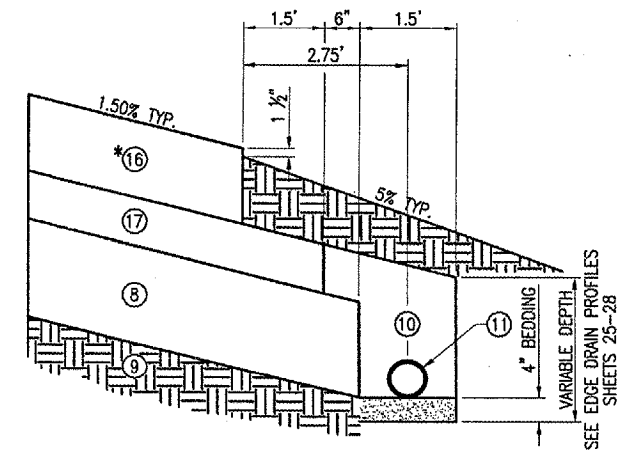
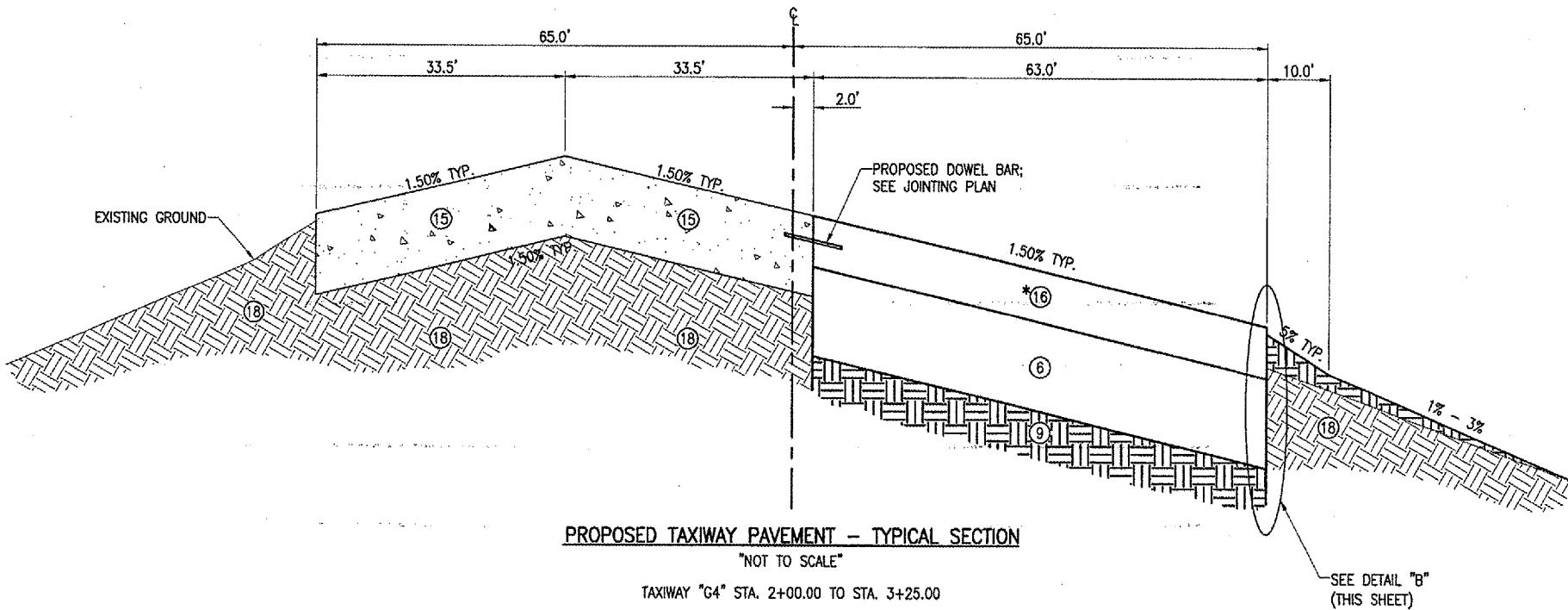
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LEGEND FOR TYPICAL SECTION

- ⑥ PROPOSED AR209: 8" CRUSHED AGGREGATE BASE COURSE
- ⑧ PROPOSED AR155: SOIL PROCESSING - 16"
- ⑨ PROPOSED AR152: COMPACTED SUBGRADE
- ⑩ PROPOSED AR705: POROUS BACKFILL No. 1
- ⑪ PROPOSED AR705: 6" PERFORATED UNDERDRAIN W/SOCK
- ⑮ EXISTING AR501: 18 1/2" PCC PAVEMENT
- *⑯ PROPOSED AR501: 14 1/2" PCC PAVEMENT
- ⑰ PROPOSED AR201: 4" BITUMINOUS BASE COURSE
- ⑱ EXISTING GROUND



* THE CONTRACTOR IS BIDDING ON 14.5" PCC PAVEMENT, EVEN THOUGH THE SCHEDULE OF PRICES IN THE BID DOCUMENTS STATE 14" PCC PAVEMENT.



DATE	REVISION	BY

DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

REL. Project No. 802-041X10	REVIEWED	06/07/05
Drawings R-521TYP.DWG	DRAWN	
Scale N/A	LAYOUT	
Date 11/12/04		

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED
 TYPICAL SECTIONS
 TAXIWAY "G4"

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JUN 13, 2005 11:04 AM CCC

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SOIL PROCESSING - 16"

OPTIMUM MOISTURE AND DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED IN ACCORDANCE WITH ASTM D1557 FOR AIRCRAFT WEIGHING 60,000 LBS. OR GREATER.

THE SUBGRADE SHALL BE COMPACTED TO A DENSITY OF 93% OF MODIFIED PROCTOR DENSITY.

PROPOSED MILLING AREA (1809 S.Y.)

EXISTING INLET: 1E
STA. 372+17.67, 118' LT.
TOP ELEV. 669.50
FLOW ELEV. 666.80

UNION DRAINAGE DISTRICT LINE #1
21" R.C.P.

18" R.C.P.

EXISTING MANHOLE: MH-2E
STA. 372+18.00, 28' RT.
TOP ELEV. 674.43
FLOW ELEV. 665.11
(TO BE ADJUSTED TO ELEV. 674.84)

EXISTING MANHOLE: MH-1E
STA. 370+74.33, 125' RT.
TOP ELEV. 671.32
FLOW ELEV. 664.8±
TOP TO BE ADJUSTED TO ELEV. 672.50

EXISTING INLET: 2E
STA. 372+18.33, 125.5' RT.
TOP ELEV. 669.40
FLOW ELEV. 665.81

8" UNDERDRAIN

LEGEND

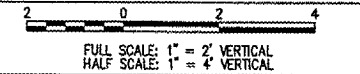
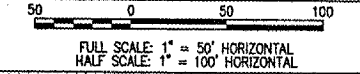
- EXISTING PAVEMENT
- PROPOSED BITUMINOUS MILLING
- PROPOSED BITUMINOUS PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING INSPECTION HOLE
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

THE EXISTING ASPHALT PAVEMENT EDGES WILL BE BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED BY BRUSH OR SPRAY TO GET TOTAL COVERAGE ON THE EDGE SURFACE PRIOR TO BITUMINOUS BASE OR SURFACE COURSE MATERIAL BEING PLACED ADJACENT TO THE EDGE. THIS WORK IS CONSIDERED INCIDENTAL TO THE PRICE PER SQUARE YARD FOR BITUMINOUS BASE COURSE.

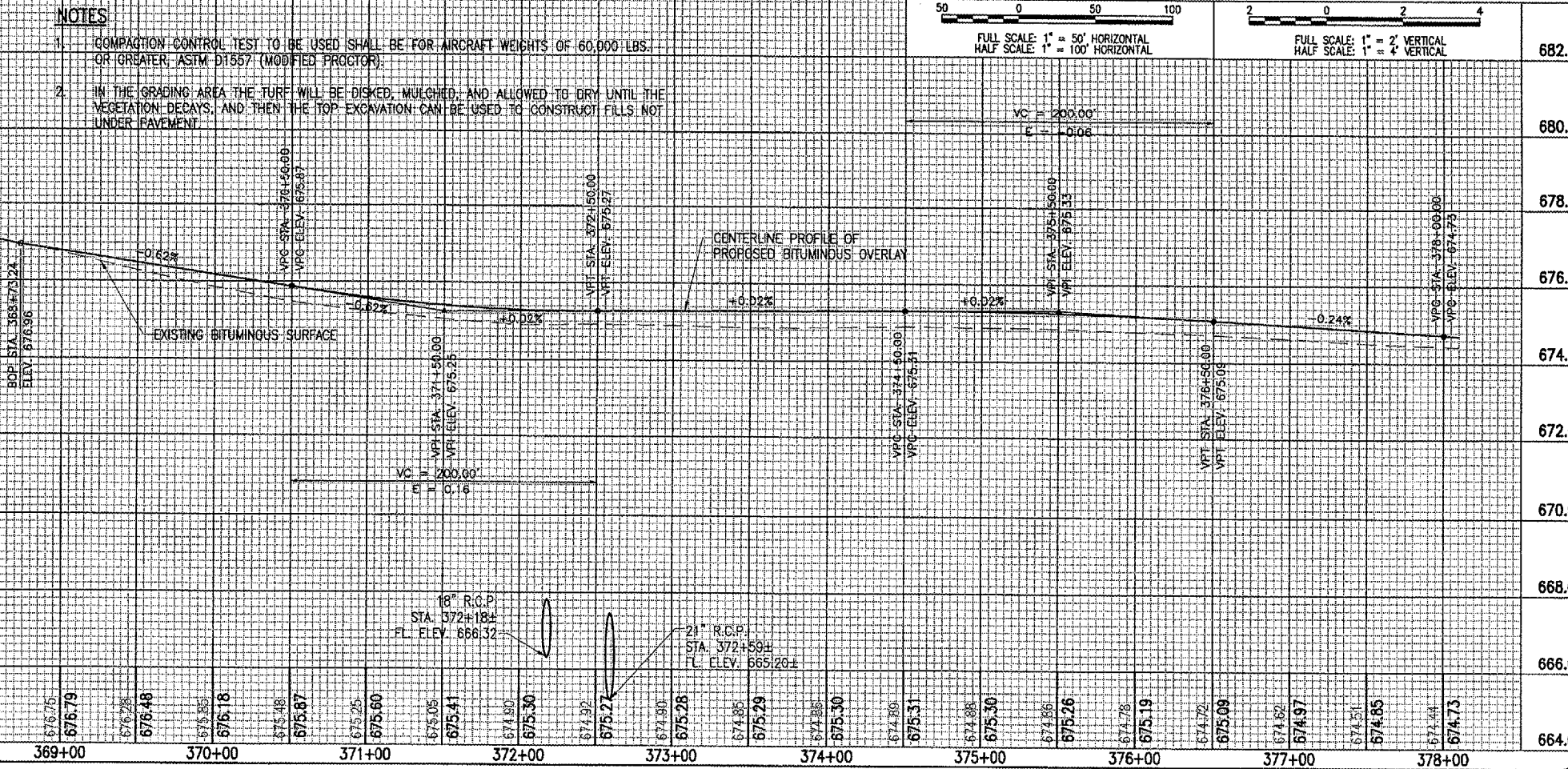
NOTES

1. COMPACTION CONTROL TEST TO BE USED SHALL BE FOR AIRCRAFT WEIGHTS OF 60,000 LBS. OR GREATER, ASTM D1557 (MODIFIED PROCTOR).
2. IN THE GRADING AREA THE TURF WILL BE DISKED, MULCHED, AND ALLOWED TO DRY UNTIL THE VEGETATION DECAYS, AND THEN THE TOP EXCAVATION CAN BE USED TO CONSTRUCT FILLS NOT UNDER PAVEMENT.



EARTHWORK DISTRIBUTION					
LOCATION	EXCAVATION	EMBANKMENT	SHRINKAGE (EMBANKMENT x 1.40)	BORROW	WASTE
TAXIWAY "G"	8,685 C.Y.	6,554 C.Y.	9,176 C.Y.	491 C.Y.	0 C.Y.
TAXIWAY "G3"	1,229 C.Y.	177 C.Y.	248 C.Y.	0 C.Y.	981 C.Y.
TAXIWAY "G4"	574 C.Y.	28 C.Y.	39 C.Y.	0 C.Y.	546 C.Y.
TOTALS	10,488 C.Y.	6,759 C.Y.	9,463 C.Y.	491 C.Y.	1,527 C.Y.
TOTAL WASTE: 1,527 C.Y. - 491 C.Y. = 1,036 C.Y.					
ANY EXCESS EXCAVATION WILL BE WASTED BETWEEN STATIONS 384+00 TO 389+00 BY EXTENDING THE NORTH SHOULDER BEYOND 85 FEET FROM THE TAXIWAY CENTERLINE.					

SUMMARY OF QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	10,448	
AR155540	BY-PRODUCT LIME	TON	708	
AR155616	SOIL PROCESSING - 16"	S.Y.	16,429	
AR201610	BITUMINOUS BASE COURSE	TON	6,948	
AR201630	BITUMINOUS BASE TEST SECTION	EACH	1	
AR208540	OVERSIZE AGGREGATE	TON	9,592	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	4,312	
AR401610	BITUMINOUS SURFACE COURSE	TON	4,747	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR501514	14.5" PCC PAVEMENT	S.Y.	8,735	
AR501530	PCC TEST BATCH	EACH	1	
AR602510	BITUMINOUS PRIME COAT	GAL.	2,864	
AR603510	BITUMINOUS TACK COAT	GAL.	4,653	



DE165

MATCHLINE STA. 378+00

DATE	REVISION	BY



DECATUR, ILLINOIS

NET Project No. 802-DATXXD	FILENAME R-701PMP.DWG	SCALE 1"=50'H, 1"=2'V	DATE 03/18/05
LAYOUT	CCC	03/18/05	
DRAWN	CCC	03/18/05	
REVIEWED	WJM	06/07/05	



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Offices Nationwide

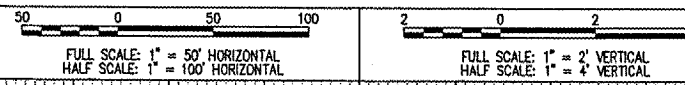
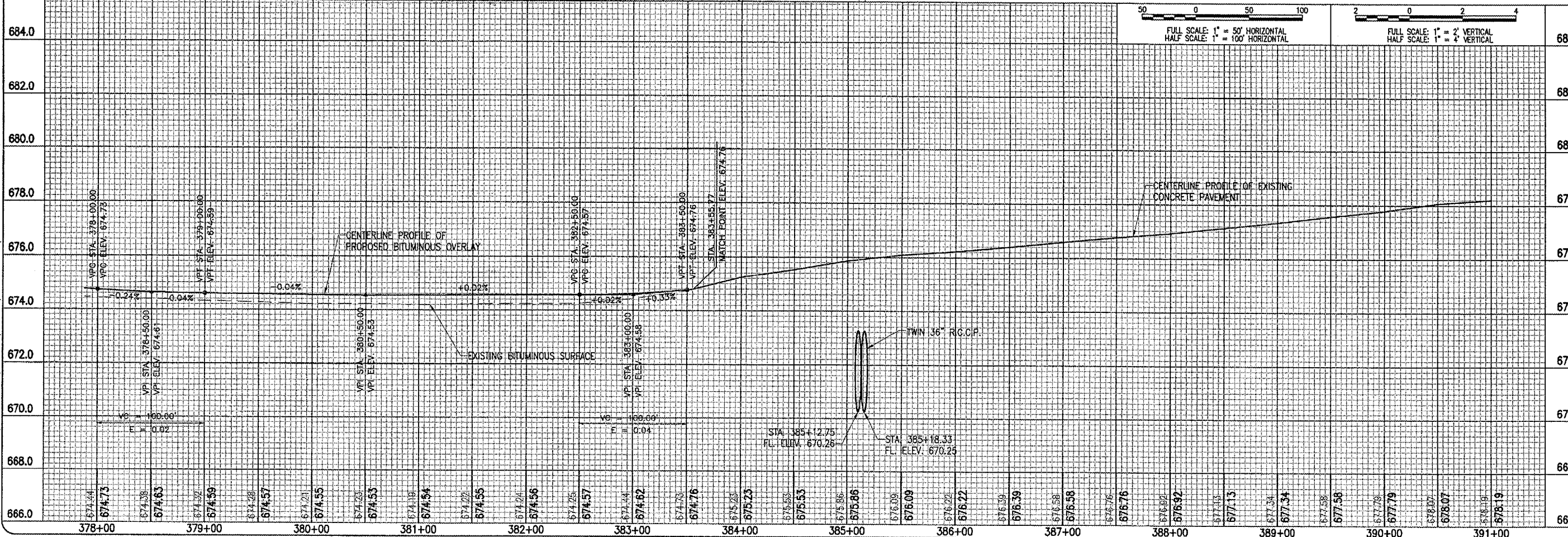
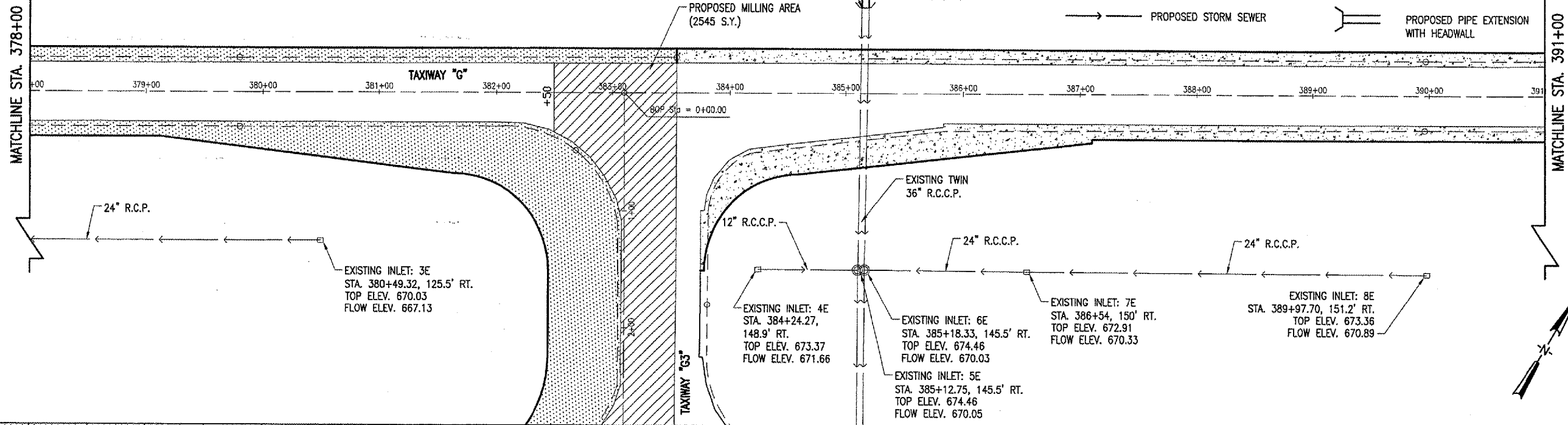
TAXIWAY G WIDENING
PHASE II
PROPOSED PLAN & PROFILE
TAXIWAY "G"
STA. 367+86.06 TO STA. 378+00

BITUMINOUS QUANTITIES				
ITEM No.	DESCRIPTION	LOCATION	S.Y.	TONS
AR201610	BITUMINOUS BASE COURSE	BITUMINOUS WIDENING	8,182	4,909
AR201610	BITUMINOUS BASE COURSE	PCC WIDENING	8,494	2,039
AR401610	BITUMINOUS SURFACE COURSE	BITUMINOUS WIDENING	8,023	1,926
AR401610	BITUMINOUS SURFACE COURSE	BITUMINOUS OVERLAY	10,992	2,821

LEGEND

- EXISTING PAVEMENT
- PROPOSED BITUMINOUS MILLING
- PROPOSED BITUMINOUS PAVEMENT
- PROPOSED P.C.C. PAVEMENT
- EXISTING UNDERDRAIN
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING HEADWALL
- EXISTING INSPECTION HOLE
- PROPOSED INLET
- PROPOSED MANHOLE
- PROPOSED PIPE EXTENSION WITH HEADWALL

DE165



DATE	REVISION	BY

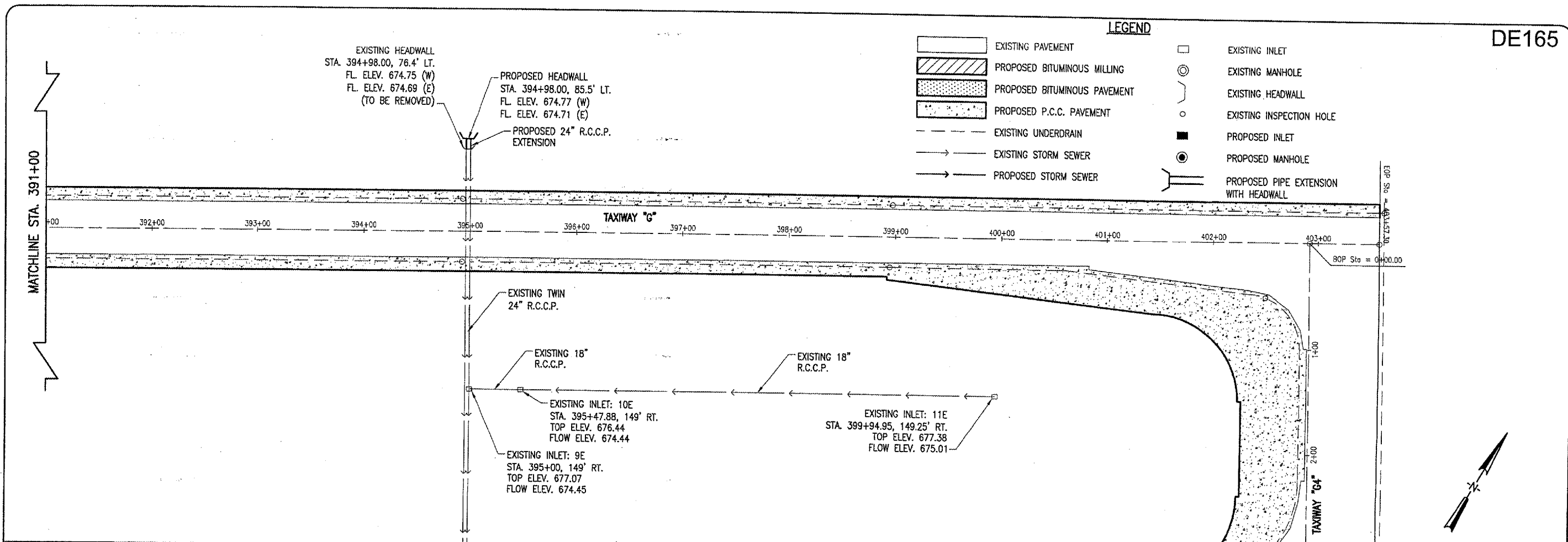


181 Project No. 802-041XYD	FILENAME R-701PMP.DWG	LAYOUT	CCC	03/18/05
Scale 1"=50'H 1"=2'V	Date 03/18/05	DRAWN	CCC	03/18/05
		REVIEWED	WJM	06/07/05

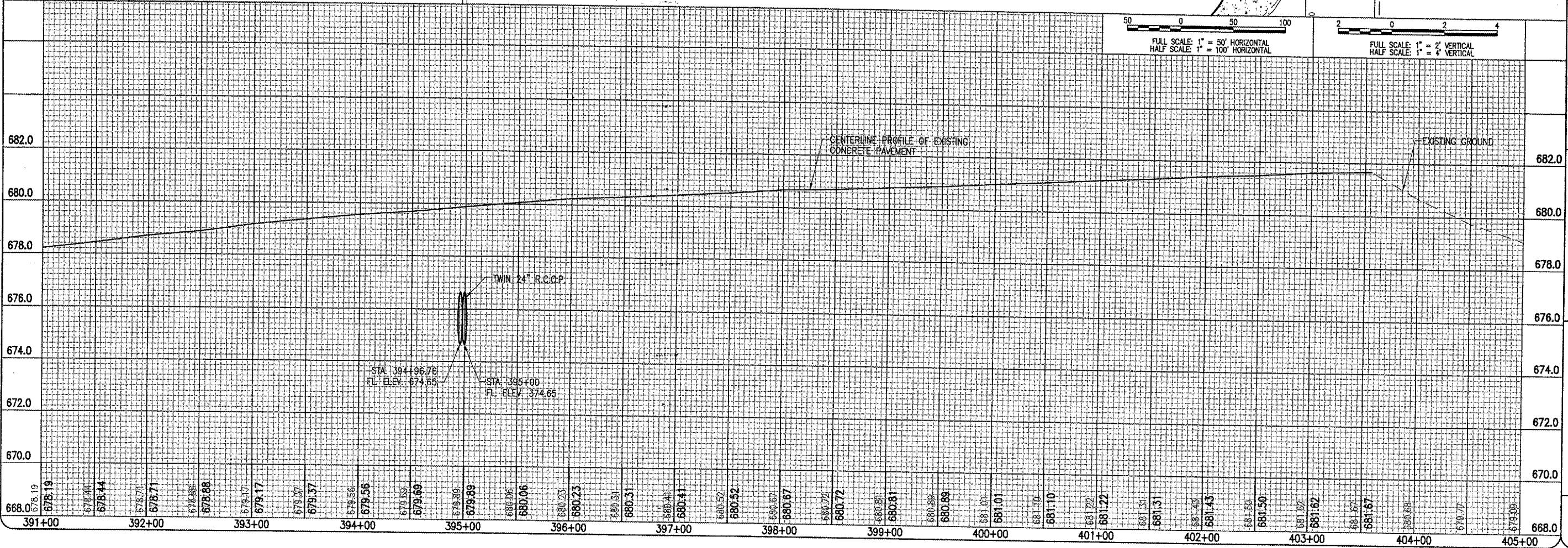
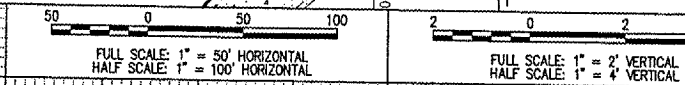


TAXIWAY G WIDENING
PHASE II
PROPOSED PLAN & PROFILE
TAXIWAY "G"
STA. 378+00 TO STA. 391+00

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DE165



DATE	REVISION	BY

DECATUR, ILLINOIS

IL PROJ.: DEC-3414

A.I.P. PROJ.: 3-17-0033-28

REVISED	W/M	DATE

REF. PROJECT No. 802-04TXD
 Filename: R-701PWP.DWG
 Scale: 1"=50'H, 1"=2'V
 Date: 03/18/05

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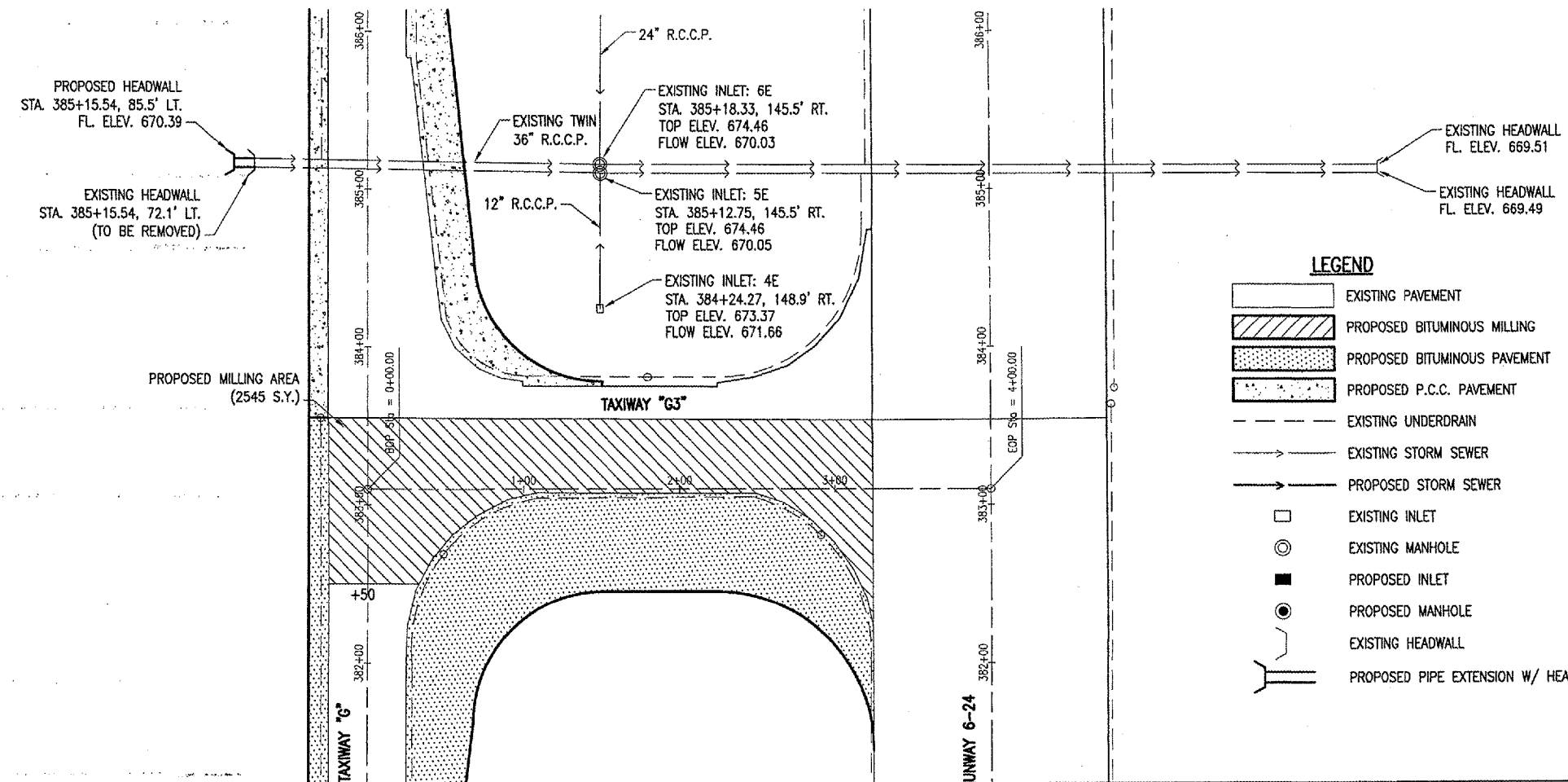
TAXIWAY G WIDENING PHASE II

PROPOSED PLAN & PROFILE TAXIWAY "G"

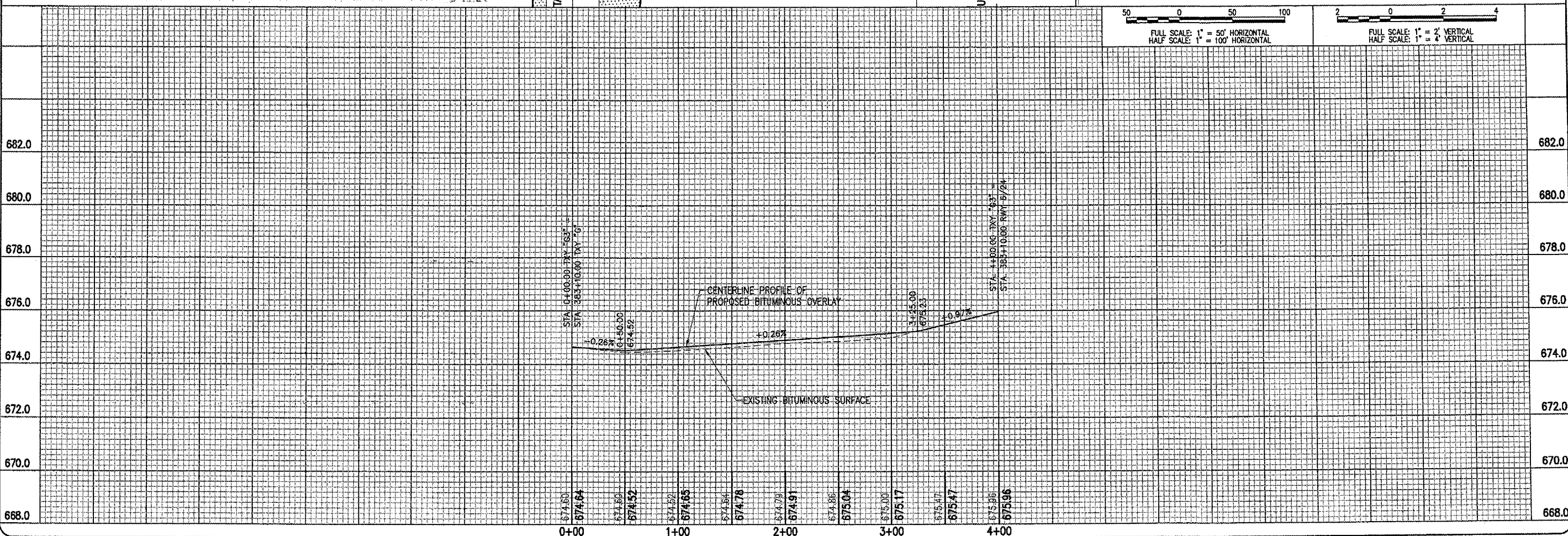
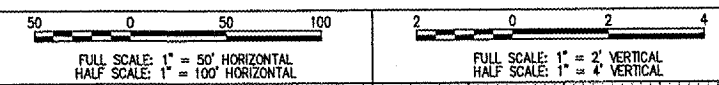
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DE165



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED BITUMINOUS MILLING
 - PROPOSED BITUMINOUS PAVEMENT
 - PROPOSED P.C.C. PAVEMENT
 - EXISTING UNDERDRAIN
 - EXISTING STORM SEWER
 - PROPOSED STORM SEWER
 - EXISTING INLET
 - EXISTING MANHOLE
 - PROPOSED INLET
 - PROPOSED MANHOLE
 - EXISTING HEADWALL
 - PROPOSED PIPE EXTENSION W/ HEADWALL



DATE	REVISION	BY

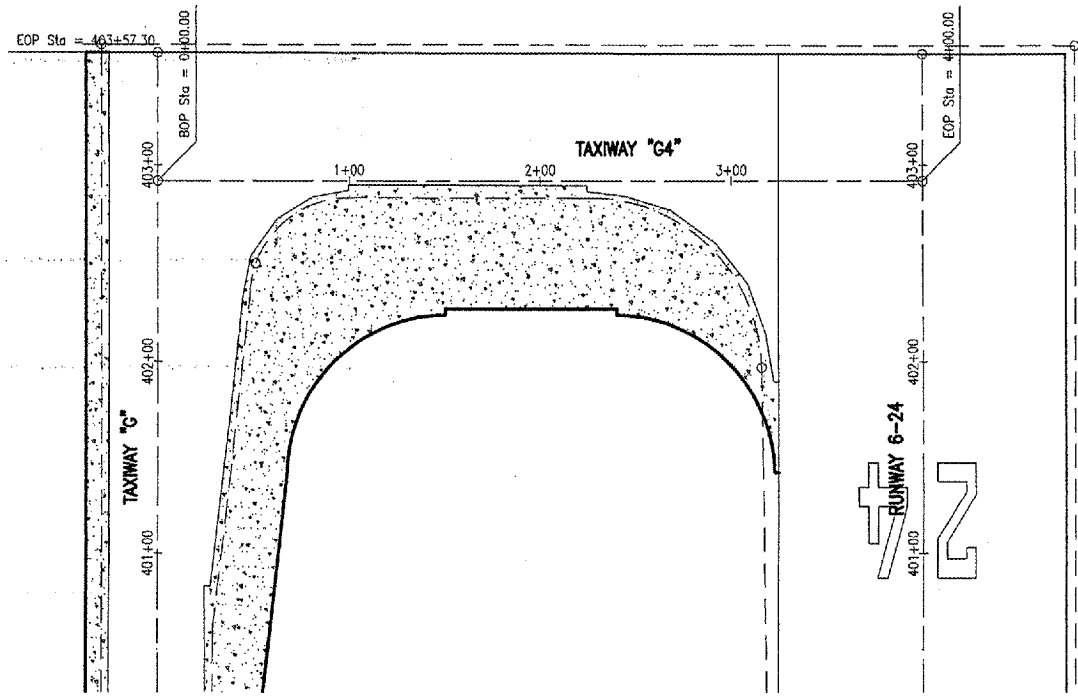
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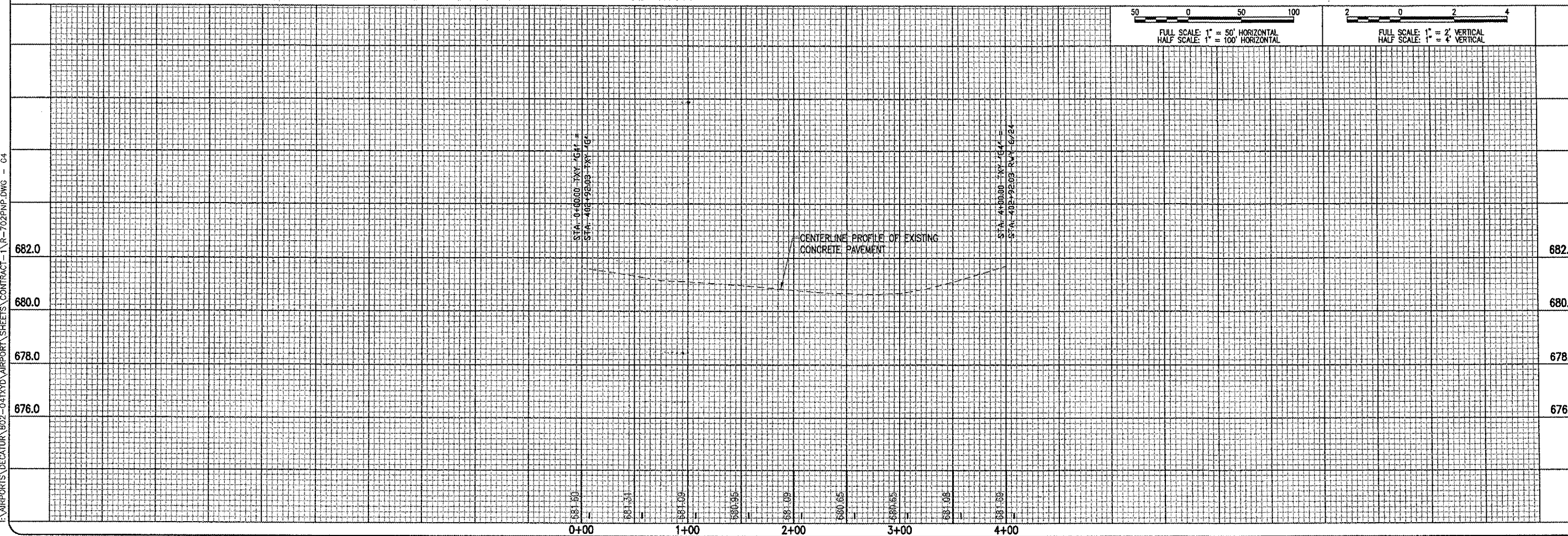
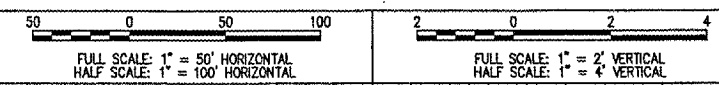
LAYOUT	CCC	03/18/05
DRAWN	CCC	03/18/05
REVIEWED	WJM	06/07/05

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED PLAN & PROFILE
 TAXIWAY "G3"
 STA. 0+00 TO STA. 4+00



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED P.C.C. PAVEMENT
 - EXISTING STORM SEWER
 - PROPOSED STORM SEWER
 - EXISTING UNDERDRAIN
 - EXISTING INLET
 - EXISTING MANHOLE
 - PROPOSED INLET
 - PROPOSED MANHOLE
 - EXISTING HEADWALL



REVISION	DATE	BY

DECATUR AIRPORT
DECATUR, ILLINOIS

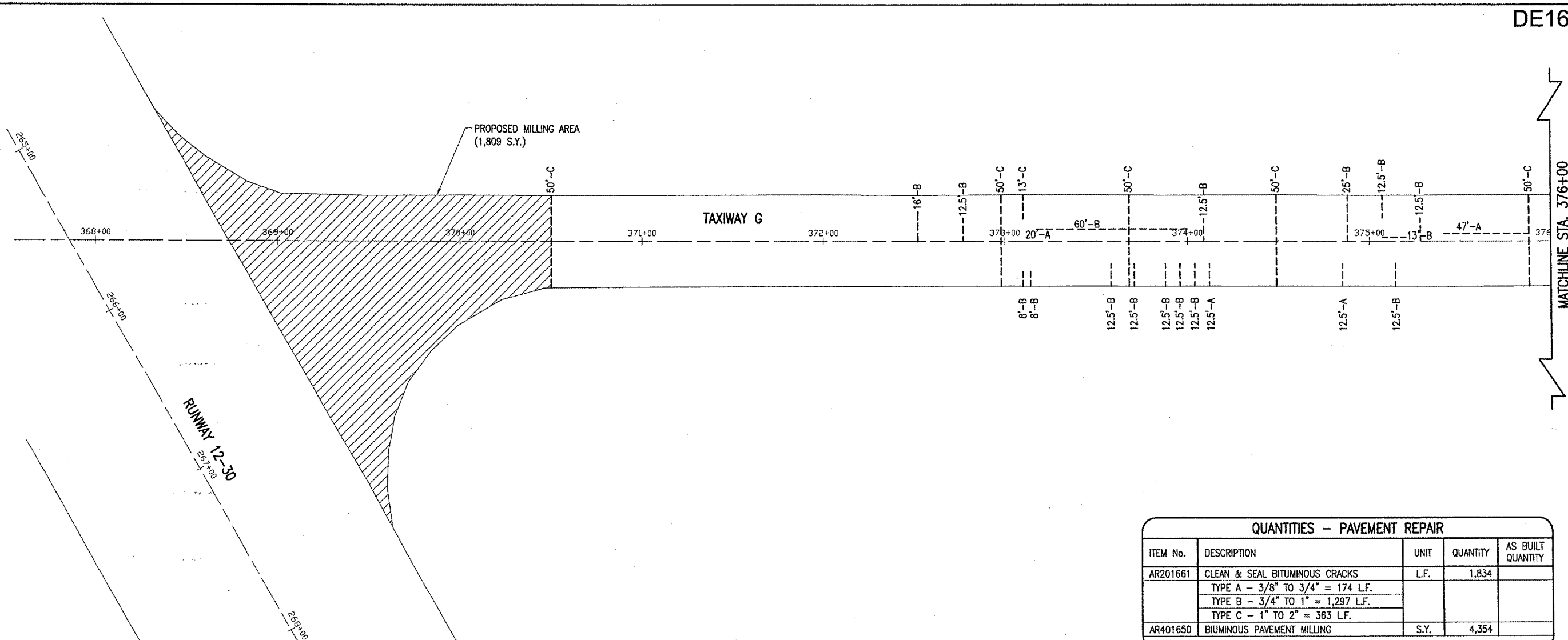
ILL. PROJ.: DEC-3414 A.I.P. PROJ.: J-17-0033-28

REI Project No. 802-041XYD	CCC	03/18/05
Drawn R-702PNP.DWG	CCC	03/18/05
Scale 1"=50'H, 1"=2'V	WJM	06/07/05
Date 03/18/05		
LAYOUT		
DRAWN		
REVIEWED		

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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED PLAN & PROFILE
 TAXIWAY "G4"
 STA. 0+00 TO STA. 4+00

16
 16 of 110 sheets



MATCHLINE STA. 376+00

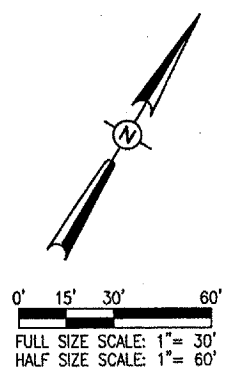
QUANTITIES - PAVEMENT REPAIR				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	1,834	
	TYPE A - 3/8" TO 3/4" = 174 L.F.			
	TYPE B - 3/4" TO 1" = 1,297 L.F.			
	TYPE C - 1" TO 2" = 363 L.F.			
AR401650	BIUMINOUS PAVEMENT MILLING	S.Y.	4,354	

CRACK IDENTIFICATION

- TYPE A - 3/8" TO 3/4"
- TYPE B - 3/4" TO 1"
- TYPE C - 1" TO 2"

LEGEND

- EXISTING PAVEMENT
- PROPOSED BITUMINOUS MILLING (VAR. DEPTH)
- PROPOSED CRACK REPAIR LOCATIONS



BITUMINOUS PAVEMENT MILLING (AR401650)

THE DESIGNATED MILLING AREAS ON THIS SHEET AND THE FOLLOWING SHEET WILL BE CUT OR TRIMMED AS SHOWN ON THE CROSS-SECTIONS. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE SPECIAL PROVISIONS FOR BITUMINOUS SURFACE COURSE, 401-4.14 SURFACE TEST OF STANDARD SPECIFICATIONS.

THE EXISTING PAVEMENT WILL BE SAWS IF A VERTICAL FACE IS NOT ACHIEVED BY THE MILLING OPERATIONS. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

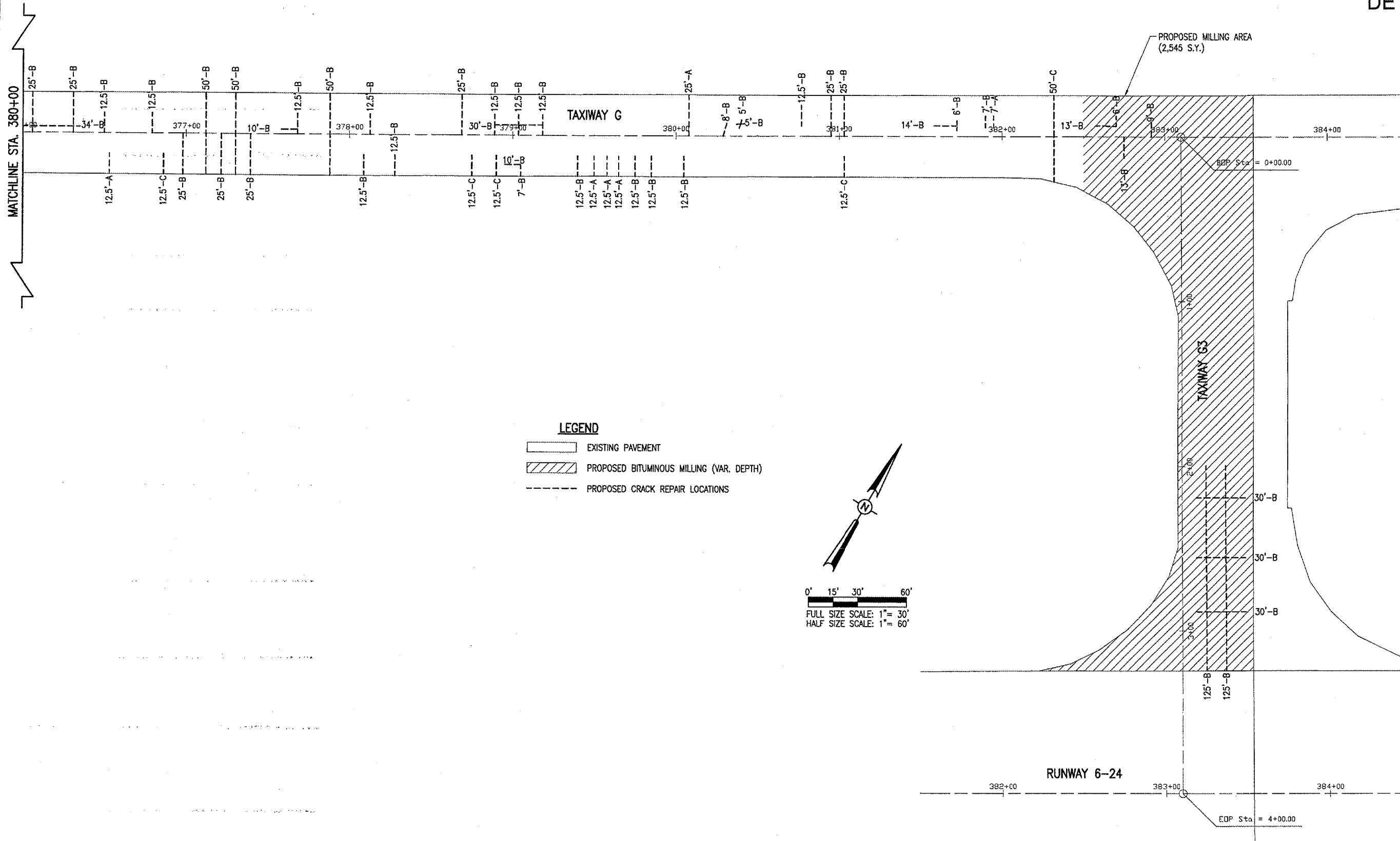
CLEAN & SEAL BITUMINOUS CRACKS (AR201661)

THE CRACKS SHOWN FOR CLEANING AND SEALING WERE DETERMINED FROM A CRACK SURVEY COMPLETED IN APRIL 2005. THE CRACKS SHOWN ARE CURRENTLY FILLED WITH A CRAF-CO JOINT SEALER. THE RESIDENT ENGINEER WILL REVIEW THE COMPLETED SEAL CRACKS TO DETERMINE IF THE SEALING IS SATISFACTORY OR THE CRACK SHOULD BE CLEANED AND RESEALED.

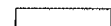
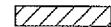
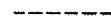
THE CLEANING AND SEALING OF BITUMINOUS CRACKS WILL BE COMPLETED IN ACCORDANCE TO SPECIFICATION ITEM: AR201661 "CLEAN & SEAL BITUMINOUS CRACKS."

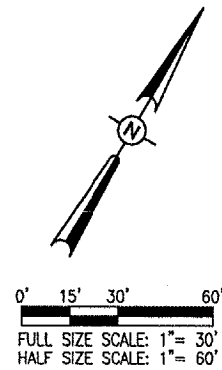
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DATE	REVISION	BY	
HANSON Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62705-2886 Offices Nationwide			
LAYOUT DRAWN REVIEWED		CCC CCC WJM	03/05 03/05 06/07/05
TAXIWAY G WIDENING PHASE II PROPOSED PAVEMENT PREPARATION PLAN STA. 367+85.60 TO STA. 376+00			
17			
IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28 17 of 110 sheets			



LEGEND

-  EXISTING PAVEMENT
-  PROPOSED BITUMINOUS MILLING (VAR. DEPTH)
-  PROPOSED CRACK REPAIR LOCATIONS



DATE	REVISION	BY

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 A.I.P. PROJ.: DEC-3414
 I.L. PROJ.: 3-17-0033-28

ILL. Project No. 802-04TXD Estimate R-111PRP.DWG Scale 1"=30' Date 031/05	LAYOUT DRAWN REVIEWED	CCC CCC WJM	031/05 031/05 06/07/05
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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED PAVEMENT
 PREPARATION PLAN
 STA. 376+00 TO STA. 384+53

INSPECTION HOLES AND UNDERDRAIN REMOVAL

EXISTING UNDERDRAINS THAT ARE LOCATED IN THE PROPOSED PAVEMENT SECTION OR THE 16" SOIL PROCESSING AREA WILL BE REMOVED.

EXISTING UNDERDRAINS THAT ARE LOCATED BENEATH THE 16" SOIL PROCESS AREA WILL BE EXCAVATED.

HOLES THAT REMAIN FROM REMOVING UNDERDRAIN PIPE AT MANHOLES OR PIPE SECTIONS WILL BE REPAIRED. THE REPAIR WILL RESTORE REINFORCING AND FULL WALL THICKNESS WITH AR610 STRUCTURAL CONCRETE.

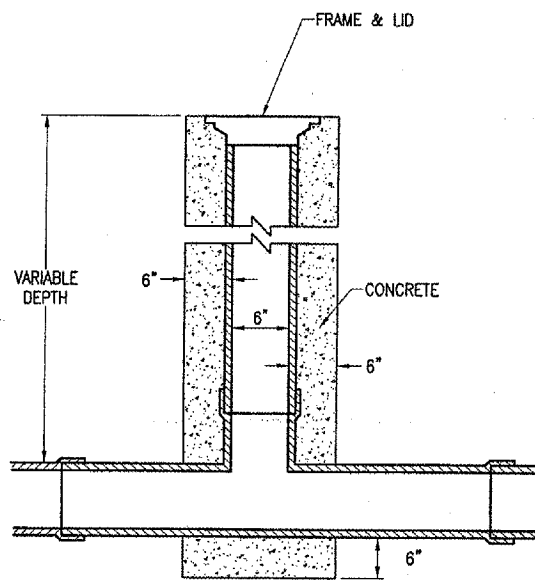
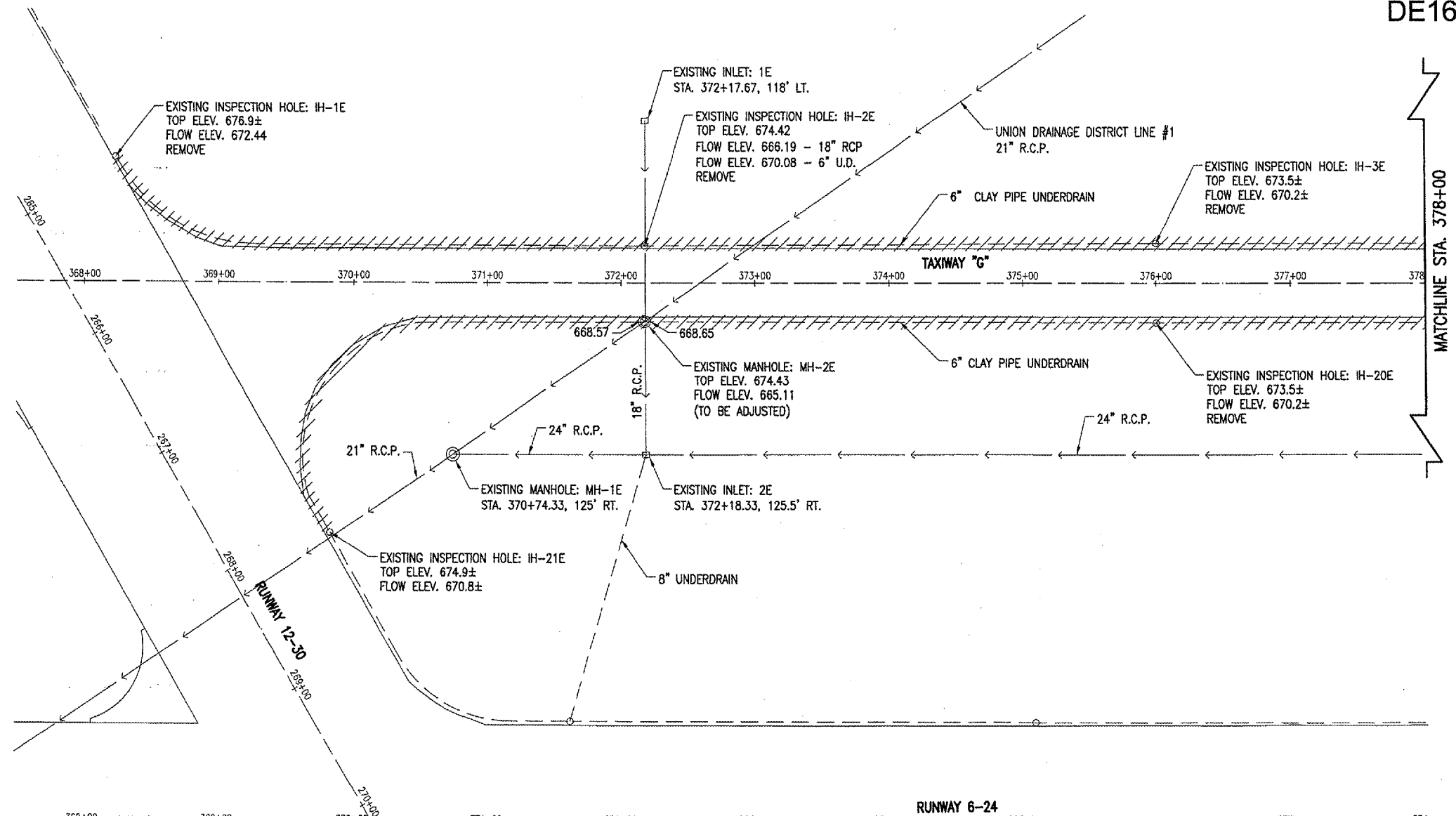
ALONG THE LINE OF UNDERDRAIN REMOVAL, INSPECTION HOLES WILL BE REMOVED TO A DEPTH BENEATH THE SOIL PROCESS ELEVATION.

WHERE THE UNDERDRAIN PIPE IS EXCAVATED, P.E. TUBING OR BROKEN CLAY PIPE WILL NOT BE PERMITTED IN THE FILL USED FOR EMBANKMENTS.

WHERE THE UNDERDRAIN PIPE IS EXCAVATED BELOW THE SOIL PROCESSING DEPTH, THE EXCAVATION WILL BE BACKFILLED WITH A GRANULAR MATERIAL COMPLYING TO AR209 SPECIFICATIONS. LOOSE EARTH MATERIAL WILL BE REMOVED FROM THE EXCAVATION PRIOR TO PLACING THE AR209 GRANULAR MATERIAL.

THE REMOVED INSPECTION HOLES AND PIPE MATERIAL WILL BE DISPOSED OFF AIRPORT SITE BY CONTRACTOR.

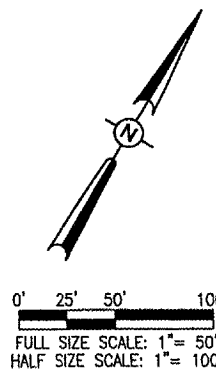
PAYMENT FOR THE UNDERDRAIN EXCAVATION WILL BE MADE UNDER ITEM: AR705900 "REMOVE UNDERDRAIN" PER LINEAR FOOT. THE PRICE WILL INCLUDE REMOVAL OF INSPECTION HOLES, EXCAVATING AND REMOVING PIPE, BACKFILLING, COMPACTING AND MAKING REPAIRS TO MANHOLES OR PIPE STRUCTURES WHERE UNDERDRAIN PIPE IS REMOVED.



EXISTING INSPECTION HOLE-TYPE A
NOT TO SCALE

LEGEND

- ▭ EXISTING PAVEMENT
- EXISTING STORM SEWER
- - - EXISTING UNDERDRAIN
- /// EXISTING UNDERDRAIN EXCAVATED
- EXISTING INLET
- ⊙ EXISTING MANHOLE
- EXISTING INSPECTION HOLE
- { EXISTING HEADWALL



DE165

MATCHLINE STA. 378+00

JUN 10, 2005 9:22 AM CCC
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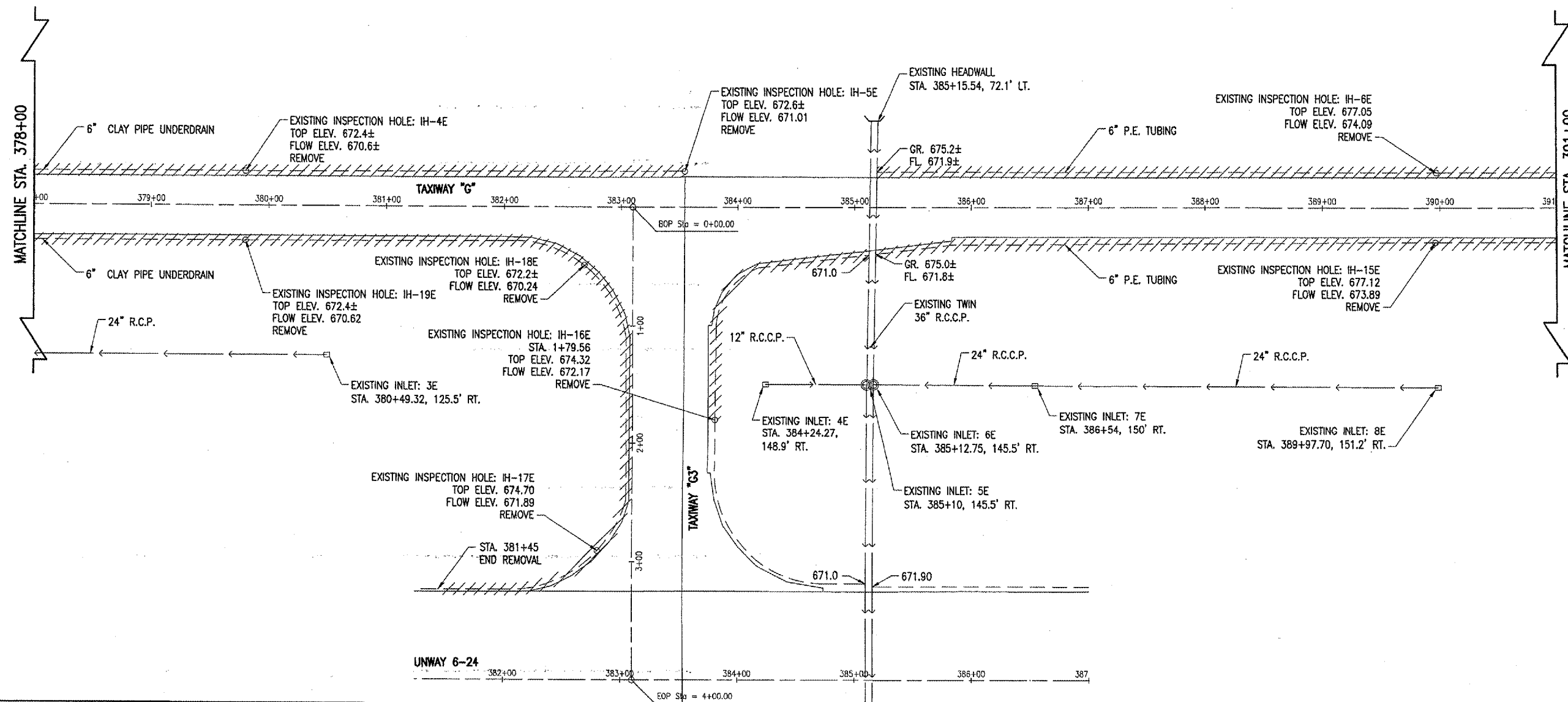
DATE	REVISION	BY

DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 I.L. PROJ.: DEC-3414

Proj. No. 802-04TXD Drawn: R-131DRN.DWG Scale: 1" = 50' Date: 03/18/05		
LAYOUT	CCC	03/18/05
DRAWN	CCC	03/18/05
REVIEWED	WJM	06/07/05

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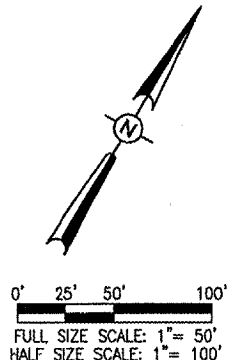
TAXIWAY G WIDENING
 PHASE II
 EXISTING UNDERDRAIN
 REMOVAL PLAN
 STA. 367+86.06 TO STA. 378+00



AR705900 "REMOVE UNDERDRAIN"

STRUCTURES	LOCATION	DISTANCE (L.F.)
IH-1E TO IH-2E	STA. 368+22.5 LT. TO STA. 372+17.5 LT.	419.0
IH-2E TO 21" R.C.P.	STA. 372+17.5 LT. TO STA. 373+00 LT.	82.5
IH-21E TO MH-2E	STA. 369+82.8 RT. TO STA. 372+17.5 RT.	372.4
MH-2E TO IH-20E	STA. 372+17.5 RT. TO STA. 376+00 RT.	382.5
36" R.C.C.P. TO IH-6E	STA. 385+19 LT. TO STA. 389+96.5 LT.	477.5
IH-6E TO IH-7E	STA. 389+96.5 LT. TO STA. 394+92.5 LT.	496.0
24" R.C.C.P. TO IH-8E	STA. 395+00 LT. TO STA. 398+97.5 LT.	397.5
IH-8E TO IH-9E	STA. 398+97.5 LT. TO STA. 403+61.5 LT.	464.0
IH-9E TO "G4" 2+50	STA. 403+61.5 LT. TO "G4" STA. 2+50 LT.	275.4
IH-16E TO 36" R.C.C.P.	"G3" STA. 1+79.5 LT. TO STA. 385+17.5 RT.	239.5
36" R.C.C.P. TO IH-15E	STA. 385+21 RT. TO STA. 389+96.5 RT.	475.5
IH-15E TO IH-14E	STA. 389+96.5 RT. TO STA. 394+92.5 RT.	496.0
24" R.C.C.P. TO IH-13E	STA. 395+00 RT. TO STA. 398+94.5 RT.	394.5
IH-13E TO IH-12E	STA. 398+94.5 RT. TO STA. 402+50 RT.	357.0
IH-12E TO IH-11E	STA. 402+50 RT. TO "G4" STA. 3+15.5 RT.	327.0
IH-11E TO 401+42	"G4" STA. 13+15.5 RT. TO STA. 401+42 RT.	55.3
21" R.C.P. TO IH-3E	STA. 373+00 LT. TO STA. 376+00 LT.	300.0
IH-3E TO IH-4E	STA. 376+00 LT. TO STA. 379+80 LT.	380.0
IH-4E TO IH-5E	STA. 379+80 LT. TO STA. 383+54.7 LT.	374.7
IH-20E TO IH-19E	STA. 376+00 RT. TO STA. 379+80 RT.	380.0
IH-19E TO IH-18E	STA. 379+80 RT. TO STA. 382+68.75 RT.	293.4
IH-18E TO IH-17E	STA. 382+68.75 RT. TO "G3" STA. 2+91 RT.	260.7
IH-17E TO 381+45	"G3" STA. 2+91 RT. TO "G" STA. 381+45 RT.	144.6
TOTAL		7,845.0

- LEGEND**
- EXISTING PAVEMENT
 - EXISTING STORM SEWER
 - EXISTING UNDERDRAIN
 - EXISTING UNDERDRAIN EXCAVATED
 - EXISTING INLET
 - EXISTING MANHOLE
 - EXISTING INSPECTION HOLE
 - EXISTING HEADWALL



DATE	REVISION	BY

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<small> I.E.I. Project No. 802-04EXD Filename: R-131DRN.DWG Scale: 1" = 50' Date: 03/18/05 </small>	<small> CCC 03/18/05 CCC 03/18/05 WJM 06/07/05 </small>
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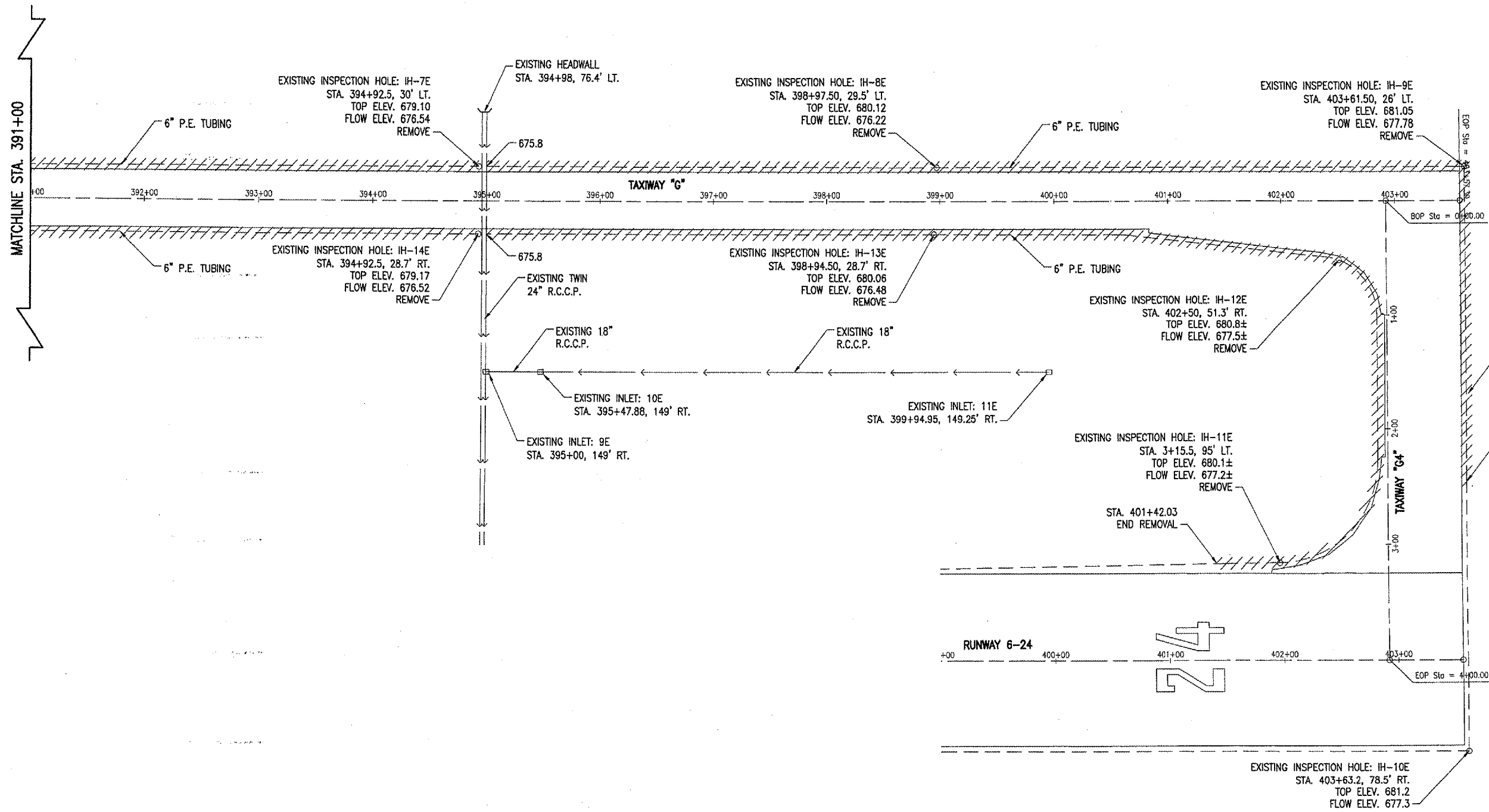
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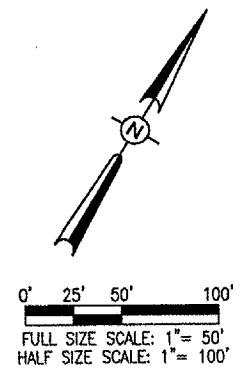
TAXIWAY G WIDENING
PHASE II
 EXISTING UNDERDRAIN
 REMOVAL PLAN
 STA. 378+00 TO STA. 391+00

JUN 10, 2005 9:23 AM CCC
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JUN 10, 2005 9:23 AM CCC
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- LEGEND**
- EXISTING PAVEMENT
 - EXISTING STORM SEWER
 - EXISTING UNDERDRAIN
 - EXISTING UNDERDRAIN EXCAVATED
 - EXISTING INLET
 - EXISTING MANHOLE
 - EXISTING INSPECTION HOLE
 - EXISTING HEADWALL



DATE	REVISION	BY

DECATUR, ILLINOIS

IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

REV. PROJECT No.	802-041XYD
Filename	R-131DRN.DWG
Scale	1"=50'
Date	03/18/05
LAYOUT	CCC 03/18/05
DRAWN	CCC 03/18/05
REVIEWED	WJM 06/07/05

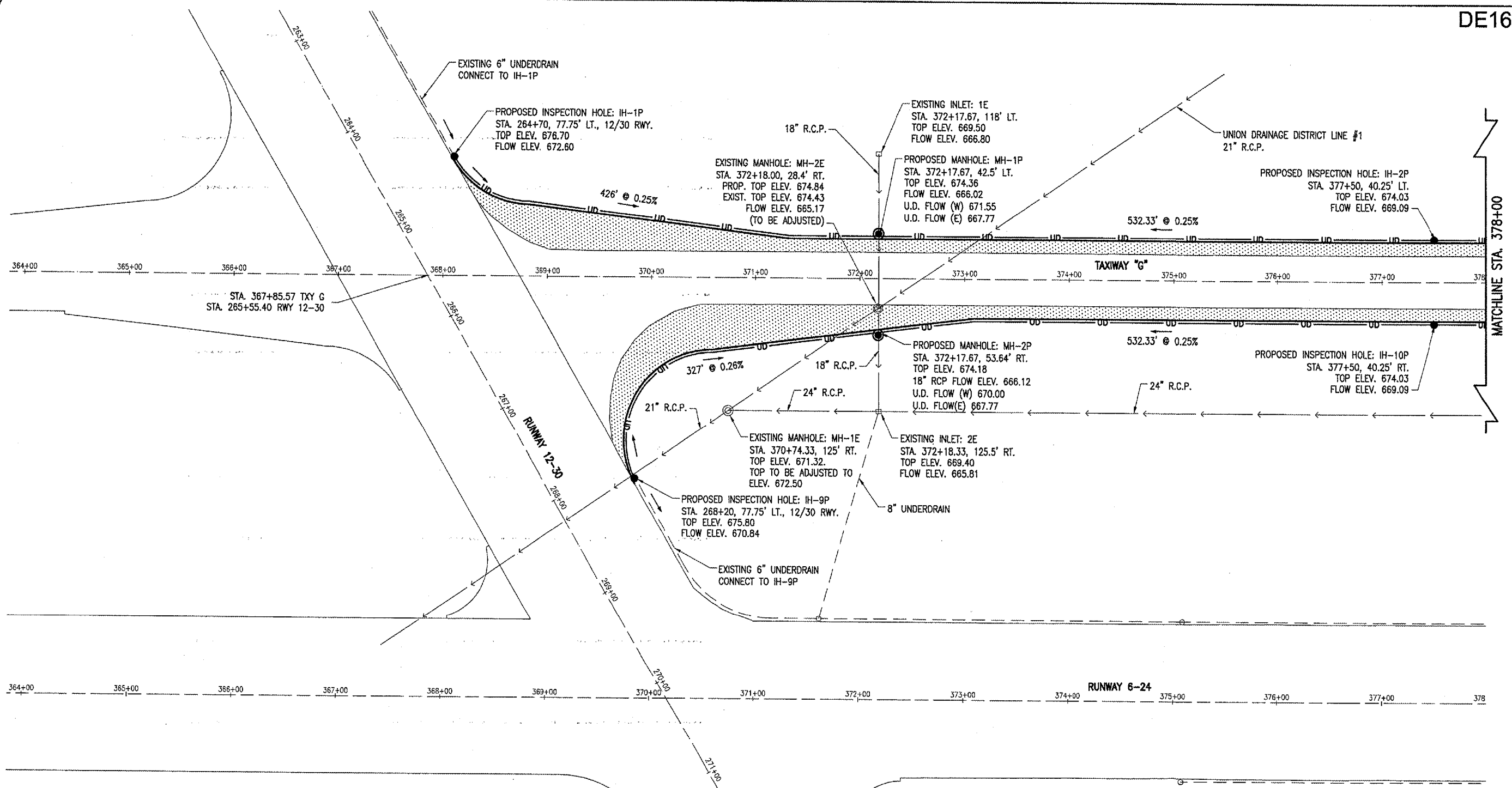
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**TAXIWAY G WIDENING
PHASE II**

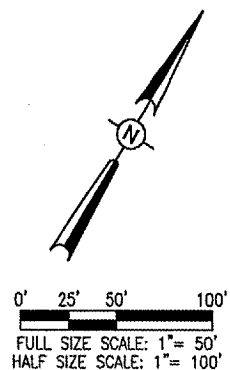
EXISTING UNDERDRAIN
REMOVAL PLAN

STA. 391+00 TO STA. 403+57.30



QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR701524	24" RCP, CLASS IV	L.F.	38	
AR701536	36" RCP, CLASS IV	L.F.	46	
AR705411	POROUS BACKFILL No. 1	C.Y.	1,327	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	7,628	
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	19	
AR751540	MANHOLE 4'	EACH	2	
AR751943	ADJUST MANHOLE	EACH	2	
AR752524	CONCRETE HEADWALL 24"	EACH	1	
AR752636	CONCRETE HEADWALL 36"	EACH	1	
AR752903	REMOVE HEADWALL	EACH	2	

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED BITUMINOUS PAVEMENT
 - EXISTING STORM SEWER
 - PROPOSED UNDERDRAIN
 - EXISTING UNDERDRAIN
 - PROPOSED INSPECTION HOLE
 - EXISTING INSPECTION HOLE
 - EXISTING INLET
 - PROPOSED MANHOLE
 - EXISTING MANHOLE



REVISION	DATE

DECATUR, ILLINOIS

 IL PROJ.: DEC-3414

HEI Project No. 802-04TXD	03/18/05
Client R-132DRN.DWG	03/18/05
Scale 1"=50'	03/18/05
Date 03/18/05	06/07/05
LAYOUT CCC	WJM
DRAWN CCC	
REVIEWED	

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TAXIWAY G WIDENING

PHASE II

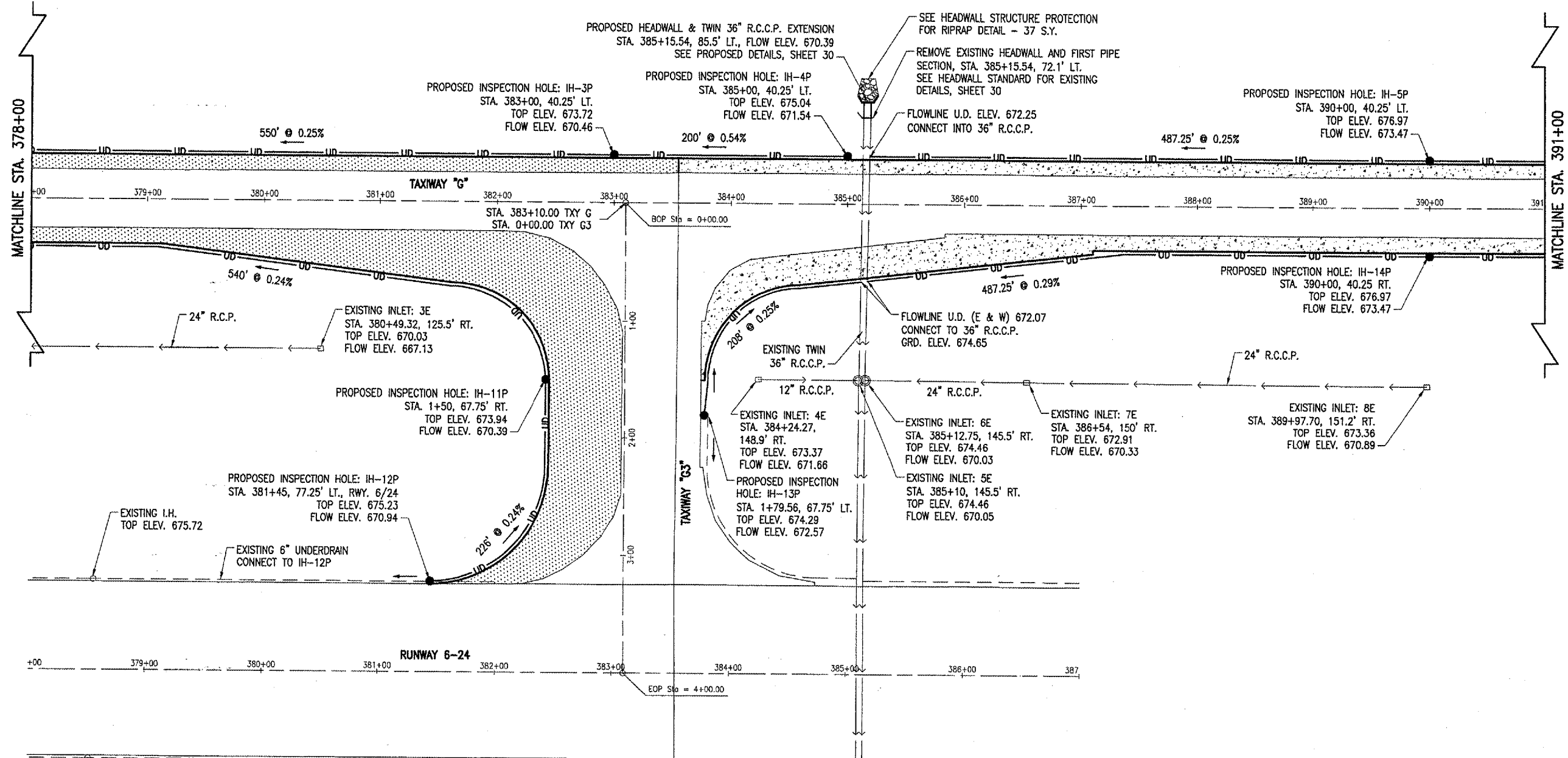
 PROPOSED

 DRAINAGE PLAN

 STA. 367+86.06 TO STA. 378+00

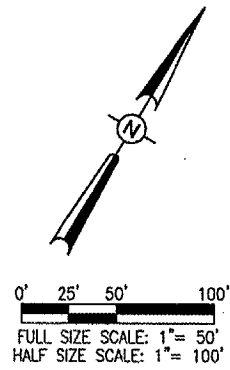
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NOTE
 WHERE THE UNDERDRAIN PIPE IS TO BE INSERTED INTO THE EXISTING R.C.C.P., A CORE DRILL SHALL BE USED TO CUT THE HOLE IN THE R.C.C.P. A DRILL SIZE OF 1" GREATER OF THE OUTSIDE DIAMETER OF THE UNDERDRAIN WILL BE USED. THE UNDERDRAIN PIPE WILL BE INSERTED TO THE INSIDE WALL THICKNESS OF THE R.C.C.P. A GROUT PATCHING MATERIAL WILL BE PLACED AROUND THE INSERTED PIPE FOR THE FULL R.C.C.P. WALL THICKNESS. GOOD CONSTRUCTION PRACTICES WILL BE FOLLOWED TO ASSURE A LEAK PROOF PATCH.

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED BITUMINOUS PAVEMENT
 - PROPOSED CONCRETE PAVEMENT
 - EXISTING STORM SEWER
 - PROPOSED UNDERDRAIN
 - EXISTING UNDERDRAIN
 - PROPOSED PIPE EXTENSION W/ HEADWALL
 - EXISTING HEADWALL
 - PROPOSED INSPECTION HOLE
 - EXISTING INSPECTION HOLE
 - EXISTING INLET
 - EXISTING MANHOLE/INLET



DATE	REVISION	BY

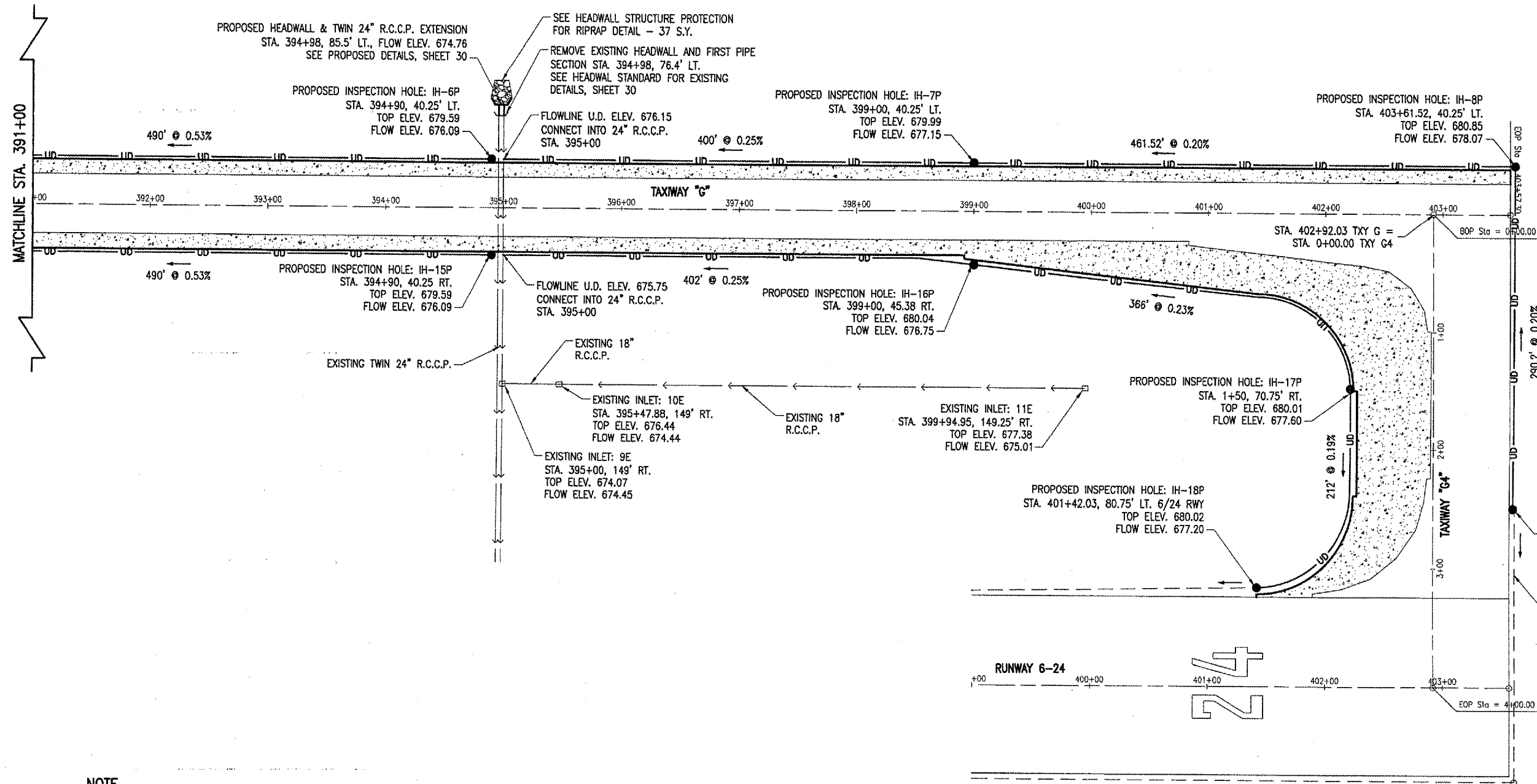
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 DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEC Project No. 802-041XXD	
Drawings R-132DRN.DWG	
Scale 1"=50'	
Date 03/18/05	
LAYOUT CCC	03/18/05
DRAWN CCC	03/18/05
REVIEWED WJM	05/07/05

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED
 DRAINAGE PLAN
 STA. 378+00 TO STA. 391+00

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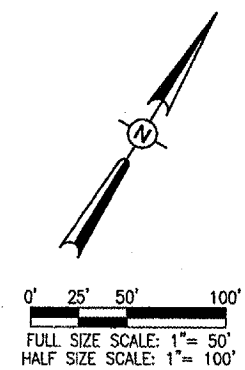


NOTE

WHERE THE UNDERDRAIN PIPE IS TO BE INSERTED INTO THE EXISTING R.C.C.P., A CORE DRILL SHALL BE USED TO CUT THE HOLE IN THE R.C.C.P. A DRILL SIZE OF 1" GREATER OF THE OUTSIDE DIAMETER OF THE UNDERDRAIN WILL BE USED. THE UNDERDRAIN PIPE WILL BE INSERTED TO THE INSIDE WALL THICKNESS OF THE R.C.C.P. A GROUT PATCHING MATERIAL WILL BE PLACED AROUND THE INSERTED PIPE FOR THE FULL R.C.C.P. WALL THICKNESS. GOOD CONSTRUCTION PRACTICES WILL BE FOLLOWED TO ASSURE A LEAK PROOF PATCH.

LEGEND

- EXISTING PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- PROPOSED UNDERDRAIN
- PROPOSED PIPE EXTENSION W/ HEADWALL
- EXISTING HEADWALL
- PROPOSED INSPECTION HOLE
- EXISTING INSPECTION HOLE
- EXISTING INLET



JUN 10, 2005 9:25 AM CCC
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DATE	REVISION

DECATUR, ILLINOIS

 I.L. PROJ.: DEC-3414

FILE	DATE	BY	REVIEWED
LAYOUT	03/18/05	CCC	CCC
DRAWN	03/18/05	CCC	WJM
REVIEWED	06/07/05	WJM	

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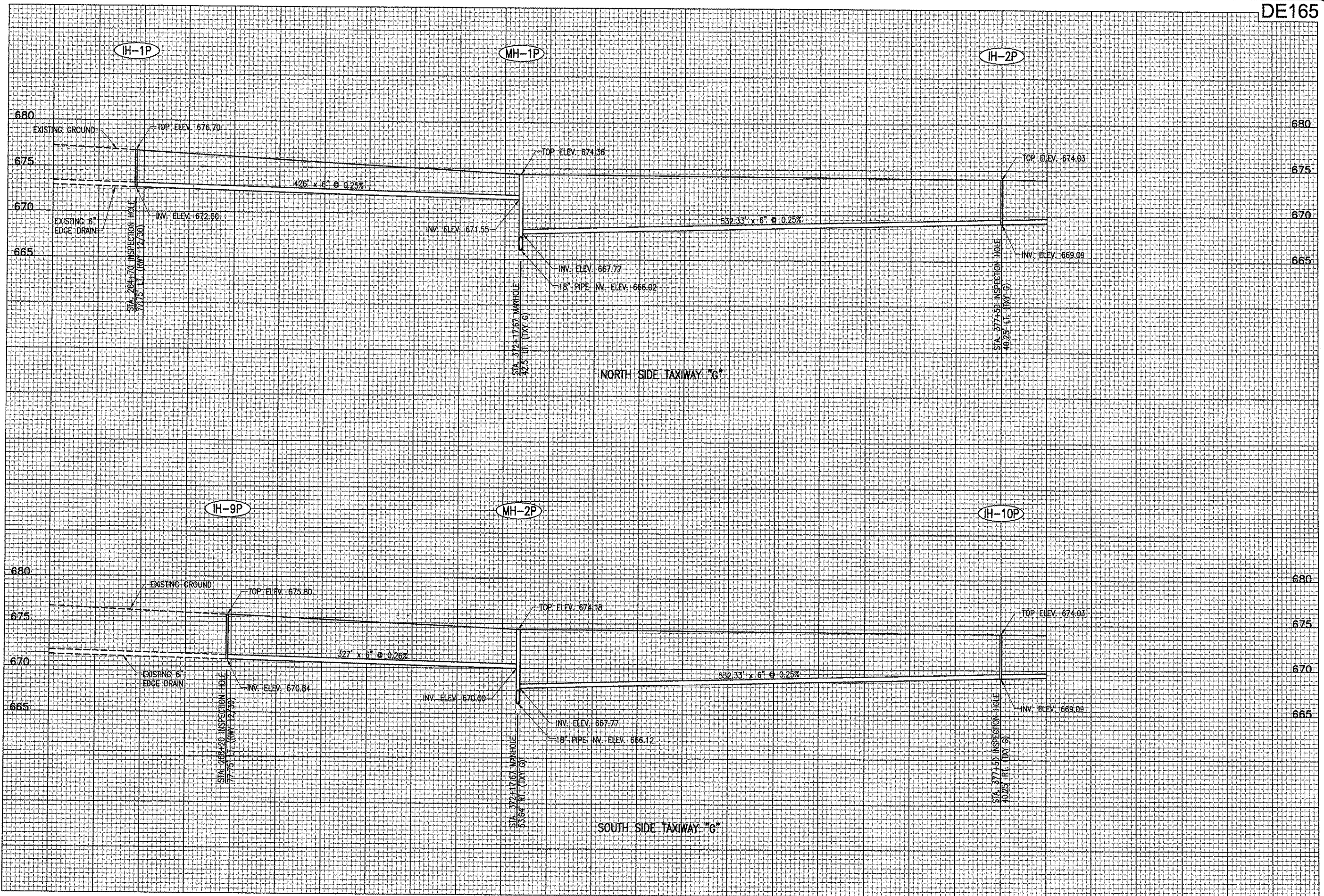
TAXIWAY G WIDENING

PHASE II

 PROPOSED

 DRAINAGE PLAN

 STA. 391+00 TO STA. 403+57.30



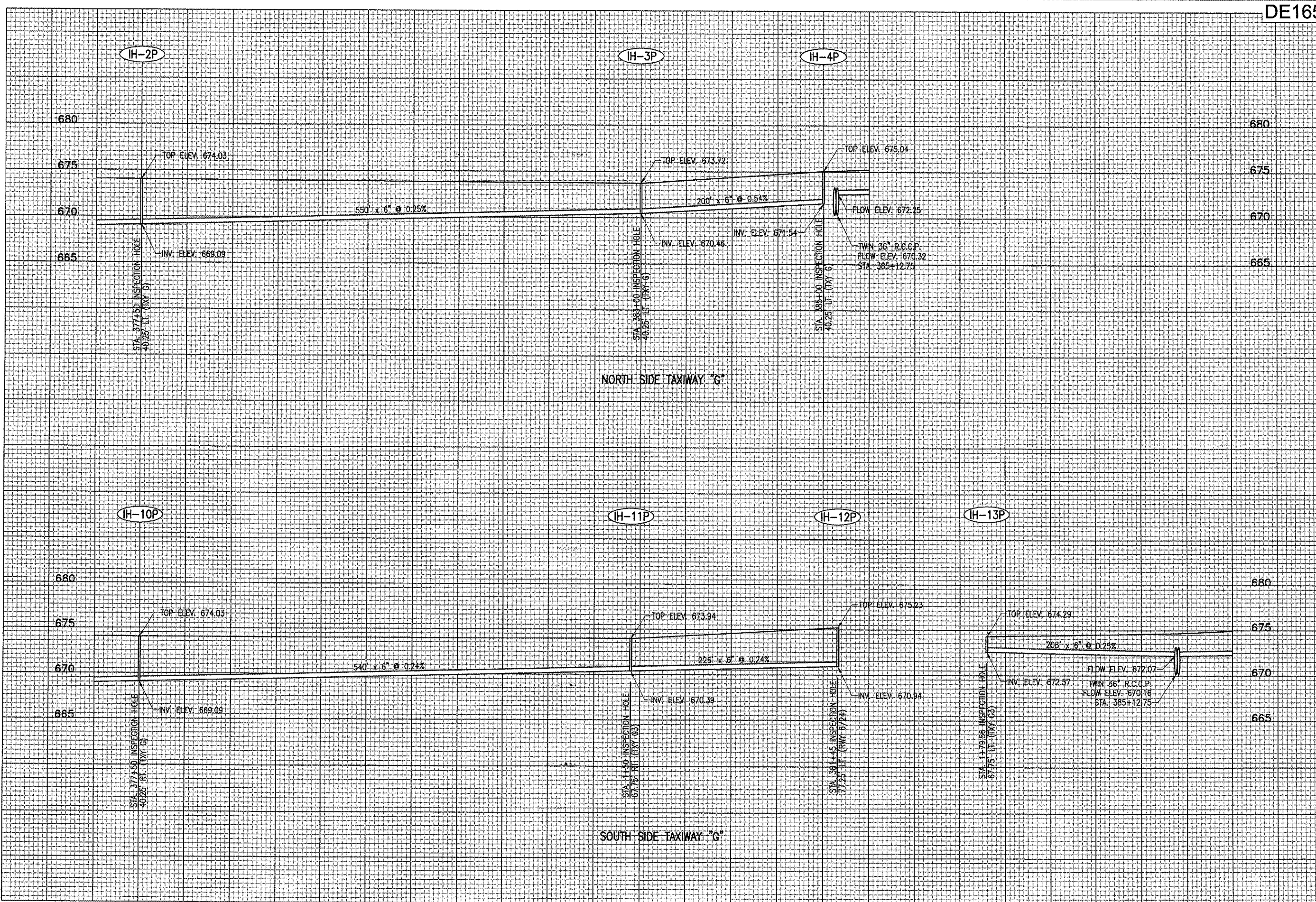
DATE	REVISION	BY



HEI Project No. 802-04TXD	CCC	03/18/05
Filename: R-703UND.DWG	CCC	03/18/05
Scale: 1"=50'H, 1"=5'V	WJM	06/07/05
Date: 03/18/05		



TAXIWAY G WIDENING
 PHASE II
 PROPOSED EDGE
 DRAIN PROFILES



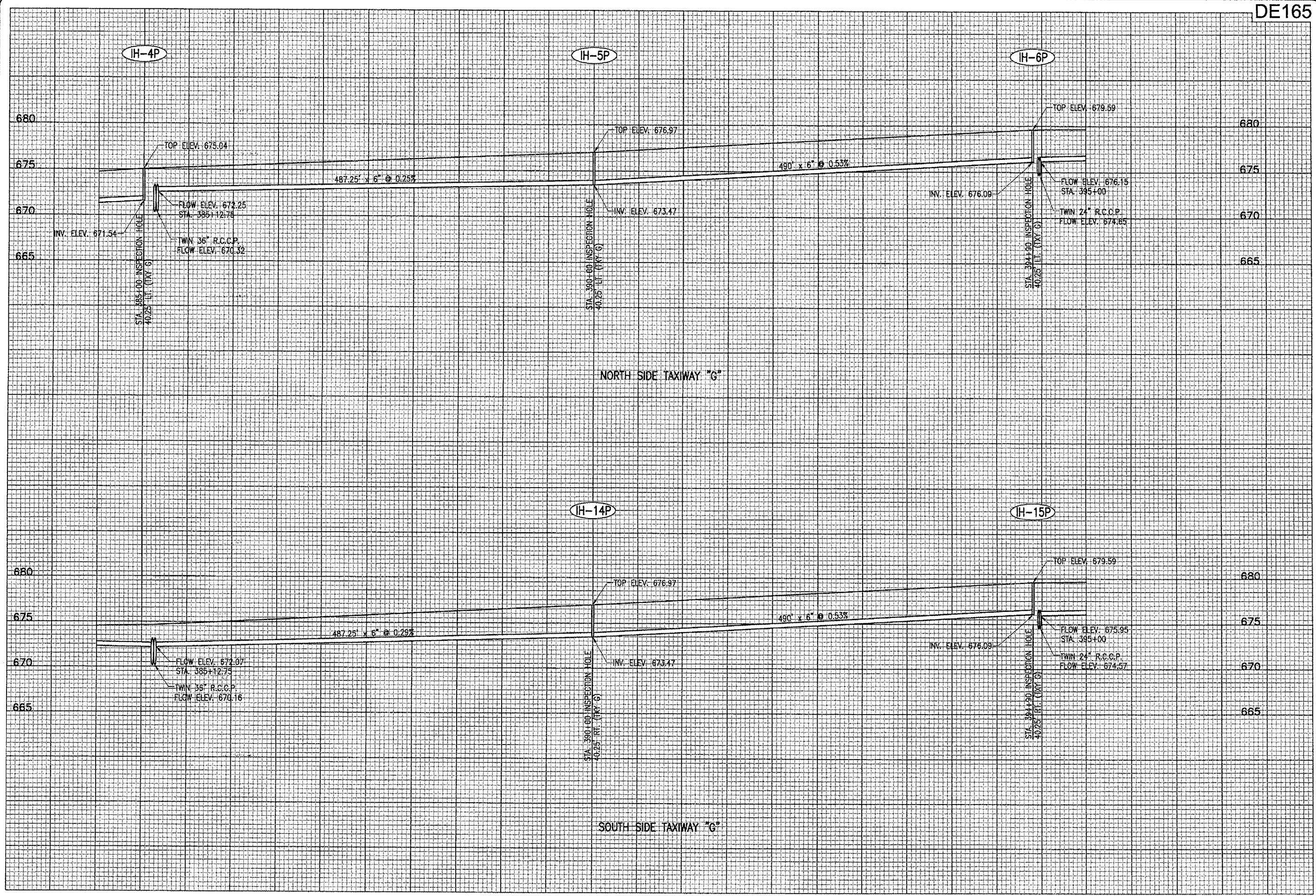
DATE	REVISION	BY

DECATUR AIRPORT
 DECATUR, ILLINOIS
 IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

IET Project No. 802-04TXD Filename: R-703UND.DWG Scale: 1"=50'H 1"=5'V Date: 03/18/05	03/18/05 03/18/05 06/07/05
LAYOUT	CCC
DRAWN	CCC
REVIEWED	WJM

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 PHASE II
 PROPOSED EDGE
 DRAIN PROFILES



REVISION	DATE	BY



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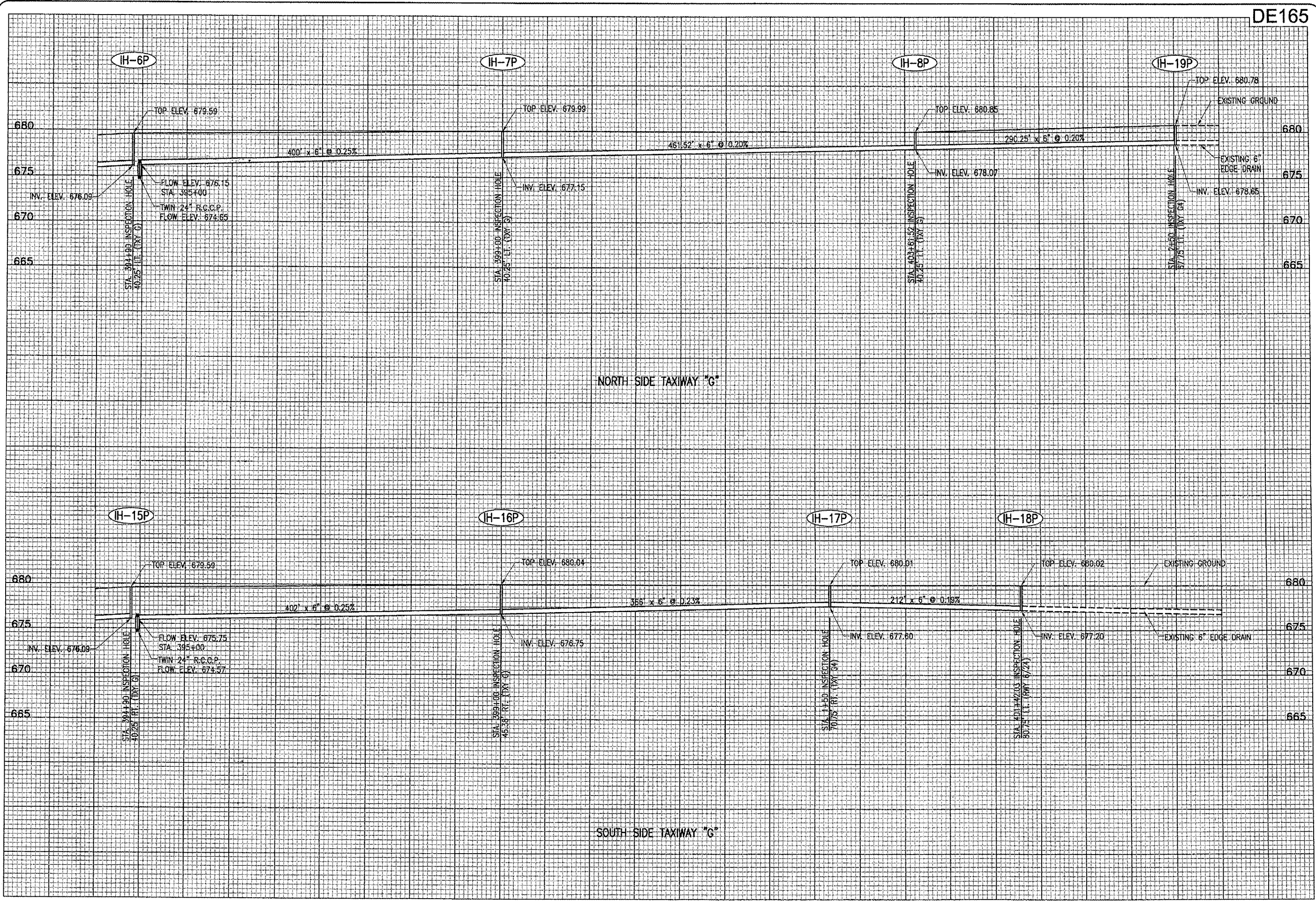
IL PROJ.: DEC-3414 A.L.P. PROJ.: 3-17-0033-28

HEL Project No. 802-04TXD Filename R-703UND.DWG Scale 1"=50'H, 1"=5'V Date 03/18/05	LAYOUT CCC 03/18/05 DRAWN CCC 03/18/05 REVIEWED WJM 06/07/05
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TAXIWAY G WIDENING
 PHASE II
 PROPOSED EDGE
 DRAIN PROFILES



DE165

DATE	REVISION	BY



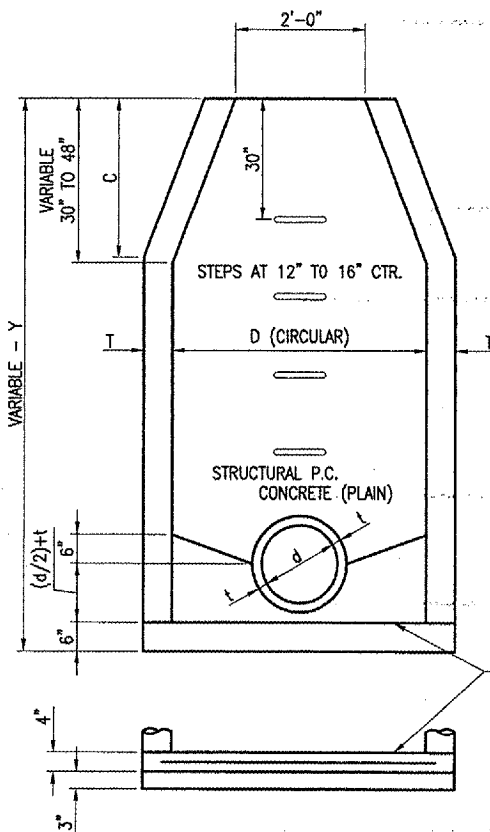
DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 I.L. PROJ.: DEC-3414

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LAYOUT	CCC	03/18/05	
DRAWN	CCC	03/18/05	
REVIEWED	WJM	06/07/05	

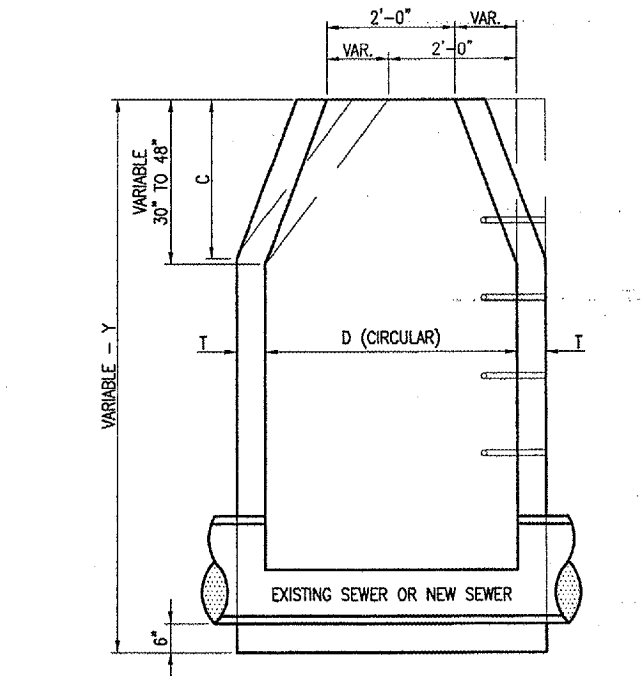


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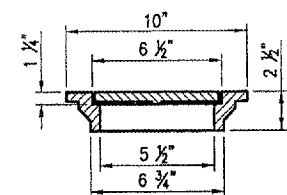
TAXIWAY G WIDENING
 PHASE II
 PROPOSED EDGE
 DRAIN PROFILES



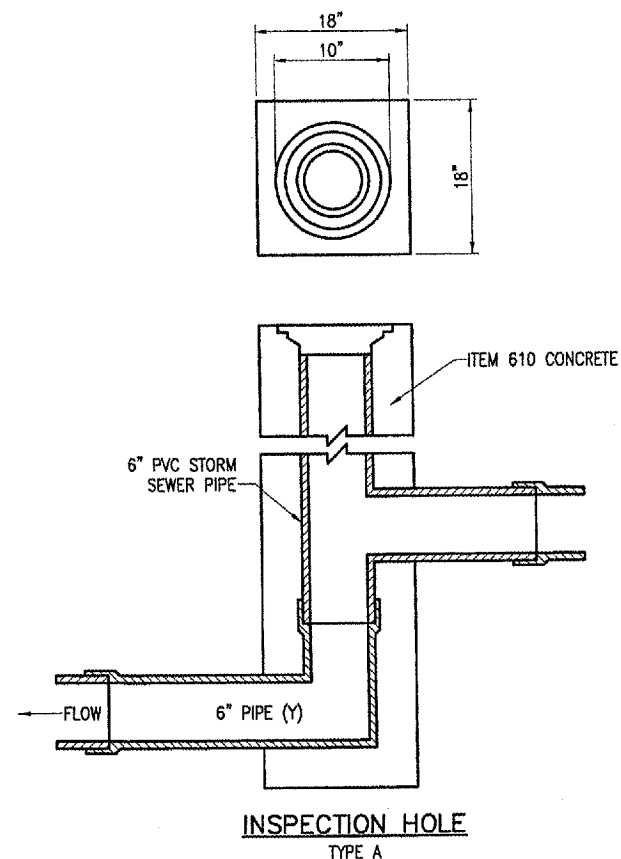
MANHOLE DETAILS
"NOT TO SCALE"



ALT. MATERIAL FOR WALLS	D	C	T	D	C	T
PRECAST REINFORCED CONC. RINGS	4'	2'-6"	5"	5'	4'-0"	6"
MONOLITHIC CONCRETE	4'	2'-6"	6"	5'	4'-0"	8"



CAST IRON FRAME AND COVER
NEENAH R-6013, DEETER 1810,
EAST JORDAN 2790-6 OR
APPROVED EQUAL



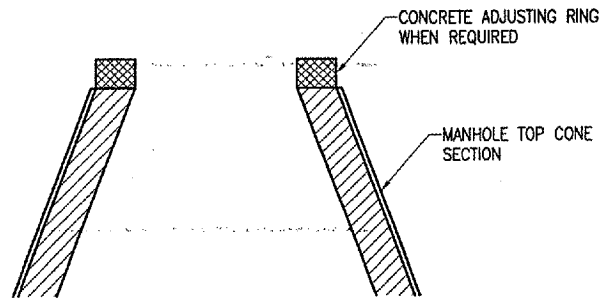
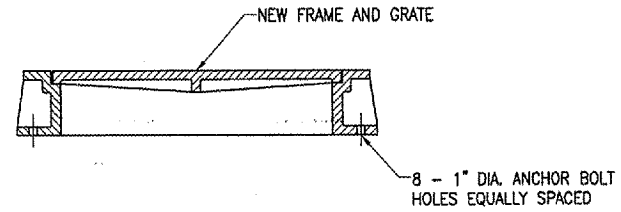
INSPECTION HOLE
TYPE A

INSPECTION HOLE NOTES

- DIAMETER OF PIPE AS SPECIFIED.
- 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF INSPECTION HOLES.
- THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON-REINFORCED).
- THE INSPECTION FRAME AND LID SHALL BE EAST JORDAN 2790-6, NEENAH R-6013 OR DEETER 1810.
- THE PROPOSED INSPECTION HOLE WILL BE PAID FOR UNDER THE FOLLOWING:
ITEM AR705630 "UNDERDRAIN INSPECTION HOLE"

MANHOLE NOTES

- MANHOLE TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE PER MANHOLE SHALL INCLUDE THE FRAME, LID AND STEPS PER UNIT.
- THE FRAME AND LID SHALL BE NEENAH R-3492 OR EQUAL, ANCHORED TO THE MANHOLE STRUCTURE.
- MANHOLE STEPS SHALL BE NEENAH R-1980-1. 12" TO 15" C.C. STEPS TO BE INSTALLED WHEN Y IS GREATER THAN 5'. THE COST OF THE STEPS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EACH MANHOLE.
- THE PROPOSED MANHOLES WILL BE PAID FOR UNDER THE FOLLOWING:
ITEM AR751540 "MANHOLE 4"



ADJUST MANHOLE DETAIL
"NOT TO SCALE"

ADJUSTED MANHOLE NOTES

- THE FRAME AND GRATE WILL BE REMOVED AND TURNED OVER TO THE AIRPORT DIRECTOR FOR SALVAGE. HE CAN ELECT FOR THE CONTRACTOR TO DISPOSE OF THE MATERIAL.
- ANY PREVIOUS MATERIALS, BRICK, MORTAR, OR CONCRETE ADDED TO THE TOP MANHOLE SECTION WILL BE REMOVED. THE TOP OF THE MANHOLE STRUCTURE IS TO BE INSPECTED FOR PREVIOUS DAMAGE. A SUFFICIENT AMOUNT OF CONCRETE WILL BE SAWED OFF THE TOP OF THE SECTION TO PROVIDE A TOP SURFACE WITH SOUND UNDAMAGED CONCRETE.
- A NEENAH FOUNDRY COMPANY R-3492 SERIES AIRPORT MANHOLE FRAME AND LID, CATALOG R-3492 OR EQUAL SHALL BE PLACED ON THE MANHOLE STRUCTURE. THE FRAME WILL BE ANCHORED TO THE MANHOLE CONCRETE SECTION WITH EIGHT 1" DIA. ANCHOR BOLTS. THE LID WILL BE BOLTED TO THE MANHOLE FRAME.
- A CONCRETE MANHOLE ADJUSTING RING CAN BE USED TO OBTAIN THE PROPER ELEVATION FOR THE TOP OF THE LID ELEVATION.
- THE CONCRETE ADJUSTING RING SHALL CONFORM TO THE REQUIREMENTS OF ASTM C478. THE RING DIAMETER WILL MATCH THE TOP OF THE CONE SECTION OF THE MANHOLE, AND BE OF EQUAL WALL THICKNESS TO THE STRUCTURE. THE RING WILL BE BOLTED TO THE CONE SECTION USING 1" DIA. ANCHOR BOLTS.
- MORTAR LEVELING BETWEEN CONE AND RING WILL BE ALLOWED. THE MORTAR SHALL CONSIST OF ONE PART PORTLAND CEMENT AND TWO PARTS SAND. THE PORTLAND CEMENT SHALL CONFORM TO THE REQUIREMENTS OF ASTM C150, TYPE 1. THE SAND SHALL CONFORM TO THE REQUIREMENTS OF ASTM C144.
- THE PROPOSED ADJUSTED MANHOLE WILL BE PAID FOR UNDER THE FOLLOWING:
ITEM AR751943 "ADJUST MANHOLE"

DATE	REVISION	BY

DECATUR, ILLINOIS
A.I.P. PROJ.: 3-17-0033-28
IL PROJ.: DEC-3414

LAYOUT	CCC	03/18/05
DRAWN	CCC	03/18/05
REVIEWED	WJM	06/07/05

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Springfield, Illinois 62703-2886
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TAXIWAY G WIDENING
PHASE II
DRAINAGE DETAILS

JUN 10, 2005 9:44 AM CCC
E:\AIRPORTS\DECATUR\B02-04TXD\AIRPORT\SHEETS\CONTRACT-1\1R-531DRN.DWG - Details 1

TABLE OF DIMENSIONS

DESIGN NO.	NOMINAL DIAM. of PIPE	SLOPE of FILL	DIMENSIONS SAME FOR ALL MULTIPLES					DIMENSIONS FOR 2 PIPES	
			A	B	C	F	G	D	E
D375-1*	375	1:1*	495	260	590	750	940	1.55 m	2.61 m
(D15-1*)	(15)		(19)	(10)	(23)	(29*)	(37)	(5'-1")	(8'-6")
D375-2	375	1:2	660	260	590	1.0 m	940	1.55 m	2.96 m
(D15-2)	(15)		(26)	(10)	(23)	(3'-3")	(37)	(5'-1")	(9'-8")
D450-1*	450	1:1*	495	330	660	750	1.02 m	1.71 m	2.77 m
(D18-1*)	(18)		(19)	(13)	(26)	(29*)	(3'-4")	(5'-7")	(9'-0")
D450-2	450	1:2	660	330	660	1.0 m	1.02 m	1.71 m	3.12 m
(D18-2)	(18)		(26)	(13)	(26)	(3'-3")	(3'-4")	(5'-7")	(10'-2")
D600-1*	600	1:1*	640	410	840	965	1.22 m	2.11 m	3.47 m
(D24-1*)	(24)		(25)	(16)	(33)	(38)	(4'-0")	(6'-11")	(11'-4")
D600-2	600	1:2	860	410	840	1.28 m	1.22 m	2.11 m	3.92 m
(D24-2)	(24)		(34)	(16)	(33)	(4'-2")	(4'-0")	(6'-11")	(12'-10")
D750-1*	750	1:1*	760	485	990	1.14 m	1.37 m	2.41 m	4.02 m
(D30-1*)	(30)		(30)	(19)	(39)	(3'-9")	(4'-6")	(7'-11")	(13'-2")
D750-2	750	1:2	1.01 m	485	990	1.5 m	1.37 m	2.41 m	4.53 m
(D30-2)	(30)		(3'-4")	(19)	(39)	(4'-11")	(4'-6")	(7'-11")	(14'-10")
D900-1*	900	1:1*	915	560	1.17 m	1.36 m	1.57 m	2.75 m	4.67 m
(D36-1*)	(36)		(36)	(22)	(3'-10")	(4'-5")	(5'-2")	(9'-0")	(15'-6")
D900-2	900	1:2	1.22 m	560	1.17 m	1.79 m	1.57 m	2.75 m	5.28 m
(D36-2)	(36)		(4'-0")	(22)	(3'-10")	(5'-10")	(5'-2")	(9'-0")	(17'-6")

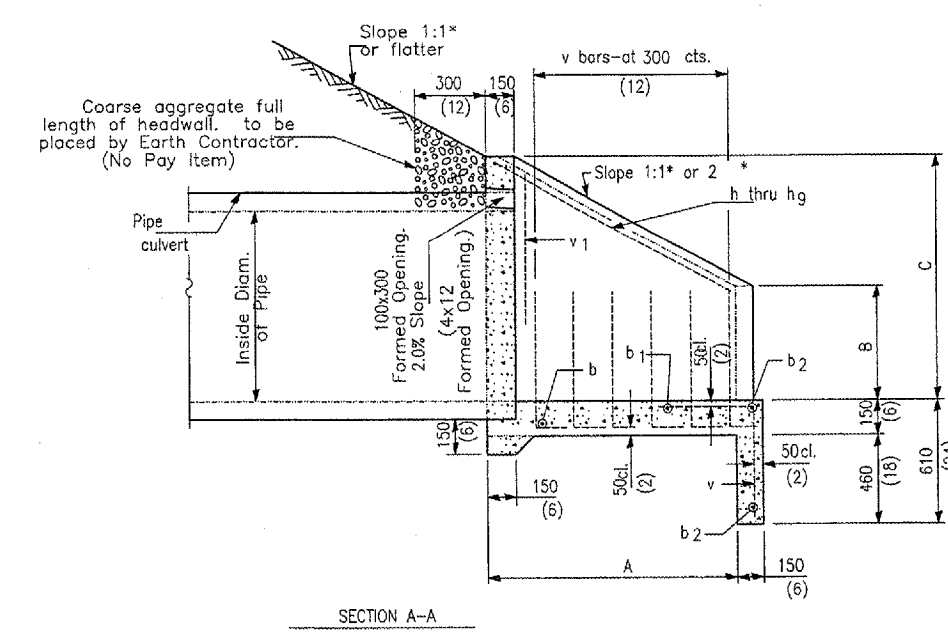
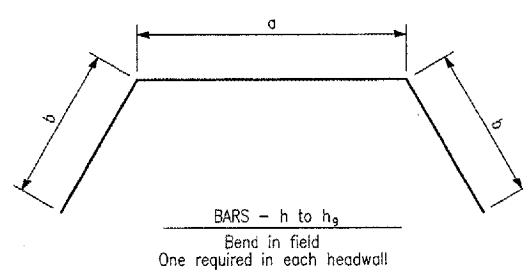
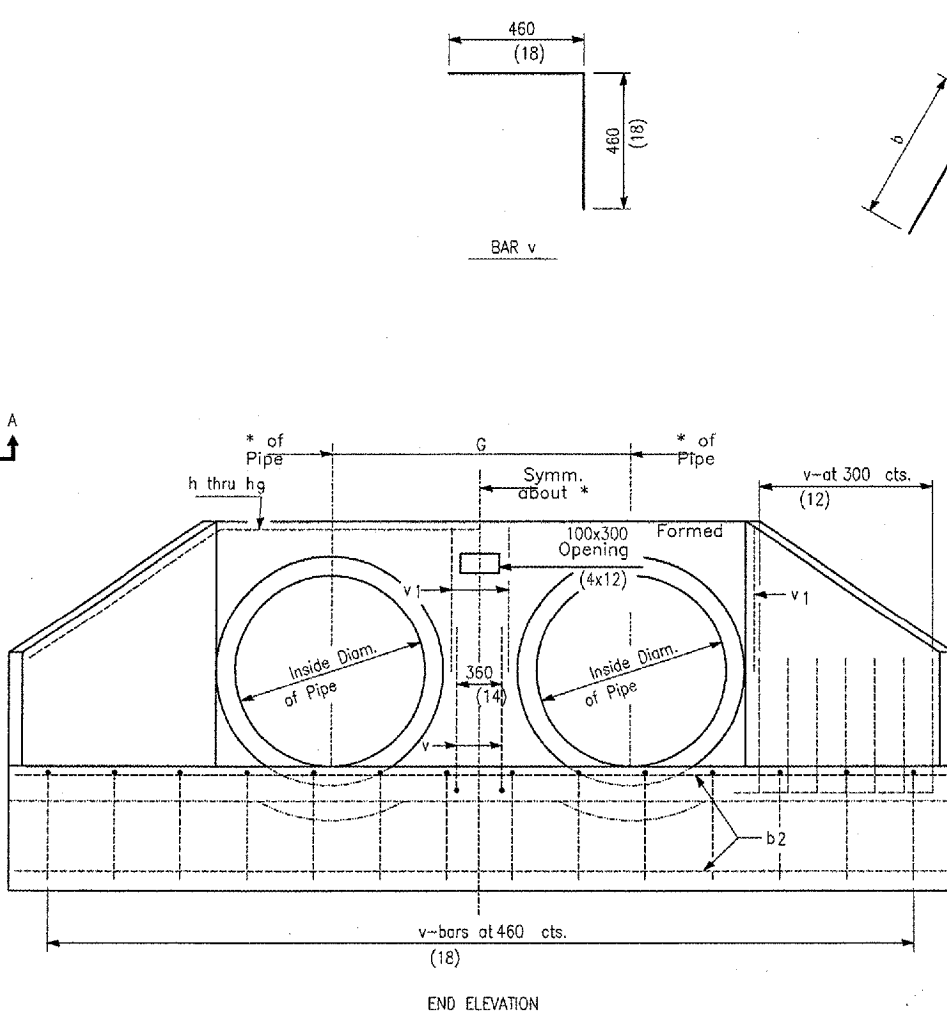
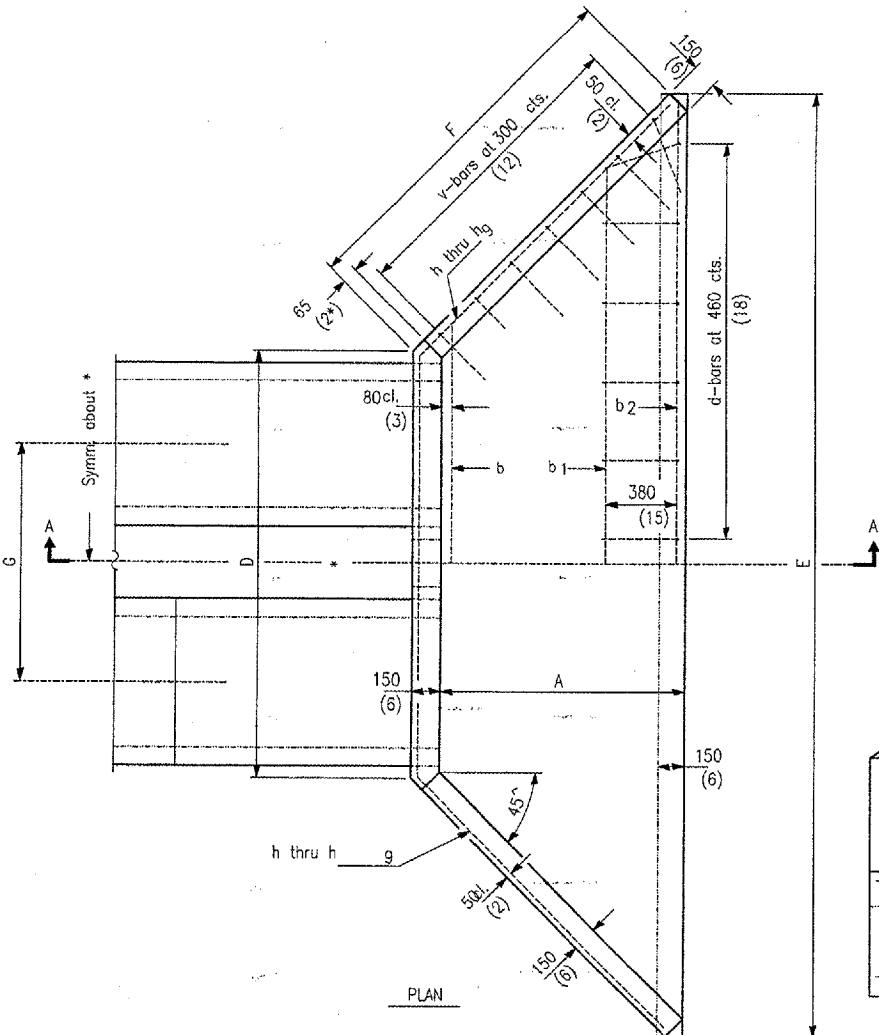
DIMENSIONS FOR No. 15 BARS-h to h₉ (No. 4) BARS-h to h₉

MARK	2 PIPES	
	a	b
h	1.5 m (4'-11")	740 (29)
h ₁	1.5 m (4'-11")	1.0 m (3'-3")
h ₂	1.65 m (5'-5")	740 (29)
h ₃	1.65 m (5'-5")	1.0 m (3'-3")
h ₄	2.06 m (6'-9")	990 (39)
h ₅	2.06 m (6'-9")	1.3 m (4'-3")
h ₆	2.36 m (7'-9")	1.18 m (3'-10")
h ₇	2.36 m (7'-9")	1.49 m (4'-10")
h ₈	2.75 m (9'-0")	1.45 m (4'-9")
h ₉	2.75 m (9'-0")	1.75 m (5'-9")

NOTE:
THE DESIGN FOR THE HEADWALLS ARE:
TWIN 24" PIPES - D24-2
TWIN 36" PIPES - D36-2

TABLE OF QUANTITIES

DESIGN NO.	Concrete 2 End Secs. cu. m (cu. yds.)	REINFORCEMENT BARS IN ONE END SECTION										TOTAL Qty. 2 End Secs. kg (lbs.)		
		No. 15-h Bars (No. 4)-h Bars		No. 15-b ₂ Bars (No. 4)-b ₂ Bars		No. 15-b ₁ Bars (No. 4)-b ₁ Bars		No. 15-b Bars (No. 4)-b Bars		No. 15-v ₁ Bars (No. 4)-v ₁ Bars			No. 15-v Bars (No. 4)-v Bars	
		MARK	LENGTH	QTY.	LENGTH	QTY.	LENGTH	QTY.	LENGTH	QTY.	LENGTH		QTY.	LENGTH
D375-1* (D15-1*)	1.1 (1.4)	h	2.98 m (9'-9")	2	2.5 m (8'-3")			1	1.9 m (6'-3")			14	70 (100)	
D375-2 (D15-2)	1.4 (1.8)	h ₁	3.5 m (11'-6")	2	2.82 m (9'-3")	1	2.21 m (7'-3")	1	1.9 m (6'-3")			17	90 (130)	
D450-1* (D18-1*)	1.3 (1.7)	h ₂	3.13 m (10'-3")	2	2.59 m (8'-6")			1	2.06 m (6'-9")			14	70 (100)	
D450-2 (D18-2)	1.5 (2.0)	h ₃	3.65 m (12'-0")	2	2.97 m (9'-9")	1	2.36 m (7'-9")	1	2.06 m (6'-9")			17	90 (130)	
D600-1* (D24-1*)	1.8 (2.4)	h ₄	4.04 m (13'-3")	2	3.35 m (11'-0")	1	2.74 m (9'-0")	1	2.44 m (8'-0")	4	790 (31)	18	110 (160)	
D600-2 (D24-2)	2.3 (3.0)	h ₅	4.66 m (15'-3")	2	3.81 m (12'-6")	1	3.2 m (10'-6")	1	2.44 m (8'-0")	4	790 (31)	21	130 (180)	
D750-1* (D30-1*)	2.3 (3.0)	h ₆	4.72 m (15'-6")	2	3.89 m (12'-9")	1	3.12 m (10'-3")	1	2.51 m (8'-3")	4	790 (31)	19	120 (170)	
D750-2 (D30-2)	2.9 (3.8)	h ₇	5.34 m (17'-6")	2	4.42 m (14'-6")	1	3.73 m (12'-3")	1	2.51 m (8'-3")	4	790 (31)	22	140 (190)	
D900-1* (D36-1*)	3.0 (3.9)	h ₈	5.65 m (18'-6")	2	4.57 m (15'-0")	1	3.81 m (12'-6")	1	2.97 m (9'-9")	4	1,07 m (3'-6")	22	150 (200)	
D900-2 (D36-2)	3.8 (5.0)	h ₉	6.25 m (20'-6")	2	5.18 m (17'-0")	1	4.42 m (14'-6")	1	2.97 m (9'-9")	4	1,07 m (3'-6")	26	170 (230)	

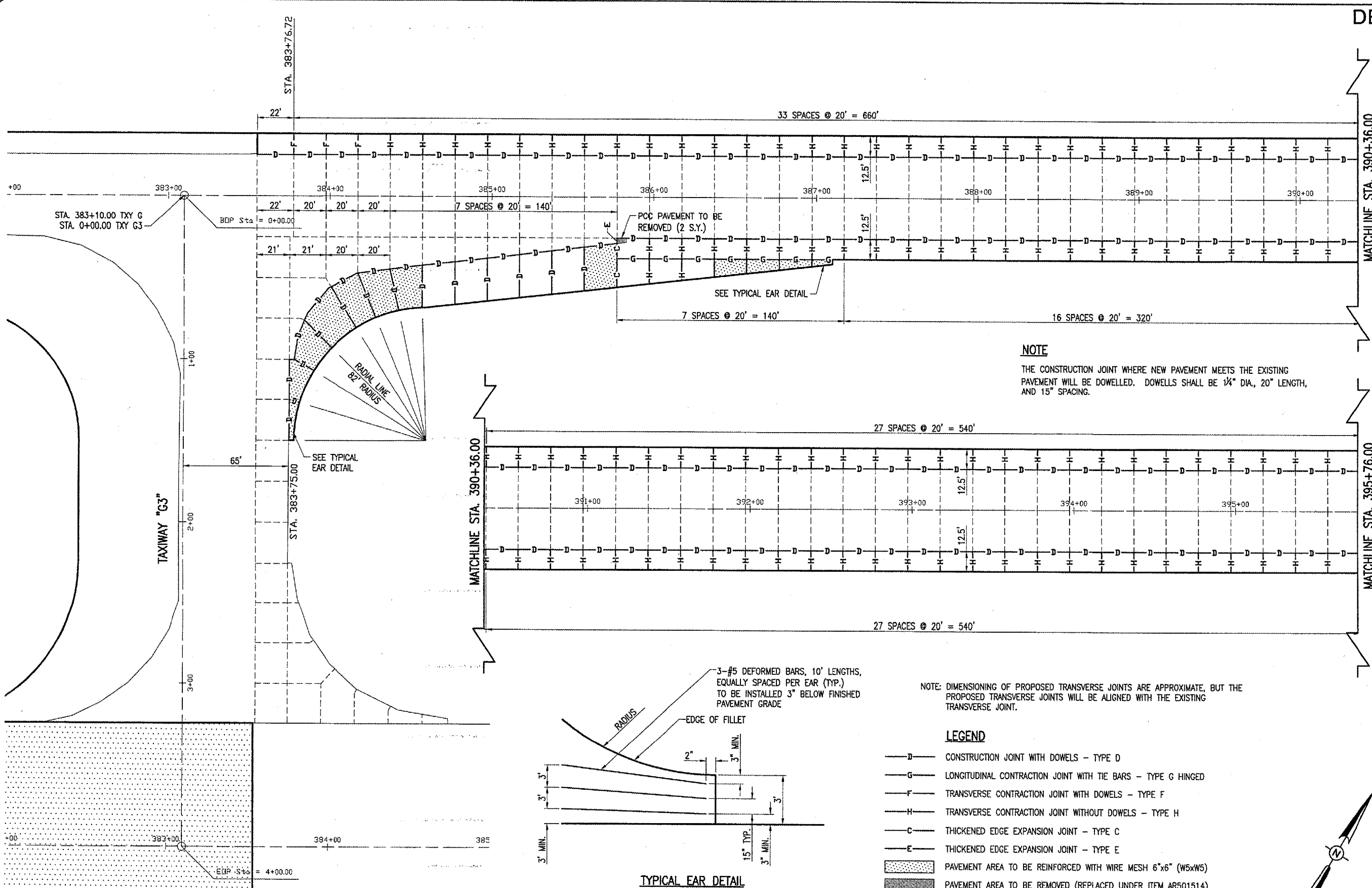


JUN 10, 2005 9:44 AM CCC
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BY	
REVISION	
DATE	
 HANSON Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62705-2886 Offices Nationwide	
Project No. 802-04TXD Drawing No. R-531DRN.DWG Scale N/A Date 03/18/05	CCC 03/18/05 CCC 03/18/05 WJM 06/07/05
TAXIWAY G WIDENING PHASE II DRAINAGE DETAILS	
30 <small>30 of 110 sheets</small>	

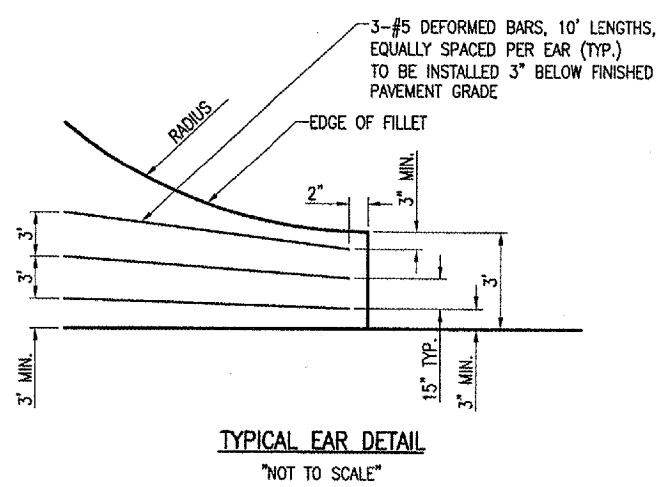
IL PROJ: DEC-3414
A.I.P. PROJ: 3-17-0033-28

JUN 10, 2005 9:40 AM CCC
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DE165

NOTE
 THE CONSTRUCTION JOINT WHERE NEW PAVEMENT MEETS THE EXISTING PAVEMENT WILL BE DOWELLED. DOWELLS SHALL BE 1/4" DIA., 20" LENGTH, AND 15" SPACING.



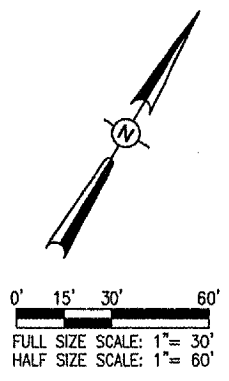
NOTE: DIMENSIONING OF PROPOSED TRANSVERSE JOINTS ARE APPROXIMATE, BUT THE PROPOSED TRANSVERSE JOINTS WILL BE ALIGNED WITH THE EXISTING TRANSVERSE JOINT.

LEGEND

- D — CONSTRUCTION JOINT WITH DOWELS - TYPE D
- G — LONGITUDINAL CONTRACTION JOINT WITH TIE BARS - TYPE G HINGED
- F — TRANSVERSE CONTRACTION JOINT WITH DOWELS - TYPE F
- H — TRANSVERSE CONTRACTION JOINT WITHOUT DOWELS - TYPE H
- C — THICKENED EDGE EXPANSION JOINT - TYPE C
- E — THICKENED EDGE EXPANSION JOINT - TYPE E
- ▨ PAVEMENT AREA TO BE REINFORCED WITH WIRE MESH 6"x6" (W5xW5)
- ▩ PAVEMENT AREA TO BE REMOVED (REPLACED UNDER ITEM AR501514)

QUANTITIES

ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR501900	REMOVE PCC PAVEMENT	S.Y.	295	



DATE	REVISION	BY

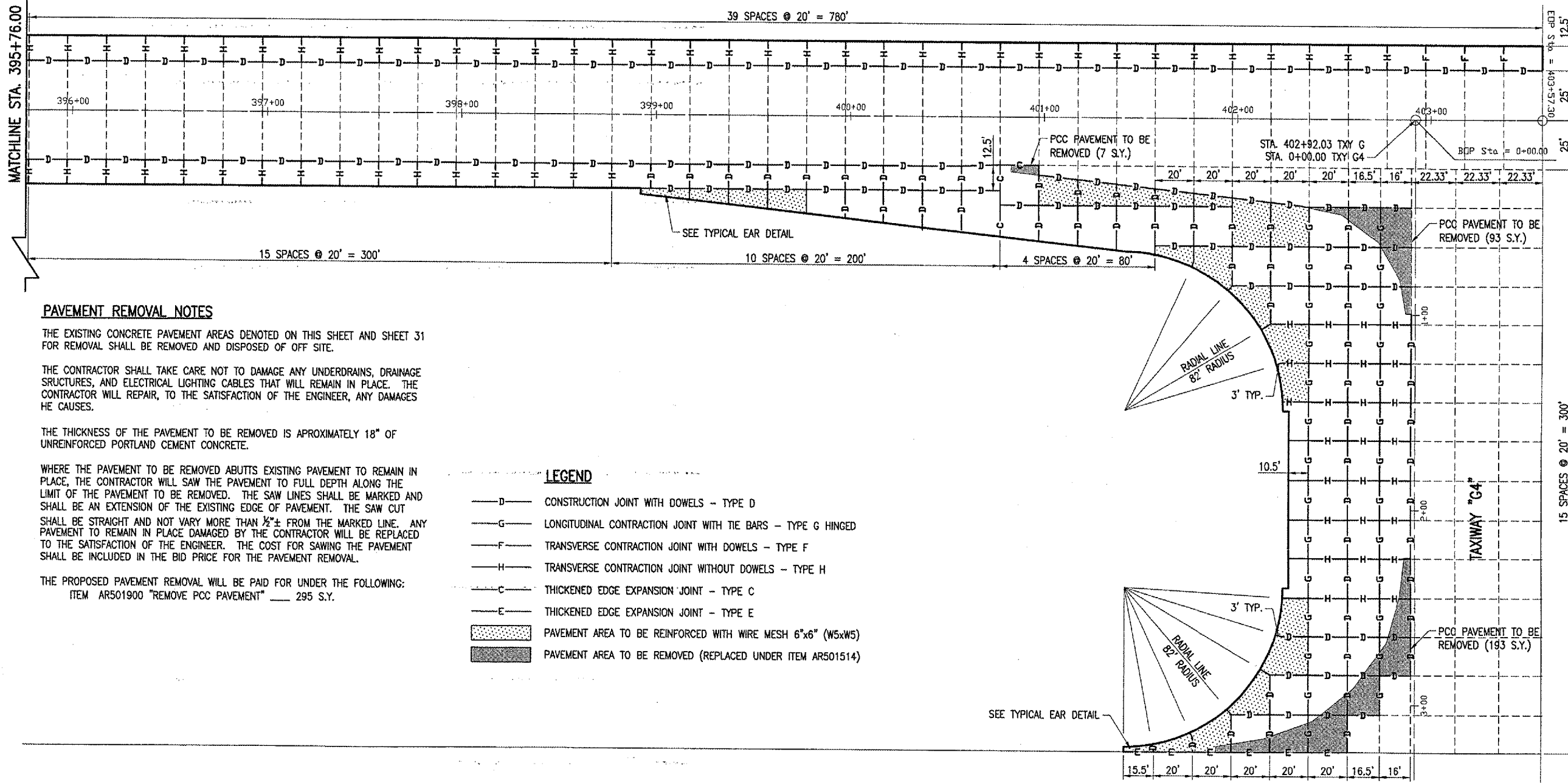
DECATUR, ILLINOIS

IEL Project No. 802-04TXD
 Plan No. 161JUNT.DWG
 Scale 1"=30'
 Date 10/28/04

DRAWN	WJM	DATE
		10/28/04
REVIEWED	WJM	DATE
		06/07/05

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TAXIWAY G WIDENING
PHASE II
 PROPOSED
 JOINTING PLAN
 STA. 383+54.66 TO 395+77.30



PAVEMENT REMOVAL NOTES

THE EXISTING CONCRETE PAVEMENT AREAS DENOTED ON THIS SHEET AND SHEET 31 FOR REMOVAL SHALL BE REMOVED AND DISPOSED OF OFF SITE.

THE CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE ANY UNDERDRAINS, DRAINAGE STRUCTURES, AND ELECTRICAL LIGHTING CABLES THAT WILL REMAIN IN PLACE. THE CONTRACTOR WILL REPAIR, TO THE SATISFACTION OF THE ENGINEER, ANY DAMAGES HE CAUSES.

THE THICKNESS OF THE PAVEMENT TO BE REMOVED IS APPROXIMATELY 18" OF UNREINFORCED PORTLAND CEMENT CONCRETE.

WHERE THE PAVEMENT TO BE REMOVED ABUTTS EXISTING PAVEMENT TO REMAIN IN PLACE, THE CONTRACTOR WILL SAW THE PAVEMENT TO FULL DEPTH ALONG THE LIMIT OF THE PAVEMENT TO BE REMOVED. THE SAW LINES SHALL BE MARKED AND SHALL BE AN EXTENSION OF THE EXISTING EDGE OF PAVEMENT. THE SAW CUT SHALL BE STRAIGHT AND NOT VARY MORE THAN 1/2" ± FROM THE MARKED LINE. ANY PAVEMENT TO REMAIN IN PLACE DAMAGED BY THE CONTRACTOR WILL BE REPLACED TO THE SATISFACTION OF THE ENGINEER. THE COST FOR SAWING THE PAVEMENT SHALL BE INCLUDED IN THE BID PRICE FOR THE PAVEMENT REMOVAL.

THE PROPOSED PAVEMENT REMOVAL WILL BE PAID FOR UNDER THE FOLLOWING:
ITEM AR501900 "REMOVE PCC PAVEMENT" — 295 S.Y.

LEGEND

- D — CONSTRUCTION JOINT WITH DOWELS - TYPE D
- G — LONGITUDINAL CONTRACTION JOINT WITH TIE BARS - TYPE G HINGED
- F — TRANSVERSE CONTRACTION JOINT WITH DOWELS - TYPE F
- H — TRANSVERSE CONTRACTION JOINT WITHOUT DOWELS - TYPE H
- C — THICKENED EDGE EXPANSION JOINT - TYPE C
- E — THICKENED EDGE EXPANSION JOINT - TYPE E
- [Pattern] PAVEMENT AREA TO BE REINFORCED WITH WIRE MESH 6"x6" (W5xW5)
- [Pattern] PAVEMENT AREA TO BE REMOVED (REPLACED UNDER ITEM AR501514)

JUN 10, 2005 9:41 AM CCC I:\AIRPORTS\DECATUR\802-04TXID\AIRPORT\SHEETS\CONTRACT-1\R-161\INT.DWG - STA. 395+77.30 TO 403+57.30

DATE	REVISION

DECATUR, ILLINOIS

IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

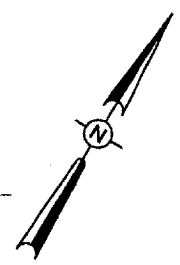
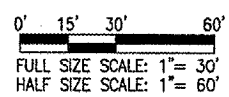
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LAYOUT	REVIEWED
DRAWN	

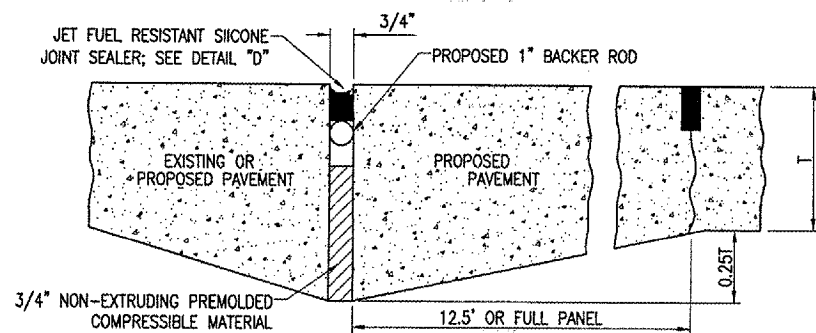
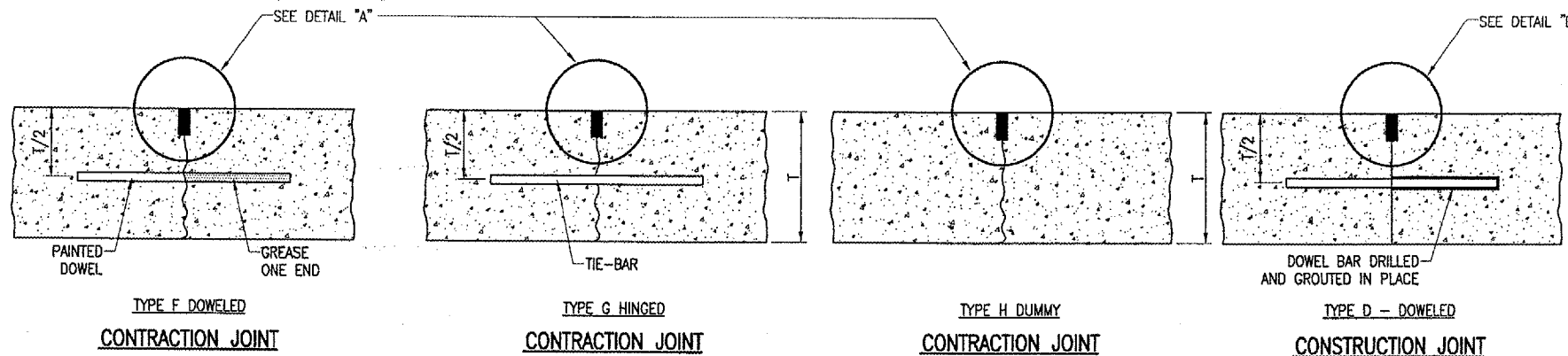
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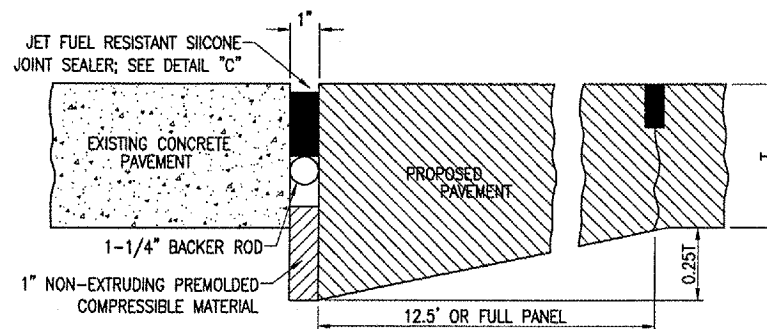
TAXIWAY G WIDENING PHASE II

PROPOSED JOINTING PLAN
 STA. 395+77.30 TO 403+57.30

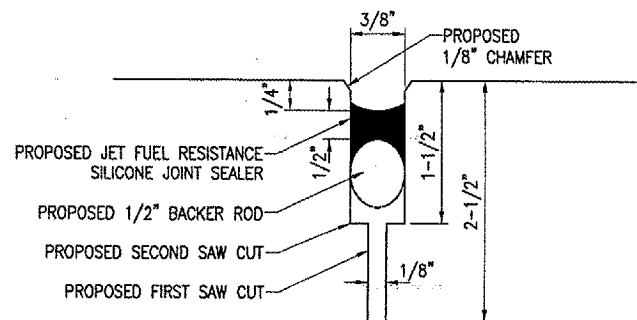




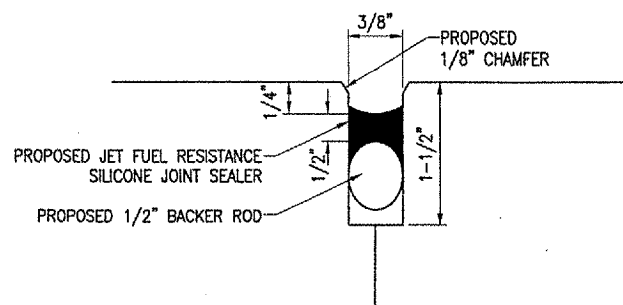
TYPE-C THICKENED EDGE
EXPANSION JOINT



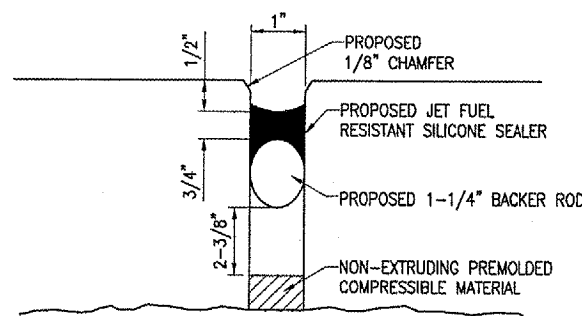
TYPE-E THICKENED EDGE EXPANSION JOINT



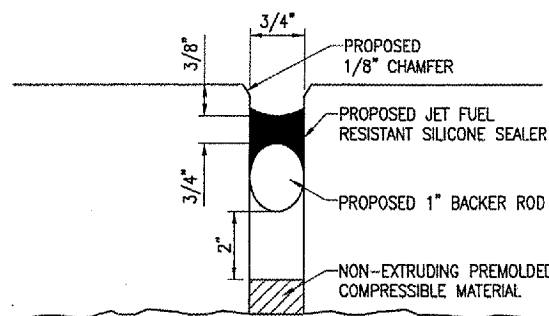
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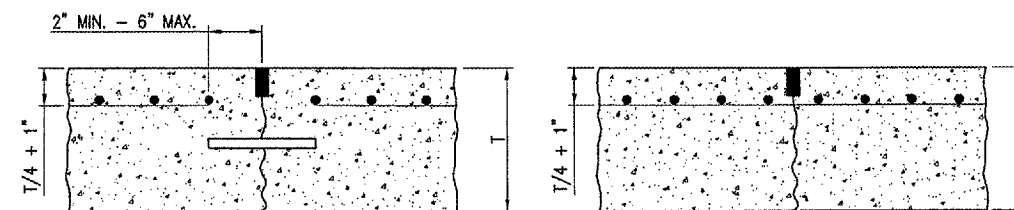
DETAIL "B"
NOT TO SCALE



DETAIL "C"
NOT TO SCALE



DETAIL "D"
NOT TO SCALE



DOWELED JOINT

WITHOUT TIE BARS

CONSTRUCTION OR CONTRACTION JOINTS
WITH STEEL REINFORCEMENT

JOINTING NOTES

- ALL EXPOSED JOINT EDGES SHALL BE CHAMFERED ACCORDING TO THE JOINT DETAILS ON THIS SHEET.
- ALL LONGITUDINAL AND TRANSVERSE CONTRACTION AND CONSTRUCTION JOINTS SHALL BE SAWS.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS SHALL BE 1-1/4" DIA., 20" LENGTH AND 15" SPACING.
- ALL DOWELS ARE TO BE PLACED AT A POINT NOT TO EXCEED 7-1/2 INCHES OR CLOSER THAN 6 INCHES FROM A LONGITUDINAL OR TRANSVERSE JOINT.
- ALL TIE-BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
- ALL TIE-BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" OR CLOSER THAN 6" FROM A TRANSVERSE, CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 30" ON CENTERS AND SHALL BE NO. 5 DEFORMED BARS, 30" IN LENGTH.
- DOWELS IN TRANSVERSE, CONTRACTION, AND CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
- DOWELS LOCATED IN THE CONSTRUCTION JOINTS WILL BE DRILLED AND GROUTED IN PLACE.
- ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE +1/8" FOR CONSTRUCTION JOINTS AND +1/4" FOR CONTRACTION JOINTS.
- THE COST OF ALL DOWEL BARS, TIE-BARS, PAVING MESH, SAWING, NEOPRENES AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.
- PRIOR TO PLACING ADJACENT PAVEMENT SECTIONS, THE VERTICAL EDGE SHALL BE CHECKED FOR TRUENESS, IF THE FACE IS BURRED OR IRREGULAR, THE CONTRACTOR SHALL GRIND, STONE OR SAW THE FACE TO PRODUCE A SMOOTH AND STRAIGHT EDGE.
- WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL ROPE THE TRANSVERSE JOINTS.
- JOINT SEALANT SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-2.5.
- CURING COMPOUND WILL BE AS SPECIFIED IN THE SPECIFICATIONS, ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.
- ALL NON-ALIGNED EDGES WILL BE SAWS FULL DEPTH.
- WIRE MESH IS TO BE SECURELY HELD IN PLACE BY MEANS OF SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING OR SAGGING DURING AND AFTER THE CONCRETE PLACEMENT. SUPPORT PINS ARE TO BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6 INCHES INTO THE SUBGRADE.
- PRIOR TO PLACING MATERIAL HAULING CONSTRUCTION EQUIPMENT ON THE NEW PAVEMENT, THE PSI OF THE PAVEMENT IS TO REACH 3000 POUNDS STRENGTH.

BY	
REVISION	
DATE	

DECATUR, ILLINOIS

 I.L. PROJ.: DEC-3414

HE: Project No.	802-04TYD
File Name	R-56 JUNT.DWG
Scale	N/A
Date	03/18/05
LAYOUT	CCC 03/18/05
DRAWN	CCC 03/18/05
REVIEWED	WJM 06/07/05

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TAXIWAY G WIDENING

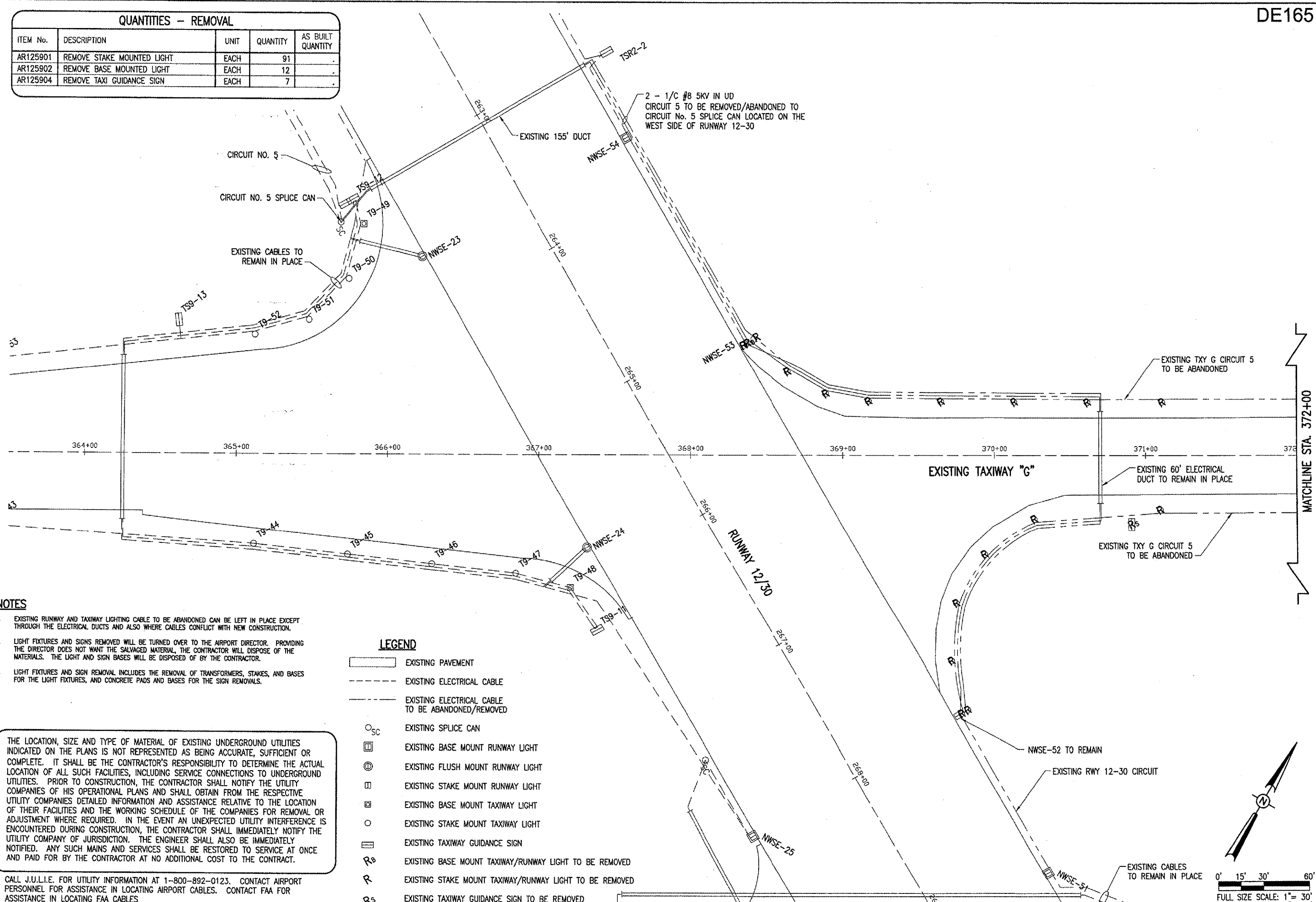
PHASE II

 PROPOSED PCC PAVEMENT

 JOINTING DETAILS

JUN 10, 2005 9:48 AM CCC
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QUANTITIES - REMOVAL				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	91	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	12	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	7	

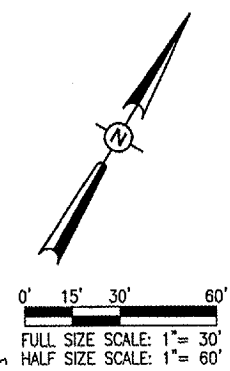


- NOTES**
- EXISTING RUNWAY AND TAXIWAY LIGHTING CABLE TO BE ABANDONED CAN BE LEFT IN PLACE EXCEPT THROUGH THE ELECTRICAL DUCTS AND ALSO WHERE CABLES CONFLICT WITH NEW CONSTRUCTION.
 - LIGHT FIXTURES AND SIGNS REMOVED WILL BE TURNED OVER TO THE AIRPORT DIRECTOR. PROVIDING THE DIRECTOR DOES NOT WANT THE SALVAGED MATERIAL, THE CONTRACTOR WILL DISPOSE OF THE MATERIALS. THE LIGHT AND SIGN BASES WILL BE DISPOSED OF BY THE CONTRACTOR.
 - LIGHT FIXTURES AND SIGN REMOVAL INCLUDES THE REMOVAL OF TRANSFORMERS, STAKES, AND BASES FOR THE LIGHT FIXTURES, AND CONCRETE PADS AND BASES FOR THE SIGN REMOVALS.

- LEGEND**
- EXISTING PAVEMENT
 - EXISTING ELECTRICAL CABLE
 - EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
 - EXISTING SPLICE CAN
 - EXISTING BASE MOUNT RUNWAY LIGHT
 - EXISTING FLUSH MOUNT RUNWAY LIGHT
 - EXISTING STAKE MOUNT RUNWAY LIGHT
 - EXISTING BASE MOUNT TAXIWAY LIGHT
 - EXISTING STAKE MOUNT TAXIWAY LIGHT
 - EXISTING TAXIWAY GUIDANCE SIGN
 - EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
 - EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
 - EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES



DATE	REVISION	BY

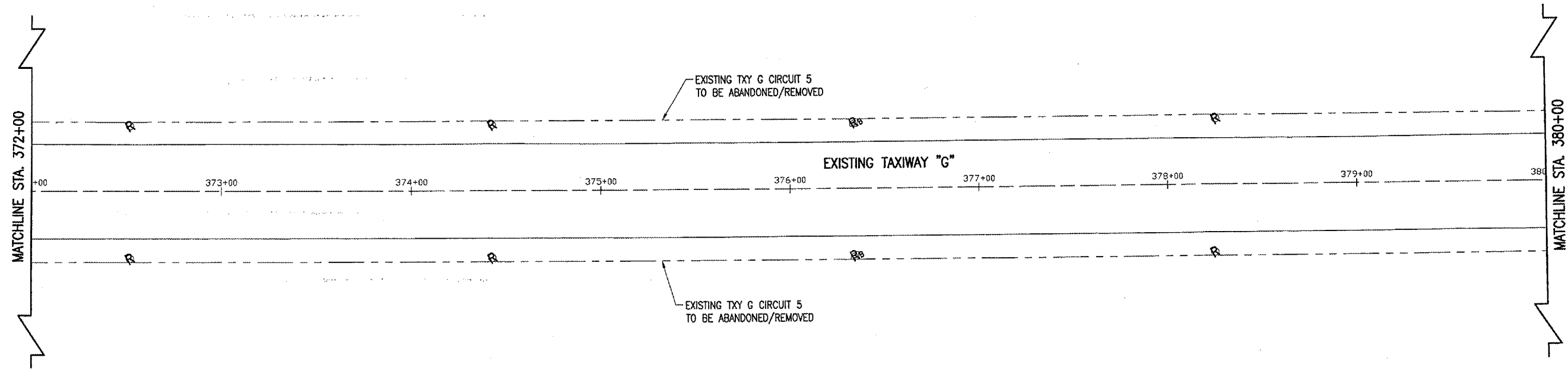
DECATUR AIRPORT
DECATUR, ILLINOIS
 IL PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-00333-28

REV.	DATE	BY	DESCRIPTION
03/17/05	CCC	CCC	LAYOUT
03/17/05	CCC	CCC	DRAWN
06/07/05	NUM	NUM	REVIEWED







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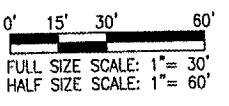
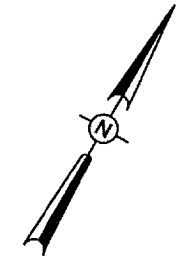
**TAXIWAY G WIDENING
 PHASE II**
 EXISTING ELECTRICAL
 REMOVAL PLAN
 STA. 367+85.60 TO STA. 372+00

JUN 10, 2005 9:31 AM CCC
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LEGEND

-  EXISTING PAVEMENT
-  EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
-  EXISTING BASE MOUNT TAXIWAY LIGHT
-  EXISTING STAKE MOUNT TAXIWAY LIGHT
-  EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
-  EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED



DATE	REVISION	BY

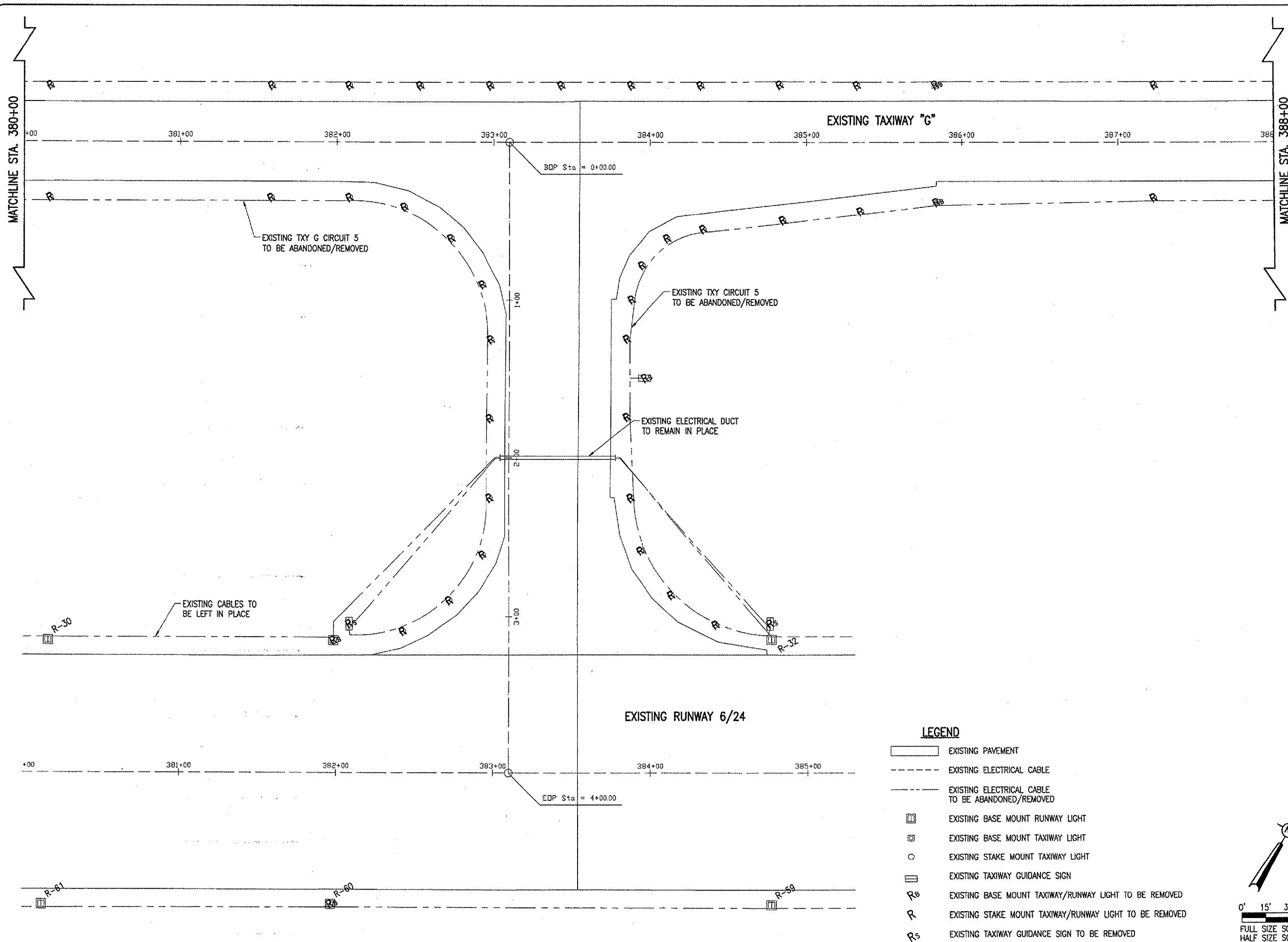
DECATUR AIRPORT
DECATUR, ILLINOIS
 IL PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEI Project No. 802-04TYXD	CCC	03/17/05
Drawings R-141ELE.DWG	CCC	03/17/05
Scale 1"=30'	WUM	06/07/05
Date 03/17/05		
LAYOUT		
DRAWN		
REVIEWED		

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**TAXIWAY G WIDENING
 PHASE II**
 EXISTING ELECTRICAL
 REMOVAL PLAN
 STA. 372+00 TO STA. 380+00

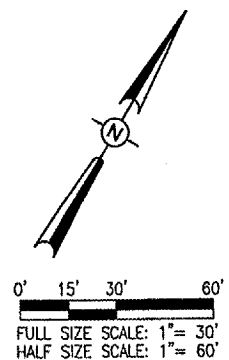
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DE165

LEGEND

- EXISTING PAVEMENT
- EXISTING ELECTRICAL CABLE
- EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
- EXISTING BASE MOUNT RUNWAY LIGHT
- EXISTING BASE MOUNT TAXIWAY LIGHT
- EXISTING STAKE MOUNT TAXIWAY LIGHT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED



DATE	REVISION	BY

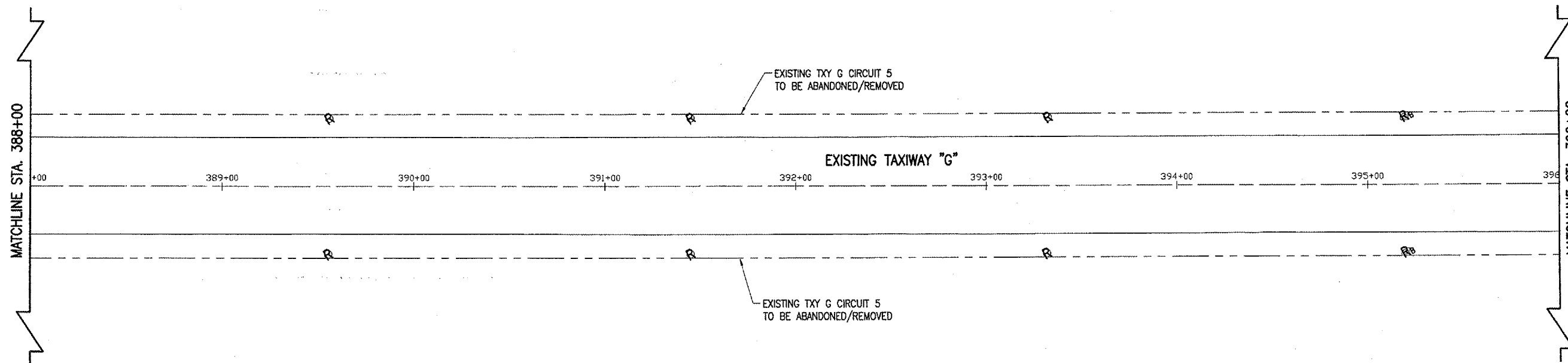
DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

FILE PROJECT NO.	DATE	CC	CCC	WJM
802-04TXID	03/17/05			
Filename: R-141ELE.DWG	03/17/05			
Scale: 1"=30'	06/07/05			
Date: 03/17/05				

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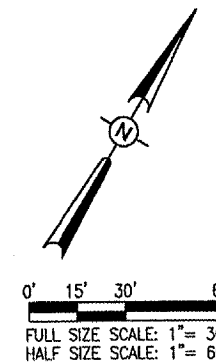
**TAXIWAY G WIDENING
 PHASE II**
**EXISTING ELECTRICAL
 REMOVAL PLAN**
 STA. 380+00 TO STA. 388+00

DE165



LEGEND

- EXISTING PAVEMENT
- EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
- EXISTING BASE MOUNT TAXIWAY LIGHT
- EXISTING STAKE MOUNT TAXIWAY LIGHT
- EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED



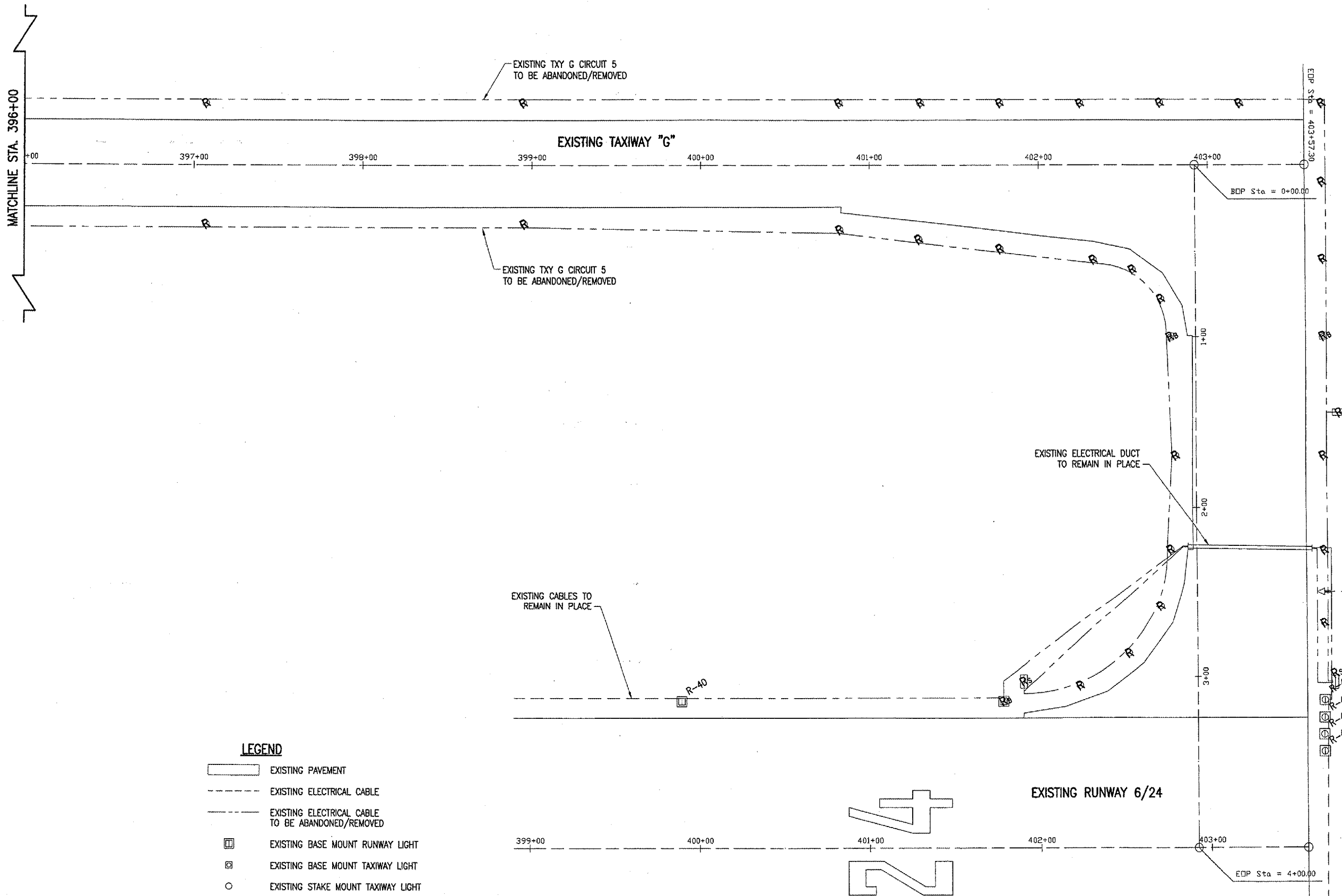
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 I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

HEI Project No.	802-04TXD
Filename	R-14ELE.DWG
Scale	1" = 30'
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DRAWN	CCC 03/17/05
REVIEWED	WJM 06/07/05


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**TAXIWAY G WIDENING
 PHASE II**
 EXISTING ELECTRICAL
 REMOVAL PLAN
 STA. 388+00 TO STA. 396+00



- LEGEND**
- EXISTING PAVEMENT
 - EXISTING ELECTRICAL CABLE
 - EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
 - EXISTING BASE MOUNT RUNWAY LIGHT
 - EXISTING BASE MOUNT TAXIWAY LIGHT
 - EXISTING STAKE MOUNT TAXIWAY LIGHT
 - EXISTING TAXIWAY GUIDANCE SIGN
 - EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
 - EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
 - EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED

JUN 10, 2005 9:31 AM CCC
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DATE	REVISION	BY



ILL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

HEI Project No. 802-04TXWD	
Drawn R-141ELE.DWG	
Scale 1"=30'	
Date 03/17/05	
LAYOUT	CC 03/17/05
DRAWN	CCC 03/17/05
REVIEWED	WJM 06/07/05



TAXIWAY G WIDENING PHASE II
EXISTING ELECTRICAL REMOVAL PLAN
 STA. 396+00 TO STA. 403+57.30

NOTES

ALL TAXIWAY LIGHTS ARE TO BE LOCATED 10 FEET FROM THE CONCRETE PAVEMENT EDGE OF TAXIWAY "G" AND THE NEAR EDGE OF TAXIWAY SIGNS WILL BE LOCATED 15 FEET FROM THE EDGE.

REFER TO THE LIGHTING STANDARD SHEET AND SPECIFICATION FOR DETAILS FOR THE STAKE AND BASE LIGHT, AND TAXIWAY SIGNS.

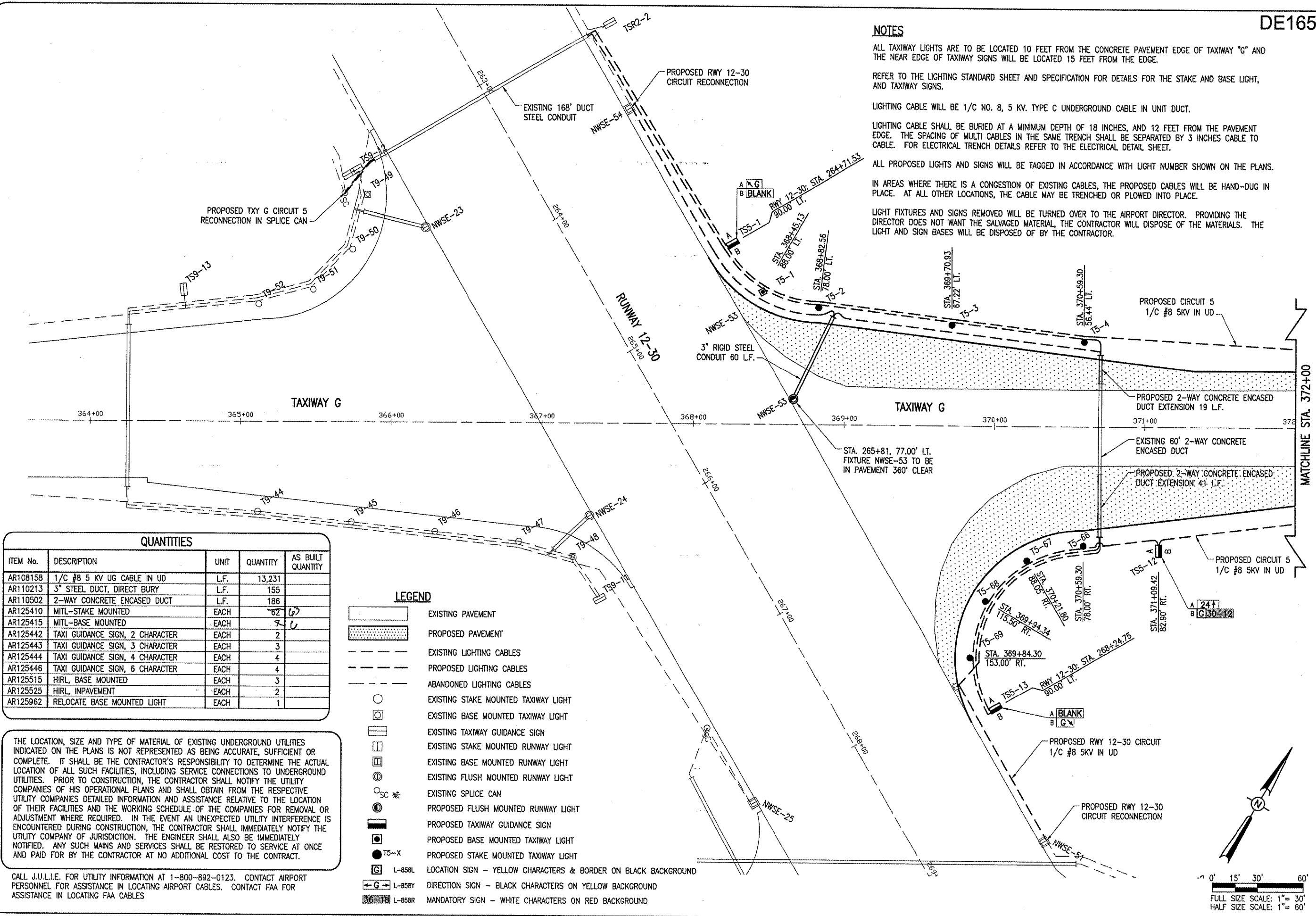
LIGHTING CABLE WILL BE 1/C NO. 8, 5 KV. TYPE C UNDERGROUND CABLE IN UNIT DUCT.

LIGHTING CABLE SHALL BE BURIED AT A MINIMUM DEPTH OF 18 INCHES, AND 12 FEET FROM THE PAVEMENT EDGE. THE SPACING OF MULTI CABLES IN THE SAME TRENCH SHALL BE SEPARATED BY 3 INCHES CABLE TO CABLE. FOR ELECTRICAL TRENCH DETAILS REFER TO THE ELECTRICAL DETAIL SHEET.

ALL PROPOSED LIGHTS AND SIGNS WILL BE TAGGED IN ACCORDANCE WITH LIGHT NUMBER SHOWN ON THE PLANS.

IN AREAS WHERE THERE IS A CONGESTION OF EXISTING CABLES, THE PROPOSED CABLES WILL BE HAND-DUG IN PLACE. AT ALL OTHER LOCATIONS, THE CABLE MAY BE TRENCHED OR PLOWED INTO PLACE.

LIGHT FIXTURES AND SIGNS REMOVED WILL BE TURNED OVER TO THE AIRPORT DIRECTOR. PROVIDING THE DIRECTOR DOES NOT WANT THE SALVAGED MATERIAL, THE CONTRACTOR WILL DISPOSE OF THE MATERIALS. THE LIGHT AND SIGN BASES WILL BE DISPOSED OF BY THE CONTRACTOR.



QUANTITIES

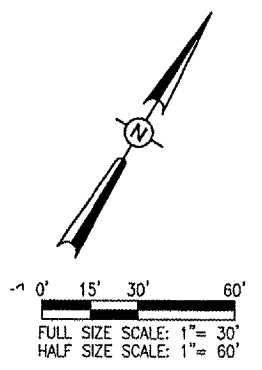
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	13,231	
AR110213	3" STEEL DUCT, DIRECT BURY	L.F.	155	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	186	
AR125410	MITL-STAKE MOUNTED	EACH	62	63
AR125415	MITL-BASE MOUNTED	EACH	7	6
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	3	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	4	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	4	
AR125515	HIRL, BASE MOUNTED	EACH	3	
AR125525	HIRL, INPAVEMENT	EACH	2	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	1	

LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING LIGHTING CABLES
- PROPOSED LIGHTING CABLES
- ABANDONED LIGHTING CABLES
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING FLUSH MOUNTED RUNWAY LIGHT
- EXISTING SPLICE CAN
- PROPOSED FLUSH MOUNTED RUNWAY LIGHT
- PROPOSED TAXIWAY GUIDANCE SIGN
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- L-858L LOCATION SIGN - YELLOW CHARACTERS & BORDER ON BLACK BACKGROUND
- L-858Y DIRECTION SIGN - BLACK CHARACTERS ON YELLOW BACKGROUND
- L-858R MANDATORY SIGN - WHITE CHARACTERS ON RED BACKGROUND

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES



DATE	REVISION

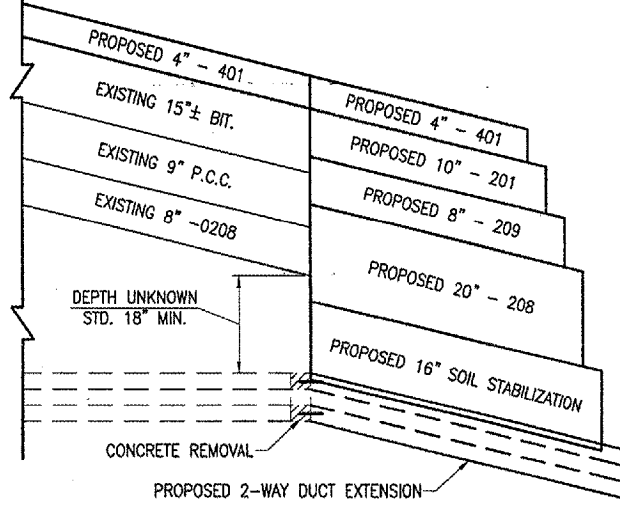
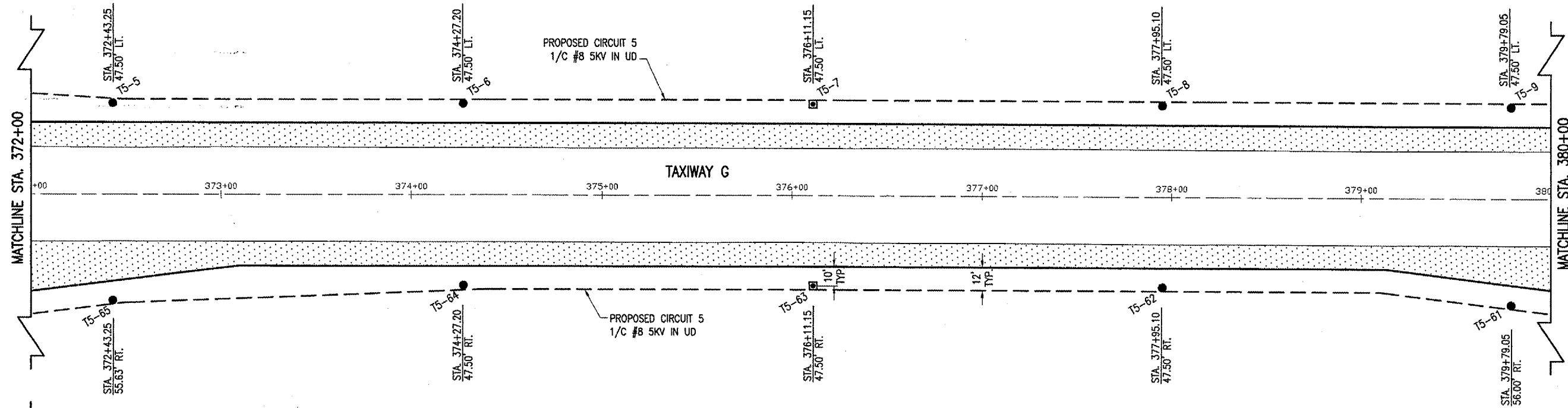
DECATUR AIRPORT
DECATUR, ILLINOIS
 ILL. PROJ.: DEC-3414
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DATE	BY	REVISION
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03/17/05	CCC	DRAWN
06/07/05	WJM	REVIEWED

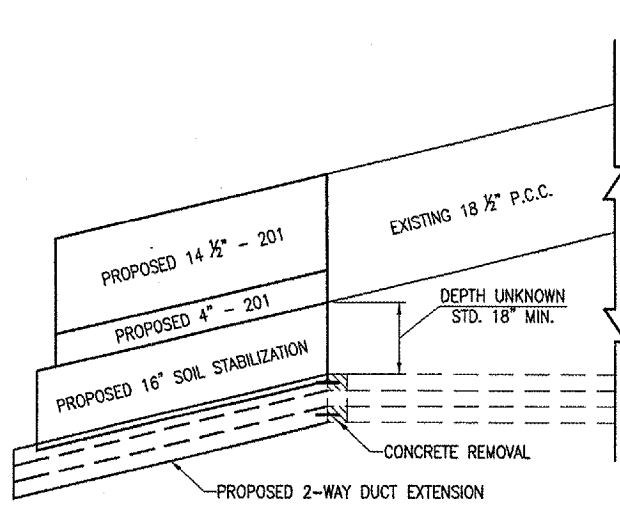
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TAXIWAY G WIDENING
 PHASE II
 PROPOSED
 ELECTRICAL PLAN
 STA. 367+85.60 TO STA. 372+00

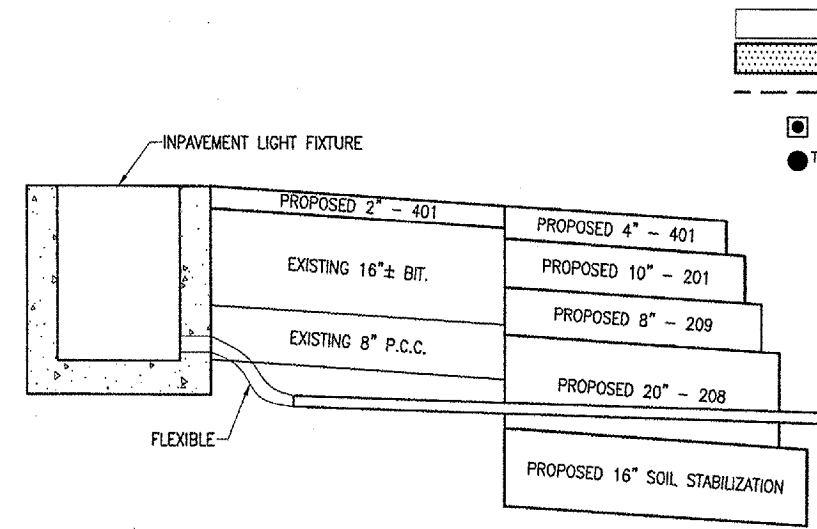
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2-WAY DUCT EXTENSION - FLEXIBLE PAVEMENT WIDENING
"NOT TO SCALE"



2-WAY DUCT EXTENSION - RIGID PAVEMENT WIDENING
"NOT TO SCALE"



INPAVEMENT FIXTURE - DUCT
"NOT TO SCALE"

LEGEND

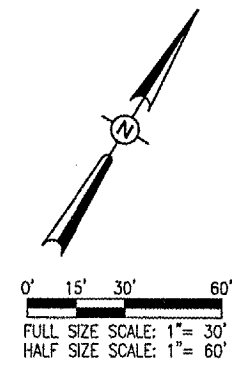
- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- PROPOSED LIGHTING CABLES
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- T5-X

NOTES

1. THE CONCRETE ENCASUREMENT SHALL BE CUT BACK A SUFFICIENT DISTANCE, MINIMUM 4", TO PLACE A COLLAR ON THE EXISTING DONDUIT TO CONNECT THE PROPOSED DUCT.
2. NEW REBAR SHALL BE DRILLED INTO THE EXISTING CONCRETE ENCASUREMENT PER THE ELECTRICAL STANDARD SHEET.
3. THE PROPOSED 2-WAY DUCT EXTENSION SHOULD BE PLACED BENEATH THE SOIL STABILIZATION DEPTH IF POSSIBLE.
4. PROVIDING THE SOIL STABILIZATION IS COMPLETED PRIOR TO THE 2-WAY DUCT EXTENSION WORK, THE EXCAVATED AREA FOR THE DUCT WILL BE BACKFILLED WITH CRUSHED AGGREGATE BASE COURSE PER SPECIFICATION AR209510. THE BACKFILL WILL EXTEND TO THE TOP OF THE SUBGRADE.

NOTE

THE EXISTING PAVEMENT WILL BE TRENCHED FROM THE INPAVEMENT LIGHT TO THE EXISTING EDGE OF PAVEMENT. THE 3" RIGID STEEL CONDUIT WILL BE BURIED AT A MINIMUM DEPTH OF 32" BELOW THE PROPOSED SURFACE THROUGH THE PROPOSED PAVEMENT AREA. THE 20" CA-2 AGGREGATE WILL BE PLACED PRIOR TO INSTALLATION OF THE DUCT THROUGH THE PROPOSED PAVEMENT AREA. A TRENCH WILL BE CUT INTO THE AGGREGATE TO PLACE THE CONDUIT. THE TRENCHES WILL BE BACKFILLED WITH 610 STRUCTURAL CONCRETE. THE 610 CONCRETE BACKFILL IS PART OF THE WORK ITEM. AS AN ALTERNATE TO THE DESCRIBED 3" RIGID STEEL CONDUIT WORK ITEM, A 3" DIRECT BORE PER ITEM AR110 METHOD IS ACCEPTABLE. THE ALTERNATE METHOD WILL BE PAID AS THE PLAN QUANTITY OF 3" RIGID STEEL CONDUIT.



REVISION	DATE	BY

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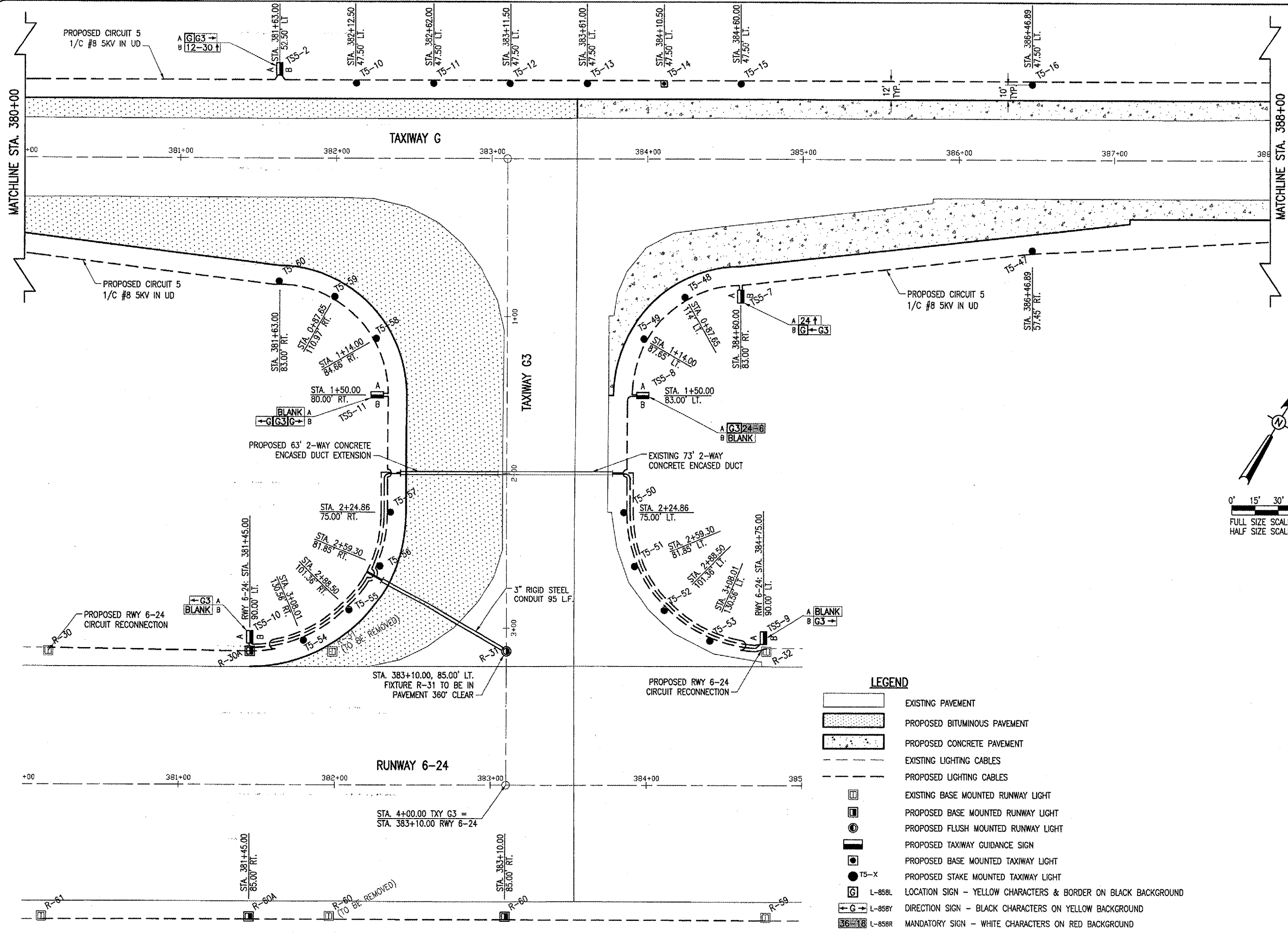
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Drawn R-142ELE.DWG	DATE 03/17/05
Scale 1"=30'	DATE 03/17/05
Sheet 03/17/05	DATE 03/17/05
LAYOUT	CCC
DRAWN	CCC
REVIEWED	WJM

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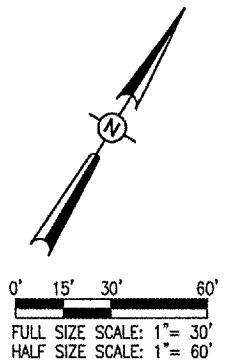
TAXIWAY G WIDENING PHASE II
PROPOSED ELECTRICAL PLAN
STA. 372+00 TO STA. 380+00

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JUN 27, 2005 1:10 PM CCC
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DE165



- LEGEND**
- [Pattern: Dotted] EXISTING PAVEMENT
 - [Pattern: Stippled] PROPOSED BITUMINOUS PAVEMENT
 - [Pattern: Dashed] PROPOSED CONCRETE PAVEMENT
 - [Line: Dashed] EXISTING LIGHTING CABLES
 - [Line: Solid] PROPOSED LIGHTING CABLES
 - [Symbol: Square with dot] EXISTING BASE MOUNTED RUNWAY LIGHT
 - [Symbol: Square with dot] PROPOSED BASE MOUNTED RUNWAY LIGHT
 - [Symbol: Circle with dot] PROPOSED FLUSH MOUNTED RUNWAY LIGHT
 - [Symbol: Square with dot] PROPOSED TAXIWAY GUIDANCE SIGN
 - [Symbol: Square with dot] PROPOSED BASE MOUNTED TAXIWAY LIGHT
 - [Symbol: Circle with dot] PROPOSED STAKE MOUNTED TAXIWAY LIGHT
 - [Symbol: Square with G] L-85BL LOCATION SIGN - YELLOW CHARACTERS & BORDER ON BLACK BACKGROUND
 - [Symbol: Square with G and arrow] L-85BY DIRECTION SIGN - BLACK CHARACTERS ON YELLOW BACKGROUND
 - [Symbol: Square with 36-18] L-85BR MANDATORY SIGN - WHITE CHARACTERS ON RED BACKGROUND

REVISION	DATE	BY

DECATUR AIRPORT
DECATUR, ILLINOIS

ILL. PROJ.: DEC-3414
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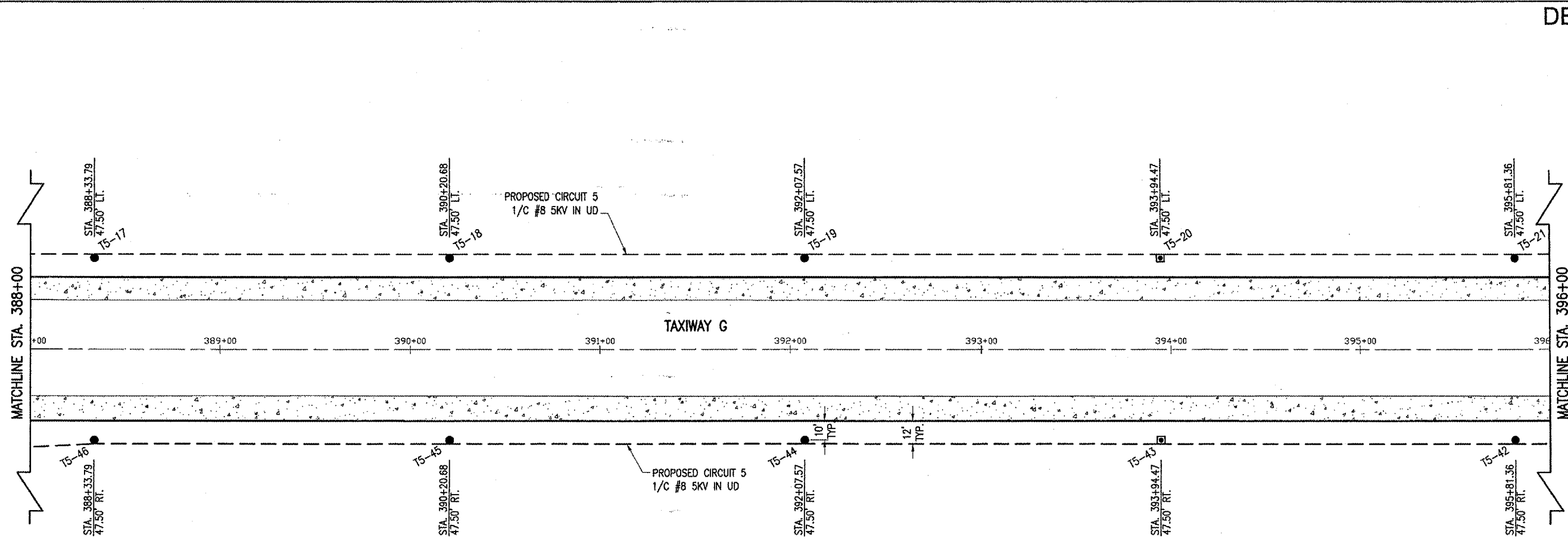
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**TAXIWAY G WIDENING
 PHASE II**

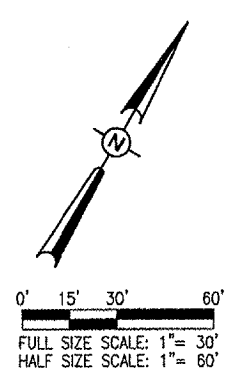
**PROPOSED
 ELECTRICAL PLAN**

STA. 380+00 TO STA. 388+00



LEGEND

- EXISTING PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED LIGHTING CABLES
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- TS-X PROPOSED STAKE MOUNTED TAXIWAY LIGHT



DE165

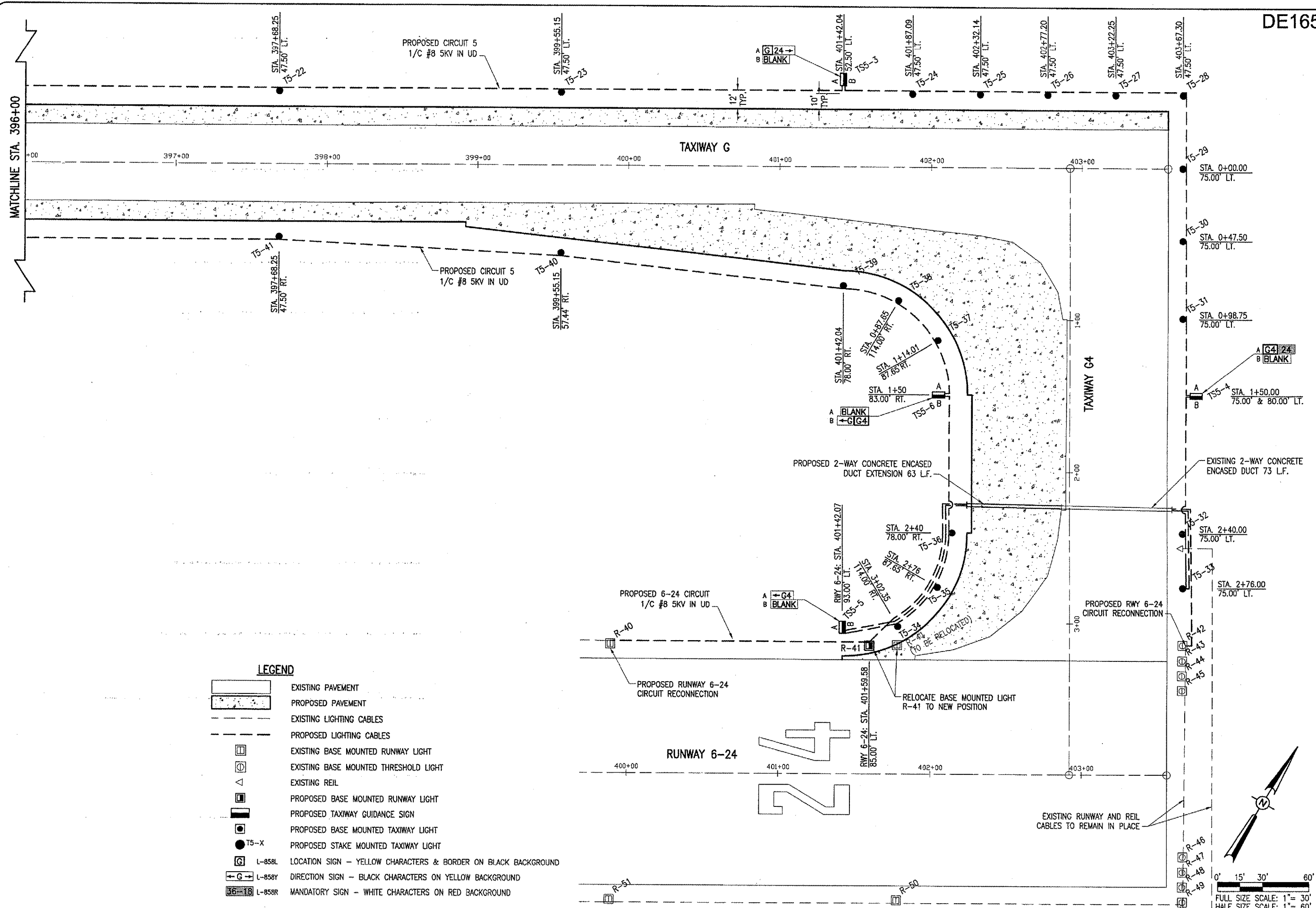
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EEI Project No. 802-04TXD	
Revision R-142ELE.DWG	
Scale 1" = 30'	
Date 03/17/05	
LAYOUT	CCC 03/17/05
DRAWN	CCC 03/17/05
REVIEWED	WJM 06/07/05

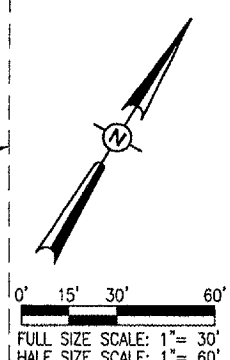
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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED
 ELECTRICAL PLAN
 STA. 388+00 TO STA. 396+00



LEGEND

	EXISTING PAVEMENT
	PROPOSED PAVEMENT
	EXISTING LIGHTING CABLES
	PROPOSED LIGHTING CABLES
	EXISTING BASE MOUNTED RUNWAY LIGHT
	EXISTING BASE MOUNTED THRESHOLD LIGHT
	EXISTING REIL
	PROPOSED BASE MOUNTED RUNWAY LIGHT
	PROPOSED TAXIWAY GUIDANCE SIGN
	PROPOSED BASE MOUNTED TAXIWAY LIGHT
	PROPOSED STAKE MOUNTED TAXIWAY LIGHT
	L-858L LOCATION SIGN - YELLOW CHARACTERS & BORDER ON BLACK BACKGROUND
	L-858Y DIRECTION SIGN - BLACK CHARACTERS ON YELLOW BACKGROUND
	L-858R MANDATORY SIGN - WHITE CHARACTERS ON RED BACKGROUND



DE165

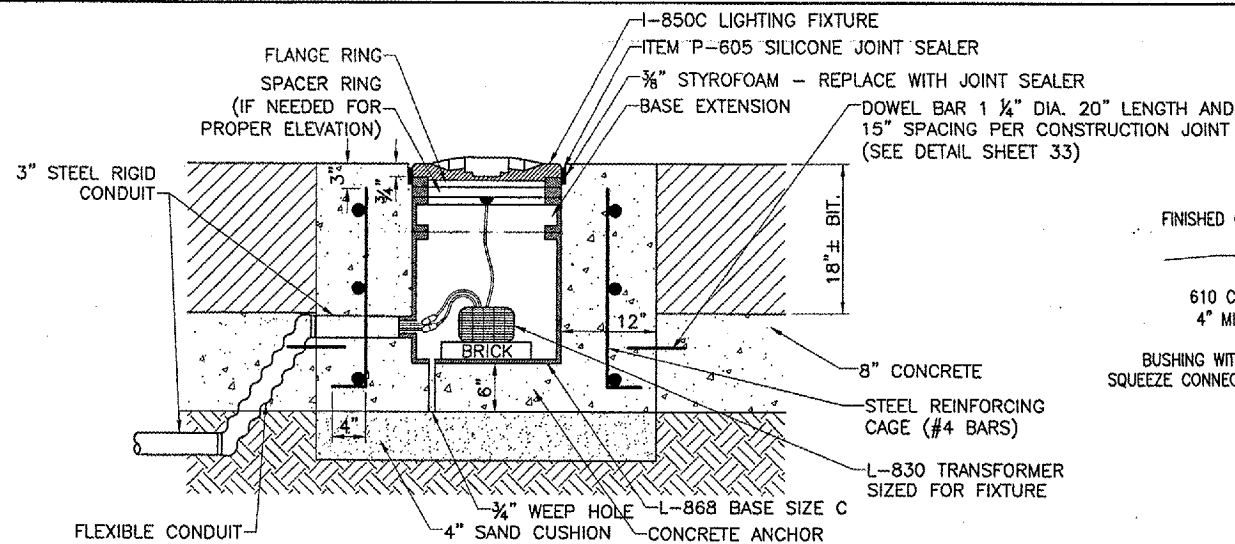
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DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

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File Name	R-142ELE.DWG
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Date	03/17/05
LAYOUT	CC 03/17/05
DRAWN	CCC 03/17/05
REVIEWED	WGM 06/07/05

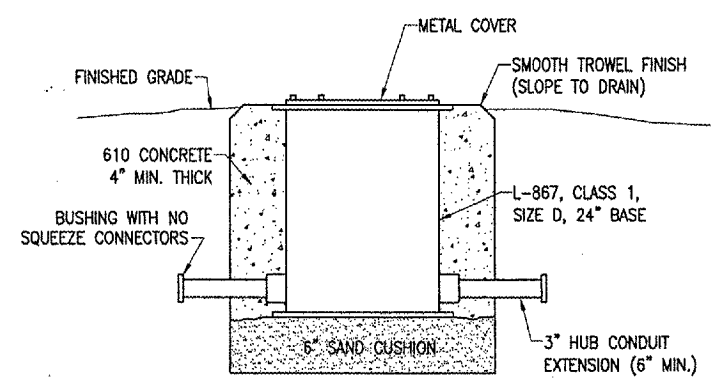
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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED
 ELECTRICAL PLAN
 STA. 396+00 TO STA. 403+57.30

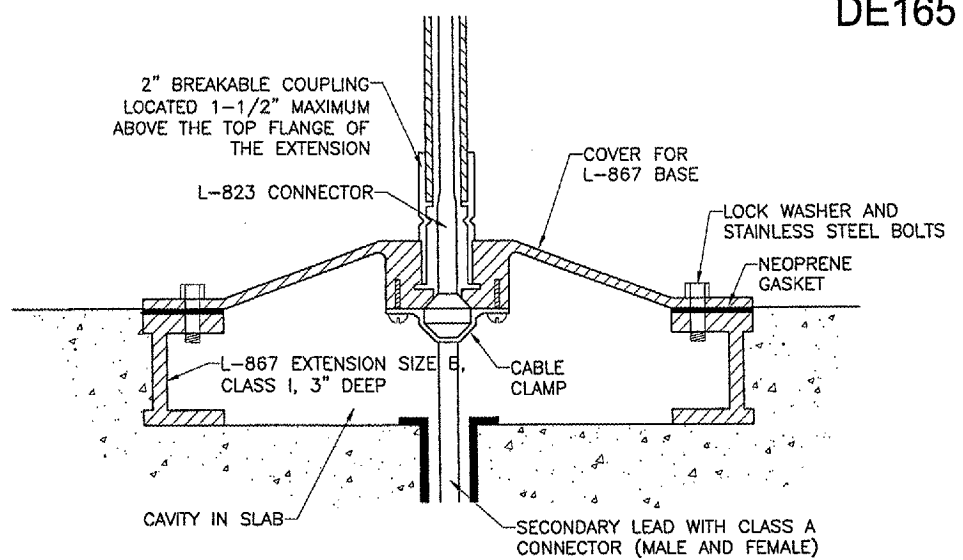


IN-PAVEMENT RUNWAY LIGHT
(NOT TO SCALE)

NOTE:
FLEXIBLE CONDUIT MAY BE CONNECTED TO THE BASE EITHER THROUGH A HUB OR GROMMET.



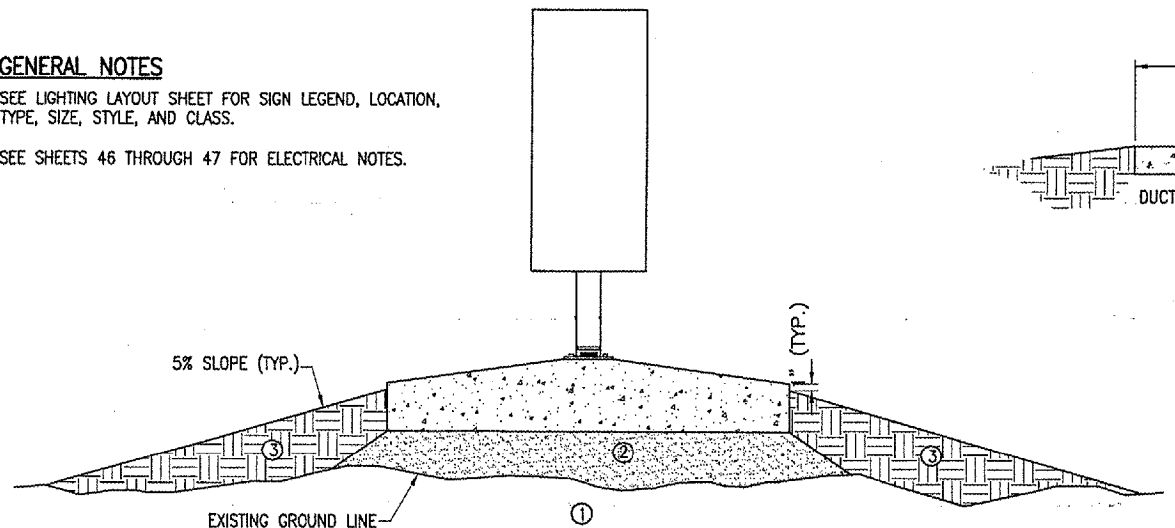
SPLICE CAN DETAIL
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DETAIL "A"
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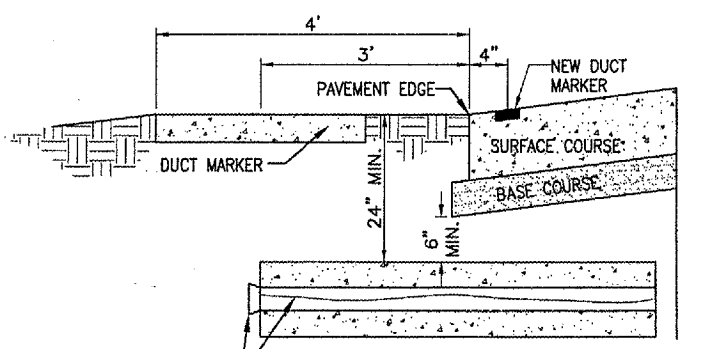
GENERAL NOTES

SEE LIGHTING LAYOUT SHEET FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.
SEE SHEETS 46 THROUGH 47 FOR ELECTRICAL NOTES.



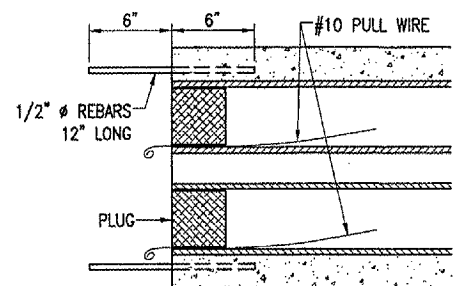
SIGN DETAIL
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- ① EXISTING SOD TO BE STRIPPED AND REMOVED
- ② SAND BACKFILL, VARIABLE DEPTH
- ③ PROPOSED TOPSOIL BACKFILL MATERIAL

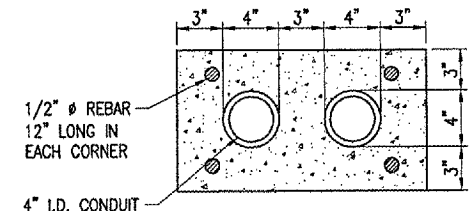


#10 PULL WIRE COIL A MINIMUM OF 3' AT DUCT ENDS. INSTALL APPROVED PLUGS IN END OF DUCTS NOT USED.

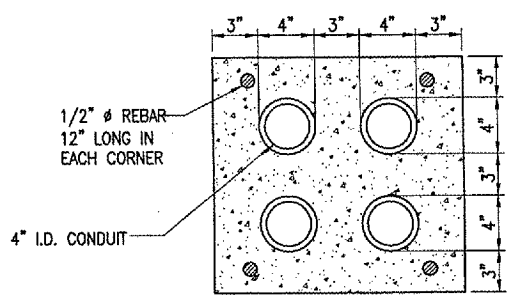
UNDERGROUND ELECTRICAL DUCT
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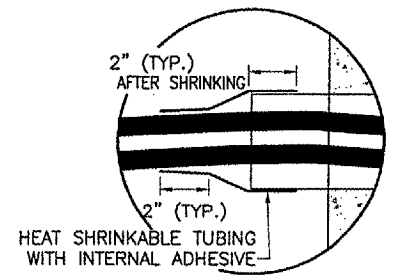
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2-DUCT BANK
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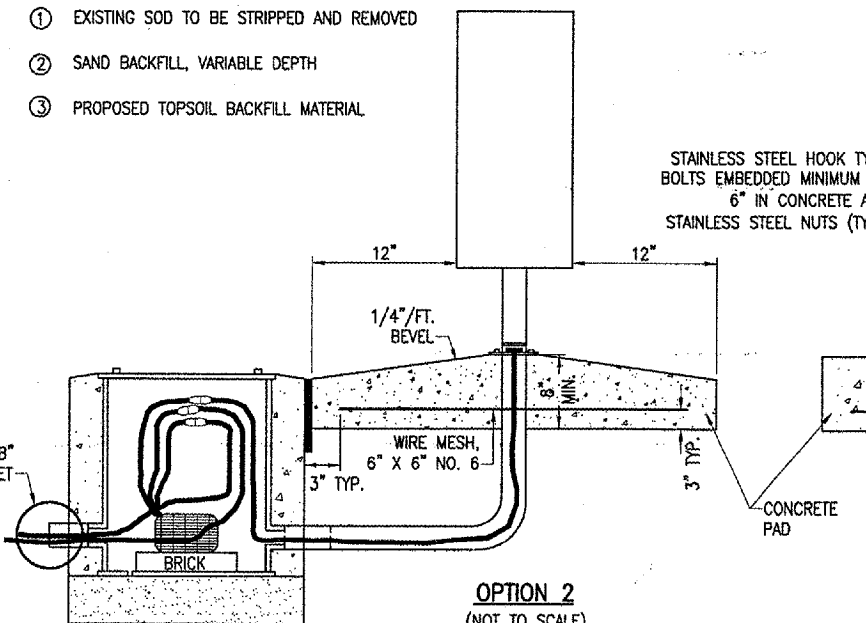


4-DUCT BANK
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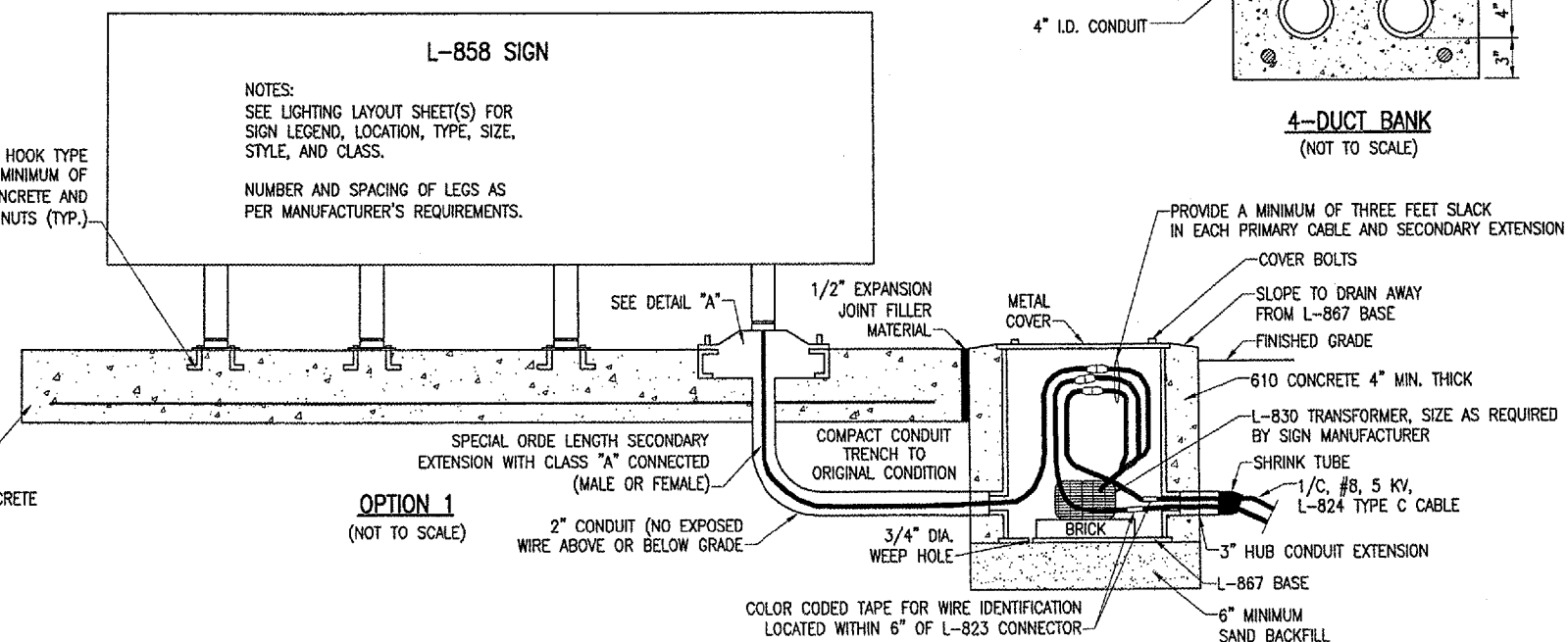


DETAIL "B"
(NOT TO SCALE)

SEE DETAIL "B" ON THIS SHEET



OPTION 2
(NOT TO SCALE)



OPTION 1
(NOT TO SCALE)

BY	
REVISION	
DATE	

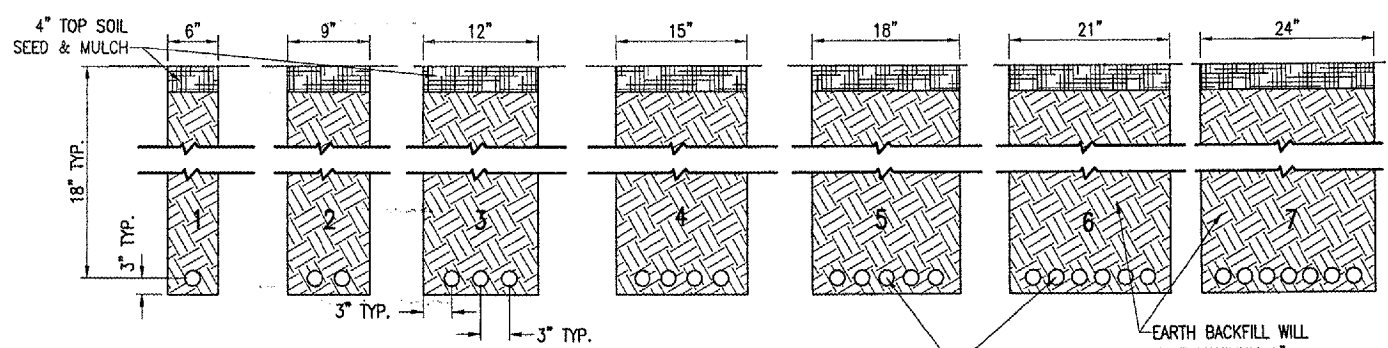
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REVIEWED	WJM 06/07/05

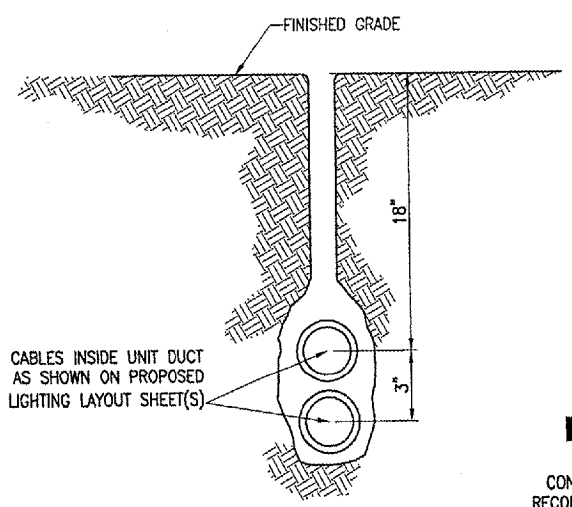
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TAXWAY G WIDENING
PHASE II
ELECTRICAL DETAILS

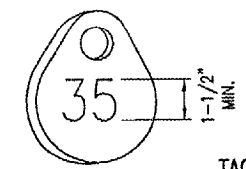
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NOTES:
 DETAIL NUMBERS INDICATE NO. OF CABLES.
 TRENCHES WITH MORE THAN SEVEN CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
 DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH.

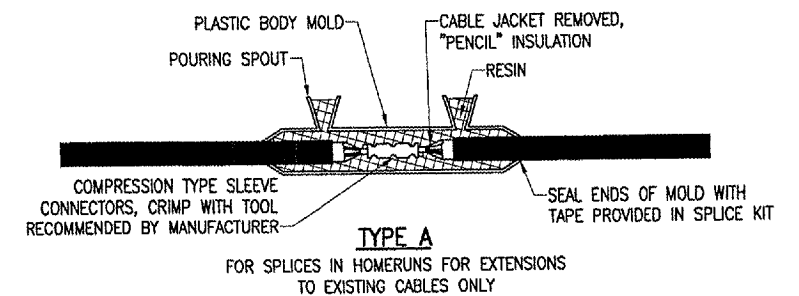


PLOWED CABLE
(NOT TO SCALE)



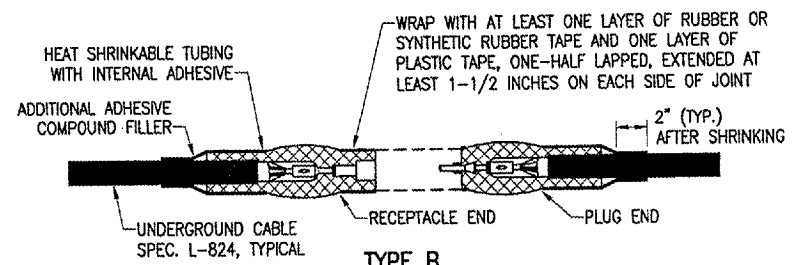
TAG DETAIL
(NOT TO SCALE)

NOTE:
 AFFIX NON-CORROSIVE TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY.



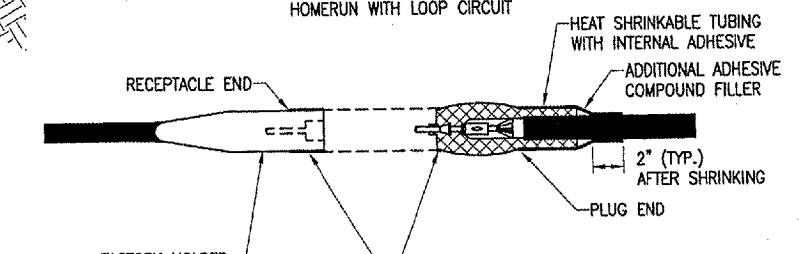
TYPE A

FOR SPLICES IN HOMERUNS FOR EXTENSIONS TO EXISTING CABLES ONLY



TYPE B

FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



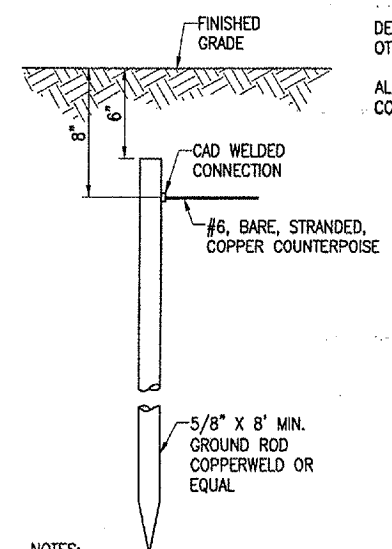
TYPE C

FOR SPLICES AT RUNWAY LIGHTS

NOTES:
 SEE PROPOSED LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE.
 INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.

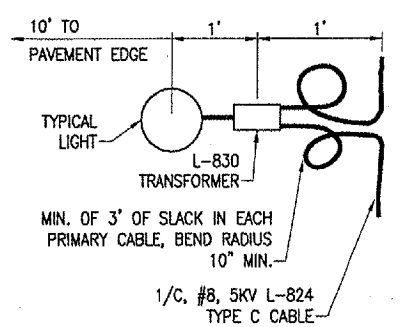
CABLE SPLICES
(NOT TO SCALE)

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

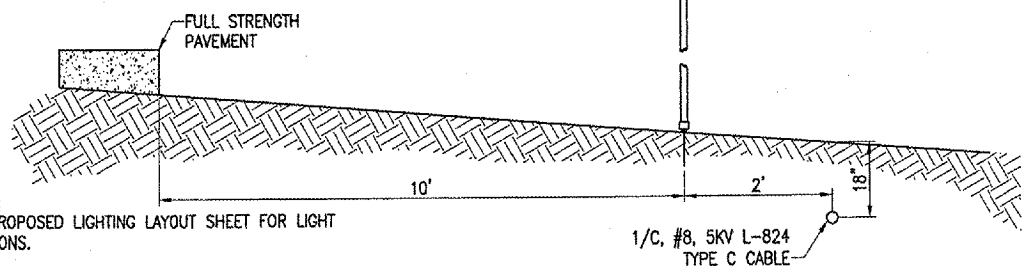


CABLE TRENCHES
(NOT TO SCALE)

NOTES:
 TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.
 THE RESISTANCE TO GROUND OF THE COUNTERPOISE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
 COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.



PLAN VIEW

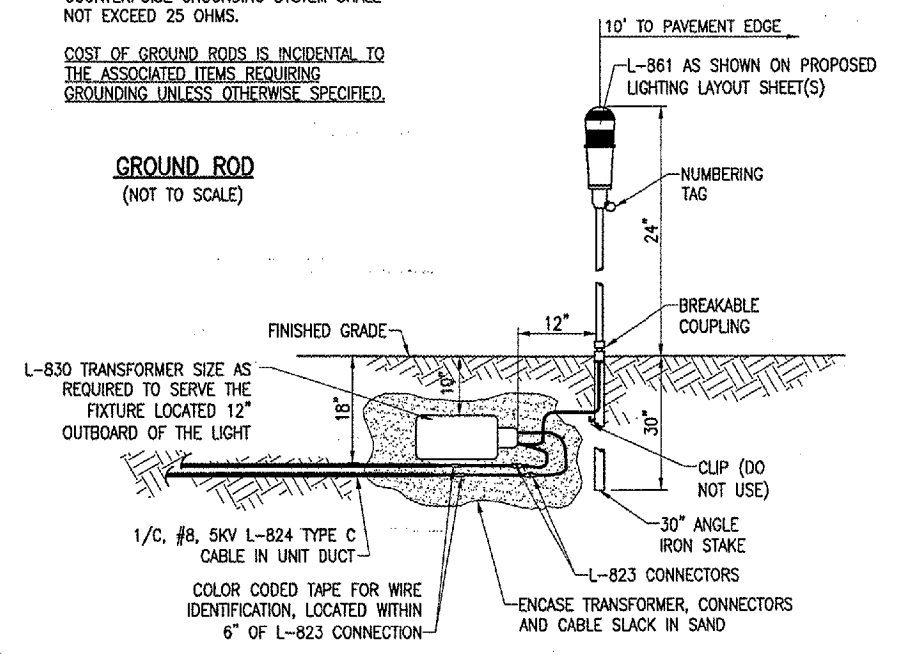


PROFILE VIEW

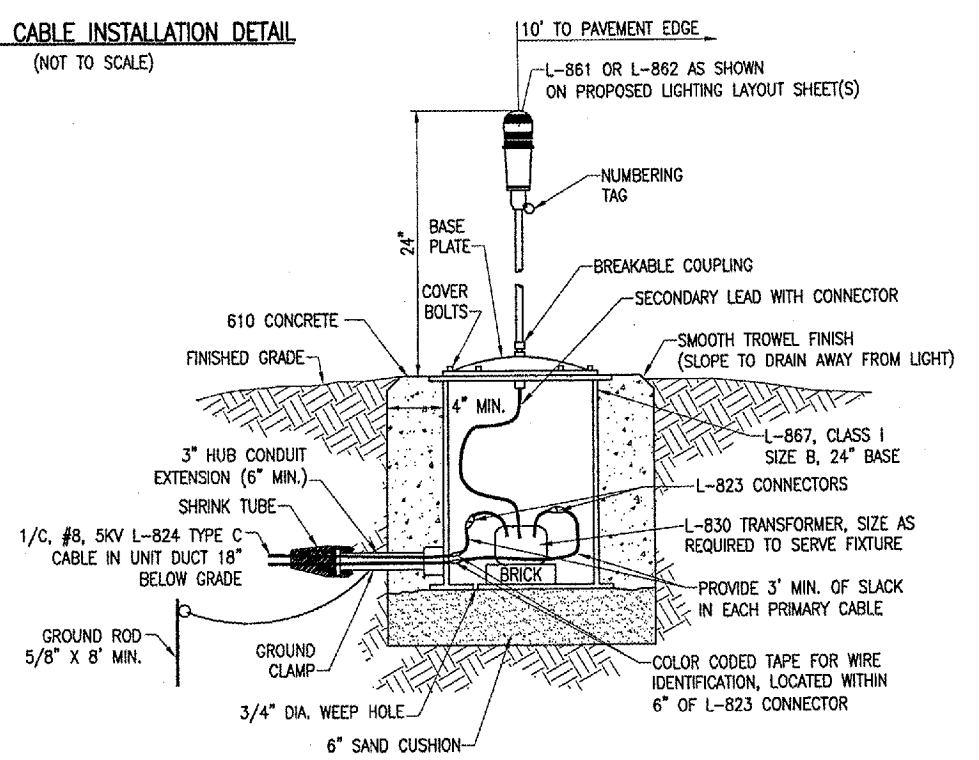
NOTES:
 SEE PROPOSED LIGHTING LAYOUT SHEET FOR LIGHT LOCATIONS.

LIGHT AND CABLE INSTALLATION DETAIL
(NOT TO SCALE)

GROUND ROD
(NOT TO SCALE)



MEDIUM INTENSITY LIGHT - STAKE MOUNTED
(NOT TO SCALE)



MEDIUM/HIGH INTENSITY LIGHT - BASE MOUNTED
(NOT TO SCALE)

BY	
REVISION	
DATE	

DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 ILL. PROJ.: DEC-3414

HEB Project No.	802-04TX0
Filename	R-541ELE.DWG
Scale	N/A
Date	03/08/05
LAYOUT	CCC 03/18/05
DRAWN	CCC 03/18/05
REVIEWED	WJM 06/07/05

HANSON
 Engineers, Architects, Scientists
 Springfield, Illinois 62703-2886
 Offices Nationwide

TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL DETAILS

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GENERAL

1. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE (LATEST RECOGNIZED VERSION) AND LOCAL REGULATIONS.
2. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM, INCLUDING FAA APPROVED EQUIPMENT, ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
3. IN CASE THE CONTRACTOR ELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTORS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATION, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
4. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
5. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES STYLES, CLASSES, ETC. MAY BE APPROVED.
6. ANY AND ALL INSTRUCTIONS FROM THE ENGINEER TO THE CONTRACTOR REGARDING CHANGES IN OR DEVIATIONS FROM THE PLANS AND SPECIFICATIONS SHALL BE IN WRITING WITH COPIES SENT TO THE AIRPORT SPONSOR AND THE FAA FIELD OFFICE (ADO/AFO). THE CONTRACTOR SHALL NOT ACCEPT ANY VERBAL INSTRUCTIONS FROM THE RESIDENT ENGINEER REGARDING ANY CHANGES FROM THE PLANS AND SPECIFICATIONS.
7. A MINIMUM OF THREE COPIES OF THE INSTRUCTION BOOK SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, PAPI, REIL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
 - A. A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
 - B. THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
 - C. INSTALLATION INSTRUCTIONS.
 - D. START-UP INSTRUCTIONS.
 - E. PREVENTATIVE MAINTENANCE REQUIREMENTS.
 - F. CHART FOR TROUBLE-SHOOTING.
 - G. COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT - "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OF THE NARRATIVE SHALL SHOW VOLTAGE/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLE-SHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL DIFFERENT MODES.
 - H. PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
 - I. SAFETY INSTRUCTIONS.

POWER AND CONTROL

1. STENCIL ALL ELECTRICAL EQUIPMENT TO IDENTIFY FUNCTION, CIRCUIT VOLTAGE AND PHASE. WHERE THE EQUIPMENT CONTAINS FUSES, ALSO STENCIL THE FUSE OR FUSE LINK AMPERE RATING. WHERE THE EQUIPMENT DOES NOT HAVE SUFFICIENT STENCILING AREA, THE STENCILING SHALL BE DONE ON THE WALL NEXT TO THE UNIT. THE LETTERS SHALL BE ONE INCH HIGH AND PAINTED IN WHITE OR BLACK TO PROVIDE THE HIGHEST CONTRAST WITH THE BACKGROUND.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK, BLACK AND RED SHALL BE USED FOR SINGLE-PHASE, THREE WIRE SYSTEMS AND BLACK, RED AND BLUE SHALL BE USED FOR THREE-PHASE SYSTEMS. NEUTRAL CONDUCTORS, SIZE NO. 6 AWG OR SMALLER, SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS LARGER THAN NO. 6 AWG SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE WIREWAYS.
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
5. LOW VOLTAGE (600 V.) AND HIGH VOLTAGE (5000 V.) CONDUCTORS SHALL BE INSTALLED IN SEPARATE WIREWAYS.
6. NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
7. THE MINIMUM SIZE OF PULL/JUNCTION BOXES, REGARDLESS OF THE QUANTITY AND SIZE OF THE CONDUCTORS SHOWN, SHALL BE AS FOLLOWS:
 - A. IN STRAIGHT PULLS THE LENGTH OF THE BOX SHALL NOT BE LESS THAN EIGHT TIMES THE TRADE DIAMETER OF THE LARGER CONDUIT. THE TOTAL AREA (INCLUDING THE CONDUIT CROSS-SECTIONAL AREA) OF A BOX END SHALL BE AT LEAST 3 TIMES GREATER THAN THE TOTAL TRADE CROSS-SECTIONAL AREA OF THE CONDUITS TERMINATING AT THE END.
 - B. IN ANGLE PULLS OR 'U' PULLS THE DISTANCE BETWEEN EACH CONDUIT ENTRY INSIDE THE BOX AND THE OPPOSITE WALL OF THE BOX SHALL NOT BE LESS THAN SIX (6) TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT. THIS DISTANCE SHALL BE INCREASED FOR ADDITIONAL ENTRIES BY THE AMOUNT OF THE SUM OF THE DIAMETERS OF ALL OTHER CONDUIT ENTRIES ON THE SAME WALL AS THE BOX. THE DISTANCE BETWEEN CONDUIT ENTRIES ENCLOSING THE SAME CONDUCTOR SHALL NOT BE LESS THAN SIX TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT.
8. A RUN OF CONDUIT BETWEEN TERMINATIONS AT EQUIPMENT ENCLOSURES, SQUARE DUCTS AND PULL/JUNCTION BOXES, SHALL NOT CONTAIN MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL), INCLUDING THOSE BENDS LOCATED IMMEDIATELY AT THE TERMINATIONS, CAST, CONDUIT TYPE OUTLETS SHALL NOT BE TREATED AS PULL/JUNCTION BOXES.
9. EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
10. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
11. CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC MOLDED CASE, PERMANENT TRIP WITH 100 AMPERE, MINIMUM FRAME.
12. DUAL LUGS SHALL BE USED WHERE TWO (2) WIRES, SIZE NO. 6 OR LARGER, ARE TO BE CONNECTED TO THE SAME TERMINAL.
13. ALL WALL MOUNTED EQUIPMENT ENCLOSURES SHALL BE MOUNTED ON WOODEN MOUNTING BOARDS.
14. WOODEN EQUIPMENT MOUNTING BOARDS SHALL BE PLYWOOD, EXTERIOR TYPE, 3/4 INCH, MINIMUM, THICKNESS, BOTH SIDES PAINTED WITH ONE COAT OF PRIMER AND TWO COATS OF GRAY OIL-BASED PAINT.
15. RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED. THE MINIMUM TRADE SIZE SHALL BE 3/4 INCH.
16. ALL RIGID CONDUIT SHALL BE TERMINATED AT CONSTANT CURRENT REGULATORS WITH A SECTION (10" MINIMUM) OF FLEXIBLE CONDUIT.
17. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
18. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC. SHALL BE GALVANIZED.
19. USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE NO. 4 AWG OR LARGER UNDERGROUND WIRE IS INSTALLED, USE INSULATED BUSHINGS.
20. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
21. WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE AND COVER WITH INSULATING VARNISH FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
22. UNLESS OTHERWISE NOTED, ALL INDOOR SINGLE CONDUCTOR CONTROL WIRING SHALL BE NO. 12 AWG. MINIMUM.
23. THE FOLLOWING SHALL APPLY TO RELAY/CONTACTOR PANELS/ENCLOSURES:
 - A. ALL COMPONENTS SHALL BE MOUNTED IN DUST PROOF ENCLOSURE(S) WITH VERTICALLY HINGED COVERS.
 - B. THE ENCLOSURE(S) SHALL HAVE AMPLE SPACE FOR THE CIRCUIT COMPONENTS, TERMINAL BLOCKS AND INCOMING AND INTERNAL WIRING.
 - C. ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITHOUT CONNECTORS ARE NOT ACCEPTABLE.
 - D. WHEN THE ENCLOSURE COVER IS OPENED, ALL CIRCUIT COMPONENTS, WIRING AND TERMINALS SHALL BE EXPOSED AND ACCESSIBLE WITHOUT REMOVAL OF ANY PANELS, COVERS, ETC., EXCEPT THOSE COVERING HIGH VOLTAGE COMPONENTS.
 - E. ACCESS TO, OR REMOVAL OF A CIRCUIT COMPONENT OR TERMINAL BLOCK WILL NOT REQUIRE THE REMOVAL OF ANY OTHER CIRCUIT COMPONENT OR TERMINAL BLOCK.
 - F. EACH CIRCUIT COMPONENT SHALL BE CLEARLY IDENTIFIED INDICATING ITS CORRESPONDING NUMBER SHOWN ON THE DRAWINGS AND ITS FUNCTION.
 - G. A COMPLETE WIRING DIAGRAM (NOT A SCHEMATIC DIAGRAM) SHALL BE MOUNTED ON THE INSIDE OF THE COVER. THE DIAGRAM SHALL REPRESENT EACH CONDUCTOR BY A SEPARATE LINE.
 - H. THE DIAGRAM SHALL IDENTIFY EACH CIRCUIT COMPONENT AN NUMBERING AND COLOR OF EACH TERMINAL CONDUCTOR AND TERMINAL.
 - I. ALL WIRING SHALL BE NEATLY TRAINED AND LACED.
 - J. MINIMUM WIRE SIZE SHALL BE NO. 12 AWG.

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I&E Project No. 802-041XX File Name: R-541ELE.DWG Scale: N/A Date: 03/08/05	LAYOUT DRAWN REVIEWED	CCC CCC WJM	03/18/05 03/18/05 06/07/05
 HANSON Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62703-2898 Offices Nationwide		TAXIWAY G WIDENING PHASE II ELECTRICAL NOTES	
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FIELD LIGHTING NOTES



1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED L-824 TYPE. INSULATION VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE-GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT.
5. THE CABLE ENTRANCE INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS 'A' (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.
15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.

GROUNDING NOTES

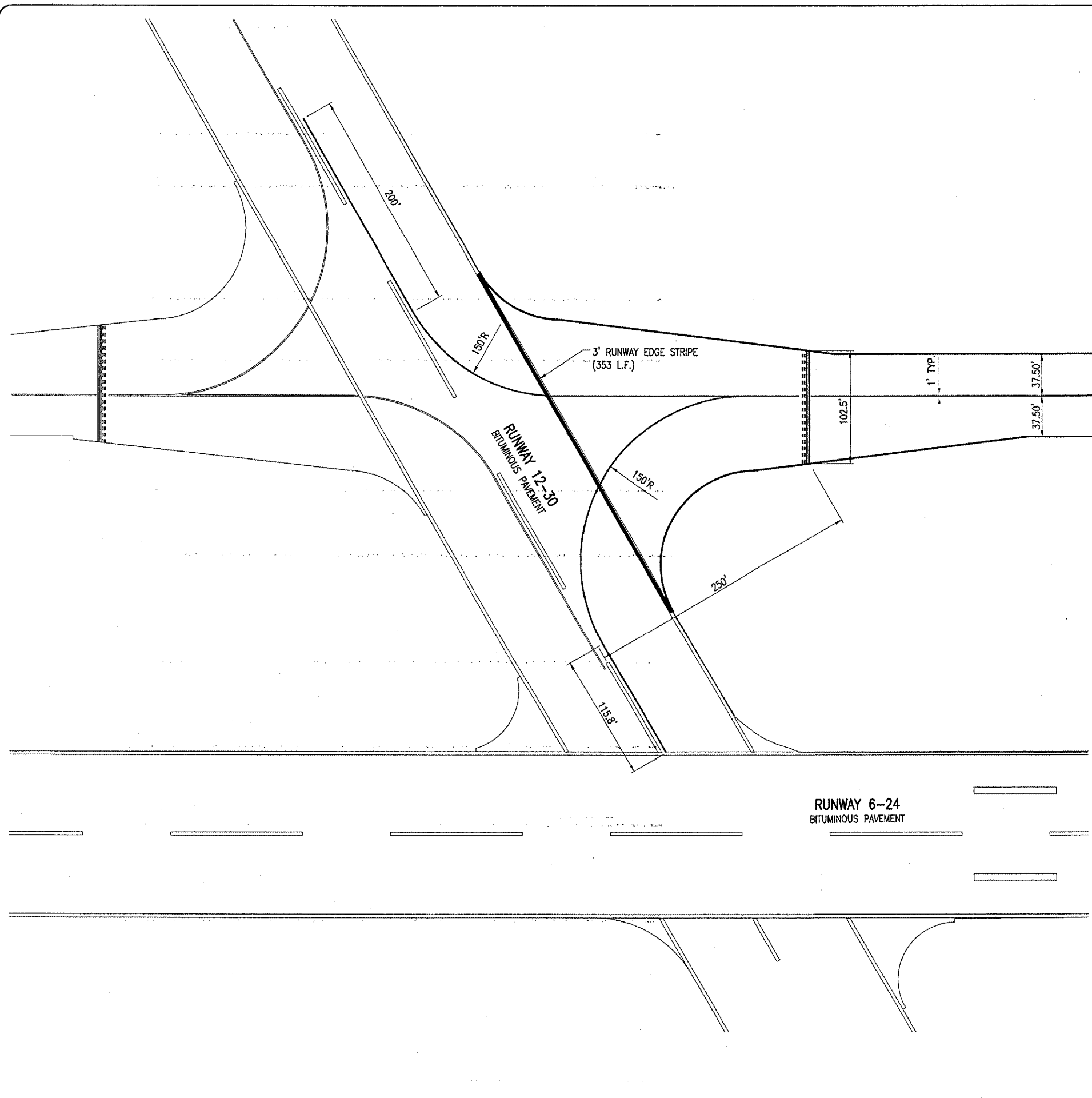
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FLANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.
20. ENTRANCES INTO L-867 BASES SHALL BE SEALED WITH HEAT SHRINK.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "T" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3000 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE-ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.

1. ALL GROUND CONNECTIONS TO GROUND RODS, BUSES, PANELS, ETC. SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUGS AND GROUND CLAMPS SOLDERED OR BOLT AND WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS.
2. TOP OF GROUND RODS SHALL BE TEN (10) INCHES BELOW GRADE.
3. THE RESISTANCE TO GROUND OF THE VAULT GROUNDING SYSTEM WITH THE COMMERCIAL POWER LINE NEUTRAL DISCONNECTED SHALL NOT EXCEED 10 OHMS.

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DATE	REVISION			
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ILL. PROJECT NO.: 802-041XYD ELEVATION: R-54 ELE.DWG SCALE: N/A DATE: 03/08/05		LAYOUT: CCC DRAWN: CCC REVIEWED: WJM	DATE: 03/18/05 DATE: 03/18/05 DATE: 06/07/05	
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MARKING QUANTITIES	
DESCRIPTION	TOTAL AREA (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	5,797
TAXIWAY HOLDING POSITIONS - YELLOW	1,088
TOTAL YELLOW:	6,885
RUNWAY EDGE STRIPE - WHITE	2,624
TOTAL WHITE:	2,624
HOLDING POSITION MARKINGS - BLACK	1,812
TAXIWAY CENTERLINE - BLACK	2,627
RUNWAY EDGE STRIPE - BLACK	158
TOTAL BLACK:	4,597
TOTAL MARKING:	14,106

MARKING REMOVAL QUANTITIES	
DESCRIPTION	TOTAL AREA (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	868

MARKING NOTES

ALL RUNWAY MARKING WILL BE SOLID AND WHITE IN COLOR.

THE TAXIWAY CENTERLINE STRIPE AND HOLDING POSITION MARKING WILL BE SOLID AND YELLOW IN COLOR. THE HOLDING POSITION MARKING WILL BE AS SPECIFIED IN THE DETAIL SHOWN ON THE FOLLOWING SHEET.

ALL PROPOSED MARKING ON OR ADJACENT TO CONCRETE PAVEMENT WILL BE OUTLINED WITH A 6" WIDE (BLACK) STRIPE.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THE PROPOSED MARKING PLAN SHEETS.

ALL YELLOW & WHITE PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

ALL MARKING ON EXISTING AND PROPOSED CONCRETE PAVEMENT AND HOLDING LINES SHALL HAVE A 6 INCH WIDE (MIN.) BLACK OUTLINE.

THE EXISTING HOLD LINES ON CONCRETE PAVEMENT WILL BE PAINTED OVER WITH BLACK PAINT PRIOR TO PLACING THE NEW HOLD LINES ON TAXIWAY "G3" AND TAXIWAY "G4".

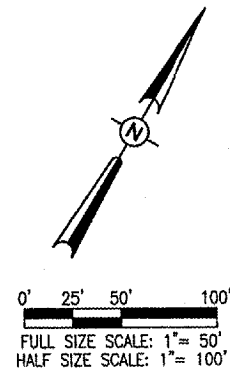
ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620520 "PAVEMENT MARKING-WATERBORNE" _____ PER SQ. FT.

WHERE CALLED FOR, THE CENTERLINE TAXIWAY WILL BE REMOVED BY SAND BLASTING OR WATER BLAST METHOD.

ALL MARKING REMOVAL ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620900 "PAVEMENT MARKING REMOVAL" _____ PER SQ. FT.

LEGEND

	EXISTING PAVEMENT
	PROPOSED PAVEMENT
	EXISTING MARKING
	PROPOSED MARKING



DE165

MATCHLINE STA. 378+00

DATE	REVISION	BY

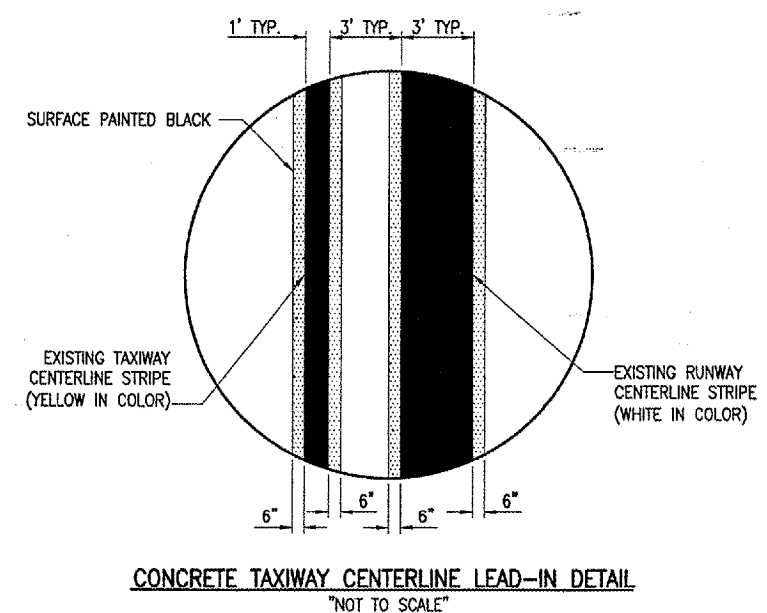
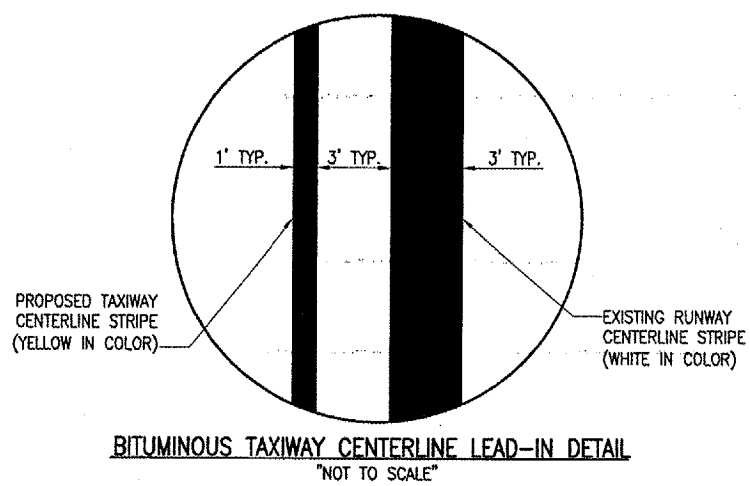
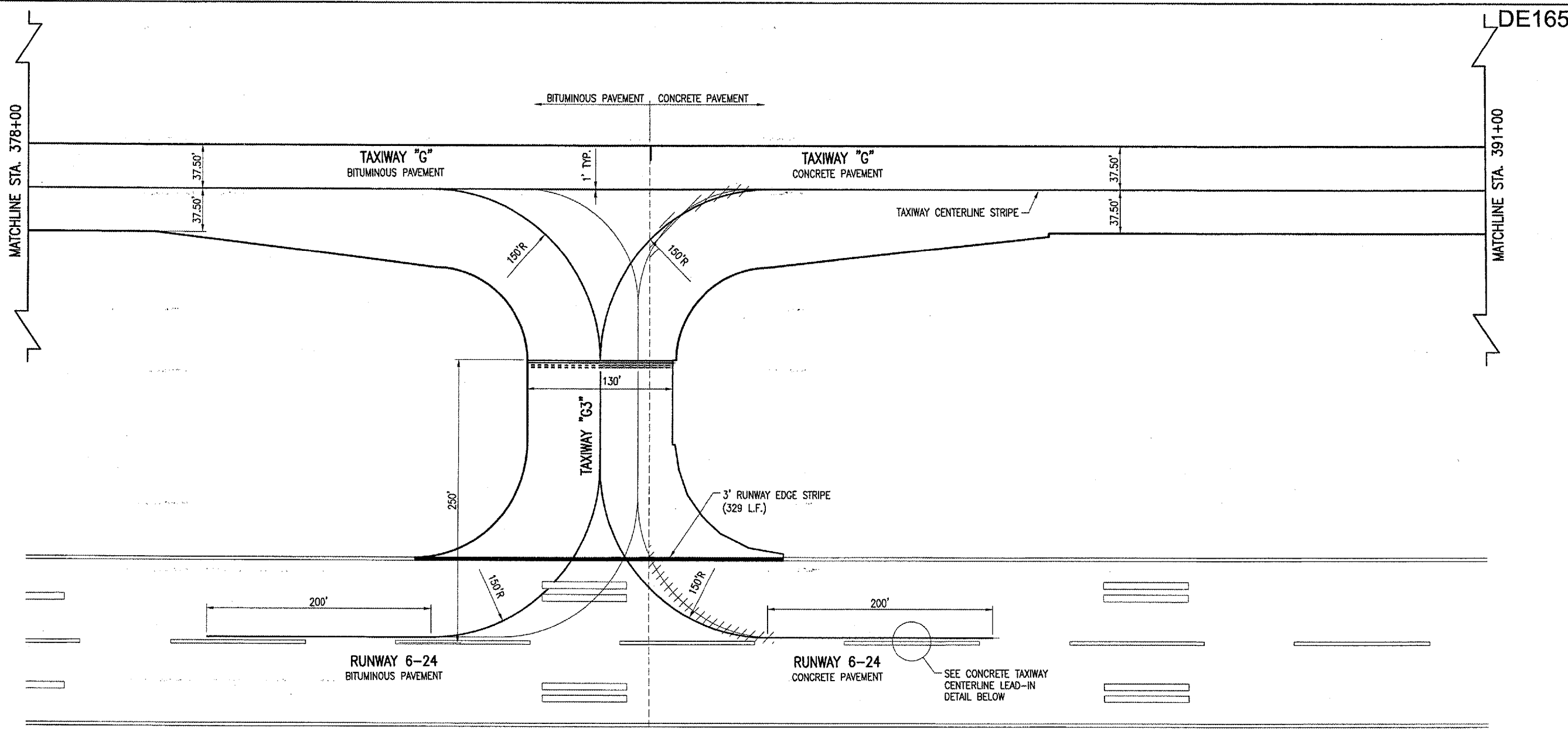


ILL. Project No. 802-04TXD	CCC	03/18/05
Drawings R-151MRK.DWG	CCC	03/18/05
Scale 1"=50'	WJM	06/07/05
Date 03/18/05		
LAYOUT		
DRAWN		
REVIEWED		

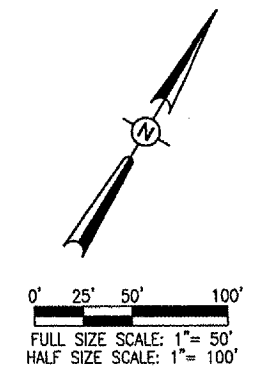


TAXIWAY G WIDENING
 PHASE II
 PROPOSED
 MARKING PLAN
 STA. 367+86.06 TO STA. 378+00

JUN 10, 2005 9:39 AM CCC
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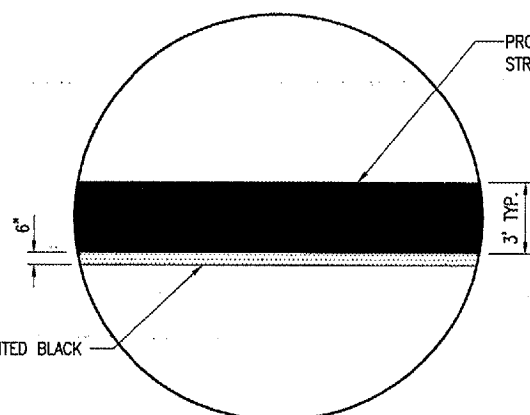
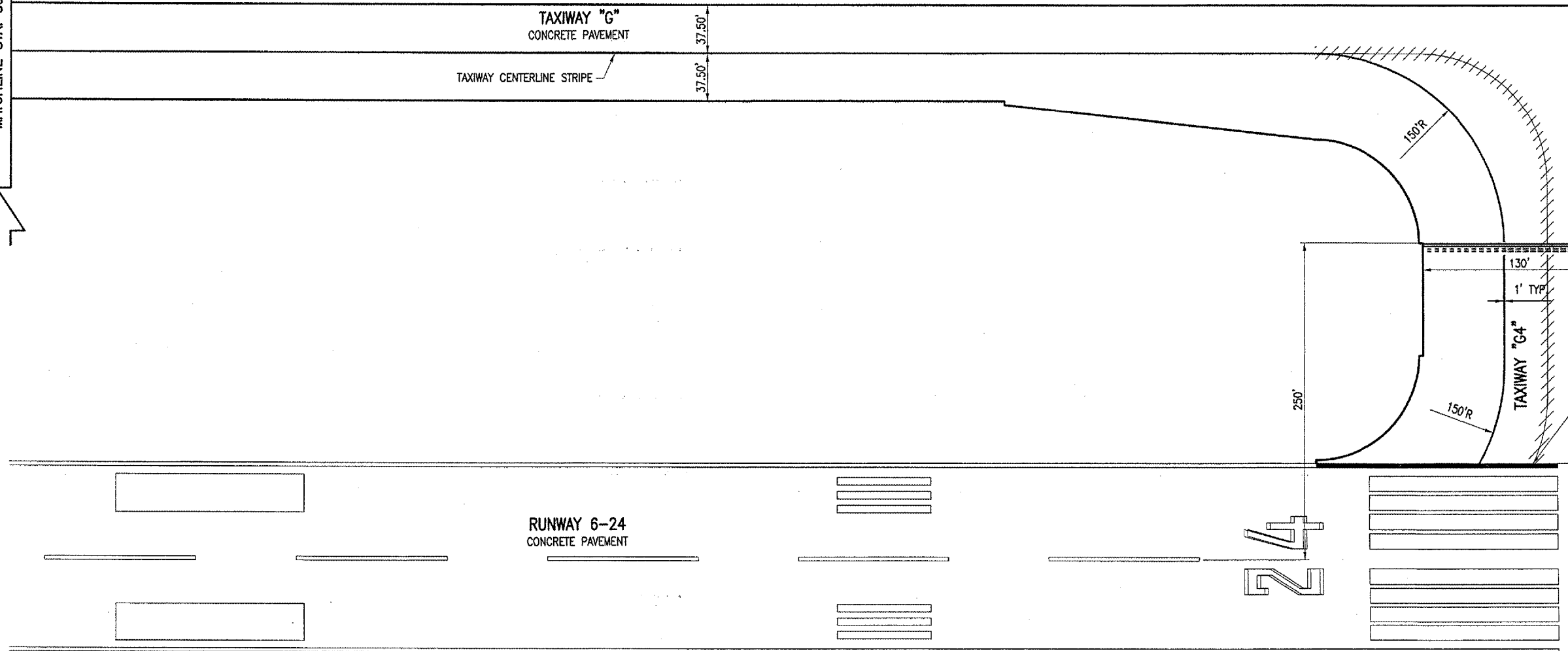


- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PAVEMENT
 - EXISTING MARKING
 - PROPOSED MARKING
 - MARKING TO BE REMOVED

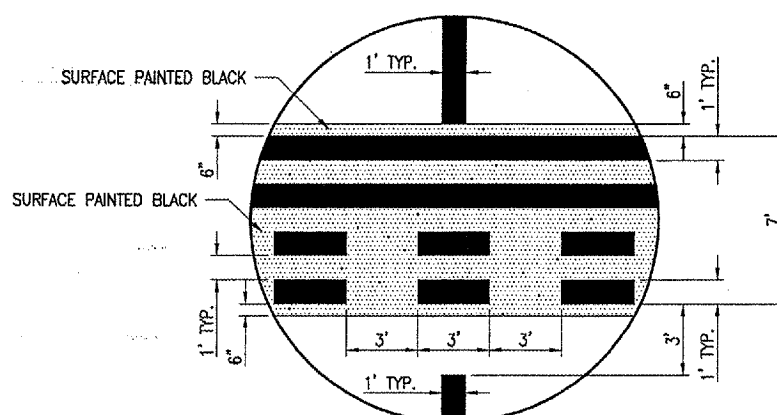


	BY				
	REVISION				
	DATE				
 DECATUR, ILLINOIS					
I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28					
I&E Project No. 802-04TXD Element R-151MRK.DWG Scale 1"=50' Date 03/18/05	LAYOUT CCC 03/18/05 DRAWN CCC 03/18/05 REVIEWED WJM 06/07/05	 HANSON Engineers, Architects, Scientists 1625 West Lake Street Springfield, Illinois 62703-2886 Offices Nationwide			
TAXIWAY G WIDENING PHASE II		PROPOSED MARKING PLAN STA. 378+00 TO STA. 391+00			
49					
49 of 110 sheets					

MATCHLINE STA. 391+00

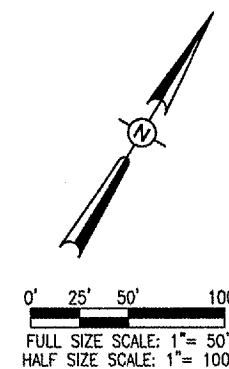


CONCRETE RUNWAY EDGE STRIPE DETAIL
"NOT TO SCALE"



HOLDING POSITION DETAIL
"NOT TO SCALE"

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED PAVEMENT
 - EXISTING MARKING
 - PROPOSED MARKING
 - MARKING TO BE REMOVED



DATE	REVISION	BY

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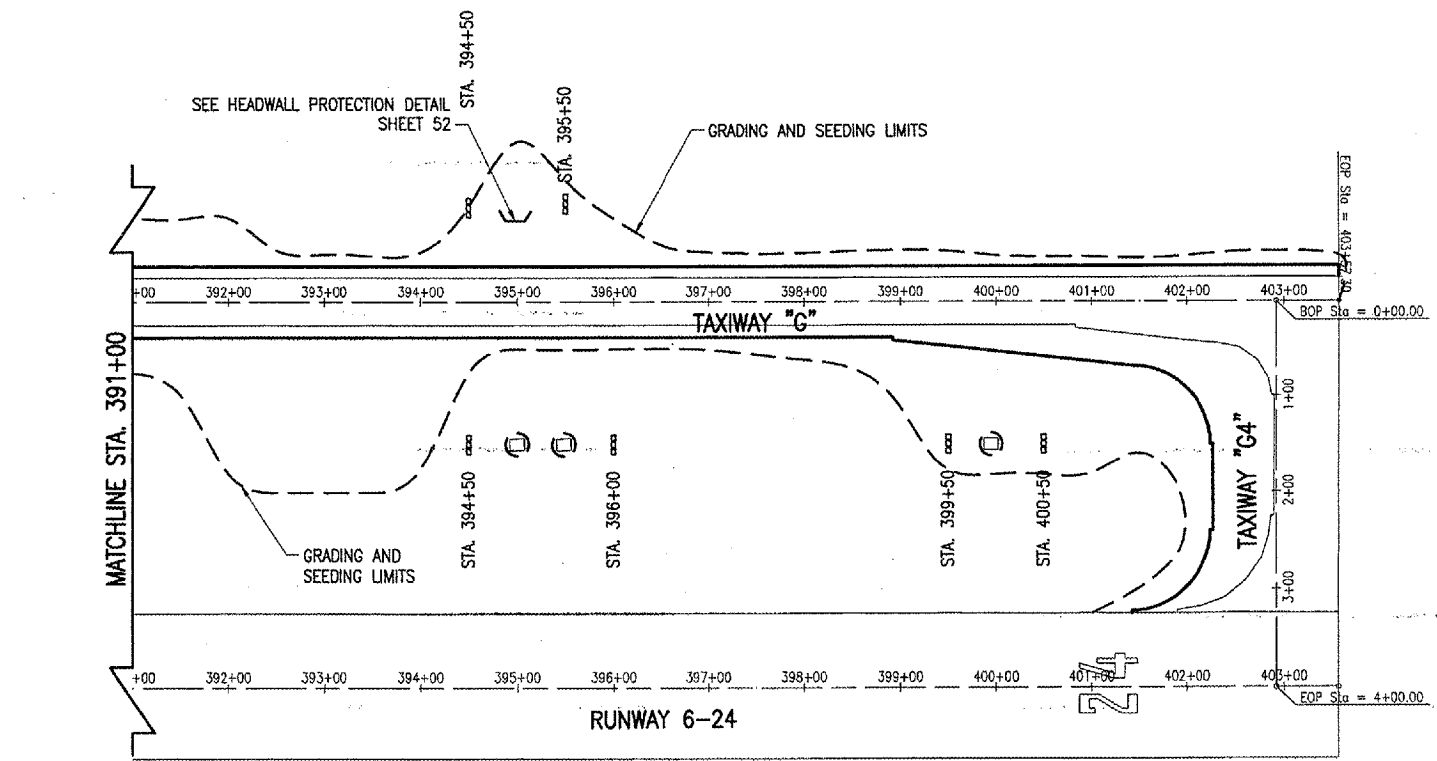
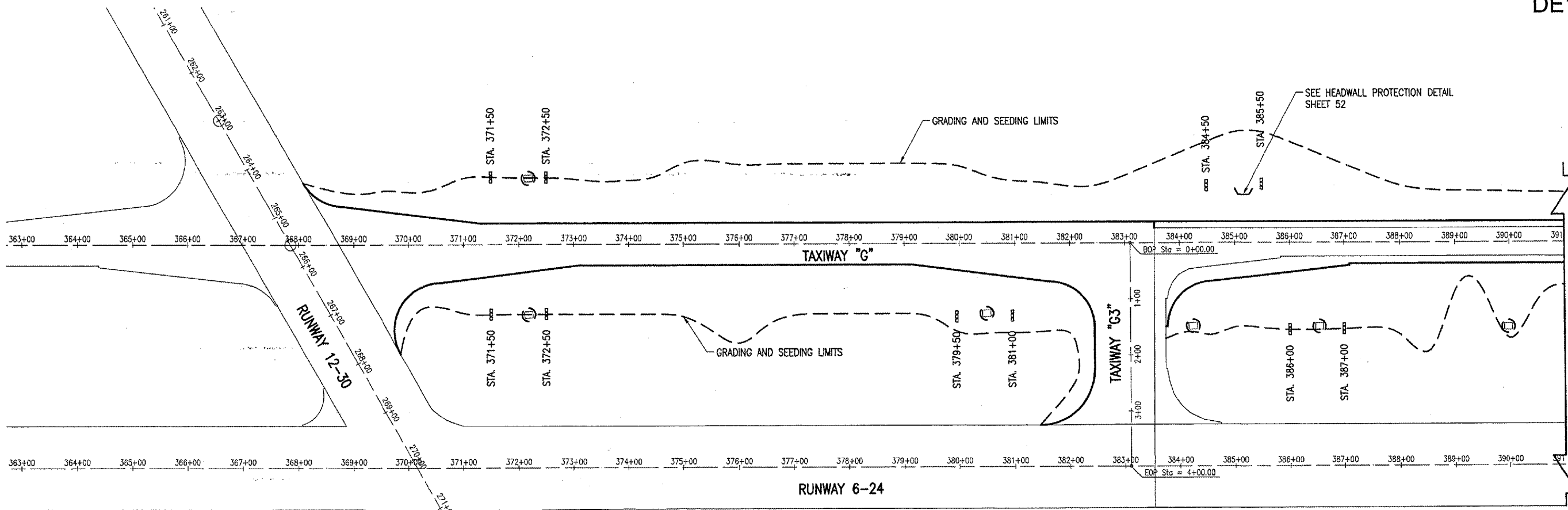
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NO.	DATE	BY	REVIEWED
1	03/18/05	CCC	CCC
2	03/18/05	CCC	CCC
3	06/07/05	WJM	WJM

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED
 MARKING PLAN
 STA. 391+00 TO STA. 403+57.30

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SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR156511	DITCH CHECK	EACH	16
AR156520	INLET PROTECTION	EACH	9
AR156521	HEADWALL PROTECTION	EACH	2
AR156540	RIPRAP	S.Y.	74
AR901510	SEEDING	ACRE	12
AR908510	MULCHING	ACRE	12

SEEDING DATA

THE GRADING LIMITS ARE SHOWN ON THIS SHEET BY A HEAVY DASHED LINE (---). ALL AREAS WITHIN THESE LIMITS, EXCEPT THE PROPOSED PAVEMENT, SHALL BE LIMED, FERTILIZED AND SEEDING IN ACCORDANCE WITH THE FOLLOWING FORMULA AND RATES OF APPLICATION.

LIME (TONS PER ACRE) _____ 2
 FERTILIZER (MINIMUM POUNDS OF AVAILABLE PLANT FOOD PER ACRE)

N	90
P ₂ O ₅	350
K ₂ O	240
TOTAL	680

SEEDING (MINIMUM POUNDS OF PURE LIVE SEED PER ACRE)
 ALTA FESCUE _____ 80
 PERENNIAL RYEGRASS _____ 30
 CREEPING RED FESCUE _____ 20
 SPRING OATS _____ 48
 AREA TO BE SEEDING _____ 12 ACRES
 DATE SEEDING COMPLETED _____

MULCHING DATA

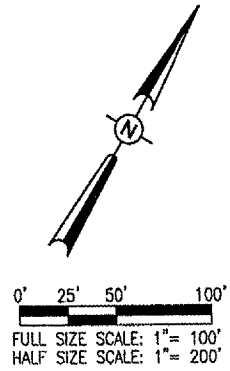
ALL EARTHEN AREAS WITHIN THE GRADING LIMITS WILL BE MULCHED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS AT THE RATE OF 2 TONS PER ACRE.
 AREA TO BE MULCHED _____ 12 ACRES
 DATE MULCHING COMPLETED _____

NOTE

REFER TO THE CROSS-SECTIONS FOR GRADING AND SEEDING MATCH POINTS.

LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- PROPOSED GRADING AND SEEDING LIMITS
- PROPOSED INLET PROTECTION
- PROPOSED PIPE EXTENSION W/ HEADWALL
- DITCH CHECK



DATE	REVISION	BY

HA PROJECT NO. 802-041XXD
 DRAWING NO. 181SWP.DWG
 SCALE: 1" = 100'
 DATE: 04/15/05

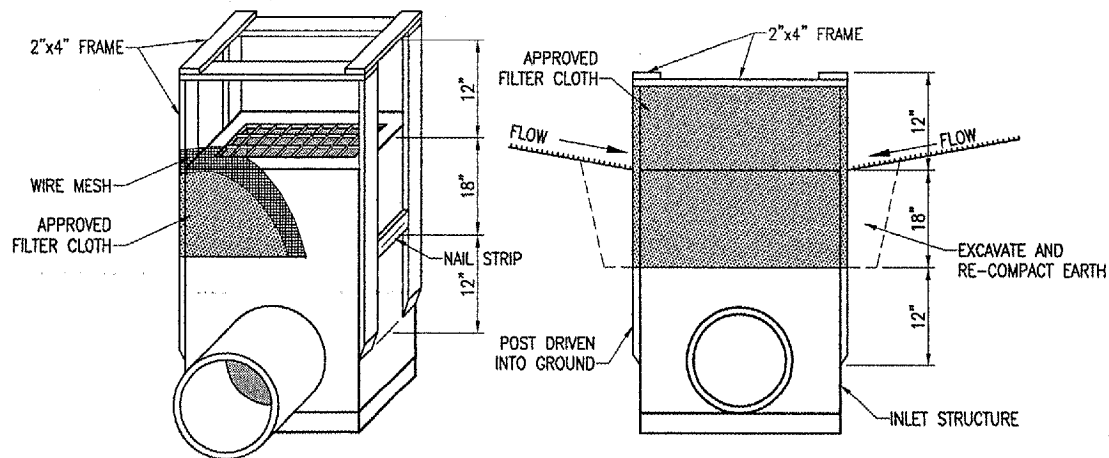
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DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 I.L. PROJ.: DEC-3414

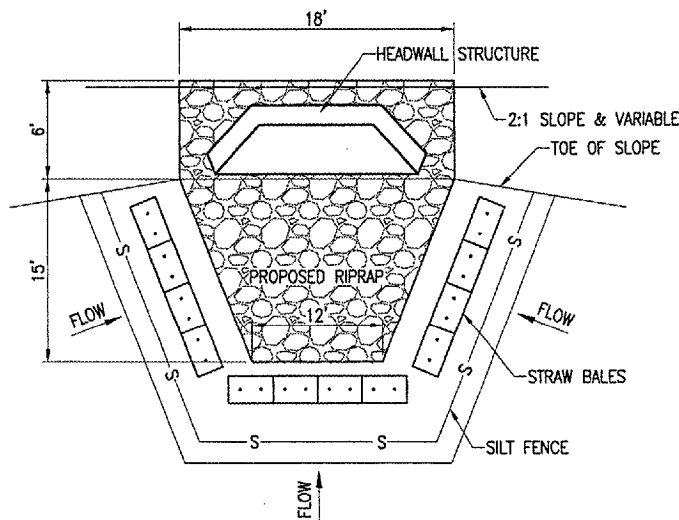
LAYOUT	WJM	DPE	WJM
	04/15/05	04/15/05	06/07/05
DRAWN			
REVIEWED			

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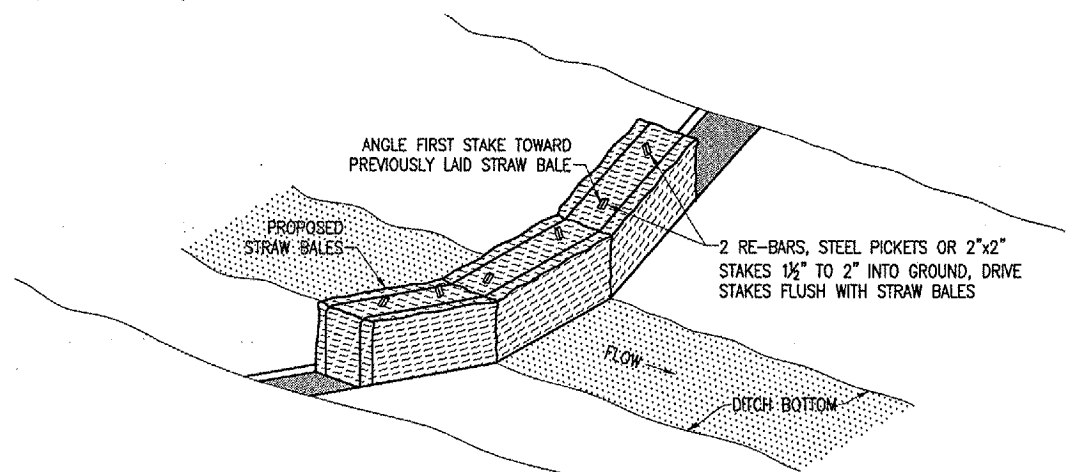
TAXIWAY G WIDENING
 PHASE II
 PROPOSED GRADING, SEEDING
 LIMITS, & EROSION CONTROL
 STA. 367+85.57 TO 403+57.30



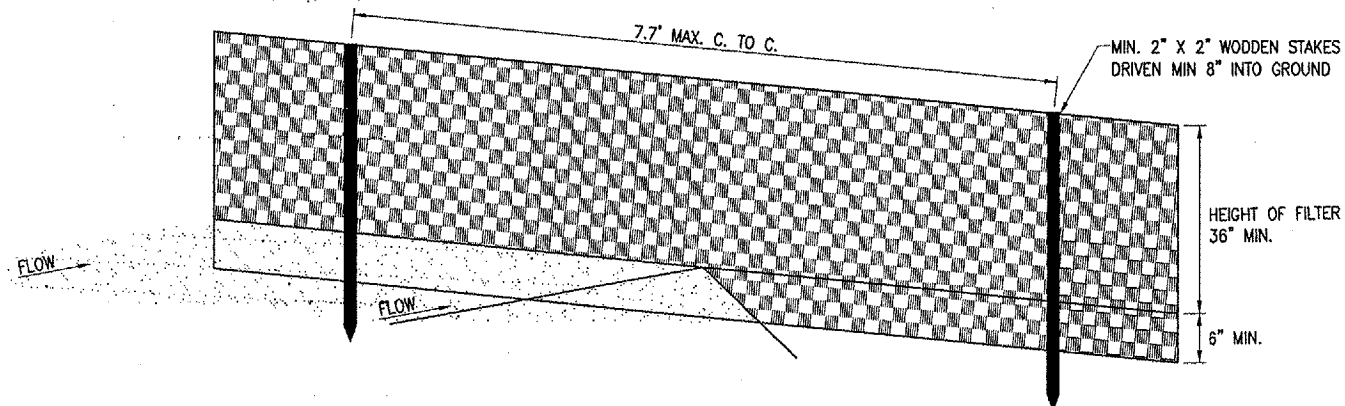
INLET PROTECTION DETAIL
"NOT TO SCALE"



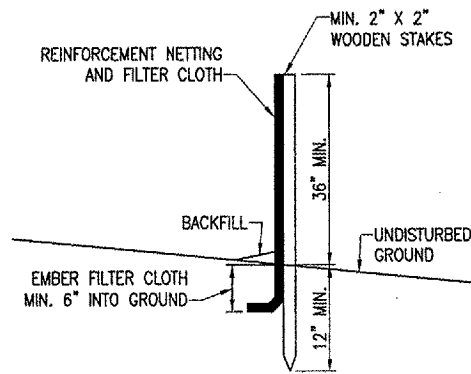
HEADWALL STRUCTURE PROTECTION
"NOT TO SCALE"



DITCH CHECK DETAIL
"NOT TO SCALE"



PERSPECTIVE VIEW



SECTION

SILT FENCE DETAIL
"NOT TO SCALE"

HEADWALL STRUCTURE PROTECTION NOTES

THE PROPOSED HEADWALL STRUCTURE PROTECTION WILL BE CONSTRUCTED ACCORDING TO THE DETAILS SHOWN ON THIS SHEET AND WILL INCLUDE STRAW BALES AND SILT FENCE.

THE RIPRAP QUANTITY IN THE HEADWALL STRUCTURE PROTECTION WILL BE PAID SEPERATELY BY ITEM: AR156540 "RIPRAP."

THE HEADWALL STRUCTURE PROTECTION WILL BE PAID FOR UNDER ITEM: AR156521 "HEADWALL PROTECTION" 2 EACH.

RIPRAP NOTES FOR HEADWALL STRUCTURE PROTECTION

THE PROPOSED RIPRAP WILL BE PLACED TO THE DIMENSIONS AS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

INSTALLATION OF FILTER FABRIC WILL BE REQUIRED UNDER THE STONE RIPRAP. THE FILTER FABRIC MATERIAL WILL BE INCLUDED AS PART OF THE RIPRAP INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE BED OF THE RIPRAP WILL BE 12" IN DEPTH AND SHALL BE TRIMMED AND SHAPED TO CONFORM WITH THE FINISHED GRADES.

THE RIPRAP WILL BE PAID FOR UNDER ITEM: AR156540 "RIPRAP" 74 S.Y.

INLET PROTECTION NOTES

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS SHOWN AND DETAILED ON THIS SHEET.

THE PROPOSED INLET PROTECTION WILL PREVENT ANY EROSION TO ENTER THE PROPOSED AND EXISTING STORM SEWER SYSTEM.

THE EROSION CONTROL MEASURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SEDIMENT CONTROL.

THE SILT FENCE WILL BE PAID FOR UNDER THE INLET PROTECTION ITEM.

THE EROSION CONTROL MEASURES WILL BE PAID FOR UNDER ITEM: AR156520 "INLET PROTECTION" 9 EACH.

DITCH CHECK NOTES:

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS SHOWN AND DETAILED ON THIS SHEET.

THE DITCH CHECK LOCATIONS ARE AS SPECIFIED ON SHEET 51.

THE EROSION CONTROL MEASURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SEDIMENT CONTROL.

THE DITCH CHECKS WILL BE PAID FOR UNDER ITEM: AR156511 "DITCH CHECK" 16 EACH.

EROSION CONTROL NOTES

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS DETAILED ON THIS EROSION CONTROL PLAN AND IN ACCORDANCE WITH THE SPECIFICATIONS.

EROSION CONTROL MEASURES ARE GOVERNED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SPECIFICATION FOR SOIL EROSIONS AND SEDIMENT CONTROL.

FILTER FENCE FOR STRUCTURE PROTECTION AS SHOWN ON THIS SHEET.



THE PROPOSED INLET PROTECTION WILL BE PLACED AT THE LOCATIONS SHOWN ON THIS SHEET.

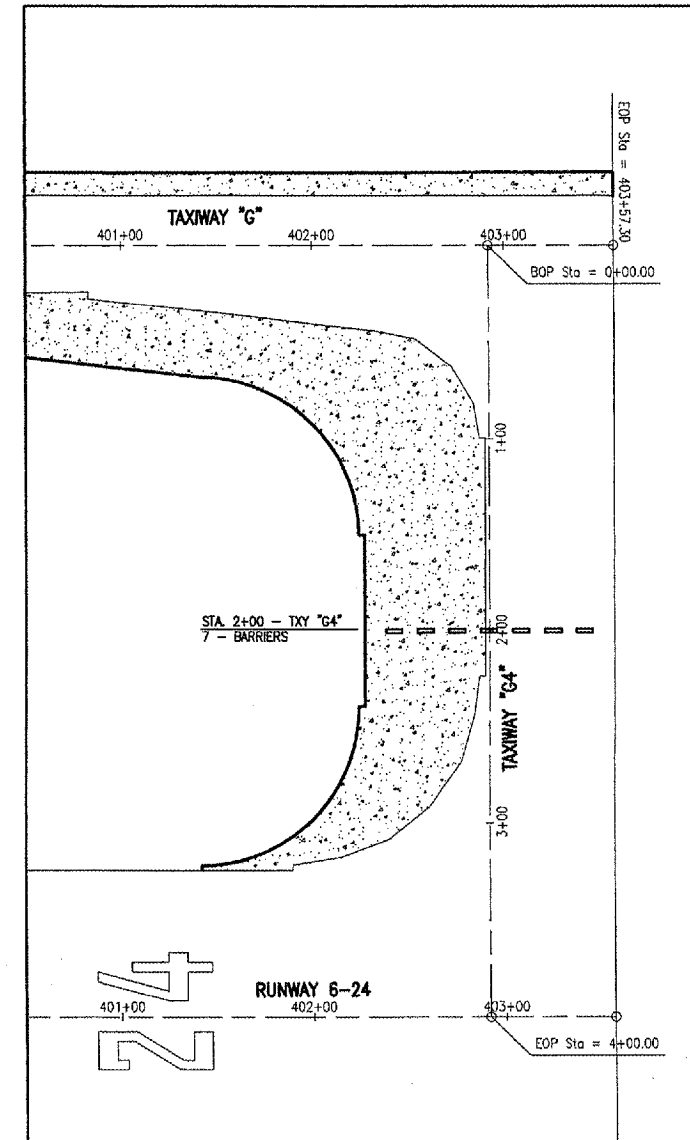
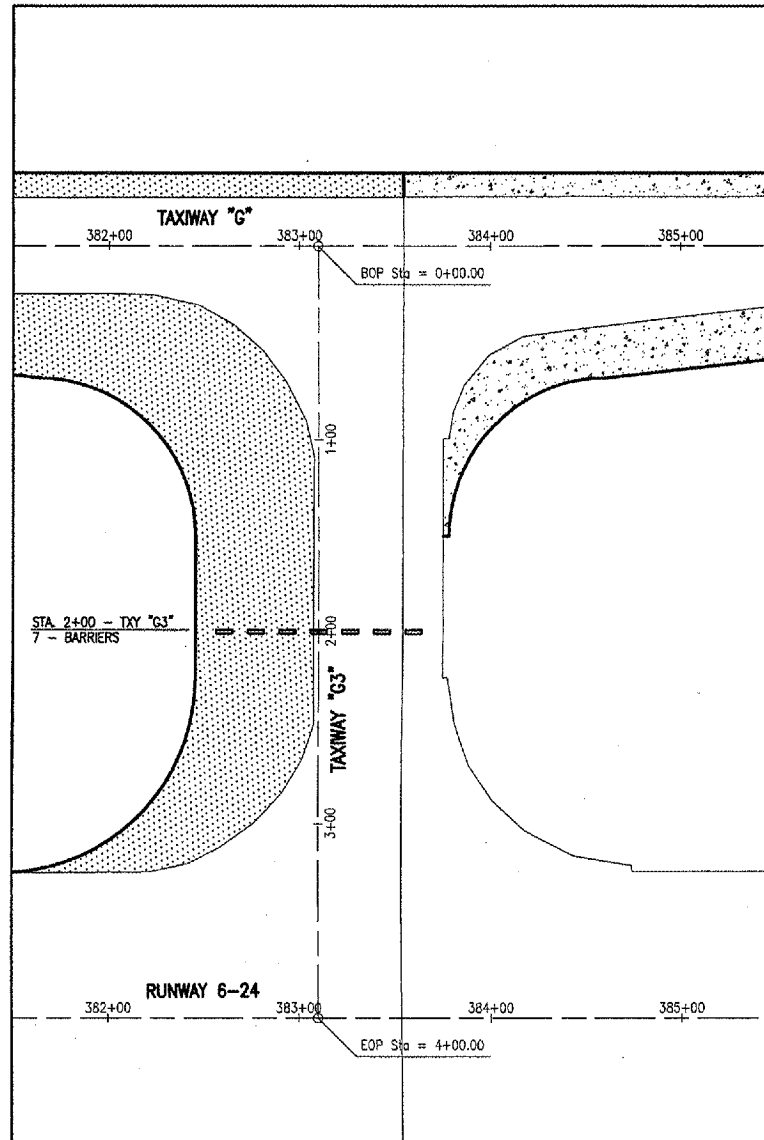
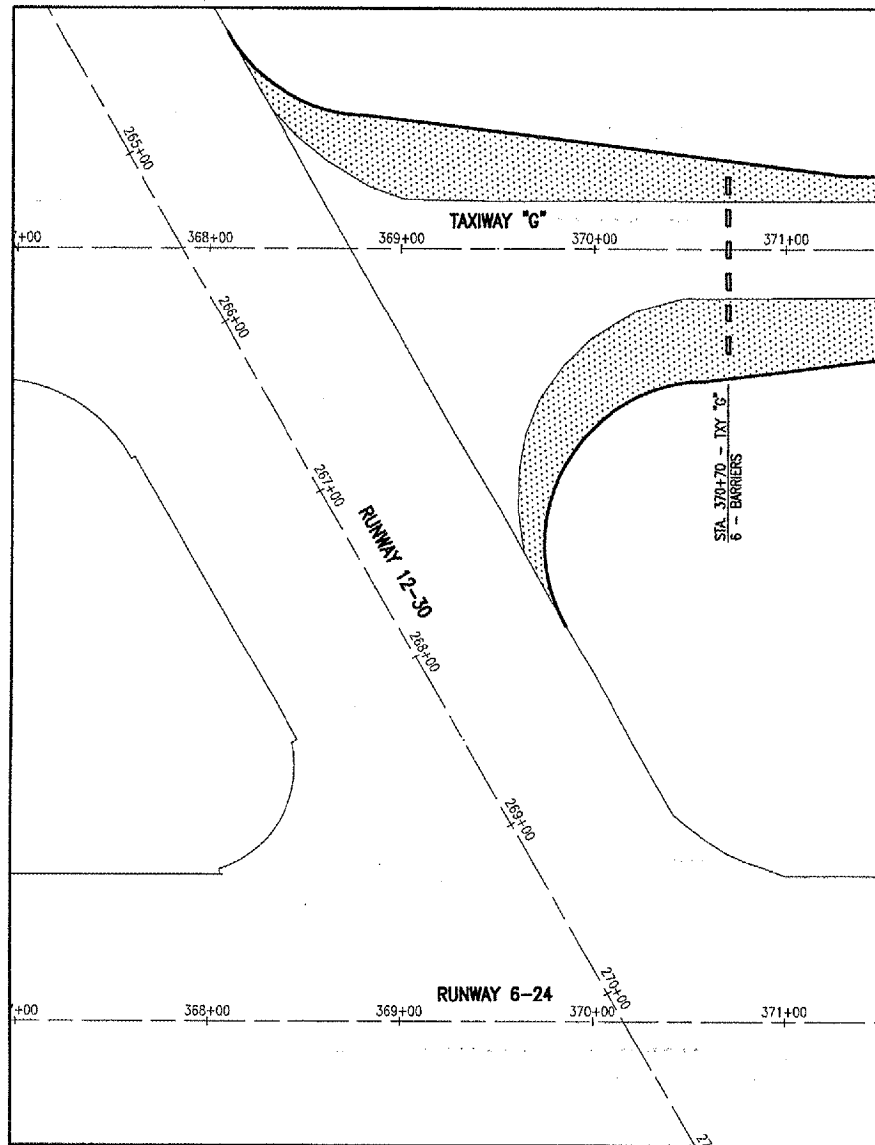
INSPECTION OF THE INLET PROTECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.

EROSION CONTROL FENCE SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM WATER DRAINAGE.

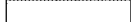



COST OF REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE UNIT PRICE FOR ITEM: AR156520 "INLET PROTECTION."

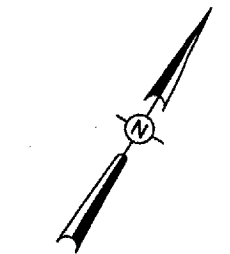
INLET PROTECTION IS TO INCLUDE ALL INCIDENTALS NECESSARY TO CONSTRUCT THESE ITEMS AND WILL BE PAID FOR UNDER ITEM: AR156520 "INLET PROTECTION."

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DATE										
 DECATUR, ILLINOIS										
I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28										
I.E. Engr. No. 802-041XXD Plans No. 581 SWP.DWG Scale N/A Date 10/28/04	<table border="1"> <tr> <td>LAYOUT</td> <td>WJM</td> <td>10/28/04</td> </tr> <tr> <td>DRAWN</td> <td>CCC</td> <td>10/28/04</td> </tr> <tr> <td>REVIEWED</td> <td>WJM</td> <td>06/07/05</td> </tr> </table>	LAYOUT	WJM	10/28/04	DRAWN	CCC	10/28/04	REVIEWED	WJM	06/07/05
LAYOUT	WJM	10/28/04								
DRAWN	CCC	10/28/04								
REVIEWED	WJM	06/07/05								
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TAXIWAY G WIDENING PHASE II PROPOSED EROSION CONTROL DETAILS & NOTES										
52 <small>52 of 110 sheets</small>										



LEGEND

-  EXISTING PAVEMENT
-  PROPOSED BITUMINOUS PAVEMENT
-  PROPOSED CONCRETE PAVEMENT
-  PROPOSED BARRIERS



0' 25' 50' 100'
 FULL SIZE SCALE: 1" = 50'
 HALF SIZE SCALE: 1" = 100'

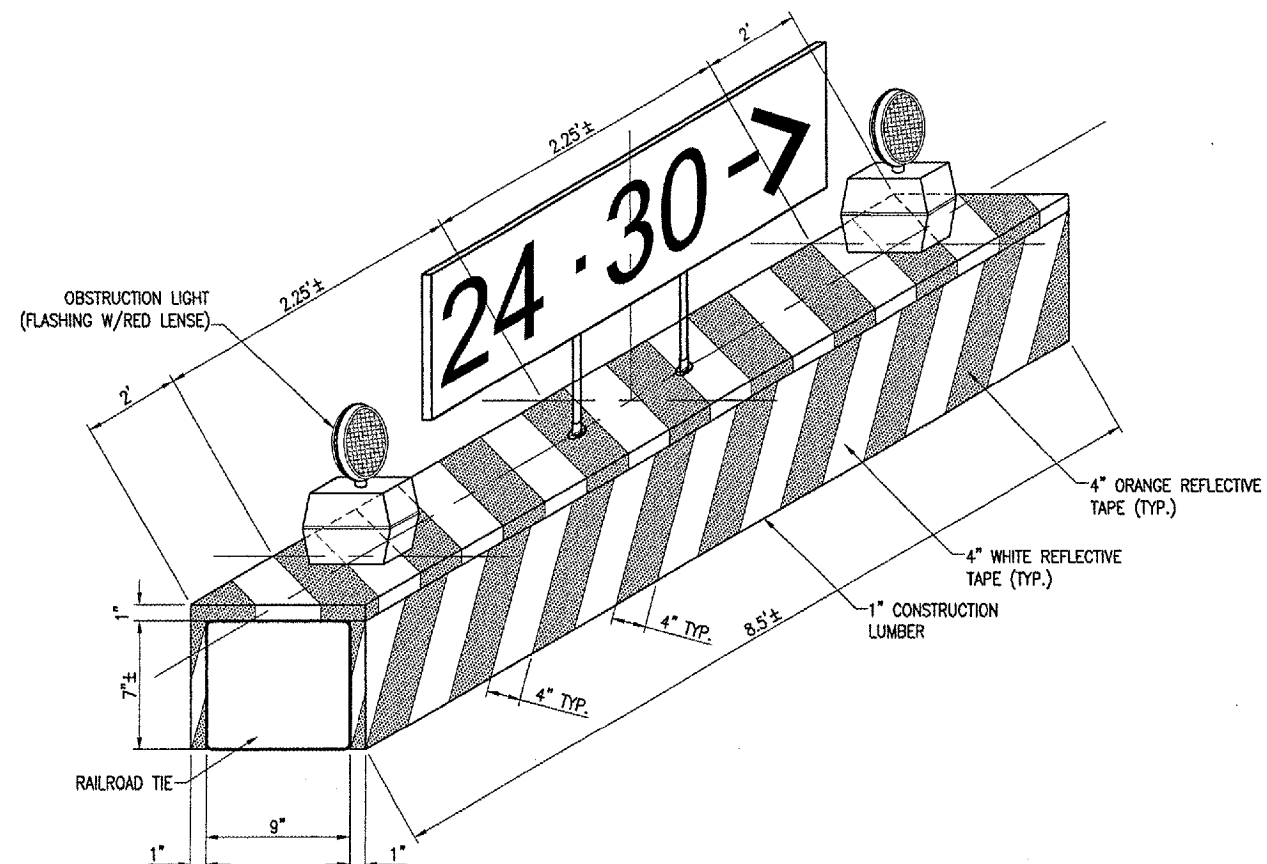
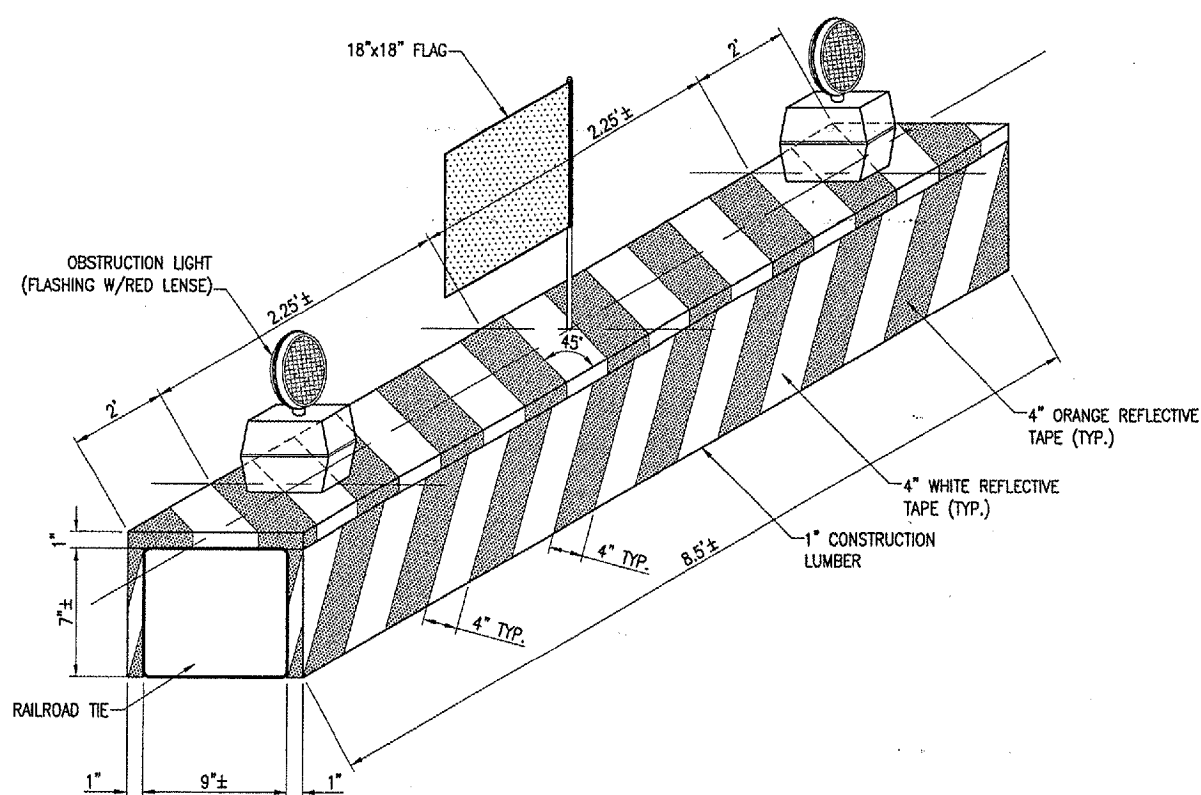
DATE	REVISION	BY



LAYOUT	WJM	WJM	WJM
DRAWN	CCC	CCC	CCC
REVIEWED	WJM	WJM	WJM

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED TEMPORARY
 BARRIER LOCATIONS



TEMPORARY MARK AND LIGHTING NOTES:

1. THE BARRIER WILL BE CONSTRUCTED OF A NEW OR SALVAGED RAILROAD TIE (7'±H x 9'±W x 8.5'±L) CAPPED WITH 1" CONSTRUCTION GRADE LUMBER ON THREE SIDES WITH 4" WHITE AND 4" ORANGE REFLECTIVE TAPE ON THREE SIDES WITH (2) OBSTRUCTION LIGHTS AND (1) 18"x18" FLAG.
2. CONTRACT No. 1: ONCE TAXIWAY "G", EAST OF RUNWAY, 12-30 IS CLOSED FOR CONSTRUCTION, BARRIERS WILL BE PLACED ON TAXIWAY "G" EAST OF RUNWAY 12-30, ON TAXIWAYS "G3" AND "G4". THE BARRIERS WILL BE LOCATED AT THE DESIGNATED STATIONS. INITIALLY, THE BARRIERS WILL BE PLACED ON EXISTING PAVEMENT, AND AS THE PAVEMENTS ARE WIDENED, THE BARRIERS WILL EXTEND ACROSS THE NEW PAVEMENT.
3. CONTRACT No 2: DURING THE RECONSTRUCTION OF RUNWAY 6-24, THE RUNWAY WILL BE CLOSED AT SPECIFIED TIMES. WHEN RUNWAY 6-24 IS CLOSED FOR AIR OPERATION, BARRIERS WILL BE PLACED ON TAXIWAY "G" EAST OF RUNWAY 12-30.
4. BARRIERS THAT ARE IN PLACE AND CONFLICT WITH CONSTRUCTION OPERATIONS CAN BE TEMPORARILY RELOCATED AND REPLACED AS SOON AS THE CONSTRUCTION SEQUENCE IS COMPLETED, BUT NO LATER THAN THE END OF THE CONSTRUCTION DAY.
5. THE SEPARATION DISTANCE BETWEEN EACH BARRIER WILL BE 8 FEET.
6. THE OBSERVATION LIGHTS WILL BE THE SAME FIXTURES AS USED ON A BARRICADE WITH A RED FLASHING LIGHT. THE FIXTURES WILL BE MOUNTED 2 FEET FROM EITHER END OF THE BARRIER AND CENTERED ON THE BARRIER.
7. ONE 18" SQUARE ORANGE FLAG, EXTENDED, WITH MOUNTING DOWEL AND BRACKET. MOUNT THE FLAG CENTERED WITH THE LENGTH AND WIDTH OF THE BARRIER. THE HEIGHT OF THE FLAG WILL BE 30"±.
8. ONE REFLECTIVE SIGN, UNLIGHTED, L-858Y INFORMATIONAL SIZE 1, STYLE 4, MODE 1 WITH FRANGIBLE LEGS AS MANUFACTURED BY SIEMENS AIRFIELD SOLUTIONS, INC. THE SIGN IS TO BE SECURELY MOUNTED TO THE TOP CENTER OF ONE OF THE BARRIERS. THE BARRIER WILL BE LOCATED ON THE TAXIWAY "G" CENTERLINE AND IN LINE WITH THE OTHER LIGHTED BARRIERS.
9. 19 BARRIERS WITH OBSTRUCTION LIGHTS AND FLAG ARE REQUIRED. 1 BARRIER WITH OBSTRUCTION LIGHTS AND DIRECTIONAL SIGN IS REQUIRED.
10. THE BARRIERS WILL BE LOCATED AS SHOWN ON SHEET 53.
11. PAYMENT FOR PROVIDING THE BARRIERS, INCLUDING THE LIGHTS, FLAGS, AND SIGN SHALL BE PAID FOR AS A WORK ITEM UNDER ITEM: AR150530 "TRAFFIC MAINTENANCE" PER LUMP SUM.

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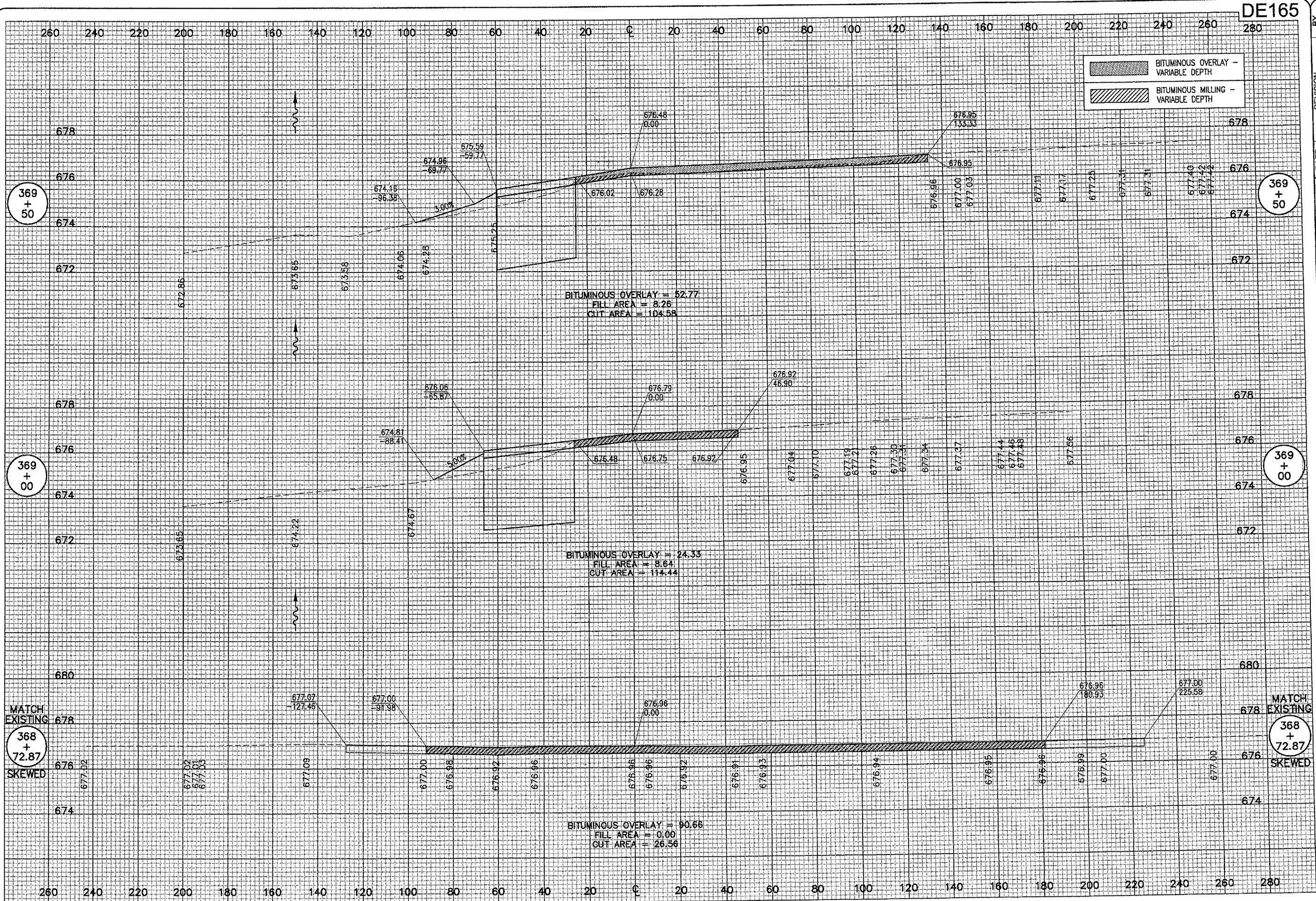

DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

IED Project No. 802-04TYXD File No. 501BAR.DWG Scale N/A Date 10/28/04	LAYOUT WJM 10/28/04 DRAWN CCC 10/28/04 REVIEWED WJM 06/07/05
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**TAXIWAY G WIDENING
 PHASE II**
TEMPORARY BARRIER DETAIL

JUN 10, 2005 10:25 AM CCC
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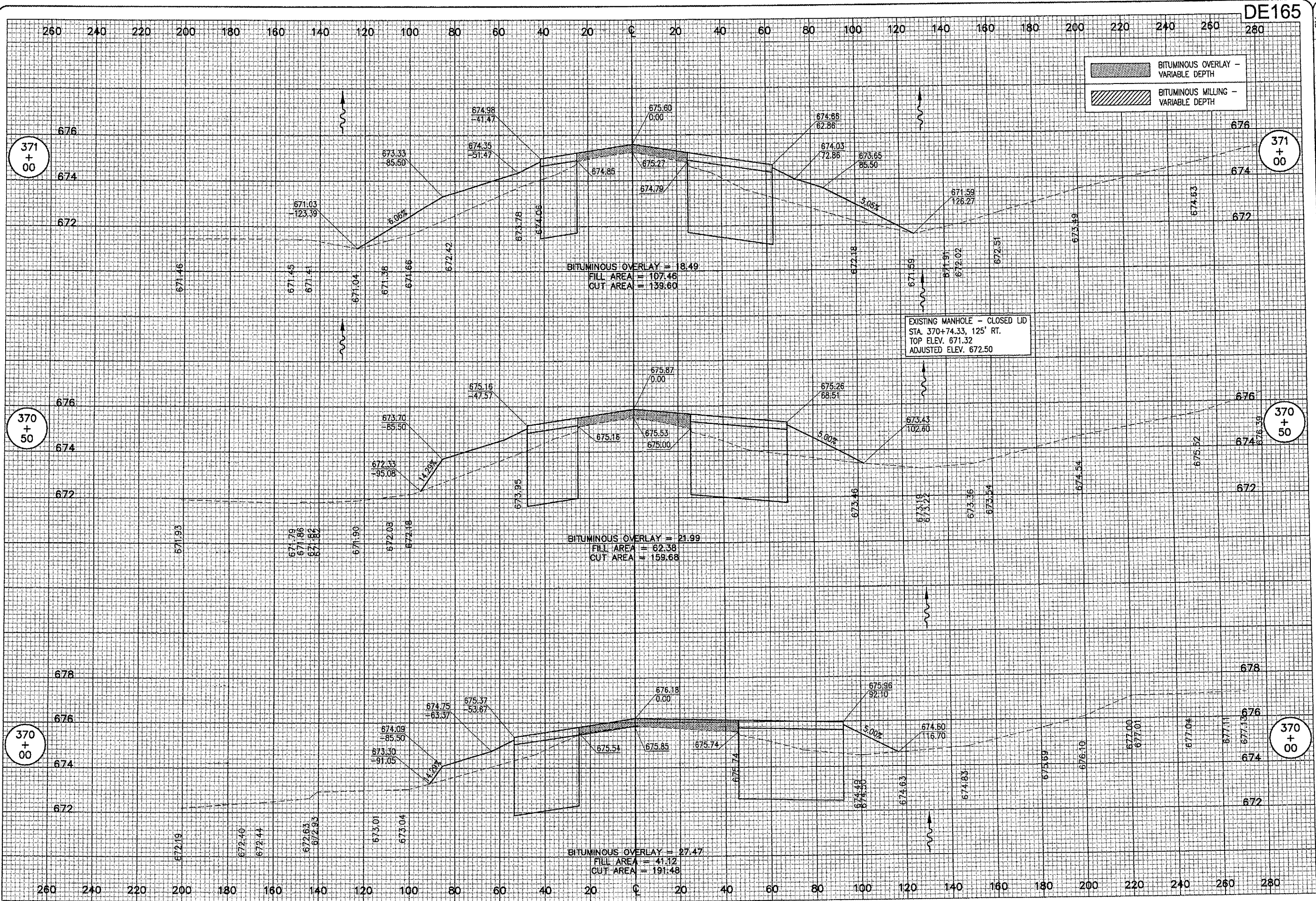
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DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 I.L. PROJ.: DEC-3414

HEI Project No. 802-04TXD	FILED	DATE
Plan No. 301-XS.DWG	CCC	01/27/05
Scale 1" = 50'	CCC	01/27/05
Date 01/27/05	CAH	06/07/05
LAYOUT	CCC	01/27/05
DRAWN	CCC	01/27/05
REVIEWED	CAH	06/07/05

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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 368+72.87 TO STA. 369+50

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DATE	REVISION	BY

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IL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

REL. Project No. 802-04TXD	
Plan No. 302-XS.DWG	
Scale: 1"=50'	
Date: 01/27/05	
LAYOUT	CCC 01/27/05
DRAWN	CCC 01/27/05
REVIEWED	CAH 06/07/05

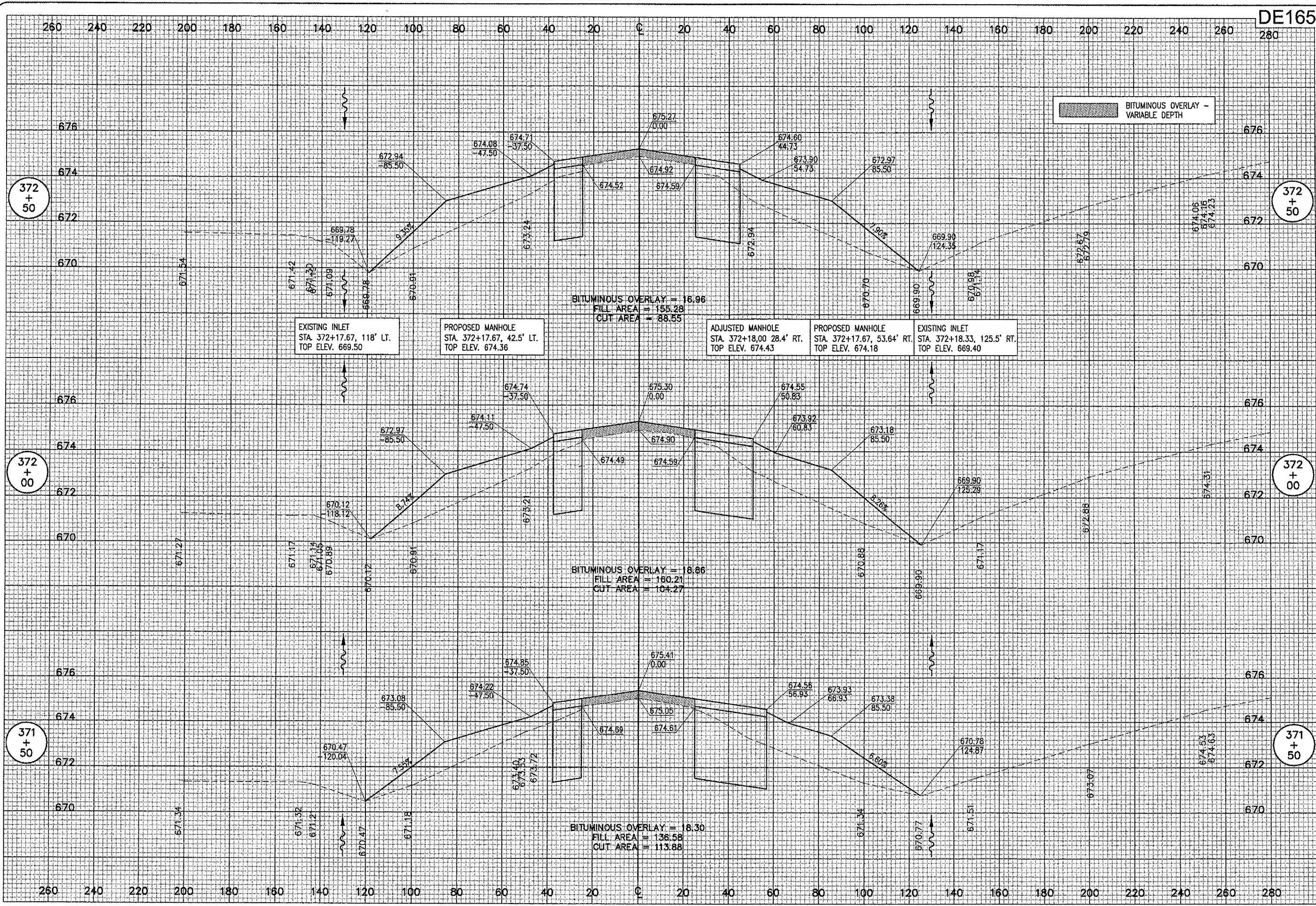
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Office: Natlarkwa

**TAXIWAY G WIDENING
PHASE II**

PROPOSED CROSS-SECTIONS
TAXIWAY "G"
STA. 370+00 TO STA. 371+00

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REVISION	
DATE	

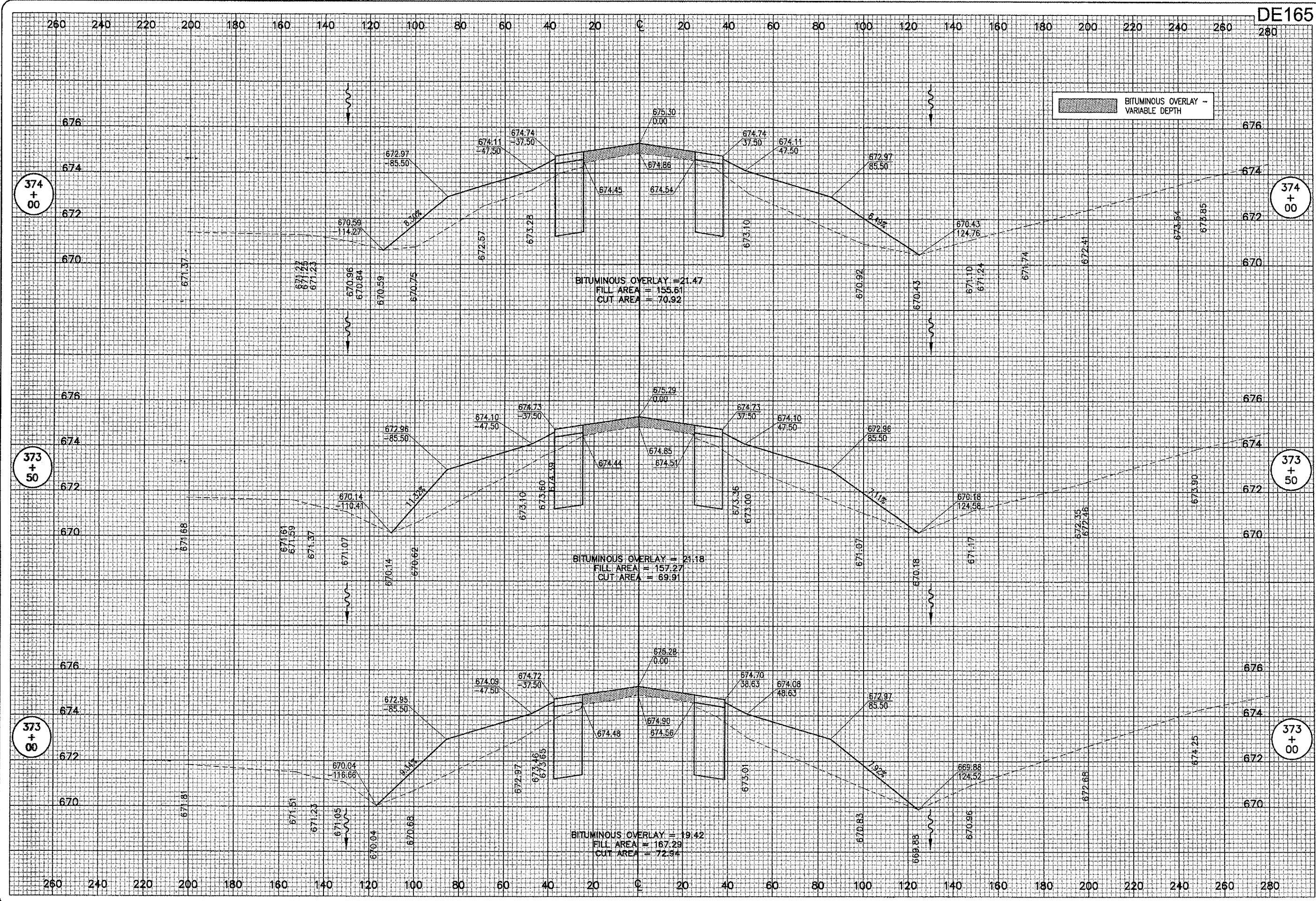
DECATUR AIRPORT
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

HE Project No. 802-04TXD	CCC	01/27/05
Drawn: 303-XS.DWG	CCC	01/27/05
Scale: 1" = 50'	CAH	06/07/05
Date: 01/27/05		

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**TAXIWAY G WIDENING
 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 371+50 TO STA. 372+50

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IL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

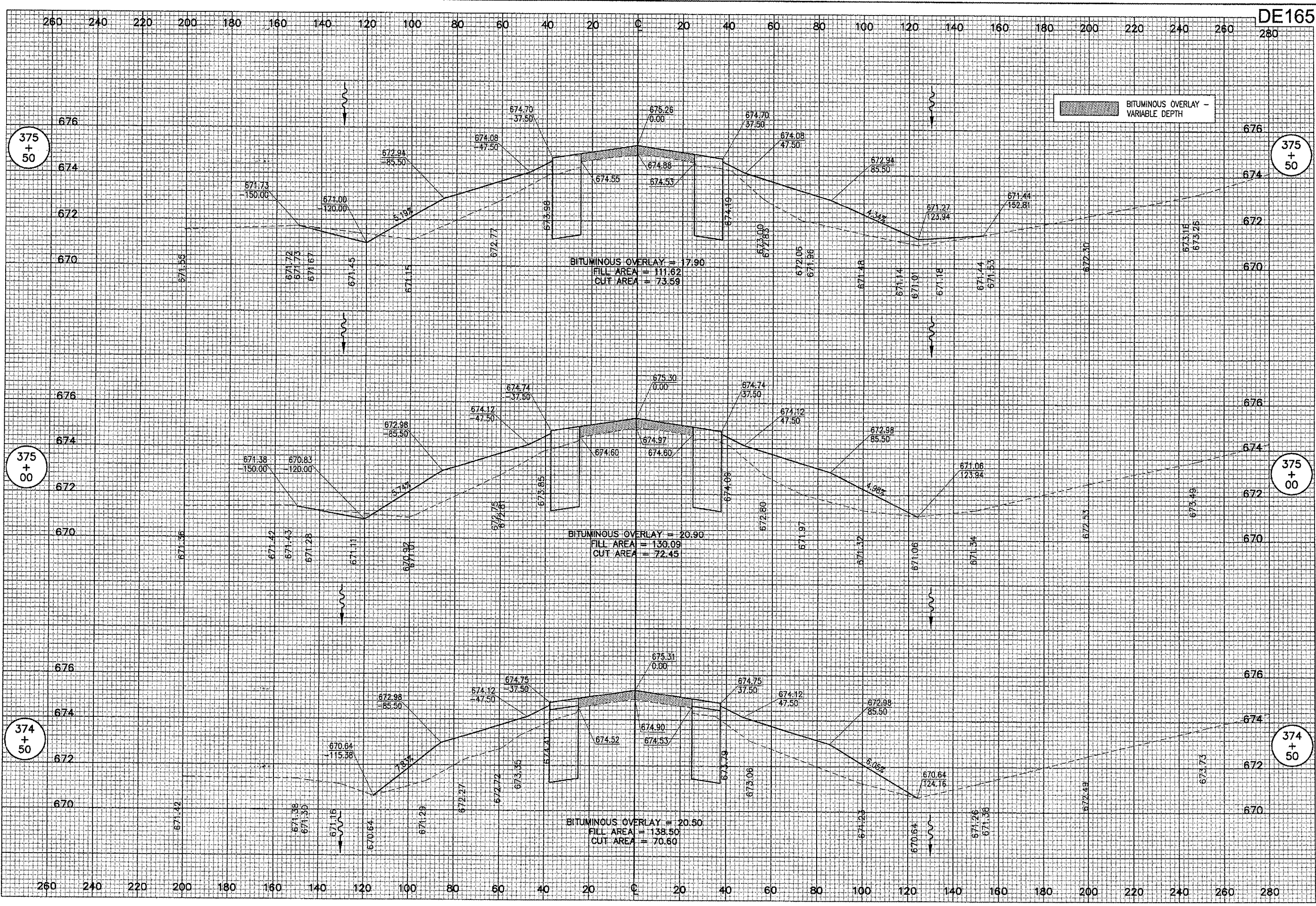
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01/27/05	CCC	CCC
01/27/05	CCC	CCC
06/07/05	CAH	CAH




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TAXIWAY G WIDENING
 PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 373+00 TO STA. 374+00

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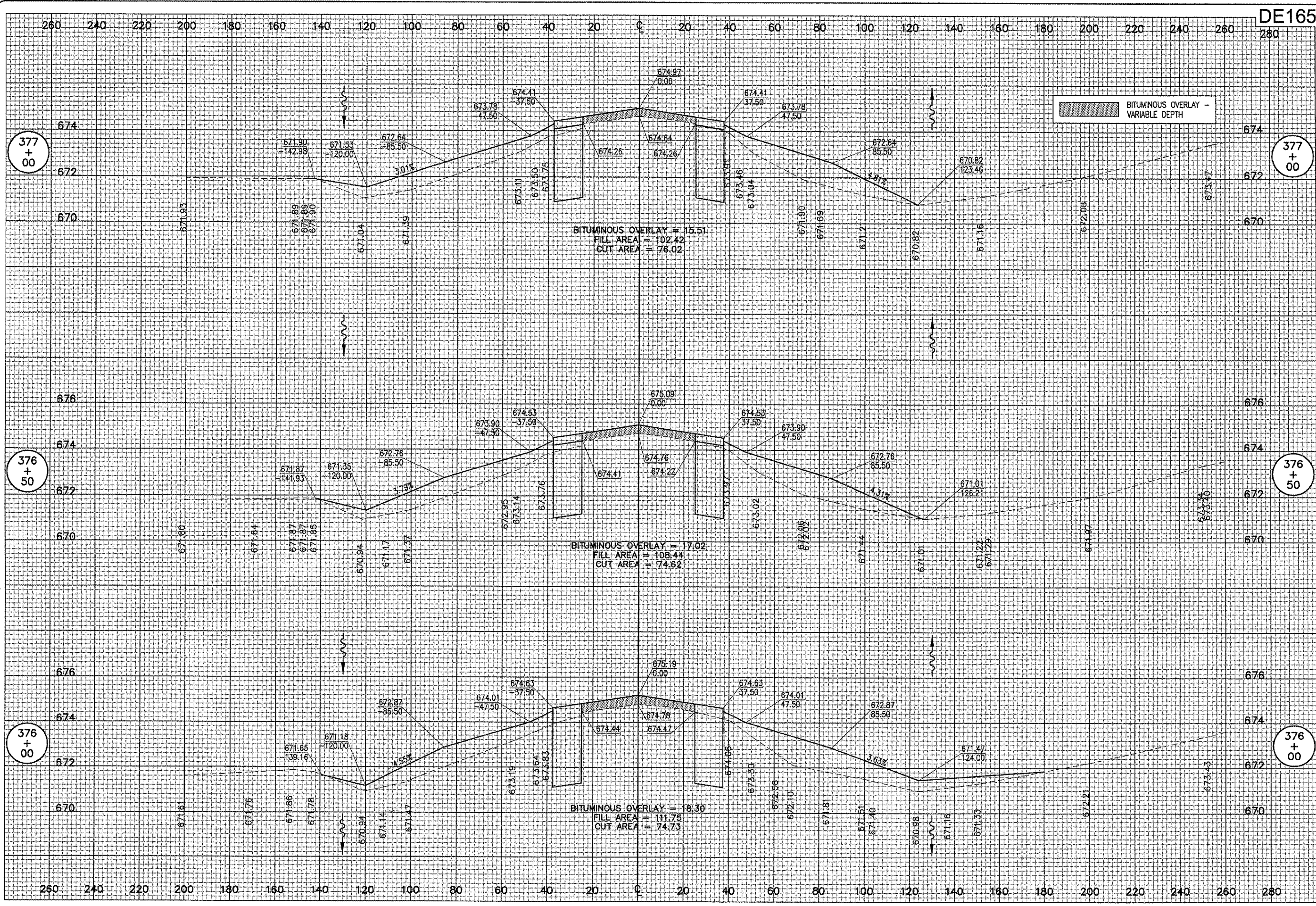

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 I.L. PROJ.: DEC-3414

HEI Project No.	CCC	01/27/05
802-04TXD		
Plan: R-305-XS.DWG		
Scale: 1" = 50'		
Date: 01/27/05		
LAYOUT	CCC	01/27/05
DRAWN	CCC	01/27/05
REVIEWED	CAH	06/07/05


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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 374+50 TO STA. 375+50

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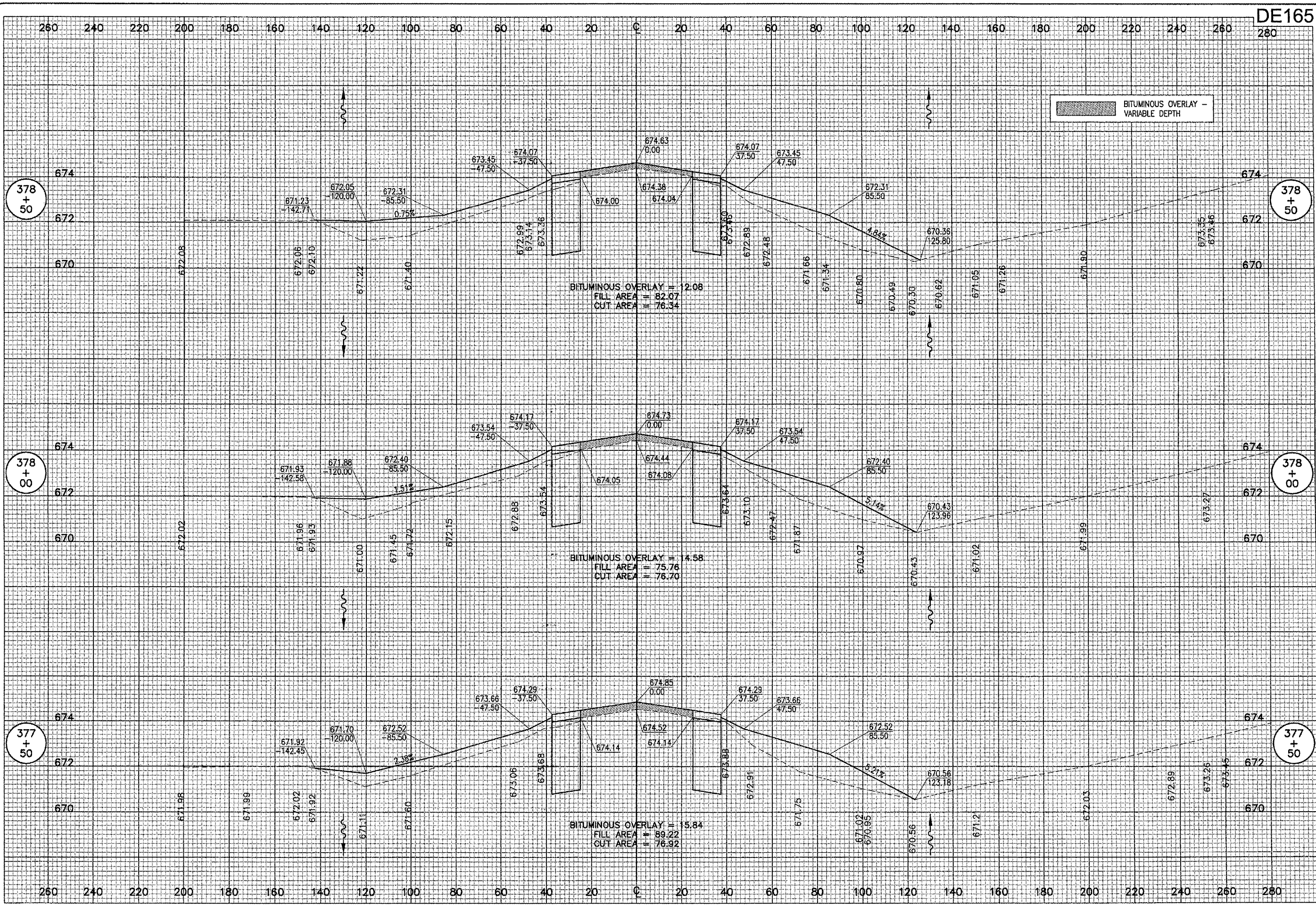


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HE Project No. 802-04TXD File: R-306-XS.DWG Scale: 1"=50' Date: 11/11/04	
LAYOUT	CCC 11/11/04
DRAWN	CCC 11/11/04
REVIEWED	CAH 06/07/05
TAXIWAY G WIDENING PHASE II PROPOSED CROSS-SECTIONS TAXIWAY "G" STA. 376+00 TO STA. 377+00	
60	
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 I.L. PROJ.: DEC-3414
 A.L.P. PROJ.: 3-17-0033-28


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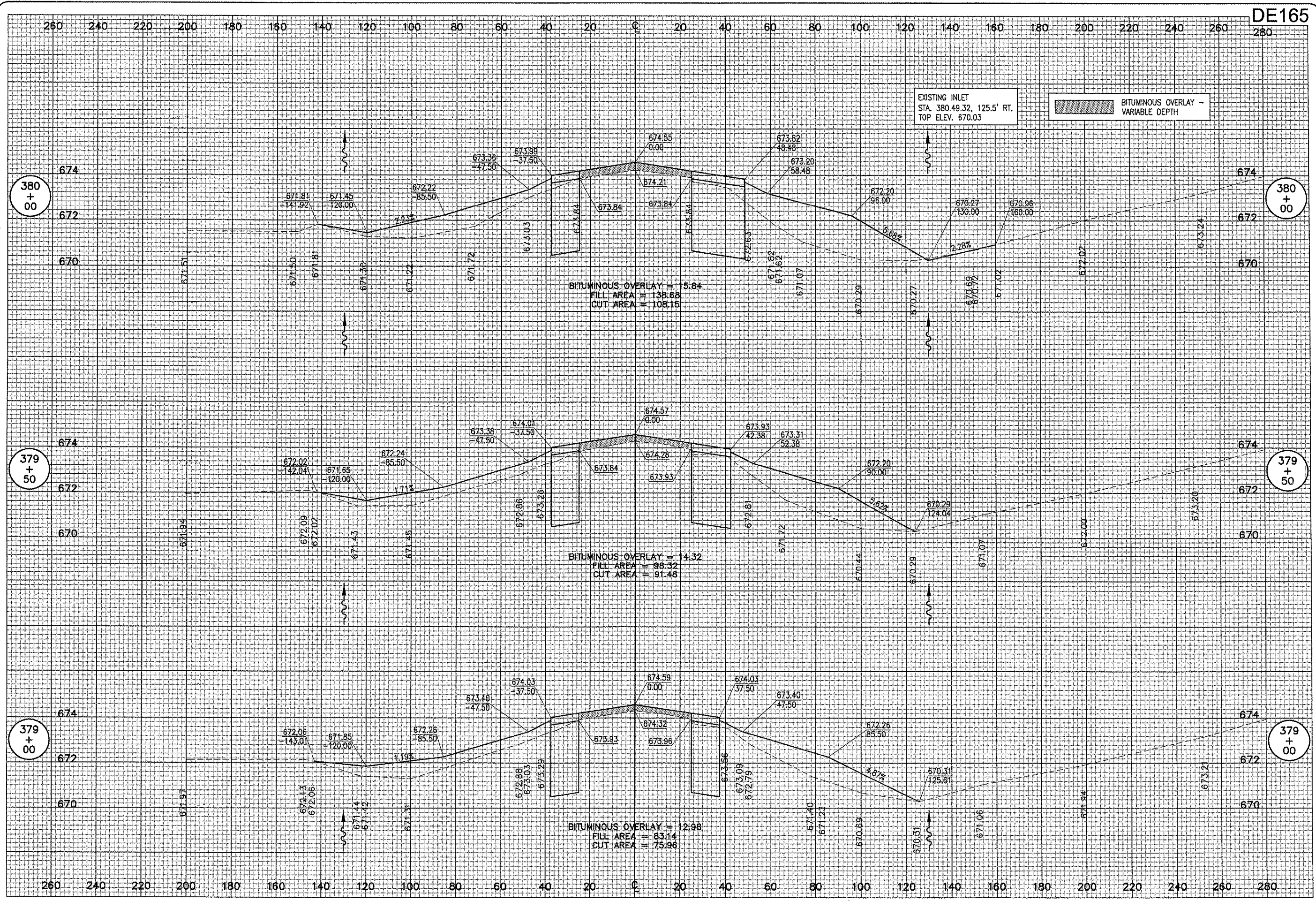

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 A.I.P. PROJ.: DEC-3414
 IL PROJ.: DEC-3414

HE Project No. 802-041XXD	FILED	DATE
Plan R-307-XS.DWG	CCC	11/11/04
Scale 1"=50'	CCC	11/11/04
Date 11/11/04	CAH	06/07/05



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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 377+50 TO STA. 378+50

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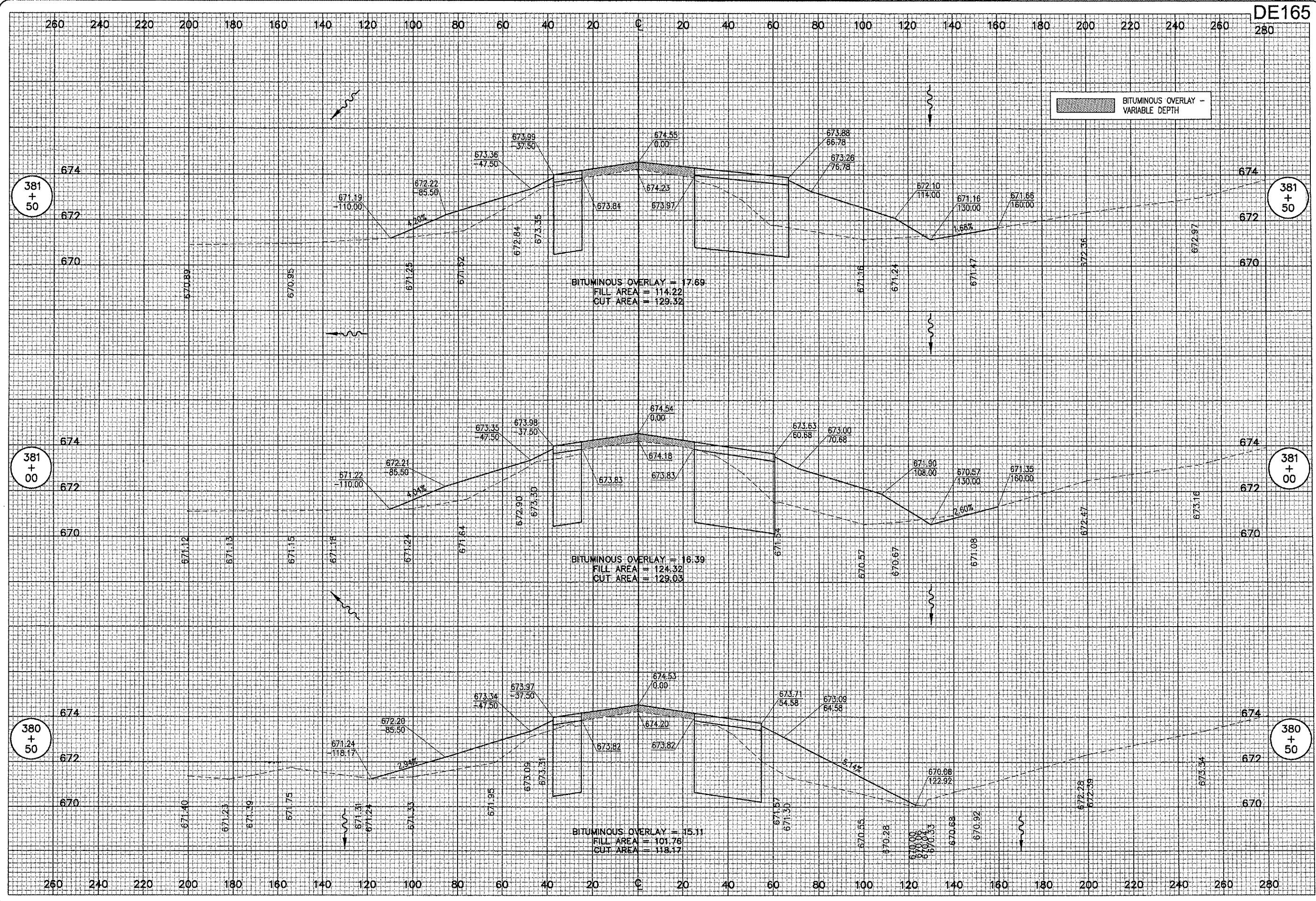

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REG. Project No. 802-04TYXD Plansheet 308-XS.DWG Scale 1" = 50' Date 11/11/04	LAYOUT CCC 11/11/04 DRAWN CCC 11/11/04 REVIEWED CAH 06/07/05
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PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 379+00 TO STA. 380+00

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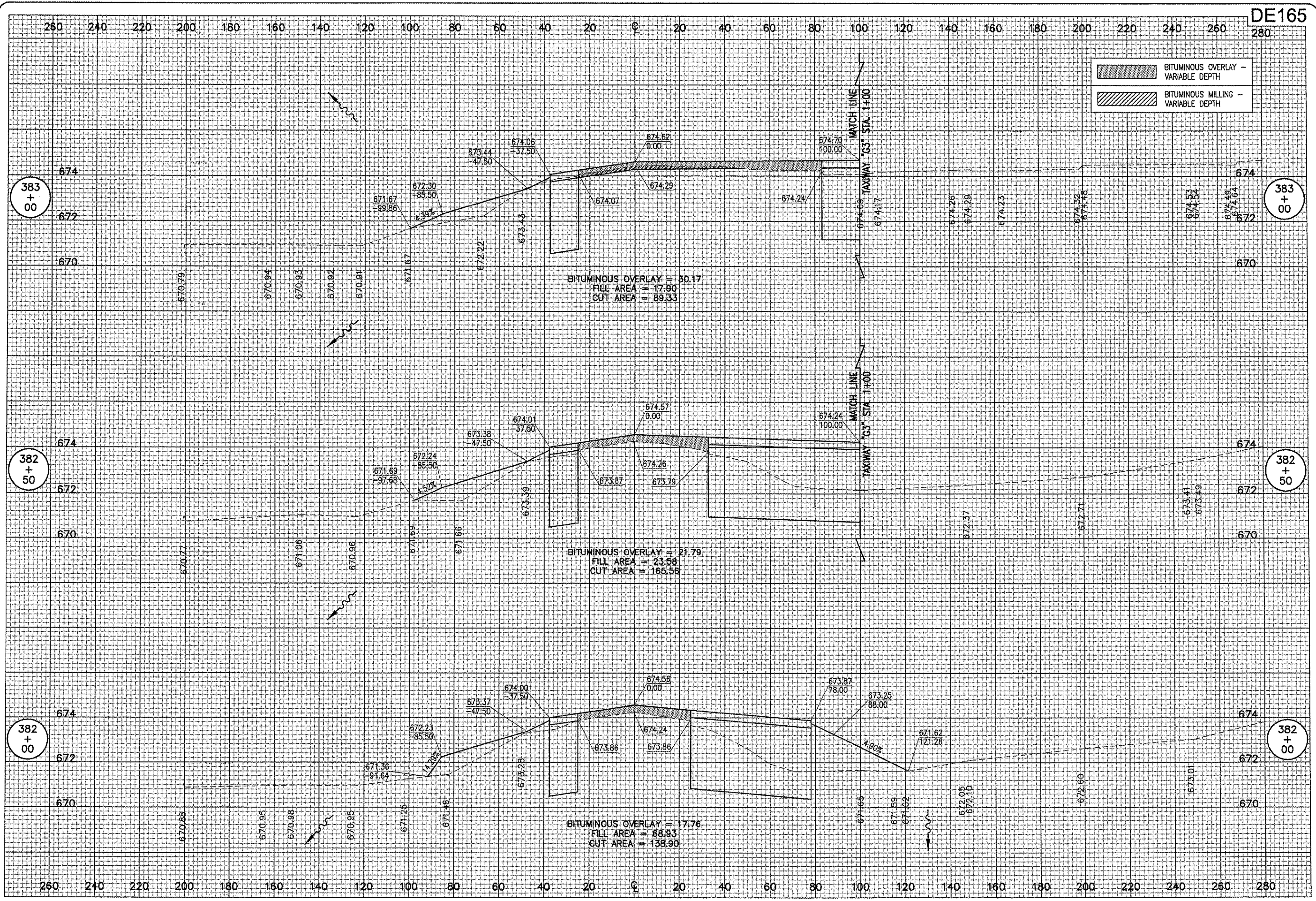
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FILED 309-XS.DWG	CCC	11/11/04
Scale 1" = 50'	CCC	11/11/04
Date 11/11/04	CAH	06/07/05
LAYOUT		
DRAWN		
REVIEWED		



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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 380+50 TO STA. 381+50

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BITUMINOUS OVERLAY - VARIABLE DEPTH
 BITUMINOUS MILLING - VARIABLE DEPTH

DATE	REVISION	BY

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 A.I.P. PROJ.: 3-17-0033-28

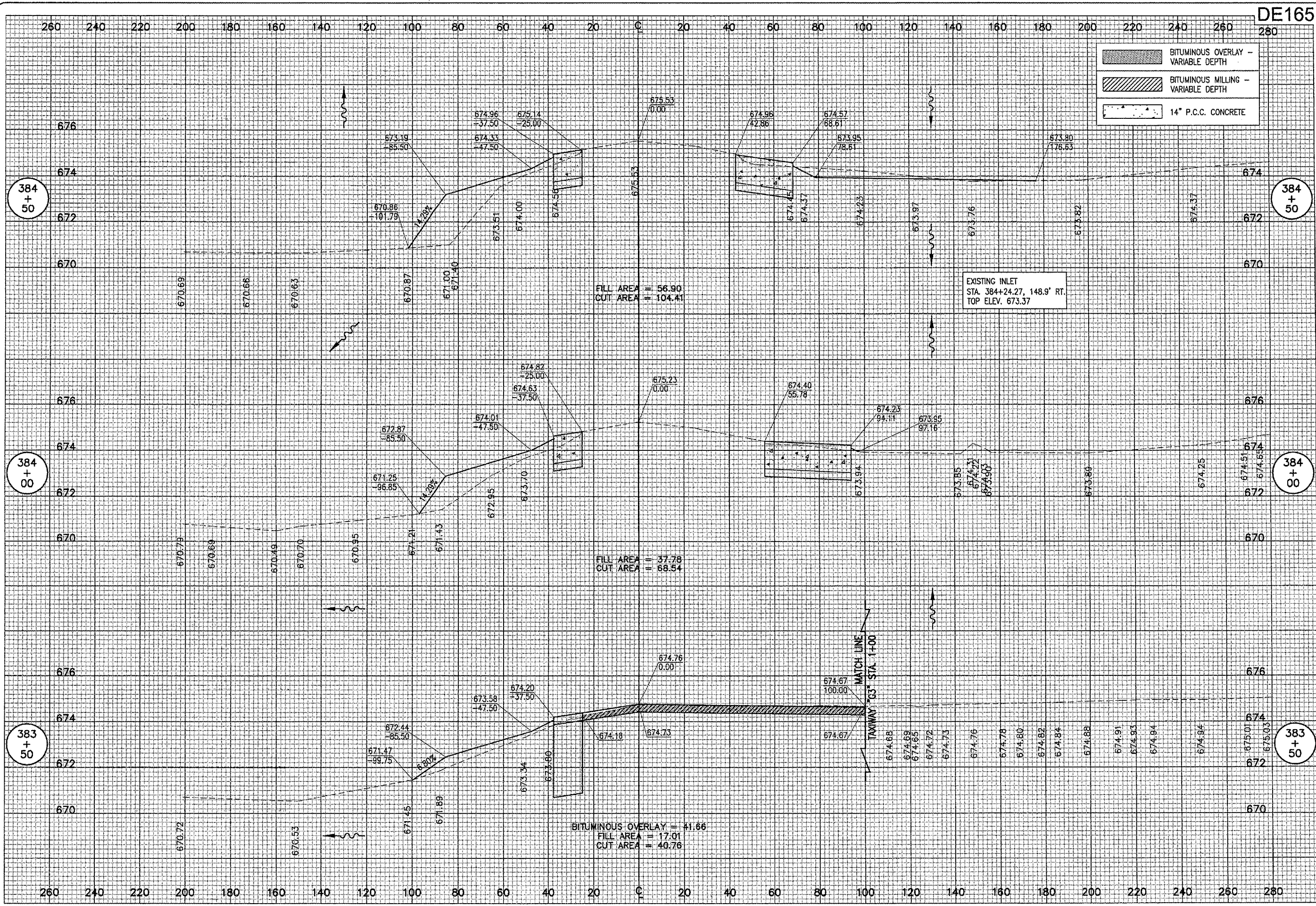
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 Scale: 1" = 50'
 Date: 11/11/04

LAYOUT	CCC	11/11/04
DRAWN	CCC	11/11/04
REVIEWED	CAH	06/07/05

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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 382+00 TO STA. 383+00

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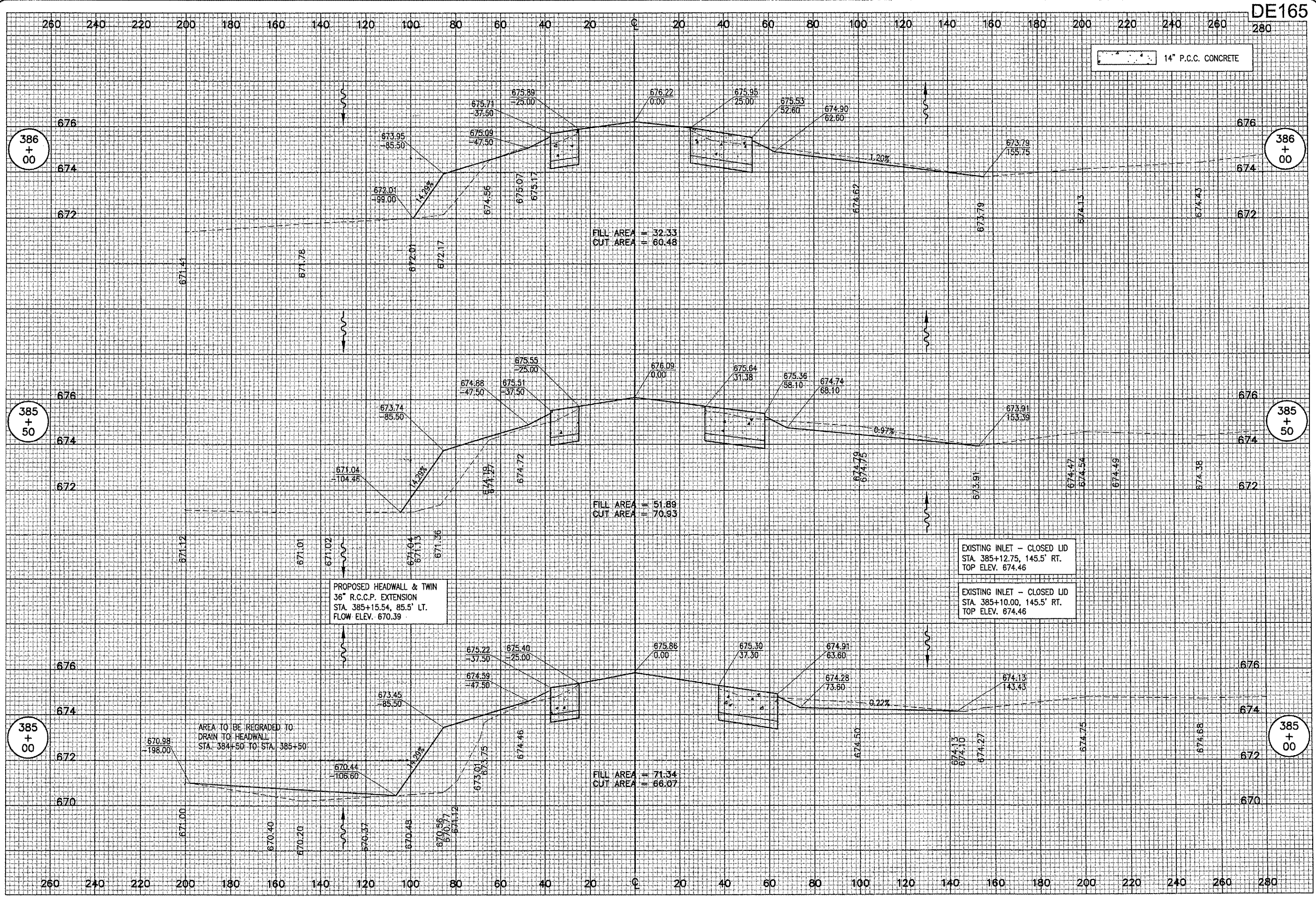
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 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEL. PROJ. NO.	802-04TXD
FILE NO.	311-XS.DWG
SCALE	1" = 50'
DATE	11/11/04
LAYOUT	CCC
DRAWN	CCC
REVIEWED	CAH
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	11/11/04
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 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 383+50 TO STA. 384+50

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HB Project No. 802-04TXND
 File No. 312-XS.DWG
 Scale 1" = 50'
 Date 11/11/04

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DRAWN	CCC	11/11/04
REVIEWED	CAH	06/07/05

TAXIWAY G WIDENING PHASE II

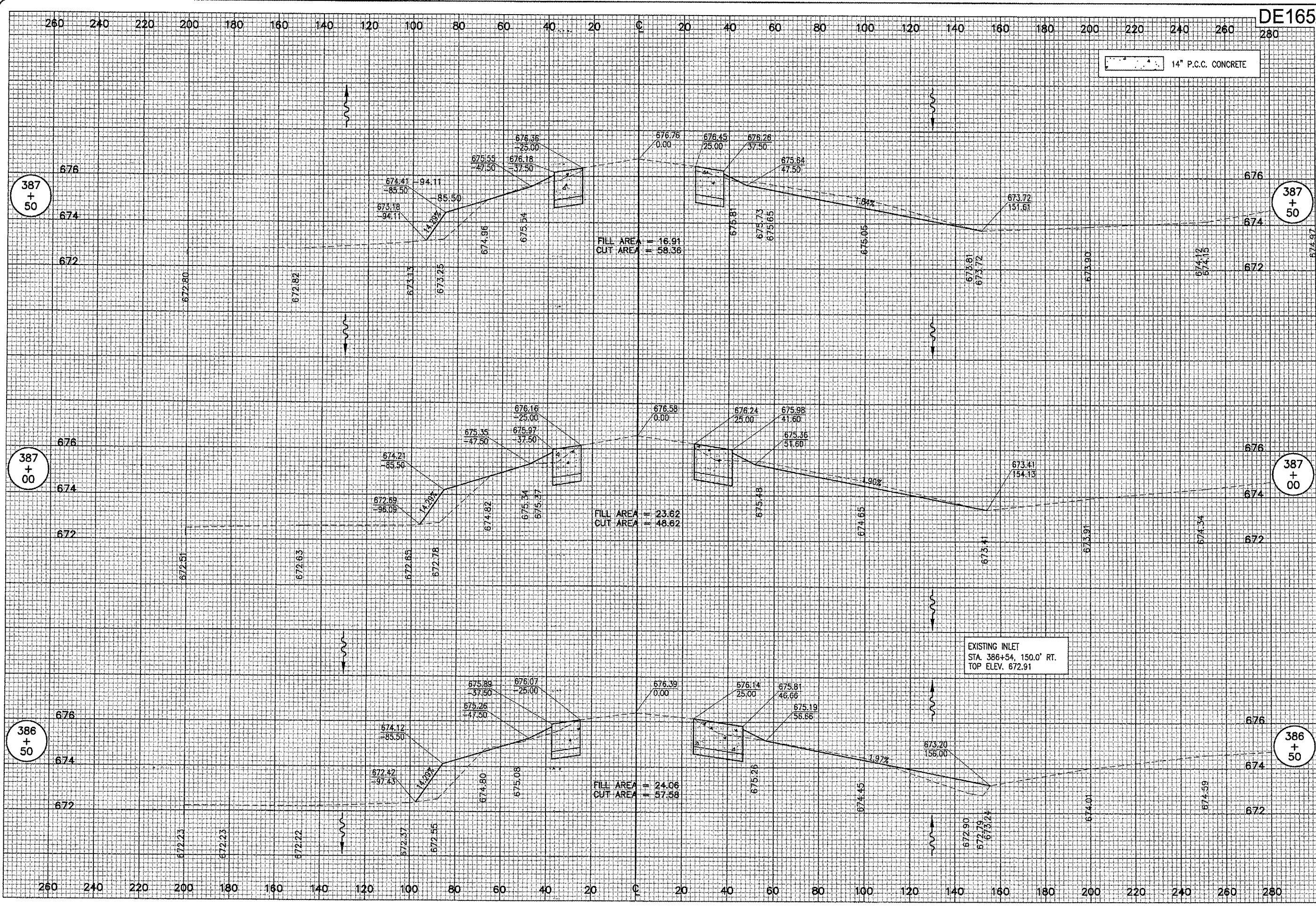
PROPOSED CROSS-SECTIONS TAXIWAY "G"

STA. 385+00 TO STA. 386+00

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86 of 110 sheets

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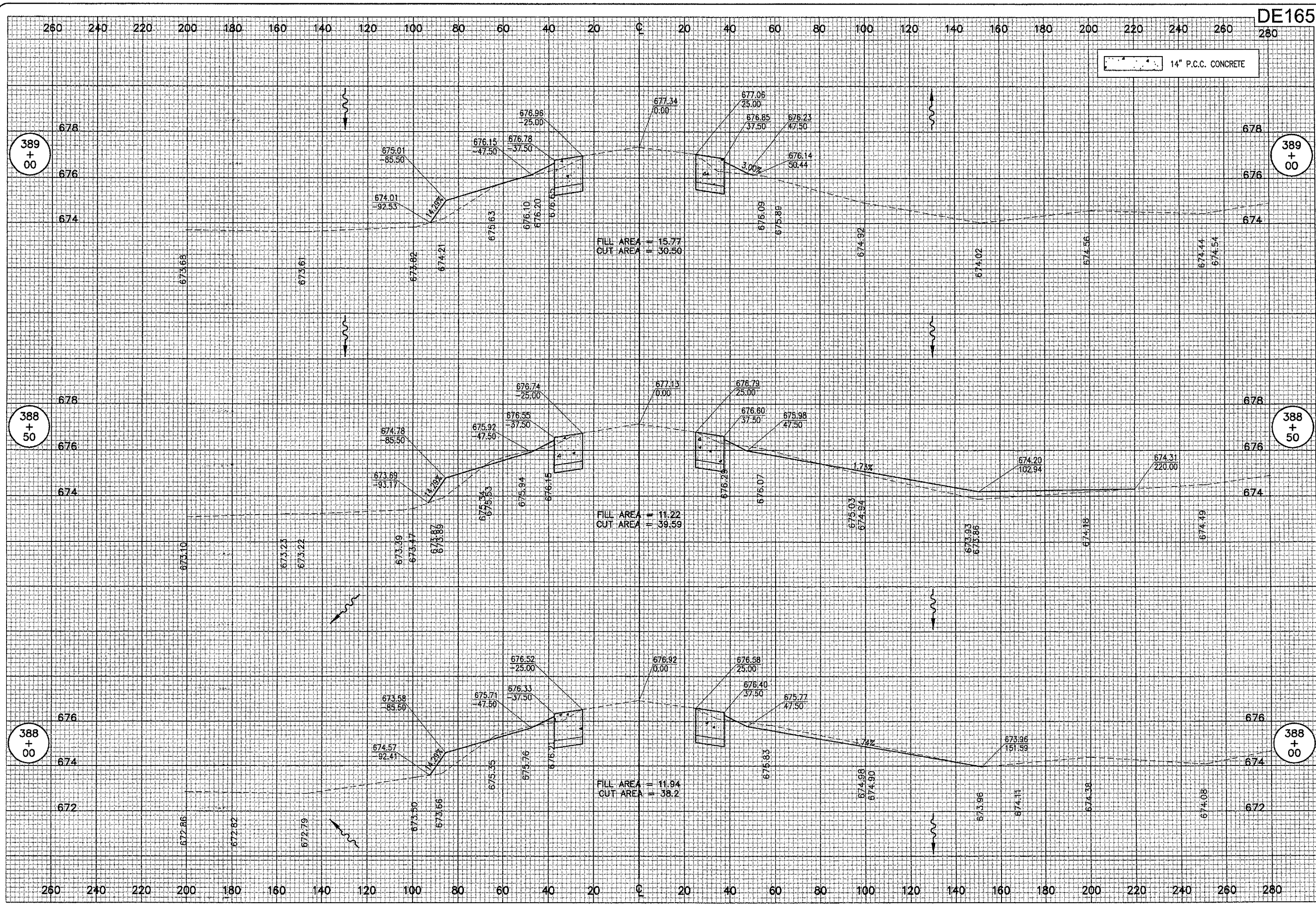
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HEL Project No. 802-041XYD	FILED 11/11/04
Drawn: 313-XS.DWG	Scale: 1" = 50'
Date: 11/11/04	
LAYOUT	CCC 11/11/04
DRAWN	CCC 11/11/04
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 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 386+50 TO STA. 387+50

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14" P.C.C. CONCRETE

DATE	REVISION

IL Project No. 802-04TXD
 Plan No. 314-XS.DWG
 Scale 1" = 50'
 Date 11/11/04

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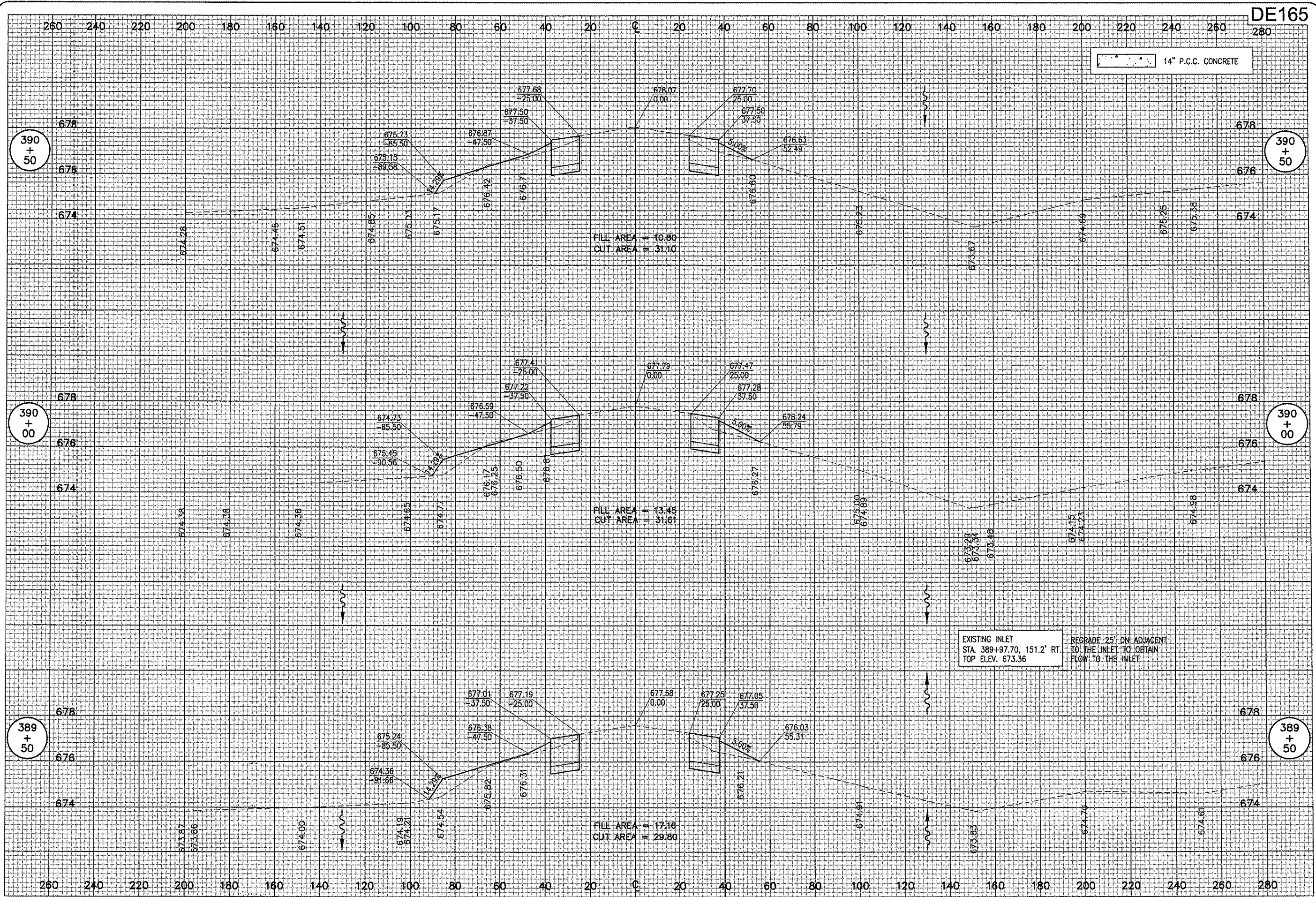
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IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28


LAYOUT	CCC	11/11/04
DRAWN	CCC	11/11/04
REVIEWED	CAH	06/07/05

TAXIWAY G WIDENING
PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 388+00 TO STA. 389+00

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DATE	REVISION

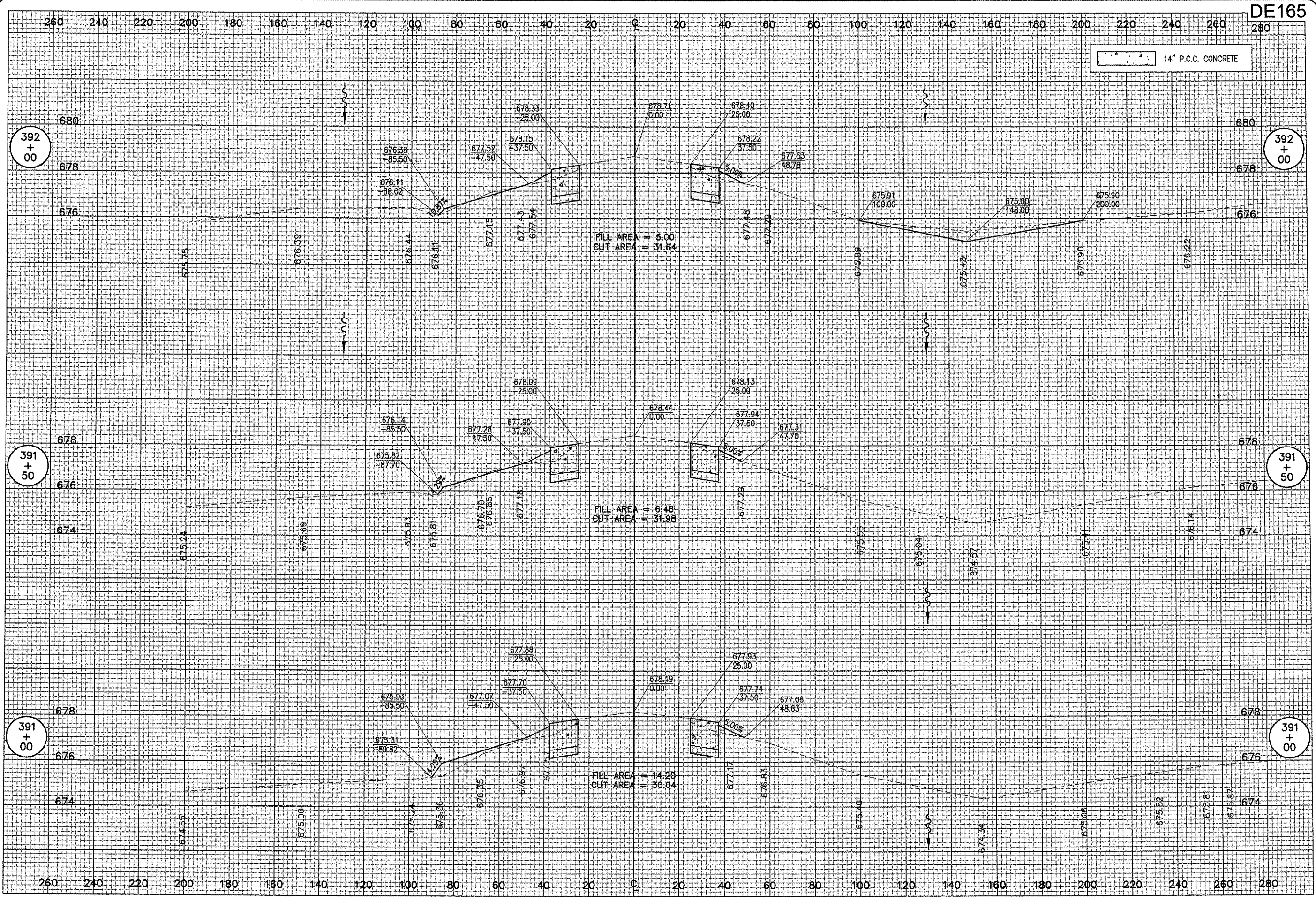

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 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 389+50 TO STA. 390+50

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14" P.C.C. CONCRETE

FILL AREA = 5.00
 CUT AREA = 31.64

FILL AREA = 6.48
 CUT AREA = 31.98

FILL AREA = 14.20
 CUT AREA = 30.04

DATE	REVISION

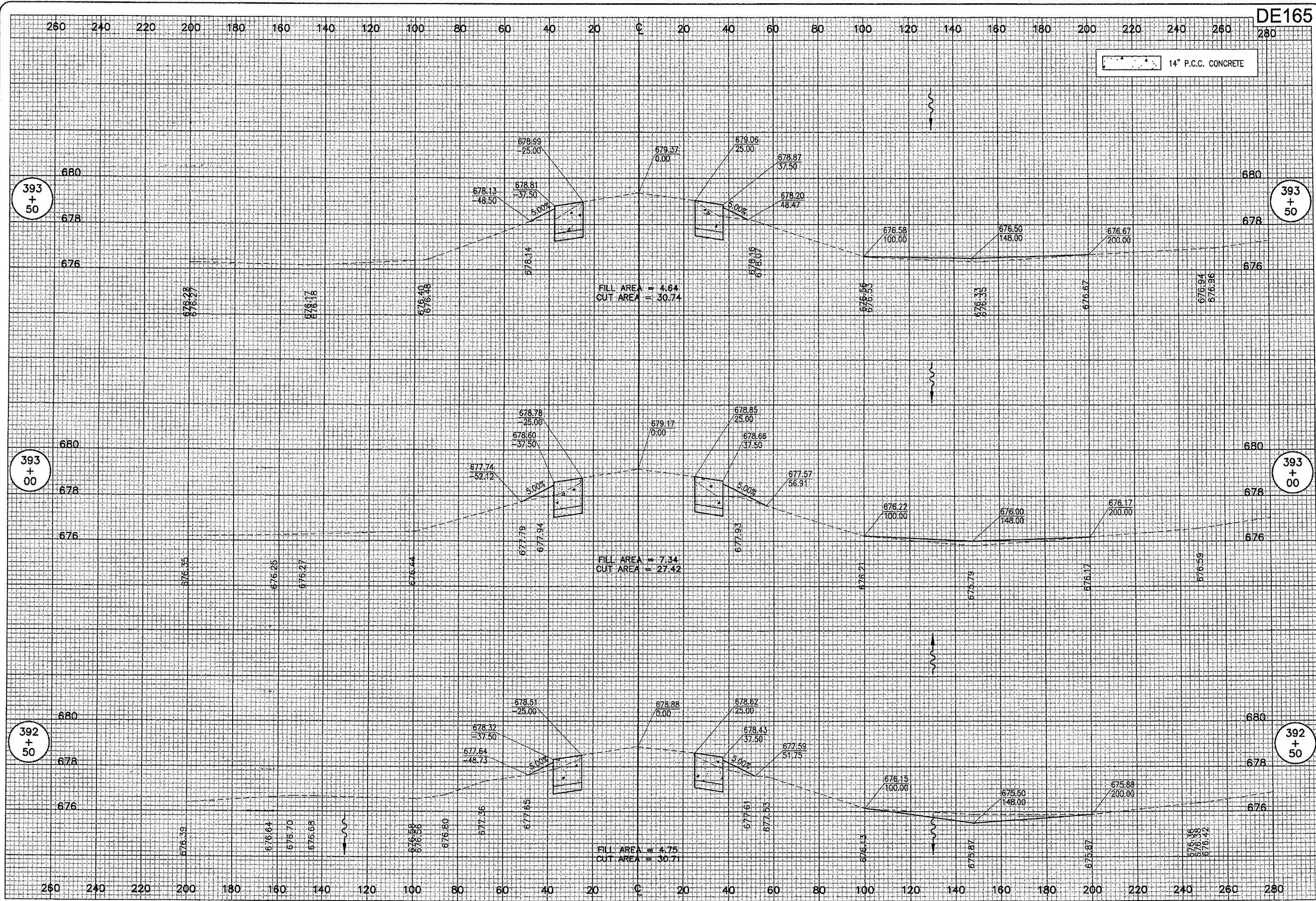
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 I.L. PROJ.: DEC-3414

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Files: R-316-XS.DWG	CCC	11/11/04
Scale: 1" = 50'	CCC	06/07/05
Date: 11/11/04	CAH	
LAYOUT		
DRAWN		
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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 391+00 TO STA. 392+00

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DATE	REVISION	BY

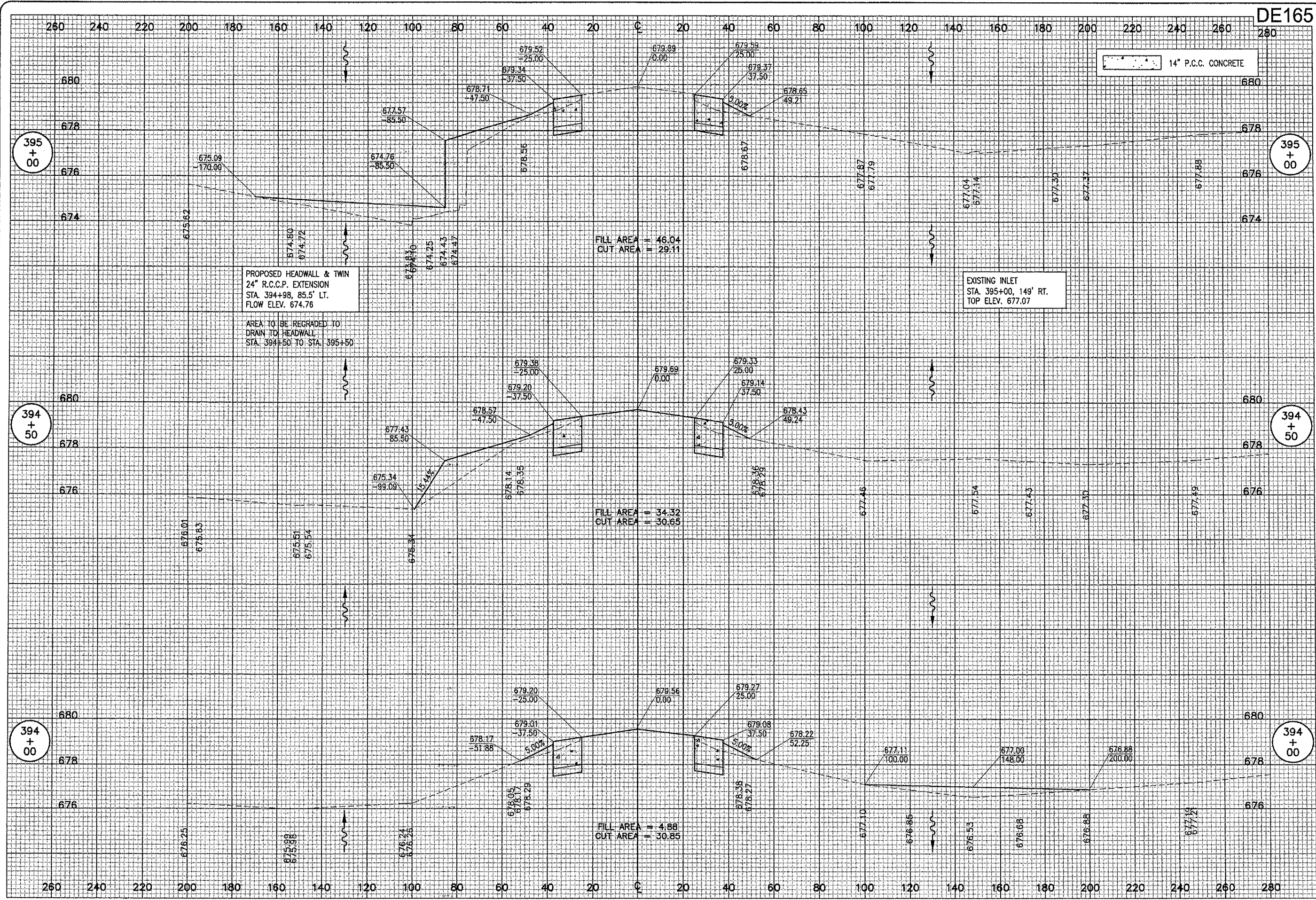
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 I.L. PROJ.: DEC-3414
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FILE	DATE	BY	REVIEWED
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317-XS.DWG	11/11/04	CCC	CAH
Scale: 1" = 50'	06/07/05		

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 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 392+50 TO STA. 393+50

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DATE	REVISION	BY



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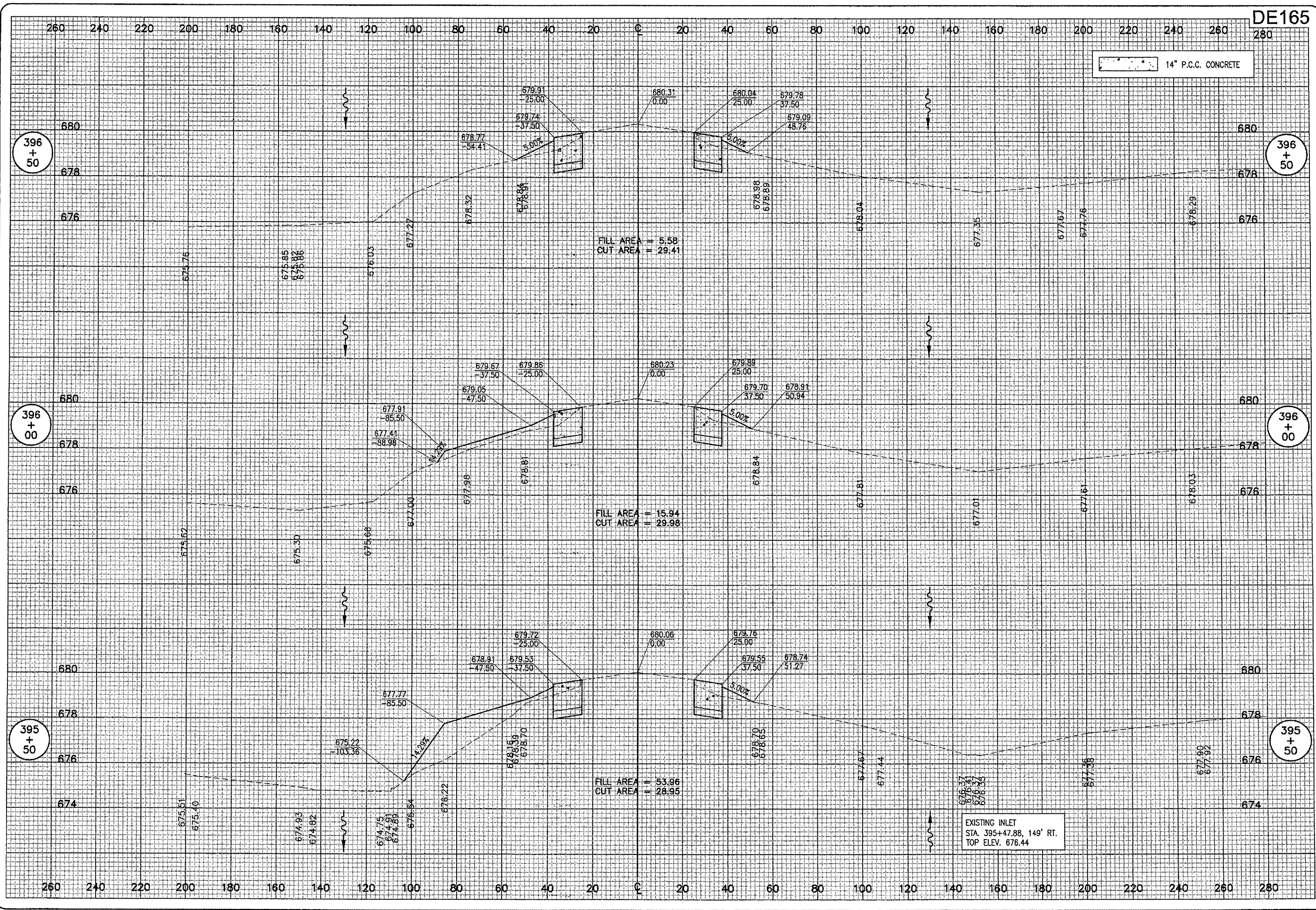
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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 394+00 TO STA. 395+00

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IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

DATE	BY	REVIEWED
11/11/04	CCC	CCC
11/11/04	CCC	CCC
06/07/05	CAH	CAH

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 File: R-319-XS.DWG
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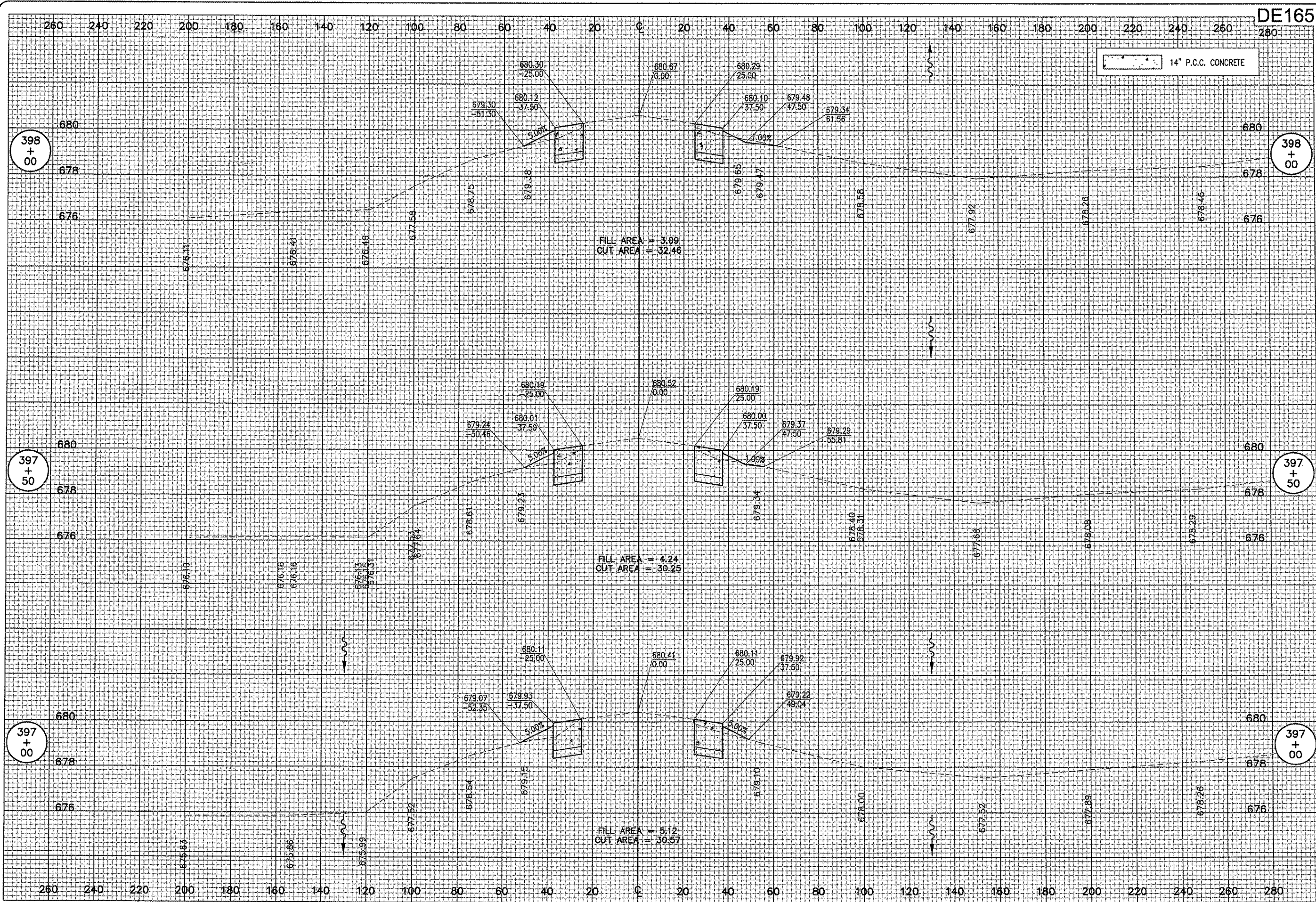
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 PHASE II**

PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 395+50 TO STA. 396+50

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DATE	REVISION

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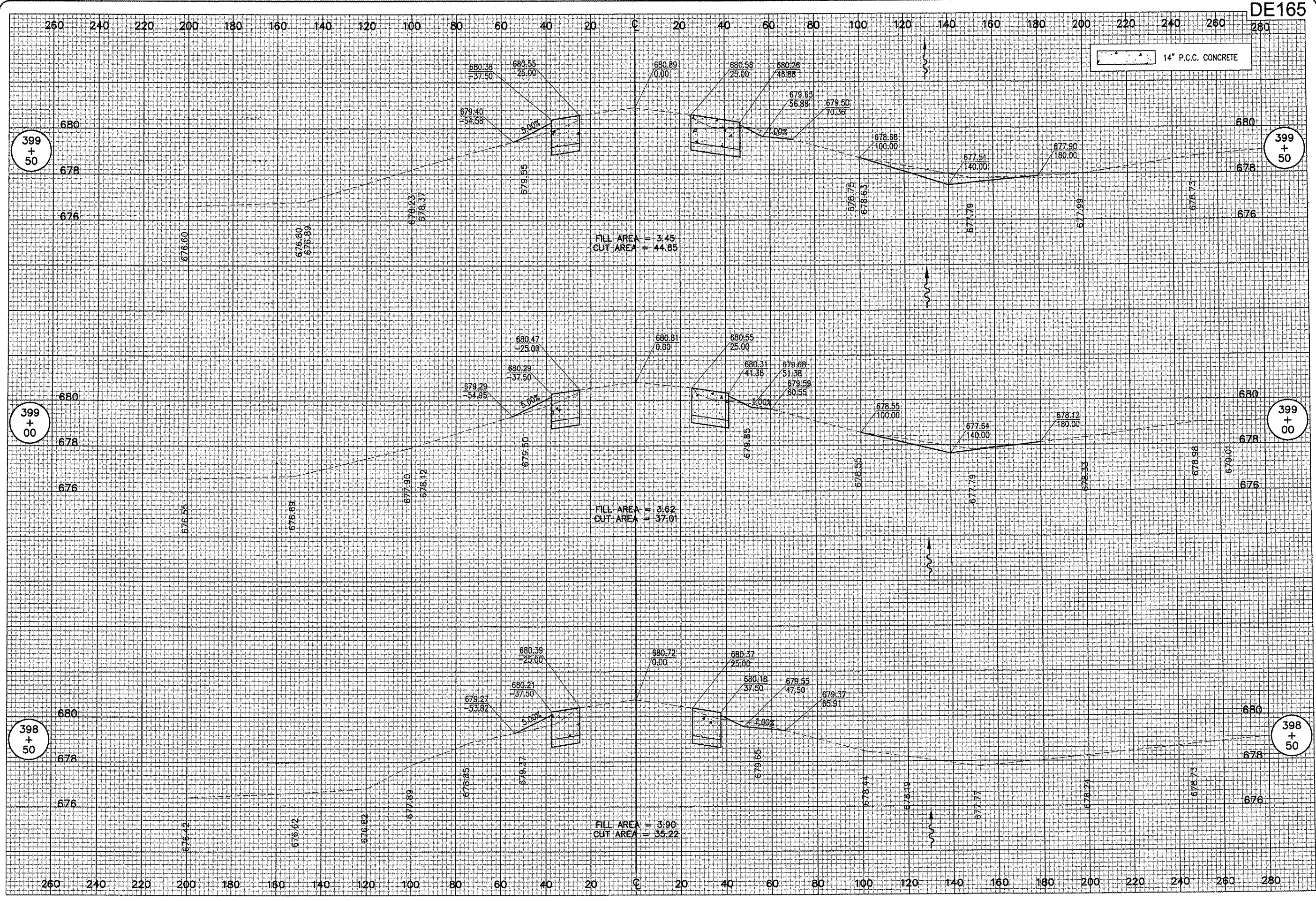
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CCC	11/11/04
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CAH	06/07/05

HEL Project No. 802-04TXD
 Plan: 8-320-XS.DWG
 Scale: 1" = 50'
 Date: 11/11/04

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 PHASE II**
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 397+00 TO STA. 398+00

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14" P.C.C. CONCRETE

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 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEI Project No. 802-04TYD	CCC	11/11/04
Plan No. 321-XS.DWG	CCC	11/11/04
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Date 11/11/04		
LAYOUT		
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REVIEWED		

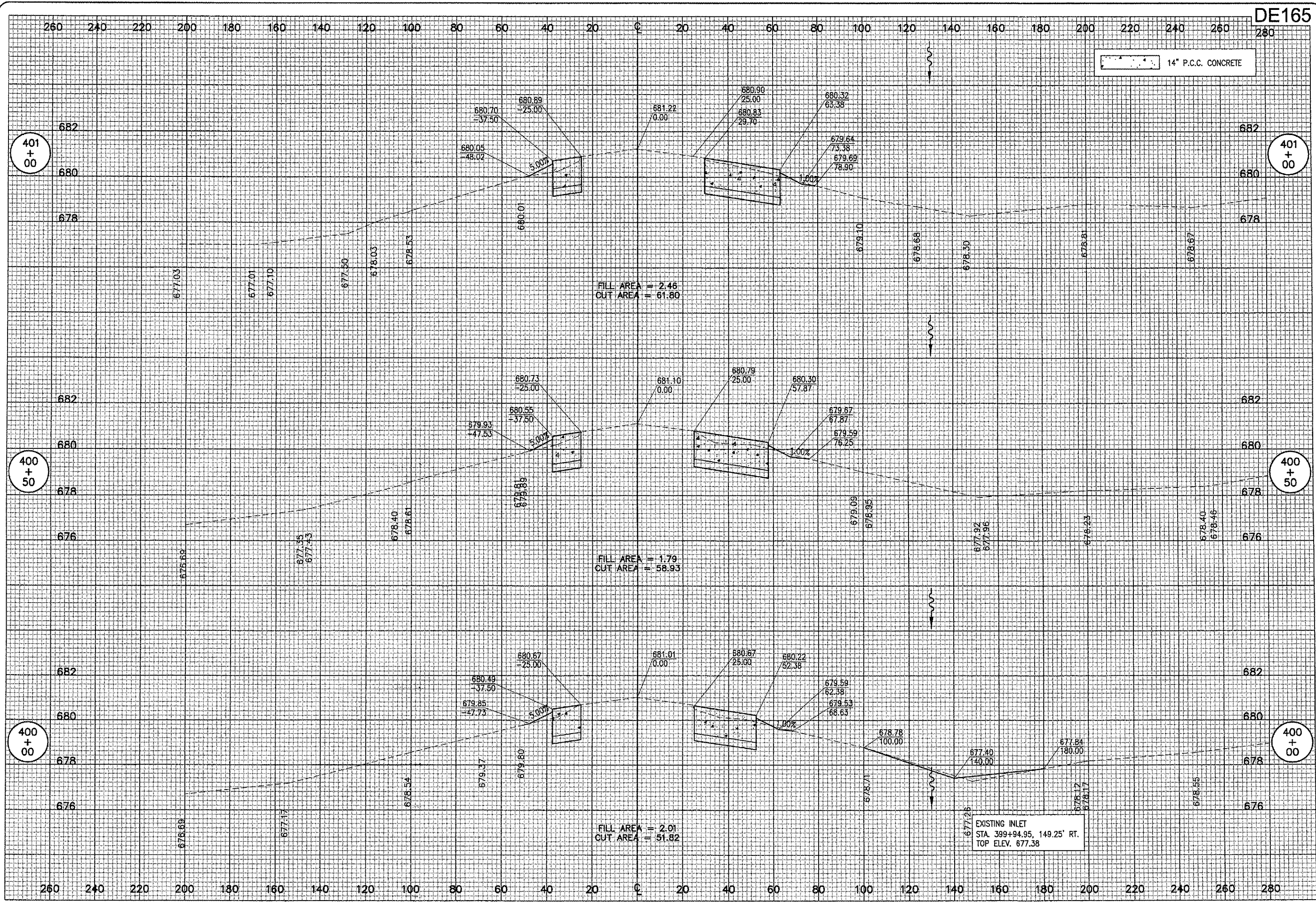
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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 398+50 TO STA. 399+50

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14" P.C.C. CONCRETE

DATE	REVISION	BY

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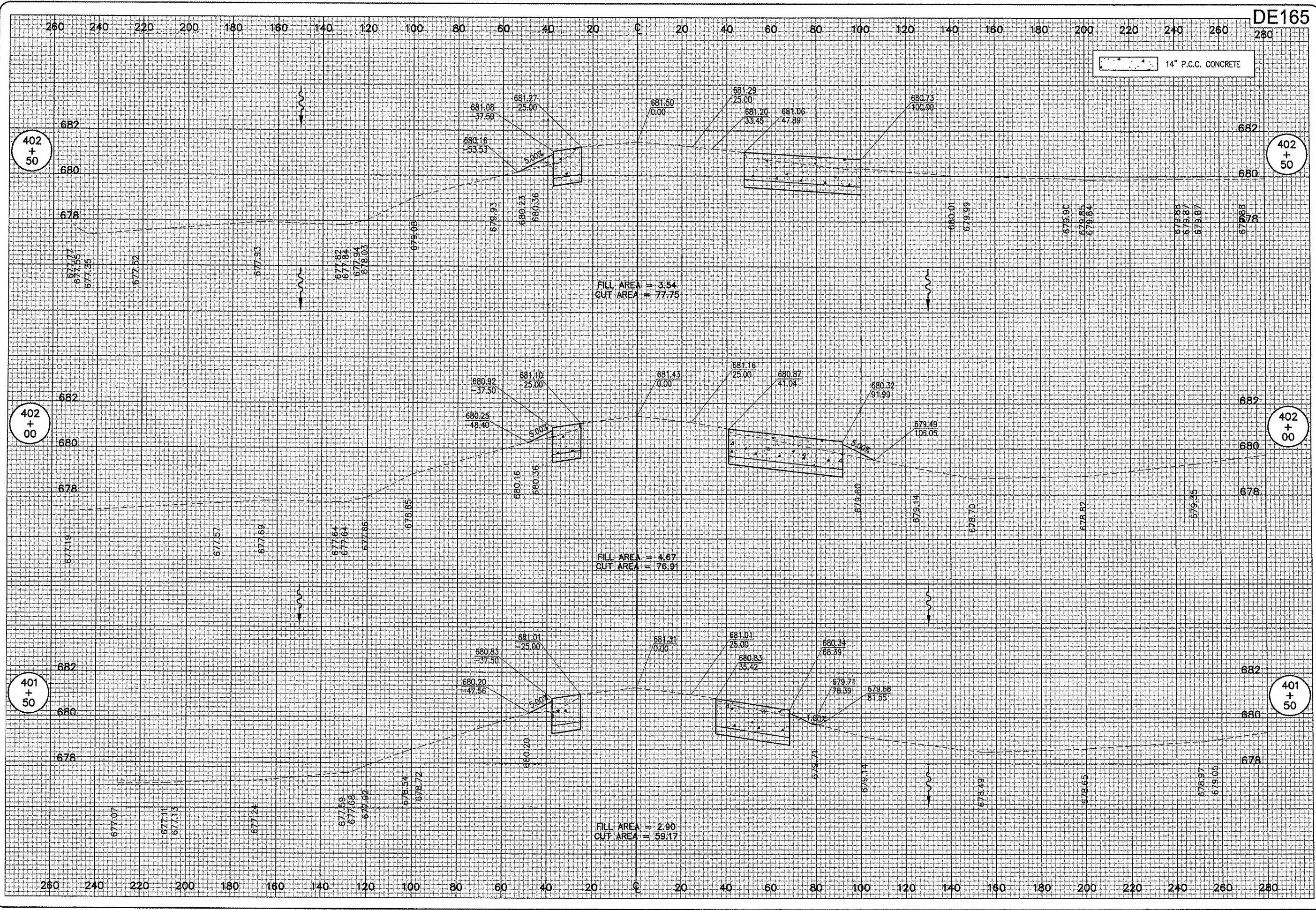
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11/11/04			11/11/04		
06/07/05			06/07/05		

H.I. Project No. 802-041XYD
 Plans R-322-XS.DWG
 Scale: 1" = 50'
 Date: 11/11/04

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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 400+00 TO STA. 401+00

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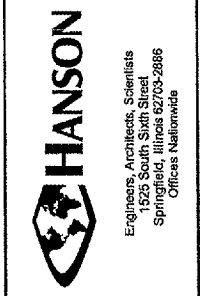


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REVISION	

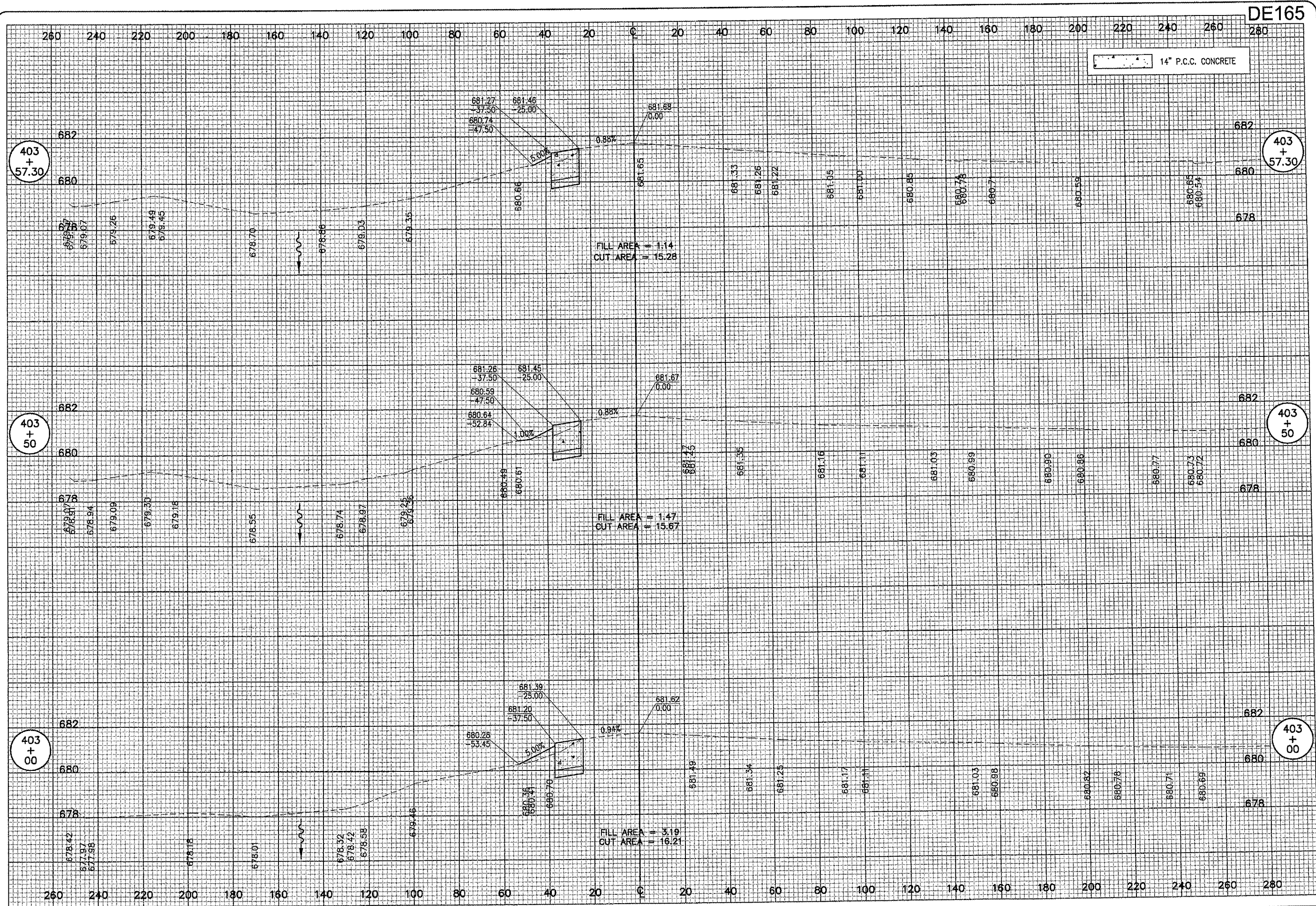


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Plan No.	323-XS.DWG
Scale	1" = 50'
Date	11/11/04
LAYOUT	CCC 11/11/04
DRAWN	CCC 11/11/04
REVIEWED	CAH 06/07/05



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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 401+50 TO STA. 402+50

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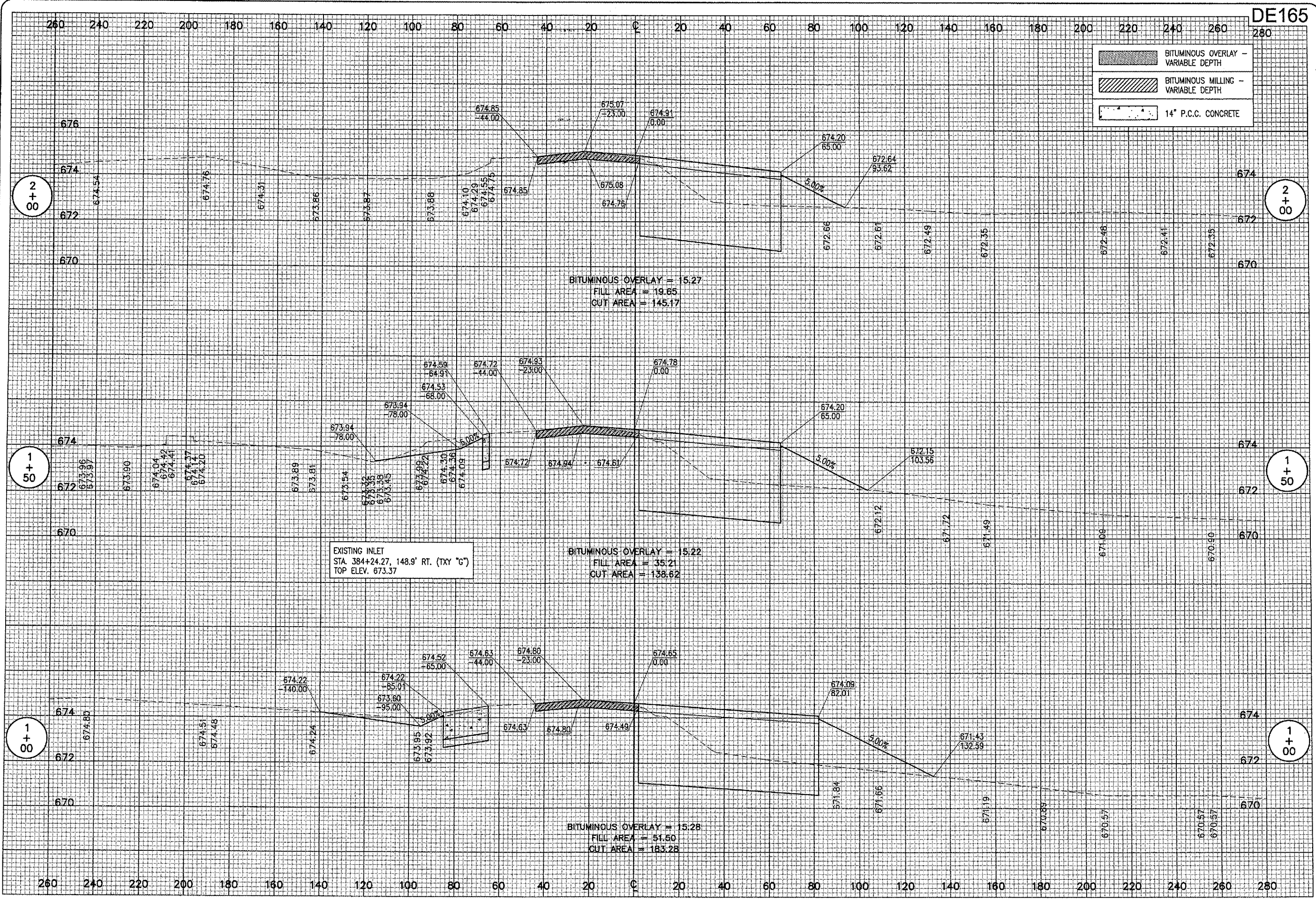
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File # 324-XS.DWG	CCC	11/11/04
Scale 1" = 50'	CAH	06/07/05
Date 11/11/04		



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 PROPOSED CROSS-SECTIONS
 TAXIWAY "G"
 STA. 403+00 TO STA. 403+55.85

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REVISION	
DATE	

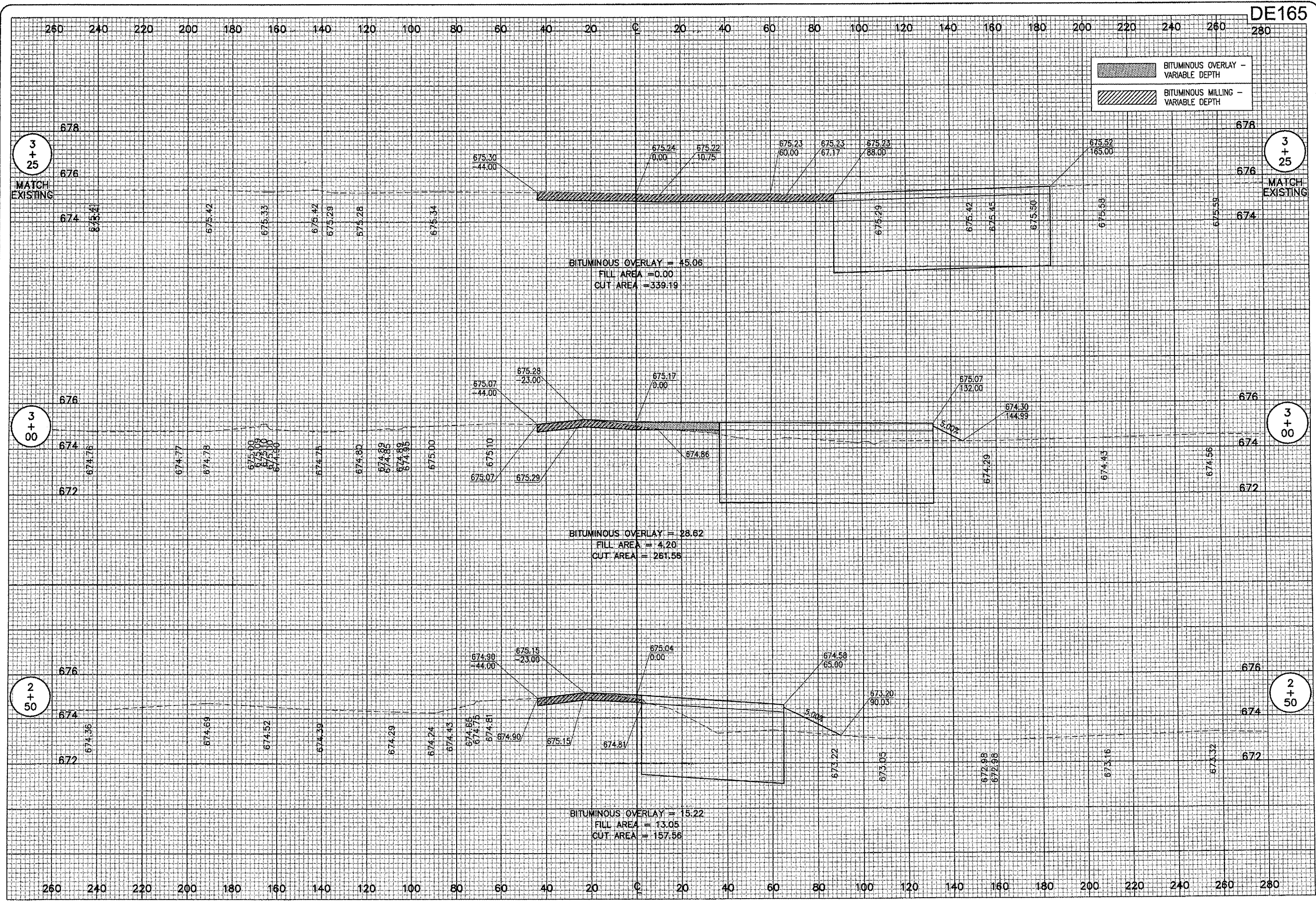


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File No. 325-XS.DWG	
Scale 1" = 50'	
Date 11/11/04	
LAYOUT	CCC 11/11/04
DRAWN	CCC 11/11/04
REVIEWED	CAH 06/07/05



TAXIWAY G WIDENING
 PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G3"
 STA. 1+00 TO STA. 2+00

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DE165

BITUMINOUS OVERLAY - VARIABLE DEPTH
 BITUMINOUS MILLING - VARIABLE DEPTH

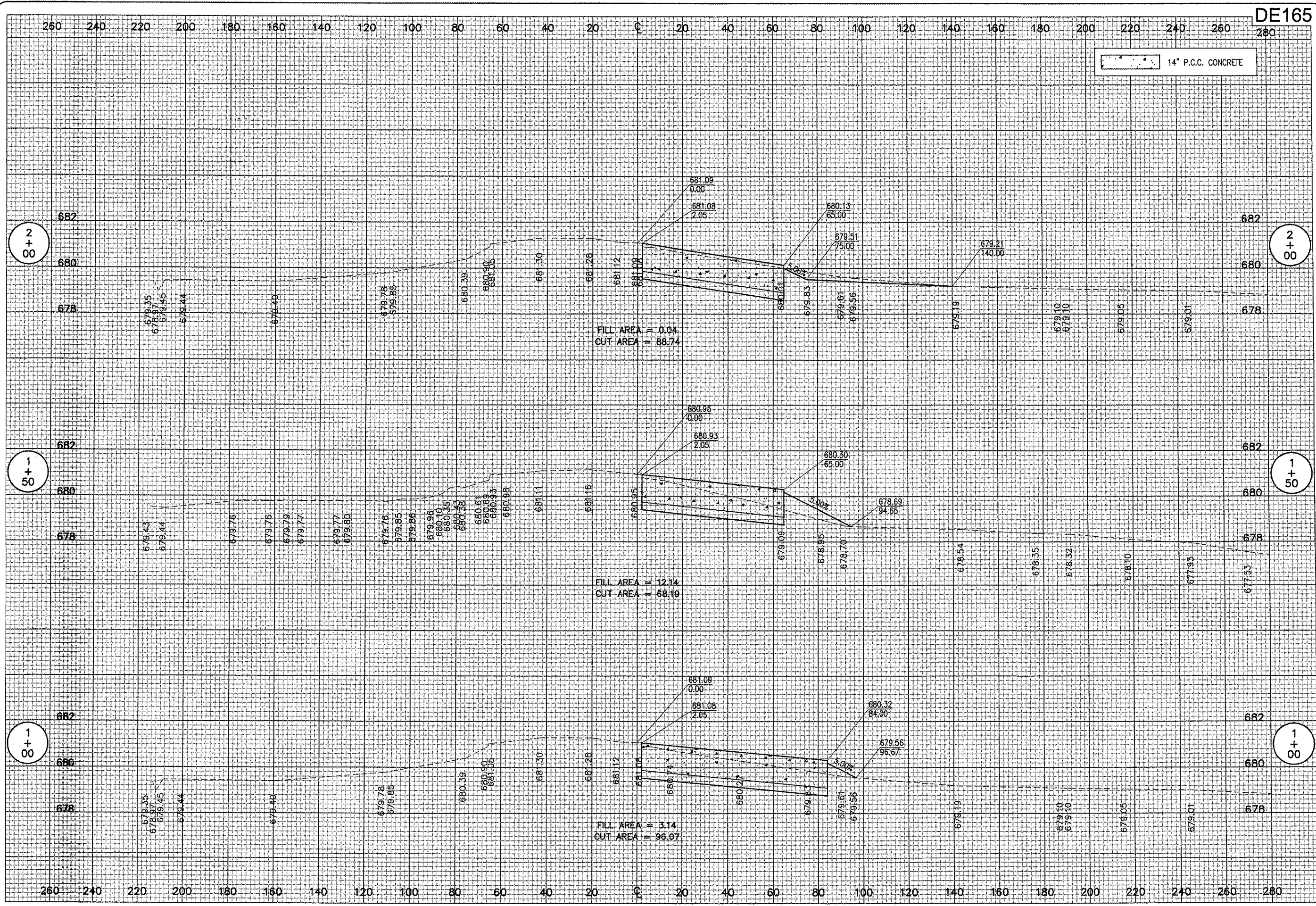
DATE	REVISION	BY

HA DECATUR AIRPORT
 DECATUR, ILLINOIS
 IL PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEI Project No. 802-04TYD	FILED	11/11/04
Drawn: R-326-XS.DWG	DATE	11/11/04
Scale: 1" = 20'H, 1" = 2'V.	DATE	11/11/04
DATE: 11/11/04	LAYOUT	CCC
	DRAWN	CCC
	REVIEWED	CAH

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G3"
 STA. 2+50 TO STA. 3+25



DE165

14" P.C.C. CONCRETE

DATE	REVISION	BY

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 ILL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

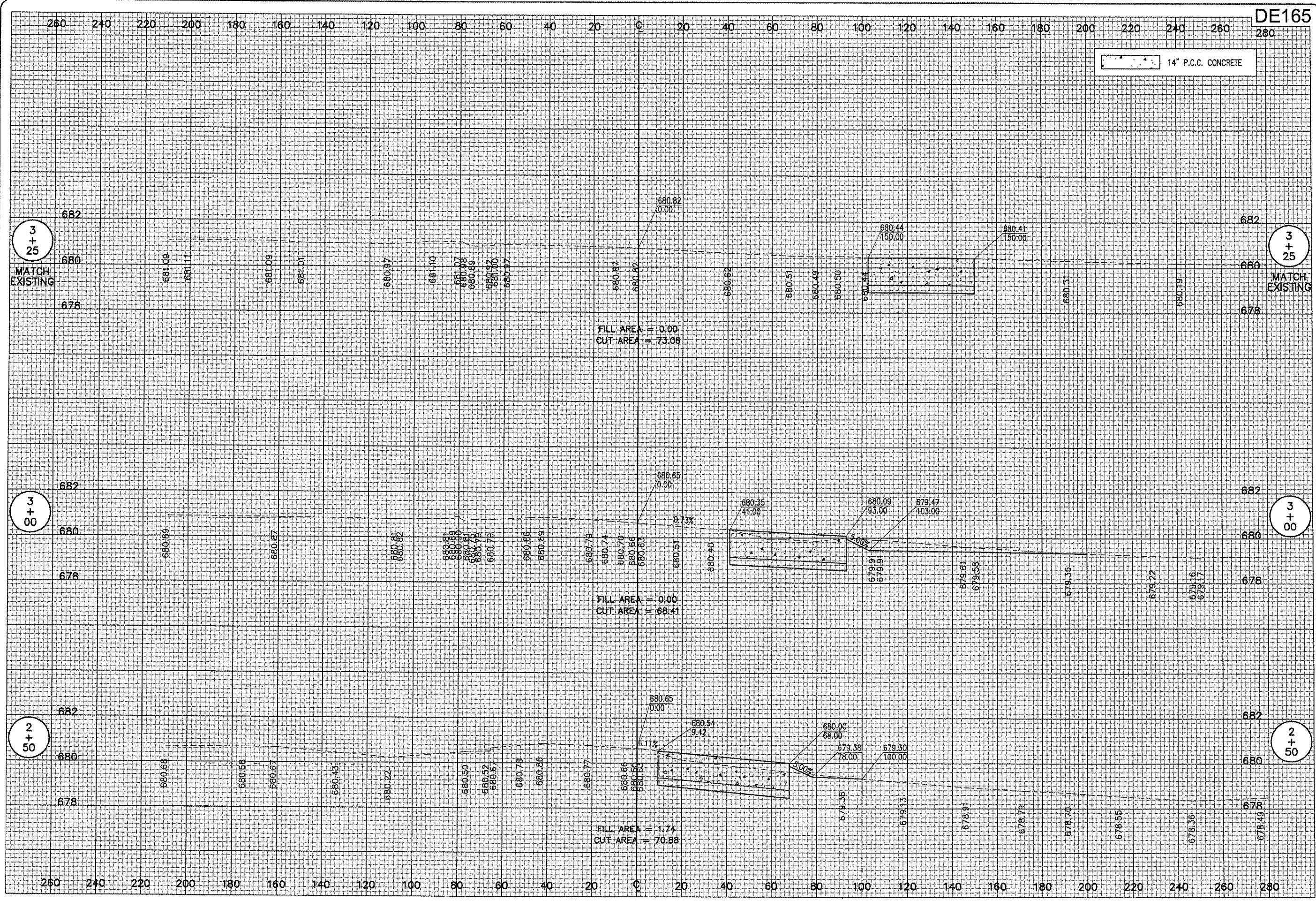
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 File No. 8-327-XS.DWG
 Scale 1" = 50'
 Date 11/11/04

LAYOUT	CCC	11/11/04
DRAWN	CCC	11/11/04
REVIEWED	CAH	06/07/05

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TAXIWAY G WIDENING
 PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G4"
 STA. 1+00 TO STA. 2+00

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DE165

14" P.C.C. CONCRETE

FILL AREA = 0.00
 CUT AREA = 73.06

FILL AREA = 0.00
 CUT AREA = 68.41

FILL AREA = 1.74
 CUT AREA = 70.88

DATE	REVISION	BY





HE Project No. 802-04TXXD	CCC	11/11/04
Plan No. 32B-XS.DWG	CCC	11/11/04
Scale: 1" = 20'H, 1" = 2'V.	CAH	06/07/05
Date: 11/11/04		

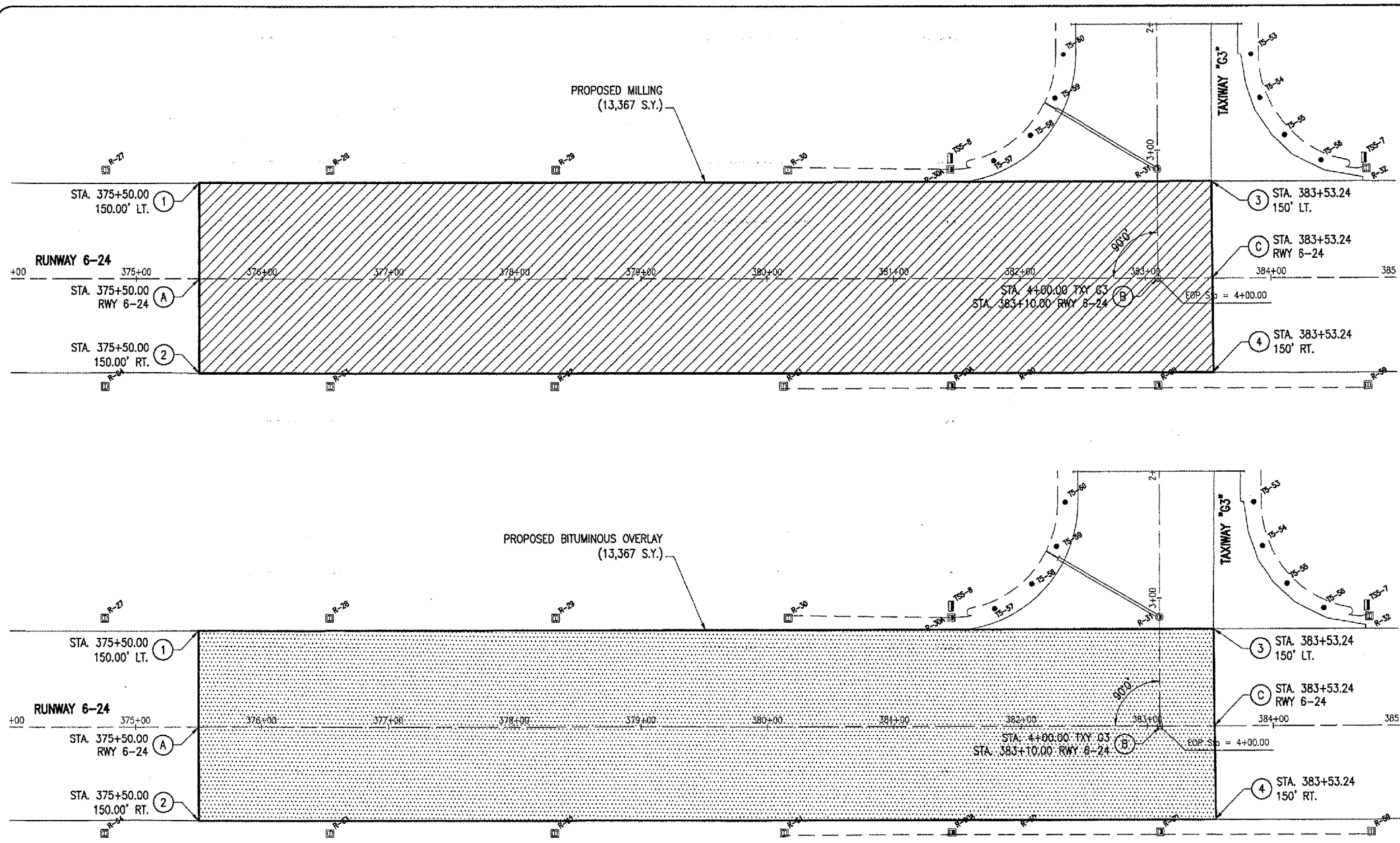


TAXIWAY G WIDENING
 PHASE II
 PROPOSED CROSS-SECTIONS
 TAXIWAY "G4"
 STA. 2+50 TO STA. 3+25

CONTRACT No. 2

83 83 of 110 sheets	TAXIWAY G WIDENING PHASE II	 Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62761 Offices Nationwide	HEI Project No. 802-041XXD Drawings R-0030TY.DWG Scale N/A Date 11/12/04	 DECATUR, ILLINOIS I.L. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28	DATE	REVISION	BY
	CONTRACT No. 2		LAYOUT DRAWN REVIEWED		05/07/05		

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BITUMINOUS PAVEMENT MILLING (AR401650) DE165

THE DESIGNATED MILLING AREAS ON THIS SHEET WILL BE CUT OR TRIMMED AS SHOWN ON THE CROSS-SECTIONS. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE SPECIAL PROVISIONS FOR BITUMINOUS SURFACE COURSE, 401-4.14 SURFACE TEST OF STANDARD SPECIFICATIONS.

THE EXISTING PAVEMENT WILL BE SAWS IF A VERTICAL FACE IS NOT ACHIEVED BY THE MILLING OPERATIONS. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

DATE	REVISION

DECATUR AIRPORT
DECATUR, ILLINOIS

IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

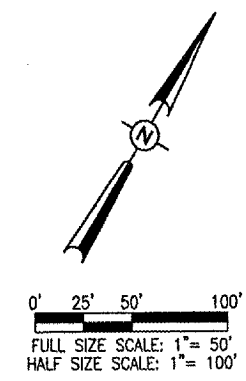
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Drawn: P-121CON.DWG	Scale: 1"=50'	
LAYOUT	WJM	10/28/04
DRAWN	CCC	10/28/04
REVIEWED	WJM	06/07/05

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TAXIWAY G WIDENING
PHASE II
RWY 6-24 RECONSTRUCTION
PROPOSED
CONSTRUCTION PLAN

LEGEND

- EXISTING PAVEMENT
- PROPOSED MILLING
- PROPOSED BITUMINOUS PAVEMENT



SCOPE OF WORK

THE WORK CONSISTS OF THE RECONSTRUCTION OF RUNWAY 6/24 BETWEEN STA. 375+50 AND STA. 383+53.24. THE WORK INCLUDES MILLING, ASPHALT PAVING, GROOVING, EARTH SHOULDERS, SEEDING, MULCHING, RUNWAY AND TAXIWAY MARKING, ASPHALT MATCHING ON TAXIWAY "G3" AND THE PCC PAVEMENT ON RUNWAY 6/24.

COORDINATE DATA - RWY "6-24" CENTERLINE

ID.	STATION	OFFSET	NORTHING	EASTING
A	375+50.00	©	1154275.55	837153.05
B	383+10.00	©	1154661.62	837807.69
C	383+53.24	©	1154683.58	837844.94

COORDINATE DATA - RUNWAY "6-24"

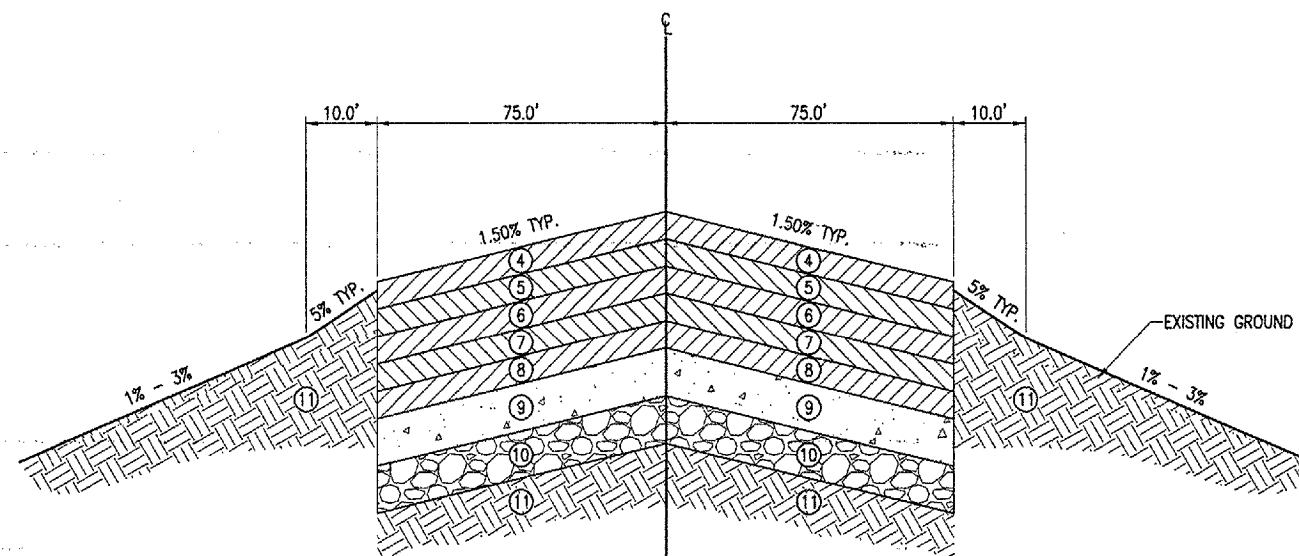
NO.	STATION	OFFSET	NORTHING	EASTING
1	375+50.00	150.00' LT.	1154340.15	837114.96
2	375+50.00	150.00' RT.	1154211.28	837190.95
3	383+53.24	150.00' LT.	1154748.79	837806.48
4	383+53.24	150.00' RT.	1154619.43	837882.77

QUANTITIES - PAVEMENT

ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR401610	BITUMINOUS SURFACE COURSE	TON	3,079	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	13,387	
AR603510	BITUMINOUS TACK COAT	GAL.	2,677	

LEGEND FOR TYPICAL SECTION

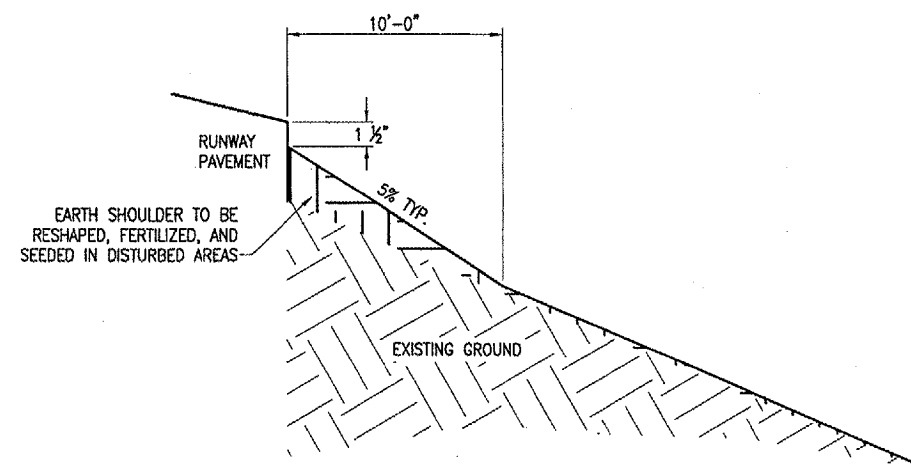
- ① PROPOSED AR401: BITUMINOUS SURFACE COURSE - 4" (2 LIFTS)
 - ② PROPOSED AR603: BITUMINOUS TACK COAT (RATE PER SPECIFICATION)
 - ③ PROPOSED AR602: BITUMINOUS PRIME COAT (RATE PER SPECIFICATION)
 - ④ EXISTING AR401: BITUMINOUS SURFACE COURSE - 4"
 - ⑤ EXISTING AR201: BITUMINOUS BASE COURSE - 2"-3"
 - ⑥ EXISTING AR401: BITUMINOUS SURFACE COURSE - 2"
 - ⑦ EXISTING AR201: BITUMINOUS BASE COURSE - 0"-10" VARIABLE DEPTH
 - ⑧ EXISTING AR401: BITUMINOUS SURFACE COURSE - 4"
 - ⑨ EXISTING AR501: PCC PAVEMENT - 9"
 - ⑩ EXISTING AR208: AGGREGATE SUBBASE COURSE - 8"
 - ⑪ EXISTING GROUND
- BITUMINOUS MILLING - VARIABLE 3" TO 5"



EXISTING RUNWAY PAVEMENT - TYPICAL SECTION

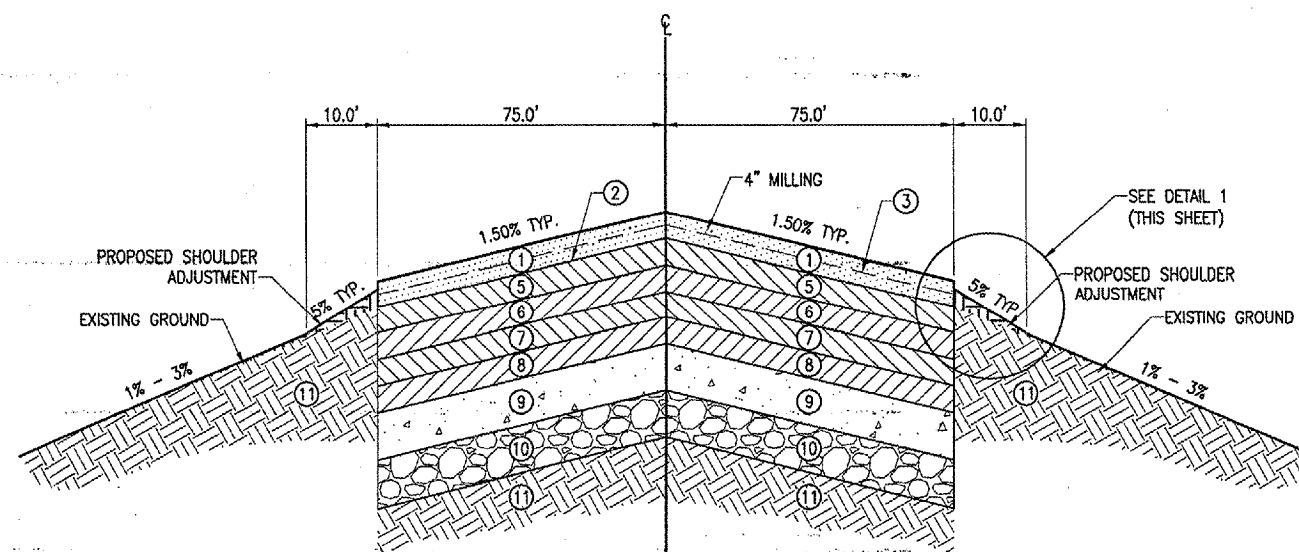
"NOT TO SCALE"

RUNWAY "6/24" STA. 375+50 TO STA. 383+56.20



DETAIL 1

EARTH SHOULDER



PROPOSED RUNWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"

RUNWAY "6/24" STA. 375+50 TO STA. 383+56.20

NOTE:


THE CONTRACTOR WILL REGRADE, SEED AND FERTILIZE THE DISTURBED EARTH SHOULDER. THE GRADING SHALL SHAPE THE EARTH SHOULDER TO CONFORM TO THE EARTH SHOULDER DETAIL. FERTILIZING AND SEEDING SHALL BE ACCOMPLISHED IN ACCORDANCE TO SPECIFICATION ITEM AR901510.

NO ADDITIONAL PAYMENT FOR THE GRADING AND SEEDING ITEM WILL BE MADE TO THE CONTRACTOR. PAYMENT FOR THE WORK SHALL BE INCLUDED IN THE BID AMOUNT PER TON FOR THE BITUMINOUS SURFACE COURSE.

ANY TURF AREAS DISTURBED BY THE CONTRACTOR WILL BE RESHAPED, FERTILIZED AND SEEDED ACCORDING TO THE ABOVE PARAGRAPH.

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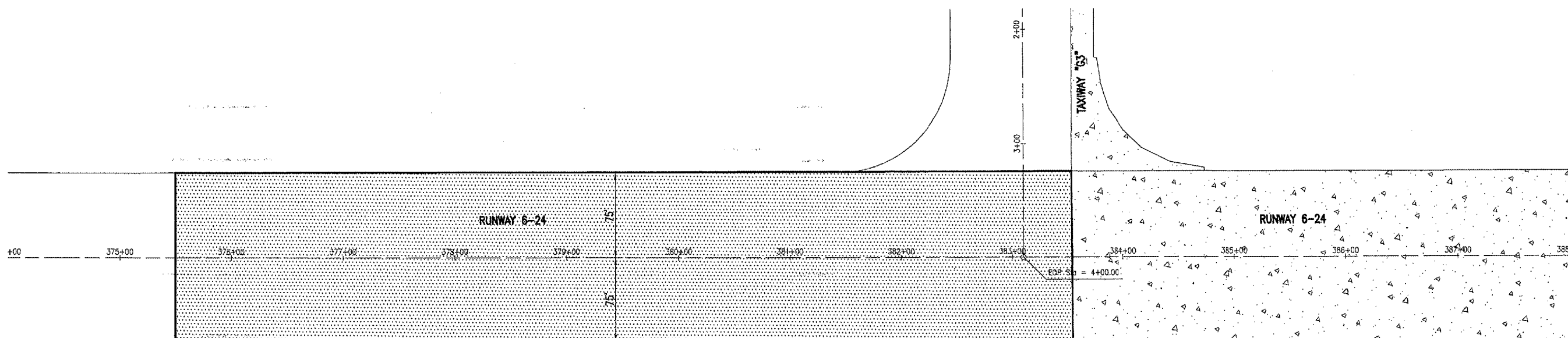
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DECATUR, ILLINOIS
 A.I.P. PROJ.: 3-17-0033-28
 ILL. PROJ.: DEC-3414

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Filename R-522TYP.DWG	DRAWN	
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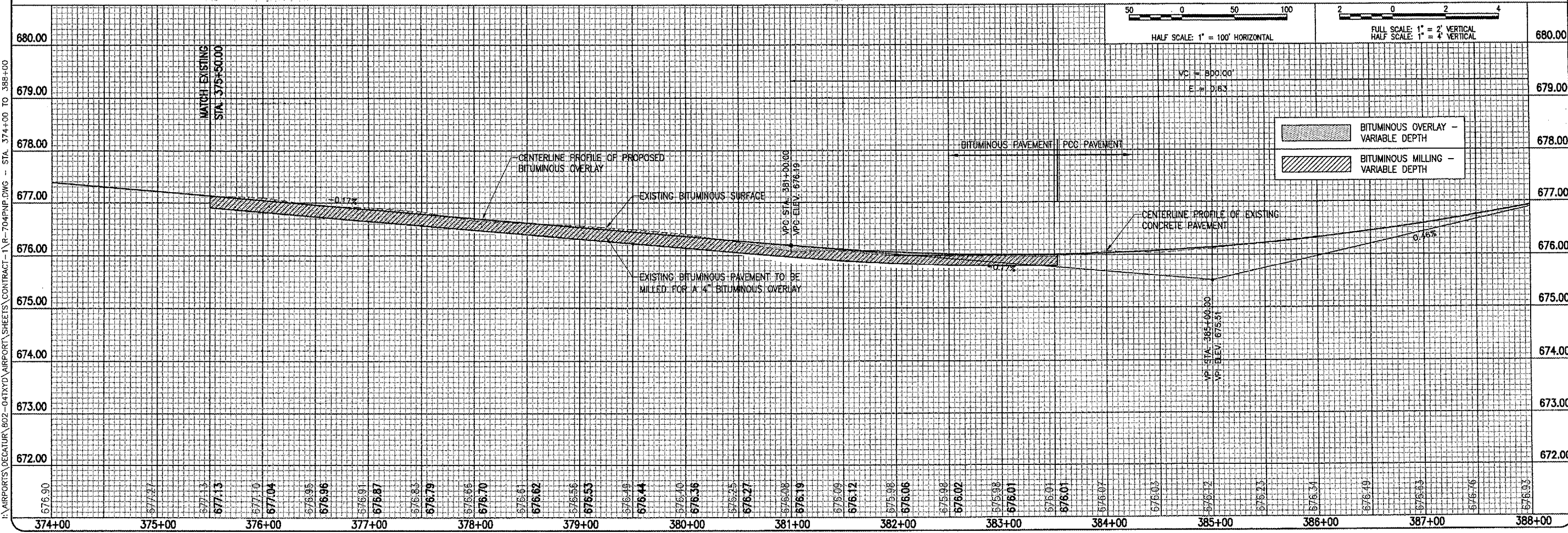
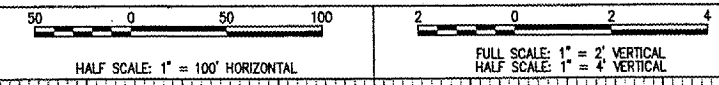
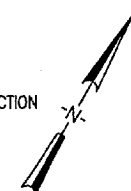

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TAXIWAY G WIDENING
PHASE II
 RWY 6-24 RECONSTRUCTION
 EXISTING & PROPOSED
 TYPICAL SECTIONS



LEGEND

- EXISTING BITUMINOUS PAVEMENT
- EXISTING P.C.C. PAVEMENT
- PROPOSED BITUMINOUS RECONSTRUCTION



DATE	REVISION	BY

DECATUR AIRPORT
DECATUR, ILLINOIS

ILL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

LAYOUT	WJM	CCC	WJM

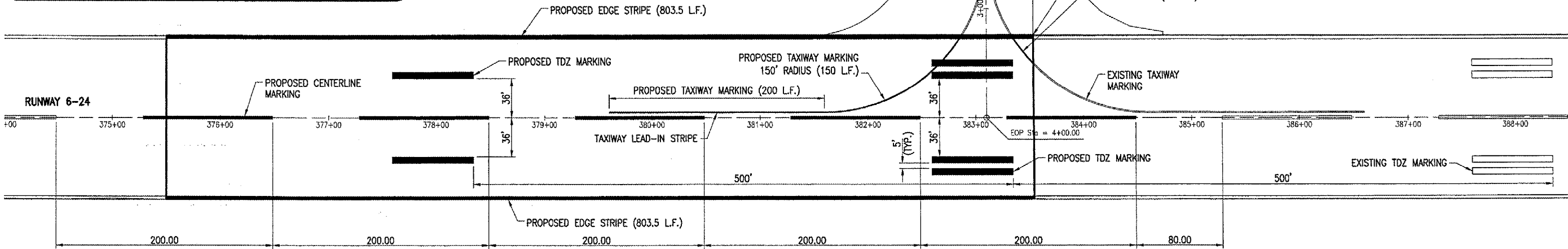
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 DRAWING No. 704PMP.DWG
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 Date 03/29/05

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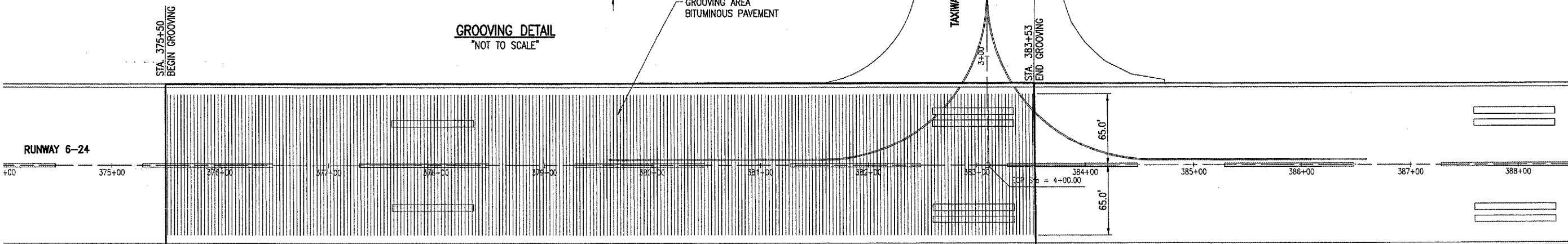
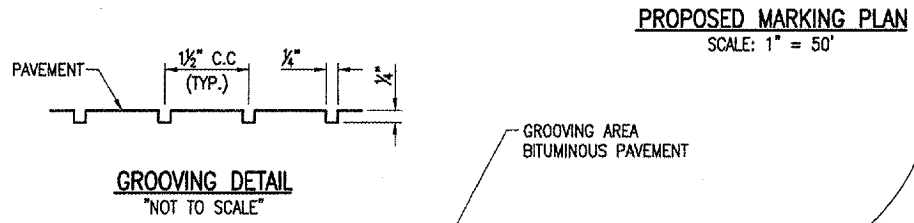
**TAXIWAY G WIDENING
 PHASE II**
 RWY 6-24 RECONSTRUCTION
 PROPOSED PLAN & PROFILE
 STA. 375+50 TO 383+53.24

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PAVEMENT MARKING-WATERBORNE - AR620520				
WORK ITEM	MARKING COLOR	UNIT AREA	NUMBER REQUIRED	TOTAL AREA (S.F.)
RUNWAY 6-24 CENTERLINE STRIPE	WHITE	360	5	1,800
TOUCHDOWN ZONE	WHITE	450	6	2,700
EDGE STRIPES	WHITE	2410.5	2	4,821
TAXIWAY LEAD-IN STRIPES	YELLOW	380	AS SHOWN	380
			TOTAL	9,701



PAVEMENT GROOVING - RUNWAY 18-36			
ITEM No.	DESCRIPTION	UNIT	TOTAL
AR401640	BITUMINOUS PAVEMENT GROOVING	S.Y.	11,602

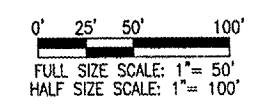
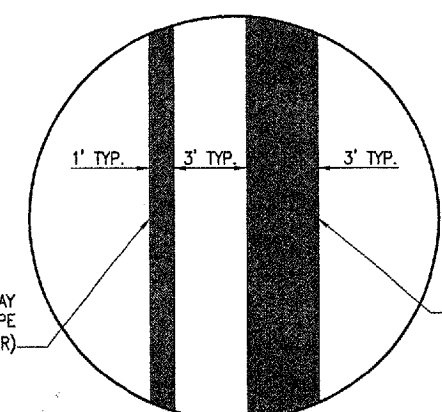


MARKING NOTES

- ALL PROPOSED RUNWAY MARKING SHALL BE SOLID AND WHITE IN COLOR.
- ALL PROPOSED TAXIWAY MARKING SHALL BE SOLID, 1 FOOT IN WIDTH, AND YELLOW IN COLOR.
- THE RUNWAY CENTERLINE SHALL BE 3 FEET WIDE BY 120 FEET IN LENGTH AND SPACED AS SHOWN ON THE PLANS.
- RUNWAY EDGE STRIPES SHALL BE 3 FEET WIDE BY THE LENGTH SHOWN ON THE PLANS.
- TOUCHDOWN ZONE (TDZ) MARKINGS CONSIST OF GROUPS OF ONE, TWO, OR THREE RECTANGULAR BARS, SYMMETRICALLY ARRANGED IN PAIRS ABOUT THE RUNWAY CENTERLINE. THE TDZ MARKINGS SHALL BE 6 FEET WIDE BY 75 FEET LONG. SEE THIS SHEET FOR LOCATIONS AND SPACING.
- ALL PROPOSED MARKING WILL BE PAINTED WITH TWO APPLICATIONS. THE FIRST APPLICATION WILL SERVE AS TEMPORARY MARKING AND SHALL BE APPLIED NO SOONER THAN 48 HOURS AFTER THE FINAL PLACEMENT OF THE BITUMINOUS SURFACE COURSE. THE FIRST APPLICATION WILL ALLOW THE RUNWAYS TO BE OPENED DURING THE CURE PERIOD REQUIRED FOR THE GROOVING. AFTER THE GROOVING IS COMPLETED, THE SECOND APPLICATION OF MARKING WILL BE APPLIED.
- ALL PROPOSED MARKING WILL BE APPLIED WITH A REFLECTIVE MEDIA ON THE SECOND APPLICATION.
- CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

GROOVING NOTES

- GROOVING WILL NOT COMMENCE UNTIL THE PAVEMENT HAS CURED SUFFICIENTLY THAT IT DOES NOT RAVEL OR SUSTAIN OTHER DAMAGE DURING THE GROOVING OPERATION.
- THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
- RUNWAY 6/24 WILL BE GROOVED CONTINUOUSLY FROM STATION 375+50 TO STA. 383+53.
- THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP, AND PLACED 1/2" CENTERS.
- ALL SOLID MATERIALS GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND HAULED TO A LOCATION OFF THE AIRPORT SITE AT THE CONTRACTOR'S OWN EXPENSE.
- NO MATERIAL WHICH IS HARMFUL TO VEGETATION WILL BE FLUSHED FROM THE GROOVED PAVEMENT ONTO THE TURFED SHOULDER, BUT WILL BE PICKED UP AND HAULED TO A LOCATION OFF THE AIRPORT SITE AT THE CONTRACTOR'S OWN EXPENSE.
- AT THE END OF EACH WORK DAY, THE RUNWAY WILL BE FLUSHED, CLEANED, AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.
- PAVEMENT GROOVING WILL BE PAID FOR UNDER ITEM:
AR401640 "BITUMINOUS PAVEMENT GROOVING" PER S.Y.



DATE	REVISION	BY

IL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

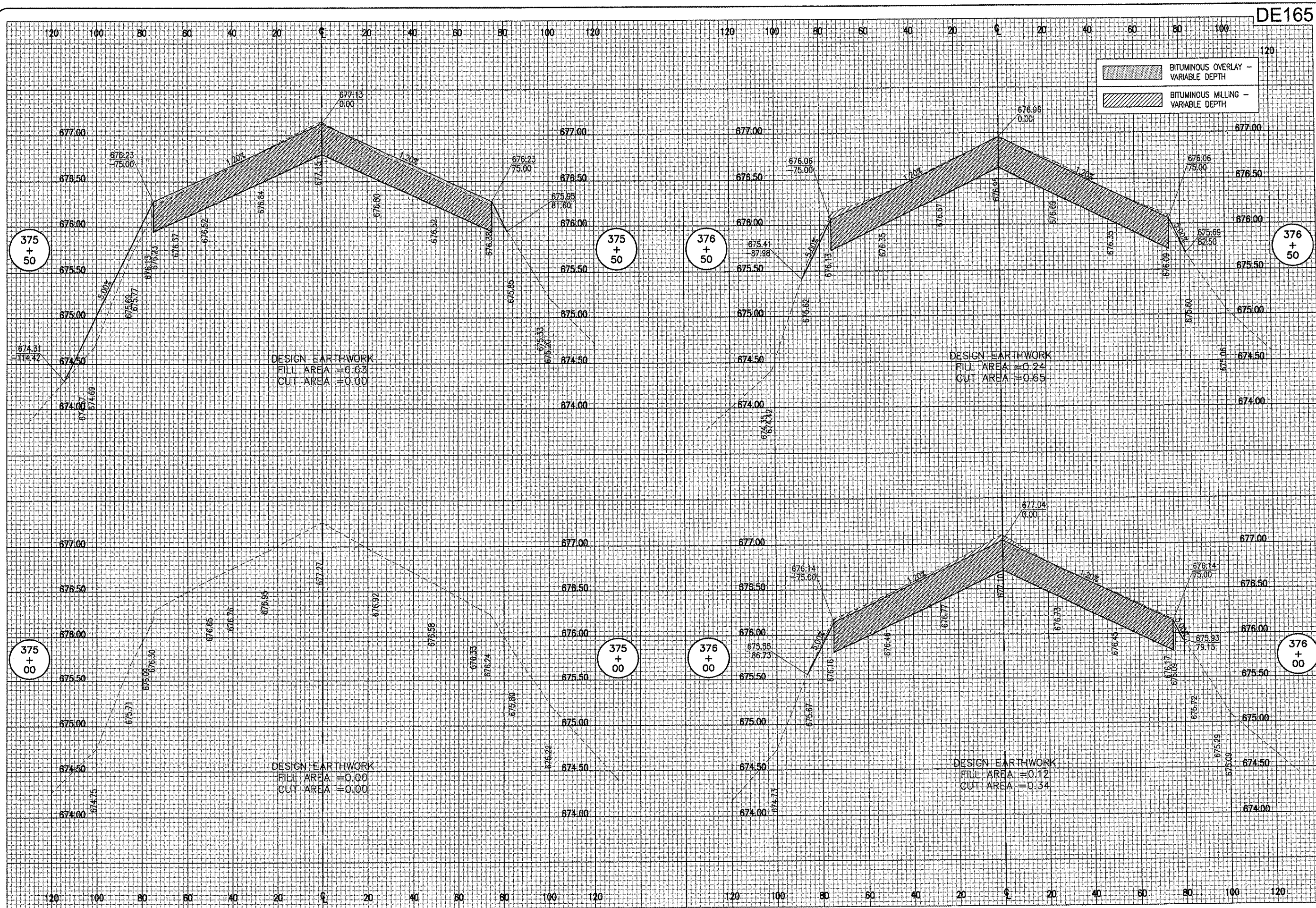
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Plan No. 152MRK.DWG	CCC	10/28/04
Scale 1"=50'	WUM	06/07/05
Date 10/28/04		

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TAXIWAY G WIDENING
 PHASE II
 RWY 6/24 RECONSTRUCTION &
 PROPOSED MARKING &
 GROOVING PLAN

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DE165

BITUMINOUS OVERLAY - VARIABLE DEPTH
 BITUMINOUS MILLING - VARIABLE DEPTH

BY	REVISION	DATE

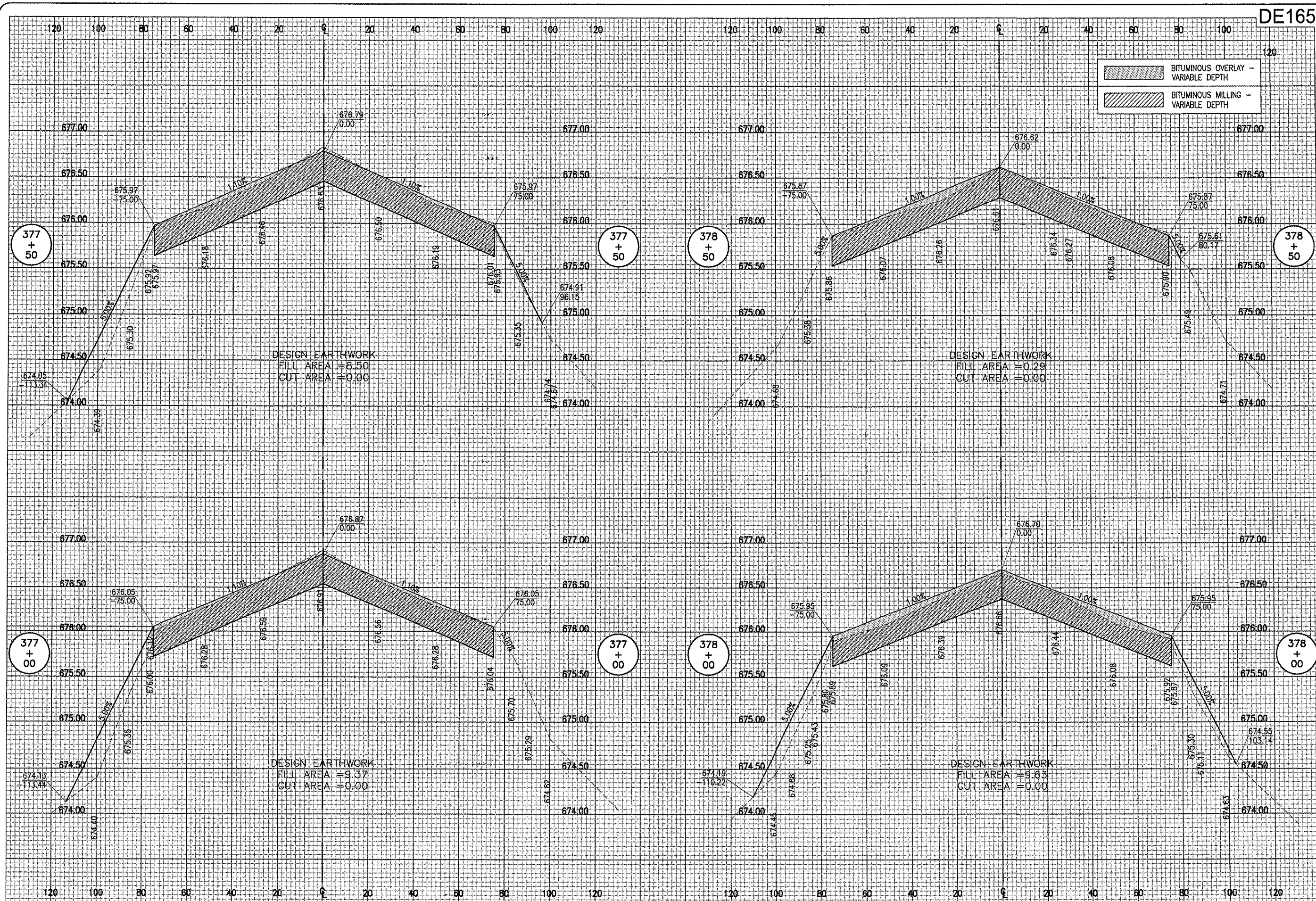
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 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

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Scale 1"=50'	CCC	01/27/05
Date 01/27/05	CAH	06/07/05
LAYOUT		
DRAWN		
REVIEWED		

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TAXIWAY G WIDENING
 PHASE II
 RWY 6-24 RECONSTRUCTION
 PROPOSED CROSS-SECTIONS
 STA. 375+00 TO STA. 376+50

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DE165

	BITUMINOUS OVERLAY - VARIABLE DEPTH
	BITUMINOUS MILLING - VARIABLE DEPTH

BY	
REVISION	
DATE	



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IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

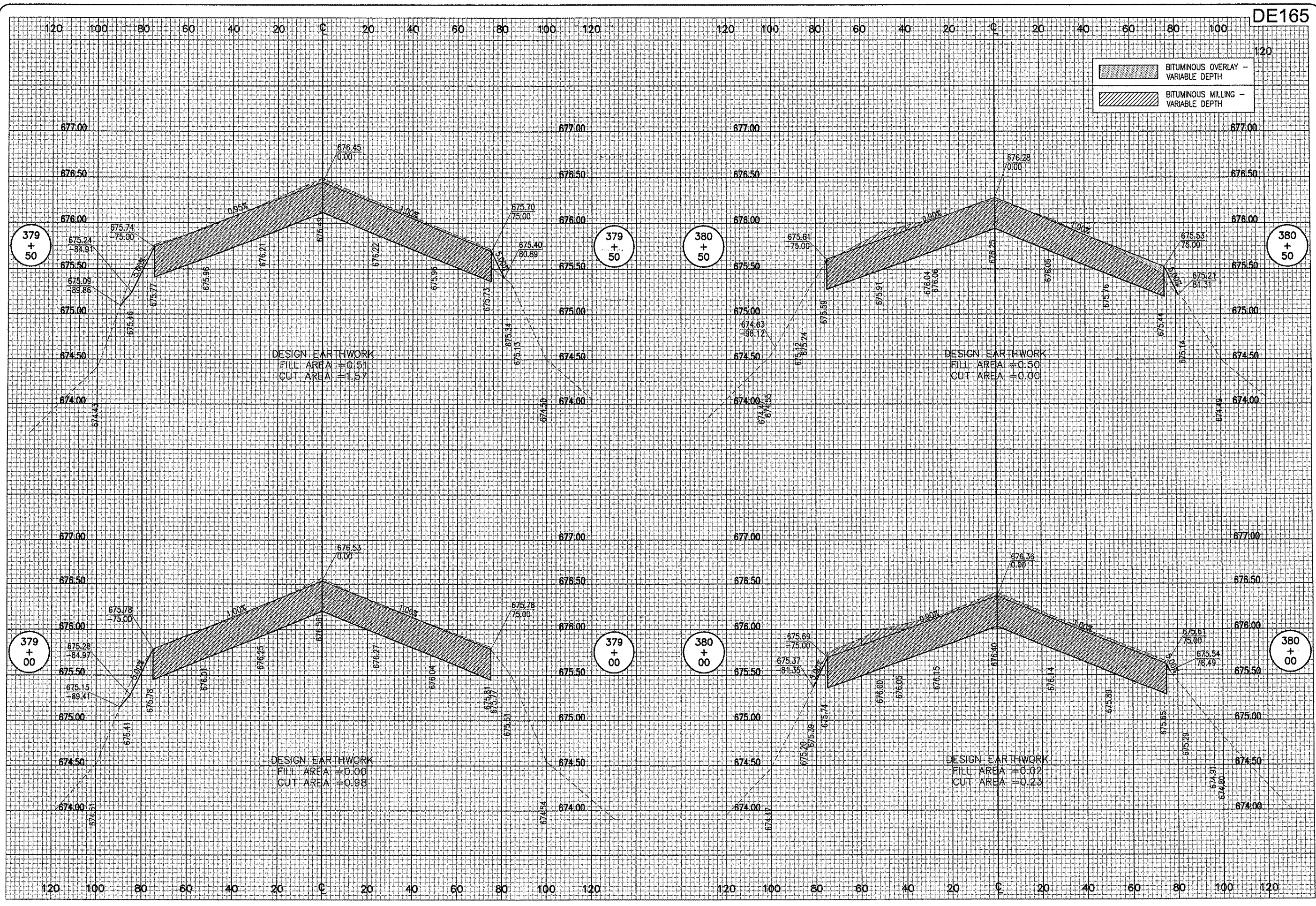
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Date	01/27/05	
LAYOUT	CCC	01/27/05
DRAWN	CCC	01/27/05
REVIEWED	CAH	06/07/05



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TAXIWAY G WIDENING
 PHASE II
 RWY 6-24 RECONSTRUCTION
 PROPOSED CROSS-SECTIONS
 STA. 377+00 TO STA. 378+50

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DE165

BITUMINOUS OVERLAY - VARIABLE DEPTH
 BITUMINOUS MILLING - VARIABLE DEPTH

REVISION	DATE

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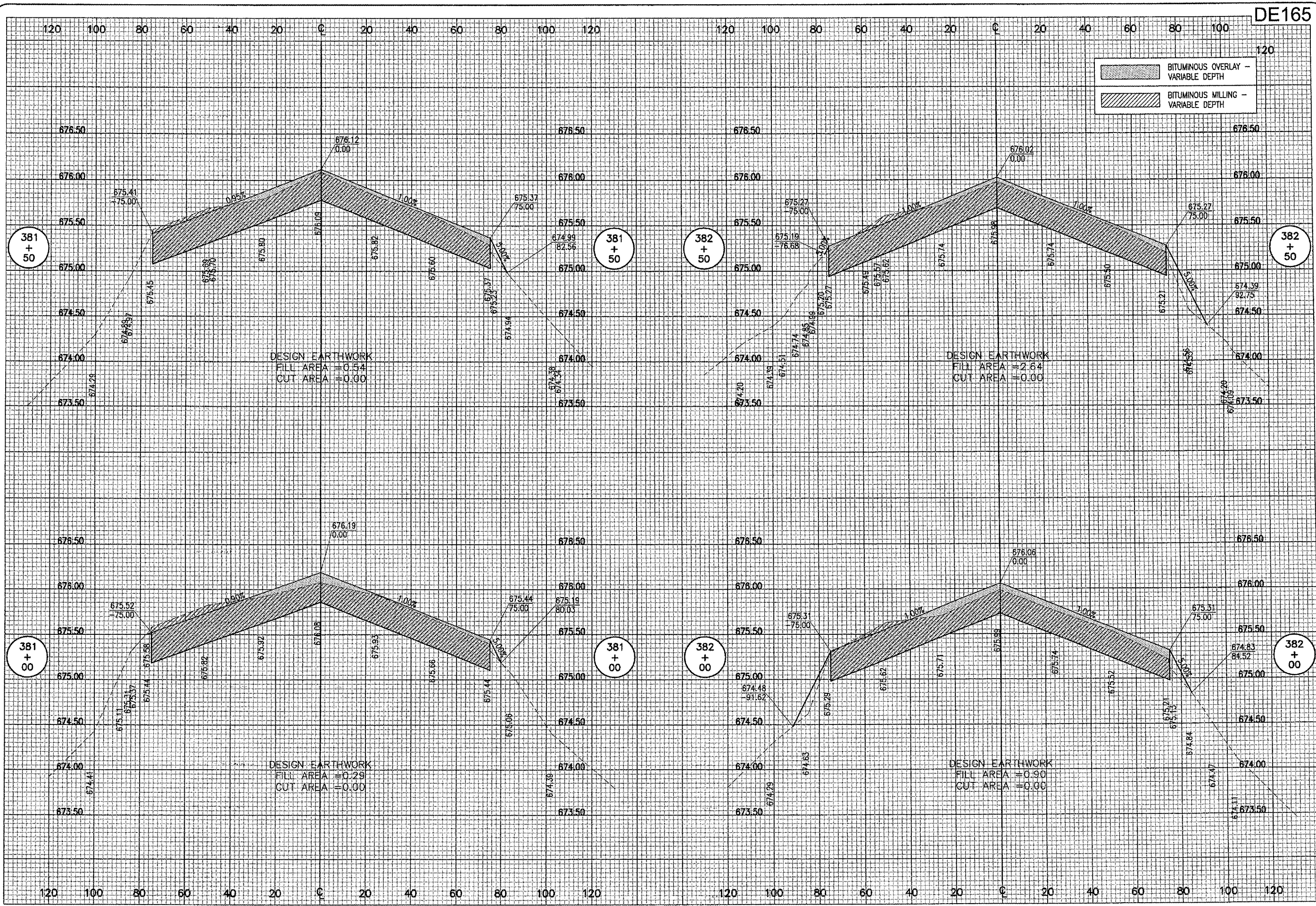
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ILL. Permit No. 802-04TXD
 File No. 333-XS.DWG
 Scale: 1"=50'
 Date: 01/27/05

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**TAXIWAY G WIDENING
 PHASE II**
 RWY 6-24 RECONSTRUCTION
 PROPOSED CROSS-SECTIONS
 STA. 379+00 TO STA. 380+50

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DATE	REVISION	BY

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 A.I.P. PROJ.: 3-17-0033-28

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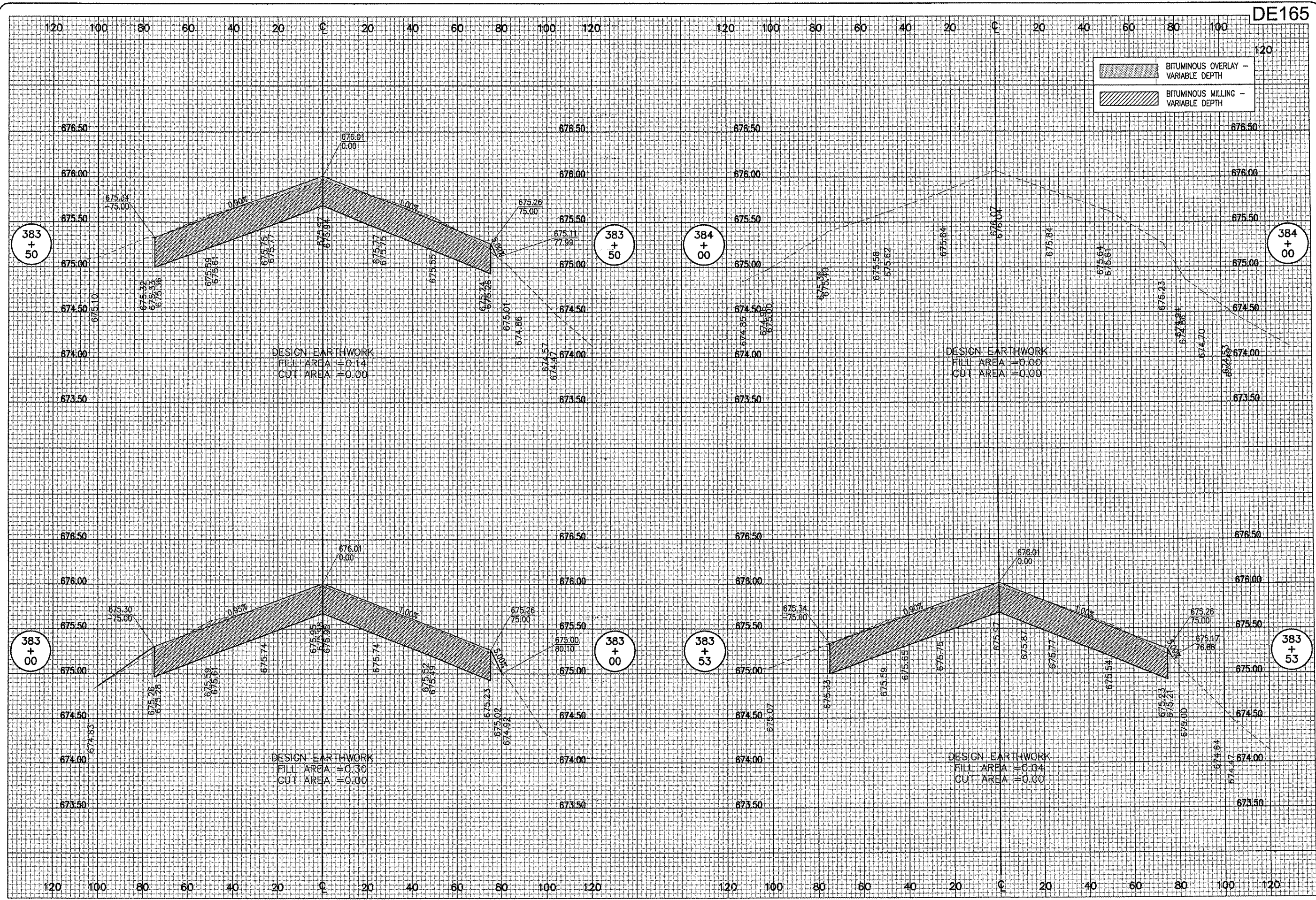
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01/27/05	CCC	LAYOUT
01/27/05	CCC	DRAWN
06/07/05	CAH	REVIEWED

HE Project No. 802-04TXD
 File No. R-334-XS.DWG
 Scale: 1" = 50'
 Date: 01/27/05

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 PHASE II
 RWY 6-24 RECONSTRUCTION
 PROPOSED CROSS-SECTIONS
 STA. 381+00 TO STA. 382+50

JUN 10, 2005 12:50 PM CCC
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DE165

BITUMINOUS OVERLAY - VARIABLE DEPTH
 BITUMINOUS MILLING - VARIABLE DEPTH

DESIGN EARTHWORK
 FILL AREA = 0.14
 CUT AREA = 0.00

DESIGN EARTHWORK
 FILL AREA = 0.00
 CUT AREA = 0.00

DESIGN EARTHWORK
 FILL AREA = 0.30
 CUT AREA = 0.00

DESIGN EARTHWORK
 FILL AREA = 0.04
 CUT AREA = 0.00

REVISION	DATE



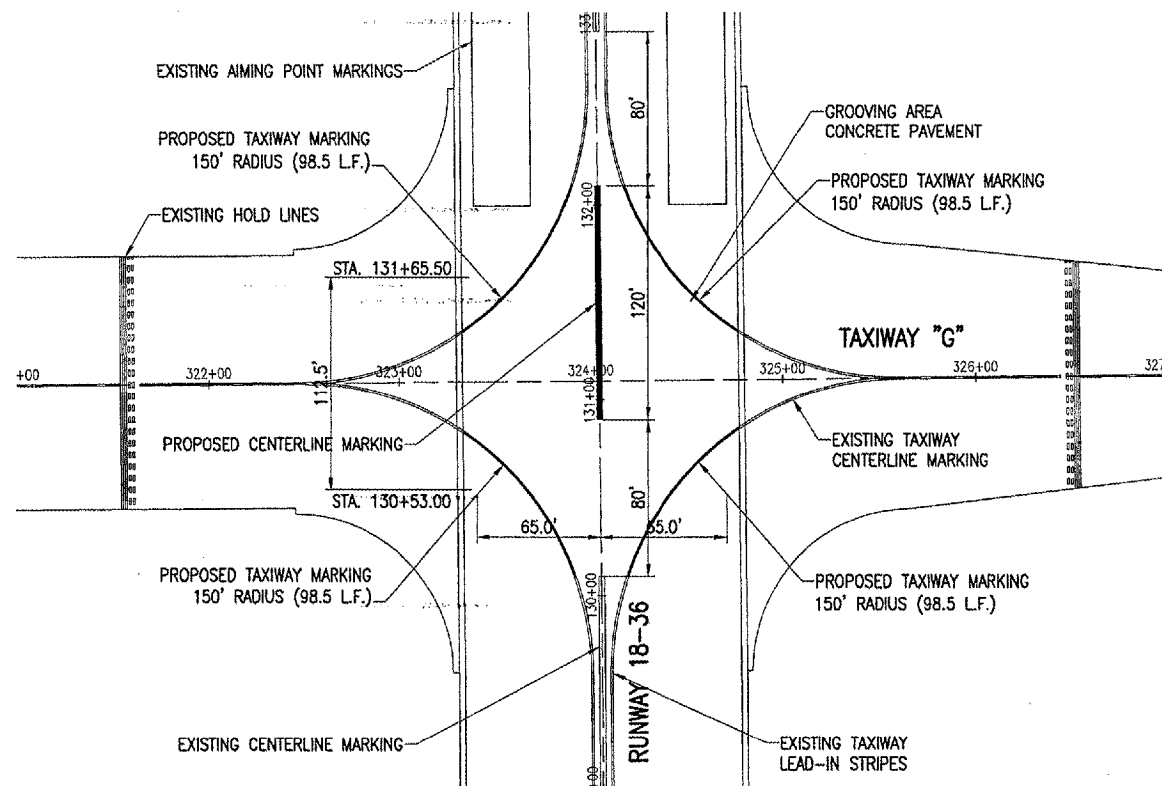
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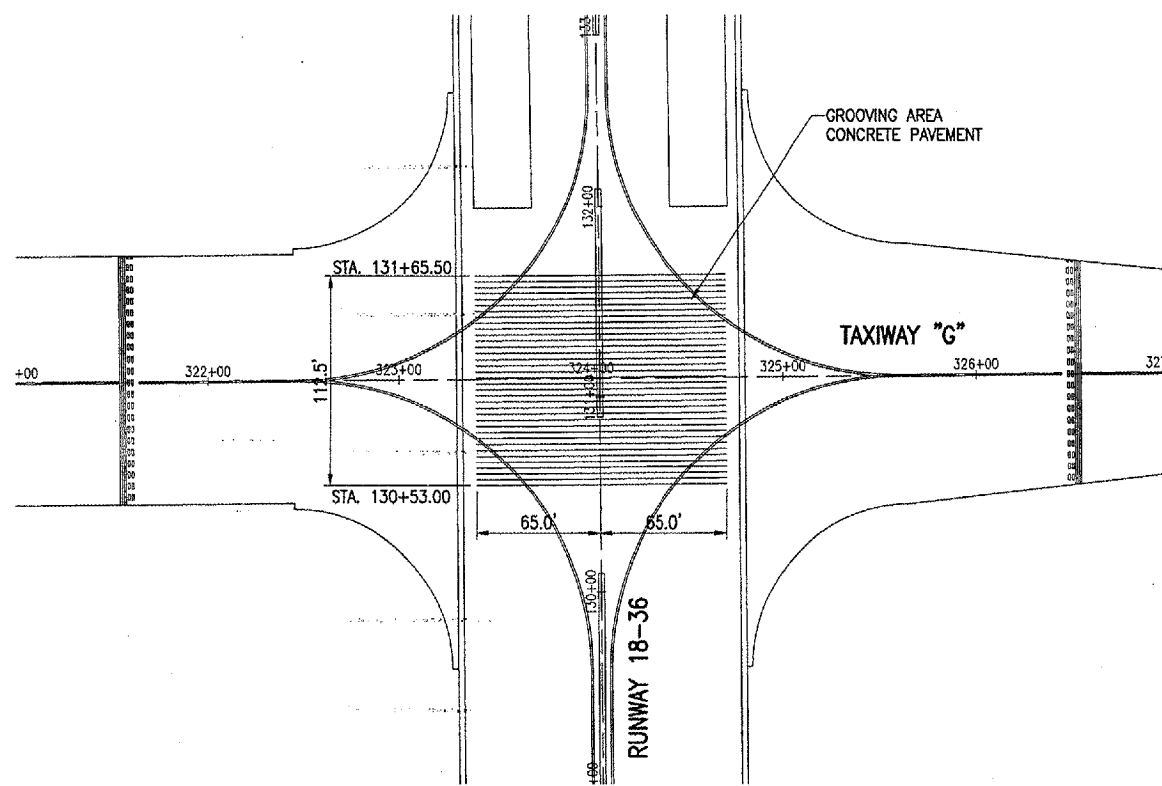


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 Springfield, Illinois 62705-2886
 Champaign, Illinois

TAXIWAY G WIDENING
 PHASE II
 RWY 6-24 RECONSTRUCTION
 PROPOSED CROSS-SECTIONS
 STA. 383+00 TO STA. 384+00



PROPOSED MARKING PLAN
 SCALE: 1" = 50'



PROPOSED GROOVING PLAN
 SCALE: 1" = 50'

MARKING NOTES

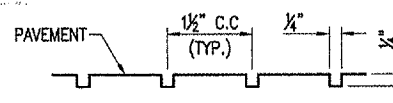
ALL PROPOSED RUNWAY MARKING SHALL BE SOLID AND WHITE IN COLOR.
 ALL PROPOSED TAXIWAY MARKING SHALL BE SOLID, 1 FOOT IN WIDTH, AND YELLOW IN COLOR.
 THE RUNWAY CENTERLINE SHALL BE 3 FEET WIDE BY 120 FEET IN LENGTH AND SPACED AS SHOWN ON THE PLANS.
 ALL PROPOSED MARKING WILL BE PAINTED WITH TWO APPLICATIONS. THE FIRST APPLICATION WILL SERVE AS TEMPORARY MARKING AND SHALL BE APPLIED NO SOONER THAN 48 HOURS AFTER THE FINAL PLACEMENT OF THE BITUMINOUS SURFACE COURSE. THE FIRST APPLICATION WILL ALLOW THE RUNWAYS TO BE OPENED DURING THE CURE PERIOD REQUIRED FOR THE GROOVING. AFTER THE GROOVING IS COMPLETED, THE SECOND APPLICATION OF MARKING WILL BE APPLIED.
 ALL PROPOSED MARKING WILL BE APPLIED WITH A REFLECTIVE MEDIA ON THE SECOND APPLICATION.
 CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

PAVEMENT MARKING-WATERBORNE - AR620520				
WORK ITEM	MARKING COLOR	UNIT AREA	NUMBER REQUIRED	TOTAL AREA (S.F.)
RUNWAY 18-36 CENTERLINE STRIPE	WHITE	360	1	360
TAXIWAY LEAD-IN STRIPES	YELLOW	98.5	4	394
			TOTAL	754

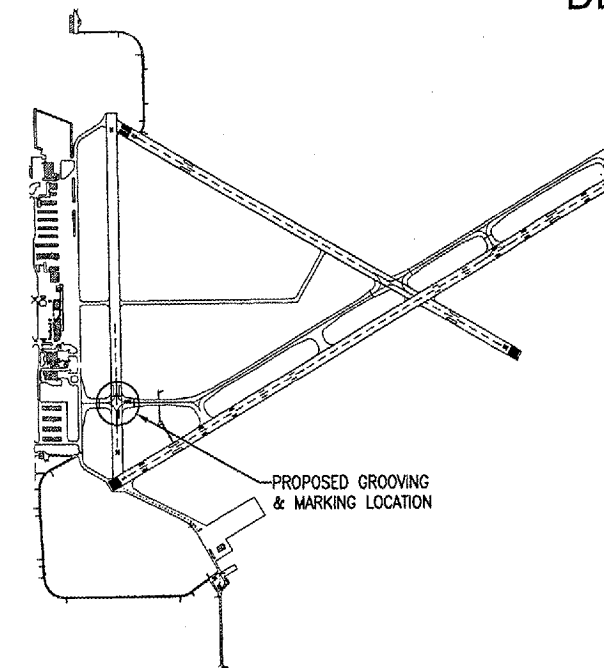
GROOVING NOTES

THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
 RUNWAY 18/36 WILL BE GROOVED CONTINUOUSLY FROM STATION 130+53 TO STATION 131+64.
 THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP, AND PLACED 1 1/2" CENTERS.
 ALL SOLID MATERIALS GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND HAULED TO A LOCATION OFF THE AIRPORT SITE AT THE CONTRACTOR'S OWN EXPENSE.
 NO MATERIAL WHICH IS HARMFUL TO VEGETATION WILL BE FLUSHED FROM THE GROOVED PAVEMENT ONTO THE TURFED SHOULDER, BUT WILL BE PICKED UP AND HAULED TO A LOCATION OFF THE AIRPORT SITE AT THE CONTRACTOR'S OWN EXPENSE.
 AT THE END OF EACH WORK DAY, THE RUNWAY WILL BE FLUSHED, CLEANED, AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.
 PAVEMENT GROOVING WILL BE PAID FOR UNDER ITEM:
 AR501540 "PCC PAVEMENT GROOVING" PER S.Y.

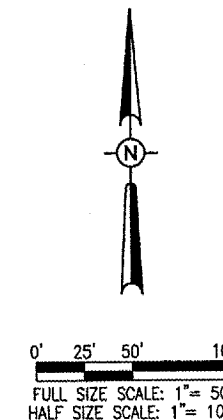
PAVEMENT GROOVING - RUNWAY 18-36			
ITEM No.	DESCRIPTION	UNIT	TOTAL
AR501540	PCC PAVEMENT GROOVING	S.Y.	1,625



GROOVING DETAIL
 "NOT TO SCALE"



KEY MAP
 "NOT TO SCALE"



DATE	REVISION

HANSON
 ENGINEERS, ARCHITECTS, SCIENTISTS
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Offices Nationwide

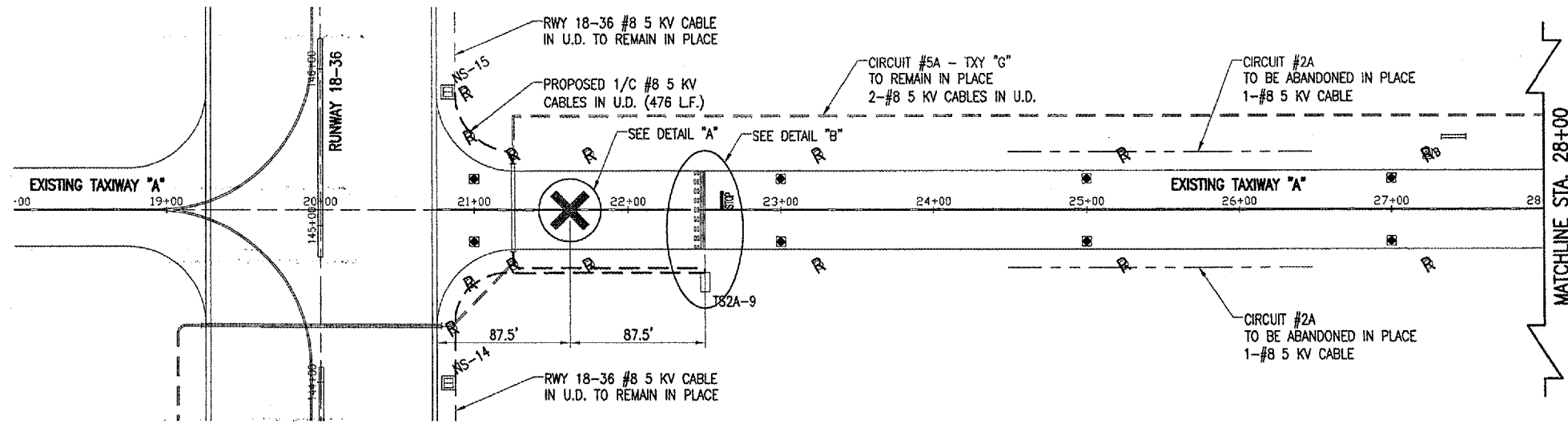
DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

DATE	BY	REVIEWED
10/28/04	WJM	WJM
10/28/04	CCC	CCC
06/07/05	WJM	WJM

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TAXIWAY G WIDENING
PHASE II
 RWY 18-36
 PROPOSED MARKING &
 GROOVING PLAN

JUN 10, 2005 9:40 AM CCC
 I:\AIRPORTS\DECATUR\802-04TXXD\AIRPORT SHEETS\CONTRACT-1\18-152MRK.DWG - 18-36



LIGHTS AND SIGN REMOVAL NOTES:

ALL STAKE MOUNTED AND BASE MOUNTED TAXIWAY LIGHTS AND TAXIWAY GUIDANCE SIGNS ON EXISTING TAXIWAYS "A" AND "D" THAT ARE NOTED FOR REMOVAL IN THIS PLAN WILL BE REMOVED BY THE CONTRACTOR. STAKES, BASES, AND TRANSFORMERS WILL BE REMOVED, AND NO. 8 POWER CABLE, CIRCUIT 2A, WILL BE ABANDONED IN PLACE. CONCRETE BASES FOR THE LIGHTS AND SIGNS WILL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR.

LIGHT FIXTURES, SIGNS AND TRANSFORMERS WILL BE TURNED OVER TO THE AIRPORT DIRECTOR. PROVIDING THE DIRECTOR DOES NOT WANT THE SALVAGED MATERIALS, THE CONTRACTOR WILL DISPOSED OF THE MATERIALS.

ALL DISTURBED AREAS, AS A RESULT OF THE REMOVALS, WILL BE REGRADED, HOLES FILLED WITH TOP SOIL MATERIAL, SMOOTH GRADED, SEEDED AND FERTILIZED ACCORDING TO SPECIFICATION ITEM: AR901510.

PAYMENT FOR THESE ITEMS OF WORK WILL BE AS FOLLOWS:
 AR125901 "REMOVE STAKE MOUNTED LIGHT" _____ PER EACH.
 AR125902 "REMOVE BASE MOUNTED LIGHT" _____ PER EACH.
 AR125904 "REMOVE TAXI GUIDANCE SIGN" _____ PER EACH.

MARKING REMOVAL NOTES:

THE EXISTING TAXIWAY PAVEMENT MARKING ON TAXIWAY "A" EAST OF THE EAST EDGE OF PAVEMENT ON RUNWAY 18-36 AND TAXIWAY "D" WEST OF THE CENTERLINE OF RUNWAY 12-30 WILL BE REMOVED. THE MARKING CONSIST OF A CONTINUOUS 1 FOOT WIDE YELLOW CENTERLINE STRIPE AND TWO YELLOW TAXIWAY HOLD LINES. THE METHOD OF REMOVAL WILL BE BY GRINDING OR WATER BLASTING.

PAYMENT FOR THIS ITEM OF WORK WILL BE AS FOLLOWS:
 AR620900 "PAVEMENT MARKING REMOVAL" _____ PER SQ. FT.

RETROFLECTIVE MARKER NOTES:

ELEVATED RETROFLECTIVE MARKERS SHALL BE SPACED ACCORDING TO THE STATION LOCATIONS GIVEN ON THIS SHEET.

THE MARKERS WILL BE LOCATED 20' RIGHT AND LEFT OF THE CENTERLINE OF THE SERVICE ROAD. THE PLACEMENT SHALL BE SUCH THAT THE LATERAL PLACEMENT AND HEIGHT HAVE A SATISFACTORY AND UNIFORM ALIGNMENT. MARKERS NOT HAVING SATISFACTORY AND UNIFORM NIGHT APPEARANCE SHALL BE ADJUSTED AT THE CONTRACTOR'S EXPENSE UNTIL THEY DO CONFORM.

A CORE HOLE WILL BE CUT INTO THE BITUMINOUS PAVEMENT OF SUFFICIENT WIDTH TO ACCEPT THE SOIL MOUNT ANCHOR TO A DEPTH OF 6 INCHES. THE ANCHOR WILL BE GROUTED INTO PLACE. THE BOTTOM OF THE TUBE WILL BE SET FLUSH WITH THE EXISTING PAVEMENT.

THE ELEVATED RETROFLECTIVE MARKERS ARE TO BE INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS AND THE SATISFACTION OF THE ENGINEER. THE MARKERS WILL COMPLY WITH SPECIFICATION ITEM AR125100 AND MEET THE REQUIREMENTS OF FAA SPECIFICATION L-853 AND BE MANUFACTURED BY HONEYWELL, FLIGHT LIGHT OR APPROVED EQUAL. THE MARKER IS TO BE AN 853 FIXTURE, SOIL MOUNT MOUNTING BASE, 24" TUBE HEIGHT, YELLOW TUBE COLOR, 3 REFLECTIVE BANDS, AND WHITE BAND COLOR.

PAYMENT FOR THIS ITEM OF WORK WILL BE AS FOLLOWS:
 AR125100 "ELEVATED RETROFLECTIVE MARKER" _____ PER EACH.

STOP SIGN NOTES:

ALL ROADWAY SIGNS WILL BE PLACED AT LOCATIONS SHOWN ON THE PLANS. PLACEMENT AND CONSTRUCTION OF THE SIGNS WILL BE DONE IN COMPLIANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS WELL AS THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS. IN THE EVENT OF A CONFLICT IN CONSTRUCTION REQUIREMENTS OR PROCEDURES, THE MORE STRINGENT POLICY WILL GOVERN.

SIGN DESIGNATION NUMBERS ARE AS DEFINED BY THE FEDERAL HIGHWAY ADMINISTRATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE TOP HEIGHT OF THE METAL STOP SIGN PANEL WILL BE SET AT 4 FEET ABOVE THE PAVEMENT SURFACE.

DIRECTLY BENEATH THE STOP SIGN, A SECOND SIGN WILL BE DISPLAYED "CONTACT ATC." THE SIGN MATERIALS WILL BE THE SAME AS FOR THE STOP SIGN. THE SIGN WILL HAVE A WHITE BACKGROUND WITH BLACK LETTERS AND A BLACK BORDER. THE HEIGHT OF THE LETTERS WILL BE 2 INCHES. A SHOP DRAWING IS REQUIRED.

THE SETTING OF THE POST, WOOD, WILL REQUIRE CORING THROUGH THE EXISTING PAVEMENT.

PAYMENT FOR THIS ITEM OF WORK WILL BE AS FOLLOWS:
 AR910200 "ROADWAY SIGNAGE" _____ PER EACH.

ROADWAY MARKING NOTES:

THE ROADWAY MARKING WILL CONSIST OF CENTERLINE STRIPES, EDGE STRIPES, STOP BARS WITH LETTERING, AND TAXIWAY CLOSURE CROSSES (X).

THE CENTERLINE STRIPES WILL BE 6 INCHES IN WIDTH AND 10 FEET IN LENGTH FOLLOWED BY 30 FEET OF UNMARKED SPACE WILL BE YELLOW IN COLOR. THE CENTERLINE STRIPES WILL EXTEND FROM THE NEAR EDGE OF RUNWAY 18-36 TO THE NEAR EDGE OF RUNWAY 12-30.

THE EDGE STRIPES, BOTH SIDES, WILL BE 6 INCHES IN WIDTH AND WHITE IN COLOR. THE EDGE STRIPE WILL BE CONTINUOUS FROM THE NEAR EDGE OF RUNWAY 18-36 TO THE NEAR EDGE OF RUNWAY 12-30.

THE STOP BAR AND STOP LETTERS WILL BE WHITE IN COLOR. THE STOP BAR WILL BE 12 FEET IN LENGTH AND 2 FEET IN WIDTH. THE HEIGHT OF THE LETTERS WILL BE 6 FEET AND FOLLOW THE ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARDS.

THE TAXIWAY CLOSURE CROSSES (X) WILL BE LOCATED AND DIMENSIONED ACCORDING TO DETAIL "A" ON THIS SHEET AND SHEET _____.

PAYMENT FOR THIS ITEM OF WORK WILL BE AS FOLLOWS:
 AR620520 "PAVEMENT MARKING-WATERBORNE" _____ PER SQ. FT.

LIGHTED SIGN NOTES:

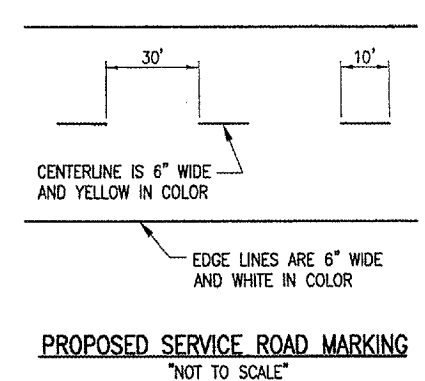
EXISTING SIGN No. TS2A-9 AND T2A-S5 WILL REMAIN IN PLACE AND BE REWIRED TO THE ADJACENT RUNWAY CIRCUIT. 1/C #8 5 KV CABLE IN U.D. WILL WIRE THE SIGNS INTO THE NEW CIRCUITS. CABLING WILL BE PAID FOR UNDER ITEM:
 AR108158 "1/C #8 5 KV UG CABLE IN UD." _____ PER L.F.

THE 12-30 RUNWAY CIRCUIT WILL BE INTERRUPTED AND NEW CABLE RUN FROM FIXTURE NWSE-18 TO SIGN T2A-S5 THROUGH THE ELECTRICAL DUCT TO FIXTURE NWSE-19.

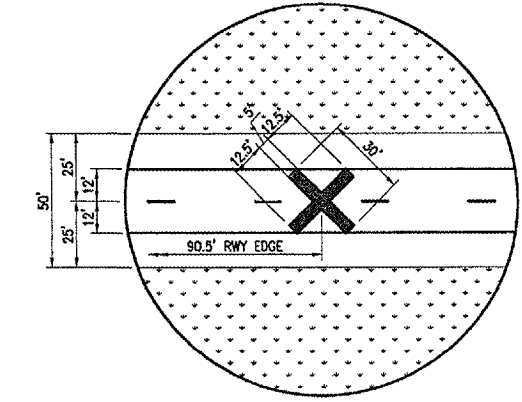
THE 18-36 RUNWAY CIRCUIT WILL BE INTERRUPTED AND NEW CABLE RUN FROM FIXTURE NS-14 TO SIGN TS2A-9 THROUGH THE ELECTRICAL DUCT TO FIXTURE NS-15.

THE TWO EXISTING SIGNS ARE 6 CHARACTER PANEL SIGNS AND THE SIGN PANELS WILL BE CHANGED AS DETAILED ON THE PLANS. THE SIGN DESIGNATION IS A MANDATORY SIGN WITH WHITE INSCRIPTION ON A RED BACKGROUND.

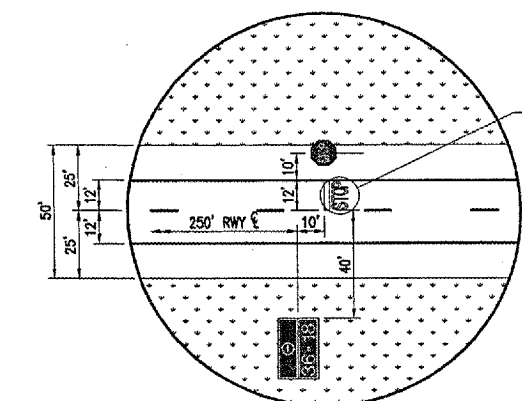
PAYMENT FOR THE SIGN MODIFICATION WILL BE UNDER ITEM:
 AR125470 "MODIFY EXISTING SIGN PANEL" _____ PER EACH.



PROPOSED SERVICE ROAD MARKING
 "NOT TO SCALE"



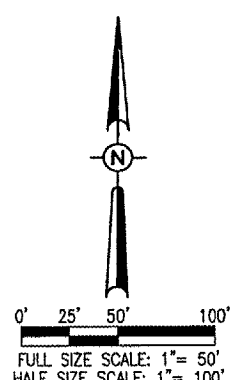
DETAIL A
 "NOT TO SCALE"



DETAIL B
 "NOT TO SCALE"

LEGEND

	EXISTING PAVEMENT
	EXISTING ELECTRICAL CABLE
	EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
	EXISTING BASE MOUNT RUNWAY LIGHT
	EXISTING BASE MOUNT TAXIWAY LIGHT TO BE REMOVED
	EXISTING STAKE MOUNT TAXIWAY LIGHT TO BE REMOVED
	EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED
	PROPOSED ELEVATED RETROFLECTIVE MARKER
	PROPOSED ELECTRICAL CABLE



FULL SIZE SCALE: 1" = 50'
 HALF SIZE SCALE: 1" = 100'

QUANTITIES			
ITEM No.	DESCRIPTION	UNIT	TOTAL
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	978
AR125100	ELEVATED RETROREFLECTIVE MARKER	EACH	40
AR125470	MODIFY EXISTING SIGN PANEL	EACH	2
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	53
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	6
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	4,390
	SERVICE ROAD CENTERLINE STRIPES - 415 S.F.		
	SERVICE ROAD EDGE STRIPE (LT.) - 1645.5 S.F.		
	SERVICE ROAD EDGE STRIPE (RT.) - 1657.5 S.F.		
	STOP BAR - 24 S.F. x 2 = 48 S.F.		
	LETTER "S" - 3.2 S.F. x 2 = 6.4 S.F.		
	LETTER "T" - 2.2 S.F. x 2 = 4.4 S.F.		
	LETTER "O" - 3.4 S.F. x 2 = 6.8 S.F.		
	LETTER "P" - 3.0 S.F. x 2 = 6.0 S.F.		
	CLOSED TAXIWAY MARKING (YELLOW X) - 300 S.F. x 2 = 600 S.F.		
AR620900	PAVEMENT MARKING REMOVAL	S.F.	4,322
	TAXIWAY CENTERLINE STRIPE - 4,018 S.F.		
	TAXIWAY HOLD LINES - 152 S.F. x 2 = 304 S.F.		
AR910200	ROADWAY SIGNAGE	EACH	2

REVISION BY

DATE

DECATUR, ILLINOIS

DECATUR AIRPORT

HAUSON

Engineers, Architects, Scientists
 1525 South Sixth Street
 Springfield, IL 62768
 Offices Nationwide

TAXIWAY G WIDENING
 PHASE II

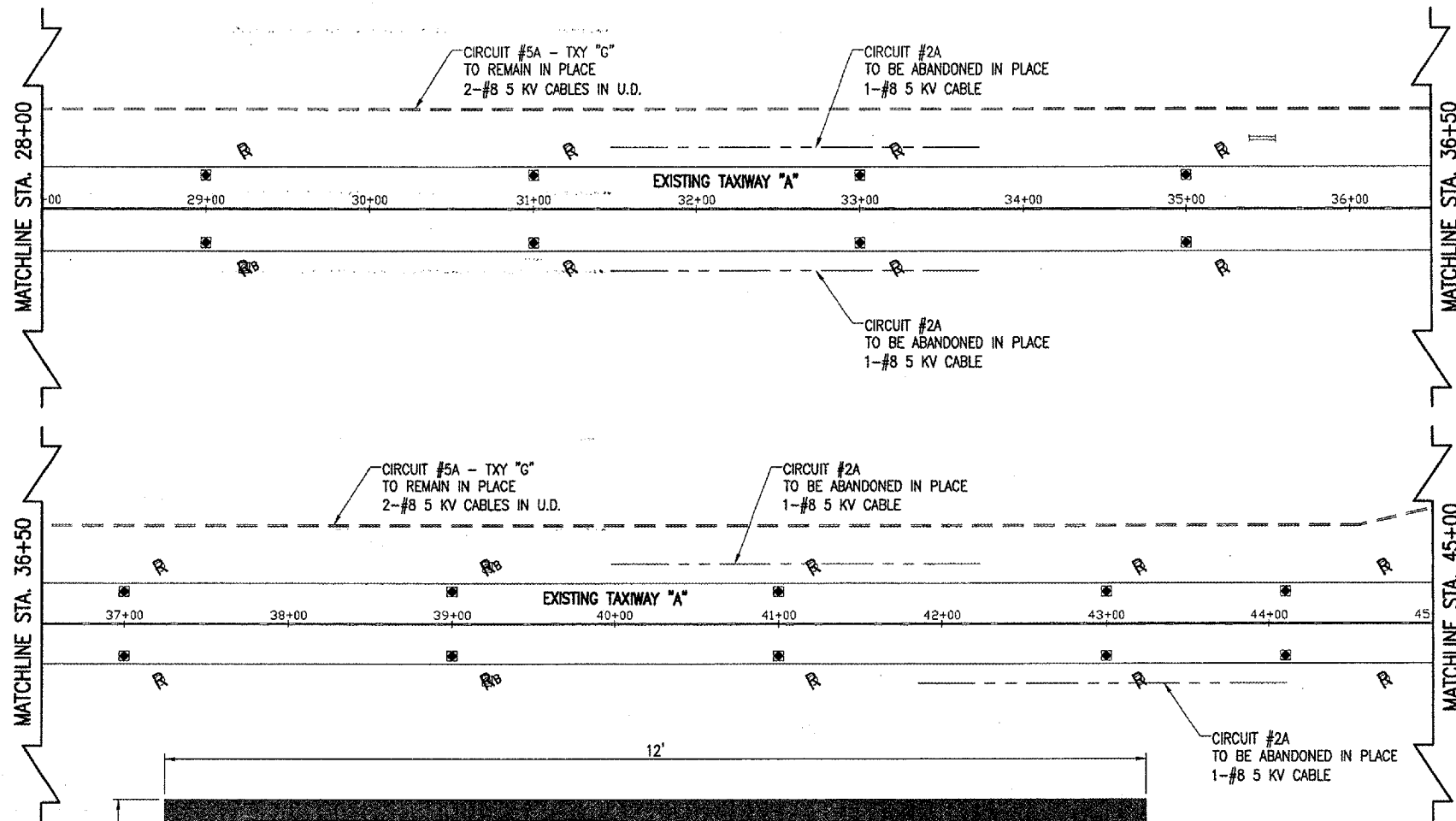
PROPOSED SERVICE ROAD
 CONSTRUCTION PLAN
 STA. 21+00 TO 28+00

94

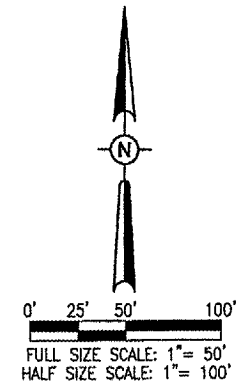
94 of 110 sheets

IL PROJ. DEC-3414 A.I.P. PROJ. 3-17-0033-28

JUN 10, 2005 9:20 AM CCC
 I:\AIRPORTS\DECATUR\802-04TXVD\AIRPORT SHEETS\CONTRACT-1\1R-122CON.DWG - Taxiway A

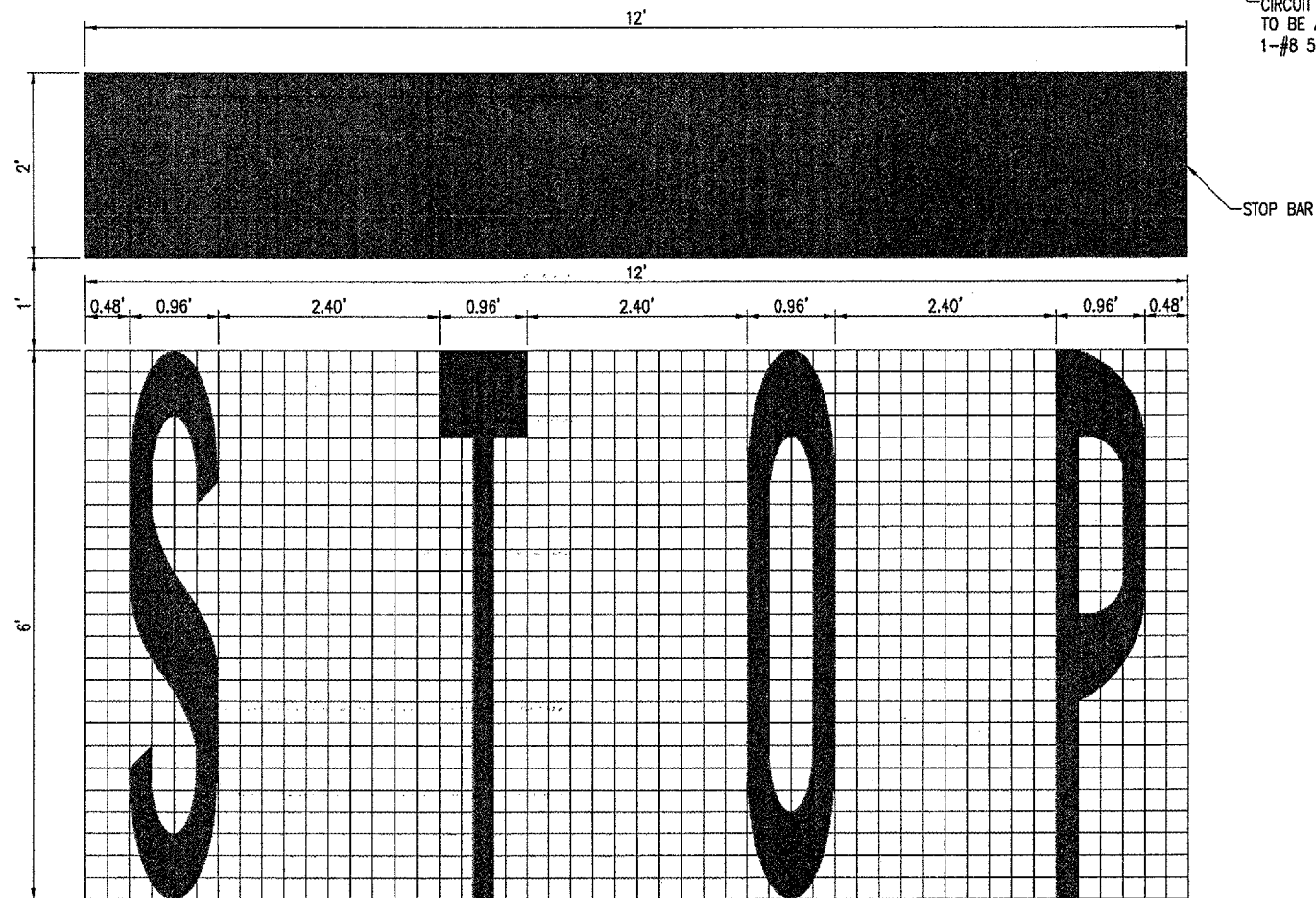


- LEGEND**
- EXISTING PAVEMENT
 - EXISTING ELECTRICAL CABLE
 - EXISTING ELECTRICAL CABLE TO BE ABANDONED/REMOVED
 - EXISTING BASE MOUNT TAXIWAY LIGHT TO BE REMOVED
 - EXISTING STAKE MOUNT TAXIWAY LIGHT TO BE REMOVED
 - PROPOSED ELEVATED RETROFLECTIVE MARKER



PROPOSED RETROFLECTIVE MARKER DETAIL
 "NOT TO SCALE"

RETROFLECTIVE MARKERS	
STATION LOCATIONS	
21+00*	41+00
23+00*	43+00
25+00	44+10
27+00	45+20.32 PC
29+00	45+85.77
31+00	46+51.22 PT
33+00	47+53.80
35+00	49+53.80
37+00	51+53.80*
39+00	53+53.80*



"STOP" LETTERING DETAIL
 "NOT TO SCALE"

LEGEND HEIGHT	a
1.8m (6')	74 (2.9)
2.4m (8')	96 (3.8)

LETTER GRID SCALE

THE SPACE BETWEEN ADJACENT LETTERS OR NUMERALS SHOULD BE APPROXIMATELY 75 (3') FOR 1.8m (6') LEGEND AND 100 (4') FOR 2.4m (8') LEGEND.

DATE	REVISION	BY



IL PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28

HL Project No. 802-04TXVD		
Flgnum R-122CON.DWG		
Scale 1"=50'		
Date 04/14/05		
LAYOUT CCC	04/14/05	
DRAWN CCC	04/14/05	
REVIEWED WJM	06/07/05	



TAXIWAY G WIDENING
 PHASE II
 PROPOSED SERVICE ROAD
 CONSTRUCTION PLAN
 STA. 28+00 TO 45+00

ELECTRICAL LEGEND - ONE-LINE DIAGRAM

	CABLE TERMINATOR/LUG
	TRANSFORMER
	DISCONNECT SWITCH
	FUSIBLE DISCONNECT SWITCH
	CIRCUIT BREAKER
	THERMAL MAGNETIC CIRCUIT BREAKER
	FUSE
	TRANSIENT VOLTAGE SURGE SUPPRESSOR OR SURGE PROTECTOR DEVICE
	GROUND - GROUND ROD, GROUNDING ELECTRODE, OR AT EARTH POTENTIAL
	INDICATING LIGHT
	MOTOR
	LOAD, MOTOR, $\frac{1}{2}$ = HORSEPOWER
	ELECTRIC UTILITY METER BASE
	JUNCTION BOX WITH SPLICE
	EQUIPMENT, XXX = DEVICE DESCRIPTION
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	PANELBOARD WITH MAIN LUGS
	PANELBOARD WITH MAIN BREAKER
	FUSE PANEL WITH MAIN FUSE PULLOUT
	DUPLEX RECEPTACLE 120V SINGLE PHASE GROUNDING TYPE
	CONTROL STATION
	TRANSFER SWITCH
	ENGINE GENERATOR SET

ELECTRICAL LEGEND - SCHEMATIC

	NORMALLY OPEN (N.O.) CONTACT
	NORMALLY CLOSED (N.C.) CONTACT
	STARTER COIL, * = STARTER NUMBER
	OVERLOAD RELAY CONTACT
	CONTROL RELAY, * = CONTROL RELAY NUMBER
	RELAY, * = RELAY NUMBER
	TOGGLE SWITCH / 2 POSITION SWITCH
	2-POSITION SELECTOR SWITCH
	3-POSITION SELECTOR SWITCH (H-O-A SHOWN)
	2 POLE DISCONNECT SWITCH
	3 POLE DISCONNECT SWITCH
	PHOTOCELL
	TERMINAL BLOCK, * = TERMINAL NUMBER
	DEVICE TERMINAL, * = DEVICE TERMINAL NUMBER
	INTERNAL PANEL WIRING
	FIELD WIRING
	FUSE
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	GROUND, GROUND ROD
	INDUSTRIAL CONTROL RELAY OR LIGHTING CONTACTOR
	S1 CUTOUT HANDLE REMOVED
	S1 CUTOUT HANDLE INSERTED
	N.O. THERMAL SWITCH
	N.C. THERMAL SWITCH

ELECTRICAL ABBREVIATIONS

A.F.F.	ABOVE FINISHED FLOOR
A. AMP	AMPERES
ATS	AUTOMATIC TRANSFER SWITCH
AWG	AMERICAN WIRE GAUGE
BKR	BREAKER
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CR	CONTROL RELAY
CU	COPPER
DPDT	DOUBLE POLE DOUBLE THROW
DPST	DOUBLE POLE SINGLE THROW
EM	EMERGENCY
EMT	ELECTRICAL METALLIC TUBING
ENCL	ENCLOSURE
EP	EXPLOSION PROOF
ES	EMERGENCY STOP
ETM	ELAPSE TIME METER
GFCI	GROUND FAULT CIRCUIT INTERRUPTER
GFI	GROUND FAULT INTERRUPTER
GND	GROUND
GRSC	GALVANIZED RIGID STEEL CONDUIT
HID	HIGH INTENSITY DISCHARGE
HOA	HAND OFF AUTOMATIC
HP	HORSEPOWER
HPS	HIGH PRESSURE SODIUM
J	JUNCTION BOX
KVA	KILOVOLT AMPERE(S)
KW	KILOWATTS
LC	LIGHTING CONTACTOR
LTFMC	LIQUID TIGHT FLEXIBLE METAL CONDUIT (UL LISTED)
LP	LIGHTING PANEL
MAX	MAXIMUM
MCB	MAIN CIRCUIT BREAKER
MCM	THOUSAND CIRCULAR MIL
MCP	MAIN DISTRIBUTION PANEL
MH	METAL HALIDE
MIN	MINIMUM
MLO	MAIN LUGS ONLY
NC	NORMALLY CLOSED
NO	NORMALLY OPEN
NTS	NOT TO SCALE
OHE	OVERHEAD ELECTRIC
OL	OVERLOAD
PB	PULL BOX
PC	PHOTO CELL
PDB	POWER DISTRIBUTION BLOCK
PNL	PANEL

ELECTRICAL ABBREVIATIONS (CONTINUED)

RCPT	RECEPTACLE
R	RELAY
S	STARTER
SPD	SURGE PROTECTION DEVICE
SPST	SINGLE POLE SINGLE THROW
TVSS	TRANSIENT VOLTAGE SURGE SUPPRESSOR
TYP	TYPICAL
UG	UNDERGROUND
UGE	UNDERGROUND ELECTRIC
V	VOLTS
W/	WITH
W/O	WITHOUT
WP	WEATHER PROOF
XFER	TRANSFER
XFRM	TRANSFORMER

AIRPORT EQUIPMENT ABBREVIATIONS

CCR	CONSTANT CURRENT REGULATOR
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MTL	MEDIUM INTENSITY TAXIWAY LIGHT
NDB	NON-DIRECTIONAL BEACON
PAPI	PRECISION APPROACH PATH INDICATOR
PLASI	PULSE LIGHT APPROACH SLOPE INDICATOR
REIL	RUNWAY END IDENTIFIER LIGHT
VASI	VISUAL APPROACH SLOPE INDICATOR
WC	WIND CONE

ELECTRICAL LEGEND - PLANS

	CONDUIT (EXPOSED)
	CONDUIT OR UNIT DUCT (CONCEALED OR BURIED)
	DUCT
	BURIED/UNDERGROUND ELECTRIC
	UNDERGROUND ELECTRIC
	OVERHEAD ELECTRIC
	POLE MOUNTED HID FIXTURE
	DUPLEX CONVENIENCE RECEPTACLE, 120V, SINGLE PHASE, GROUNDING TYPE, 48" A.F.F. EXCEPT AS NOTED
	WALL OR CEILING MTD. JUNCTION BOX. CONFIGURATION VARIES WITH USE
	SINGLE THROW DISCONNECT SWITCH
	SINGLE THROW, FUSIBLE DISCONNECT SWITCH
	ENCLOSED CIRCUIT BREAKER
	CONTROL PANEL
	MOTOR. ESTIMATED H.P. AS INDICATED.
	MOTOR
	TRANSFORMER
	ELECTRIC UTILITY METER
	ENCLOSURE
	CIRCUIT BREAKER PANEL-SEE SCHEDULES
	GROUND ROD
	LONG SLASHES INDICATE NEUTRAL. SHORT SLASHES INDICATE HOT OR SWITCHED LEG. G = SEPARATE GROUND WIRE.
	HOME RUN TO PANEL. PNL A INDICATES PANEL. 1,3,5 INDICATES CIRCUIT NUMBERS
	SINGLE POLE SWITCH
	FRACTIONAL HP STARTER
	CONTACTOR
	SURFACE MOUNTED OR CHAIN HUNG FLUORESCENT FIXTURE
	WALL OR CEILING MTD. INCANDESCENT OR HID FIXTURE.

NOTES:

- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
- CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES WITH THE RESPECTIVE FACILITY OWNER PERSONNEL AND THE AIRPORT DIRECTOR.
- COLOR CODE PHASE AND NEUTRAL CONDUCTOR INSULATION FOR NO. 6 AWG OR SMALLER. PROVIDE COLORED INSULATION OR COLORED MARKING TAPE FOR PHASE AND NEUTRAL CONDUCTORS FOR NO. 4 AWG AND LARGER. INSULATED GROUND CONDUCTORS SHALL HAVE GREEN COLORED INSULATION FOR ALL. CONDUCTOR AWG AND/OR KCMIL. STANDARD COLORS FOR POWER WIRING AND BRANCH CIRCUITS SHALL BE AS FOLLOWS:

120/240 VAC, 1 PHASE, 3 WIRE
 PHASE A BLACK
 PHASE B RED
 NEUTRAL WHITE
 GROUND GREEN

BY	
REVISION	
DATE	

HANSON
 DECATUR, ILLINOIS
 PROJECT: 3-17-0033-28
 ILL. PROJ.: DEC-3414

REL. PROJECT NO.	802-047X7D
DATE	04/22/05
SCALE	NONE
DATE	04/25/05
DATE	06/08/06
LAYOUT	KNL
DRAWN	MM
REVIEWED	CAH

HANSON
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 Springfield, Illinois 62703-2886
 Offices Nationwide

TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL LEGEND
 AND ABBREVIATIONS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.







CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES

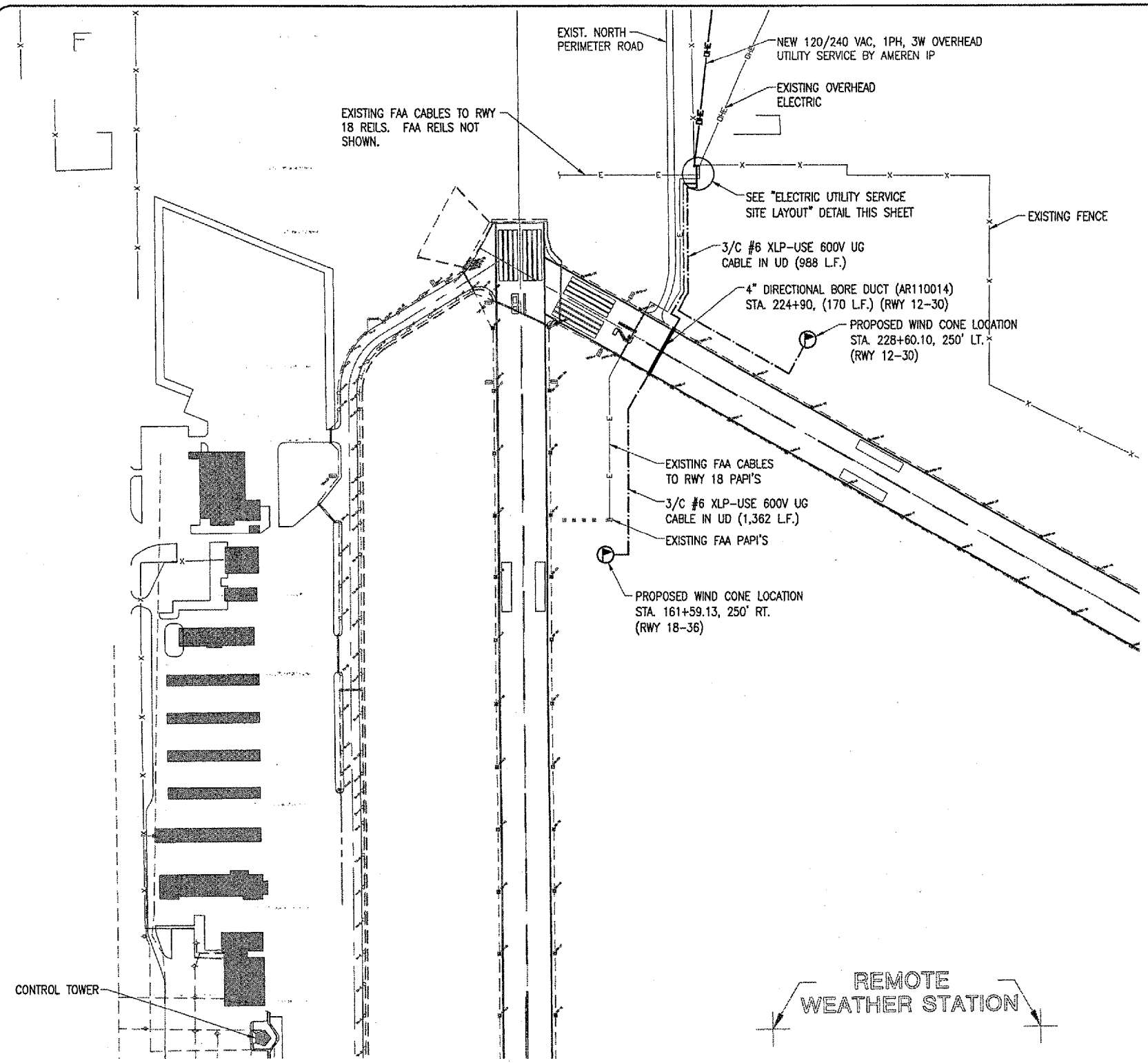
SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AR107408	L-806 WIND CONE 8' LIGHTED	EA	2
AR108656	3/C #6 600V UG CABLE IN UD	L.F.	2,350
AR110014	4" DIRECTIONAL BORE	L.F.	170
AR800579	ELECTRIC SERVICE FOR RWY 12 AND RWY 18 WIND CONES	L.S.	1

WIND CONE NOTES
 THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

COORDINATE NEW ELECTRIC SERVICE WITH THE SERVING ELECTRIC UTILITY COMPANY (AMEREN IP, 2460 N. JASPER ST. MC H-15, DECATUR, IL 62526, ATTN MS. TAMMIE HAUGERUD, SENIOR ENGINEERING REP, PHONE: 217-425-6032, CELL PHONE: 217-412-6455, FAX: 217-424-7012)

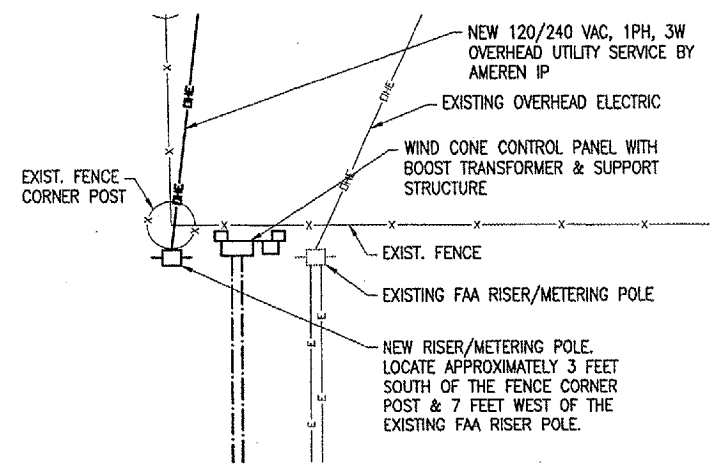
LEGEND

-  EXISTING PAVEMENT
-  PROPOSED DUCT
-  PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD - AR108656
-  PROPOSED SUPPLEMENTAL WIND CONE
-  EXISTING ELECTRICAL CABLES
-  EXISTING UNDERGROUND ELECTRICAL CABLES (FAA, UTILITY, ETC.)

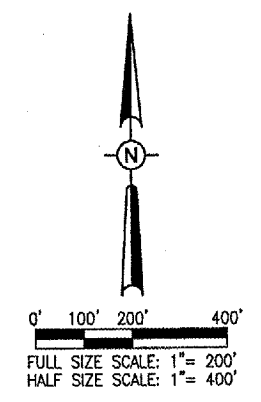


ELECTRICAL SITE PLAN FOR RWY 12 & RWY 18 WIND CONES

REMOTE WEATHER STATION



ELECTRIC UTILITY SERVICE SITE LAYOUT
 N.T.S.



DATE	REVISION	BY

DECATUR AIRPORT
 DECATUR, ILLINOIS
 I.L. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

REVISED	CAH	06/07/05
DRAWN	MY	04/25/05
LAYOUT	KNL	04/22/05

HANSON
 Engineers, Architects, Scientists
 Springfield, Illinois 62702-2886
 Offices Nationwide

TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL SITE PLAN
 RUNWAY 12 & RUNWAY 18
 WIND CONES

JUN 10, 2005 12:53 PM CCC
 I:\AIRPORTS\DECATUR\802-04TXYD\AIRPORT\1\E-105.DWG - 200E

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

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






SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AR107408	L-806 WIND CONE 8' LIGHTED	EA	2
AR108656	3/C #6 600V UG CABLE IN UD	L.F.	3,550
AR110014	4" DIRECTIONAL BORE	L.F.	170
AR800580	ELECTRIC SERVICE FOR RWY 24 AND RWY 30 WIND CONES	L.S.	1

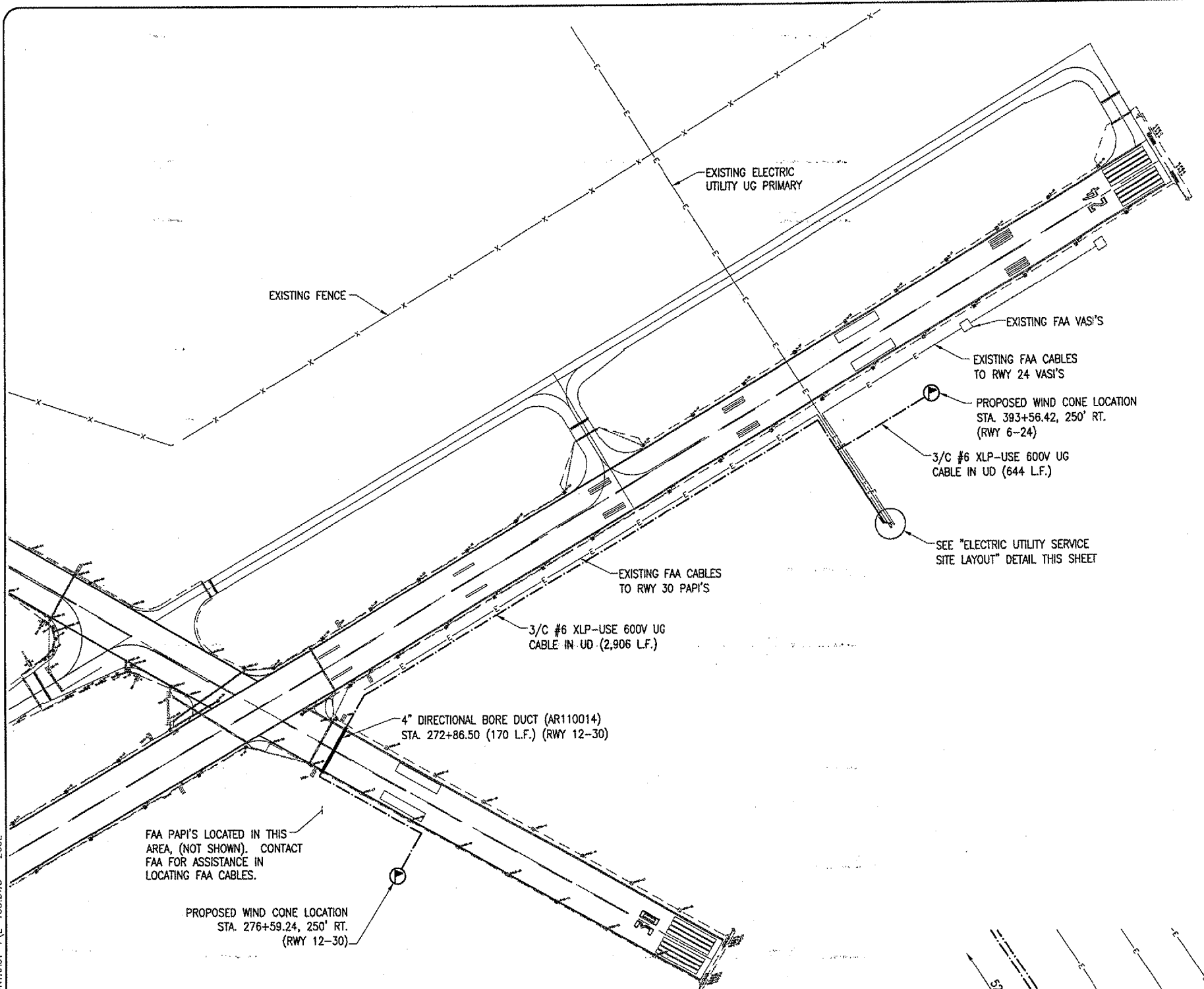
WIND CONE NOTES

THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

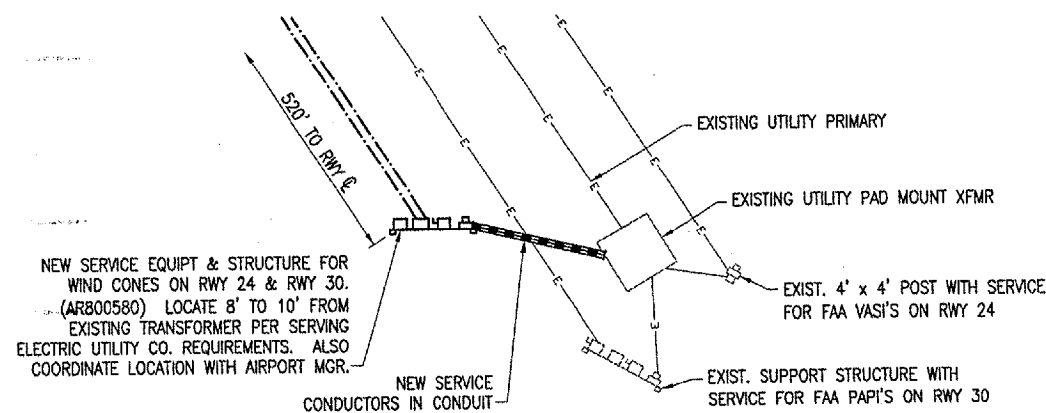
COORDINATE NEW ELECTRIC SERVICE WITH THE SERVING ELECTRIC UTILITY COMPANY (AMEREN IP, 2480 N. JASPER ST. MC H-15, DECATUR, IL 62526, ATTN MS. TAMMIE HAUGERUD, SENIOR ENGINEERING REP, PHONE: 217-425-6032, CELL PHONE: 217-412-6455, FAX: 217-424-7012)

LEGEND

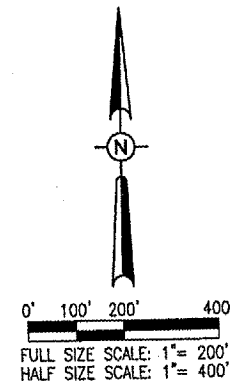
-  EXISTING PAVEMENT
-  PROPOSED DUCT
-  PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD - AR108656
-  PROPOSED SUPPLEMENTAL WIND CONE
-  EXISTING ELECTRICAL CABLES
-  EXISTING UNDERGROUND ELECTRICAL CABLES (FAA, UTILITY, ETC.)
-  EXISTING WIND CONE



ELECTRICAL SITE PLAN FOR RWY 24 & RWY 30 WIND CONES



ELECTRIC UTILITY SERVICE SITE LAYOUT
N.T.S.



DATE	REVISION

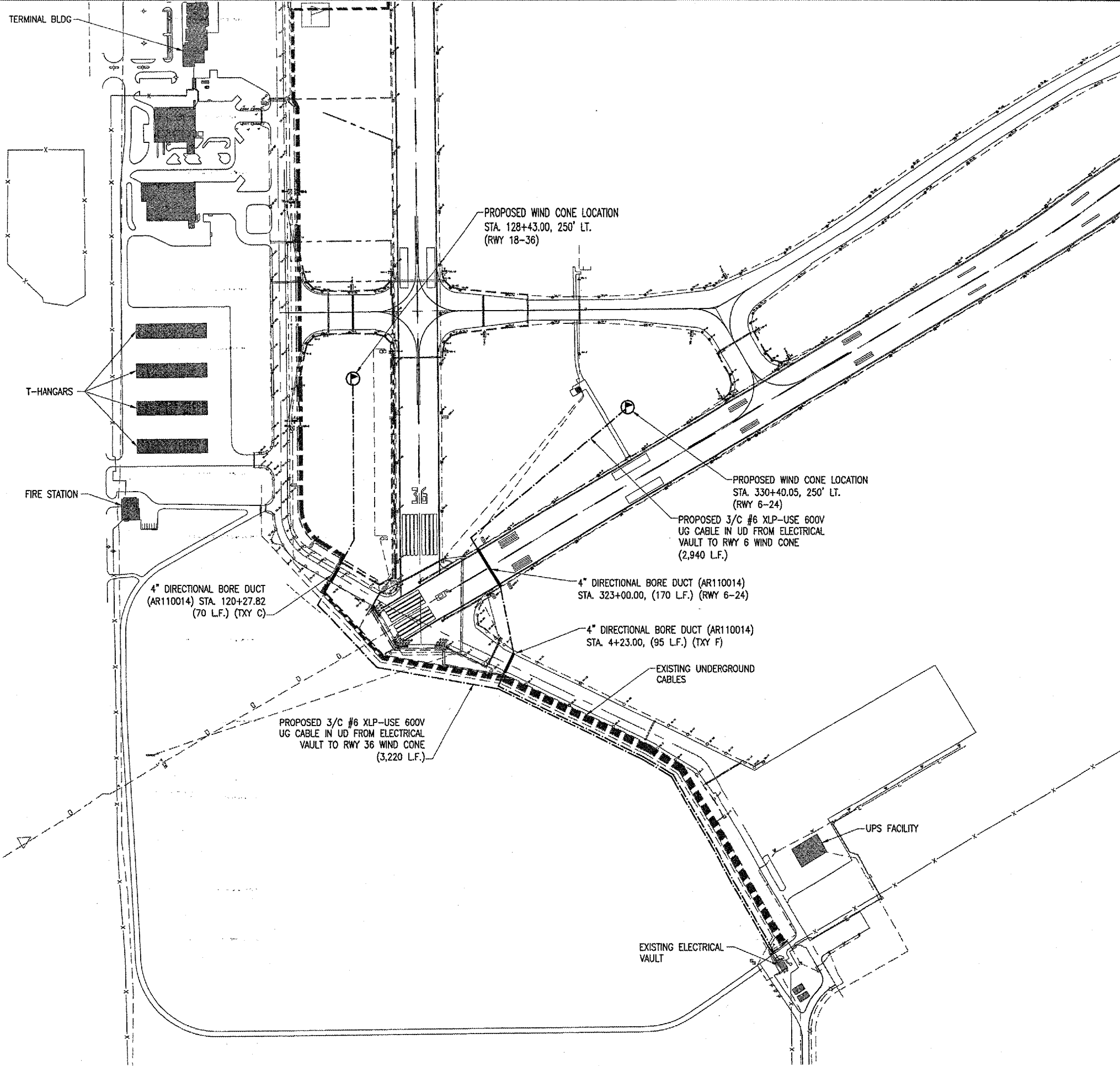


HE Project No. 802-04TXD	HE Project No. 802-04TXD	HE Project No. 802-04TXD	HE Project No. 802-04TXD
Drawn: E-103.DWG	Drawn: E-103.DWG	Drawn: E-103.DWG	Drawn: E-103.DWG
Scale: AS SHOWN	Scale: AS SHOWN	Scale: AS SHOWN	Scale: AS SHOWN
Date: XX/XX/XX	Date: XX/XX/XX	Date: XX/XX/XX	Date: XX/XX/XX
LAYOUT: KNL 04/22/05	LAYOUT: KNL 04/22/05	LAYOUT: KNL 04/22/05	LAYOUT: KNL 04/22/05
DRAWN: MV 04/25/05	DRAWN: MV 04/25/05	DRAWN: MV 04/25/05	DRAWN: MV 04/25/05
REVIEWED: CAH 06/07/06	REVIEWED: CAH 06/07/06	REVIEWED: CAH 06/07/06	REVIEWED: CAH 06/07/06



TAXIWAY G WIDENING
PHASE II
ELECTRICAL SITE PLAN
RUNWAY 24 & RUNWAY 30
WIND CONES

JUN 10, 2005 12:52 PM CCC
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CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES

SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)

ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AR107408	L-806 WIND CONE 8' LIGHTED	EA	2
AR108656	3/C #6 600V UG CABLE IN UD	L.F.	6,160
AR110014	4" DIRECTIONAL BORE	L.F.	335
AR800581	ELECTRICAL POWER FOR RWY 6 AND RWY 36 WIND CONES	L.S.	1

WIND CONE NOTES
THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

LEGEND

- EXISTING PAVEMENT
- PROPOSED DUCT
- PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD - AR108656
- PROPOSED SUPPLEMENTAL WIND CONE
- EXISTING ELECTRICAL CABLES
- EXISTING WIND CONE

0' 100' 200' 400'

FULL SIZE SCALE: 1" = 200'
HALF SIZE SCALE: 1" = 400'

DATE	REVISION	BY

HANSON
Engineers, Architects, Scientists
1525 South Sixth Street
Springfield, Illinois 62703-2888
Offices Nationwide

DECATUR, ILLINOIS

IL PROJ: DEC-3414 A.I.P. PROJ: 3-17-0033-28


ISS. Project No. 802-04TXD	FILED	DATE
Filename E-102.DWG	AS SHOWN	04/22/05
Scale XX/XX/XX		04/25/05
LAYOUT	KNL	06/07/06
DRAWN	WV	
REVIEWED	CAH	

HANSON

Engineers, Architects, Scientists
1525 South Sixth Street
Springfield, Illinois 62703-2888
Offices Nationwide

TAXIWAY G WIDENING
PHASE II
ELECTRICAL SITE PLAN
RUNWAY 6 & RUNWAY 36
WIND CONES

DATE	REVISION	BY



DECATUR, ILLINOIS

 ILL. PROJ.: DEC-3414

HEL. Project No. 802-041XXD	04/22/05
Filename E-104.DWG	04/25/05
Scale 1/4" = 1'-0"	06/06/06
Date XX/XX/XX	
LAYOUT	KHL
DRAWN	MW
REVIEWED	CAH



HANSON

 Engineers, Architects, Scientists

 1525 South State Street

 Springfield, Illinois 62703-2888

 Offices Nationwide

TAXIWAY G WIDENING

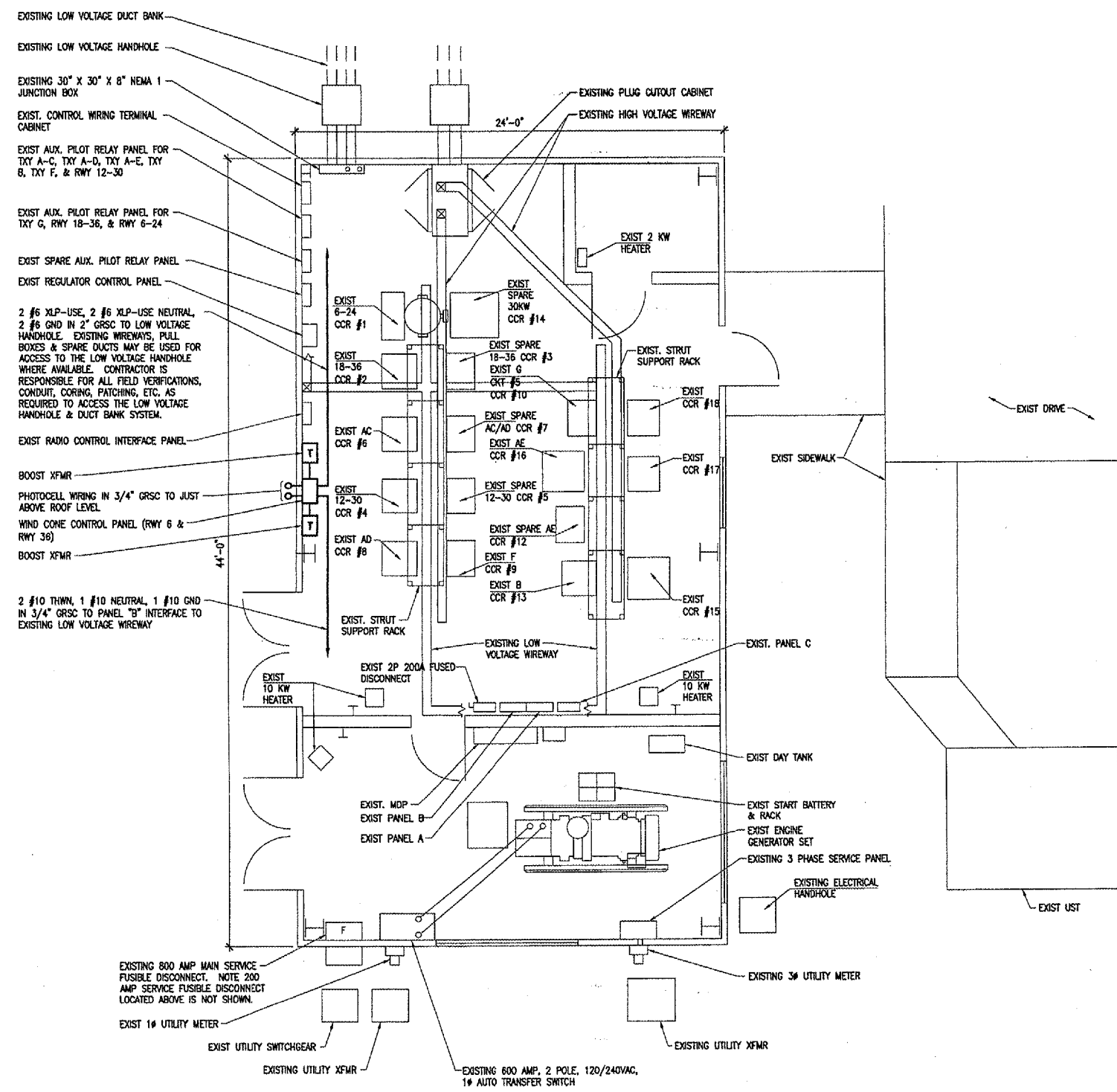
PHASE II

ELECTRICAL VAULT PLAN

RUNWAY 6 & RUNWAY 36

WIND CONE WORK

- NOTES**
1. ALL VAULT WORK AND/OR POWER OUTAGES SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER.
 2. SEE "ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 6 & RUNWAY 36 WIND CONES" SHEET FOR INFO ON WIRING REQUIREMENTS.
 3. HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME RACEWAY.
 4. ELECTRICAL POWER & WIND CONE CONTROL WORK FOR RUNWAY 6 & RUNWAY 36 WIND CONES SHALL BE PAID FOR UNDER AR800581 ELECTRIC POWER FOR RUNWAY 6 & RUNWAY 36 WIND CONES PER LUMP SUM. ALL POWER & CONTROL CABLE AT THE VAULT AND INTO THE LOW VOLTAGE HANDHOLE ADJACENT TO THE VAULT SHALL BE PAID FOR UNDER AR800581.
 5. ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.



ELECTRICAL VAULT PLAN FOR
RWY 6 & RWY 36 WIND CONES

SCALE 1/4" = 1'-0"
 2 0 4 8 FEET

JUN 10, 2005 12:53 PM CCC
 I:\AIRPORTS\DECATUR\802-041XXD\AIRPORT\SHEETS\CONTRACT-1\E-104.DWG - Work-FLR

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

NOTE:
THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.

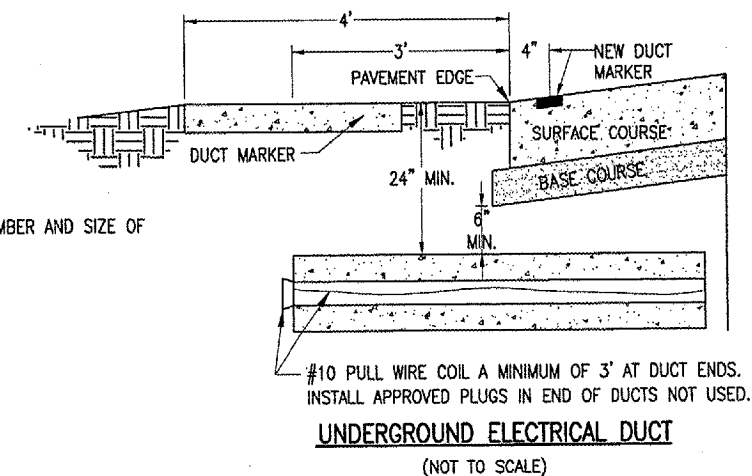
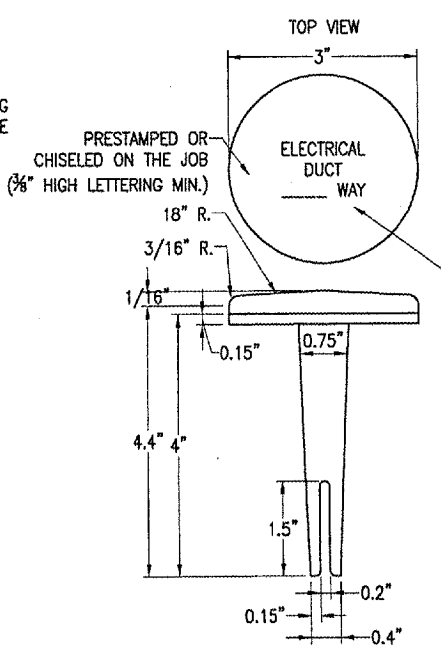
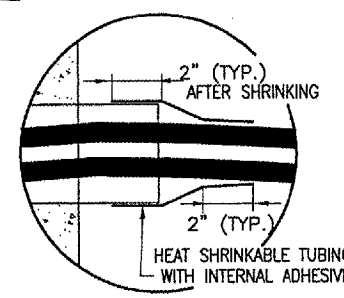
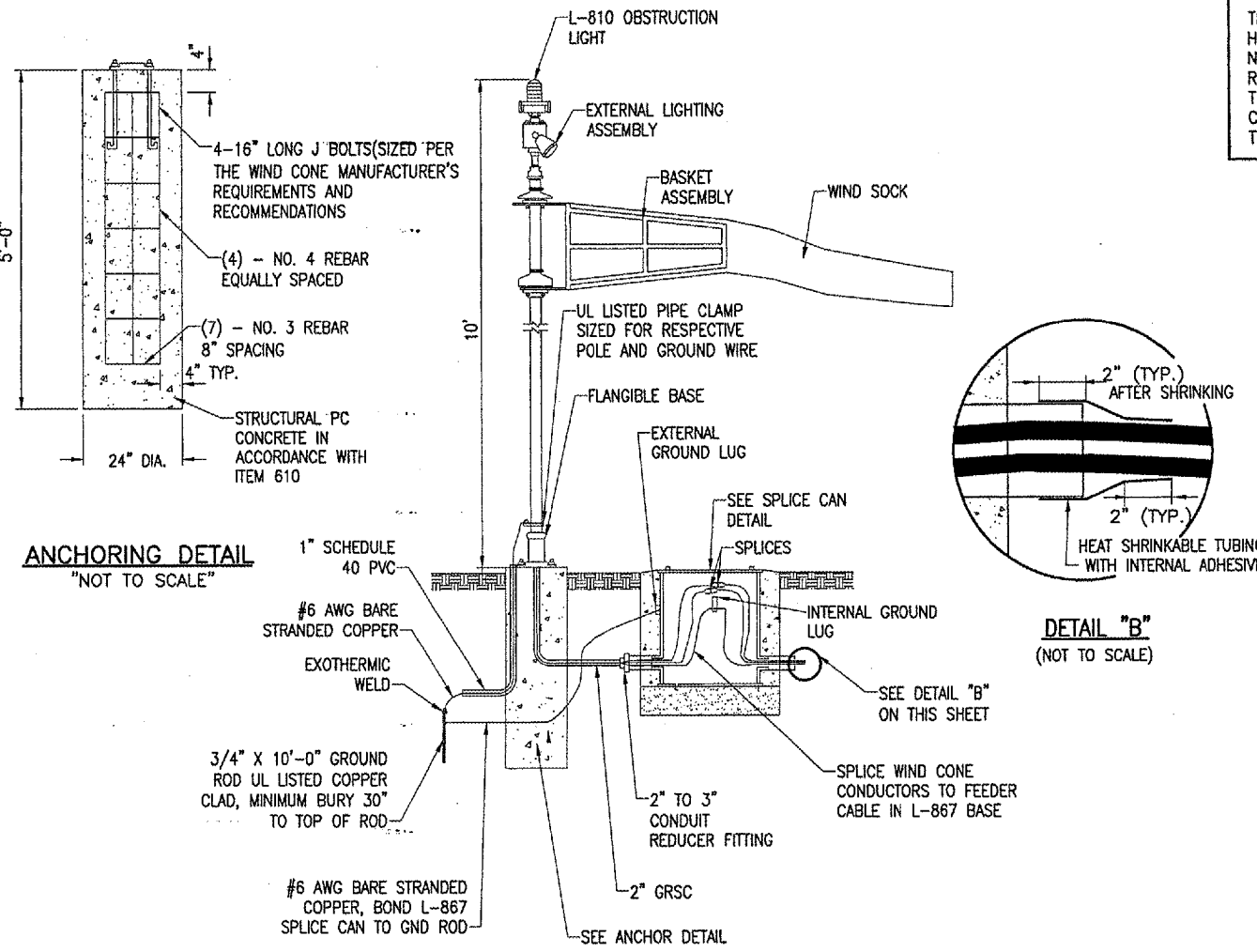
BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE INFORMED AS DESCRIBED IN NOTE 4.

CABLE MARKERS SHALL BE PLACED AT CHANGES OF DIRECTION AND EVERY 200' ALONG CABLE RUNS.

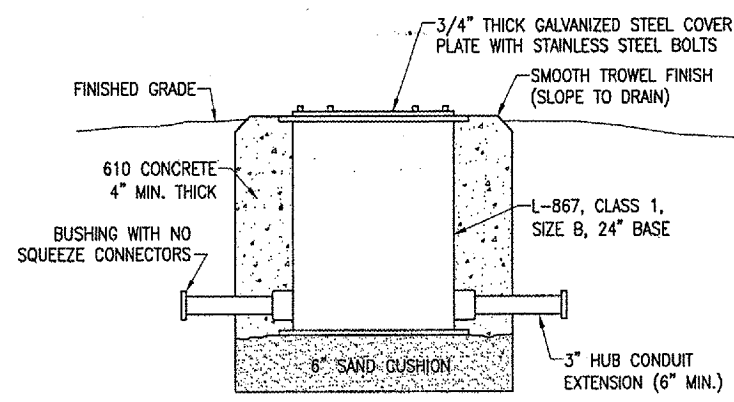
LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.

GENERAL NOTES
SEE LIGHTING LAYOUT SHEET FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.

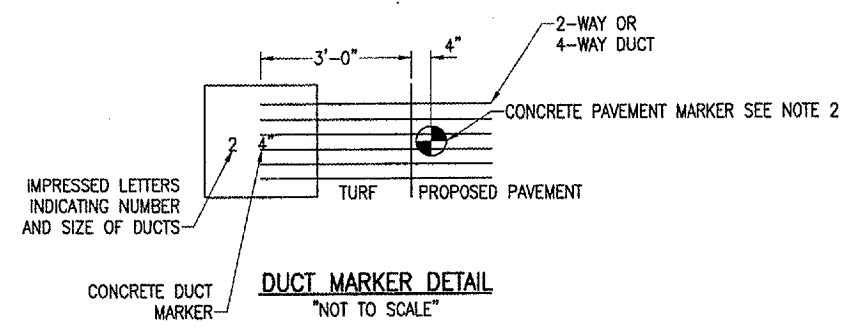
SEE SHEETS 16 THROUGH 17 FOR ELECTRICAL NOTES



EXTERNALLY LIGHTED L806 WIND CONE
NOTE:
WIND CONE SHALL ALSO INCLUDE INTERNAL LIGHTING OPTION

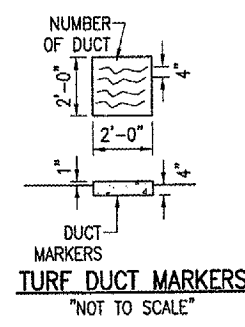


SPLICE CAN DETAIL
(NOT TO SCALE)
INCLUDE INTERNAL AND EXTERNAL GROUND LUGS

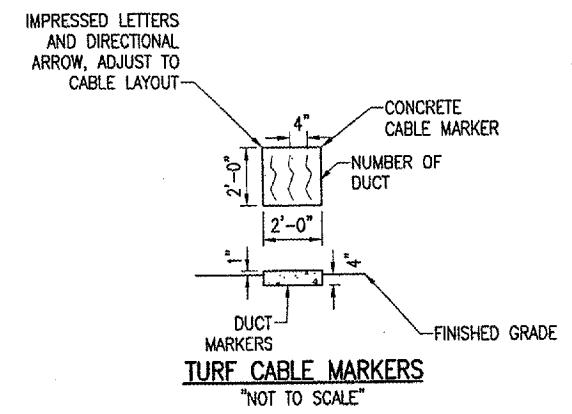


DUCT MARKER DETAIL
"NOT TO SCALE"

NOTE:
TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.



TURF DUCT MARKERS
"NOT TO SCALE"



TURF CABLE MARKERS
"NOT TO SCALE"

BY	
REVISION	
DATE	

DECATUR AIRPORT
DECATUR, ILLINOIS
A.L.P. PROJ.: 3-17-0033-28
I.L. PROJ.: DEC-3414

IEE Project No.	802-04TXVD
Fluorescence	R-549ELE.DWG
Scale	NONE
Date	xx/xx/xx
LAYOUT	KNL 04/22/05
DRAWN	MDR 03/11/05
REVIEWED	CAH 06/07/06

HANSON
Engineers, Architects, Scientists
1925 South State Street
Springfield, Illinois 62703-2886
Offices Nationwide

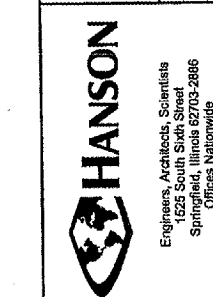
TAXIWAY G WIDENING
PHASE II
WIND CONE
ELEVATION DETAIL

JUN 10, 2005 9:47 AM CCC
F:\AIRPORTS\DECATUR\802-04TXVD\AIRPORT SHEETS\CONTRACT-1\R-549ELE.DWG - ELEC DETAILS

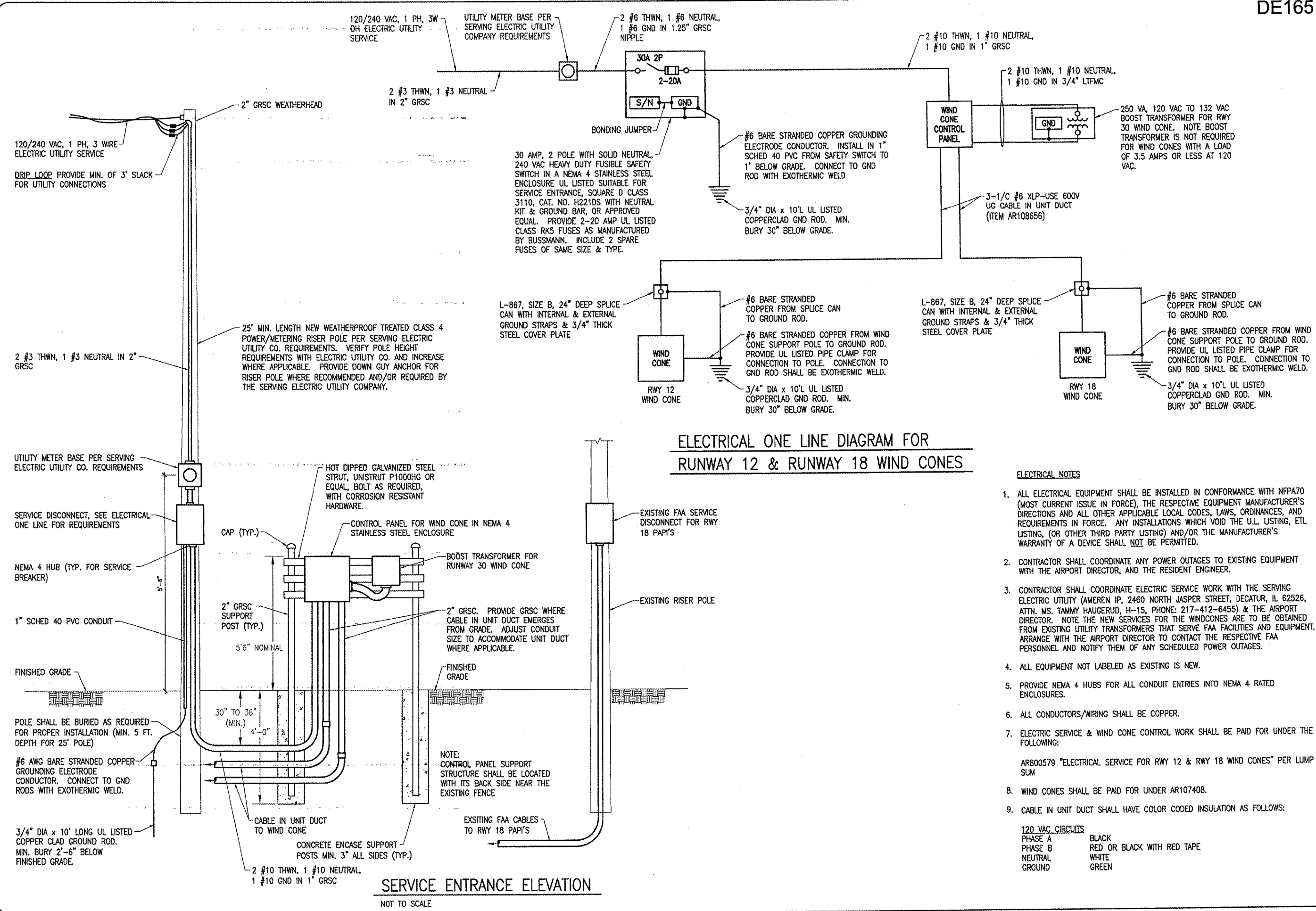
BY	
REVISION	
DATE	

DECATUR AIRPORT
DECATUR, ILLINOIS
 A.L.P. PROJ.: 3-17-0033-28
 I.L. PROJ.: DEC-3414

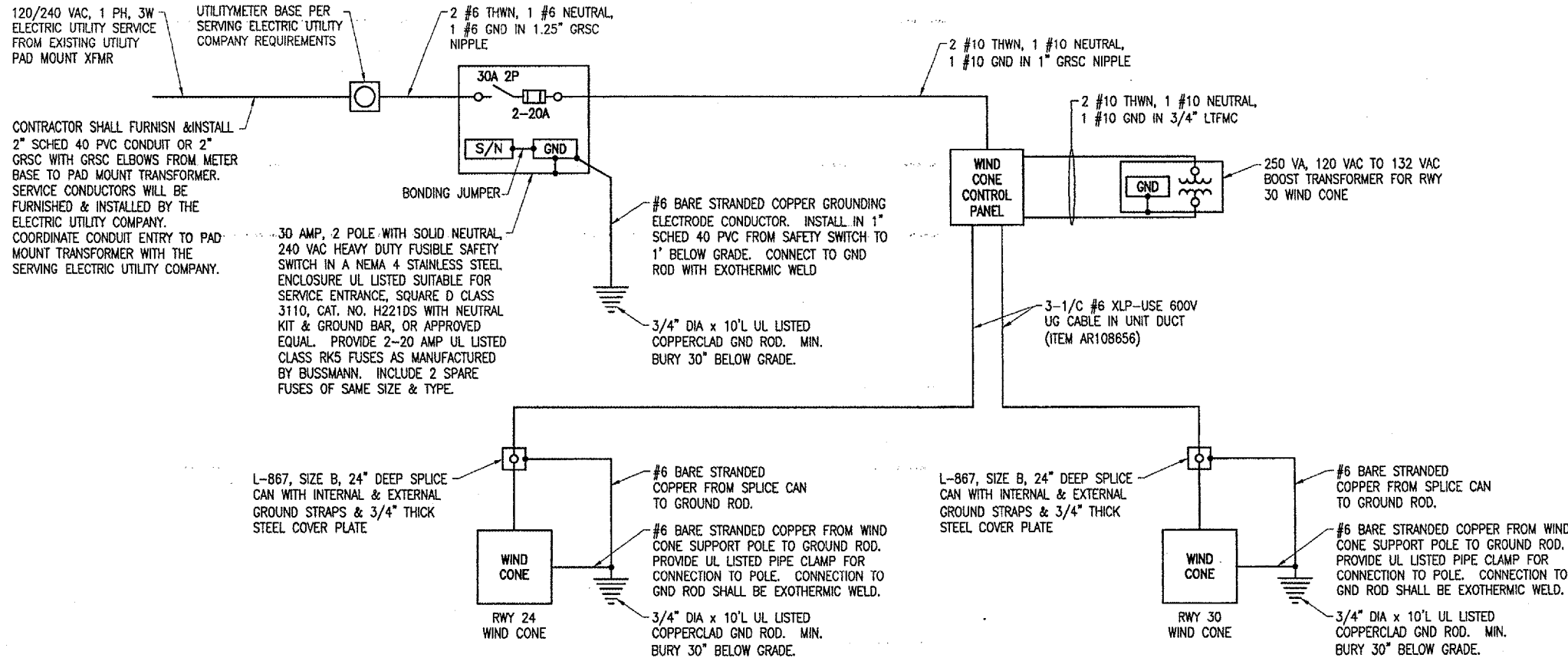
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File Name	E-602.DWG	LAYOUT	KNL
Scale	NONE	DRAWN	MY
Date	XX/XX/XX	REVIEWED	CAH



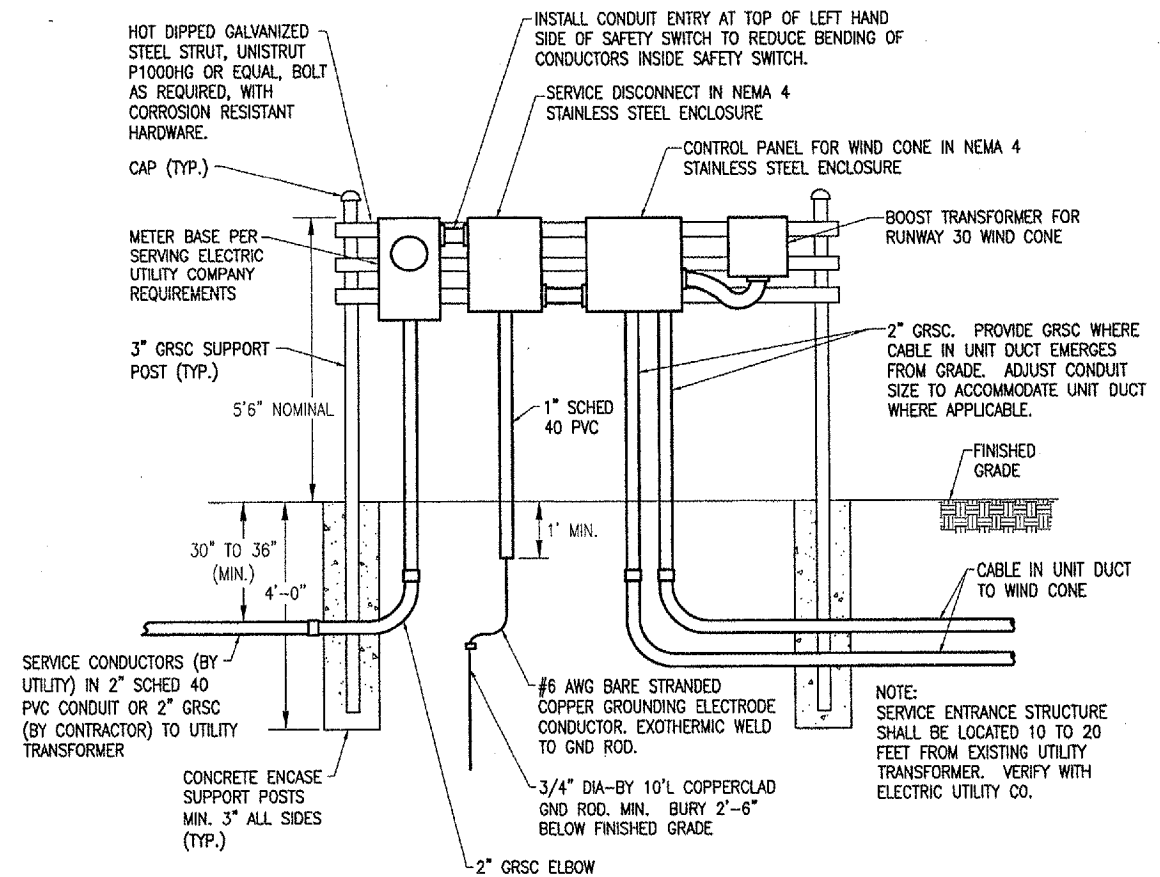
TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL ONE LINE
 DIAGRAM FOR RUNWAY 12
 & RUNWAY 18 WIND CONES



JUN 10, 2005 12:55 PM CCC
 I:\AIRPORTS\DECATUR\802-041XXD\AIRPORT SHEETS\CONTRACT-1\E-602.DWG - Work-FLR



ELECTRICAL ONE LINE DIAGRAM FOR
RUNWAY 24 & RUNWAY 30 WIND CONES



SERVICE ENTRANCE ELEVATION

NOT TO SCALE

ELECTRICAL NOTES

- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
- CONTRACTOR SHALL COORDINATE ANY POWER OUTAGES TO EXISTING EQUIPMENT WITH THE AIRPORT DIRECTOR, AND THE RESIDENT ENGINEER.
- CONTRACTOR SHALL COORDINATE ELECTRIC SERVICE WORK WITH THE SERVING ELECTRIC UTILITY (AMEREN IP, 2460 NORTH JASPER STREET, DECATUR, IL 62526, ATTN. MS. TAMMY HAUGERUD, H-15, PHONE: 217-412-6455) & THE AIRPORT DIRECTOR. NOTE THE NEW SERVICES FOR THE WINDCONES ARE TO BE OBTAINED FROM EXISTING UTILITY TRANSFORMERS THAT SERVE FAA FACILITIES AND EQUIPMENT. ARRANGE WITH THE AIRPORT DIRECTOR TO CONTACT THE RESPECTIVE FAA PERSONNEL AND NOTIFY THEM OF ANY SCHEDULED POWER OUTAGES.
- ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
- PROVIDE NEMA 4 HUBS FOR ALL CONDUIT ENTRIES INTO NEMA 4 RATED ENCLOSURES.
- ALL CONDUCTORS/WIRING SHALL BE COPPER.
- ELECTRIC SERVICE & WIND CONE CONTROL WORK SHALL BE PAID FOR UNDER THE FOLLOWING:
AR800580 "ELECTRICAL SERVICE FOR RWY 24 & RWY 30 WIND CONES" PER LUMP SUM
- WIND CONES SHALL BE PAID FOR UNDER AR107408.
- CABLE IN UNIT DUCT SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:

120 VAC CIRCUITS

PHASE A	BLACK
PHASE B	RED OR BLACK WITH RED TAPE
NEUTRAL	WHITE
GROUND	GREEN

DATE	REVISION	BY

DECATUR, ILLINOIS
A.I.P. PROJ.: 3-17-0033-28
IL PROJ.: DEC-3414

REVISED	06/03/06	CAH
DRAWN	04/25/05	MV
LAYOUT	04/22/05	KNL

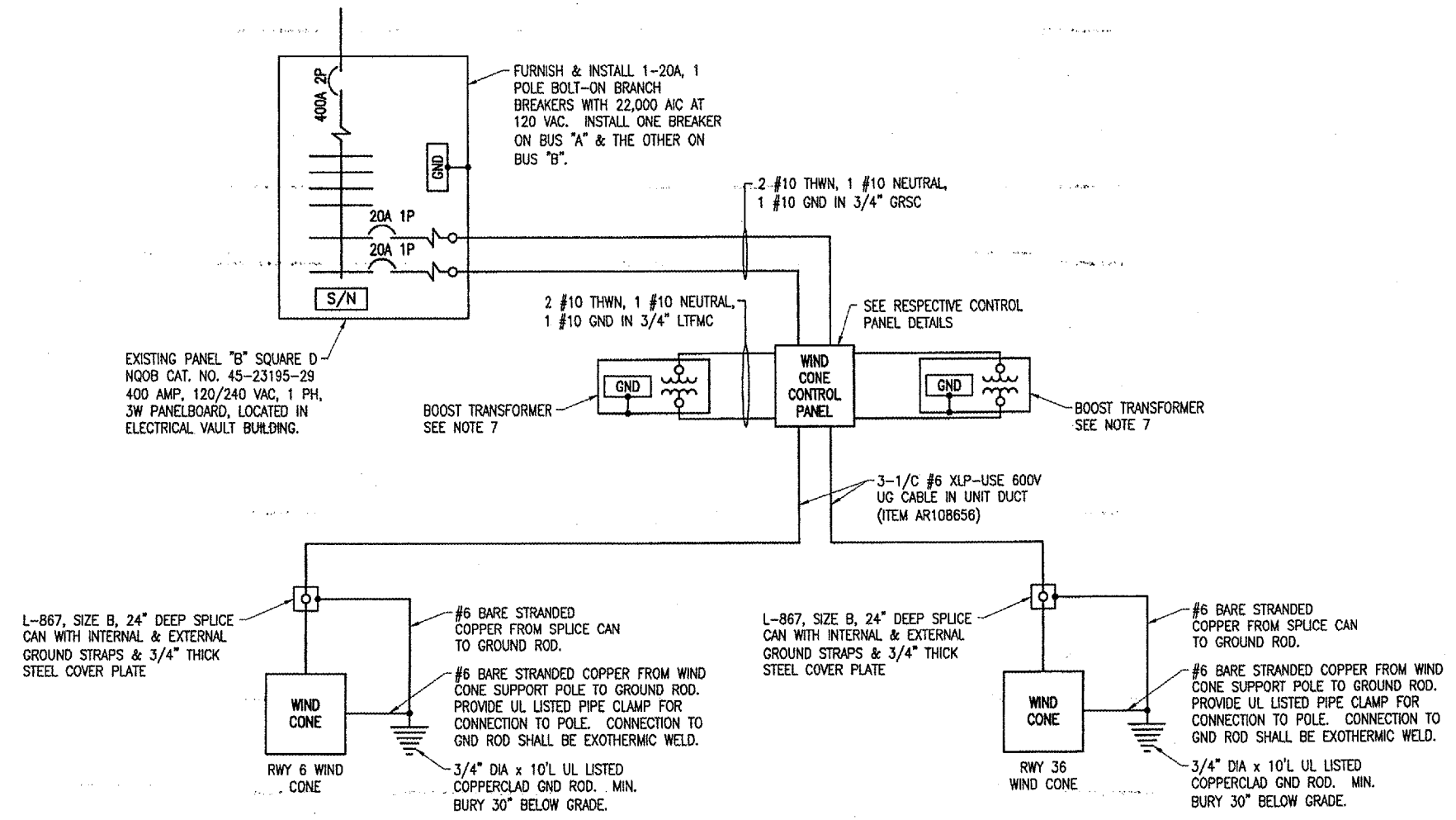
HANSON
Engineers, Architects, Scientists
1528 South Sixth Street
Springfield, Illinois 62703-2886
Office: Nationwide

TAXIWAY G WIDENING
PHASE II
ELECTRICAL ONE LINE
DIAGRAM FOR RUNWAY 24
& RUNWAY 30 WIND CONES

JUN 10, 2005 12:54 PM CCC
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JUN 10, 2005 12:55 PM CCC
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DE165



**ELECTRICAL ONE LINE DIAGRAM FOR
 RUNWAY 6 & RUNWAY 36 WIND CONES**

ELECTRICAL NOTES

1. ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
2. CONTRACTOR SHALL COORDINATE ANY POWER OUTAGES TO EXISTING EQUIPMENT WITH THE AIRPORT DIRECTOR, AND THE RESIDENT ENGINEER.
3. ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
4. ALL CONDUCTORS/WIRING SHALL BE COPPER.
5. ELECTRICAL POWER & WIND CONE CONTROL WORK FOR RUNWAY 6 & RUNWAY 36 WIND CONES SHALL BE PAID FOR UNDER AR800581 "ELECTRIC POWER FOR RWY 6 & RWY 36 WIND CONES" PER LUMP SUM.
6. WIND CONES SHALL BE PAID FOR UNDER AR107408.
7. BOOST TRANSFORMER FOR WIND CONE WITH A LOAD OF APPROXIMATELY 3.5 AMPS SHALL BE 250 VA 120 VAC TO 132 VAC UNIT, SQUARE D CAT. NO. 250S43B OR APPROVED EQUAL. BOOST TRANSFORMER FOR A WIND CONE WITH A LOAD OF APPROXIMATELY 7 AMPS SHALL BE 250 VA 120 VAC, 136 VAC UNIT, SQUARE D CAT. NO. 250S46B OR APPROVED UQUAL.
8. CABLE IN UNIT DUCT SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:
 120 VAC CIRCUITS
 PHASE A BLACK
 PHASE B RED OR BLACK WITH RED TAPE
 NEUTRAL WHITE
 GROUND GREEN

DATE	REVISION	BY



HL Project No.	802-04TXD
Filename	E-603.DWG
Scale	NONE
Date	xx/xx/xx
LAYOUT	KNL 04/22/05
DRAWN	KV 04/25/05
REVIEWED	CAH 06/07/05



**TAXIWAY G WIDENING
 PHASE II**
 ELECTRICAL ONE LINE
 DIAGRAM FOR RUNWAY 6
 & RUNWAY 36 WIND CONES

BY	
REVISION	
DATE	

DECATUR, ILLINOIS
 ILL. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

REV. PROJECT No.	802-04TXXD
Revision	E-604.DWG
Scale	NONE
Date	xx/xx/xx
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REVIEWED	CAH 06/06/06

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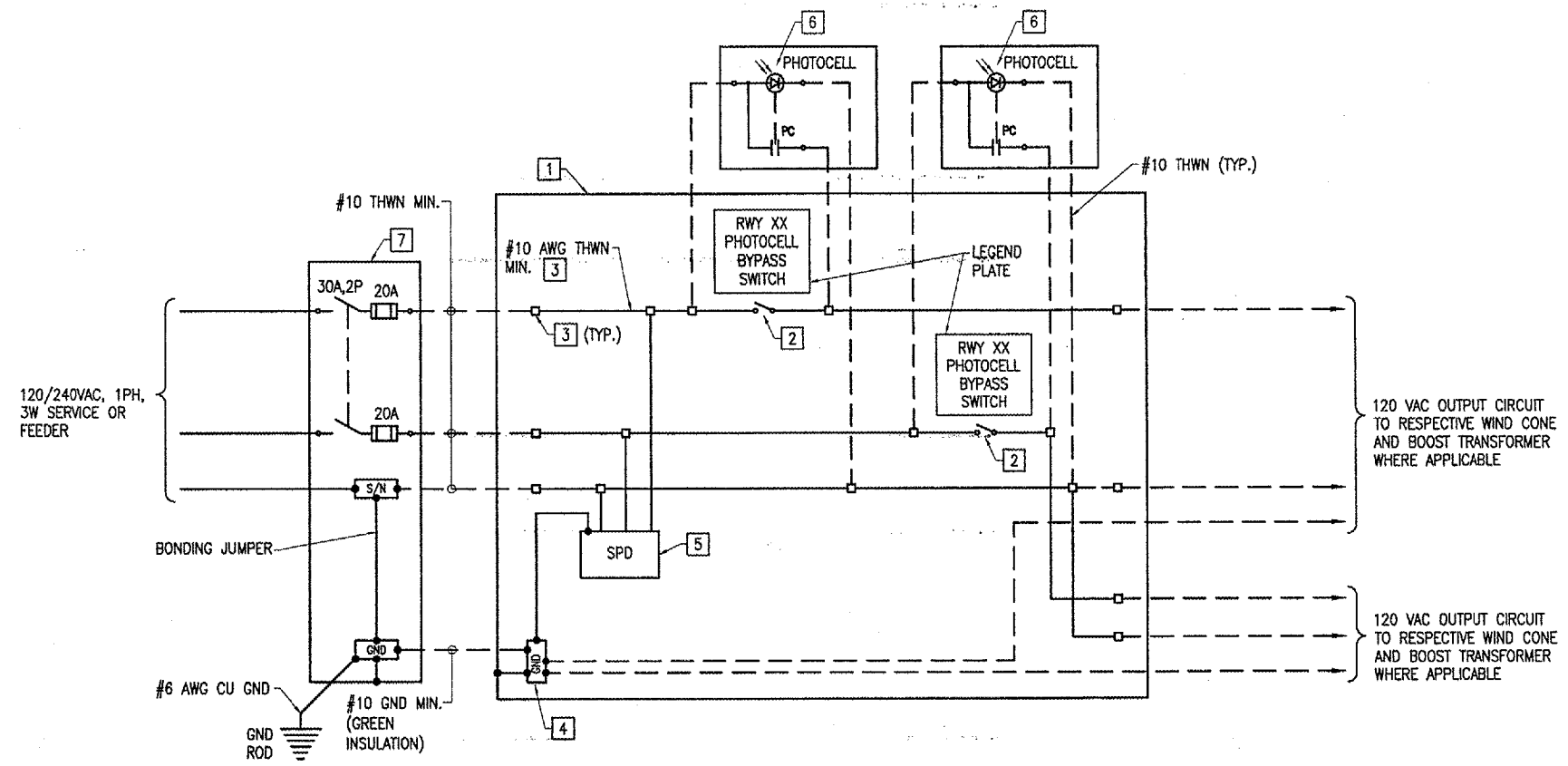
TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL ONE LINE
 DIAGRAM FOR RUNWAY 6
 & RUNWAY 36 WIND CONES

KEYED NOTES

- CONTROL PANEL ENCLOSURE ADEQUATELY SIZED TO HOLD THE RESPECTIVE COMPONENTS AND EQUIPMENT. PANEL ENCLOSURE SHALL BE UL LISTED NEMA 4X STAINLESS STEEL WITH HINGED COVER FOR OUTDOOR APPLICATIONS. PANEL ENCLOSURE SHALL BE UL LISTED NEMA 12 PAINTED STEEL WITH HINGED COVER FOR THE UNIT LOCATED IN THE AIRPORT ELECTRICAL VAULT. PROVIDE INNER DOOR TO MOUNT PHOTOCELL BYPASS SWITCHES.
- 20 AMP, 120 VAC, 1 POLE SPECIFICATION GRADE TOGGLE SWITCH. MOUNT ON PANEL INNER DOOR PROVIDE LEGEND PLATES LABELED "RUNWAY XX PHOTOCELL BYPASS SWITCH" WHERE "XX" IS THE RESPECTIVE RUNWAY MEMBER. PROVIDE ADDITIONAL LEGEND PLATES LABELED "ON" & "PHOTOCELL", TO INDICATE SWITCH FUNCTION & POSITIONS.
- POWER CONTROL WIRING SHALL BE SIZED AS REQUIRED PER NEC MINIMUM #10 AWG TYPE MTW, THW, OR THWN, COPPER. TERMINAL BLOCKS FOR POWER & CONTROL WIRING SHALL BE 600 VOLT, WITH AMPERAGE RATINGS IN CONFORMANCE WITH NEC TABLE 310-16 USING 75 DEGREE C WIRE FOR THE RESPECTIVE WIRE LUG RANGE, BOX LUG TYPE, SQUARE D CLASS 9080, TYPE GC6, OR APPROVED EQUAL. PROVIDE A MINIMUM OF 8 SPARE TERMINAL BLOCKS TO ACCOMMODATE CONNECTIONS TO BOOST TRANSFORMERS.
- EQUIPMENT GROUNDING BAR: PROVIDE A GROUNDING BAR MOUNTED AND BONDED INSIDE THE PANEL ENCLOSURE, ADEQUATELY SIZED TO ACCOMMODATE ALL GROUND CONDUCTORS TO OR FROM THE CONTROL PANEL. TERMINATE ONE GROUND WIRE PER LUG/TERMINAL.
- U.L. LISTED PER UL1449, AC SURGE PROTECTOR SUITABLE FOR 120/240 VAC, 1 PH, 3W PLUS GROUND SYSTEM, WITH SURGE CURRENT RATING OF 40 KA (MIN.), 8x20 MICROSECOND WAVE, PER MODE, AND STATUS INDICATION LIGHTS, JOSLYN MODEL 1265-21, SQUARE D CAT. NO. TVS120XR40S OR APPROVED EQUAL. MAINTAIN LEADS AS SHORT & AS STRAIGHT AS POSSIBLE. INCLUDE MOUNTING BRACKET.
- PHOTOCELL RATED 2000 WATTS AT 120 VAC, WITH OFF DELAY, AND -40 DEGREE C TO 60 DEGREE C OPERATING TEMPERATURE RANGE, TORK MODEL NO. 2101, OR APPROVED EQUAL. WHERE CONTROL PANEL IS LOCATED INSIDE A BUILDING THE PHOTOCELL SHALL BE MOUNTED JUST ABOVE ROOF LEVEL OF RESPECTIVE BUILDING WHERE CONTROL PANEL IS INSTALLED. PHOTOCELL SHALL FACE NORTH.
- 30 AMP, 2 POLE, 240 VAC, U.L. LISTED, HEAVY DUTY FUSIBLE SAFETY SWITCH IN A NEMA 4X STAINLESS STEEL ENCLOSURE, WITHOUT KNOCKOUTS, SQUARE D CLASS 3110, CAT. NO. H221DS OR APPROVED EQUAL. PROVIDE TWO 20 AMP, U.L. LISTED CLASS RK5 FUSES AS MANUFACTURED BY BUSSMANN. INCLUDE 2 SPARE FUSES OF THE SAME SIZE AND TYPE. INCLUDE LEGEND PLATE "LABELED SERVICE DISCONNECT 120/240 VAC, 1 PH, 3W MAX FUSE SIZE: 20 AMP." SEE "ELECTRICAL ONE LINE DIAGRAM FOR WIND CONES" FOR ADDITIONAL DETAILS.

NOTES

- FURNISH & INSTALL A WEATHERPROOF WARNING LABEL FOR EACH SERVICE DISCONNECT & CONTROL PANEL TO WARN PERSONS OF POTENTIAL ELECTRIC ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "FLASH PROTECTION".
- ELECTRIC SERVICE/POWER & WIND CONE CONTROL WORK SHALL BE PAID FOR UNDER THE FOLLOWING:
 AR800579 ELECTRIC SERVICE FOR RUNWAY 12 AND RUNWAY 18 WIND CONES - PER LUMP SUM
 AR800580 ELECTRIC SERVICE FOR RUNWAY 24 AND RUNWAY 30 WIND CONES - PER LUMP SUM
 AR800581 ELECTRIC POWER FOR RUNWAY 6 AND RUNWAY 36 WIND CONES - PER LUMP SUM
- PROVIDE LEGEND PLATES FOR THE RESPECTIVE WIND CONE CONTROL PANELS AS FOLLOWS:
 "CONTROL PANEL RWY 6 WIND CONE & RWY 36 WIND CONE"
 "CONTROL PANEL RWY 24 WIND CONE & RWY 30 WIND CONE"
 "CONTROL PANEL RWY 12 WIND CONE & RWY 18 WIND CONE"



CONTROL PANEL FOR WIND CONES

(TYPICAL FOR 3)

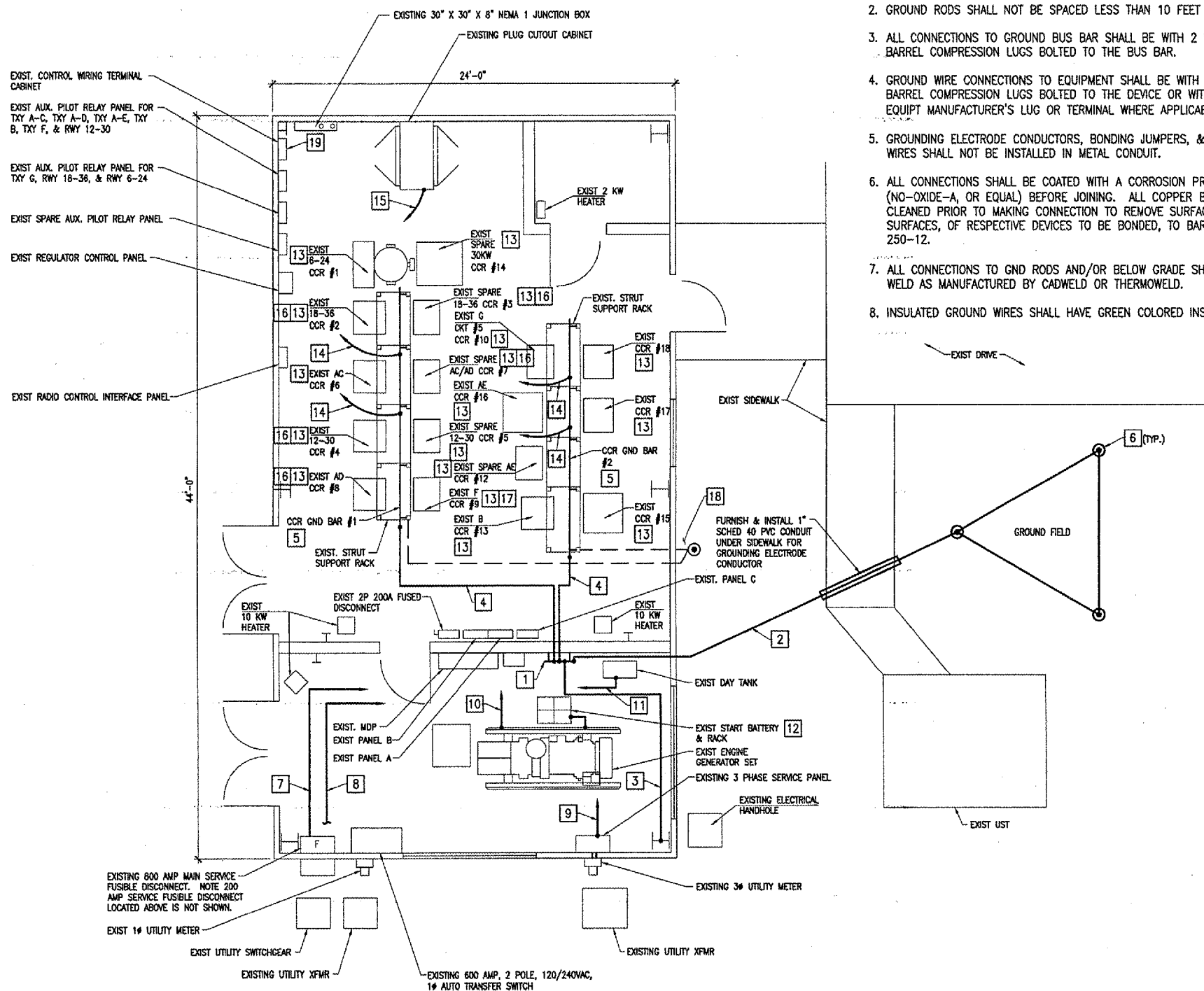
NOTES

- SEE "VAULT GROUND RISER" AND "GROUNDING DETAILS" SHEETS FOR ADDITIONAL GROUNDING REQUIREMENTS & DETAILS.
- GROUND RODS SHALL NOT BE SPACED LESS THAN 10 FEET APART.
- ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
- GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIPT MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
- GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT.
- ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (NO-OXIDE-A, OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTION TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE MEAL, PER NEC 250-12.
- ALL CONNECTIONS TO GND RODS AND/OR BELOW GRADE SHALL BE EXOTHERMIC WELD AS MANUFACTURED BY CADWELD OR THERMOWELD.
- INSULATED GROUND WIRES SHALL HAVE GREEN COLORED INSULATION.

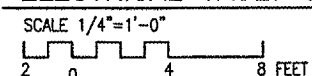
KEYED NOTES

- MAIN GROUND BAR; 1/4" THICK BY 4" WIDE BY 18" LONG COPPER BUS BAR WITH WALL MOUNTING BRACKETS & INSULATORS, HARGER LIGHTNING PROTECTION INC. CAT. NO. GB14418N, OR APPROVED EQUAL. LOCATE 4' ABOVE FINISHED FLOOR.
- #3/0 AWG BARE STRANDED COPPER GROUNDING ELECTRODE CONDUCTOR FROM MAIN GROUND BAR TO GROUND FIELD. PROVIDE 1" PVC SLEEVE AT BLDG EXIT. CONNECT TO GROUND BAR WITH 2-HOLE TONGUE LONG BARREL DOUBLE COMPRESSION CRIMP LUG & APPROPRIATE BOLTS, NUTS, & WASHERS. CONNECT TO GROUND FIELD WITH EXOTHERMIC WELD.
- #3/0 AWG COPPER GROUNDING ELECTRODE CONDUCTOR FROM MAIN GROUND BAR TO BUILDING STEEL COLUMN. CONNECTIONS SHALL BE WITH 2-HOLE TONGUE LONG BARREL DOUBLE COMPRESSION CRIMP LUG BOLTED TO RESPECTIVE DEVICE.
- #3/0 AWG COPPER IN 1" SCHED 40 PVC FROM MAIN GROUND BAR TO RESPECTIVE CCR GROUND BUS.
- 1/4" THICK BY 2" WIDE COPPER BUS BAR TO REPLACE EXISTING GROUND BUS. MOUNT TO EXISTING STRUT SUPPORT RACK. GROUND BUS BAR LENGTH SHALL SPAN SUPPORT RACK. INCLUDE MOUNTING HARDWARE.
- 3/4" x 10 FT. LONG UL LISTED COPPERCLAD GROUND ROD. CONNECT GROUND RODS WITH #3/0 BARE STRANDED COPPER TO FORM A GROUND FIELD. GROUND RODS SHALL NOT BE SPACED LESS THAN 10 FEET APART. ADJUST LOCATION TO AVOID INTERFERENCE WITH EXISTING SIDEWALKS OR PAVEMENT. CONNECTIONS TO GND RODS SHALL BE EXOTHERMIC WELD.
- #3/0 AWG COPPER IN 1" SCHED 40 PVC FROM 800 AMP SERVICE DISCONNECT GROUND BUS TO MAIN GROUND BAR.
- #2 AWG COPPER IN 1" SCHED 40 PVC FROM 200 AMP SERVICE DISCONNECT GROUND BUS TO MAIN GROUND BAR.
- #2 AWG COPPER IN 1" SCHED 40 PVC FROM 200 AMP, 3 PHASE SERVICE PANEL GROUND BUS TO MAIN GROUND BAR.
- #1/0 AWG COPPER FROM ENGINE GENERATOR FRAME TO MAIN GROUND BAR.
- #6 AWG COPPER FROM DAY TANK TO MAIN GROUND BAR.
- BOND BATTERY RACK FRAME TO ENGINE GENERATOR FRAME WITH #6 AWG COPPER BONDING JUMPER.
- FOR EACH CONSTANT CURRENT REGULATOR DISCONNECT EXISTING #6 AWG COPPER BONDING JUMPER FROM EXISTING VAULT GROUND BUS AND RECONNECT TO NEW CCR GND BUS. SEE NOTE 3.
- BOND HIGH VOLTAGE & LOW VOLTAGE WIREWAYS TO RESPECTIVE CCR GROUND BAR WITH #6 AWG COPPER BONDING JUMPER (WIREWAYS NOT SHOWN FOR CLARITY). SEE NOTES 3 & 4.
- BOND CUTOUT ENCLOSURE TO RESPECTIVE CCR GROUND BAR WITH #6 AWG COPPER BONDING JUMPER. SEE NOTES 3 & 4.
- EXISTING EQUIPT GROUND WIRE WITH 240 VAC FEEDER CIRCUIT FROM PANELBOARD TO CCR ORIGINATES FROM NEUTRAL BUS IN THE PANELBOARD. DISCONNECT EQUIPMENT GROUND WIRE FROM NEUTRAL BUS AND RE-TERMINATE ON EQUIPMENT GROUND BAR IN PANELBOARD. REMOVE WHITE TAPE & RE-IDENTIFY WITH GREEN TAPE AT ALL POINTS OF ACCESS.
- REPLACE EXISTING #10 AWG BRANCH CIRCUIT CONDUCTORS WITH 2 #6 THWN, 1 #6 GND.
- EXISTING GROUND RODS CONNECTED TO THE EXISTING CCR GROUND BARS SHALL BE DISCONNECTED & REMOVED.
- EXISTING CONTROL WIRING FOR TAXIWAY G, CIRCUIT #5 THAT RUNS FROM THE CONTROL TOWER TO THE BAGGAGE ROOM IN THE TERMINAL BUILDING AND ON TO THE VAULT SHALL BE DISCONNECTED & RECONNECTED TO AVAILABLE SPARE CONDUCTORS INSTALLED PREVIOUSLY IN 2005. SEE SPECIAL PROVISION SPEC SECTION AR109200 FOR DETAILS.

ALL WORK SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ITEM AR109200 INSTALL ELECTRICAL EQUIPMENT PER LUMP SUM



ELECTRICAL VAULT GROUNDING PLAN



BY	
REVISION	
DATE	

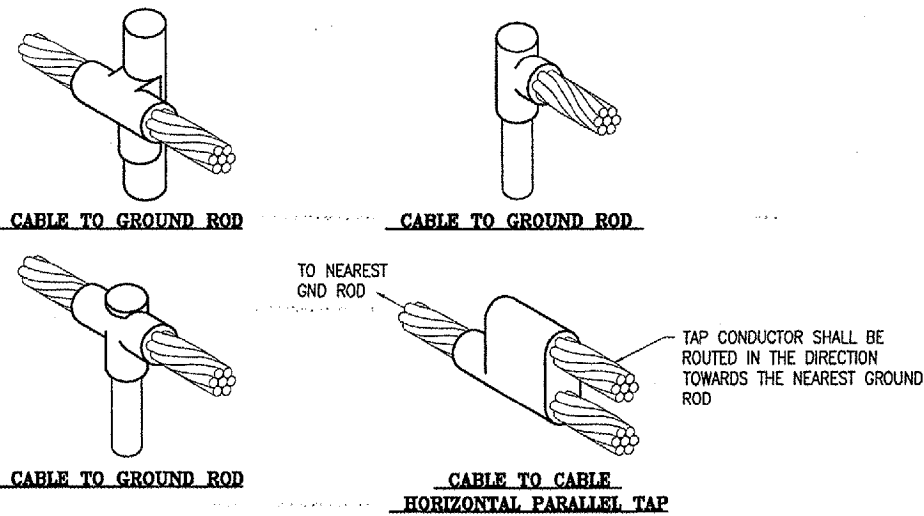
DECATUR AIRPORT
 DECATUR, ILLINOIS
 ILL. PROJ.: DEC-3414
 A.I.P. PROJ.: 3-17-0033-28

HEI Project No.	802-041X10
Filename	E-101.DWG
Scale	1/4" = 1'-0"
Date	XX/XX/XX
LAYOUT	KNL 04/22/05
DRAWN	MV 04/25/05
REVIEWED	CAH 06/07/06

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TAXIWAY G WIDENING
 PHASE II
 ELECTRICAL VAULT
 GROUNDING PLAN

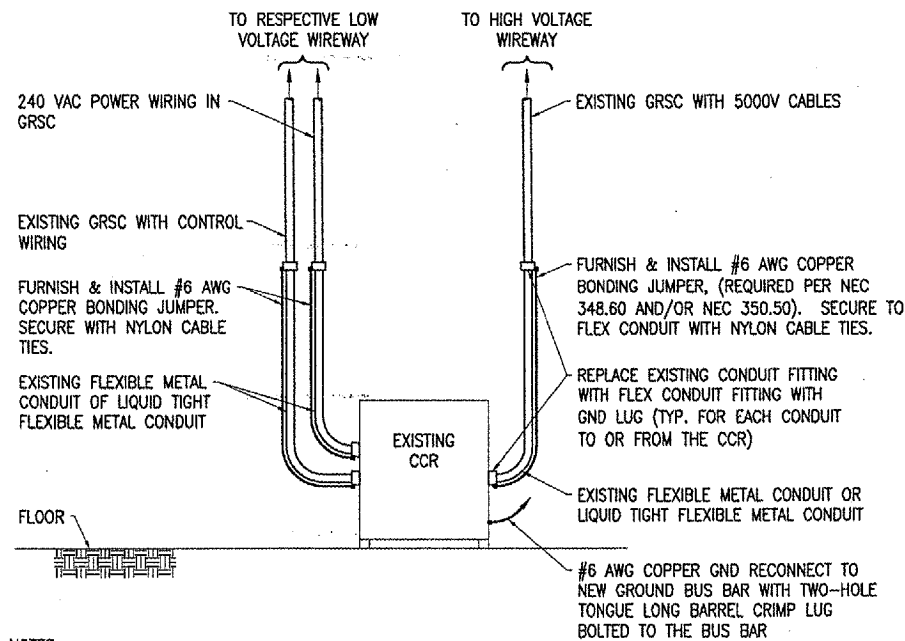
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DETAIL NOTES

- EXOTHERMIC WELDS SHALL BE CADWELD AS MANUFACTURED BY ERICO PRODUCTS, SOLON, OHIO, ULTRAWELD AS MANUFACTURED BY HARGER LIGHTNING PROTECTION & GROUNDING EQUIPMENT, GRAYSLAKE, IL, OR THERMOWELD AS MANUFACTURED BY CONTINENTAL INDUSTRIES, TULSA, OKLAHOMA. VERIFY PROPER SIZES, MOLDS, TYPES, AND REQUIREMENTS FOR THE RESPECTIVE APPLICATION WITH THE MANUFACTURER, AND INSTALL PER THEIR DIRECTIONS.
- FOR APPLICATIONS TO GALVANIZED STEEL OR PAINTED STEEL, REMOVE GALVANIZING AND/OR PAINT & CLEAN THE SURFACE TO EXPOSE BARE STEEL BEFORE MAKING EXOTHERMIC WELD CONNECTION.
- INDIVIDUAL GROUNDING ELECTRODE CONDUCTORS SHALL NOT BE INSTALLED IN METAL CONDUIT. INSTALL GROUNDING ELECTRODE CONDUCTORS IN SCHED 40 PVC CONDUIT AS REQUIRED IN FOUNDATIONS, FOR PROTECTION, WHERE ENTERING ENCLOSURES, ETC.

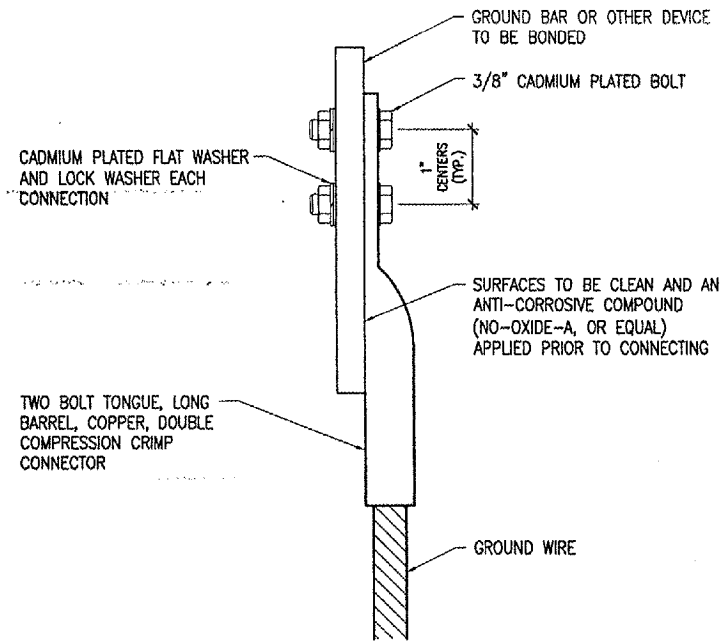
EXOTHERMIC WELD DETAILS



NOTES

- THE EXISTING 5000V CIRCUIT CONDUCTORS FROM EACH CONSTANT CURRENT REGULATOR TO THE RIGID CONDUIT & HIGH VOLTAGE WIREWAY ARE INSTALLED IN FLEXIBLE METAL CONDUIT. REPLACE THE EXISTING CONDUIT TERMINATION FITTINGS WITH EXTERNAL GROUND LUG TYPE FLEX CONDUIT FITTINGS TO ACCOMMODATE AN EXTERNAL BONDING JUMPER. FURNISH & INSTALL A #6 AWG COPPER EXTERNAL BONDING JUMPER FOR EACH FLEXIBLE METAL CONDUIT ASSOCIATED WITH THE 5000V SERIES CIRCUITS. THIS APPLIES TO THE FOLLOWING CONSTANT CURRENT REGULATORS: CCR #1, CCR #2, CCR #3, CCR #4, CCR #6, CCR #7, CCR #8, CCR #9, CCR #10, CCR #12, & CCR #13.
- THE EXISTING 240 VAC POWER & CONTROL CIRCUIT CONDUCTORS FROM THE RESPECTIVE RIGID CONDUIT TO EACH CONSTANT CURRENT REGULATOR ARE INSTALLED IN FLEXIBLE METAL CONDUIT. REPLACE THE EXISTING CONDUIT TERMINATION FITTINGS WITH EXTERNAL GROUND LUG TYPE FLEX CONDUIT FITTINGS TO ACCOMMODATE AN EXTERNAL BONDING JUMPER. FURNISH & INSTALL A #6 AWG COPPER EXTERNAL BONDING JUMPER FOR EACH FLEXIBLE METAL CONDUIT. THIS APPLIES TO THE FOLLOWING CONSTANT CURRENT REGULATORS: CCR #1, CCR #2, CCR #3, CCR #4, CCR #6, CCR #7, CCR #8, CCR #9, CCR #10, CCR #12, & CCR #13.

CCR GROUNDING UPGRADES

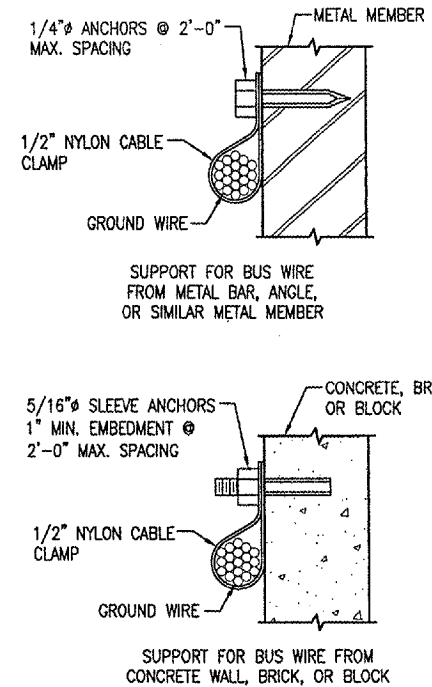


2 HOLE LONG BARREL COMPRESSION LUG TABLE		
WIRE SIZE	BURNDY CAT. NO.	THOMAS & BETTS CAT. NO.
#6 AWG STRANDED	YA6C-2TC38	256-30695-1158
#2 AWG STRANDED	YA2C-2TC38	256-30695-1160
#1/0 AWG STRANDED	YA25-2TC38	256-30695-1162
#2/0 AWG STRANDED	YA26-2TC38	256-30695-1116
#3/0 AWG STRANDED	YA27-2TC38	54816BE

NOTES

- ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
- GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIPMENT MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
- GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT.
- ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (NO-OXIDE-A, OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE METAL, PER NEC 250-12.

GROUNDING LUG CONNECTION DETAIL



NOTE: WHERE NOTED ON THE PLANS GROUND WIRES SHALL BE RUN IN SCHED 40 PVC CONDUIT (SIZE AS DETAILED). PVC CONDUIT CONTAINING GROUND CONDUCTORS SHALL NOT HAVE METAL SUPPORTS THAT COMPLETELY ENIRCLE THE CONDUIT. USE NYLON BOLTS, NUTS, WASHERS, AND/OR REINFORCED FIBERGLASS STRUT SUPPORT. DO NOT USE CABLE TIES NOR TIE WRAPS.

TYPICAL GROUND WIRE SUPPORT DETAIL

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BY	
REVISION	
DATE	
<p>HANSON Engineers, Architects, Scientists 1825 S. Main St. Springfield, Illinois 62703-2886 Offices Nationwide</p>	
<p>TAXIWAY G WIDENING PHASE II GROUNDING DETAILS</p>	
<p>109 X of 110 sheets</p>	
<p>REV. PROJECT NO. 802-04TXYD E-501.DWG SCALE NONE DATE XX/XX/XX</p>	<p>LAYOUT KNL 04/22/05 DRAWN MV 04/25/05 REVIEWED CAH 06/07/06</p>
<p>DECATUR, ILLINOIS A.I.P. PROJ.: 3-17-0033-28 I.L. PROJ.: DEC-3414</p>	

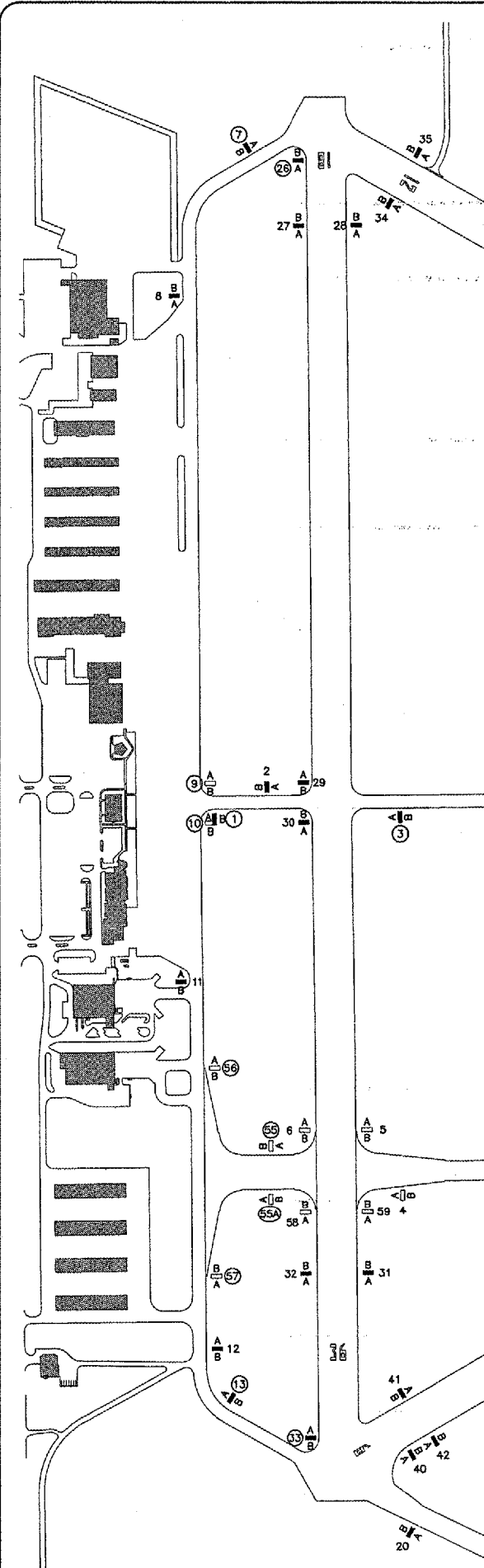
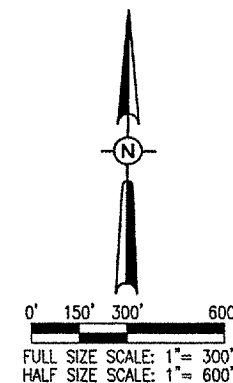
QUANTITIES - (THIS SHEET)				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR125452	TAXI GUIDANCE SIGN, 12 CHARACTER	EACH	1	
AR125470	MODIFY EXISTING SIGN PANEL	EACH	10	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	1	

NOTE

- FOR SIGNS NOTED TO BE MODIFIED, THE CONTRACTOR WILL ORDER THE INDIVIDUAL PANELS OR FULL PANELS, WHICH EVER IS REQUIRED, TO INSTALL INTO THE SIGN TO HAVE THE FACE OF THE SIGN READ AS STATED FOR THE PROPOSED SIDE A AND SIDE B IN THE GUIDANCE SIGN DATA TABLE.
- THE EXISTING SIGNS TO BE MODIFIED ARE EITHER STANDARD SIGN COMPANY OR AIRFIELD GUIDANCE SIGN (AGM). THE DATA TABLE DENOTES THE MANUFACTURE.
- THE STANDARD SIGN COMPANY SIGNS HAVE INDIVIDUAL SIGN PANEL CHARACTERS AND CAN BE ORDERED SEPARATELY AND INSTALLED.
- SIGN NUMBER 55, 56, AND 57 ARE AIRFIELD GUIDANCE SIGN (AGM) SIGNS. ON THESE SIGNS, THE ENTIRE MESSAGE PER FACE WILL REQUIRE CHANGING.
- REMOVED SIGNS, TRANSFORMERS, AND SIGN PANELS WILL BE TURNED OVER TO THE AIRPORT DIRECTOR. PROVIDING THE DIRECTOR DOES NOT WANT THE SALVAGED MATERIALS, THE CONTRACTOR WILL DISPOSE OF THE MATERIALS.
- ALL DISTURBED AREAS, AS A RESULT OF THE REMOVALS, WILL BE REGRADED, HOLES FILLED WITH TOP SOIL MATERIAL, SMOOTH GRADED, SEEDED AND FERTILIZED ACCORDING TO SPECIFICATION ITEM: AR901510.
- PAYMENT FOR THESE ITEMS OF WORK WILL BE AS FOLLOWS:
 AR125452 "TAXI GUIDANCE SIGN, 12 CHARACTER" _____ PER EACH.
 AR125470 "MODIFY EXISTING SIGN PANEL" _____ PER EACH.
 AR125904 "REMOVE TAXI GUIDANCE SIGN" _____ PER EACH.

GUIDANCE SIGN LEGEND

- A** LOCATION SIGN - YELLOW ON BLACK, L-858L
- 18-36** MANDATORY SIGN - WHITE ON RED, L-858R
- ←A→** DESTINATION AND DIRECTION SIGN - BLACK ON YELLOW
- STANDARD A - OLD STANDARD SIGN WITH PANEL SECTIONS
- STANDARD B - NEW STANDARD LUMACURVE SIGN
- ⑦ GUIDANCE SIGN DATA LISTING



GUIDANCE SIGN DATA

SIGN/TAG NUMBER	SIDE A		SIDE B		EXISTING CHARACTERS	LETTER SIZE	MANUFACTURE TYPE	COMMENTS
	EXISTING	PROPOSED	EXISTING	PROPOSED				
①	G →		← C B →	← A →	-	12"	STANDARD A	TO BE MODIFIED
③	24•30 ↑	⊘	A 36-18	36-18	-	12"	STANDARD B	TO BE MODIFIED
⑦	B	A	B 12-30 18-36 ↓	A 12-30 18-36 ↓	-	12"	STANDARD B	TO BE MODIFIED
⑨	← A B C ↑	← A A A ↑	B	A	-	12"	STANDARD	TO BE MODIFIED
⑩	C	A	C B ↑ A →	A ↑ A A →	-	12"	STANDARD	TO BE MODIFIED
⑬	C 18-36 K 24-B	A 18-36 K 24-B	C	A	12	12"	STANDARD B	TO BE REMOVED; NEW BASE AND SIGN INSTALLED
⑯	D		A ↗		-	12"		TO BE REMOVED; SHOWN & COUNTED ON SHEET 96
⑰	D	⊘	D 12-30	12-30	-	12"	STANDARD B	TO BE MODIFIED
⑳	↙ C	↙ A	18	NO CHANGE	-	12"	STANDARD A	TO BE MODIFIED
㉓	C ↘	A ↘	36	NO CHANGE	-	12"	STANDARD A	TO BE MODIFIED
㉖	D →		BLANK		-	12"	STANDARD A	TO BE REMOVED; SHOWN & COUNTED ON SHEET 96
㉗	← D		BLANK		-	12"	STANDARD A	TO BE REMOVED; SHOWN & COUNTED ON SHEET 96
⑤⑤	G ← C →	BLANK	G 18-36	NO CHANGE	-	12"	AGM	TO BE MODIFIED
⑤⑤A	BLANK	BLANK	← C G C →	← A G A →	-	12"	AGM	TO BE MODIFIED
⑤⑥	← G C	← G A	C APRON ↑	APRON ↑	-	12"	AGM	TO BE MODIFIED
⑤⑦	C G →	A G →	C 36•6 ↑	36•6 ↑	-	12"	AGM	TO BE MODIFIED

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 HANSON <small>Engineers, Architects, Scientists 1525 South Sixth Street Springfield, Illinois 62705-2886 Offices Nationwide</small>	TAXIWAY G WIDENING PHASE II PROPOSED GUIDANCE SIGN MODIFICATIONS & SIGN DATA	110 <small>110 of 110 sheets</small>	ILL. PROJ.: DEC-3414 A.I.P. PROJ.: 3-17-0033-28	REVISION DATE	
IPI Project No. 802-04TXD File Name R-123CON.DWG Scale 1"=xx' Date 05/31/05	LAYOUT DRAWN CCC REVIEWED WJM	Date 05/31/05 Date 05/31/05 Date 06/07/05			