

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO EXECUTIVE AIRPORT

REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING



ILLINOIS PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD



Kris
 06-07-18
 Exp. 11-30-19

811 Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

JUNE 8, 2018

CHICAGO EXECUTIVE AIRPORT

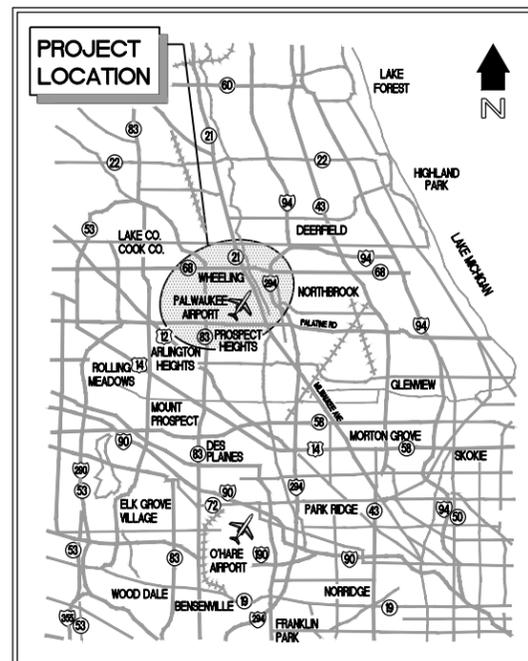
TOWNSHIP: 42 NORTH WHEELING TOWNSHIP
 RANGE: 11 EAST (SECTION: 13)
 COOK COUNTY

CMT 16290-04
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 062-069052

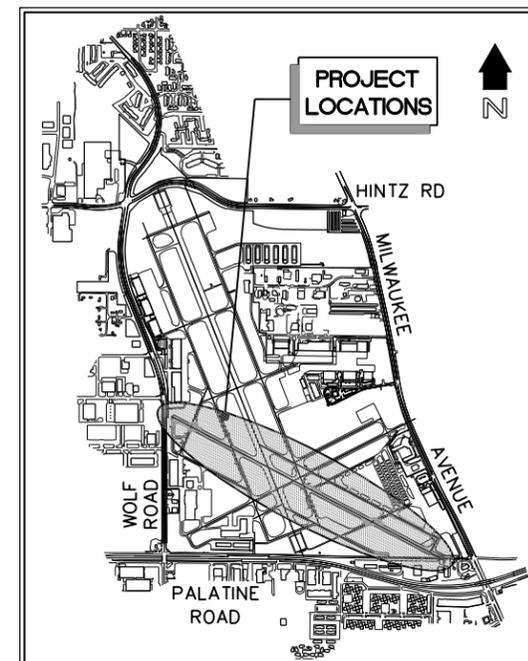
SUBMITTED BY *Kris*
 KRIS SALVATERA, PE
 DATE 06-07-18

CHICAGO EXECUTIVE AIRPORT

APPROVED *Jamie Abbott* EXECUTIVE DIRECTOR
 JAMIE ABBOTT
 DATE 06-07-18



LOCATION MAP



SITE PLAN

PROJECT INFORMATION

CONTRACTOR:
 RESIDENT ENGINEER:
 ORIGINAL CONTRACT AMOUNT:
 FINAL CONSTRUCTION COST:
 IDOT LETTING DATE:
 IDOT AWARD DATE:
 NOTICE TO PROCEED:
 START OF CONSTRUCTION:
 SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION

VILLAGE OF WHEELING - 847.459.2600
 CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG

NPDES #
 FAA AIRSPACE #
 CCDD LPC-663 DATED 11/16/12
 MWRDGC PERMIT # 03-246 & RL 09-063
 VILLAGE APP FOR CONSTRUCTION PERMIT #
 VILLAGE FLOODPLAIN PERMIT #
 CONTRACTORS REGISTRATION WITH VILLAGE
 VILLAGE SITE ALTERATION PERMIT #
 CITY APPLICATION FOR PERMIT #
 CITY FLOODPLAIN PERMIT #
 CITY SITE GRADING PERMIT #
 CONTRACTOR'S REGISTRATION WITH CITY

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NOTES

- SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. **FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED.** EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL TO THE CONTRACT.
- WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, OR THE CONTRACTOR IS PLANNING ON WORKING ON OR AROUND FAA CABLES, CONDUITS OR EQUIPMENT, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL TO THE CONTRACT. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.
- ALL ELEVATIONS SHOWN ON PLANS ARE IN 1929 DATUM. SUBTRACT 0.24 FEET FROM ELEVATIONS SHOWN TO OBTAIN 1988 NAVD.

MUNICIPALITIES GENERAL NOTES

- THE CHICAGO EXECUTIVE AIRPORT IS A JOINT OWNERSHIP BY BOTH THE VILLAGE OF WHEELING AND CITY OF PROSPECT HEIGHTS. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH VILLAGE AND CITY CODES, ORDINANCES AND STANDARDS AS APPLICABLE.
- ALL CONTRACTORS AND SUBCONTRACTORS SHALL BE REGISTERED WITH THE VILLAGE AND CITY PRIOR TO THE NOTICE TO PROCEED. ALL REGISTRATION FEES SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL WORK WITH THE AIRPORT AND ENGINEER TO SECURE THE REQUIRED VILLAGE AND CITY LOCAL CONSTRUCTION PERMITS PRIOR TO THE NOTICE TO PROCEED.
- THE CONTRACTOR SHALL COORDINATE WITH THE VILLAGE AND CITY AT THE WEEKLY PROGRESS MEETINGS AND SHALL NOTIFY THE CITY OF PROSPECT HEIGHTS (847.398.6700) AND THE VILLAGE OF WHEELING (847.459.2600) A MINIMUM OF 48 HOURS PRIOR TO ANY REQUIRED VILLAGE/CITY INSPECTIONS.
- ALL STORM SEWERS AND SANITARY SEWERS ON THE AIRPORT SITE ARE OWNED, OPERATED AND MAINTAINED BY THE CHICAGO EXECUTIVE AIRPORT UNLESS LABELED OTHERWISE.

EXISTING CONDITIONS LEGEND

- ☒ EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- ☒ EXISTING BASE MOUNTED HIGH INTENSITY RUNWAY LIGHT
- ☒ EXISTING STAKE MOUNTED HIGH INTENSITY RUNWAY LIGHT
- EXISTING IN PAVEMENT MOUNTED HIGH INTENSITY RUNWAY LIGHT
- ⌵ EXISTING ELEVATED RETROREFLECTIVE MARKER
- ◻ ◻ ◻ ◻ EXISTING VISUAL APPROACH SLOPE INDICATOR (VASI)
- ◁ ▷ EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- ◻◻◻◻ EXISTING APPROACH LIGHTING SYSTEM
- ▭ EXISTING AIRFIELD GUIDANCE SIGN
- ☒ EXISTING WINDCONE
- ⊙ EXISTING WINDCONE WITH SEGMENTED CIRCLE
- ◻ ET EXISTING ELECTRICAL TRANSFORMER
- EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
- ☒ EXISTING ELECTRICAL HANDHOLE
- ◻ EXISTING STORM INLET
- ◻ EXISTING SLOPE BOX
- ▷ EXISTING FLARED END SECTION
- ⋈ EXISTING HEADWALL
- ⊙ EXISTING CLEANOUT STRUCTURE
- ⊕ EXISTING FLUSH MOUNTED FIRE HYDRANT
- ⊕ EXISTING WATER VALVE
- ☒ EXISTING UTILITY PEDESTAL
- ▬▬▬▬ EXISTING CONDUIT/DUCT BANK
- 16/34— EXISTING RUNWAY 16/34 CIRCUIT
- 12/30— EXISTING RUNWAY 12/30 CIRCUIT
- 6/24— EXISTING RUNWAY 6/24 CIRCUIT
- 16APP— EXISTING RUNWAY 16 APPROPACH LIGHTING CIRCUIT
- G/D— EXISTING TAXIWAY G AND D CIRCUIT
- DLMY— EXISTING TAXIWAY D, L, M, N, Y AND Z CIRCUIT
- ABEF— EXISTING TAXIWAY A, B, E AND F CIRCUIT
- KILO— EXISTING TAXIWAY KILO CIRCUIT
- HFC— EXISTING TAXIWAY HFC CIRCUIT
- REIL— EXISTING REIL CABLES
- VASI— EXISTING VASI CABLES
- FAA— EXISTING FAA CABLES
- FAA COMED— EXISTING COMED SERVICE TO FAA ILS SYSTEM
- FAA WIND— EXISTING FAA WIND INSTRUMENT CABLES
- PRIESTER— EXISTING PRIESTER AVIATION CABLES
- ⌋⌋ EXISTING STORM SEWER
- UD— EXISTING UNDERDRAIN
- v— EXISTING WATERMAIN
- SAN— EXISTING SANITARY SEWER
- SAN— EXISTING SEWER IN CASING PIPE
- T— EXISTING TELEPHONE LINES
- CATV— EXISTING CABLE TELEVISION LINES

SUMMARY OF QUANTITIES

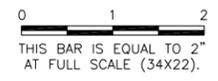
BASE BID - REHABILITATE RUNWAY 12/30				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
*AR110946	ADJUST ELECTRICAL HANDHOLE	EACH	7	
*AR110947	ADJUST ELECTRICAL MANHOLE	EACH	1	
*AR125942	ADJUST BASE MOUNTED LIGHT	EACH	40	
AR150510	ENGINEERS FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR152480	SHOULDER ADJUSTMENT	SY	3800	
AR156520	INLET PROTECTION	EACH	6	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	920	
AR201663	SAND MIX CRACK REPAIR	LF	455	
AR401610	BITUMINOUS SURFACE COURSE	TON	3900	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401640	BITUMINOUS PAVEMENT GROOVING	SY	19200	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	26700	
AR401655	BUTT JOINT CONSTRUCTION	SY	5275	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	280	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	240	
AR501900	REMOVE PCC PAVEMENT	SY	22	
AR603510	BITUMINOUS TACK COAT	GAL	4625	
AR605540	CLEAN & SEAL JOINTS	LF	5170	
AR605542	CLEAN & SEAL EXPANSION JOINTS	LF	594	
AR620520	PAVEMENT MARKING-WATERBORNE	SF	36060	
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	5081	
AR620900	PAVEMENT MARKING REMOVAL	SF	3952	
AR705945	ADJUST COLLECTION STRUCTURE	EACH	6	
AR901510	SEEDING	ACRE	2	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	2	
ADDITIVE ALTERNATE 1 - REHABILITATE RUNWAY 12/30 LIGHTING				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
*AS108108	1/C #8 5 KV UG CABLE	LF	17260	
*AS109210	VAULT MODIFICATIONS	LS	1	
*AS109321	10 KW REGULATOR, STYLE 1	EACH	1	
*AS110202	2" PVC DUCT, DIRECT BURY	LF	1125	
*AS125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	1	
*AS125453	TAXI GUIDANCE SIGN, 13 CHARACTER	EACH	1	
*AS125454	TAXI GUIDANCE SIGN, 14 CHARACTER	EACH	3	
*AS125470	MODIFY EXISTING SIGN PANEL	EACH	4	
*AS125904	REMOVE TAXI GUIDANCE SIGN	EACH	4	
*AS125922	REPLACE BASE MOUNTED LIGHT	EACH	60	
AS152480	SHOULDER ADJUSTMENT	SY	100	
AS620520	PAVEMENT MARKING-WATERBORNE	SF	2724	
AS620525	PAVEMENT MARKING-BLACK BORDER	SF	1355	
AS620900	PAVEMENT MARKING REMOVAL	SF	3795	
AS901510	SEEDING	ACRE	0.02	
AS908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	0.02	

*DENOTES SPECIALTY ITEM.

IL CONTRACT: PA061
IL LETTING ITEM: 10A
IL PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
 INDEX TO SHEETS,
 SUMMARY OF QUANTITIES AND GENERAL NOTES

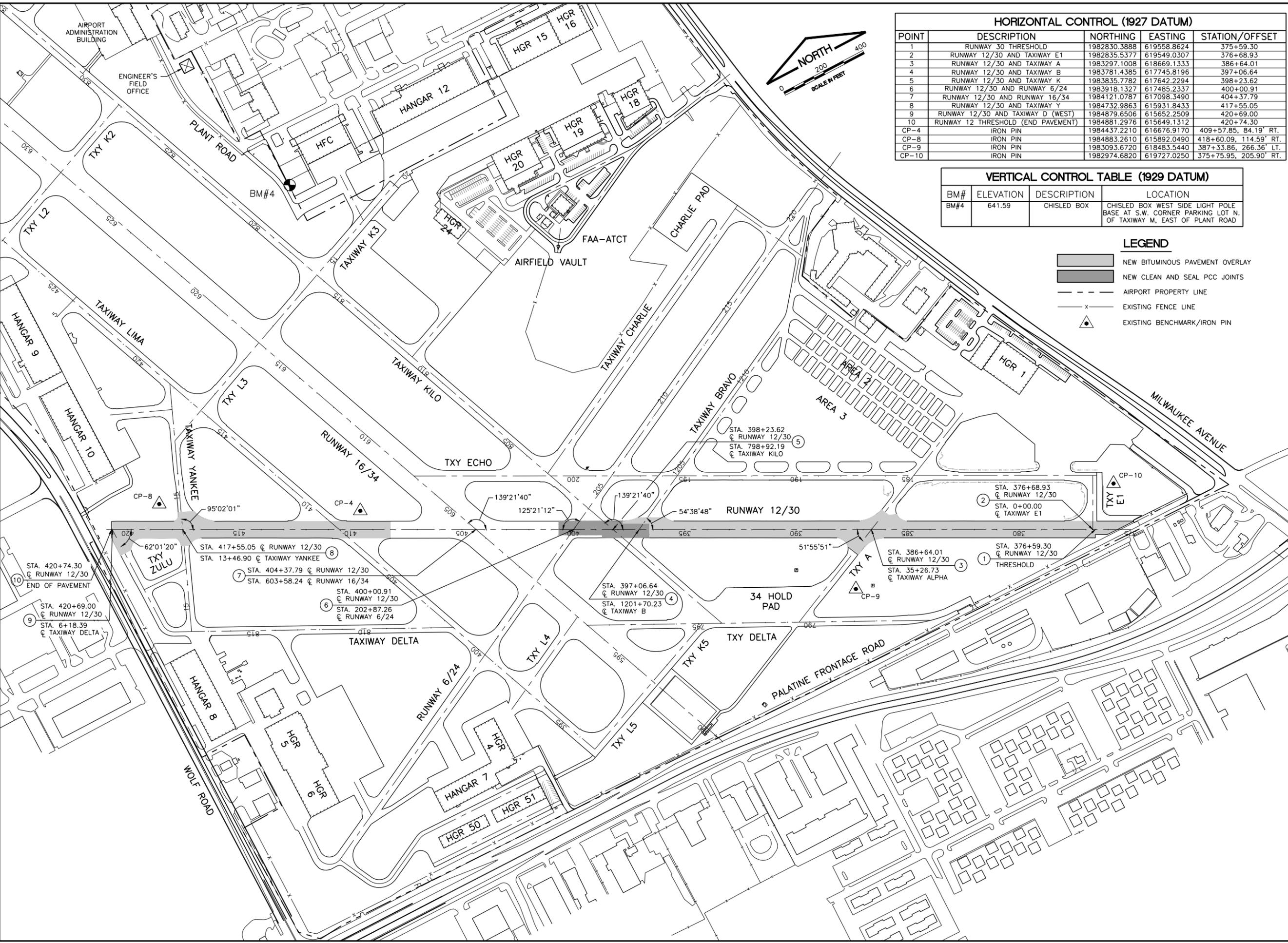
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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

DATE: Friday, June 8, 2018 4:22:32 PM
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 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
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HORIZONTAL CONTROL (1927 DATUM)				
POINT	DESCRIPTION	NORTHING	EASTING	STATION/OFFSET
1	RUNWAY 30 THRESHOLD	1982830.3888	619558.8624	375+59.30
2	RUNWAY 12/30 AND TAXIWAY E1	1982835.5377	619549.0307	376+68.93
3	RUNWAY 12/30 AND TAXIWAY A	1983297.1008	618669.1333	386+64.01
4	RUNWAY 12/30 AND TAXIWAY B	1983781.4385	617745.8196	397+06.64
5	RUNWAY 12/30 AND TAXIWAY K	1983835.7782	617642.2294	398+23.62
6	RUNWAY 12/30 AND RUNWAY 6/24	1983918.1327	617485.2337	400+00.91
7	RUNWAY 12/30 AND RUNWAY 16/34	1984121.0787	617098.3490	404+37.79
8	RUNWAY 12/30 AND TAXIWAY Y	1984732.9863	615931.8433	417+55.05
9	RUNWAY 12/30 AND TAXIWAY D (WEST)	1984879.6506	615652.2509	420+69.00
10	RUNWAY 12 THRESHOLD (END PAVEMENT)	1984881.2976	615649.1312	420+74.30
CP-4	IRON PIN	1984437.2210	616676.9170	409+57.85, 84.19' RT.
CP-8	IRON PIN	1984883.2610	615892.0490	418+60.09, 114.59' RT.
CP-9	IRON PIN	1983093.6720	618483.5440	387+33.86, 266.36' LT.
CP-10	IRON PIN	1982974.6820	619727.0250	375+75.95, 205.90' RT.

VERTICAL CONTROL TABLE (1929 DATUM)			
BM#	ELEVATION	DESCRIPTION	LOCATION
BM#4	641.59	CHISLED BOX	CHISLED BOX WEST SIDE LIGHT POLE BASE AT S.W. CORNER PARKING LOT N. OF TAXIWAY M, EAST OF PLANT ROAD

- LEGEND**
- NEW BITUMINOUS PAVEMENT OVERLAY
 - NEW CLEAN AND SEAL PCC JOINTS
 - AIRPORT PROPERTY LINE
 - EXISTING FENCE LINE
 - EXISTING BENCHMARK/IRON PIN

IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
 SITE PLAN AND PROJECT CONTROL PLAN

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	DKP
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

SHEET 3 OF 37 SHEETS

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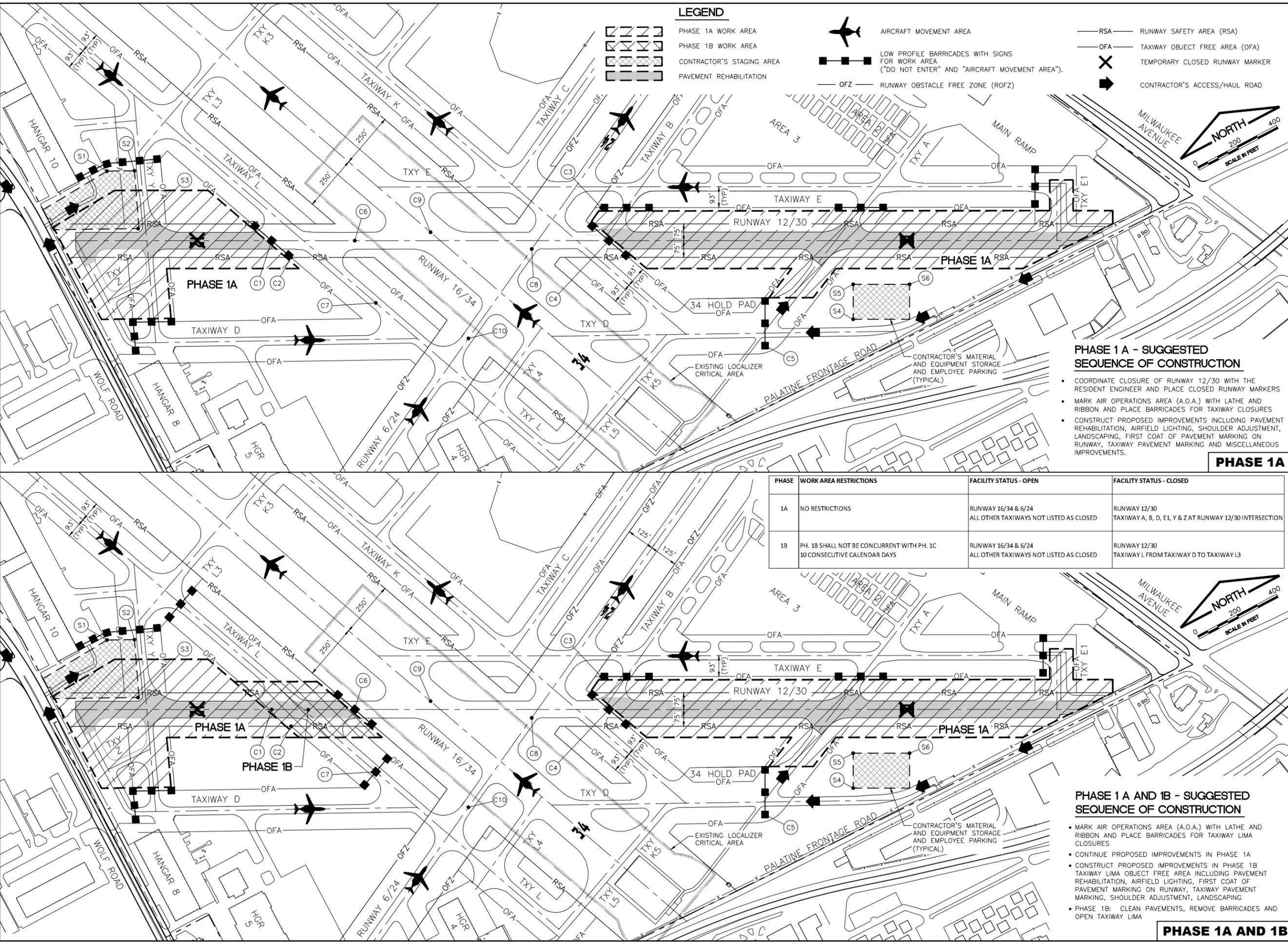
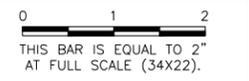
LEGEND

- PHASE 1A WORK AREA
- PHASE 1B WORK AREA
- CONTRACTOR'S STAGING AREA
- PAVEMENT REHABILITATION
- AIRCRAFT MOVEMENT AREA
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- OFZ - RUNWAY OBSTACLE FREE ZONE (ROFZ)
- RSA - RUNWAY SAFETY AREA (RSA)
- OFA - TAXIWAY OBJECT FREE AREA (OFA)
- TEMPORARY CLOSED RUNWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

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PHASE 1A - SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF RUNWAY 12/30 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES
- CONSTRUCT PROPOSED IMPROVEMENTS INCLUDING PAVEMENT REHABILITATION, AIRFIELD LIGHTING, SHOULDER ADJUSTMENT, LANDSCAPING, FIRST COAT OF PAVEMENT MARKING ON RUNWAY, TAXIWAY PAVEMENT MARKING AND MISCELLANEOUS IMPROVEMENTS.

PHASE	WORK AREA RESTRICTIONS	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
1A	NO RESTRICTIONS	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 TAXIWAY A, B, D, E1, Y & Z AT RUNWAY 12/30 INTERSECTION
1B	PH. 1B SHALL NOT BE CONCURRENT WITH PH. 1C 10 CONSECUTIVE CALENDAR DAYS	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 TAXIWAY L FROM TAXIWAY D TO TAXIWAY L3

PHASE 1A AND 1B - SUGGESTED SEQUENCE OF CONSTRUCTION

- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY LIMA CLOSURES
- CONTINUE PROPOSED IMPROVEMENTS IN PHASE 1A
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 1B TAXIWAY LIMA OBJECT FREE AREA INCLUDING PAVEMENT REHABILITATION, AIRFIELD LIGHTING, FIRST COAT OF PAVEMENT MARKING ON RUNWAY, TAXIWAY PAVEMENT MARKING, SHOULDER ADJUSTMENT, LANDSCAPING
- PHASE 1B: CLEAN PAVEMENTS, REMOVE BARRICADES AND OPEN TAXIWAY LIMA

CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
SEQUENCE OF CONSTRUCTION - SHEET 1

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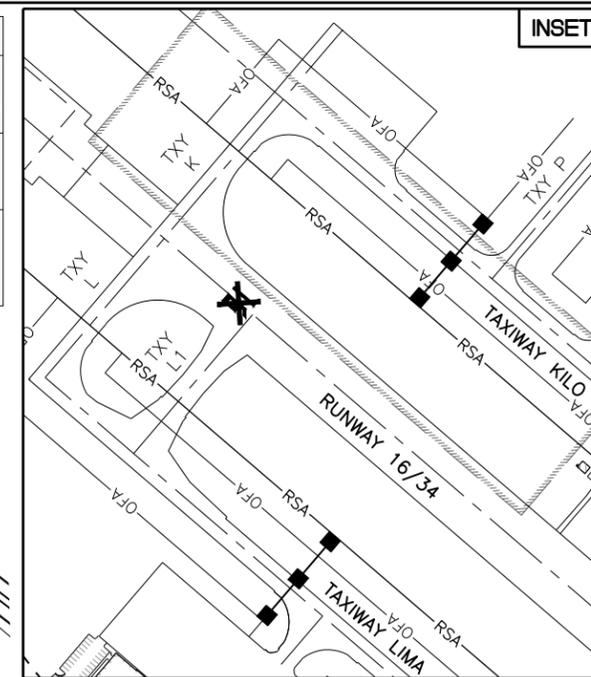
CHICAGO EXECUTIVE AIRPORT

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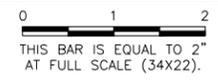
SHEET 4 OF 37 SHEETS

PHASE	WORK AREA RESTRICTIONS	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
1A	NO RESTRICTIONS	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 TAXIWAY A, B, D, E1, Y & Z AT RUNWAY 12/30 INTERSECTION
1C	PH. 1C SHALL NOT BE CONCURRENT WITH PH. 1B 10 CONSECUTIVE CALENDAR DAYS	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 & 6/24 TAXIWAY K FROM TAXIWAY D TO TAXIWAY E
2	PH. 2 SHALL BE CONCURRENT WITH PH. 1A & 1C ONE (1) WEEKEND FROM 10 PM FRIDAY TO 6 AM MONDAY	ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30, 16/34 & 6/24 TAXIWAY E FROM TAXIWAY C TO E1 AIRPORT CLOSED TO FIXED-WING AIRCRAFT



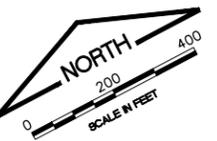
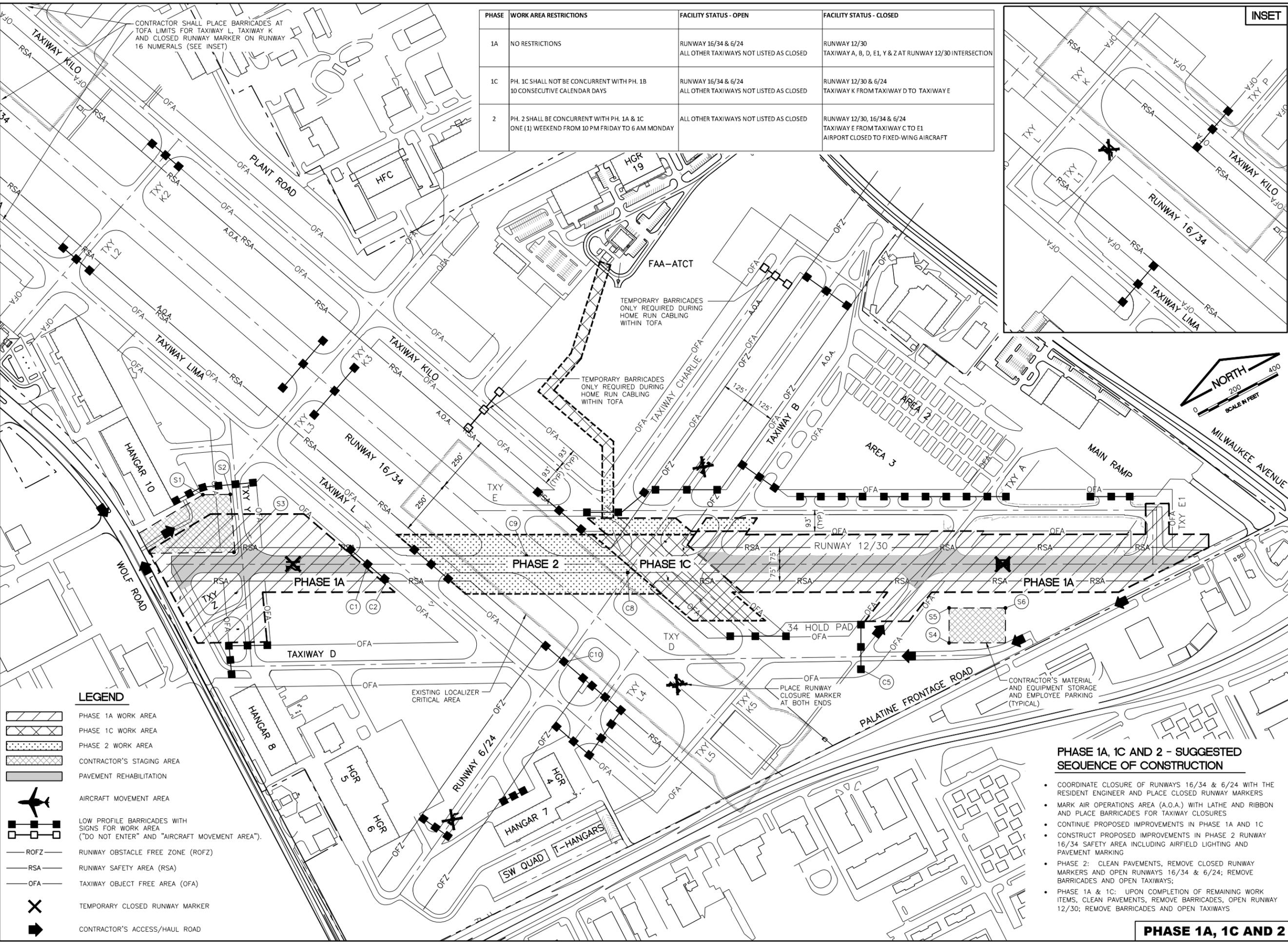
IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



REF: DWG: 16290-04-Runway12-30 base-NEW.dwg
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CONTRACTOR SHALL PLACE BARRICADES AT TOFA LIMITS FOR TAXIWAY L, TAXIWAY K AND CLOSED RUNWAY MARKER ON RUNWAY 16 NUMERALS (SEE INSET)



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
SEQUENCE OF CONSTRUCTION - SHEET 3

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	DKP
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	KWS
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

SHEET 6 OF 37 SHEETS

LEGEND

- PHASE 1A WORK AREA
- PHASE 1C WORK AREA
- PHASE 2 WORK AREA
- CONTRACTOR'S STAGING AREA
- PAVEMENT REHABILITATION
- AIRCRAFT MOVEMENT AREA
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- RUNWAY OBSTACLE FREE ZONE (ROFZ)
- RUNWAY SAFETY AREA (RSA)
- TAXIWAY OBJECT FREE AREA (OFA)
- TEMPORARY CLOSED RUNWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD

PHASE 1A, 1C AND 2 - SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE CLOSURE OF RUNWAYS 16/34 & 6/24 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES
- CONTINUE PROPOSED IMPROVEMENTS IN PHASE 1A AND 1C
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 2 RUNWAY 16/34 SAFETY AREA INCLUDING AIRFIELD LIGHTING AND PAVEMENT MARKING
- PHASE 2: CLEAN PAVEMENTS, REMOVE CLOSED RUNWAY MARKERS AND OPEN RUNWAYS 16/34 & 6/24; REMOVE BARRICADES AND OPEN TAXIWAYS;
- PHASE 1A & 1C: UPON COMPLETION OF REMAINING WORK ITEMS, CLEAN PAVEMENTS, REMOVE BARRICADES, OPEN RUNWAY 12/30; REMOVE BARRICADES AND OPEN TAXIWAYS

PHASE 1A, 1C AND 2

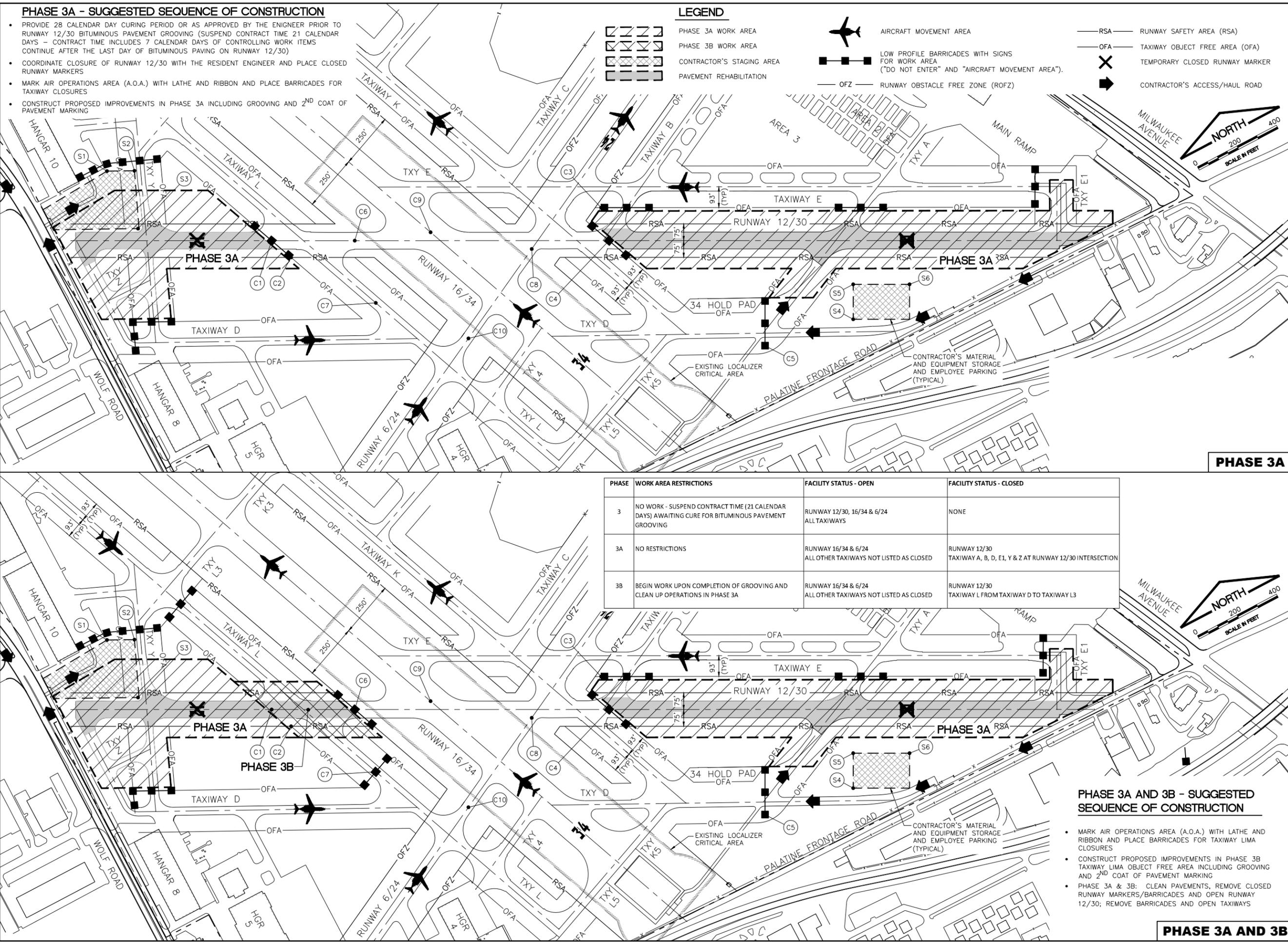
PHASE 3A - SUGGESTED SEQUENCE OF CONSTRUCTION

- PROVIDE 28 CALENDAR DAY CURING PERIOD OR AS APPROVED BY THE ENGINEER PRIOR TO RUNWAY 12/30 BITUMINOUS PAVEMENT GROOVING (SUSPEND CONTRACT TIME 21 CALENDAR DAYS - CONTRACT TIME INCLUDES 7 CALENDAR DAYS OF CONTROLLING WORK ITEMS CONTINUE AFTER THE LAST DAY OF BITUMINOUS PAVING ON RUNWAY 12/30)
- COORDINATE CLOSURE OF RUNWAY 12/30 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY CLOSURES
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 3A INCLUDING GROOVING AND 2ND COAT OF PAVEMENT MARKING

LEGEND

- PHASE 3A WORK AREA
- PHASE 3B WORK AREA
- CONTRACTOR'S STAGING AREA
- PAVEMENT REHABILITATION
- AIRCRAFT MOVEMENT AREA
- LOW PROFILE BARRICADES WITH SIGNS FOR WORK AREA ("DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA").
- RUNWAY OBSTACLE FREE ZONE (ROFZ)
- RSA - RUNWAY SAFETY AREA (RSA)
- OFA - TAXIWAY OBJECT FREE AREA (OFA)
- TEMPORARY CLOSED RUNWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD

REF: DWG: 16290-04-Runway 12-30 base-NEW.dwg
 DATE: Friday, June 8, 2018 4:23:33 PM
 FILE: K:\Chicago\16290-04-Runway 12-30\Draw\Sheets\seq4.dwg
 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
 IMAGE FILES: pwknewlogo_CEA-4-Color-Small (2).jpg
 CEA Alignments.dwg



IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

PHASE	WORK AREA RESTRICTIONS	FACILITY STATUS - OPEN	FACILITY STATUS - CLOSED
3	NO WORK - SUSPEND CONTRACT TIME (21 CALENDAR DAYS) AWAITING CURE FOR BITUMINOUS PAVEMENT GROOVING	RUNWAY 12/30, 16/34 & 6/24 ALL TAXIWAYS	NONE
3A	NO RESTRICTIONS	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 TAXIWAY A, B, D, E1, Y & Z AT RUNWAY 12/30 INTERSECTION
3B	BEGIN WORK UPON COMPLETION OF GROOVING AND CLEAN UP OPERATIONS IN PHASE 3A	RUNWAY 16/34 & 6/24 ALL OTHER TAXIWAYS NOT LISTED AS CLOSED	RUNWAY 12/30 TAXIWAY L FROM TAXIWAY D TO TAXIWAY L3

PHASE 3A AND 3B - SUGGESTED SEQUENCE OF CONSTRUCTION

- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES FOR TAXIWAY LIMA CLOSURES
- CONSTRUCT PROPOSED IMPROVEMENTS IN PHASE 3B TAXIWAY LIMA OBJECT FREE AREA INCLUDING GROOVING AND 2ND COAT OF PAVEMENT MARKING
- PHASE 3A & 3B: CLEAN PAVEMENTS, REMOVE CLOSED RUNWAY MARKERS/BARRICADES AND OPEN RUNWAY 12/30; REMOVE BARRICADES AND OPEN TAXIWAYS

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

SEQUENCE OF CONSTRUCTION - SHEET 4

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: DKP
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: KWS
 DATE: 06/08/2018
 JOB No: 16290-04-00

FINAL

SHEET 7 OF 37 SHEETS

DATE: Friday, June 8, 2018 4:23:40 PM
FILE: K:\Chicago\ceda\16290-04_Renobway12-30\Draw\Sheets\gen_Notes1.dwg
UPDATE BY: Kris Solvatero
LAYOUT: Layout1
IMAGE FILES: pavknew\LOGO_CEA-4-COLOR-Small (2).jpg
XREF DWG: bchnt_runway_ncs.dwg

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED.
- THE AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATION PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, RUNWAY AND TAXIWAY CLOSED MARKERS, SAFETY AND OBJECT FREE AREAS, LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. LOW PROFILE BARRICADES SHALL CONFORM TO THE DETAILS IN THE PLANS AND SEQUENCE OF CONSTRUCTION. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE WORK AREA BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS AND OBJECT FREE AREAS.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON-WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- CHICAGO EXECUTIVE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA AND RUNWAY SAFETY AREA OF ACTIVE TAXIWAYS AND RUNWAYS.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

CONTRACTOR CROSSING RUNWAY SAFETY AREAS (RSA) AND TAXIWAY OBJECT FREE AREAS (TOFA)

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREAS (RSA) / RUNWAY OBSTACLE FREE ZONE (OFZ) AND TAXIWAY OBJECT FREE AREAS (TOFA)

RUNWAYS:

- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. ANY WORK WITHIN THE RUNWAY SAFETY AREA OR RUNWAY OBSTACLE FREE AREA WILL REQUIRE A RUNWAY CLOSURE. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED PER FAA REQUIREMENTS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL SHALL BE STOCKPILED WITHIN THE RSA. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN, EQUIPMENT AND BARRICADES.

TAXIWAYS:

- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE FOR WORK WITHIN THE TAXIWAY OBJECT FREE AREA. ANY WORK WITHIN THE TAXIWAY OBJECT FREE AREA WILL REQUIRE A TAXIWAY CLOSURE. WORK WITHIN THE TAXIWAY OBJECT FREE AREA SHALL BE EXPEDITED AND AT THE END OF EACH WORKING PERIOD THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE TAXIWAY TO BE REOPENED PER FAA REQUIREMENTS. IF NECESSARY, STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE TOFA AT NO ADDITIONAL COST TO THE CONTRACT. NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED OR STORED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT.

ALLOWABLE CONSTRUCTION HOURS

- THE ALLOWABLE CONSTRUCTION HOURS FOR THE VILLAGE OF WHEELING AND THE CITY OF PROSPECT HEIGHTS ARE FROM 7 AM TO 6 PM, MONDAY THROUGH SATURDAY. THE AIRPORT WILL SEEK A WAIVER WITH THE VILLAGE AND CITY TO ALLOW CONSTRUCTION OUTSIDE OF THOSE HOURS FOR THE PHASES SHOWN TO BE COMPLETED OVER WEEKENDS ONLY. AT ALL OTHER TIMES, IT IS EXPECTED THE CONTRACTOR WILL ADHERE TO THE VILLAGE AND CITY NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOUR POLICIES. SHOULD THE CONTRACTOR REQUIRE ADDITIONAL WORKING HOURS, HE SHALL REQUEST, THROUGH THE RESIDENT ENGINEER, THAT THE VILLAGE AND CITY BE CONTACTED TO REQUEST ADDITIONAL WAIVER OF THE NOISE ORDINANCE POLICY. ANY FINES LEVIED BY THE VILLAGE OR CITY TO THE AIRPORT FOR VIOLATIONS OF THE NOISE ORDINANCE AND ALLOWABLE CONSTRUCTION HOURS SHALL BE PAID BY THE CONTRACTOR.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE TAXIWAY SITE DUE TO ADJACENT BUILDING CONSTRUCTION

- HAWTHORNE HANGAR AND APRON DEVELOPMENT IN EAST QUADRANT
- AIRFIELD PAVEMENT REPAIRS
- AIRFIELD PAVEMENT REMARKING

GROUND CONTROL FREQUENCY: 121.7 MHz
AIR CONTROL FREQUENCY: 119.9 MHz

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

ELECTRICAL NOTES - ALL PHASES

- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
- WHEN FAA CABLES ARE REQUIRED TO BE LOCATED. A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.
- SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED. EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL AND AT THE CONTRACTOR'S EXPENSE.

TABLE 1 - CRITICAL POINTS

WORK AREA	POINT	APPROXIMATE ELEVATION OF GROUND (1929 DATUM)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (1929 DATUM)	LATITUDE (NAD 83)	LONGITUDE (NAD 83)
1A	C1	646	SEMI/DUMP TRUCK - 25'	671	42°06'51.34"	87°54'15.50"
1A	C2	645	SEMI/DUMP TRUCK - 25'	670	42°06'50.28"	87°54'14.95"
1A	C3	642	SEMI/DUMP TRUCK - 25'	667	42°06'45.78"	87°53'57.26"
1A	C4	644	SEMI/DUMP TRUCK - 25'	669	42°06'44.33"	87°53'57.76"
1A	C5	642	SEMI/DUMP TRUCK - 25'	667	42°06'36.97"	87°53'52.49"
1B	C6	644	SEMI/DUMP TRUCK - 25'	669	42°06'49.61"	87°54'11.13"
1B	C7	644	SEMI/DUMP TRUCK - 25'	669	42°06'46.74"	87°54'11.76"
1C	C8	643	SEMI/DUMP TRUCK - 25'	668	42°06'45.61"	87°54'02.06"
2	C9	645	SEMI/DUMP TRUCK - 25'	670	42°06'48.36"	87°54'06.91"
1C	C10	646	SEMI/DUMP TRUCK - 25'	671	42°06'43.42"	87°54'07.88"
STAGING	S1	643	PICK UP TRUCK - 10'	653	42°06'57.48"	87°54'22.22"
STAGING	S2	644	PICK UP TRUCK - 10'	654	42°06'56.92"	87°54'20.77"
STAGING	S3	646	PICK UP TRUCK - 10'	656	42°06'54.59"	89°54'22.19"
STAGING	S4	641	PICK UP TRUCK - 10'	651	42°06'36.17"	87°53'47.11"
STAGING	S5	640	PICK UP TRUCK - 10'	650	42°06'37.54"	87°53'46.14"
STAGING	S6	643	PICK UP TRUCK - 10'	653	42°06'36.37"	87°53'43.18"

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS

RUNWAY	16/34	12/30	6/24
APPROACH CATEGORY	D	B	B
DESIGN GROUP	III	II	I
DESIGN AIRCRAFT	GULFSTREAM 550	KING AIR B200	CESSNA 421
APPROACH SPEED	141 KNOTS	103 KNOTS	96 KNOTS
WINGSPAN	94 FEET	55 FEET	42 FEET
TAIL HEIGHT	25.8 FEET	15.0 FEET	11.6 FEET
STRENGTH (MGTW)	90,500 LBS.	12,500 LBS.	7,450 LBS.
LENGTH	97 FEET	44 FEET	37 FEET
RUNWAY SAFETY AREA WIDTH (RSA)	500 (250' FROM C)	150 (75' FROM C)	120 (60' FROM C)
(1)(3) RWY OBJECT FREE AREA WIDTH (ROFA)	800	500	400
TAXIWAY SAFETY AREA WIDTH (TSA)	118	79	49
(1)(3) TXY OBJECT FREE AREA WIDTH (TOFA)	186 (93' FROM C)	131(65.5' FROM C)	89 (44.5' FRM C)

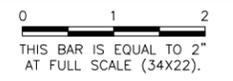
DATA FROM 2009 CEA APPROVED ALP

- TOFA, ROFA, RSA, CRITICAL AREAS, PART 77 AND DEPARTURE/APPROACH SURFACES ARE SHOWN FOR RUNWAY 12/30 WHICH SHALL BE CLOSED DURING THE CONSTRUCTION PHASE DURATION.
- RUNWAY 12/30 END COORDINATES ARE NOT CHANGING IN THIS PROJECT.
- NO CONSTRUCTION WITHIN THESE LIMITS WILL BE ALLOWED WITHOUT THE RUNWAY AND/OR TAXIWAY CLOSED.
- AT THE END OF THE WORK DAY NO CONSTRUCTION EQUIPMENT SHALL BE PARKED/STORED WITHIN THE OBJECT FREE AREA(S) OF OPEN OR CLOSED RUNWAYS AND/OR TAXIWAYS. PARKED/STORED EQUIPMENT HEIGHT SHALL NOT PENETRATE PART 77 SURFACE(S).

IL. CONTRACT: PA061
IL. LETTING ITEM: 10A
IL. PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS - SHEET 1

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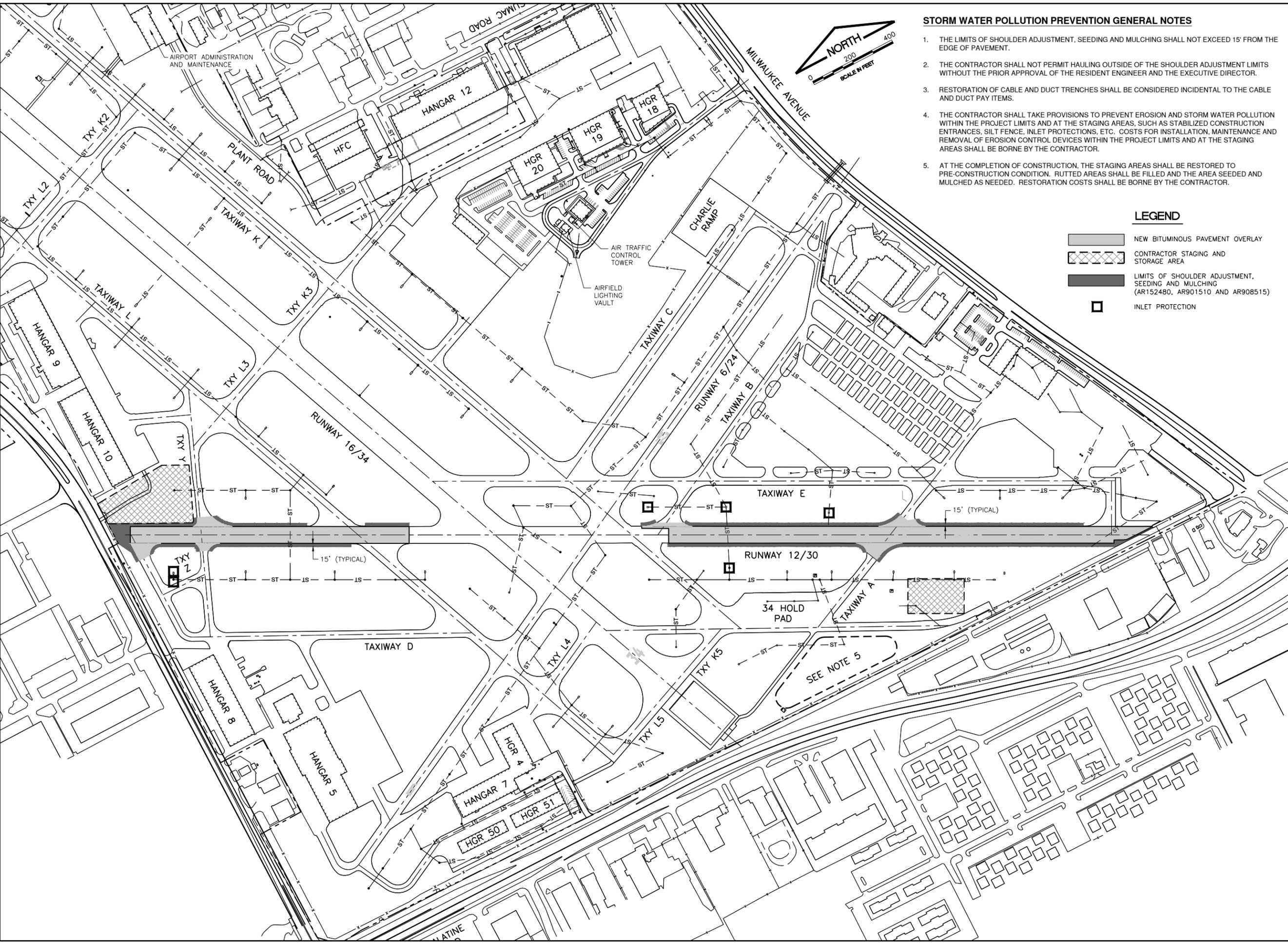
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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: DKP
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: KWS
DATE: 06/08/2018
JOB No: 16290-04-00

FINAL

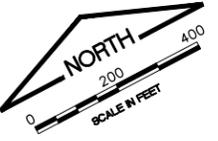
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 REF: DWG: hbrct_runway_12_30_base_NEW.dwg
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 CEA Alignments.dwg



- STORM WATER POLLUTION PREVENTION GENERAL NOTES**
1. THE LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING SHALL NOT EXCEED 15' FROM THE EDGE OF PAVEMENT.
 2. THE CONTRACTOR SHALL NOT PERMIT HAULING OUTSIDE OF THE SHOULDER ADJUSTMENT LIMITS WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND THE EXECUTIVE DIRECTOR.
 3. RESTORATION OF CABLE AND DUCT TRENCHES SHALL BE CONSIDERED INCIDENTAL TO THE CABLE AND DUCT PAY ITEMS.
 4. THE CONTRACTOR SHALL TAKE PROVISIONS TO PREVENT EROSION AND STORM WATER POLLUTION WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS, SUCH AS STABILIZED CONSTRUCTION ENTRANCES, SILT FENCE, INLET PROTECTIONS, ETC. COSTS FOR INSTALLATION, MAINTENANCE AND REMOVAL OF EROSION CONTROL DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREAS SHALL BE BORNE BY THE CONTRACTOR.
 5. AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREAS SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.

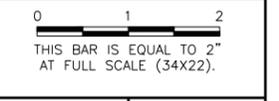
LEGEND

- NEW BITUMINOUS PAVEMENT OVERLAY
- CONTRACTOR STAGING AND STORAGE AREA
- LIMITS OF SHOULDER ADJUSTMENT, SEEDING AND MULCHING (AR152480, AR901510 AND AR908515)
- INLET PROTECTION



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

STORMWATER POLLUTION PREVENTION PLAN

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	DKP
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SHEET 10 OF 37 SHEETS

DATE: Friday, June 8, 2018 4:24:04 PM
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UPDATE BY: Kris Solvatero
LAYOUT: SWPPP NOTES - 1
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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT THE CHICAGO EXECUTIVE AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, ELECTRICAL WORK, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL.

REMOVAL, ADJUSTMENTS AND INSTALLATION OF ELECTRICAL AND MISCELLANEOUS ITEMS.

VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.

TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.

BITUMINOUS PAVEMENT GROOVING.

INSTALLATION OF NEW PAVEMENT MARKING.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 25 ACRES OF WHICH 2 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE DES PLAINES RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCIDENTAL TO THE CONTRACT.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT # _____
DATE ISSUED _____
DATE EXPIRED _____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: CHICAGO EXECUTIVE AIRPORT MARKED: REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
SECTION: 13 PROJECT NUMBER: PWK-4552
COUNTY: COOK CONTRACT NUMBER: 3-17-SBGP-133/139/TBD (PA061)

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____
PRINTED NAME: _____ TITLE: _____
NAME OF FIRM: _____
STREET ADDRESS: _____
CITY, STATE, ZIP: _____
PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

RECORD OF SITE DISTURBANCE AND STABILIZATION

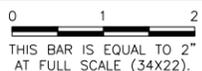
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
CONSTRUCTION CEASED: EXPLANATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.

IL. CONTRACT: **PA061**
IL. LETTING ITEM: **10A**
IL. PROJECT: **PWK-4552**
S.B.G. PROJECT: **3-17-SBGP-133/139/TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS - SHEET 1

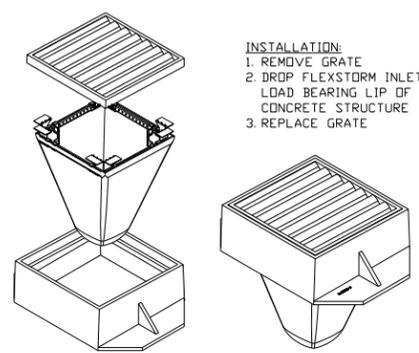
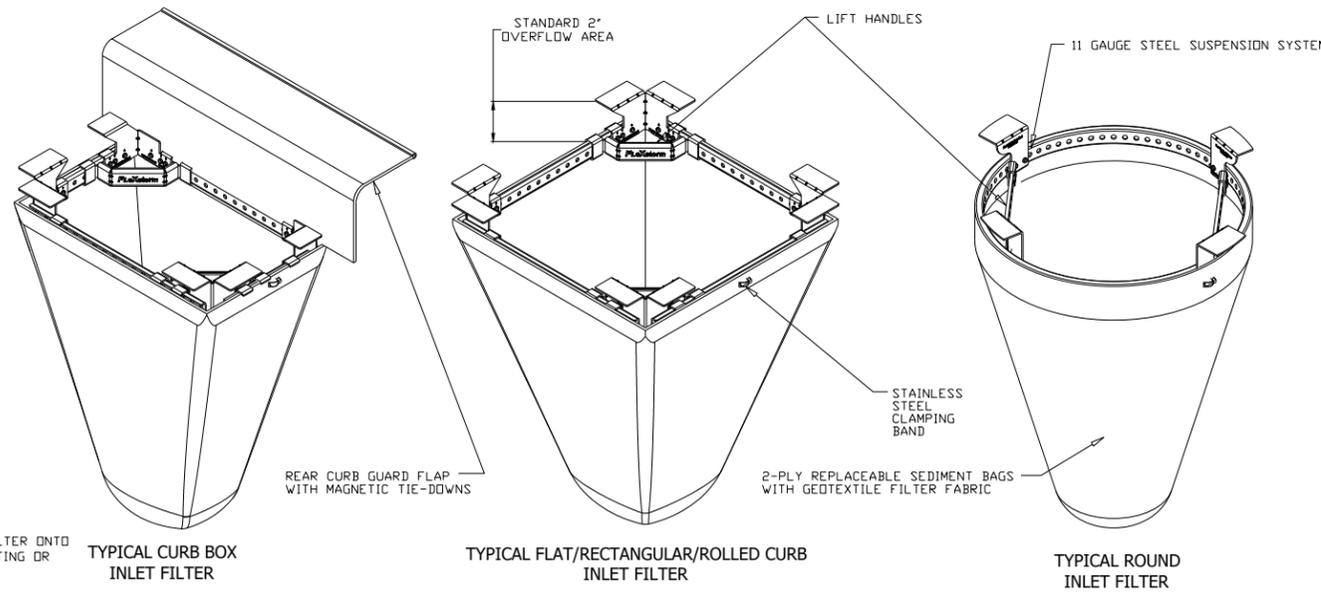
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CHICAGO EXECUTIVE AIRPORT

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DATE: **06/08/2018**
JOB No: **16290-04-00**

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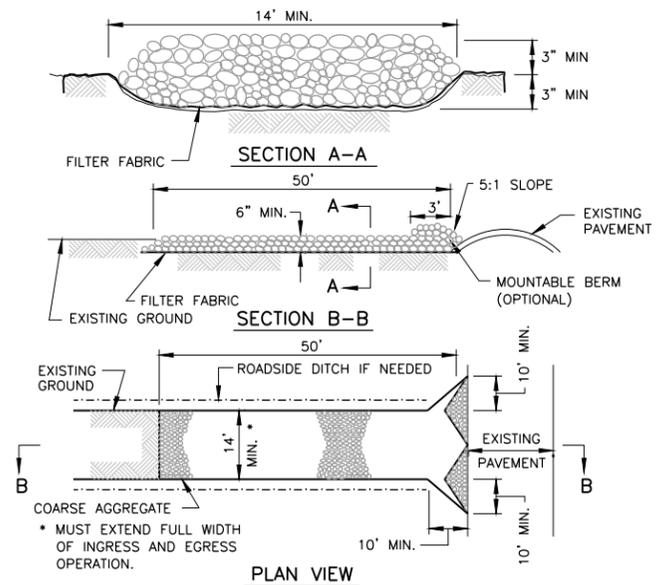
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 DATE: Friday, June 8, 2018 4:24:12 PM
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Inlet Filter Specifications			
Material Property	Test Method	Value (min ave)	
> Inner Filter Bag Specs (2 ft³ min vol)			
Grab Tensile	ASTM D 4632	100 lbs	200 lbs
Puncture Strength	ASTM D 4833	65 lbs	90 lbs
Trapezoidal Tear	ASTM D 4533	45 lbs	75 lbs
UV Resistance	ASTM D 4355	70% at 500 hrs	90%
App Open Size (AOS)	ASTM D 4751	70 sieve (.212 mm)	40 sieve (.425 mm)
Permittivity	ASTM D 4491	2.0 /sec	2.1/sec
Water Flow Rate	ASTM D 4491	145 gpm/sqft	145 gpm/sqft
> Polyester Outer Reinforcement Bag Specifications			
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%	
Thickness	ASTM D 1777	.040 +/- .005	
> Frame Construction			
A36 Structural Steel; 11 Gauge; Zinc Plated	ASTM A 576	Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi	



INLET PROTECTION / SEDIMENT TRAP
NOT TO SCALE

1. STORM SEWER INLET PROTECTION SHALL BE INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL

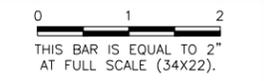


1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. STABILIZED CONSTRUCTION ENTRANCE(S) SHALL BE INCIDENTAL TO THE CONTRACT.

STABILIZED CONSTRUCTION ENTRANCE
FROM NRCS STANDARD DRAWING NO. IL-630 (MODIFIED)

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS - SHEET 2

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: DKP
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: KWS
 DATE: 06/08/2018
 JOB No: 16290-04-00

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SHEET 12 OF 37 SHEETS

ROLLED EROSION CONTROL PRODUCTS

Staking Pattern Guide

Stake within 2" of the end of wattle

2" or less

6" Min.

Wood Stake

Soil

Notes:

1. Overlap minimum is the diameter of the roll.
2. 4" spacing for wattles.
3. 2" spacing for rolled excelsior.
4. Or space according to manufacturer's specifications.

Stake Detail

3' Deep Trench

Flow

Channel Bottom

Wood Stake (to only penetrate netting)

Soil

Compost Filter Sock Detail

1"x2" Wood Stake (or equal)

Place additional Compost Filter Material to fill the seam between the tube and the ground.

Flow

Soil

When compost filter sock ditch check is used, place a compost berm upstream of the filter sock (see IUM 805). A trench is not required.

Notes:

1. Drawings are not to scale.
2. Ends of wattles or rolled excelsior shall be turned at least 6" upslope.
3. Recommended stakes are 1 1/8" wide x 1 1/8" thick x 30" long.
4. Stakes shall not extend above the straw wattle more than 2".
5. Spacing the toe of the upstream ditch check shall create a horizontal line with the top of the downstream ditch check.
6. When compost filter sock ditch check is used, place a compost berm upstream of the filter sock (see IUM 805). A trench is not required.

REFERENCE Project	DESIGNED Date	DESIGNED Date	DESIGNED Date
CHECKED Date	CHECKED Date	CHECKED Date	CHECKED Date
APPROVED Date	APPROVED Date	APPROVED Date	APPROVED Date

STANDARD DWG. NO.
IUM-514
 SHEET 1 OF 1
 DATE 8-19-11

SILT FENCE PLAN

ELEVATION

Filter Fabric

Fastener - Min. No. 10 Gage Wire
4 Per Post Required. (Typ.)

5' Max (Typ)

2' Min

18" Min (Typ)

FABRIC ANCHOR DETAIL

Filter Fabric

Direction of Flow

Undisturbed Ground Line

6" Min

6" Min

Compacted Backfill

NOTES:

1. Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
2. Filter fabric shall meet the requirements of material specification 592 Geotextile Table 1 or 2, Class I with equivalent opening size of at least 30 for nonwoven and 40 for woven, and Item 156 specifications
3. Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

REFERENCE Project	DESIGNED Date	DESIGNED Date	DESIGNED Date
CHECKED Date	CHECKED Date	CHECKED Date	CHECKED Date
APPROVED Date	APPROVED Date	APPROVED Date	APPROVED Date

STANDARD DWG. NO.
IUM-620A
 SHEET 1 OF 2
 DATE 3-16-12

SILT FENCE - SPLICING TWO FENCES

ATTACHING TWO SILT FENCES

Step 1

Step 2

Step 3

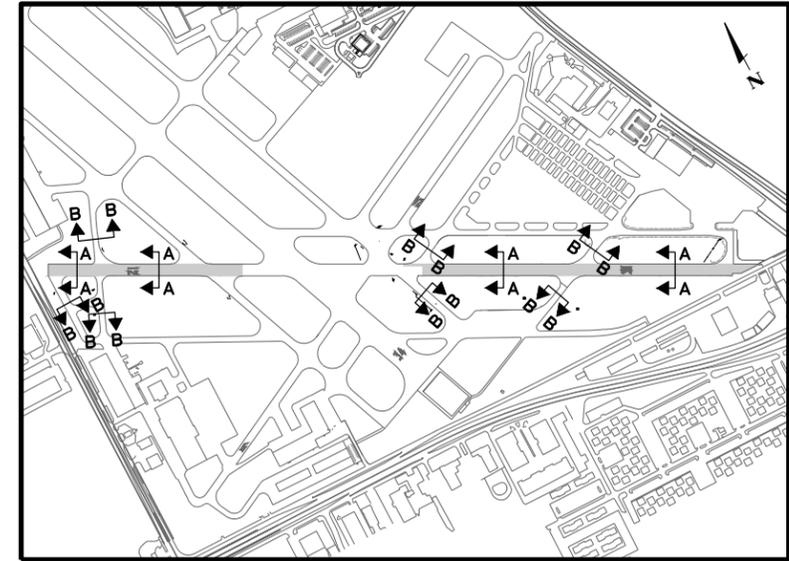
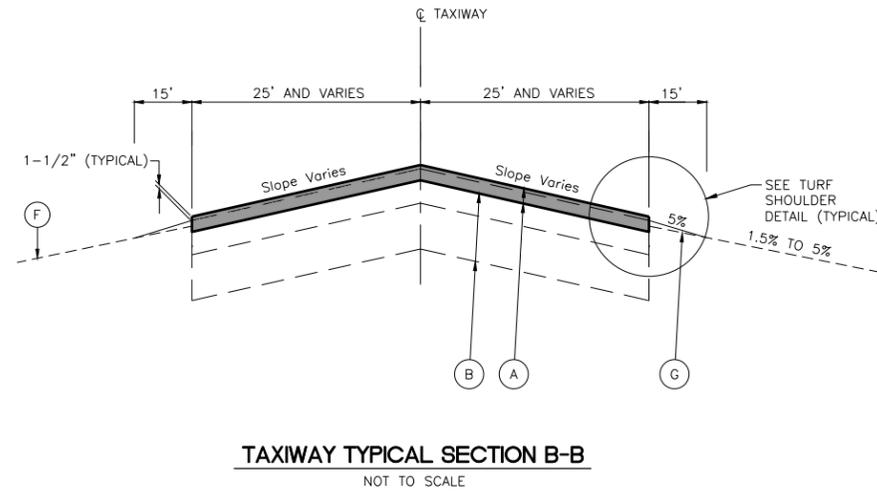
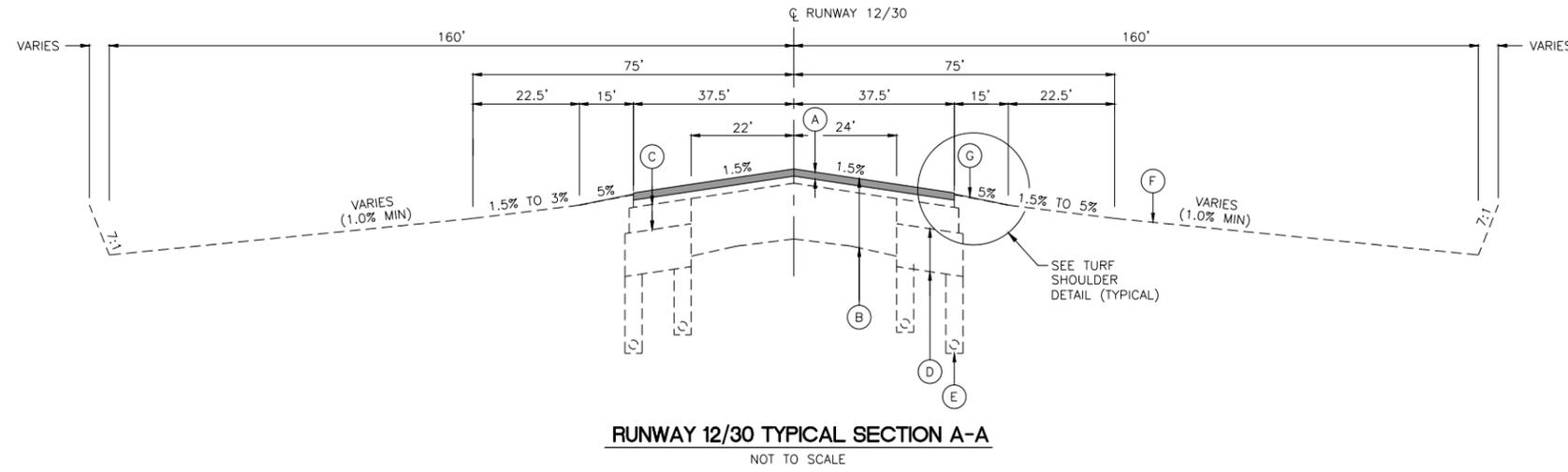
1. Place the end post of the second fence inside the end post of the first fence.
2. Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
3. Cut the fabric near the bottom of the stakes to accommodate the 6" flap.
4. Drive both posts a minimum of 18 inches into the ground and bury the flap.
5. Compact backfill (particularly at splices) completely to prevent stormwater piping.

REFERENCE Project	DESIGNED Date	DESIGNED Date	DESIGNED Date
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APPROVED Date	APPROVED Date	APPROVED Date	APPROVED Date

STANDARD DWG. NO.
IUM-620B(W)
 SHEET 1 OF 1
 DATE 3-16-2012

PROJECT SITE AREA AND STAGING AREA PROTECTION NOTES:

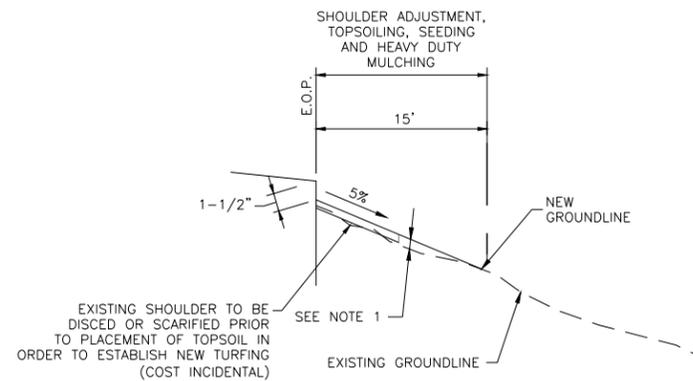
1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE PROJECT SITE AREA AND STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.



KEY MAP

LEGEND

- (A) NEW VARIABLE DEPTH BITUMINOUS SURFACE MILLING
 NEW 2" BITUMINOUS SURFACE COURSE (401)
 NEW BITUMINOUS SURFACE COURSE LEVELING (AS REQUIRED)(401)
 NEW BITUMINOUS TACK COAT (603)
- (B) EXISTING BITUMINOUS PAVEMENT (DEPTH VARIES)
- (C) EXISTING 7" CRUSHED AGGREGATE BASE COURSE
- (D) EXISTING 12" POROUS GRANULAR EMBANKMENT
- (E) EXISTING UNDERDRAIN
- (F) EXISTING GROUNDLINE
- (G) SHOULDER ADJUSTMENT, TOPSOILING, SEEDING
 AND HEAVY DUTY MULCHING



TURF SHOULDER DETAIL
 NOT TO SCALE

NOTES:

1. TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480)
2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED.

IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

TYPICAL SECTIONS

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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
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JOB No:	16290-04-00

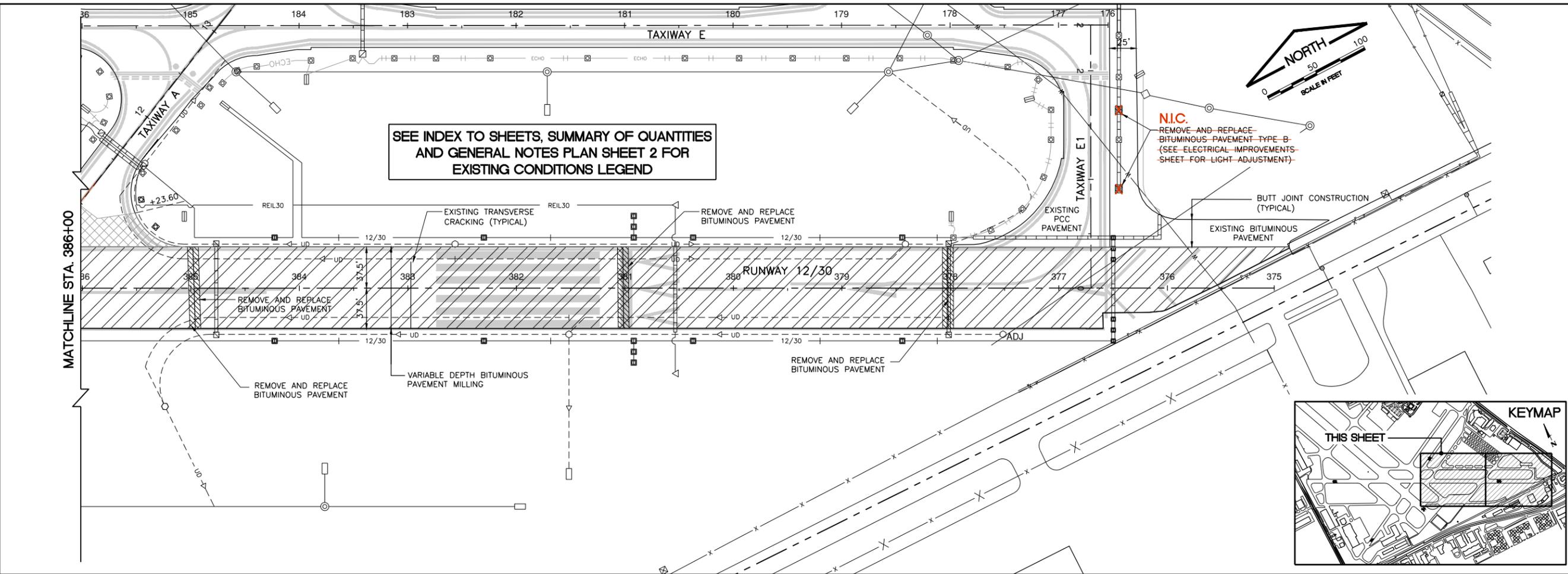
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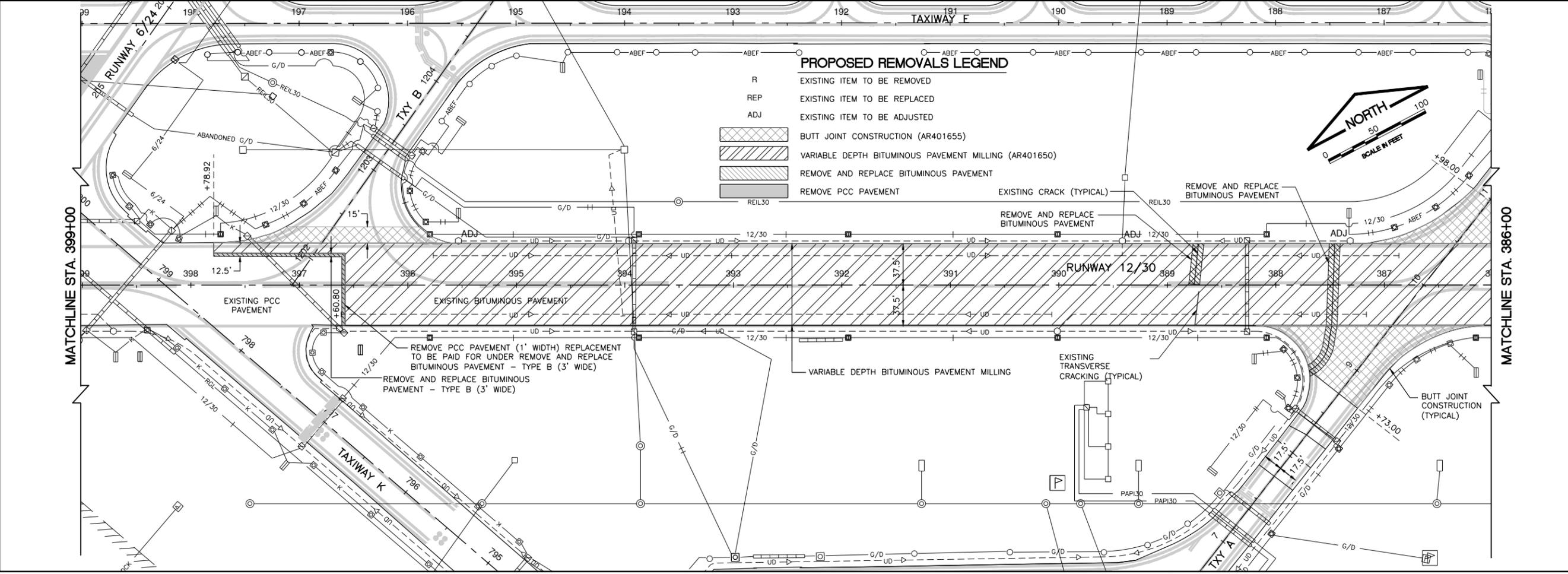
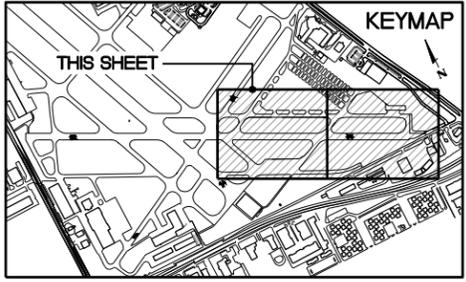
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SEE INDEX TO SHEETS, SUMMARY OF QUANTITIES
 AND GENERAL NOTES PLAN SHEET 2 FOR
 EXISTING CONDITIONS LEGEND



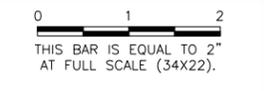
PROPOSED REMOVALS LEGEND

R	EXISTING ITEM TO BE REMOVED
REP	EXISTING ITEM TO BE REPLACED
ADJ	EXISTING ITEM TO BE ADJUSTED
	BUTT JOINT CONSTRUCTION (AR401655)
	VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING (AR401650)
	REMOVE AND REPLACE BITUMINOUS PAVEMENT
	REMOVE PCC PAVEMENT

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
**EXISTING CONDITIONS AND PROPOSED
 REMOVALS - SHEET 1**

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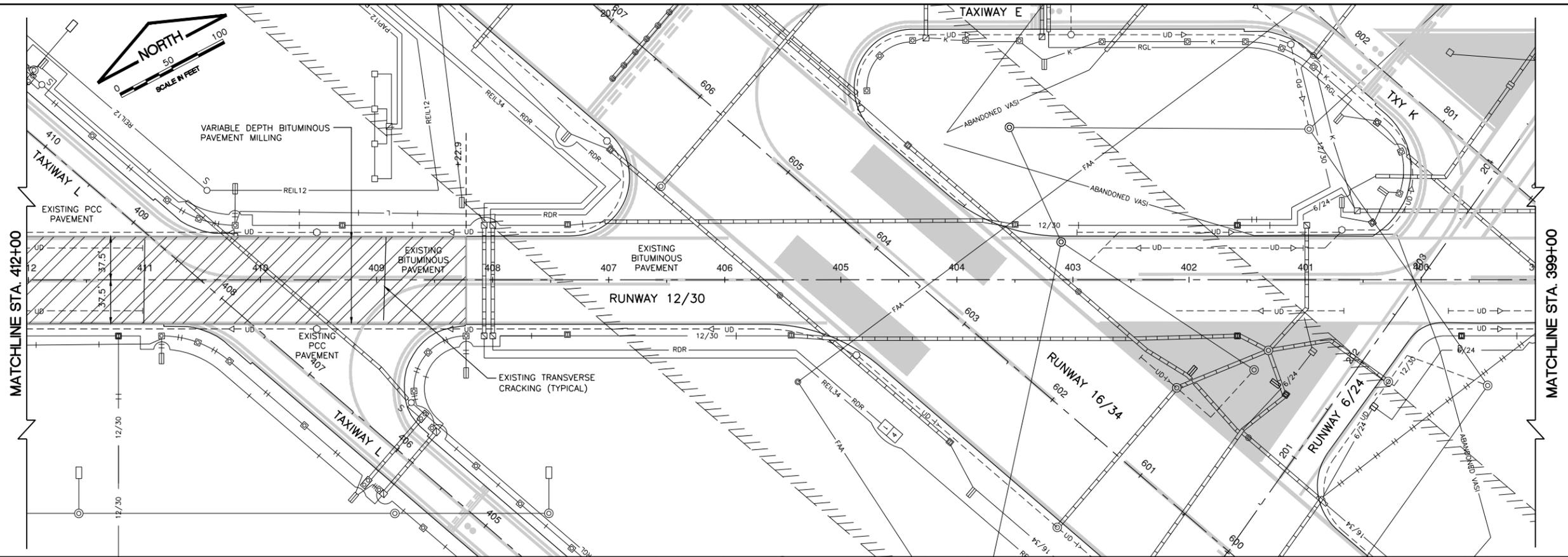
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 SHEET 14 OF 37 SHEETS

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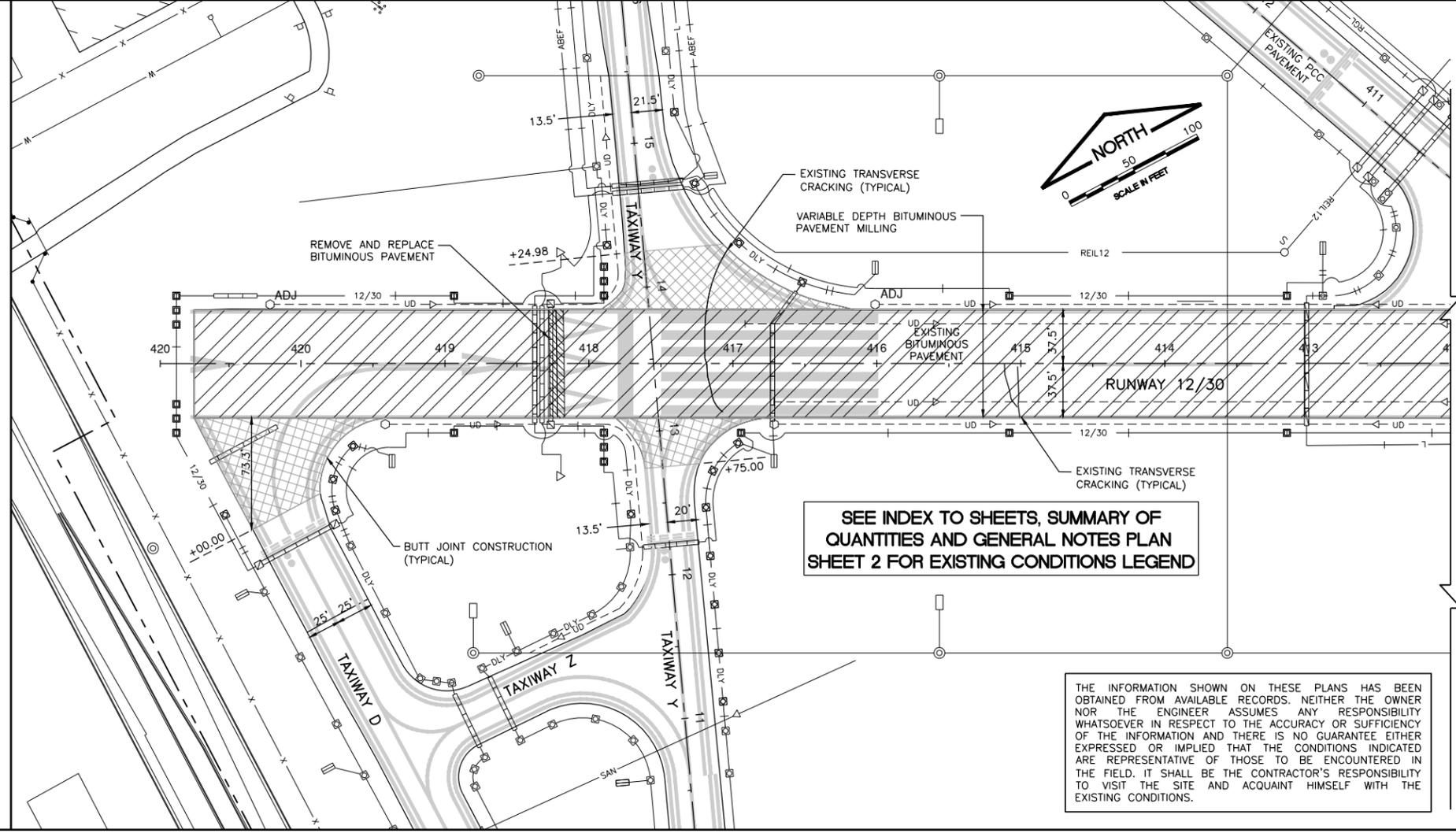
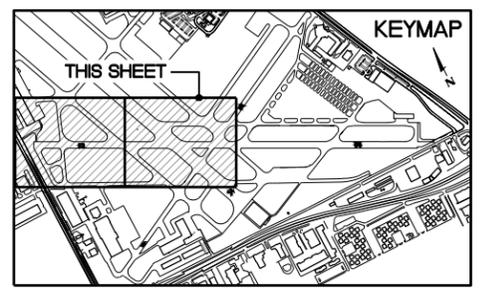


PROPOSED REMOVALS LEGEND

- R EXISTING ITEM TO BE REMOVED
- REP EXISTING ITEM TO BE REPLACED
- ADJ EXISTING ITEM BE ADJUSTED
- [Hatched Box] BUTT JOINT CONSTRUCTION (AR401655)
- [Diagonal Hatched Box] VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING (AR401650)
- [Cross-hatched Box] REMOVE AND REPLACE BITUMINOUS PAVEMENT
- [Solid Grey Box] REMOVE PCC PAVEMENT

NOTES

- PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS FOUND TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR. OTHER CRACKS AS NOTED DURING THE INSPECTION SHALL BE SEALED UNDER ITEM AR201661, CLEAN AND SEAL BITUMINOUS CRACKS
- THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.
- EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- DRAINAGE COLLECTION STRUCTURES TO BE ADJUSTED SHALL BE DETERMINED IN THE FIELD AND THE NEW RIM ELEVATION SHALL MATCH THE NEW GRADING ELEVATIONS.
- REFER TO SHEET "BITUMINOUS MILLING DEPTH TABLE - RUNWAY 12/30" FOR LOCATIONS WHERE BITUMINOUS MILLING MAY NOT BE REQUIRED FOR SURFACE LEVELING AREAS.

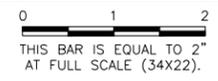


SEE INDEX TO SHEETS, SUMMARY OF QUANTITIES AND GENERAL NOTES PLAN SHEET 2 FOR EXISTING CONDITIONS LEGEND

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 2

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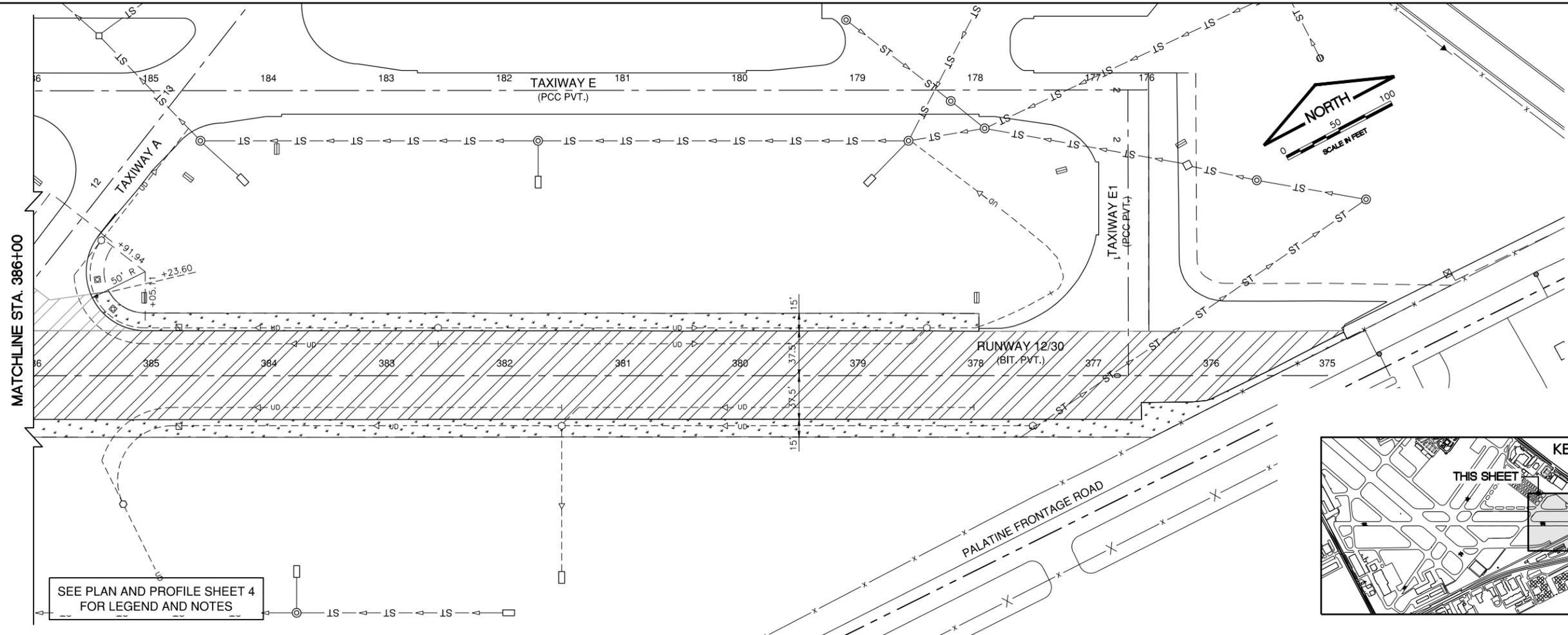
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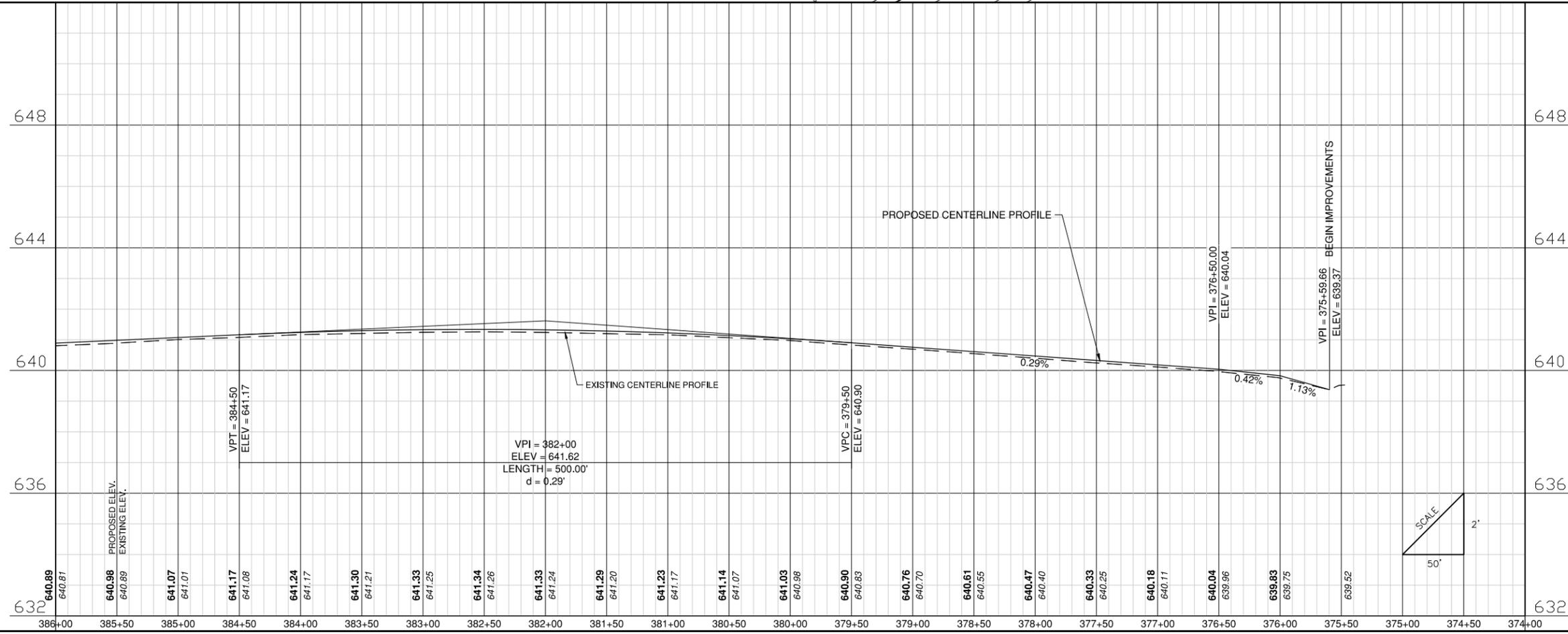
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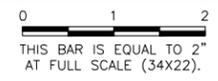


SEE PLAN AND PROFILE SHEET 4
 FOR LEGEND AND NOTES



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
RUNWAY 12/30 PLAN AND PROFILE - SHEET 1

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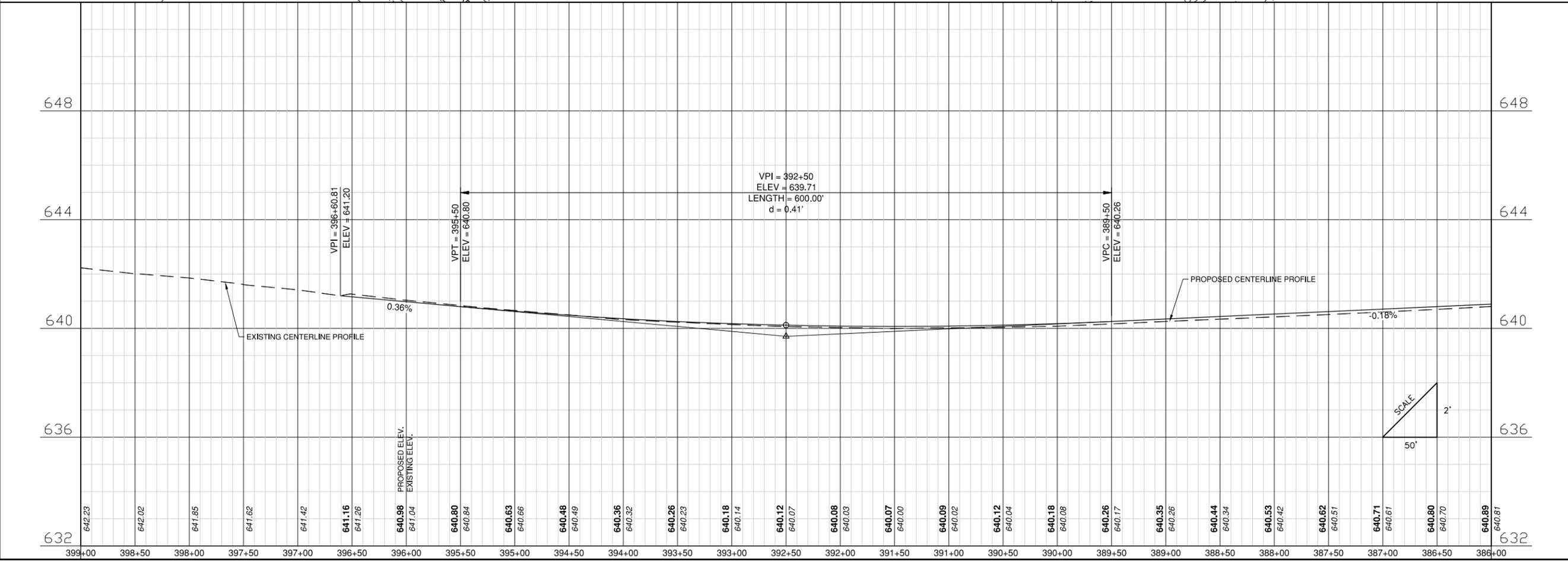
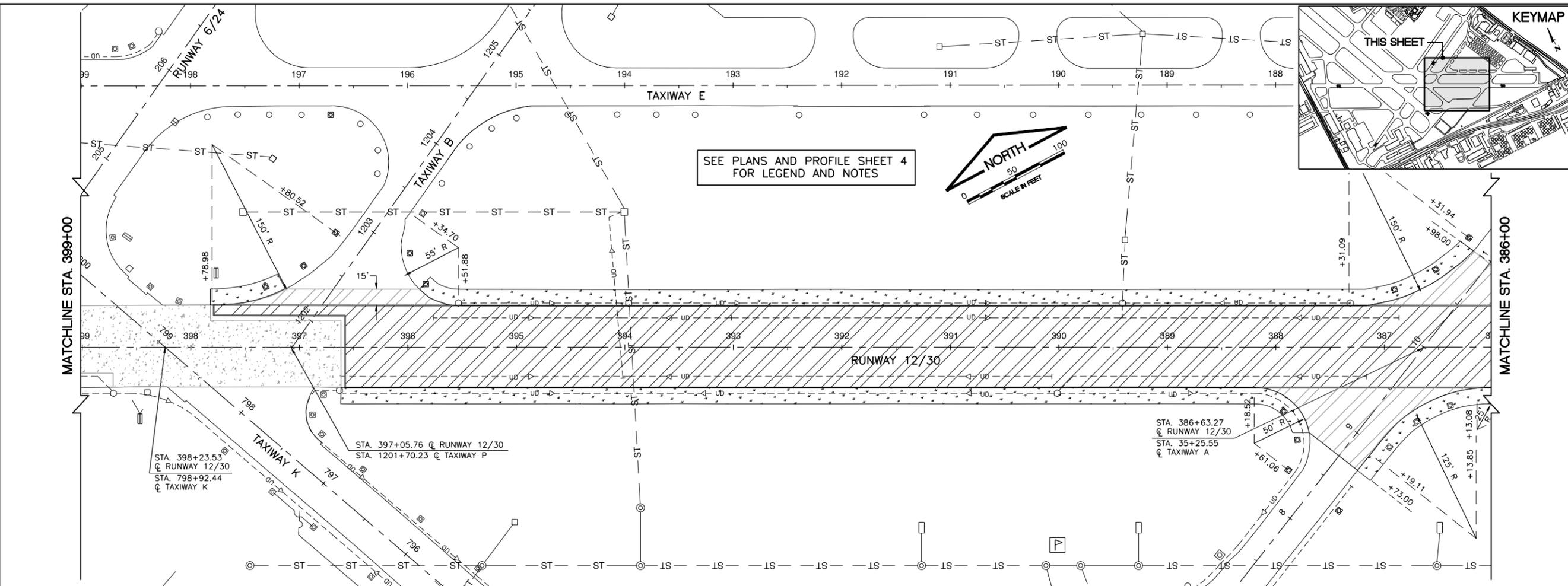
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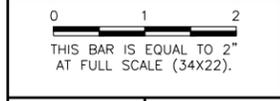
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IL CONTRACT: PA061
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CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

RUNWAY 12/30 PLAN AND PROFILE - SHEET 2

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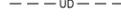
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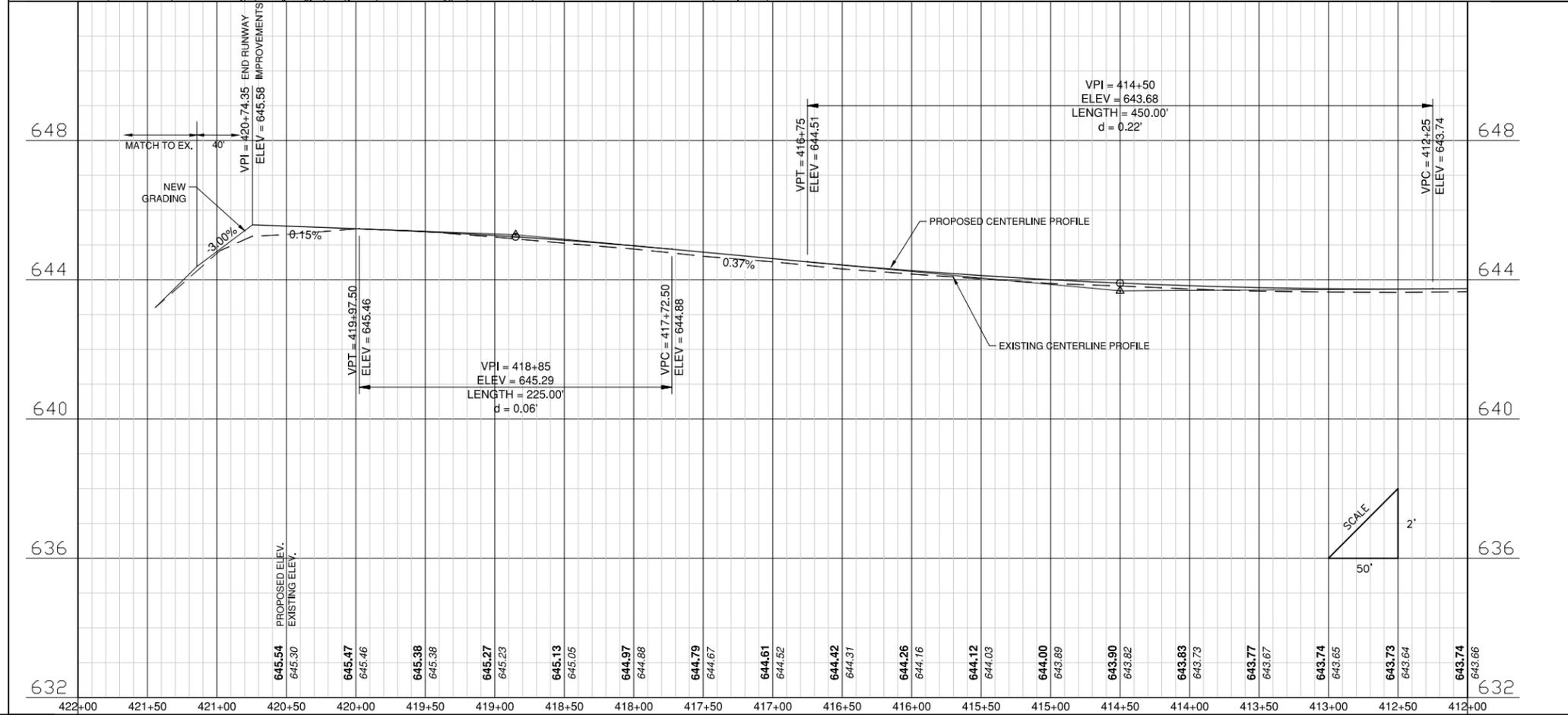
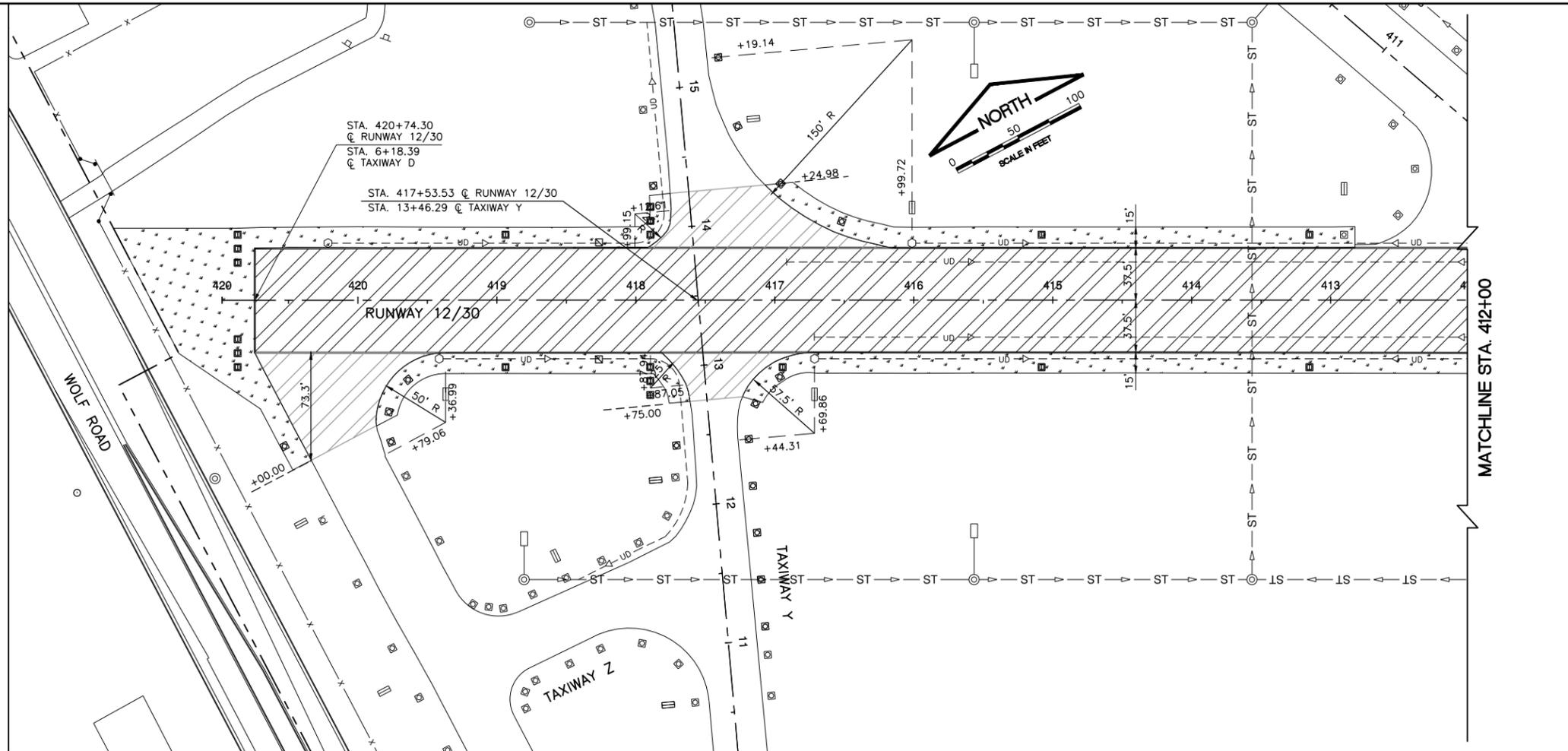
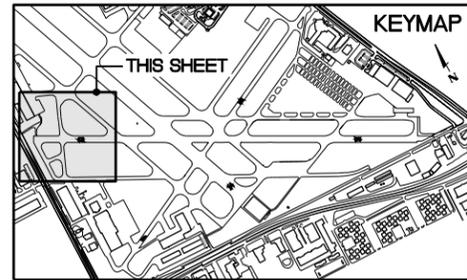
SHEET 17 OF 37 SHEETS

LEGEND

-  NEW 2" BITUMINOUS OVERLAY
-  NEW SHOULDER ADJUSTMENT, SEEDING AND MULCHING
-  CLEAN AND SEAL PCC JOINTS
-  EXISTING STORM SEWER
-  EXISTING INLET/MANHOLE/SLOPE BOX
-  EXISTING UNDERDRAIN
-  EXISTING UNDERDRAIN COLLECTION STRUCTURE
-  EXISTING FENCE
-  EXISTING ELECTRIC HANDHOLE

NOTES

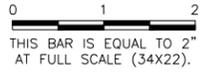
1. IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING**

RUNWAY 12/30 PLAN AND PROFILE - SHEET 4

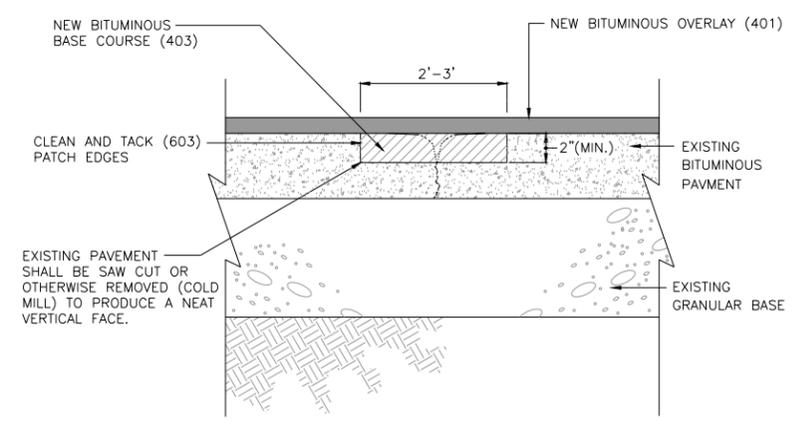
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 JOB No: 16290-04-00

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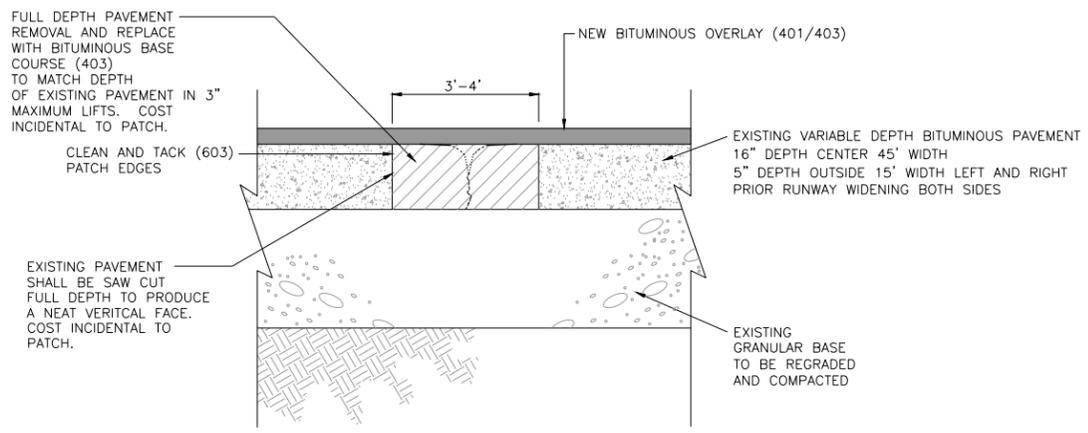
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REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A

NOT TO SCALE

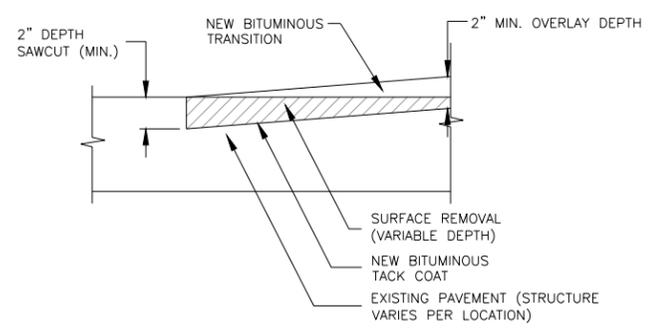


REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B

NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. THIS WORK SHALL BE DETERMINED IN THE FIELD WITH THE AIRPORT/ENGINEER AFTER INSPECTION OF THE MILLED SURFACE..

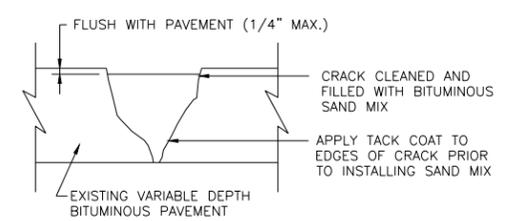


BUTT JOINT DETAIL

NOT TO SCALE

NOTE

1. BUTT JOINT PAID FOR AS BITUMINOUS BUTT JOINT CONSTRUCTION (AR401655).

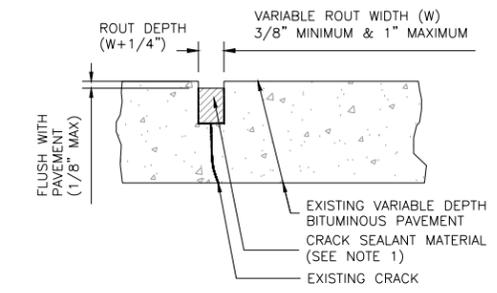


HMA SAND MIX CRACK REPAIR DETAIL

NOT TO SCALE

CRACK REPAIR NOTES:

1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
3. CONTRACTOR SHALL ROUTE, CRACK FLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.



CLEAN AND SEAL BITUMINOUS CRACKS (AR201661)

NOT TO SCALE

CRACK SEALING NOTES:

1. THE CRACK ROUTING DIMENSIONS WILL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL.
2. CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.

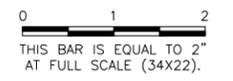
NOTES

1. AFTER MILLING IS COMPLETE, THE AREA SHALL BE THOROUGHLY CLEANED. ALL CRACKS LESS THAN 1" WIDE SHALL BE CLEANED AND SEALED. CRACKS LARGER THAN 1" WIDE SHALL BE CLEANED AND REPAIRED WITH SAND MIX.

IL. CONTRACT: **PA061**
 IL. LETTING ITEM: **10A**
 IL. PROJECT: **PWK-4552**
 S.B.G. PROJECT: **3-17-SBGP-133/139/TBD**

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
PAVEMENT REHABILITATION DETAILS

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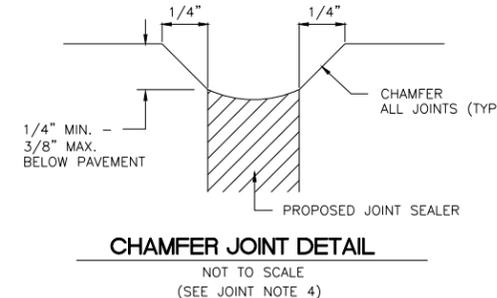
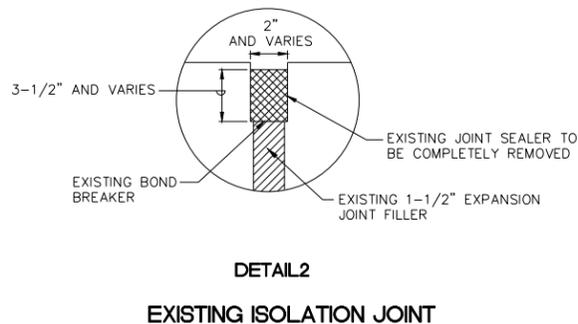
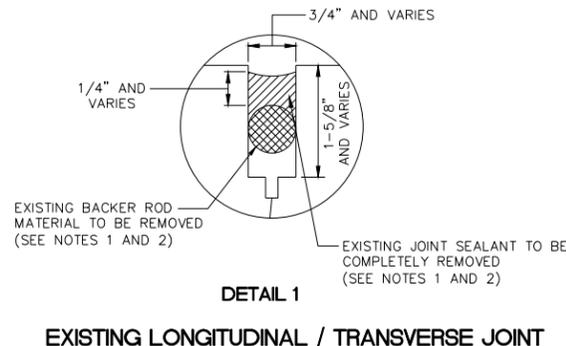
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

DATE: Friday, June 8, 2018 4:28:00 PM
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 UPDATE BY: Kris Solvatero
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 CEA Alignments.dwg

NOTES

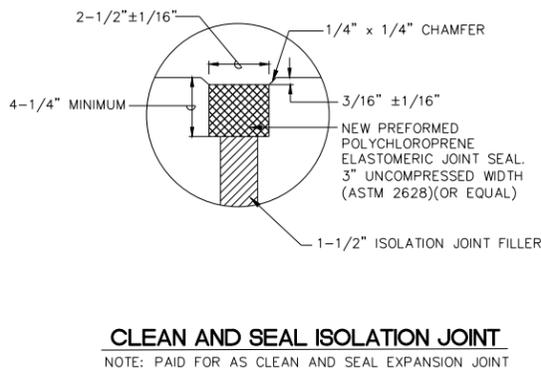
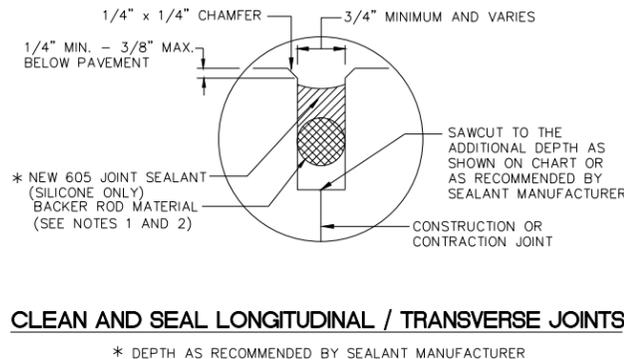
- THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT EXECUTIVE DIRECTOR.
- PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.
- FOR VARYING JOINT WIDTHS AND DEPTHS, THERE SHALL BE NO ADDITIONAL COST FOR DEEPER SAWCUTS, LARGER BACKER RODS AND SEALANT MATERIAL USED.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST. CHAMFERS ARE NOT NEEDED WHERE CHAMFER IS NOT CONSTRUCTIBLE AT SMALLER SPALLING AREAS.



CLEAN AND SEAL JOINT CHART

EXISTING JOINT WIDTH	SUGGESTED BACKER ROD DIAMETER (SEE NOTE A)	SUGGESTED JOINT TOTAL DEPTH
1/2"	5/8"	1"-5/8"
5/8"	3/4"	1"-7/8"
3/4"	7/8"	2"-1/8"
7/8"	1"	2"-1/4"
1"	1"-1/4"	2"-7/8"
1"-1/8"	1"-1/2"	3"-3/8"
1"-1/4"	1"-1/2"	3"-1/2"

NOTE A: IF LARGER SIZE IS USED, CONTRACTOR SHALL INCREASE SAW DEPTH (JOINT TOTAL DEPTH) AS REQUIRED AT NO ADDITIONAL COST TO THE CONTRACT.



	DETAIL 1 AND 2	
	HOT/GOLD POUR	SILICONE
W=WIDTH OF SEALANT RESERVOIR (IN.)	1/2	3/8
D=DEPTH OF SEALANT RESERVOIR (IN.)	1/2	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	3/4	1/2

NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

LEGEND

- TYPE B - ISOLATION JOINT (SEE DETAIL 2)
- TYPE E - HINGED CONSTRUCTION JOINT (KEYED)(SEE DETAIL 1)
- TYPE F - DOWELED CONTRACTION JOINT (SEE DETAIL 1)
- TYPE G - HINGED CONTRACTION JOINT (SEE DETAIL 1)
- TYPE H - DUMMY CONTRACTION JOINT (SEE DETAIL 1)
- LIMITS OF WORK (CLEAN AND SEAL JOINTS)

N.I.C. NEW 10\"/>

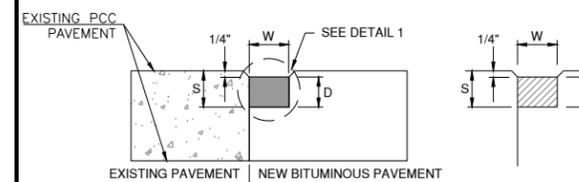
CLEAN AND SEAL ISOLATION JOINT (TYPICAL)

N.I.C. NEW PCC SIGN PAD (ADDITIVE ALTERNATE NO. 1)

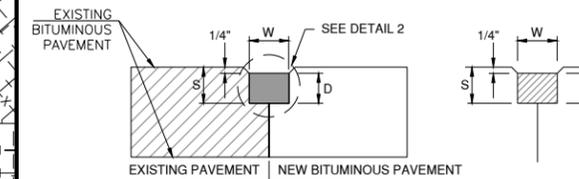
NEW CONCRETE/BITUMINOUS JOINT SEALING (TYPICAL)

NOTES

- SEE SHEET "MISCELLANEOUS DETAILS" FOR ADDITIONAL JOINTING DETAILS.



CONCRETE / BITUMINOUS INTERFACE JOINT SEALING NOT TO SCALE



BITUMINOUS / BITUMINOUS INTERFACE JOINT SEALING NOT TO SCALE

NOTES

- THE JOINT BETWEEN NEW AND EXISTING BITUMINOUS PAVEMENT SHALL BE SAWED AND SEALED (PROJECT LIMITS), PAID FOR AS CLEAN AND SEAL JOINTS.
- THE JOINT BETWEEN NEW BITUMINOUS AND EXISTING PCC PAVEMENTS SHALL BE SAWED AND SEALED, PAID FOR AS CLEAN AND SEAL JOINTS.

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

PAVEMENT JOINT SEALING DETAILS

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: KWS
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 06/08/2018
 JOB No: 16290-04-00

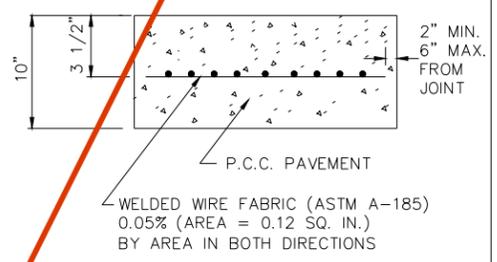
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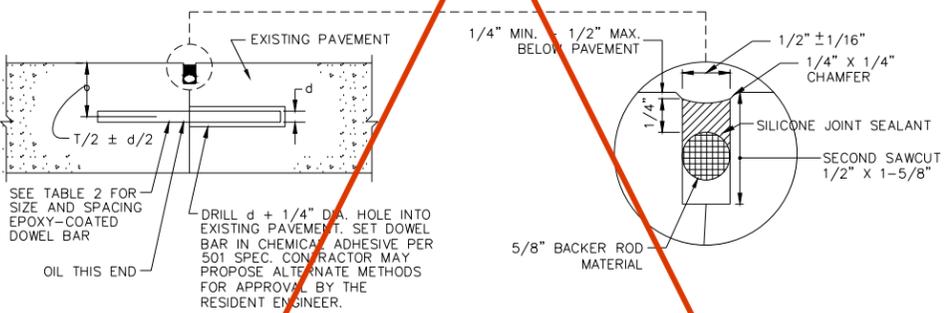
JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEAMANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR, TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A706, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RE-STRAIGHTENED DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A706 CAN BE USED FOR CONSTRUCTION REQUIRING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCLUDED IN THE ASSOCIATED PCC PAVEMENT PAY ITEM.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING DOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST.

N.I.C.



ODD SHAPED PANEL REINFORCEMENT



TYPE D DOWELED CONSTRUCTION JOINTS

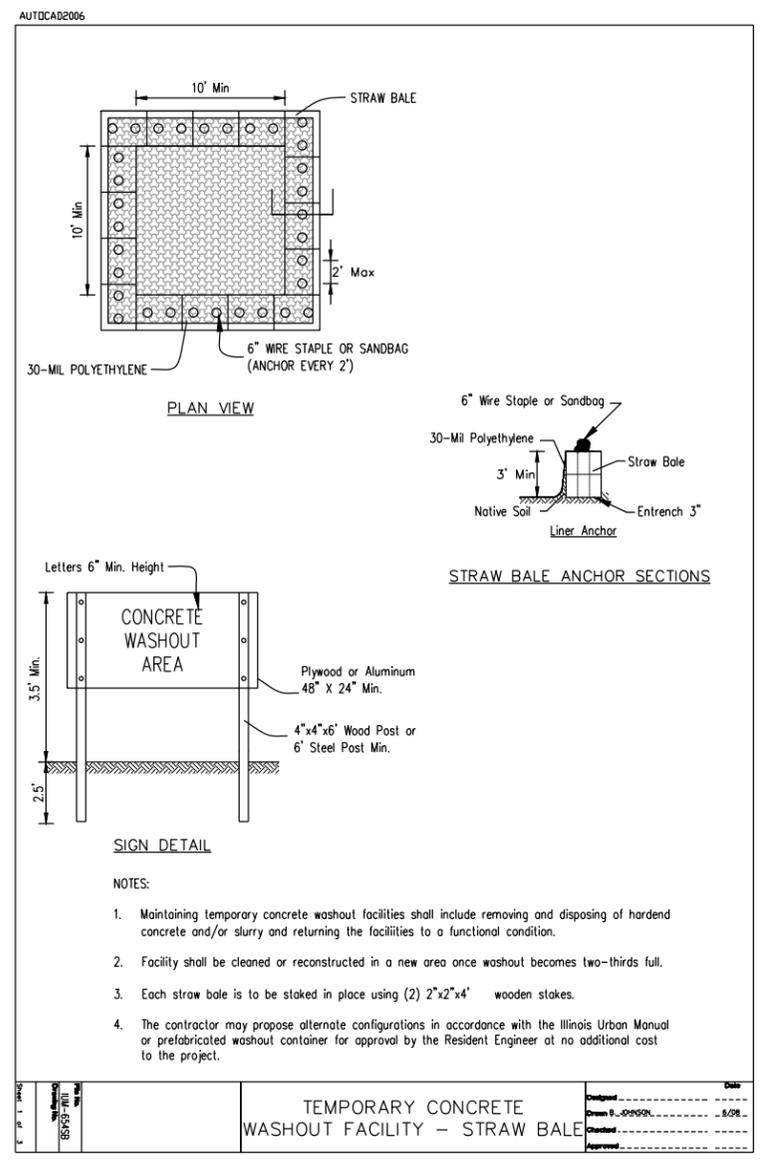
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ± 1/4"
10	2.5"

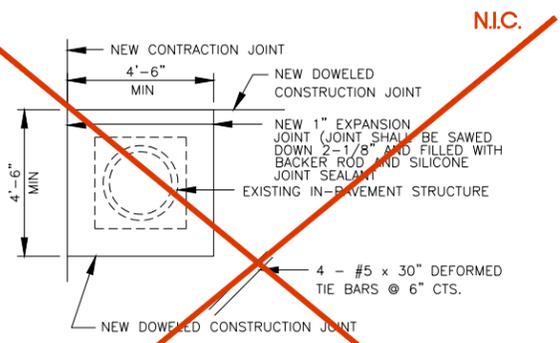
TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
10	1"	19"	12"	#5	30"	30"

DIMENSION TABLES



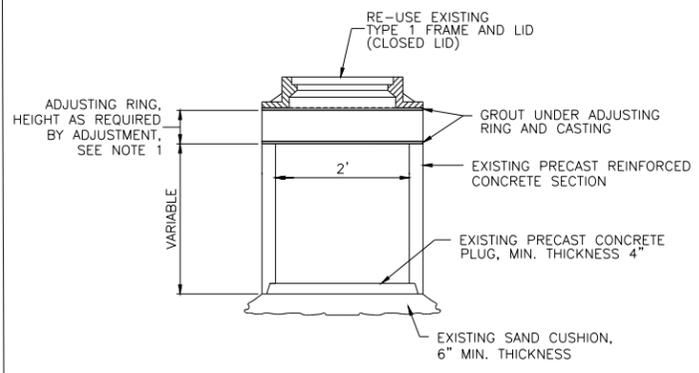
NOTE: TEMPORARY CONCRETE WASHOUT FACILITY IS INCIDENTAL TO THE CONTRACT.



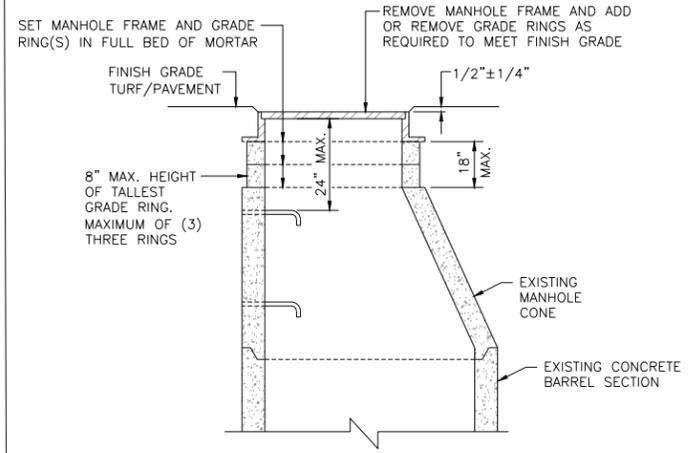
PAVEMENT BLOCKOUT DETAIL FOR EXISTING IN-PAVEMENT STRUCTURES

NOT TO SCALE
 ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING IN-PAVEMENT STRUCTURES CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIELD CONDITIONS.

N.I.C.



ADJUST UNDERDRAIN COLLECTION STRUCTURE



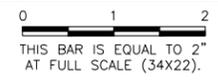
ADJUST MANHOLE (DRAINAGE OR ELECTRICAL)

- STRUCTURE ADJUSTMENT NOTES:**
- CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
 - THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
 - THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
 - ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
 - CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
 - TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
 - AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
 - EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.

IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

MISCELLANEOUS DETAILS

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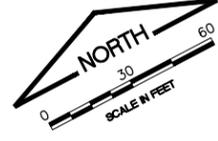
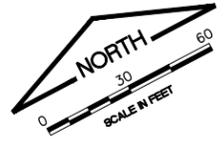
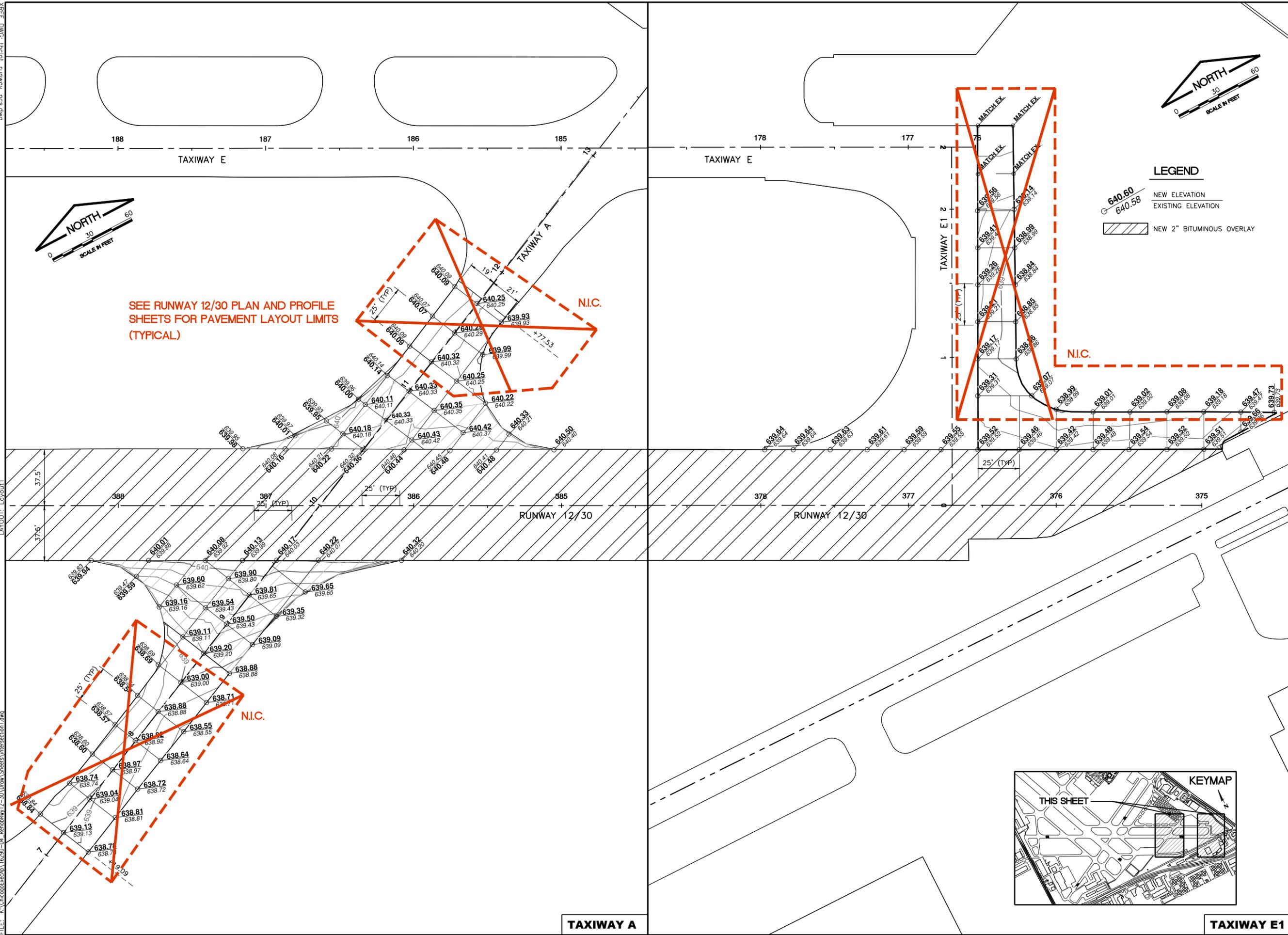
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

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UPDATE BY: Kris Solvatera
 LAYOUT: Layout1

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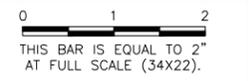
SEE RUNWAY 12/30 PLAN AND PROFILE SHEETS FOR PAVEMENT LAYOUT LIMITS (TYPICAL)

LEGEND

- 640.60 NEW ELEVATION
- 640.58 EXISTING ELEVATION
- [Hatched Box] NEW 2" BITUMINOUS OVERLAY

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
 INTERSECTION GRADING PLAN
 TAXIWAYS A AND E1

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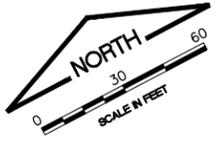
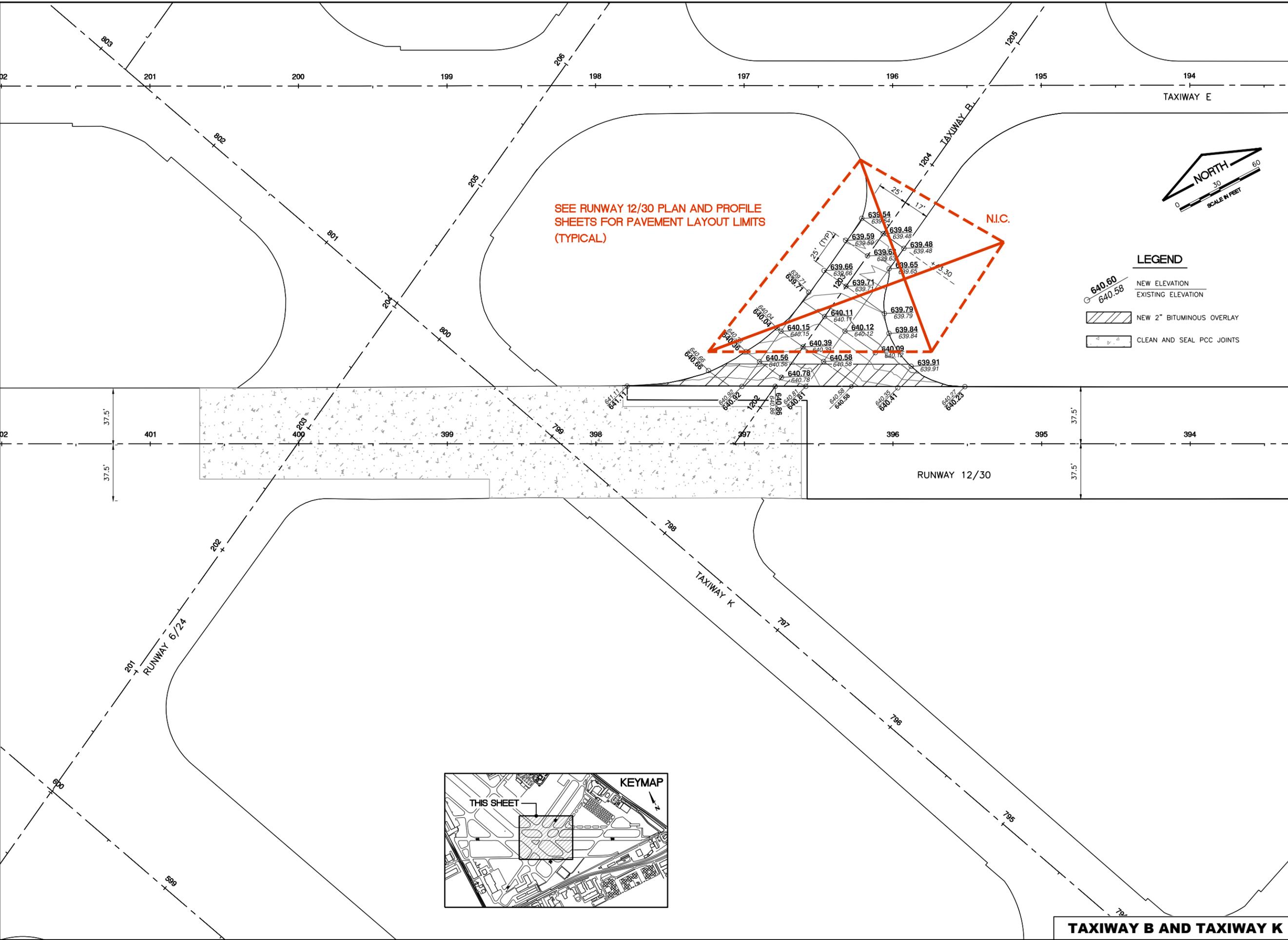
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DRAWN BY:	JRO
CHECKED BY:	DKP
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JOB No:	16290-04-00

FINAL

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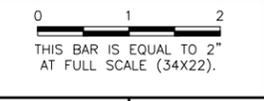
UPDATE BY: Kris Solvatero
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IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

INTERSECTION GRADING PLAN
 TAXIWAYS B AND K

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CHICAGO EXECUTIVE AIRPORT

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DATE:	06/08/2018
JOB No:	16290-04-00

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SHEET 24 OF 37 SHEETS

TAXIWAY B AND TAXIWAY K

DATE: Friday, June 8, 2018 4:29:34 PM
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 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
 IMAGE FILES: bwknewLOGO_CEA-4-Color-Small (2).jpg
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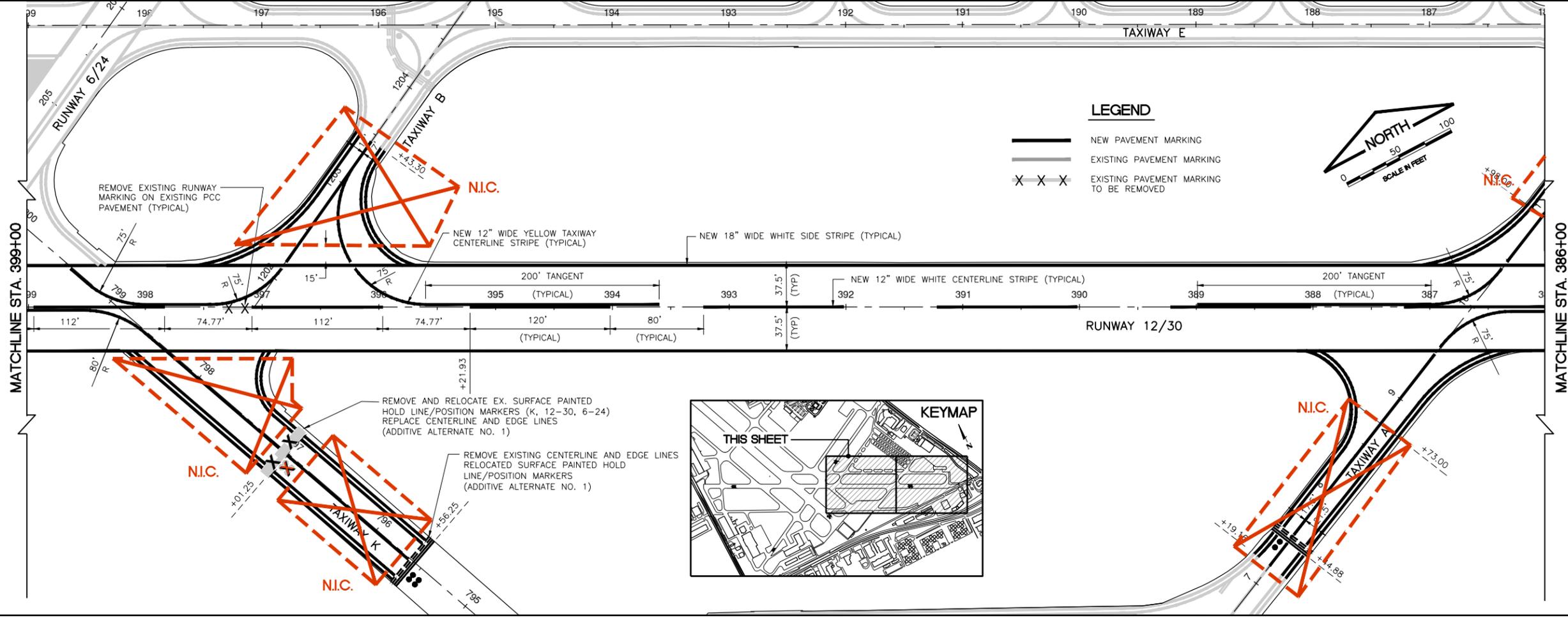
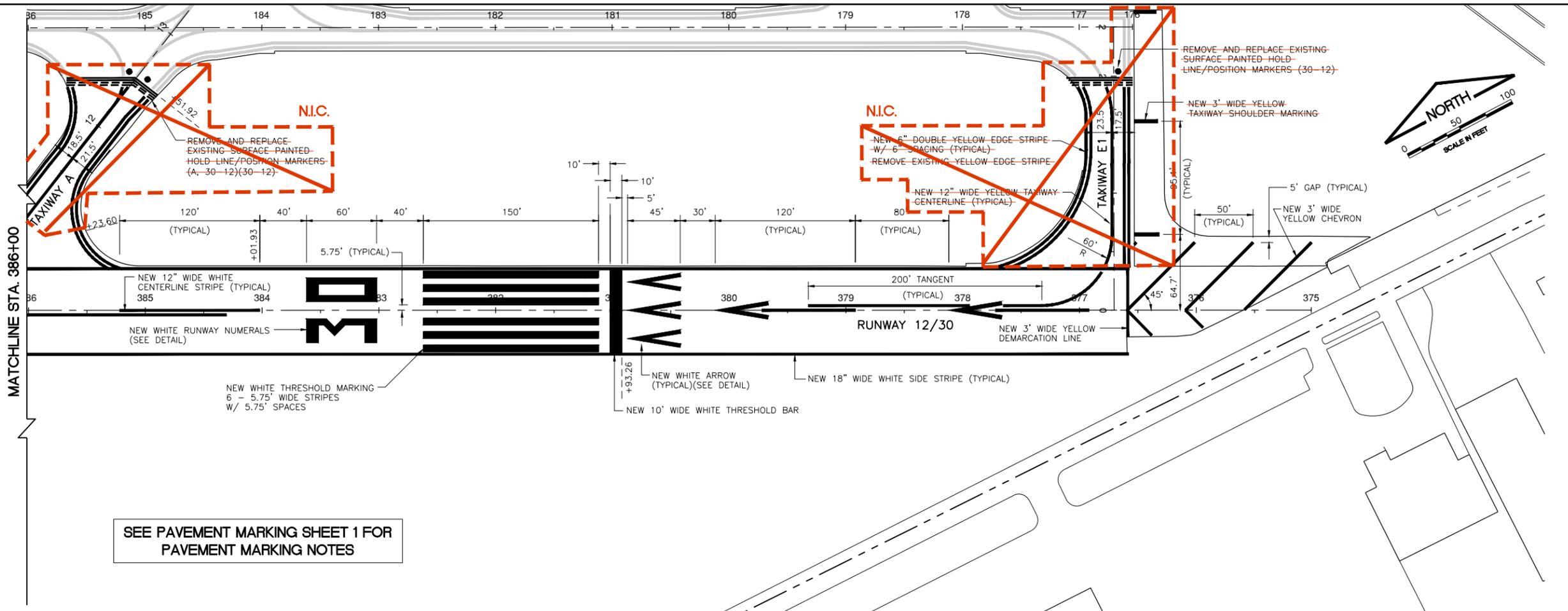
BITUMINOUS PAVEMENT MILLING (AR401650) - MILLING DEPTHS															
STATION	37.5' LEFT OF CENTERLINE			18.75' LEFT OF CENTERLINE			CENTERLINE			18.75' RIGHT OF CENTERLINE			37.5' RIGHT OF CENTERLINE		
	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH
375+59.66	NA	NA	NA	NA	NA	NA	639.37	639.37	0.17	639.64	639.64	0.17	639.52	639.52	0.17
376+00.00	NA	NA	NA	639.55	639.31	-0.08	639.83	639.76	0.10	639.63	639.71	0.25	639.42	639.42	0.17
376+50.00	NA	NA	NA	639.76	639.66	0.07	640.04	639.96	0.09	639.78	639.71	0.10	639.51	639.51	0.17
377+00.00	639.63	639.50	0.03	639.90	639.87	0.13	640.18	640.11	0.10	639.89	639.82	0.10	639.59	639.59	0.17
377+50.00	639.77	639.67	0.07	640.05	639.98	0.10	640.33	640.25	0.09	639.98	639.97	0.16	639.63	639.63	0.17
378+00.00	639.82	639.80	0.15	640.19	640.12	0.10	640.47	640.40	0.10	640.19	640.15	0.12	639.91	639.65	-0.10
378+50.00	640.06	639.87	-0.02	640.33	640.28	0.11	640.62	640.55	0.10	640.33	640.33	0.16	640.05	639.98	0.10
379+00.00	640.20	639.99	-0.04	640.48	640.42	0.11	640.76	640.70	0.11	640.48	640.43	0.12	640.19	640.14	0.12
379+50.00	640.35	640.12	-0.07	640.62	640.35	-0.11	640.90	640.83	0.09	640.62	640.56	0.11	640.33	640.31	0.14
380+00.00	640.48	640.27	-0.05	640.75	640.67	0.09	641.03	640.98	0.11	640.75	640.68	0.10	640.47	640.44	0.14
380+50.00	640.59	640.40	-0.02	640.86	640.79	0.10	641.14	641.07	0.10	640.86	640.81	0.11	640.58	640.54	0.13
381+00.00	640.67	640.44	-0.06	640.95	640.89	0.11	641.23	641.17	0.10	640.95	640.89	0.11	640.66	640.62	0.13
381+50.00	640.73	640.62	0.06	641.01	640.95	0.11	641.29	641.20	0.08	641.01	640.94	0.09	640.72	640.65	0.09
382+00.00	640.77	640.57	-0.03	641.05	640.98	0.10	641.33	641.24	0.08	641.05	640.99	0.11	640.76	640.71	0.11
382+50.00	640.79	640.57	-0.05	641.06	640.98	0.08	641.34	641.26	0.09	641.06	640.99	0.10	640.77	640.70	0.10
383+00.00	640.78	640.68	0.06	641.05	640.97	0.09	641.33	641.25	0.08	641.05	640.99	0.10	640.76	640.71	0.12
383+50.00	640.74	640.54	-0.04	641.02	640.95	0.10	641.30	641.21	0.08	641.02	640.95	0.10	640.73	640.69	0.13
384+00.00	640.69	640.50	-0.02	640.96	640.88	0.09	641.24	641.17	0.10	640.96	640.90	0.11	640.67	640.62	0.11
384+50.00	640.61	640.38	-0.06	640.88	640.79	0.07	641.17	641.08	0.08	640.88	640.81	0.10	640.59	640.53	0.11
385+00.00	640.52	640.48	0.13	640.79	640.69	0.06	641.07	641.01	0.10	640.79	640.72	0.09	640.50	640.40	0.07
385+50.00	640.43	640.25	-0.02	640.70	640.60	0.07	640.98	640.89	0.07	640.73	640.66	0.10	640.48	640.41	0.10
386+00.00	640.34	640.21	0.04	640.61	640.51	0.06	640.89	640.81	0.08	640.68	640.62	0.11	640.46	640.48	0.19
386+50.00	640.24	640.08	0.00	640.52	640.39	0.03	640.80	640.70	0.07	640.55	640.49	0.10	640.30	640.26	0.12
387+00.00	640.15	640.02	0.04	640.43	640.32	0.06	640.71	640.61	0.06	640.43	640.36	0.09	640.14	640.04	0.07
387+50.00	640.06	639.90	0.01	640.34	640.19	0.02	640.62	640.51	0.06	640.34	640.23	0.06	640.05	639.90	0.02
388+00.00	639.97	639.86	0.06	640.25	640.12	0.04	640.53	640.42	0.06	640.25	640.11	0.03	639.95	639.81	0.03
388+50.00	639.88	639.77	0.06	640.16	640.02	0.03	640.44	640.34	0.07	640.16	640.00	0.01	639.86	639.74	0.05
389+00.00	639.79	639.56	-0.07	640.07	639.97	0.07	640.35	640.26	0.08	640.07	639.97	0.07	639.77	639.62	0.01
389+50.00	639.70	639.54	0.00	639.97	639.86	0.06	640.26	640.17	0.08	639.97	639.87	0.06	639.68	639.61	0.10
390+00.00	639.62	639.55	0.10	639.90	639.80	0.07	640.18	640.08	0.07	639.90	639.80	0.07	639.60	639.56	0.13
390+50.00	639.56	639.41	0.01	639.84	639.75	0.07	640.12	640.04	0.09	639.84	639.75	0.08	639.55	639.51	0.12
391+00.00	639.53	639.36	0.00	639.81	639.72	0.08	640.09	640.02	0.10	639.81	639.76	0.11	639.51	639.48	0.14
391+50.00	639.52	639.31	-0.04	639.79	639.70	0.07	640.07	640.00	0.10	639.79	639.70	0.08	639.51	639.46	0.12
392+00.00	639.53	639.39	0.03	639.80	639.72	0.09	640.08	640.03	0.12	639.80	639.72	0.09	639.51	639.51	0.16
392+50.00	639.56	639.40	0.00	639.84	639.76	0.08	640.12	640.07	0.11	639.84	639.75	0.08	639.55	639.51	0.13
393+00.00	639.61	639.53	0.09	639.90	639.83	0.10	640.18	640.14	0.13	639.90	639.82	0.09	639.61	639.59	0.14
393+50.00	639.70	639.58	0.04	639.98	639.92	0.10	640.26	640.23	0.14	639.98	639.92	0.10	639.68	639.66	0.14
394+00.00	639.80	639.80	0.16	640.08	640.03	0.11	640.36	640.32	0.13	640.08	640.03	0.12	639.79	639.77	0.15
394+50.00	639.93	639.83	0.06	640.20	640.16	0.13	640.48	640.50	0.18	640.20	640.17	0.14	639.91	639.92	0.18
395+00.00	640.07	640.04	0.14	640.35	640.38	0.20	640.63	640.66	0.20	640.35	640.38	0.20	640.06	640.10	0.21
395+50.00	640.31	640.19	0.05	640.52	640.56	0.21	640.80	640.84	0.21	640.52	640.54	0.19	640.22	640.28	0.23
396+00.00	640.43	640.38	0.12	640.70	640.73	0.20	640.98	641.04	0.22	640.70	640.73	0.19	640.41	640.38	0.14
396+50.00	640.60	640.57	0.14	640.88	640.99	0.28	641.16	641.26	0.27	640.97	641.01	0.21	640.77	640.79	0.19
396+60.80	640.64	640.64	0.17	640.91	640.94	0.19	641.20	641.20	0.17	641.01	641.05	0.21	640.82	640.82	0.17

NOTES

- MILL DEPTH IS SHOWN IN FEET.
- THE BITUMINOUS PAVEMENT MILLING DEPTHS ARE PROVIDED TO ACHIEVE THE 2" BITUMINOUS OVERLAY DEPTH.
- NEGATIVE BITUMINOUS MILLING DEPTHS REFLECT A REQUIRED BITUMINOUS LEVELING TO ACHIEVE THE 2" BITUMINOUS OVERLAY. NO BITUMINOUS MILLING IS REQUIRED IN THESE LOCATIONS.
- BITUMINOUS LEVELING SHALL BE PLACED AT THE NEGATIVE MILL DEPTH LOCATIONS AS SPECIFIED IN THE TABLE TO ACHIEVE THE APPROPRIATE DEPTH PRIOR TO THE 2" BITUMINOUS OVERLAY. BITUMINOUS LEVELING SHALL BE PAID FOR UNDER AR401610 - BITUMINOUS SURFACE COURSE.
- NO ADDITIONAL PAYMENT FOR MILLING DEPTHS GREATER THAN SPECIFIED IN THE PLANS WILL BE MADE
- N/A ELEVATIONS ARE LOCATIONS OUTSIDE THE RUNWAY PAVEMENT.

BITUMINOUS PAVEMENT MILLING (AR401650) - MILLING DEPTHS															
STATION	37.5' LEFT OF CENTERLINE			18.75' LEFT OF CENTERLINE			CENTERLINE			18.75' RIGHT OF CENTERLINE			37.5' RIGHT OF CENTERLINE		
	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH	PROP ELEV.	EXST ELEV.	MILL DEPTH
408+22.86	643.27	643.27	0.17	643.60	643.60	0.17	643.83	643.83	0.17	643.61	643.61	0.17	643.29	643.29	0.16
408+50.00	643.26	643.26	0.17	643.55	643.61	0.22	643.83	643.86	0.20	643.55	643.60	0.22	643.27	643.12	0.02
409+00.00	643.25	643.25	0.17	643.53	643.49	0.13	643.81	643.78	0.14	643.53	643.49	0.12	643.25	643.17	0.08
409+50.00	643.23	643.24	0.18	643.52	643.46	0.10	643.80	643.74	0.10	643.52	643.46	0.10	643.24	643.20	0.13
410+00.00	643.12	643.12	0.17	643.46	643.46	0.17	643.79	643.72	0.09	643.51	643.46	0.11	643.22	643.21	0.15
410+50.00	643.21	643.22	0.17	643.50	643.44	0.10	643.78	643.68	0.07	643.52	643.41	0.05	643.26	643.26	0.17
411+00.00	643.20	643.10	0.06	643.48	643.44	0.12	643.77	643.67	0.07	643.46	643.42	0.12	643.15	643.15	0.17
411+50.00	643.19	643.11	0.09	643.47	643.39	0.09	643.75	643.66	0.07	643.46	643.40	0.11	643.15	643.15	0.17
412+00.00	643.17	643.11	0.11	643.46	643.36	0.07	643.74	643.66	0.08	643.46	643.38	0.09	643.17	643.17	0.17
412+50.00	643.17	643.13	0.12	643.45	643.37	0.08	643.73	643.64	0.07	643.44	643.37	0.10	643.14	643.14	0.17
413+00.00	643.18	643.08	0.07	643.46	643.36	0.06	643.74	643.65	0.07	643.46	643.37	0.07	643.23	643.12	0.06
413+50.00	643.21	643.16	0.12	643.49	643.39	0.06	643.77	643.67	0.07	643.49	643.40	0.08	643.26	643.13	0.03
414+00.00	643.26	643.17	0.08	643.54	643.44	0.07	643.82	643.73	0.08	643.54	643.44	0.07	643.26	643.15	0.06
414+50.00	643.34	643.24	0.07	643.62	643.53	0.08	643.90	643.82	0.09	643.62	643.53	0.07	643.34	643.27	0.10
415+00.00	643.43	643.33	0.07	643.72	643.62	0.07	644.00	643.89	0.06	643.72	643.60	0.05	643.43	643.32	0.06
415+50.00	643.55	643.47	0.08	643.83	643.73	0.07	644.11	644.03	0.08	643.83	643.74	0.07	643.55	643.52	0.13
416+00.00	643.69	643.61	0.08	643.97	643.88	0.08	644.25	644.16	0.08	643.97	643.87	0.07	643.69	643.56	0.04
416+50.00	643.85	643.79	0.11	644.13	644.06	0.09	644.41	644.31	0.07	644.13	643.98	0.01	643.85	643.65	-0.04
417+00.00	644.03	644.01	0.14	644.31	644.22	0.07	644.59	644.52	0.09	644.31	644.17	0.02	644.03	643.89	0.03
417+50.00	644.21	644.22	0.17	644.49	644.43	0.10	644.78	644.67	0.06	644.49	644.39	0.06	644.21	644.14	0.09
418+00.00	644.39	644.19	-0.04	644.68	644.56	0.05	644.96	644.88	0.09						

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 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
 IMAGE FILES: pwknewLOGO_CEA-4-Color-Small (2).jpg
 REF DWG: 16290-04_Renobkwy12-30_base-NEW.dwg
 CEA Alignments.dwg



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

PAVEMENT MARKING - SHEET 1

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: KWS
 DATE: 06/08/2018
 JOB No: 16290-04-00

FINAL

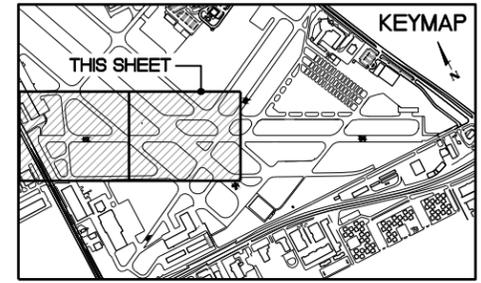
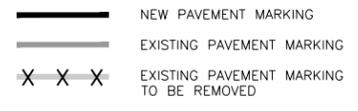
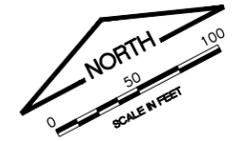
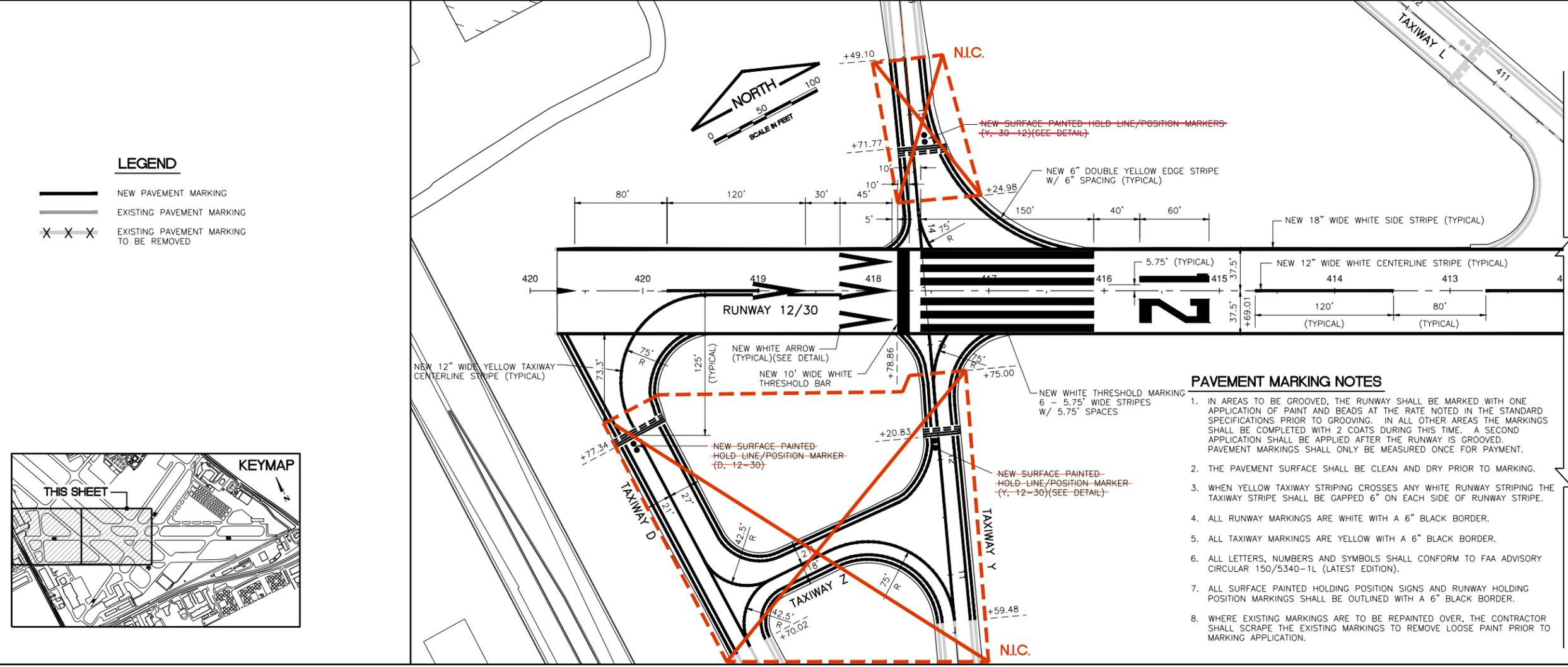
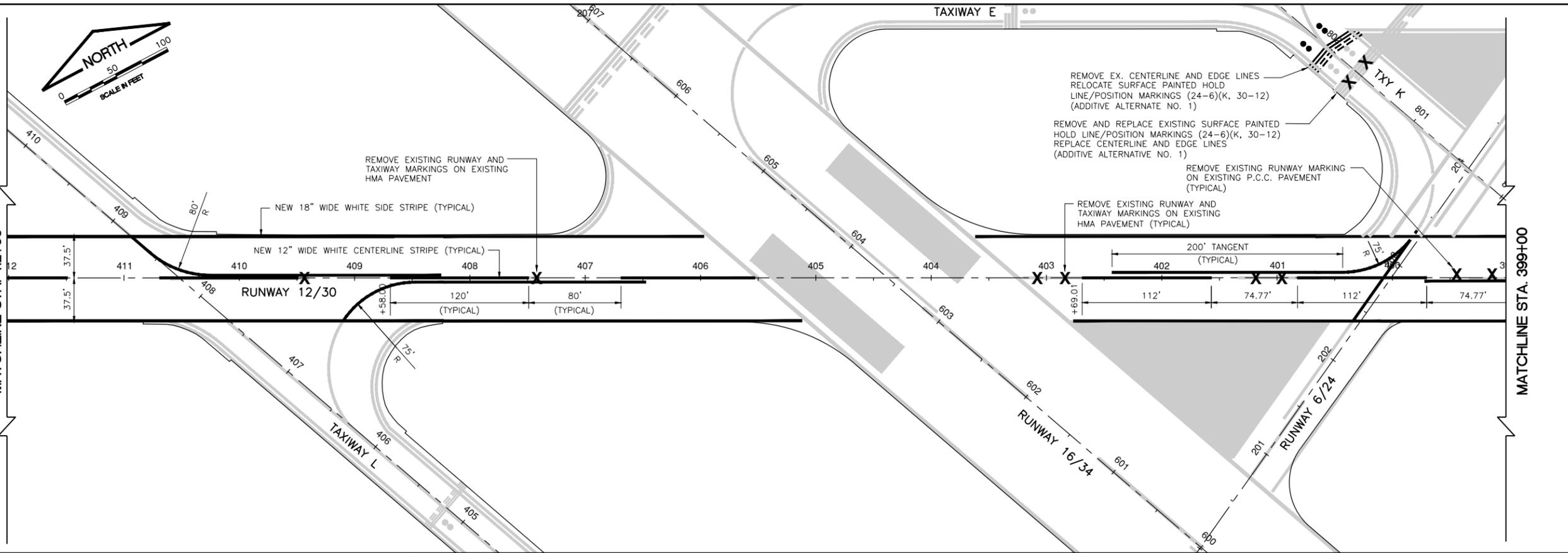
SHEET 27 OF 37 SHEETS

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IMAGE FILES: pwknewLOGO_CEA-4-Color-Small (2).jpg

UPDATE BY: Kris Solvatero
 LAYOUT: Layout1

DATE: Friday, June 8, 2018 4:30:09 PM
 FILE: K:\Chicago\Exec\16290-04_Renobkwy12-30\Drawn\Sheets\gmark1.dwg



REMOVE EX. CENTERLINE AND EDGE LINES
 RELOCATE SURFACE PAINTED HOLD
 LINE/POSITION MARKINGS (24-6)(K, 30-12)
 (ADDITIVE ALTERNATE NO. 1)

REMOVE AND REPLACE EXISTING SURFACE PAINTED
 HOLD LINE/POSITION MARKINGS (24-6)(K, 30-12)
 REPLACE CENTERLINE AND EDGE LINES
 (ADDITIVE ALTERNATE NO. 1)

REMOVE EXISTING RUNWAY MARKING
 ON EXISTING P.C.C. PAVEMENT
 (TYPICAL)

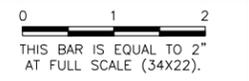
REMOVE EXISTING RUNWAY AND
 TAXIWAY MARKINGS ON EXISTING
 HMA PAVEMENT (TYPICAL)

REMOVE EXISTING RUNWAY AND
 TAXIWAY MARKINGS ON EXISTING
 HMA PAVEMENT (TYPICAL)

- PAVEMENT MARKING NOTES**
1. IN AREAS TO BE GROOVED, THE RUNWAY SHALL BE MARKED WITH ONE APPLICATION OF PAINT AND BEADS AT THE RATE NOTED IN THE STANDARD SPECIFICATIONS PRIOR TO GROOVING. IN ALL OTHER AREAS THE MARKINGS SHALL BE COMPLETED WITH 2 COATS DURING THIS TIME. A SECOND APPLICATION SHALL BE APPLIED AFTER THE RUNWAY IS GROOVED. PAVEMENT MARKINGS SHALL ONLY BE MEASURED ONCE FOR PAYMENT.
 2. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
 3. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING THE TAXIWAY STRIPE SHALL BE GAPPED 6" ON EACH SIDE OF RUNWAY STRIPE.
 4. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER.
 5. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER.
 6. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).
 7. ALL SURFACE PAINTED HOLDING POSITION SIGNS AND RUNWAY HOLDING POSITION MARKINGS SHALL BE OUTLINED WITH A 6" BLACK BORDER.
 8. WHERE EXISTING MARKINGS ARE TO BE REPAINTED OVER, THE CONTRACTOR SHALL SCRAPE THE EXISTING MARKINGS TO REMOVE LOOSE PAINT PRIOR TO MARKING APPLICATION.

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

PAVEMENT MARKING - SHEET 2

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DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: KWS
 DATE: 06/08/2018
 JOB No: 16290-04-00

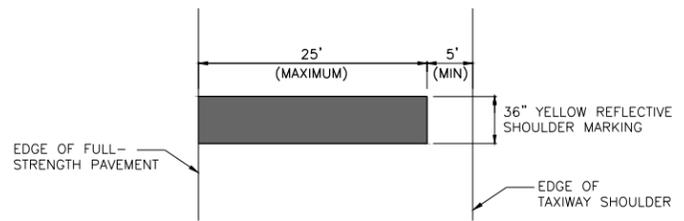
FINAL
 SHEET 28 OF 37 SHEETS

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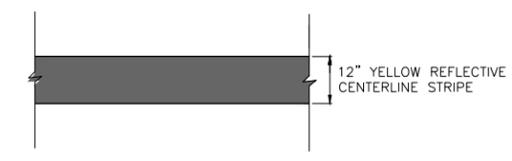
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UPDATE BY: Kris Solvatera
LAYOUT: Layout1

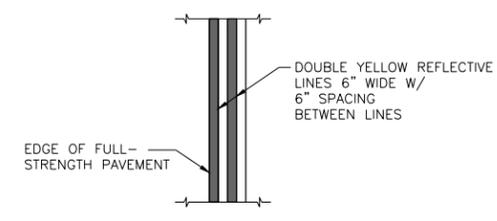
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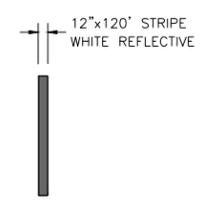
TAXIWAY SHOULDER MARKING
NOT TO SCALE



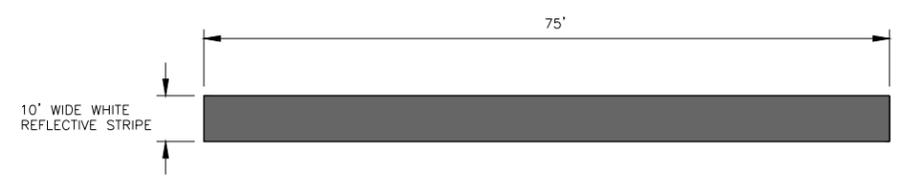
TAXIWAY CENTERLINE DETAIL
NOT TO SCALE



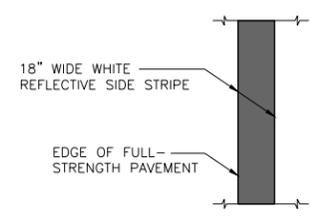
TAXIWAY EDGE MARKINGS CONTINUOUS
NOT TO SCALE



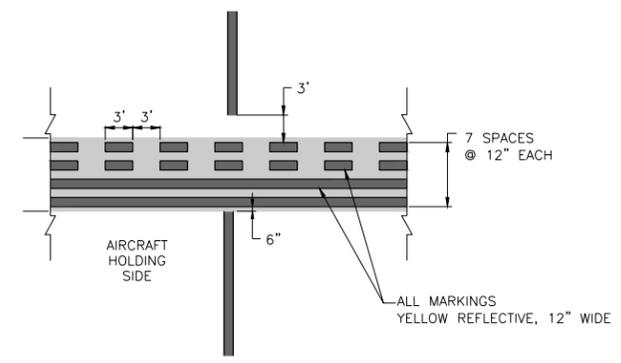
RUNWAY CENTERLINE - DASHED
NOT TO SCALE



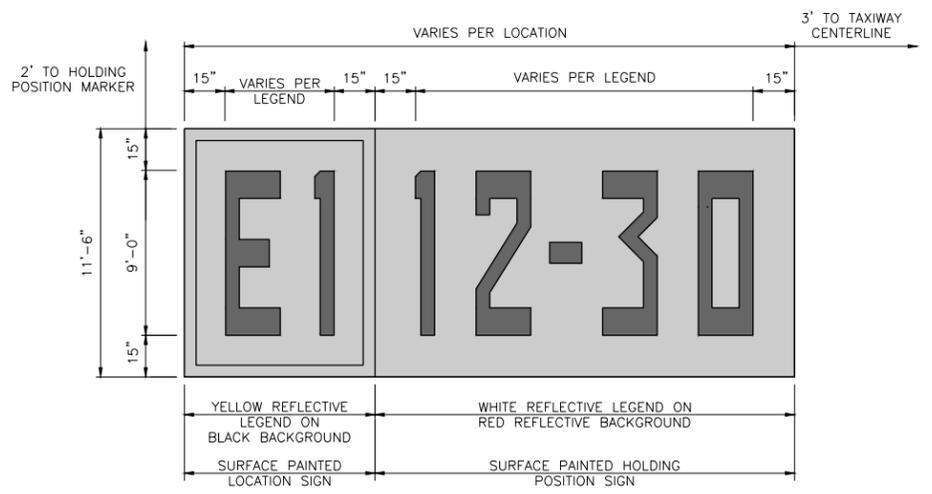
RUNWAY THRESHOLD BAR
NOT TO SCALE



RUNWAY EDGE MARKING CONTINUOUS
NOT TO SCALE

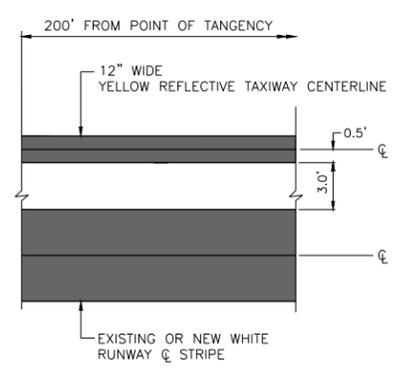


RUNWAY HOLDING POSITION MARKING
NOT TO SCALE

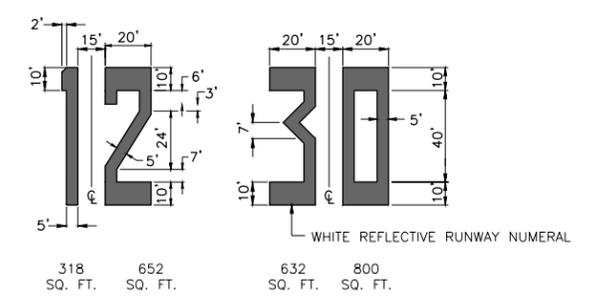


SURFACE PAINTED HOLDING POSITION SIGN
NOT TO SCALE

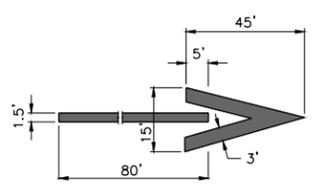
1. ALL SURFACE PAINTED HOLDING POSITION SIGNS SHALL HAVE A RED REFLECTIVE BACKGROUND WITH A WHITE REFLECTIVE INSCRIPTION.
2. ALL SURFACE PAINTED LOCATION SIGNS SHALL HAVE A BLACK NON-REFLECTIVE BACKGROUND WITH A REFLECTIVE YELLOW INSCRIPTION. THE YELLOW OUTLINE SHALL BE REFLECTIVE.
3. ALL SURFACE PAINTED SIGNS SHALL BE OUTLINED WITH A 6\"/>



TAXIWAY LEAD-IN TANGENT DETAIL
NOT TO SCALE



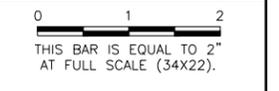
NUMERAL DETAILS
NO SCALE



ARROW DETAIL
N.T.S.

IL. CONTRACT: PA061
IL. LETTING ITEM: 10A
IL. PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

PAVEMENT MARKING DETAILS

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	KWS
DATE:	06/08/2018
JOB No:	16290-04-00

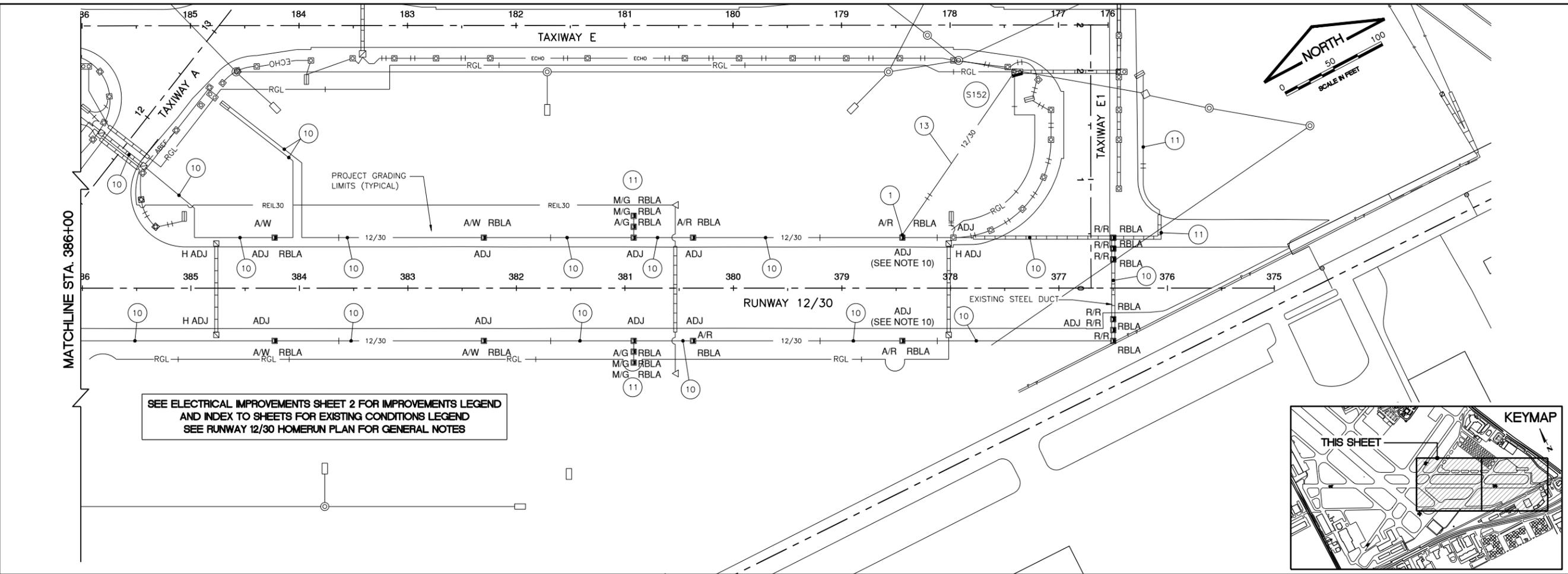
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SHEET 29 OF 37 SHEETS

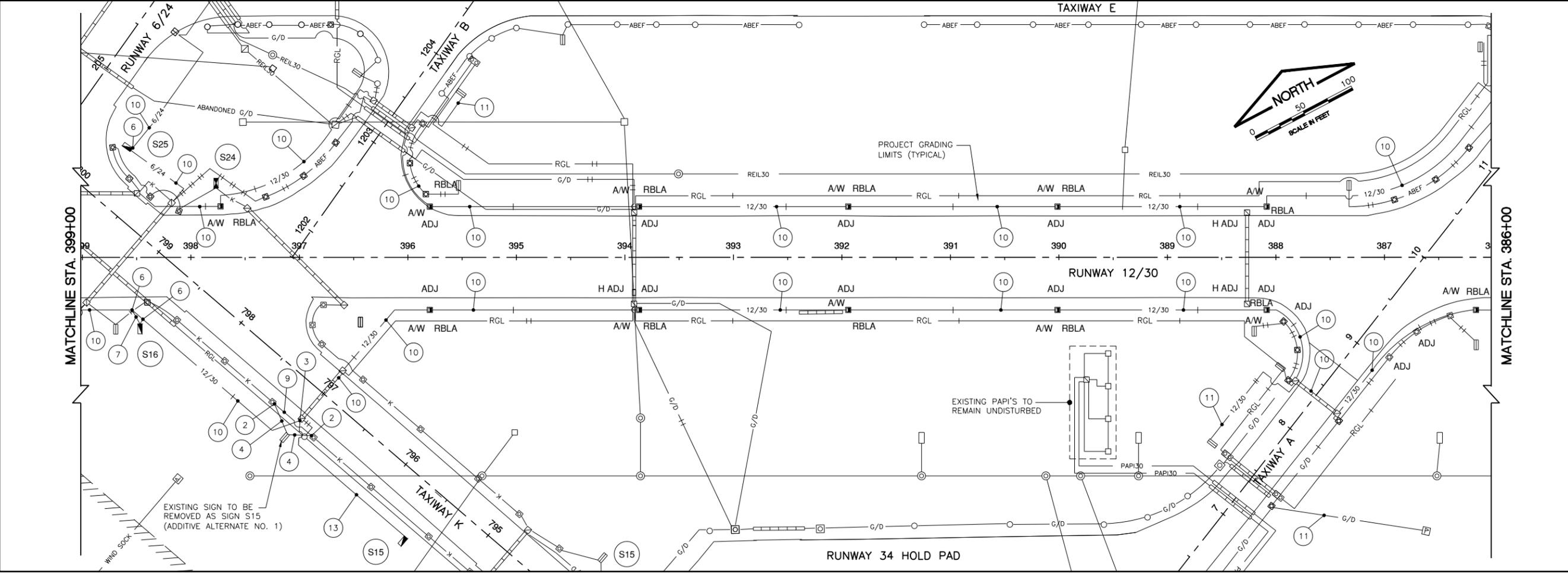
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 CEA Alignments.dwg

UPDATE BY: Kris Solvatero
 LAYOUT: Layout1

DATE: Friday, June 8, 2018 4:30:39 PM
 FILE: K:\Chicago\16290-04_Renovation\12-30_Draw\Sheets\elec1.dwg



SEE ELECTRICAL IMPROVEMENTS SHEET 2 FOR IMPROVEMENTS LEGEND
 AND INDEX TO SHEETS FOR EXISTING CONDITIONS LEGEND
 SEE RUNWAY 12/30 HOMERUN PLAN FOR GENERAL NOTES



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

ELECTRICAL IMPROVEMENTS - SHEET 1

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 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 06/08/2018
 JOB No: 16290-04-00

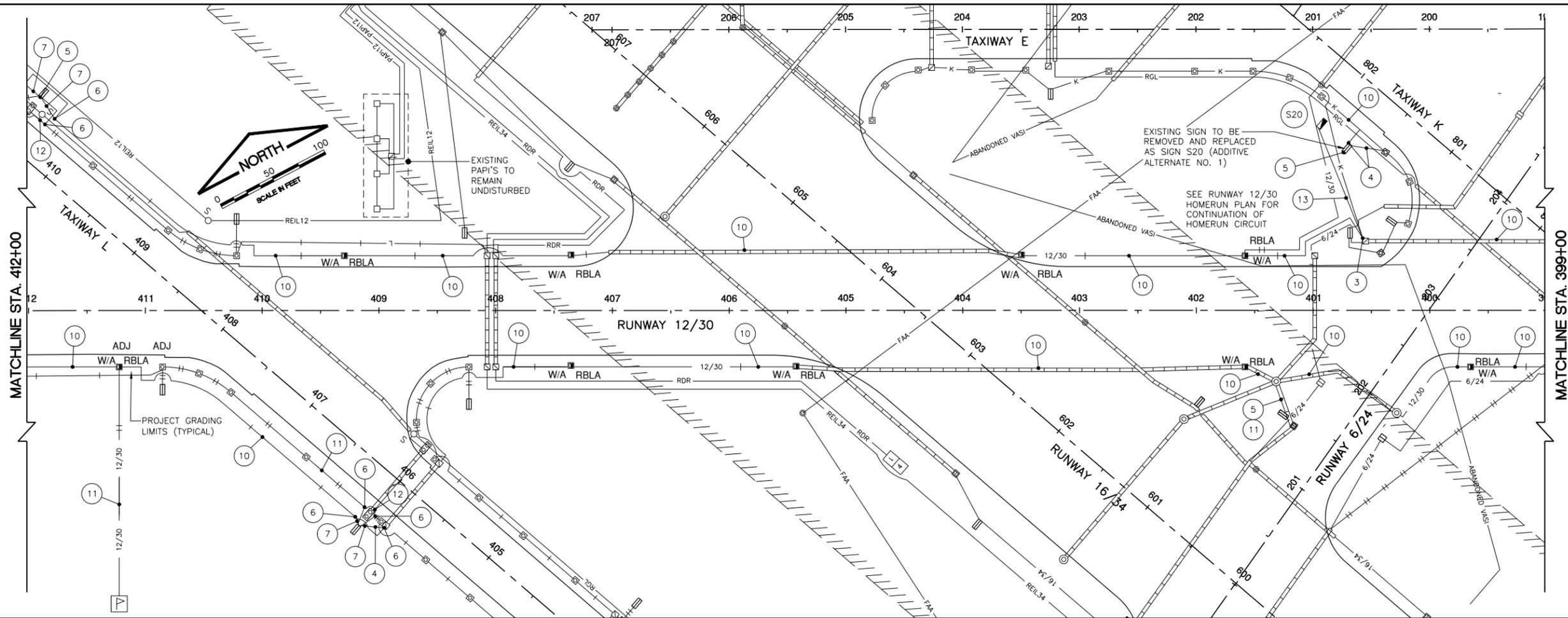
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SHEET 30 OF 37 SHEETS

REF: DWG: 16290-04_Renobw12-30_Drn_Sheets_elec2.dwg
 DATE: Friday, June 8, 2018 4:30:58 PM
 FILE: K:\Chicago\16290-04_Renobw12-30_Drn_Sheets_elec2.dwg

IMAGE FILES: pwrnewLOGO_CEA-4-Color-Small (2).jpg
 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1

DATE: Friday, June 8, 2018 4:30:58 PM
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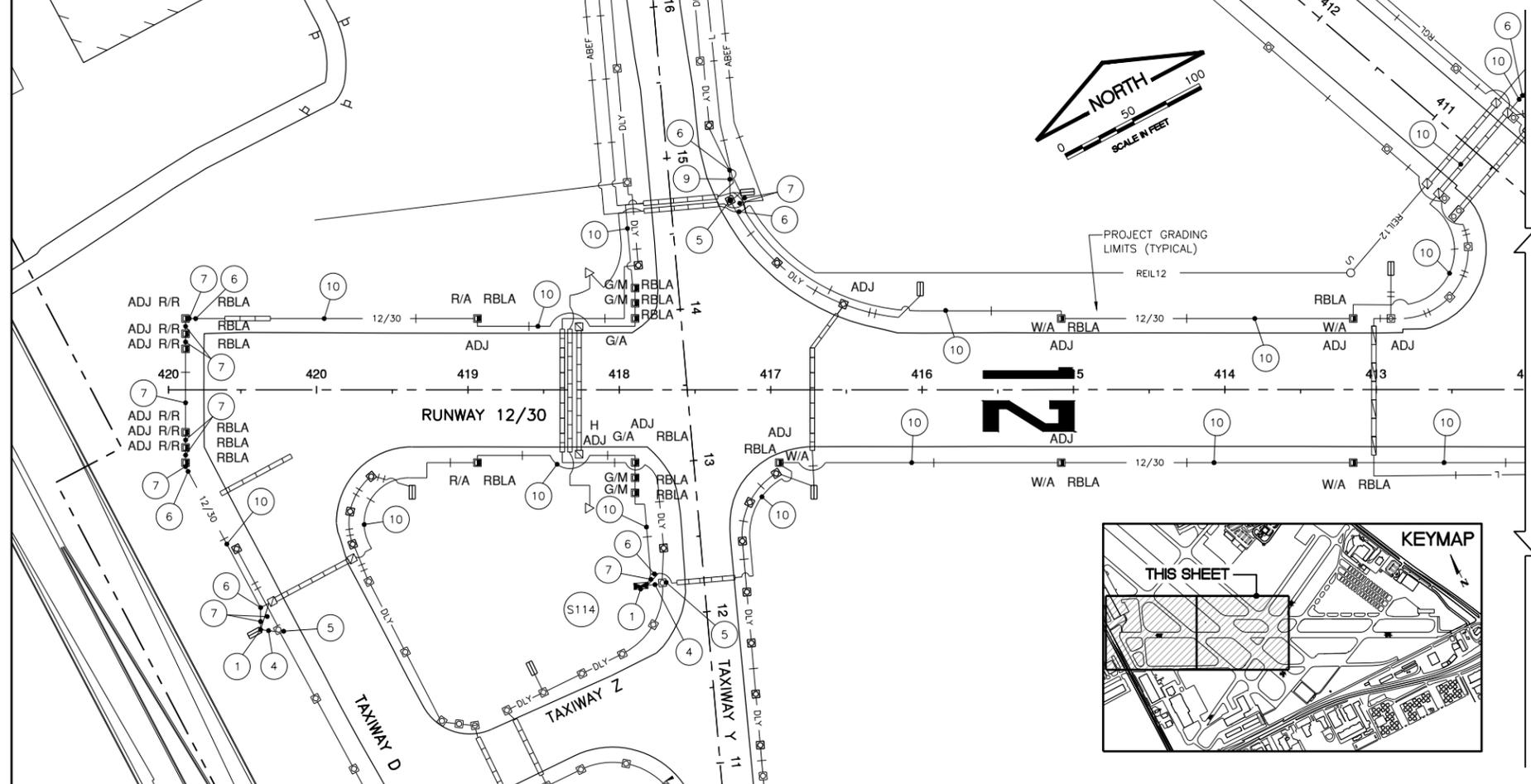
SEE ELECTRICAL IMPROVEMENTS SHEET 2 FOR IMPROVEMENTS LEGEND AND INDEX TO SHEETS FOR EXISTING CONDITIONS LEGEND
 SEE RUNWAY 12/30 HOMERUN PLAN FOR GENERAL NOTES

LEGEND

- ADJ BASE MOUNTED LIGHT TO BE ADJUSTED (BASE BID)
- H ADJ HANDHOLE TO BE ADJUSTED (BASE BID)
- RBLA REPLACE MEDIUM INTENSITY BASE MOUNTED RUNWAY EDGE/THRESHOLD LIGHT (ADDITIVE ALTERNATE 1)
- W/A WHITE LENS / AMBER LENS (ADDITIVE ALTERNATE 1)
- A/G AMBER LENS /GREEN LENS (ADDITIVE ALTERNATE 1)
- G/M GREEN LENS/MASKED LENS (ADDITIVE ALTERNATE 1)
- R/R RED LENS/RED LENS (ADDITIVE ALTERNATE 1)
- A/R AMBER LENS/RED LENS (ADDITIVE ALTERNATE 1)
- SC SPLICE CAN (ADDITIVE ALTERNATE 1)
- NS NEW TAXI GUIDANCE SIGN (ADDITIVE ALTERNATE 1)
- RS REPLACE TAXI GUIDANCE SIGN PANELS ONLY (ADDITIVE ALTERNATE 1)
- RS REPLACE TAXI GUIDANCE SIGN (ADDITIVE ALTERNATE 1)

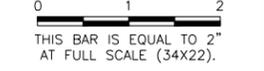
KEYED NOTES

- 1 CORE NEW OPENING IN EXISTING LIGHT OR SIGN BASE
- 2 DISCONNECT EXISTING CONDUIT, CONNECT NEW CONDUIT
- 3 CORE OPENING IN EXISTING HANDHOLE/MANHOLE
- 4 EXISTING DUCT TO BE ABANDONED
- 5 DISCONNECT CABLE TO SIGN, COMPLETE CIRCUIT AT LIGHT
- 6 LOCATE EXISTING CONDUIT, CONNECT TO NEW CONDUIT
- 7 NEW 2" PVC DUCT, DIRECT BURY WITH 1/C #8 5KV UG CABLE (RUNWAY 12/30 CIRCUIT)
- 8 NEW 2" GRS CONDUIT, JACKED, WITH 2-1/C #8 5KV UG CABLE (TAXIWAY CIRCUIT)
- 9 NEW 2" PVC DUCT, DIRECT BURY WITH 1/C #8 5KV UG CABLE (TAXIWAY CIRCUIT)
- 10 NEW 1/C #8 5KV UG CABLE IN EXISTING DUCT
- 11 NEW 2-1/C #8 5KV UG CABLE IN EXISTING DUCT
- 12 NEW 2" PVC DUCT, DIRECT BURY WITH 2-1/C #8 5 KV UG CABLE (TAXIWAY CIRCUIT)
- 13 NEW 2" PVC DUCT, DIRECT BURY WITH 2-1/C #8 5 KV UG CABLE (RUNWAY 12/30 CIRCUIT)
- 14 NEW 2" PVC DUCT, DIRECT BURY WITH 2-1/C #8 5 KV UG CABLE (RUNWAY 6/24 CIRCUIT)



IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
ELECTRICAL IMPROVEMENTS - SHEET 2

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CHICAGO EXECUTIVE AIRPORT

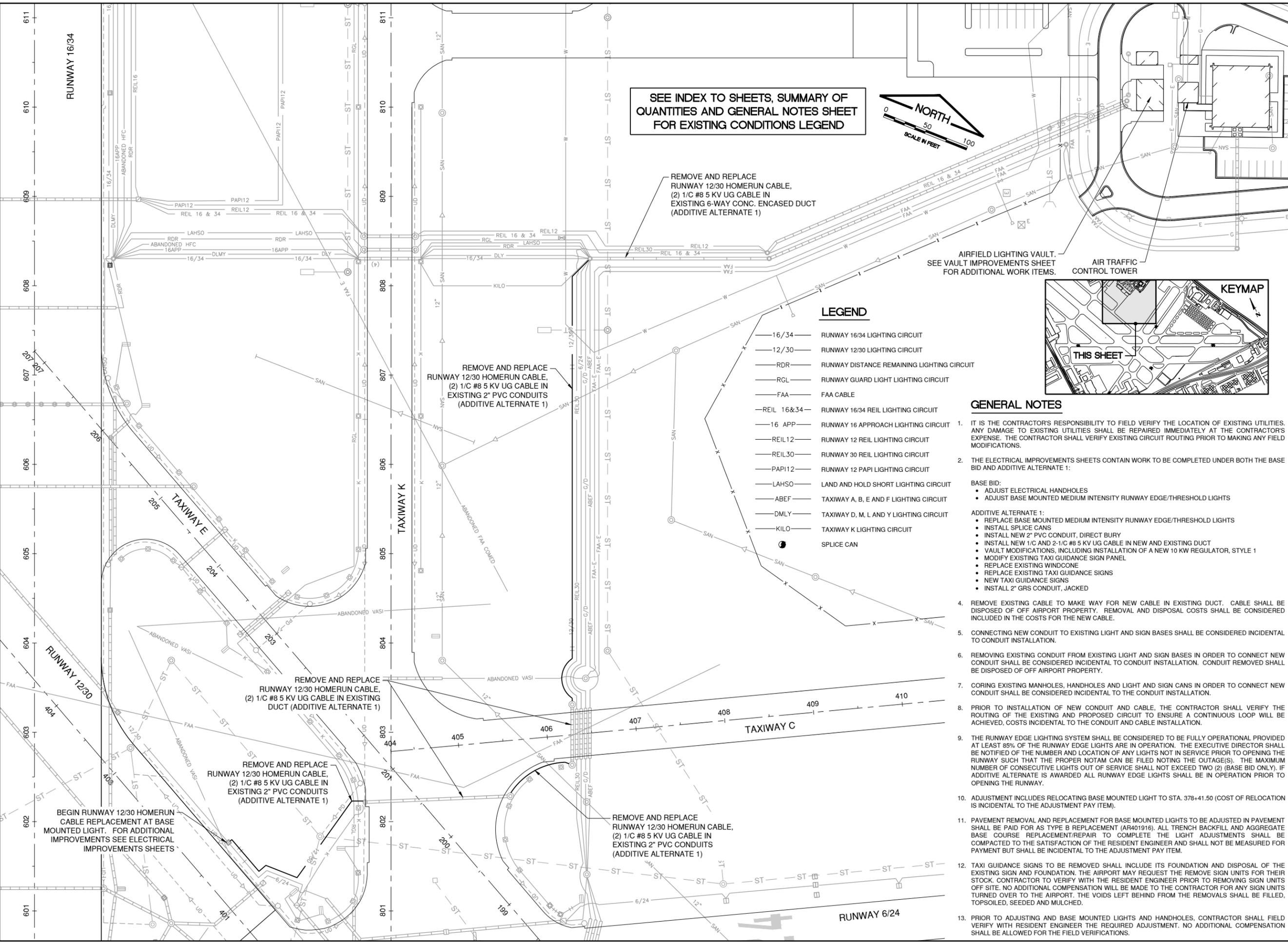
DESIGN BY:	DKP
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL
 SHEET 31 OF 37 SHEETS

REF: DWG: 16312-runway-homerun-plan.dwg
 Runway 12-30 base-NEW.dwg
 CEA Alignments.dwg

UPDATE BY: Kris Solvatero
 LAYOUT: HOMERUN PLAN

DATE: Friday, June 8, 2018 4:31:12 PM
 FILE: K:\Chicago\16312-04-Rehab\12-30\Drawn\Sheets\homerun-Plan.dwg



SEE INDEX TO SHEETS, SUMMARY OF QUANTITIES AND GENERAL NOTES SHEET FOR EXISTING CONDITIONS LEGEND



REMOVE AND REPLACE RUNWAY 12/30 HOMERUN CABLE, (2) 1/C #8 5 KV UG CABLE IN EXISTING 6-WAY CONC. ENCASED DUCT (ADDITIVE ALTERNATE 1)

REMOVE AND REPLACE RUNWAY 12/30 HOMERUN CABLE, (2) 1/C #8 5 KV UG CABLE IN EXISTING 2" PVC CONDUITS (ADDITIVE ALTERNATE 1)

REMOVE AND REPLACE RUNWAY 12/30 HOMERUN CABLE, (2) 1/C #8 5 KV UG CABLE IN EXISTING DUCT (ADDITIVE ALTERNATE 1)

REMOVE AND REPLACE RUNWAY 12/30 HOMERUN CABLE, (2) 1/C #8 5 KV UG CABLE IN EXISTING 2" PVC CONDUITS (ADDITIVE ALTERNATE 1)

BEGIN RUNWAY 12/30 HOMERUN CABLE REPLACEMENT AT BASE MOUNTED LIGHT. FOR ADDITIONAL IMPROVEMENTS SEE ELECTRICAL IMPROVEMENTS SHEETS

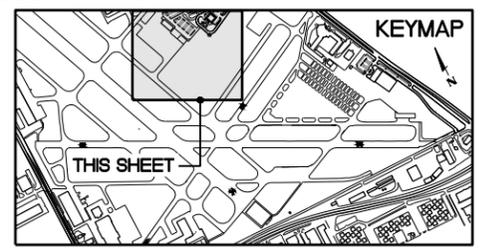
REMOVE AND REPLACE RUNWAY 12/30 HOMERUN CABLE, (2) 1/C #8 5 KV UG CABLE IN EXISTING 2" PVC CONDUITS (ADDITIVE ALTERNATE 1)

AIRFIELD LIGHTING VAULT. SEE VAULT IMPROVEMENTS SHEET FOR ADDITIONAL WORK ITEMS.

AIR TRAFFIC CONTROL TOWER

LEGEND

- 16/34 — RUNWAY 16/34 LIGHTING CIRCUIT
- 12/30 — RUNWAY 12/30 LIGHTING CIRCUIT
- RDR — RUNWAY DISTANCE REMAINING LIGHTING CIRCUIT
- RGL — RUNWAY GUARD LIGHT LIGHTING CIRCUIT
- FAA — FAA CABLE
- REIL 16&34 — RUNWAY 16/34 REIL LIGHTING CIRCUIT
- 16 APP — RUNWAY 16 APPROACH LIGHTING CIRCUIT
- REIL12 — RUNWAY 12 REIL LIGHTING CIRCUIT
- REIL30 — RUNWAY 30 REIL LIGHTING CIRCUIT
- PAPI12 — RUNWAY 12 PAPI LIGHTING CIRCUIT
- LAHSO — LAND AND HOLD SHORT LIGHTING CIRCUIT
- ABEF — TAXIWAY A, B, E AND F LIGHTING CIRCUIT
- DMLY — TAXIWAY D, M, L AND Y LIGHTING CIRCUIT
- KILO — TAXIWAY K LIGHTING CIRCUIT
- SPlice CAN

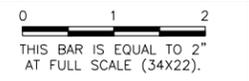


GENERAL NOTES

1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL VERIFY EXISTING CIRCUIT ROUTING PRIOR TO MAKING ANY FIELD MODIFICATIONS.
2. THE ELECTRICAL IMPROVEMENTS SHEETS CONTAIN WORK TO BE COMPLETED UNDER BOTH THE BASE BID AND ADDITIVE ALTERNATE 1:
 - BASE BID:
 - ADJUST ELECTRICAL HANDHOLES
 - ADJUST BASE MOUNTED MEDIUM INTENSITY RUNWAY EDGE/THRESHOLD LIGHTS
 - ADDITIVE ALTERNATE 1:
 - REPLACE BASE MOUNTED MEDIUM INTENSITY RUNWAY EDGE/THRESHOLD LIGHTS
 - INSTALL SPLICE CANS
 - INSTALL NEW 2" PVC CONDUIT, DIRECT BURY
 - INSTALL NEW 1/C AND 2-1/C #8 5 KV UG CABLE IN NEW AND EXISTING DUCT
 - VAULT MODIFICATIONS, INCLUDING INSTALLATION OF A NEW 10 KW REGULATOR, STYLE 1
 - MODIFY EXISTING TAXI GUIDANCE SIGN PANEL
 - REPLACE EXISTING WINDCONE
 - REPLACE EXISTING TAXI GUIDANCE SIGNS
 - NEW TAXI GUIDANCE SIGNS
 - INSTALL 2" GRS CONDUIT, JACKED
4. REMOVE EXISTING CABLE TO MAKE WAY FOR NEW CABLE IN EXISTING DUCT. CABLE SHALL BE DISPOSED OF OFF AIRPORT PROPERTY. REMOVAL AND DISPOSAL COSTS SHALL BE CONSIDERED INCLUDED IN THE COSTS FOR THE NEW CABLE.
5. CONNECTING NEW CONDUIT TO EXISTING LIGHT AND SIGN BASES SHALL BE CONSIDERED INCIDENTAL TO CONDUIT INSTALLATION.
6. REMOVING EXISTING CONDUIT FROM EXISTING LIGHT AND SIGN BASES IN ORDER TO CONNECT NEW CONDUIT SHALL BE CONSIDERED INCIDENTAL TO CONDUIT INSTALLATION. CONDUIT REMOVED SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
7. CORING EXISTING MANHOLES, HANDHOLES AND LIGHT AND SIGN CANS IN ORDER TO CONNECT NEW CONDUIT SHALL BE CONSIDERED INCIDENTAL TO THE CONDUIT INSTALLATION.
8. PRIOR TO INSTALLATION OF NEW CONDUIT AND CABLE, THE CONTRACTOR SHALL VERIFY THE ROUTING OF THE EXISTING AND PROPOSED CIRCUIT TO ENSURE A CONTINUOUS LOOP WILL BE ACHIEVED. COSTS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.
9. THE RUNWAY EDGE LIGHTING SYSTEM SHALL BE CONSIDERED TO BE FULLY OPERATIONAL PROVIDED AT LEAST 85% OF THE RUNWAY EDGE LIGHTS ARE IN OPERATION. THE EXECUTIVE DIRECTOR SHALL BE NOTIFIED OF THE NUMBER AND LOCATION OF ANY LIGHTS NOT IN SERVICE PRIOR TO OPENING THE RUNWAY SUCH THAT THE PROPER NOTAM CAN BE FILED NOTING THE OUTAGE(S). THE MAXIMUM NUMBER OF CONSECUTIVE LIGHTS OUT OF SERVICE SHALL NOT EXCEED TWO (2) (BASE BID ONLY). IF ADDITIVE ALTERNATE IS AWARDED ALL RUNWAY EDGE LIGHTS SHALL BE IN OPERATION PRIOR TO OPENING THE RUNWAY.
10. ADJUSTMENT INCLUDES RELOCATING BASE MOUNTED LIGHT TO STA. 378+41.50 (COST OF RELOCATION IS INCIDENTAL TO THE ADJUSTMENT PAY ITEM).
11. PAVEMENT REMOVAL AND REPLACEMENT FOR BASE MOUNTED LIGHTS TO BE ADJUSTED IN PAVEMENT SHALL BE PAID FOR AS TYPE B REPLACEMENT (AR401916). ALL TRENCH BACKFILL AND AGGREGATE BASE COURSE REPLACEMENT/REPAIR TO COMPLETE THE LIGHT ADJUSTMENTS SHALL BE COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND SHALL NOT BE MEASURED FOR PAYMENT BUT SHALL BE INCIDENTAL TO THE ADJUSTMENT PAY ITEM.
12. TAXI GUIDANCE SIGNS TO BE REMOVED SHALL INCLUDE ITS FOUNDATION AND DISPOSAL OF THE EXISTING SIGN AND FOUNDATION. THE AIRPORT MAY REQUEST THE REMOVE SIGN UNITS FOR THEIR STOCK. CONTRACTOR TO VERIFY WITH THE RESIDENT ENGINEER PRIOR TO REMOVING SIGN UNITS OFF SITE. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR ANY SIGN UNITS TURNED OVER TO THE AIRPORT. THE VOIDS LEFT BEHIND FROM THE REMOVALS SHALL BE FILLED, TOPSOILED, SEEDDED AND MULCHED.
13. PRIOR TO ADJUSTING AND BASE MOUNTED LIGHTS AND HANDHOLES, CONTRACTOR SHALL FIELD VERIFY WITH RESIDENT ENGINEER THE REQUIRED ADJUSTMENT. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE FIELD VERIFICATIONS.

IL CONTRACT: PA061
 IL LETTING ITEM: 10A
 IL PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

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CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
 RUNWAY 12/30 HOMERUN PLAN

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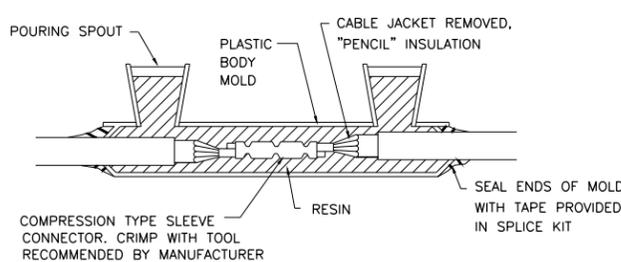
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DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

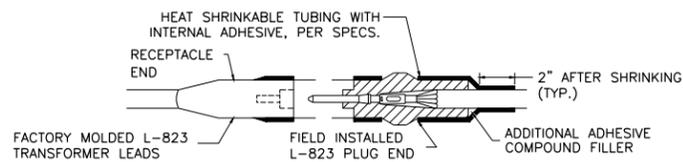
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SHEET 32 OF 37 SHEETS



TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
NOT TO SCALE

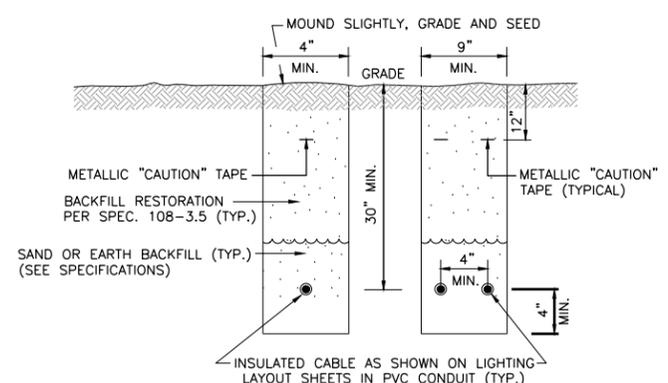


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
NOT TO SCALE

SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED PRIMARY CONNECTOR KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.

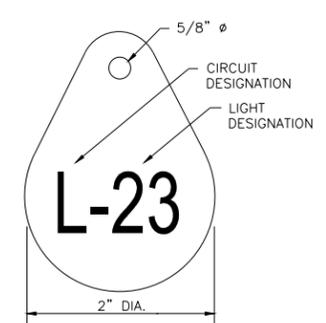


TRENCH NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.

TRENCH DETAIL

NOT TO SCALE

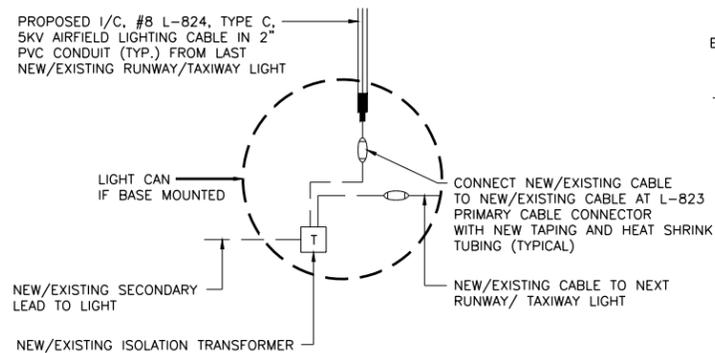


LIGHT IDENTIFICATION DETAIL

NOT TO SCALE

NOTES

- INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS AND SIGNS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

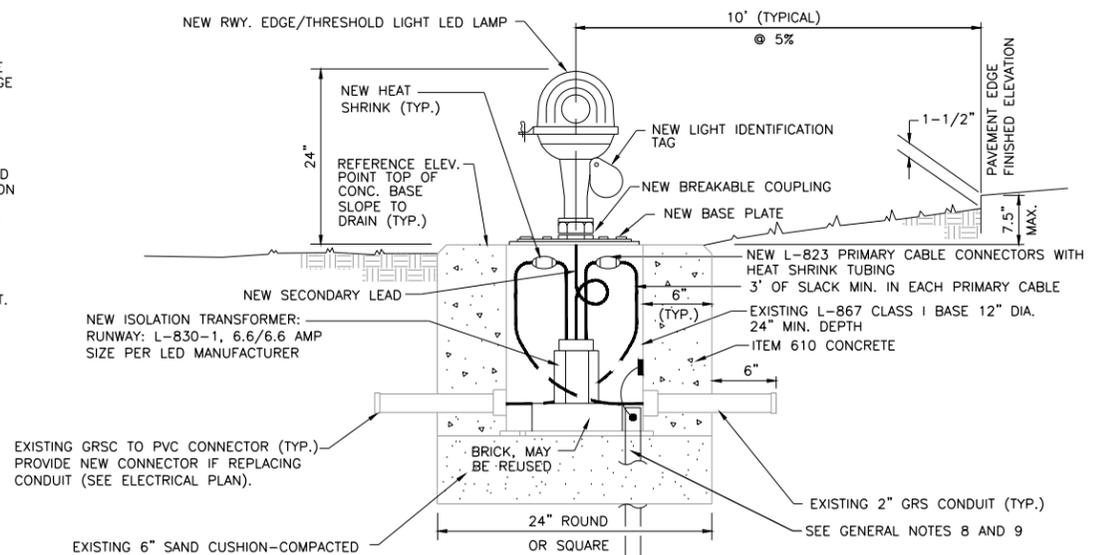


RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE

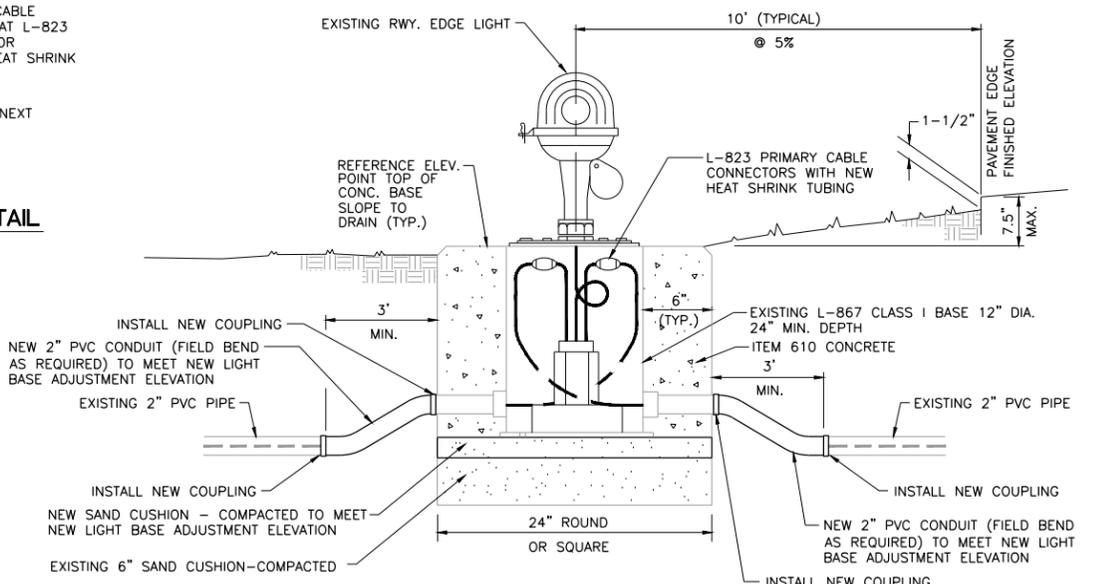
GENERAL NOTES

- CONCRETE BASES FOR BASE MOUNTED LIGHTS, SIGNS AND SPLICE CANS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (ITEM 610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/2 #8 5 KV, L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR AND BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE. AT NO ADDITIONAL COST.
- ALL RUNWAY/TAXIWAY EDGE LIGHTS SHALL HAVE 2" DIAMETER COLUMN AND FRANGIBLE COUPLINGS, UNLESS OTHERWISE NOTED.
- INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.
- INSTALL 1/2 #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.
- CONTRACTOR SHALL SURVEY THE LOCATIONS OF THE EXISTING IN-PAVEMENT DUCT MARKERS PRIOR TO MILLING THE SURFACE. AFTER PAVING IS COMPLETE, NEW DUCT MARKERS SHALL BE REINSTALLED, COSTS INCIDENTAL.



REPLACE BASE MOUNTED MEDIUM INTENSITY RUNWAY EDGE/THRESHOLD LIGHT (LED) DETAIL

NOT TO SCALE



ADJUST BASE MOUNTED LIGHTS (BASE BID)

NOT TO SCALE

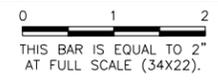
METHOD OF CONSTRUCTION

- UNPLUG AND REMOVE FIXTURE. ADJUST SHOULDERS TO GRADE.
- EXCAVATE BASE MOUNTED LIGHT AND LIGHTING CIRCUIT.
- REMOVE LIGHTING CIRCUIT FROM BASE CAN.
- PLACE AND COMPACT SAND CUSHION AS REQUIRED BY AMOUNT OF ADJUSTMENT.
- NEW CONDUIT/COUPLING AS REQUIRED.
- REPLACE BASE CAN AND REPLACE LIGHTING CIRCUIT. SEAL CABLE ENTRANCES WITH DUCT SEAL.
- PLUG FIXTURE BACK INTO CIRCUIT AND REPLACE HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE OVER ENTIRE CONNECTOR (2 PER FIXTURE).
- BACKFILL EXCAVATED EARTH MATERIALS AND COMPACT.

IL CONTRACT: PA061
IL LETTING ITEM: 10A
IL PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

ELECTRICAL DETAILS - SHEET 1

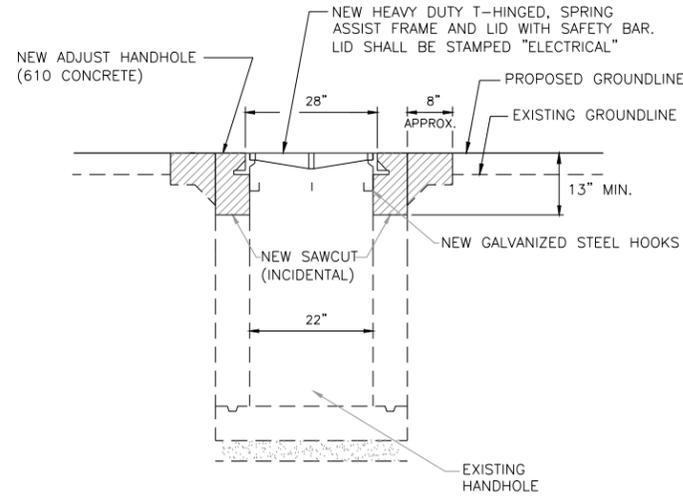
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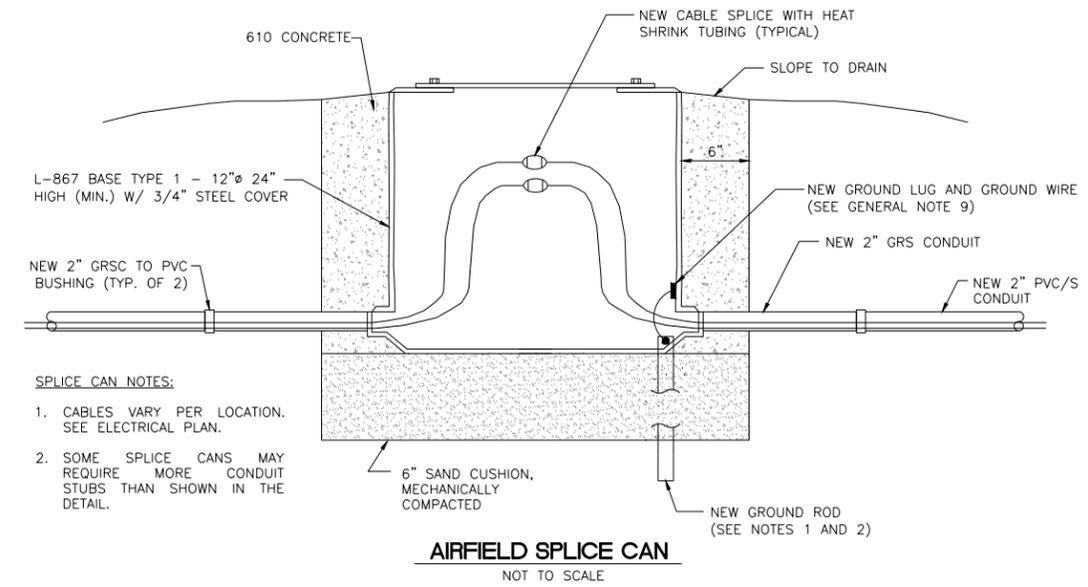
CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	KWS
DATE:	06/08/2018
JOB No:	16290-04-00

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ADJUST ELECTRIC HANDHOLE
NOT TO SCALE

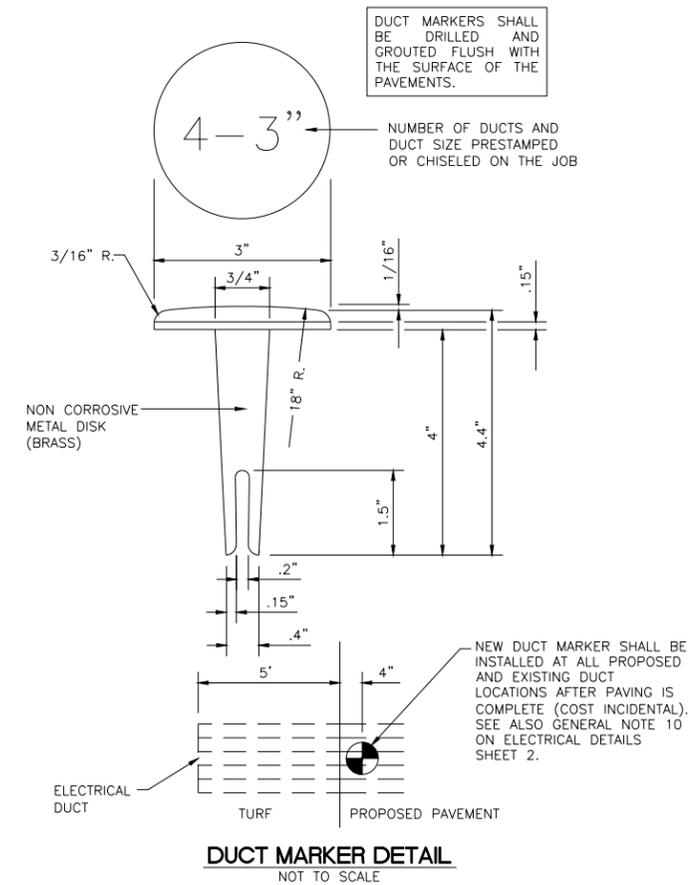


SPLICE CAN NOTES:

1. CABLES VARY PER LOCATION. SEE ELECTRICAL PLAN.
2. SOME SPLICE CANS MAY REQUIRE MORE CONDUIT STUBS THAN SHOWN IN THE DETAIL.

NOTES:

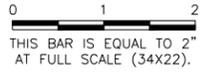
1. INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.
2. INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.



IL. CONTRACT: PA061
IL. LETTING ITEM: 10A
IL. PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

ELECTRICAL DETAILS - SHEET 2

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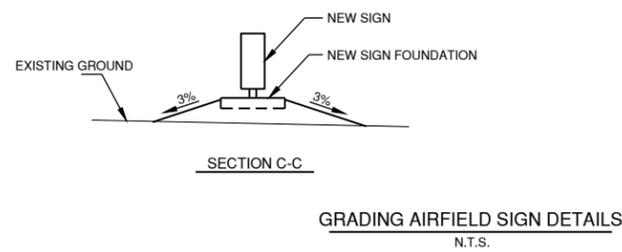
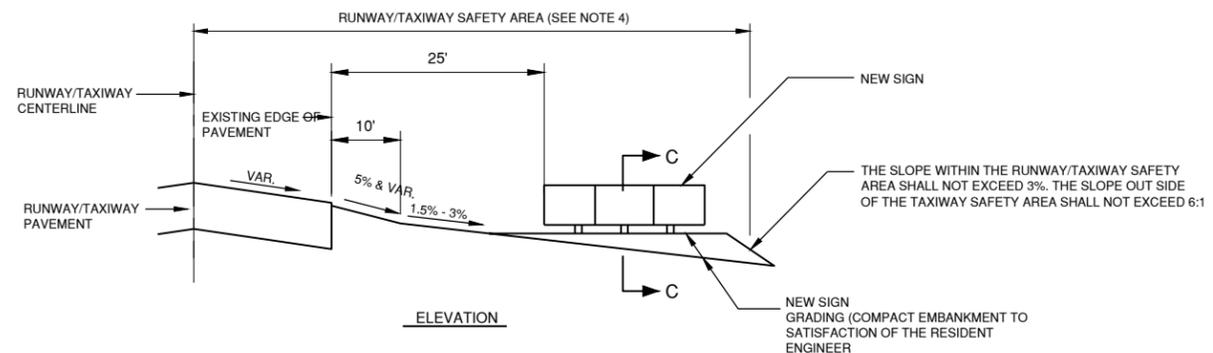
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

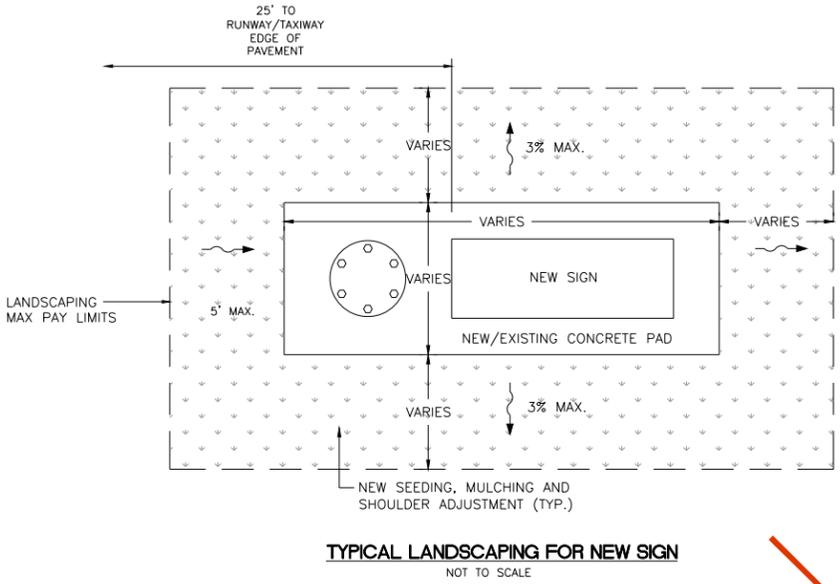
FINAL

XREF DWG: bcrnt_runway_ncs.dwg
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 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
 DATE: Friday, June 8, 2018 4:31:28 PM
 FILE: K:\Chicago\16290-04_Renobway\2-30_Draw\Sheets\electrical.dwg

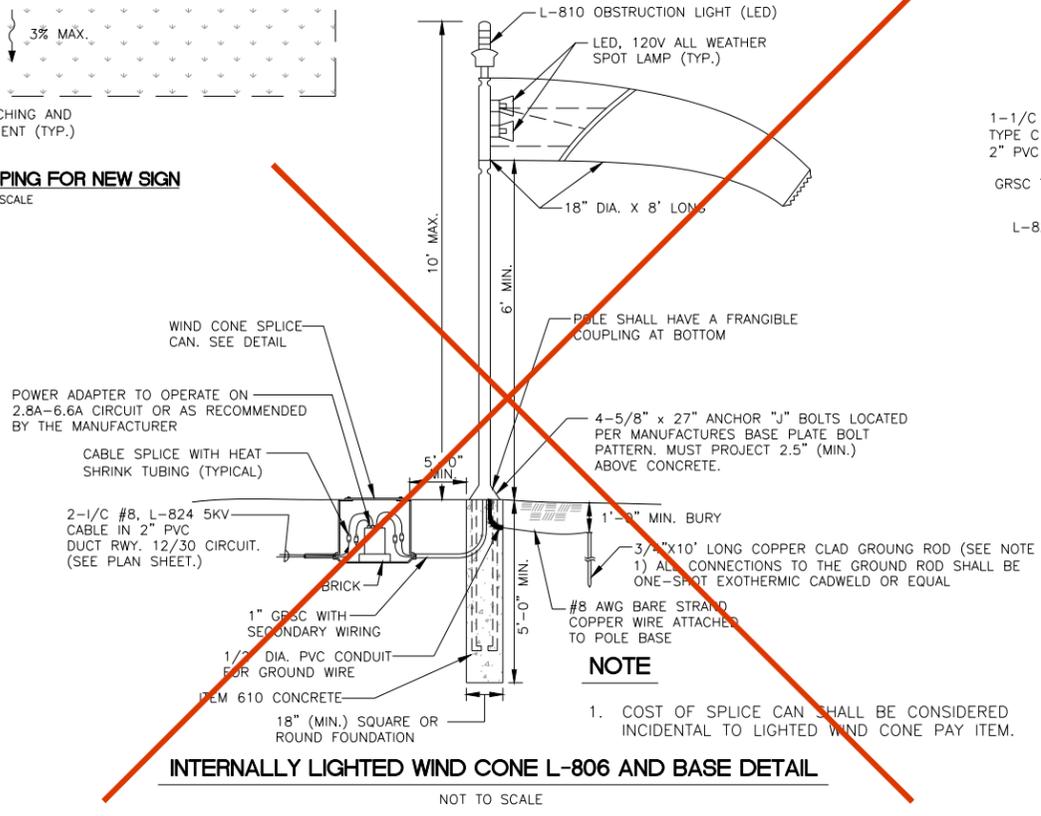


- NOTES:**
- SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES/SLOPES IN THE FIELD
 - IT IS ESTIMATED THAT 2 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT THE SIGN BASE FOUNDATIONS. ALL MATERIAL USED TO CONSTRUCT THE EMBANKMENT SHALL BE APPROVED BY THE RESIDENT ENGINEER. ALL OFFSITE MATERIAL USED TO CONSTRUCT THE EMBANKMENT SHALL BE CATEGORIZED AS CLEAN CONSTRUCTION OR DEMOLITION DEBRIS IN ACCORDANCE WITH SECTION 905-1.2 OF THE SPECIAL PROVISIONS. COSTS TO CONSTRUCT THE EMBANKMENT SHALL BE INCIDENTAL TO THE SIGN PAY ITEM. ESTIMATED QUANTITY OF EMBANKMENT IS FOR INFORMATION ONLY AND NO ADDITIONAL COMPENSATION SHALL BE MADE FOR ANY ADDITIONAL EMBANKMENT NEEDED.
 - ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
 - DIMENSIONS:**
 RUNWAY 16/34 = 250'
 RUNWAY 12/30 = 75'
 RUNWAY 6/24 = 60'
 TAXIWAY L/K = 59'
 TAXIWAY E/E1/Y = 59'

N.I.C.



N.I.C.



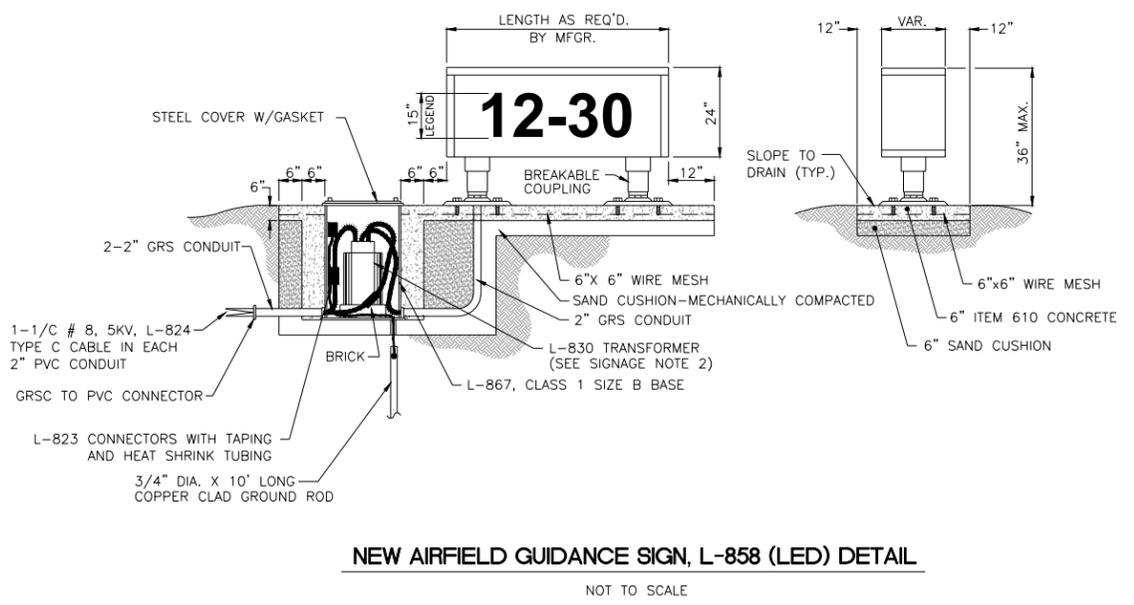
AIRFIELD SIGNAGE SCHEDULE						
SIGN NUMBER	NUMBER OF CHARACTERS	SIDE	NEW SIGN LEGEND	SIGN TYPE	SIGN LOCATION	COMMENTS
S15	14	N S		0,2 2,1,1	STA. 795+56.25, 50' LT. C TAXIWAY K	REMOVE EXISTING SIGN AND BASE NEW SIGN AND BASE
S16	13	N S		0 1,1	STA. 398+48.9, 61.1' LT. C RUNWAY 12/30	REMOVE EXISTING SIGN AND BASE NEW SIGN AND BASE
S20	14	N S		2,1,1 2,3	STA. 802+01.00, 50' LT. C TAXIWAY K	REMOVE EXISTING SIGN AND BASE NEW SIGN AND BASE
S24	2	N S		3 3	STA. 397+77, 63.7' RT. C RUNWAY 12/30	REPLACE SIGN PANELS
S25	14	E W		1,1 0	STA. 204+52.6, 53.0' RT. C RUNWAY 6/24	REMOVE EXISTING SIGN AND BASE NEW SIGN AND BASE
S41	5	F S		0,2 2,3,3	STA. 199+40, 55.0' LT. C TAXIWAY E	NEW SIGN AND BASE
S109	3	N S		0,2 3,2	STA. 10+59.4, 10.5' LT. C TAXIWAY Y	REPLACE SIGN PANELS
S114	6	N S		0,2 2,1	STA. 12+20.8, 37.4' LT. C TAXIWAY Y	REPLACE SIGN PANELS
S152	4	N S		2,1 0	STA. 1+98.5, 63.5' LT. C TAXIWAY E1	NEW SIGN AND BASE

SIGN TYPE LEGEND

- 0 BLANK PANEL
- 1 WHITE INSCRIPTION WITH BLACK BORDER ON RED BACKGROUND
- 2 YELLOW INSCRIPTION ON BLACK BACKGROUND
- 3 BLACK ON YELLOW BACKGROUND

NOTE

- GUIDANCE SIGN PANEL SIZE WILL BE BASED ON THE MANUFACTURER'S RECOMMENDATION.



SIGNAGE NOTES

- ALL SIGNS ARE 2-SIDED LED SIGNS, CLASS 2 AS MANUFACTURED BY ADB OR APPROVED EQUAL. SIGNS SHALL BE CAPABLE OF BEING CONNECTED TO A 3-STEP OR 5-STEP REGULATOR.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR LED SIGNS.
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL TO DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY THE SPECIAL PROVISIONS.
- CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION.

IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
 S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

ELECTRICAL DETAILS - SHEET 3

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
 DATE: 06/08/2018
 JOB No: 16290-04-00

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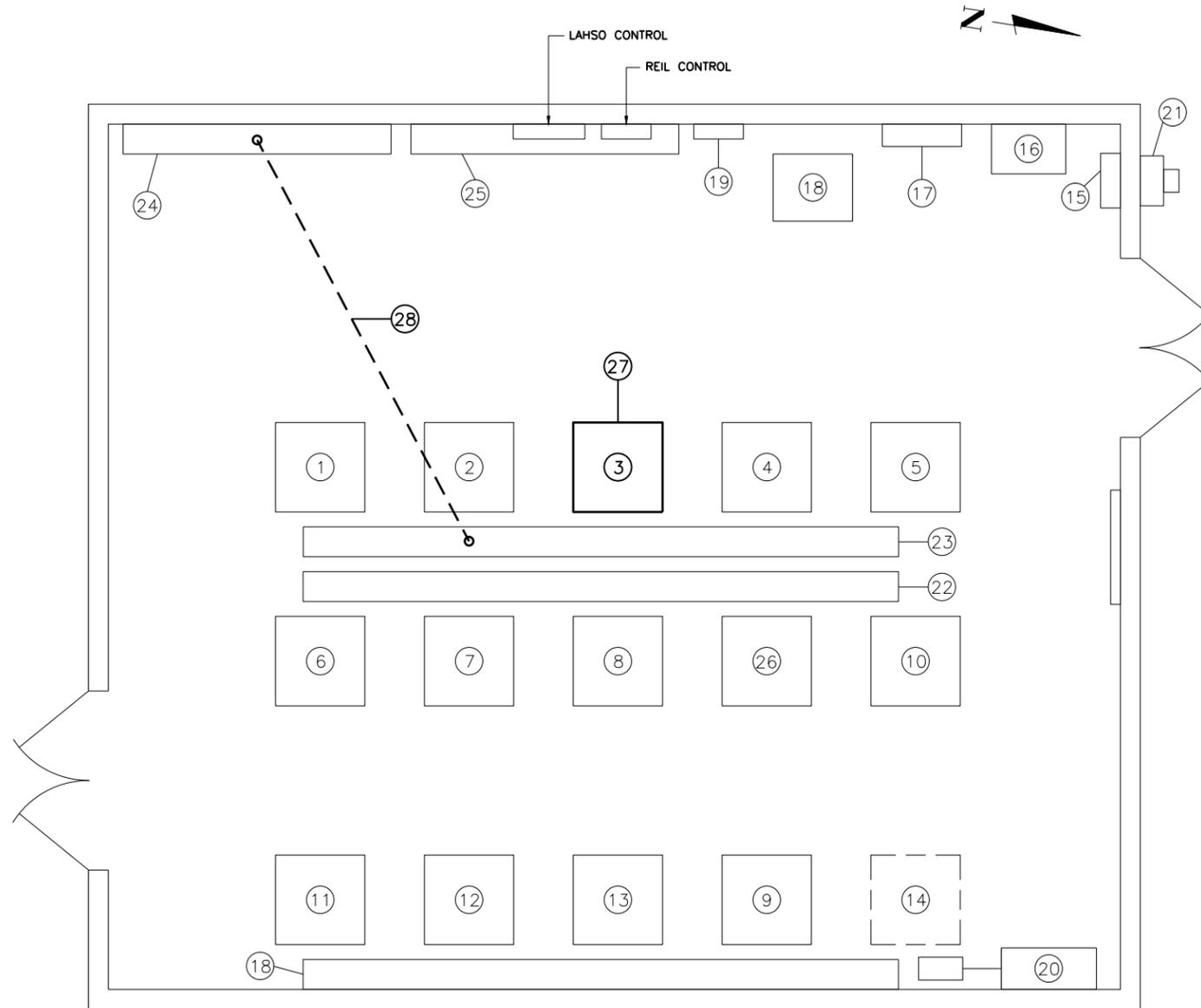
SHEET 35 OF 37 SHEETS

PROPOSED VAULT NOMENCLATURE

- ① EXISTING 30KW (5-STEP) REGULATOR FOR RUNWAY 16/34.
- ② EXISTING 20KW (5-STEP) REGULATOR FOR RUNWAY 6/24.
- ③ EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 12/30 TO BE REMOVED.**
- ④ EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY G,D AND 34 HOLD APRON.
- ⑤ EXISTING 10KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RDR.
- ⑥ EXISTING 15KW (3-STEP) REGULATOR FOR RUNWAY 16/34 RGL.
- ⑦ EXISTING 30 KW (5-STEP) REGULATOR (SPARE).
- ⑧ EXISTING 10 KW (3-STEP) REGULATOR FOR A, B, D, E, F, Y, L AND A.
- ⑨ EXISTING 30KW (5-STEP) SPARE.
- ⑩ EXISTING 20KW (3-STEP) REGULATOR FOR RUNWAY 16 APPROACH.
- ⑪ EXISTING 30 KW (5-STEP) REGULATOR FOR LIMA.
- ⑫ EXISTING 30KW (3-STEP) REGULATOR FOR TAXIWAY K.
- ⑬ EXISTING 10KW (3-STEP) REGULATOR FOR TAXIWAY C.
- ⑭ FUTURE AIRFIELD LIGHTING REGULATOR.
- ⑮ EXISTING MAIN UTILITY SERVICE CIRCUIT BREAKER DISCONNECT. 800A, 480V, 3-POLE.
- ⑯ EXISTING AUTOMATIC TRANSFER SWITCH. 800A, 480V, 3-POLE.
- ⑰ EXISTING HIGH VOLTAGE POWER DISTRIBUTION PANEL. 480V, 3-PHASE WITH 800AMP MAIN CIRCUIT BREAKER.
- ⑱ EXISTING 150KVA, 480V-280Y/120V, 3ø, 4-WIRE TRANSFORMER.
- ⑲ EXISTING LOW VOLTAGE LIGHTING PANEL. 208Y/120V, 3-PHASE WITH 400AMP MAIN CIRCUIT BREAKER.
- ⑳ EXISTING PLC CONTROL CABINET.
- ㉑ EXISTING 800AMP CT CABINET.
- ㉒ EXISTING 12"x12"x12' LONG LOW VOLTAGE WIREWAY.
- ㉓ EXISTING 12"x12"x12' LONG HIGH VOLTAGE WIREWAY.
- ㉔ EXISTING 12"x12"x9' LONG HIGH VOLTAGE WIREWAY.
- ㉕ EXISTING 12"x12"x9' LONG LOW VOLTAGE WIREWAY.
- ㉖ EXISTING 30KW (3-STEP) REGULATOR FOR HFC APRON.
- ㉗ NEW 10KW (3-STEP) REGULATOR FOR RUNWAY 12/30.**
- ㉘ INSTALL NEW 2-1/C #8 5KV, L-824 CABLES IN EXISTING CONDUIT FROM NEW 30KW SPARE REGULATOR TO HIGH VOLTAGE WIREWAY. INSTALL L-823 CONNECTORS.**

NOTES:

- 1. ALL PROPOSED WORK OR ITEMS BEING MODIFIED ARE SHOWN IN BOLD. ALL OTHER ITEMS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY.
- 2. CONTRACTOR SHALL REMOVE EXISTING 10KW REGULATOR (RUNWAY 12/30 APPROACH) AND DISPOSE OF IT OFF OF AIRPORT PROPERTY.
- 3. CONTRACTOR SHALL FURNISH AND INSTALL NEW 10KW (3-STEP) REGULATOR FOR RUNWAY 12/30 EDGE LIGHTING CIRCUIT.
- 4. CONNECT EXISTING POWER AND CONTROLS FOR RUNWAY 12/30 REGULATOR..



AIRFIELD ELECT. VAULT PLAN VIEW
1/4" = 1'-0"

IL. CONTRACT: PA061
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IL. PROJECT: PWK-4552
S.B.G. PROJECT: 3-17-SBGP-133/139/TBD

REVISIONS

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0 1 2
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CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING

VAULT IMPROVEMENTS

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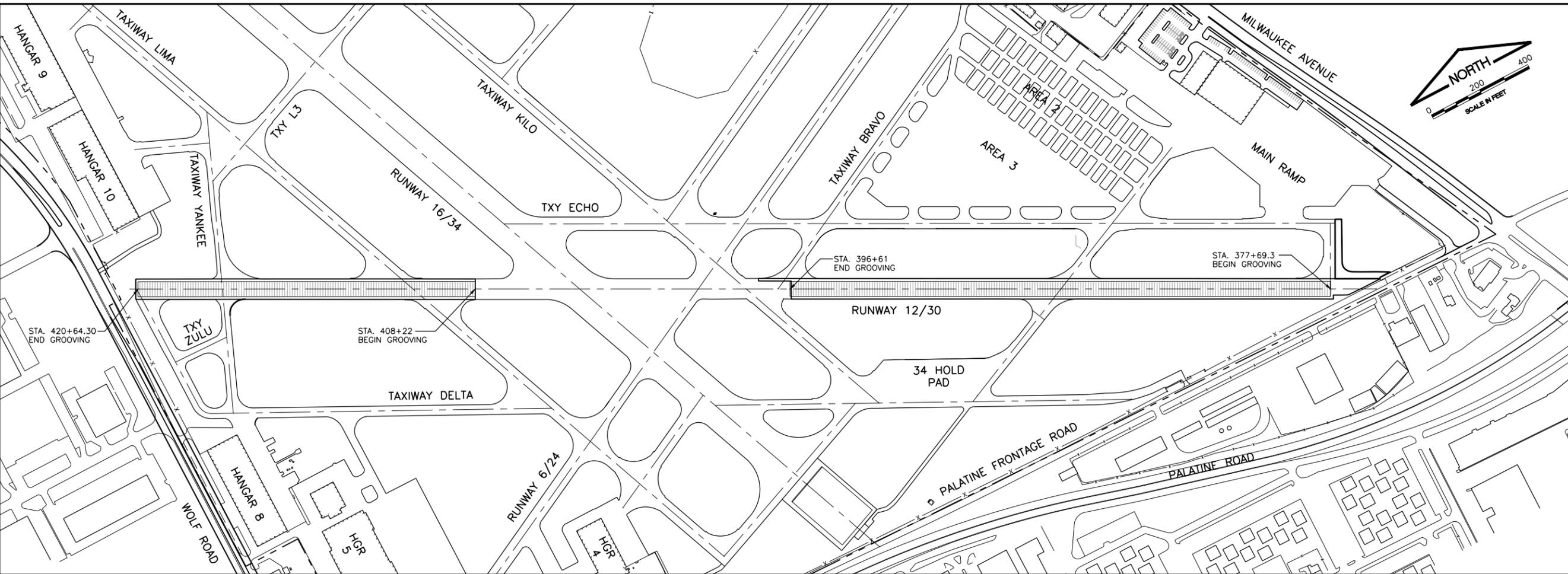
CHICAGO EXECUTIVE AIRPORT

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	06/08/2018
JOB No:	16290-04-00

FINAL

ADDITIVE ALTERNATE NO. 1

DATE: Friday, June 8, 2018 4:31:44 PM
 FILE: K:\Chicago\Exec\16290-04_Renobw12-30\Draw\Sheets\Grooving_Plan.dwg
 UPDATE BY: Kris Solvatero
 LAYOUT: Layout1
 REF DWG: 16290-04_Renobw12-30_Base-NEW.dwg
 CEA Alignments.dwg
 IMAGE FILES: BWKML000_CEA-4-Color-Small (2).jpg
 CEA Alignments.dwg



IL. CONTRACT: PA061
 IL. LETTING ITEM: 10A
 IL. PROJECT: PWK-4552
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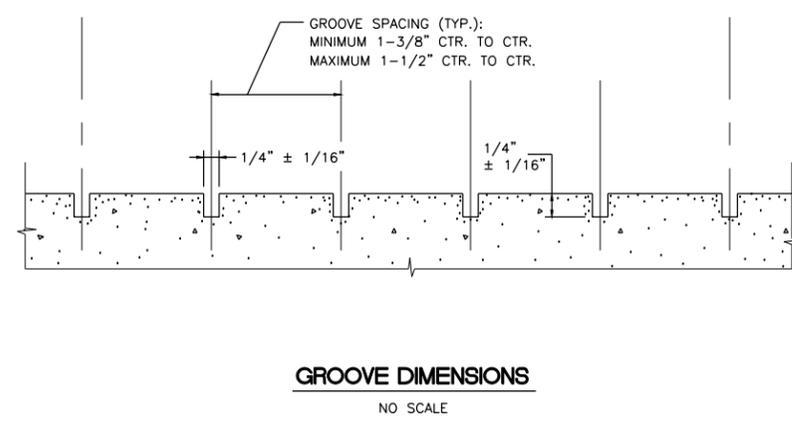
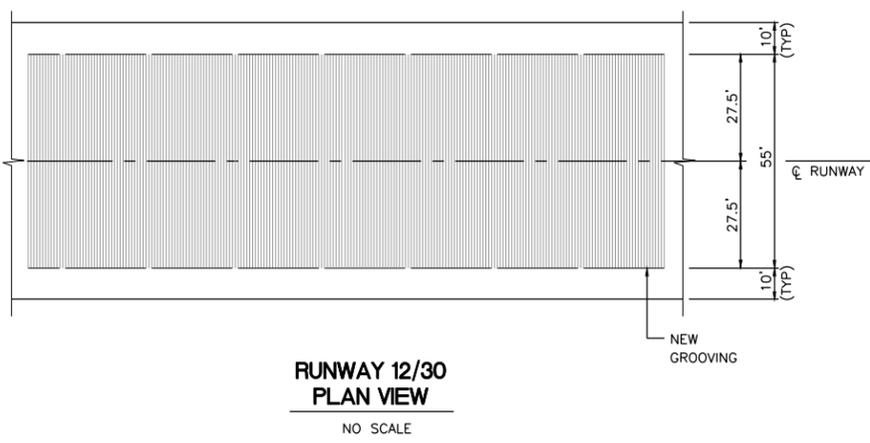
CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE RUNWAY 12/30 AND RUNWAY LIGHTING
GROOVING PLAN AND DETAILS

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CHICAGO EXECUTIVE AIRPORT

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FINAL

SHEET 37 OF 37 SHEETS



GROOVING NOTES

- AFTER COMPLETION OF HMA PAVING, RUNWAY 12/30 SHALL BE ALLOWED TO CURE A MINIMUM OF 30 DAYS PRIOR TO THE COMMENCEMENT OF GROOVING OPERATIONS.
- SEE PAVEMENT MARKING PLANS AND NOTES.
- GROOVING SHALL EXTEND OUTWARD 27.5' FROM CENTERLINE OF RUNWAY 12/30.
- SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
- EXTREME CARE SHALL BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES AND SUBSURFACE WIRING. GROOVES SHALL BE SAWS NO CLOSER THAN 6" AND NO MORE THAN 18" TO SUCH FACILITIES.
- CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- AT THE CONTRACTOR'S OPTION, SLURRY MAY BE STORED ON SITE AT A LOCATION WITHIN THE CONTRACTOR STAGING AND STORAGE AREA(S). THE CONTRACTOR SHALL BE ALLOWED TO CONSTRUCT A TEMPORARY WASHOUT PIT IN ACCORDANCE WITH STANDARD PRACTICES. A WASHOUT PIT SHALL BE ALLOWED TO BE CONSTRUCTED AT EACH STAGING AREA. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE THE SLURRY FOR PROPER FINAL DISPOSAL OFF AIRPORT PROPERTY WITHIN TWO BUSINESS DAYS TO PREVENT WATERFOWL FROM BEING ATTRACTED TO THE AREA. NO ADDITIONAL COMPENSATION FOR THE EXCAVATION AND RESTORATION OF THE WASHOUT PIT(S) SHALL BE ALLOWED.