

FAP 347 (IL Route 38)
Contract Number 60I22
Kane & DuPage Counties

IDOT PROJECT LABOR AGREEMENT DETERMINATION

Item 64

To: Ann L. Schneider, Secretary

From: William R. Frey, Interim Director of Highways



Date: November 22, 2011

Re: FAP 347 - Contract Number 60I22 - Kane & Dupage Counties

~~{Letting: January 20, 2012, Completion Date: October 31, 2013}~~

April 27, 2012

In accordance with Executive Order 2003-13 (Blagojevich), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds). *See Attachment A*
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances. *See Attachment A.*
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time. *See Attachment A.*
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project

FAP 347 (IL Route 38)
Contract Number 60122
Kane & DuPage Counties
Item 136

that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

8) This project presents specific safety concerns to the traveling public and a PLA, will ensure labor force continuity and stability, decreasing the length of the safety concern. *See Attachment A.*

9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project.

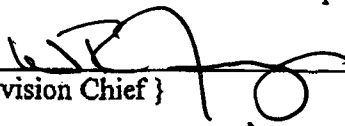
10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process.

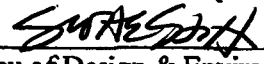
11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.).


12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:
See Attachment A.


FAP 347 (IL Route 38)
Contract Number 60I22
Kane & DuPage Counties

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees.

Agreed:  12/4/11
{Division Chief } (Date)

Agreed:  12/5/11
{Bureau of Design & Environment} (Date)

Agreed:  12.5.11
{Regional Engineer} (Date)

Approved:  12/6/11
Ann L. Schneider, Secretary (Date)

FHWA concurrence in the PLA for the above mentioned contract.

<u>Gregory G. Nadeau</u>	<u>11/22/2011</u>
FHWA Deputy Administrator	(see attached approval page)

FAP 347 (IL Route 38)
Contract Number 60I22
Kane & DuPage Counties

Attachment A:

Justification for use of a Project Labor Agreement for Contract No. 60I22 within Kane and DuPage Counties.

The use of a Project Labor Agreement on this project is consistent with all State and Local statutory and regulatory requirements.

Item 2: This project is federally funded.

Item 3: The estimate project cost is \$32,863,000.

Any disruption in the continuity of this project due to labor issues would result in delayed deadlines that would cause safety concerns for the traveling public. Lapsing trade contracts during the first stage of the project would be addressed through the use of a Project Labor Agreement. It is the Illinois Department of Transportation's finding that the large skilled workforce needed for this project can be provided by union trades involved and is a necessary requirement for a Project Labor Agreement.

Item 4: The project is being stages over two construction seasons.

The proposed scope of work includes widen and reconstruct Roosevelt Road – Illinois Route 38 (F.A.P. 347), on a new alignment, from approximately 1000' east of Kirk Road to 1500' east of Technology Boulevard. The improvement will provide 2 lanes in each direction, with a 30 foot barrier median and pavement bounded by curb and gutter on both sides. The length of the project is approximately 1.63 miles. The Union Pacific Railroad currently crosses IL Route 38 at grade, at a skewed angle, just east of the signalized intersection of Kautz Road. The improvement will provide a new grade separation and bridge structure (proposed Structure No. 045-0079) to carry Illinois Route 38 and Kautz Road over the Union Pacific Railroad. The proposed improvement will also place a tee intersection of Kautz Road with Illinois Route 38 on the new structure. A signalized intersection will be incorporated at this location. The new structure will have a total length of 229'-7 1/8" back to back of abutments along the local tangent. The structure's width will be approximately 214'-0" and on a skew of 50 degrees to proposed IL Route 38. The structure will be a single span steel girder bridge with an 8" composite concrete deck and a span length of 166'-5 1/2" measured along the girders.

During Stage 1, the bridge structure, mechanically stabilized earth (MSE) walls, and new alignment of IL Route 38 will be built. Vehicular traffic of IL Route 38 will be kept open and will be maintained on the existing pavement, providing two lanes in each direction through the work zone. Kautz Road will also be kept open and vehicular traffic will be maintained on the existing pavement, providing one lane in each direction. Earth

excavation and embankment will be placed in preparation for Stage 3 work of Kautz Road.

During Stage 2, vehicular traffic of Illinois Route 38 will be reduced to one lane in each direction utilizing the existing two westbound lanes. The existing raised median will be removed and temporary pavement is to be placed. The eastbound lanes of Illinois Route 38 will be reconstructed to meet the new alignment. Kautz Road will be kept open and vehicular traffic will be maintained on the existing pavement, providing one lane in each direction.

During Stage 3, vehicular traffic of Illinois Route 38 will be reduced to one lane in each direction utilizing the existing two eastbound lanes and the new alignment. The westbound lanes of Illinois Route 38 will be reconstructed to meet the new alignment. During this stage, Kautz Road will be reconstructed on a new alignment and to meet the new grade change. Kautz Road will be closed to through traffic during reconstruction and a detour is provided.

The completion date for the project is October 31, 2013.

Item 8: Any disruption to the contractor's schedule due to labor issues may result in additional hardship on the traveling public. The corridor already experiences severe congestion due to the at-grade railroad crossing of the Union Pacific. IL Route 38 is a major east-west route through Kane and DuPage Counties and handles approximately 22,000 to 35,000 vehicles per day. Any work delay during stage 2 will be noticed more since Illinois Route 38 will be reduced to one lane in each direction.

As with any project under construction, some motorists will choose to find an alternate route to avoid driving in a construction zone. Due to the limited network of east-west routes in this area, there are no easily accessible, uncongested alternate routes. Therefore, additional volume added to adjacent routes raises concerns for safety and delay on those routes as well as Illinois Route 38.

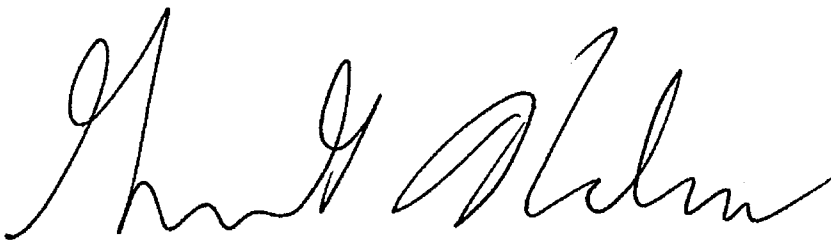
Item 12: The project extends through two construction seasons. The trades that are expected to be used on this project include but are not limited to be the following: Laborers, Carpenters, Operators, Teamsters, Cement Masons, Electricians and Ironworkers. Any work stoppage due to expiring union contracts will have major impacts to the construction schedule and the timely completion of the project.

PLA Request

Approval of Project Labor Agreement

Disapproval of Project Labor Agreement

Reason for disapproval:

A handwritten signature in black ink, appearing to read "Anthony Adams", written over a horizontal line.

Signature

11/22/11

Date

Execution Page

Illinois Department of Transportation

William R. Frey *AW*

William R. Frey, P.E., Interim Director of Highways

Matthew R. Hughes *T. dm*

Matthew R. Hughes, Director Finance & Administration

Ellen Scharzle-Haskins

Ellen Scharzle-Haskins, Chief Counsel

Ann L. Schneider *2*

Ann L. Schneider, Secretary

Susan Wolfsberger

4/11/12
(Date)

By Susan Wolfsberger,
AA II

Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the local unions listed below:

Michael J. Parigian

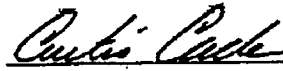
March 28, 2012

(Date)

List Union Locals:



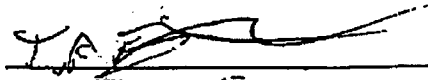
Jim Allen
Bricklayers



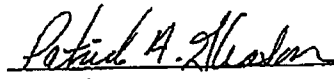
Curtis Cade
United Association

*

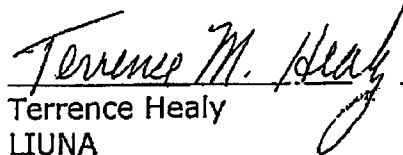
Ed Christensen, Elevator
Constructors



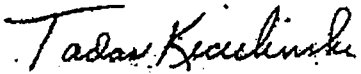
Terry Fitzmaurice
Painters



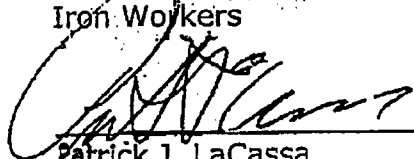
Pat Gleason
Teamsters



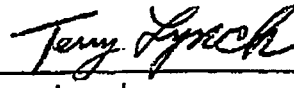
Terrence Healy
LIUNA



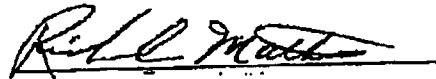
Tadas Kiciulinski
Iron Workers



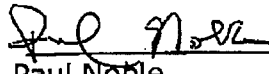
Patrick J. LaCassa
OPCMIA



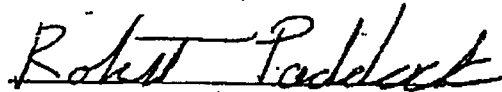
Terry Lynch
Heat & Frost Insulators & Allied
Workers




Richard Mathis
Roofers



Paul Noble
IBEW



Robert Paddock
IUOE



Gary Perinar Jr.
Carpenters



Robert Schneider
Sheet Metal Workers



John Skermont
Boilermakers

*only if Elevator Constructors master agreement
language is attached to PLA