

WAUKEGAN PORT DISTRICT WAUKEGAN, LAKE COUNTY, ILLINOIS

FINAL CONSTRUCTION PLANS FOR WAUKEGAN NATIONAL AIRPORT

REHABILITATE RUNWAY 5-23

ILLINOIS PROJECT: UGN-4655
S.B.G. PROJECT: 3-17-SBGP-XX

APRIL 19, 2019

811 Know what's below.
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J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
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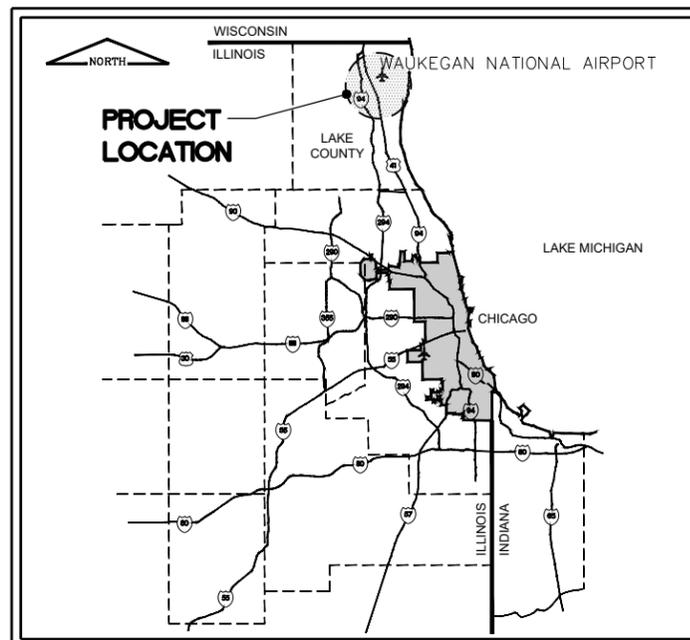
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

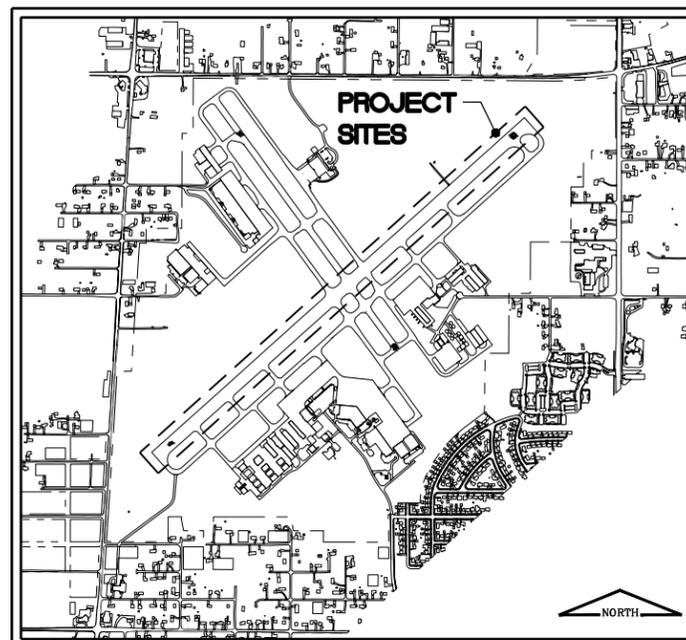


WAUKEGAN NATIONAL AIRPORT
WAUKEGAN PORT DISTRICT
2601 Plane Rest Drive
Waukegan, Illinois 60087
Telephone: 847.244.0055
Fax: 847.244.3813

APPROVED BY *SKIP GOSS*
SKIP GOSS, AIRPORT MANAGER
DATE _____ APRIL 19, _____ 2019



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E. BEFORE
EXCAVATING AT 811
WAUKEGAN NATIONAL AIRPORT

TOWNSHIP: 46 NORTH
RANGE: 12 EAST
SECTION: 31 AND 32
COUNTY: LAKE
TOWNSHIP: BENTON

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY C
DESIGN AIRCRAFT GROUP III (GULFSTREAM 450)

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180226-03



SUBMITTED BY *Matthew N. Demos, P.E.*
MATTHEW N. DEMOS, P.E.
DATE _____ APRIL 19, _____ 2019

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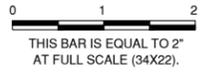
SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR152480	SHOULDER ADJUSTMENT	SY	50	
AR201663	SAND MIX CRACK REPAIR	LF	300	
AR401610	BITUMINOUS SURFACE COURSE	TON	8,700	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
A4401640	BITUMINOUS PAVEMENT GROOVING	SY	86,390	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	100,005	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	125	
AR603510	BITUMINOUS TACK COAT	GAL	12,000	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	105,200	

IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

REVISIONS

NUMBER	BY	DATE



**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

**INDEX TO SHEETS/
 SUMMARY OF QUANTITIES**

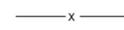


DESIGN BY: MND
 DRAWN BY: JRO
 CHECKED BY: MND
 APPROVED BY: MND
 DATE: 4/19/2019
 JOB No: 180226-03

Waukegan Airport Base 2013.dwg
Waukegan Airport Local.dwg
Waukegan Airport_Base_alignments.dwg

PROJECT REFERENCE POINTS			
NO.	NORTHING	EASTING	STATION / LOCATION
1	2094483.68	1107871.09	STA. 200+25.00 CL RUNWAY 5-23 STA. 97+50.13 CL TAXIWAY A1
2	2094867.40	1108299.33	STA. 206+00.00 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A2
3	2095217.75	1108690.33	STA. 211+25.00 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A3
4	2095597.53	1109114.17	STA. 216+94.10 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A4
5	2096373.23	1109979.88	STA. 228+56.50 CL RUNWAY 5-23 STA. 10+00.00 CL TAXIWAY B
6	2096541.53	1110167.71	STA. 231+08.70 CL RUNWAY 5-23 STA. 127+64.62 CL RUNWAY 14-32
7	2096709.03	1110354.64	STA. 233+59.70 CL RUNWAY 5-23 STA. 10+00.00 CL TAXIWAY C
8	2097075.53	1110763.66	STA. 239+08.90 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A5
9	2097521.31	1111261.16	STA. 245+76.90 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A6
10	2098300.82	1112131.12	STA. 257+45.00 CL RUNWAY 5-23 STA. 0+00.00 CL TAXIWAY A7
11	2098454.24	1112302.34	STA. 259+74.90 CL RUNWAY 5-23 STA. 162+50.13 CL TAXIWAY A8
12	2098471.18	1112321.24	STA. 260+00.28 CL RUNWAY 5-23
13	2094466.99	1107852.47	STA. 200+00.00 CL RUNWAY 5-23

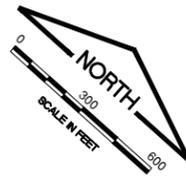
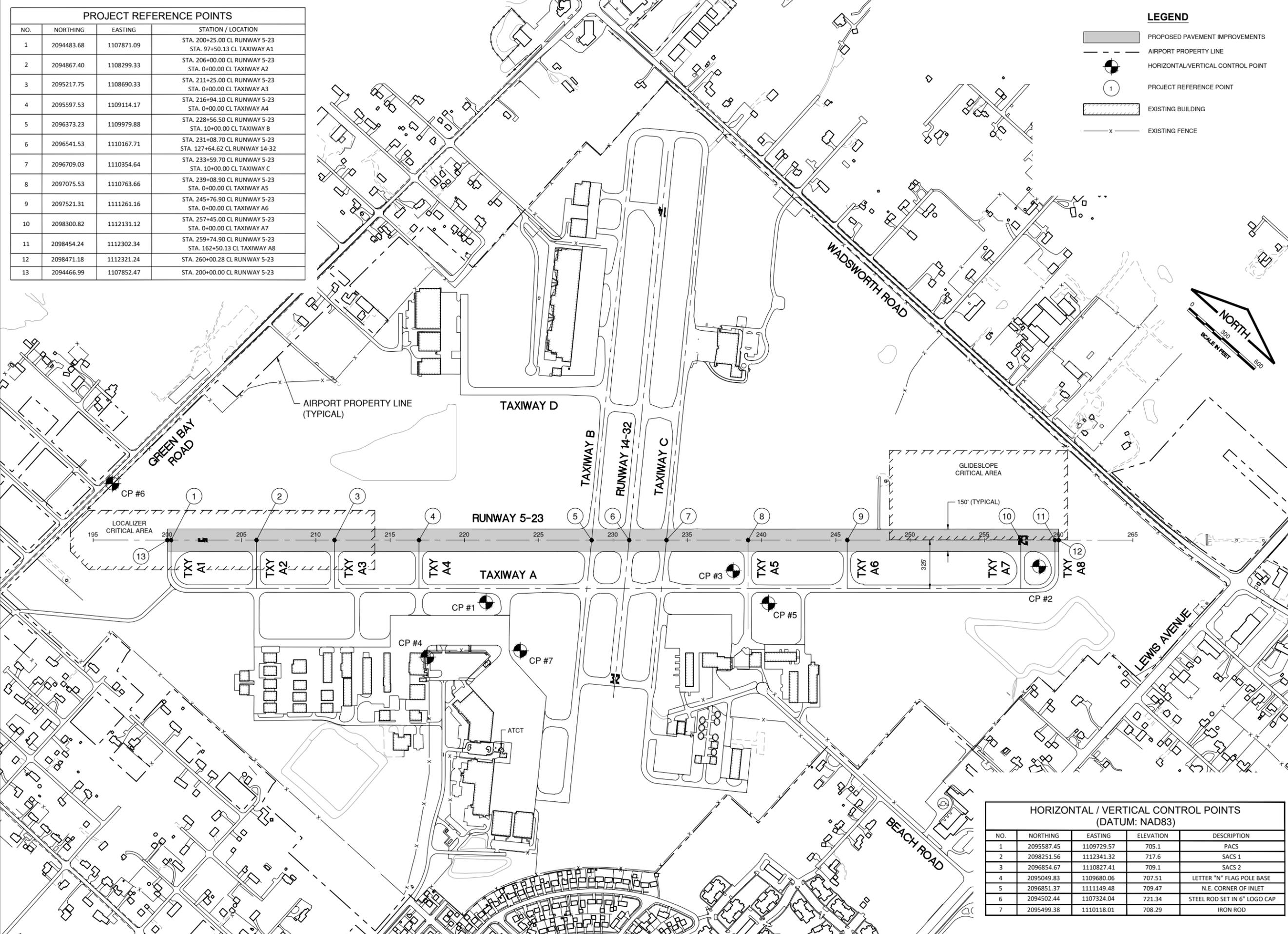
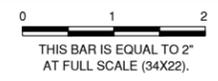
LEGEND

-  PROPOSED PAVEMENT IMPROVEMENTS
-  AIRPORT PROPERTY LINE
-  HORIZONTAL/VERTICAL CONTROL POINT
-  PROJECT REFERENCE POINT
-  EXISTING BUILDING
-  EXISTING FENCE

IL. CONTRACT: **WA074**
IL. LETTING ITEM: **08A**
IL. PROJECT: **UGN-4655**
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REVISIONS

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**WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23
SITE PLAN / PROJECT CONTROL PLAN**

HORIZONTAL / VERTICAL CONTROL POINTS (DATUM: NAD83)				
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	2095587.45	1109729.57	705.1	PACS
2	2098251.56	1112341.32	717.6	SACS 1
3	2096854.67	1110827.41	709.1	SACS 2
4	2095049.83	1109680.06	707.51	LETTER "N" FLAG POLE BASE
5	2096851.37	1111149.48	709.47	N.E. CORNER OF INLET
6	2094502.44	1107324.04	721.34	STEEL ROD SET IN 6" LOGO CAP
7	2095499.38	1110118.01	708.29	IRON ROD

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DRAWN BY: JRO
CHECKED BY: MND
APPROVED BY: MND
DATE: 4/19/2019
JOB No: 180226-03

DATE: Wednesday, June 26, 2019 10:06:43 AM
FILE: K:\Waukegan\18226-03\00_Dimaps\2-51\Project\18226-03_Site Plan.dwg

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 86 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.
3. WORK AREAS AND DESCRIPTIONS

MOBILIZATION

COMPLETE MOBILIZATION DURING THE MOBILIZATION TIME PERIOD.

BITUMINOUS TEST STRIP

AN AREA LOCATED ON RUNWAY 5-23 WILL BE DEDICATED FOR USE FOR A BITUMINOUS TEST STRIP. THIS WORK WILL INCLUDE, BUT NOT LIMITED TO, BITUMINOUS MILLING AND REPLACEMENT.

WORK AREA 1 - ALL RUNWAYS CLOSED

WORK AREA 1 GENERALLY INCLUDES BITUMINOUS MILLING, PARTIAL DEPTH PAVEMENT REPAIRS, CRACK REPAIRS, BITUMINOUS PAVING, AND PAVEMENT MARKING (FIRST COAT) WITHIN THE RSA OF RUNWAY 5-23.

WORK AREA 2A

WORK AREA 2A GENERALLY INCLUDES BITUMINOUS GROOVING AND SECOND COAT OF PAVEMENT MARKING WITHIN THE RSA OF RUNWAY 5-23, WEST OF THE TOFA OF TAXIWAY B.

WORK AREA 2B

WORK AREA 2B GENERALLY INCLUDES BITUMINOUS GROOVING, SECOND COAT OF PAVEMENT MARKING AND SHOULDER ADJUSTMENTS WITHIN THE RSA OF RUNWAY 5-23 AND RUNWAY 14-32, EAST OF THE WORK AREA 2A LIMITS.

4. RESTRICTIONS

GENERAL (ALL WORK AREAS)

ALL WORK AREAS WITHIN AN RSA OR TOFA MUST BE RESTORED AND INSPECTED TO MEET FAA CRITERIA, INCLUDING TEMPORARY BITUMINOUS TAPERS AND PER THE CONTRACTOR'S CSPP, PRIOR TO RETURNING THE AREA TO AIR TRAFFIC OPERATIONS. SEE CSPP NOTES IN THE PLANS FOR ADDITIONAL INFORMATION ON INSPECTION REQUIREMENTS.

ALL WORK AREAS ARE LOCATED INSIDE THE AOA SECURITY FENCE, AND ARE SUBJECT TO THE LIMITATIONS DESCRIBED IN SPECIFICATIONS, PART 1 - GENERAL PROVISIONS.

WORK CONDUCTED UTILIZING EXTENDED CLOSURES AS INDICATED IN THE PLANS WILL ALLOW THE CONTRACTOR TO CONDUCT WORK 24 HOURS A DAY, 7 DAYS A WEEK OR UNLESS SPECIFICALLY NOTED FOR THE RESTRICTIONS IN EACH WORK AREA. THE CLOSURE WILL ALLOW THE CONTRACTOR TO COMPLETE ALL WORK INSIDE THE RESPECTIVE TOFA OR RSA, AND TO A DISTANCE OUTSIDE THE TOFA OR RSA AS NOT TO ENCR OACH THE TOFA OR RSA WITH MANPOWER OR EQUIPMENT ONCE THE CLOSURE PERIOD IS CONCLUDED.

SIMULTANEOUS CLOSURES OF AIRFIELD PAVEMENTS TO COMPLETE WORK IN MORE THAN ONE WORK AREA AT A TIME WILL NOT BE ALLOWED EXCEPT AS SPECIFICALLY NOTED. ALL WORK WITHIN A WORK AREA MUST BE SUBSTANTIALLY COMPLETED AND ACCEPTED PRIOR TO CLOSING ANY OTHER AIRFIELD PAVEMENT FOR WORK IN REMAINING ASSOCIATED WORK AREAS.

BITUMINOUS TEST STRIP AND PRE-CONSTRUCTION FIELD WORK

THE BITUMINOUS TEST STRIP AND PRE-CONSTRUCTION FIELD WORK MUST BE COMPLETED AND ACCEPTED PRIOR TO CLOSING AIRFIELD PAVEMENTS FOR PRODUCTION WORK IN ALL WORK AREAS.

THE SCHEDULED RUNWAY 5-23 CLOSURE FOR THIS WORK WILL BE LIMITED TO 1 WEEKEND ONLY, STARTING AT 8:00 PM ON FRIDAY UNTIL 12:00 NOON THE FOLLOWING SUNDAY, BEFORE RE-OPENING RUNWAY 5-23 TO AIRCRAFT OPERATIONS. RUNWAY 14-32 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THIS PERIOD.

WORK AREA 1 - ALL RUNWAYS CLOSED

AN EXTENDED SIMULTANEOUS CLOSURE OF RUNWAY 5-23 AND RUNWAY 14-32 IS REQUIRED TO COMPLETE WORK WITHIN THE RSA OF BOTH RUNWAYS, EXCLUDING GROOVING AND SECOND COAT OF PAVEMENT MARKING. SCHEDULED RUNWAY AND TAXIWAY CLOSURES FOR THIS AREA WILL BE LIMITED TO 1 WEEK STARTING AT 6:00 AM ON MONDAY UNTIL 6:00 AM THE FOLLOWING SATURDAY MORNING, BEFORE RE-OPEJNING THE RUNWAYS TO AIRCRAFT OPERATIONS.

WORK AREA 2A

AN EXTENDED CLOSURE OF RUNWAY 5-23 IS REQUIRED TO COMPLETE WORK WITHIN THE RSA OF THE RUNWAY.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO GROOVING OPERATIONS AND SECOND COAT OF PAVEMENT MARKING. ALL PAVEMENT GRADE CORRECTIONS AND SMOOTHNESS REMEDIES MUST BE COMPLETED AND ACCEPTED BY THE ENGINEER PRIOR TO GROOVING.

SCHEDULED RUNWAY CLOSURES FOR BITUMINOUS GROOVING AND SECOND COAT OF PAVEMENT MARKING IN THIS AREA WILL BE LIMITED TO 1 WEEKEND ONLY, STARTING AT 8:00 PM ON FRIDAY UNTIL 12:00 NOON THE FOLLOWING SUNDAY, BEFORE RE-OPENING RUNWAY 5-23 TO AIRCRAFT OPERATIONS. RUNWAY 14-32 WILL CONTINUALLY BE OPEN FOR AIRCRAFT OPERATIONS DURING THIS PERIOD.

WORK WITHIN WORK AREA 2A AND 2B CAN NOT BE COMPLETED CONCURRENTLY.

WORK AREA 2B

AN EXTENDED CLOSURE OF RUNWAY 5-23 AND RUNWAY 14-32 IS REQUIRED TO COMPLETE THE WORK WITHIN THE RSA OF BOTH RUNWAYS.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO GROOVING OPERATIONS AND SECOND COAT OF PAVEMENT MARKING. ALL PAVEMENT GRADE CORRECTIONS AND SMOOTHNESS REMEDIES MUST BE COMPLETED AND ACCEPTED BY THE ENGINEER PRIOR TO GROOVING.

THE CONTRACTOR MUST ALLOW ALL NEW BITUMINOUS PAVEMENTS TO CURE A MINIMUM OF 30 DAYS PRIOR TO GROOVING OPERATIONS AND SECOND COAT OF PAVEMENT MARKING. ALL PAVEMENT GRADE CORRECTIONS AND SMOOTHNESS REMEDIES MUST BE COMPLETED AND ACCEPTED BY THE ENGINEER PRIOR TO GROOVING.

SCHEDULED RUNWAY CLOSURES FOR BITUMINOUS GROOVING, SECOND COAT OF PAVEMENT MARKING AND SHOULDER ADJUSTMENTS IN THIS AREA WILL BE LIMITED TO 1 WEEKEND ONLY, STARTING AT 8:00 PM ON FRIDAY UNTIL 12:00 NOON THE FOLLOWING SUNDAY, BEFORE RE-OPENING BOTH RUNWAYS FOR AIRCRAFT OPERATIONS.

WORK WITHIN WORK AREA 2A AND 2B CAN NOT BE COMPLETED CONCURRENTLY.

5. DURATIONS AND MILESTONES:

MILESTONE 1

SUBSTANTIALLY COMPLETE MOBILIZATION, INCLUDING BITUMINOUS TEST STRIP AND ANY REQUIRED PRE-CONSTRUCTION FIELD WORK WITHIN 30 CALENDAR DAYS FROM THE NOTICE TO PROCEED.

MILESTONE 2

SUBSTANTIALLY COMPLETE WORK WITHIN WORK AREA 1, EXCLUDING BITUMINOUS GROOVING AND SECOND COAT OF PAVEMENT MARKING, WITHIN 43 CALENDAR DAYS FROM THE NOTICE TO PROCEED.

MILESTONE 3

SUBSTANTIALLY COMPLETE WORK IN WORK AREAS 2A AND 2B, INCLUDING BITUMINOUS GROOVING, SECOND COAT OF PAVEMENT MARKING AND SHOULDER ADJUSTMENTS, WITHIN 86 CALENDAR DAYS FROM THE NOTICE TO PROCEED.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. NAVAIDS THAT COULD BE AFFECTED

- 1. THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
2. EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
3. EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
4. PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
5. IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE(S) SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE WAUKEGAN AIR TRAFFIC CONTROL TOWER GROUND CONTROL (121.65 MHZ) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
7. THE CONTRACTORS STORAGE AND STAGING AREAS WILL BE AS SHOWN ON THE SITE PLAN.
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREAS.
10. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

IL CONTRACT: WA074

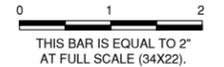
IL LETTING ITEM: 08A

IL PROJECT: UGN-4655

S.B.G. PROJECT: 3-17-SBGP-XX

REVISIONS

Table with 3 columns: NUMBER, BY, DATE



WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23

CONSTRUCTION ACTIVITY PLAN NOTES
SHEET 1 OF 2

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DESIGN BY: MND

DRAWN BY: JRO

CHECKED BY: MND

APPROVED BY: MND

DATE: 4/19/2019

JOB No: 180226-03

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.
3. THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.
4. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 22', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
2. BECAUSE OF THE CRITICAL LIMITATIONS WHICH THE CLOSURE OF A RUNWAY PLACES ON THE USERS OF THE AIRPORT, LIQUIDATED DAMAGES IN THE AMOUNT OF **\$2,500** MAY BE DEDUCTED FROM THE MONIES OWED THE CONTRACTOR. FOR EACH HALF HOUR OVER THE CLOSURE PERIODS FOR THE BITUMINOUS TEST STRIP, WORK AREA 1, WORK AREA 2A AND WORK AREA 2B UNTIL THE SPECIFIED WORK IS COMPLETED AND THE ASSOCIATED RUNWAY IS OPENED TO AIRCRAFT OPERATIONS. DAMAGES MAY BE ASSESSED STARTING IMMEDIATELY AFTER EACH OF THE CLOSURE PERIODS WHEN EITHER RUNWAY IS NOT OPENED. THESE ASSESSED DAMAGES ARE IN ADDITION TO THE DEDUCTIONS FOR EACH DAY OF OVERRUN IN TOTAL CONTRACT TIME AS DEFINED IN SECTION 80, PROSECUTION AND PROGRESS, OF THE CONTRACT DOCUMENTS. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS FOR MORE INFORMATION ON RUNWAY AND TAXIWAY CLOSURES.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

16. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 22'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. UPON COMPLETION OF THE PROJECT, THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE RETURNED TO THE AIRPORT. THE CONTRACTOR SHALL SUPPLY TWO PORTABLE CLOSED RUNWAY CLOSURE MARKERS FOR THE OTHER RUNWAY.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

1. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
2. LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

1. ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE RUNWAY TO BE CLOSED.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

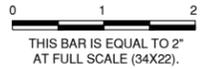
19. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

REVISIONS

NUMBER	BY	DATE



**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

**CONSTRUCTION ACTIVITY PLAN NOTES
 SHEET 2 OF 2**

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DATE:	4/19/2019
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FAA CRITICAL POINT TABLE								
POINT	WORK AREA	NORTHING	EASTING	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT (FT)	ABOVE GROUND ELEVATION
1	1, 2A	2094586.4174	1107611.3089	42°24'58.26"	87°52'35.39"	722	22	744
2	1, 2A	2094214.0701	1107945.0116	42°24'54.57"	87°52'30.97"	722	22	744
3	1, 2A	2096513.8121	1109762.3084	42°25'17.19"	87°52'06.58"	707	22	729
4	1, 2A	2096107.8512	1110058.4979	42°25'13.16"	87°52'02.66"	708	22	730
5	1, 2B	2098723.8291	1112228.7184	42°25'38.88"	87°51'33.53"	718	22	740
6	1, 2B	2098351.4818	1112562.4211	42°25'35.19"	87°51'29.11"	713	22	735
7	ALL	2093564.3345	1107816.7396	42°24'48.16"	87°52'32.73"	728	22	750
8	ALL	2093564.0514	1107966.7433	42°24'48.15"	87°52'30.73"	728	22	750
9	ALL	2098293.9293	1112943.6987	42°25'34.60"	87°51'24.03"	697	22	719
10	ALL	2098554.0493	1113339.2264	42°25'37.15"	87°51'18.74"	691	22	713

NAVAID FACILITY STATUS DURING CONSTRUCTION			
ELEMENT	WORK AREA 1	WORK AREA 2A	WORK AREA 2B
RUNWAY 23 LOCALIZER/DME	OTS	OTS	OTS
RUNWAY 23 MALSR	OTS	OTS	OTS
RUNWAY 5 VASI	OTS	OTS	OTS
RUNWAY 23 VASI	OTS	OTS	OTS
RUNWAY 14 VASI	OTS	ACTIVE	OTS
RUNWAY 23 GLIDESLOPE ANTENNA	OTS	OTS	OTS
AIRFIELD ROTATING BEACON	ACTIVE	ACTIVE	ACTIVE

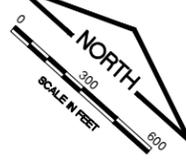
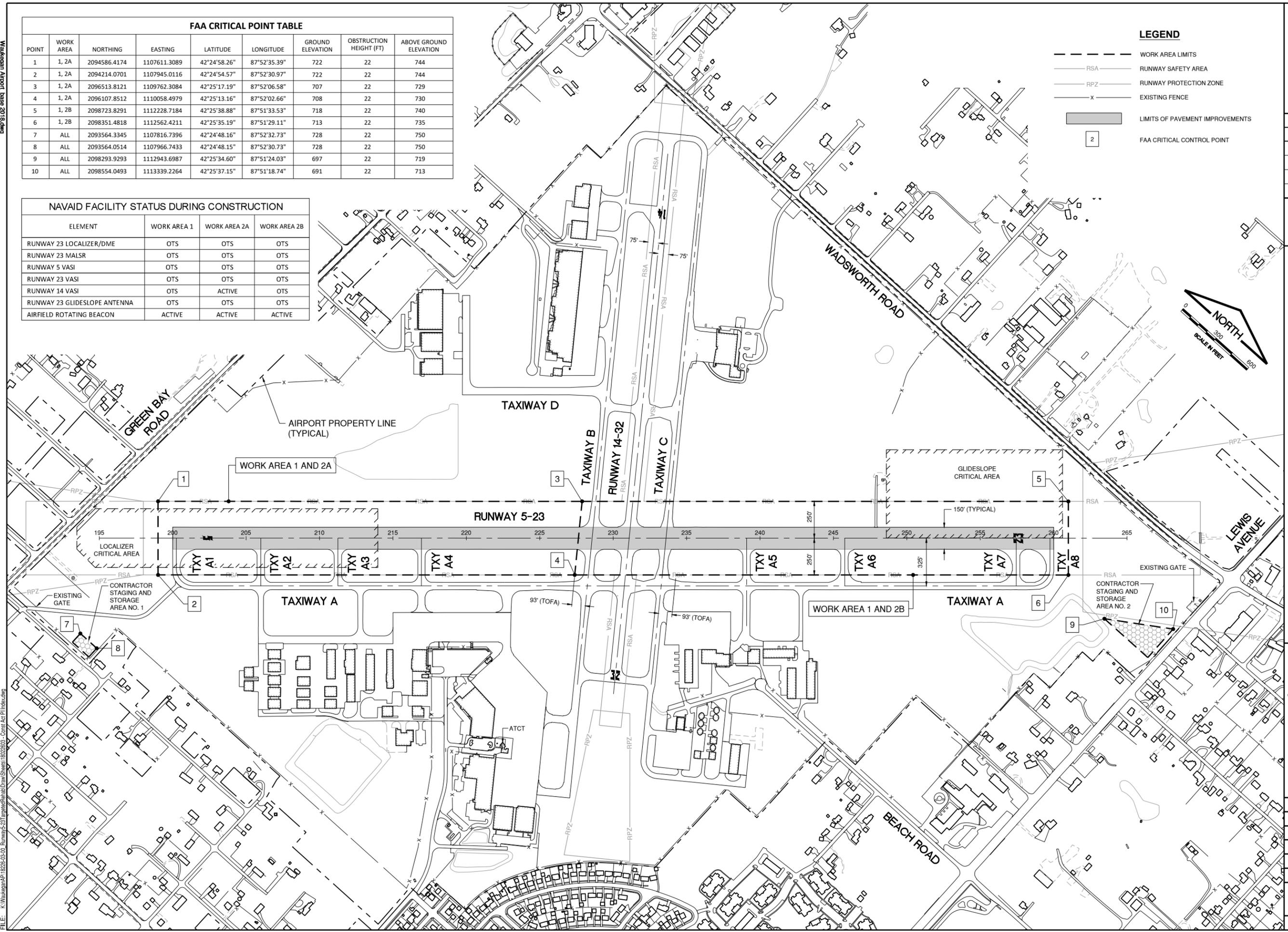
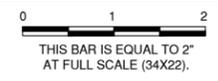
LEGEND

- WORK AREA LIMITS
- RSA RUNWAY SAFETY AREA
- RPZ RUNWAY PROTECTION ZONE
- EXISTING FENCE
- LIMITS OF PAVEMENT IMPROVEMENTS
- FAA CRITICAL CONTROL POINT

IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

REVISIONS

NUMBER	BY	DATE



**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23
 CONSTRUCTION ACTIVITY PLAN INDEX**

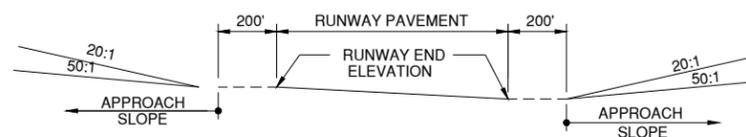
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CLOSED RUNWAY MARKER NOTES

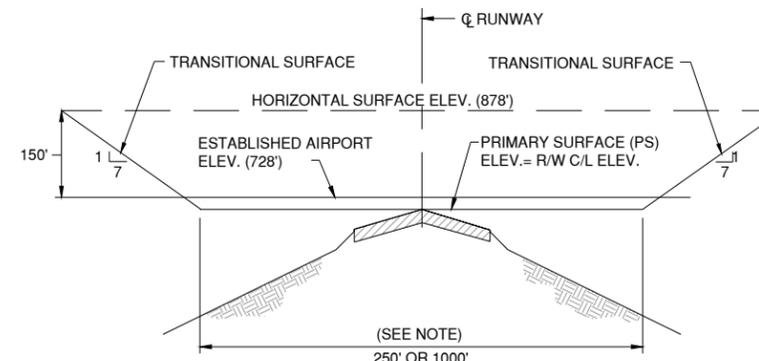
1. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL.
2. FOR RUNWAY CLOSURES INVOLVING A SINGLE RUNWAY, IT IS ANTICIPATED THAT THE AIRPORT SHALL MOBILIZE THE AIRPORT OWNED LIGHTED "X'S ON EACH END OF THE CLOSED RUNWAY. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN THE LIGHTED "X'S DURING EACH CLOSURE PERIOD. CONTRACTOR SUPPLIED MAINTENANCE SHALL INCLUDE, BUT NOT BE LIMITED TO, FUELING, REPLACING LAMPS, CHECKING AND SUPPLYING OIL, ETC. IF ANY DAMAGE IS INCURRED TO THE AIRPORT LIGHTED "X'S DURING CONSTRUCTION BY THE CONTRACTOR, THE CONTRACTOR SHALL REPLACE THE LIGHTED "X" IN KIND AT NO COST TO THE CONTRACT OR AIRPORT.
3. FOR RUNWAY CLOSURES INVOLVING MORE THAN ONE RUNWAY, OR IF THE AIRPORT OWNED LIGHTED "X'S ARE NOT AVAILABLE, THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING YELLOW CROSSES AT THE LOCATION AND DIMENSIONS DETAILED ON THE SEQUENCE OF CONSTRUCTION AND PER AC 150/5370-2 (LATEST EDITION). THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
4. TEMPORARY CLOSED RUNWAY MARKERS SHALL BE YELLOW.
5. TEMPORARY MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
6. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
7. MARKERS AND LIGHTED "X'S ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN. LIGHTED "X'S SHALL FACE THE APPROACH OF EACH RUNWAY END.
8. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
5	724.7	34:1
23	723.1	50:1
14	727.6	20:1
32	712.1	20:1

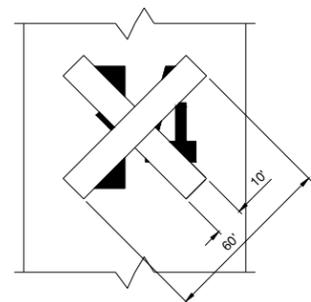


TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

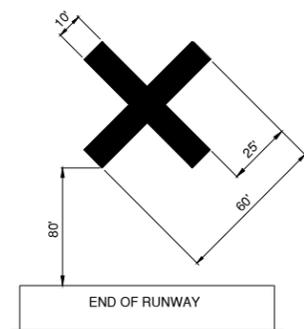
NOTE:

IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY
 RUNWAY 14-32 250' PS (125' LT & RT OF CENTERLINE)
 RUNWAY 5-23 1000' PS (500' LT & RT OF CENTERLINE)



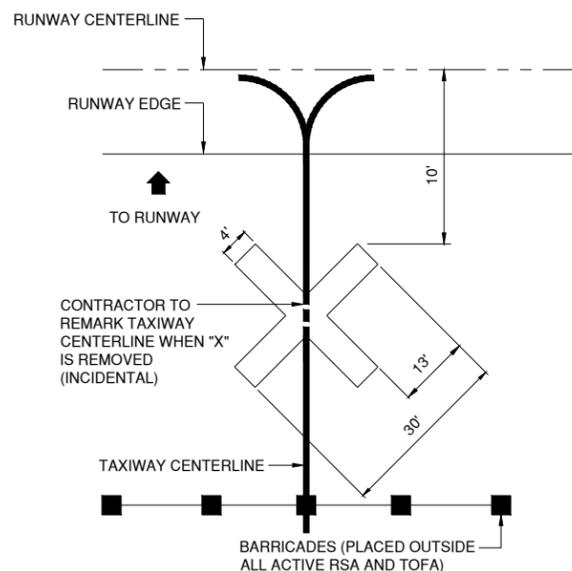
TEMPORARY CLOSED RUNWAY MARKER DETAIL

ON PAVEMENT - NO SCALE



OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL

NO SCALE

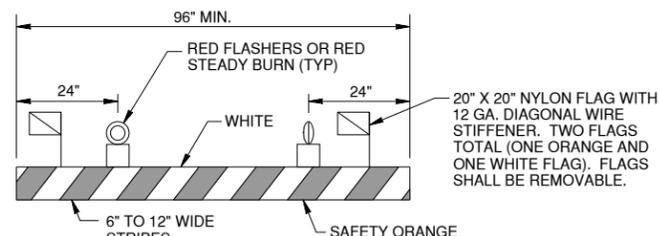


CLOSED TAXIWAY MARKER DETAIL

NOT TO SCALE



PLAN VIEW



SIDE VIEW

AIRSIDE LOW PROFILE LIGHTED BARRICADE

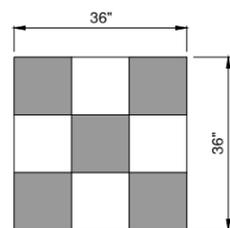
NOT TO SCALE

BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

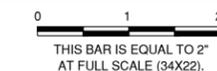
AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

REVISIONS

NUMBER	BY	DATE



THIS BAR IS EQUAL TO 2' AT FULL SCALE (34X22).

**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

CONSTRUCTION ACTIVITY PLAN DETAILS

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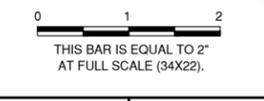


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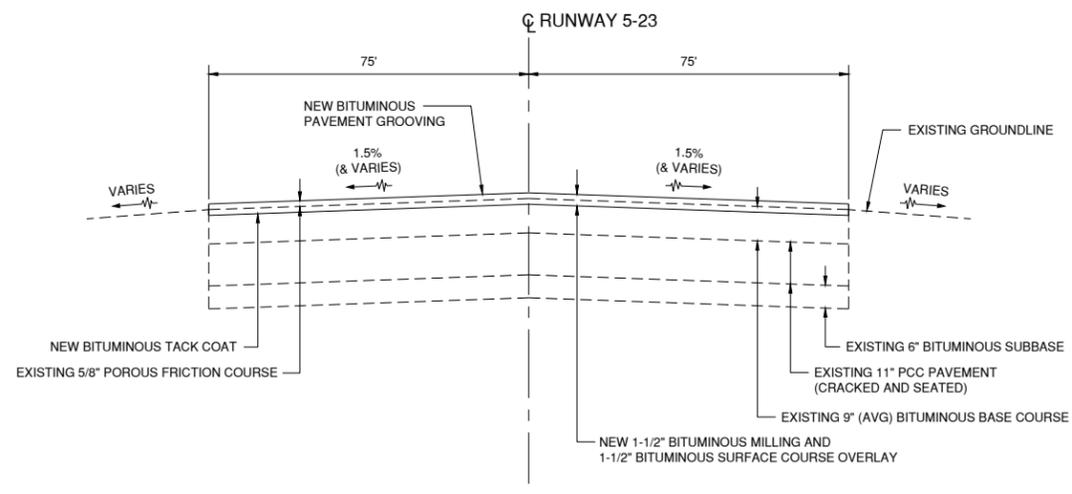
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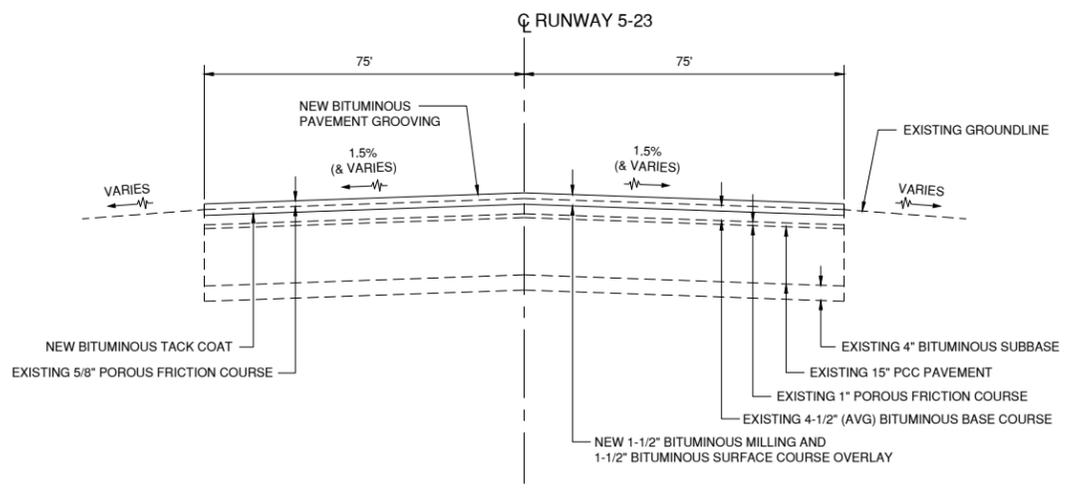


**WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23**

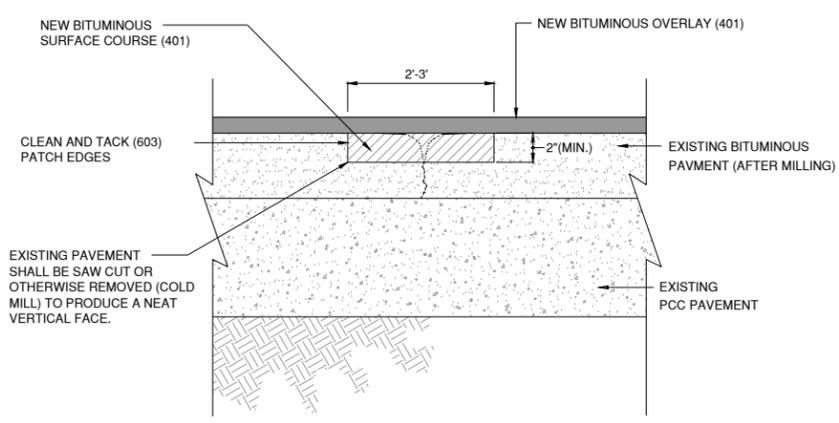
TYPICAL SECTIONS



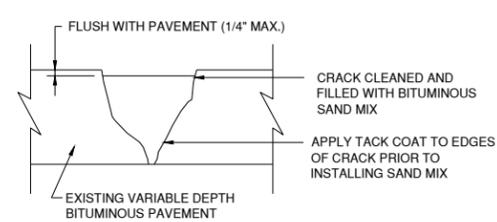
**RUNWAY 5-23 TYPICAL SECTION A-A
(STA. 200+00 TO 246+00)**
NOT TO SCALE



**RUNWAY 5-23 TYPICAL SECTION B-B
(STA. 246+00 TO 260+00)**
NOT TO SCALE



**REMOVE AND REPLACE
BITUMINOUS PAVEMENT TYPE A**
NOT TO SCALE



HMA SAND MIX CRACK REPAIR DETAIL
NOT TO SCALE

CRACK REPAIR NOTES:

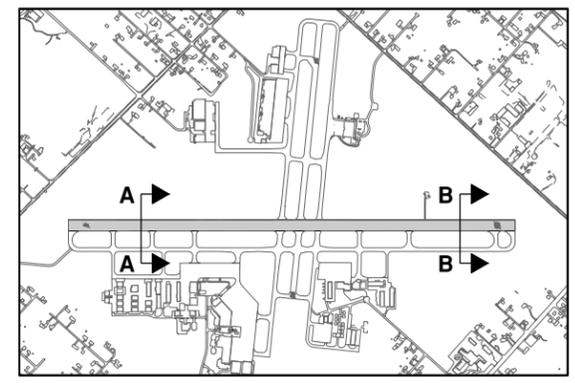
1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1\"/>

PAVEMENT REMOVAL AND REPLACEMENT NOTES

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORD DRAWINGS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. THIS WORK SHALL BE DETERMINED IN THE FIELD WITH THE AIRPORT/ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.

NOTES

1. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF THE EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
2. CONSTRUCT TRANSITION AT THE END OF EACH DAYS PAVING IN ACCORDANCE WITH THE TEMPORARY BITUMINOUS TRANSITION RAMP DETAIL. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRANSITION RAMP AS THE NUMBER AND SIZE OF THE TRANSITIONS ARE DEPENDANT ON THE CONTRACTOR SCHEDULING AND PRODUCTION RATES.
3. CONTRACTOR SHALL EXERCISE EXTREME CARE WHEN MILLING NEAR IN-PAVEMENT LIGHT CANS AND UNDERDRAIN INSPECTION HOLES TO AVOID DAMAGE TO THE LIGHT CANS AND INSPECTION HOLES TO REMAIN. DAMAGED ITEMS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.



KEY MAP


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WAUKEGAN NATIONAL AIRPORT

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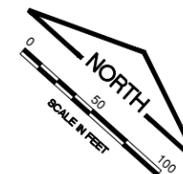
LEGEND

-  PROPOSED 1-1/2" BITUMINOUS PAVEMENT MILLING AND NEW 1-1/2" BITUMINOUS SURFACE COURSE (401)
-  EXISTING DRAINAGE STRUCTURE
-  EXISTING END SECTION
-  EXISTING AIRFIELD CABLE
-  EXISTING CONDUIT/DUCT BANK (NUMBER OF WAYS INDICATED)
-  ST EXISTING STORM SEWER
-  UD EXISTING UNDERDRAIN
-  W EXISTING WATERMAIN
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING AIRFIELD GUIDANCE SIGN
-  EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT
-  EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT
-  EXISTING STAKE MOUNTED RUNWAY EDGE LIGHT
-  EXISTING BASE MOUNTED RUNWAY EDGE LIGHT
-  EXISTING UNDERDRAIN INSPECTION HOLE
-  EXISTING VASI
-  EXISTING WIND CONE

NOTES

1. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
2. THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AFTER INSPECTION OF THE MILLED SURFACE LOCATION SHOWN OR APPROXIMATE ONLY.
3. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.

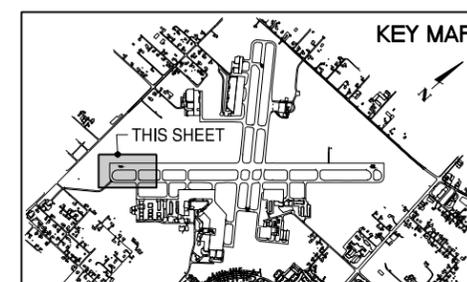
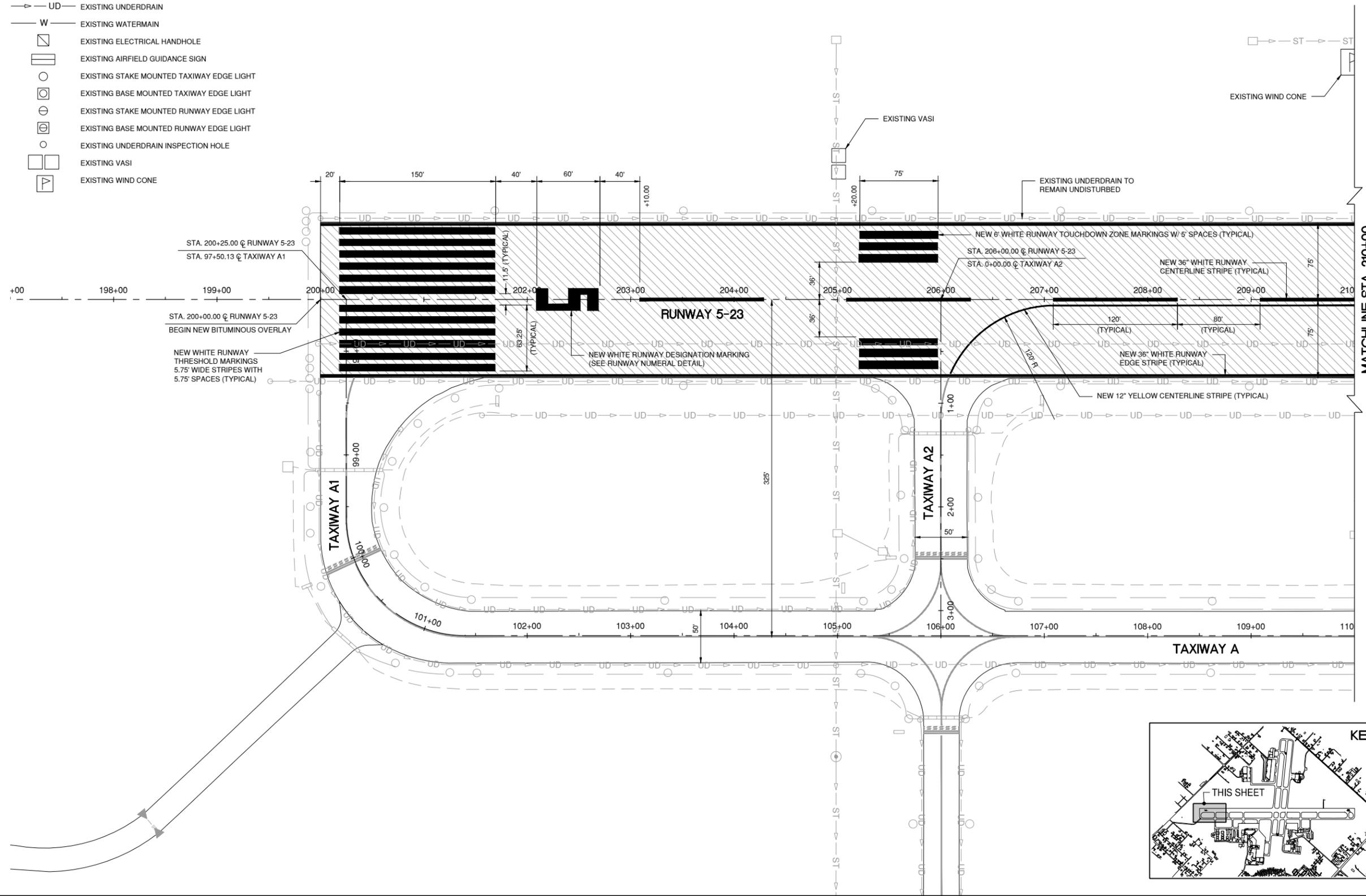
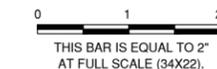
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IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23

PROPOSED IMPROVEMENTS
SHEET 1 OF 5

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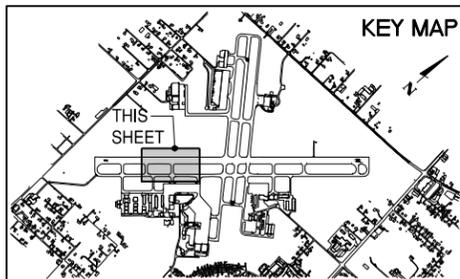


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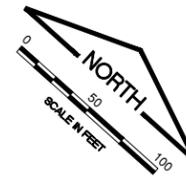
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CHECKED BY:	MND
APPROVED BY:	MND
DATE:	4/19/2019
JOB No:	180226-03

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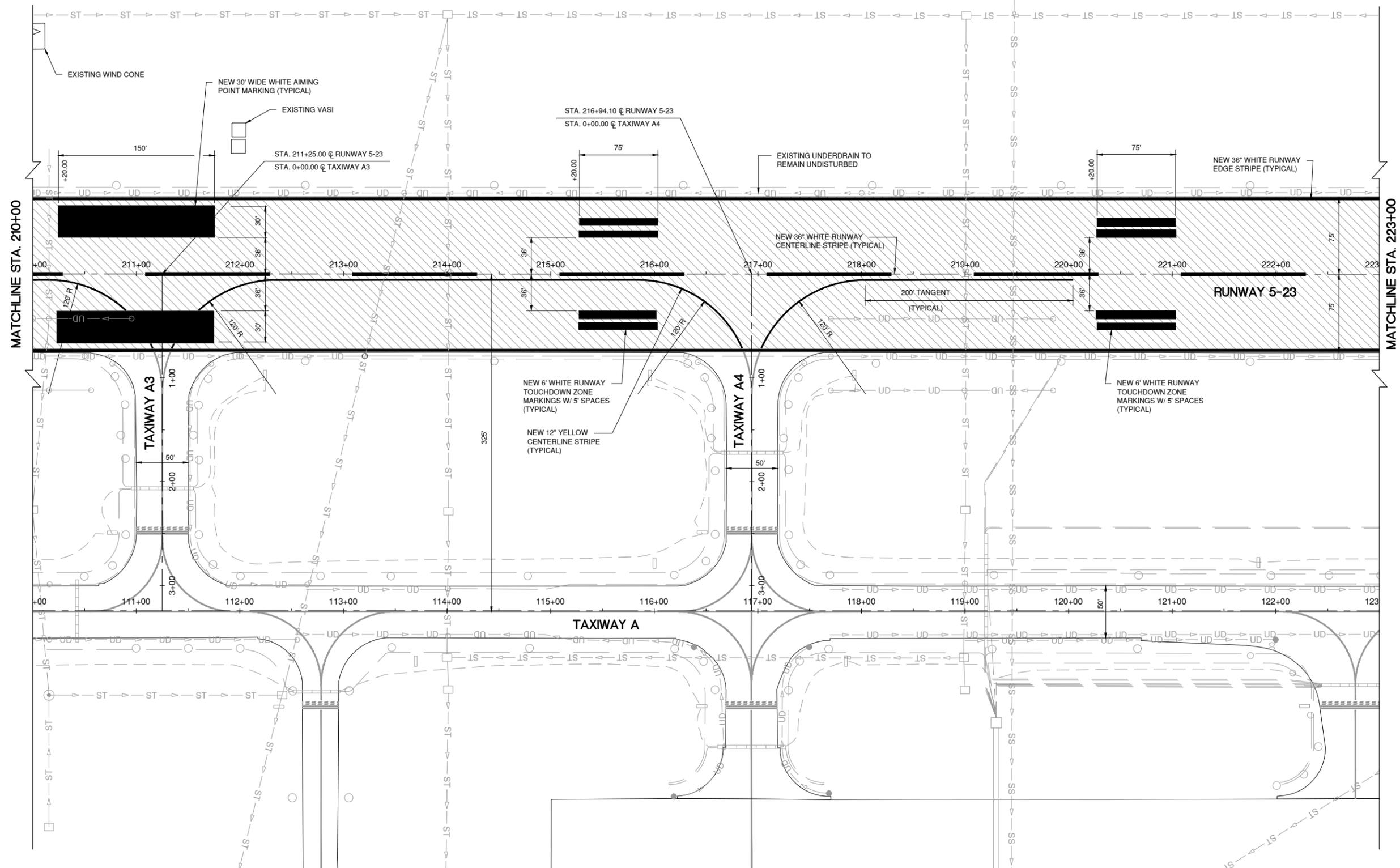
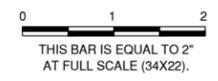
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SEE PROPOSED IMPROVEMENTS SHEET 1 FOR LEGEND AND NOTES



IL. CONTRACT: **WA074**
 IL. LETTING ITEM: **08A**
 IL. PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

REVISIONS		
NUMBER	BY	DATE



**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

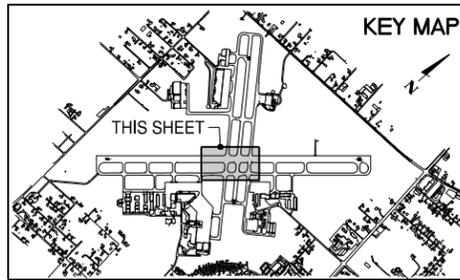
**PROPOSED IMPROVEMENTS
 SHEET 2 OF 5**

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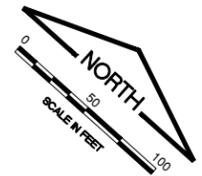
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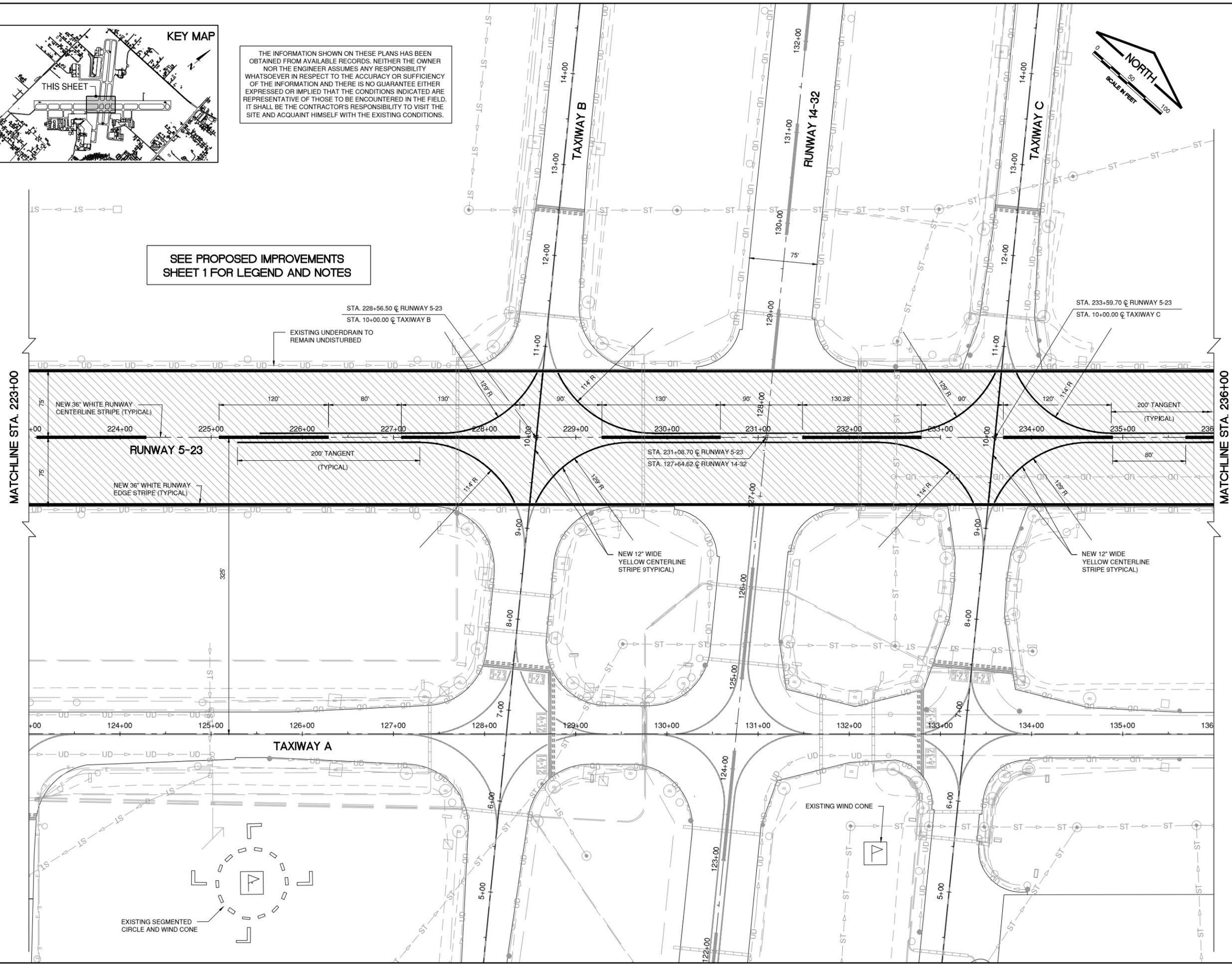
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 Waukegan Airport Base 2019.dwg



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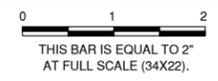


SEE PROPOSED IMPROVEMENTS SHEET 1 FOR LEGEND AND NOTES



IL CONTRACT: **WA074**
 IL LETTING ITEM: **08A**
 IL PROJECT: **UGN-4655**
 S.B.G. PROJECT: **3-17-SBGP-XX**

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**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

**PROPOSED IMPROVEMENTS
 SHEET 3 OF 5**

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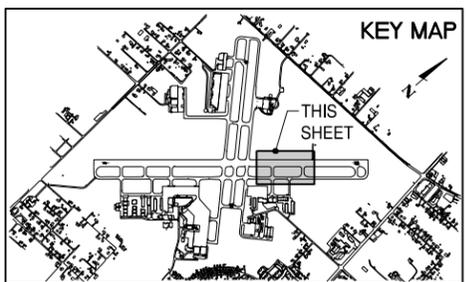
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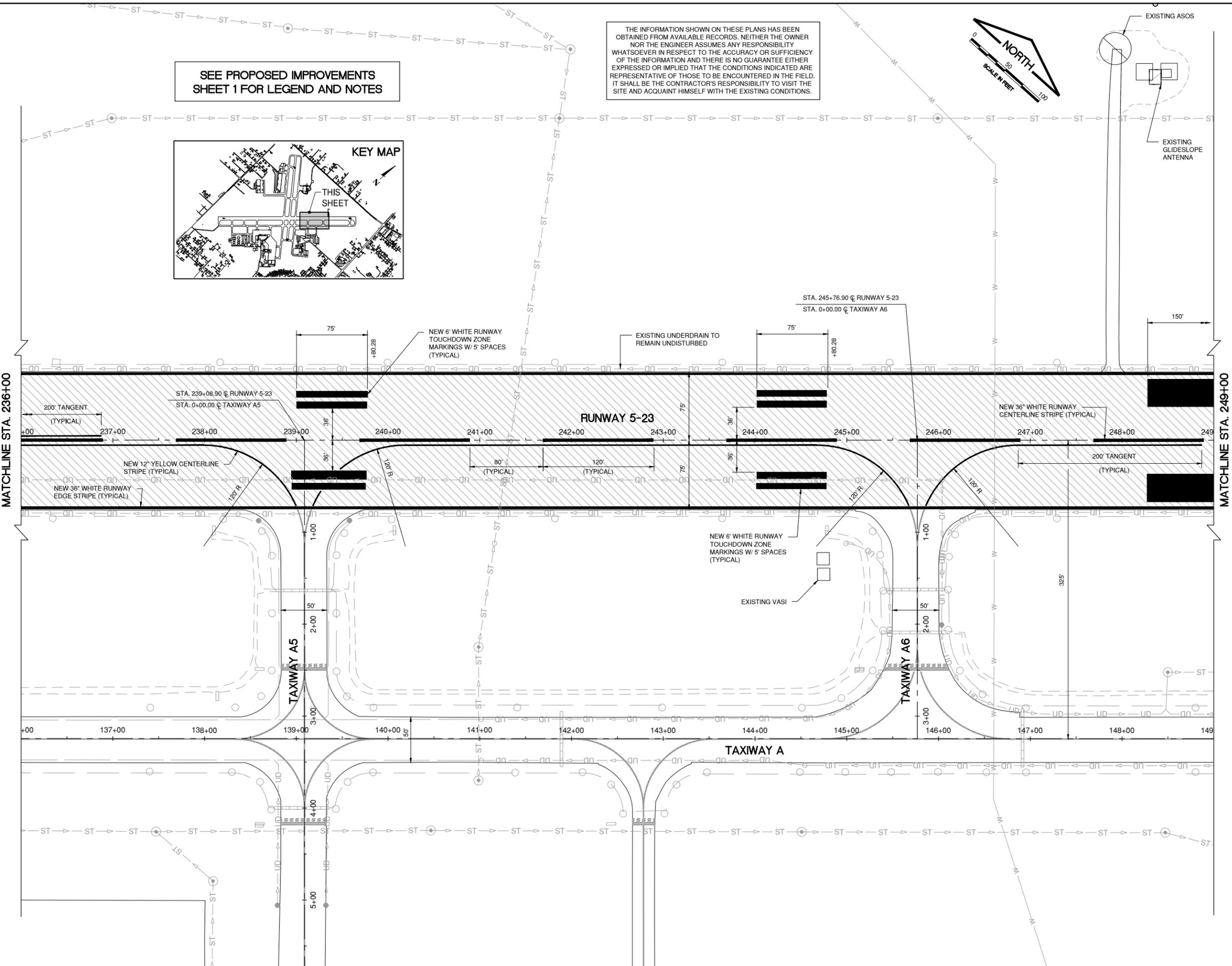
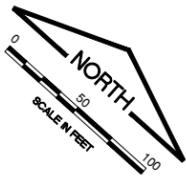
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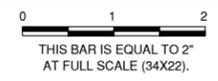


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**WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23**

**PROPOSED IMPROVEMENTS
SHEET 4 OF 5**

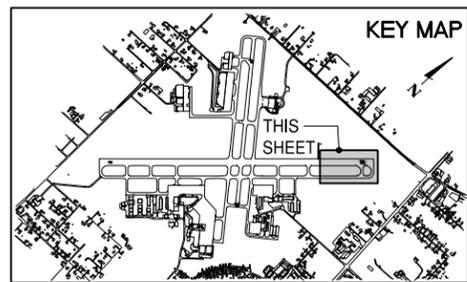
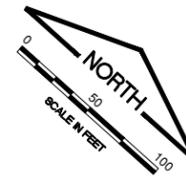
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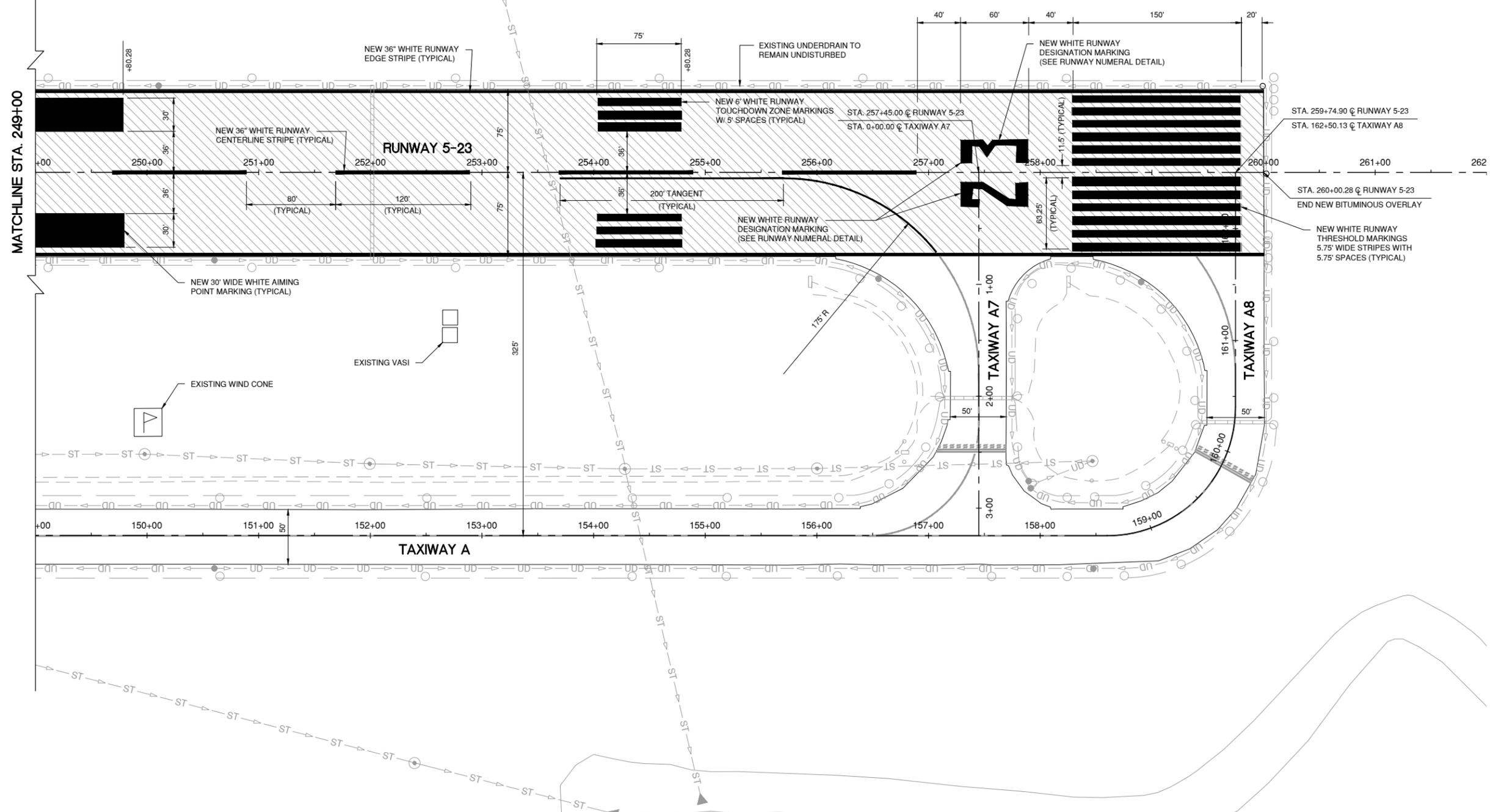
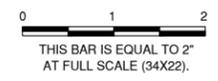
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 SHEET 1 FOR LEGEND AND NOTES

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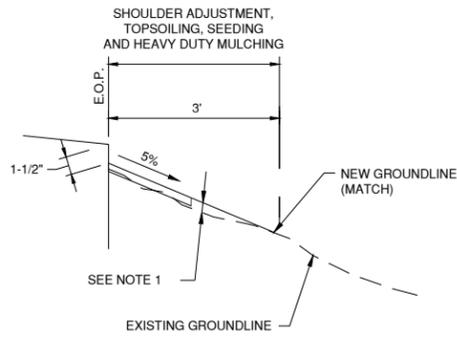
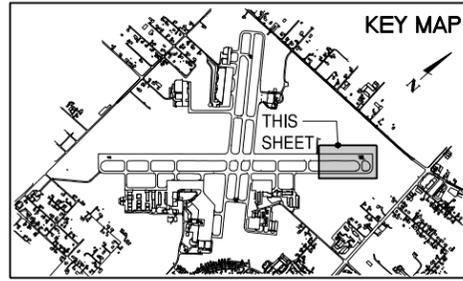
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**PROPOSED IMPROVEMENTS
 SHEET 5 OF 5**

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**TURF SHOULDER DETAIL
OFF END OF RUNWAY 23**
NOT TO SCALE

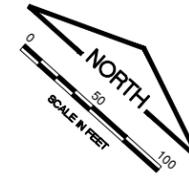
NOTES:

1. TOPSOIL REQUIRED FOR FILL SHALL BE INCLUDED IN THE COSTS FOR SHOULDER ADJUSTMENT (AR152480)
2. LIMITS OF TURF SHOULDER SHALL BE ADJUSTED IN THE FIELD AS REQUIRED.

NOTES

1. SHOULDER ADJUSTMENT SHALL BE AS SHOWN ON THE DETAIL ON THIS SHEET.
2. A NOMINAL 3' WIDTH WILL BE REQUIRED.
3. ACTUAL SHOULDER ADJUSTMENT LOCATION MAY VARY AT THE ENGINEER'S DIRECTION.

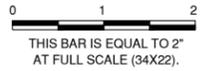
LEGEND



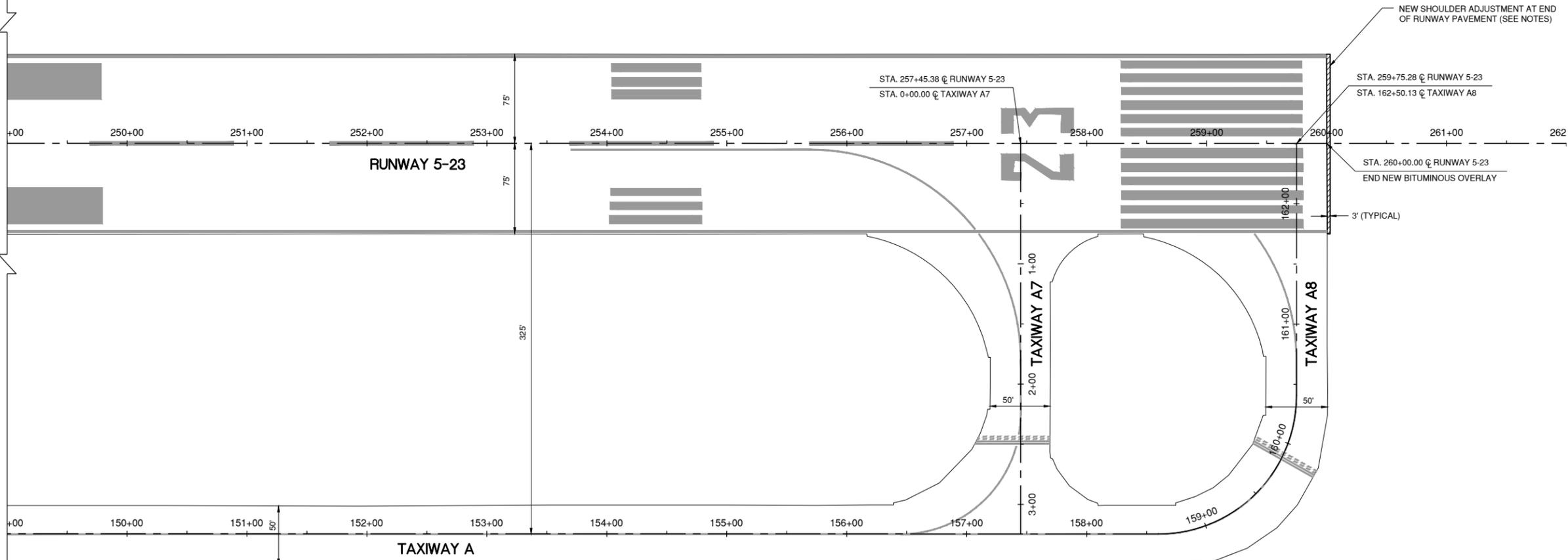
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MATCHLINE STA. 249+00



**WAUKEGAN NATIONAL AIRPORT
WAUKEGAN, ILLINOIS
REHABILITATE RUNWAY 5-23**

LANDSCAPING PLAN

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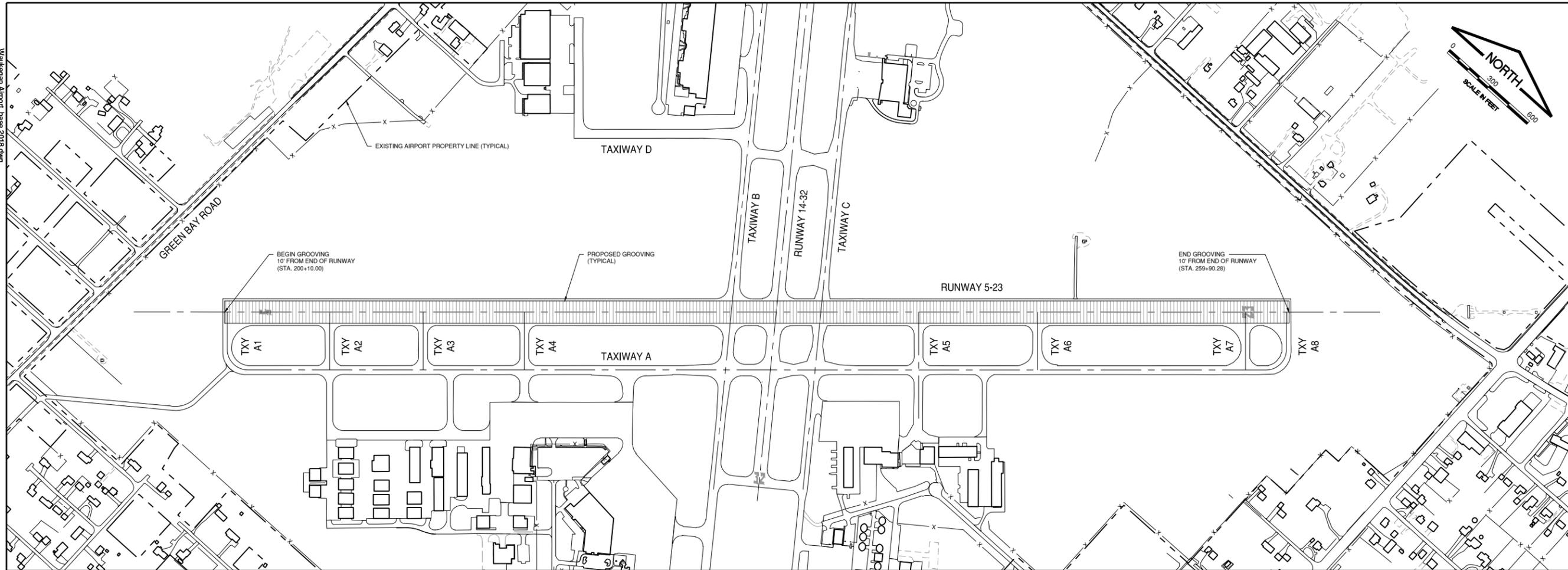
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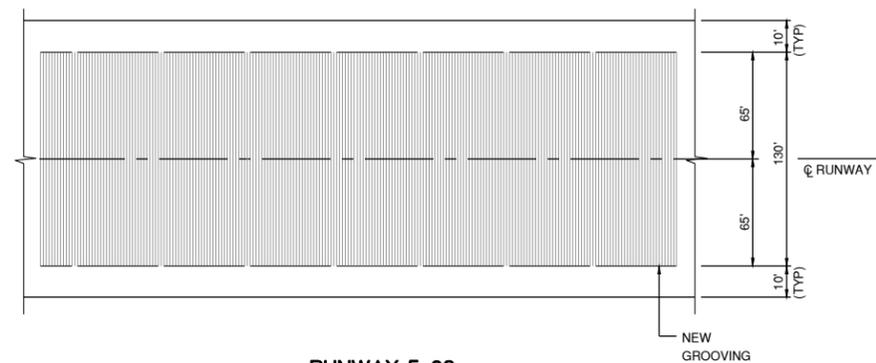
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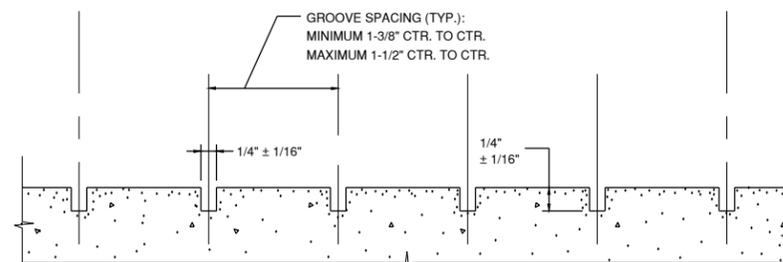
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN, ILLINOIS
 REHABILITATE RUNWAY 5-23**

GROOVING PLAN AND DETAILS



**RUNWAY 5-23
 PLAN VIEW**
 NO SCALE



GROOVE DIMENSIONS
 NO SCALE

GROOVING NOTES

1. AFTER COMPLETION OF HMA PAVING, RUNWAY 5-23 SHALL BE ALLOWED TO CURE A MINIMUM OF 30 DAYS PRIOR TO THE COMMENCEMENT OF GROOVING OPERATIONS.
2. SEE PAVEMENT MARKING PLANS AND NOTES.
3. GROOVING SHALL EXTEND OUTWARD 65' FROM CENTERLINE OF RUNWAY 5-23.
4. SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
5. EXTREME CARE SHALL BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" TO SUCH FACILITIES.
6. CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.

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