

# CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

## SPRINGFIELD AIRPORT AUTHORITY SPRINGFIELD, ILLINOIS

IL. PROJ. NO. SPI-4281

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES

JUNE 14, 2013

SUMMARY OF BASE BID QUANTITIES			
ITEM NO.	Description	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1
AR209650	AGGREGATE BASE PREPARATION	S.Y.	16750
AR401610	BITUMINOUS SURFACE COURSE	TON	3850
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	16750
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	10
AR401911	REMOVE & REPLACE BIT. SURFACE	S.Y.	355
AR501604	4" PCC SIDEWALK	S.F.	75
AR501900	REMOVE PCC PAVEMENT	S.Y.	60
AR602510	BITUMINOUS PRIME COAT	GAL	8375
AR603510	BITUMINOUS TACK COAT	GAL	2512
AR620510	PAVEMENT MARKING	S.F.	9900
AR620900	PAVEMENT MARKING REMOVAL	S.F.	1400
AR625511	SEAL COAT	S.Y.	3500
SUMMARY OF ADDITIVE ALTERNATE #1 QUANTITIES			
AS209650	AGGREGATE BASE PREPARATION	S.Y.	3500
AS401610	BITUMINOUS SURFACE COURSE	TON	825
AS401651	BITUMINOUS PAVEMENT MILLING	S.Y.	3500
AS602510	BITUMINOUS PRIME COAT	GAL	1750
AS603510	BITUMINOUS TACK COAT	GAL	525

SHEET LIST TABLE	
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04	CONSTRUCTION ACTIVITY PLAN 2
05	CSSP - CONSTRUCTION SAFETY PHASING PLAN NOTES
06	CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS
07	EXISTING CONDITIONS AND REMOVALS LEGEND AND NOTES
08	EXISTING CONDITIONS AND REMOVALS 1
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**811** Know what's below. Call before you dig. J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

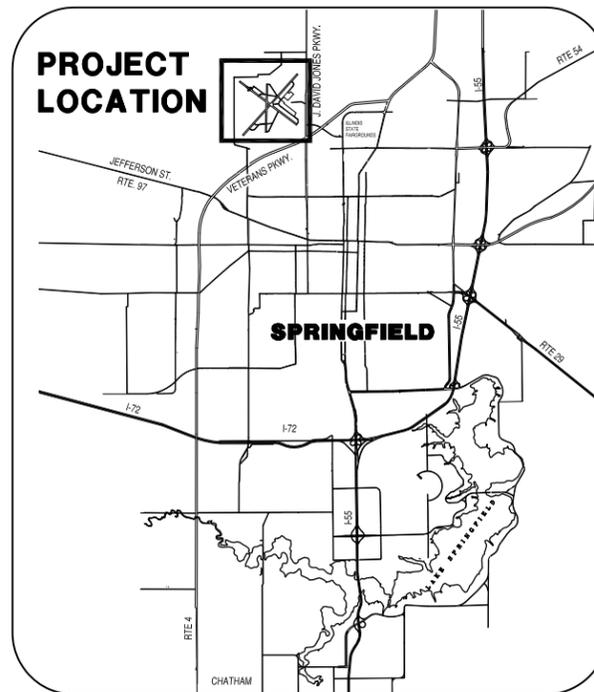
**DESIGN INFORMATION**

**GEOMETRIC CRITERIA**  
AIRPLANE DESIGN GROUP II  
AIRCRAFT APPROACH CATEGORY C

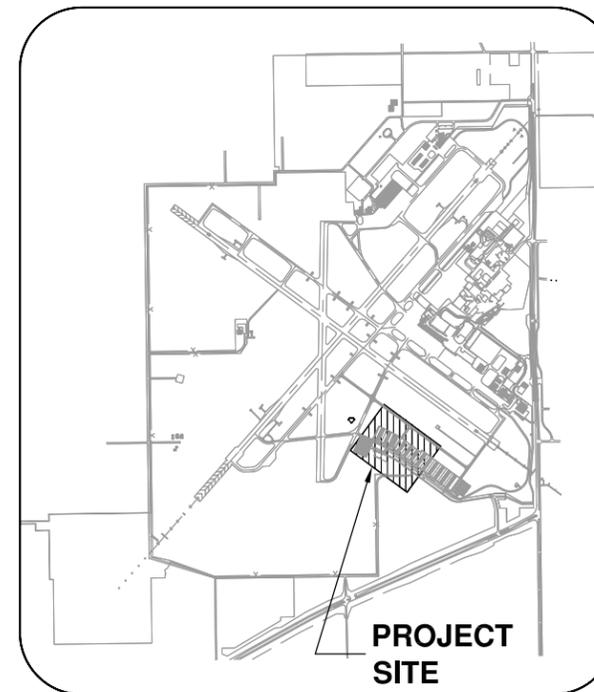
**PERIMETER ROADWAY**  
LOW SPEED URBAN STREET  
30 MPH DESIGN SPEED

CALL J.U.L.I.E. BEFORE EXCAVATING  
1-800-892-0123

TOWNSHIP: 16 NORTH  
RANGE: 5 WEST OF THE 4TH P.M.  
SECTION: 16  
COUNTY: SANGAMON  
CIVIL TOWNSHIP: CAPITAL



LOCATION MAP



SITE PLAN

Exp. 11-30-2013  
  
 Christopher R. Gault  
 7-8-2013

ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD AIRPORT AUTHORITY

SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

APPROVED   
 EXECUTIVE DIRECTOR OF AVIATION

DATE \_\_\_\_\_

**GMT**  
 CRAWFORD MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS

■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY   
 DATE 8-2013

CMT JOB NUMBER: 130350100

**CA016**

REVISIONS

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 SITE PLAN

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APPROVED BY:	RLV
DATE:	JUNE 14, 2013
JOB No:	130350100
IL. PROJ. NO.	SPI-4281
SHEET	02 OF 20 SHEETS

**GENERAL NOTES**

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLANS.
- THE CONTRACTOR SHALL CONFIRM WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER THAT ALL RUNWAYS, TAXIWAYS, AND APRONS WITHIN THE CONSTRUCTION LIMITS HAVE BEEN CLOSED TO AIRCRAFT TRAFFIC PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL HAVE PERSONNEL CAPABLE OF MONITORING AIRCRAFT COMMUNICATIONS. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED RADIO TRAINING TO MONITOR THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT, ASPHALT MILLINGS, PCC AND OTHER CONSTRUCTION DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 93' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE DESIGNATED GRADING LIMITS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- WORK SHALL BE DONE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR AC 150/5370-2, LATEST EDITION, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
  - THE CONTRACTOR'S ACCESS TO THE WORK AREA SHALL BE AS SHOWN IN THE PLANS.
  - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
  - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT FOR OPERATION OF THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
  - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
  - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
  - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
  - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY SAFETY AREAS AND TAXIWAY OFA'S.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY THE CONTRACTORS VEHICLES ACCESSING THE WORK SITE OR DEPARTING THE WORK SITE IMMEDIATELY FOLLOWING SAID VEHICLE.

**GROUND CONTROL FREQUENCY - 121.90**  
**AIR CONTROL FREQUENCY - 121.30**  
**MAXIMUM HEIGHT OF EQUIPMENT - 25'**

**CRITICAL POINT TABLE**

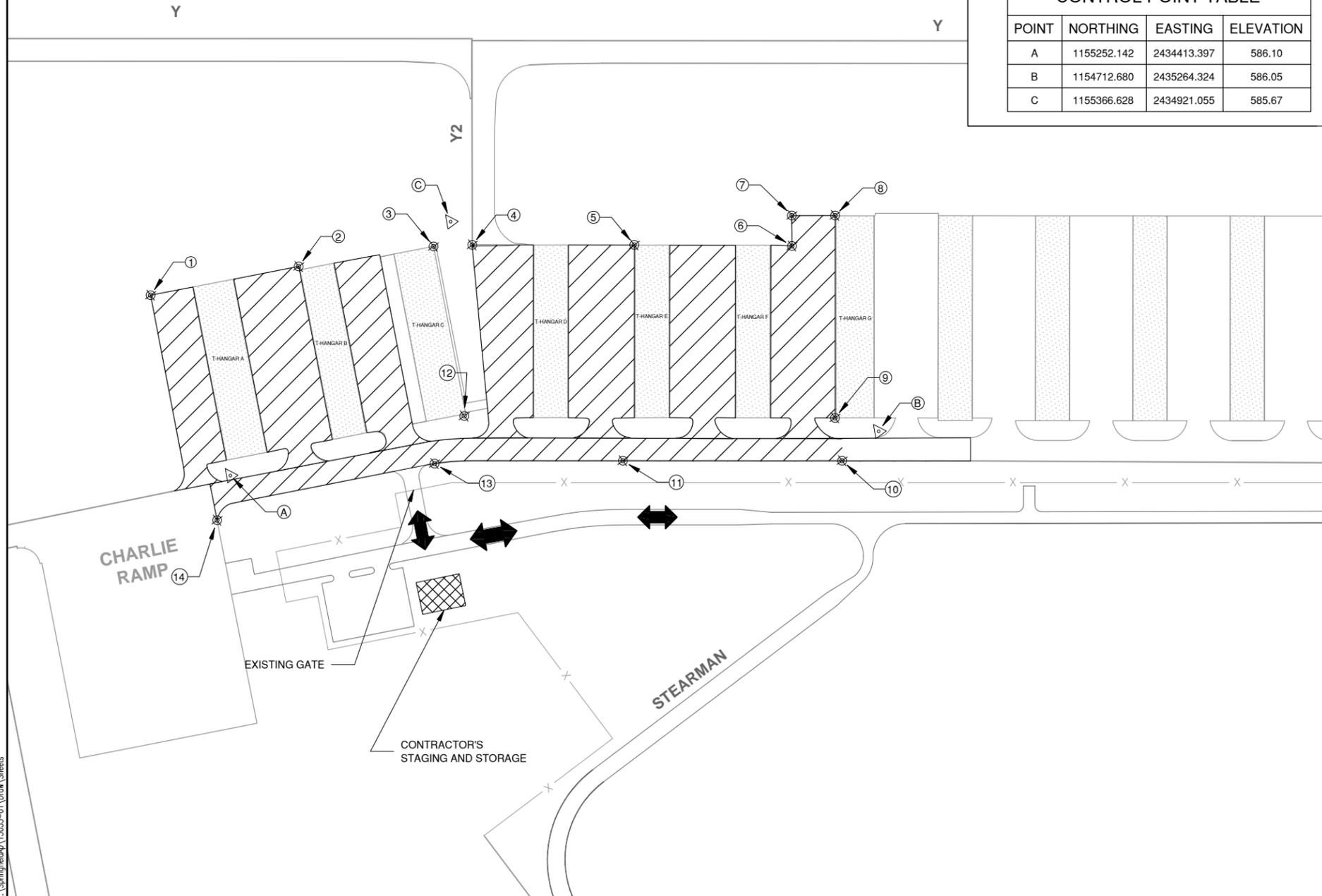
POINT	LATITUDE	LONGITUDE	ELEVATION
1	N39° 50' 19.90"	W89° 40' 32.14"	585.66
2	N39° 50' 18.90"	W89° 40' 29.44"	586.25
3	N39° 50' 17.91"	W89° 40' 27.05"	586.15
4	N39° 50' 17.58"	W89° 40' 26.41"	585.92
5	N39° 50' 16.10"	W89° 40' 23.82"	586.30
6	N39° 50' 14.64"	W89° 40' 21.32"	586.08
7	N39° 50' 15.02"	W89° 40' 20.96"	585.71
8	N39° 50' 14.62"	W89° 40' 20.27"	586.52
9	N39° 50' 12.14"	W89° 40' 22.66"	586.51
10	N39° 50' 11.55"	W89° 40' 23.06"	585.18
11	N39° 50' 13.55"	W89° 40' 26.56"	585.51
12	N39° 50' 15.55"	W89° 40' 28.56"	586.12
13	N39° 50' 15.24"	W89° 40' 29.60"	585.36
14	N39° 50' 16.53"	W89° 40' 33.74"	586.24

**CONTROL POINT TABLE**

POINT	NORTHING	EASTING	ELEVATION
A	1155252.142	2434413.397	586.10
B	1154712.680	2435264.324	586.05
C	1155366.628	2434921.055	585.67

**LEGEND**

- CONTRACTOR'S ACCESS ROUTE
- WORK AREA
- CRITICAL POINT
- CONTROL POINT



NORTH  
**KEYMAP**

THIS SHEET



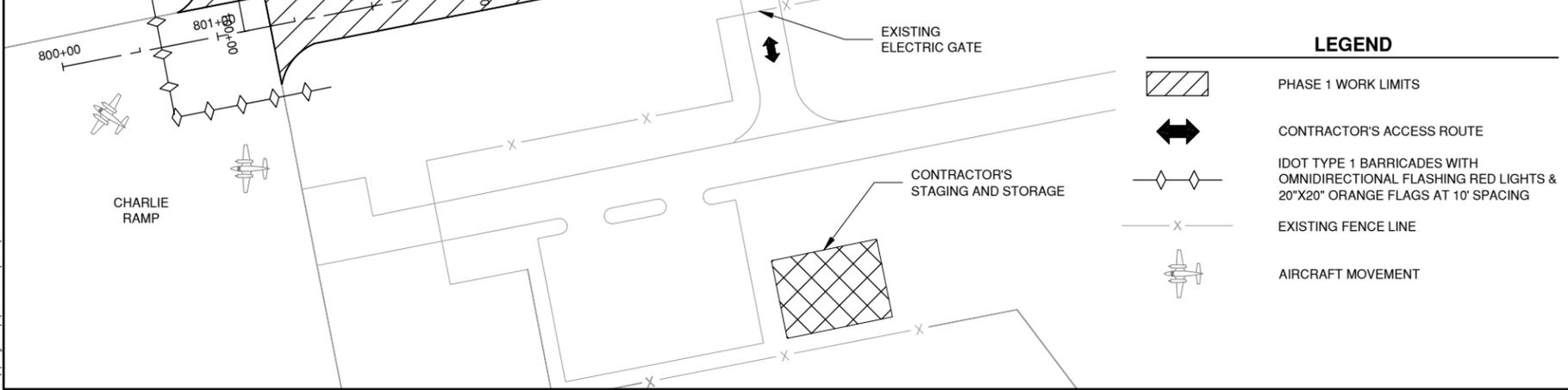
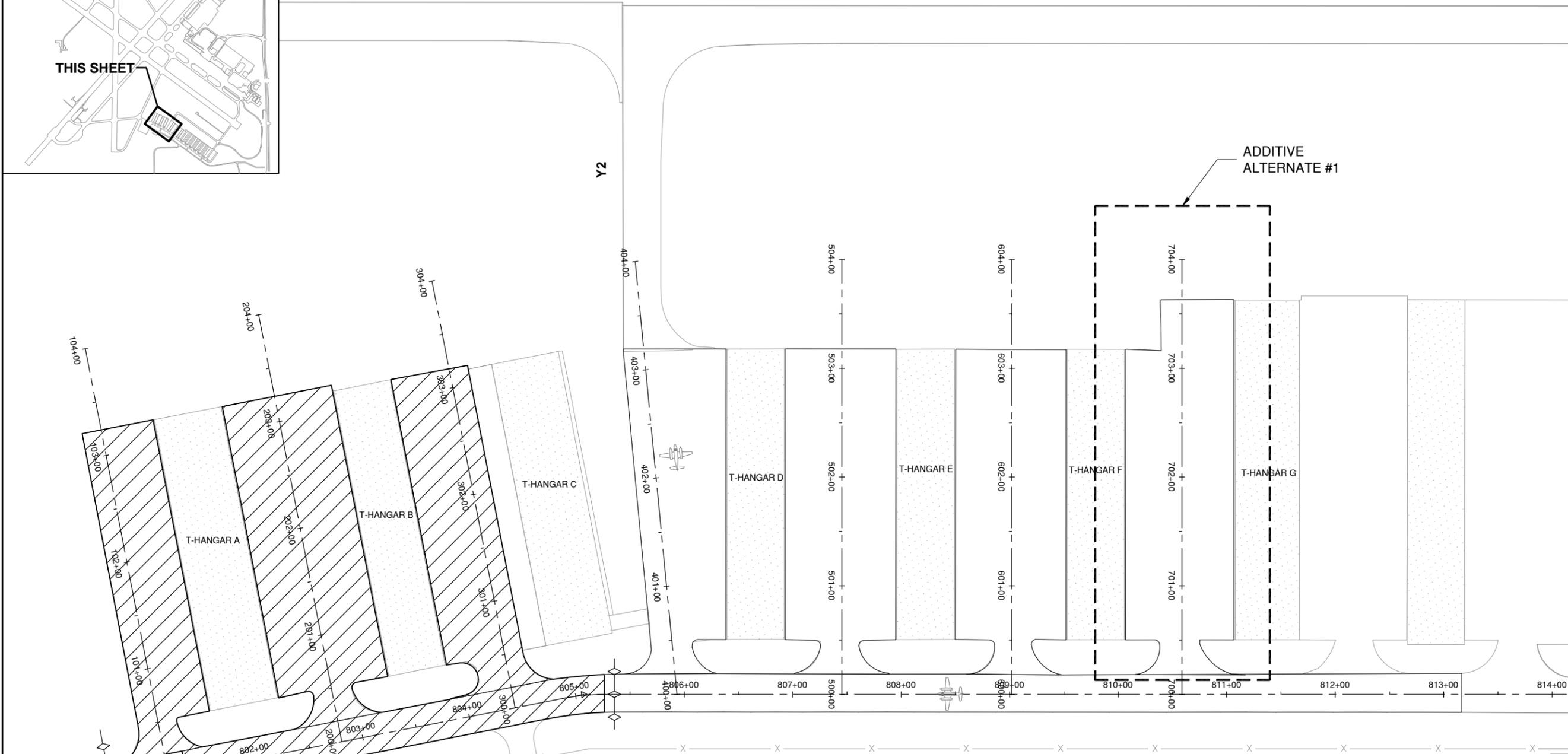
FILE: 1303501-GC103.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 7/12/2013 8:30 AM  
 SPI-BASE CURRENT GEOMETRY  
 1303501\_BASE\_PROP\_GEO  
 KEYMAP

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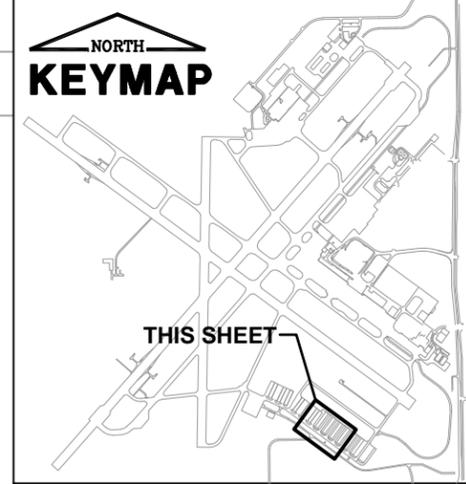
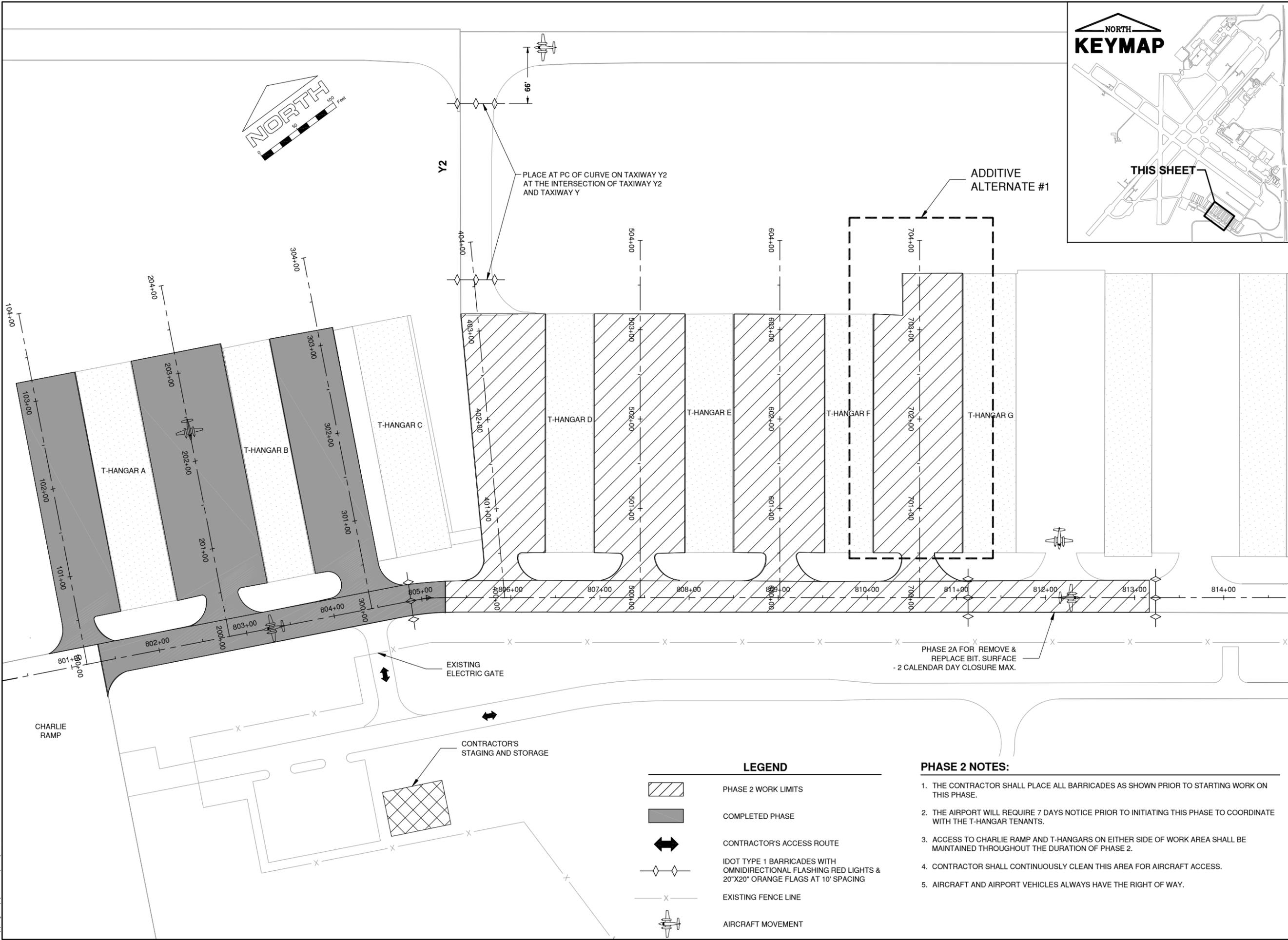
  
**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**CONSTRUCTION ACTIVITY PLAN 1**

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SHEET 03 OF 20 SHEETS	

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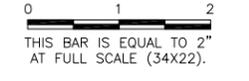


FILE: 1303501-GC104.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 7/12/2013 8:30 AM

SPI-BASE CURRENT GEOMETRY  
 1303501\_BASE\_PROP\_GEO  
 KEYMAP

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**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**CONSTRUCTION ACTIVITY PLAN 2**

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**LEGEND**

	PHASE 2 WORK LIMITS
	COMPLETED PHASE
	CONTRACTOR'S ACCESS ROUTE
	IDOT TYPE 1 BARRICADES WITH OMNIDIRECTIONAL FLASHING RED LIGHTS & 20"X20" ORANGE FLAGS AT 10' SPACING
	EXISTING FENCE LINE
	AIRCRAFT MOVEMENT

- PHASE 2 NOTES:**
1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
  2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE T-HANGAR TENANTS.
  3. ACCESS TO CHARLIE RAMP AND T-HANGARS ON EITHER SIDE OF WORK AREA SHALL BE MAINTAINED THROUGHOUT THE DURATION OF PHASE 2.
  4. CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA FOR AIRCRAFT ACCESS.
  5. AIRCRAFT AND AIRPORT VEHICLES ALWAYS HAVE THE RIGHT OF WAY.

PHASE 2A FOR REMOVE & REPLACE BIT. SURFACE  
 - 2 CALENDAR DAY CLOSURE MAX.

1. GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

2. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

3. PHASING

- 1. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

4. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CONTRACTOR EMPLOYEES MAY BE REQUIRED TO OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
5. CONTRACTOR WORK CREWS MUST MONITOR THE GROUND CONTROL FREQUENCY AT ALL TIMES. THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS. BE AWARE OF TENANT AIRCRAFT MOVEMENT NEAR THE WORK AREAS.
6. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
7. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
8. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
9. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
10. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
11. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL, ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
12. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- 1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- 1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. THE CONTRACTOR SHALL NOTIFY THE AIRPORT 7 DAYS BEFORE STARTING WORK IN EACH PHASE. THIS WILL ENSURE THAT THE AIRPORT CAN CONTACT TENANTS ABOUT MOVING AIRCRAFT DURING THE TIME OF CONSTRUCTION.
4. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911 AND SAFETY.
6. CONTACTS FOR THIS PROJECT ARE AS DISCUSSED IN THE PRE-CONSTRUCTION MEETING.

10. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR AND AIRPORT SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

15. PROTECTION

- 1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 93' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

16. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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REVISIONS

Table with 3 columns: NUMBER, BY, DATE. Contains one empty row for revisions.



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES
CSSP - CONSTRUCTION SAFETY PHASING
PLAN NOTES

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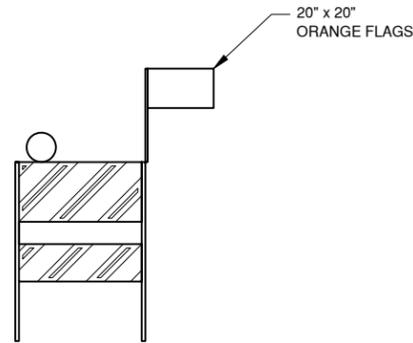
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**SECURITY NOTES**

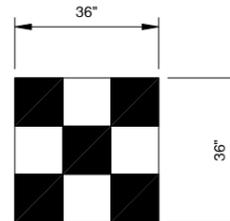
1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
3. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

**CONSTRUCTION ACTIVITY GENERAL NOTES**

1. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT.
2. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
3. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
4. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
5. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
6. DELAYS, CHANGES IN SCHEDULING, OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
8. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND EROSION CONTROL ITEMS AS SHOWN IN THE PLANS PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
9. CONSTRUCTED RELATED ITEMS REQUIRING THE CLOSURE OF TAXIWAYS SHALL REQUIRE CLOSE COORDINATION WITH THE AIRPORT. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION. THE AIRPORT SHALL REQUIRE 7 DAYS NOTIFICATIONS PRIOR TO THE CLOSURE OF TAXIWAYS.
10. CONSTRUCTION BARRICADES SHALL BE SET AT THE LIMITS OF THE WORK AREA OF EACH PHASE. DURING PAVING OPERATIONS, BARRICADES MAY BE PLACED UP TO 30' BEYOND THE WORK LIMIT TO ALLOW ROOM FOR PAVING OPERATIONS. OFFSETTING THE BARRICADES TO THIS LOCATION SHALL BE COORDINATED WITH THE AIRPORT IN ADVANCE. IN THE EVENT OF A CONFLICT BETWEEN CONSTRUCTION OPERATIONS AND TAXIING AIRCRAFT, THE CONTRACTOR SHALL CEASE OPERATIONS AND RETURN THE BARRICADES TO THE EDGE OF THE WORKING LIMITS. ALL BARRICADES SHALL BE LOCATED AT THE EDGE OF THE WORKING LIMITS AT THE END OF EACH WORKING DAY.
11. IF DURING CONSTRUCTION OPERATIONS NOTICEABLE DESTRUCTION TO THE SUBBASE OCCURS AFTER MILLING, CORRECTIVE ACTION SHALL TAKE PLACE TO MITIGATE THIS ISSUE.
12. EXISTING AGGREGATE SUBBASE SHALL BE RECOMPACTED AFTER MILLING AND BEFORE THE BITUMINOUS PAVING OPERATION.
13. PAVEMENT AROUND SELECT INLETS AND MANHOLES CALLED OUT ON THE PROPOSED IMPROVEMENTS SHEET SHALL BE RECONSTRUCTED IN ACCORDANCE TO THE DETAIL AND NOTES ON THE PAVING & MISCELLANEOUS DETAILS SHEET.



**FLASHER BARRICADE DETAIL-IDOT TYPE 1**  
 N.T.S.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
 N.T.S.

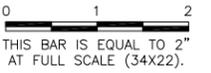
**FLASHER BARRICADE NOTES**

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

**CA016**

REVISIONS

NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 CONSTRUCTION ACTIVITY PLAN NOTES AND  
 DETAILS

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SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 EXISTING CONDITIONS AND REMOVALS  
 LEGEND AND NOTES

**LEGEND**

	NEW PAVEMENT
	EXISTING PAVEMENT
	SAW CUT EDGE (INCIDENTAL TO MILLING)
	EXISTING FENCE
	EXISTING BORING
	EXISTING PAVEMENT MARKING
	PAVEMENT MARKING REMOVAL
	FIBER OPTIC TELEPHONE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING OVERHEAD LIGHT
	EXISTING ELECTRICAL METER
	EXISTING TRANSFORMERS
	EXISTING GAS LINE
	EXISTING GAS REGULATOR
	EXISTING GAS METER
	EXISTING WATER LINE
	EXISTING FIRE HYDRANT
	EXISTING WATER METER
	EXISTING WATER VALVE
	EXISTING EXISTING STORM SEWER
	EXISTING INLET
	EXISTING STORM
	EXISTING END SECTION
	PROTECT EXISTING STRUCTURE
	EXISTING ELECTRIC CKT #2

**NOTES:**

- EXISTING PAVEMENT STRUCTURE IN WORKING LIMITS:  
 - 4" BITUMINOUS SURFACE COURSE (401)  
 - 4" BITUMINOUS CRUSHED AGGREGATE BASE COURSE (209)
- PROTECT ALL EDGES OF PAVEMENT THAT ARE TO REMAIN IN PLACE WHILE SAW CUTTING AND DURING ENTIRE TIME OF CONSTRUCTION OPERATIONS.
- PROTECT ANY EXISTING INLETS OR MANHOLES THAT ARE WITHIN THE WORKING LIMITS OF THE PROJECT.
- PROTECT ANY UTILITIES ON SIDES OF BUILDINGS.
- IF SUBGRADE REPAIR IS NEEDED, ENSURE THAT NO DAMAGE OCCURS TO THE EXISTING UNDERDRAIN LINES.
- SAW CUT EDGES WHERE CALLED OUT ON THIS SHEET. SAW CUT THE BITUMINOUS MATERIAL ADJACENT TO CONCRETE EDGES, THIS SHALL BE THE LINE AS TO WHERE THE MILLING LIMITS ARE TO EXTEND.
- FOR MILLING LIMITS SEE PROPOSED IMPROVEMENTS 1 SHEETS.
- EXISTING LEAD IN LINE TO T-HANGARS SHALL BE SURVEYED PRIOR TO MILLING BITUMINOUS SURFACE AND ARE TO BE REMARKED TO THE EXISTING LOCATION AFTER PAVING NEW BITUMINOUS.

**CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE**

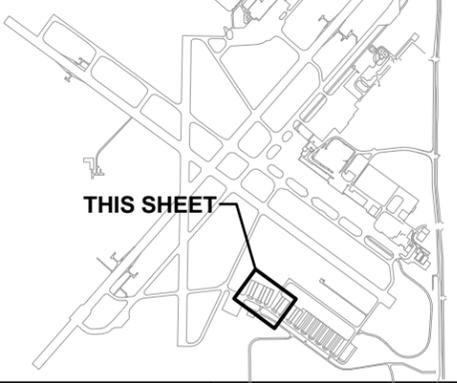
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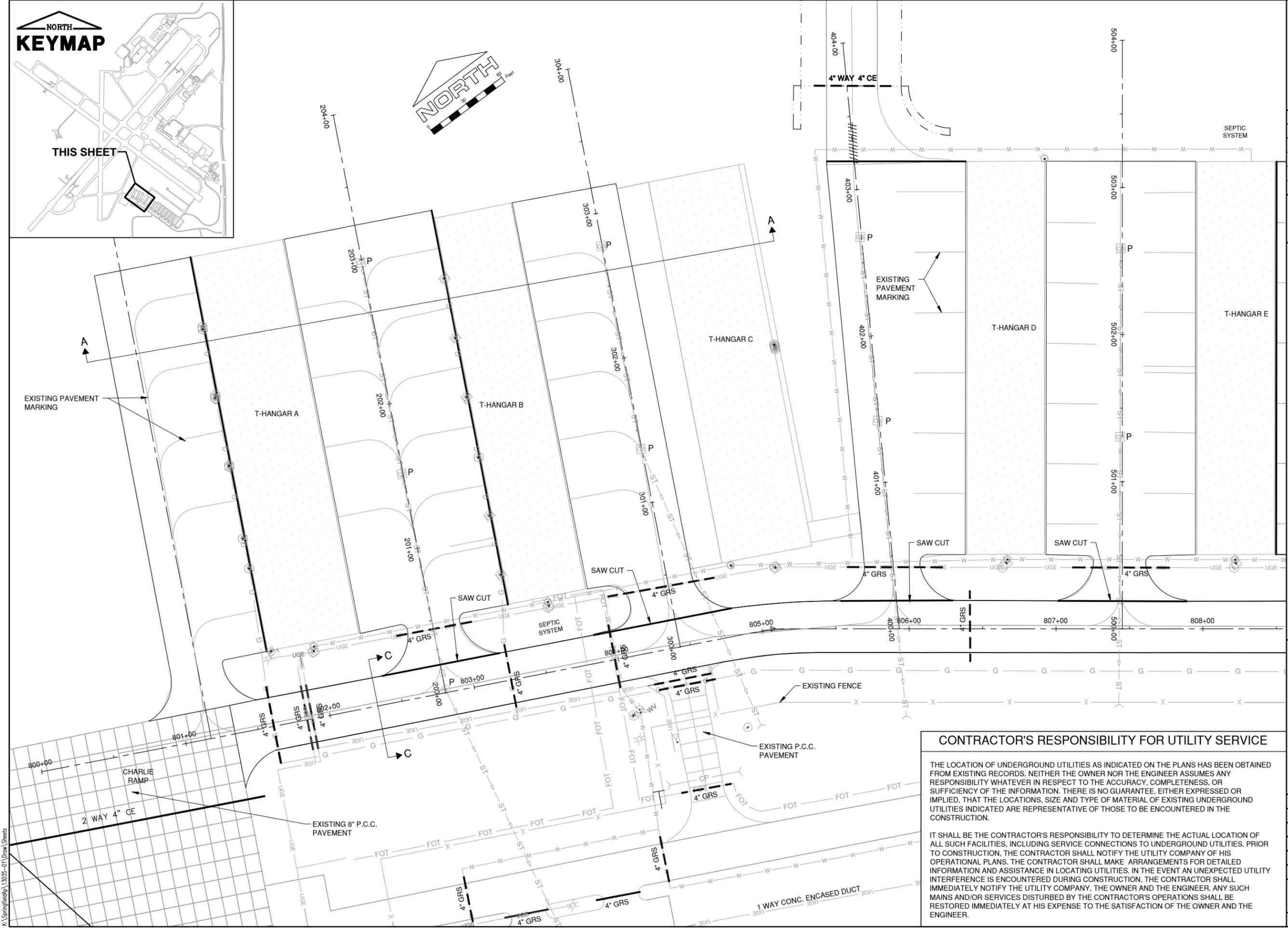
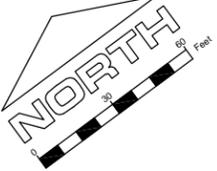
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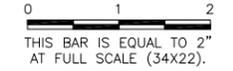
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**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**EXISTING CONDITIONS AND REMOVALS 1**

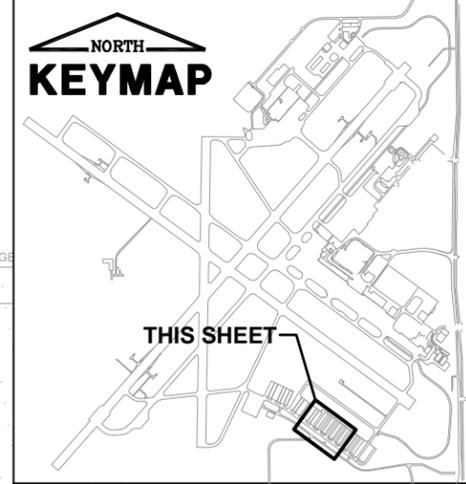
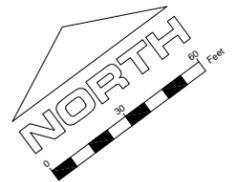
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**CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE**

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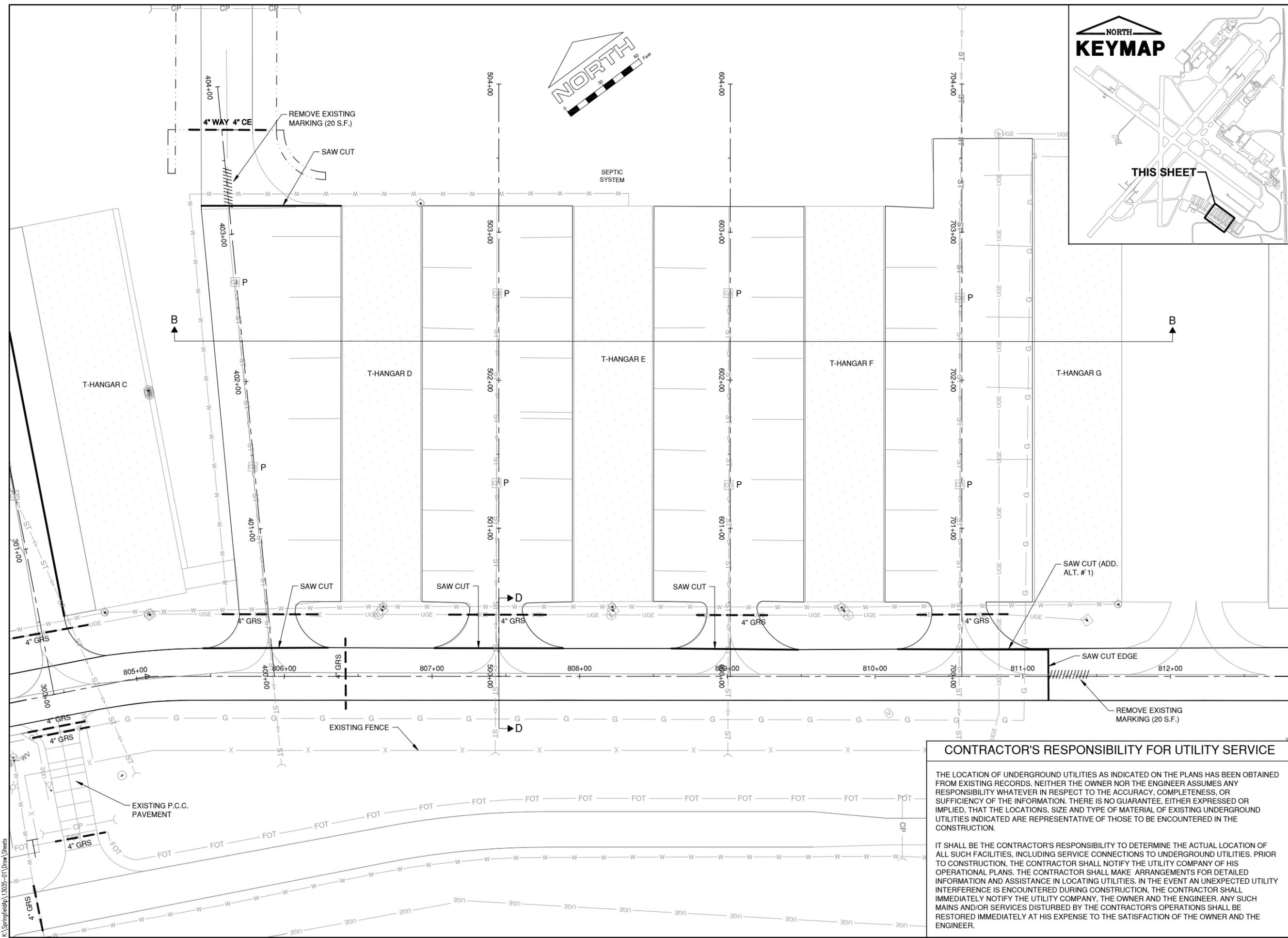


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**CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE**

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**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**EXISTING CONDITIONS AND REMOVALS 2**

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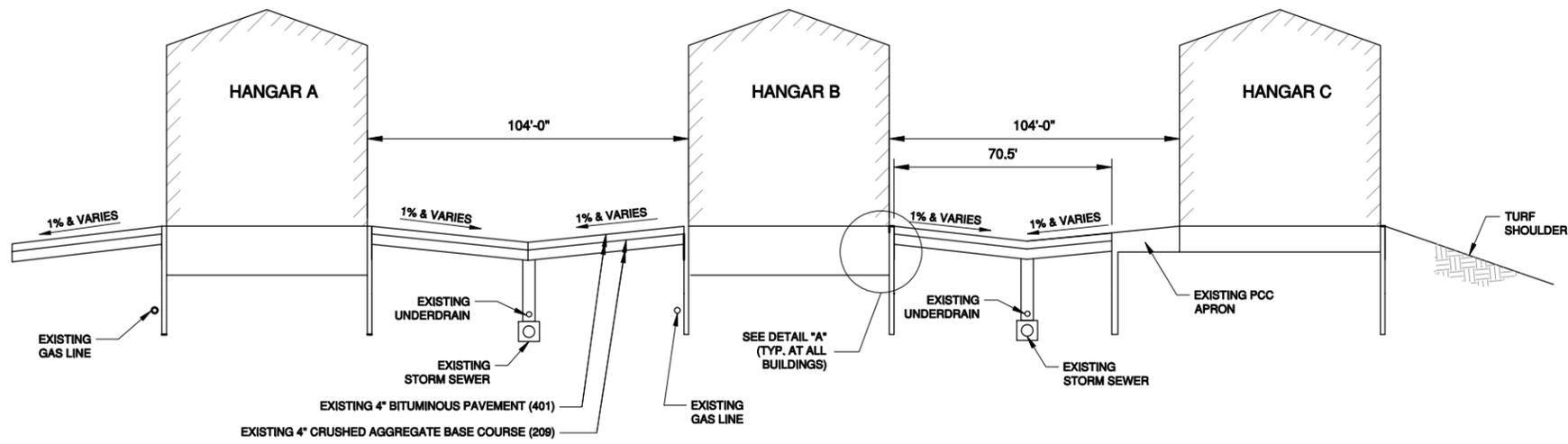


SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

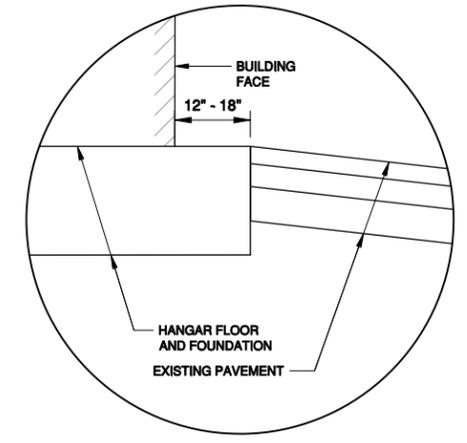
REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 EXISTING PAVEMENT TYPICAL SECTIONS

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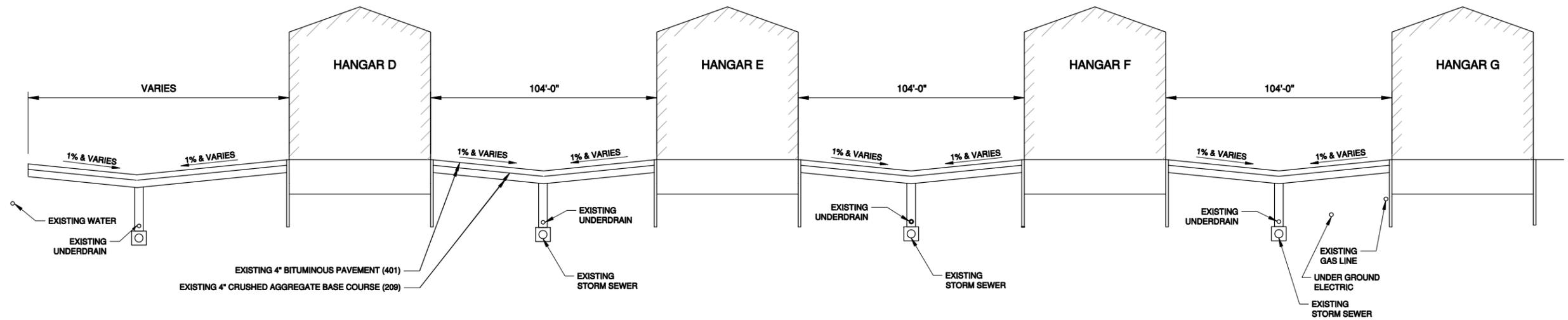
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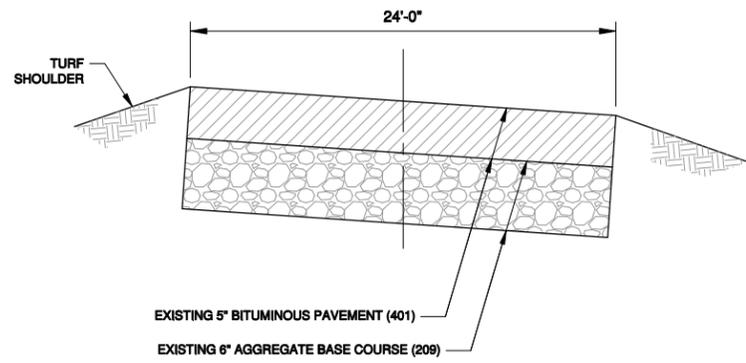
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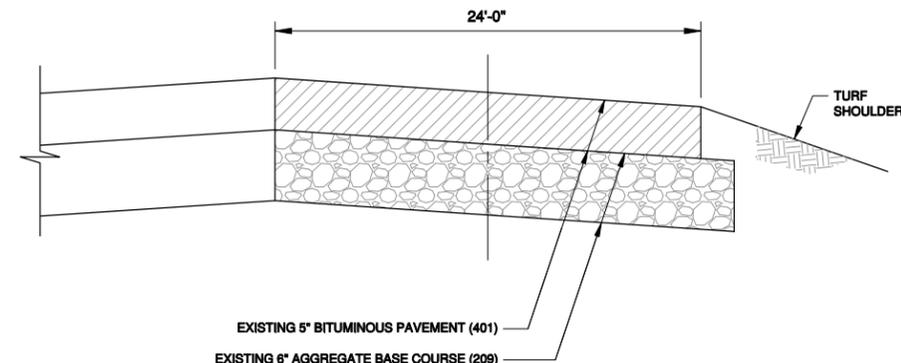
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SECTION VIEW B-B  
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SECTION VIEW C-C  
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SECTION VIEW D-D  
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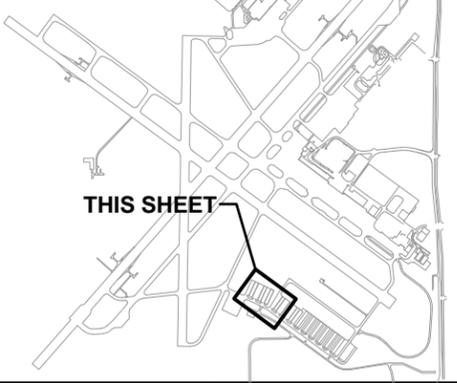
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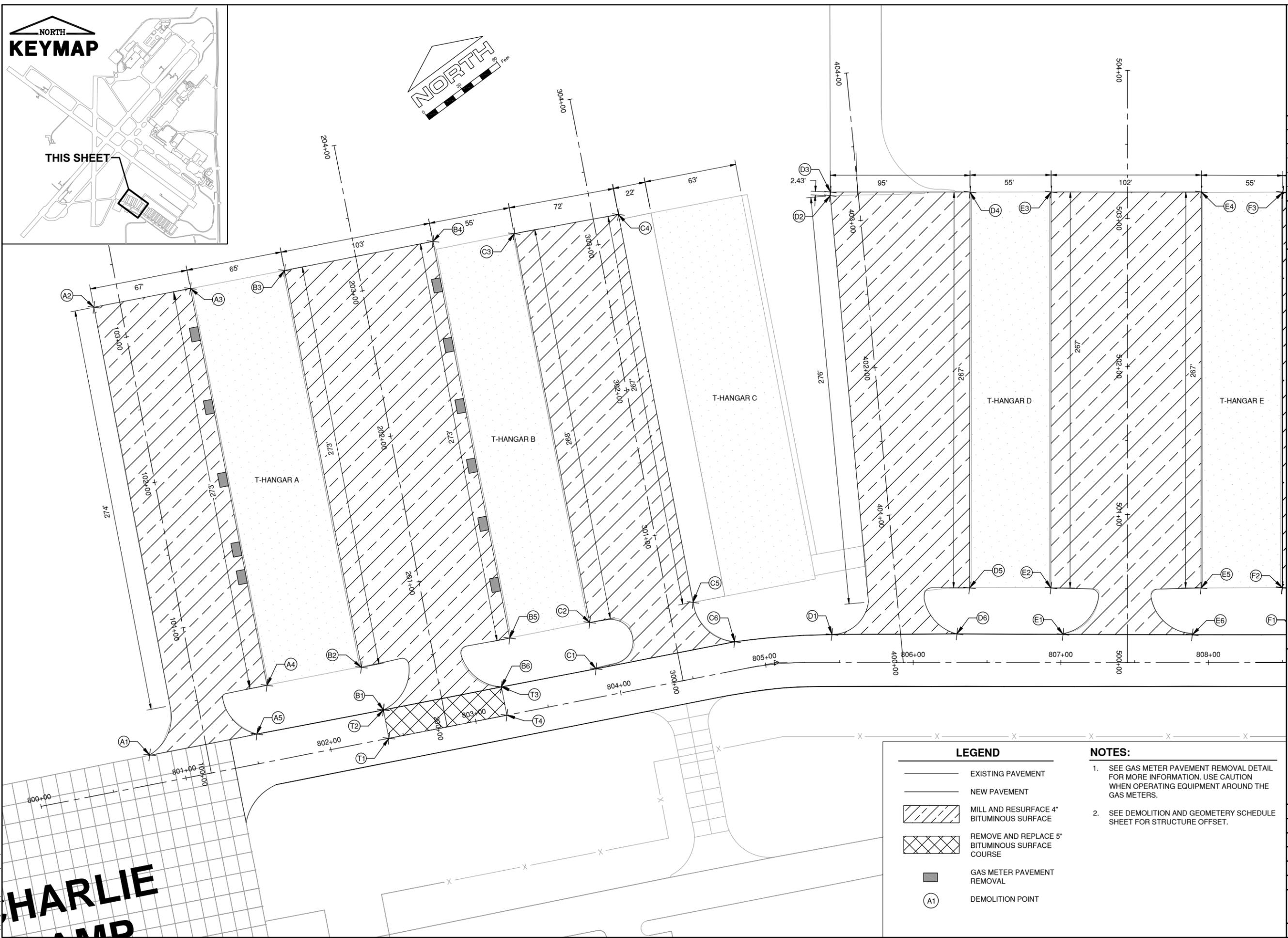
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K:\Springfield\13035-01\Draw\Other

NORTH  
**KEYMAP**



THIS SHEET



**LEGEND**

	EXISTING PAVEMENT
	NEW PAVEMENT
	MILL AND RESURFACE 4" BITUMINOUS SURFACE
	REMOVE AND REPLACE 5" BITUMINOUS SURFACE COURSE
	GAS METER PAVEMENT REMOVAL
	DEMOLITION POINT

- NOTES:**
- SEE GAS METER PAVEMENT REMOVAL DETAIL FOR MORE INFORMATION. USE CAUTION WHEN OPERATING EQUIPMENT AROUND THE GAS METERS.
  - SEE DEMOLITION AND GEOMETRY SCHEDULE SHEET FOR STRUCTURE OFFSET.

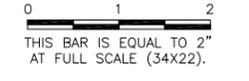
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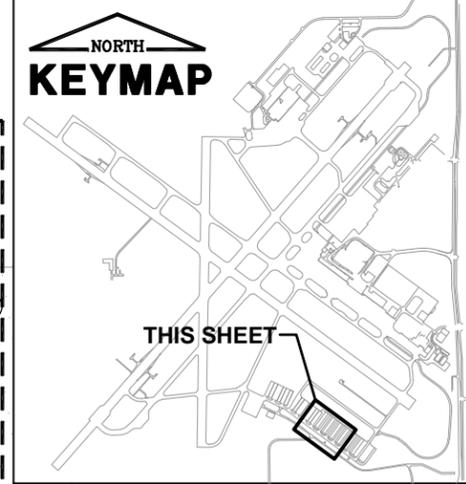
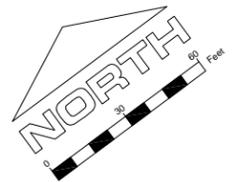
**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**DEMOLITION PLAN 1**

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CHARLIE RAMP



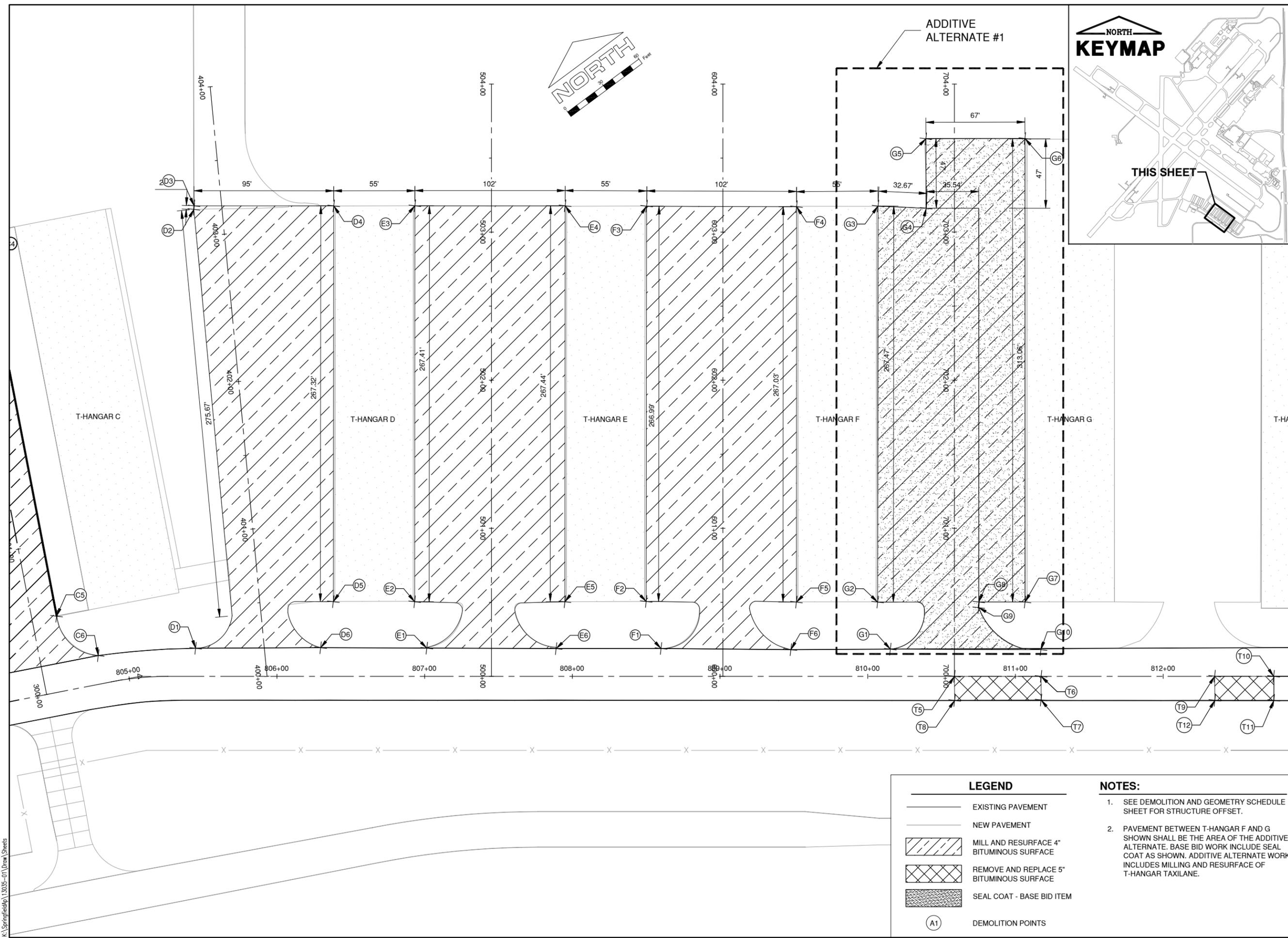
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**LEGEND**

	EXISTING PAVEMENT
	NEW PAVEMENT
	MILL AND RESURFACE 4" BITUMINOUS SURFACE
	REMOVE AND REPLACE 5" BITUMINOUS SURFACE
	SEAL COAT - BASE BID ITEM
	DEMOLITION POINTS

- NOTES:**
- SEE DEMOLITION AND GEOMETRY SCHEDULE SHEET FOR STRUCTURE OFFSET.
  - PAVEMENT BETWEEN T-HANGAR F AND G SHOWN SHALL BE THE AREA OF THE ADDITIVE ALTERNATE. BASE BID WORK INCLUDE SEAL COAT AS SHOWN. ADDITIVE ALTERNATE WORK INCLUDES MILLING AND RESURFACE OF T-HANGAR TAXILANE.



**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**DEMOLITION PLAN 2**

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APPROVED BY:	RLV
DATE:	JUNE 14, 2013
JOB No:	130350100
IL. PROJ. NO. SPI-4281	
SHEET 12 OF 20 SHEETS	

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REVISIONS

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

DEMOLITION POINTS				
POINT	OBJECT	BASELINE	STATION	OFFSET
A1	DEMOLITION POINT	LANE A	100+20	38.7' L
A2	DEMOLITION POINT	LANE A	103+24	17.8' L
A3	DEMOLITION POINT	LANE A	103+24	48.8' R
A4	DEMOLITION POINT	LANE A	100+51	48.3' R
A5	DEMOLITION POINT	LANE A	100+20	35.3' R
B1	DEMOLITION POINT	LANE B	200+20	40.2' L
B2	DEMOLITION POINT	LANE B	200+51	49.4' L
B3	DEMOLITION POINT	LANE B	203+24	49.0' L
B4	DEMOLITION POINT	LANE B	203+24	53.3' R
B5	DEMOLITION POINT	LANE B	200+51	53.0' R
B6	DEMOLITION POINT	LANE B	200+19	41.2' R
C1	DEMOLITION POINT	LANE C	300+19	56.3' L
C2	DEMOLITION POINT	LANE C	300+51	54.5' L
C3	DEMOLITION POINT	LANE C	303+18	54.5' L
C4	DEMOLITION POINT	LANE C	303+17	17.3' R
C5	DEMOLITION POINT	LANE C	300+51	16.7' R
C6	DEMOLITION POINT	LANE C	300+19	39.3' R
D1	DEMOLITION POINT	LANE D	400+24	46.3' L
D2	DEMOLITION POINT	LANE D	403+19	18.9' L
D3	DEMOLITION POINT	LANE D	403+21	18.6' L
D4	DEMOLITION POINT	LANE D	403+12	75.5' R
D5	DEMOLITION POINT	LANE D	400+46	49.5' R
D6	DEMOLITION POINT	LANE D	400+16	38.0' R
E1	DEMOLITION POINT	LANE E	500+19	43.7' L
E2	DEMOLITION POINT	LANE E	500+50	52.0' L
E3	DEMOLITION POINT	LANE E	503+18	51.8' L
E4	DEMOLITION POINT	LANE E	503+18	49.9' R
E5	DEMOLITION POINT	LANE E	500+50	49.7' R
E6	DEMOLITION POINT	LANE E	500+19	43.8' R
F1	DEMOLITION POINT	LANE F	600+19	41.9' L
F2	DEMOLITION POINT	LANE F	600+51	52.0' L
F3	DEMOLITION POINT	LANE F	603+18	51.7' L
F4	DEMOLITION POINT	LANE F	603+17	49.9' R
F5	DEMOLITION POINT	LANE F	600+50	49.7' R
F6	DEMOLITION POINT	LANE F	600+18	46.0' R
G1	DEMOLITION POINT	LANE G	700+18	43.4' L
G2	DEMOLITION POINT	LANE G	700+50	52.0' L
G3	DEMOLITION POINT	LANE G	703+18	51.7' L
G4	DEMOLITION POINT	LANE G	703+16	19.1' L
G5	DEMOLITION POINT	LANE G	703+63	19.3' L
G6	DEMOLITION POINT	LANE G	703+63	47.7' R
G7	DEMOLITION POINT	LANE G	700+50	47.8' R
G8	DEMOLITION POINT	LANE G	700+50	16.5' R
G9	DEMOLITION POINT	LANE G	700+46	16.4' R
G10	DEMOLITION POINT	LANE G	700+18	58.5' R
T1	DEMOLITION POINT	T-HANGAR ACCESS	802+40	0.0'
T2	DEMOLITION POINT	T-HANGAR ACCESS	802+40	19.5' L
T3	DEMOLITION POINT	T-HANGAR ACCESS	803+21	19.4' L
T4	DEMOLITION POINT	T-HANGAR ACCESS	803+21	0.0'
T5	DEMOLITION POINT	T-HANGAR ACCESS	810+59	0.0' R
T6	DEMOLITION POINT	T-HANGAR ACCESS	811+17	0.0'
T7	DEMOLITION POINT	T-HANGAR ACCESS	811+17	16.0' R
T8	DEMOLITION POINT	T-HANGAR ACCESS	810+59	16.3' R
T9	DEMOLITION POINT	T-HANGAR ACCESS	812+35	0.1' L
T10	DEMOLITION POINT	T-HANGAR ACCESS	812+75	0.0'
T11	DEMOLITION POINT	T-HANGAR ACCESS	812+75	16.3' R
T12	DEMOLITION POINT	T-HANGAR ACCESS	812+35	16.2' R



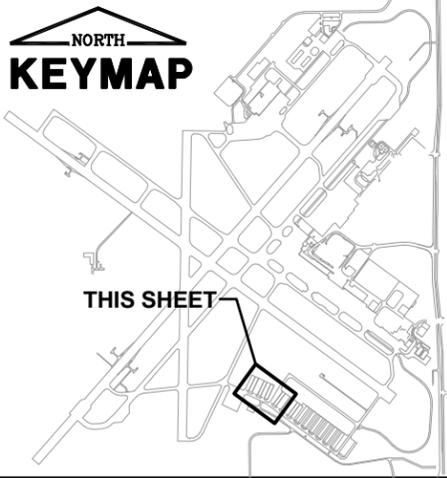
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REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 DEMOLITION AND GEOMETRY SCHEDULE

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**NORTH**  
**KEYMAP**



THIS SHEET

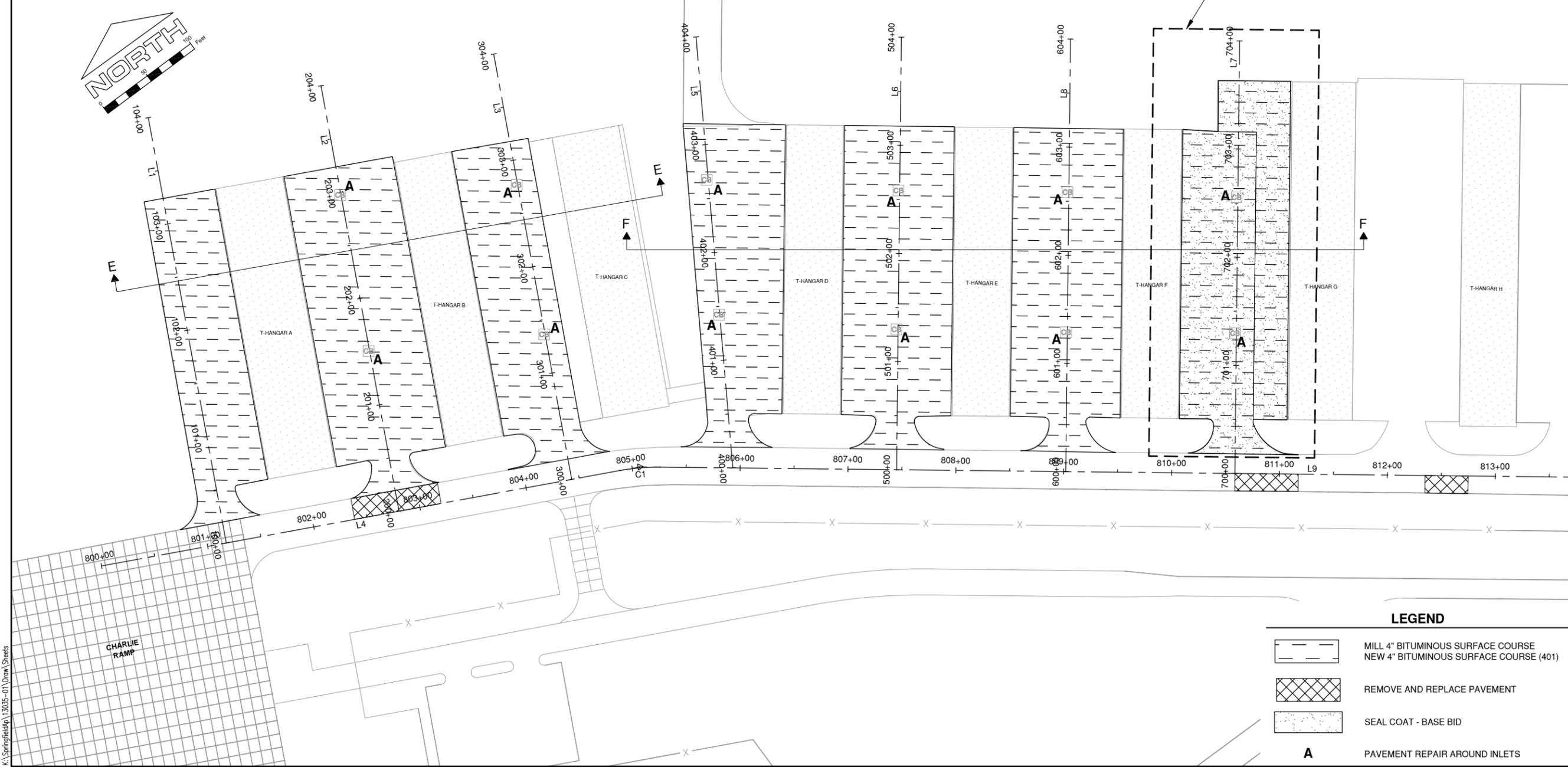
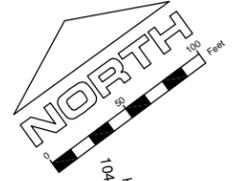
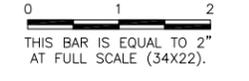
ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	BASELINE A	STA. 100+00.00	N:1155249.4955 E:2434356.7049	STA. 104+00.00	N: 1155611.3602 E: 2434527.1571
L2	BASELINE B	STA. 200+00.00	N:1155180.1427 E:2434503.9386	STA. 204+00.00	N: 1155542.0074 E: 2434674.3908
L3	BASELINE C	STA. 300+00.00	N:1155110.7895 E:2434651.1762	STA. 304+00.00	N: 1155472.6542 E: 2434821.6283
L5	BASELINE D	STA. 400+00.00	N:1155032.4126 E:2434778.7413	STA. 404+00.00	N: 1155376.1986 E: 2434983.2191
L6	BASELINE E	STA. 500+00.00	N:1154942.5557 E:2434901.2249	STA. 504+00.00	N: 1155265.0765 E: 2435137.8265

ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L8	BASELINE F	STA. 600+00.00	N:1154849.8328 E:2435027.6161	STA. 604+00.00	N: 1155172.3536 E: 2435264.2177
L7	BASELINE G	STA. 700+00.00	N:1154757.1181 E:2435153.9961	STA. 704+00.00	N: 1155079.6390 E: 2435390.5976
L4	TAXIWAY CENTERLINE	STA. 800+00.00	N:1155299.5701 E:2434250.4084	STA. 804+87.72	N: 1155091.7325 E: 2434691.6232
C1	TAXIWAY CENTERLINE	CENTER STA. 805+07.05	PI N:1155083.4949 PI E:2434709.1107	R = 200.000	CENTER N: 1154910.8015 E: 2434606.3942
L9	TAXIWAY CENTERLINE	STA. 805+26.26	N:1155072.0606 E:2434724.6968	STA. 817+00.00	N: 1154377.7765 E: 2435671.0800

FILE: 1303501-CP101.dwg  
UPDATE BY: TJ Heavisides  
PLOT DATE: 7/12/2013 8:32 AM  
SPI-BASE CURRENT GEOMETRY KEYMAP

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ADDITIVE ALTERNATE #1 - MILL 4" SURFACE AND NEW 4" SURFACE. BASE BID - SEAL COAT. IF ADDITIVE ALTERNATE #1 IS AWARDED THE SEAL COAT ITEM SHALL BE DELETED.

**LEGEND**

	MILL 4" BITUMINOUS SURFACE COURSE
	NEW 4" BITUMINOUS SURFACE COURSE (401)
	REMOVE AND REPLACE PAVEMENT
	SEAL COAT - BASE BID
	PAVEMENT REPAIR AROUND INLETS

**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**PROPOSED IMPROVEMENTS**

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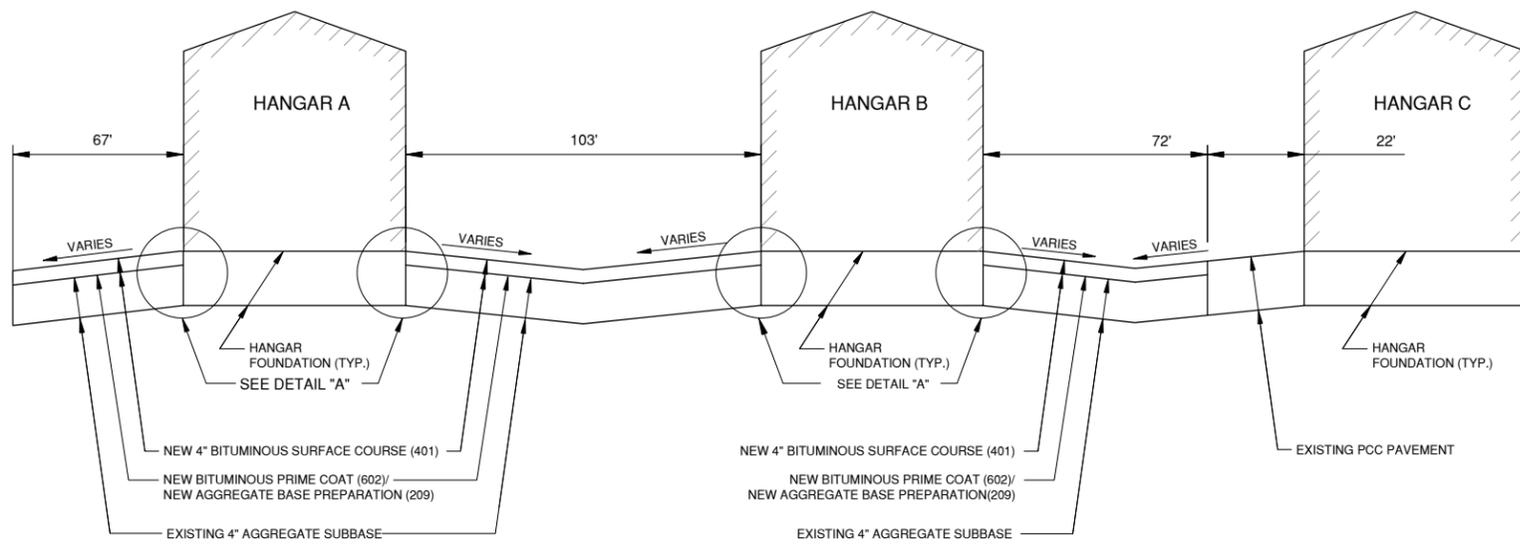
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APPROVED BY:	RLV
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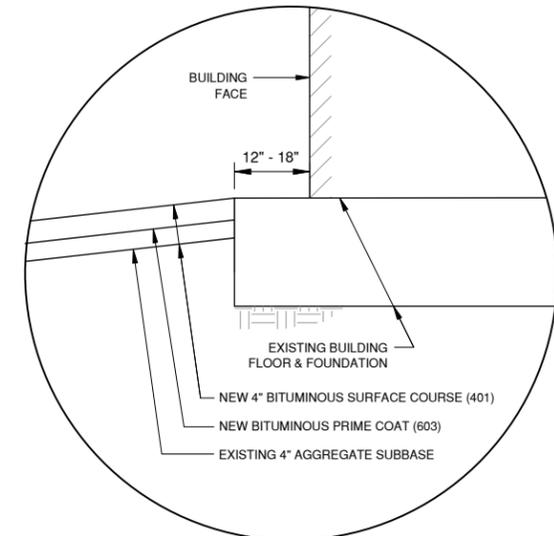
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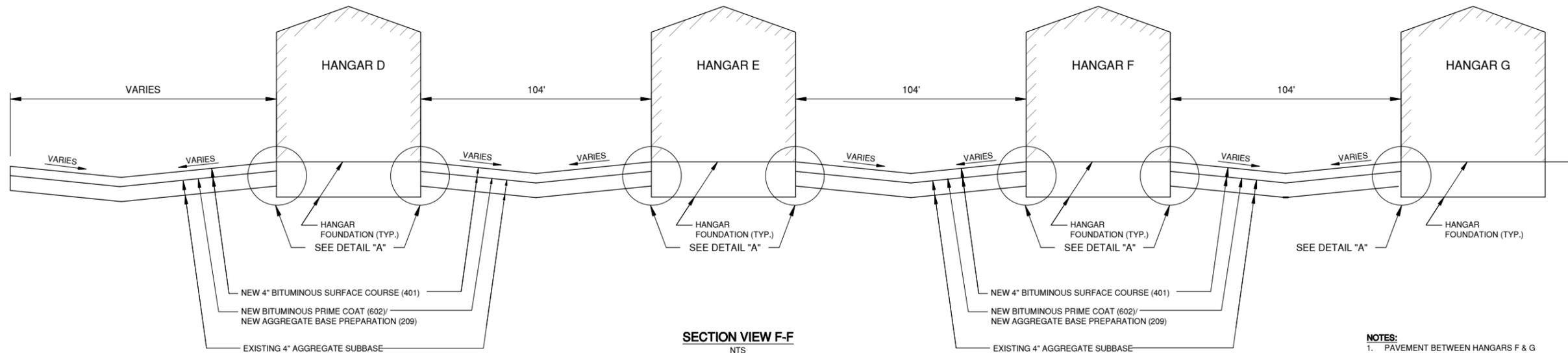
0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



**SECTION VIEW E-E**  
 NTS



**DETAIL "A"**  
 NTS



**SECTION VIEW F-F**  
 NTS

**NOTES:**  
 1. PAVEMENT BETWEEN HANGARS F & G SHALL BE SEAL COATED UNDER THE BASE BID. THE ADDITIVE ALTERNATE #1 IS SHOWN IN THE ABOVE TYPICAL SECTION.



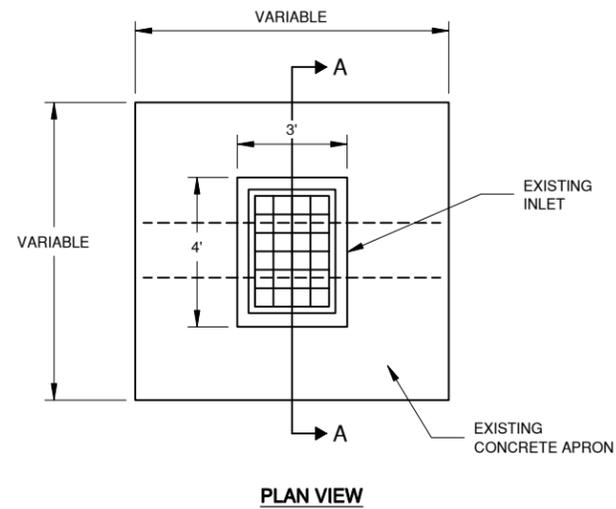
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**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**TYPICAL SECTIONS**

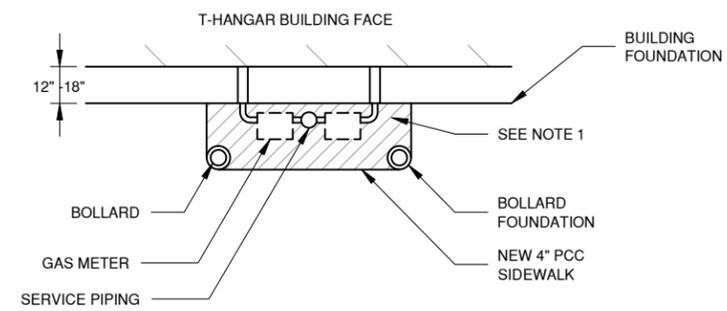
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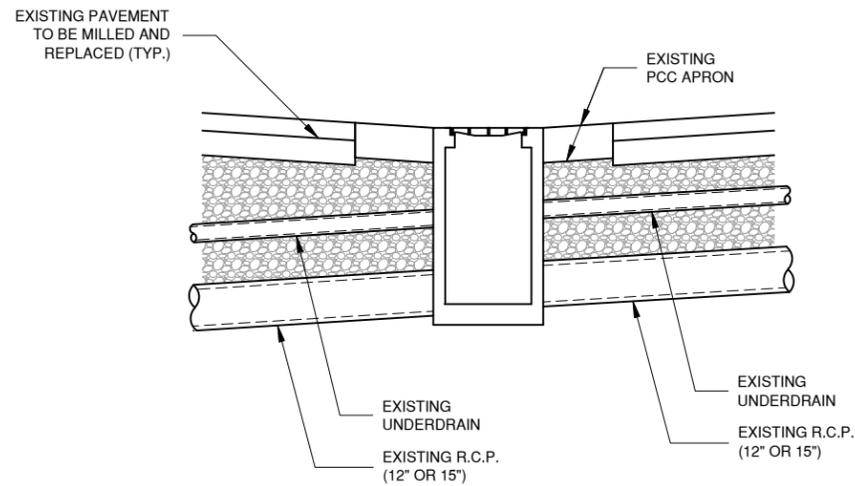
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SHEET	15 OF 20 SHEETS



**PLAN VIEW**



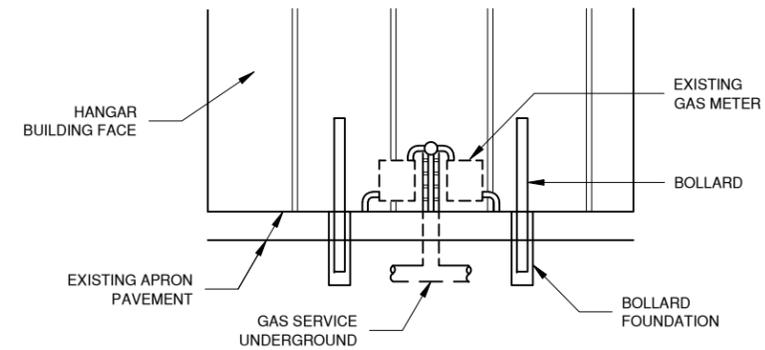
**GAS METER PLAN VIEW**



**SECTION A-A**

**INLET PAVEMENT DETAIL**

NTS



**GAS METER ELEVATION VIEW**

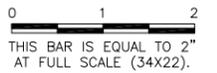
- NOTE:  
 1. REMOVE EXISTING BITUMINOUS PAVEMENT IN THE HATCH AREA SHOWN. GRADE EXISTING SUB BASE FOR 4\"/>

**GAS METER PAVEMENT REMOVAL DETAIL**

NTS

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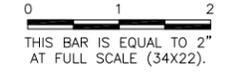
REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 DRAINAGE AND GAS METER DETAILS

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**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**STAKING PLAN 1**



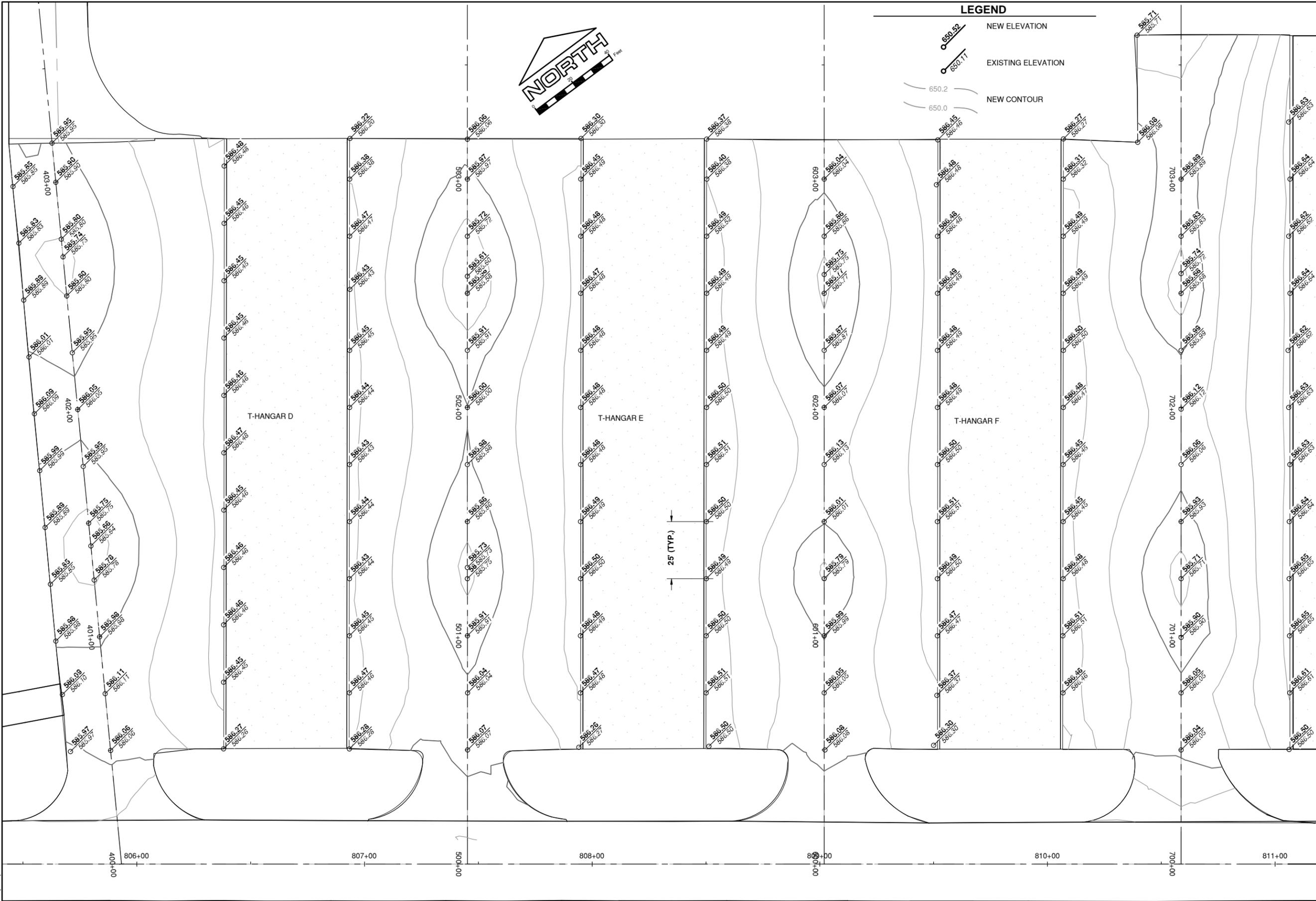
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SHEET 17 OF 20 SHEETS	



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**LEGEND**

- NEW ELEVATION
- EXISTING ELEVATION
- 650.2 NEW CONTOUR
- 650.0



FILE: 1303501-CG102.dwg  
 UPDATE BY: TJ Heavisides  
 PLOT DATE: 7/12/2013 8:33 AM

SPI-BASE CURRENT GEOMETRY  
 1303501\_BASE\_PROP\_GEO

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**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

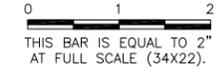
**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**STAKING PLAN 2**

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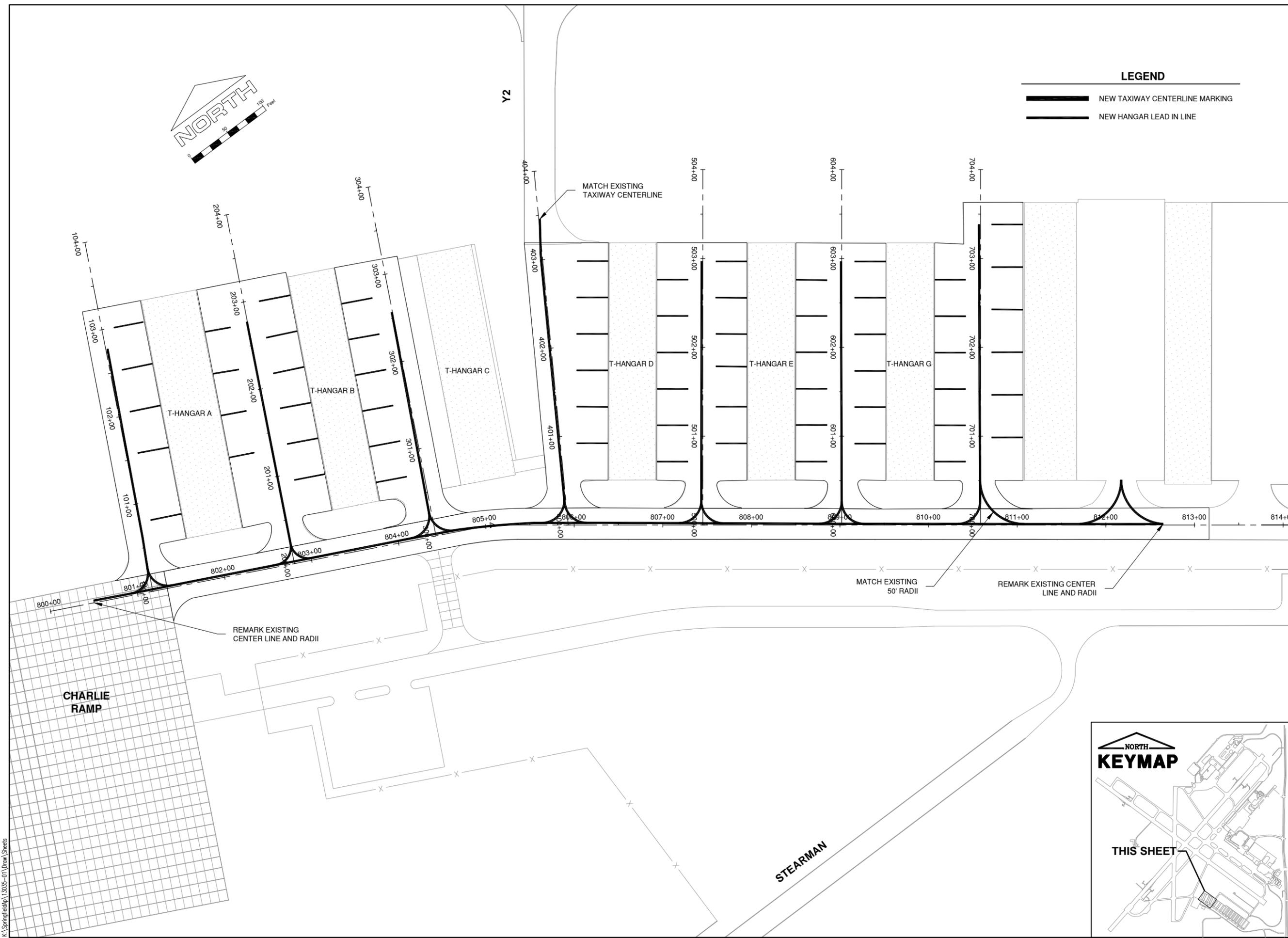
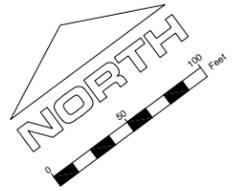
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**LEGEND**

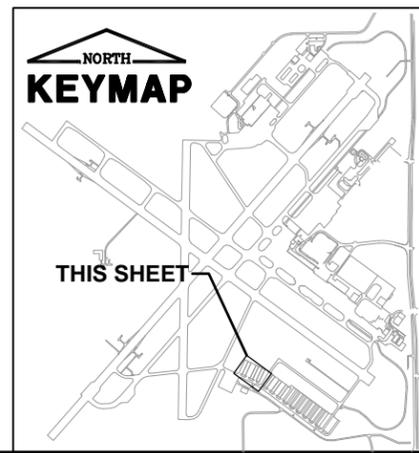
- NEW TAXIWAY CENTERLINE MARKING
- NEW HANGAR LEAD IN LINE



  
**SPRINGFIELD AIRPORT AUTHORITY**  
**ABRAHAM LINCOLN CAPITAL AIRPORT**  
**SPRINGFIELD, ILLINOIS**

**REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES**  
**MARKING PLAN**

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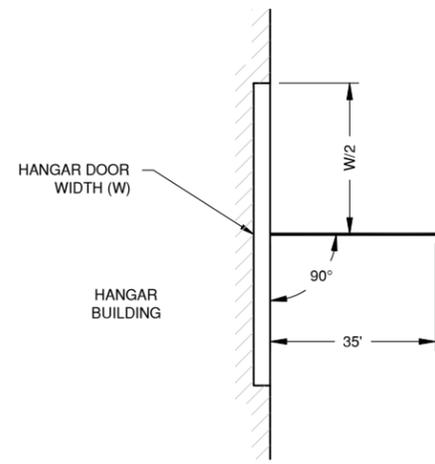
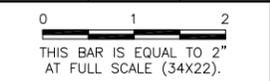


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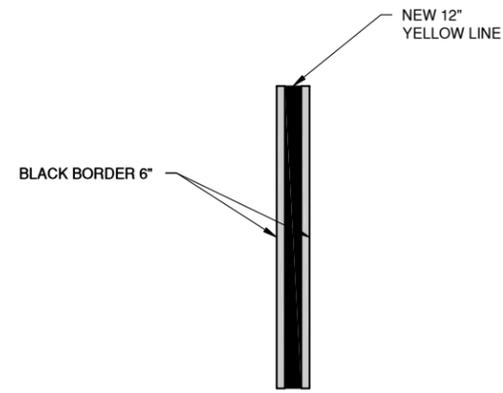
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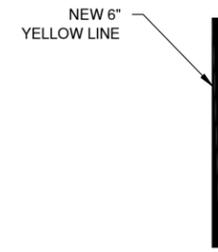
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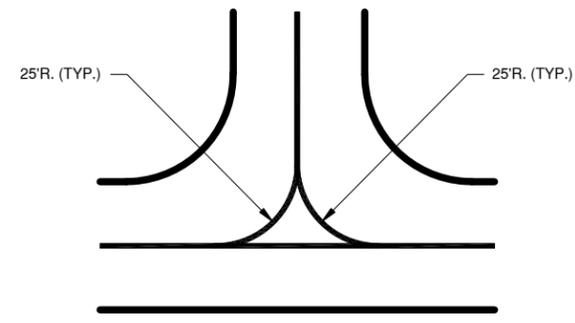
**LEAD-IN MARKING  
 LOCATION DETAIL**  
 N.T.S.



**TAXIWAY/TAXILANE  
 CENTER LINE MARKING**  
 N.T.S.

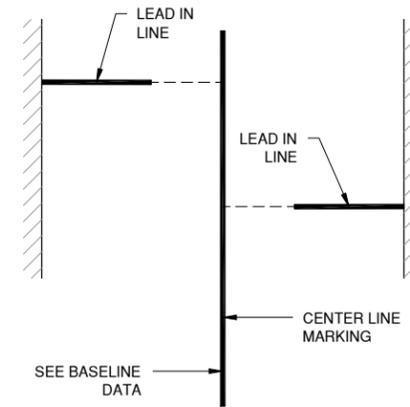


**HANGAR LEAD IN LINE**  
 N.T.S.



**TAXILANE TURN IN  
 MARKING DETAIL**  
 N.T.S.

- NOTE:**
1. PORTIONS OF THE EXISTING TAXILANE TURN IN MARKING MAY NEED TO BE REMOVED PRIOR TO MARKING NEW TURN IN. PAYMENT MADE FOR REMOVAL UNDER AR620900 - REMOVE PAVEMENT MARKING.
  2. RADIUS IS A TYPICAL 25' UNLESS SHOWN OTHERWISE.



**TAXILANE MARKING  
 DETAIL**  
 N.T.S.



SPRINGFIELD AIRPORT AUTHORITY  
 ABRAHAM LINCOLN CAPITAL AIRPORT  
 SPRINGFIELD, ILLINOIS

REHABILITATE CHARLIE RAMP T-HANGAR TAXILANES  
 MARKING DETAILS

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