

FOR INDEX OF SHEETS, SEE SHEET NO. 2

08-01-2025 LETTING ITEM 031

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROPOSED  
HIGHWAY PLANS

FAP ROUTE 317 (US24)  
SECTION (15VB, F) BRR  
PROJECT NHPP-MJTR (673)  
IDOT D4 BRIDGE PERSERVATION  
FULTON COUNTY

C-94-059-24

HIGHWAY STANDARDS

- 000001-08 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS  
001006 DECIMAL OF AN INCH AND OF A FOOT  
482001-02 HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT  
482006-03 HMA SHOULDER ADJACENT TO RIGID PAVEMENT  
483001-06 PCC SHOULDER  
630001-13 STEEL PLATE BEAM GUARDRAIL  
630301-09 SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS  
631031-18 TRAFFIC BARRIER TERMINAL, TYPE 6  
701006-05 OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE  
701011-04 OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY  
701201-05 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH  
701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS  
701306-04 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH  
701321-19 LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER  
701326-04 LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS GREATER THAN OR EQUAL TO 45 MPH  
701901-10 TRAFFIC CONTROL DEVICES  
704001-08 TEMPORARY CONCRETE BARRIER  
780001-05 TYPICAL PAVEMENT MARKINGS  
781001-04 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS  
782006-01 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

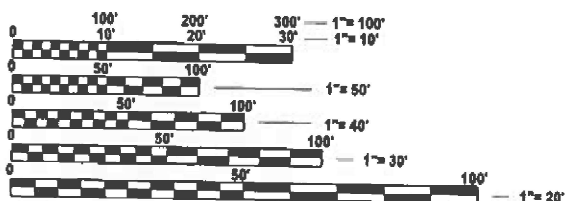
DISTRICT 4 DETAILS

- 406101 BUTT JOINTS  
630101 GUARDRAIL EROSION CONTROL TREATMENTS  
780001 TYPICAL PAVEMENT MARKINGS

TRAFFIC DATA

SN 029-0007  
US 24 OVER BNSF RAILROAD

2023 ADT 1350  
MU% 13.3%  
SU% 8.9%  
POSTED SPEED LIMIT 55 MPH

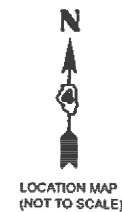


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER DAVID BROVIK (309)671-3493  
PROJECT MANAGER ELIAS ELDERZI (309)671-3482  
CATALOG NO. 036506-00D  
CONTRACT NO. 68J13

GROSS LENGTH = 805 FT. = 0.152 MILE  
NET LENGTH = 805 FT. = 0.152 MILE



SN 029-0007  
STA. 228+53 TO  
STA. 231+74



ERIC J. MESCHÉWSKI ILLINOIS P.E. 062-065709  
EXPIRES 11/30/2025  
SIGNATURE AND SEAL APPLY TO ALL SHEETS  
EXCEPT AS NOTED BELOW:  
SHEETS 18-30

5/2/2025  
DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	1
		ILLINOIS	CONTRACT NO. 68J13	

D-94-037-24



BRIDGE DECK & APPROACH SLAB REPAIR. BRIDGE  
EXPANSION JOINT REPLACEMENT. ROADWAY TRANSITION  
AND GUARDRAIL REPLACEMENT TO SN 029-0007.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED May 9 20 25  
June 27 20 25  
REGIONAL ENGINEER  
ENGINEER OF DESIGN AND ENVIRONMENT  
June 27 20 25  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

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PC:204 00

204.00

EFFECTIVE JUNE 1, 1999

REVISED NOVEMBER 1, 2020

ENVIRONMENTAL REVIEWS

PRIOR TO THE USE OF ANY PROPOSED BORROW AREAS, USE AREAS (TEMPORARY ACCESS ROADS, DETOURS, RUN-AROUNDS, ETC.) AND/OR WASTE AREAS, THE CONTRACTOR SHALL FILE THE REQUIRED ENVIRONMENTAL RESOURCE REQUEST SURVEYS ACCORDING TO SECTION 107.22 OF THE STANDARD SPECIFICATIONS. THESE SURVEYS ARE REQUIRED IN ORDER FOR THE DEPARTMENT TO CONDUCT CULTURAL AND BIOLOGICAL RESOURCE SURVEYS FOR THE PROPOSED SITE.

THE REQUIRED ENVIRONMENTAL RESOURCE DOCUMENTATION SHALL INCLUDE THE FOLLOWING:

- BDE FORM 2289 (BORROW SITE REVIEW)
- BDE FORM 2290 (WASTE/USE AREA REVIEW)
- A LOCATION MAP SHOWING THE SIZE LIMITS AND LOCATION OF THE USE AREA
- COLOR PHOTOGRAPHS DEPICTING THE USE AREA
- BORROW AREA ENTRY AGREEMENT FORM - D4 PI0101

PRIOR TO ANY WASTE MATERIALS BEING REMOVED FROM THE CONSTRUCTION SITE THE REQUIRED ENVIRONMENTAL RESOURCE SURVEYS SHALL BE OBTAINED AND FILED BY THE CONTRACTOR. EXCESS WASTE PRODUCTS REMOVED FROM THE CONSTRUCTION SITE SHALL BE DISPOSED OF AS REQUIRED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS.

ANY PROTRUDING METAL BARS SHALL BE REMOVED PRIOR TO THE DISPOSAL OF BROKEN CONCRETE AT APPROVED DISPOSAL SITES.

PLEASE NOTE THAT A MINIMUM OF FOUR WEEKS SHALL BE ALLOWED FOR THE DISTRICT TO OBTAIN THE REQUIRED WASTE SITE ENVIRONMENTAL CLEARANCES AND SIX WEEKS FOR THE REQUIRED BORROW SITE ENVIRONMENTAL CLEARANCES.

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOURS NOTIFICATION IS REQUIRED.
- THE MATERIAL USED FOR CONSTRUCTION OF PERMANENT AGGREGATE DRIVEWAYS SHALL BE GRAVEL OR CRUSHED STONE, AS DIRECTED BY THE ENGINEER, TO REPLACE IN KIND THE EXISTING AGGREGATE DRIVEWAYS. NO ADDITIONAL COMPENSATION SHALL BE PROVIDED FOR THIS REQUIREMENT, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE PAY ITEM FOR THE AGGREGATE AS SPECIFIED ON THE PLANS.
- BUTT JOINTS SHALL NOT BE MILLED MORE THAN THREE (3) DAYS PRIOR TO PLACEMENT OF THE HMA SURFACE COURSE.
- PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL PAY ITEM.
- THE RESIDENT ENGINEER SHALL CONTACT OPERATIONS TO VERIFY THE LOCATION OF NO PASSING ZONES PRIOR TO PLACEMENT OF CENTERLINE PAVEMENT MARKINGS.
- CONTINUOUS PAVING OPERATIONS ON THE MAIN ROADWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION OF THE HOT-MIX ASPHALT SURFACE. NO INTERRUPTIONS FOR SIDE ROADS, ENTRANCES, TURN LANES, ETC. WILL BE ALLOWED.
- STATIONING IS APPROXIMATE AND IS PROVIDED FOR REFERENCE ONLY.
- AFTER PLACEMENT OF THE BRIDGE DECK OVERLAY, THE RESIDENT ENGINEER SHALL NOTIFY THE DISTRICT BRIDGE MAINTENANCE ENGINEER OF THE "AS CONSTRUCTED" MILLING DEPTH AND OVERLAY THICKNESS FOR UPDATING THE ILLINOIS HIGHWAY INFORMATION SYSTEM.
- THE ENGINEER SHALL DETERMINE ANY PROFILE ADJUSTMENTS REQUIRED ON THE APPROACH PAVEMENTS AND APPROACH ROADWAYS TO ADDRESS SETTLED APPROACH PAVEMENT. ESTIMATE BUTT JOINT AND HMA OVERLAY LIMITS ARE PROVIDED IN THE PLANS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE OF STAGING AREA OR REMOVAL LINES SHOWN ON PLANS SHALL BE REPLACED AT NO COST TO THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS, PAVEMENT PATCHING, OR DRAINAGE ADJUSTMENT PRIOR TO MILLING OR RESURFACING.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OF REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE RESIDENT ENGINEER SHALL CONTACT THE IDOT AREA TRAFFIC ENGINEER A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH SHALL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- GRATES AND DRAINAGE STRUCTURES ON SHOULDERS SHALL BE SECURED BEFORE TRAFFIC IS SHIFTED ONTO THEM.

TEMPORARY TRAFFIC SIGNAL NOTES

- TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH STANDARD 701321 EXCEPT WHEN MODIFIED ON THESE PLAN SHEETS.
- THE CONTRACTOR SHALL INSTALL EITHER DETECTOR LOOPS OR MICROWAVE DETECTORS FOR USE WITH THE TEMPRARY TRAFFIC SIGNALS IN ACCORDANCE WITH HIGHWAY STANDRAD 701321.
- THE ENGINEER OF TRAFFIC SHALL APPROVE ALL TIMING PARAMETERS.
- THE CONTRACTOR SHALL CONTACT TONY BRIDSON, DISTRICT 4 TRAFFIC SIGNAL TECHNICIAN, AT (309) 672-4464, FORTY-EIGHT (48) HOURS PRIOR TO SIGNAL TURN-ON.
- ALL TRAFFIC SIGNAL SECTIONS SHALL HAVE 12" DIAMETER LED LENSES.
- THE TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PLACED AT THE LOCATIONS INDICATED ON THE PLAN SHEETS OR AS DIRECTED BY THE ENGINEER.
- THE TEMPORARY TRAFFIC SIGNAL INSTALLATION SHALL CONFORM TO ALL MUTCD STANDARDS.
- A MINIMUM OF 2 SIGNAL HEADS ARE REQUIRED PER APPROACH.
- ALL SIGNAL HEADS SHALL BE EQUIPPED WITH REFLECTORIZED BACKPLATES.
- ALL LABOR, EQUIPMENT, AND MATERIALS REQUIRED TO COMPLY WITH THESE REQUIREMENTS AND PLAN SHEET DETAILS SHALL BE INCLUDED IN THE CONTRACT BID PRICE FOR THE TEMPORARY BRIDGE SIGNAL INSTALLATION. THERE WILL BE NO ADDITIONAL COMPENSATION.

MAINTENANCE OF TRAFFIC NOTES

- THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE MOT PLANS.
- ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE MOT PAVEMENT MARKINGS SHALL BE REMOVED. ALL TEMPORARY PAVEMENT MARKINGS WHICH CONFLICT WITH THE NEXT STAGE SHALL BE REMOVED. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED BY COMPLETION OF THE PROJECT.
- ALL TRAFFIC CONTROL DEVICES USED FOR MOT, AS DETAILED IN THE PLANS OR HIGHWAY STANDARDS SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN THE MOT SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
- ALL EXISTING SIGNS WITHIN MOT LIMITS WHICH ARE OBSCURED BY OR CONFLICT WITH CONSTRUCTION OPERATIONS AND MOT, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.
- A FULL-DEPTH SAW CUT SHALL BE REQUIRED BETWEEN PAVEMENT, CURB, AND/OR GUTTER TO REMAIN AND PAVEMENT, CURB AND/OR GUTTER TO BE REMOVED PRIOR TO BEGINNING OF REMOVAL WORK. COST TO BE INCLUDED WITH APPLICABLE REMOVAL ITEMS.
- THE PAY ITEM "BARRIER WALL REFLECTORS, TYPE C" IS INTENDED SOLELY TO BE USED FOR REFLECTOR APPLICATIONS ON PERMANENT AND EXISTING CONCRETE BARRIER AND IS NOT TO BE USED FOR TEMPORARY CONCRETE BARRIER WHERE THE TYPE C REFLECTOR IS CONSIDERED INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER PER HIGHWAY STANDARD 704001.
- DUE TO PROPOSED TEMPORARY WIDTH RESTRICTIONS, THE CONTRACTOR MUST CONTACT THE IDOT D4 TRAFFIC CONTROL SUPERVISOR 21 DAYS IN ADVANCE OF BEGINNING WORK.

JOB-SPECIFIC NOTES

- ALL DAMAGE DUE TO THE CONTRACTOR OPERATION TO PAVEMENT AND RPM'S OUTSIDE THE REMOVAL LIMITS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

SURFACE TYPE	RESIDUAL RATE (POUND/SQ FT)
MILLED (HMA OR PCC)	0.08
EXISTING PAVEMENT	0.08
FOG COAT (BETWEEN LIFTS)	0.08

MODEL: SN 029-0007  
FILE NAME: X:\0H\2023\20230225-06\Design\CADD\Rdwy\Sheets\Other\0468\13D468\13-shit-gen-note.dgn

<div><div>GR<sup>AE</sup>EF</div><div>8501 W. Higgins Road, Suite 280 Chicago, Illinois 60634 (773) 399-012</div></div>	USER NAME = 2189	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, LIST OF HIGHWAY STANDARDS, AND GENERAL NOTES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						317	(15VB,F)BRR	FULTON	38	2
		CHECKED -	REVISED -						CONTRACT NO. 68J13				
	PLOT DATE = 5/8/2025	DATE -	REVISED -		SCALE: N/A	SHEET 1	OF 1	SHEETS	STA. N/A	TO STA. N/A	ILLINOIS FED. AID PROJECT		

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

The following mixture requirements are applicable for this project:				
Location(s):	Mainline		Shoulder	Shoulder
Mixture Use(s):	Polymerized Surface 1.5"		Shoulders	HMA Pavement Widening
AC/PG:	PG 70-28		PG 58-28	PG 58-22
Design Air Voids:	4.0% @ N=50		4.0% @ N=50	4.0% @ N=50
Mixture Composition: (Mixture Gradation)	IL 9.5		IL 9.5	IL 19.0
Friction Aggregate:	Mix D		Mix D	N/A
Quality Management Program:	QCQA		QCQA	QCQA
MTD:	No		No	No
<div>Note: 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.</div> <div>2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y./in., unless otherwise noted.</div> <div>3) Sublot sizes for PTP and QCP mixes will be 10 00 tons, unless otherwise agreed to by the Engineer and the paving contractor.</div>				

MODEL: HMA Mix Table  
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MODEL: SQO 01  
FILE NAME: X:\O\H\2023\20230225-06\Design\CADD\Drawn\Sheets\Other\0468J13D468J13-shh-SQO.dgn

GRAEF

8501 W. Higgins Road, Suite 280

Chicago, Illinois 60634 (773) 399-002

USER NAME	= 2189	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 5/20/2025	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES				
SCALE:	N/A	SHEET 1	OF 5	SHEETS
STA.	N/A	TO STA.	N/A	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB,F)BRR	FULTON	38	4
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

				CONSTRUCTION CODE
				80% FEDERAL
				20% STATE
				BRIDGE
				0047
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	S.N. 029-0007
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	3	3
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	135	135
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	168	168
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	98	98
40600990	TEMPORARY RAMP	SQ YD	100	100
40604160	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	19.6	19.6
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	134	134
44000300	CURB REMOVAL	FOOT	60	60
44004250	PAVED SHOULDER REMOVAL	SQ YD	136	136
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	136	136
50102400	CONCRETE REMOVAL	CU YD	9.9	9.9
50300255	CONCRETE SUPERSTRUCTURE	CU YD	9.9	9.9
50300300	PROTECTIVE COAT	SQ YD	1,150	1,150
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1,280	1,280



MODEL: SQO.02  
FILE NAME: X:\O\H\2023\20230256-06\Design\CADD\Drawn\Sheets\Other\0468\13D\468\13-shh-SQO.dgn

GRAEF

8501 W. Higgins Road, Suite 280

Chicago, Illinois 60634 (773) 399-002

USER NAME	= 2189
PLOT DATE	= 5/20/2025

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
SCALE:	N/A	SHEET 2	OF 5 SHEETS
STA.	N/A	TO STA.	N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB,F)BRR	FULTON	38	5
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

				CONSTRUCTION CODE
				80% FEDERAL 20% STATE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0047
				S.N. 029-0007
50800515	BAR SPLICERS	EACH	24	24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	68	68
60260100	INLETS TO BE ADJUSTED	EACH	2	2
60600605	CONCRETE CURB, TYPE B	FOOT	60	60
*	63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	167
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1
*	63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	3
63200310	GUARDRAIL REMOVAL	FOOT	415	415
67100100	MOBILIZATION	L SUM	1	1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1

\*= SPECIALTY ITEM

MODEL: SQO.03  
FILE NAME: X:\O\H\2023\20230256-06\Design\CADD\Drawn\Sheets\Other\0468J13D468J13-shh-SQO.dgn

				CONSTRUCTION CODE
				80% FEDERAL 20% STATE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0047
				S.N. 029-0007
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	60.00	60.00
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1
70106700	TEMPORARY RUMBLE STRIPS	EACH	6	6
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	14.00	14.00
70300100	SHORT TERM PAVEMENT MARKING	FOOT	201	201
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	67	67
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	2,555	2,555
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	48	48
70400100	TEMPORARY CONCRETE BARRIER	FOOT	500	500
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	500	500
70600251	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
70600352	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE,NARROW), TEST LEVEL 3	EACH	2	2
72400205	REMOVE AND RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1
72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4

\*= SPECIALTY ITEM

GR

EF

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Chicago, Illinois 60634 (773) 399-002

USER NAME = 2189	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 5/20/2025	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
SCALE: N/A	SHEET 3	OF 5 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB,F)BRR	FULTON	38	6
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

MODEL: SQO 04  
FILE NAME: X:\O\H\2023\20230225-06\Design\CADD\Drawn\Sheets\Other\0468J13D468J13-shh-SQO.dgn

				CONSTRUCTION CODE	
				80% FEDERAL 20% STATE	
				BRIDGE	
				0047	
				S.N. 029-0007	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY		
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	3,551	3,551	
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	35	35	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	10	10	
78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	100	100	
* 78200006	GUARDRAIL REFLECTORS, TYPE B	EACH	18	18	
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	20	20	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	110	110	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1,846	1,846	
X0326444	SURFACE FILLER (SPECIAL)	GALLON	1	1	
X5030550	PROTECTIVE COAT (SPECIAL)	SQ YD	407	407	
* X6330725	STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)	FOOT	24	24	
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	123	123	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	15	15	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	93	93	

\*= SPECIALTY ITEM



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DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

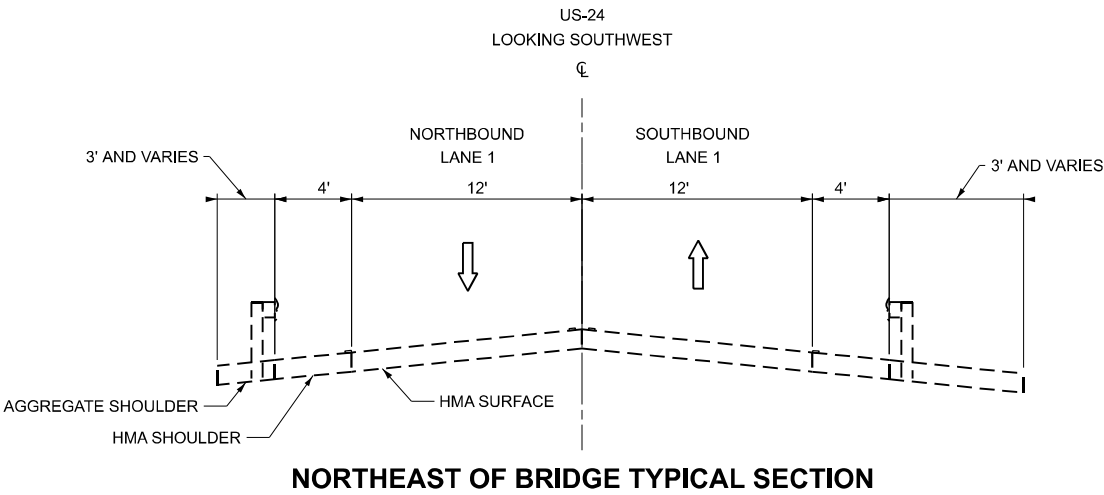
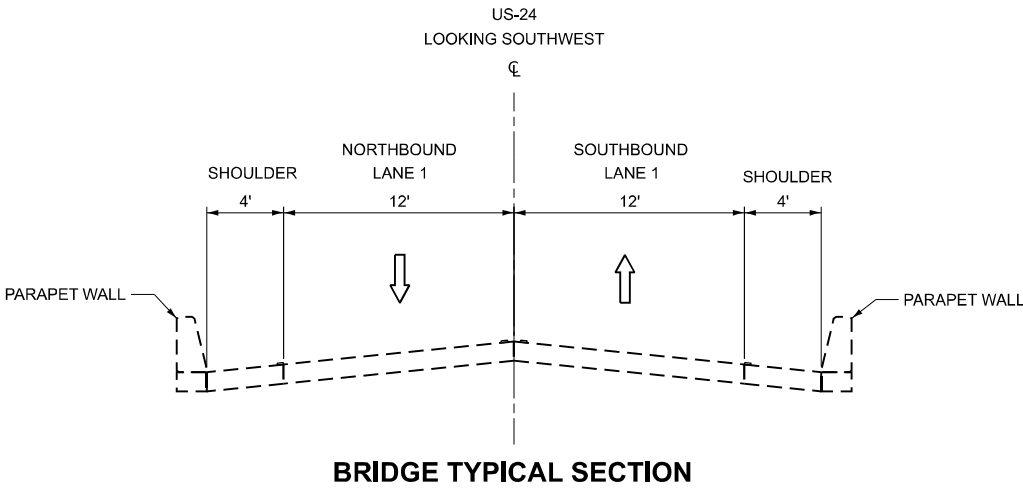
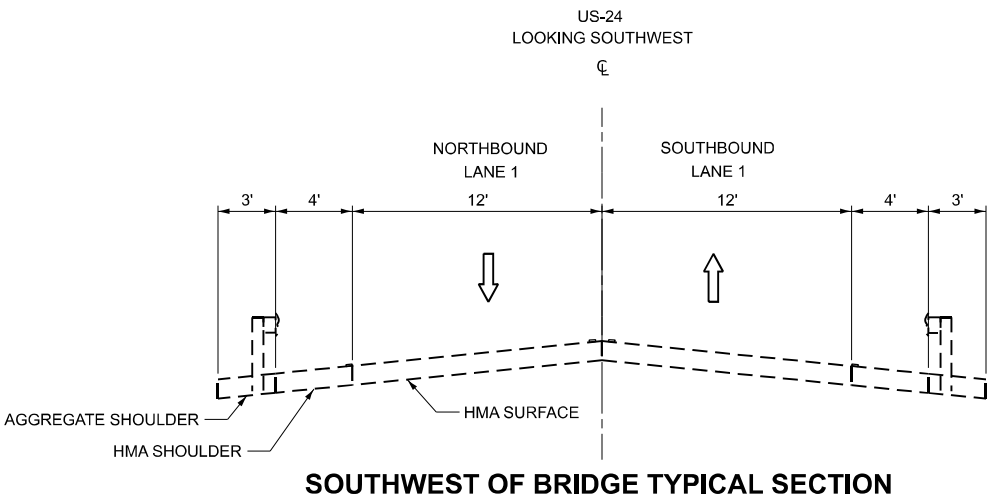
SUMMARY OF QUANTITIES			
SCALE: N/A	SHEET 4	OF 5 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB,F)BRR	FULTON	38	7
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

MODEL: SOQ 05  
FILE NAME: X:\OH\2023\20230256-06\Design\CADD\Rdwy\Sheets\Other\13\468J13-sht-SOQ.dgn

[illegible]

<div><div>GRAEF</div><div>8501 N. Higgins Road, Suite 280 Chicago, Illinois 60634 (773) 399-012</div></div>	USER NAME = 2189	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div>	<div>SUMMARY OF QUANTITIES</div>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						317	(15VB,F)BRR	FULTON	38	8
		CHECKED -	REVISED -		CONTRACT NO. 68J13								
	PLOT DATE = 5/20/2025	DATE	REVISED -										
					SCALE: N/A	SHEET 5	OF 5 SHEETS	STA. N/A	TO STA. N/A				

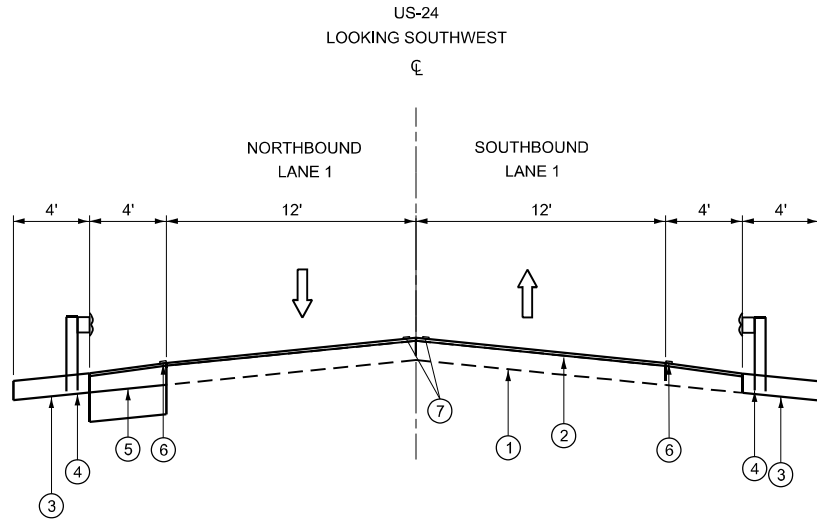


**NOTES**

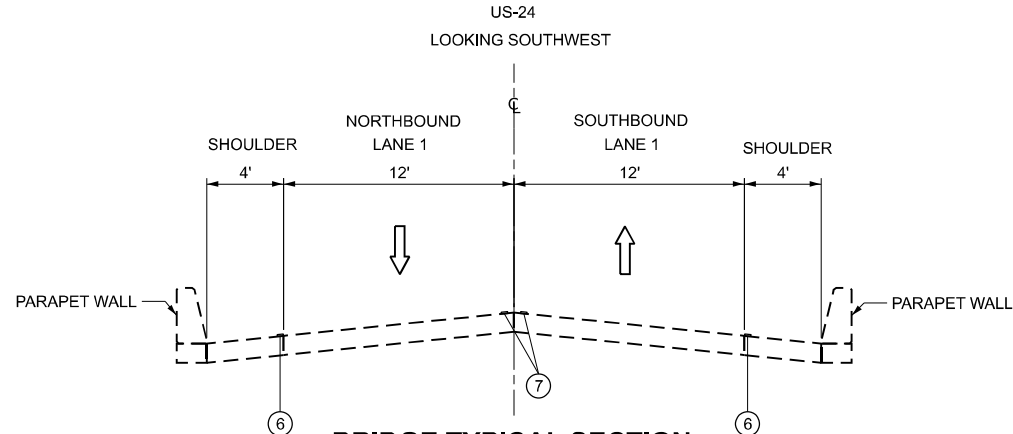
1. RECORD PLANS UNAVAILABLE AT THIS LOCATION

MODEL: SN 029-0007  
FILE NAME: X:\CH\2023\20230256-06\Design\CADD\Draw\Sheets\Plan\468.113\468.113-sh-typical.dgn

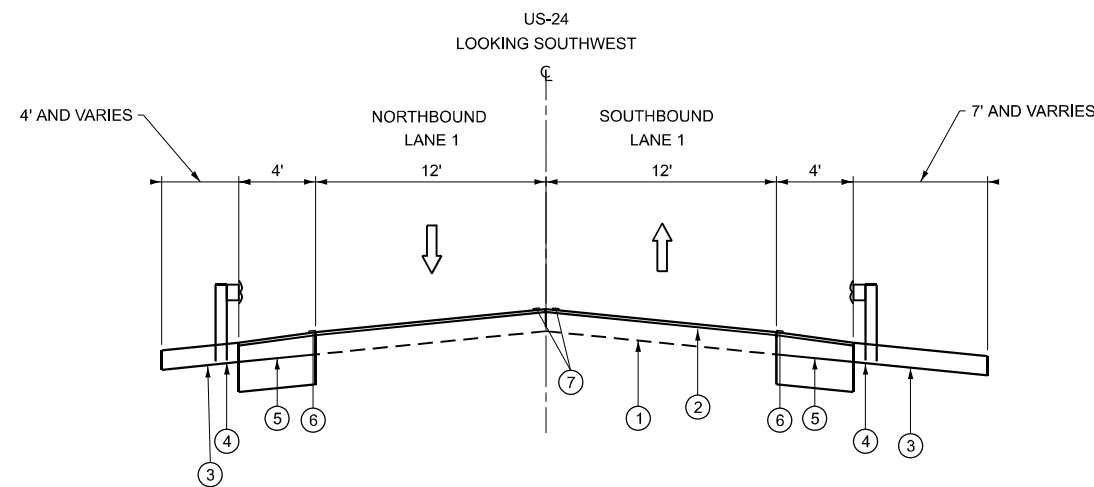
<div>GR<sub>AEF</sub></div> <div>850 W. Higgins Road, Suite 280 Chicago, Illinois 60634 (773) 399-012</div>	USER NAME = 2189	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING TYPICAL SECTIONS SN 029-0007 US-24 OVER BNSF RR			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					317	(15VB,F)BRR	FULTON	38	9
		CHECKED -	REVISED -					CONTRACT NO. 68J13				
	PLOT DATE = 5/8/2025	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



SOUTHWEST OF BRIDGE TYPICAL SECTION



BRIDGE TYPICAL SECTION



NORTHEAST OF BRIDGE TYPICAL SECTION

LEGEND

- ① EXISTING PAVEMENT
- ② POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 (1½")  
HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT  
POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)
- ③ GUARDRAIL AGGREGATE EROSION CONTROL
- ④ STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS
- ⑤ EXCAVATING AND GRADING EXISTING SHOULDER  
HOT-MIX ASPHALT SHOULDERS, 8"  
AGGREGATE SUBGRADE IMPROVEMENT 12"
- ⑥ PAINT PAVEMENT MARKING - LINE 4" (WHITE)
- ⑦ PAINT PAVEMENT MARKING - LINE 4" (YELLOW)  
SEE NOTE 4

NOTES

- 1. SEE ROADWAY PLANS FOR LOCATIONS FOR NEW GUARDRAIL AND SHOULDERS.
- 2. SEE BUTT JOINT SHEETS FOR BUTT JOINT CONSTRUCTION DETAILS.
- 3. BRIDGE TYPICAL SECTION PROVIDED FOR REFERENCE ONLY AND IS NOT INTENDED TO BE REPRESENTATIVE OF THE STRUCTURE ITSELF.
- 4. THE RESIDENT ENGINEER SHALL CONTACT OPERATIONS TO VERIFY THE LOCATION OF NO PASSING ZONES PRIOR TO PLACEMENT OF CENTERLINE PAVEMENT MARKINGS.

MODEL: SN 029-0007  
FILE NAME: X:\CH\2023\20230225-06\Design\CADD\Draw\Sheets\Plan\468.1\3-sh-TypicalProp.dgn

## PAVING SCHEDULE

	EXCAVATING AND GRADING EXISTING SHOULDER	AGGREGATE SUBGRADE IMPROVEMENT 12"	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	POLYMERIZED HOT- MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	HOT-MIX ASPHALT SHOULDERS, 8"	GUARDRAIL AGGREGATE EROSION CONTROL
	20200600	30300112	40600295	40600982	40604160	44000155	48203029	Z0001002
LOCATION	(UNIT)	(SQ YD)	(POUND)	(SQ YD)	(TON)	(SQ YD)	(SQ YD)	(TON)
SN 029-0007								
NW QUADRANT	1	60.4	23	18.0	2.7	13.5	60.5	44
NE QUADRANT	1	42.2	23	18.0	2.7	13.5	42.5	32
SW QUADRANT			61	31.0	7.2	53.5		22
SE QUADRANT	1	32.4	61	31.0	7.2	53.5	33.0	25
SUBTOTAL	3	135	168	98	19.6	134	136	123
GRAND TOTALS	3	135	168	98	19.6	134	136	123

## TEMPORARY RAMP SCHEDULE

	<b>TEMPORARY RAMP</b>
	<b>40600990</b>
<b>LOCATION</b>	<b>(SQ YD)</b>
<b>SN 029-0007</b>	
NORTH APPROACH	36.0
SOUTH APPROACH	36.0
SOUTH PAVEMENT	28.0
<b>SUBTOTAL</b>	<b>100</b>

## PAVED SHOULDER REMOVAL SCHEDULE

	PAVED SHOULDER REMOVAL
	44004250
LOCATION	(SQ YD)
SN 0296-0007	
NORTHEAST QUADRANT	42.7
NORTHWEST QUADRANT	61.0
SOUTHEAST QUADRANT	32.3
SOUTHWEST QUADRANT	0
SUBTOTAL	136
GRAND TOTALS	136

## CURB & GUTTER SCHEDULE

	CURB REMOVAL	CONCRETE CURB, TYPE B
	44000300	60600605
LOCATION	(FOOT)	(FOOT)
SN 029-0007		
ENTIRE LOCATION	60	60
SUBTOTAL	60	60
GRAND TOTALS	60	60

### INLETS TO BE ADJUSTED SCHEDULE

	<b>INLETS TO BE ADJUSTED</b>
	<b>60260100</b>
<b>LOCATION</b>	<b>(EACH)</b>
<b>SN 029-0007</b>	
SE QUADRANT	1
SW QUADRANT	1
<b>SUBTOTAL</b>	<b>2</b>
<b>GRAND TOTALS</b>	<b>2</b>

## GUARDRAIL SCHEDULE

	STEEL PLATE BEAM GUARDRAIL TYPE A, 6 FOOT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 2	TRAFFIC BARRIER TERMINAL, TYPE 6	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	GUARDRAIL REMOVAL	TERMINAL MARKER - DIRECT APPLIED	GUARDRAIL REFLECTORS , TYPE B	STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)
	63000001	63100045	63100085	63100167	63200310	72501000	78200006	63000001
LOCATION	(FOOT)	(EACH)	(EACH)	(EACH)	(FOOT)	(EACH)	(EACH)	(FOOT)
SN 029-0007								
NW QUADRANT	97		1	1	156.8	1	6	
NE QUADRANT	47		1	1	107.8	1	4	
SW QUADRANT		1	1		67.4	1	4	24
SE QUADRANT	23		1	1	83	1	4	
SUBTOTAL	167	1	4	3	415	4	18	24
GRAND TOTALS	167	1	4	3	415	4	18	24

## TRAFFIC CONTROL SURVEILLANCE SCHEDULE

	TRAFFIC CONTROL SURVEILLANCE
	70103815
LOCATION SN 029-0007	(CAL DA)
ENTIRE LOCATION	60
SUBTOTAL	60
GRAND TOTALS	60

## TEMPORARY RUMBLE STRIPS SCHEDULE

	<b>TEMPORARY RUMBLE STRIPS</b>
<b>LOCATION</b>	<b>70106700</b>
<b>SN 029-0007</b>	<b>(EACH)</b>
NORTHBOUND	3
SOUTHBOUND	3
<b>SUBTOTAL</b>	<b>6</b>
<b>GRAND TOTALS</b>	<b>6</b>

## CHANGEABLE MESSAGE SIGN SCHEDULE

	CHANGEABLE MESSAGE SIGN
	70107025
LOCATION	(CAL DA)
SN 036-0041	
ENTIRE LOCATION	14
SUBTOTAL	14
GRAND TOTALS	14



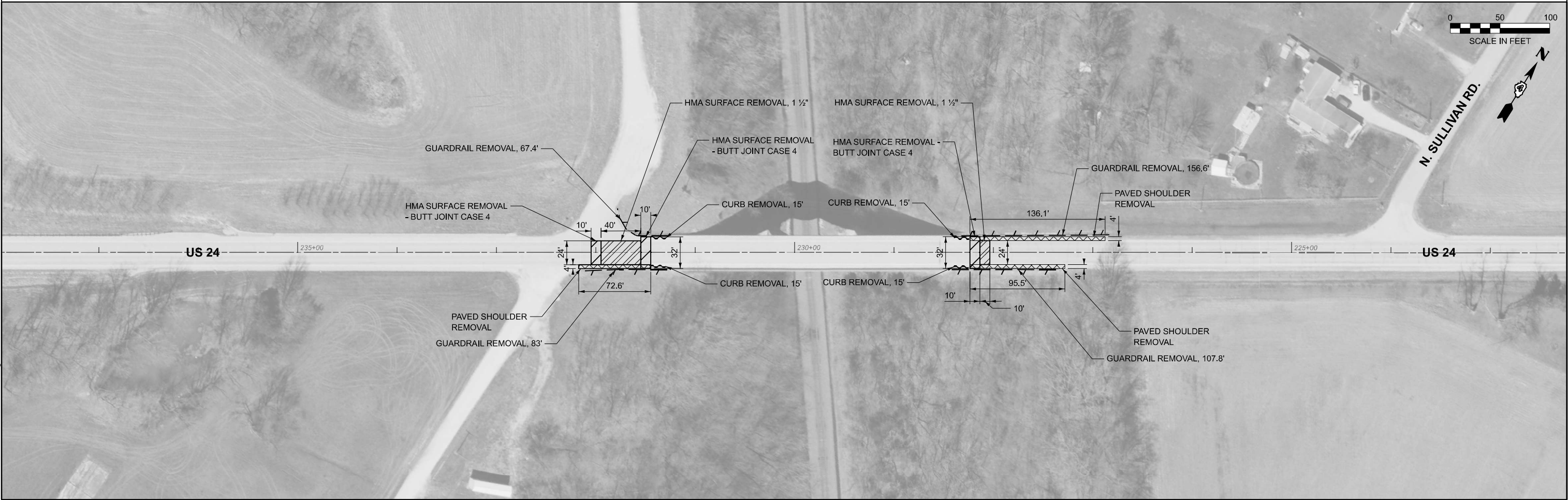
	SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE	PAINT PAVEMENT MARKING - LINE 4"	PAINT PAVEMENT MARKING - LINE 24"	RAISED REFLECTIVE PAVEMENT MARKER	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	PAVEMENT MARKING REMOVAL - WATER BLASTING
	70300100	70300150	70307120	70307210	78001110	78001180	78100100	78100200	78300200	78300202
LOCATION	(FOOT)	(SQ FT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(EACH)	(EACH)	(EACH)	(SQ FT)
SN 029-0007										
ENTIRE LOCATION	201	67	2555	48	3551	35	10	100	110	1846
SUBTOTAL	201	67	2555	48	3551	35	10	100	110	1846
GRAND TOTALS	201	67	2555	48	3551	35	10	100	110	1846

	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (NON REDIRECTIVE, NARROW), TEST LEVEL 3
	70400100	70400200	70600251	70600352
LOCATION SN 029-0007	(FOOT)	(FOOT)	(EACH)	(EACH)
ENTIRE LOCATION	500.0	475.0	2	2
SUBTOTAL	500	475	2	2
GRAND TOTALS	500	475	2	2




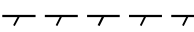

	REMOVE AND RELOCATE SIGN PANEL ASSEMBLY - TYPE A
	72400205
LOCATION	(EACH)
SN 029-0007	
SOUTHWEST CORNER	1
SUBTOTAL	1
GRAND TOTALS	1

	<b>BARRIER WALL REFLECTORS, TYPE C</b>
	<b>78200011</b>
<b>LOCATION</b>	<b>(EACH)</b>
<b>SN 029-0007</b>	
ENTIRE LOCATION	20
<b>SUBTOTAL</b>	<b>20</b>
<b>GRAND TOTALS</b>	<b>20</b>

MODEL: Default  
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**REMOVAL LEGEND**

-  — HMA SURFACE REMOVAL, BUTT JOINT
-  — HMA SURFACE REMOVAL
-  — PAVED SHOULDER REMOVAL
-  — GUARDRAIL REMOVAL
-  — CURB REMOVAL

**GR&EF**  
8501 W. Higgins Road Suite 280  
Chicago, Illinois 60634 (773) 399-012

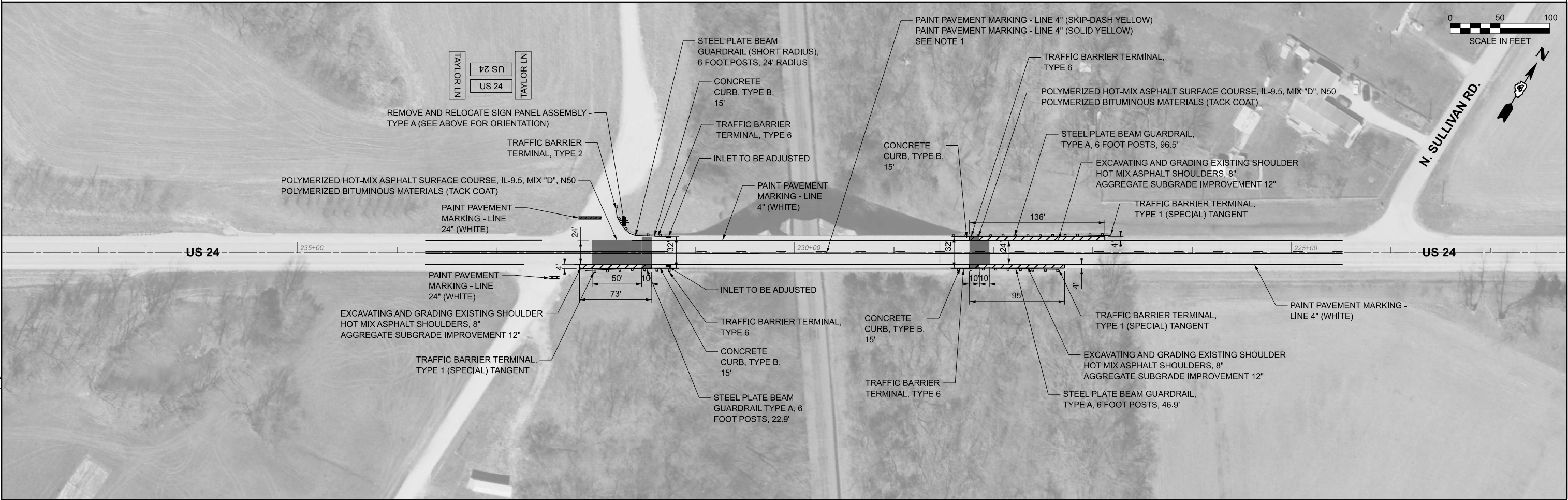
USER NAME = 2189	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 5/8/2025	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN  
SN 029-0007 US-24 OVER BNSF RR**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB,F)BRR	FULTON	38	13
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				



LEGEND

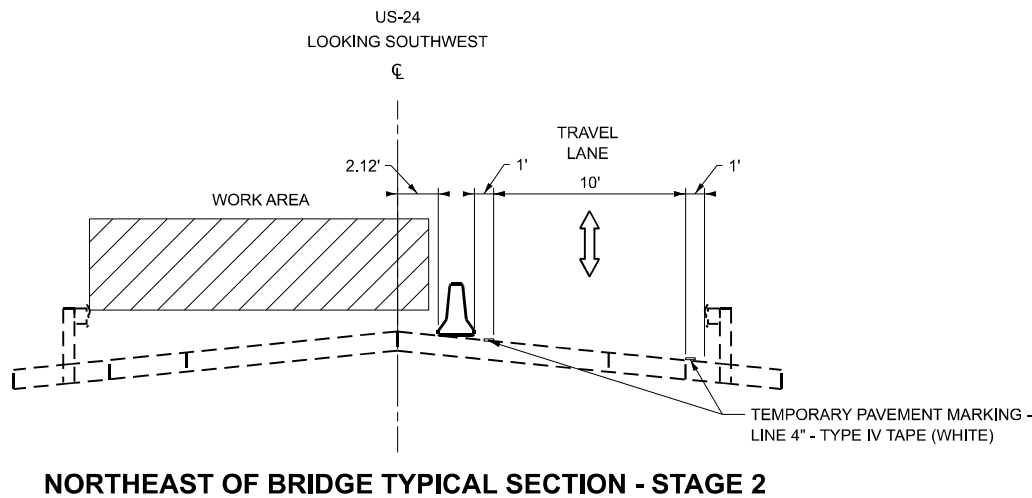
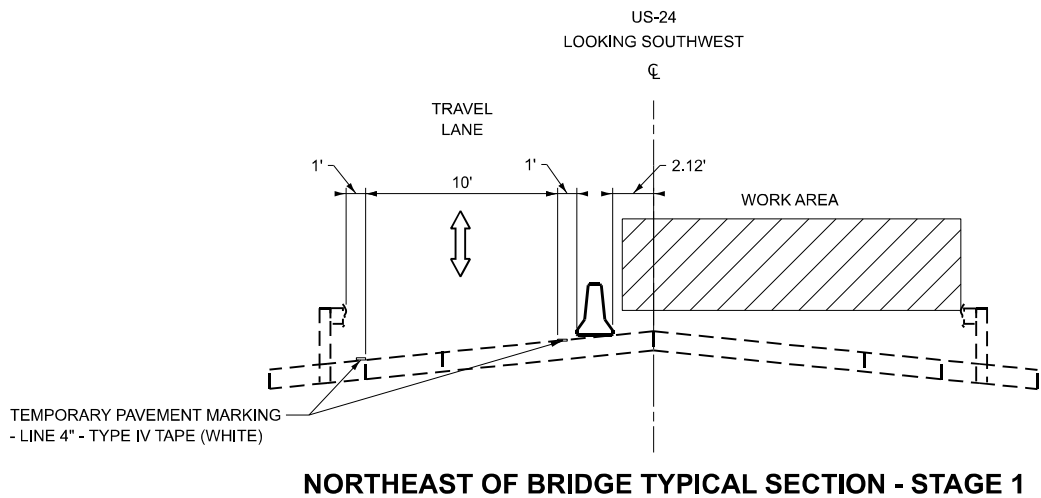
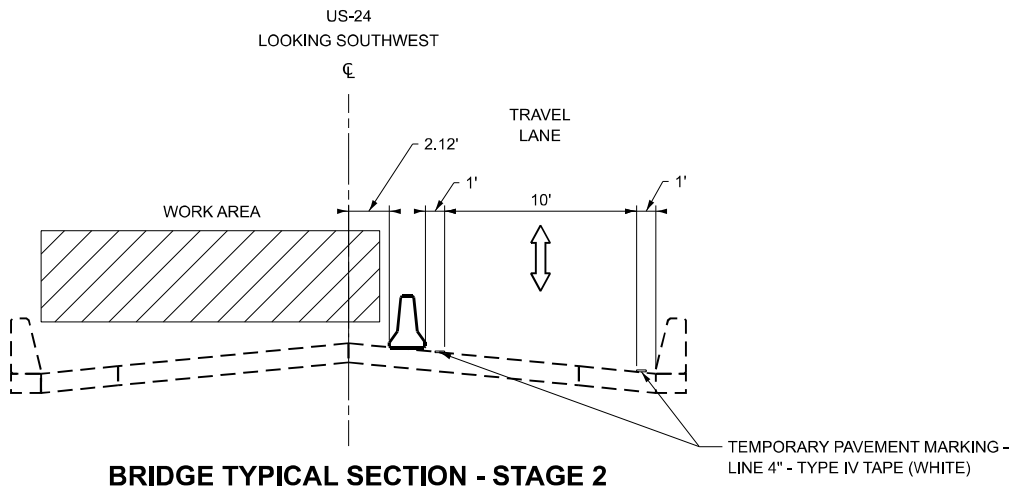
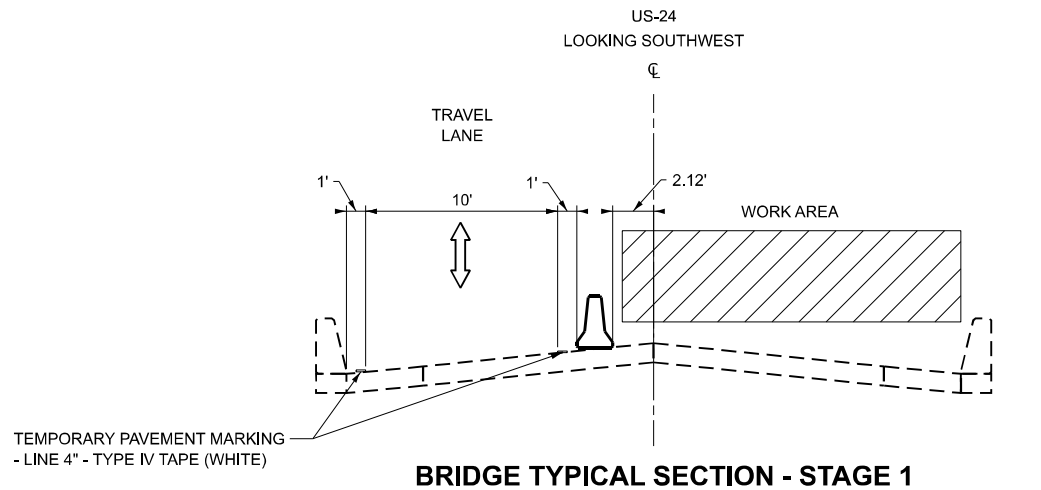
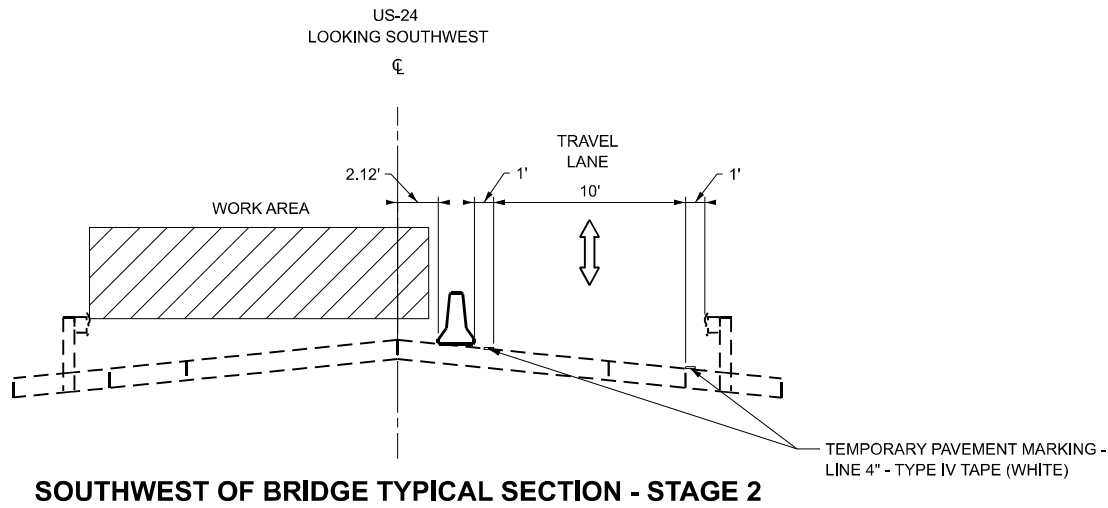
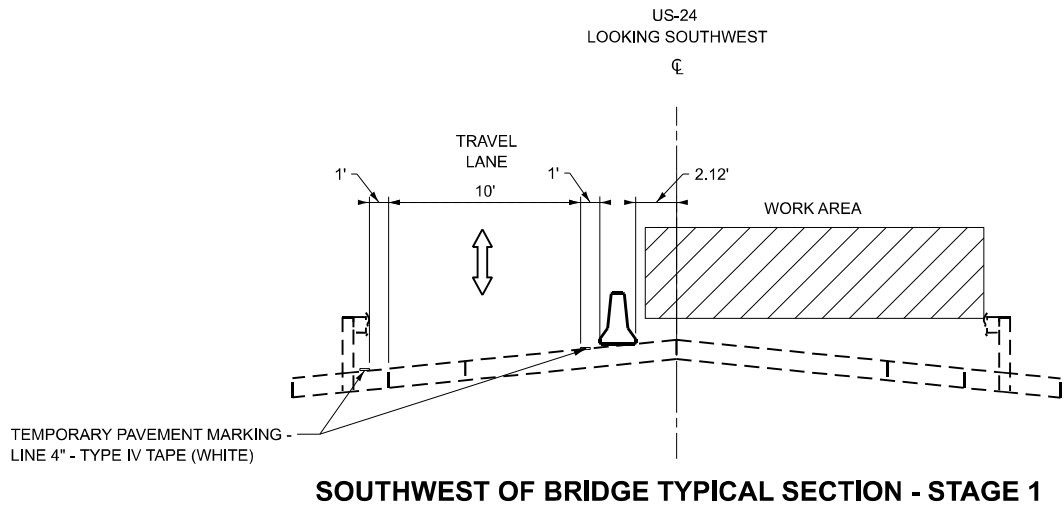
- POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50  
POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)
- STEEL PLATE BEAM GUARDRAIL
- EXCAVATING AND GRADING EXISTING SHOULDER  
HOT MIX ASPHALT SHOULDERS, 8"  
AGGREGATE SUBGRADE IMPROVEMENT 12"

NOTES

- THE RESIDENT ENGINEER SHALL CONTACT OPERATIONS TO VERIFY THE LOCATION OF NO PASSING ZONES PRIOR TO PLACEMENT OF CENTERLINE PAVEMENT MARKINGS

MODEL: Default  
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<div>GR@EF</div> <div>8501 W. Higgins Road Suite 280 Chicago, Illinois 60634 (773) 399-012</div>	USER NAME = 2189		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SN 029-0007 US-24 OVER BNSF RR			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN -	REVISED -					317	(15VB,FJBR)	FULTON	38	14
			CHECKED -	REVISED -					CONTRACT NO. 68J13				
	PLOT DATE = 5/8/2025		DATE -	REVISED -		SCALE:	SHEET	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



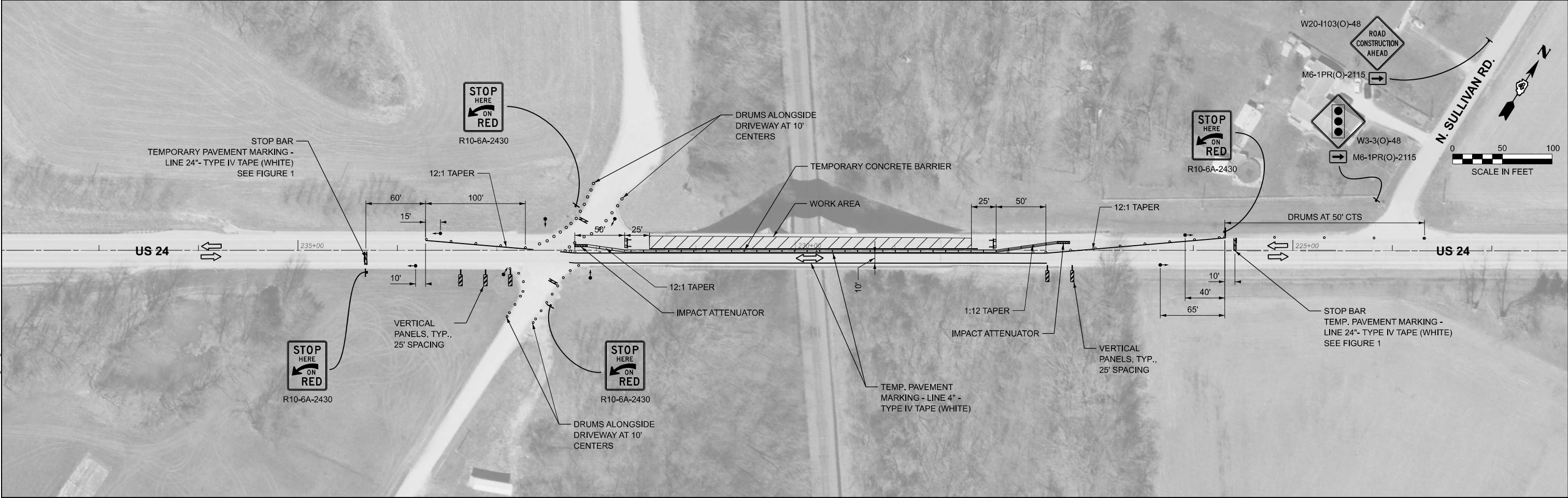
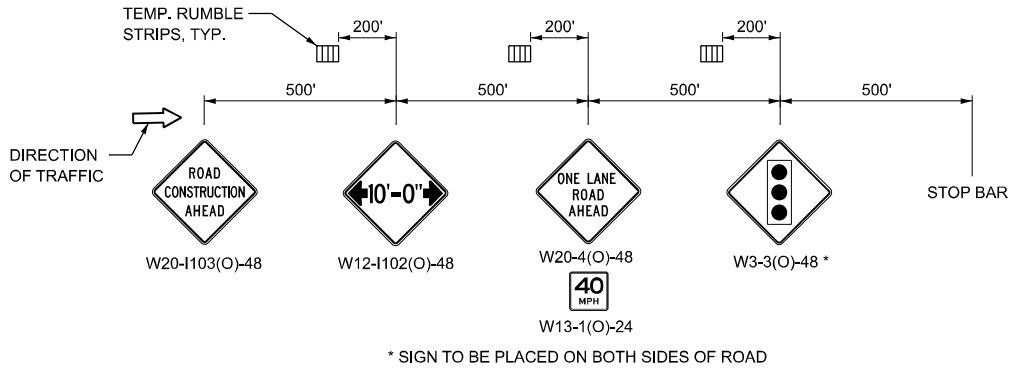
- NOTES**
1. FOR ALL SECTIONS, THERE SHALL BE 6 INCHES BETWEEN THE TOE OF THE TEMPORARY CONCRETE BARRIER AND THE BEGINNING OF THE WORK AREA.

MODEL: SN 029-0007  
FILE NAME: X:\OH\2023\20230225-06\Design\CADD\Drawn\Sheets\Plan\468.1\3-D\468.1\3-sh-H-MOT\Typical.dgn

<div>GR<sup>AE</sup>EF</div> <div>8501 W. Higgins Road Suite 280 Chicago, Illinois 60634 (773) 399-012</div>	USER NAME = 2189		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED MAINTENANCE OF TRAFFIC TYP. SECTIONS SN 029-0007 US-24 OVER BNSF RR			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN -	REVISED -					317	(15VB,FJBR)	FULTON	38	15
			CHECKED -	REVISED -					CONTRACT NO. 68J13				
	PLOT DATE = 5/8/2025		DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				



FIGURE 1 - ADVANCE SIGNAGE PLAN



LEGEND

- WORK AREA
- DRUM
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR (NON- REDIRECTIVE, NARROW), TEST LEVEL 3
- TEMPORARY TRAFFIC SIGNAL
- DOUBLE VERTICAL PANEL (SEE DETAIL)
- TYPE III BARRICADE WITH FLASHING
- SIGN
- TEMPORARY RUMBLE STRIP

NOTES

- TYPE III BARRICADES ARE TO BE IN PLACE WHEN NO WORK IS BEING PERFORMED.
- DRUMS WITHIN LANE CLOSURE TAPERS SHALL HAVE STEADY BURNING BI-DIRECTIONAL LIGHTS
- DRUMS SHALL BE PLACED AT 25' CENTER-TO-CENTER UNLESS OTHERWISE SPECIFIED.
- FOR ADDITIONAL REQUIRED DETAILS, SEE IDOT STANDARD 701321-19.
- FOR DRIVEWAY TEMP. SIGNALS, SIGNAL HEADS SHALL BE PLACED NO LESS THAN 50' BEYOND THE STOP BAR, MEASURED PERPENDICULAR.

MODEL: Default  
FILE NAME: X:\OH\2023\20230225-06\Design\CADD\Drawn\Sheets\Plan\468\13\468\13-sh-SN029-0007-US-24overBNSF.MOTstage1.dgn

The diagram illustrates the sequence of signs and rumble strips for a lane reduction. It shows a horizontal line representing the road with various signs and rumble strips placed at specific intervals. The signs are: W20-1103(O)-48 (Road Construction Ahead), W12-1102(O)-48 (10'-0" Lane Reduction), W20-4(O)-48 (One Lane Road Ahead), and W3-3(O)-48\* (Traffic Signal Ahead). The rumble strips are represented by a series of vertical bars. The intervals between the signs and rumble strips are: 500' between the first and second signs, 200' between the second sign and the first rumble strip, 500' between the first rumble strip and the third sign, 200' between the third sign and the second rumble strip, 500' between the second rumble strip and the fourth sign, and 500' between the fourth sign and the stop bar. A 'DIRECTION OF TRAFFIC' arrow points to the right. A note at the bottom states: '\* SIGN TO BE PLACED ON BOTH SIDES OF ROAD'.

TEMP. RUMBLE STRIPS, TYP.

200'

500'

500'

200'

500'

500'

500'

DIRECTION OF TRAFFIC

ROAD CONSTRUCTION AHEAD

10'-0"

ONE LANE ROAD AHEAD

40 MPH

STOP BAR

W20-1103(O)-48

W12-1102(O)-48

W20-4(O)-48

W3-3(O)-48 \*

W13-1(O)-24

\* SIGN TO BE PLACED ON BOTH SIDES OF ROAD



Existing Description: SN 029-0007 was originally built in 1954. The bridge was redecked and the east abutment and wingwalls were modified in 2001. The structure has a back-to-back length of 262'-2½" and an out-to-out width of 35'-2". The superstructure consists of a 7½" thick reinforced concrete slab supported on three-span continuous steel beams of span lengths 86'-2", 86'-2", and 86'-2". The substructure consists of reinforced concrete abutments and solid wall piers supported on precast concrete piles.

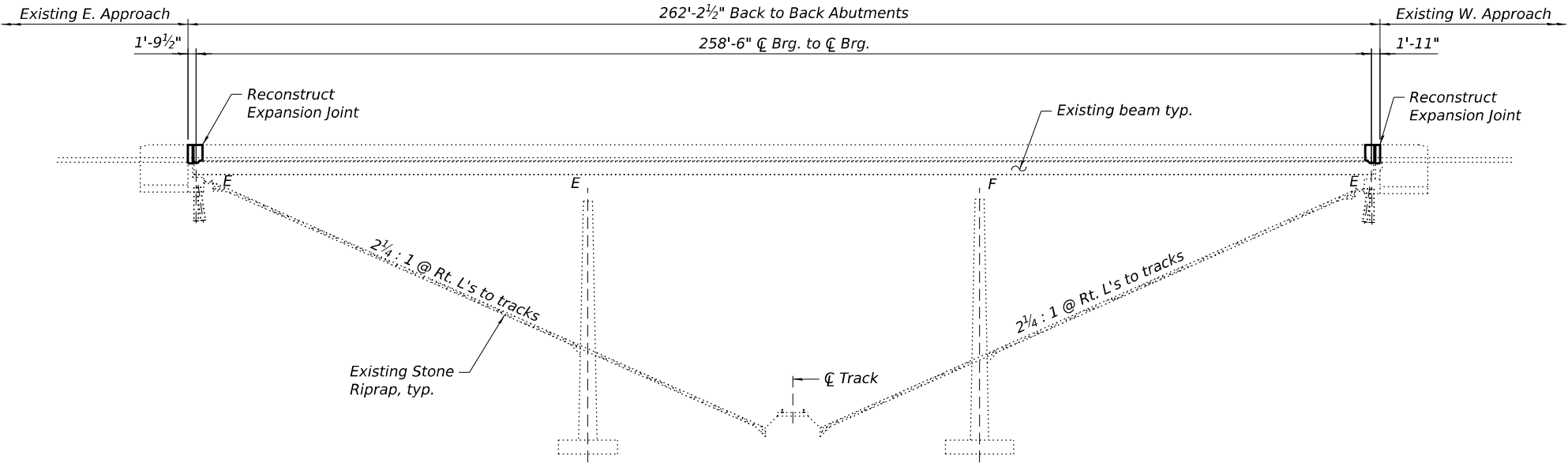
Traffic will be maintained utilizing stage construction.

No Salvage:

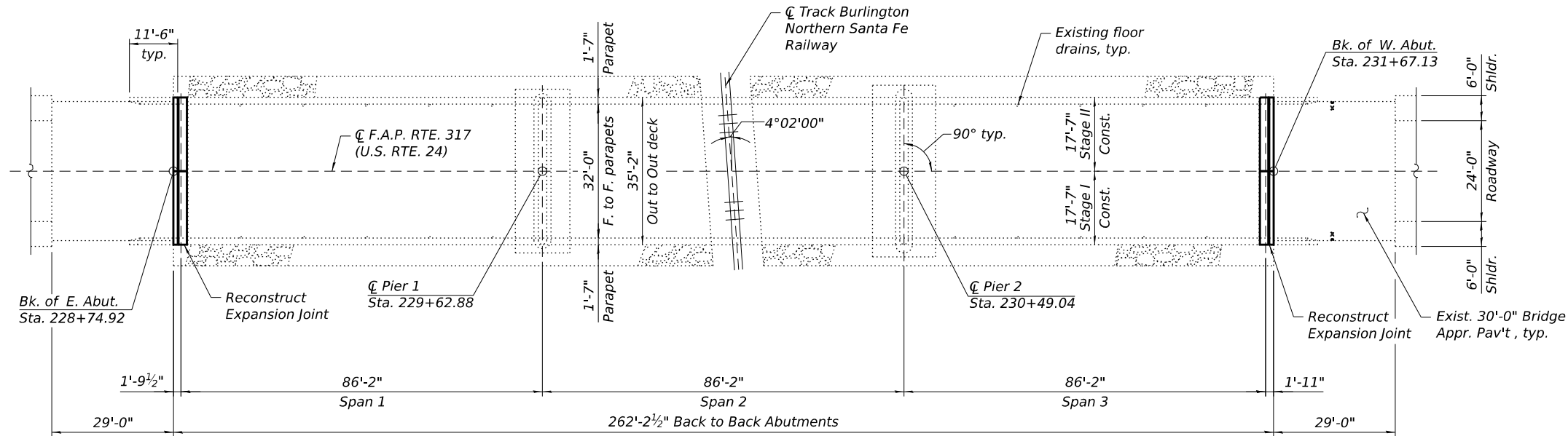
LOADING HS20-44  
Allow 25#/sq. ft for future wearing surface

DESIGN SPECIFICATIONS  
1992 AASHTO Standard Specifications  
with 1993 and 1994 Interims

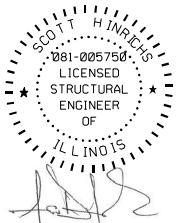
DESIGN STRESSES  
FIELD UNITS (EXIST. CONST.)  
f<sub>c</sub> = 3,500 psi (Superstructure)  
f<sub>c</sub> = 800 psi (Substructure)  
f<sub>y</sub> = 60,000 psi (Superstructure Reinforcement)  
f<sub>y</sub> = 20,000 psi (Substructure Reinforcement)  
f<sub>y</sub> = 18,000 psi (1954 Struct. Steel)  
f<sub>y</sub> = 20,000 psi (2001 Struct. Steel)  
FIELD UNITS (NEW CONST.)  
f<sub>c</sub> = 4,000 psi  
f<sub>y</sub> = 60,000 psi (Reinforcement)



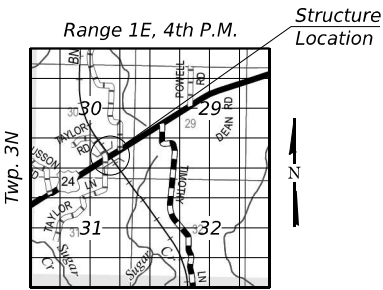
ELEVATION



PLAN



ENGINEER FULL NAME: SCOTT HINRICHS DATE: 15-02-2025  
ILLINOIS REGISTERED ENGINEER NO. 081-005750  
REGISTRATION EXPIRES 11. 30, 2026



LOCATION SKETCH

GENERAL PLAN & ELEVATION  
U.S. RTE. 24 OVER B.N.S.F. RAILROAD  
F.A.P. RTE. 317  
FULTON COUNTY  
STRUCTURE NO. 029-0007

MODEL: Default  
FILE NAME: X:\012023\20230256-06\Design\CADD\Structures\Sheets\Plan\SN-029-0007\0290007-48\13-01-GPE.dgn

**GR&EF**  
8501 W. Higgins Road, Suite 280  
Chicago, Illinois 60634 (773) 399-0102

USER NAME =	DESIGNED - JTB	REVISED -
	CHECKED - CG	REVISED -
PLOT SCALE =	DRAWN - DCP	REVISED -
PLOT DATE =	CHECKED - SH	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	18
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				



GENERAL NOTES:

1.

Reinforcement bars designated (E) shall be epoxy coated.
2.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC-SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding ¼ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
3.

Plan dimensions and details relative to the existing structure have been taken from existing plans and are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system at the contractor's expense.
5.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.
6.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.
7.

The protective coat shall be applied to the top of existing bridge deck and approach slab concrete, after deck patching has properly cured.
8.

The protective coat (Special) shall be applied to all surfaces of the parapets. See detail on sheet 5 of 14.
9.

Surface Filler (Special) shall be applied according to Special Provisions.
10.

The Contractor shall exercise caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.
11.

Deck slab repair (partial) includes quantity for approach slab repair.

INDEX OF SHEETS

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General Data
3.

Stage Construction Details
4.

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5.

Bridge Deck Repair Plan.
- 6-7.

East Abutment Expansion Joint Details I & II
- 8-9.

West Abutment Expansion Joint Details I & II
10.

Preformed Joint Strip Seal
11.

Abutment Repairs
12.

Bar Splicer Assembly and Mechanical Splicer Details
13.

Existing Plans Reference Sheet
14.

Existing Plans Reference Sheet

SCOPE OF WORK

1.

Perform partial depth slab repairs.
2.

Remove and reconstruct expansion joints at east and west abutments, and install new Preformed Joint Strip Seals.
3.

Apply Protective Coat to the reconstructed transverse expansion joints and to the surface of the existing bridge deck and approach slabs.
4.

Apply Protective Coat (Special) and Surface Filler (Special) to all faces of the parapets.
5.

Perform Structural Repair of Concrete to the Abutments as noted in the plans.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	9.9	-	9.9
Concrete Superstructure	Cu Yd	9.9	-	9.9
Protective Coat	Sq Yd	1,150	-	1,150
Reinforcement Bars, Epoxy Coated	Pound	1,280	-	1,280
Bar Splicers	Each	24	-	24
Preformed Joint Strip Seal	Foot	68	-	68
Surface Filler (Special)	Gallon	1	-	1
Protective Coat (Special)	Sq Yd	407	-	407
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq Ft	-	15	15
Deck Slab Repair (Partial)	Sq Yd	93	-	93

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL DATA  
STRUCTURE NO. 029-0007

SHEET 1 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	19
CONTRACT NO. 68J13				
		ILLINOIS	FED. AID PROJECT	

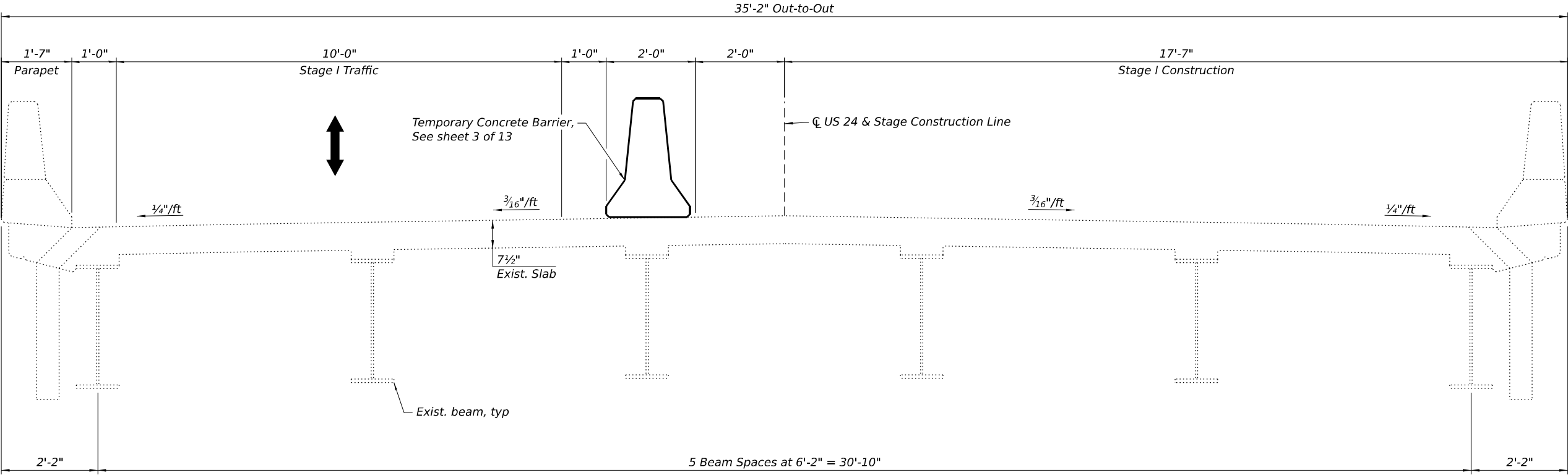


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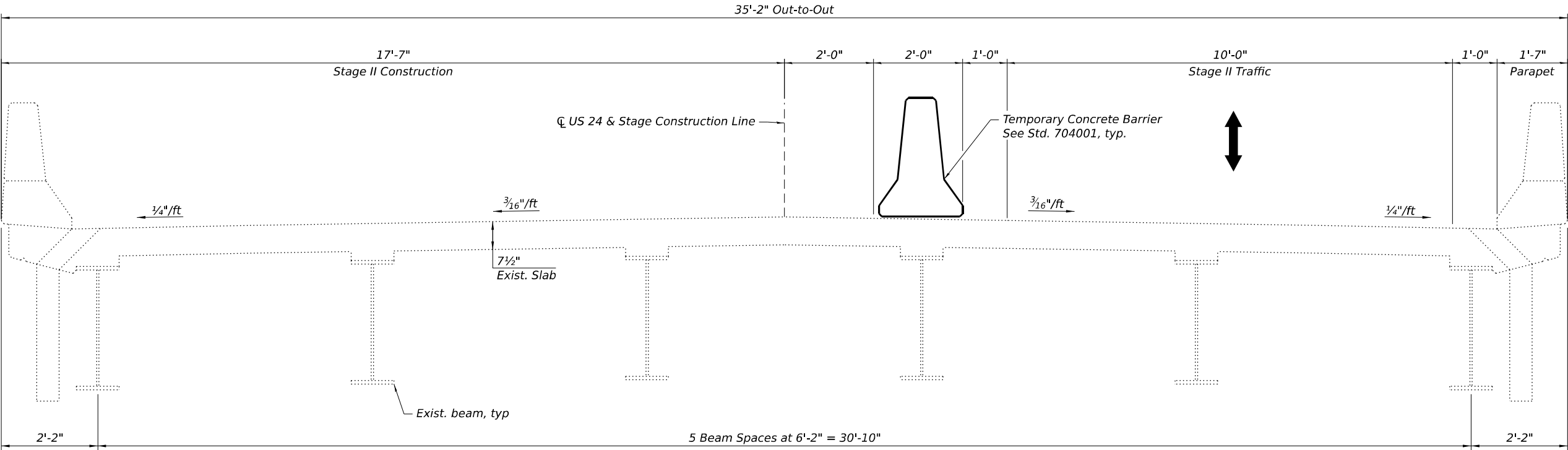
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**STAGE I CONSTRUCTION**

(Looking West)



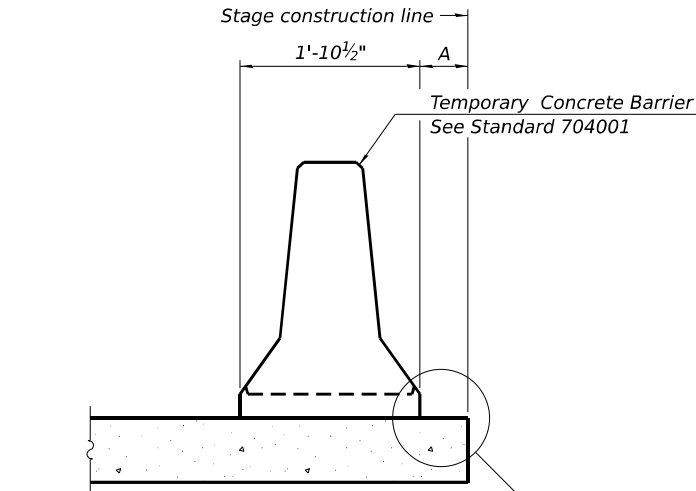
**STAGE II CONSTRUCTION**

(Looking West)

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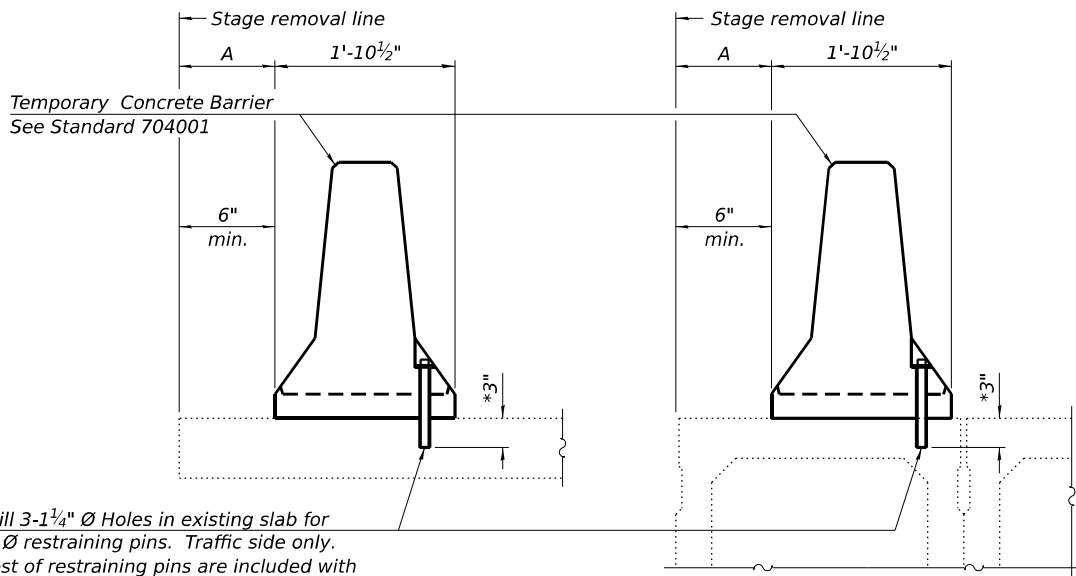
<div><div>GR̂EF</div><div>8501 W. Higgins Road, Suite 280 Chicago, Illinois 60634 (773) 399-002</div></div>	USER NAME =	DESIGNED - JTB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE CONSTRUCTION DETAILS STRUCTURE NO. 029-0007	SHEET 3 OF 14 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE =	DRAWN - DCP	REVISED -				CONTRACT NO. 68J13				
		CHECKED - SH	REVISED -				ILLINOIS FED. AID PROJECT				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

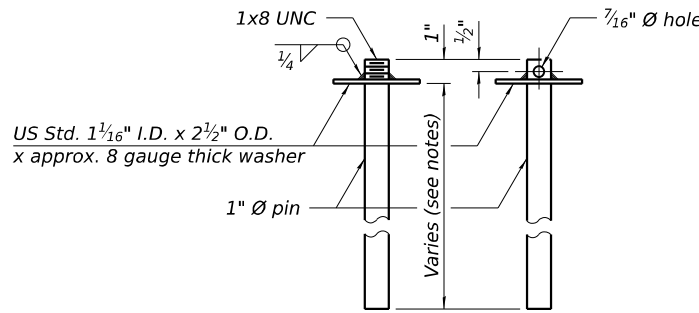


Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

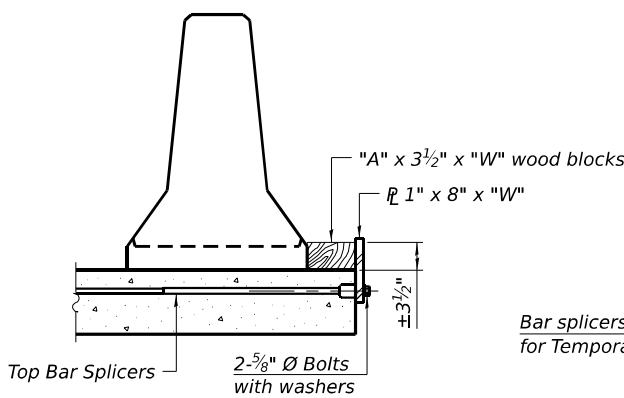
\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

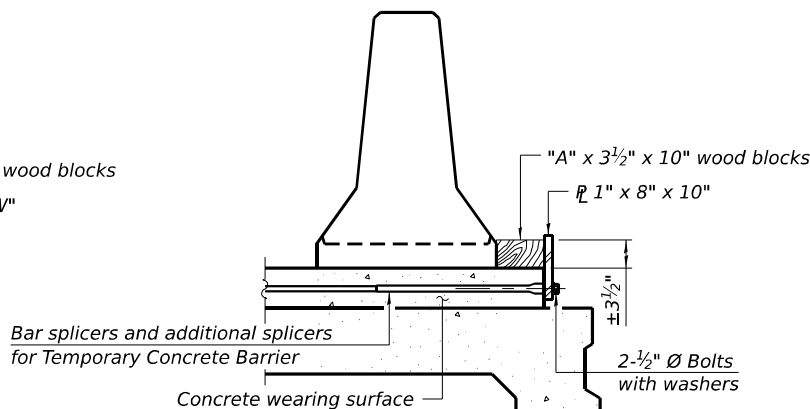


RESTRAINING PIN

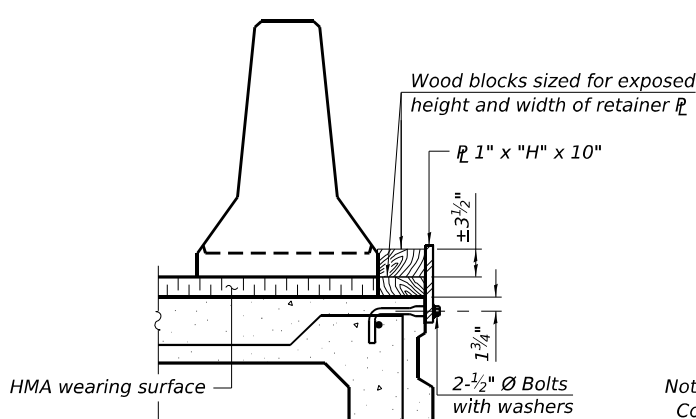
SECTIONS THRU SLAB OR DECK BEAM



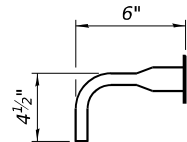
DETAIL I



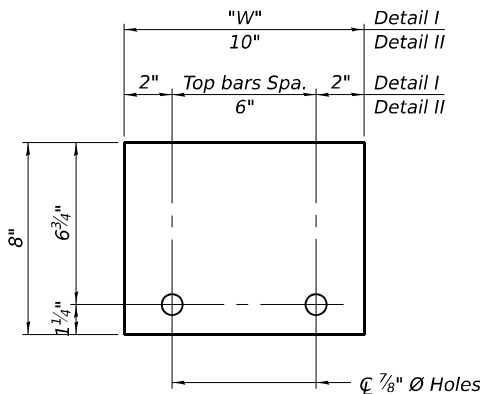
DETAIL II



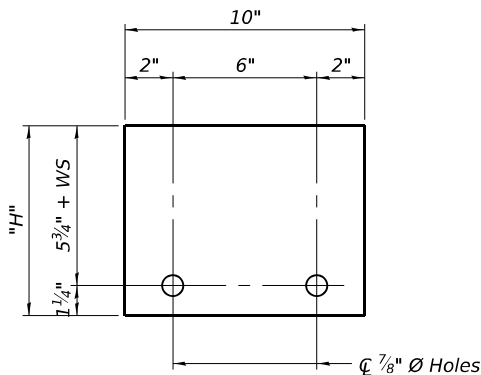
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"  
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"  
(Detail III)

Notes:  
Cost of retainer assembly is included with Temporary Concrete Barrier.  
A retainer assembly shall be located at the approximate C of each temporary concrete barrier.  
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

5-15-2023

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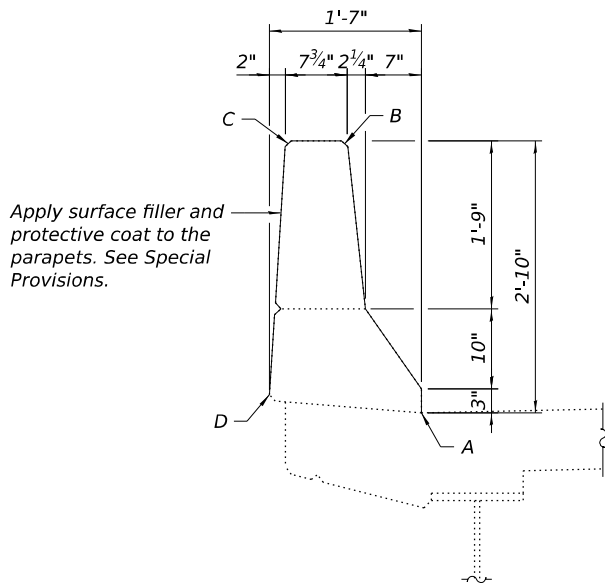
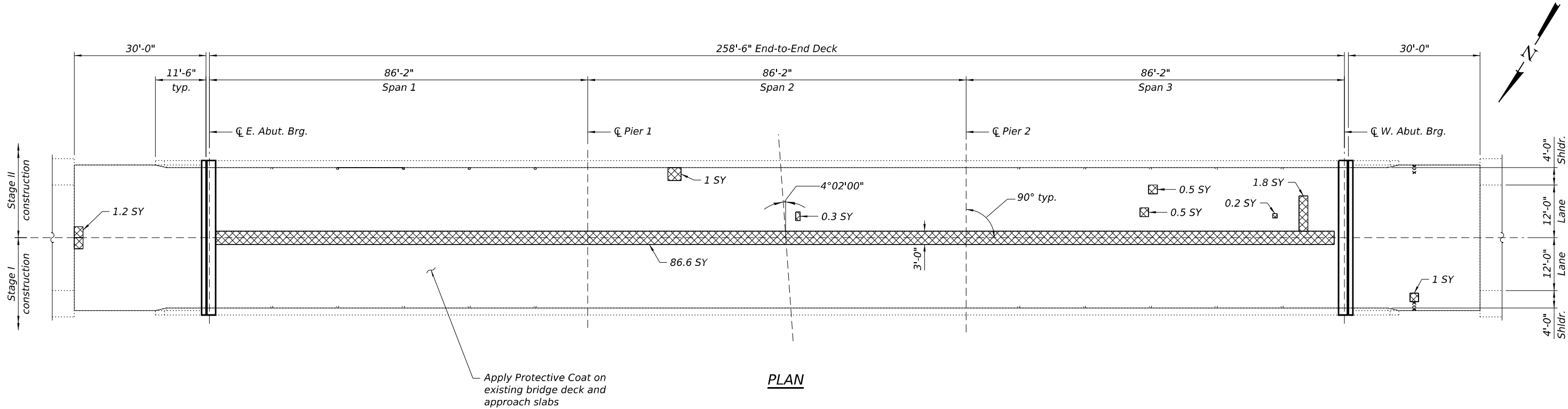
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DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER  
STRUCTURE NO. 029-0007


SHEET 4 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	21
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				



- Notes:
- See Sheets 6 thru 9 of 14 for expansion joint replacement details.
  - Deck Slab Repair (Partial Depth) include Approach Slab Repair (Partial Depth) areas are estimated and will be field verified by the Engineer prior to patching. The Engineer shall show actual locations of deck repairs on as-built plans.
  - The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
  - Protective Coat (Special) to cover from point A through points B, C, and D of the existing parapet.

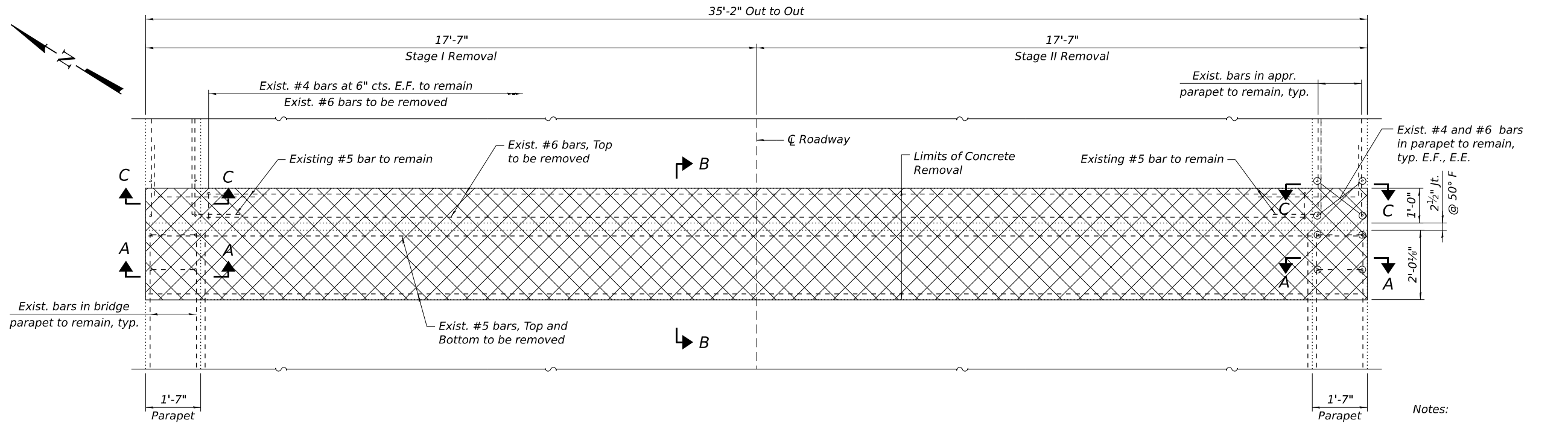
#### LEGEND

-  Deck Slab Repair (Partial Depth)
- SY Square Yard

#### BILL OF MATERIAL

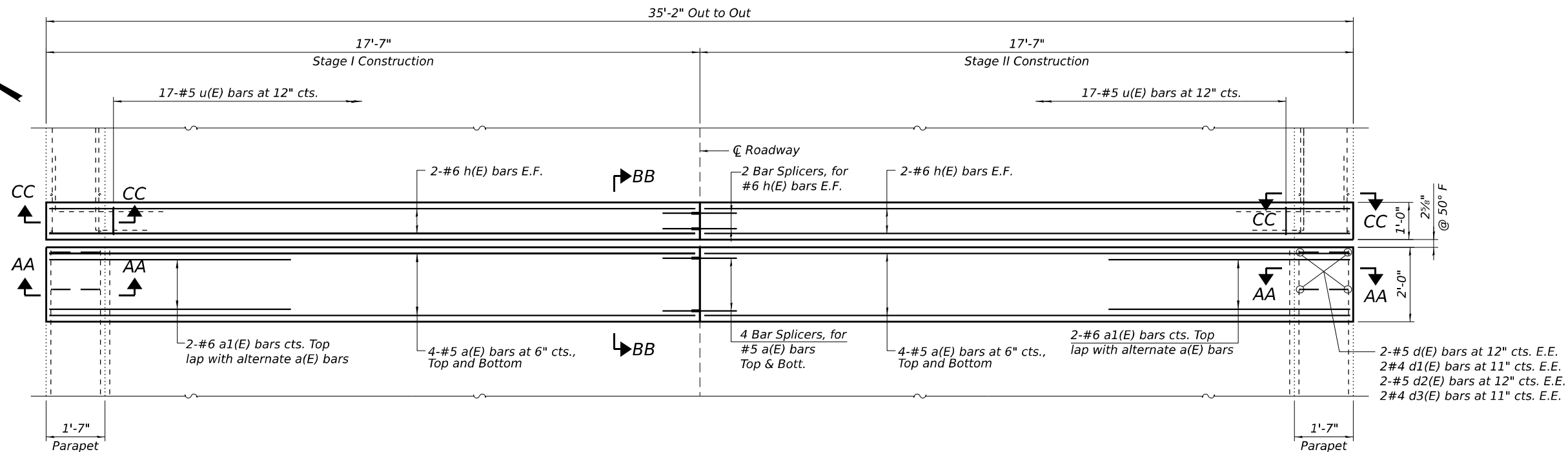
ITEM	UNIT	QUANTITY
Protective Coat	Sq Yd	937
Surface Filler (Special)	Gallon	1
Protective Coat (Special)	Sq Yd	407
Deck Slab Repair (Partial)	Sq Yd	93

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EAST ABUTMENT JOINT REMOVAL PLAN

- Notes:
- Any reinforcement bars that are to remain that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
  - For sections A-A, B-B, C-C, AA-AA, BB-BB and CC-CC, see sheet 7 of 14.



EAST ABUTMENT JOINT RECONSTRUCTION PLAN

**LEGEND**

 Concrete Removal

I.F. Inside Face  
O.F. Outside Face  
E.E. Each End  
E.F. Each Face

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT EXPANSION JOINT DETAILS I  
STRUCTURE NO. 029-0007

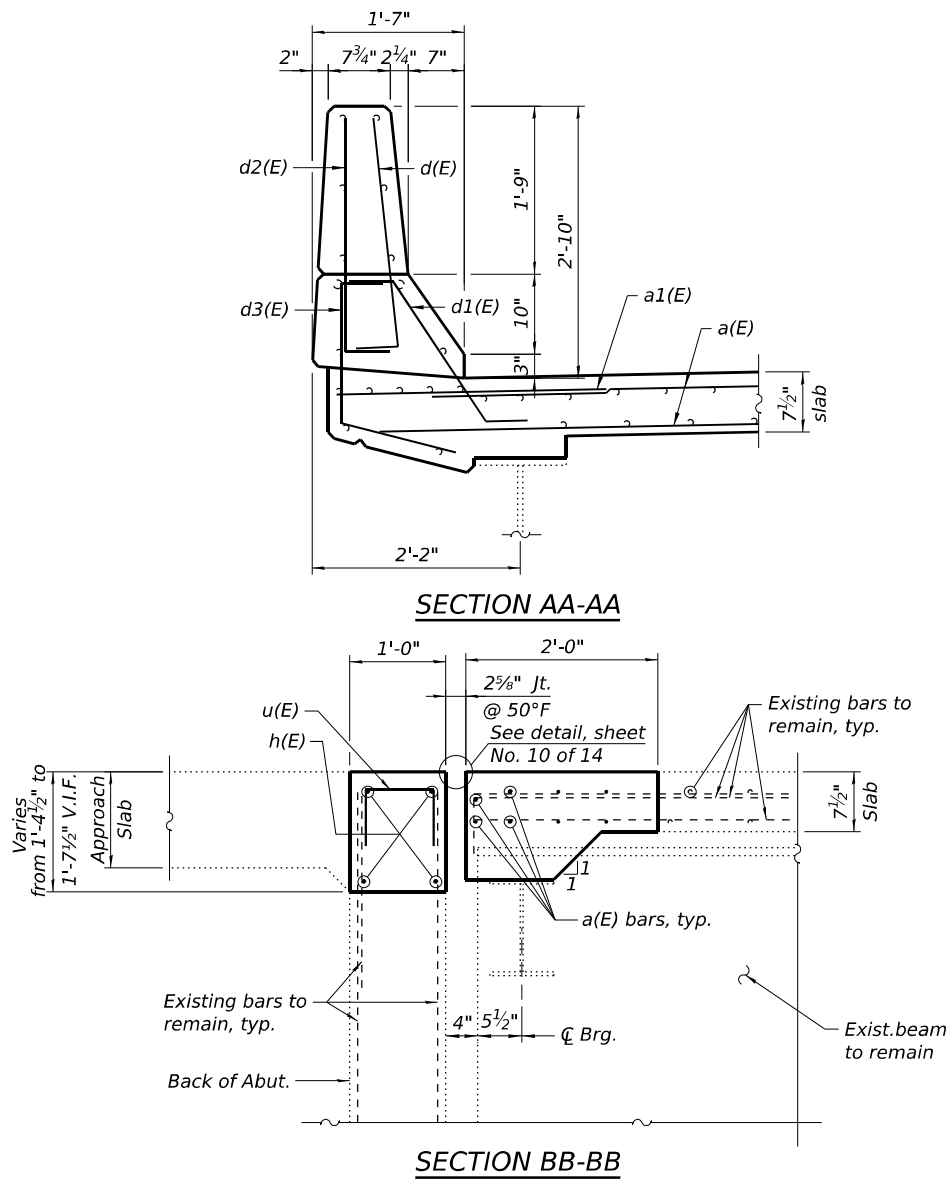
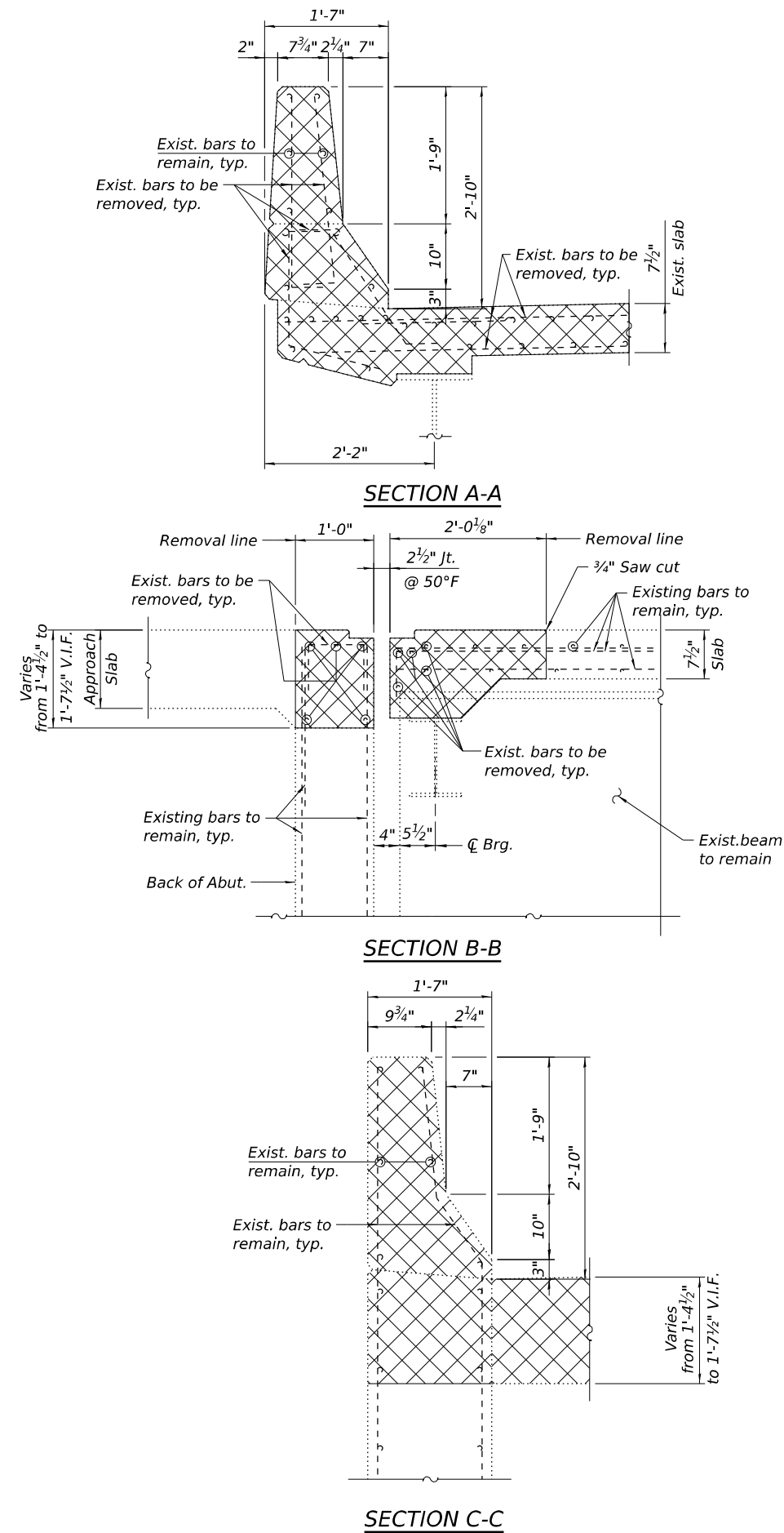
SHEET 6 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	23
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

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317	(15VB, F) BRR	FULTON	38	24
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				



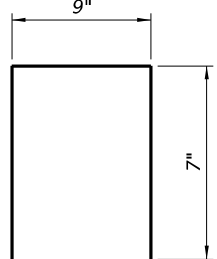
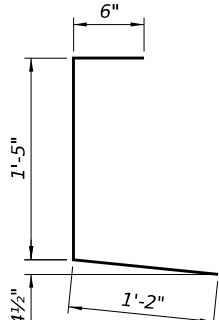
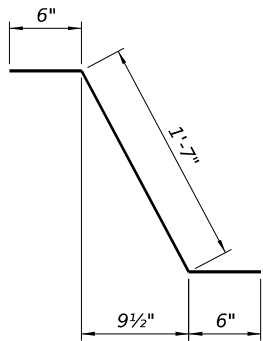
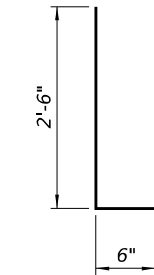
BARS d(E) & d2(E)

BAR d1(E)

BAR d3(E)

BAR u(E)

BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
a(E)	16	#5	17'-3"	—
a1(E)	4	#6	6'-6"	—
d(E)	4	#5	3'-0"	┐
d1(E)	4	#5	2'-7"	┐
d2(E)	4	#4	3'-0"	┐
d3(E)	4	#4	3'-1"	┐
h(E)	8	#6	17'-3"	—
u(E)	34	#5	1'-11"	┐
Concrete Removal			Cu. Yd.	4.9
Concrete Superstructure			Cu. Yd.	4.9
Reinforcement Bars, Epoxy Coated			Pound	640



Notes:

- For Preformed Joint Strip Seal details, see sheet 10 of 13.
- For Bar Splicer Assembly details, see sheet 12 of 13.
- Removal of Existing Expansion Joint will not be paid for separately but will be included in the cost of Concrete Removal.

LEGEND

	Concrete Removal
I.F.	Inside Face
O.F.	Outside Face
E.E.	Each End
E.F.	Each Face

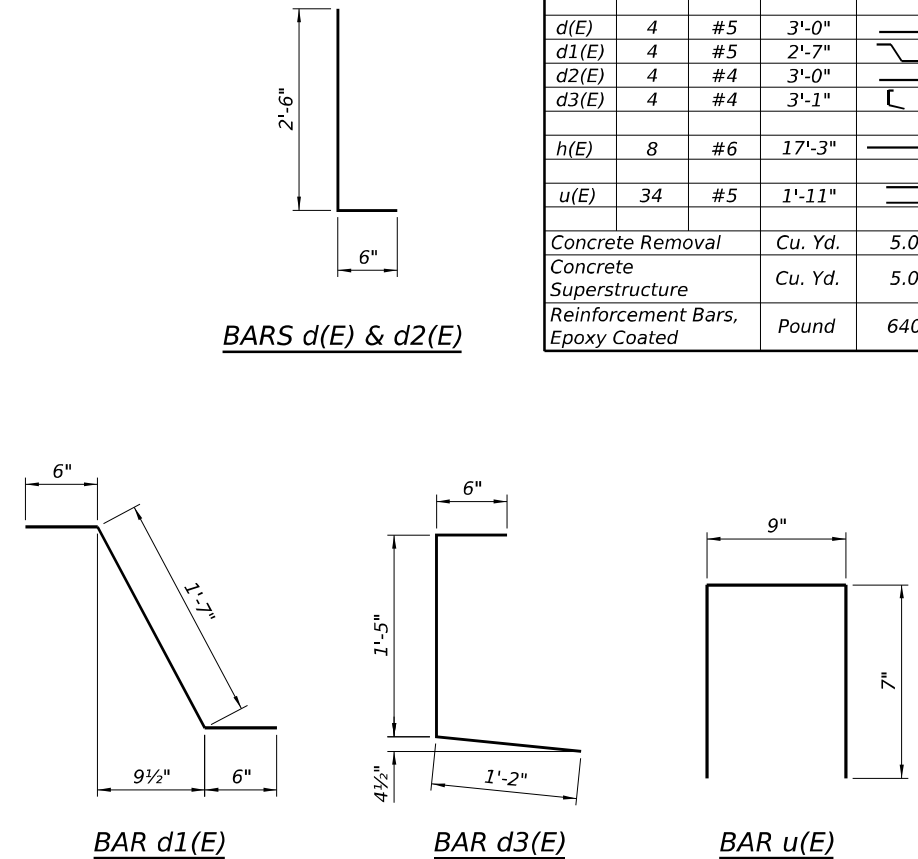
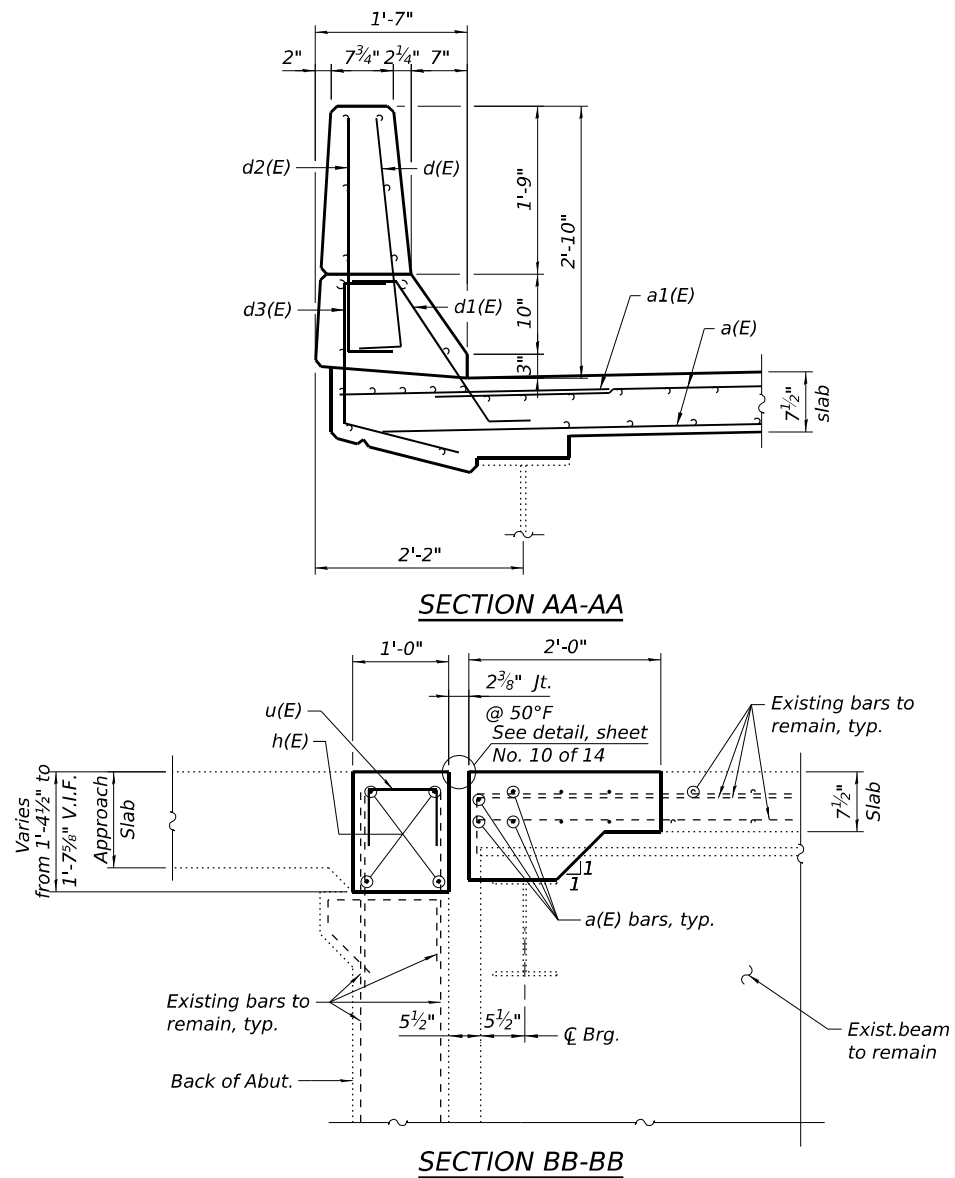
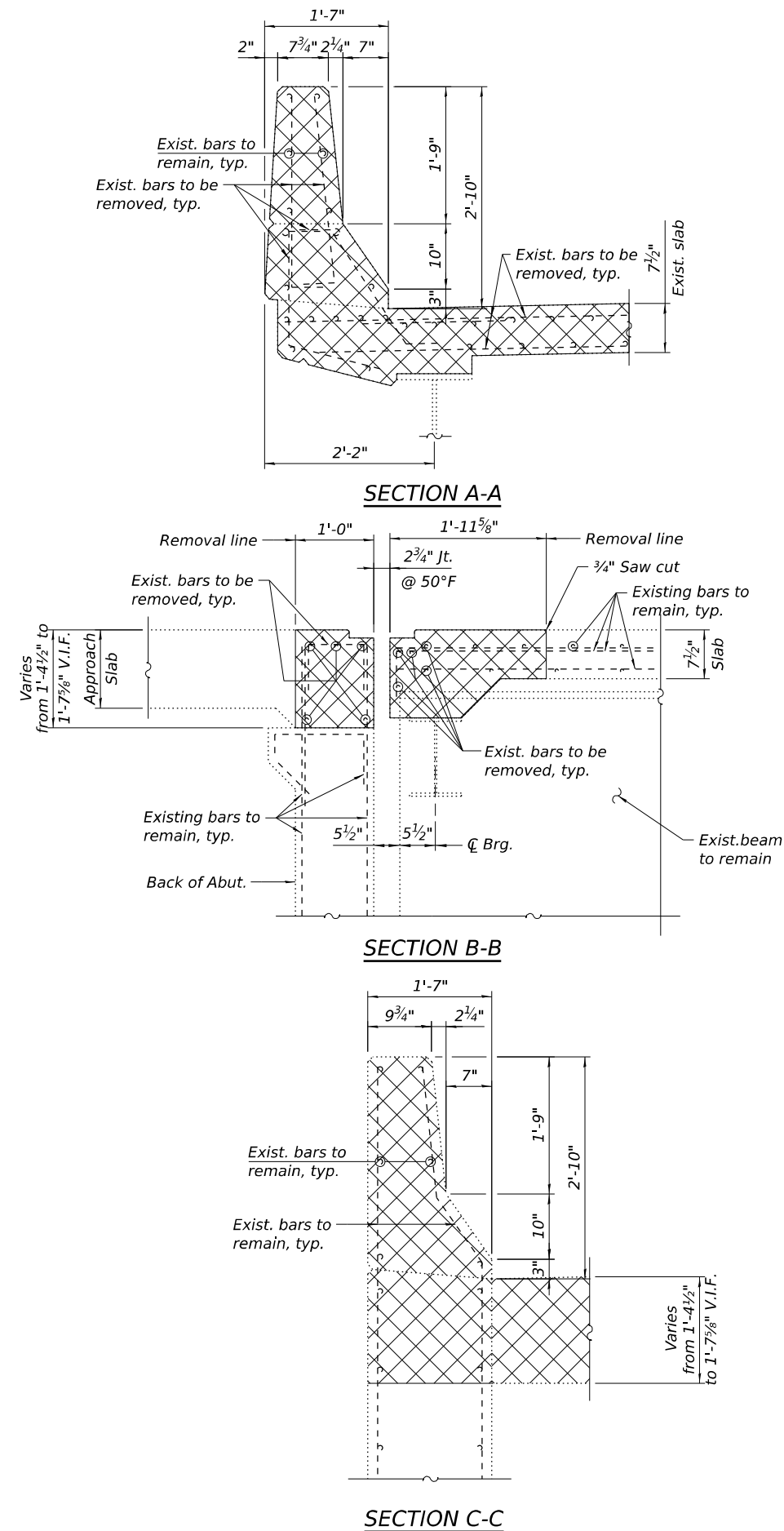




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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	26
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				



Notes:

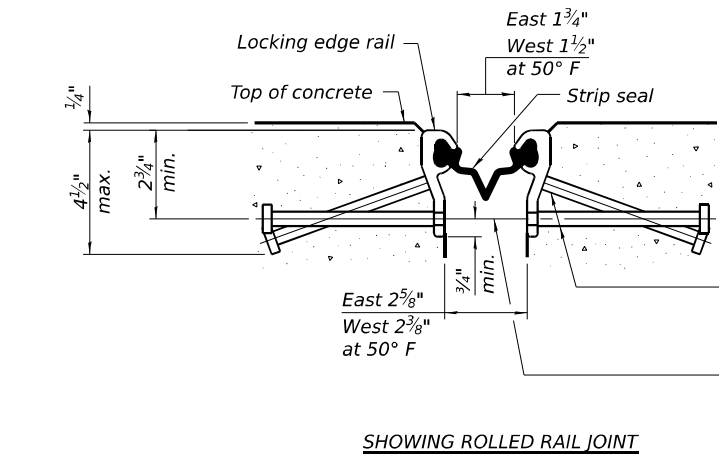
- For Preformed Joint Strip Seal details, see sheet 10 of 13.
- For Bar Splicer Assembly details, see sheet 12 of 13.
- Removal of Existing Expansion Joint will not be paid for separately but will be included in the cost of Concrete Removal.

LEGEND

	Concrete Removal
I.F.	Inside Face
O.F.	Outside Face
E.E.	Each End
E.F.	Each Face

BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
a(E)	16	#5	17'-3"	
a1(E)	4	#6	6'-6"	
d(E)	4	#5	3'-0"	
d1(E)	4	#5	2'-7"	
d2(E)	4	#4	3'-0"	
d3(E)	4	#4	3'-1"	
h(E)	8	#6	17'-3"	
u(E)	34	#5	1'-11"	
Concrete Removal			Cu. Yd.	5.0
Concrete Superstructure			Cu. Yd.	5.0
Reinforcement Bars, Epoxy Coated			Pound	640

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SHOWING ROLLED RAIL JOINT

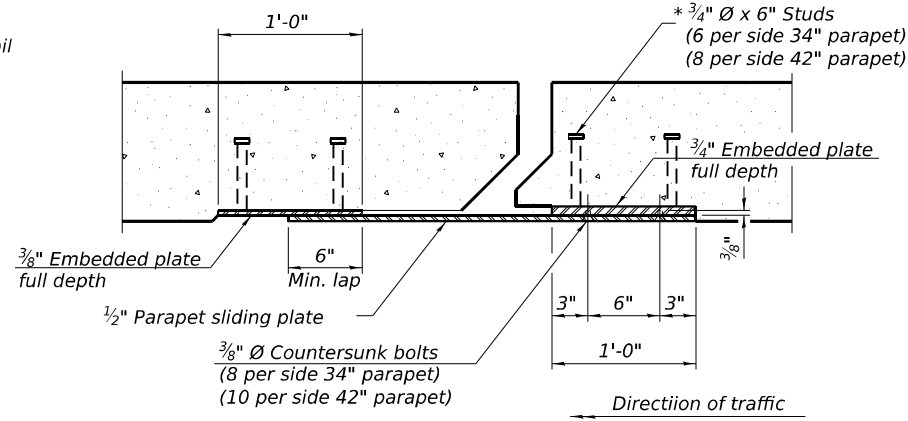
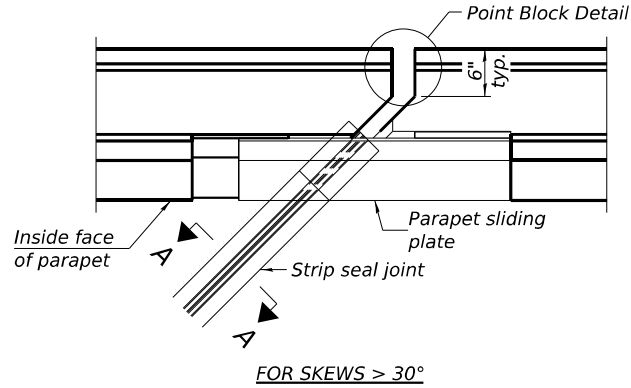
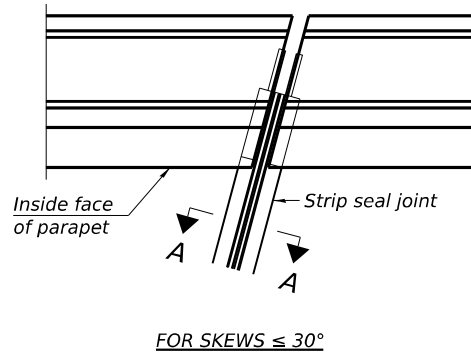
\* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" Ø threaded rods in 7/16" Ø holes at 4'-0" ± cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

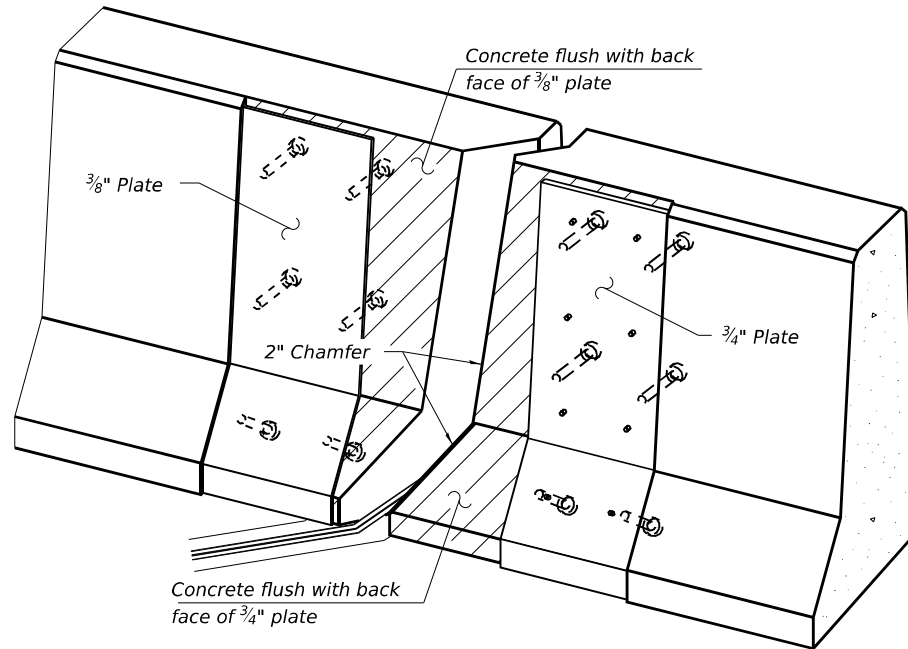
#### SECTION A-A

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

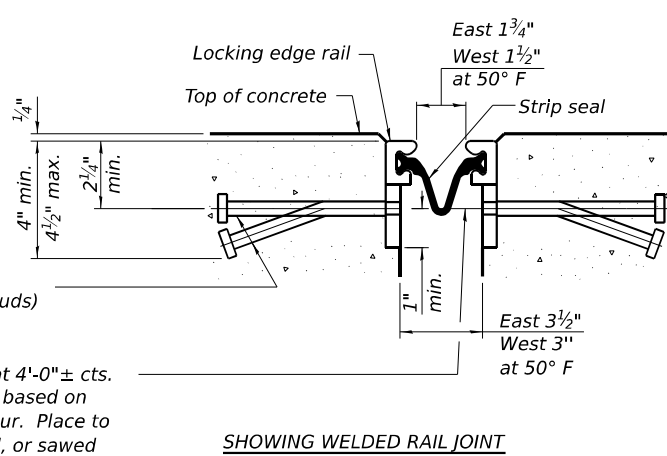
#### PLAN AT PARAPET



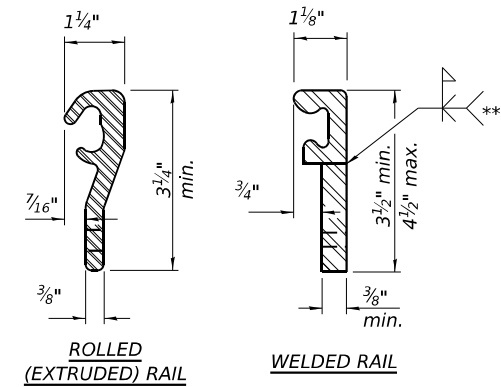
#### SECTION B-B



TRIMETRIC VIEW  
(Showing embedded plates only)

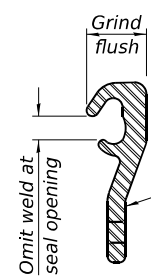


SHOWING WELDED RAIL JOINT



#### LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



#### LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

#### BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	68

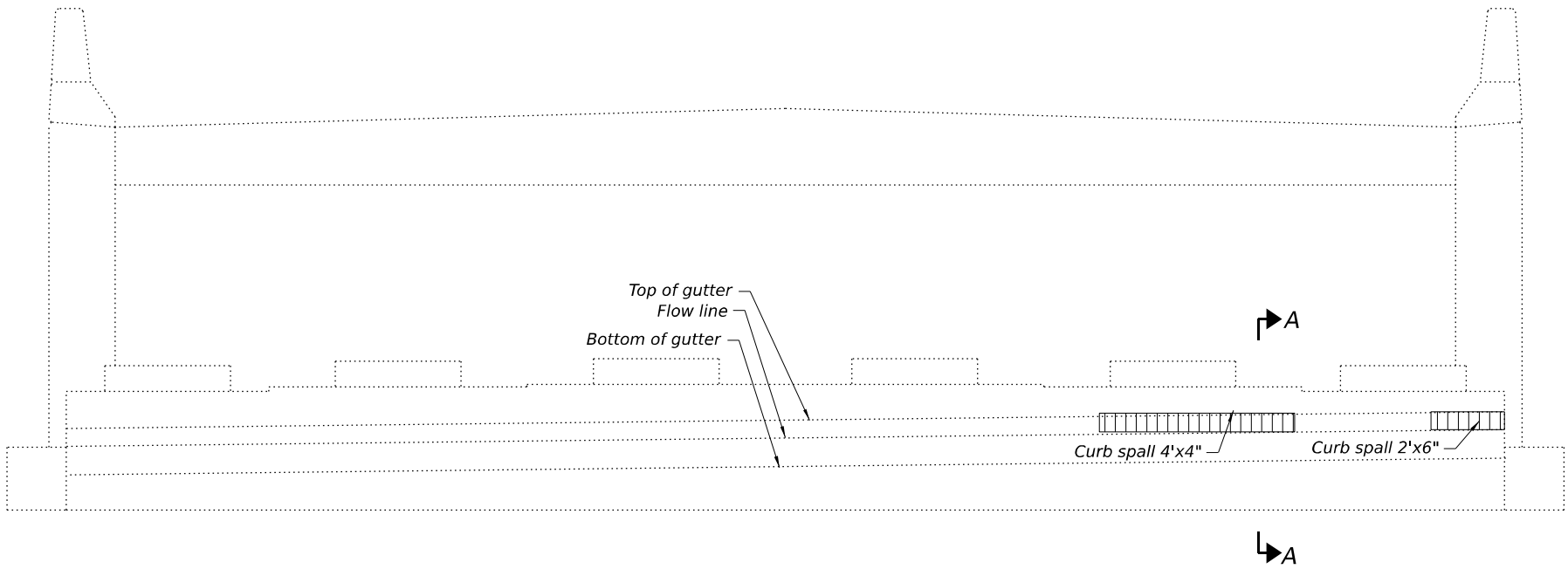
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL  
STRUCTURE NO. 029-0007

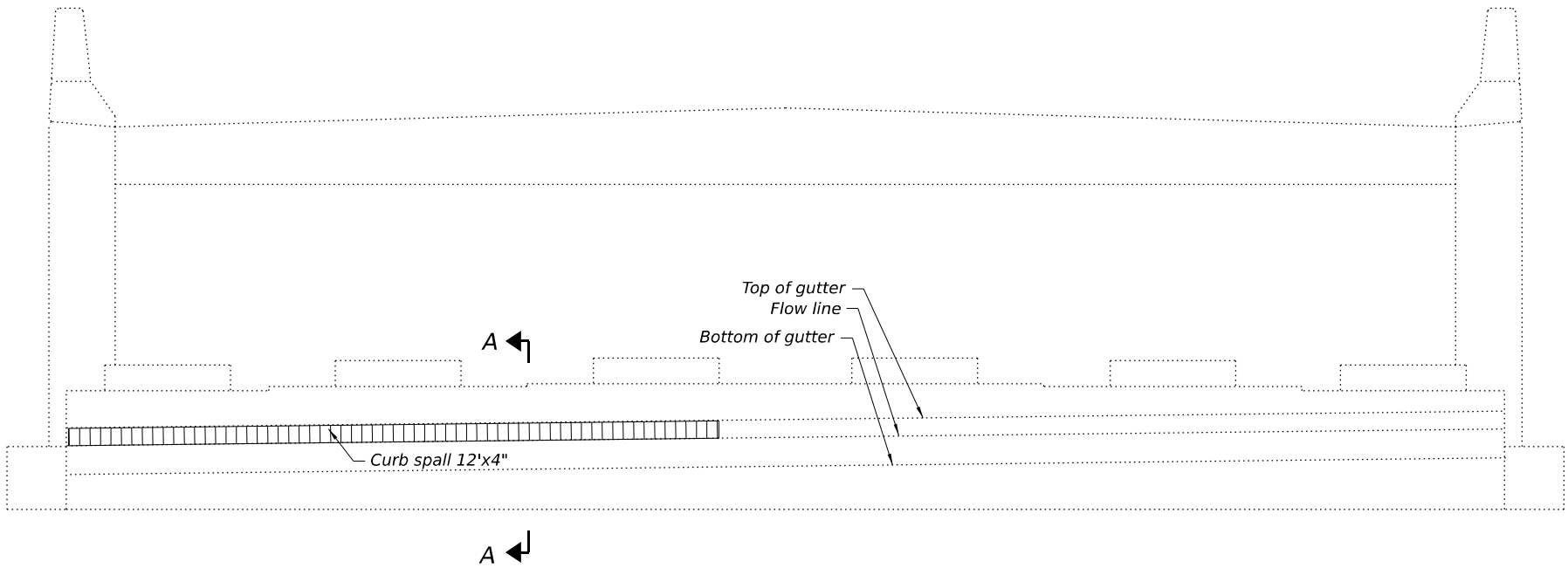
SHEET 10 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	27
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

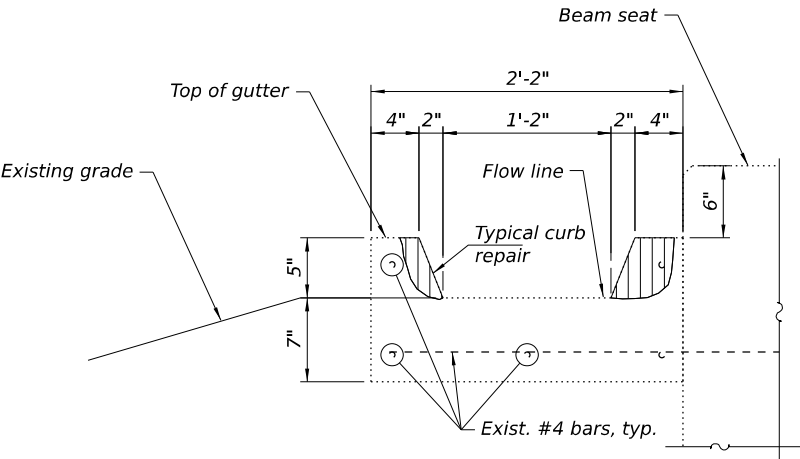
**GRÄEF**  
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Chicago, Illinois 60634 (773) 399-0102



EAST ABUTMENT - ELEVATION



WEST ABUTMENT - ELEVATION



SECTION A-A

Notes:  
Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the engineer in the field at the time of construction.

LEGEND

- Curb repair (Partial Depth)
- SY Square Yard

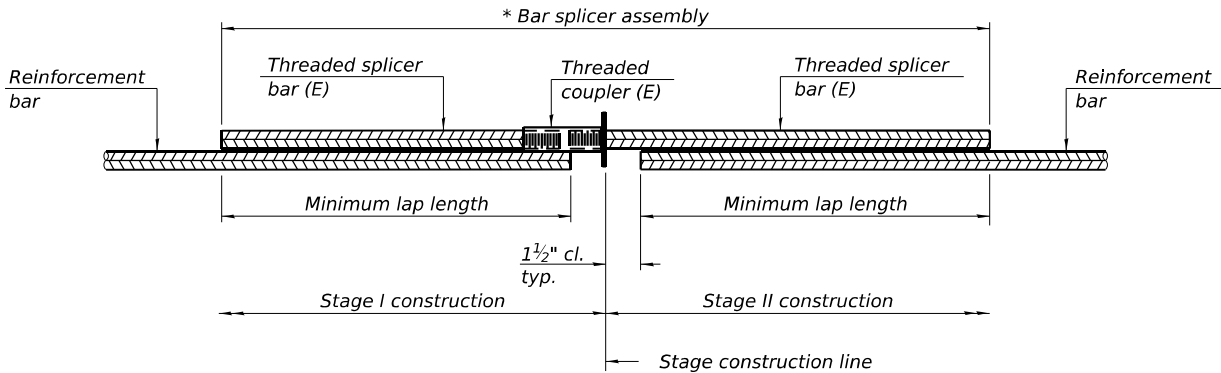
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq Ft	15

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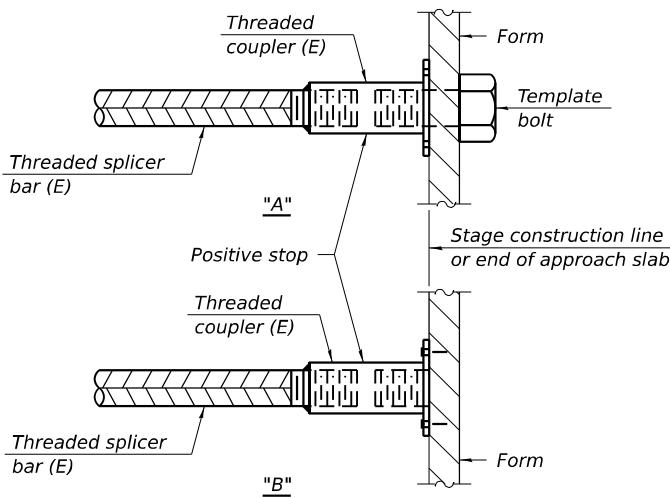
**STANDARD BAR SPLICER ASSEMBLY PLAN**

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1½" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
East Abutment Exp. Jt.	#6	4	3'-7"
West Abutment Exp. Jt.	#5	8	3'-0"
	#6	4	3'-7"
	#5	8	3'-0"

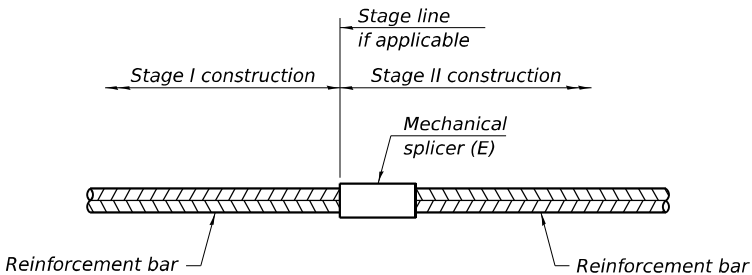


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

Notes:  
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
All reinforcement shall be lapped and tied to the splicer bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

5-15-2023

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
STRUCTURE NO. 029-0007

SHEET 12 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	29
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

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**GR̄EF**  
8501 W. Higgins Road, Suite 280  
Chicago, Illinois 60634 (773) 399-0102

5/7/2025 4:25:25 PM

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**GR&E**  
8501 W. Higgins Road, Suite 280  
Chicago, Illinois 60634 (773) 399-0042

USER NAME =	DESIGNED - JTB	REVISED -
PLOT SCALE =	CHECKED - CG	REVISED -
PLOT DATE =	DRAWN - DCP	REVISED -
	CHECKED - SH	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS REFERENCE SHEET  
STRUCTURE NO. 029-0007

SHEET 13 OF 14 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB, F) BRR	FULTON	38	30
CONTRACT NO. 68J13				
ILLINOIS FED. AID PROJECT				

SHEET  
52 OF 518

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB)	FULTON		4-4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

#### GENERAL NOTES:

Reinforcement Bars shall conform to the requirements of A.A.S.H.T.O. M-31, M-42 or M-53 Grade 60.

Cleaning and painting of the existing structural steel shall be as specified in the Special Provision for "Cleaning and Painting Existing Steel Structures". All existing structural steel within 5 feet of either side of expansion joints shall be cleaned by Method 1. The exterior and the bottom flange of the fascia beams shall be cleaned by Method 3. The aluminum epoxy mastic / acrylic paint system shall be used for painting of existing structural steel. The color of the final finish coat shall be Gray, Munsell No. 5B 7/1.

Plan dimensions and details relative to existing structure have been taken from the existing plans and are subject to normal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The Contractor will be required to mark, on top of the concrete deck, the locations of all flanges of the steel beams, prior to any removal of the bridge concrete deck. Saw cutting directly over the top of beam flanges is NOT permitted.

If the contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans for lesser design requirements, then full design submittals with the required seals will be expected by the Department, for review and approval.

Field welding of construction accessories will not be permitted to the bottom flange of beams or girders nor to the top flange for a distance equal to one-fourth the span length each way from the pier supports. Field welding in other areas will be permitted only when approved by the Engineer.

The Contractor shall anchor the sheet piling to the existing East Abutment Wall. The connection shall be approved by the Engineer.

Bridge seat sealer shall be applied to the seat area of both the abutments.

Fasteners shall be high strength bolts. Bolts shall be  $\frac{3}{4}$ " open holes  $\frac{5}{16}$ " unless otherwise noted.

Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of  $\frac{1}{8}$  inch. Adjustment shall be made either by grinding the surface or by shimming the bearing. Two  $\frac{1}{8}$ " adjustings shims, of the dimensions of the bottom bearing plate, shall be provided for each bearing in addition to all other plates or shims. For Type I Elastomeric Bearings, shims of the dimensions of top plate shall be provided and placed as detailed.

#### FOR INFORMATION ONLY

#### NOTES:

Hatched area indicates Removal of Existing Concrete Deck.

Cost of removal of existing Handrail is included with Removal of Existing Concrete Deck.

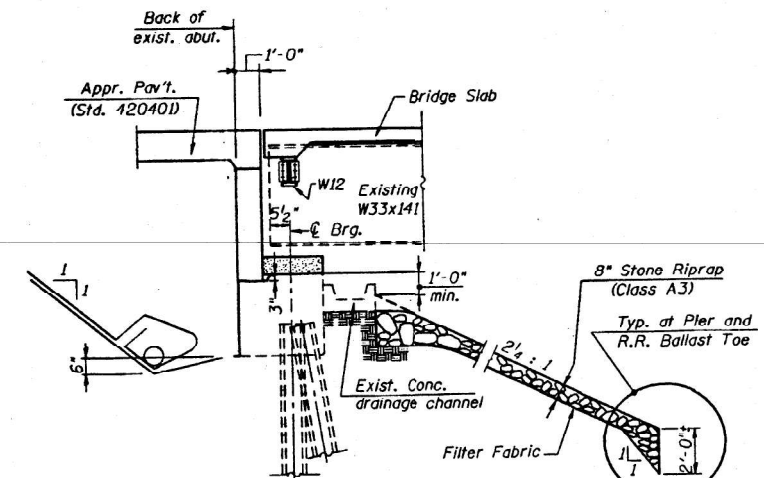
Care shall be taken not to cut or damage the vertical reinforcement in the backwalls during the removal operations.

All edges shall have  $\frac{3}{4}$ " chamfer, unless otherwise noted.

Vertical reinforcement extending into the removed areas shall be cleaned, straightened and incorporated into the new concrete.

Pay item for Temporary Concrete Barrier is included with the Roadway Plans.

For details of Temporary Concrete Barrier, see Sht. S18.



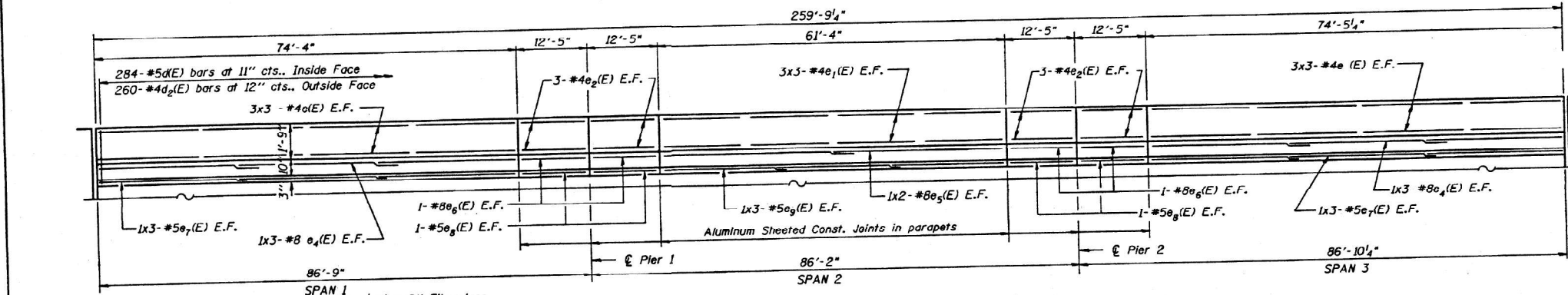
STONE RIPRAP TREATMENT  
EAST ABUTMENT SHOWN  
(SIMILAR AT WEST ABUTMENT)

REVISIONS	
NAME	DATE
G.F.L.	10-29-94
K.J.H.	07-08-00

ILLINOIS DEPARTMENT OF TRANSPORTATION	
GENERAL NOTES & CONSTRUCTION STAGING	
U.S. RTE. 24 OVER B.N.S.F. RAILROAD	
F.A.P. RTE. 317 SECTION (15VB)	
STA. 230+05.96	
FULTON COUNTY	
SCALE:	DRAWN BY: F. MUNIR
DATE:	CHECKED BY: B. SHAH
CHRISTIAN - ROGE & ASSOC.	
CHICAGO ILLINOIS	

NOTE:  
For location and spacing for Floor Drains and Drainage Scuppers, see Sht. S5

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(15VB)	FULTON	92	48
FED. ROAD DIST. NO. ILLINOIS				
FED. AID PROJECT				

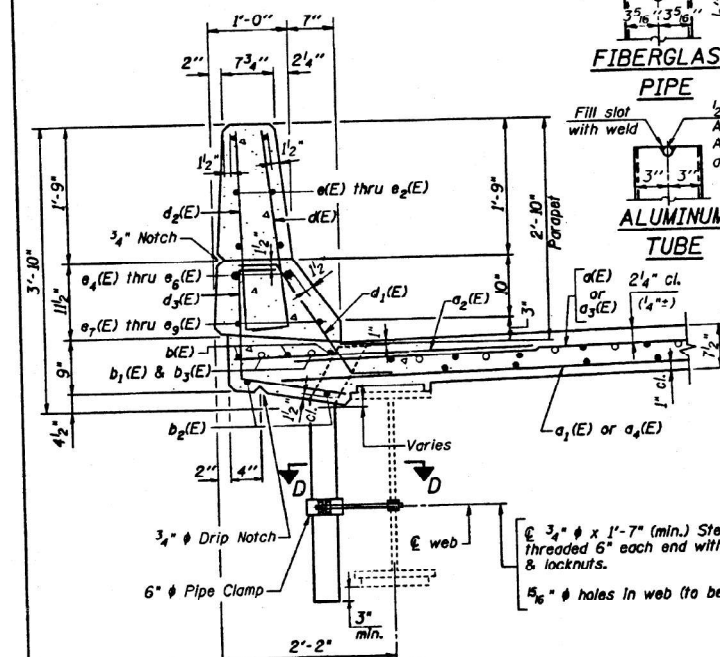


INSIDE ELEVATION OF PARAPET

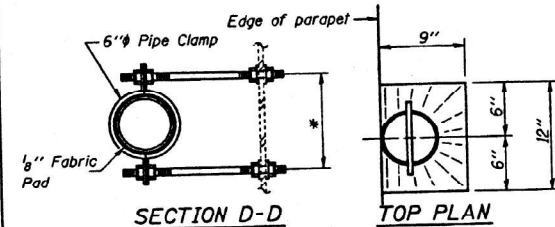
SOUTH PARAPET SHOWN  
(NORTH PARAPET OPP. HAND)

NOTATION  
E.F. = Each Face

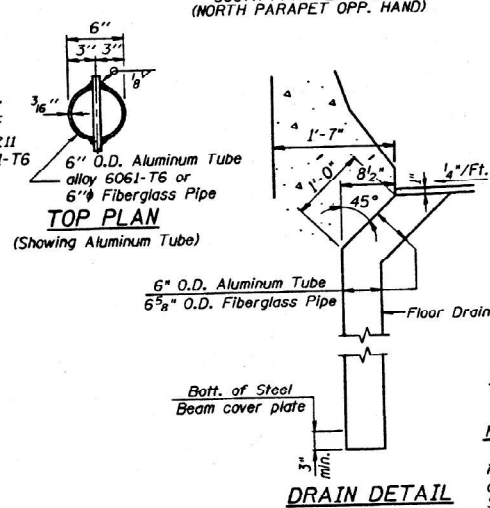
Min. Lap:  
#4 & #5 bars = 1'-8"  
#8 bars = 3'-8"



SECTION THRU PARAPET



\* Dimension as required by Pipe Clamp.



DRAIN DETAIL

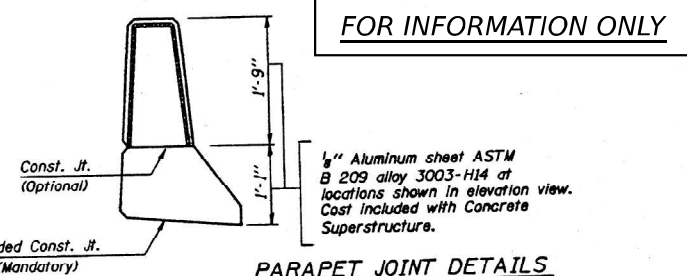
NOTES:

The exterior surfaces of the Floor Drain shall be painted with the finish coat as specified in the special provisions for cleaning and painting new metal structures. The exterior surfaces of the drain shall be cleaned according to Steel Structures Painting Council's Spec. SSPC-SPI prior to painting.

Fiberglass pipe shall conform to ASTM: D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.

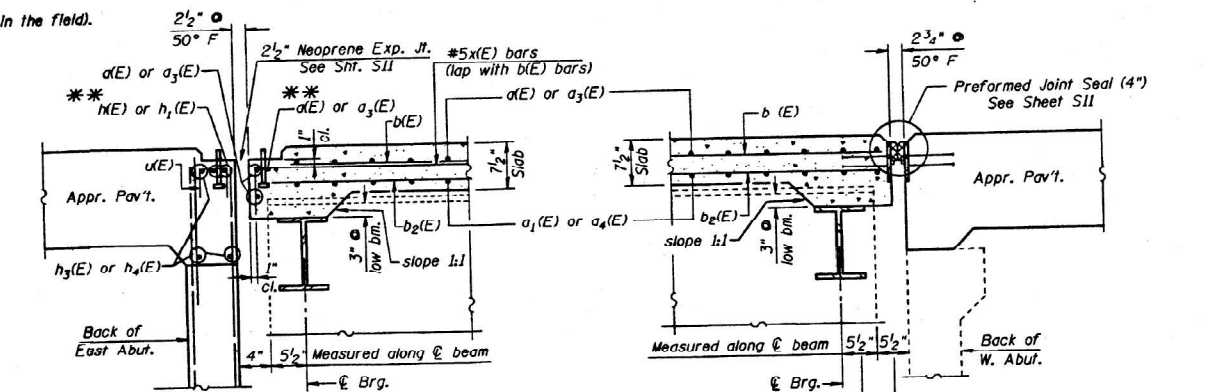
Fiberglass pipe shall conform to ASTM: D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.

### PARAPET JOINT DETAILS



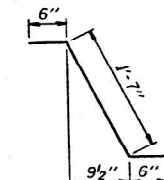
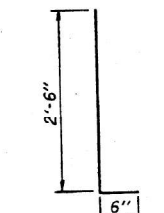
FOR INFORMATION ONLY

*1/8" Aluminum sheet ASTM  
B 209 alloy 3003-H14 at  
locations shown in elevation view.  
Cost included with Concrete  
Superstructure.*

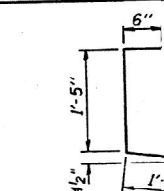
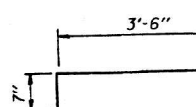


SECTION A-A

SECTION B-B

 $BAR \ d_1(E)$ 

*BARS*  $d(E)$  &  $d_2(E)$


$$BAR \ d_3(E)$$

$$\underline{BAR \ x(E)}$$

SUPERSTRUCTURE  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
$\alpha(E)$	417	#5	15'-1"	—
$\alpha_1(E)$	312	#5	15'-3"	—
$\alpha_2(E)$	416	#6	4'-0"	—
$\alpha_3(E)$	417	#5	17'-11"	—
$\alpha_4(E)$	312	#5	18'-1"	—
$\alpha(E)$	390	#5	27'-5"	—
$\beta_1(E)$	70	#6	36'-0"	—
$\beta_2(E)$	319	#5	25'-1"	—
$\beta_3(E)$	70	#6	12'-1"	—
$\alpha(E)$	568	#5	3'-0"	┐
$\alpha_1(E)$	568	#5	2'-7"	┐
$\alpha_2(E)$	520	#4	3'-0"	┐
$\alpha_3(E)$	520	#4	3'-1"	┐
$\alpha(E)$	72	#4	25'-11"	—
$\alpha_1(E)$	36	#4	21'-6"	—
$\alpha_2(E)$	48	#4	12'-1"	—
$\alpha_3(E)$	24	#8	27'-2"	—
$\alpha_4(E)$	8	#8	32'-1"	—
$\alpha_5(E)$	16	#8	12'-4"	—
$\alpha_6(E)$	24	#5	25'-10"	—
$\alpha_7(E)$	16	#5	12'-1"	—
$\alpha_8(E)$	12	#5	21'-6"	—
$\alpha(E)$	39	#5	4'-1"	┐
Bar Splicers			Each	729
Protective Coat			Sq. Yd.	1465
Floor Drains			Each	20
Reinforcement Bars, Epoxy Coated			Lb.	64,020
Concrete Superstructure			Cu. Yd.	281.3

Reinforcement bars designated (E) shall be epoxy coated.

Bars indicated thus 1x3-#5 etc., indicates 1 line of bars with 3 lengths per line.

REVISIONS	
NAME	
R.J.H.	
CRA	

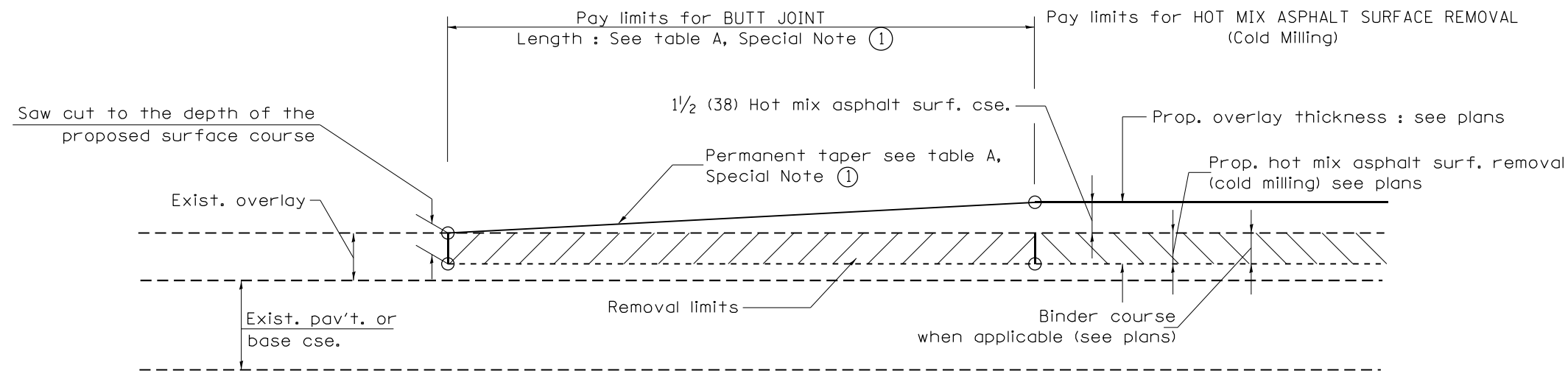
ILLINOIS DEPARTMENT OF TRANSPORTATION

PARAPET, DECK DETAILS &  
SUPERSTRUCTURE BILL OF MATERIAL  
U.S. RTE. U.S. 24 OVER  
B.N.S.F. RAILROAD  
F.A.P. RTE. 317 SECTION (15VB1)  
STATION 230+05.96  
FULTON COUNTY

SCALE: DRAWN BY: F. MINIR  
DATE: CHECKED BY: B. SHAH

CHRISTIAN - ROGE & ASSOC.  
CHICAGO ILLINOIS

SCALE: DRAWN BY: F. MUNIR  
DATE: CHECKED BY: B. SHAH  
CHRISTIAN - ROGE & ASSOC.  
CHICAGO ILLINOIS



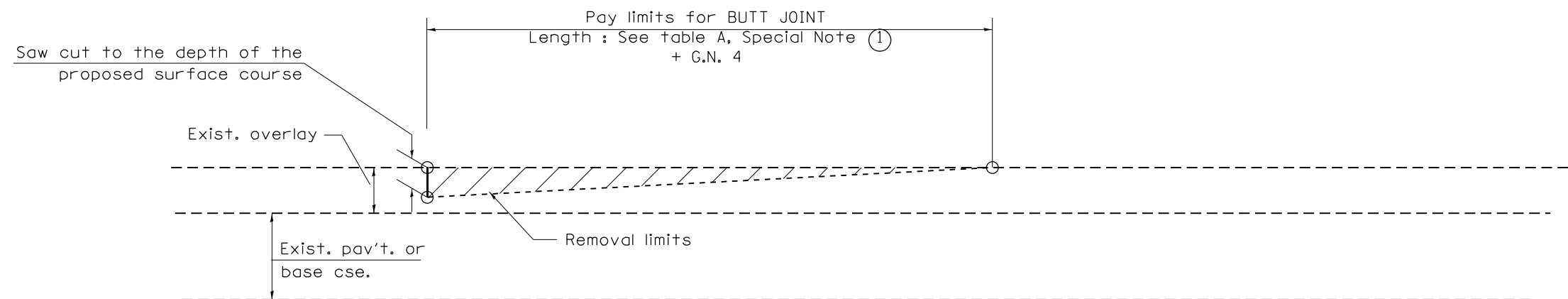
**CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

**TABLE A  
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

**GENERAL NOTES**

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.

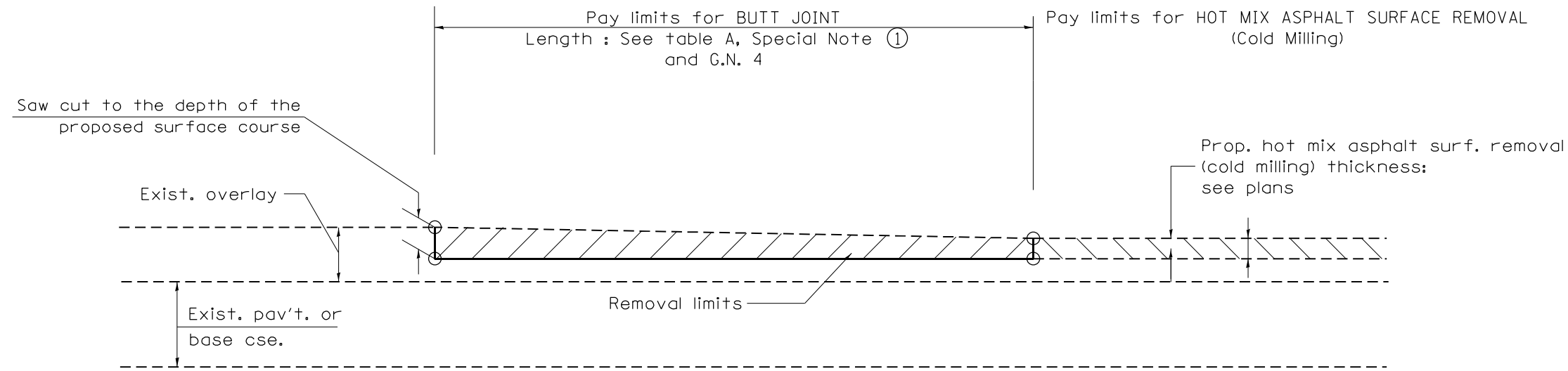


**CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

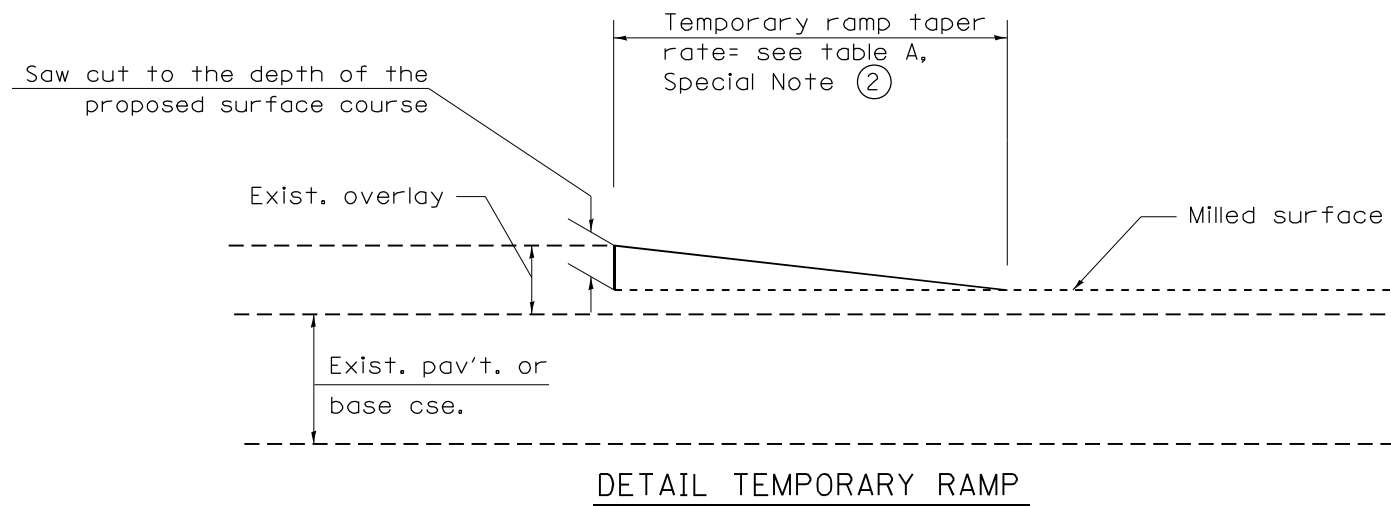
All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	SHT. 1 OF 3 CADD STD. 406101-D4	NOT TO SCALE	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	04-12-16	MINOR CORRECTIONS	R.D.								38	32
09-15-05	REVISED DESIGNER NOTE	M.M.A.	02-14-17	ADDED NOTE 5	R.D.								CONTRACT NO.	
10-16-06	REVISED TO 2007 SPEC.	M.A.	07-16-19	Wording and Spelling corrections	R.D.								FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



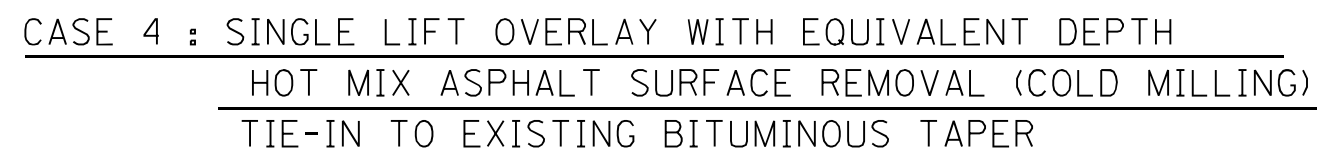


**CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)  
TIE-IN TO EXISTING BITUMINOUS TAPER**



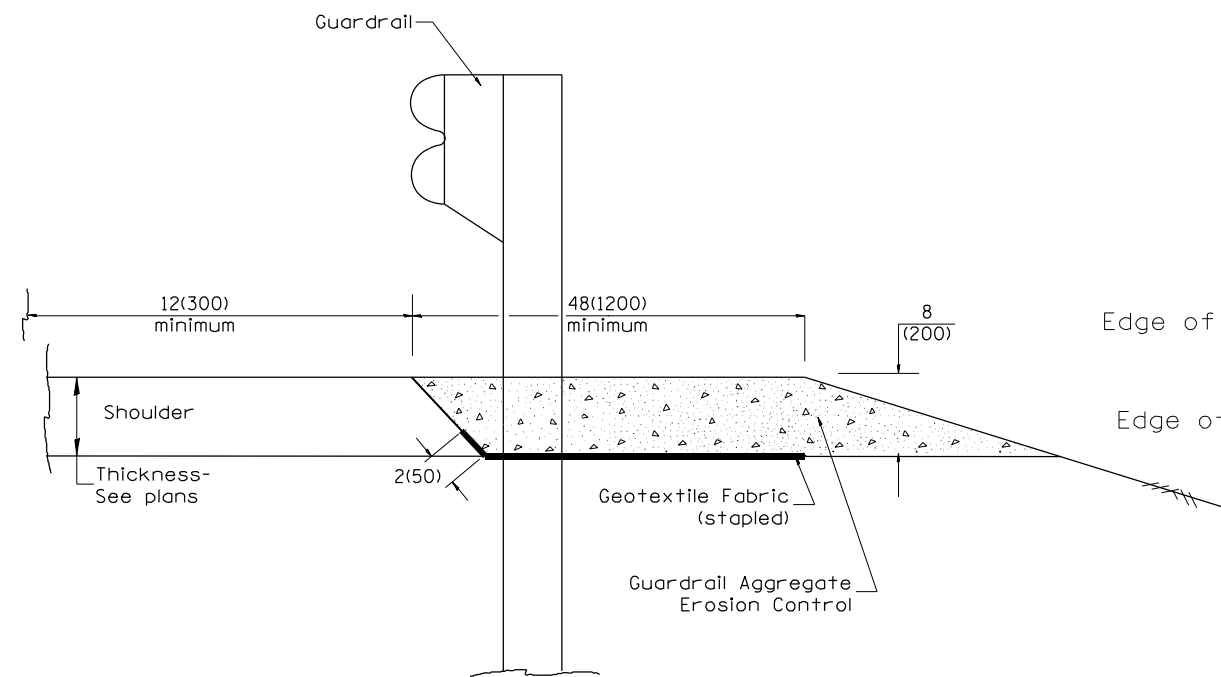
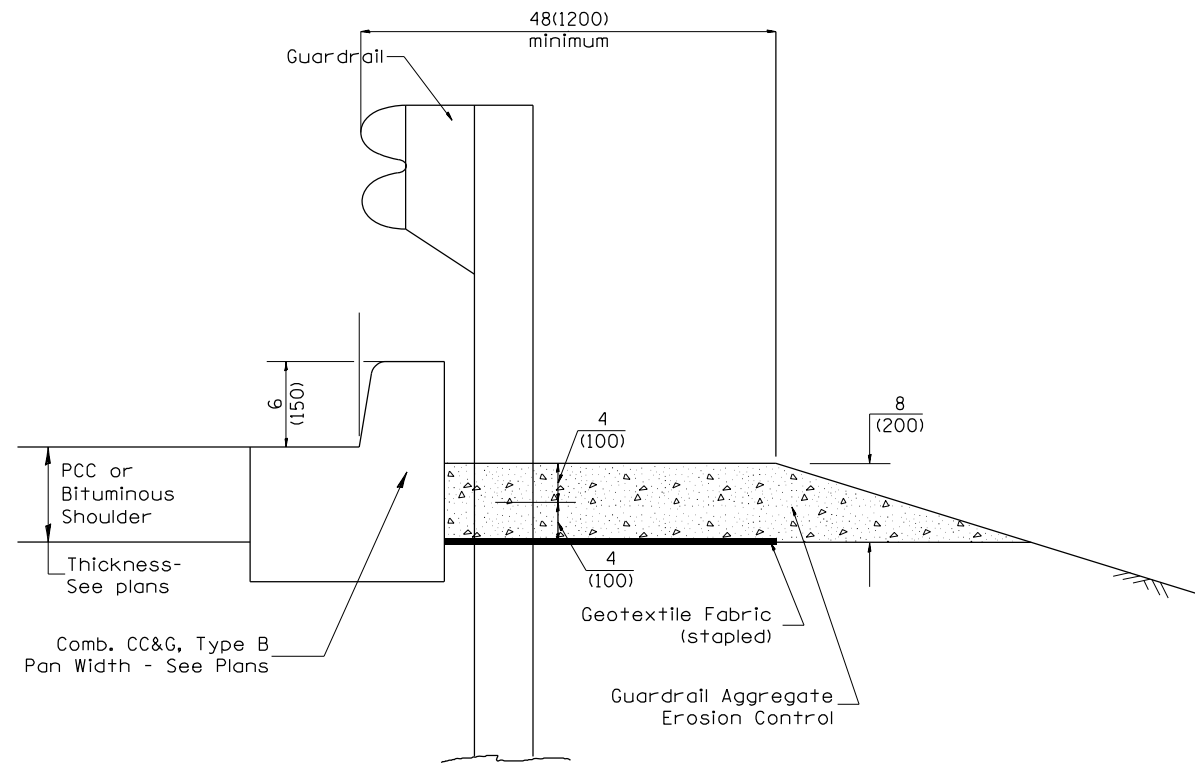
All dimensions are in inches (millimeters)  
unless otherwise noted.

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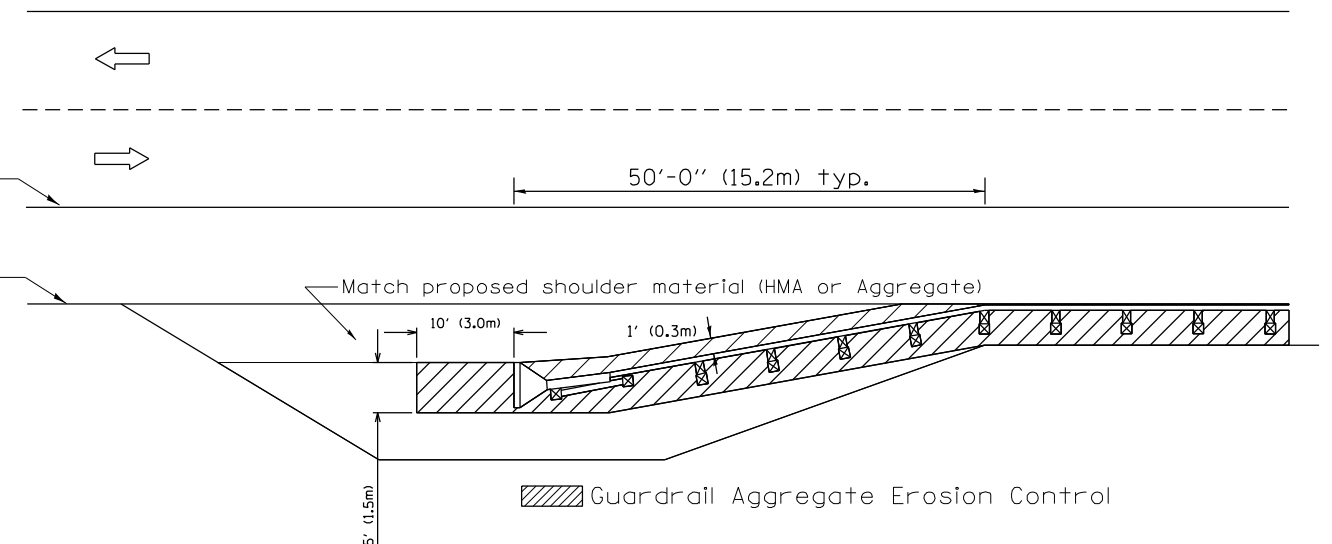
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DESIGNER NOTES:

- 1.
- 2.
- 3.
- 4.
- 5.
- 6.
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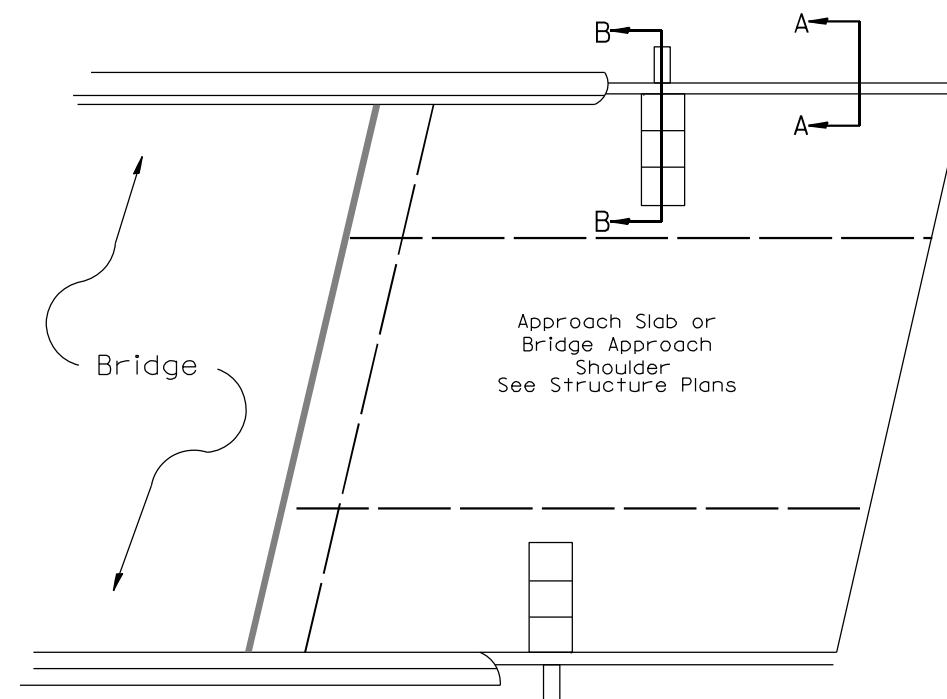
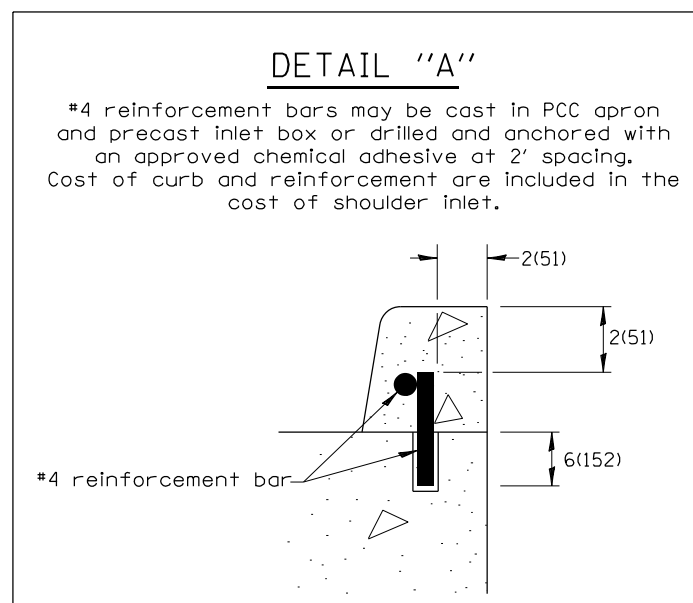


1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
  - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
  - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.

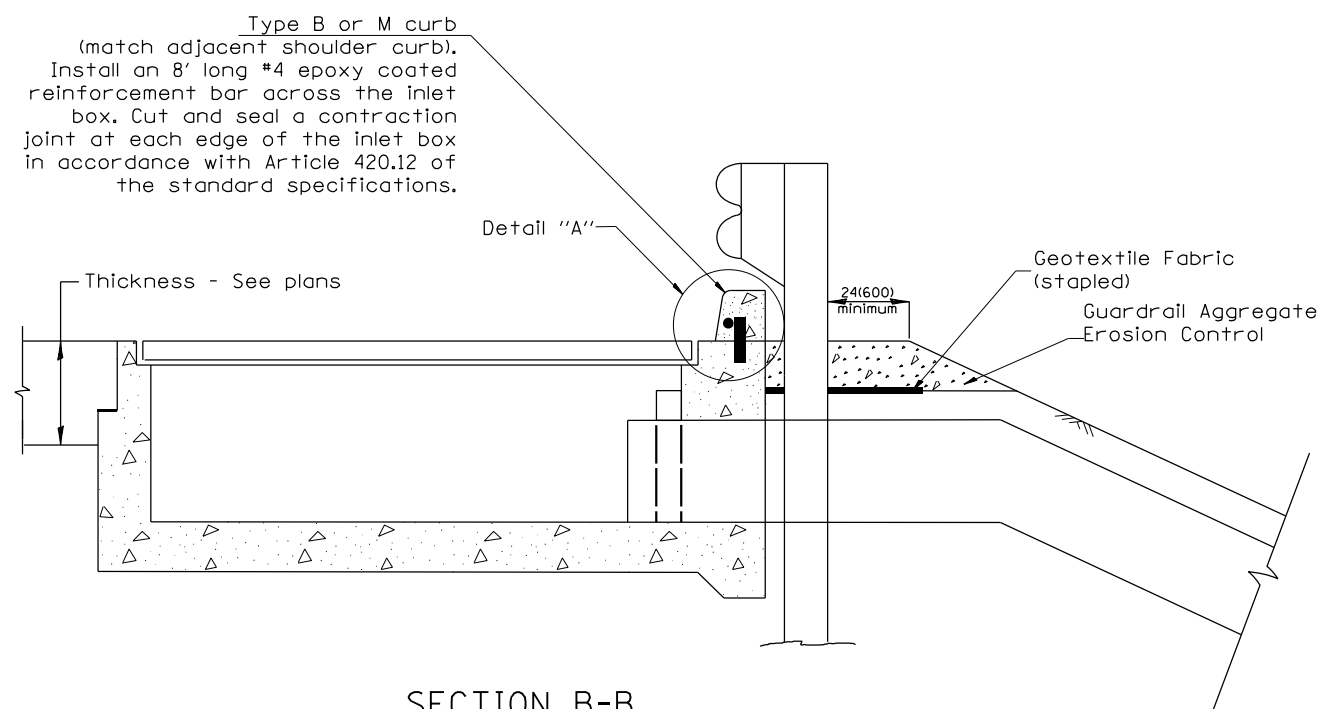


All dimensions are in inches (millimeters) unless otherwise noted.

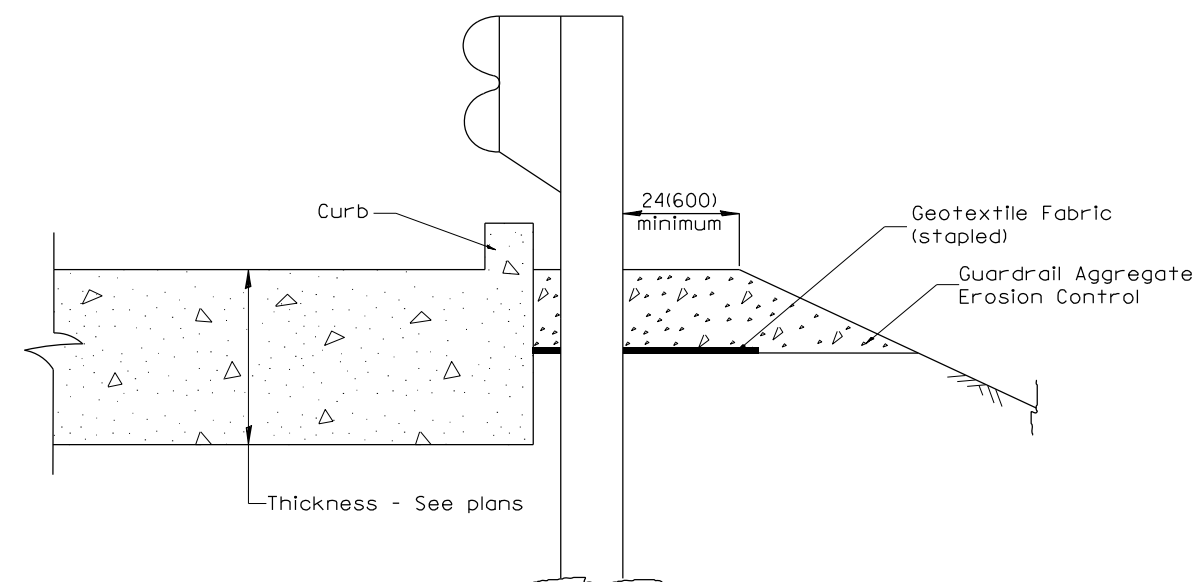
03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.	5-30-18	CHANGE B CURB TO CC&G	R.D.	<div>STATE OF ILLINOIS</div> <div>DEPARTMENT OF TRANSPORTATION</div>	<div>GUARDRAIL EROSION CONTROL TREATMENTS</div> <div>SHT. 1 OF 2</div> <div>CADD STD. 630101-D4</div> <div>NOT TO SCALE</div>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.	07-16-19	SPELLING CORRECTIONS	R.D.						38	35	
07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.											
01-26-17	REVISED	R.D.											
								CONTRACT NO.		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	



**PLAN VIEW**  
**APPROACH SLAB OR SHOULDER PLACEMENT**



**SECTION B-B**  
**TYPICAL SECTION AT INLETS**  
**TYPE E, F & G (HIGHWAY STANDARD 610001)**



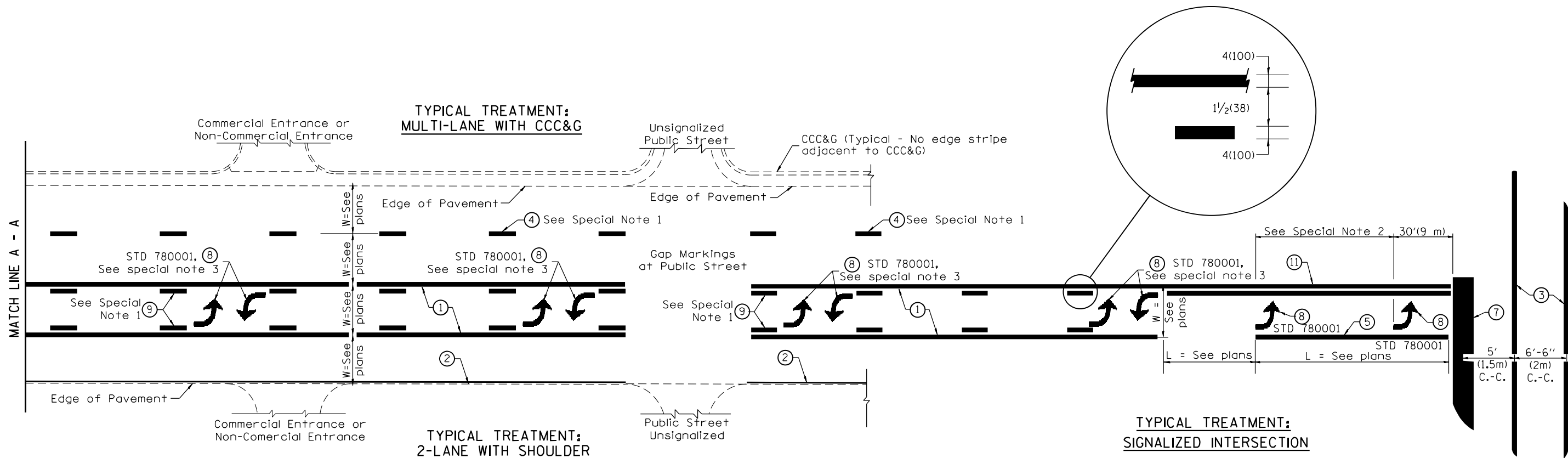
**SECTION A-A**  
**TYPICAL SECTION WITH BRIDGE APPROACH CURB**

All dimensions are in inches (millimeters) unless otherwise noted.

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DESIGNER NOTES:

1. Include State Standard 780001 (Typical Pavement Markings)



**FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE  
WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION**

**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend.  
Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A)
- ⑪ 4(100) Double Solid (Yellow)

**SPECIAL NOTES**

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
  - A. A minimum of two (2) arrows is required.
  - B. The maximum spacing between arrows is 80' (24 m).
  - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
  - A. A minimum of two (2) arrow pairs is required.
  - B. The maximum spacing between arrow pairs is 200' (61 m).
  - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
  - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

**GENERAL NOTES**

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.).
4. Areas are grooved 1" beyond each edge for the following symbols:
  - Through Arrow= 14.8 sq. ft.
  - Large Left or Right Arrow= 21.9 sq. ft.
  - 2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
  - Wrong Way Arrow= 29.5 sq. ft.
  - Railroad Crossing Symbol= 69.8 sq. ft.(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS	SHT. 1 OF 2 CADD STD. 780001-D4	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.							38	37			
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.											
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.														
NOT TO SCALE									CONTRACT NO.							
									FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

