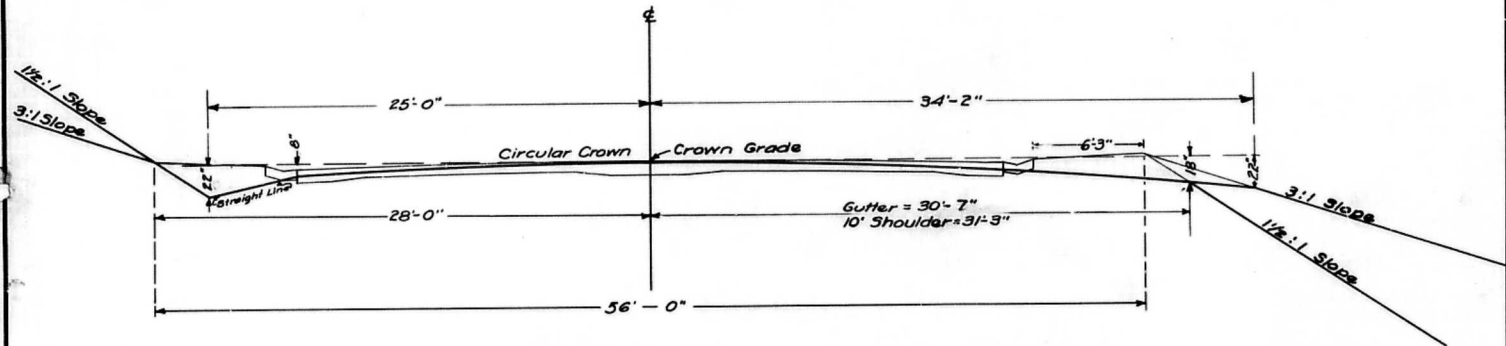


For information only

SOUND ISSUE ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
8	10 04 NRH	Peoria	93	2
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	244-A-NR	

**SPECIAL GRADING SECTION
FOR
FUTURE 40 FT. (9-9-7-9) PAVEMENT
RT. 8 SEC. 10 RA, NRH.**

Note: Heavy line indicates cross section to which earth roadway is to be graded. Light line indicates ultimate paving cross section.



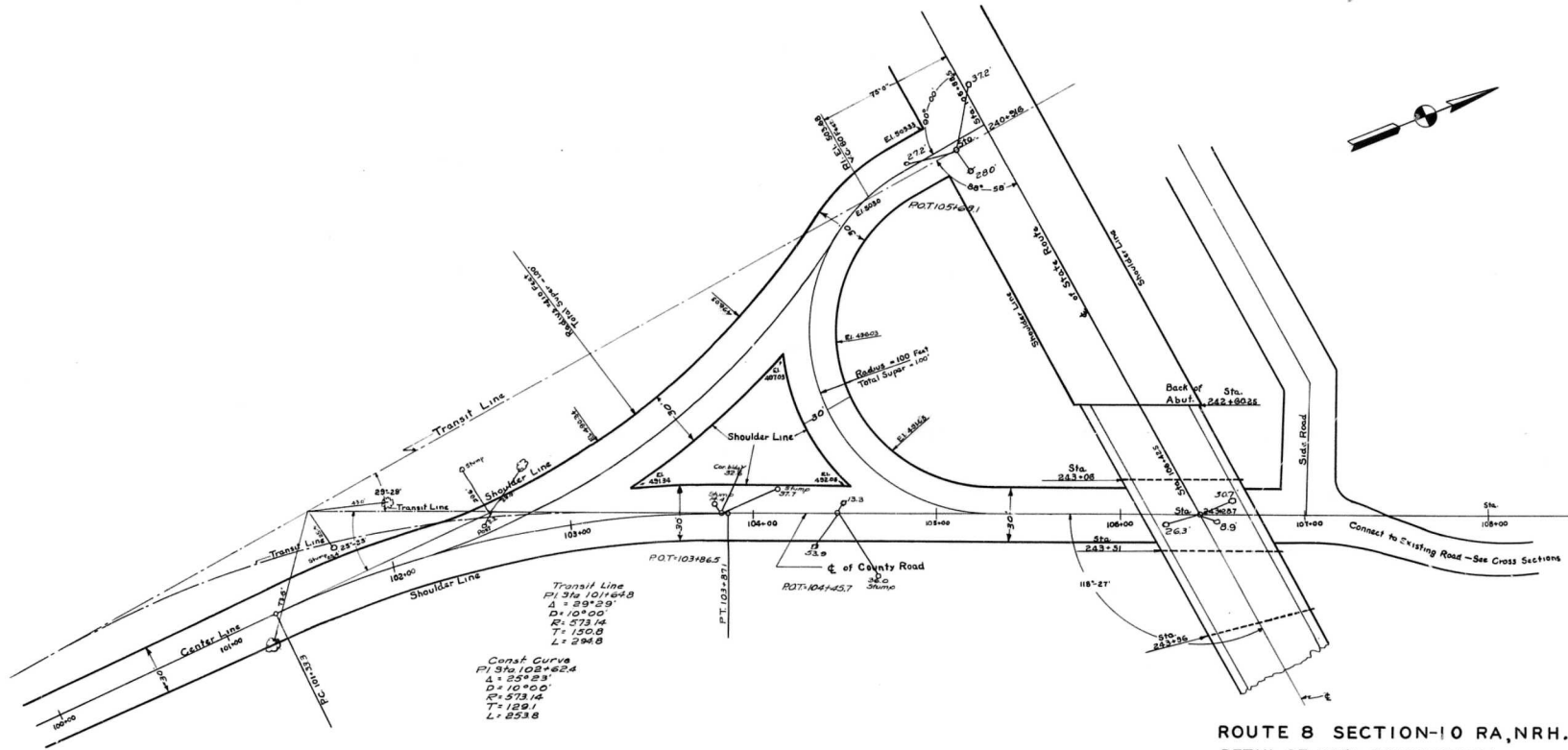
Drawn by B.V. Tilley 1-26-34. T.A. Broderick 1-29-34.

Scale 1" = 5'

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

PLAT	DATE	SEC.	COUNTY	TOTAL SQUARES	SHEET NO.
8	10-24	1000	Peoria	93	3
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT 243-42.5					

For information only



ROUTE 8 SECTION-10 RA, NRH.
DETAIL OF WYE CONNECTION
STATE AND COUNTY ROADS

STATION 243+28.7 = 106+42.5
PEORIA COUNTY

SCALE 1" = 30'

DRAWN BY RFE:HAL
DATE 3-5-34

GENERAL NOTES

A furrow shall be plowed as directed by the engineer three feet out from the top of all backlopes where no ditch is provided. A furrow shall be plowed at the top of all backlopes where the cut is six feet or greater. This furrow shall be three feet back of the edge of the slope. All backlopes within the right of way shall be trimmed to a uniform surface. Payment for the above described work shall be included in the unit price bid per cubic yard of earth excavation.

Riprap where specified shall be at least 12 inches in thickness. Clearing and grubbing shall be included in the contract unit price but per cubic yard of "Earth Excavation," "Channel Excavation," or "Borrow" and no extra compensation shall be allowed.

STANDARD GRADING SECTION NO. 1601.

25'-6" ROADBED
LT. STA. 227+50 TO STA. 237+40 CURVE "A"
RT. STA. 234+66.4 TO STA. 237+40 CURVE "B"
WEST SIDE OF CURVE "C"

STANDARD GRADING SECTION
FUTURE 40 FT. PAVEMENT- 10 FT. SHOULDERS.
STA. 237+72 TO STA. 242+80.25
LT. STA. 264+50 TO STA. 267+25

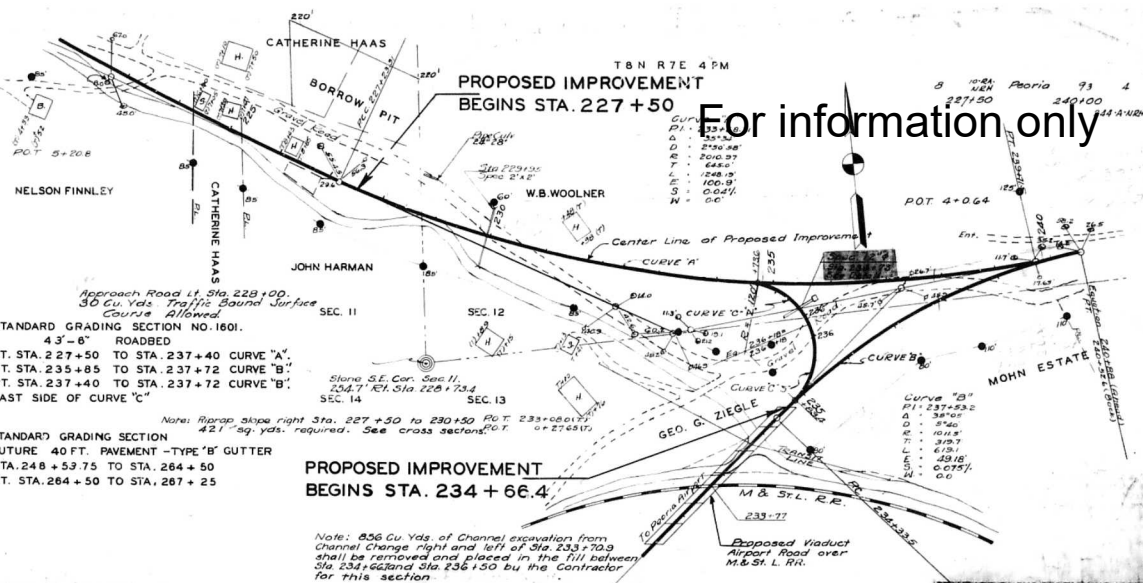
STANDARD GRADING SECTION NO. 1601.

43'-8" ROADBED
RT. STA. 227+50 TO STA. 237+40 CURVE "A"
LT. STA. 235+85 TO STA. 237+72 CURVE "B"
RT. STA. 237+40 TO STA. 237+72 CURVE "B"
EAST SIDE OF CURVE "C"

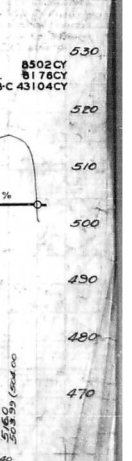
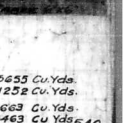
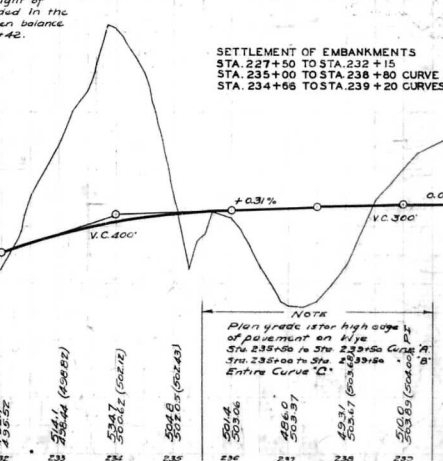
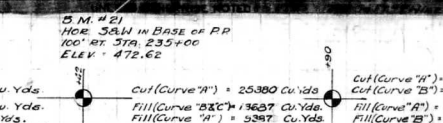
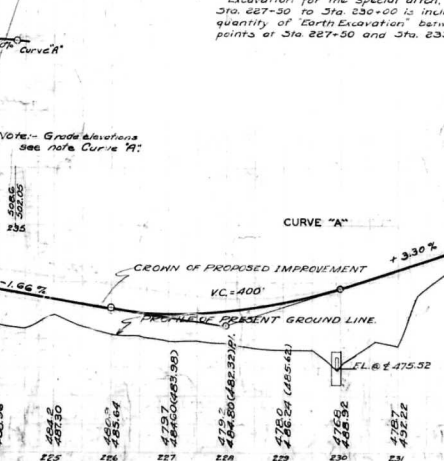
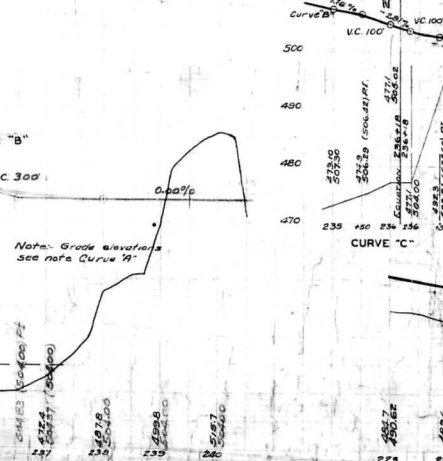
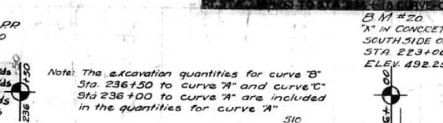
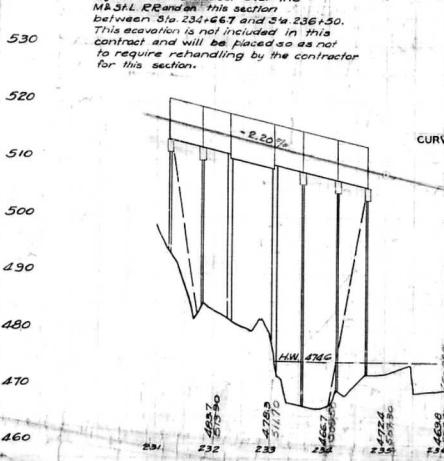
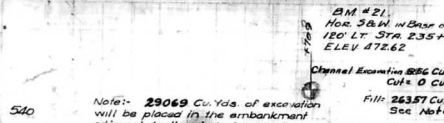
STANDARD GRADING SECTION
FUTURE 40 FT. PAVEMENT -TYPE "B" GUTTER
STA. 248+53.75 TO STA. 264+50
RT. STA. 264+50 TO STA. 267+25

Computed Earthwork Quantities for Approach Grades.

Type	Location	Cu. Yds.	Incl. Between Bal Points
Excavation	Sta. 228+00	0	Sta. 227+50 - 233+42
Fill	Sta. 233+00	1300	
Excavation	Sta. 237+00	1300	
Fill	Sta. 237+00 to Sta. 237+90	241+63	
Excavation	Sta. 237+90	241+63	



For information only

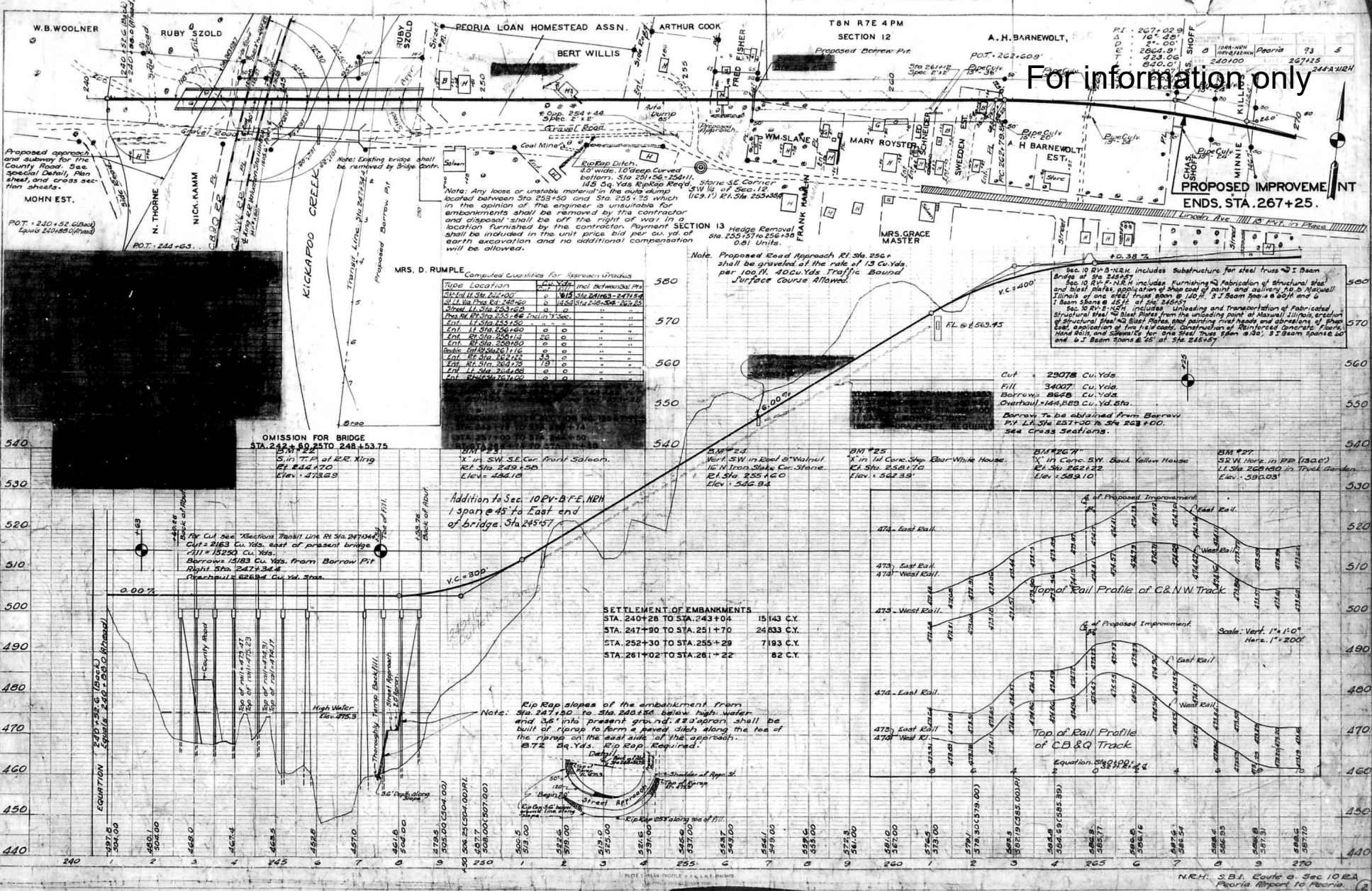


SETTLEMENT OF EMBANKMENTS
STA. 227+50 TO STA. 232+15 8502CY
STA. 233+00 TO STA. 238+00 CURVE A 8176CY
STA. 234+66 TO STA. 239+20 CURVES B-C 4310CY

NOTE: Plan grade after high slope of pavement on flye Sta. 235+00 to Sta. 239+20 Curve A Sta. 235+00 to Sta. 239+20 Curve B Entire Curve "C"

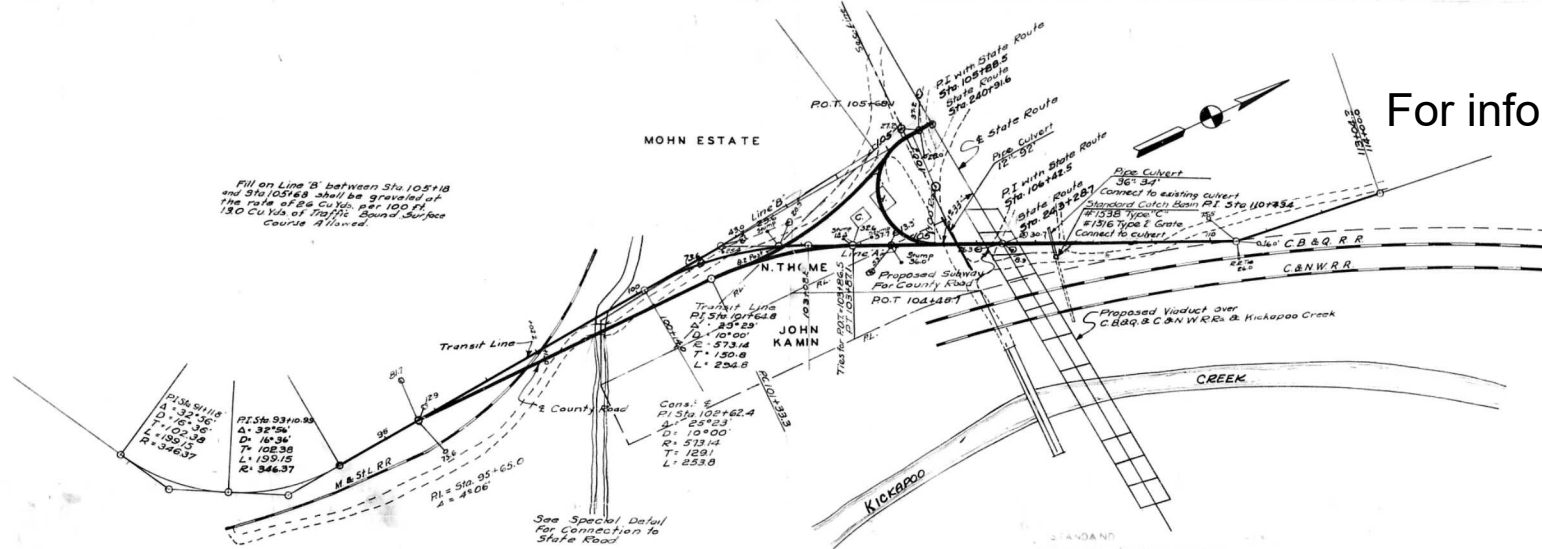
Sta. 227+50	503.6
Sta. 228+00	503.6
Sta. 229+00	503.6
Sta. 230+00	503.6
Sta. 231+00	503.6
Sta. 232+15	503.6
Sta. 233+00	503.6
Sta. 234+66	503.6
Sta. 235+00	503.6
Sta. 236+15	503.6
Sta. 237+40	503.6
Sta. 238+00	503.6
Sta. 239+20	503.6

For information only

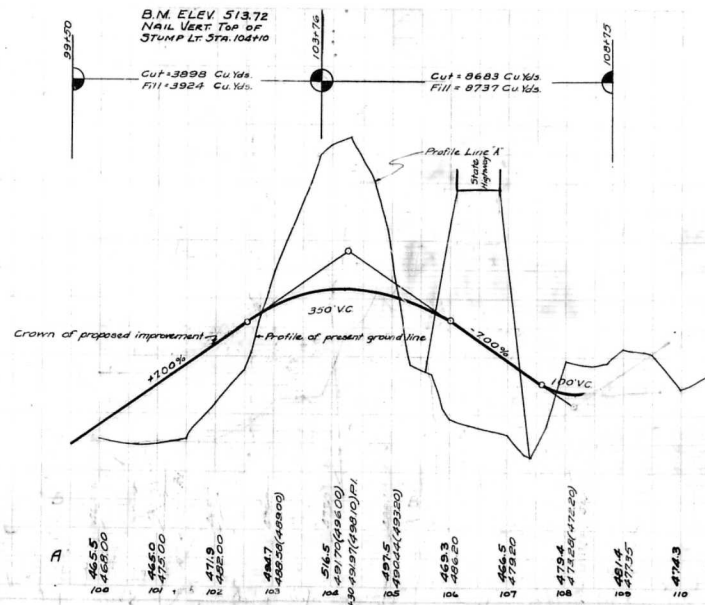


For information only

10-24 Florida 93 G
2401 916 2H (County Road) 2444 NEN



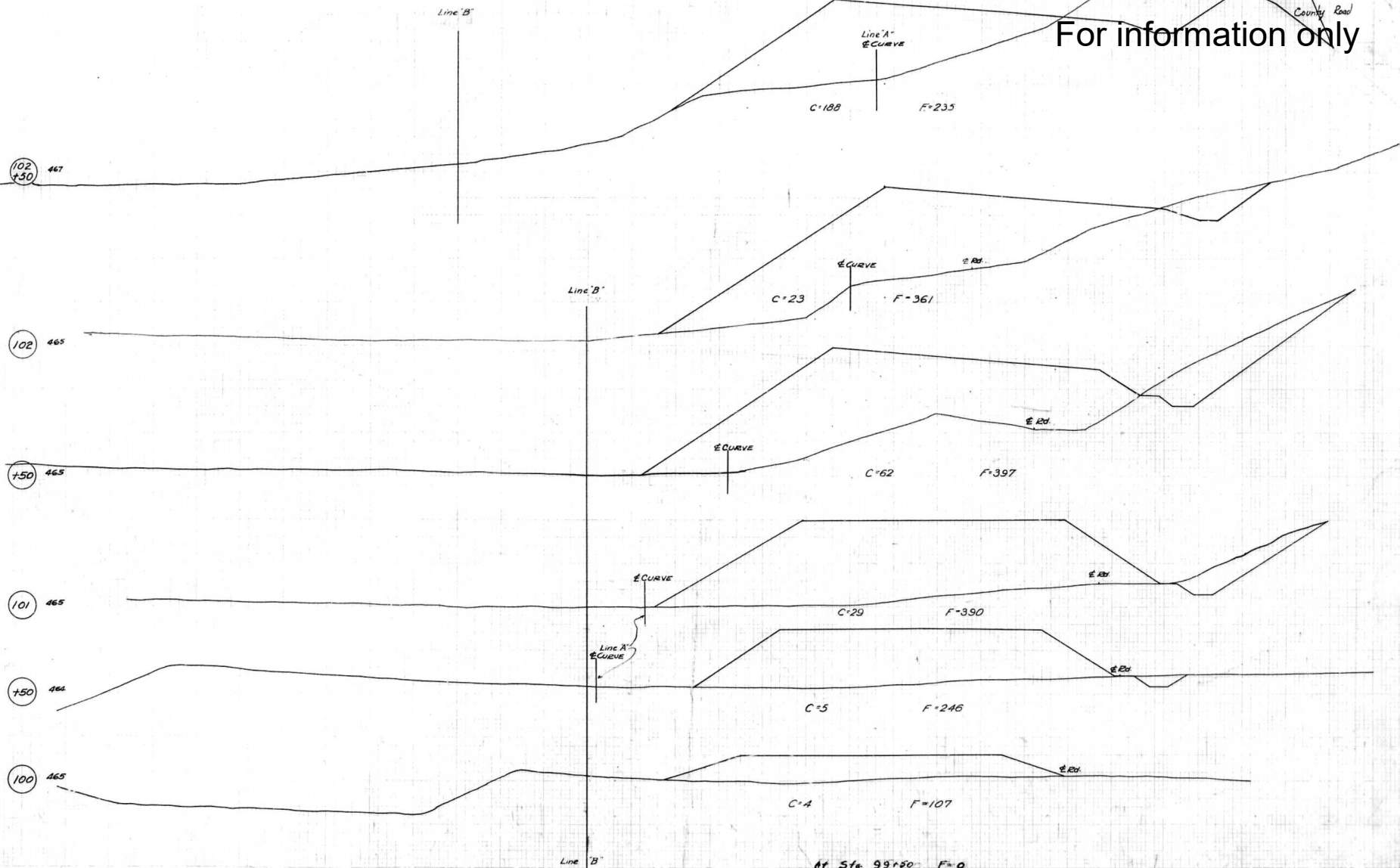
STANDARD GRADING SECTION
30 FT. ROADBED 2 IN. CROWN
SEE "X" SECTIONS



For information only

FOR
SHEET(S) 7 THRU 64

SEE
SBI ROUTE 8
SECTION 10 R
PEORIA COUNTY



County Road Connection to Route 8
Sec. 10-B-B N.P.H.

1.

For information only

Line 'B'

9 10-24 10-24 Dec 1933 16

244-A-1001

101 483
+20

C=163

F=225

E.R.

Wall

Church

104 477

C=74

F=217

E.R.

Wall

Church

182 473

C=14

F=182

E.R.

Wall

50' Line

150 472

C=0

F=143

E.R.

Edge Curve 'A'

103 472

C=0

F=207

E.T.M.

Line 'B'

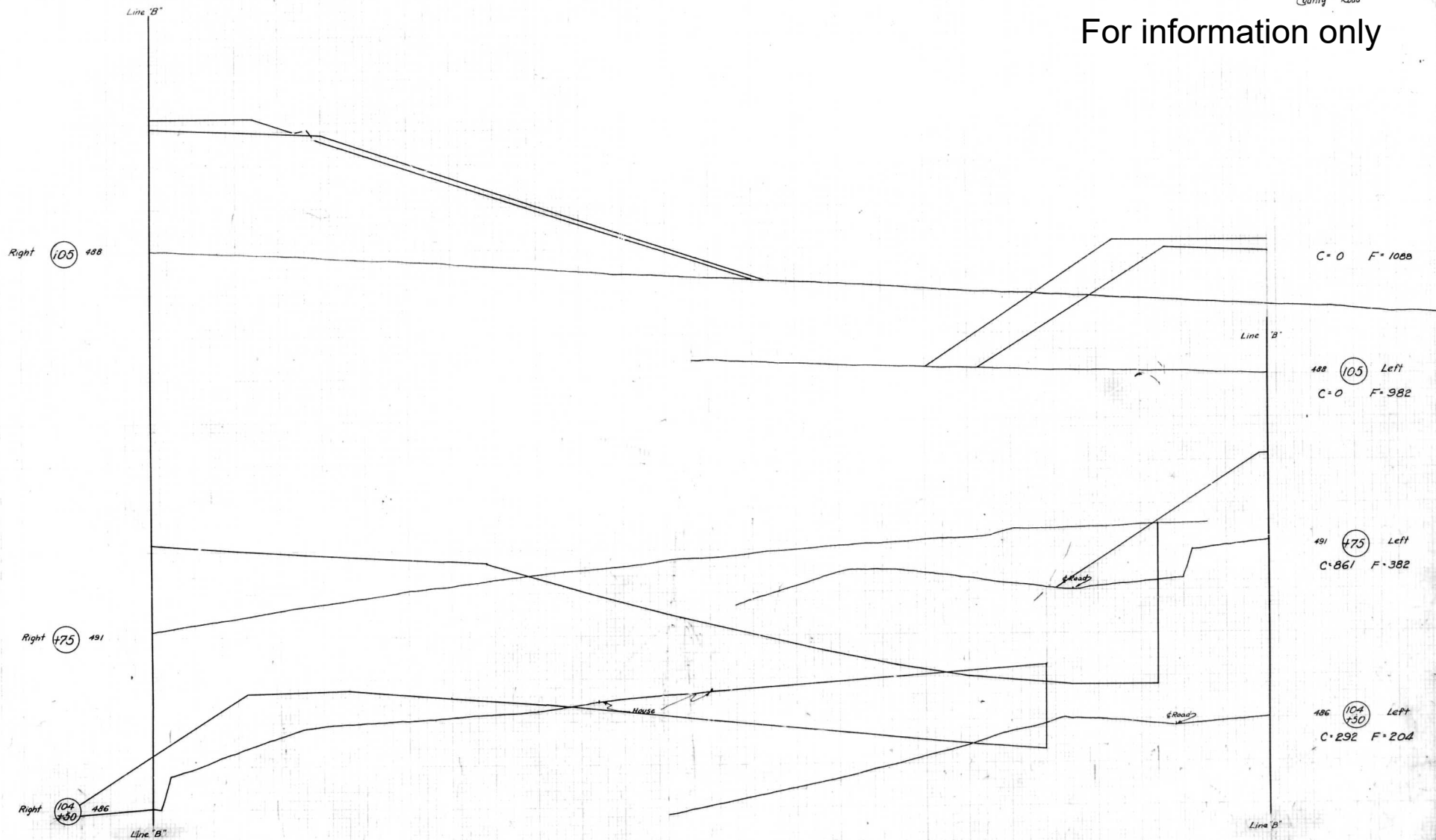
County Road Connection to Route 'B'

Sec. 10-24, N.R.H.

2

County Road

For information only



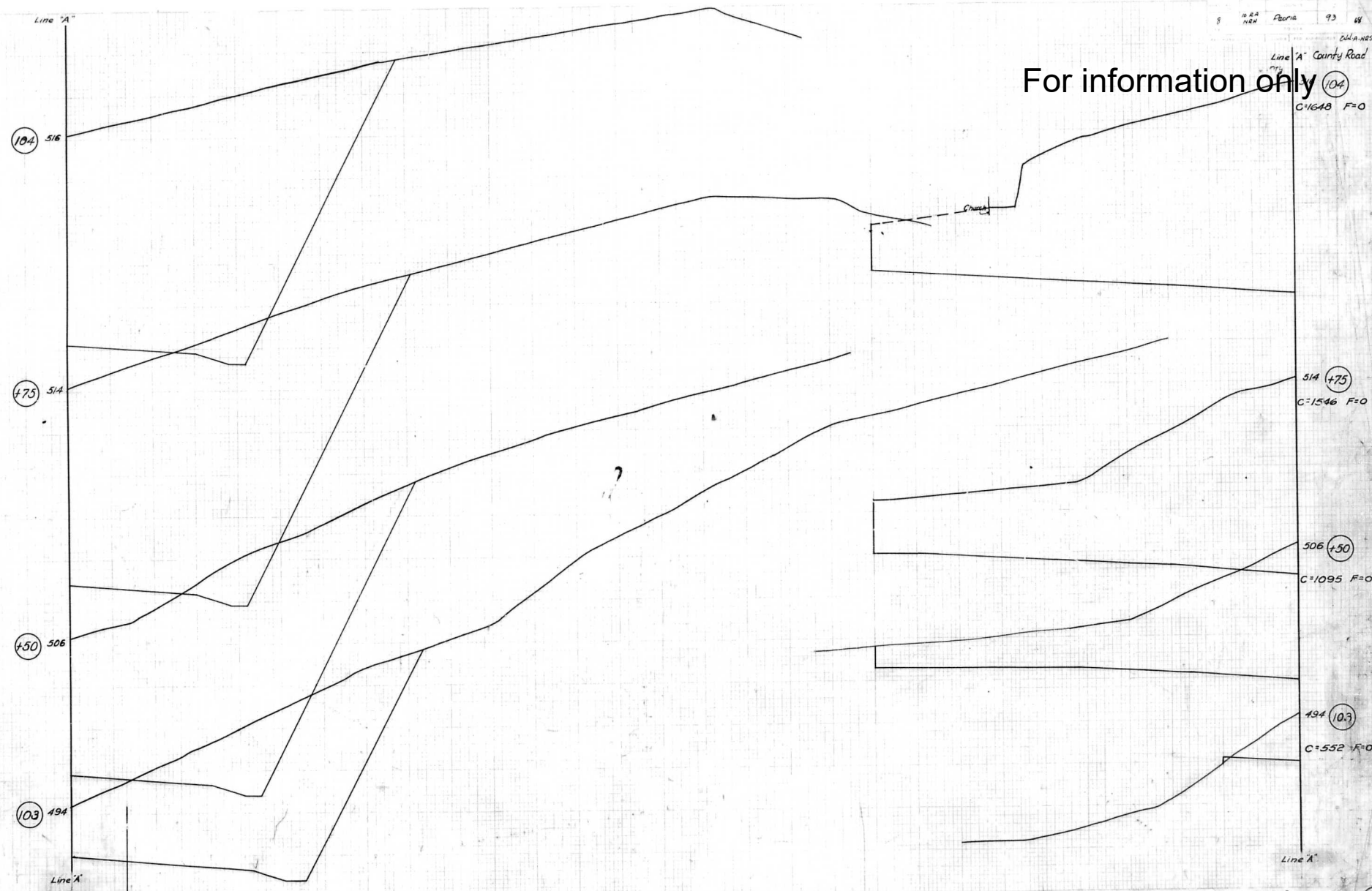
8	10 RA NRH	Florida	93	68
---	--------------	---------	----	----

Line "A" County Road

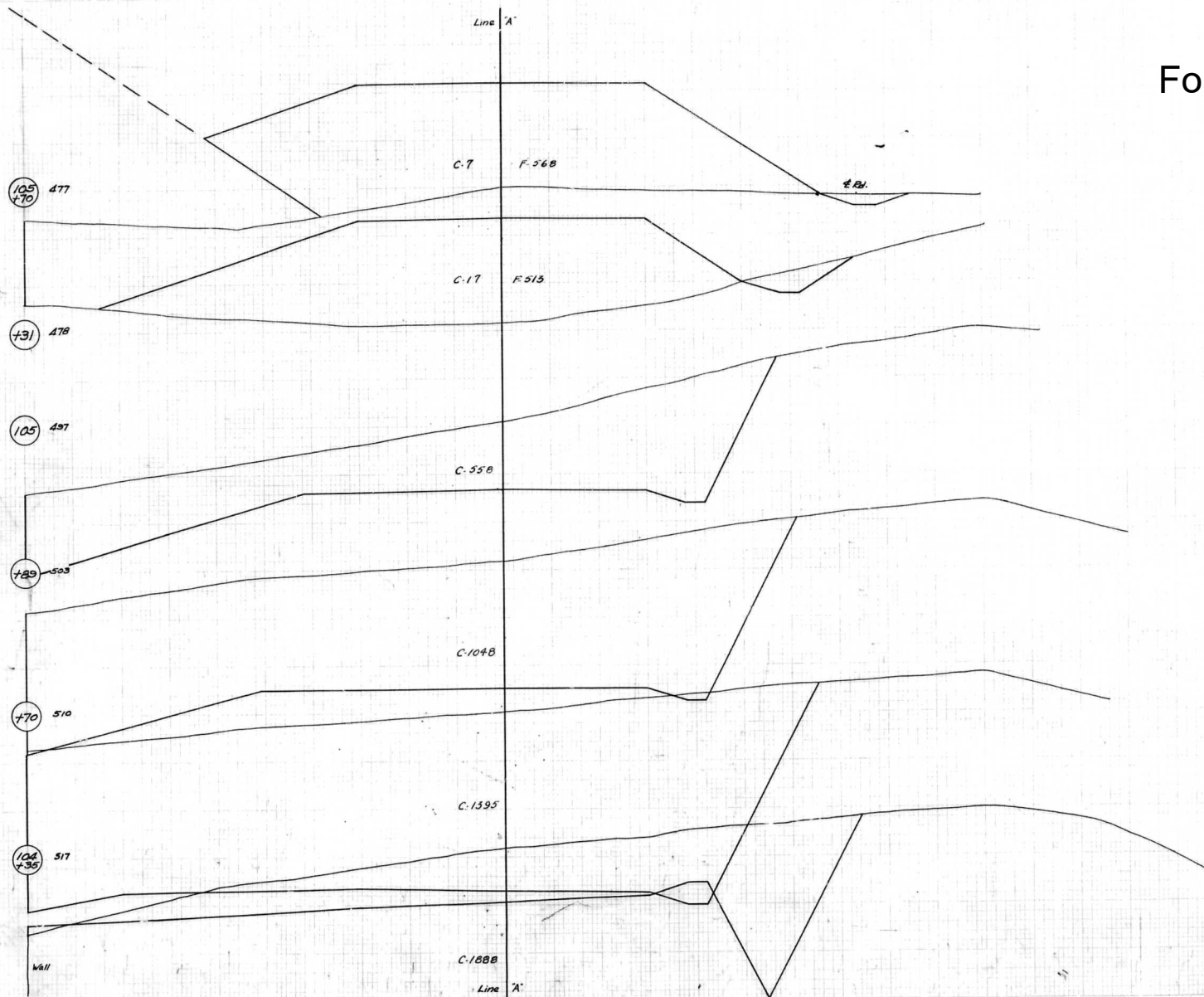
For information only (104)

C=1648 F=0

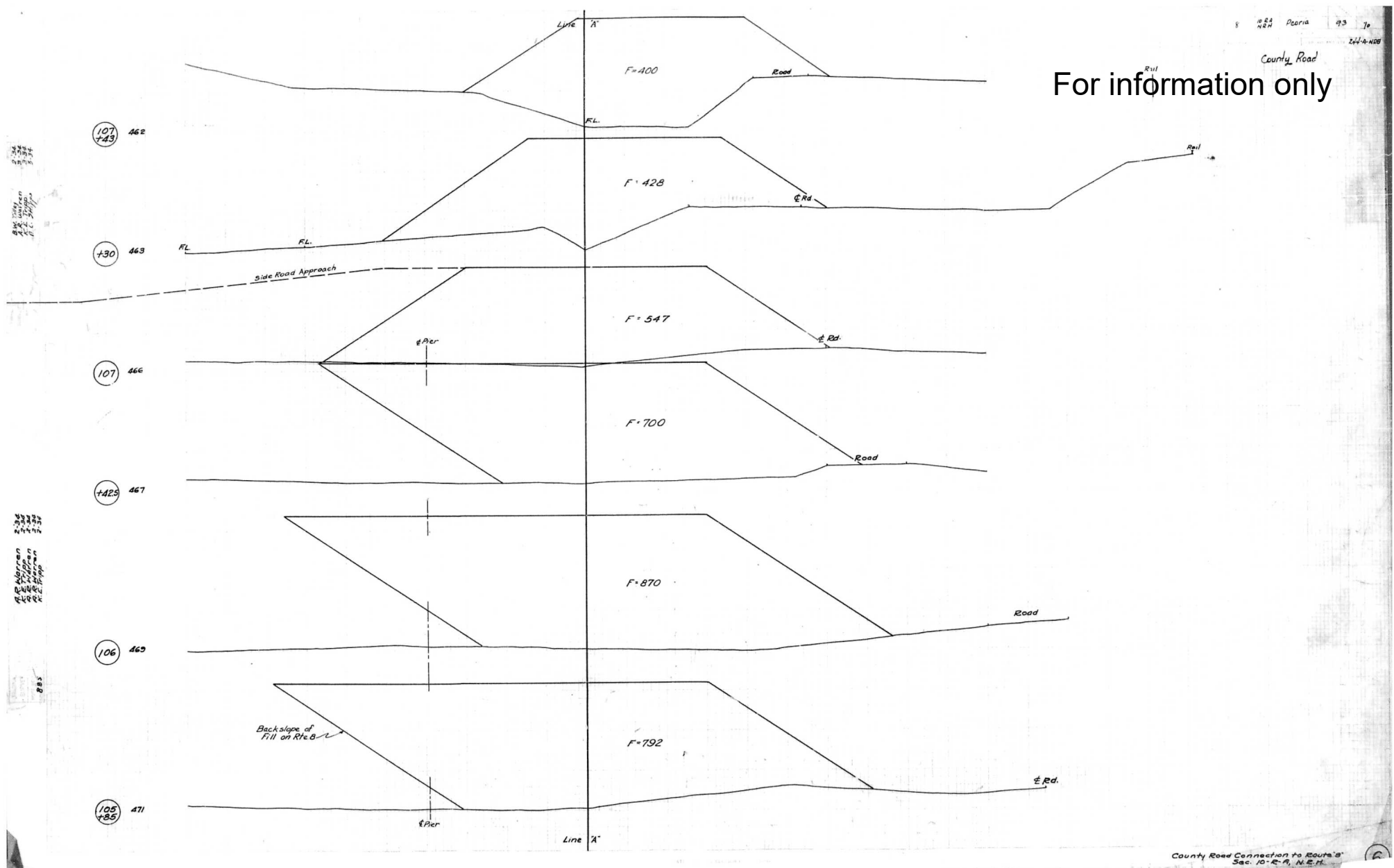
County Road Connection to Route 8
Sec. 10 - R.A., N.R.H.



For information only



For information only



County Road

For information only

Line A'

Line A'

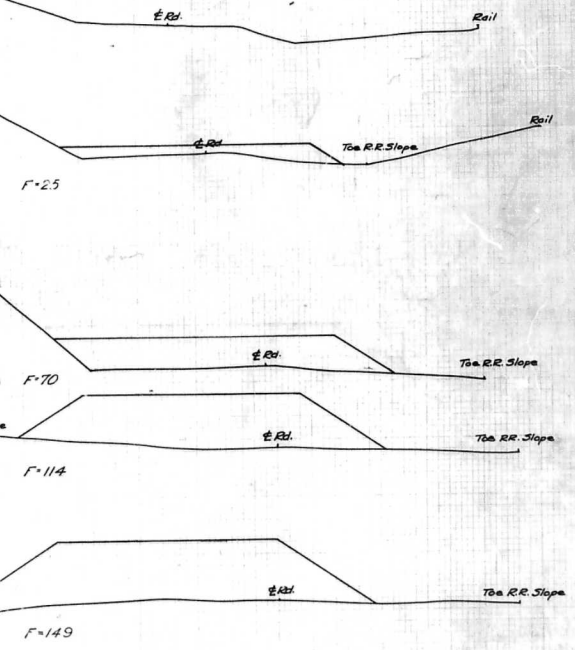
(108)
+75 479

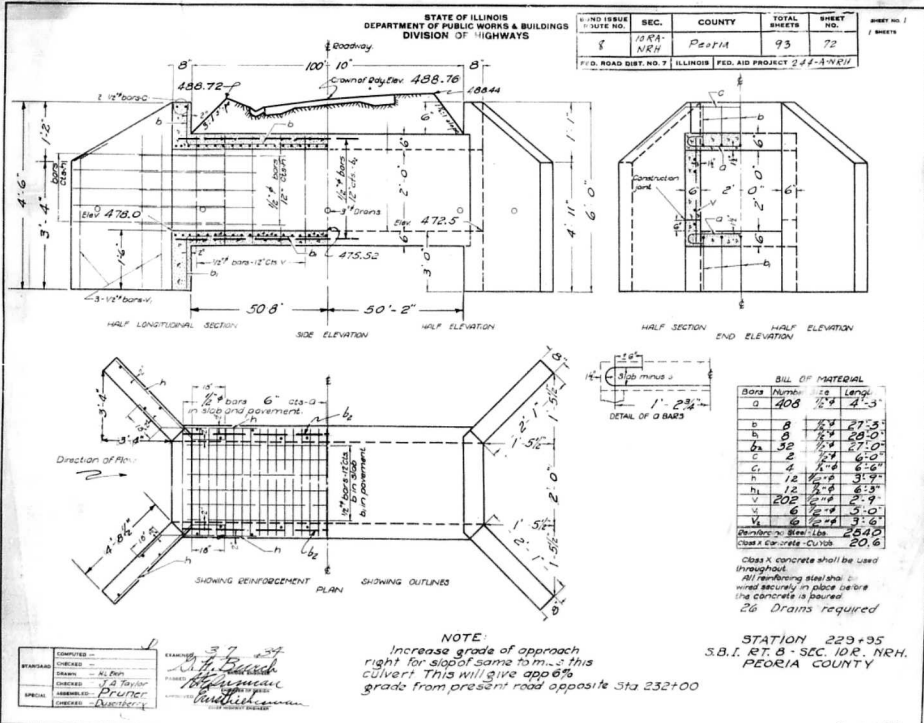
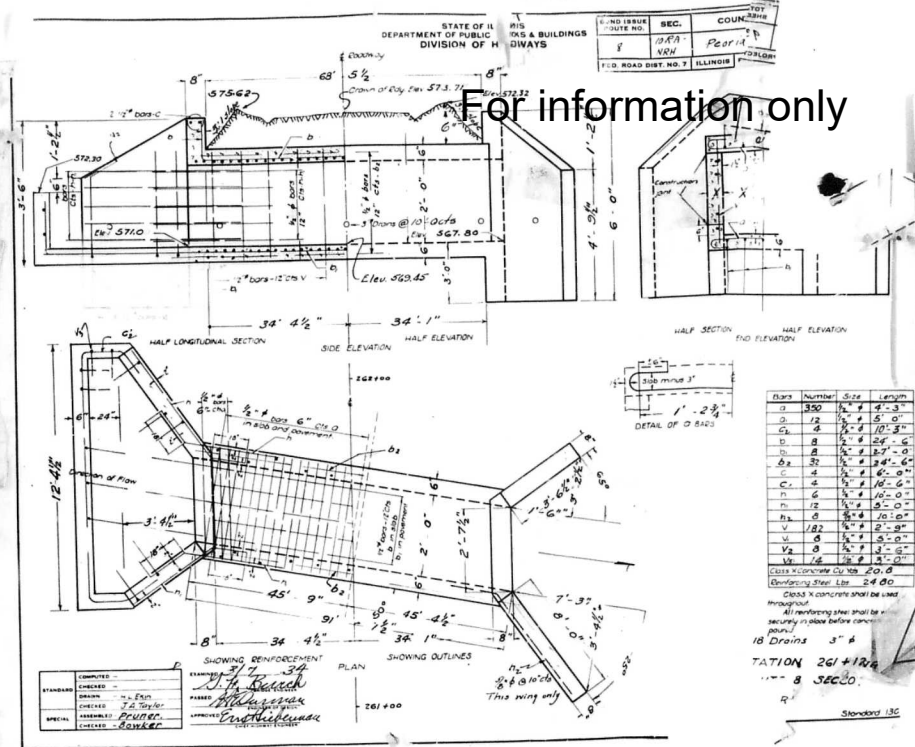
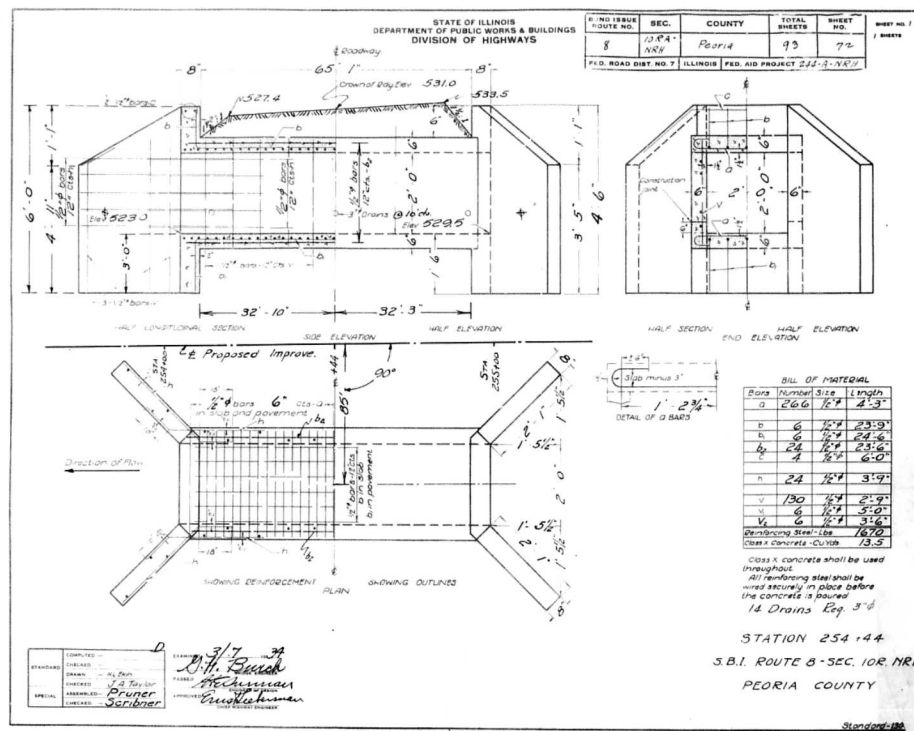
(+45) 478

(108) 479

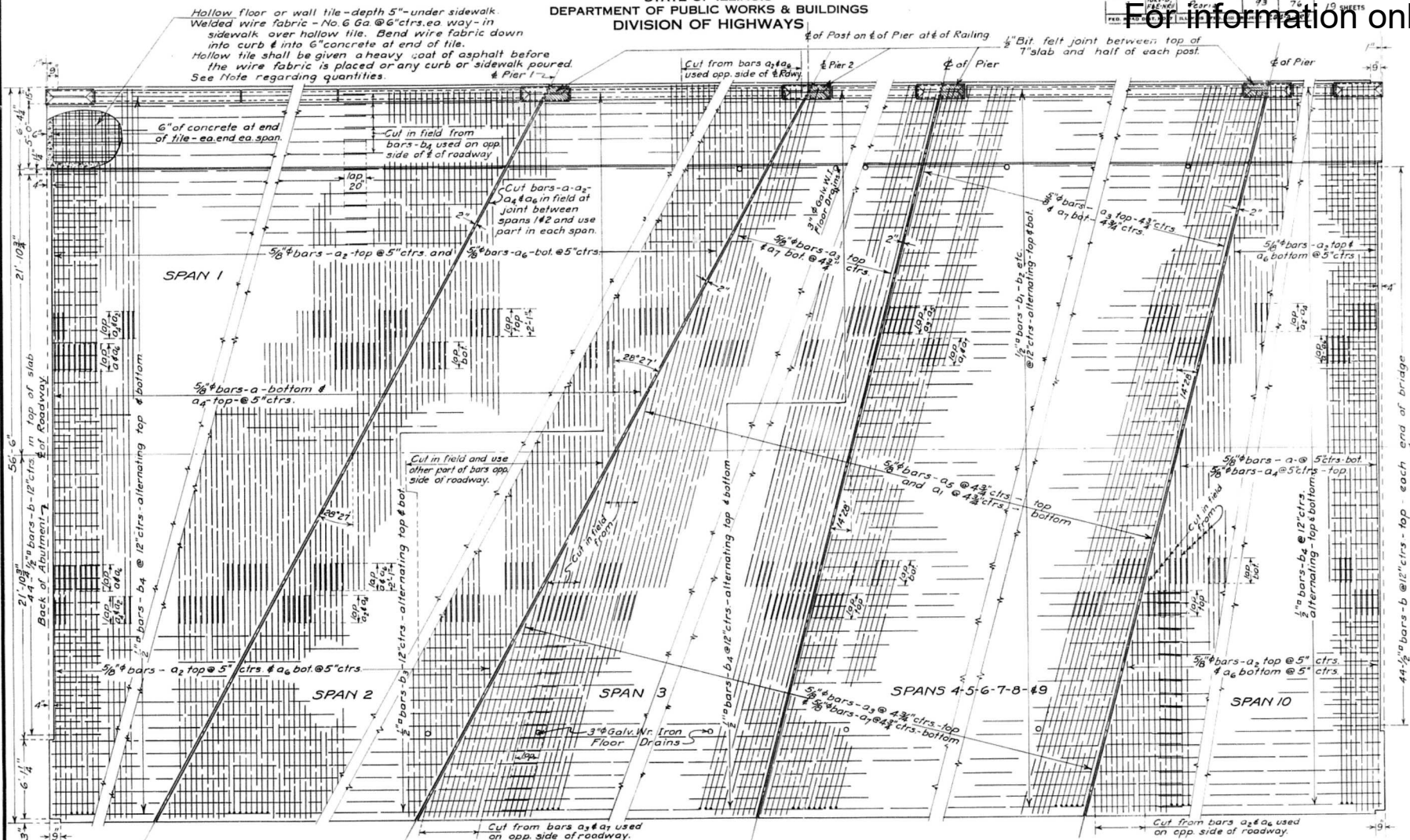
(+75) 470

(107)
+67 468





*Howlow floor or wall tile-depth 5"-under sidewalk. DEF
Welded wire fabric - No. 6 Ga @ 6" ctrs. eo way in
sidewalk over Howlow tile. Bend wire fabric down
into curb @ into 6" concrete at end of tile.
Howlow tile shall be given a heavy coat of asphalt before
the wire fabric is placed or any curb or sidewalk poured.
See Note regarding quantities. # Pier 1-2-3-4*



LOCATION # No. OF 3"Ø GALV. W.I. FLOOR DRAINS	LOCATION
1	1
2	2
3	3
4	4
5	5
6	6
7	7
8	8
9	9
10	10
11	11
12	12
13	13
14	14
15	15
16	16
17	17
18	18
19	19
20	20
21	21
22	22
23	23
24	24
25	25
26	26
27	27
28	28
29	29
30	30
31	31
32	32
33	33
34	34
35	35
36	36
37	37
38	38
39	39
40	40
41	41
42	42
43	43
44	44
45	45
46	46
47	47
48	48
49	49
50	50
51	51
52	52
53	53
54	54
55	55
56	56
57	57
58	58
59	59
60	60
61	61
62	62
63	63
64	64
65	65
66	66
67	67
68	68
69	69
70	70
71	71
72	72
73	73
74	74
75	75
76	76
77	77
78	78
79	79
80	80
81	81
82	82
83	83
84	84
85	85
86	86
87	87
88	88
89	89
90	90
91	91
92	92
93	93
94	94
95	95
96	96
97	97
98	98
99	99
100	100

Bar's	Span 1	Span 2	Span 3	Span 4	Span 5	Span 6	Span 7	Span 8	Span 9	Span 10
a ₁	109									109
a ₂	218	216	110	146	146	148	342	112	110	
a ₃			220	292	292	296	684	224	220	218
a ₄	109	108								109
a ₅			110	146	146	148	342	112	110	
a ₆	218	216								218
a ₇			220	292	292	296	684	224	220	
b ₁	44									44
b ₂				177	177					
b ₃						177				
b ₄	118	118	118					118		118
							354			

Span	No. Drains	LOCATION
1	None	
2	1	1 Ea. Gutter 4' W. of E. Pier 2
3	4	4 H. Side# 5.5 Side, @ 10' c'trs.
4	4	Each Side @ 5' from Ea. end of span.
5	4	" " " 5' " " " "
6	12	@ 10' c'trs. each side.
7	28	5 Ea. way from Ea. floor beam-Ea. side
8	8	@ 10' c'trs. each side
9	5	2 N. Side @ 3' 1/2' from E. of Pier 6
10	5	3 S. " @ 3' 1/2' " " "
11	None	

NOTE:- Quantities of Hollow Tile and Wire Fabric shown in Bill of Material on Sheet No.5 are net quantities measured in place, and no additional compensation will be allowed for laps or waste. The cost of the asphalt coating shall be included in the price of tile.

No. 46-A
PLANK ROAD BRIDGE
OVER C.B. & Q. R.R. - C. & N. V. R.R.
AND KICKAPOO CREEK
PEORIA COUNTY

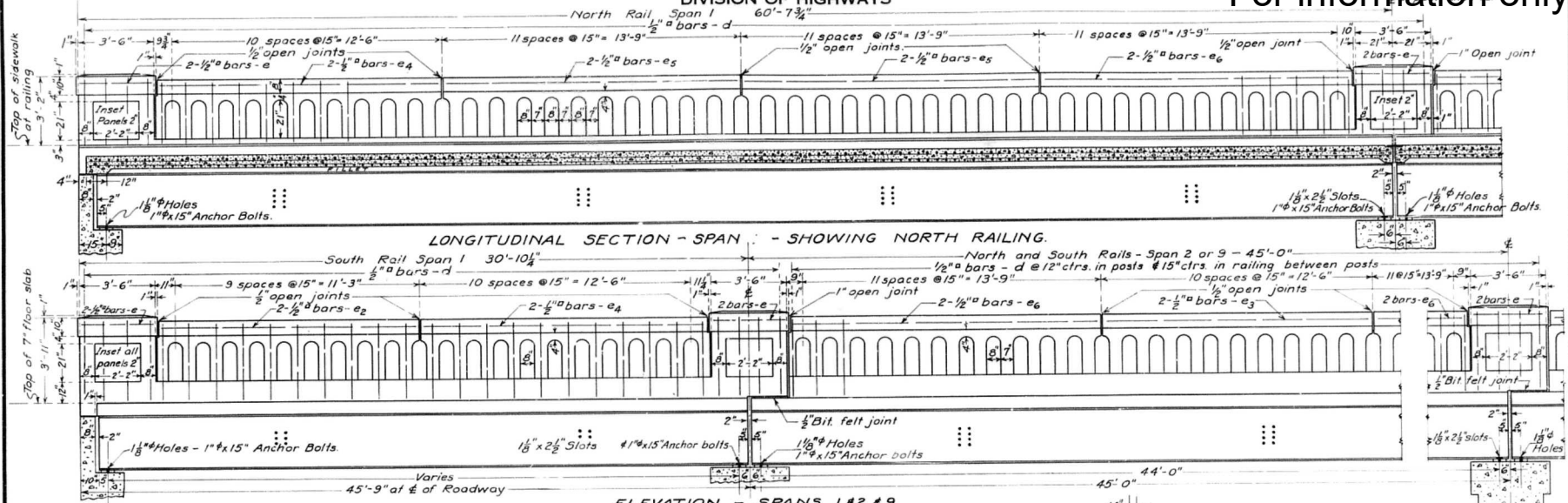
S.B.I. RT8-SEC. 10R-E N.R.H.

STATION 245+57

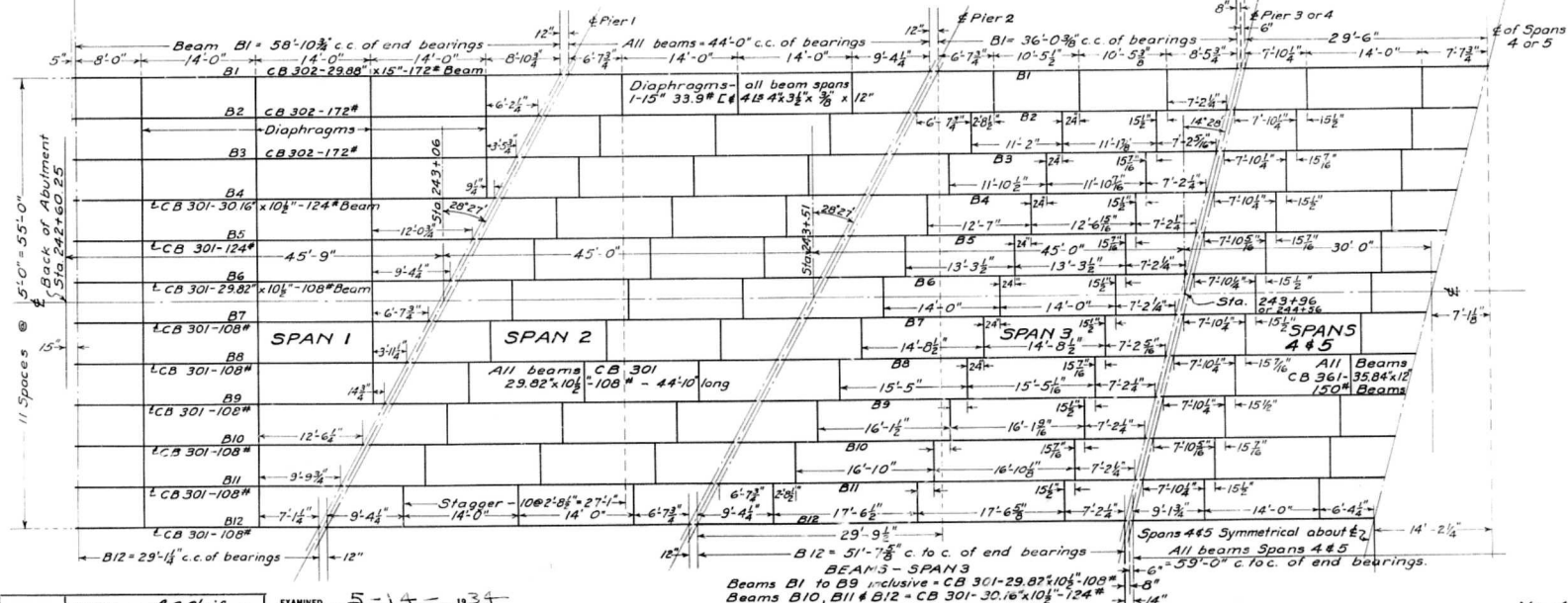
STANDARD	COMPUTED	<i>L.A. Wilson</i>	EXAMINED <i>5-14-1934</i> <i>H.T. Evers</i> BRIDGE ENGINEER PASSED <i>W. J. Brown</i> CHIEF OF DESIGN APPROVED <i>E. Dickman</i> DISTRICT ENGINEER
	CHECKED	<i>Geo. E. Morgan</i>	
	DRAWN	<i>L.A. Wilson</i>	
	CHECKED	<i>C.E. Mc</i>	
SPECIAL	ASSEMBLED	_____	
	CHECKED	_____	

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only



ELEVATION - SPANS 1 & 2



STANDARD	COMPUTED	L.A. Wilson
	CHECKED	Ray E. Morgan
	DRAWN	L.A. Wilson
	CHECKED	E.E.M.
SPECIAL	ASSEMBLED	
	CHECKED	

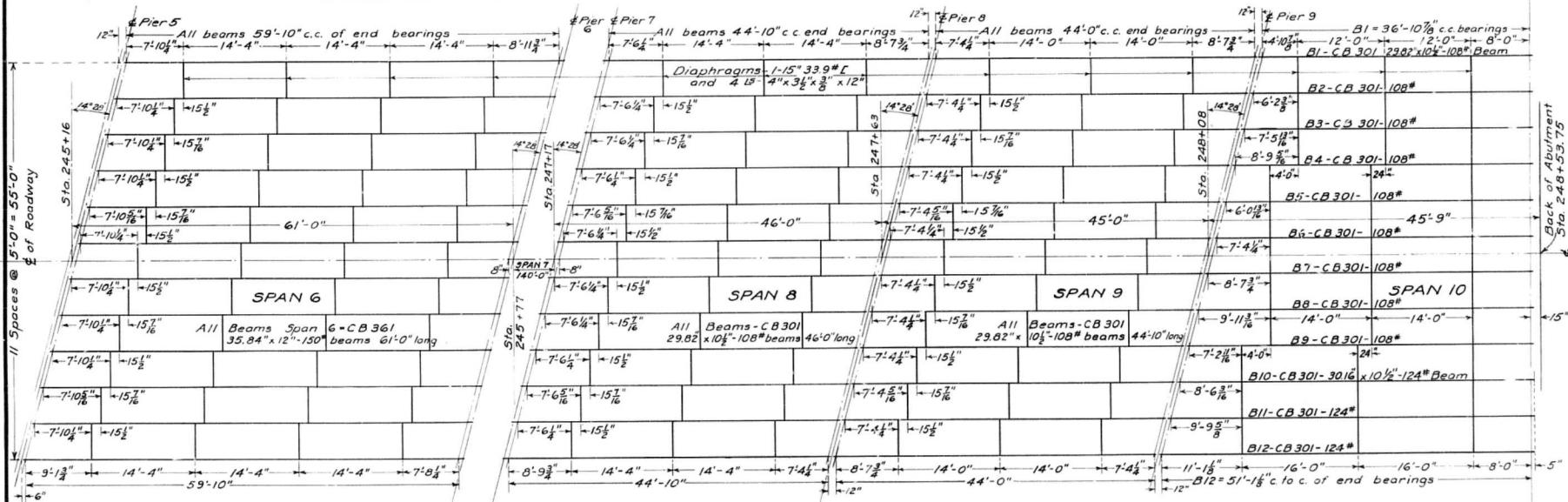
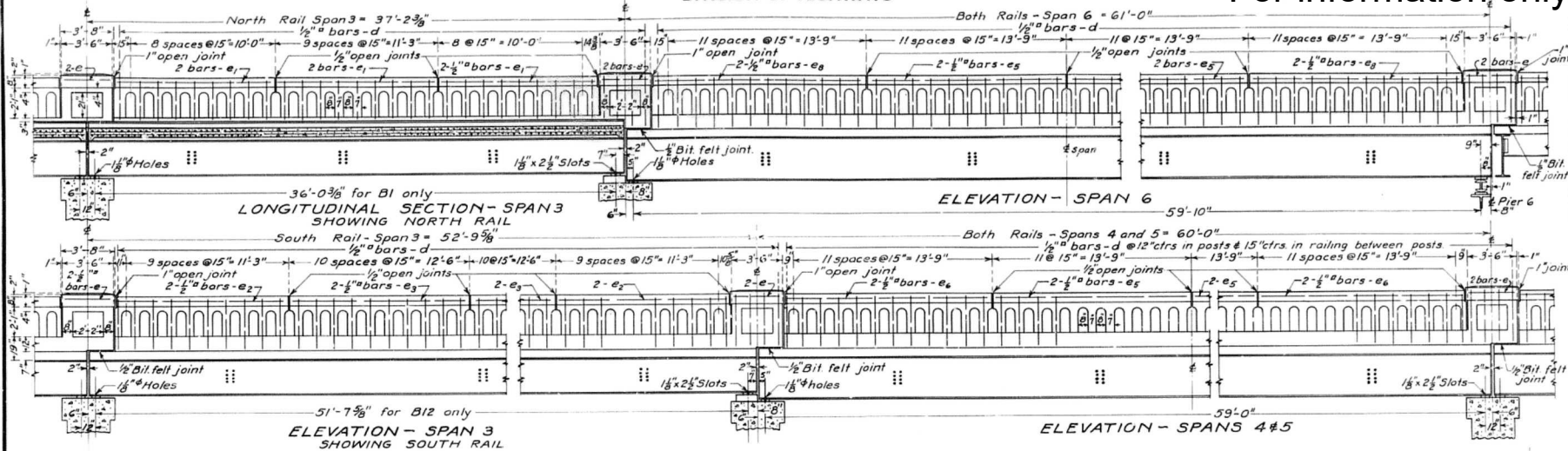
EXAMINED	5-14-1934
PASSED	[Signature]
APPROVED	[Signature]

PLAN - UNDER FLOOR
SPANS 1 TO 5 INCLUSIVE
SHOWING BEAMS AND SPACING OF DIAPHRAGMS

No. 46-A
PLANK ROAD BRIDGE
OVER C.B. & Q. R.R. - C. & N.W. R.R.
AND KICKAPOO CREEK
PEORIA COUNTY
S.B.I. R.T.B - SEC. 10 R. N.H.
STATION 245+57

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only



STANDARD	COMPUTED	L.A. Wilson
CHECKED	Checked	L.A. Wilson
DRAWN	Drawn	L.A. Wilson
CHECKED	Checked	L.A. Wilson
SPECIAL	ASSEMBLED	
	CHECKED	

EXAMINED
PASSED
APPROVED

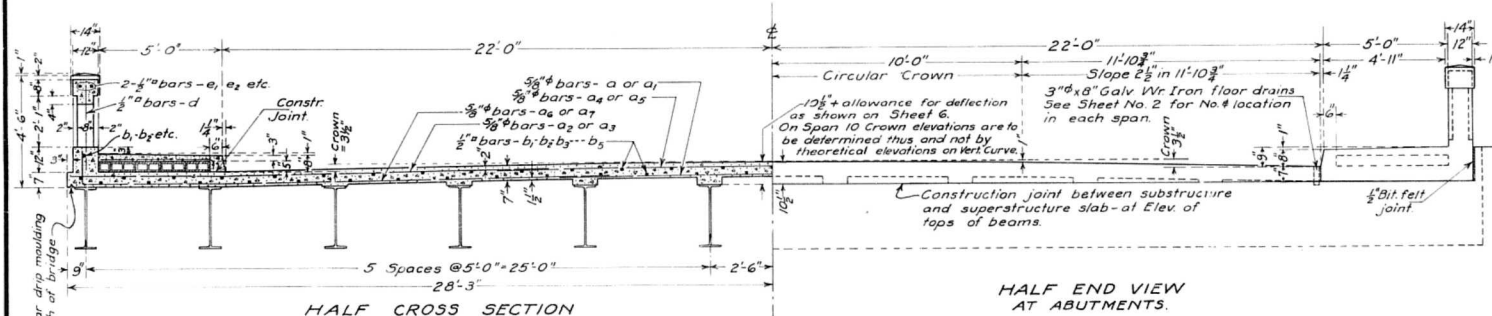
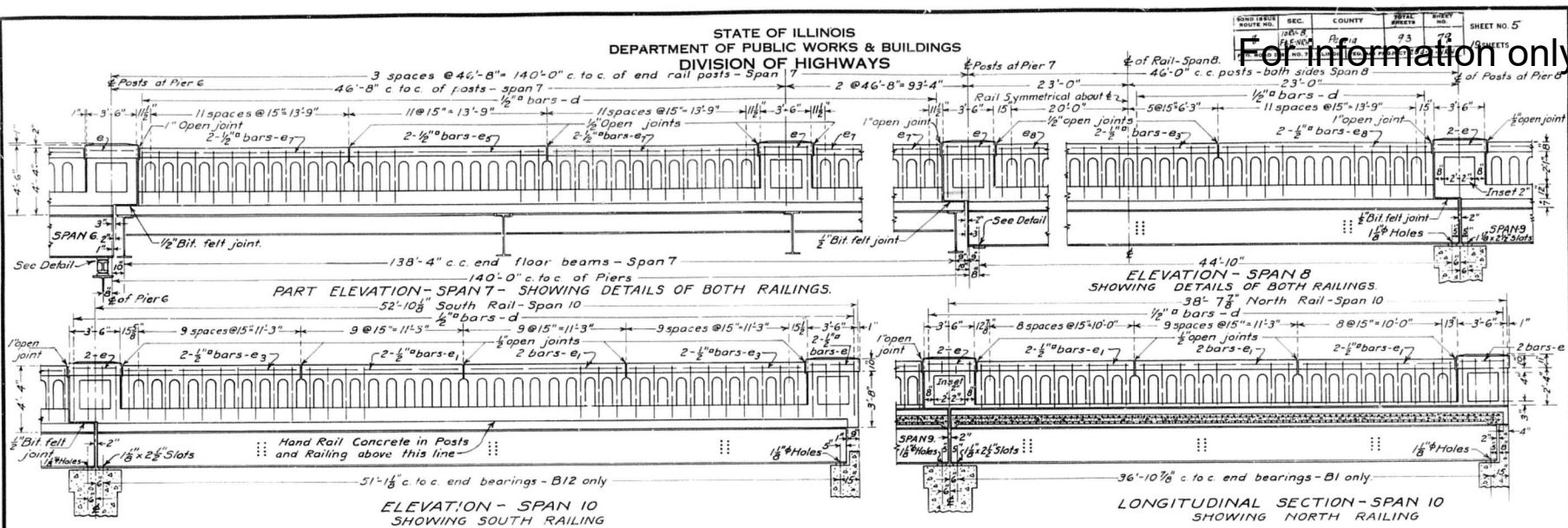
BILL OF MATERIAL - BEAM SPANS		
MATERIAL INCLUDED IN BOTH SECS. 10R-F & 10R-E.		
ITEMS	UNITS	QUANTITIES
Structural Steel	Lbs.	776,870

See Sheets Nos. 5 & 6 for Bill of Material included in Section 10R-E only.
Expansion Guards, Bearing Plates and Blast Plates included in Secs. F & E are not included in Bill of Material above.
See Sheets 15 & 16 for details & quantities for these items.

No. 46-A
PLANK ROAD BRIDGE
OVER C.B. & Q. R.R. - C. & N.W. R.R.
AND KICKAPOO CREEK
PEORIA COUNTY
S.B.I. RT. 8 - SEC. 10R-F & E - N.R.H.
STATION 245+57.

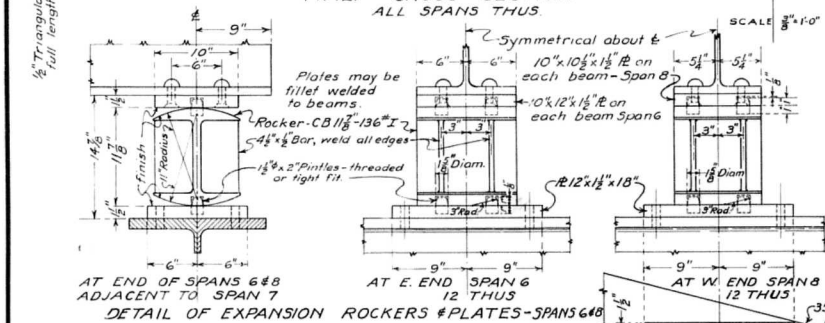
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only

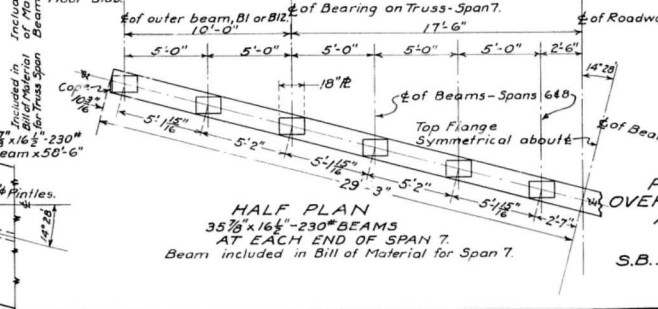


BILL OF MATERIAL
SUPERSTRUCTURE - SEC. 10R-E
10 SPANS

Bars	No	Size	Length
a	326	3/8" φ	17'-0"
a ₁	1114	"	17'-3"
a ₂	652	"	19'-3"
a ₃	2228	"	19'-9"
a ₄	326	"	22'-9"
a ₅	1114	"	22'-9"
a ₆	652	"	23'-0"
a ₇	2228	"	23'-0"
b	88	1/2" φ	3'-6"
b ₁	354	"	21'-0"
b ₂	177	"	21'-6"
b ₃	236	"	23'-3"
b ₄	472	"	23'-9"
b ₅	354	"	24'-9"



NOTES -
Class X Concrete shall be used throughout.
Reinforcing Steel shall be wired securely in place before concrete is poured.
Holes 3/8" - Rivets 3/8" except as otherwise noted.
Paint - 1 Shop coat of Blue Lead - to be furnished by the Contractor. Omit paint from tops of beam flanges that are to be in contact with floor slab.
DETAIL OF 3/8" BARS - a₆ & a₇



Class X Concrete - Cu Yds 8374
Hand Rail Concrete - Cu Yds 113.9
Reinforcement Bars - Lbs 224110
5" Hollow Floor or Wall Tile - Sq Ft 5234
Welded Wire Fabric - Sq Ft 6230
Floor Drains - Each 72

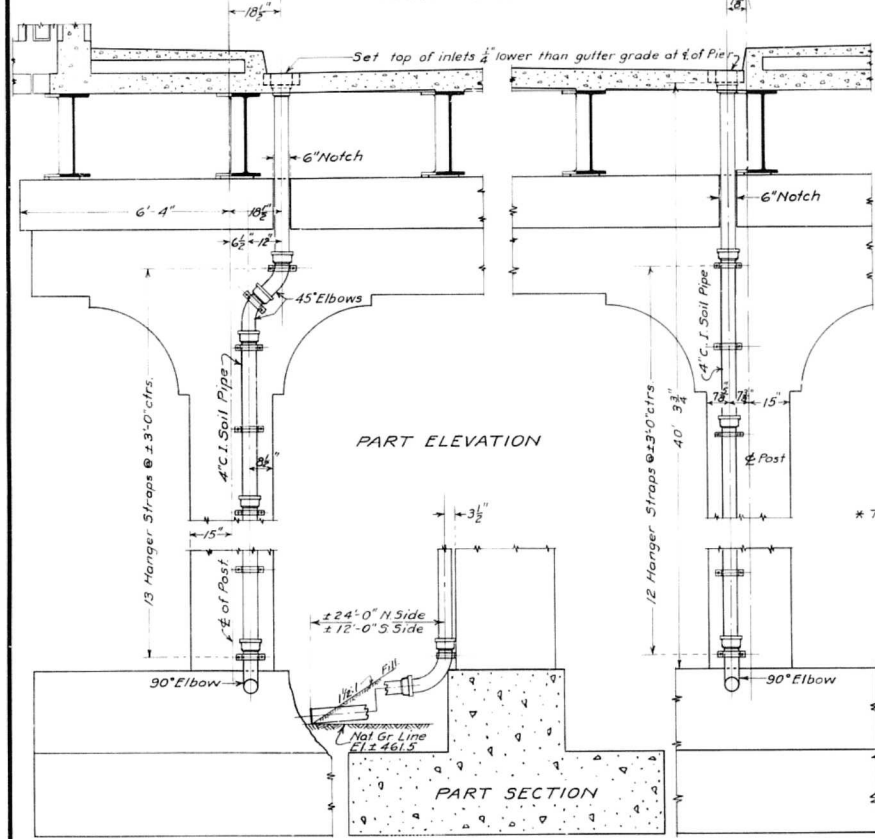
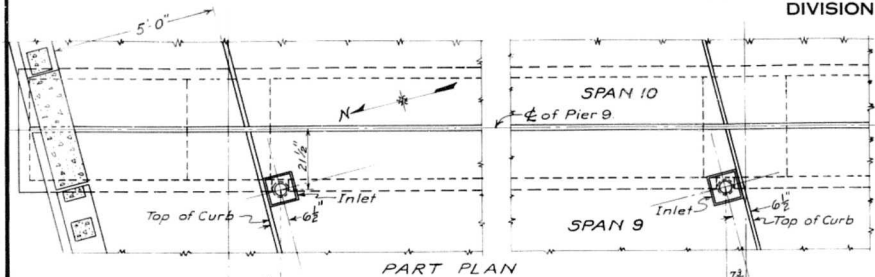
STANDARD
COMPUTED - L.A. Wilson
CHECKED - N.H. Jackson
DRAWN - L.A. Wilson
CHECKED - N.H. Jackson
SPECIAL
ASSEMBLED -
CHECKED -

EXAMINED - 5-17-1931
APPROVED -
ENGINEER OF DESIGN
CHIEF HIGHWAY ENGINEER

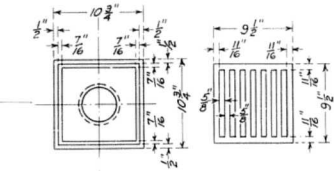
No. 46-A
PLANK ROAD BRIDGE
OVER C.B. & Q. RR. - C. & N.W. RR.
AND KICKAPOO CREEK
PEORIA COUNTY
S.B.I. RT. 8 - SEC. 10R-E
STATION 245+57

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

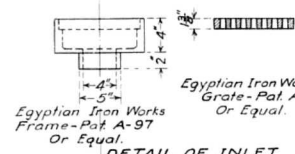
For information only



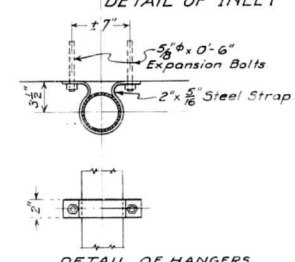
DETAIL
SHOWING DRAINS ON THE WEST SIDE OF PIER 9.



SECTION
AT BEAM OR STRINGER
ALL SPANS
SHOWING FILLET



DETAIL OF INLET



DETAIL OF HANGERS

BILL OF MATERIAL - INLETS & DRAIN PIPE
INCLUDED IN SEC. 10R-E ONLY

ITEMS	UNIT	QUANTITY
Cast Iron Inlets	Each	2
Cast Iron Soil Pipe	Lin Ft	118
* 45° Elbows		2
* 90° "		2
* Hanger Straps		25
* Expansion Bolts		50

* The cost of items marked thus shall be included in the price paid for C.I. Soil Pipe.

Notes:-
Cast Iron Soil Pipe shall conform to the requirements of Art. 68.4, Paragraphs (a) (ole) inclusive, and (h) to (j) Incl. of the Standard Specifications of the Ill. Div. of Highways. Frames & Grates shall be Gray Cast Iron conforming to Art. 72.4 of the said Specifications. The unit prices to be paid for Cast Iron Inlets and 4" Cast Iron Soil Pipe shall include the cost of furnishing and installing these items complete with fittings and hangers as shown.

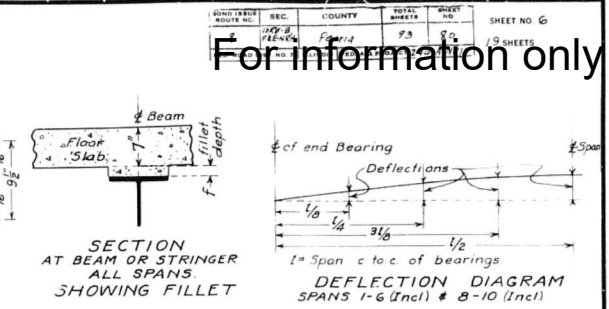


TABLE -
FILLET DEPTHS - F' FOR CROWN ONLY

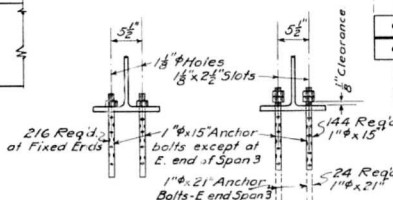
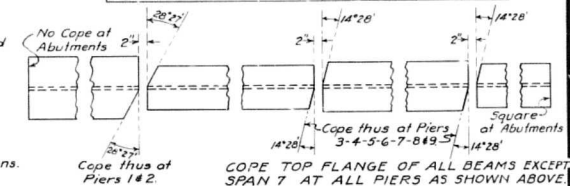
BEAM	B1	B2	B3	B4	B5	B6
	or B12	or B11	or B10	or B9	or B8	or B7
F'	0"	0"	1"	2"	2 1/2"	3"

DEFLECTION TABLE - SPANS 1-3 & 10

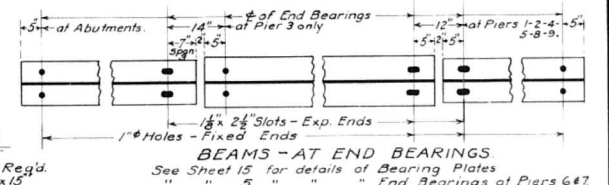
Beam	at 1/8	at 1/4	at 3/8	at 1/2	at 5/8	at 3/4	at 7/8	at 1
B1	5/16	1/8	1/16	3/16	1/8	1/16	1/8	3/16
B2	1/4	1/8	1/16	3/8	1/8	1/16	1/8	3/8
B3	3/16	1/8	1/16	1/2	1/8	1/16	1/8	1/4
B4	1/4	1/8	1/16	1/2	1/8	1/16	1/8	1/4
B5	3/16	1/8	1/16	1/2	1/8	1/16	1/8	1/4
B6	"	"	"	"	"	"	"	"
B7	1/4	1/8	1/16	1/2	1/8	1/16	1/8	1/4
B8	"	"	"	"	"	"	"	"
B9	1/4	1/8	1/16	1/2	1/8	1/16	1/8	1/4
B10	"	"	"	"	"	"	"	"
B11	"	"	"	"	"	"	"	"
B12	0"	"	"	"	"	"	"	"

DEFLECTION TABLE - SPANS 2-4 & 5-6-8-9

	at 1/8	at 1/4	at 3/8	at 1/2
Spans 2-8-9 - All Beams	1/8"	1/4"	3/8"	1/2"
Spans 4 & 5 - All Beams	1/4"	7/16"	5/8"	3/4"
Span 6 - All Beams	1/4"	1/2"	3/4"	1/6"



DETAIL - ANCHOR BOLTS



See Sheet 15 for details of Bearing Plates

DETAILS
No. 46A
PLANK ROAD BRIDGE
OVER C.B. & Q.R.R. - C. & N.W.R.R.
AND KICKAPOO CREEK
PEORIA COUNTY

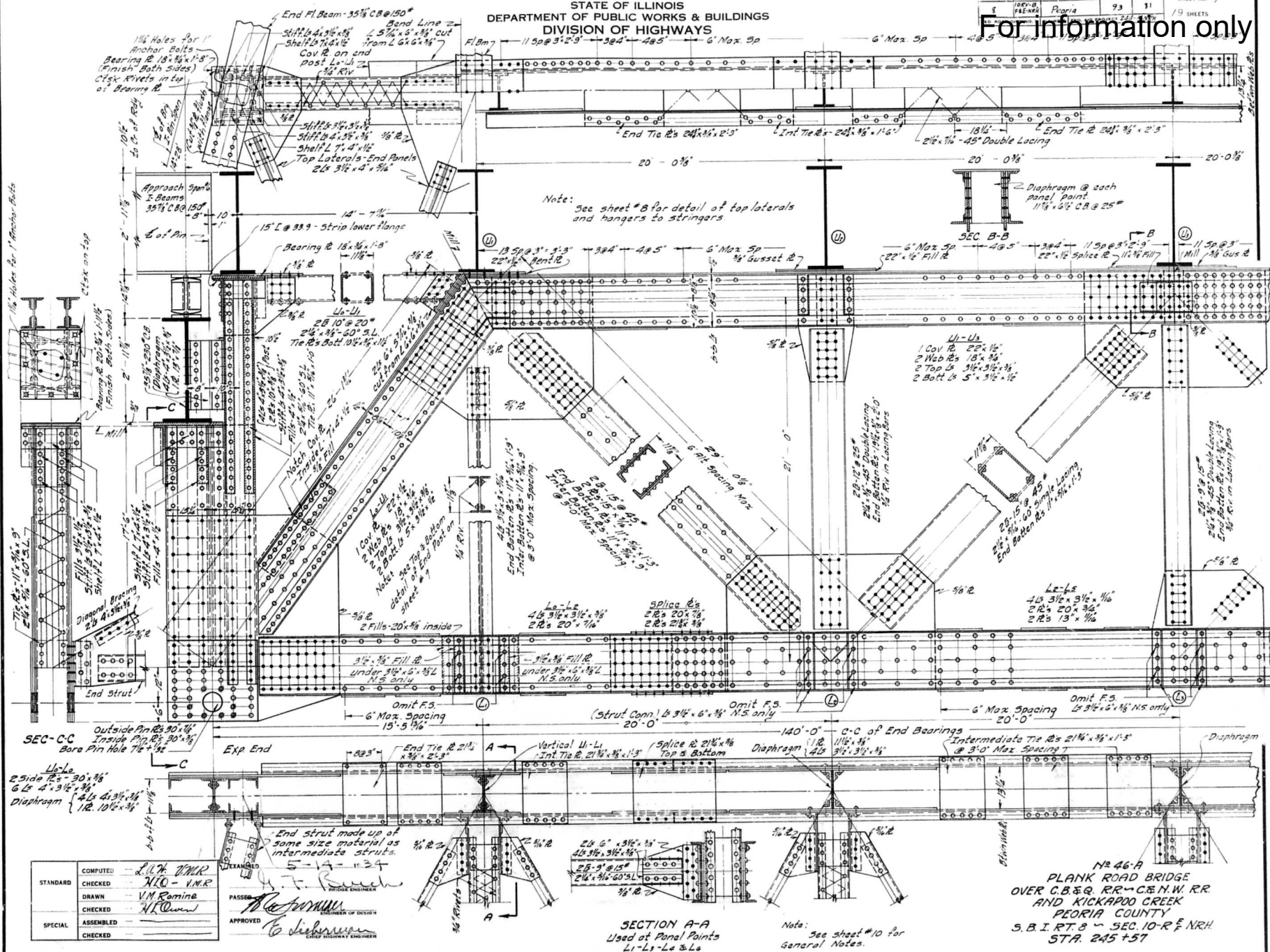
S.B.I. RT. 8 - SEC. 10R - E N.R.H.

STATION 245+57

COMPUTED	L.A. Wilson
CHECKED	N.N. Jackson
DRAWN	L.A. Wilson
CHECKED	N.N. Jackson
ASSEMBLED	
CHECKED	

EXAMINED	5-17-1937
PASSED	
APPROVED	

For information only

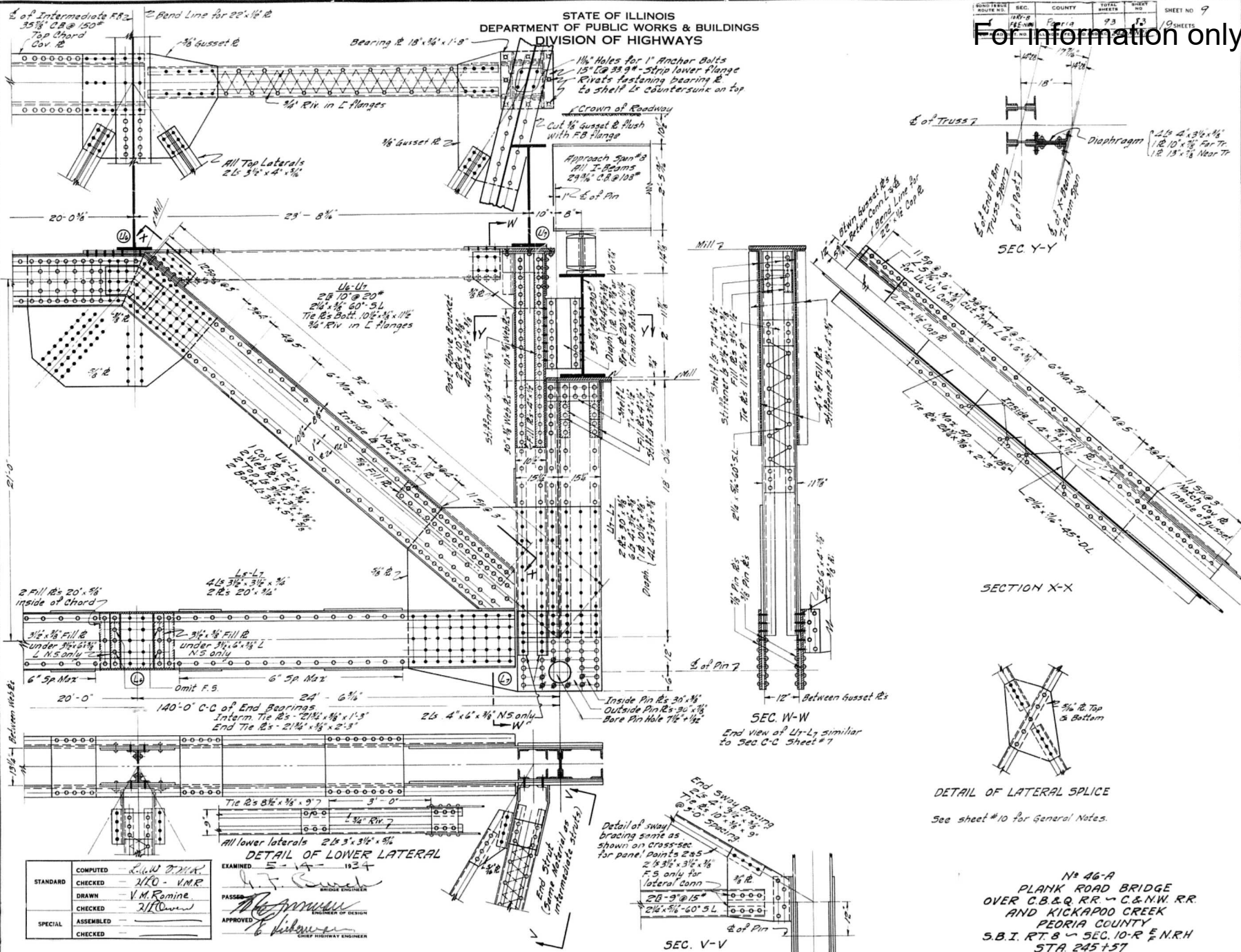


BOND ISSUE ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
8	289-B, 1669-D	DODIA	93	82

SHEET NO. 8
19 SHEETS

For information only



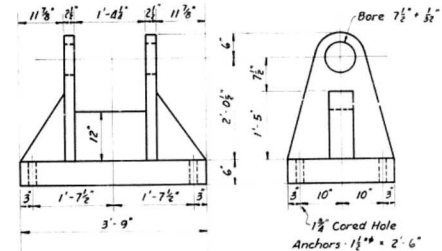
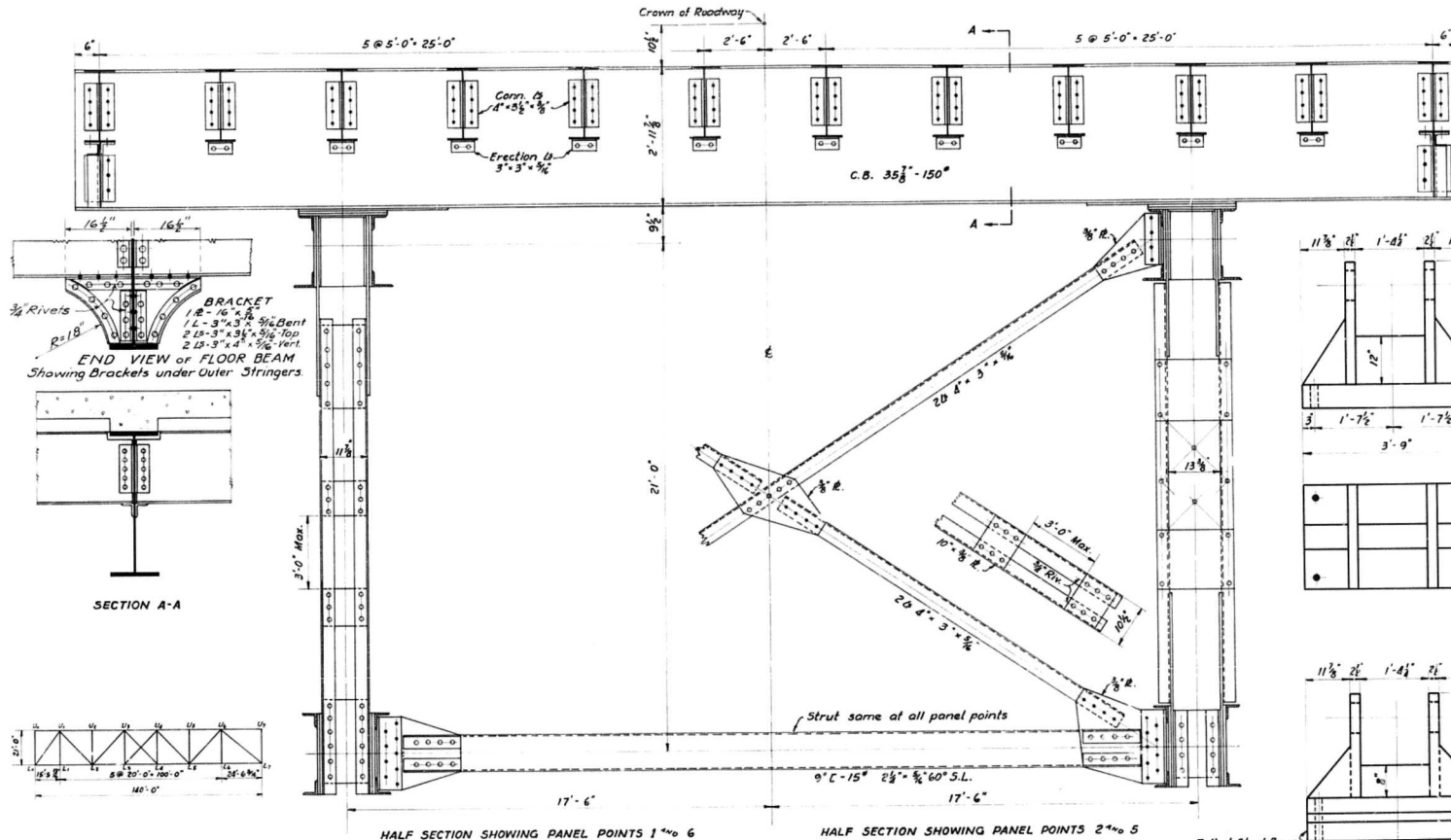


STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

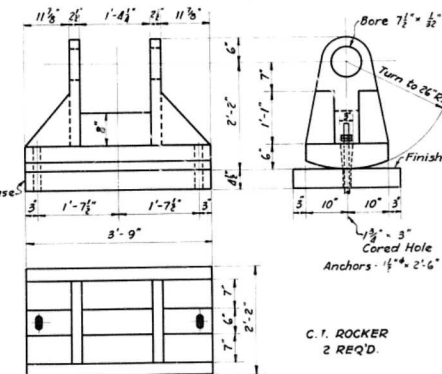
CONTRACT NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
10R-8	14E-N	Peoria	73	74

SHEET NO. 10
19 SHEETS

For information only



C.I. BOLSTER
2 REQD



C.I. ROCKER
2 REQD

General Notes

Rivets $\frac{3}{8}$ " unless noted.
Holes punched $\frac{1}{8}$ " except as noted and except as follows :-
All main truss members both shop and field shall be subpunched $\frac{1}{8}$ " and reamed to $\frac{1}{4}$ " with members assembled.
Each truss shall be assembled complete for reaming.
Connections shall be match-marked while assembled.
Floor beams shall be reamed to a hardened steel template one inch thick.
Holes may be punched full size for building up members, laterals, swaybracing and stringers.
Inspection by Illinois Division of Highways before painting.
Paint shall be furnished by Contractor; one coat of blue lead paint in shop and two coats of white lead paint in field. Omit all paint from tops of stringers.
Concrete floor shall be finished according to Article 45.3 (b) of Specifications.

BILL OF MATERIAL
SPAN 7 - SEC. E&F ONLY

Structural Steel	Lbs. — 465530
Iron Castings	Lbs. — 11900

For Bill of Material-Sec. E only see Sheet 5

STANDARD	COMPUTED	— L.W. D.M.R.
	CHECKED	— M.C.
	DRAWN	— M.C.
	CHECKED	— M.C.
SPECIAL	ASSEMBLED	— M.C.
	CHECKED	— M.C.

EXAMINED	— 5-14-1934
PASSED	— M.C.
APPROVED	— M.C.

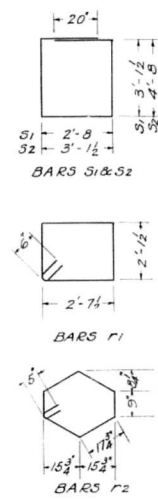
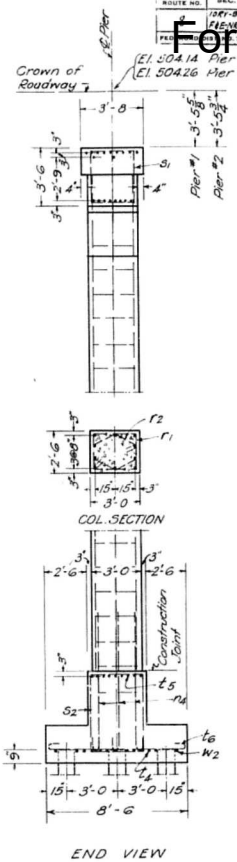
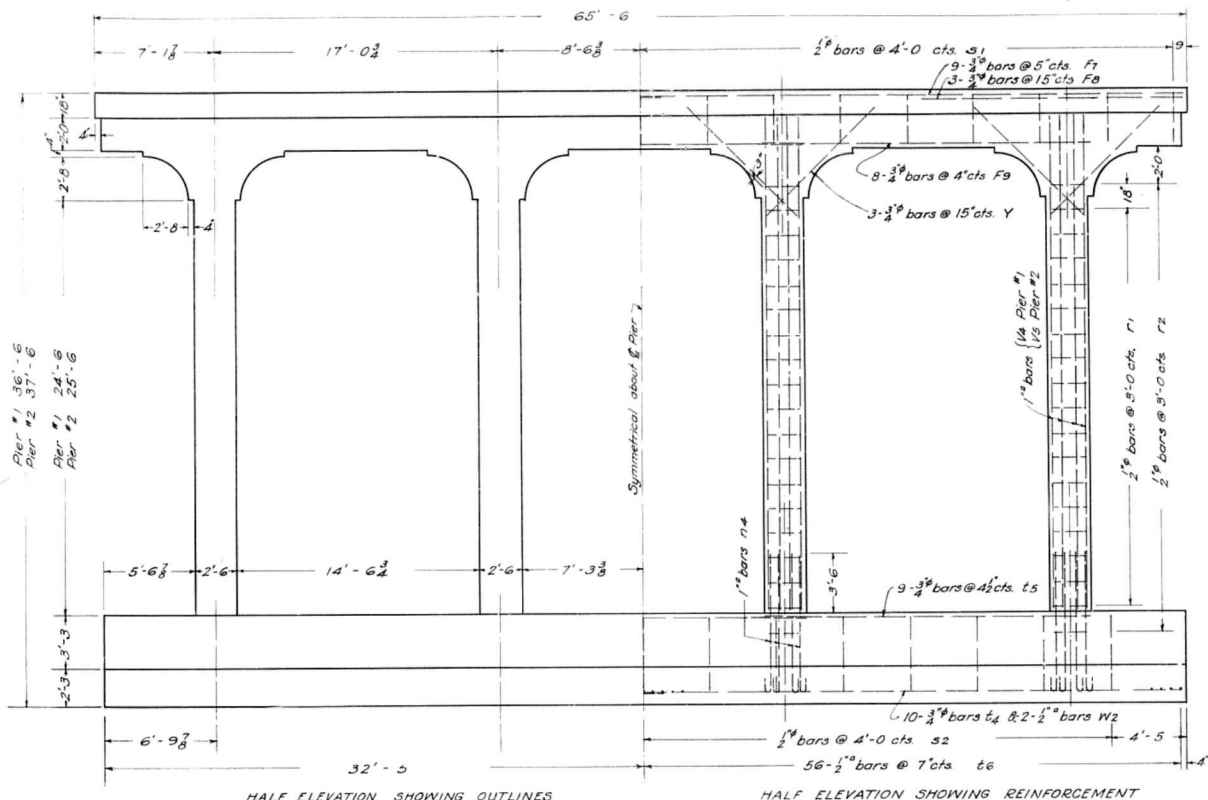
Revised 6-21-34 - E.O.H. to provide brackets under outside stringers.

Nº 46A
PLANK ROAD BRIDGE
OVER C.B. & Q. R.R. - C. & N.W. R.R.
AND KICKAPOO CREEK
PEORIA COUNTY
S.B.I. ROUTE-8 SEC. 10R-E&F-N.R.H.
STATION 245 + 57

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only

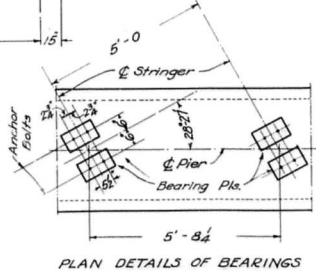
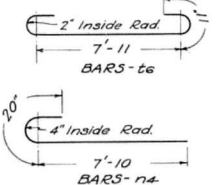
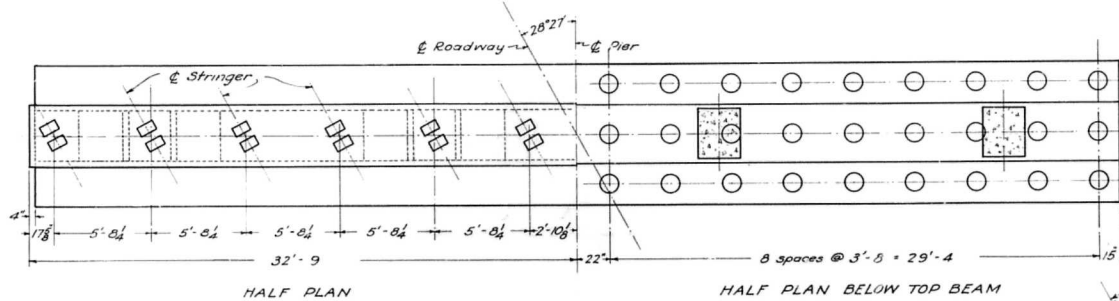
ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
100-4	100-4	Peoria	93	19



BILL OF MATERIAL - PIERS #1 & #2

Bar	No.	Size	Length
F7	36	3/4"	33'-9
F8	12	"	15'-6
F9	48	"	20'-0
Y	48	"	9'-6
V4	40	1"	29'-0
V5	40	"	30'-0
r1	72	1/2"	10'-6
r2	80	"	8'-3
t4	40	3/8"	33'-6
t6	222	1/2"	9'-6
n4	80	1"	9'-6
S1	34	1/2"	13'-3
S2	30	"	17'-3
W2	8	1/2"	33'-0
t5	54	3/4"	20'-0

Class X Concrete Cu Yds 2723
 Reinforcing Steel Lbs 22080
 Cressed Piles up to 20' Lin.Ft 2160



○ Indicates 15 Ton Cressed Piles 108 Req'd.

Class X concrete to be used throughout.
 All reinforcing steel to be wired securely in place before concrete is poured.

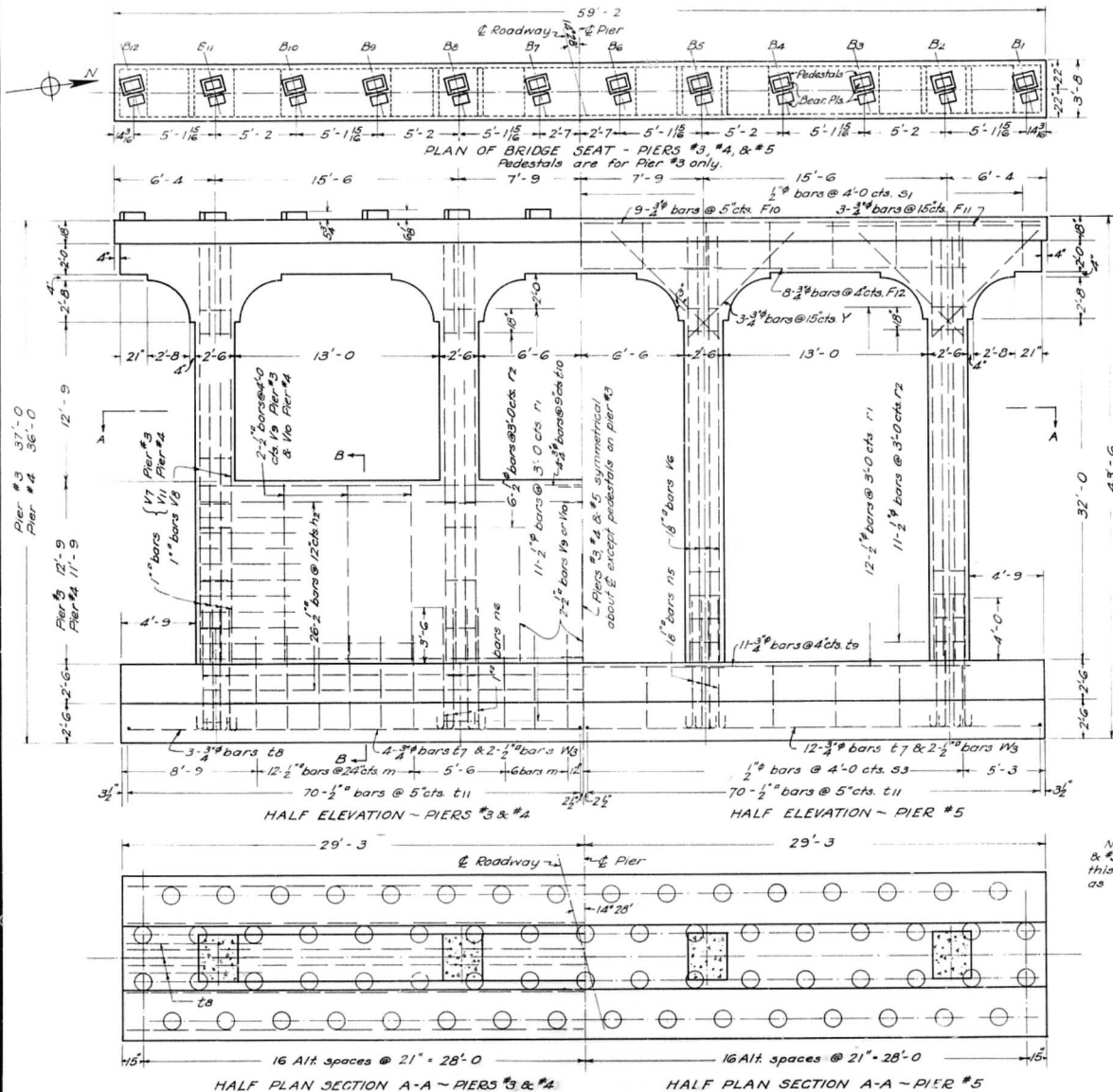
COMPUTED	— L.A. Wilson
CHECKED	— H.H. Jackson
DRAWN	— H.H. Jackson
CHECKED	— H.H. Jackson
ASSEMBLED	—
CHECKED	—

EXAMINED 5-17-1934
 PASSED [Signature]
 APPROVED [Signature]
 CHIEF HIGHWAY ENGINEER

PIERS NO.1 & NO.2
 No. 46-A
 PLANK ROAD BRIDGE
 OVER C.B. & Q.R.R. - C. & N.W.R.R.
 AND KICKAPOO CREEK
 PEORIA COUNTY
 S.B.I. RT. NO. 8 - SEC. 10R - B - NRH
 STATION 245+57

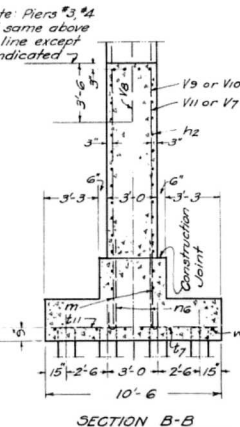
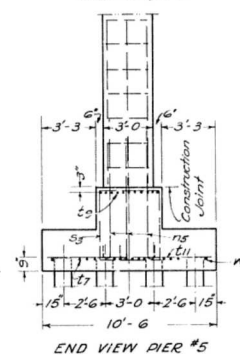
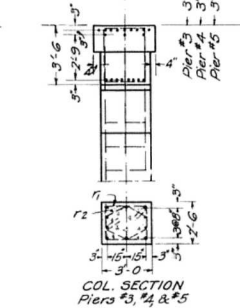
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only



Note: Pedestals B1 to B9 inc. to be 6' high. Pedestals B10 B11 & B12 to be 5' high. Pedestals to be built monolithic with coping.

Crown of Roadway



BILL OF MATERIAL - PIERS #3, #4 & #5

Bar	No.	Size	Length
F10	54	1/2"	30'-6"
F11	18	"	13'-9"
F12	72	"	18'-0"
Y	72	"	9'-6"
V11	32	1"	29'-6"
V6	40	1 1/8"	37'-3"
V7	37	1"	30'-3"
V8	16	"	21'-0"
V9	18	1/2"	12'-6"
V10	18	"	11'-6"
r1	136	1/2"	10'-6"
r2	92	"	8'-3"
s1	45	"	13'-3"
s3	15	"	17'-0"
h2	104	1/2"	25'-0"
t10	16	3/4"	25'-6"
t7	20	"	30'-3"
t8	12	"	8'-6"
t11	420	1/2"	10'-0"
m	72	"	6'-0"
n5	40	1 1/8"	10'-0"
n6	64	1"	9'-0"
w3	12	1/2"	29'-9"
t9	33	3/4"	18'-0"

Class X Concrete Cyls. 5280
Reinforcing Steel Lbs. 35880
Creosoted Piles up to 20' Lin.Ft. 3960

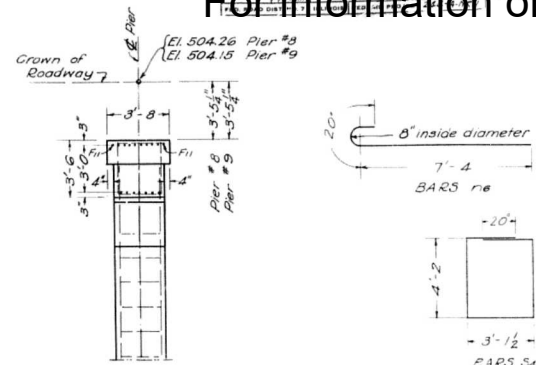
*Bar n6 detailed on sheet #

PIERS #3, #4, & #5
No. 46A
PLANK ROAD BRIDGE
OVER C&Q.R.R.-C&N.W.R.R.
AND KICKAPOO CREEK
PEORIA COUNTY
S.B.I. RT. No. 8 - SEC. 10 B - N.R.H.
STATION 245+57

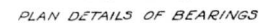
STANDARD	COMPUTED	EXAMINED
CHECKED	L.A. Wilson	S-14-1937
DRAWN	H.A. Jackson	
CHECKED	N.T. Owen	
SPECIAL	ASSEMBLED	
CHECKED		

○ Indicates 15 Ton Piles - 198 Req'd.
Class X Concrete to be used throughout.
All reinforcing steel to be wired securely in place before concrete is poured.

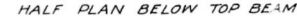
For information only



Class X Concrete	CuYds. 274.0
Reinforcing Steel	Lbs. 23,050
Creasoted Piles up to 20'	Lin.Ft. 2,160



HALF ELEVATION SHOWING REINFORCEMENT



○ Indicates 15 Ton Creasoted Piles ~ 108 Req'd.

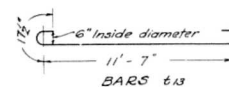
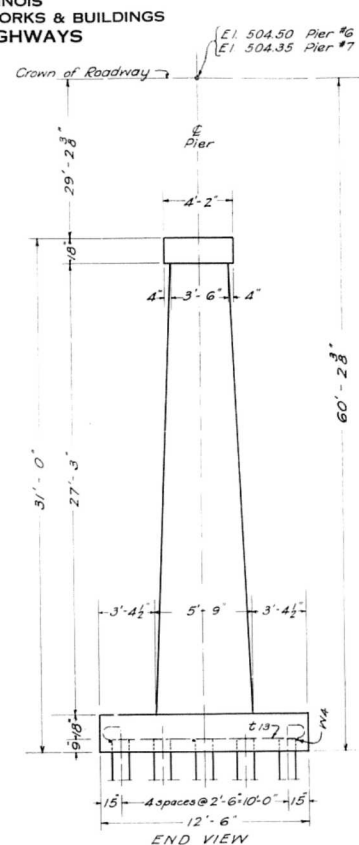
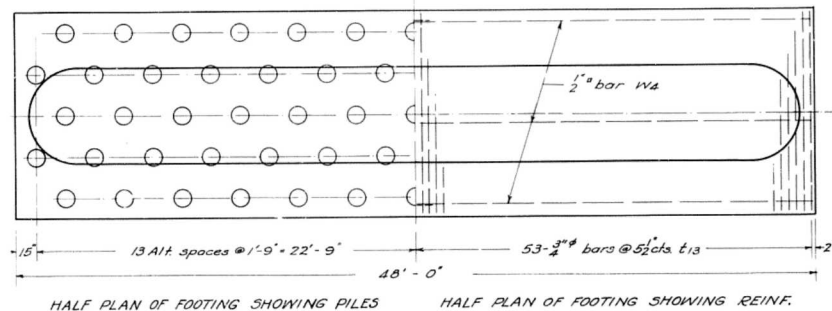
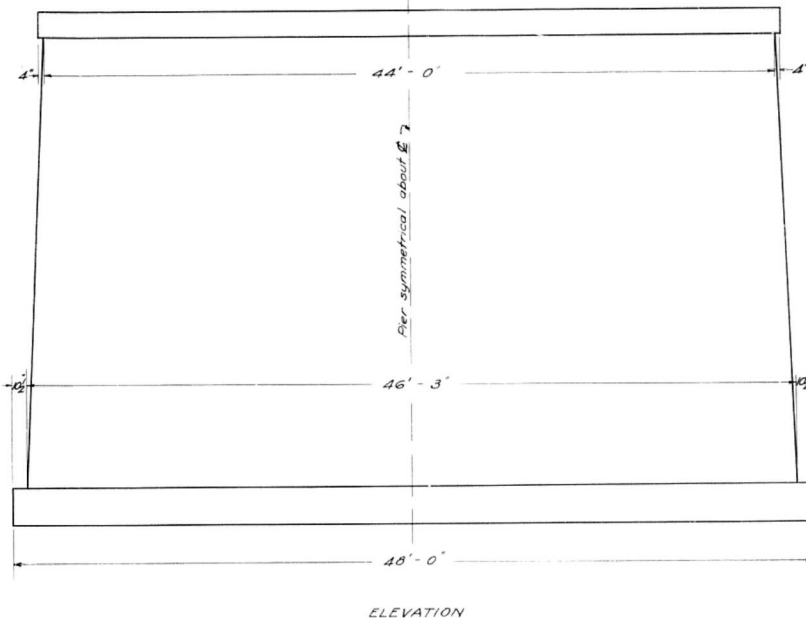
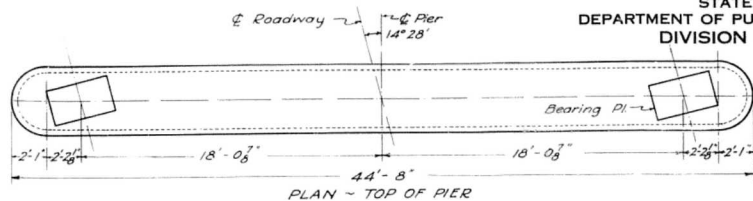
Class X concrete to be used throughout.
All reinforcing steel to be wired securely
in place before concrete is poured.

PIERS #8 & #9
No 46 A
PLANK ROAD BRIDGE
OVER C.B. & Q.R.R. - C. & N.W.R.R.
AND KICKAPOO CREEK
PEORIA COUNTY
SBI RT No. 8 - SEC. 10R-B-NRH
STATION 245+57.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only

ROUTE NO. 26-B SEC. 23-12 CO. 10-11 TWP. 36-N R. 8-E SHEET NO. 14



BILL OF MATERIAL - PIERS #6 & #7			
Bar	No.	Size	Length
t13	210	3/4"	14'-6"
w4	12	5/8"	24'-9"

Class A concrete CuYds 533.0
Reinforcing Steel Lbs. 483.3
Untreated Piles up to 20' Lin Ft 2,680

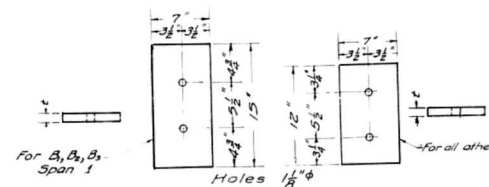
STANDARD	COMPUTED - N.H. Jackson
	CHECKED - M. L. R. R.
	DRAWN - N.H. Jackson
	CHECKED - J. H. R.
SPECIAL	ASSEMBLED -
	CHECKED -

EXAMINED - 5-14-1931	BRIDGE ENGINEER
PASSED -	ENGINEER OF DESIGN
APPROVED -	CITY ENGINEER

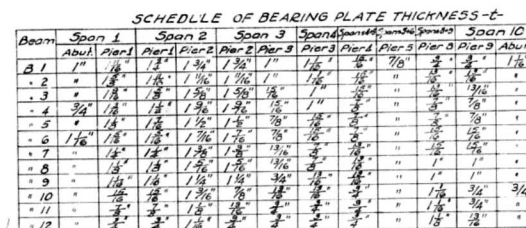
○ Indicates 20 Ton Untreated Piles - 134 Req'd
Class A concrete to be used throughout.
All reinforcing bars to be wired securely
in place before concrete is poured.

PIERS #6 & #7
No 46A
PLANK ROAD BRIDGE
OVER G.B. & Q.R.R. ~ G. & N.W.R.R.
AND KICKAPOO CREEK
ROCK COUNTY
S.B.I. RT. No 8 ~ SEC. 10-R-NR
STATION 245+57

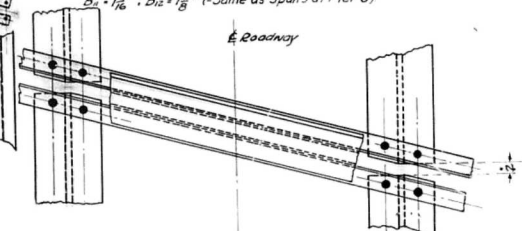
For information only



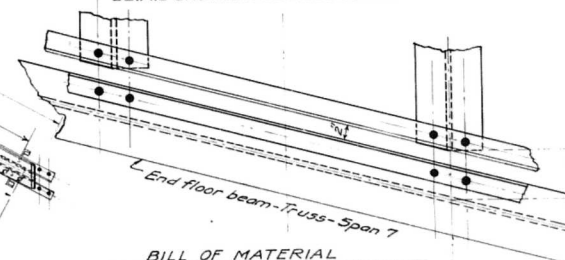
BEARING PLATES
Total No. Req'd - 6 @ 7"x15" and 186 @ 7"x12"



Note. Span 9: Pier 9 same as Span 10: Pier 9, except $B_0 = \frac{1}{16}$
 $B_1 = 1\frac{1}{2}$; $B_2 = 1\frac{1}{2}$ (= Same as Span 9 at Pier 8)



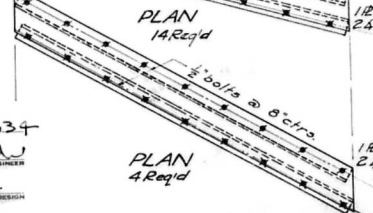
DETAIL SHOWING CONNECTION TO STRINGERS



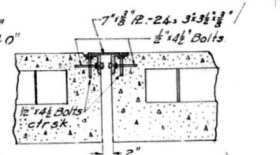
BILL OF MATERIAL	
Expansion Guards - Lbs.	34640
Bearing Plates - Lbs.	4690
Total - Structural Steel - Lbs.	39330



PLAN



PLAN
4 Req'd

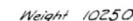


SECTION THRU SIDEWALK

EXAMINED 5-14-1934
H. F. Burch
BRIDGE ENGINEER
PASSED [Signature]
ENGINEER OF DESIGN
APPROVED [Signature]
PLATE ENGINEER

[illegible]

For information only



Bolts, nuts & fills shall be paid for at the Contract unit price for Blast Plates.

EXAMINED 5-14-1934
L. F. Kuehl
BRIDGE ENGINEER
PASSED [Signature]
ENGINEER OF DESIGN
APPROVED E. Lieberman
SUPERVISOR OF ENGINEERS

Revised 6-7-34-LA 76 Blast Plates etc changed to wrought iron

SECTION - AA -

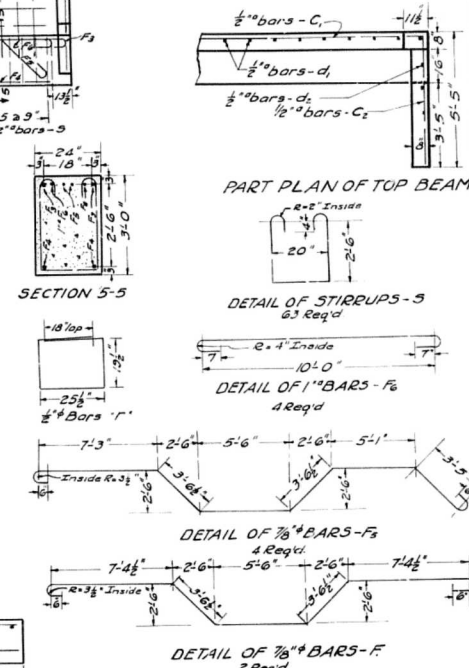
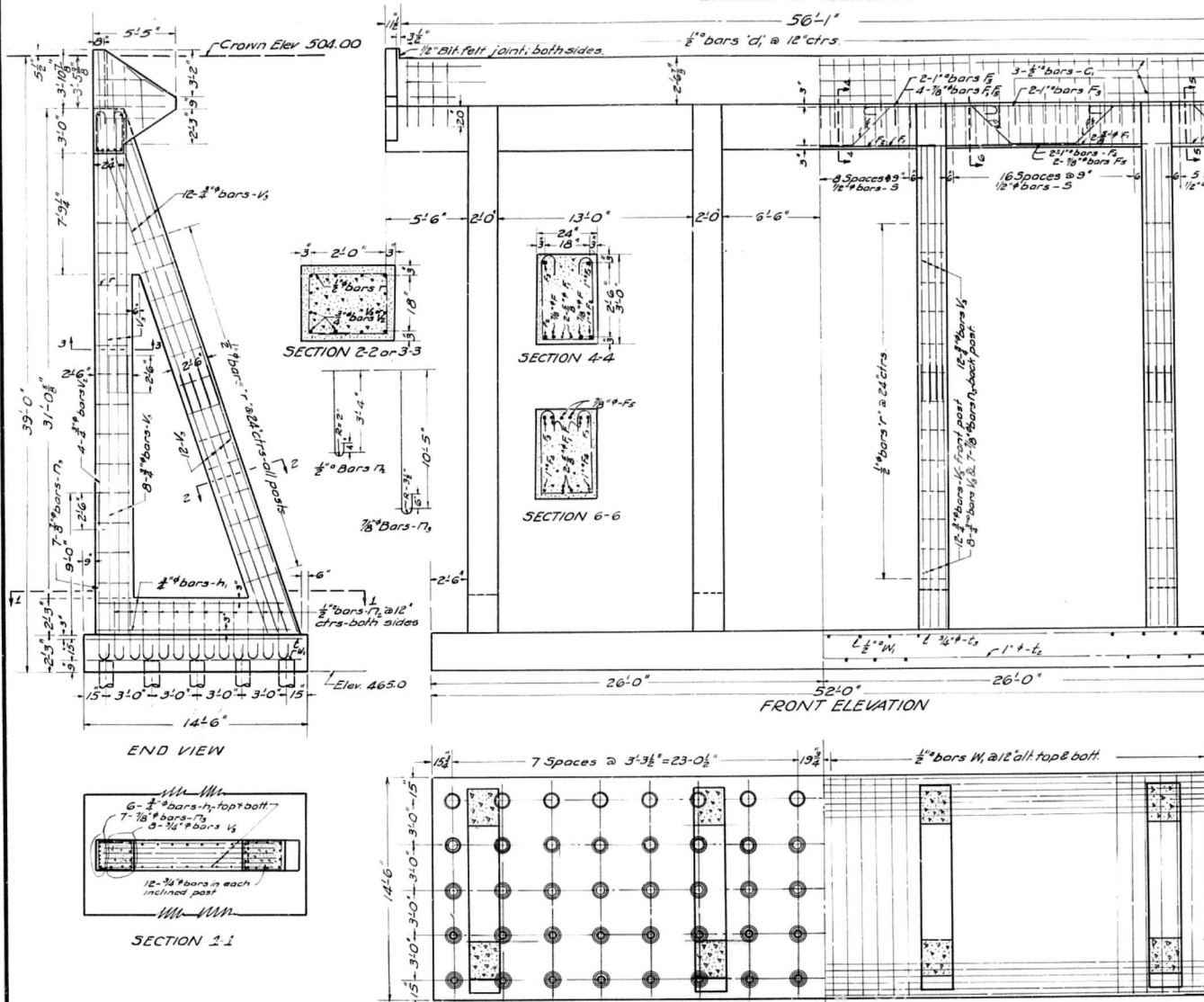
NO. 46-A
BLAST PLATES
S.B.I. RTE. 8-SEC. 10R-V-F-E-NRM.
PEORIA COUNTY
STA. 245+57

3/4" 22 Spike & Washer in Power Pole, right of
Sta 244+70, Elev. 473.69

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ENGINEER
ROUTE NO.
REV.
COUNTY
SHEET NO.
SHEET 17
19 SHEETS

For information only

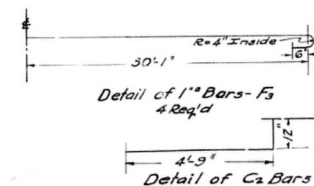


BILL OF MATERIAL

Bars	No.	Size	Length
C ₁	6	1/2"	29'-6"
C ₂	8	"	5'-9"
d ₁	56	"	4'-0"
d ₂	0	"	5'-0"
F	2	7/8"	30'-6"
F ₁	6	5/8"	16'-0"
F ₂	2	1"	16'-0"
F ₃	4	"	31'-9"
F ₄	4	"	21'-6"
F ₅	4	7/8"	31'-6"
F ₆	4	1"	13'-6"
h	48	3/8"	12'-3"
n ₁	96	1/8"	4'-3"
n ₂	28	7/8"	12'-0"
r	96	1/2"	9'-0"
s	63	1/2"	8'-6"
t ₁	70	1"	27'-6"
t ₂	70	3/4"	27'-0"
v ₁	16	3/4"	26'-6"
v ₂	160	3/8"	17'-9"
W ₁	52	1/2"	14'-3"

Class X Concrete - Cu Yds 130.0
Reinforcing Steel - lbs 18,280
Crested Piles-Up to 20' Lin Ft 160

Notes:
⊕ Denotes 12 Ton, Crested piles 16 req'd
⊙ " 15 Ton " " 32 "
⊙ " 20 Ton " " 32 "
Class X Concrete shall be used throughout
all reinforcing shall be wired securely
in place before concrete is poured



STANDARD
COMPUTED - R.A.H. - C.E.M.
CHECKED - C.E.M. R.A.W.
DRAWN - Chas E. Morgan
CHECKED - R.A.H. Wilson
SPECIAL
ASSEMBLED
CHECKED

EXAMINED 5-14-1934
PASSED BY [Signature]
APPROVED [Signature]
BRIDGE ENGINEER
ENGINEER OF DESIGN
CHIEF HIGHWAY ENGINEER

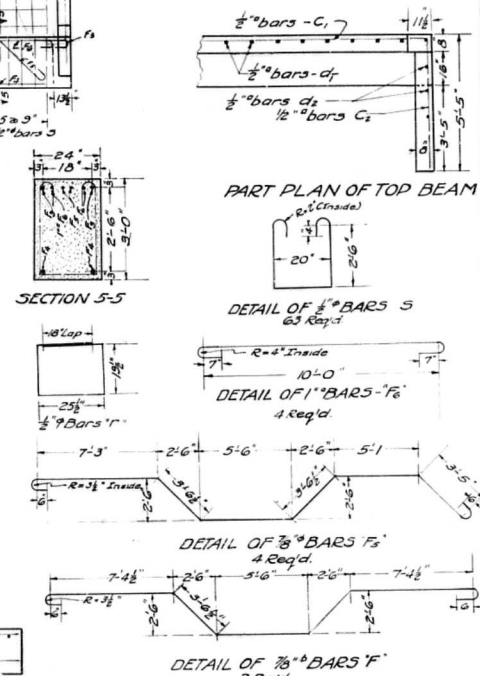
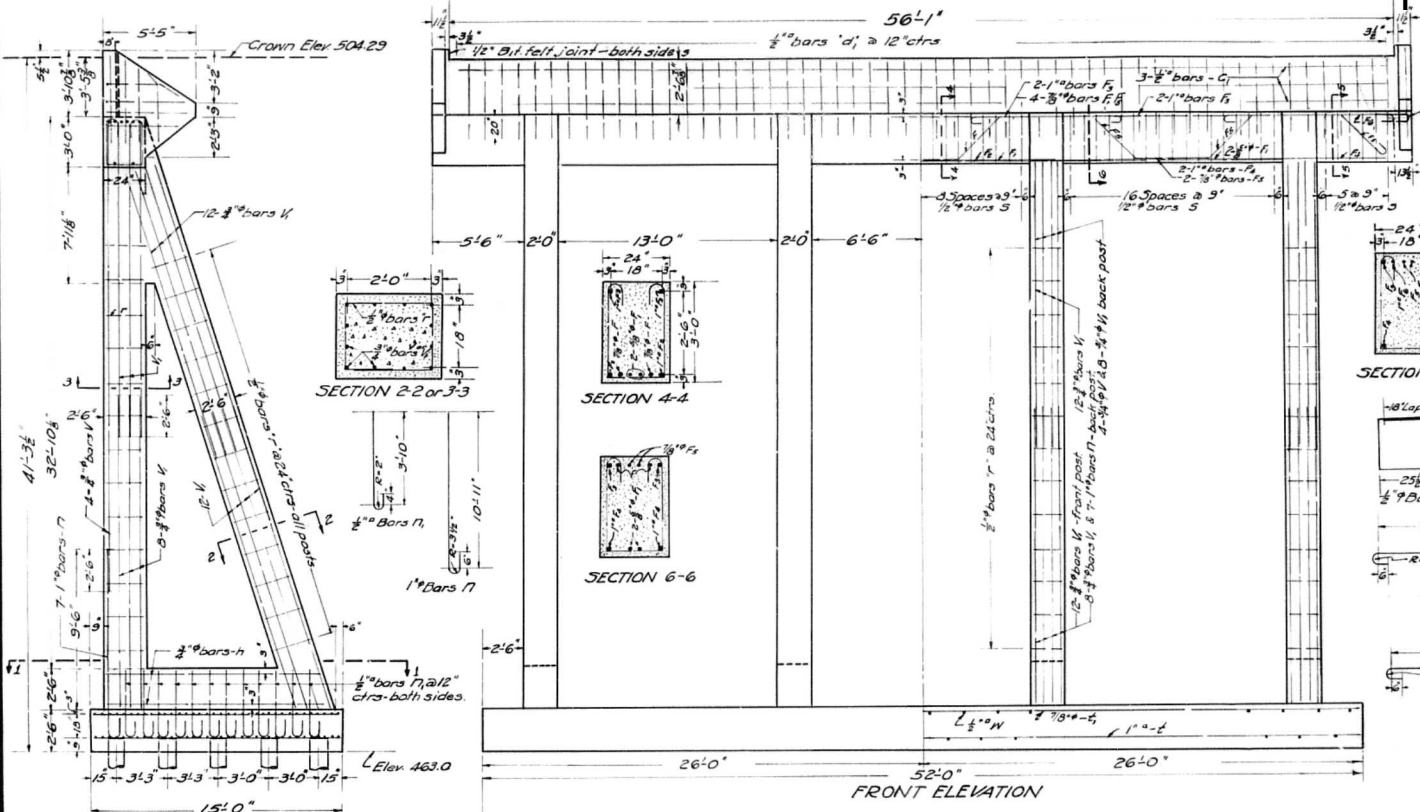
NO. 46-A
WEST ABUTMENT
S.B.I. RTE 3-SEC. 10RB-NRH
PEORIA COUNTY
STA. 245+57

B.M. 22 Spike & Washer in Power Pole; right of
5 to 244-70, Elev. 473.69

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PROJECT NO. 8
REV. 8
FEB. 1934
COUNTY Peoria
SHEET NO. 18
19 SHEETS

For information only



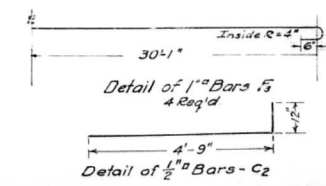
BILL OF MATERIAL

Bars	No.	Size	Length
C ₁	6	1/2"	23'-6"
C ₂	8	"	25'-9"
d	56	"	4'-0"
d ₁	2	1/2"	5'-0"
F	6	3/8"	30'-6"
F ₁	2	1/2"	16'-0"
F ₂	4	"	31'-9"
F ₃	4	"	21'-6"
F ₄	4	7/8"	31'-6"
F ₅	4	1"	13'-6"
h	48	3/4"	12'-9"
n	28	1"	12'-6"
n ₁	96	1/2"	4'-0"
n ₂	104	1/2"	3'-0"
s	63	1/2"	8'-6"
t	72	1"	27'-6"
v	72	3/4"	27'-3"
v ₁	16	3/4"	28'-0"
v ₂	160	3/4"	18'-9"
W	52	1/2"	14'-9"

Class X Concrete - Cu Yds. 143.4
Reinforcing Steel - lbs. 21,710
Crescoted Piles - Up to 20'-Link 1700

NO. 46-A
EAST ABUTMENT
S.B.I. RTE 8-SEC 10RB-NRH
PEORIA COUNTY
STA. 245 + 57

Notes:
⊙ Denotes 12 ton crescoted piles 17 reg'd.
⊙ " 15 ton " 34 "
⊙ " 22 ton " 34 "
Class X Concrete shall be used throughout.
All reinforcing shall be wired securely
in place before concrete is poured.



STANDARD	COMPUTED	- L.A.W. - C.E. 172
	CHECKED	- C.E. 172
	DRAWN	- Chas. E. Morgan
	CHECKED	- L.A. Wilson
SPECIAL	ASSEMBLED	
	CHECKED	

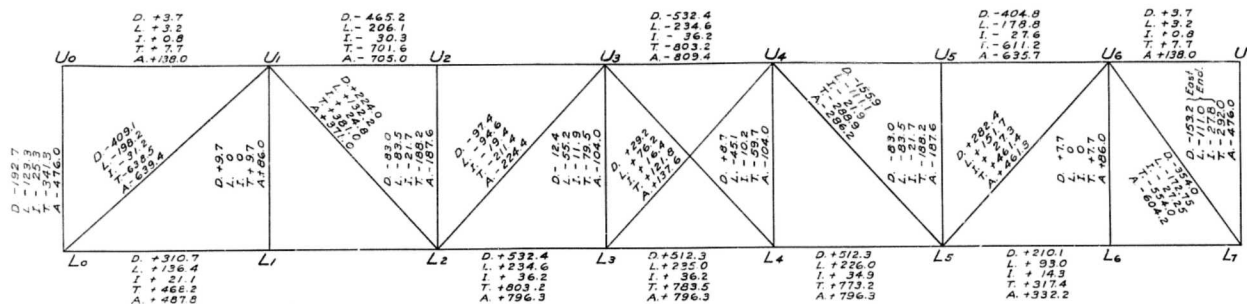
EXAMINED
PASSED
APPROVED
CHIEF HIGHWAY ENGINEER

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

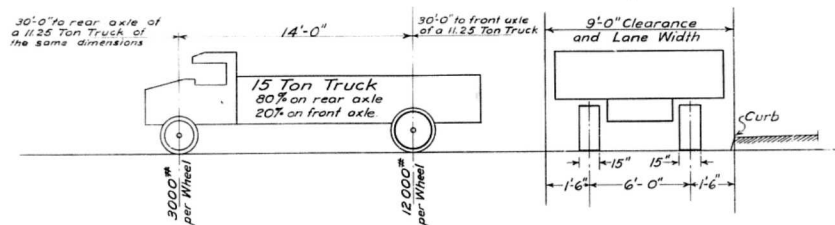
SHEET NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
10	10	Peoria	73	73

SHEET NO. 19
19 SHEETS

For information only



STRESS DIAGRAM - SPAN 7
Stresses in thousands of pounds.



H-15 TRUCK LOADING FOR LOADED LENGTHS UNDER 60 FEET.

For transverse floor beams:- 4 Parallel lines of trucks symmetrical about $\frac{1}{2}$ of roadway.

For longitudinal beams # stringers:- 5555 lines of trucks each for B3 to B10. Or 0.30 lines of trucks + 3' width of L.L. on sidewalk for B2 # B11. Or 2.0' width of L.L. on sidewalk only for B1 # B12.

Impact Coeff. = $\frac{50}{L+25}$ of Truck loading, where L = loaded length in feet.

Concentrated load 13500# for Moment
19500# for Shear

Uniform Load 480 lbs. per lin. ft. per lane

H-15 EQUIVALENT LOADING
FOR LOADED LENGTHS OF 60 FEET OR MORE.

Used for the design of trusses and piers.

Impact Coeff. = $\frac{50}{L+25}$

Note:-

All live loads to be so placed as to cause maximum stress in the member considered.

SIDEWALK LOADING.

Live load on sidewalk = P per sq. ft. where $P = (40 + \frac{3000}{L})(\frac{55-W}{50})$ in which:

L = loaded length of sidewalk in feet.
W = width of sidewalk in feet. W=5 in this case.
P = Not to exceed 100.
P = 100 for design of floor beams & stringers-Span 7.

SPAN 7									
Stringers-End Panels		Stringers-Intermed Panels		End Floor Beams		Intermediate Floor Beams		Beam-Supporting Span 6	
Moment-B11	Shear-B9	Moment	Shear	Moment	Shear	Moment	Shear	Moment	Shear
Dead Load 60100 #	5825 #	24225 #	4845 #	56200 #	22300 #	24750 #	31700 #	199100 #	64000 #
Live Load 71500	14870	66667	14333	440000	43050	425500	43050	581300	58500
Impact 15400	5020	23000	4942	150000	14850	14850	14850	157200	15800
Total 147000 #	25715 #	113892 #	24120 #	646200 #	80200 #	639420 #	89600	937600 #	138300 #

MOMENTS & SHEARS IN I-BEAM SPANS

	Beam B2-Span 1		Beams-Spans 2-8		Beams-B11-Spans 3-10		Beams-Spans 4-5-6	
	Moment	Shear	Moment	Shear	Moment	Shear	Moment	Shear
Dead Load	336200 #	23930 #	190000 #	17280 #	254500 #	20210 #	360000 #	24500 #
Live Load	224400	17610	159000	15600	196000	16720	238700	18400
Impact	31500	2690	25600	4620	28700	2610	32600	5440
Total	592100 #	44230 #	374600 #	37500 #	479200 #	39540 #	631300 #	48340 #

ALLOWABLE UNIT STRESSES IN STRUCTURAL STEEL.

Tension - 16000 #/sq. in.
Compression - 16000 - 70 #, but not to exceed 14000 #/sq. in.
Bending - 16000 #/sq. in. on extreme fibre of rolled or built up sections.
" - 24000 #/sq. in. " " pins.
Shearing - 12000 #/sq. in. on pins and shop rivets.
" - 10000 #/sq. in. " bolts " field rivets.
Bearing - 24000 #/sq. in. " pins " shop rivets.
" - 20000 #/sq. in. " bolts " field rivets.

SPECIFICATIONS -

For Design:- Standard Specifications for Highway Bridges and Incidental Structures, adopted by the American Association of State Highway Officials, dated 1931, with modifications.
For Fabrication and Erection:- Standard Specifications for Road and Bridge Construction of the Illinois Division of Highways, Adopted January 2, 1932, together with Supplemental Specifications Effective March 27, 1934.

STANDARD	COMPUTED	L. G. Wilson
CHECKED	-	H. F. Smith
DRAWN	-	L. G. Wilson
CHECKED	-	H. F. Smith
SPECIAL	ASSEMBLED	-
CHECKED	-	-

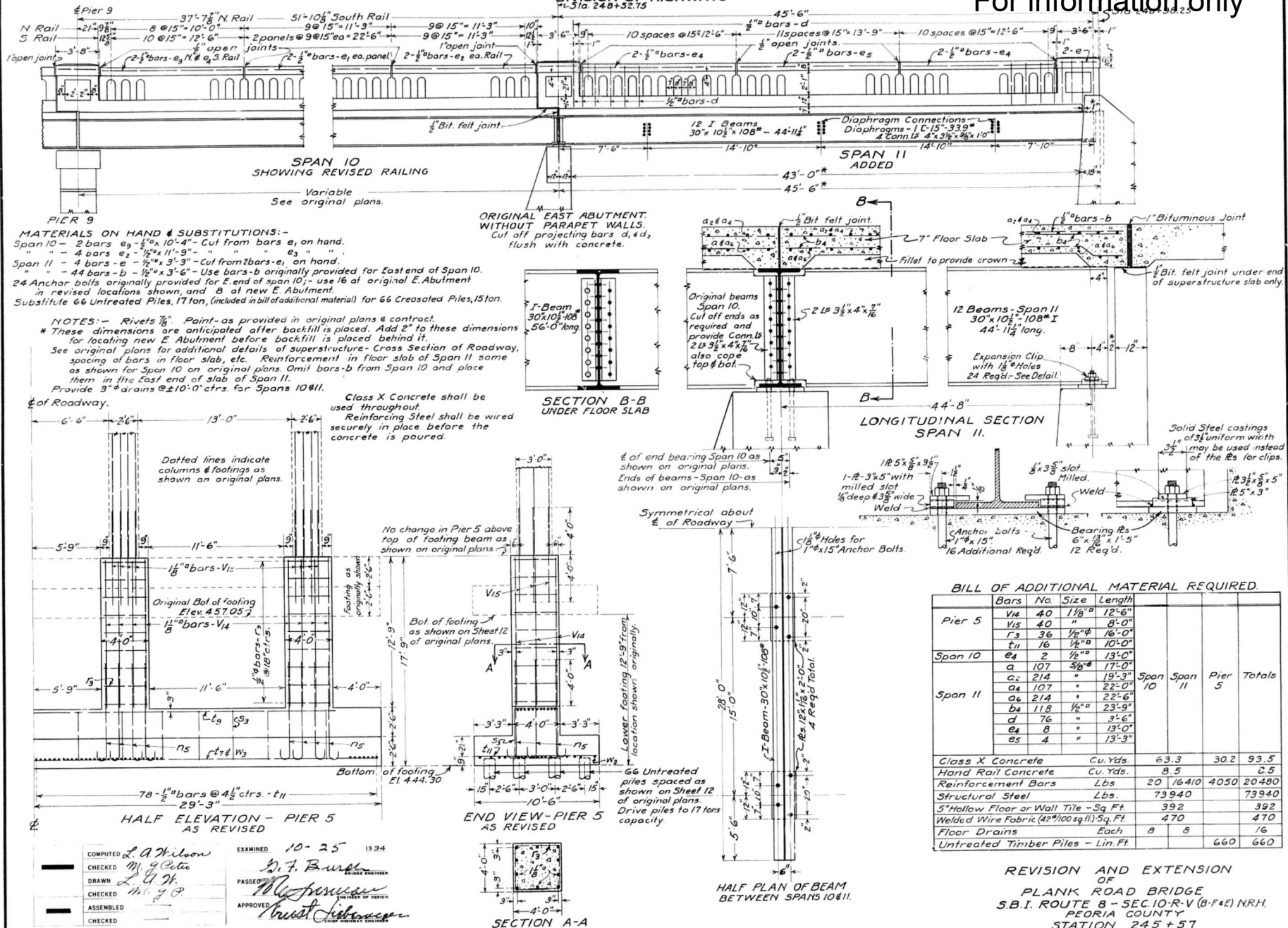
EXAMINED	6-7-1934
PASSED	H. F. Smith
APPROVED	H. F. Smith
CHECKED	H. F. Smith

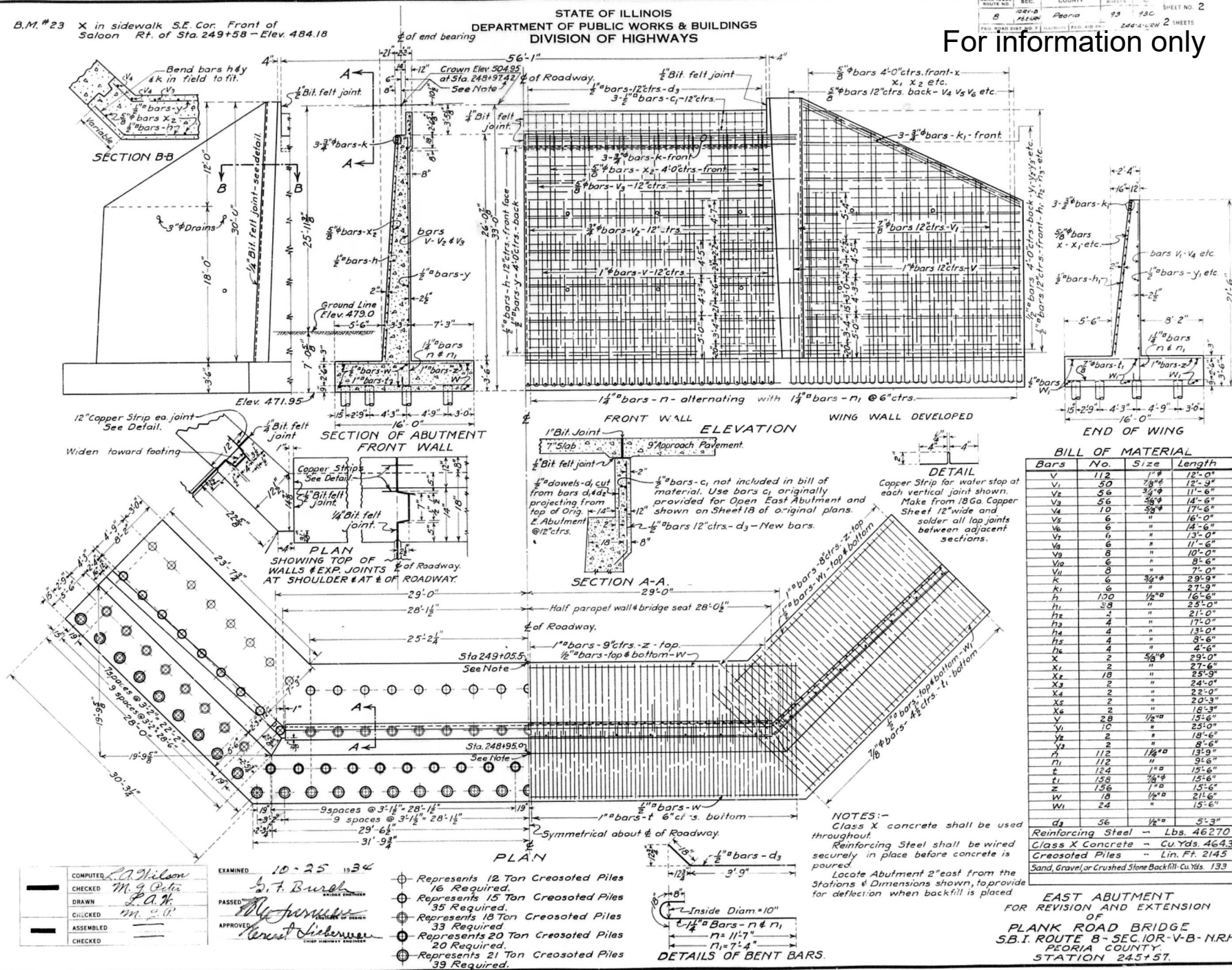
STRESS SHEET

No. 46A
PLANK ROAD BRIDGE
OVER C.B. & Q. RR. - C. & N. W. R.
AND KICKAPOO CREEK
PEORIA COUNTY.
S.B.I. RT. 8 - SECTION 10R-V-F-E-N.R.H.
STATION 245+57.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

For information only





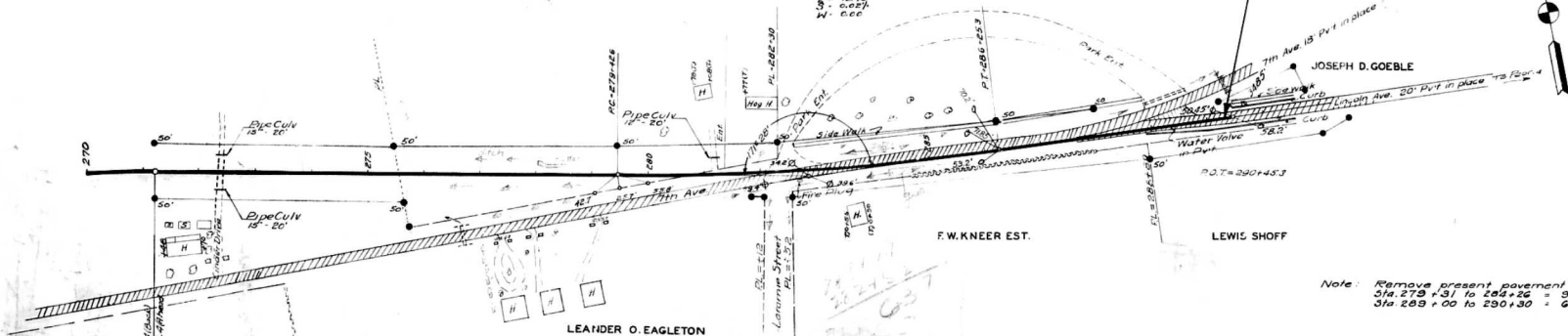
MRS. MINNIE SHOFF

W.E. STONE EST.

MADISON PARK

For information only

PI=286+84.6
 Δ=47.36
 Δ=1'-10"
 Δ=342.0
 Δ=150.0
 Δ=182.4
 Δ=18.73
 Δ=0.027
 Δ=0.027



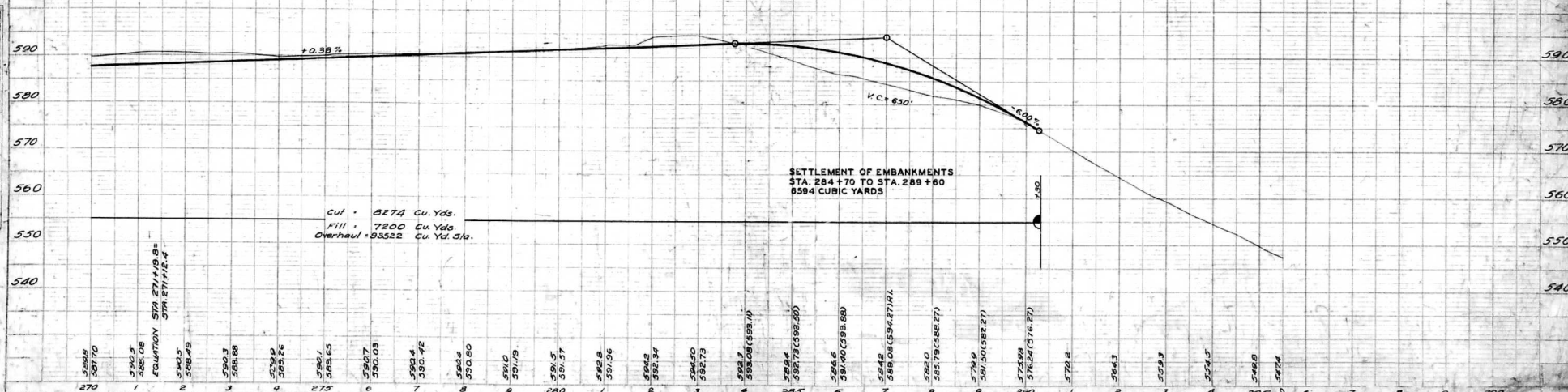
COMPUTED EARTHWORK QUANTITIES FOR APPROACH GRADIES

Type	Location	Cu Yds
Cut	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0
Fill	Sta. 272+38	1.0

Note: Remove present pavement —
 Sta. 279+31 to 282+26 = 390 Sq Yds.
 Sta. 289+00 to 290+30 = 670 Sq Yds.

B.M. #28
 Top of F.P. cor. Laramie & Lincoln Ave
 N.T.S. 382.150
 Elev. = 538.15

B.M. #29
 Sewer in P.A.
 L.T.S. 334.125
 Elev. = 535.40



SETTLEMENT OF EMBANKMENTS
 STA. 284+70 TO STA. 289+60
 8594 CUBIC YARDS

Cut = 8274 Cu. Yds.
 Fill = 7200 Cu. Yds.
 Overhaul = 93522 Cu. Yd. Sta.

N.R. 74, Route 3 9th 10R
 Peoria Airport to Peoria

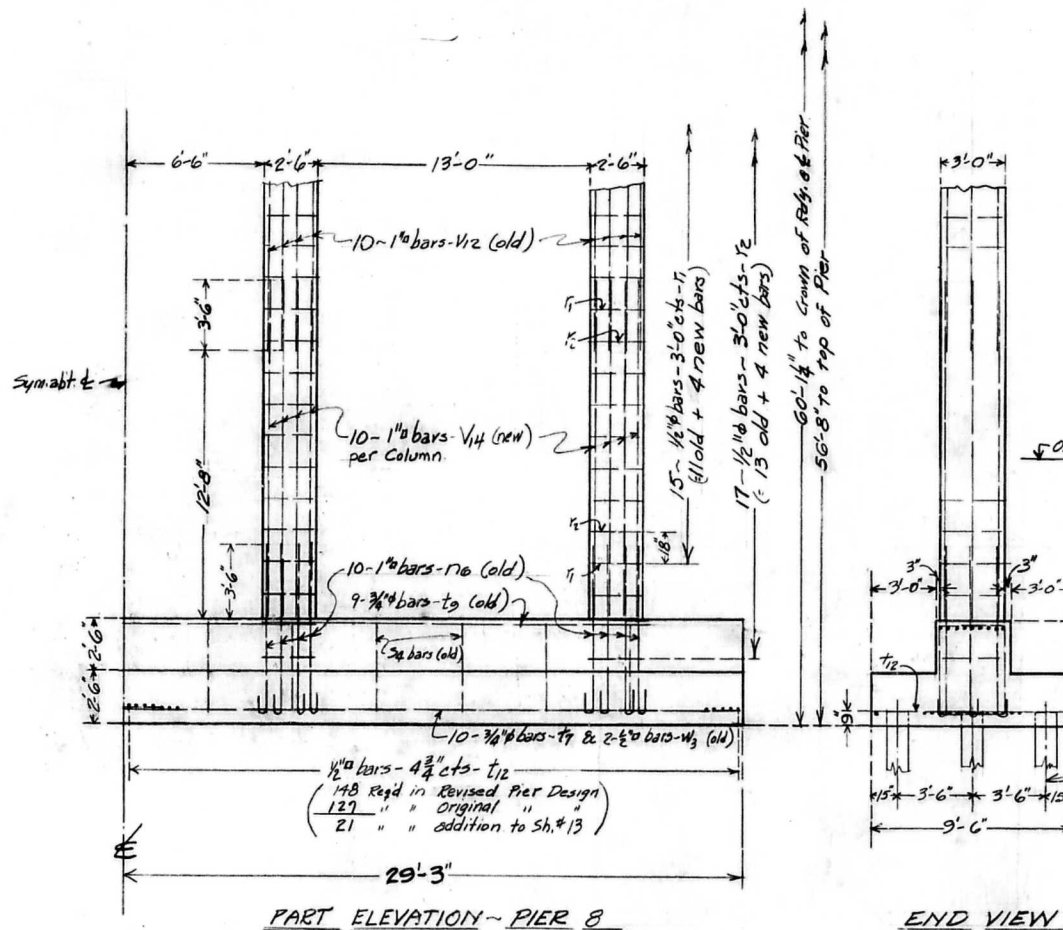
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SHEET NO. *Supplementary*
SHEETS

For information only

SUPPLEMENTARY BILL OF MATERIAL
(To be added to Quantities on Sheet 13)

Bar	No.	Size	Length
r	16	1/2"	10'-6"
r	16	1/2"	8'-3"
tie	21	1/2"	9'-3"
Via	40	1"	16'-0"
Reinforcing Steel - Lbs. 2340			
Class X Concrete - Cu. Yds. 14.1			



DETAIL OF REVISION OF PIER 8 DUE TO
LOWERING FOOTING FROM ELEV. 456.82 TO 444.15

(Details not shown, same as on Sheet 13)

STANDARD	COMPUTED	<i>R.B.M.</i>
	CHECKED	<i>A. A. Wilson</i>
	DRAWN	<i>R.B.M.</i>
	CHECKED	<i>A. A. Wilson</i>
SPECIAL	ASSEMBLED	
	CHECKED	

EXAMINED *July 26 1934*
B. F. Burch
BRIDGE ENGINEER
PASSED *[Signature]*
ENGINEER OF DESIGN
APPROVED *[Signature]*
CHIEF HIGHWAY ENGINEER

REVISION
PIER NO. 8

NO. 46A
PLANK ROAD BRIDGE
OVER C.B. & Q. R.R. - C. & N.W.R.R.
AND KICKAPOO CREEK
PEORIA COUNTY

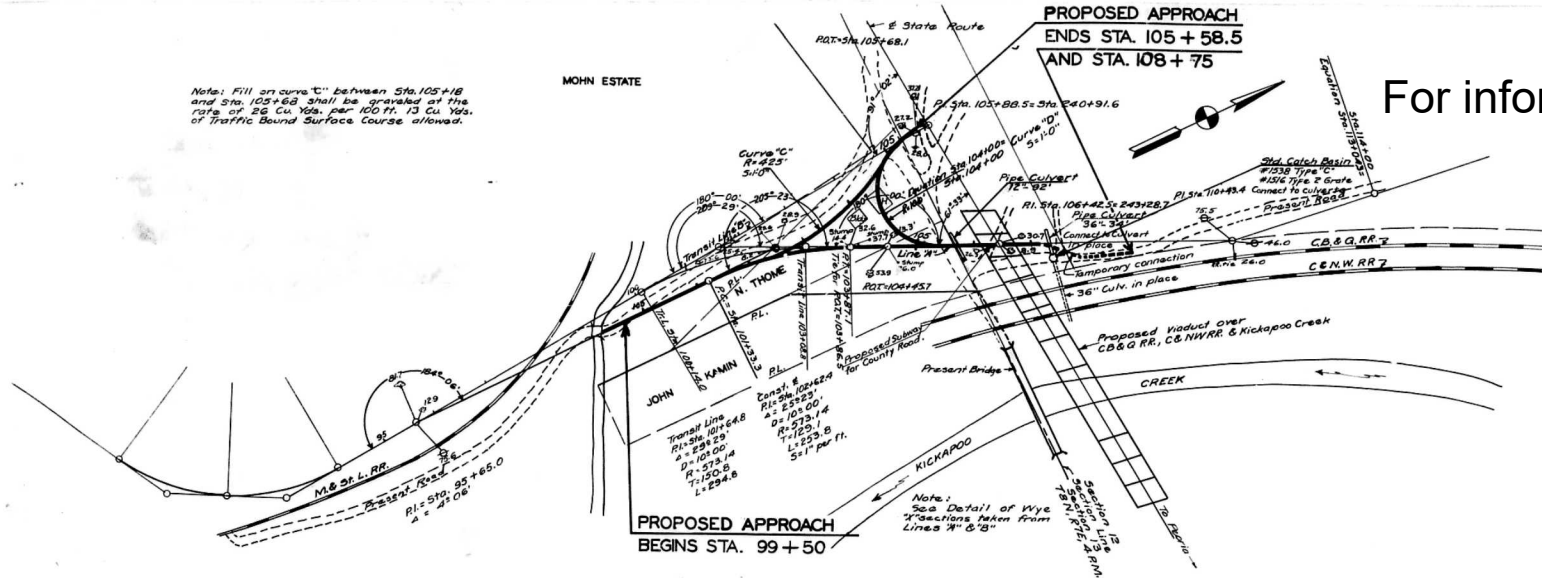
S.D.I. RT. 8 - SEC 10R-B-N.E.H.
STA. 245+57

Note: Fill on curve "C" between Sta. 105+18 and Sta. 105+68 shall be graded at the rate of 28 Cu Yds. per 100 ft. 13 Cu Yds. of Traffic Bound Surface Course allowed.

MOHN ESTATE

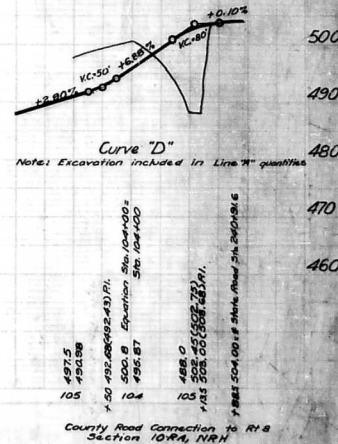
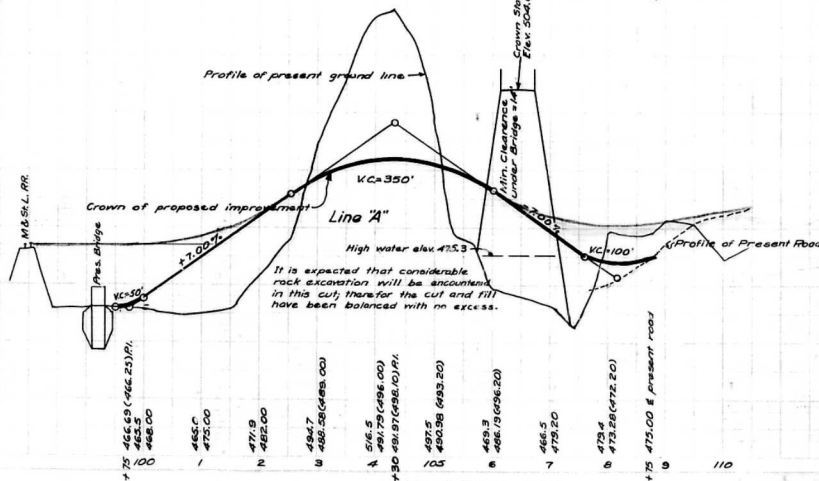
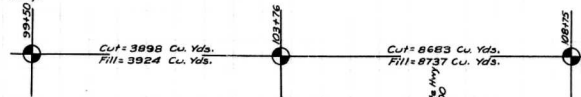
PROPOSED APPROACH
ENDS STA. 105 + 58.5
AND STA. 108 + 75

For information only



STANDARD GRADING SECTION
30' FT. ROADBED 2" IN CROWN
SEE CROSS SECTIONS

B.M. Elev. 513.72
Nail vertical in top of stump
Lt. Sta. 104+10



County Road Connection to Rt. 8
Section 10-14, N.R.H.