

PROJECT LOCATED IN :
VILLAGE OF DARIEN

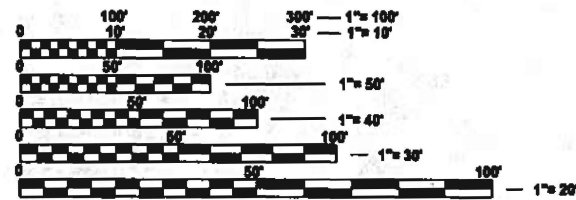
FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA :

CASS AVENUE
POSTED SPEED LIMIT = 40 MPH
TRAFFIC = 12,500 ADT (2020)
FUNCTIONAL CLASS:
MINOR ARTERIAL
MAJOR COLLECTOR

I-55
POSTED SPEED LIMIT = 55 MPH
TRAFFIC = 135,400 ADT (2023)
FUNCTIONAL CLASS:
INTERSTATE

PROJECT LOCATION
CASS AVENUE OVER I-55
STRUCTURE NO. 022-0072



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.L.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

IDOT PROJECT MANAGER: PRAVEEN KAINI, PE

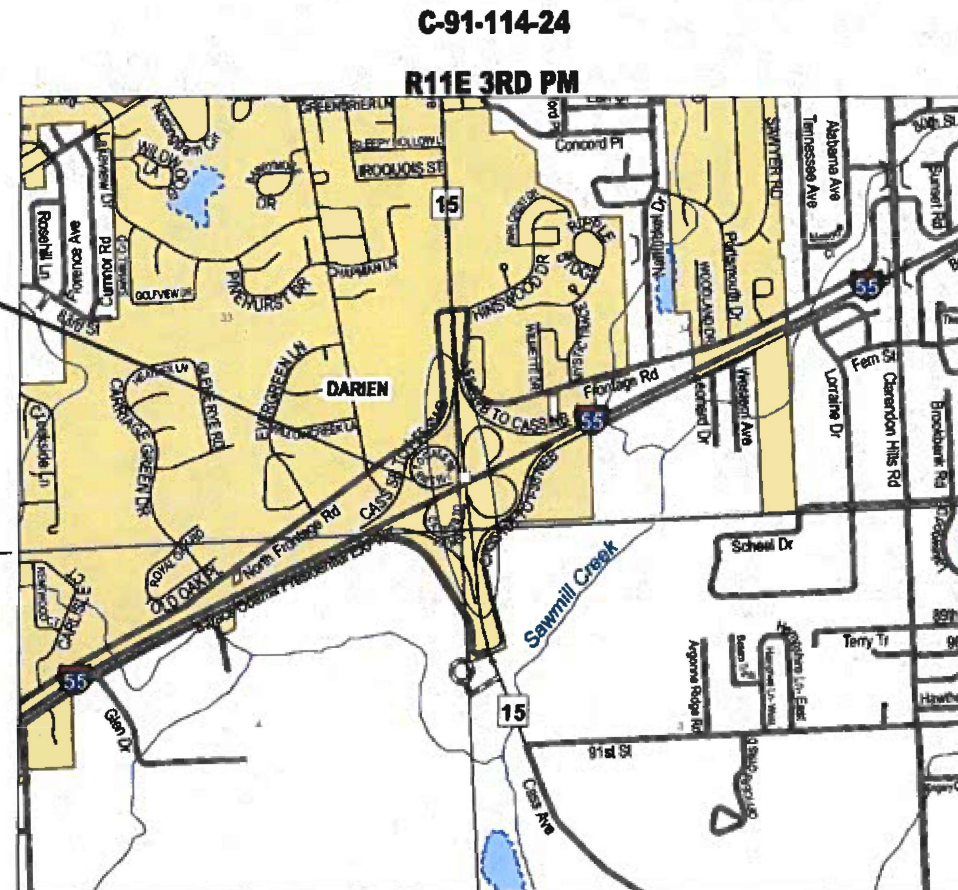
CONTRACT NO. 62W21

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PROPOSED
HIGHWAY PLANS**

**FAI ROUTE 55/I-55 (STEVENSON EXPRESSWAY)
AT CASS AVENUE**

**SECTION FAI 5523 BRIDGE REPAIR
BRIDGE DECK REPAIR & OVERLAY & JOINT REPLACEMENT
DUPAGE COUNTY**



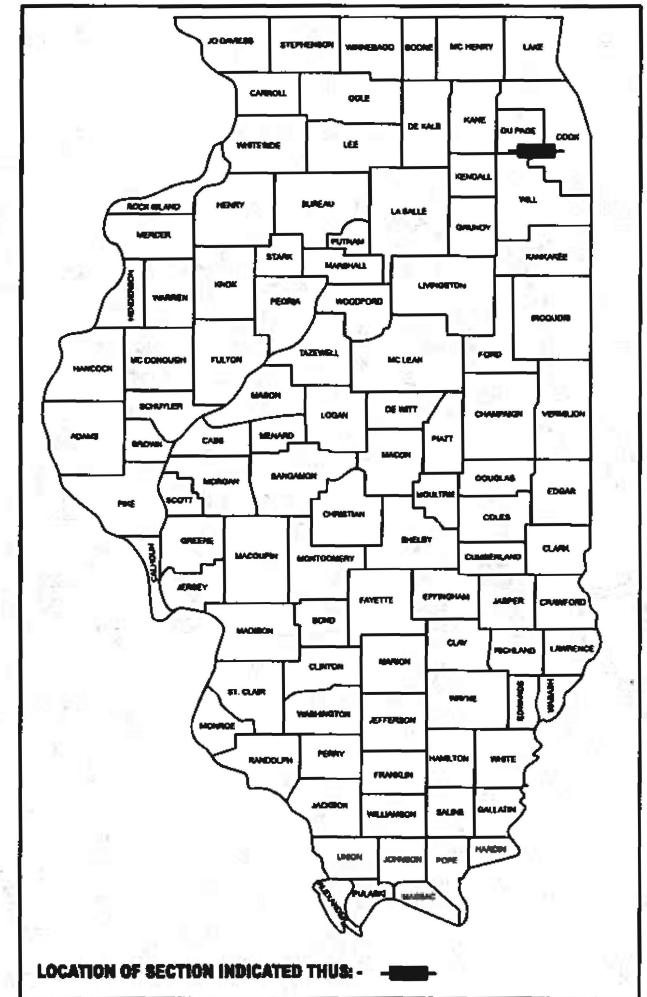
LOCATION MAP
NOT TO SCALE

GROSS LENGTH = 300 FT. = 0.057 MILE
NET LENGTH = 300 FT. = 0.057 MILE

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55A-55	FAI 5523 BRIDGE REPAIR	DUPAGE	80	1
ILLINOIS		CONTRACT NO. 62W21		

* 60 + 1 = 61 TOTAL SHEETS

D-91-100-24



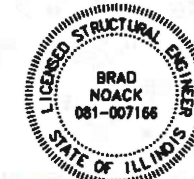
LOCATION OF SECTION INDICATED THUS: -

DBS DB STERLIN CONSULTANTS, INC.
1231 N. Western Drive, Suite 1000
Chicago, Illinois 60606
312.457.1000



David J. Cook

DAVE COOK DATE
LICENSE EXPIRES 11/30/2025
SHEET RANGE 1-15



Brad Noack

BRAD NOACK DATE
LICENSE EXPIRES 11/30/2026
SHEET RANGE 16-60

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
SUBMITTED <i>May 14, 2025</i>	REGIONAL ENGINEER
June 27, 2025	ENGINEER OF DESIGN AND ENVIRONMENT
June 27, 2025	DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

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HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
701106-02	OFF-ROAD OPERATIONS, >15' AWAY
701206-05	LANE CLOSURE 2L, 2W NIGHT ONLY FOR SPEEDS ≥ 45MPH
701400-12	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701101-05	OFF-RD OPERATIONS, MUTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701422-10	LANE CLOSURE, MULTILANE, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701423-10	LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701428-01	TRAFFIC CONTROL SETUP & REMOVAL FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
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DISTRICT 1 - HOT MIX ASPHALT MIXTURE REQUIREMENTS CHART

OPERATION	MIXTURE TYPE	AIR VOIDS (%) @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
BUTT JOINT AND HMA TAPER	POLYMERIZED HMA SURFACE COURSE, IL-9.5, "MIX E" N70; 1.75"	4.0% @ 70 Gyr.	QC/QA
TEMPORARY PAVEMENT (VARIABLE DEPTH)	HMA BINDER COURSE, IL-9.5,N70	4% @ 70 Gyr.	QC/QA
QUALITY MANAGEMENT PROGRAM (QMP) DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) QUALITY CONTROL FOR PERFORMANCE (QCP)			

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SQ. YD./IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED UNDER THE HMA SURFACE COURSE.

GENERAL NOTES

- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCA IONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND [DOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURCA THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN.HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT ERIC CAMPOS, AREA TRAFFIC FIELD ENGINEER AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL CONTACT CARLOS MUNOZ-ALBA THE DISTRICT ONE EXPRESSWAYS TRAFFIC CONTROL SUPERVISOR AT CARLOS.MUNOZ@ILLINOIS.GOV A MINIMUMOF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIEBARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
- THE CENTERLINE IS FOR INFORMATION ONLY.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD, FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS AND DIRECTED BY THE ENGINEER.

BENCHMARK

BENCHMARK DATA FROM IDOT PLANS DATED 6/22/2000
FOR INFORMATION ONLY
BRONZE DISK ON NORTHEAST WINGWALL
STRUCTURE 022-0072
ELEV = 217.533 (m)
ELEV = 713.690 (ft)

COMMITMENTS

NONE

FILE NAME: \\cam-dbs\epw21\CS_def_work_d\433013015_2\Index Of Drawings,Standards,And General Notes.dgn

<div><div>DBS</div><div>DB STERLIN CONSULTANTS, INC. 123 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.857.1006</div></div>	USER NAME	= icsM-dbstapw21\$	DESIGNED	= MBL	REVISED	=	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF DRAWINGS, STANDARDS, AND GENERAL NOTES					FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE	= 0.167' / in.	DRAWN	= YL	REVISED	=		55A-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	2					
	PLOT DATE	= 5/14/2025	CHECKED	= DJC	REVISED	=		CONTRACT NO. 62W21									
	PLOT TIME	= 10:34:33 PM	DATE	=	REVISED	=		SCALE: NTS		SHEET NO. G-2 OF G-7 SHEETS							
									ILLINOIS		FED. AID PROJECT						

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CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE
				100% STATE
				0059
				022-0072
20101400	NITROGEN FERTILIZER NUTRIENT	POUND	9	9
20101500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	9	9
20101600	POTASSIUM FERTILIZER NUTRIENT	POUND	9	9
21400100	GRADING AND SHAPING DITCHES	FOOT	140	140
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	312	312
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1
25100630	EROSION CONTROL BLANKET	SQ YD	312	312
40600370	LONGITUDINAL JOINT SEALANT	FOOT	440	440
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	982	982
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	86	86
46000029	BITUMINOUS MATERIALS (TACK COAT)	POUND	370	370
44000100	PAVEMENT REMOVAL	SQ YD	164	164
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	320	320
50102400	CONCRETE REMOVAL	CU YD	62.4	62.4

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CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE
				100% STATE
				0059
				022-0072
50157300	PROTECTIVE SHIELD	SQ YD	2,178	2,178
50300255	CONCRETE SUPERSTRUCTURE	CU YD	74.1	74.1
50300300	PROTECTIVE COAT	SQ YD	3,595	3,595
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	9,430	9,430
50800515	BAR SPLICERS	EACH	100	100
52000005	PREFORMED JOINT SEAL 1"	FOOT	272	272
52000110	PREFORMED JOINT STRIP SEAL	FOOT	473	473
58700300	CONCRETE SEALER	SQ FT	732	732
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	4	4
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	320	320
67100100	MOBILIZATION	L SUM	1	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	90	90
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	105	105
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	384	384

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CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE
				100% STATE
				0059
				022-0072
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	1,385	1,385
70307140	TEMPORARY PAVEMENT MARKING - LINE 8" - TYPE IV TAPE	FOOT	384	384
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	113	113
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,050	1,050
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	984	984
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,782	1,782
78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	384	384
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	113	113
78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7"	FOOT	208	208
78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,199	1,199
X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	16	16
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	16	16

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CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE
				100% STATE
				0059
				022-0072
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	994	994
X0325748	ACRYLIC COATING	SQ YD	210	210
X0325749	FIBER WRAP	SQ FT	1,810	1,810
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	2,277	2,277
X5051206	STRUCTURAL STEEL REPAIR	POUND	1,360	1,360
X6061462	CLEANING PAVED DITCH	FOOT	216	216
X6700407	ENGINEERING FIELD OFFICE, TYPE A (D1)	CAL MO	12	12
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	2,618	2,618
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	2,618	2,618
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	732	732
X5230172	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	2	2
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	12	12
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	2,618	2,618



DB STERLIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

USER NAME =	icsM-dbasepw215	DESIGNED =	MBL	REVISED =	
PLOT SCALE =	0.167" / in.	DRAWN =	MA	REVISED =	
PLOT DATE =	5/14/2025	CHECKED =	DJC	REVISED =	
PLOT TIME =	11:12:31 PM	DATE =		REVISED =	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES 3

SCALE: SHEET NO. G-6 OF G-7 SHEETS

FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
551-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	6
		CONTRACT NO. 62W21		
		ILLINOIS FED. AID PROJECT		

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE
				100% STATE
				0059
				022-0072
X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	275	275
Z0062458	TEMPORARY PAVEMENT (VARIABLE DEPTH)	TON	8	8

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GENERAL

1. THE TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE TRAFFIC CONTROL PLANS AT NO ADDITIONAL COST TO THE DEPARTMENT TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO THE ENGINEER AND THE DEPARTMENT FOR APPROVAL
2. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER SHALL BE INFORMED 72 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
4. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRED THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM TIME OF NOTIFICATION.
5. THE CONTRACTOR SHALL PROVIDE AT LEAST 48 HOURS PRIOR NOTICE TO THE RESIDENT ENGINEER, LOCAL EMERGENCY SERVICES, SCHOOLS, AND POST OFFICE PRIOR TO IMPLEMENTING LANE CLOSURES OR MAJOR TRAFFIC CONTROL CHANGES.
6. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
7. TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24'BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.
8. ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.

SIGNS

1. THE CONTRACTOR SHALL PROVIDE, INSTALL, MAINTAIN, AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
2. CHANGEABLE MESSAGE SIGNS SHALL BE PLACED BY THE CONTRACTOR TWO WEEKS BEFORE THE START OF CONSTRUCTION ACTIVITY AND CHANGES IN STAGES WITH THE APPROPRIATE MESSAGE DISPLAYED AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE PLACED IN ADVANCE OF THE WORK ZONE IN EACH DIRECTION OF CASS AVE. ADDITIONAL CHANGEABLE MESSAGE SIGNS HAVE BEEN INCLUDED TO BE USED AT THE RESIDENT ENGFINER'S DISCRETION. THE WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE FOR CHANGEABLE MESSAGE SIGN.

PAVEMENT MARKINGS

1. ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.

OTHER

1. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJACENT RESIDENTIAL AREAS.
2. THE CONTRACTOR SHALL MAINTAIN EXISTING ROADWAY LIGHTING DURING THE DURATION OF THE PROJECT.
3. ANY SAWCUTTING OF THE EXISTING PAVEMENT FOR STAGE CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT REMOVAL.
4. ANY PAVEMENT DROP-OFFS GREATER THAN 3 INCHES IMMEDIATELY ADJACENT TO TRAFFIC SHALL NOT BE LEFT OVERNIGHT. DROP-OFFS GREATER THAN 18 INCHES SHAL NOT BE EXPOSED TO TRAFFIC FOR GREATER THAN 48 HOURS. THE WORK TO COMPLY WITH THIS CRITERIA SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).

GENERAL SEQUENCE OF CONSTRUCTION

PRE STAGE

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL - WATER BLASTING.

STAGE 1

CLOSE I-55 EXIT RAMPS TO CASS AVE (NORTHBOUND & SOUTHBOUND). SET UP DETOUR AS SHOWN ON DETOUR PLANS SHIFT TRAFFIC TO INNER LANES OF CASS AVENUE AS SHOWN IN STAGE 1 PLANS. MAINTAIN 1 LANE OF TRAFFIC IN EACH DIRECTION. PERFORM DECK SCARIFCATION AND OVERLAY ON OUTER LANES OF BRIDGE.

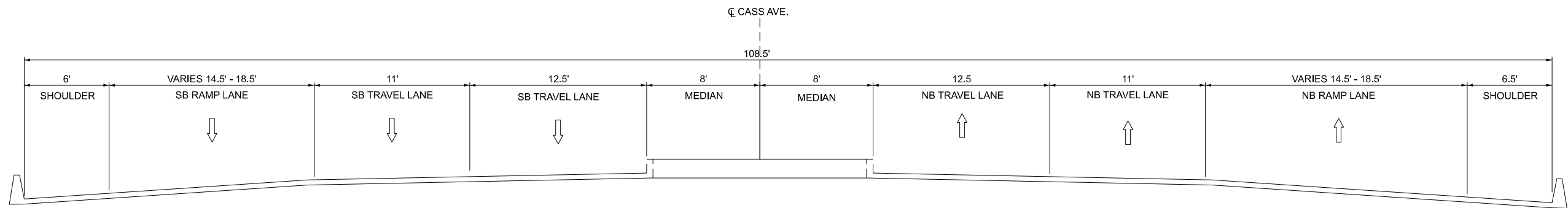
STAGE 2

OPEN I-55 EXIT RAMPS TO CASS AVE (NORTHBOUND & SOUTHBOUND). THAT WERE CLOSED IN STAGE 1. SHIFT TRAFFIC TO OUTSIDE LANES OF CASS AVENUE AS SHOWN IN STAGE 2 PLANS. MAINTAIN 2 LANES OF TRAFFIC IN EACH DIRECTION. PERFORM DECK SCARIFICATION AND OVERLAY ON INNER LANES OF BRIDGE.

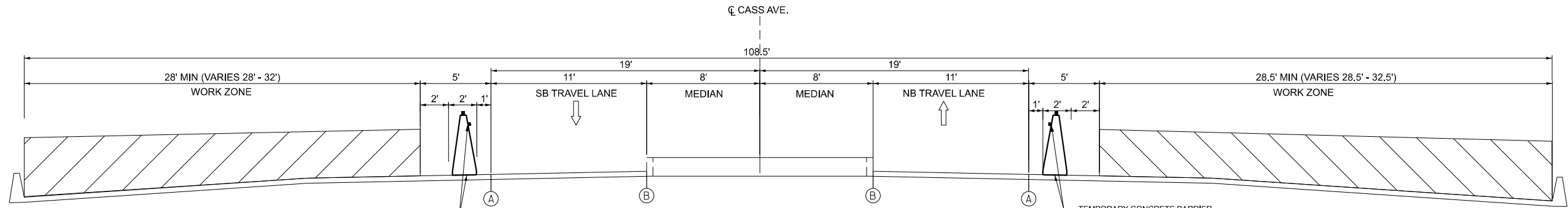
NOTE:
ENTRANCE RAMPS ONTO I-55 FROM CASS AVE TO REMAIN OPEN DURING BOTH STAGES OF CONSTRUCTION.

FILE NAME: \\nas001stepw21\CS_pof_work_dir\31113019_1\051161125-1-1-traffic_Control-General Notes And Legend.dgn

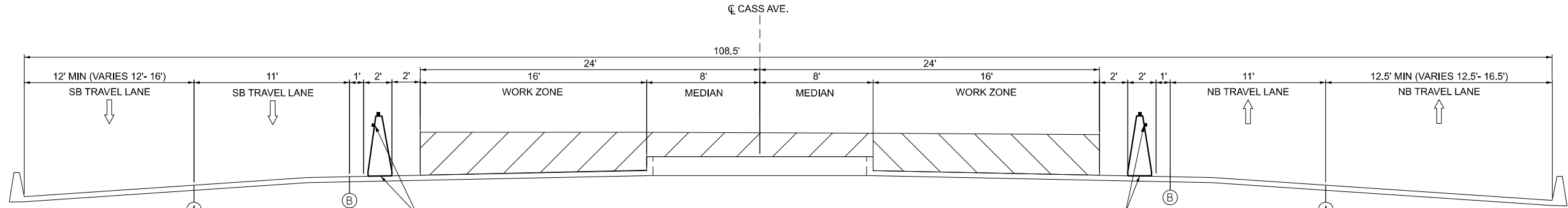
<div><div>DBS</div><div>DB STERLIN CONSULTANTS, INC. 122 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.857.1006</div></div>	USER NAME = lcsM-dbstepw21\$	DESIGNED - MBL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL GENERAL NOTES AND LEGEND			FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 0.167" / in.	DRAWN - YL	REVISED -					55/I-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	8
	PLOT DATE = 5/14/2025	CHECKED - DJC	REVISED -					CONTRACT NO. 62W21				
	PLOT TIME = 9:45:28 PM	DATE -	REVISED -		SCALE: NTS	SHEET NO. C-1 OF C-8 SHEETS			ILLINOIS	FED. AID PROJECT		



EXISTING TYPICAL CROSS SECTION
STA. 37+98 TO STA. 41+65



STAGE 1 TYPICAL CROSS SECTION
STA. 37+98 TO STA. 41+65



STAGE 2 TYPICAL CROSS SECTION
STA. 37+98 TO STA. 41+65

WORK ZONE SPEED LIMIT:

	EXISTING POSTED SPEED	PROPOSED POSTED SPEED
STAGE 1	40 MPH	40 MPH
STAGE 2	40 MPH	40 MPH

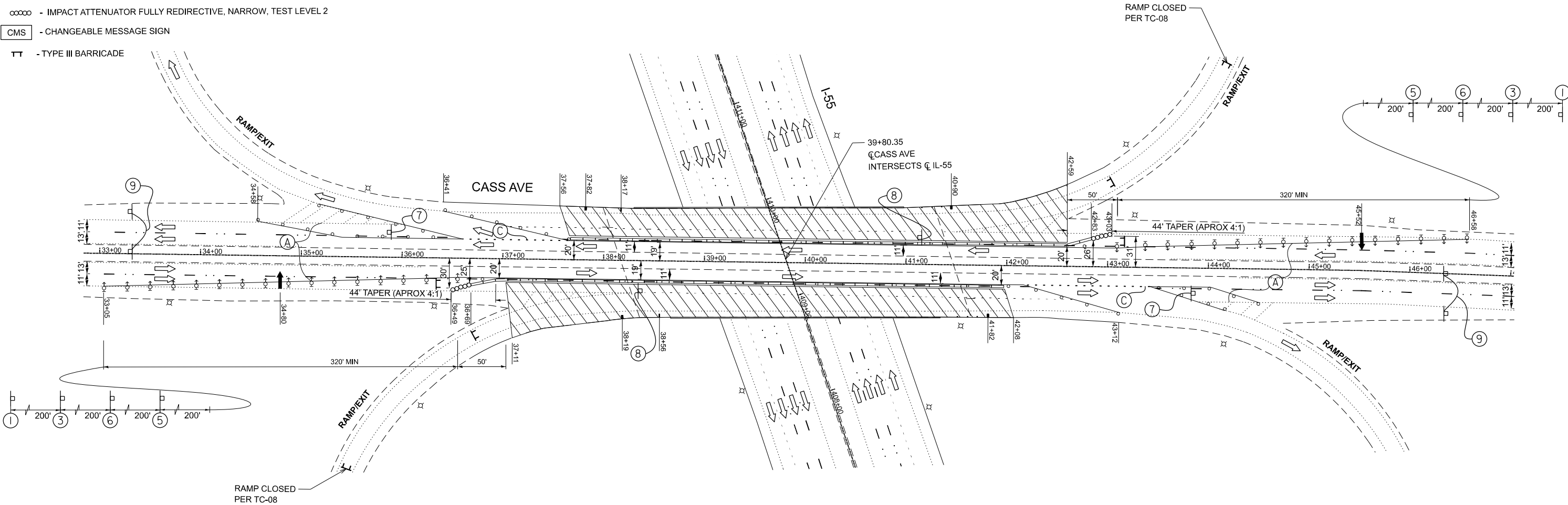
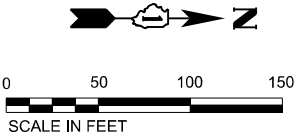
- LEGEND:**
- TEMPORARY CONCRETE BARRIER WITH MONO DIRECTIONAL CRYSTAL OR AMBER, TYPE C REFLECTORS. (PER STANDARD 704001 AND 782006)
 - WORK ZONE
 - TEMPORARY PAVEMENT MARKING - 4" WHITE
 - TRAFFIC DIRECTION
 - TEMPORARY PAVEMENT MARKING - 4" YELLOW

FILE NAME: \\cas-kdbstepw21\CS_pnf_work_dba\3301\3019_20\5116125e-hd\DOT_Typical Sections.dgn

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM OR TYPE II BARRICADE WITH SPACING AT 50' C-C ON TANGENT, 25' C-C ON SHIFTS, 10' C-C ON RADII
- DIRECTIONAL INDICATOR BARRIERS
- ARROW BOARD
- TRAFFIC DIRECTION
- TEMPORARY PAVEMENT MARKING - 4" WHITE, TYPE IV TAPE
- TEMPORARY PAVEMENT MARKING - 4" YELLOW, TYPE IV TAPE
- TEMPORARY PAVEMENT MARKING - 4" WHITE SKIP DASH TYPE IV TAPE 2' DASH, 6' SKIP
- TEMPORARY PAVEMENT MARKING - 12" WHITE, TYPE IV TAPE
- TEMPORARY TRAFFIC SIGN
- IMPACT ATTENUATOR FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- CHANGEABLE MESSAGE SIGN
- TYPE III BARRICADE

- ① W20-1103(0)-48
- ② W20-5L(0)-48
- ③ W20-5R(0)-48
- ④ W4-2L(0)-48
- ⑤ W4-2R(0)-48
- ⑥ WORK ZONE
SPEED LIMIT 40
R2-1-3648
R2-1106p-3618
- ⑦ EXIT
D4-1102(G)-3648
- ⑧ EXIT OPEN AHEAD
D4-1102(G)-3648
- ⑨ END WORK ZONE SPEED LIMIT
G20-1103-6036



TAPER LENGTH CALCULATIONS

SOUTHBOUND CASS AVE LANE SHIFT
 $L=W^2S^2/60=2^2 \cdot 40^2 \cdot 40/60=53$ FEET
 $L/2=26.6$
USE =27 FEET

SOUTHBOUND CASS AVE LANE CLOSURE
 $L=W^2S^2/60=11^2 \cdot 40^2 \cdot 40/60=293$ FEET
USE =293 FEET

NORTHBOUND CASS AVE LANE SHIFT
 $L=W^2S^2/60=2^2 \cdot 40^2 \cdot 40/60=53$ FEET
 $L/2=26.6$
USE =27 FEET

NORTHBOUND CASS AVE LANE CLOSURE
 $L=W^2S^2/60=11^2 \cdot 40^2 \cdot 40/60=293$ FEET
USE =293 FEET

FILE NAME: \\nasaldbstepw21\NCS_prof_work_dir\3301\3019_3\05116125-44\NOT-Stage 1.dgn



DB STERLIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.657.1006

USER NAME	= lcsM-dbstepw21\$	DESIGNED	- MBL	REVISED	-
PLOT SCALE	= 100,000' / in.	DRAWN	- YL	REVISED	-
PLOT DATE	= 5/14/2025	CHECKED	- DJC	REVISED	-
PLOT TIME	= 9:41:33 PM	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL
CASS AVENUE - STAGE 1

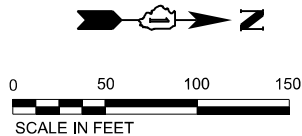
SCALE: AS SHOWN SHEET NO. C-3 OF C-8 SHEETS

FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55/I-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	10
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM OR TYPE II BARRICADE WITH SPACING AT 50' C-C ON TANGENT, 25' C-C ON SHIFTS, 10' C-C ON RADII
- DIRECTIONAL INDICATOR BARRIERS
- ARROW BOARD
- TRAFFIC DIRECTION
- TEMPORARY PAVEMENT MARKING - 4" WHITE, TYPE IV TAPE
- TEMPORARY PAVEMENT MARKING - 4" YELLOW, TYPE IV TAPE
- TEMPORARY PAVEMENT MARKING - 4" WHITE SKIP DASH TYPE IV TAPE 2' DASH, 6' SKIP
- TEMPORARY PAVEMENT MARKING - 12" WHITE, TYPE IV TAPE
- TEMPORARY TRAFFIC SIGN
- IMPACT ATTENUATOR FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- CHANGEABLE MESSAGE SIGN
- TYPE III BARRICADE

- ① W20-1103(0)-48
- ② W20-5L(0)-48
- ③ W20-5R(0)-48
- ④ W4-2L(0)-48
- ⑤ W4-2R(0)-48
- ⑥ WORK ZONE W2-1115(0)-3618
SPEED LIMIT 40 R2-1-3648
\$250 FINE MINIMUM R2-1106p-3618
- ⑦ EXIT D4-1102(G)-3648
- ⑧ EXIT OPEN AHEAD D4-1102(G)-3648
- ⑨ END WORK ZONE SPEED LIMIT G20-1103-6036
- ⑩ W24-4R(0)-48
- ⑪ W1-6R(0)-6030



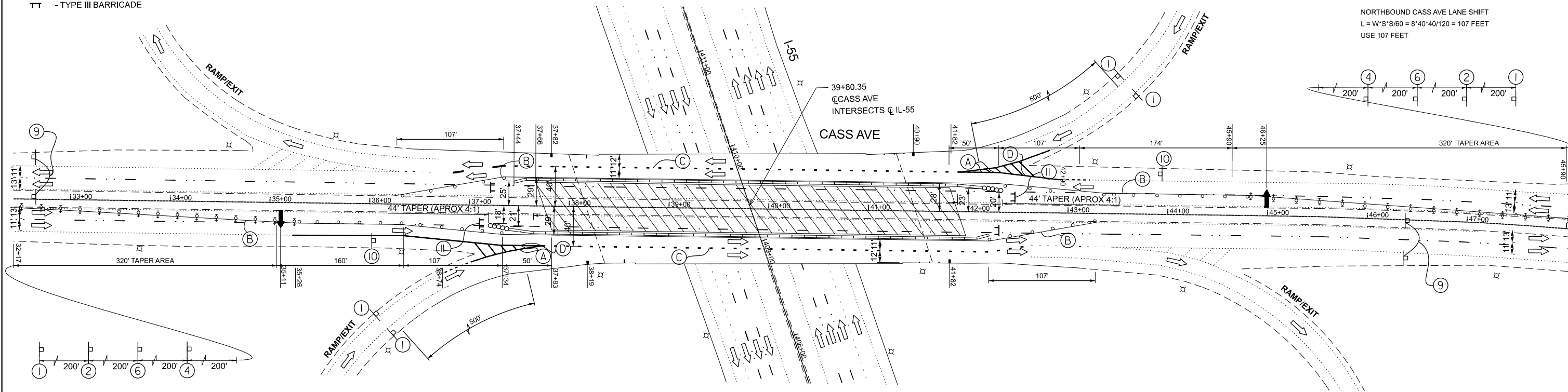
TAPER LENGTH CALCULATIONS

SOUTHBOUND CASS AVE LANE CLOSURE
 $L = W \cdot S \cdot S / 60 = 12 \cdot 40 \cdot 40 / 60 = 320$ FEET
USE 320 FEET

SOUTHBOUND CASS AVE LANE SHIFT
 $L = W \cdot S \cdot S / 120 = 8 \cdot 40 \cdot 40 / 120 = 107$ FEET
USE 107 FEET

NOUTHBOUND CASS AVE LANE CLOSURE
 $L = W \cdot S \cdot S / 60 = 12 \cdot 40 \cdot 40 / 60 = 320$ FEET
USE 320 FEET

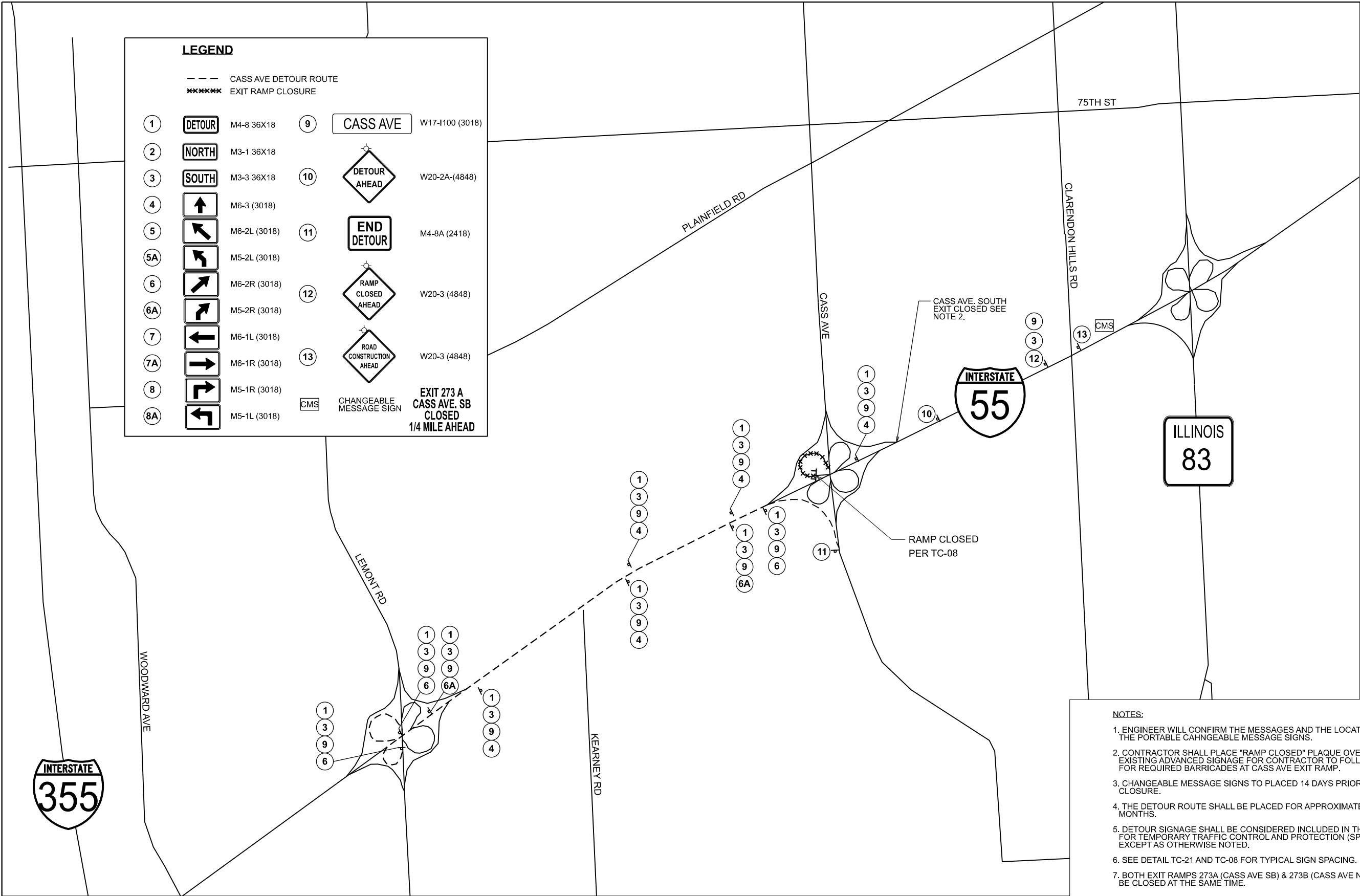
NORTHBOUND CASS AVE LANE SHIFT
 $L = W \cdot S \cdot S / 60 = 8 \cdot 40 \cdot 40 / 120 = 107$ FEET
USE 107 FEET



FILE NAME: \\nasaldbstepw21\CS_prof_work_dir\33013019_8105116125-44\NOT-Stage 2.dgn

DB STERLIN CONSULTANTS, INC. 123 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.857.1006	USER NAME = lcsM-dbstepw21\$			-	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL STAGE 2			FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100,000' / in.			-					55/4-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	11
	PLOT DATE = 5/14/2025			-		SCALE: AS SHOWN			CONTRACT NO. 62W21				
	PLOT TIME = 9:40:55 PM			-		SHEET NO. C-4 OF C-8 SHEETS			ILLINOIS FED. AID PROJECT				

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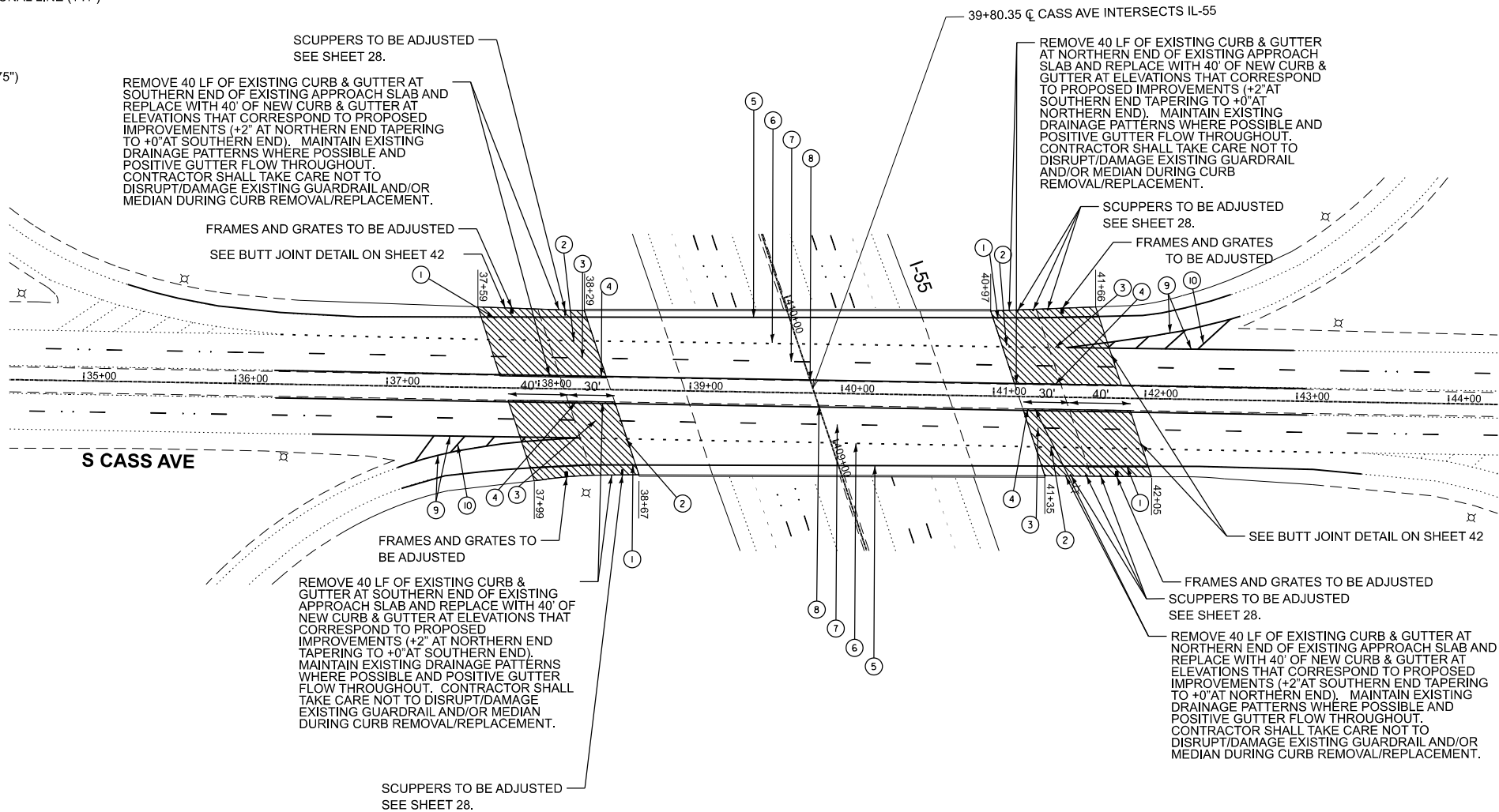


PAVEMENT MARKING LEGEND

- 1 THERMOPLASTIC PAVEMENT MARKING 4", SOLID, WHITE - EDGE LINE (TYP)
- 2 THERMOPLASTIC PAVEMENT MARKING 4", (DASH-6' SKIP)
- 3 THERMOPLASTIC PAVEMENT MARKING 4", SKIP-DASH, WHITE (10' DASH - 30' SKIP) - LANE LINES
- 4 THERMOPLASTIC PAVEMENT MARKING 4", SOLID, YELLOW - EDGE LINE (TYP)
- 5 MODIFIED URETHANE PAVEMENT MARKING 4", SOLID, WHITE
- 6 MODIFIED URETHANE PAVEMENT MARKING 4", (2' DASH- 6' SKIP)
- 7 PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, 7" (1.5" BLACK, 4" COLOR, 1.5" BLACK) GROOVED, SKIP-DASH, WHITE (10' DASH-30' SKIP) - LANE LINES.
- 8 MODIFIED URETHANE PAVEMENT MARKING 4", SOLID, YELLOW
- 9 THERMOPLASTIC PAVEMENT MARKING 8", SOLID, WHITE - EDGE LINE (TYP)
- 10 THERMOPLASTIC PAVEMENT MARKING 12", SOLID, WHITE - DIAGONAL LINE (TYP)

LEGEND

 POLYMERIZED HMA SURFACE COURSE, MIX "E", IL-9.5, N70 (1.75")



NOTES

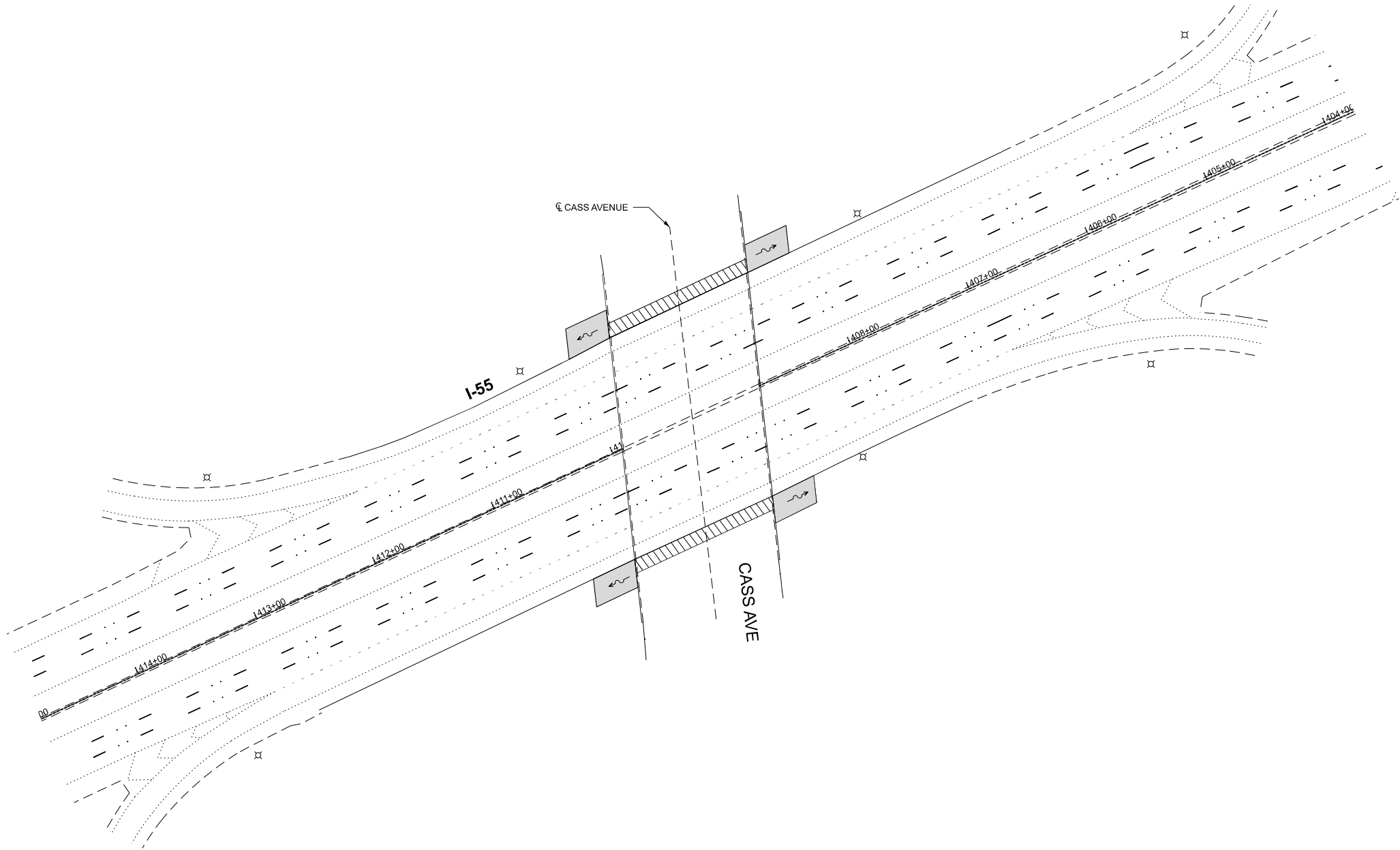
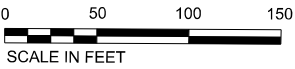
1. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS ON HMA PAVEMENT.
2. MODIFIED URETHANE PAVEMENT MARKINGS, SHALL BE USED FOR ALL PAVEMENT MARKINGS WITH IN BRIDGE LIMITS AND PCC APPROACH PAVEMENT, EXCEPT LANE LINES.
3. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS SHALL BE REMOVED AND REPLACED WITH LOW PROFILE RAISED REFLECTIVE PAVEMENT MARKINGS. IF LOW PROFILE MARKINGS ARE NOT AVAILABLE, THEY MAY BE OMITTED IF ACCEPTABLE TO RESIDENT ENGINEER.
5. ALL RAISED REFLECTIVE PAVEMENT MARRKERS SHALL BE INSTALLED ACCORDING TO IDOT D1 RRPM DETAIL TC-11

FILE NAME: \\cas-kdbstepw21\CS_pwf_work_dir\3301\3019_50D511612544-Roadway And Proposed Pavement Markings.dgn

<div>FILE NAME:  DB STERLIN CONSULTANTS, INC. 123 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.657.1096</div>	USER NAME = lcsM-dbstepw21\$		DESIGNED - MBL		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PROPOSED PAVEMENT MARKINGS					FAI RTE	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100,000' / in.		DRAWN - YL		REVISED -								55/I-55	FAI 5523 BRIDGE REPAIR		DUPAGE	60	14
	PLOT DATE = 5/14/2025		CHECKED - DJC		REVISED -			CONTRACT NO. 62W21										
	PLOT TIME = 9:40:36 PM		DATE -		REVISED -			SCALE: AS SHOWN		SHEET NO. C-7 OF C-8 SHEETS				ILLINOIS	FED. AID PROJECT			

LEGEND

- REGRADE EXISTING DITCH TO DRAIN (APPROX 35').
REMOVE/RESPREAD ALL EXCESS MATERIAL. RESTORE ALL DISTURBED
NATURAL AREAS WITH 4" TOP SOIL, SEED & BLANKET.
- REMOVE ALL SEDIMENT, DEBRIS AND OBSTRUCTIONS AT TOE OF
SLOPE WALL (APPROX 108') SO THAT AREA DRAINS EAST/WEST TO
REGRADED DITCH.
- PROPOSED FLOW DIRECTION (0.5% MINIMUM SLOPE)

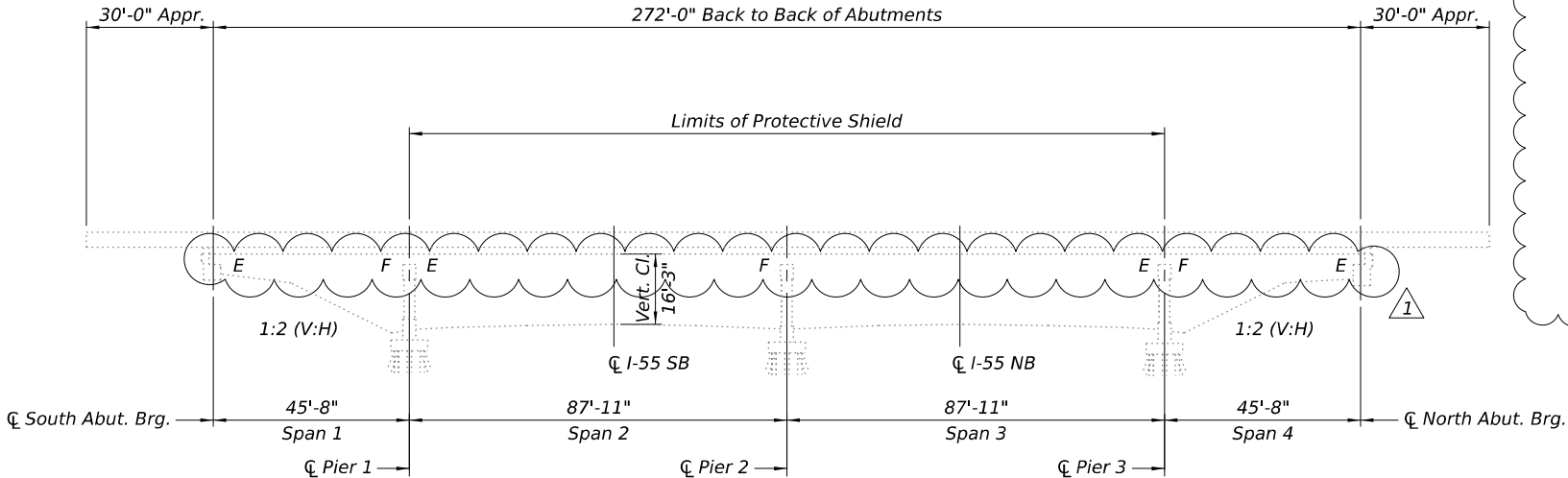


FILE NAME: \\nasal\obstepw21\CS_pwf_work_dir\3301\3019_0\05116125-414-Slope Wall Ditch Regrading.dgn

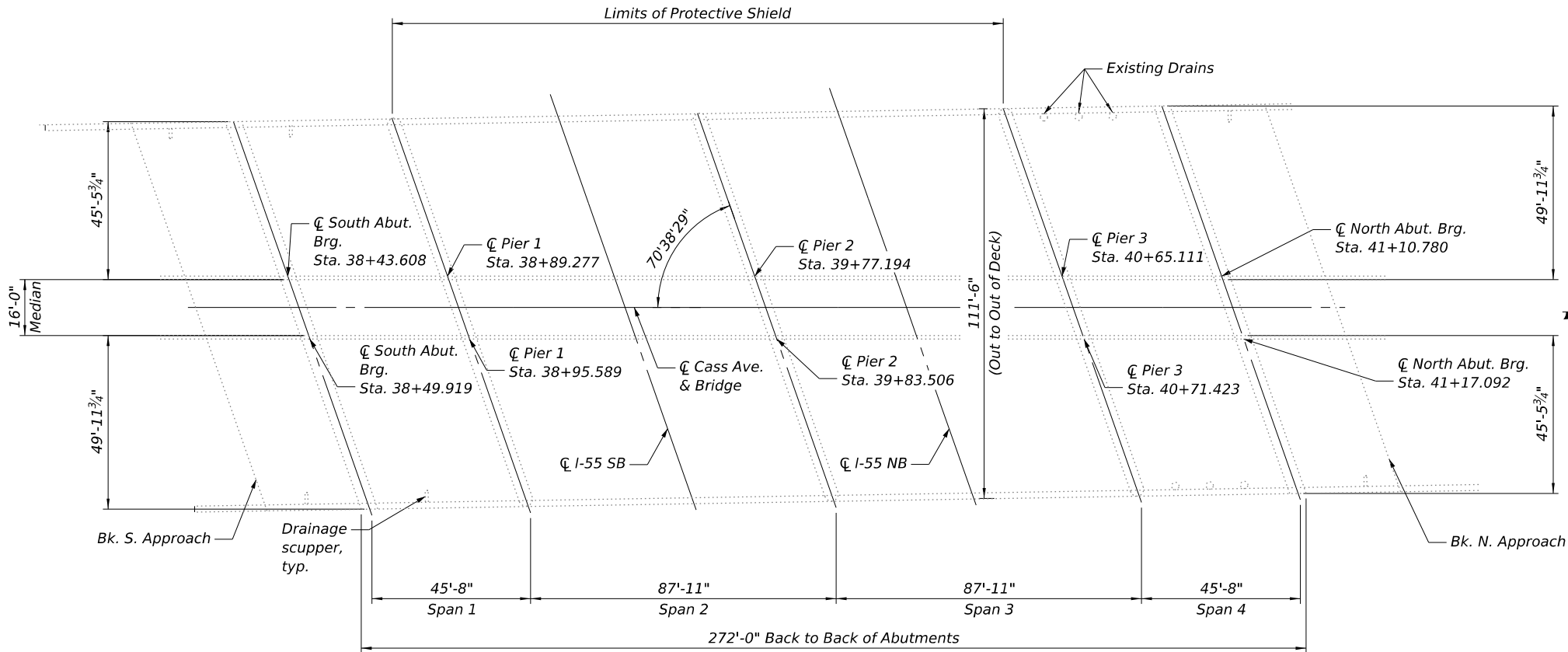
<div><div>DBS</div><div>DB STERLIN CONSULTANTS, INC. 1221 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.257.1006</div></div>	USER NAME = lcsM-obstepw21\$		DESIGNED - MBL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SLOPE WALL DITCH REGRAIDING		FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100,000' / in.		DRAWN - YL	REVISED -				55/I-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	15
	PLOT DATE = 5/14/2025		CHECKED - DJC	REVISED -		SCALE: AS SHOWN		CONTRACT NO. 62W21				
	PLOT TIME = 9:41:51 PM		DATE -	REVISED -		SHEET NO. C-8 OF C-8 SHEETS		ILLINOIS FED. AID PROJECT				

Existing Structure S.N. 022-0072 is built in 1960 and reconstructed in 2000.
The structure is a 4-span steel beams bridge with concrete deck supported by concrete piers.
The concrete piers are supported by timber piles and the pile bent abutments are supported
by concrete piles with out-to-out of 111'-6" and length of 272'-0" Bk. to Bk. of Abutments.

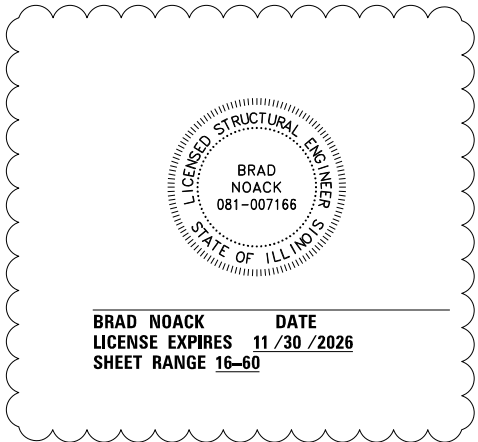
Traffic to be maintained utilizing staged construction.



ELEVATION



PLAN



DESIGN SPECIFICATIONS

(PROPOSED CONSTRUCTION)

2020 AASHTO LRFD Bridge Design Specifications, 9th Edition

LOADING HS20-44

(2000 CONSTRUCTION)

No allowance for future wearing surface.

SEISMIC DATA

S.P.C. = A

"A" = 0.04

Site Coefficient = 1.0

EXISTING DESIGN STRESSES

(Existing Structures)

f_c = 3,480 psi (24 MPa)

f_y = 58,015 psi (400 MPa) (Reinforcement)

f_y = 33,069 psi (228 MPa) (Existing Beams)

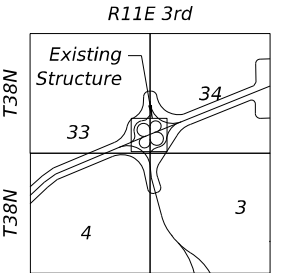
f_y = 36,260 psi (250 MPa) (Existing Beams)

PROPOSED DESIGN STRESSES

FIELD UNITS:

f_c = 4,000 psi (Superstructure)

f_y = 60,000 psi (Reinforcement)



LOCATION SKETCH

SCOPE OF WORK:

1. Provide Temporary Protective Shielding over I-55.
2. Scarify Bridge deck $\frac{3}{4}$ ".
3. Replace expansion joints at Piers 1 & 3 and Abutments.
4. Perform Structural Steel Repairs.
5. Re-seal longitudinal joint in Median.
6. Adjust and clean bridge scuppers and approach drains.
7. Apply 2 $\frac{3}{4}$ " Bridge Deck Latex Concrete Overlay on bridge slab.
8. Perform Diamond Grinding on finished Latex Concrete Overlay.
9. Perform Structural Repair of Concrete on Piers.
10. Apply Fiber Wrap on columns of Piers 1 and 3.
11. Perform Bridge Deck Grooving and apply Protective Coat to the top and inside faces of parapets, sidewalk, and latex concrete overlay.
12. Apply Concrete Sealer to new concrete placed at piers.

GENERAL PLAN AND ELEVATION

CASS AVENUE OVER I-55

FAI 055 - SECTION 055-23

BRIDGE REPAIR

DUPAGE COUNTY

STATION 39+80.35

STRUCTURE NO. 022-0072

FILE NAME: \\camdbstepw21\CS_pdt_work_dfr47113903_1\TASK 4-2W21-S00-LGPE.dgn



DB STERLIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

USER NAME	=	lcsMdbstepw21\$	DESIGNED	-	MM	REVISED	-	06-26-2025
PLOT SCALE	=		DRAWN	-	ANH	REVISED	-	
PLOT DATE	=	6/27/2025	CHECKED	-	BJN	REVISED	-	
PLOT TIME	=	12:04:56 AM	DATE	-		REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 022-0072

SCALE: SHEET NO. S-1 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DUPAGE	60	16
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

FILE NAME: \\casht-dba\pwp2\UCS_pdf_work_d\47113903_2\TASK_4-02\021-S002-GEN NOTES.dgn

GENERAL NOTES

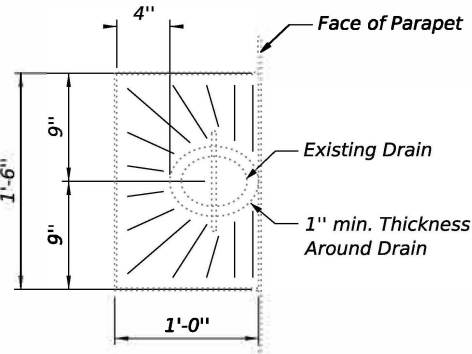
1. Reinforcement Bars designated (E) shall be Epoxy Coated.
2. Prior the pouring the new Concrete Deck for Expansion Joint Reconstruction and Deck Slab Repairs, all loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan Dimensions and Details relative to existing structure have been taken from existing plans and are subject to nominal construction variation. The Contractor shall field verify Existing Dimensions and Details affecting new Construction and make necessary approved adjustments prior to construction or ordering of materials. Such variation shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Existing reinforcement extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any existing reinforcement bars intended for reuse and damaged during the Concrete Removal operations shall be repaired or replaced using an approved Bar Splicer or Anchorage System to the satisfaction of the Engineer. Cost included with Concrete Removal.
5. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
6. The minimum thickness of the concrete overlay is 2.75" and varies as required to adjust for the existing profile and/or beam camber.
7. All Structural Steel shall conform to AASHTO Classification M-270 Gr36, unless otherwise noted.
8. Fasteners shall be ASTM F3125 GRADE A325 TY 1, mechanically galvanized bolts. Bolts 3/4" dia, holes 13/16" diam., unless otherwise noted.
9. The existing structural steel coating contains lead. The contractor shall take appropriate precautions to deal with the presence of lead on the project.
10. All new structural steel and bearing assemblies shall be "Hot Dip Galvanized for Structural Steel".
11. Diaphragm connection holes shall be 15/16" dia for 3/4" bolts. Two hardened washers shall be required at the diap. connections.

INDEX OF SHEETS

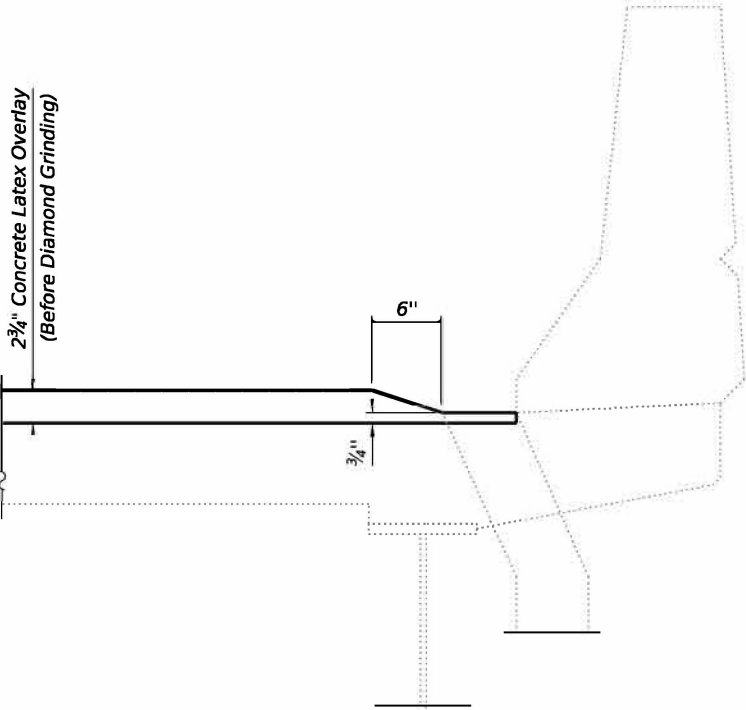
- S-1. General Plan and Elevation
- S-2. General Notes, Index of Sheets, and Summary of Quantities
- S-3. Cross Sections & Staging Construction (1 of 2)
- S-4. Cross Sections & Staging Construction (2 of 2)
- S-5. Deck Overlay and Slab Repair Details
- S-6. Expansion Joint Plan at South Abutment
- S-7. Expansion Joint Plan at Pier 1
- S-8. Expansion Joint Plan at Pier 3
- S-9. Expansion Joint Plan at North Abutment
- S-10. Expansion Joint Details (1 of 2)
- S-11. Expansion Joint Details (2 of 2)
- S-12. Preformed Joint Strip Seal (1 of 2)
- S-12a. Preformed Joint Strip Seal (2 of 2)
- S-13. Bar Splicer Assembly and Mechanical Splicer Details
- S-14. Drainage Scupper Alterations
- S-15. Pier Substructure Repairs (1 of 3)
- S-16. Pier Substructure Repairs (2 of 3)
- S-17. Pier Substructure Repairs (3 of 3)
- S-18. Pier Substructure Repairs - Column Fiber Wrap
- S-19. Structural Steel Repairs (1 of 3)
- S-20. Structural Steel Repairs (2 of 3)
- S-21. Structural Steel Repairs (3 of 3)
- S-22. Existing Plan - For Reference Only (1 of 10)
- S-23. Existing Plan - For Reference Only (2 of 10)
- S-24. Existing Plan - For Reference Only (3 of 10)
- S-25. Existing Plan - For Reference Only (4 of 10)
- S-26. Existing Plan - For Reference Only (5 of 10)
- S-27. Existing Plan - For Reference Only (6 of 10)
- S-28. Existing Plan - For Reference Only (7 of 10)
- S-29. Existing Plan - For Reference Only (8 of 10)
- S-30. Existing Plan - For Reference Only (9 of 10)
- S-31. Existing Plan - For Reference Only (10 of 10)

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
CONCRETE REMOVAL	CU YD	62.4		62.4
PROTECTIVE SHIELD	SQ YD	2,178		2,178
CONCRETE SUPERSTRUCTURE	CU YD	74.1		74.1
PROTECTIVE COAT	SQ YD	3,595		3,595
STRUCTURAL STEEL REPAIR	POUND	1,360		1,360
REINFORCEMENT BARS, EPOXY COATED	POUND	9,430		9,430
BAR SPLICERS	EACH	100		100
PREFORMED JOINT SEAL 1"	FOOT	272.0		272.0
PREFORMED JOINT STRIP SEAL	FOOT	473.0		473.0
CONCRETE SEALER	SQ FT		732	732
ACRYLIC COATING	SQ YD		210	210
FIBER WRAP	SQ FT		1,810	1,810
BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	2,277		2,277
BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	2,618		2,618
BRIDGE DECK SCARIFICATION 3/4"	SQ YD	2,618		2,618
STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT		732	732
DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	2,618		2,618
DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	2		2



OVERLAY TREATMENT AT FLOOR DRAINS
(Located in Span 4)



SECTION AT FLOOR DRAINS





FILE NAME: \\lcsM-dbstepw21\lcs pdf work dir\471\13903 18\TASK 4-62W21-S004-STAGING 2.dgn



DECK PLAN

LEGEND



Scarify $\frac{3}{4}$ "; Bridge Deck Latex Concrete Overlay, $2\frac{3}{4}$,"
Diamond Grinding $\frac{1}{4}$ ", Protective Coat



Concrete Removal at Deck Joints



HMA Overlay (See Roadway Plans)

USER NAME =	icsmkbstepw21\$	DESIGNED =	MM	REVISED =	06-26-2025
PLOT SCALE =		DRAWN =	DJB	REVISED =	
PLOT DATE =	6/27/2025	CHECKED =	BJN	REVISED =	
PLOT TIME =	12:04:55 AM	DATE =		REVISED =	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK OVERLAY AND SLAB REPAIR PLANS

SCALE: 1/16" = 1'-0"

SHEET NO. S-5 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	20
		CONTRACT NO. 62W21		
ILLINOIS		FED. AID PROJECT		

NOTES:

1. For Expansion Joint Details, see Sheet S-6 to S-11 of S-28.

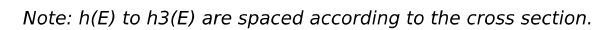
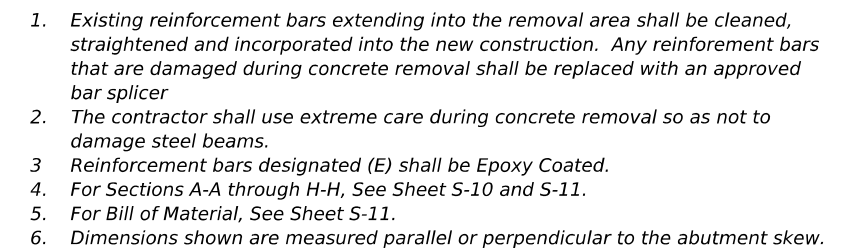
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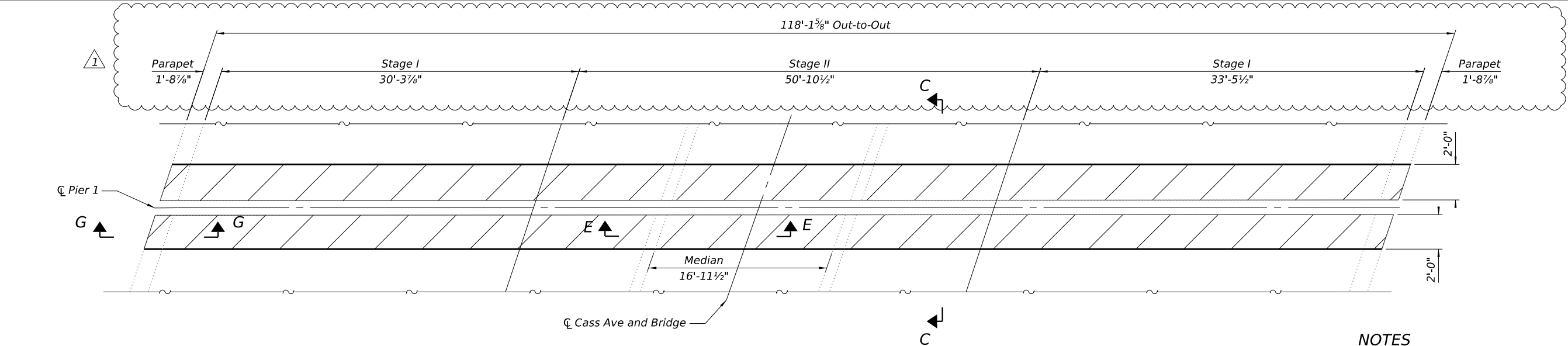
Item	Unit	Quantity
Protective Coat	Sq. Yd.	3,595
Bridge Deck Latex Concrete Overlay, 2¾ Inches	Sq. Yd.	2,618
Bridge Deck Scarification ¾"	Sq. Yd.	2,618
Diamond Grinding (Bridge Section)	Sq. Yd.	2,618

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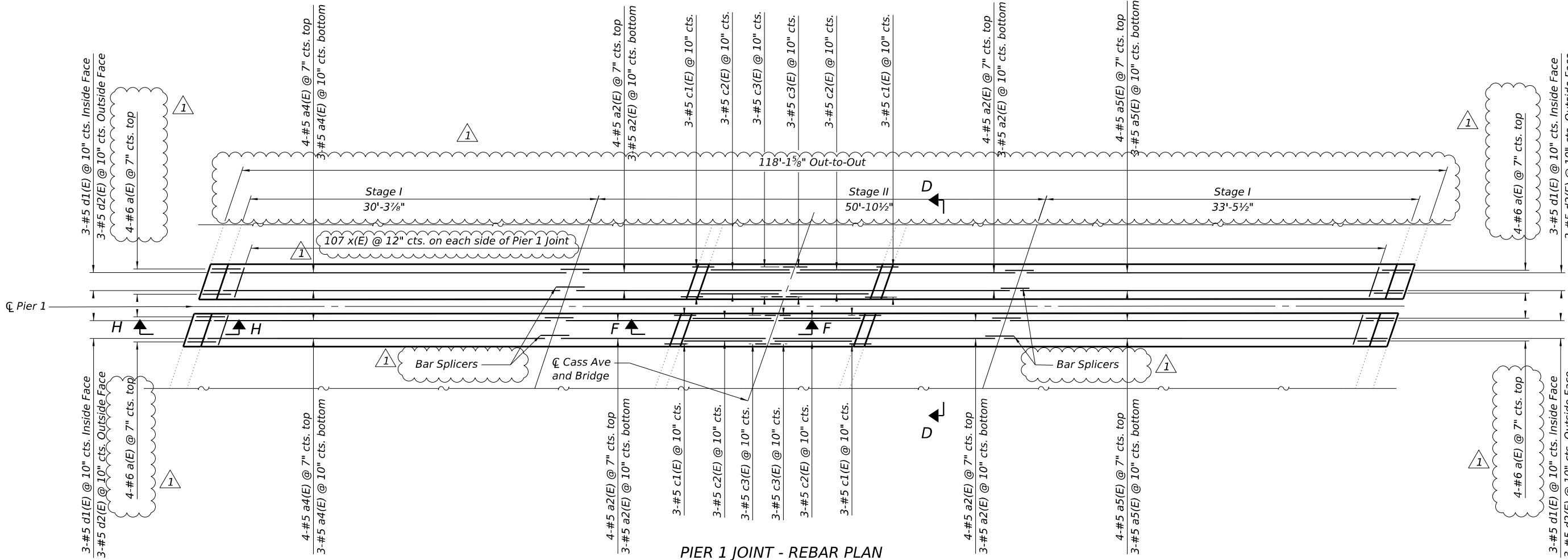
DB STERLIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006





PIER 1 JOINT - CONCRETE REMOVAL PLAN

- NOTES**
- 1. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer
 - 2. The contractor shall use extreme care during concrete removal so as not to damage steel beams.
 - 3. Reinforcement bars designated (E) shall be Epoxy Coated.
 - 4. For Sections A-A through H-H, See Sheets S-10 and S-11.
 - 5. For Bill of Material, See Sheet S-11.
 - 6. Dimensions shown are measured parallel or perpendicular to the abutment skew.



PIER 1 JOINT - REBAR PLAN

FILE NAME: \\camdbstepw21\CS_pdt_work_dfm471113903_6\TASK 4-2W21-SB07-EXP JT-PIER 1.dgn



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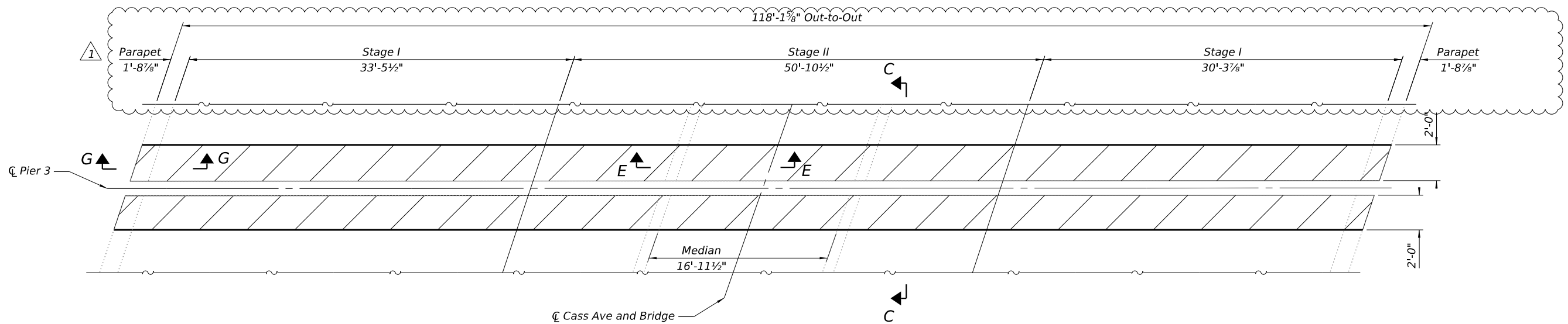
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT PLAN AT PIER 1
STRUCTURE NO. 022-0072

SCALE: 3/16" = 1'-0"

SHEET NO. S-7 OF S-31 SHEETS

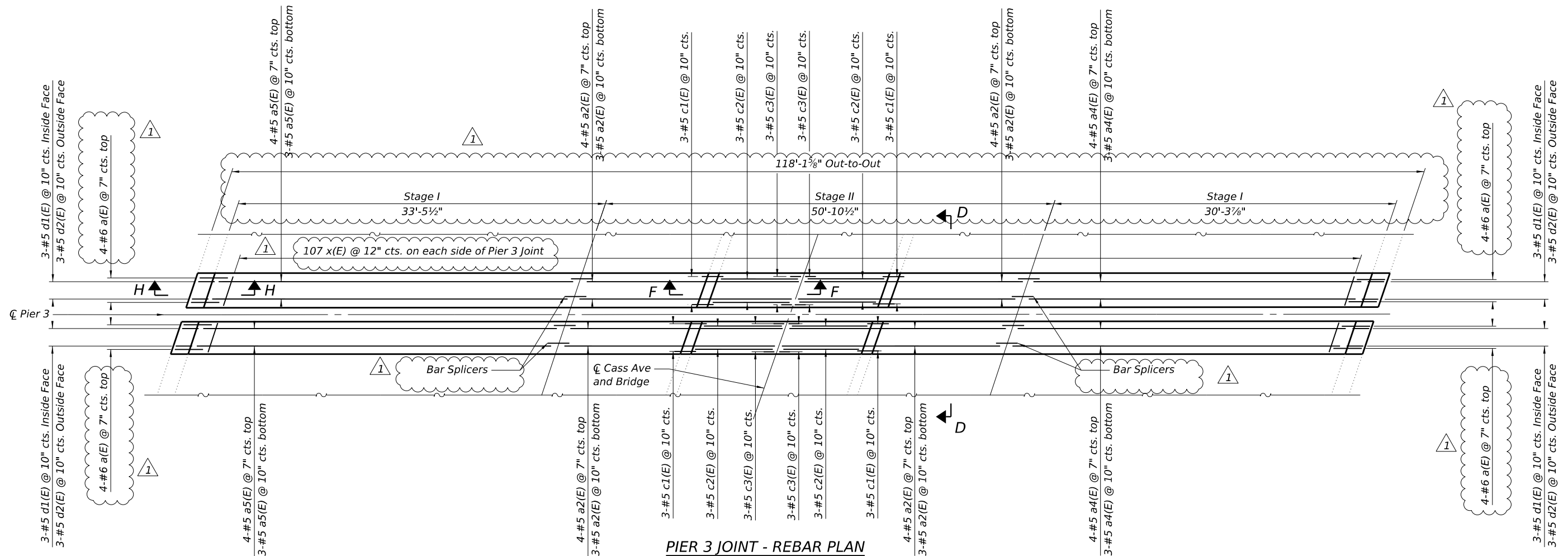
F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	22
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



PIER 3 JOINT - CONCRETE REMOVAL PLAN

NOTES

- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer
- The contractor shall use extreme care during concrete removal so as not to damage steel beams.
- Reinforcement bars designated (E) shall be Epoxy Coated.
- For Sections A-A through H-H, See Sheet S-10 and S-11.
- For Bill of Material, See Sheet S-11.
- Dimensions shown are measured parallel or perpendicular to the abutment skew.



PIER 3 JOINT - REBAR PLAN

FILE NAME: \\lcsM-dbstepw21\UCS_pdt_work_dfn471113903_7\TASK 4-20W21-S000-EXP JT-PIER 3.dgn



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PLOT TIME	= 12:07:49 AM	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT PLAN AT PIER 3
STRUCTURE NO. 022-0072

SCALE: 3/16" = 1'-0"

SHEET NO. S-8 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	23
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

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123 N. Wacker Drive, Suite 2000
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312.857.1006

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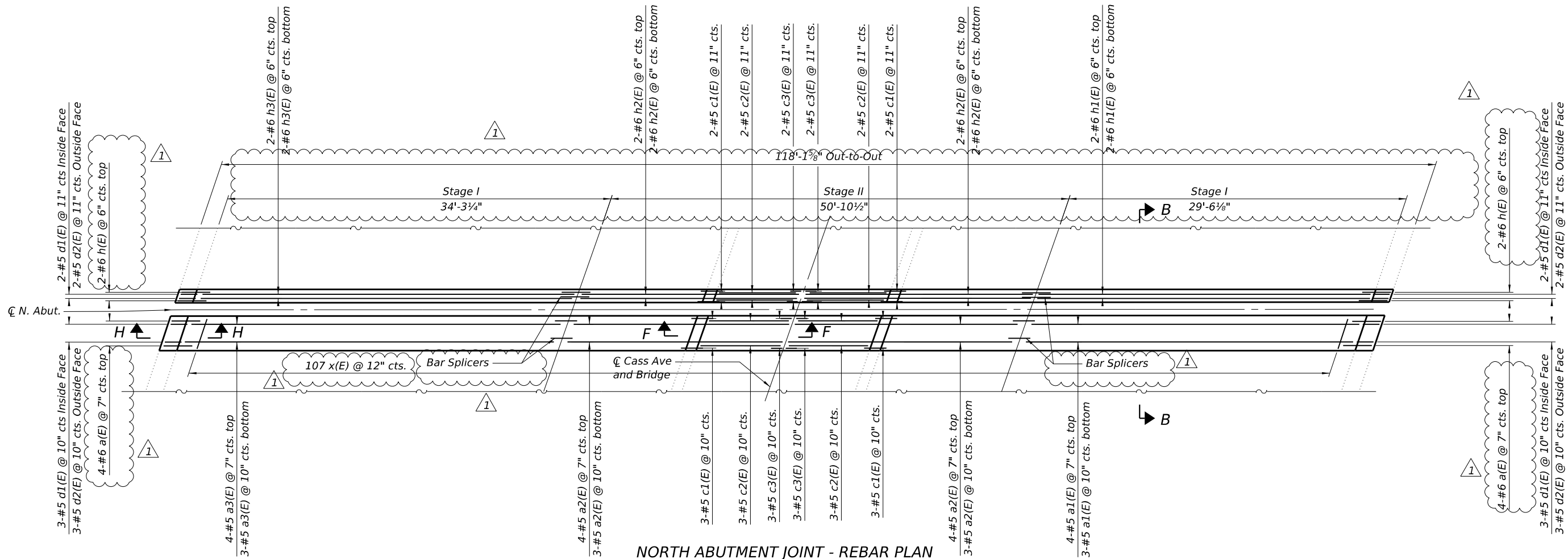
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT PLAN AT NORTH ABUTMENT
STRUCTURE NO. 022-0072

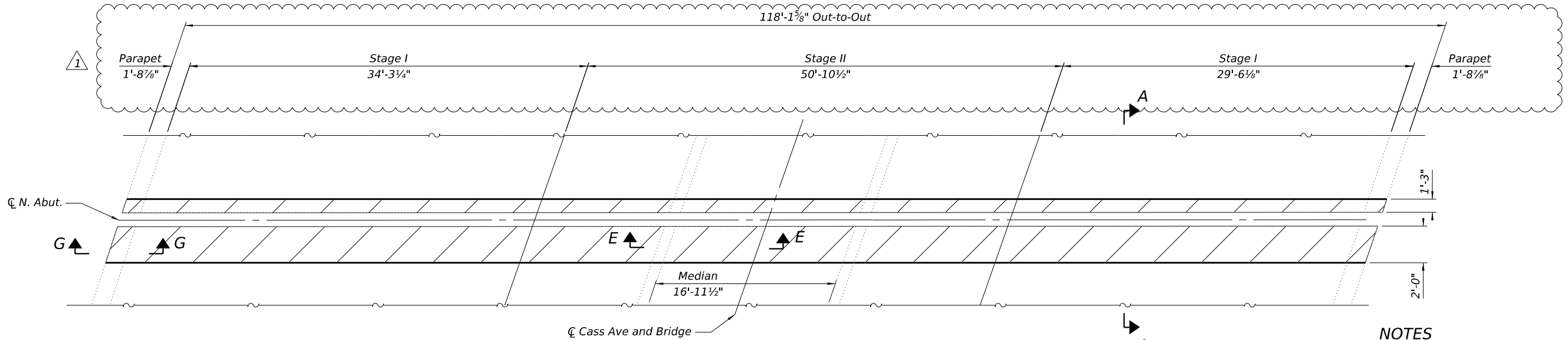
SCALE: 3/16" = 1'-0"

SHEET NO. S-9 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



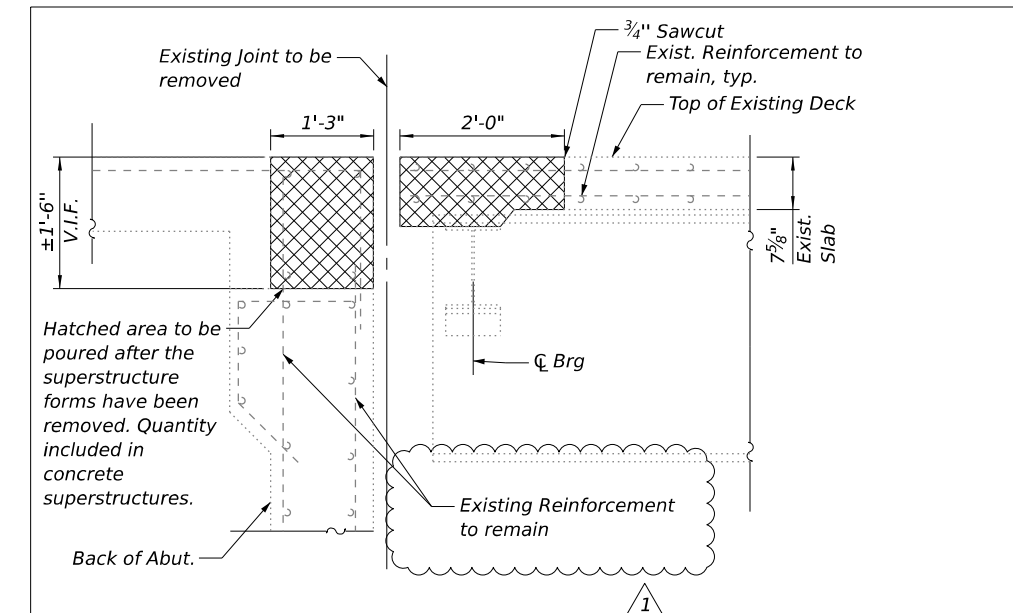
NORTH ABUTMENT JOINT - REBAR PLAN



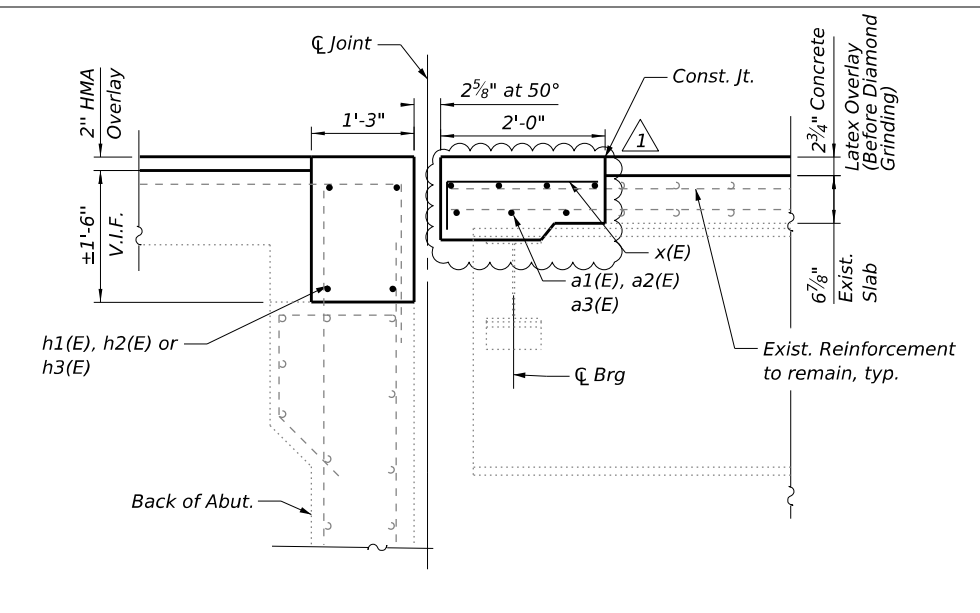
NORTH ABUTMENT JOINT - CONCRETE REMOVAL PLAN

NOTES

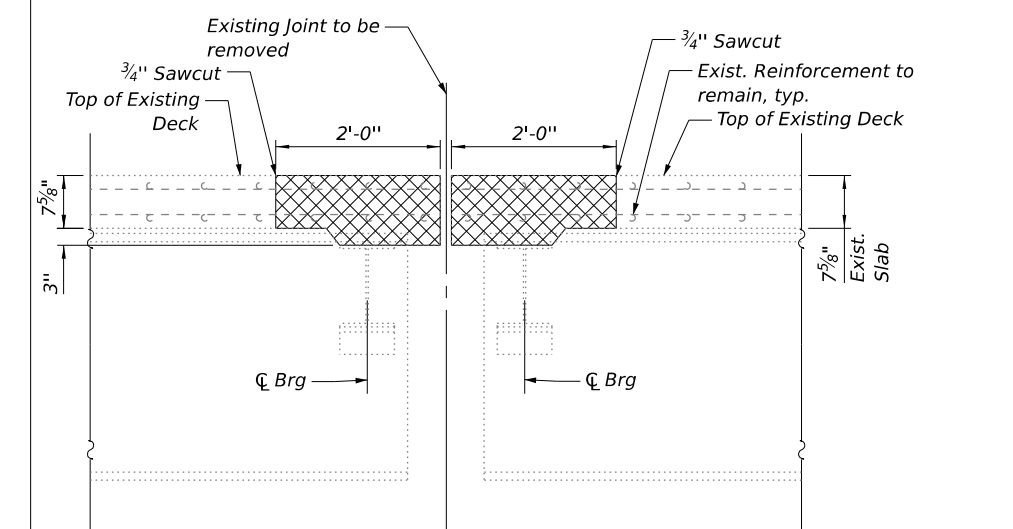
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer
- The contractor shall use extreme care during concrete removal so as not to damage steel beams.
- Reinforcement bars designated (E) shall be Epoxy Coated.
- For Sections A-A through H-H, See Sheet S-10 and S-11.
- For Bill of Material, See Sheet S-11.
- Dimensions shown are measured parallel or perpendicular to the abutment skew.



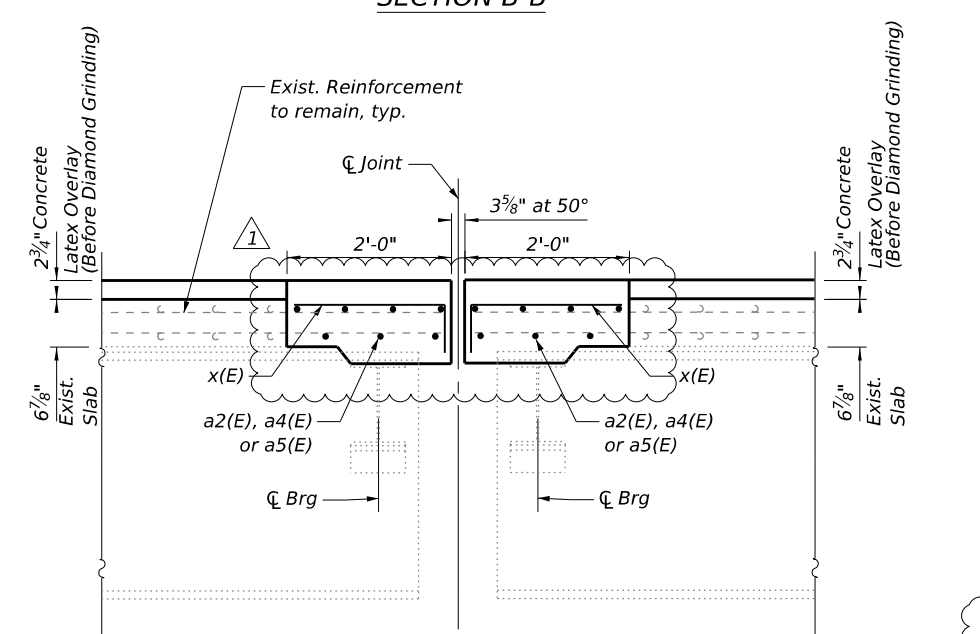
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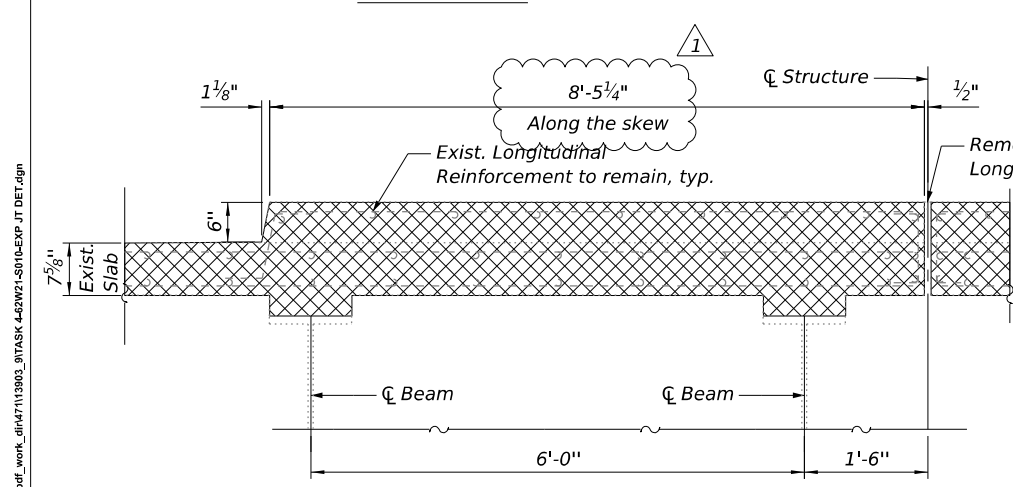
SECTION B-B



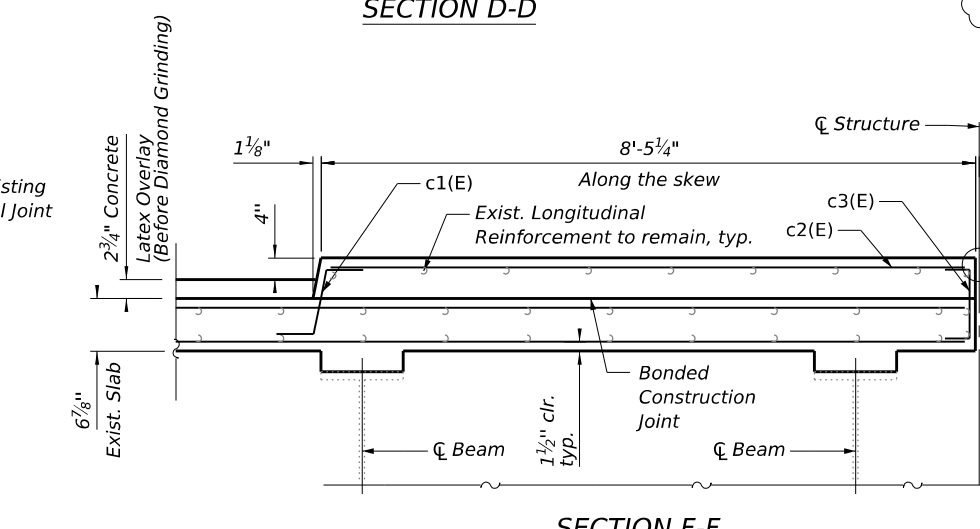
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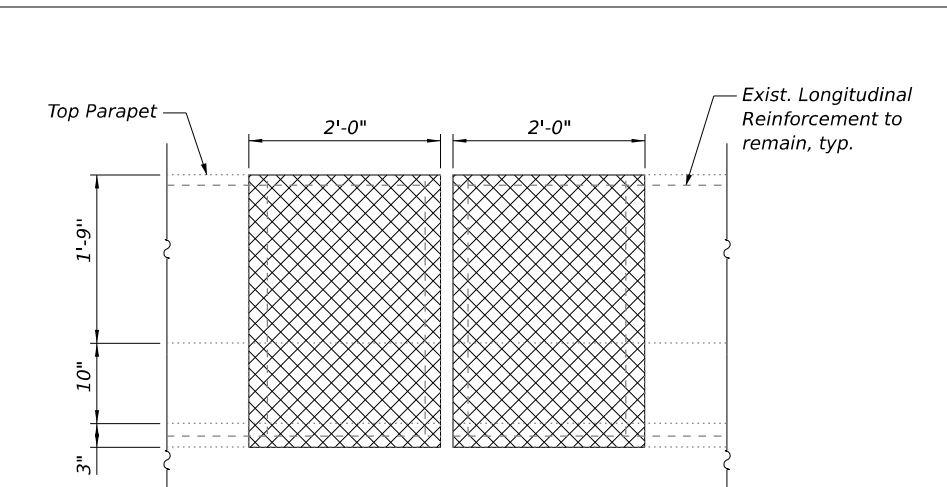
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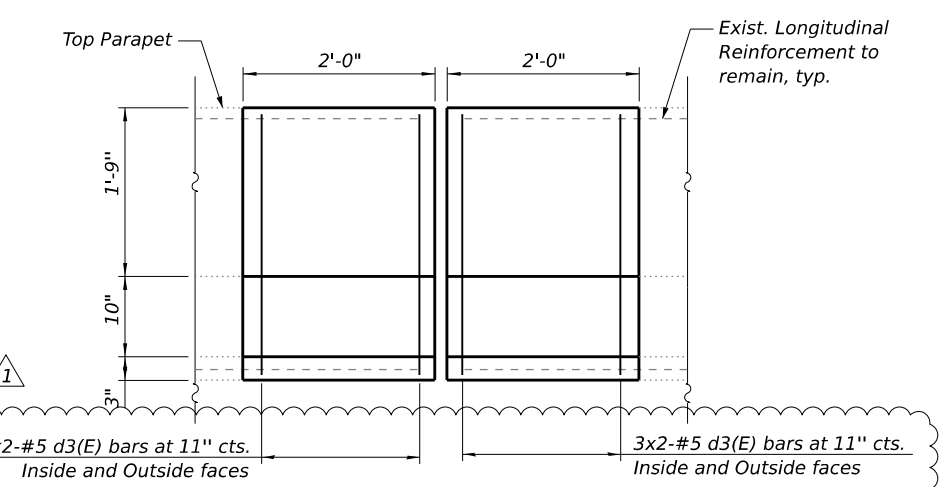
SECTION E-E
(Bridge is symmetrical along CL of structure)



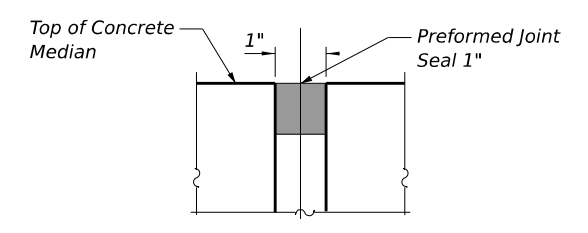
SECTION F-F
(Bridge is symmetrical along CL of structure)



**PARTIAL PARAPET ELEVATION
REMOVAL AT PIERS**



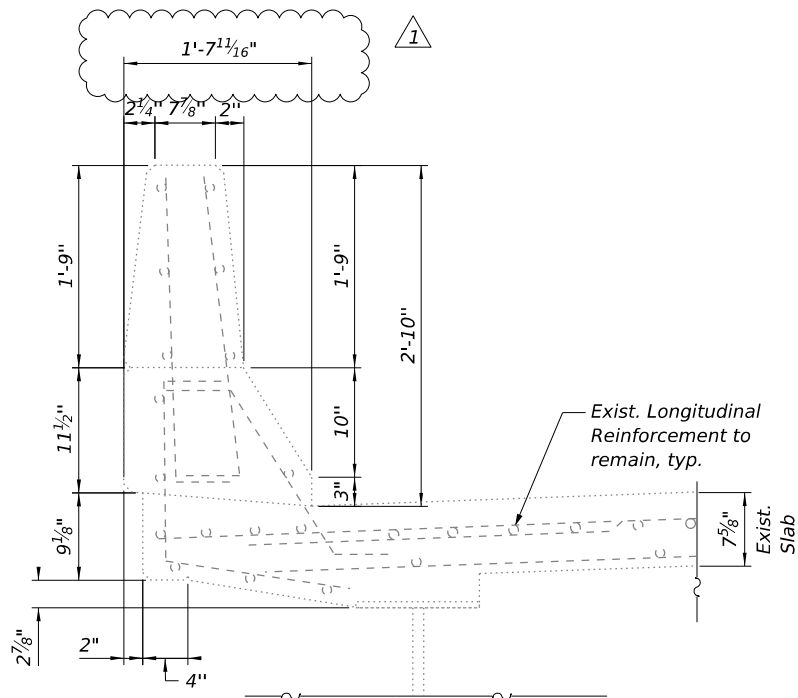
**PARTIAL PARAPET ELEVATION
PROPOSED AT PIERS**



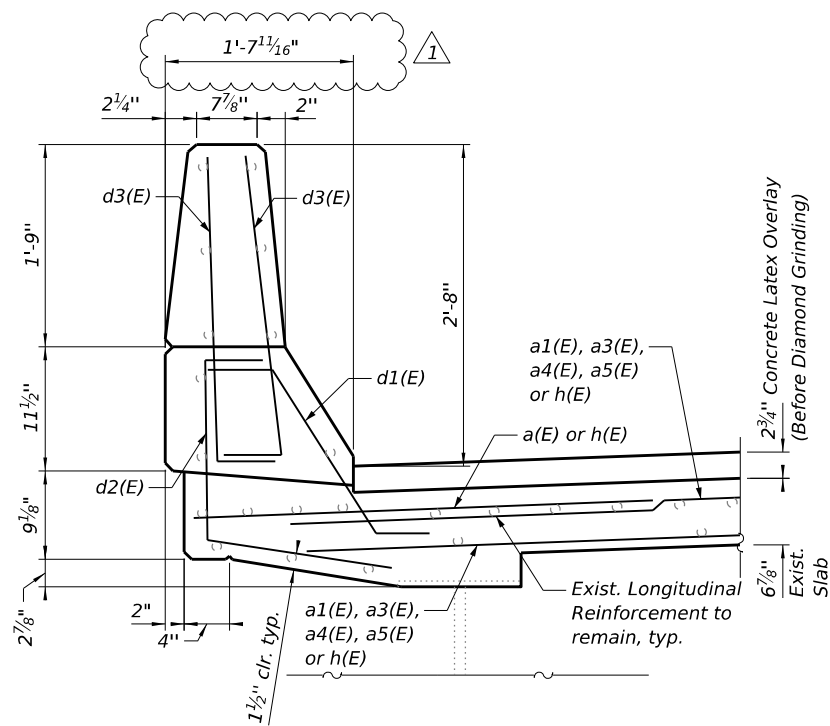
DETAIL 1

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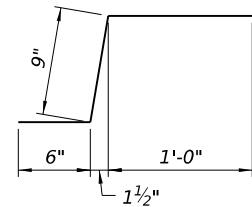
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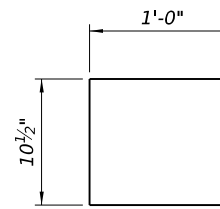
SECTION G-G
SECTION THRU PARAPET - REMOVAL



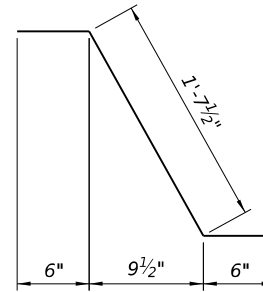
SECTION H-H
SECTION THRU PARAPET - PROPOSED



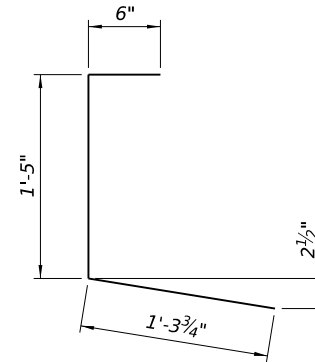
BAR c1(E)



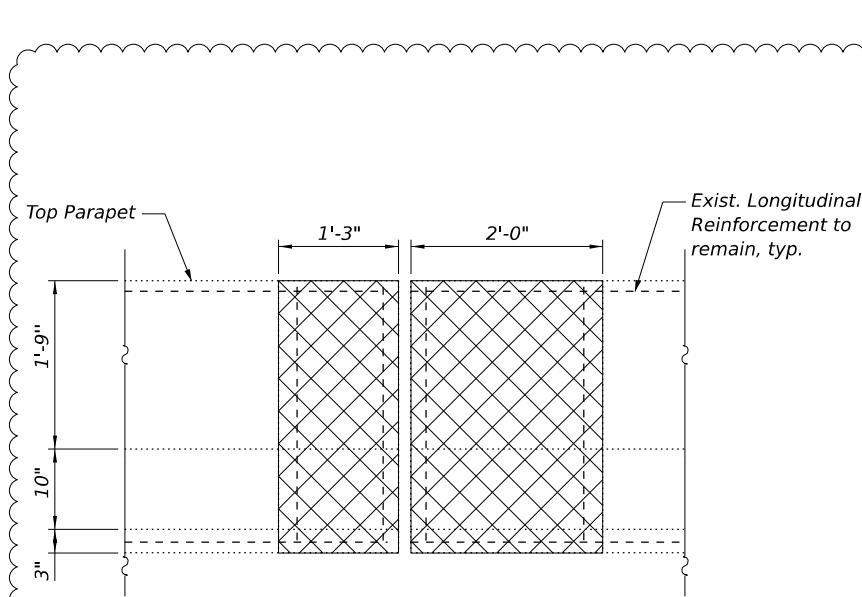
BAR c3(E)



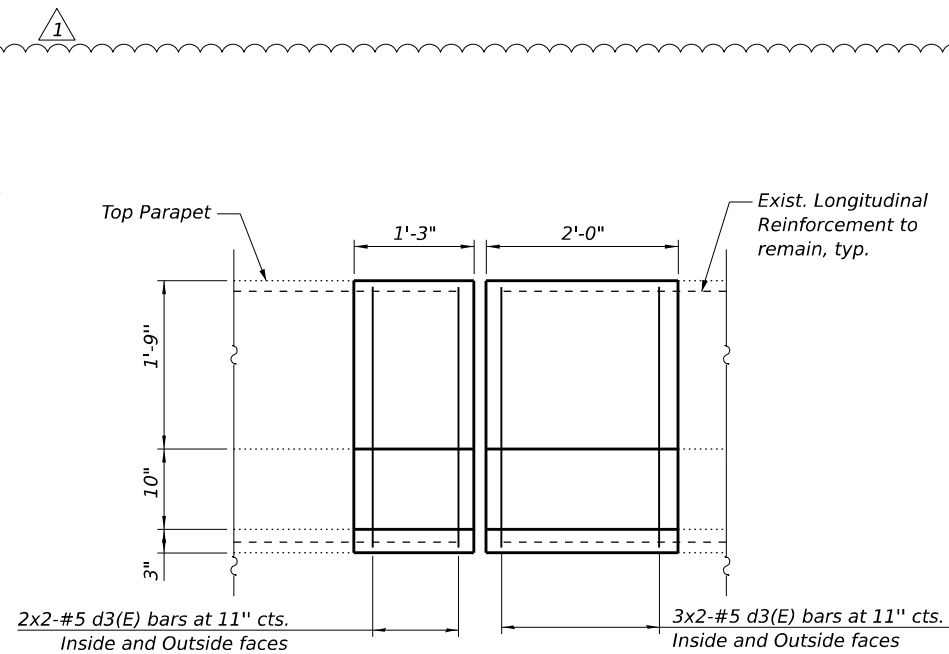
BAR d1(E)



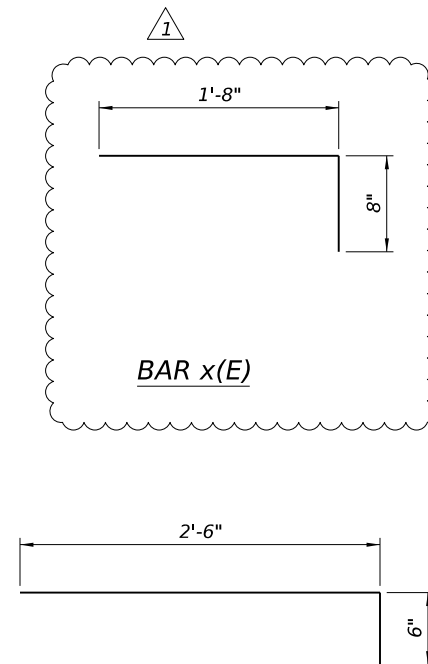
BAR d2(E)



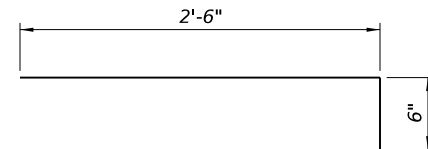
PARTIAL PARAPET ELEVATION
REMOVAL AT ABUTMENTS



PARTIAL PARAPET ELEVATION
PROPOSED AT ABUTMENTS



BAR x(E)



BAR d3(E)

**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	48	#6	6'-0"	
a1(E)	14	#5	29'-8"	
a2(E)	84	#5	25'-1"	
a3(E)	14	#5	34'-5"	
a4(E)	28	#5	30'-7"	
a5(E)	28	#5	33'-6"	
c1(E)	44	#5	2'-3"	
c2(E)	44	#5	8'-1"	
c3(E)	44	#5	2'-11"	
d1(E)	44	#5	2'-8"	
d2(E)	44	#5	3'-3"	
d3(E)	88	#5	3'-0"	
h(E)	8	#6	6'-0"	
h1(E)	8	#6	29'-8"	
h2(E)	16	#6	25'-1"	
h3(E)	8	#6	34'-5"	
x(E)	642	#5	2'-4"	
Reinforcement Bars, Epoxy Coated			Pound	9,430
Concrete Superstructure			Cu. Yd.	74.1
Concrete Removal			Cu. Yd.	62.4
Preformed Joint Seal 1"			Foot	272.0
Bar Splicers			Each	100
Protective Coat			Sq. Yd.	3,595

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT DETAILS (2 OF 2)
STRUCTURE NO. 022-0072

SCALE: SHEET NO. S-11 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				

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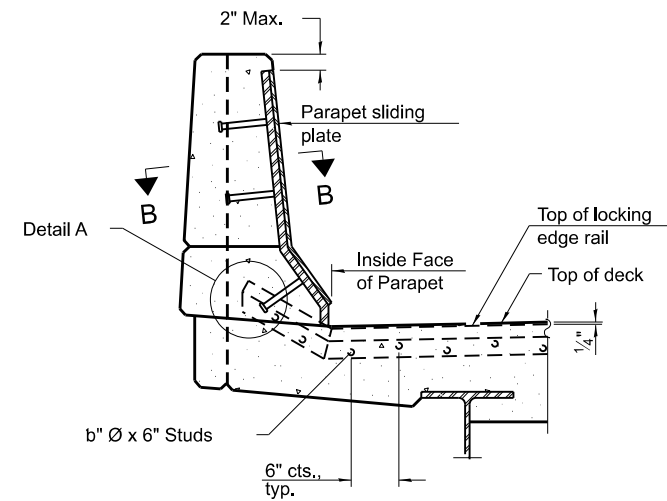
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL (1 OF 2)
STRUCTURE NO. 022-0072

SCALE: NTS

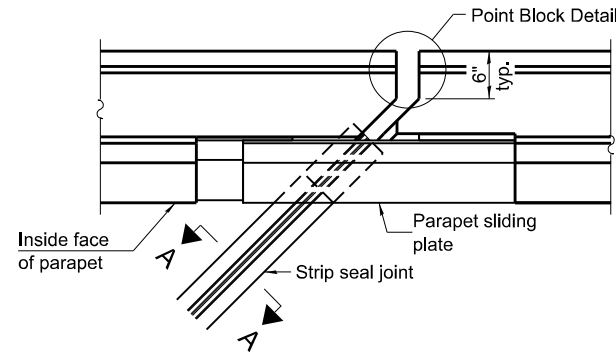
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ILLINOIS FED. AID PROJECT				

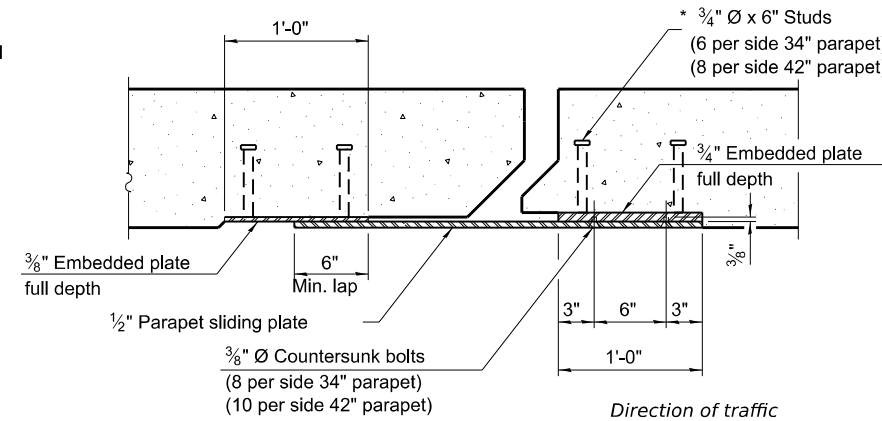


FOR SKEWS $\leq 30^\circ$

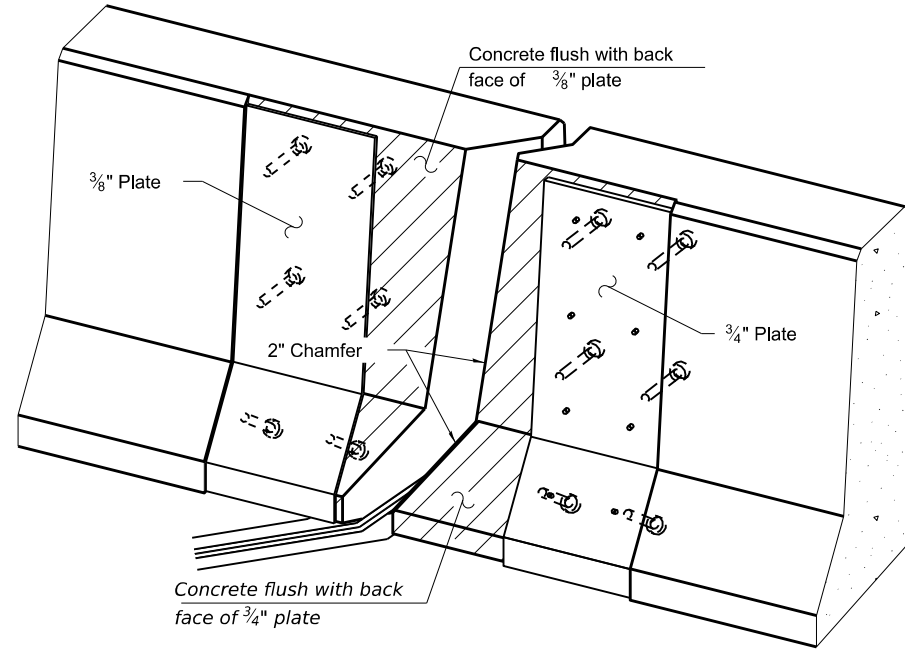
PLAN AT PARAPET



FOR SKEWS $> 30^\circ$



SECTION B-B



Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

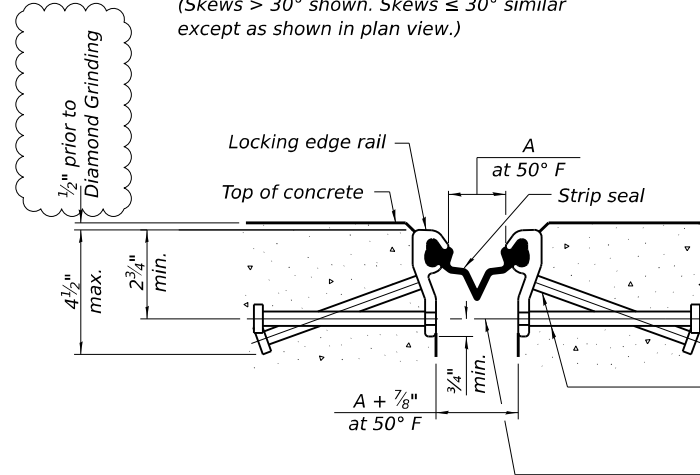
The Maximum space between locking edge rail segments shall be $\frac{1}{2}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

34" F-shape barrier shown, 42" F-shape barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

SECTION AT PARAPET
(Skews $> 30^\circ$ shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



SHOWING ROLLED RAIL JOINT

Location	A
S. Abut	$1\frac{3}{4}$ "
Pier 1	$2\frac{3}{4}$ "
Pier 3	$2\frac{3}{4}$ "
N. Abut	$1\frac{3}{4}$ "

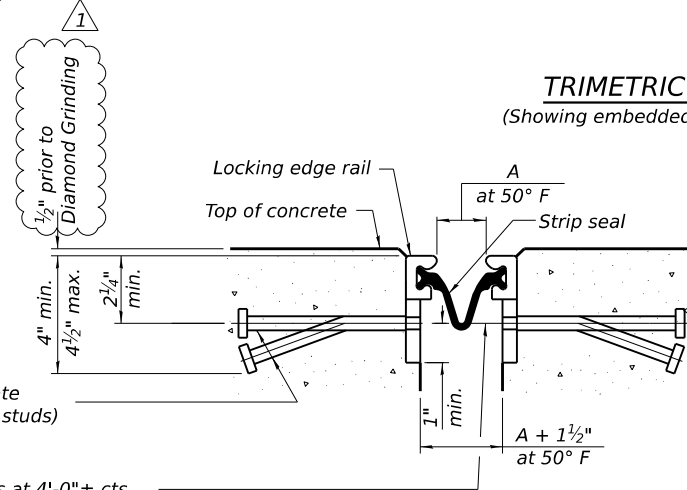
* $\frac{5}{8}$ " \varnothing x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

$\frac{3}{8}$ " \varnothing threaded rods in $\frac{7}{16}$ " \varnothing holes at $4'-0"$ \pm cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

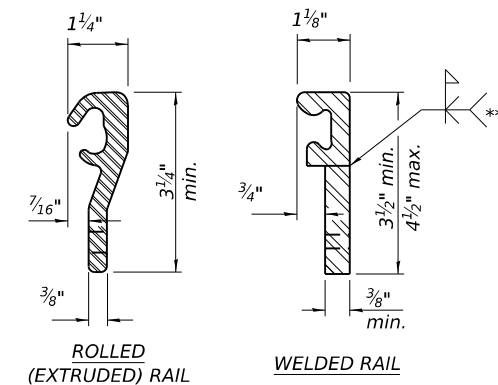
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

DETAIL A



SHOWING WELDED RAIL JOINT

TRIMETRIC VIEW
(Showing embedded plates only)



LOCKING EDGE RAILS

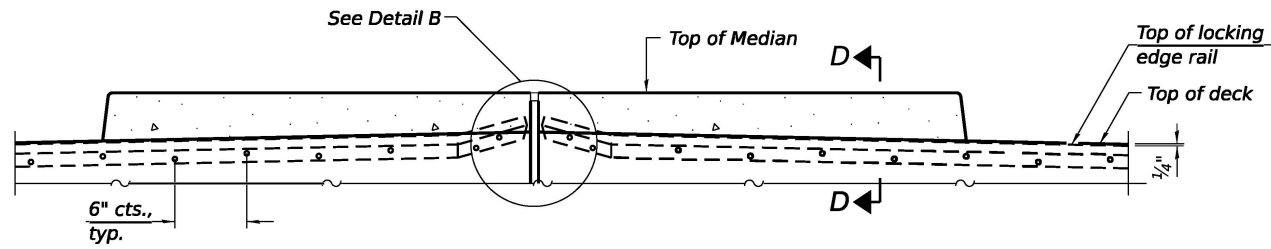
** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

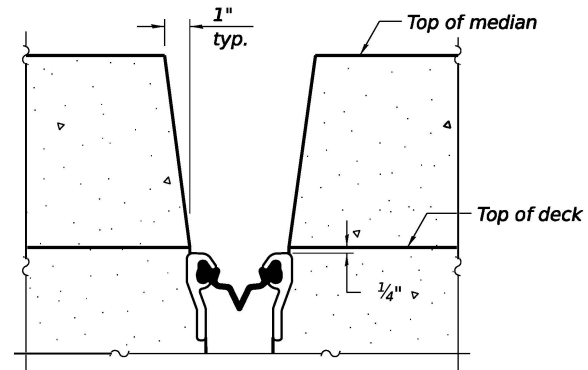
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

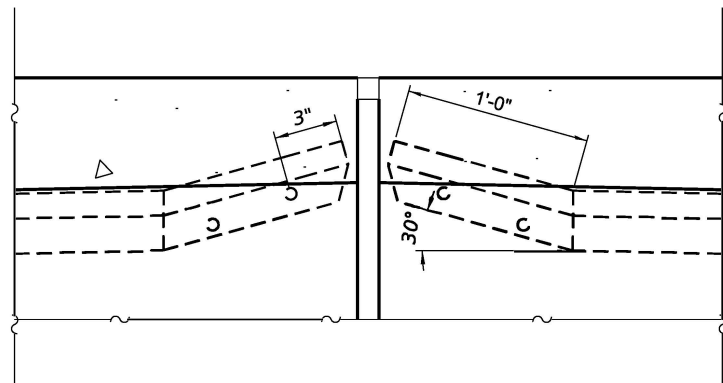
Item	Unit	Total
Preformed Joint Strip Seal	Foot	473



SECTION AT MEDIAN
For skews > 30°, chamfer acute corners 2" similar to sidewalk.



SECTION D-D
(at Rt. L's)



DETAIL B

FILE NAME: \\saw-k-dbs\p21\UCS_p21\work_d\147113903_3\TASK_4\2W21-S012a-PREF EX- JT - 2.dgn



DB STERLIN CONSULTANTS, INC.
1231 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1000

USER NAME =	lcaill@dbstapw218	DESIGNED -	AH	REVISED -	06-28-2025
PLOT SCALE =		DRAWN -	AH	REVISED -	
PLOT DATE =	6/27/2025	CHECKED -	BJN	REVISED -	
PLOT TIME =	12:08:18 AM	DATE -		REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL (2 OF 2)
STRUCTURE NO. 022-0072

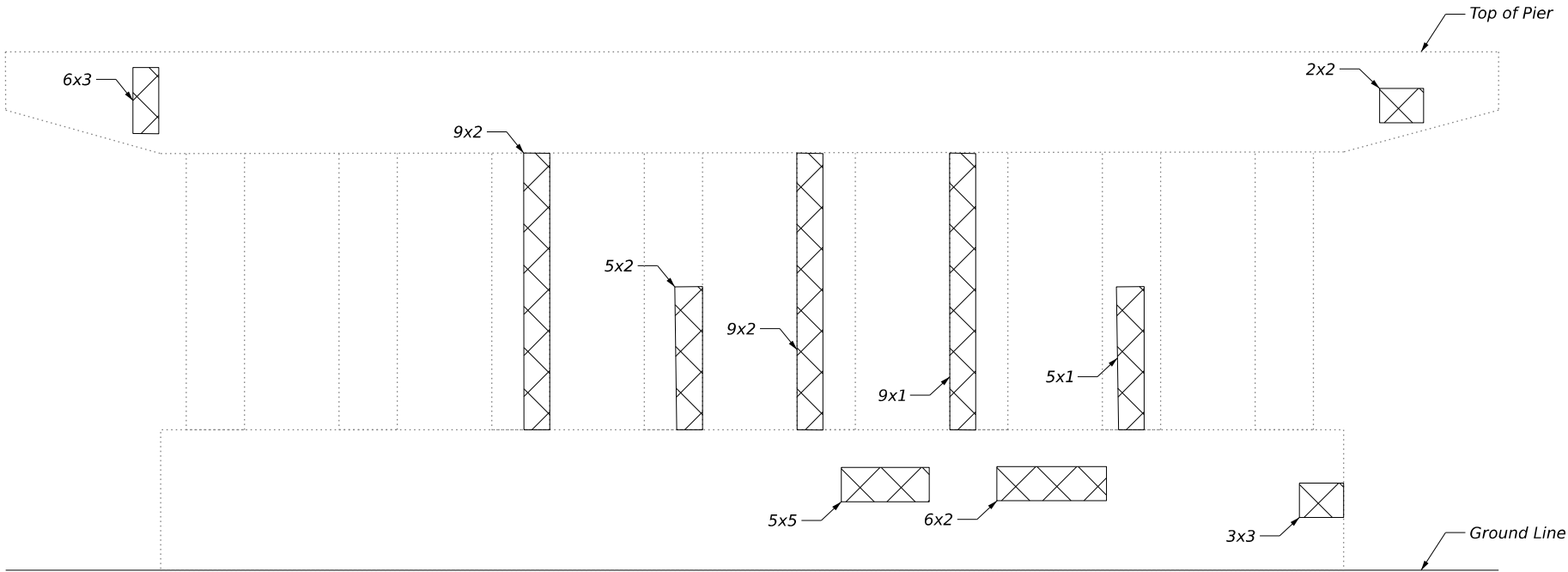
SCALE: NTS

SHEET NO. S-12A OF S-31 SHEETS

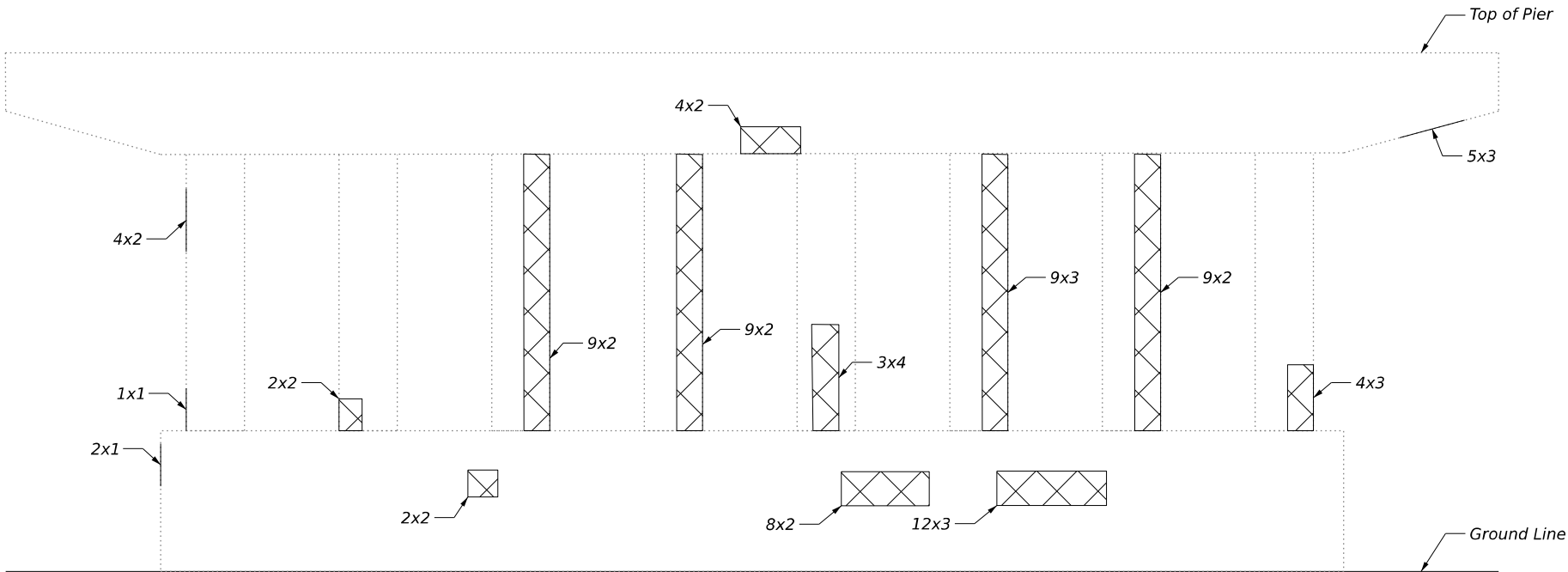
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	27A
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

FILE NAME: \\cam-dbstepw21\CS_perf_work_dir\33213903_12\TASK_4-02W21-S014-DRAIN_SCUP.dgn

FILE NAME: \\icaM-dbstepw21\CS_pdf_work_dfr47113903_131TASK_4-02W21-S015-PIER REPAIRS-1.dgn



PIER 1
(Looking North)



PIER 1
(Looking South)

NOTES

- Pier and abutment repair areas are estimated based on inspection information received. Actual repair areas and locations shall be determined by the Engineer. The Contractor shall be paid for the actual amount of repairs made and at the bid price for the respective pay item.
- Existing reinforcement bars that are cut or damaged during repair shall be replaced in kind and lapped with existing rebar or replaced with an approved bar splicer or anchorage system at the contractor's expense. No welding of rebar shall be allowed.

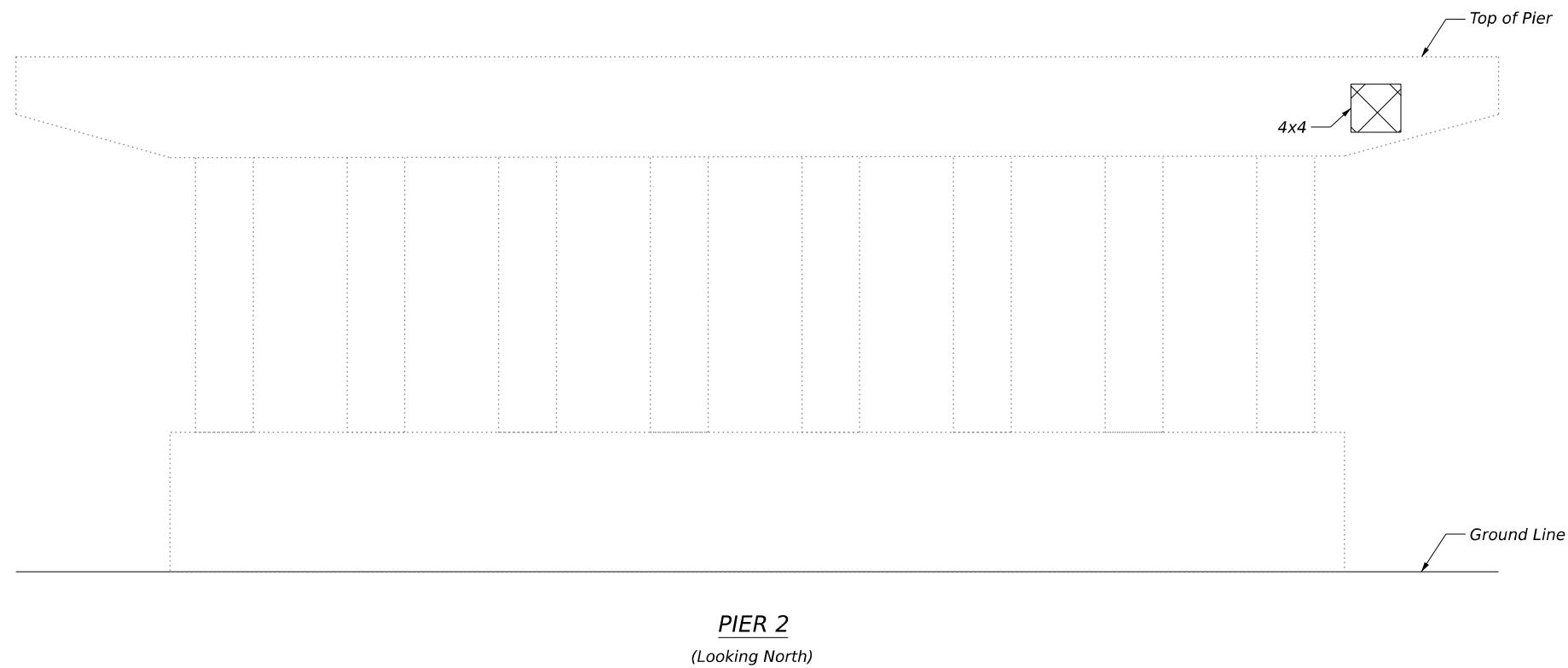
1

LEGEND



Structural Repair of Concrete (Depth Equal to less than 5 inches)

DBS DB STERLIN CONSULTANTS, INC. 123 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.857.1006	USER NAME = lcsM-dbstepw21\$	DESIGNED - MM	REVISED - 06-26-2025	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PIER SUBSTRUCTURE REPAIRS (1 OF 3) STRUCTURE NO. 022-0072		F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN - ANH	REVISED -				55	FAI-55-23	DU PAGE	60	30
	PLOT DATE = 6/27/2025	CHECKED - BJN	REVISED -		CONTRACT NO. 62W21						
	PLOT TIME = 12:06:05 AM	DATE -	REVISED -		SCALE:	SHEET NO. S-15 OF S-31 SHEETS	ILLINOIS FED. AID PROJECT				



1. *Pier and abutment repair areas are estimated based on inspection information received. Actual repair areas and locations shall be determined by the Engineer. The Contractor shall be paid for the actual amount of repairs made and at the bid price for the respective pay item.*
2. *Existing reinforcement bars that are cut or damaged during repair shall be replaced in kind and lapped with existing rebar or replaced with an approved bar splicer or anchorage system at the contractor's expense. No welding of rebar shall be allowed.*



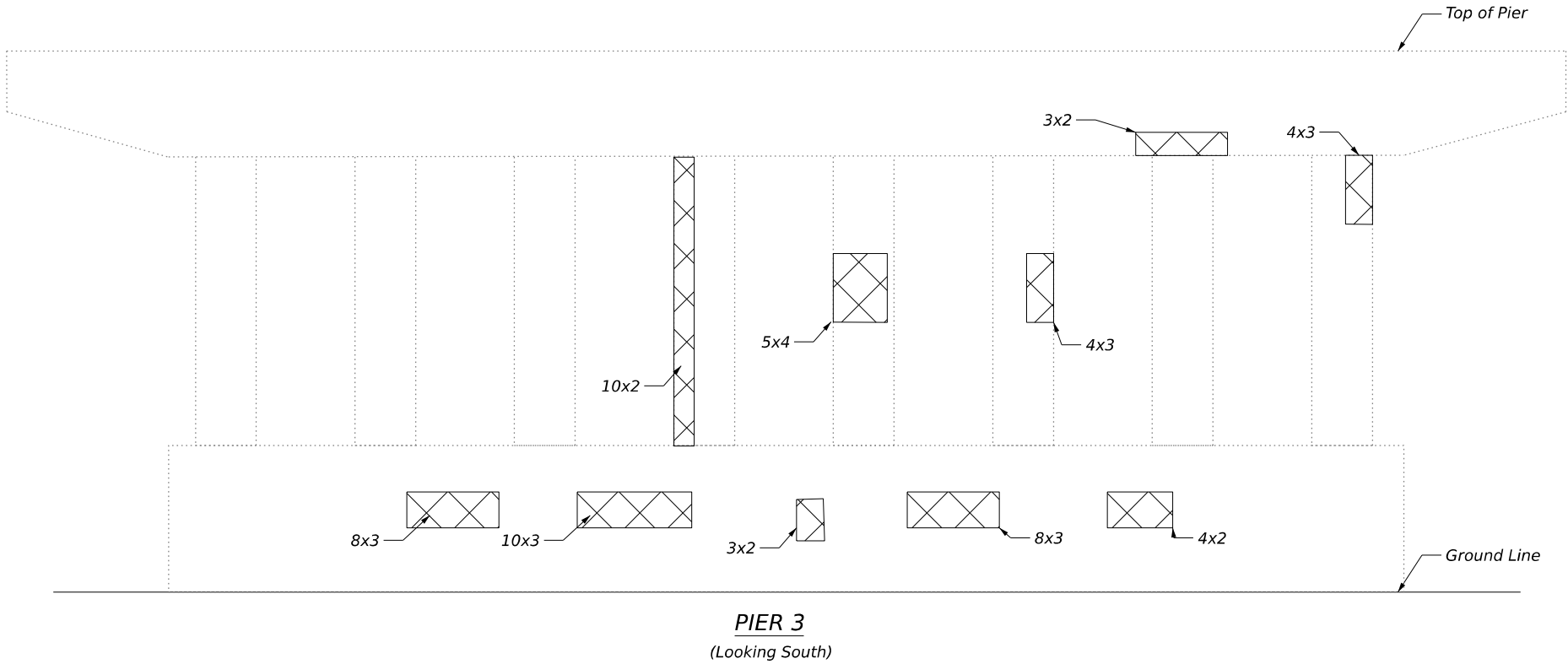
A triangle with the number 1 inside it.

Structural Repair of Concrete (Depth Equal to less than 5 inches)

FILE NAME: \\icaM-dbstepw21\CS_pdf_work_dfr47113903_151TASK_4-02W21-S017-PIER REPAIRS-3.dgn

USER NAME	= lcsM-dbstepw21\$	DESIGNED	- MM	REVISED	- 06-26-2025
PLOT SCALE	=	DRAWN	- ANH	REVISED	-
PLOT DATE	= 6/27/2025	CHECKED	- BJN	REVISED	-
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F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	32
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



BILL OF MATERIAL

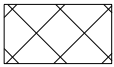
Item	Unit	Total
Structural Repair of Concrete ≤ 5"	Sq. Ft.	732
Concrete Sealer	Sq. Ft.	732

NOTES

- Pier and abutment repair areas are estimated based on inspection information received. Actual repair areas and locations shall be determined by the Engineer. The Contractor shall be paid for the actual amount of repairs made and at the bid price for the respective pay item.
- Existing reinforcement bars that are cut or damaged during repair shall be replaced in kind and lapped with existing rebar or replaced with an approved bar splicer or anchorage system at the contractor's expense. No welding of rebar shall be allowed.

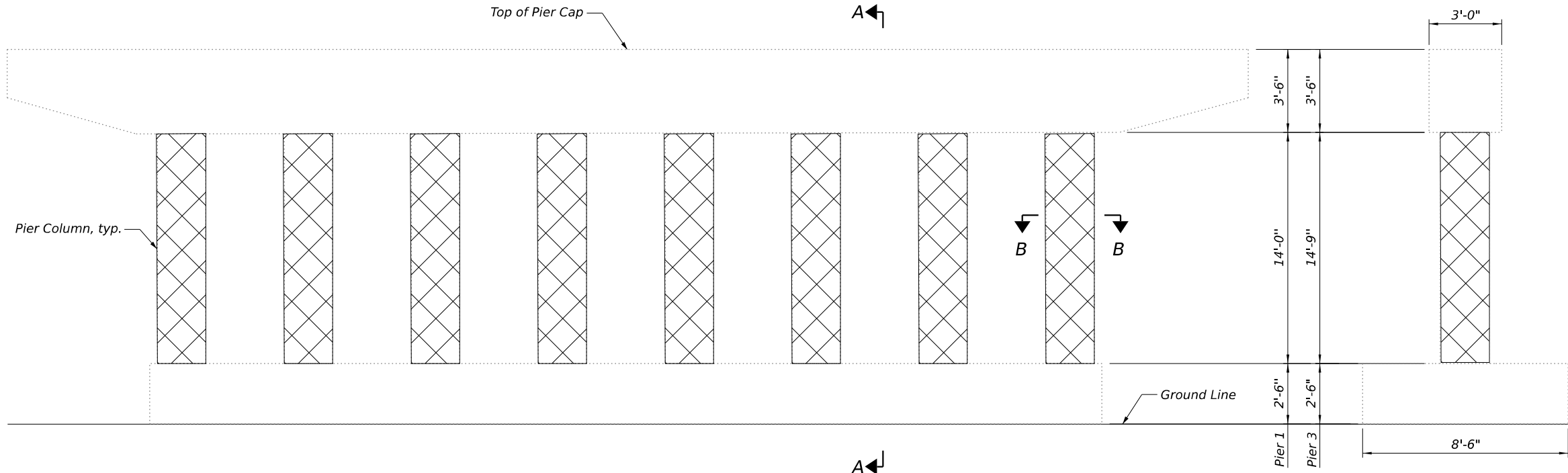
1

LEGEND



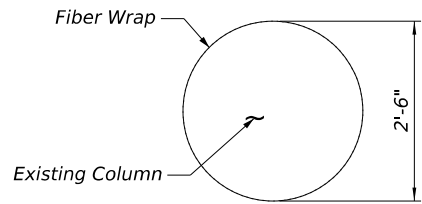
Structural Repair of Concrete (Depth Equal to less than 5 inches)

FILE NAME: \\csM-dbstepw21\CS_pdt_work_dir\33213903_16\TASK_4-2W21-S016-COL FIBER WRAP.dgn



PIER 1 - FIBER WRAP
(Pier 3 similar except as indicated)

SECTION A-A



SECTION B-B

LEGEND



Limits of Fiber Wrap

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Fiber Wrap	Sq Ft	1,810
Acrylic Coating	Sq Yd	210

NOTES

- Acrylic Coating shall be applied to all surfaces of the fiber wrap.



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123 N. Wacker Drive, Suite 2000
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312.857.1006

USER NAME	= lcsM-dbstepw21\$	DESIGNED	- MM	REVISED	-
PLOT SCALE	=	DRAWN	- MM	REVISED	-
PLOT DATE	= 5/14/2025	CHECKED	- BJN	REVISED	-
PLOT TIME	= 9:50:40 PM	DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

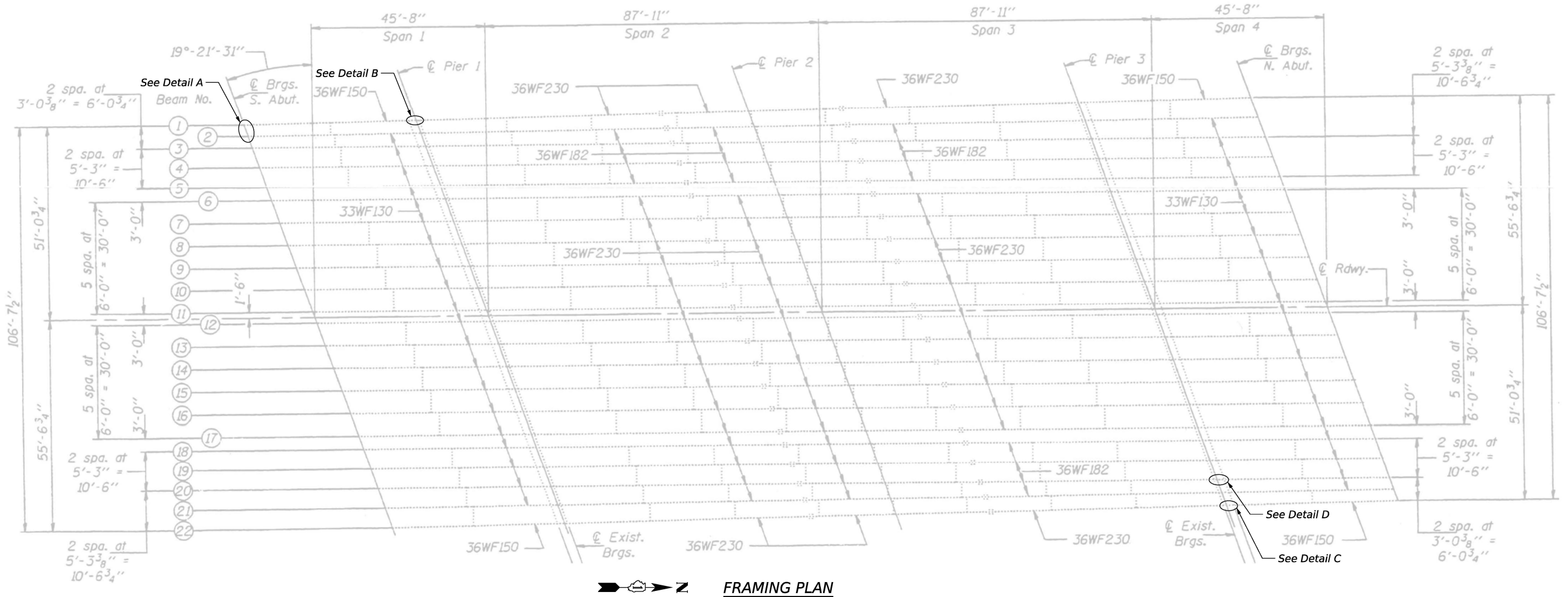
PIER SUBSTRUCTURE REPAIRS - COLUMN FIBER WRAP
STRUCTURE NO. 022-0072

SCALE:

SHEET NO. S-18 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	33
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

FILE NAME: \\cam-dbsstepw21\CS_pdf_work_dir\32113903_311TASK 4-62W21-S018-STRUCTURAL STEEL REPAIRS-1.dgn



NOTES

- All work shall conform to; Section 501 Removal of Existing Structures, Section 505 Steel Structures, Section 506.07 Shop Cleaning New Structures, Section 506.08 Painting Systems for New Structures, Section 506.09 Shop Painting New Structures and Section 506.10 Field Cleaning and Painting New Structures of the Standard Specifications for Road and Bridge Construction by Illinois Department of Transportation, latest edition. The color of the top coat shall match the color of the existing adjacent top coat.
- No repairs to structural steel shall be started or performed until lane closures are in effect above the locations of the work and lane shall remain closed at all times until the repair work is complete. No traffic or construction equipment live load shall be located in the closed lane.

BILL OF MATERIAL

Item	Unit	Total
STRUCTURAL STEEL REPAIRS	Pounds	1,360



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312.857.1006

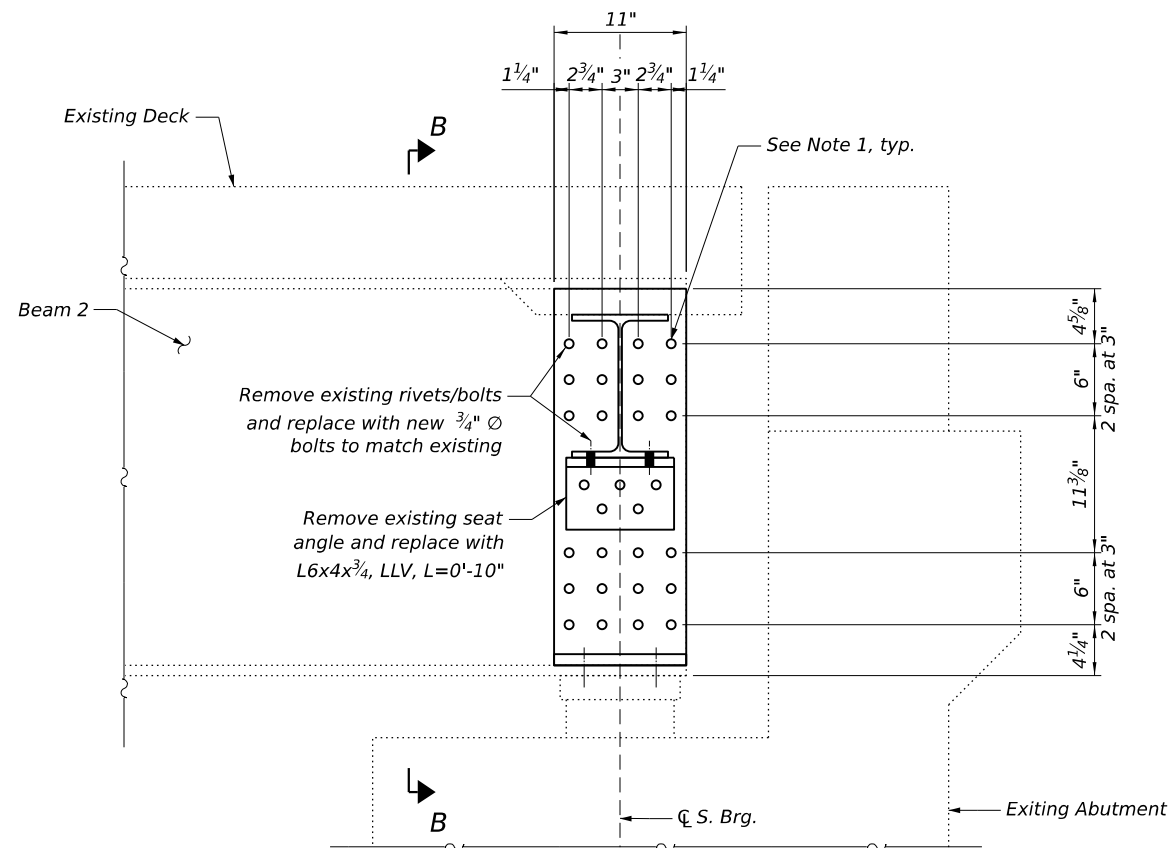
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PLOT DATE =	5/14/2025	CHECKED -	BJN	REVISED -	
PLOT TIME =	9:45:21 PM	DATE -		REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

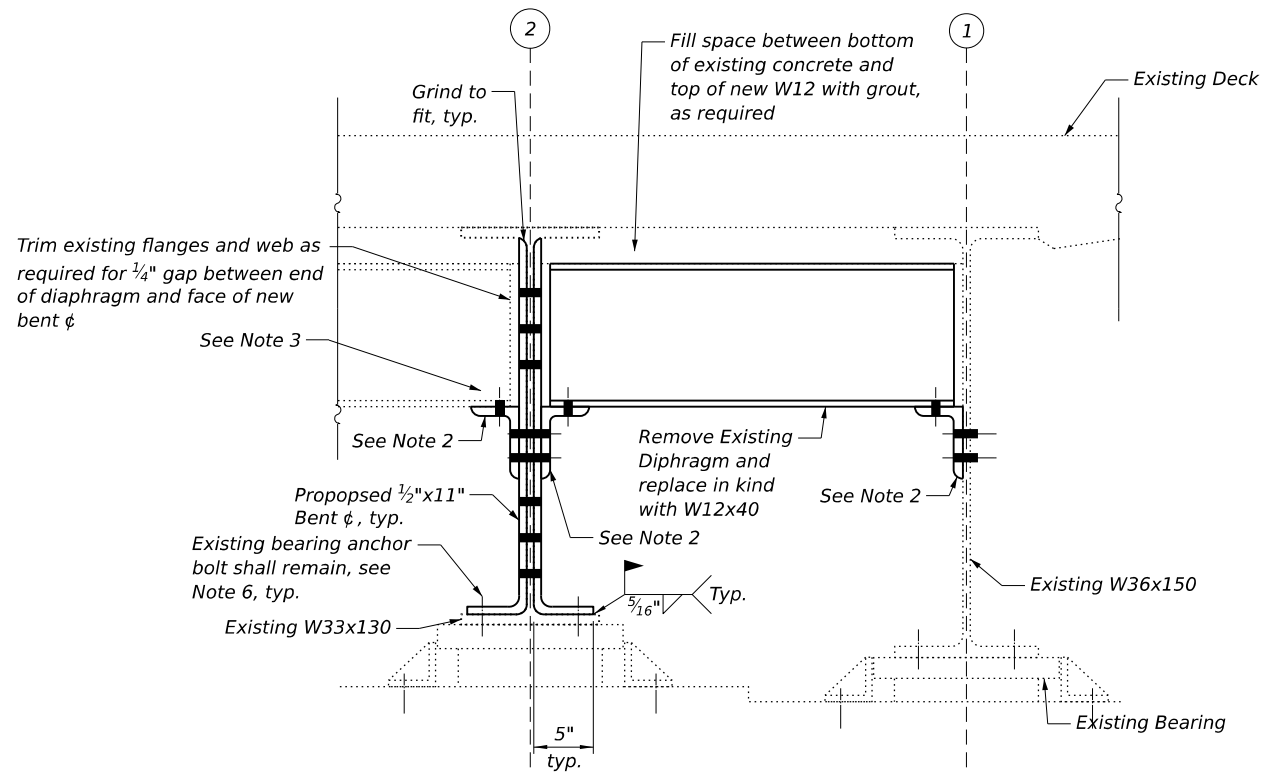
STRUCTURAL STEEL REPAIRS (1 OF 3)
STRUCTURE NO. 022-0072

SCALE: SHEET NO. S-19 OF S-31 SHEETS

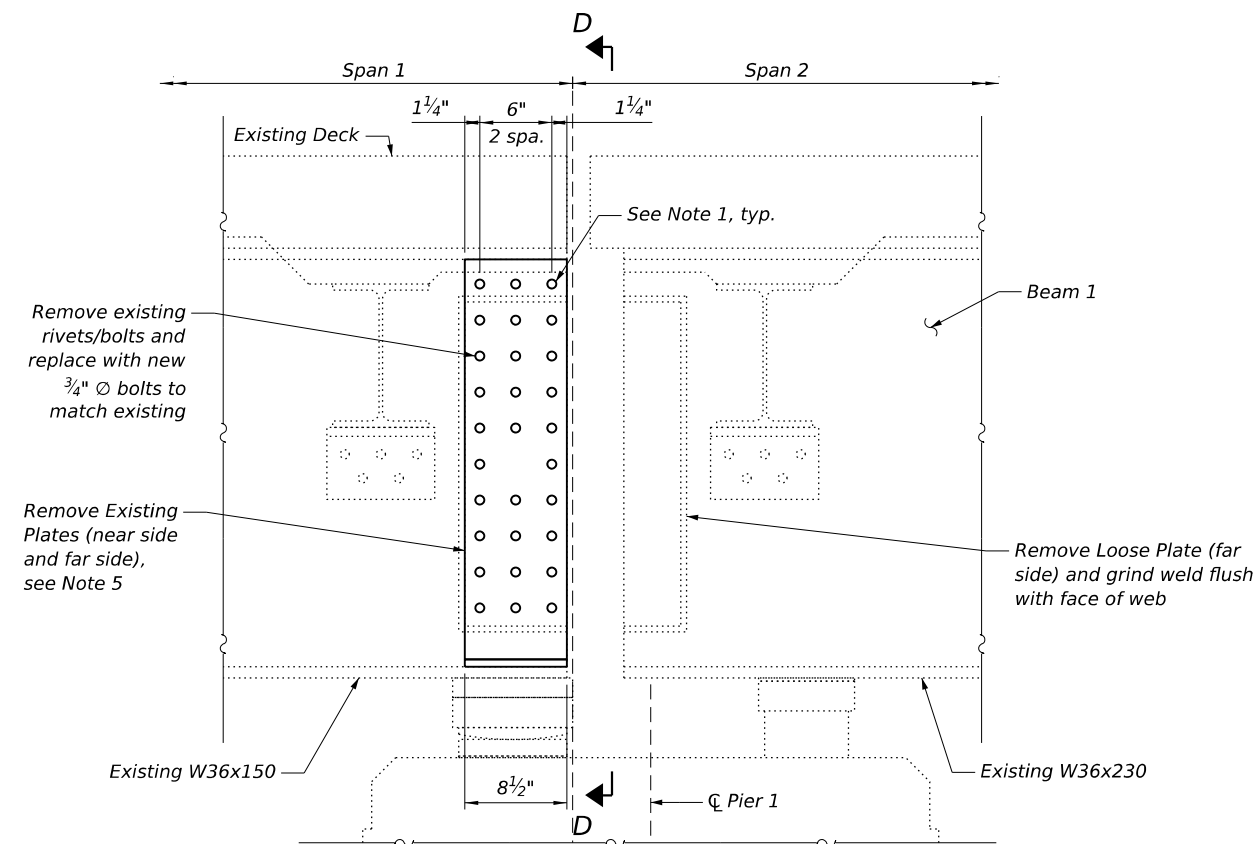
F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23		60	34
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



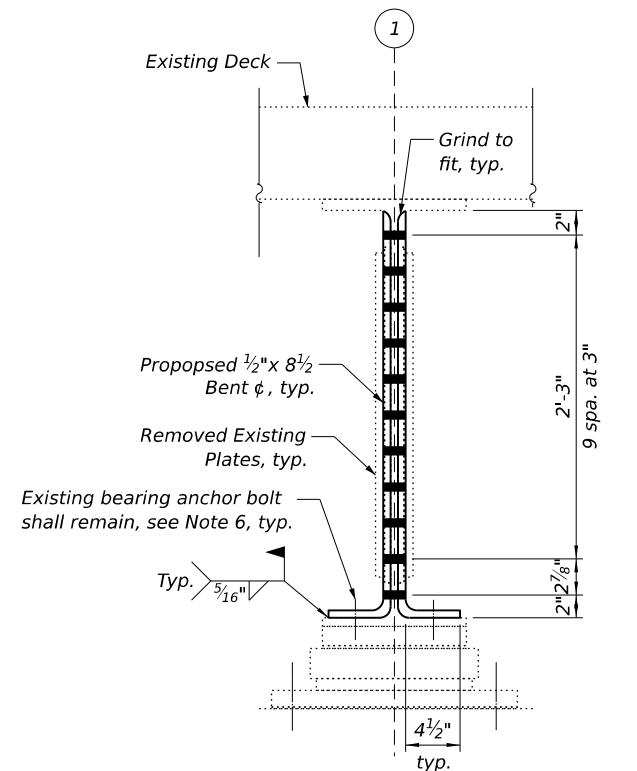
SECTION A-A (Looking East)



SECTION B-B (Looking South)



SECTION C-C (Looking West)



SECTION D-D (Looking South)

NOTES

1. All new holes shall be $\frac{13}{16}$ " ϕ for $\frac{3}{4}$ " ϕ ASTM A325 fasteners unless noted otherwise.
2. Remove existing seat angle and replace with L6x4x $\frac{3}{4}$, LLV, L=0'-10". New hole locations to match existing hole locations.
3. Temporarily shore existing diaphragm as required.
4. All new steel material shall be ASTM A36, Fy = 36 ksi. All new steel shall be coated to match existing coating.
5. Existing web plates shall be removed at location of proposed bent plate by grinding (or use of cutting wheel or similar) existing welds flush with face of web without damaging existing beam web.
6. Remove existing anchor bolt hex nut and washer to allow installation of proposed bent plate. If anchor bolt is not long enough, remove entire anchor bolt from tapped hole in bearing plate and replace in kind with longer anchor bolt.

FILE NAME: \\camdbstepw21\CS_perf_work_dir\33213903_301TASK_4-2W21-5020-STRUCTURAL STEEL REPAIRS-2.dgn

DBS

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122 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

USER NAME = lcsMdbstepw21\$
PLOT SCALE =
PLOT DATE = 5/14/2025
PLOT TIME = 9:45:48 PM

DESIGNED - CM
DRAWN - CM
CHECKED - BJN
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURAL STEEL REPAIRS (2 OF 3)
STRUCTURE NO. 022-0072

SCALE:

SHEET NO. S-20 OF S-31 SHEETS

F.A.I.
RTE

55

SECTION
FAI-55-23

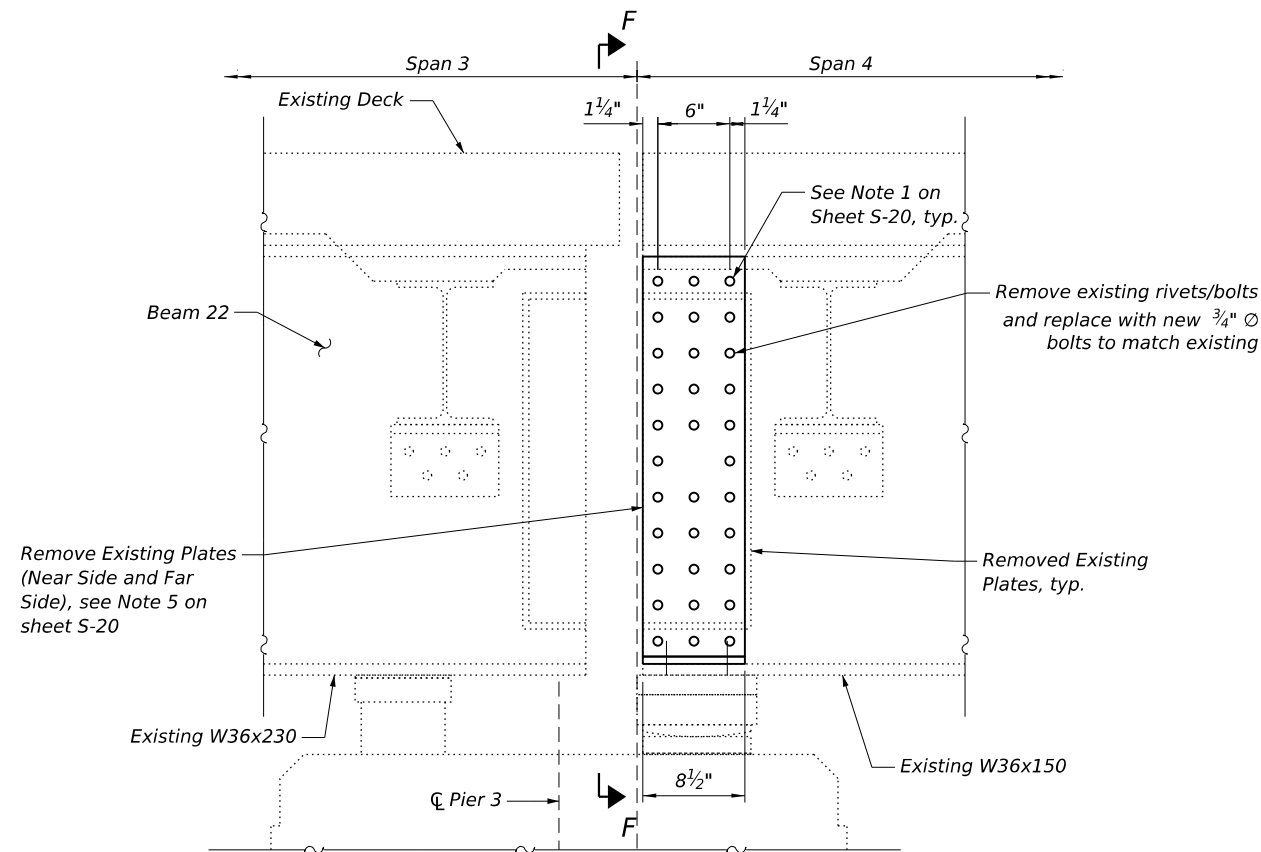
COUNTY
DU PAGE

TOTAL
SHEETS 60

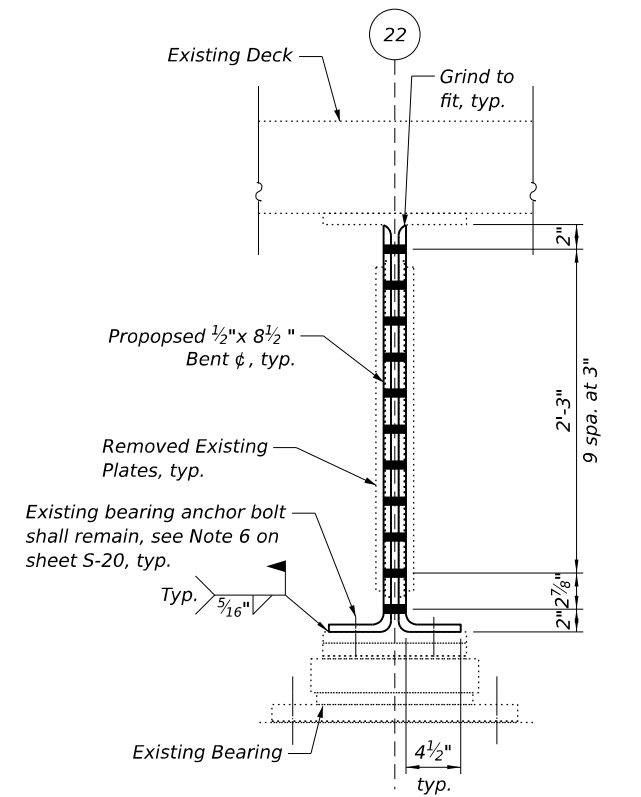
SHEET
NO. 35

CONTRACT NO. 62W21

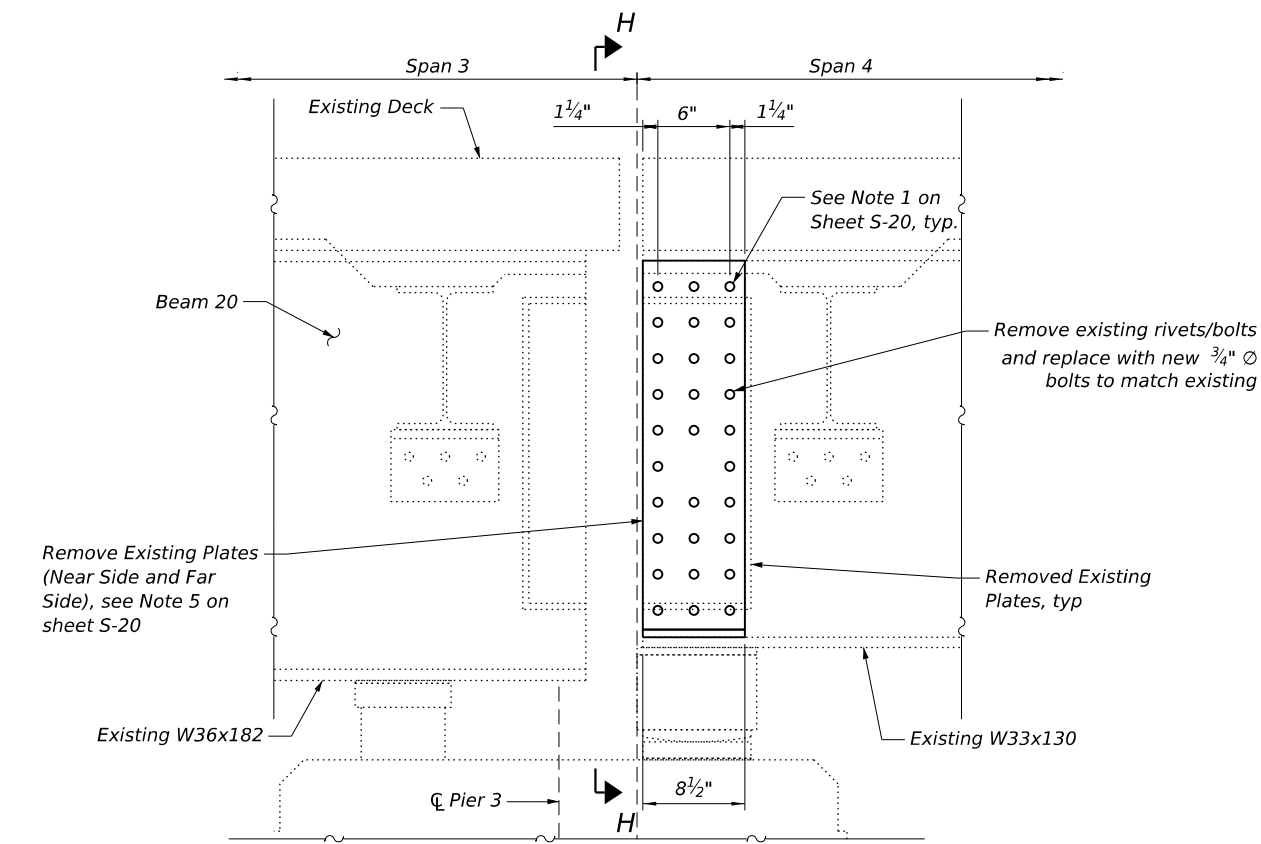
ILLINOIS FED. AID PROJECT



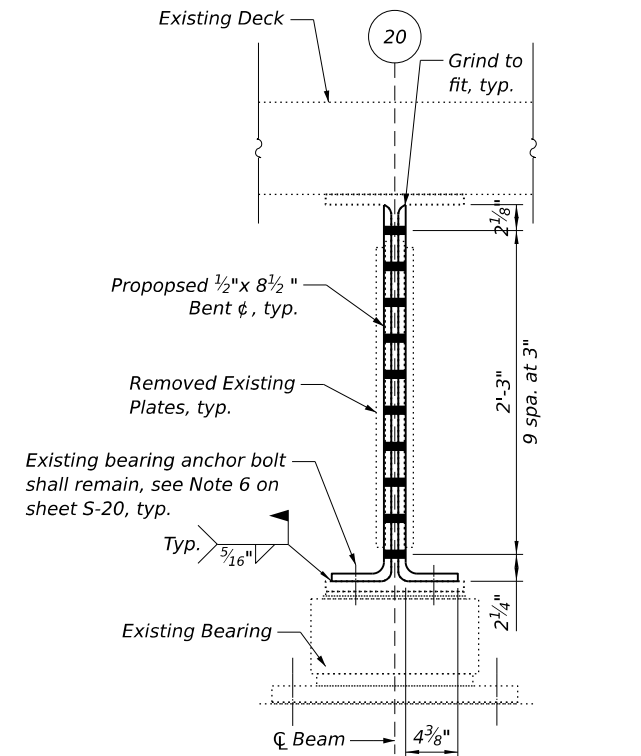
SECTION E-E (Looking West)



SECTION F-F (Looking North)



SECTION G-G (Looking West)



SECTION H-H

NOTES

- Refer to sheet S-20 for notes for additional information.

DETAIL D

DETAIL C

FILE NAME: \\camdbstepw21\CS_pdt_work_dir\332113903_331TASK_4-2W21-S021-STRUCTURAL STEEL REPAIRS-3.dgn



DB STERLIN CONSULTANTS, INC.
1221 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

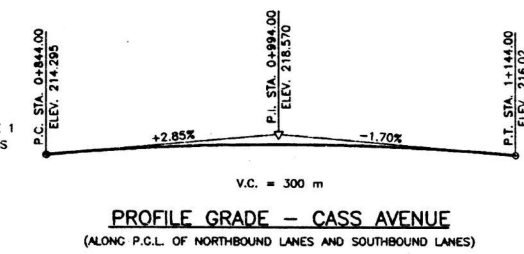
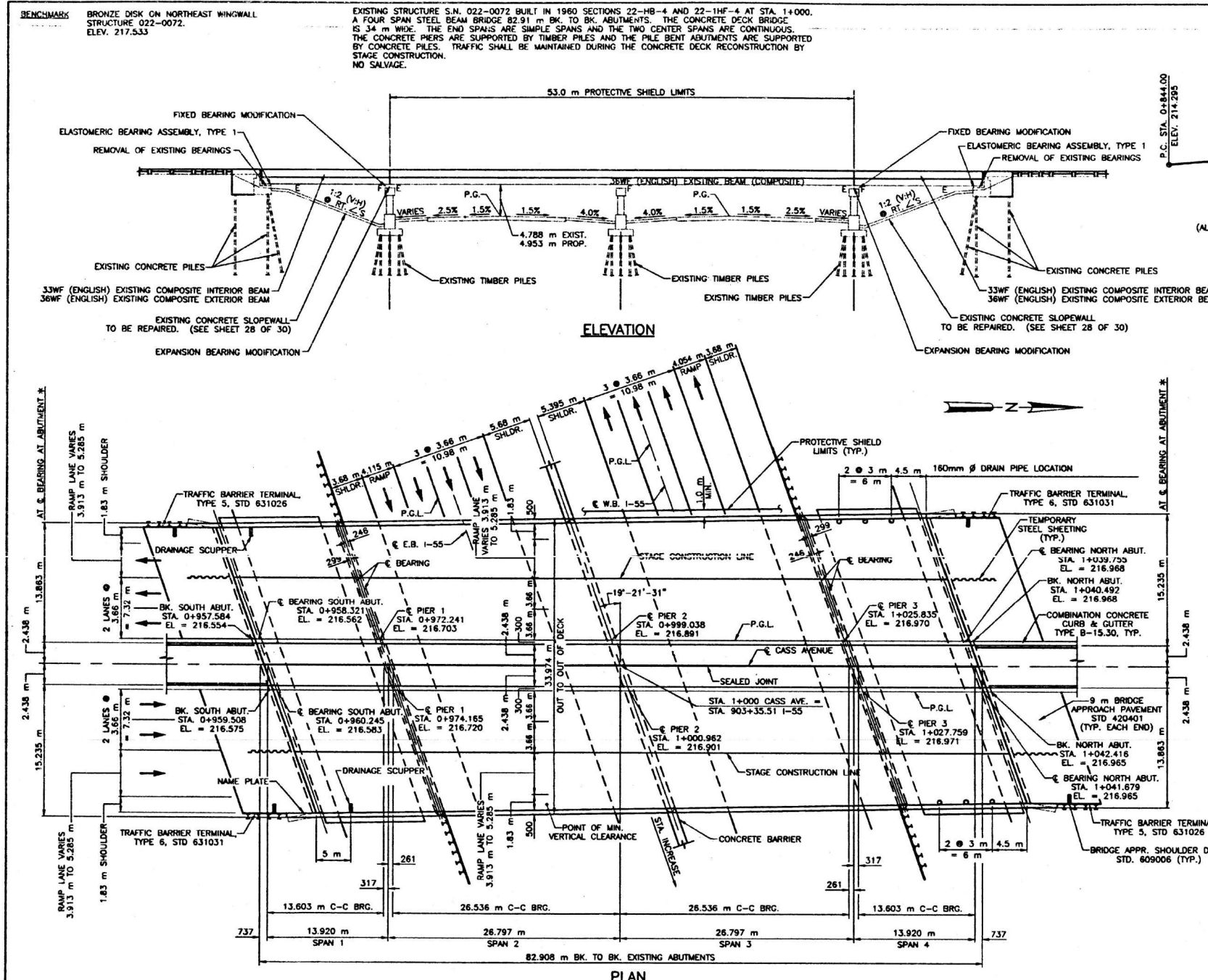
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

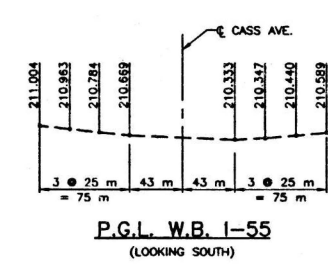
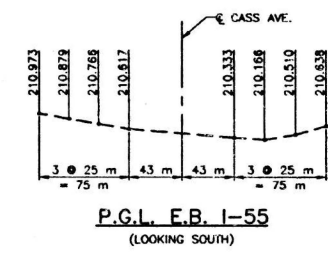
STRUCTURAL STEEL REPAIRS (3 OF 3)
STRUCTURE NO. 022-0072

SCALE: SHEET NO. S-21 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DU PAGE	60	36
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	22-1HB-4R	DUPAGE	62	16
FED. ROAD DISTRICT	ILLINOIS	FED. AID PROJECT		



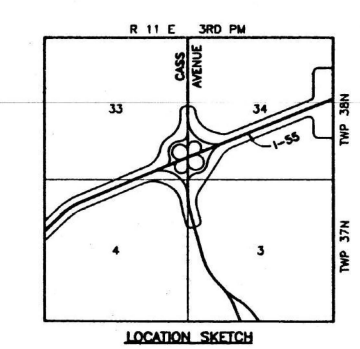
DESIGN SPECIFICATIONS
1996 AASHTO AND 1997 & 1998 INTERIM SPECIFICATIONS

LOADING MS18
ALLOW 1.2 kN/m² FOR FUTURE WEARING SURFACES

DESIGN STRESSES
f_c = 24 MPa
f_y = 400 MPa (REINF.)
f_y = 228 MPa EXISTING STEEL BEAMS
f_y = 250 MPa NEW STRUCTURAL STEEL (M270 M GR 250)

SEISMIC DATA
SEISMIC PERFORMANCE CATEGORY (SPC) = A
BEDROCK ACCELERATION COEFFICIENT (A) = 0.04 g
SITE COEFFICIENT (S) = 1.0

NOTES:
ALL DIMENSIONS ARE IN MILLIMETERS (mm) EXCEPT AS NOTED.
LIMITS OF EXISTING STRUCTURE EQUALS LIMITS OF PROPOSED STRUCTURE.
* DIMENSIONS ARE OUT TO OUT OF BRIDGE EXTENDED FROM E BEARINGS AT ABUTMENTS AND MEASURED PERPENDICULAR TO THE P.G.L.



CIVILTECH ENGINEERING, INC.
500 PARK BOULEVARD • SUITE 250
ITASCA, ILLINOIS 60143
(830) 773-3900 • FAX (830) 773-3975

APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Ralph C. Anderson
ENGINEER OF BRIDGES AND STRUCTURES



SHEET 1 OF 30	
REVISIONS	
NAME	DATE
O.J.H.	8/16/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL PLAN & ELEVATION
CASS AVENUE OVER I-55 (STEVENSON EXPRESSWAY)
F.A.I. RTE. 55 SECTION 22-1HB-4R
DUPAGE COUNTY
STA. 1+000
STR. NO. 022-0072
DATE: 6/22/2000
DRAWN BY: C.D.C.
CHECKED BY: O.J.H.

FILE NAME: \\casM-dbsstepw21\CS_work_dir\33213303_1\TASK_4-02W21-8022-EXST PLAN-1.dgn

DBS
DB STERLIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

USER NAME =	icsM-dbsstepw21	DESIGNED -	MM	REVISED -	
PLOT SCALE =		DRAWN -	DJB	REVISED -	
PLOT DATE =	5/14/2025	CHECKED -	BJN	REVISED -	
PLOT TIME =	9:51:01 PM	DATE -		REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLAN - FOR INFORMATION ONLY (1 OF 10)
STRUCTURE NO. 022-0072
SCALE: NTS
SHEET NO. S-22 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DUPAGE	60	37
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

STAGE 2 CONSTRUCTION

STAGE 1 CONSTRUCTION

LONGITUDINAL BONDED CONSTRUCTION JOINT FOR STAGE CONSTRUCTION DO NOT EDGE

a(E)

b₁(E) OR b₃(E)

2.0%

1.5%

a₂(E)

b₁(F) OR b₃(F)

b₂(E) OR b₄(E)

a₂(E)

#15(E) BAR SPLICER

584

330

457

457

Technical drawing of a parapet cross-section showing structural details and dimensions. The drawing includes the following elements:

- Dimensions:**
 - Overall width: 500
 - Top width: 320
 - Left side height: 65, 200, 55, 180, 40, 20, 230, 330, 80
 - Right side height: 865, 235, 75, 195
 - Internal width: 100
 - Bottom width: 50
- Structural Details:**
 - 3 mm ALUMINUM SHEETS AT PIER 2 AND 4.35 m FROM PIER 2
 - CONSTRUCTION JOINT (OPTIONAL)
 - BONDED CONSTRUCTION JOINT (MANDATORY)
 - 20 mm Δ DRIP NOTCH FULL LENGTH
 - VARIABLE
 - 180 mm ϕ FLOOR DRAIN
 - 195 mm SLAB
- Notes:**
 - NOTES: ALL EDGES SHALL HAVE 20 mm CHAMFER.
- Labels:**
 - FACE OF PARAPET
 - PARAPET
 - ei(E) THRU es(E)
 - es(E)
 - ds(E)
 - di(E)
 - de(E)
 - ei(E) thru es(E)
 - es(E)
 - ds(E)
 - di(E)
 - de(E)
 - ei(E)
 - es(E)
 - ds(E)
 - di(E)
 - de(E)

NOTES:

CUT LONGITUDINAL REINFORCEMENT TO CLEAR DRAINAGE SCUPPER.

MINIMUM LAP LENGTH FOR #15 BARS = 640 mm, #20 BARS = 790 mm.

BAR'S INDICATED THUS 20 x 3-#15 ETC. INDICATES 20 LINES OF BAR'S WITH 3 LENGTHS PER LINE.

SEE SHEET 9 OF 30 FOR SECTIONS B-B, C-C AND D-D.

SEE SHEET 10 OF 30 FOR PARAPET REINFORCEMENT.

*** ORDER $a_3(E)$ AND $a_2(E)$ FULL LENGTH, CUT TO FIT SKEW AND USE REMAINDER OF BARS IN OPPOSITE END OF SPAN.**

**** ORDER FULL LENGTH, CUT TO FIT SKEW AND REMAINDER OF BARS IN THE NORTHWEST QUARTER.**

VARIES 6.365 m TO 7.747 m STAGE 2 CONSTRUCTION

VARIES 16.296 m TO 17.678 m

9.931 m STAGE 1 CONSTRUCTION

500

VARIES 5.865 m TO 7.247 m

7.493 m

2.438 m

STRUCTURE & CASS AVENUE

DIMENSIONS SHOWN ON CROSS SECTION ARE AT RT. \angle s TO C. CASS AVE., UNLESS OTHERWISE NOTED

SEE SECTION E-E FOR ADDITIONAL DETAILS

SEE PARAPET SECTION THIS SHEET FOR REINFORCEMENT

SEE SECTION C-C FOR ADDITIONAL DETAILS

SEE SECTION E-E THIS SHEET

18.5 MET

2.0%

1.5%

P.G.L.

1.5%

1

2

3

4

5

6

7

8

9

10

11

12

22

23

24

19

18

17

16

15

14

13

12

4-#15 b2(E) OR b4(E) @ EQUAL SPACES BETWEEN BEAMS

200

225

200

280

5-#15 b3(E) OR b4(E) @ 300± CTS. BETWEEN BEAMS

225

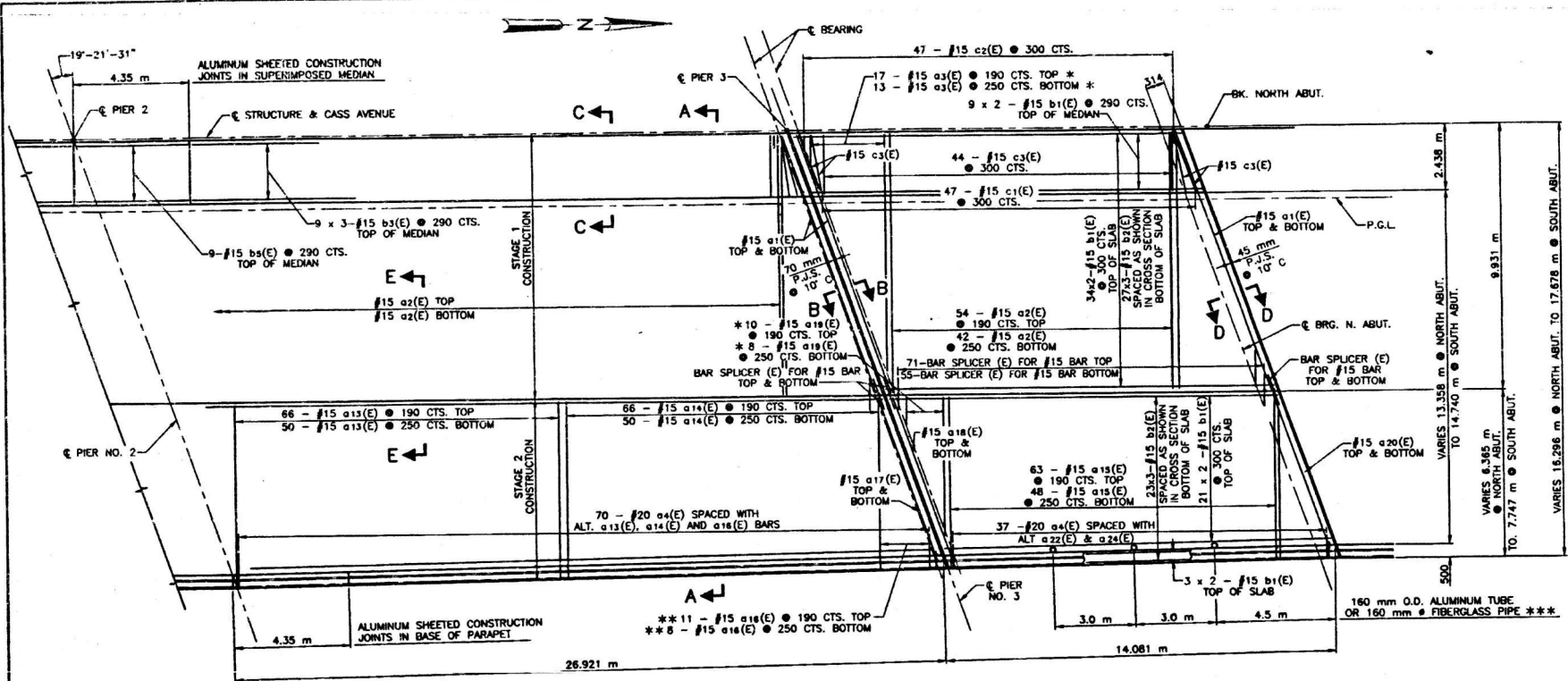
5-#15 b2(E) OR b4(E) @ 345± CTS. BETWEEN BEAMS

738 VARIES VARIES 1.600 m 1.600 m 584 330 457

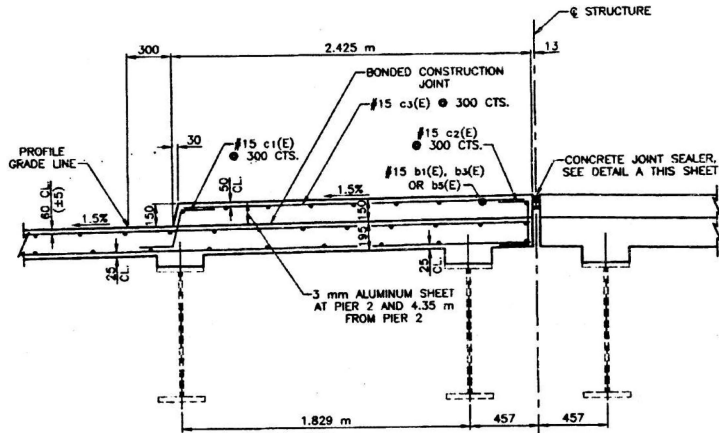
PT. \angle s

5 SPACES @ 1.829 m = 9.144 m @ RT. \angle s TO C. CASS AVE.

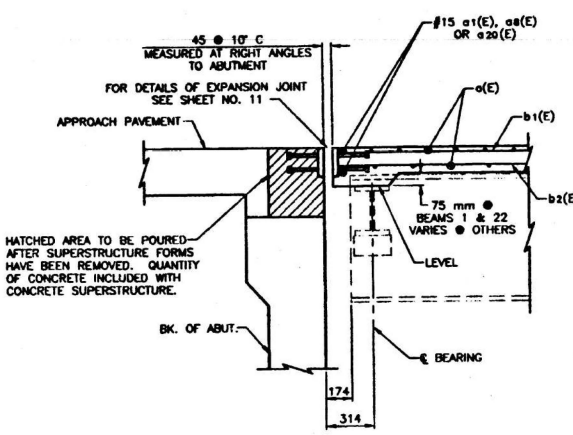
FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	22-1HB-4R	DUPAGE	62	24
FED. ROAD DISTRICT	ILLINOIS	FED. AID PROJECT		



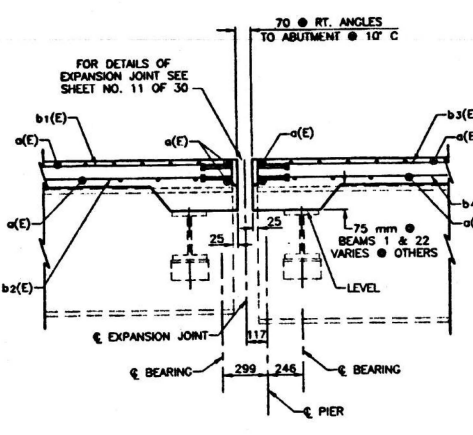
PLAN OF NORTHEAST QUARTER DECK REINFORCEMENT
(SOUTHWEST QUARTER SAME BY ROTATION 180° EXCEPT FOR SCUPPER IN SOUTHWEST CORNER)



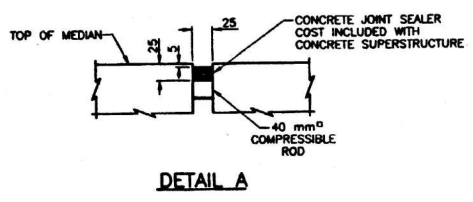
SECTION C-C



SECTION D-D
(DIMENSIONS SHOWN ARE AT RIGHT ANGLES TO ABUTMENT)



SECTION B-B
(DIMENSIONS SHOWN ARE AT RIGHT ANGLES TO PIER)



DETAIL A

- * ORDER a3(E) & a19(E) FULL LENGTH, CUT TO FIT SKEW AND USE REMAINDER OF BARS IN OPPOSITE END OF SPAN.
- ** ORDER FULL LENGTH, CUT TO FIT SKEW AND USE REMAINDER OF BARS IN THE SOUTHWEST QUARTER.
- *** SEE GENERAL PLAN FOR LOCATION OF SCUPPERS AND DRAINS.

SEE SHEET 8 OF 30 FOR SECTION A-A, C-C AND PARAPET SECTION.

SHEET 9 OF 30

REVISIONS	NAME	DATE
1	O.J.H.	8/16/00
2		
3		
4		
5		
6		
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9		
10		

ILLINOIS DEPARTMENT OF TRANSPORTATION	
DECK PLAN	
CASS AVENUE OVER I-55 (STEVENS EXPRESSWAY)	
F.A.I. RTE. 55 SECTION 22-1HB-4R	
DUPAGE COUNTY	
STA. 1+000	
STR. NO. 022-0072	
DATE: 6/22/2000	DRAWN BY: C.D.C. CHECKED BY: O.J.H.

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DB STERILIN CONSULTANTS, INC.
123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

USER NAME = lcsM-dbsstepw21	DESIGNED - MM	REVISED -
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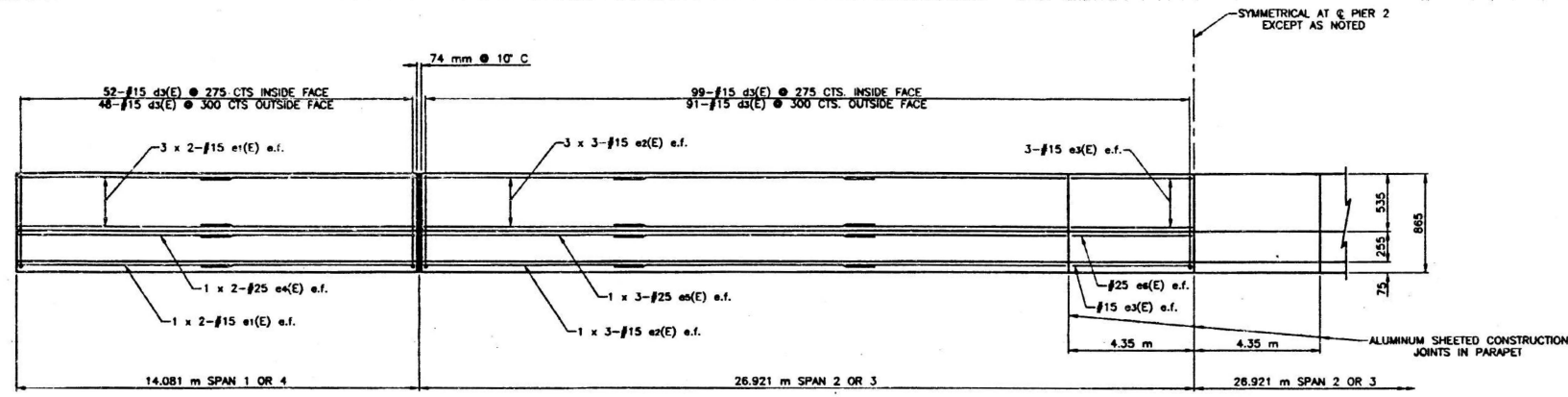
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLAN - FOR INFORMATION ONLY (3 OF 10)
STRUCTURE NO. 022-0072

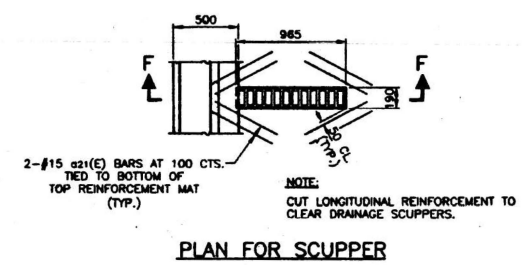
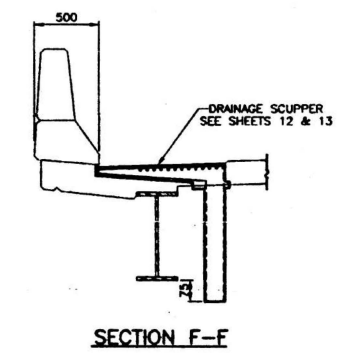
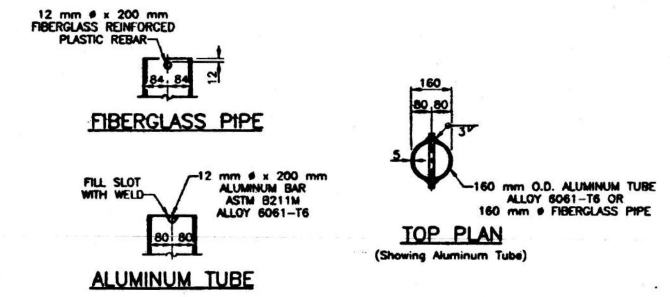
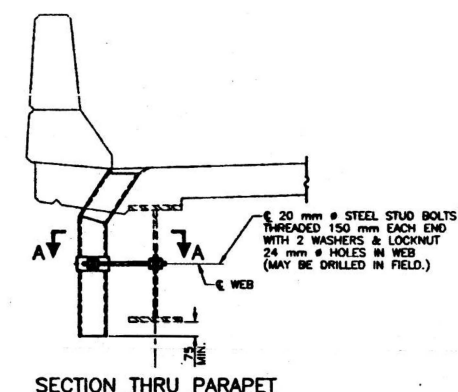
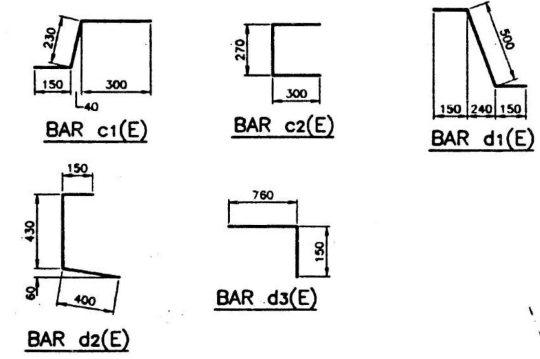
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F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DUPAGE	60	39
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 55	22-1HB-4R	DUPAGE	62	25
FED. ROAD DISTRICT	ILLINOIS	FED. AID PROJECT		



INSIDE ELEVATION OF PARAPET

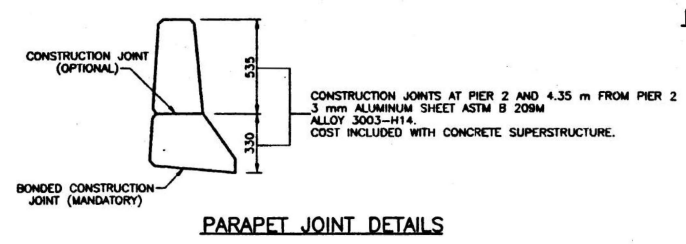
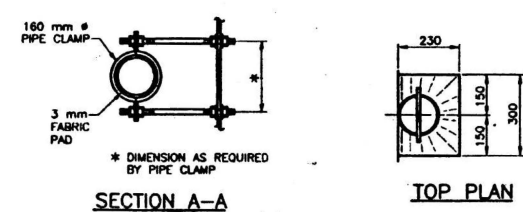


BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH(m)	SHAPE
a1(E)	24	15	10.429	
a2(E)	1310	15	9.830	
a3(E)	180	15	10.250	
a4(E)	432	20	1.20	
a5(E)	216	15	7.220	
a6(E)	250	15	7.000	
a7(E)	46	15	7.600	
a8(E)	4	15	7.900	
a9(E)	4	15	7.450	
a10(E)	4	15	7.700	
a11(E)	42	15	7.35	
a12(E)	250	15	6.766	
a13(E)	232	15	6.557	
a14(E)	232	15	6.348	
a15(E)	222	15	6.113	
a16(E)	19	15	6.750	
a17(E)	4	15	6.450	
a18(E)	4	15	6.700	
a19(E)	36	15	6.500	
a20(E)	4	15	6.250	
a21(E)	16	15	0.600	
b1(E)	452	15	7.32	
b2(E)	600	15	5.10	
b3(E)	584	15	8.23	
b4(E)	600	15	9.49	
b5(E)	36	15	4.25	
b6(E)	110	20	12.00	
b7(E)	110	20	6.80	
c1(E)	546	15	0.680	
c2(E)	546	15	0.870	
c3(E)	554	15	2.310	
d1(E)	598	15	0.800	
d2(E)	548	15	0.980	
d3(E)	1160	15	0.910	
e1(E)	56	15	7.30	
e2(E)	84	15	7.90	
e3(E)	24	15	4.25	
e4(E)	16	25	7.70	
e5(E)	24	25	8.40	
e6(E)	8	25	4.25	

ITEM	UNIT	QUANTITY
REINFORCEMENT BARS, EPOXY COATED	kg	82,730
CONCRETE SUPERSTRUCTURE	m ³	728.9
BAR SPLICERS	EACH	1514
PROTECTIVE COAT	m ²	2515

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
BARS INDICATED THUS 1 x 3-#15 ETC. INDICATES 1 LINE OF
BARS WITH 3 LENGTHS PER LINE.



NOTES:
THE EXTERIOR SURFACES OF THE FLOOR DRAINS SHALL BE PAINTED WITH THE FINISH COAT AS SPECIFIED IN THE SPECIAL
DRAWINGS FOR CLEANING AND PAINTING NEW METAL STRUCTURES. THE EXTERIOR SURFACES OF THE DRAINS SHALL BE CLEANED
ACCORDING TO STEEL STRUCTURES PAINTING STANDARDS. PAINTING STANDARDS. PAINTING STANDARDS. PAINTING STANDARDS.
FIBERGLASS PIPE SHALL CONFORM TO ASTM D 2996, WITH SHORT-TIME RUPTURE STRENGTH HOOP TENSILE STRESS OF
200 MPa MINIMUM.
ALL DIMENSIONS ARE IN MILLIMETERS (mm) EXCEPT AS NOTED.

SHEET 10 OF 30

REVISIONS

NAME	DATE

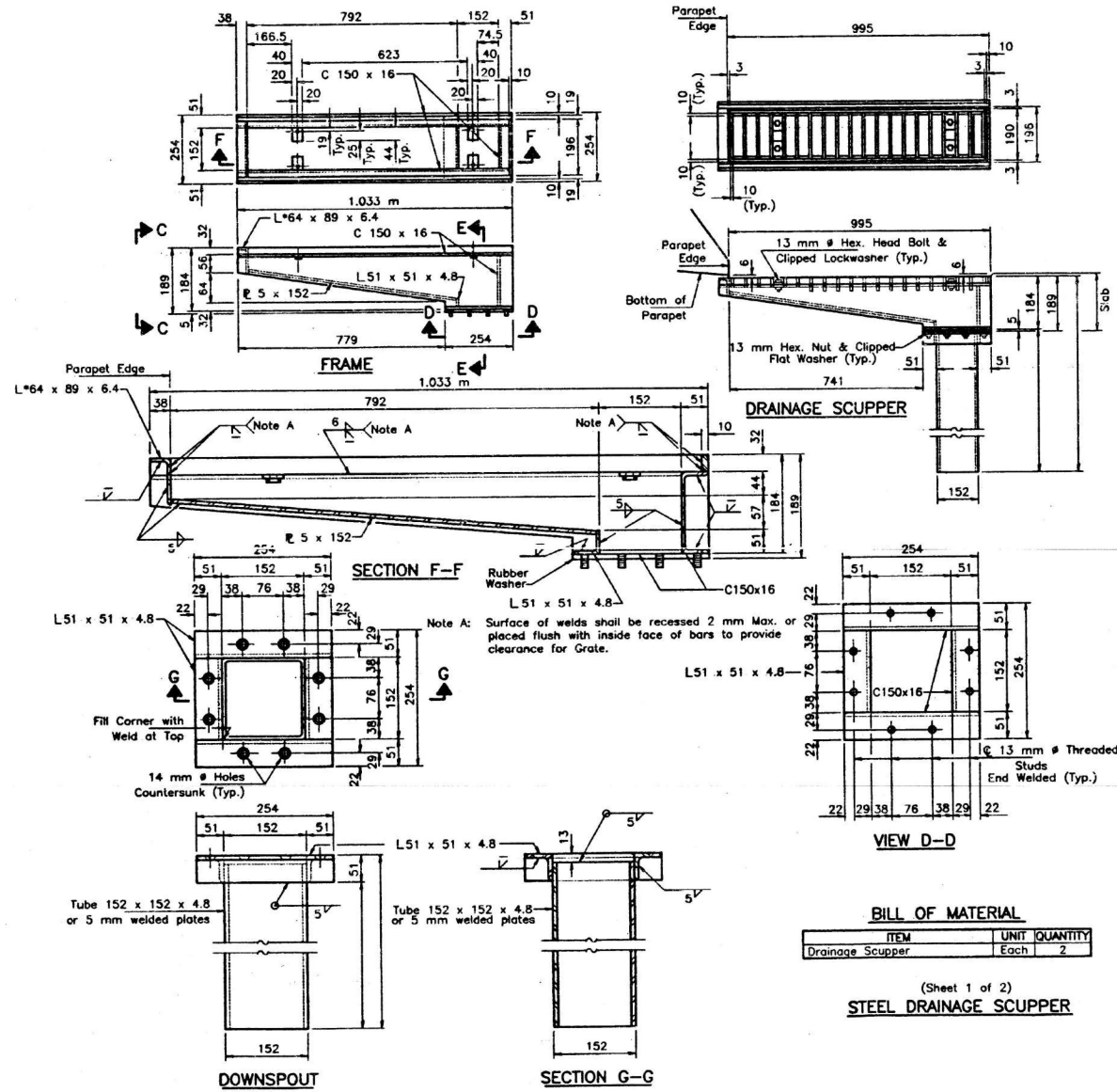
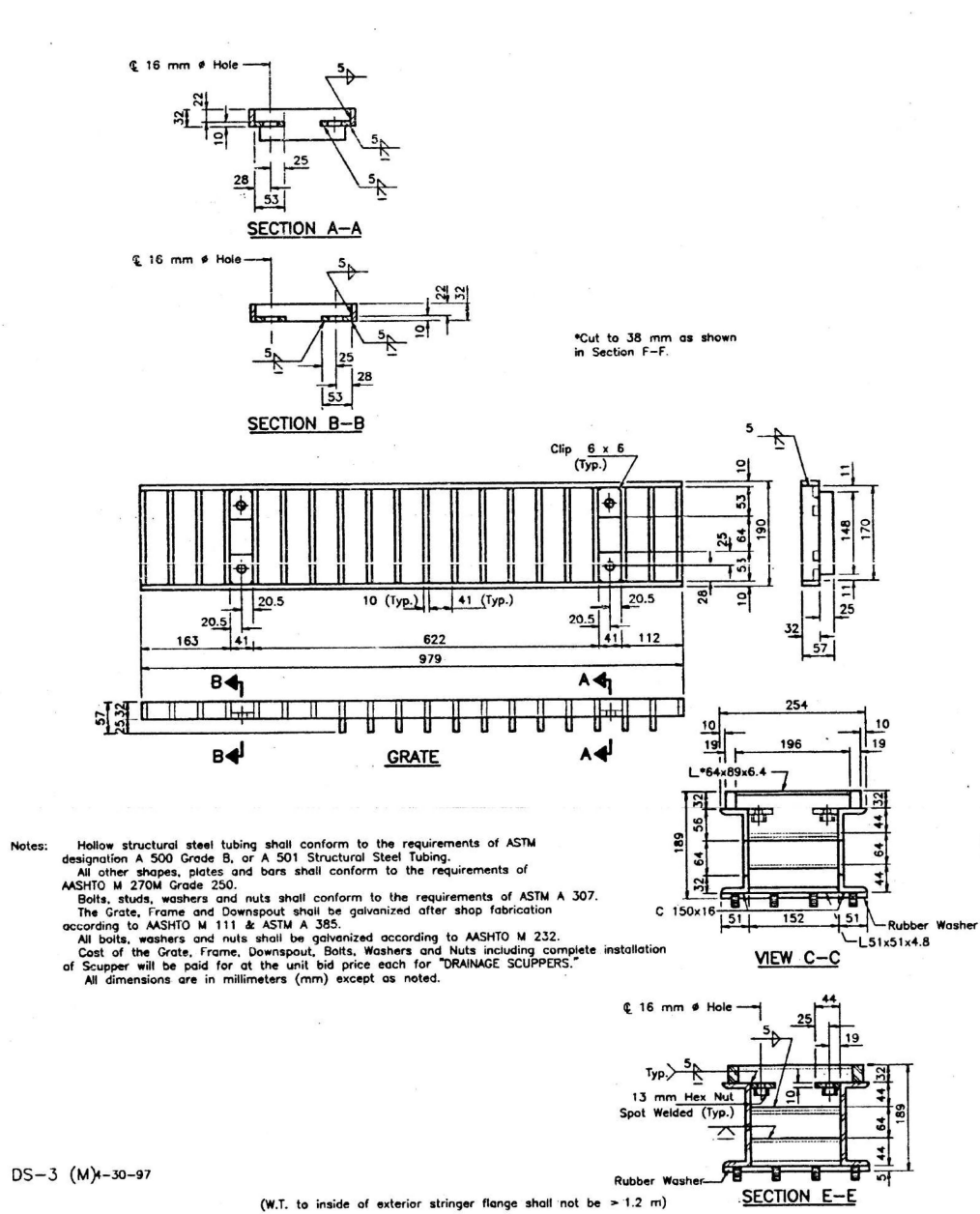
ILLINOIS DEPARTMENT OF TRANSPORTATION
**DECK PLAN DETAILS
AND PARAPET ELEVATIONS**
CASS AVENUE OVER I-55 (STEVENS EXPRESSWAY)
F.A.I. RTE. 55 SECTION 22-1HB-4R
DUPAGE COUNTY
STA. 1+000
STR. NO. 022-0072

DATE: 6/22/2000

DRAWN BY: C.D.C.
CHECKED BY: O.J.H.

FILE NAME: \\lcsM-dbs\p21\1003_24\TASK 46\2021-5025-EXIST PLAN-4.dgn

FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	22-1HB-4R	DUPAGE	62	27
FED. ROAD DISTRICT	ILLINOIS	FED. AID PROJECT		



SHEET 12 OF 30

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
STEEL DRAINAGE SCUPPER
 CASS AVENUE OVER I-55 (STEVENS EXPRESSWAY)
 F.A.I. RTE. 55 SECTION 22-1HB-4R
 DUPAGE COUNTY
 STA. 1+000
 STR. NO. 022-0072
 DATE: 6/22/2000
 DRAWN BY: C.D.C.
 CHECKED BY: O.J.H.

FILE NAME: \\cam-dbsstepw21\CS_pdw_work_dir\33213903_251TASK_4-02W21-502R-EXST PLAN-5.dgn

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 312.857.1006

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PLOT SCALE =	DRAWN - DJB	REVISED -
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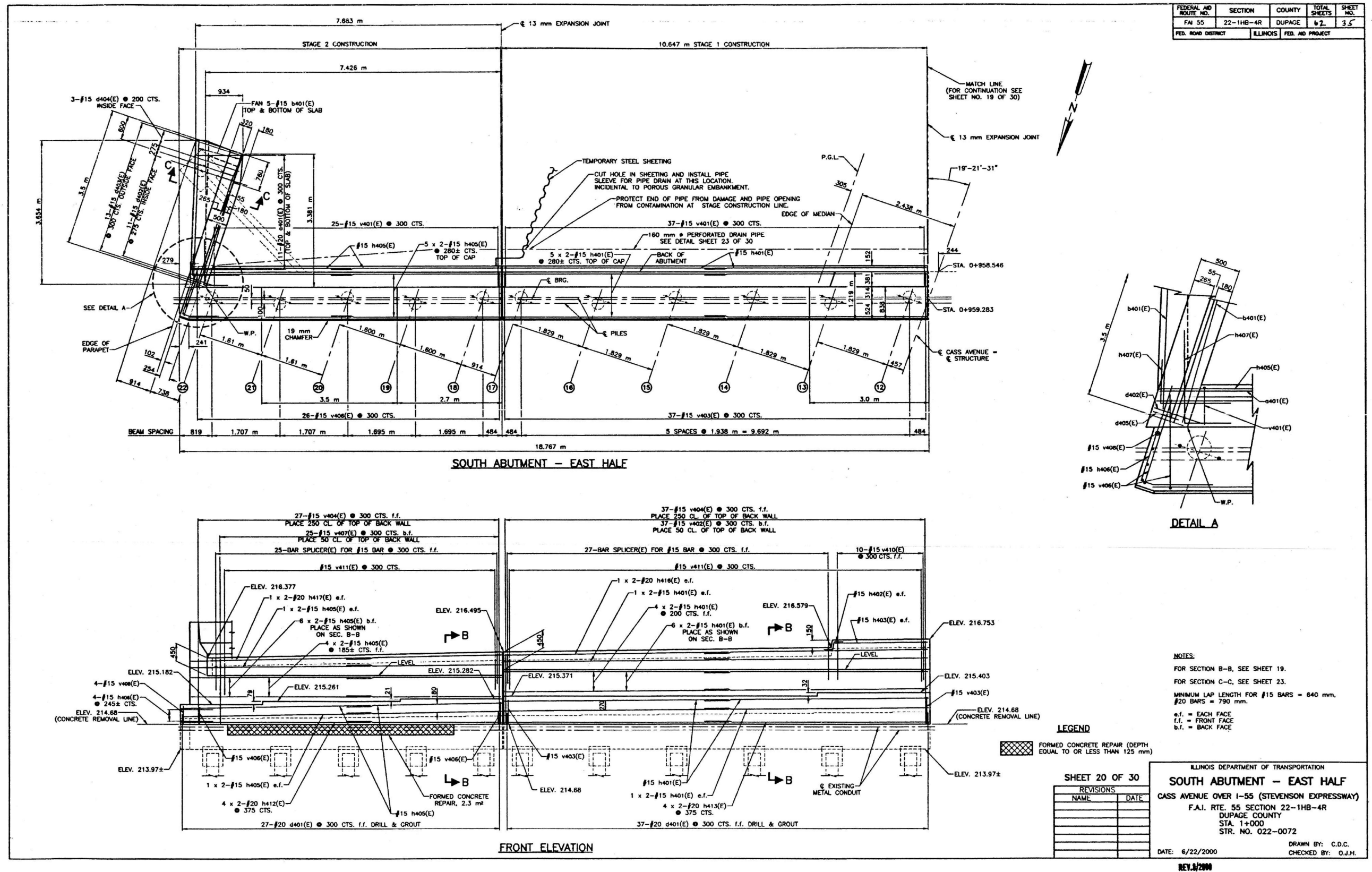
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

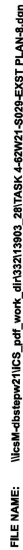
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 STRUCTURE NO. 022-0072

SCALE: NTS

SHEET NO. S-26 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DUPAGE	60	41
CONTRACT NO. 62W21				
ILLINOIS				FED. AID PROJECT





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312.857.1006

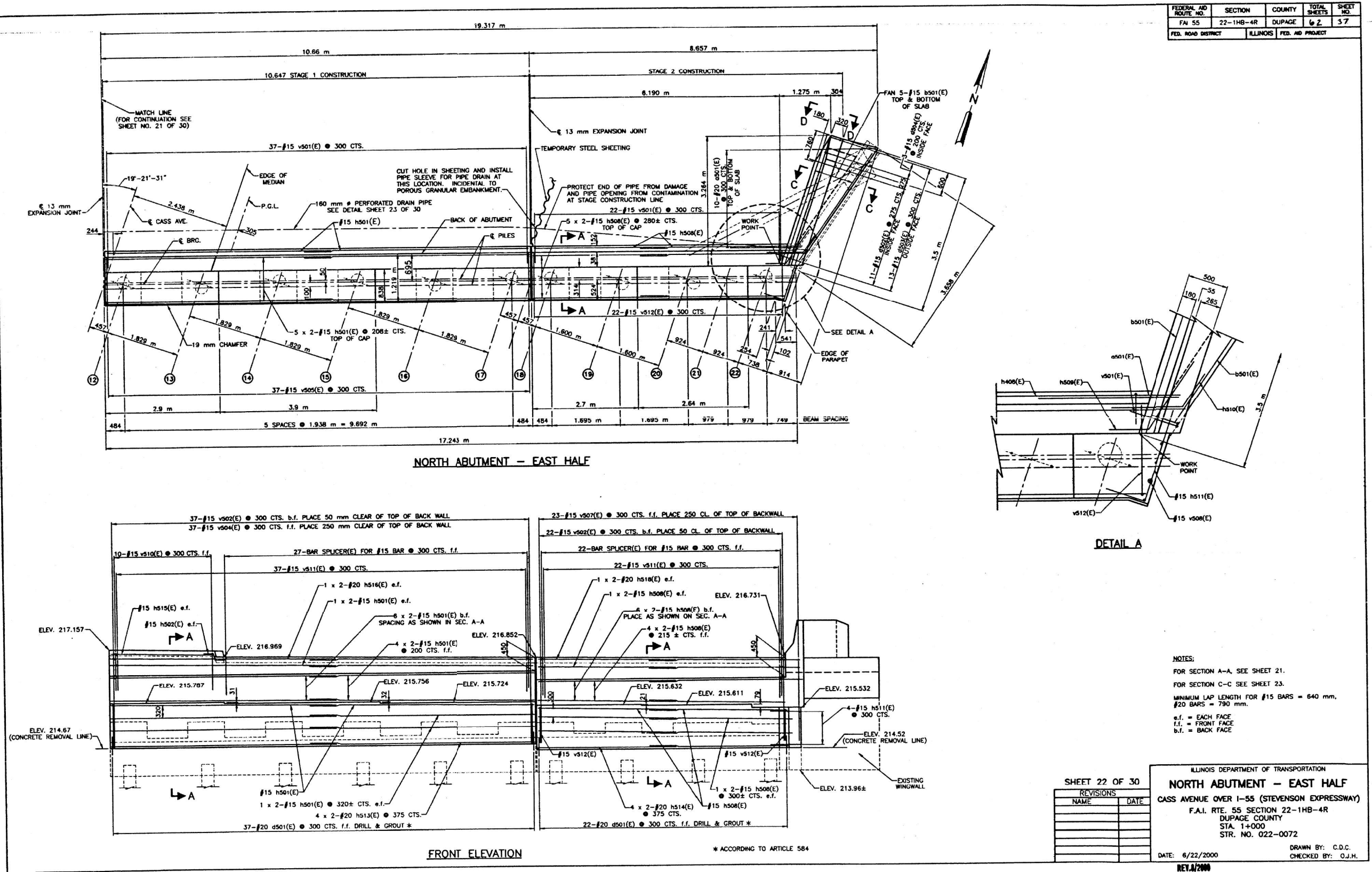
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLAN - FOR INFORMATION ONLY (9 OF 10)
STRUCTURE NO. 022-0072

SCALE: NTS SHEET NO. S-30 OF S-31 SHEETS

F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	FAI-55-23	DUPAGE	60	45
CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



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Chicago, Illinois 60606
312.857.1000

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLAN - FOR INFORMATION ONLY (10 OF 10)
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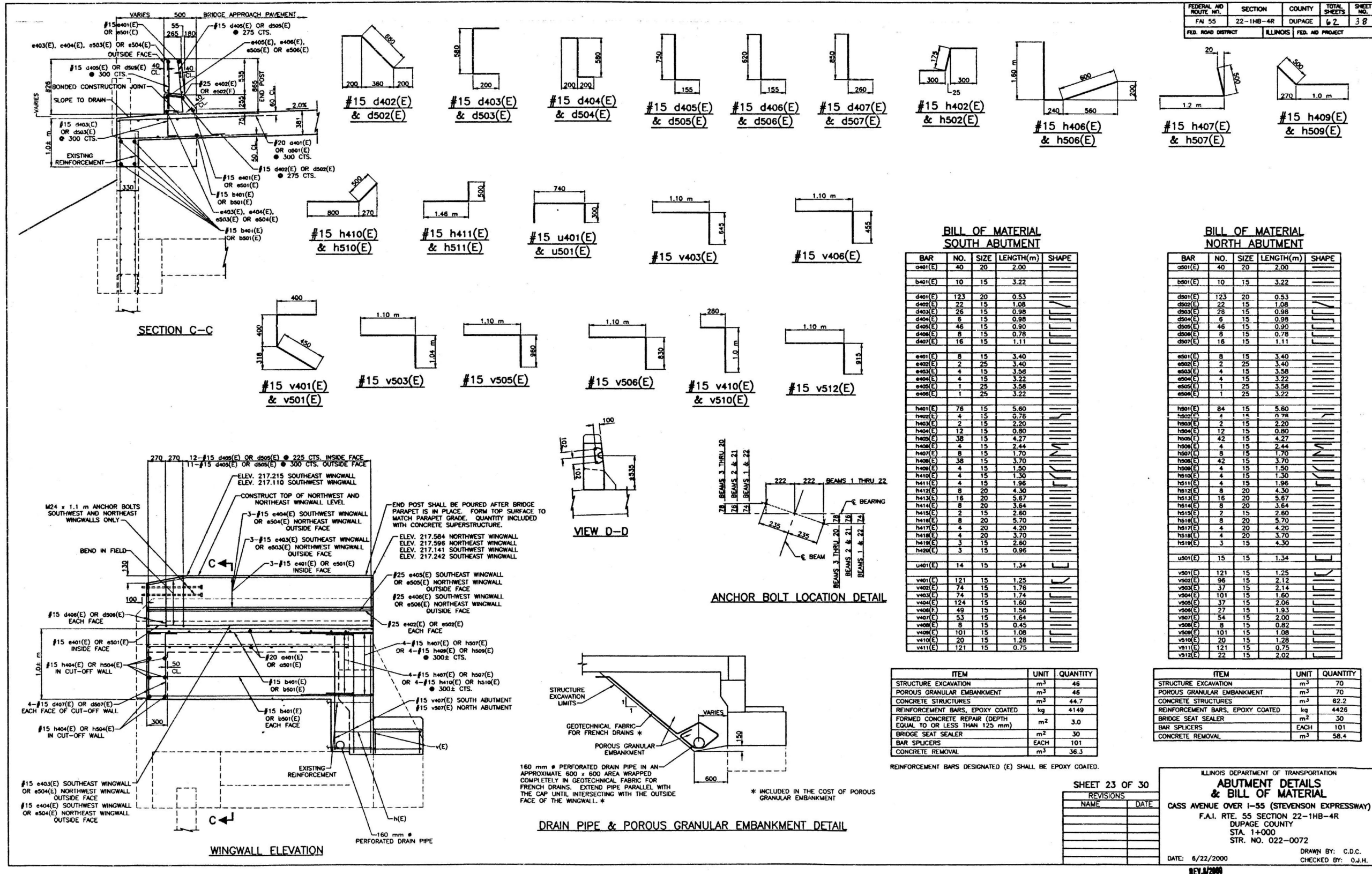
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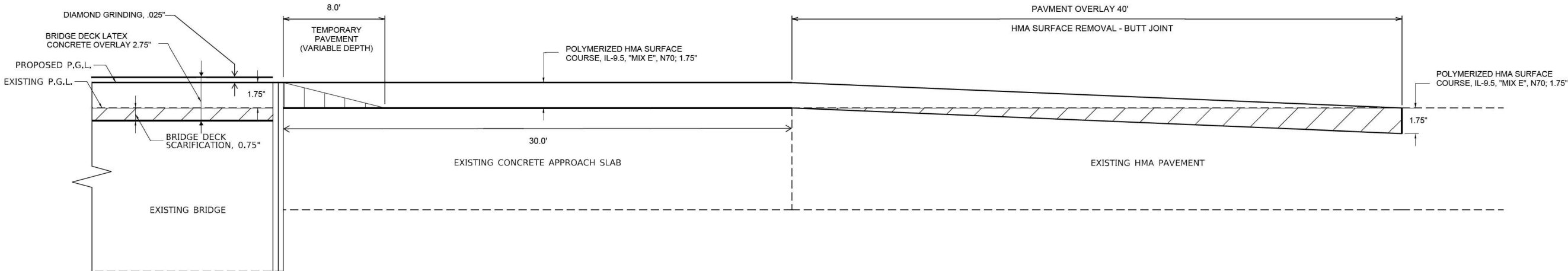
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F.A.I. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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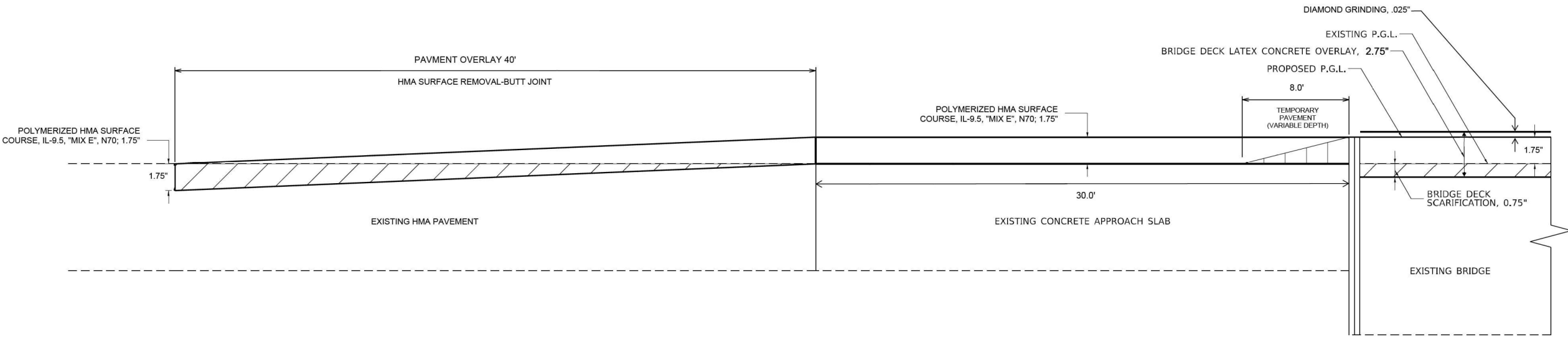
CONTRACT NO. 62W21

ILLINOIS | FED. AID PROJECT





PAVEMENT OVERLAY DETAIL

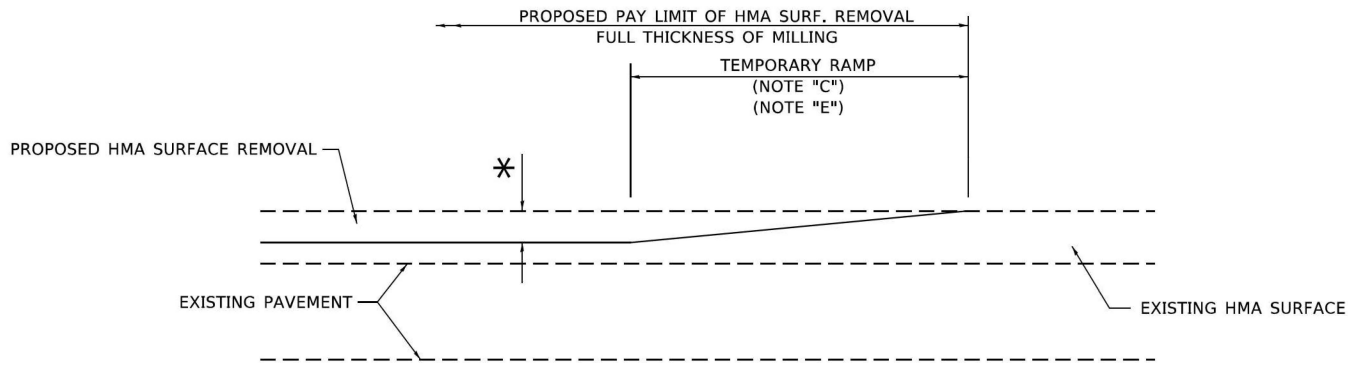


PAVEMENT OVERLAY DETAIL

NOTE:
1. THE TEMPORARY RAMP SHALL BE CONSTRUCTED IMMEDIATELY
UPON REMOVAL OF THE EXISTING HMA SURFACE

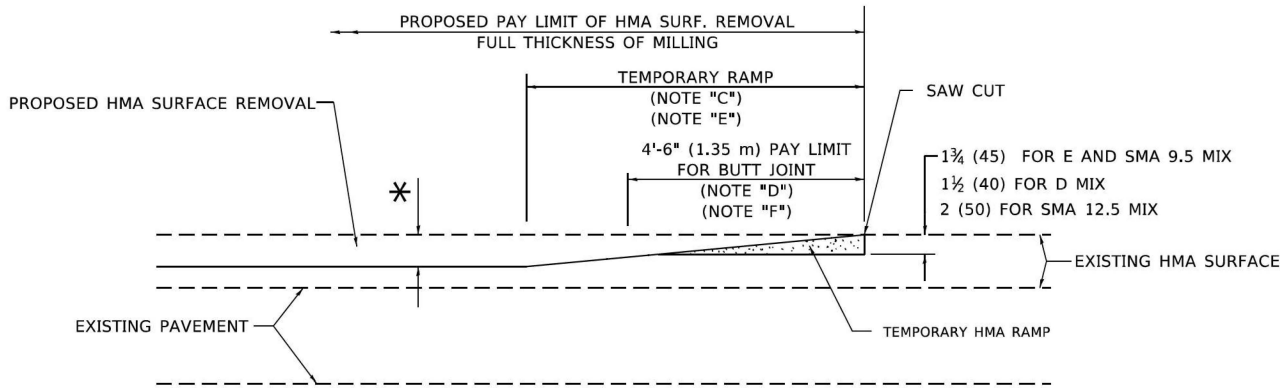
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					STA. 00+000 TO STA. 00+000												



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

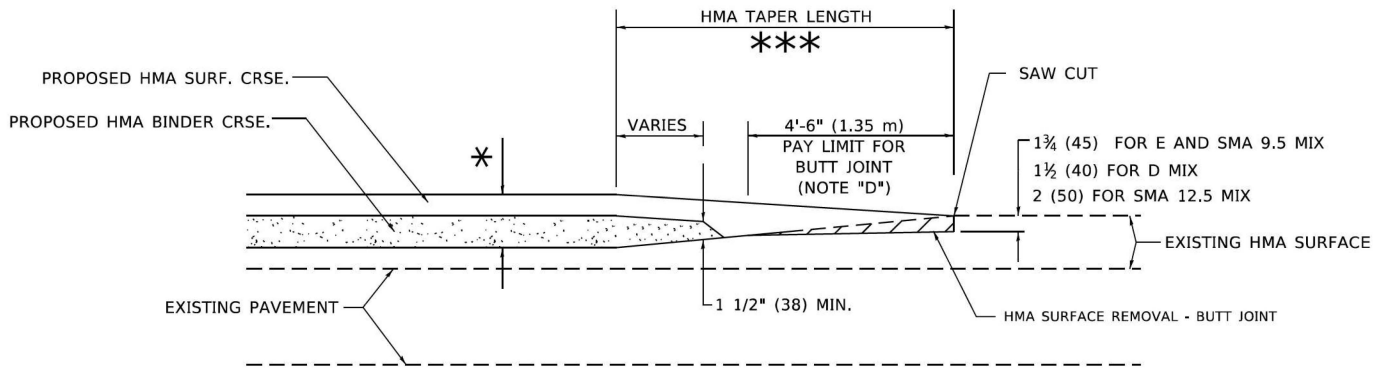
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

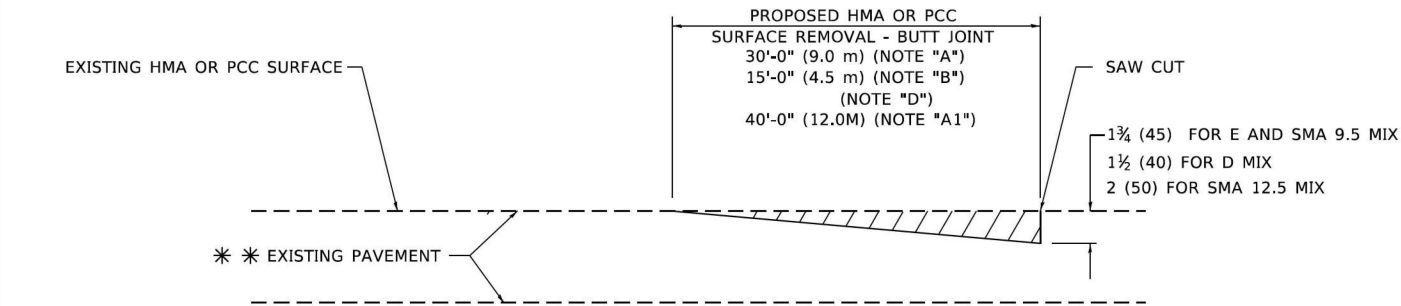
OPTION 2

TYPICAL TEMPORARY RAMP

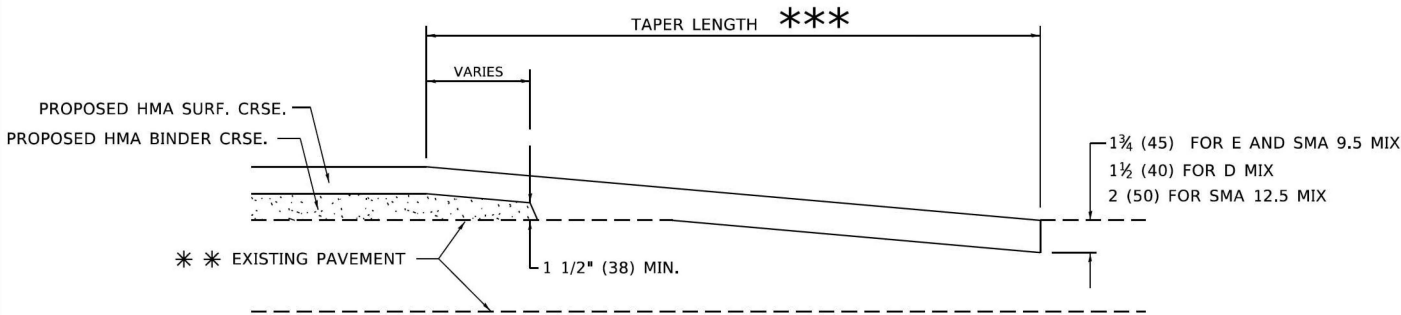


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- INTERSTATES
- MINOR SIDE ROADS.
- THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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123 N. Wacker Drive, Suite 2000
Chicago, Illinois 60606
312.857.1006

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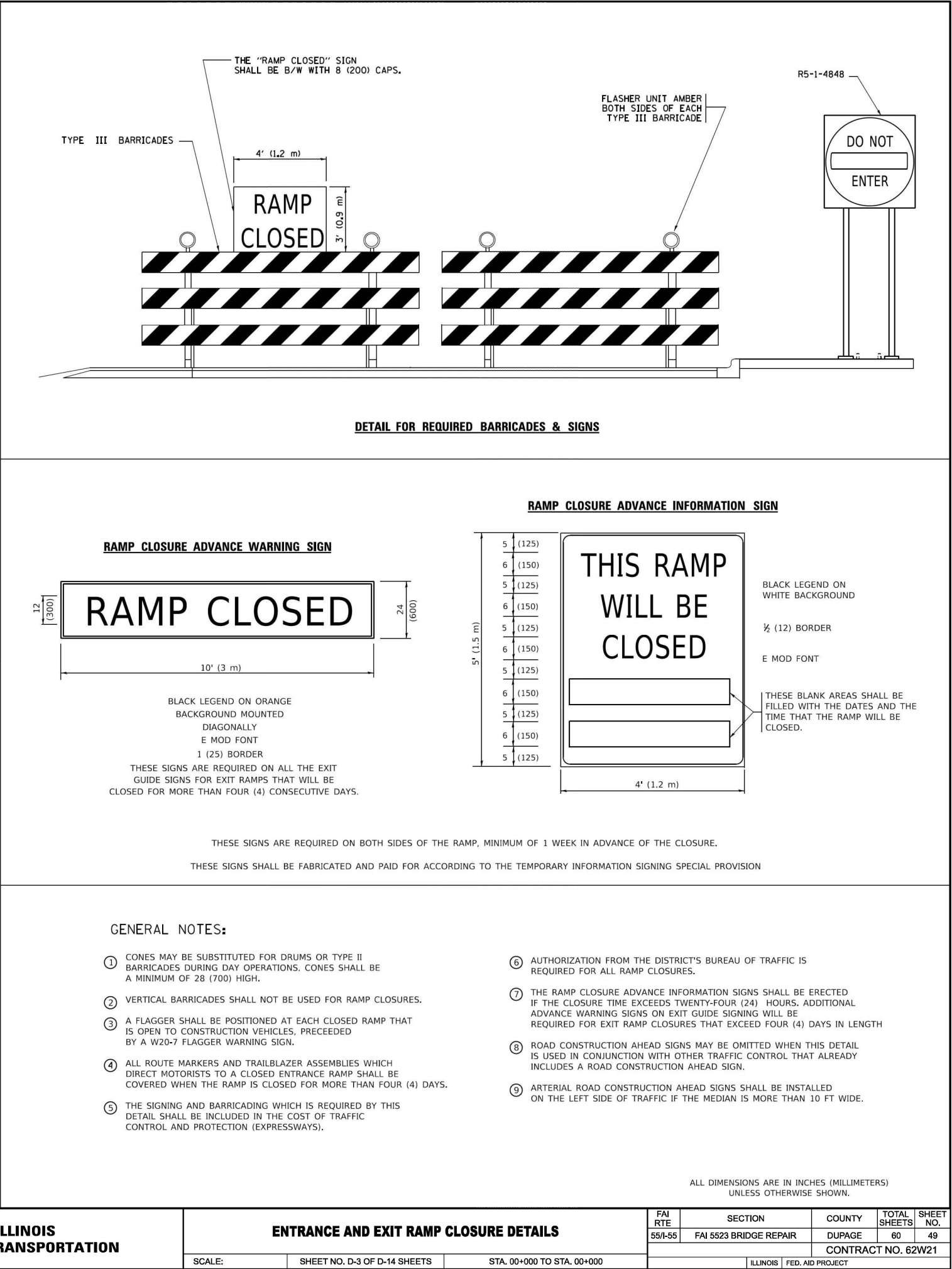
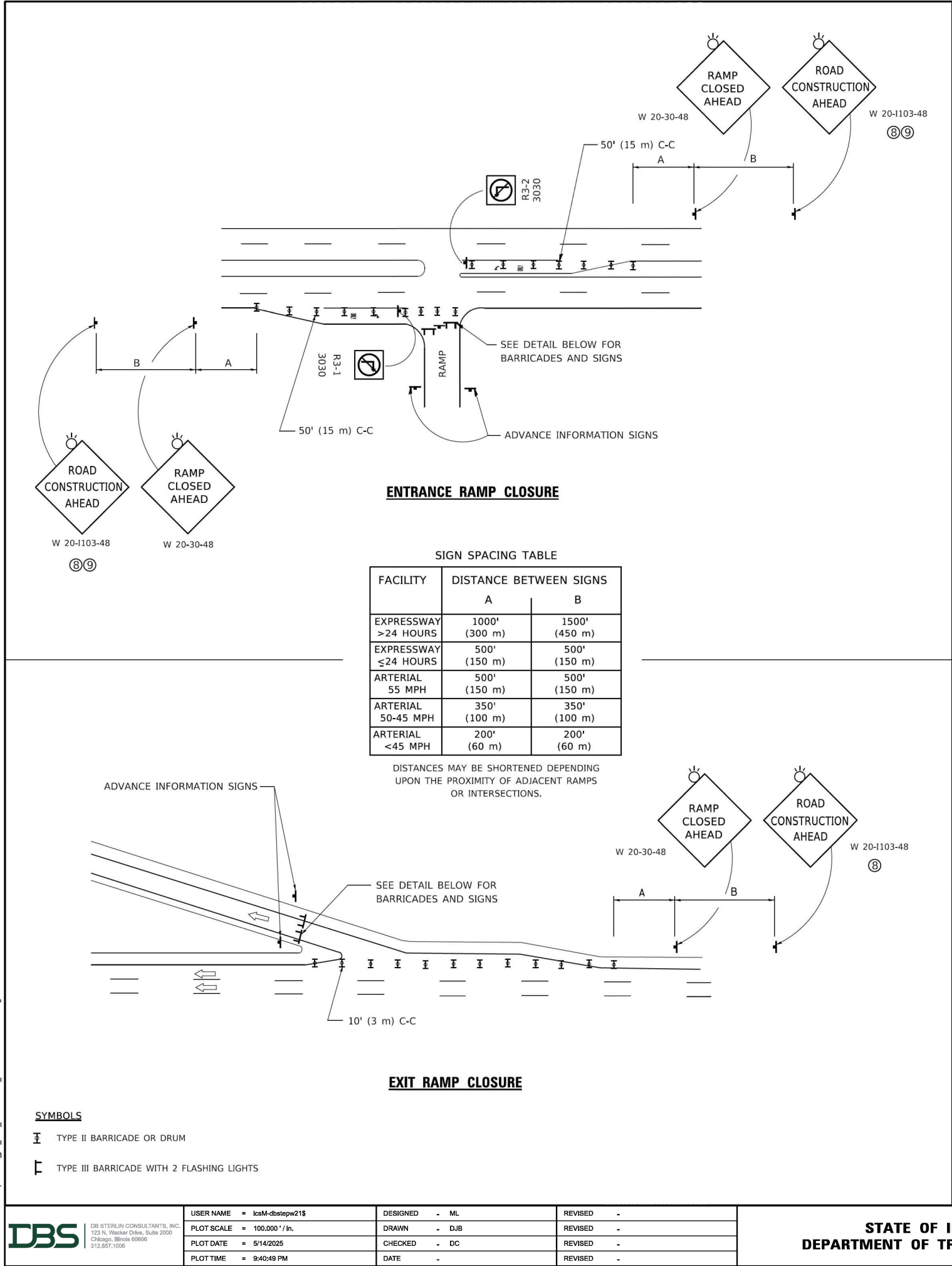
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT DETAILS

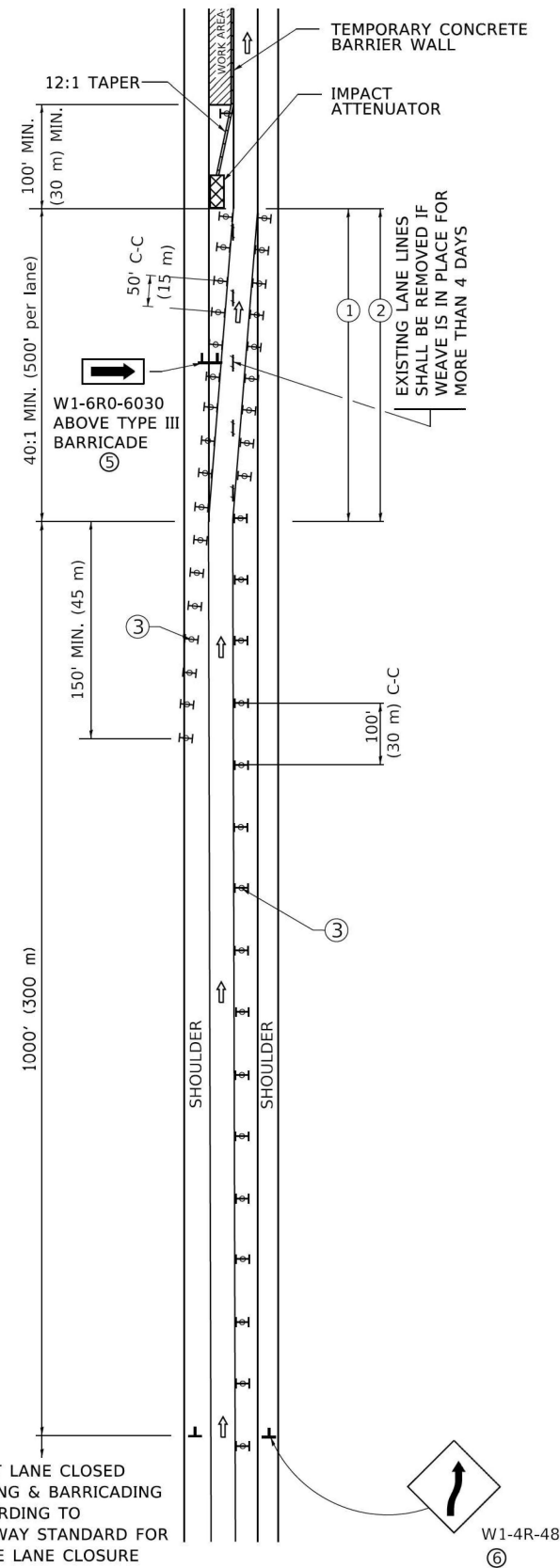
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FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

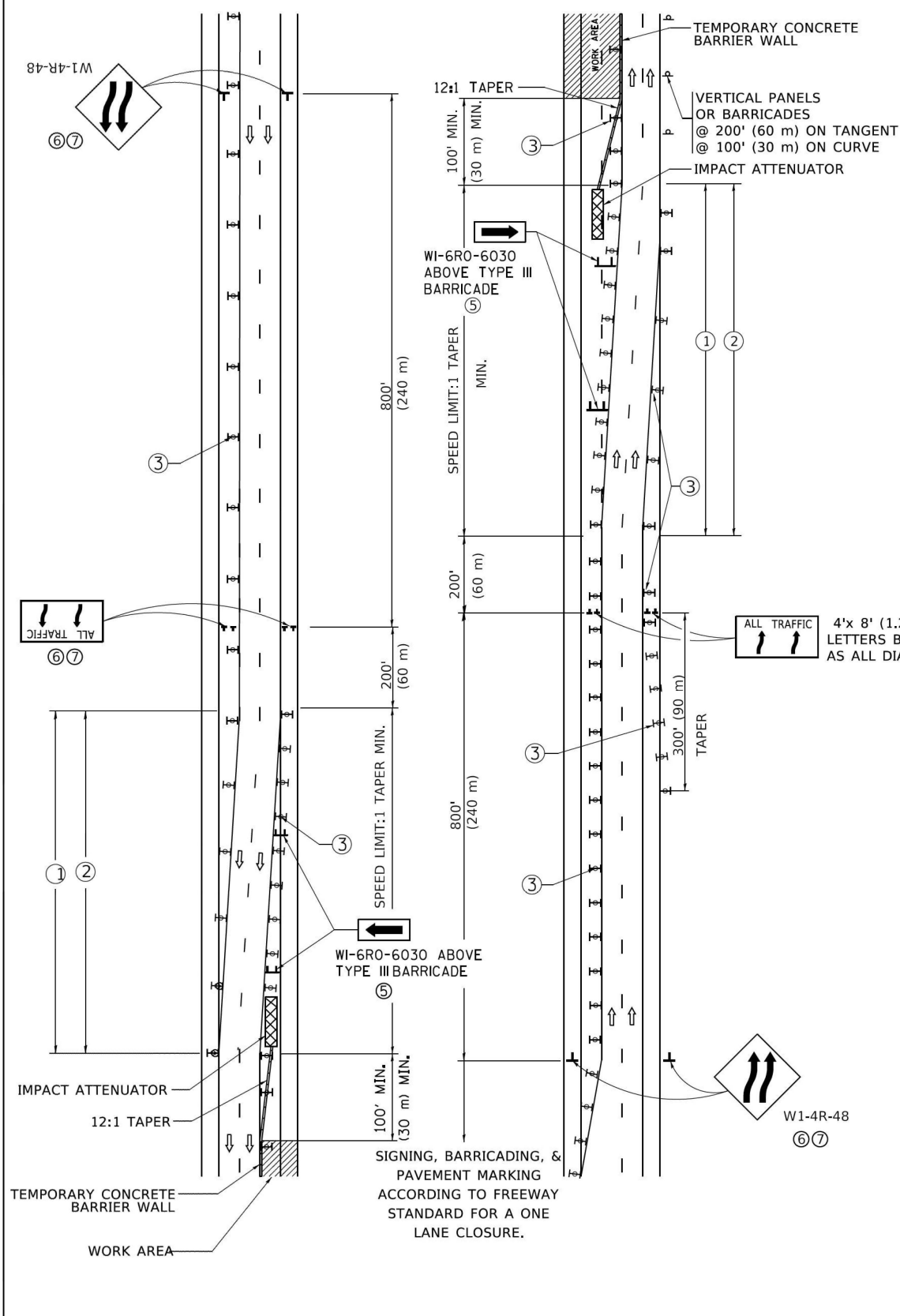
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SINGLE LANE WEAVE










MULTI-LANE WEAVE



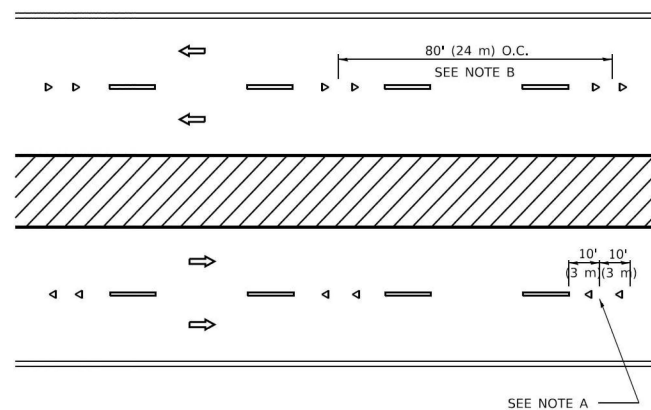
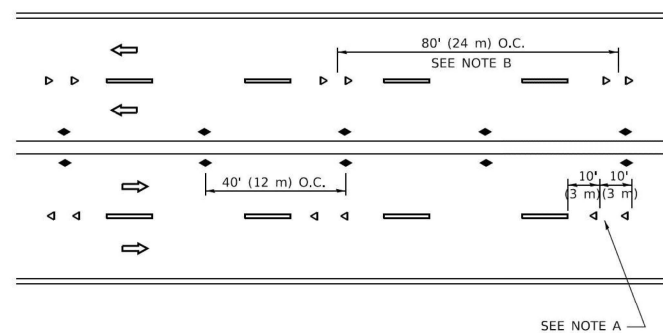
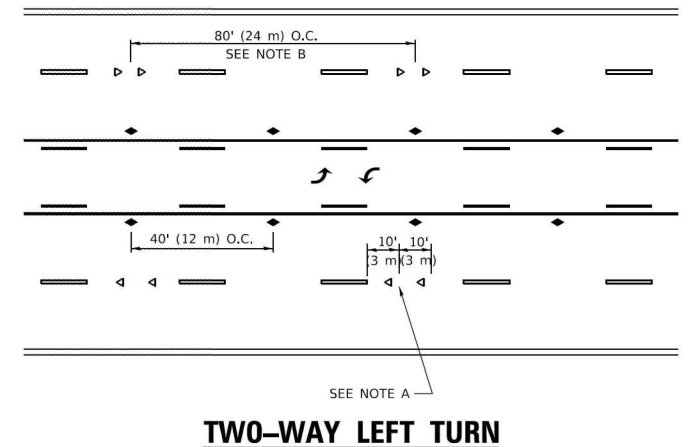
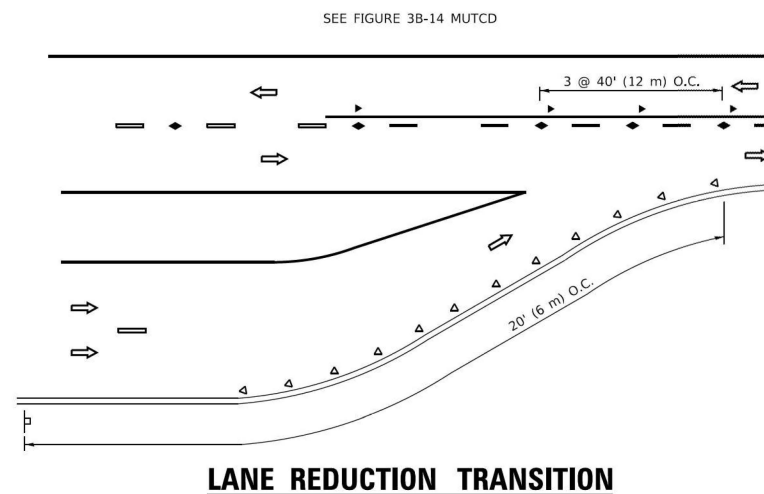
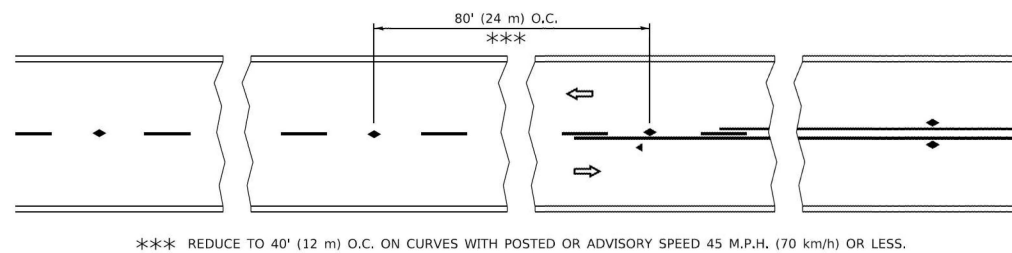
- GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- | | |
|---|--|
|  | DIRECTION OF TRAFFIC |
|  | WORK AREA |
|  | SIGN ON PORTABLE OR PERMANENT SUPPORT |
|  | TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT |
|  | TEMPORARY CONCRETE BARRIER WALL |
|  | IMPACT ATTENUATOR |
|  | |
| W24-1-48 | |
| (7) | |






ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN



- ## GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
 4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES
- ## **LANE MARKER NOTES**
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
 - B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

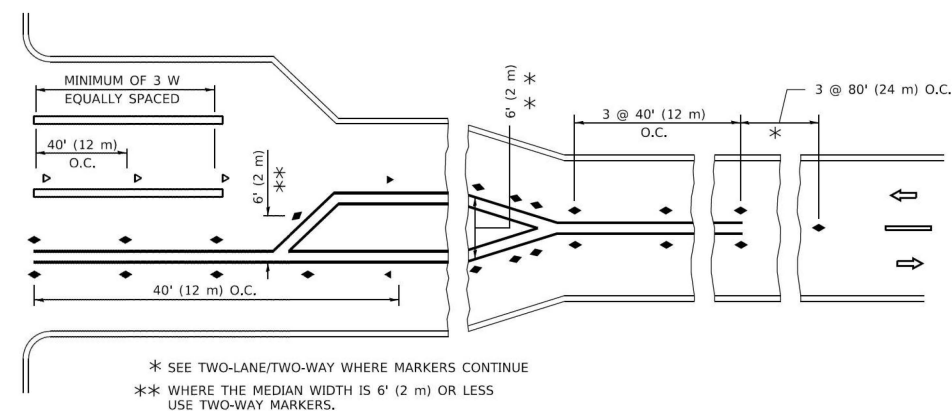
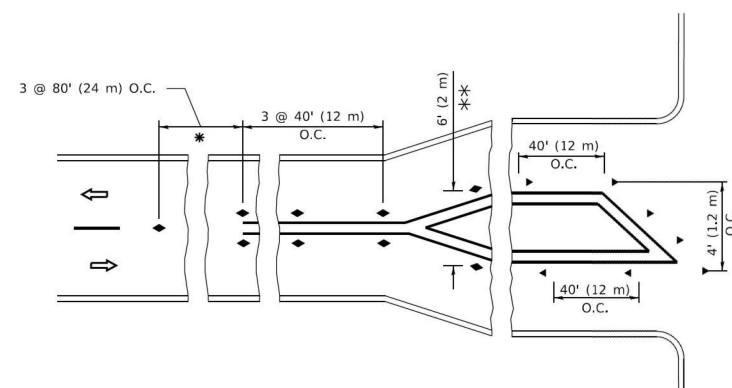
-  YELLOW STRIPE
-  WHITE STRIPE
-  ONE-WAY AMBER MARKER
-  ONE-WAY CRYSTAL MARKER (W/O)
-  TWO-WAY AMBER MARKER

LANE MARKER NOTES

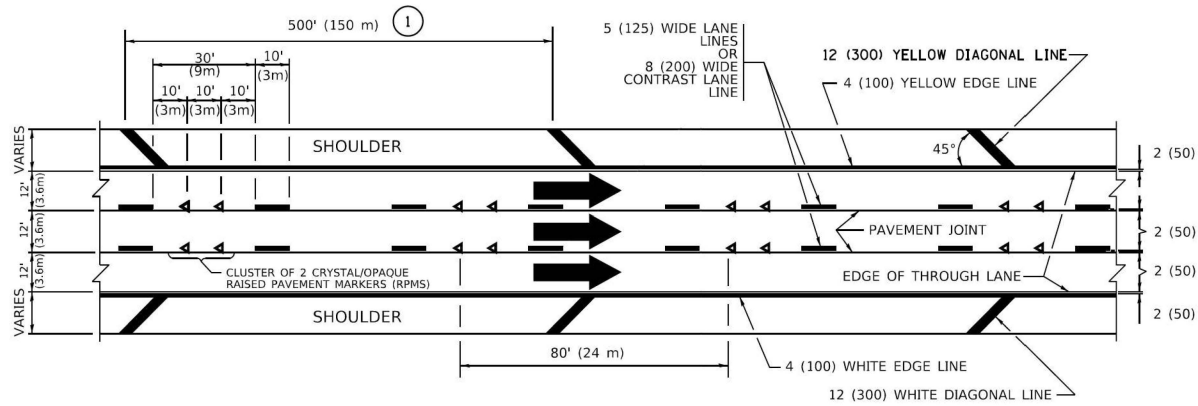
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



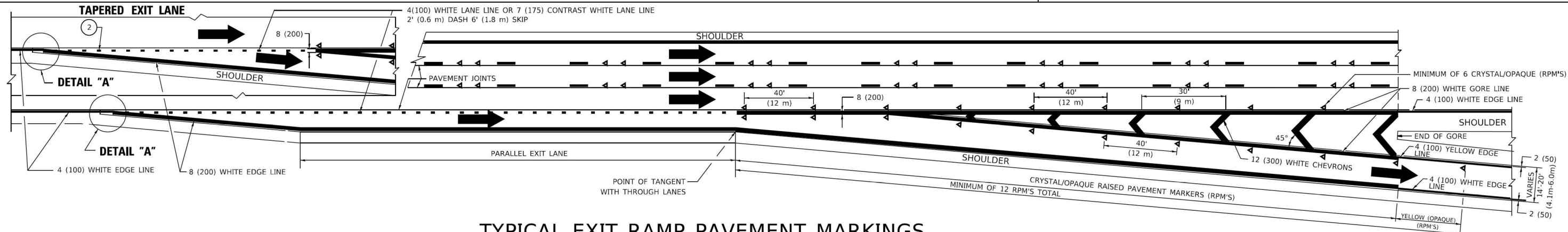
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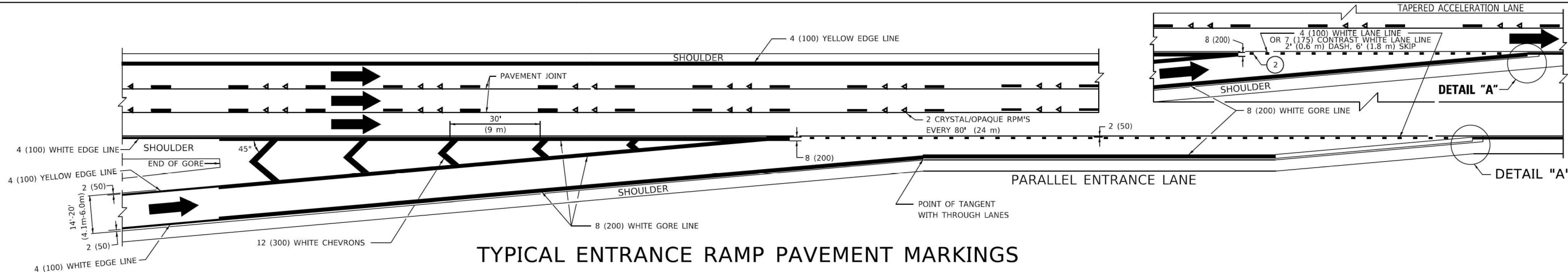
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

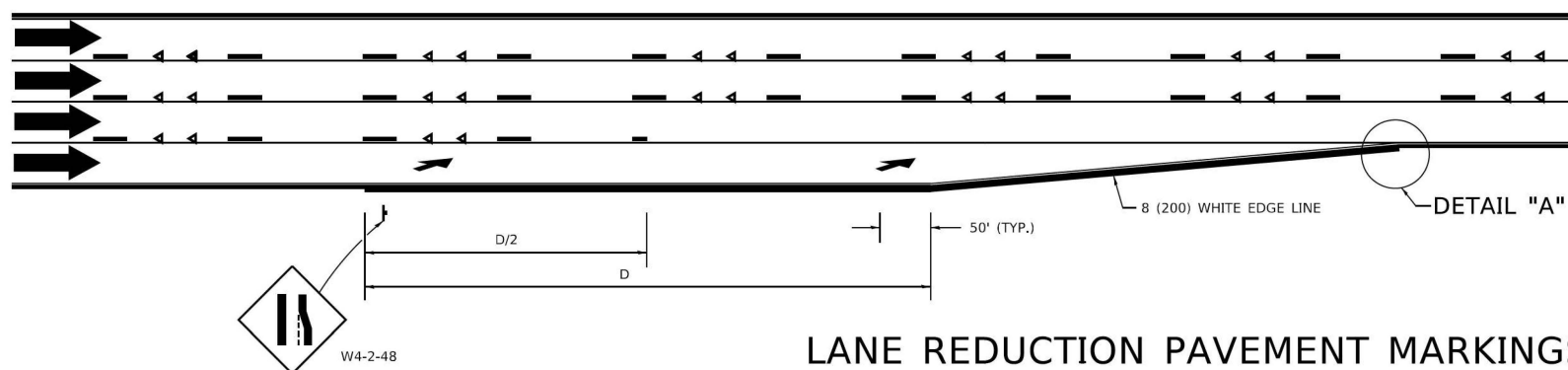
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE D, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



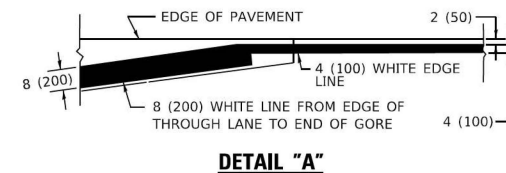
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS



NOTES:

1. THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
2. 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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312.857.1006

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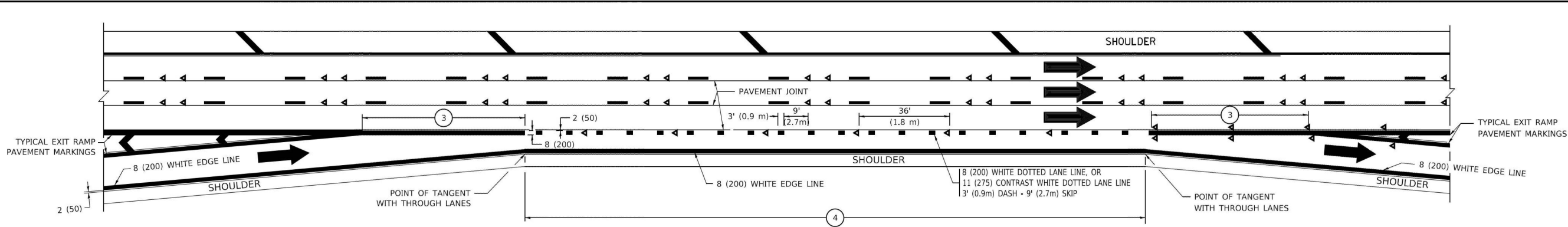
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY PAVEMENT MARKING DETAIL

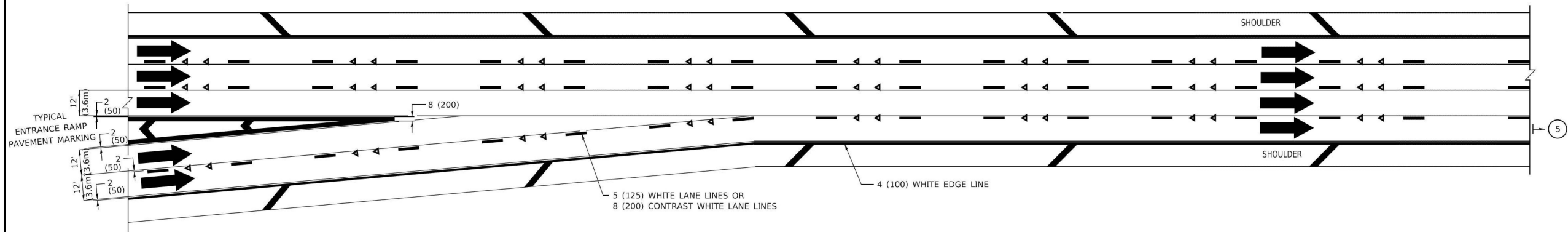
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FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

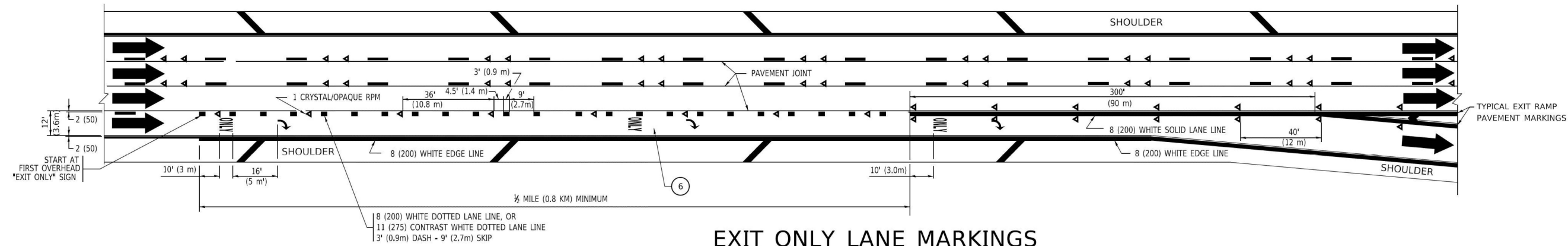
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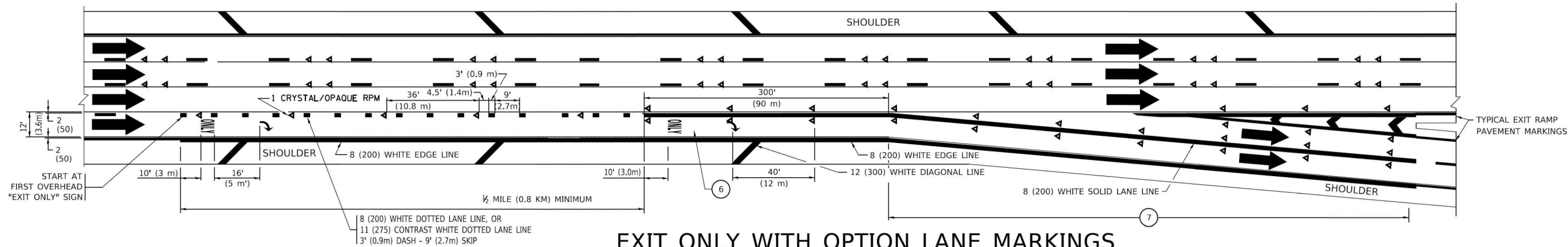
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

NOTES:

- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
- ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
- ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
- ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED..
- ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY PAVEMENT MARKING DETAIL

SCALE: SHEET NO. D-8 OF D-14 SHEETS STA. 00+000 TO STA. 00+000

FAI
RTE
55/4-55

SECTION
FAI 5523 BRIDGE REPAIR

COUNTY
DUPAGE

TOTAL
SHEETS
60

SHEET
NO.
54

CONTRACT NO. 62W21

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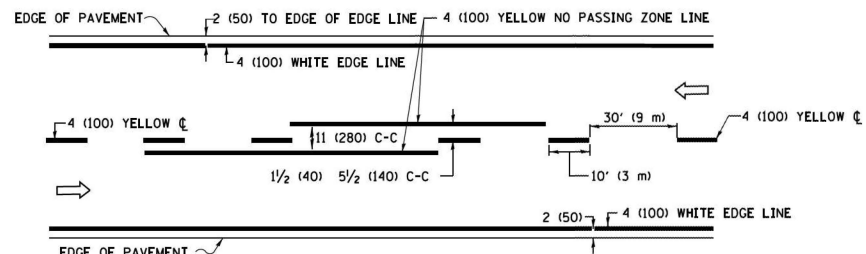
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DEPARTMENT OF TRANSPORTATION

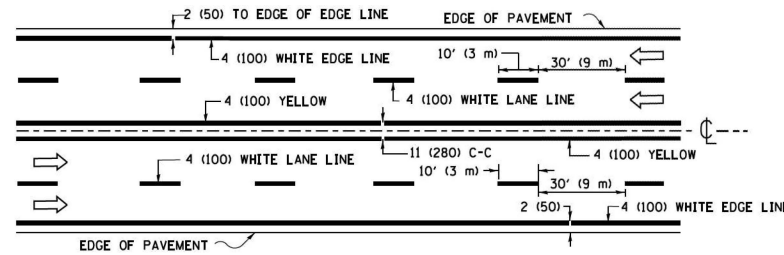
DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: SHEET NO. D-9 OF D-14 SHEETS STA. 00+000 TO STA. 00+000

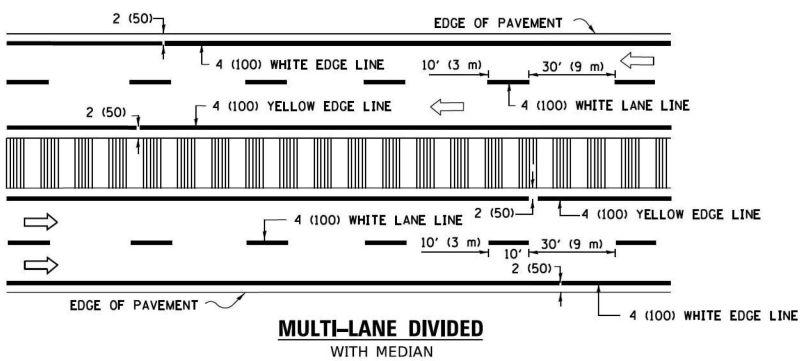
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55/4-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	55
CONTRACT NO. 62W21				
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2-LANE ROADWAY

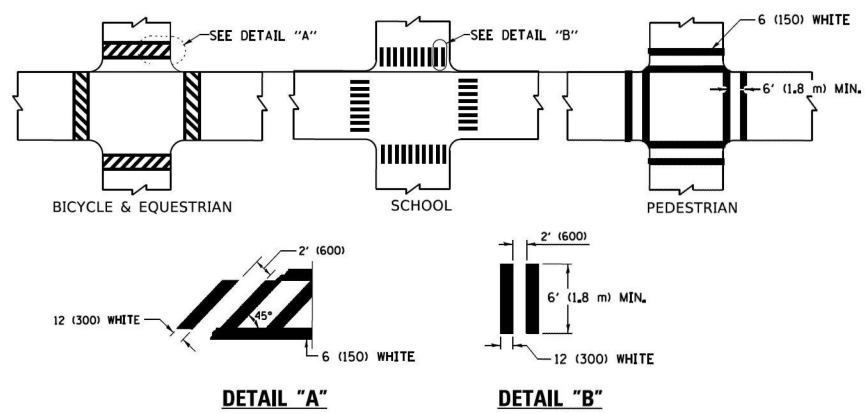


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED
WITH MEDIAN

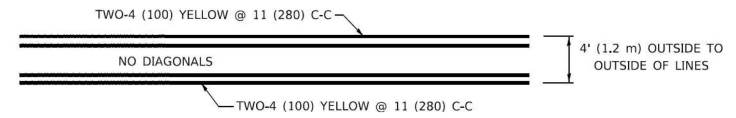
TYPICAL LANE AND EDGE LINE MARKING



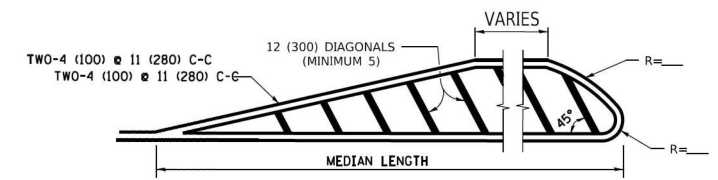
DETAIL "A" DETAIL "B"

TYPICAL CROSSWALK MARKING

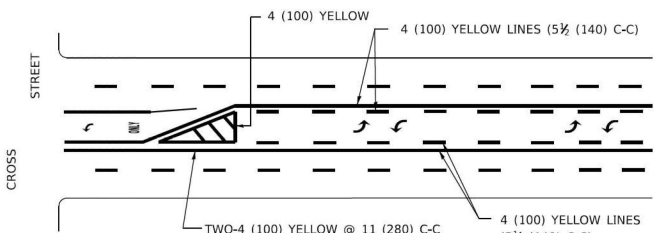
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

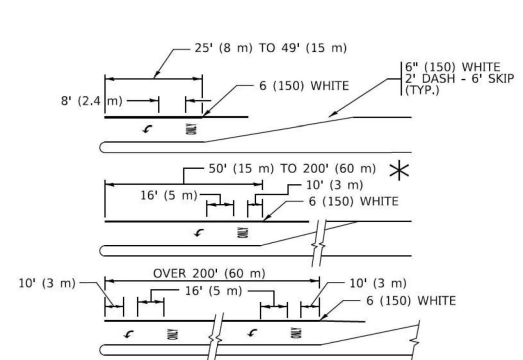


MEDIANS OVER 4' (1.2 m) WIDE



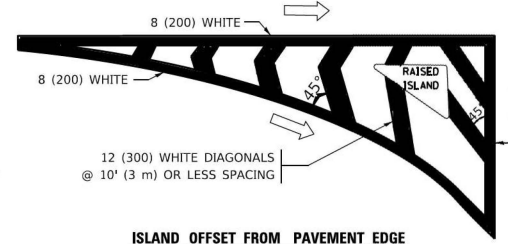
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

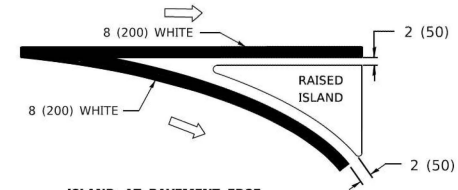


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

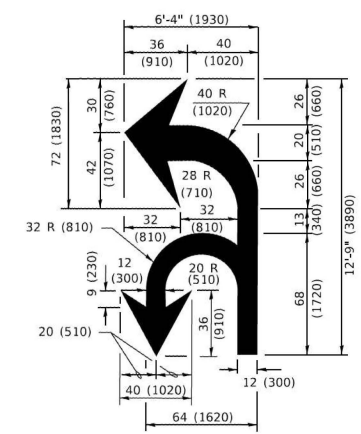


ISLAND OFFSET FROM PAVEMENT EDGE

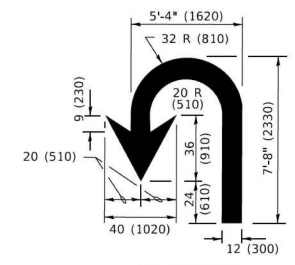


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION
LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGTUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ² EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters)
unless otherwise shown.

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

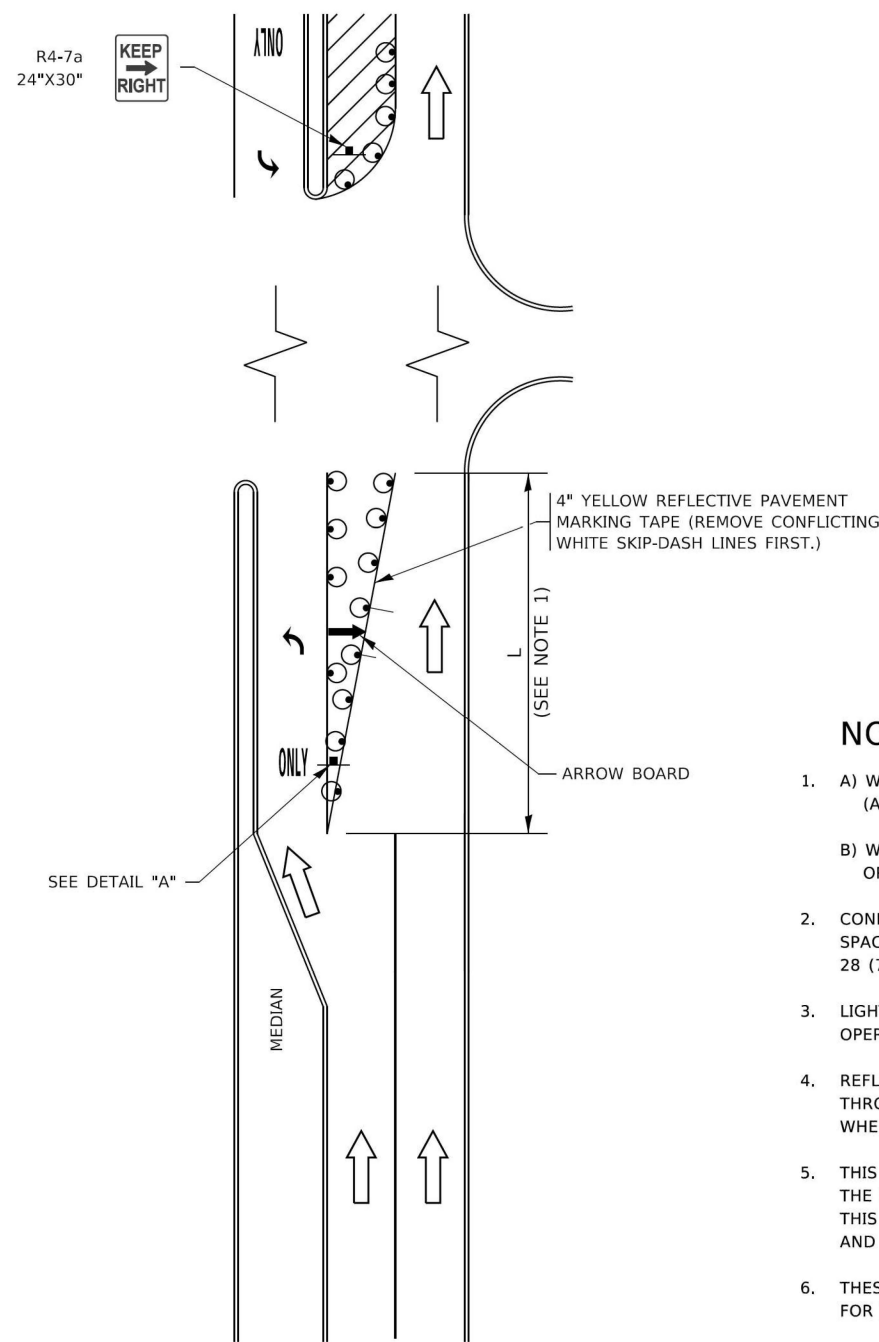
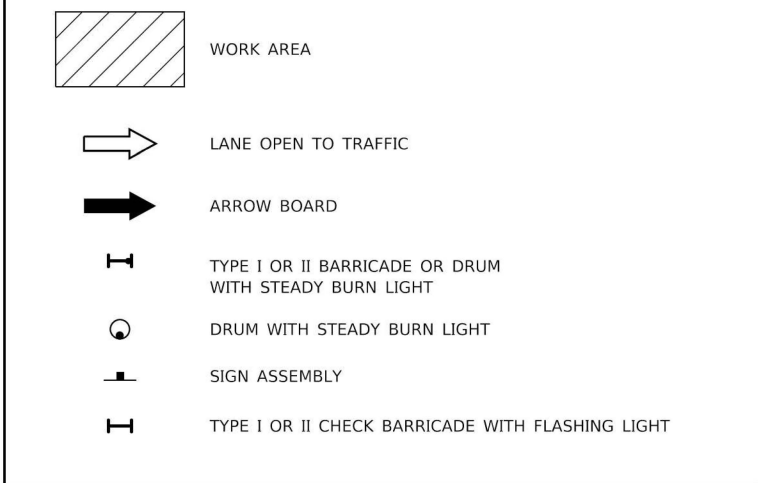


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

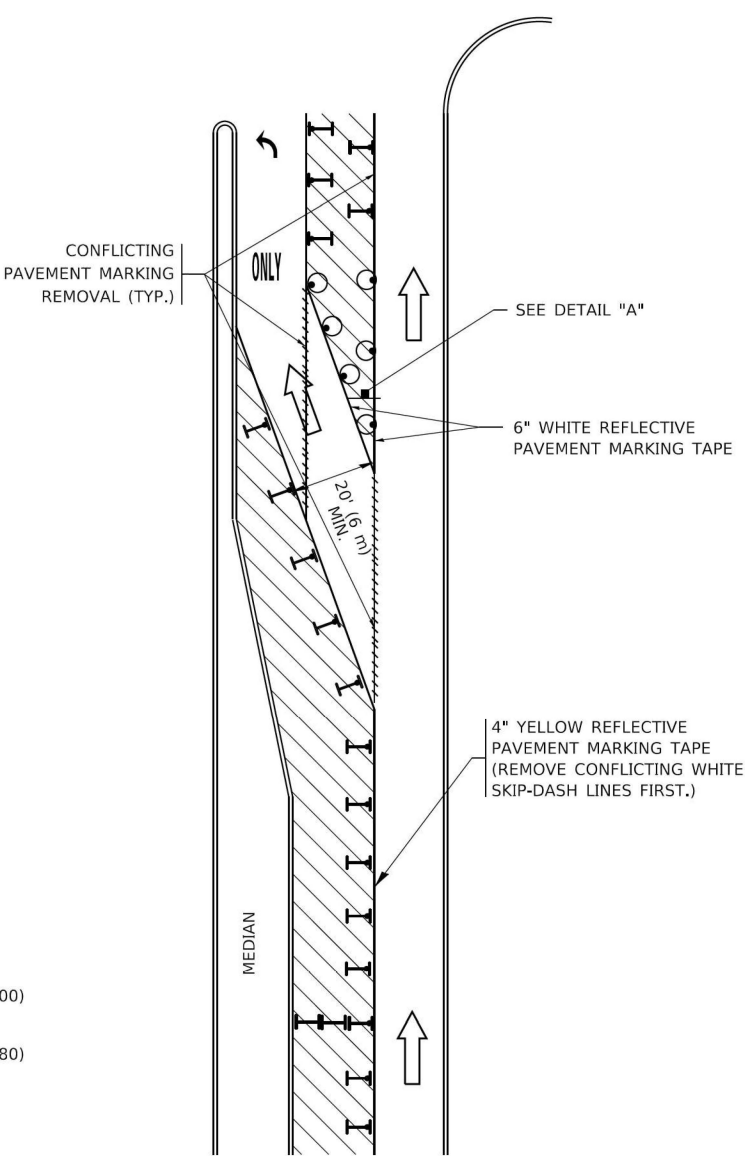
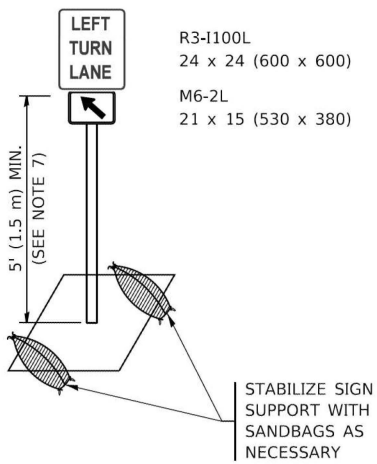


FIGURE 2



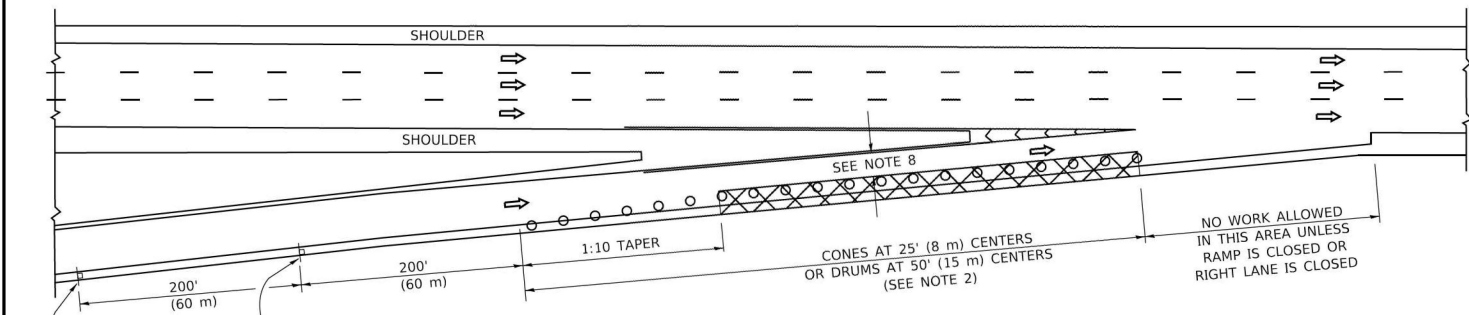
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

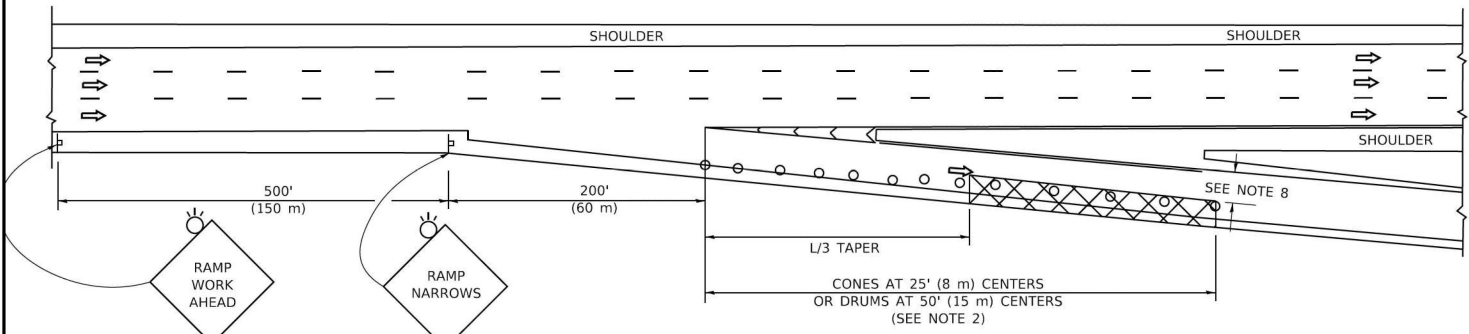
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FILE NAME:  DB STERLIN CONSULTANTS, INC. 123 N. Wacker Drive, Suite 2000 Chicago, Illinois 60606 312.857.1006	USER NAME = lcsM-obstepw21s	DESIGNED - ML	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTIVE AT TURN KEYS (TO REMAIN OPEN TO TRAFFIC)					FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100,000' / in.	DRAWN - DJB	REVISED -		55/1-55	FAI 5523 BRIDGE REPAIR	DUPAGE	60	56					
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										ILLINOIS FED. AID PROJECT				

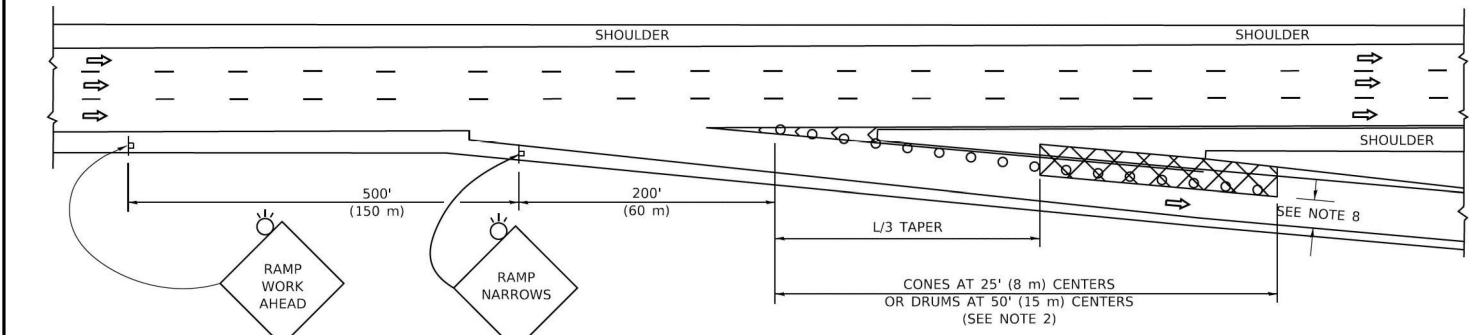
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

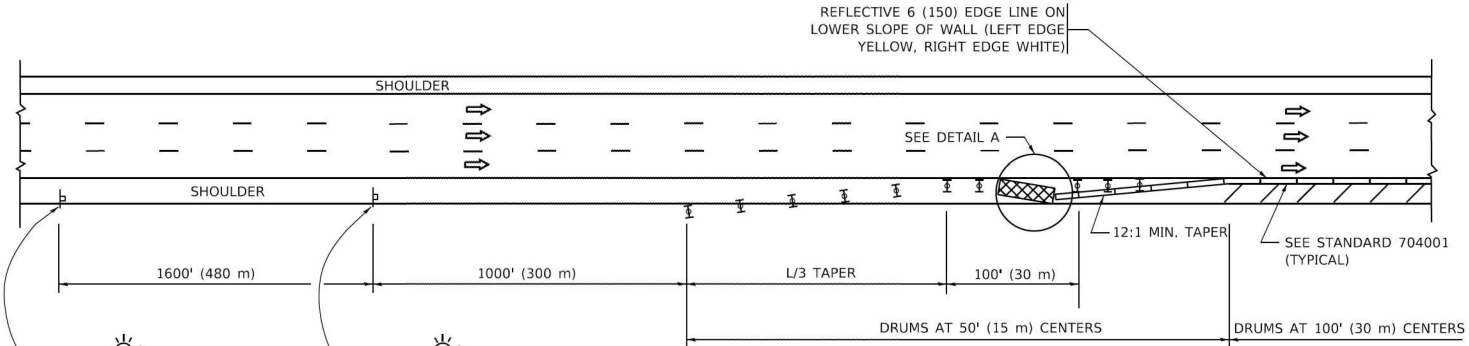
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

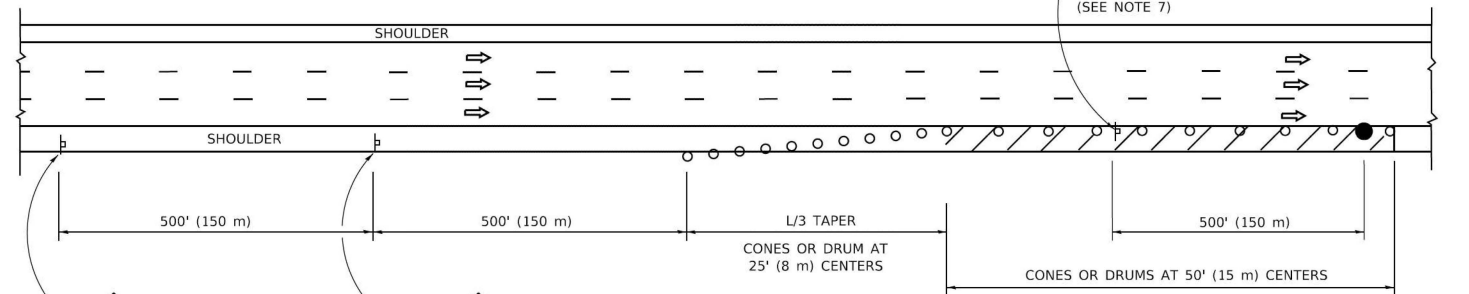
GENERAL NOTES:

- THE "L" DISTANCE EQUALS:
SPEED LIMIT FORMULAS
45 mph (80 km/h) METRIC ENGLISH
OR GREATER: $L=0.65(W)(S)$ $L=(W)(S)$
W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

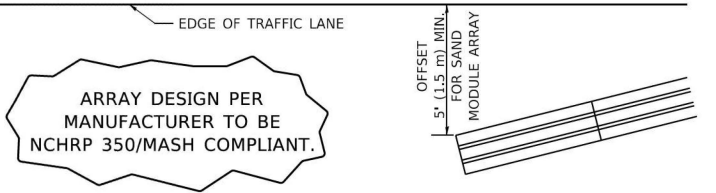


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
- 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES
AND PARTIAL RAMP CLOSURE

SCALE: SHEET NO. D-12 OF D-14 SHEETS STA. 00+000 TO STA. 00+000

FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				

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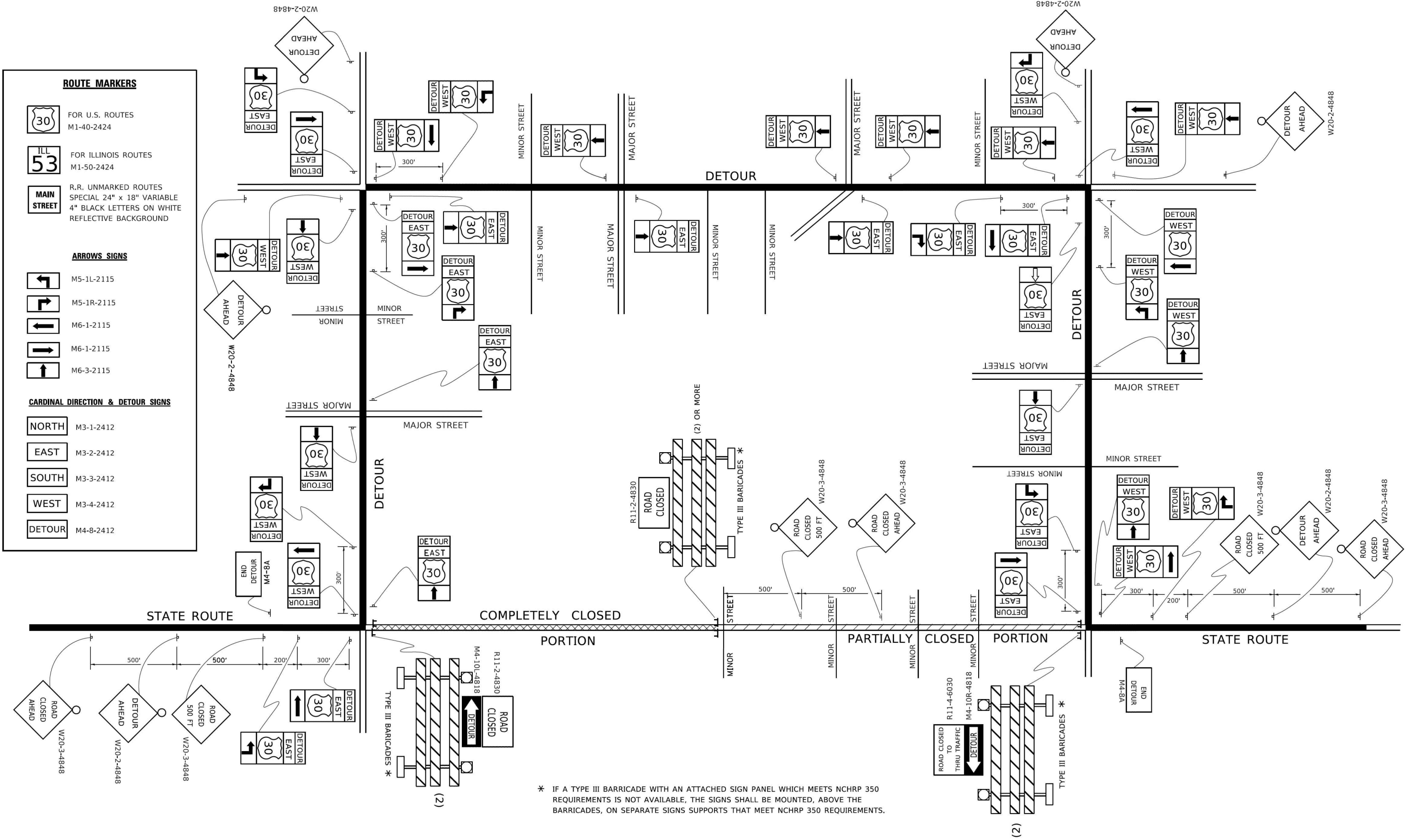
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS

SCALE: SHEET NO. D-13 OF D-14 SHEETS STA. 00+000 TO STA. 00+000

FAI RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62W21				
ILLINOIS FED. AID PROJECT				



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

