

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.P. RTE. 112	SECTION MY-RS-1	COUNTY WILL	TOTAL SHEETS 35	SHEET NO. 1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60N42	

*35+1=36

D-91-312-11

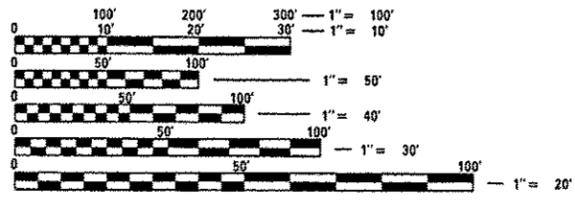
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE VILLAGE OF
ROMEovILLE AND IN THE CITY OF
CREST HILL

**PROPOSED
HIGHWAY PLANS**

**FAP 112: IL. 53 /IL. 7 (N. BROADWAY ST.)
CATON FARM RD. TO RENWICK RD.**

**SECTION: MY-RS-1
RESURFACING (3P)
WILL COUNTY
C-91-312-11**

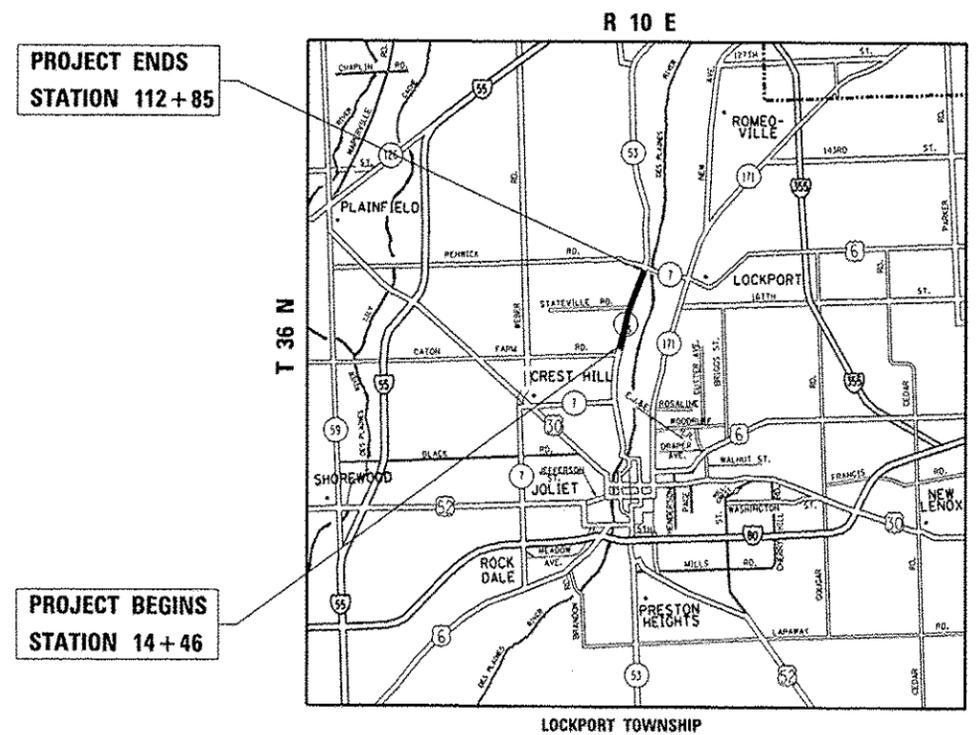


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 (OR 811)

PROJECT MANAGER: KEN ENG
PROJECT ENGINEER: JENPAI CHANG (847) 705 - 4432

CONTRACT NO. 60N42



TRAFFIC DATA:
2009 ADT = 22,300
SPEED LIMIT = 35 - 45 MPH

PROJECT BEGINS
STATION 14 + 46

PROJECT ENDS
STATION 112 + 85

GROSS LENGTH AND NET LENGTH OF PROJECT = 9,839.00 FEET = 1.86 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *Mr. J. ...* 2014
John D. Baranzano, P.E.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

June 27 2014
Cher Osman, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

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34	DISTRICT 1 - STANDARD TRAFFIC SIGNAL DESIGN DETAIL (TS-05)
35	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD, STRIPS/SHLOS, WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-10	STEEL PLATE BEAM GUARDRAIL
630201-06	PCC / HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAIL
701301-04	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701426-06	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEED > 45 MPH
701606-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-03	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT 800-892-0123 (OR 811) FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF LOCKPORT AND THE VILLAGE OF ROMEVILLE.

WHEN CONSTRUCTING SIDEWALK RAMPS FOR THE HANDICAPPED (STATE STANDARD 424001), USE TYPE B RAMPS UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS 45 MPH WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT CORY JUCIUS TRAFFIC FIELD ENGINEER, AT (815) 485-6475 (SS) OR AT (847) 705-4411 (OFFICE), A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY AND RECORD LOCATIONS OF DETECTOR LOOP FOR REPLACEMENT AT INTERSECTIONS MAINTAINED BY MUNICIPALITIES.

THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL"

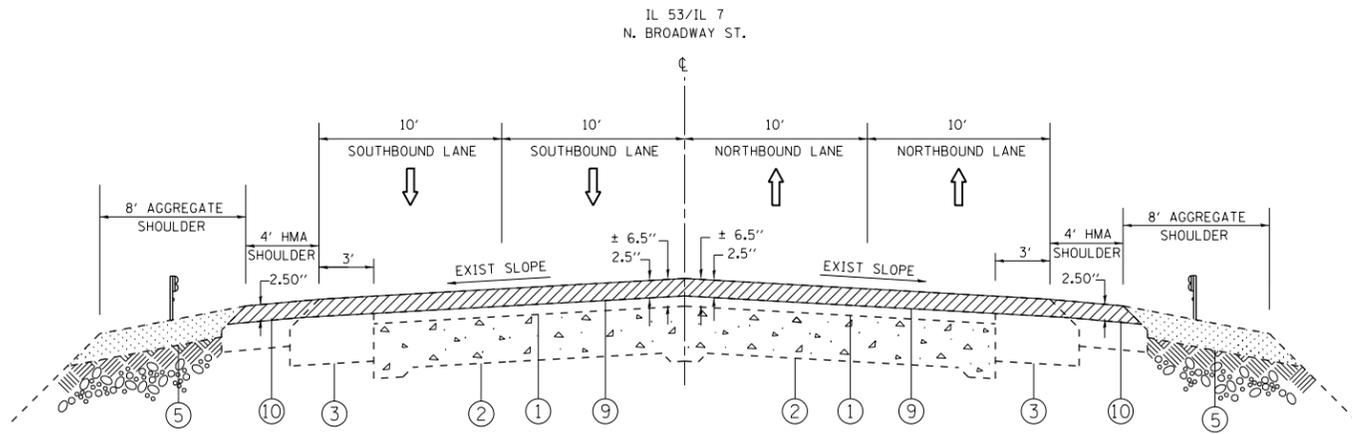
THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

FILE NAME =	USER NAME = galbenj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. 53 /IL. 7 (BROADWAY STREET) INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\p\dot\galbenj\08249591\0131	11-11-11-plandgn	DRAWN -	REVISED -			112	MY-RS-1	WILL	35	2
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -			CONTRACT NO. 60N42				
PLOT DATE = 5/29/2014		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1

Rev.

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY ROADWAY 0005						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY ROADWAY 0005					
• 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	27	27						60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4					
• 25200110	SODDING, SALT TOLERANT	SQ YD	27	27						60255500	MANHOLES TO BE ADJUSTED	EACH	3	3					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	94	94						* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	225	225					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2630	2630						* 63100167	TRAFFIC BARRIER TERMINAL, TYPE I (SPECIAL) TANGENT	EACH	6	6					
40600895	CONSTRUCTING TEST STRIP	EACH	2	2						63200310	GUARDRAIL REMOVAL	FOOT	225	225					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1016	1016						67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	6135	6135						67100100	MOBILIZATION	L SUM	1	1					
42001300	PROTECTIVE COAT	SQ YD	53	53						70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	60781	60781						70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	435	435						70300100	SHORT TERM PAVEMENT MARKING	FOOT	11125	11125					
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	150	150						70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	219	219					
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	750	750						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	47014	47014					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	578	578															



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 14+46 TO STA. 56+69

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 26+58 TO STA. 27+62 (SB)
STA. 37+41 TO STA. 39+94 (SB)

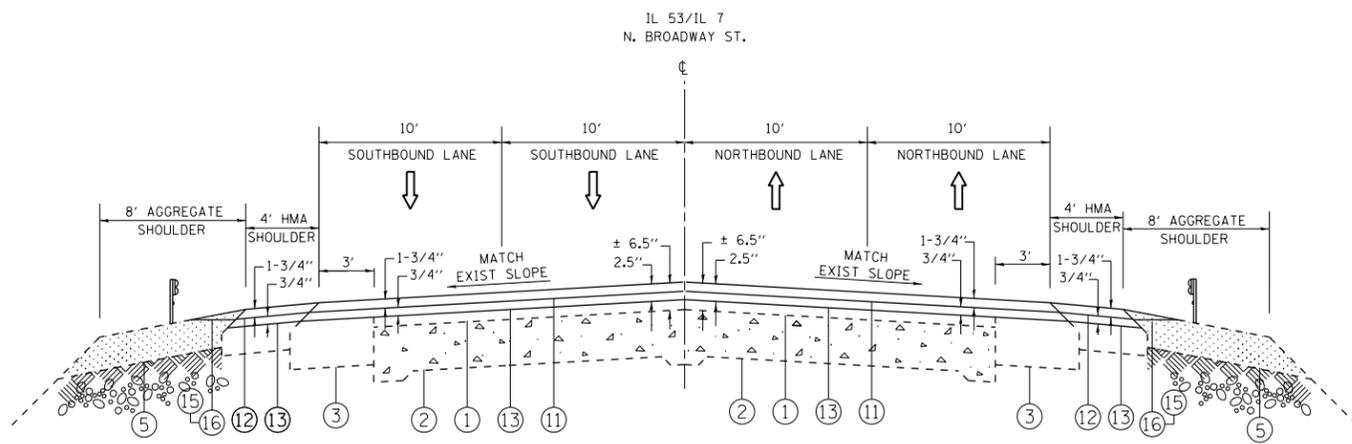
EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 25+28 TO STA. 27+17 (NB)
STA. 35+43 TO STA. 39+31 (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9'-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
- ⑤ EXISTING AGGREGATE SHOULDER, TYPE B, 8"
- ⑥ EXISTING CONCRETE BARRIER MEDIAN, TYPE B-12.12
- ⑦ EXISTING COMB. CONC. C&G, TYPE B-6.24
- ⑧ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A, 4"
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (MAINLINE)
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (SHOULDER)
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1-3/4" (MAINLINE)
- ⑫ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1-3/4" (SHOULDER)
- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- *⑭ PROPOSED COMB. CONC. C&G REMOVAL AND REPLACEMENT
- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
- " THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".



IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 14+46 TO STA. 56+69

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 26+58 TO STA. 27+62 (SB)
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HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (OMP)
MIXTURE TYPE	AIR VOIDS (%) @ NDES	
MAINLINE RESURFACING		
POLY. HMA SURFACE COURSE MIX "F", N90, 1-3/4"	4% @ 90 GYR.	OCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	OCP
HOT-MIX ASPHALT PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm), 11"	4% @ 70 GYR.	OC/OA
SHOULDER RESURFACING		
POLY. HMA SURFACE COURSE MIX "F", N90, 1-3/4"	4% @ 90 GYR.	OCP
POLY. LEVELING BINDER (MM) IL-4.75, N50, 3/4"	3.5% @ 50 GYR.	OCP
OMP DESIGNATION QUALITY CONTROL/QUALITY ASSURANCE (OC/OA) QUALITY CONTROL FOR PERFORMANCE (QCP)		

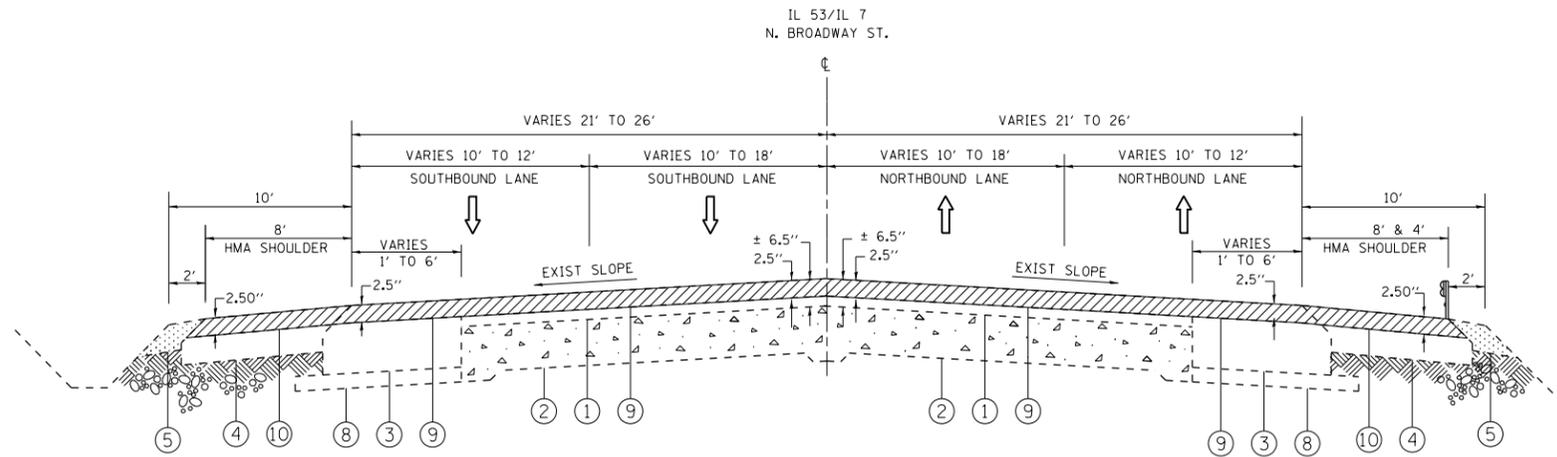
NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUND PER SQUARE YARD-INCH

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

"FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS".

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 56+69 TO STA. 58+87
STA. 71+32 TO STA. 74+65

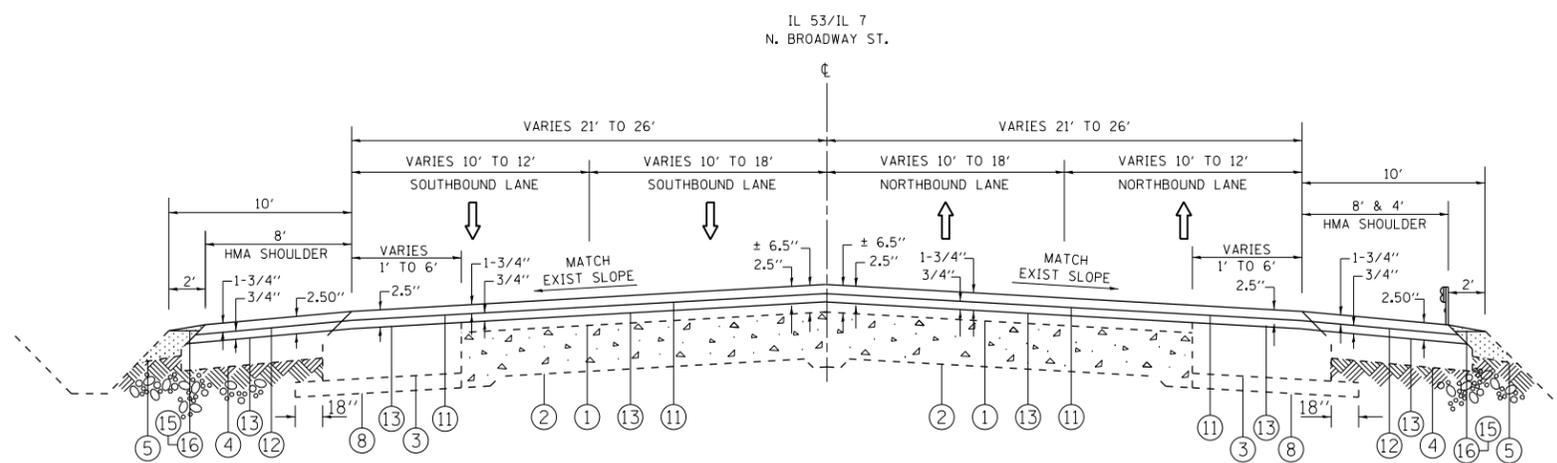
EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 58+11 TO STA. 59+04 (NB)
ATTACHED TO THE RETAINING WALL

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
- ⑤ EXISTING AGGREGATE SHOULDER, TYPE B, 8"
- ⑥ EXISTING CONCRETE BARRIER MEDIAN, TYPE B-12.12
- ⑦ EXISTING COMB. CONC. C&G, TYPE B-6.24
- ⑧ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A, 4"
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (MAINLINE)
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (SHOULDER)
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1-3/4" (MAINLINE)
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- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑭ PROPOSED COMB. CONC. C&G REMOVAL AND REPLACEMENT
- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

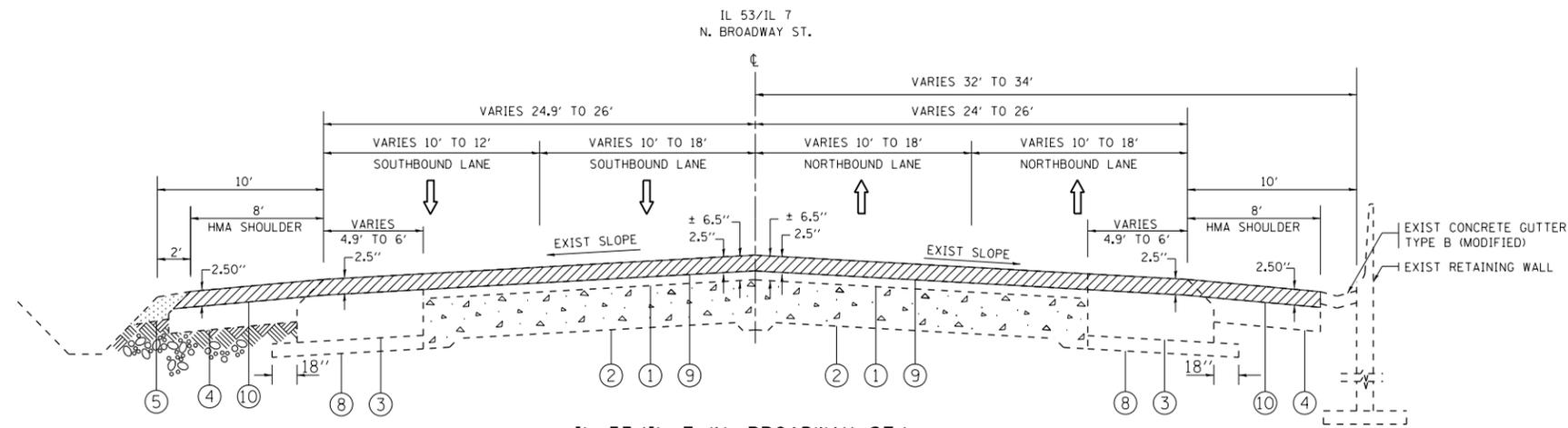
- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
- " THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".



IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 56+69 TO STA. 58+87
STA. 71+32 TO STA. 74+65

EXISTING GUARDRAIL:
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STA. 58+11 TO STA. 59+04 (NB)
ATTACHED TO THE RETAINING WALL

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 /IL 7 (N. BROADWAY STREET) EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\galbenjr\d0249591\0131211-sh-t-plan.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -					112	MY-RS-1	WILL	35	5
PLOT DATE = 5/23/2014	DATE -	CHECKED -	REVISED -		CONTRACT NO. 60N42							
					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 58+87 TO STA. 60+02

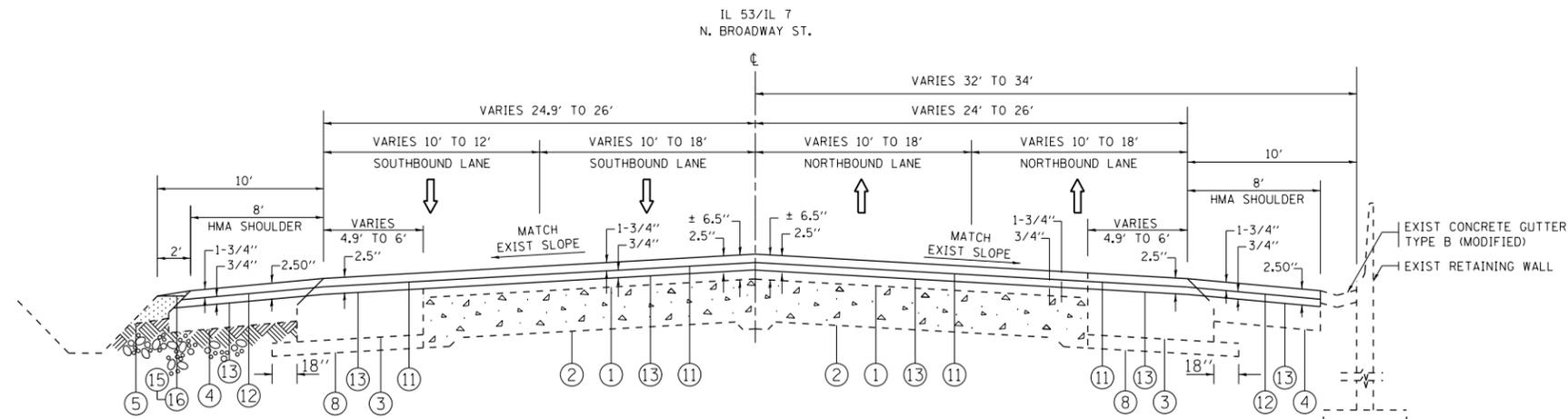
EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
STA. 58+87 TO STA. 62+95 (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
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- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

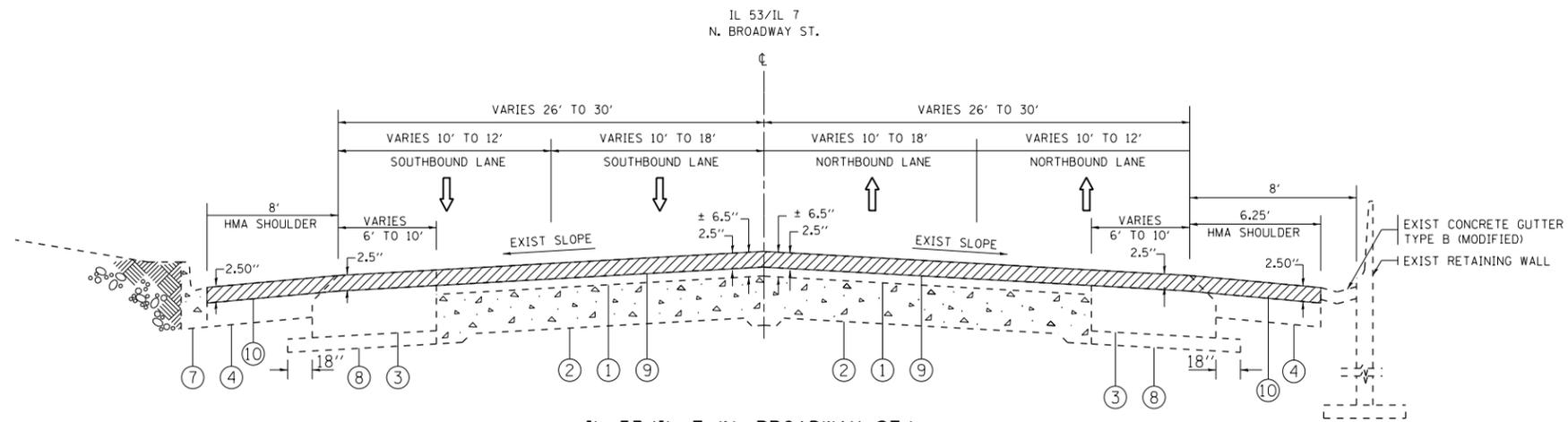
- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
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IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 58+87 TO STA. 60+02

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
STA. 58+87 TO STA. 62+95 (NB)

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 /IL 7 (N. BROADWAY STREET) EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\id\galbenjr\d0249591\0131211-sh-t-plan.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -					112	MY-RS-1	WILL	35	6
	PLOT DATE = 5/23/2014	CHECKED -	REVISED -		SCALE:			SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60N42	
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 60+02 TO STA. 63+09

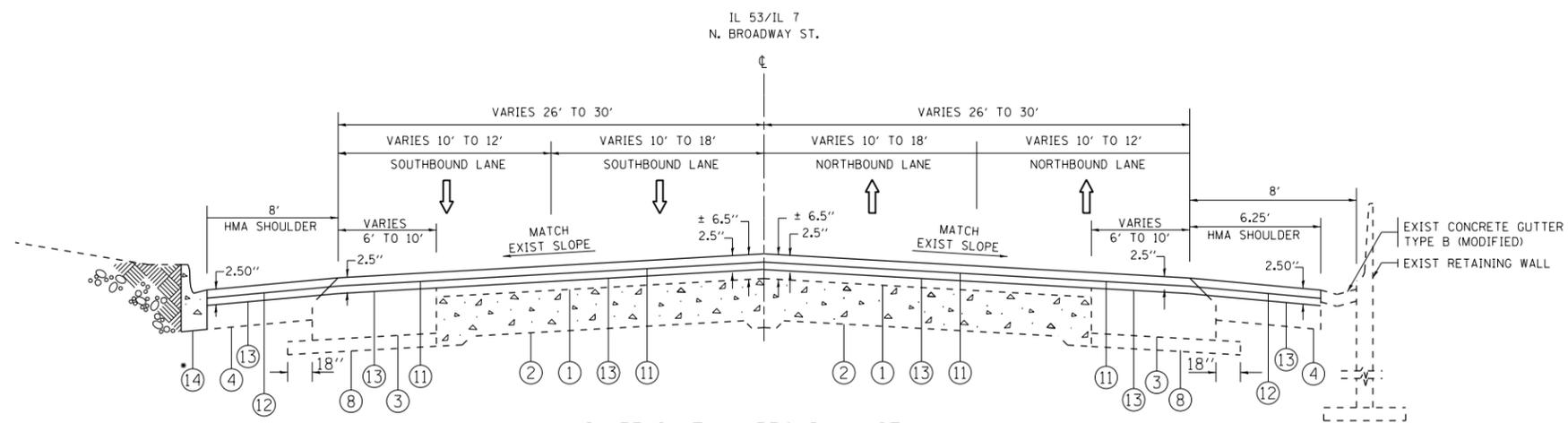
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- ① EXISTING HMA SURFACING, ± 6.5"
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- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
- " THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".



IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 60+02 TO STA. 63+09

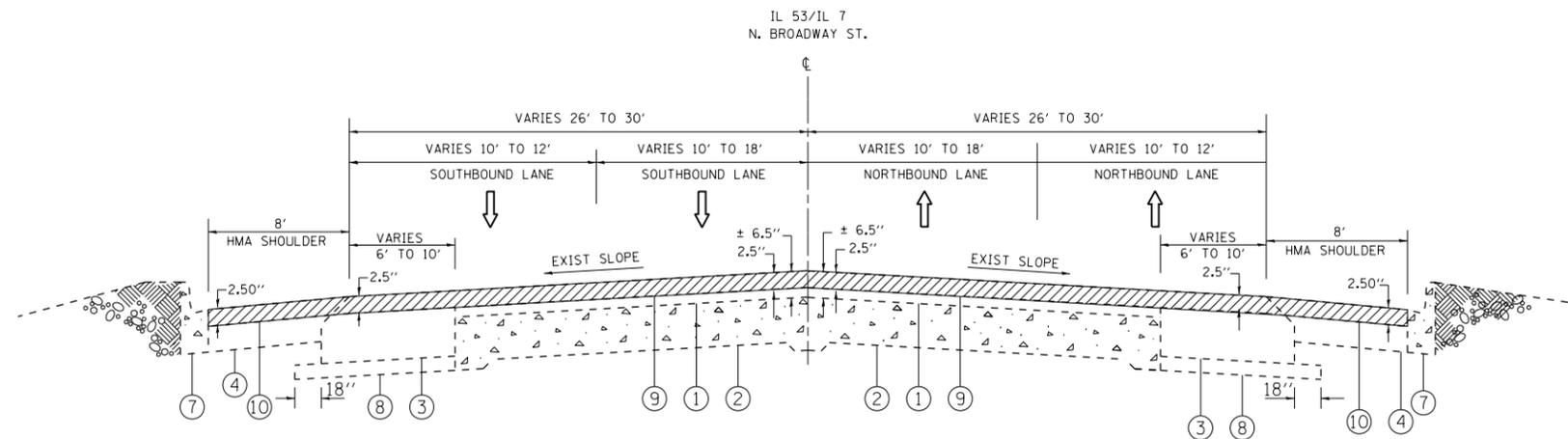
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STA. 58+87 TO STA. 62+95 (NB)

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

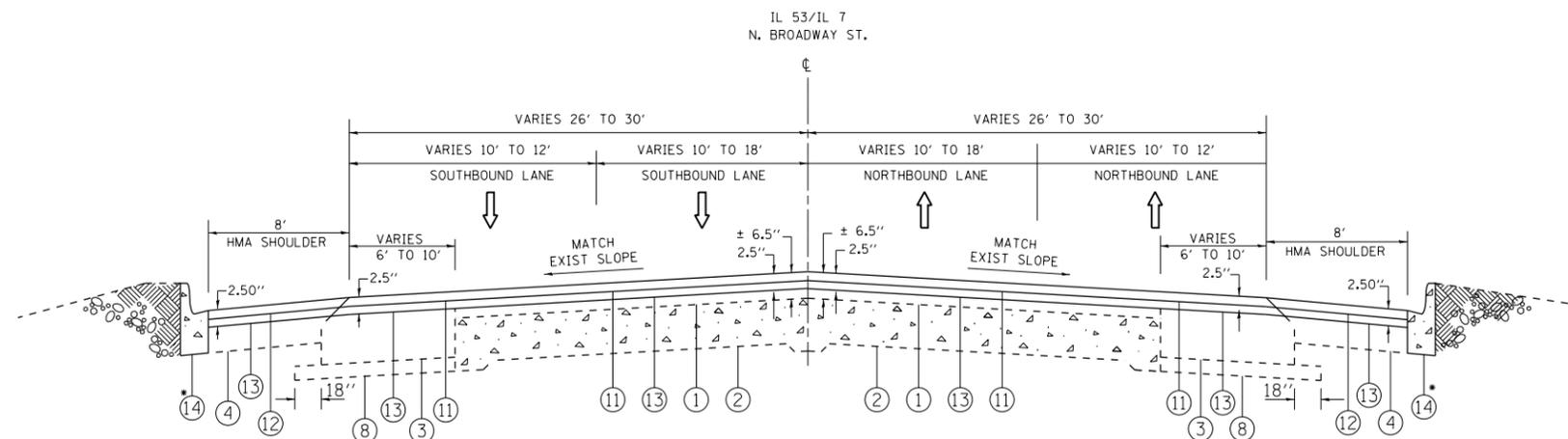
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 53 /IL 7 (N. BROADWAY STREET)			
EXISTING AND PROPOSED TYPICAL SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	7
CONTRACT NO. 60N42				
ILLINOIS FED. AID PROJECT				



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 63+09 TO DIVISION ST.



IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 63+09 TO DIVISION ST.

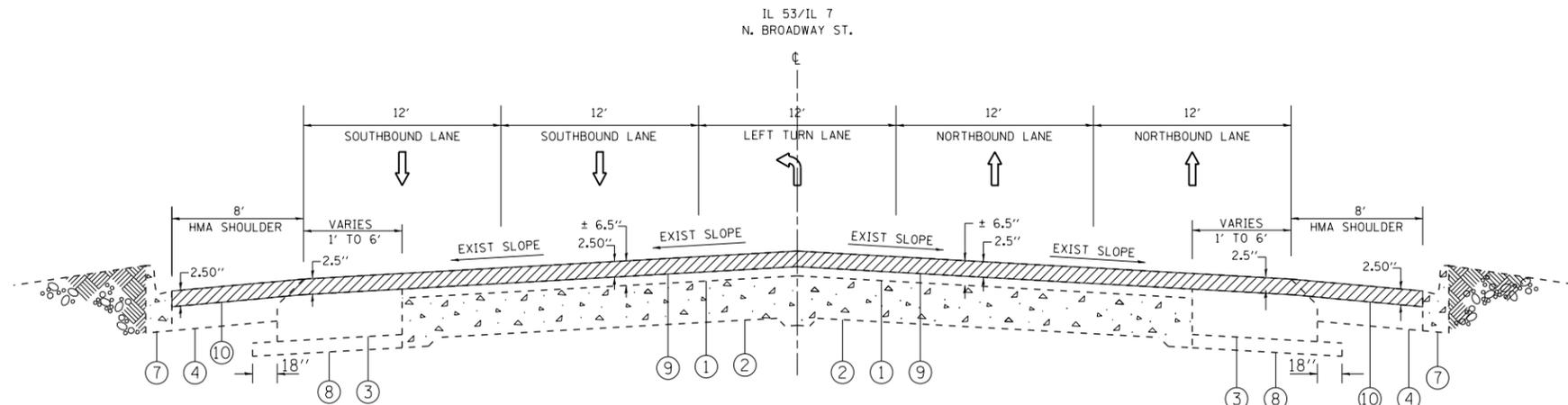
LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
- ⑤ EXISTING AGGREGATE SHOULDER, TYPE B, 8"
- ⑥ EXISTING CONCRETE BARRIER MEDIAN, TYPE B-12.12
- ⑦ EXISTING COMB. CONC. C&G, TYPE B-6.24
- ⑧ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A, 4"
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (MAINLINE)
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (SHOULDER)
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F" , N90, 1-3/4" (MAINLINE)
- ⑫ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F" , N90, 1-3/4" (SHOULDER)
- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑭ PROPOSED COMB. CONC. C&G REMOVAL AND REPLACEMENT
- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER , TYPE B
- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

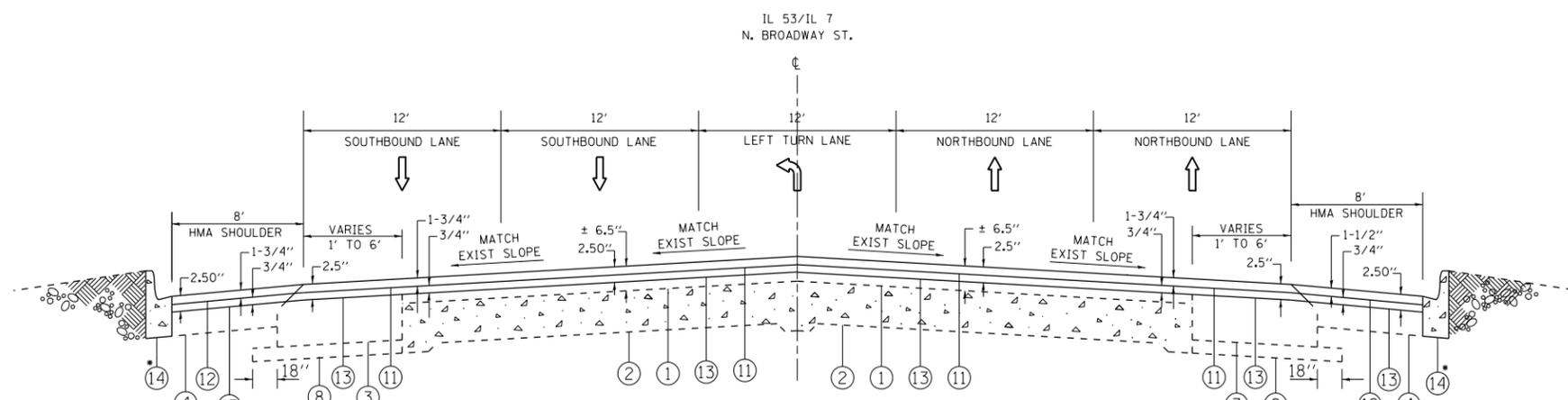
NOTE:

- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
- " THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".

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	PLOT DATE = 5/23/2014	CHECKED -	REVISED -			CONTRACT NO. 60N42				
		DATE -	REVISED -			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
AT DIVISION ST. (FACING NORTH)

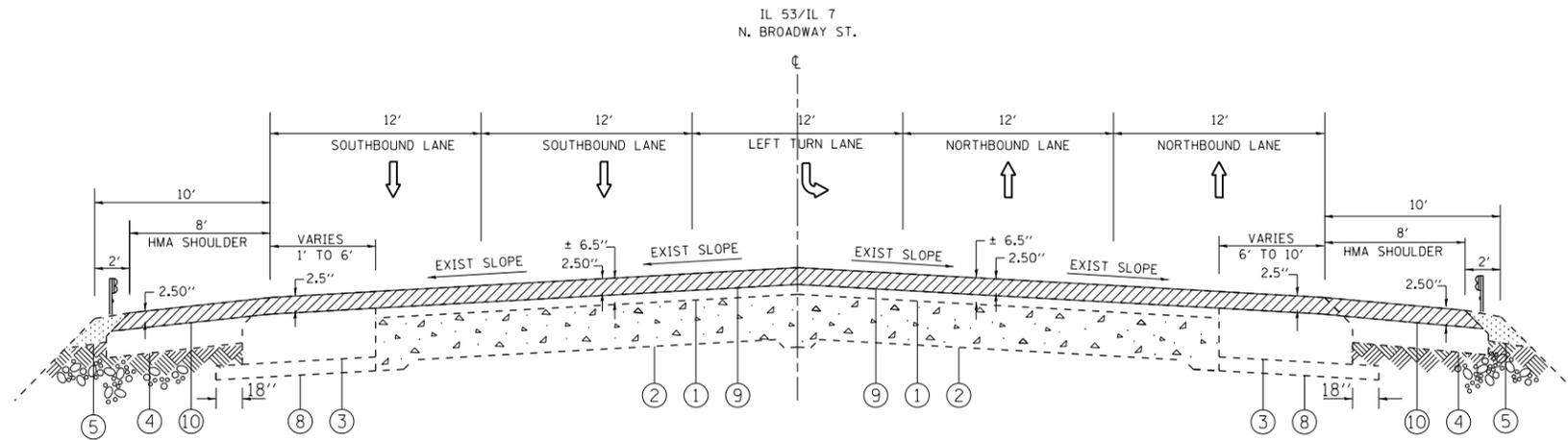


IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
AT DIVISION ST. (FACING NORTH)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9'-7"-9'')
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
- ⑤ EXISTING AGGREGATE SHOULDER, TYPE B, 8"
- ⑥ EXISTING CONCRETE BARRIER MEDIAN, TYPE B-12.12
- ⑦ EXISTING COMB. CONC. C&G, TYPE B-6.24
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- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑭ PROPOSED COMB. CONC. C&G REMOVAL AND REPLACEMENT
- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:
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" THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
AT DIVISION ST. (FACING SOUTH)

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST. TO STA. 67+32 (SB)

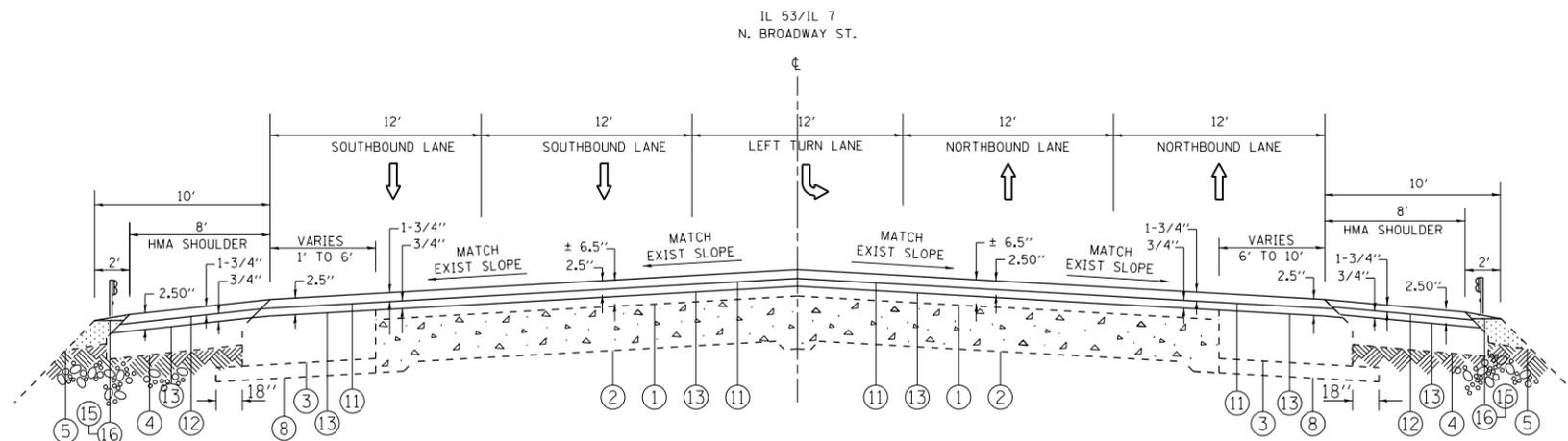
EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST TO STA. 68+56 (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
- ⑤ EXISTING AGGREGATE SHOULDER, TYPE B, 8"
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- ⑦ EXISTING COMB. CONC. C&G, TYPE B-6.24
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NOTE:

- LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
- " THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING"

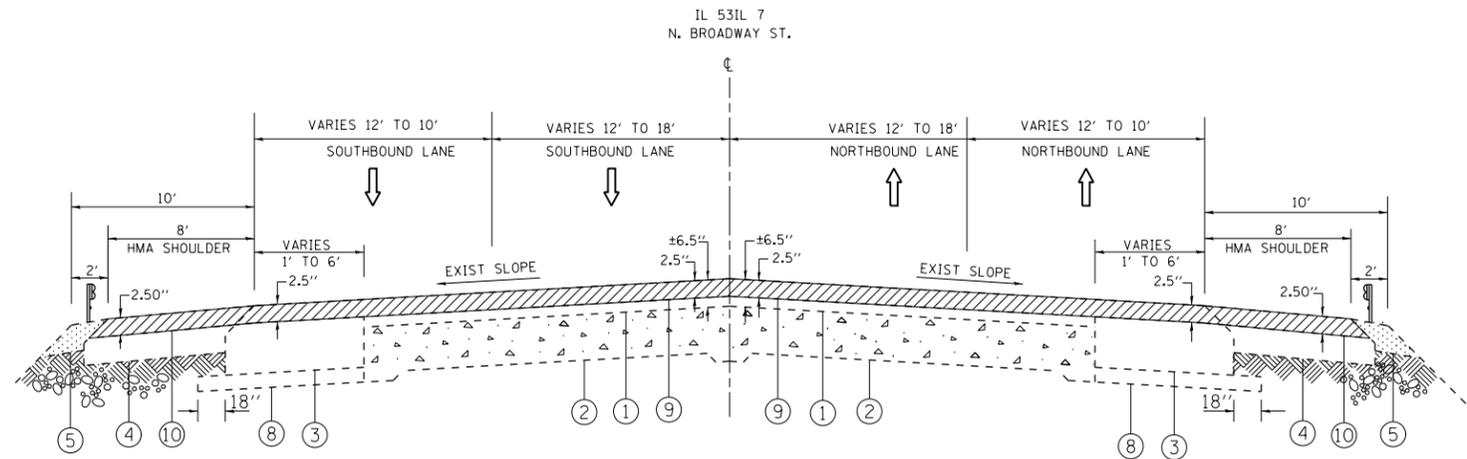


IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
AT DIVISION ST. (FACING SOUTH)

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST. TO STA. 67+32 (SB)

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST TO STA. 68+56 (NB)

FILE NAME = c:\pwork\pwork\galbenj\d0249591\0131211-sh-t-plan.dgn	USER NAME = galbenj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 /IL 7 (N. BROADWAY STREET) EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					112	MY-RS-1	WILL	35	10
PLOT DATE = 5/23/2014	DATE -	REVISED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60N42				
							ILLINOIS FED. AID PROJECT					



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
DIVISION ST. TO STA. 74+65

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST. TO STA. 67+32 (SB)

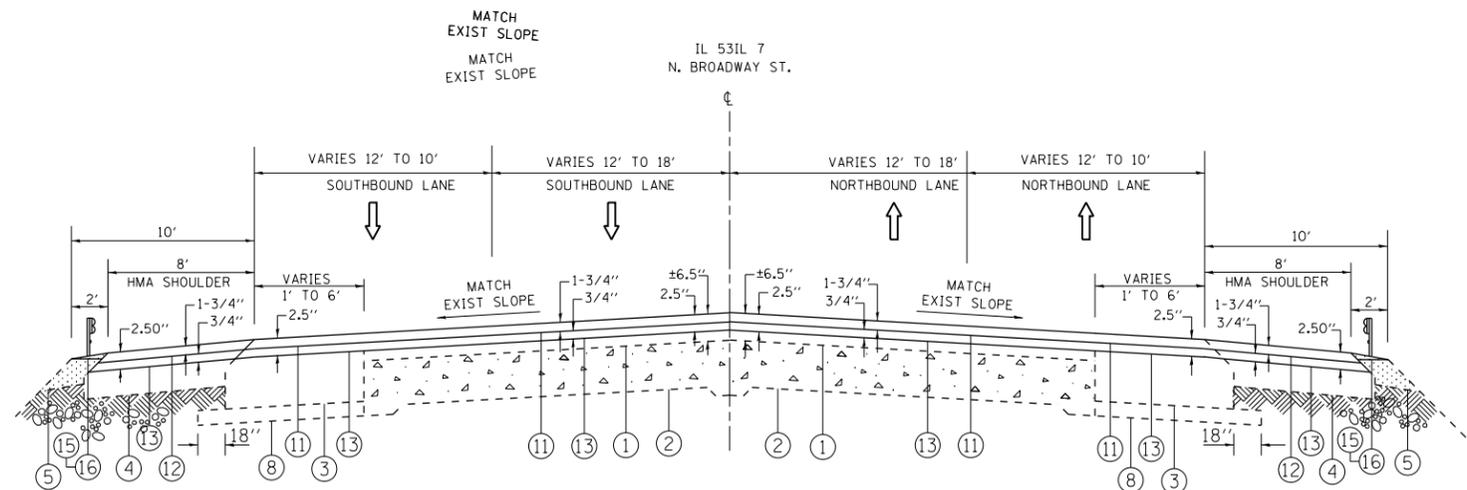
EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST TO STA. 68+56 (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9'-7"-9'-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
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- ⑭ PROPOSED COMB. CONC. C&G REMOVAL AND REPLACEMENT
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- ⑯ PROPOSED GRADING AND SHAPING SHOULDERS

NOTE:

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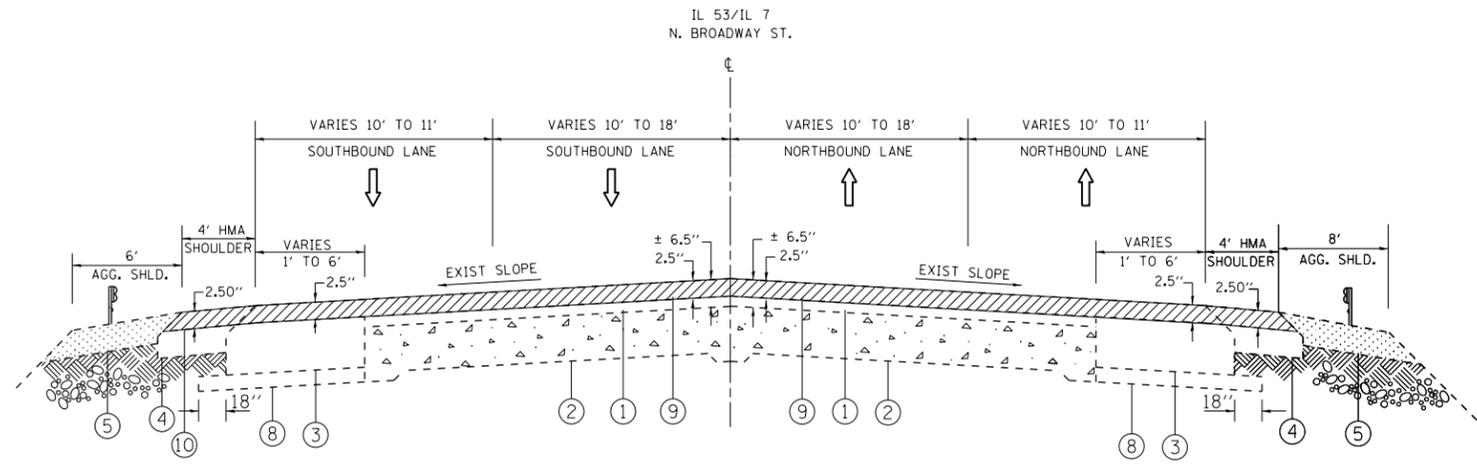


IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
DIVISION ST. TO STA. 74+65

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST. TO STA. 67+32 (SB)

EXISTING RETAINING WALL:
APPROXIMATE LOCATION IS FROM
DIVISION ST TO STA. 68+56 (NB)

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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -				112	MY-RS-1	WILL	35	11	
PLOT DATE = 5/23/2014	DATE -	REVISED -	REVISED -	SCALE:		SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 60N42				
											ILLINOIS FED. AID PROJECT	



IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 74+65 TO STA. 105+40

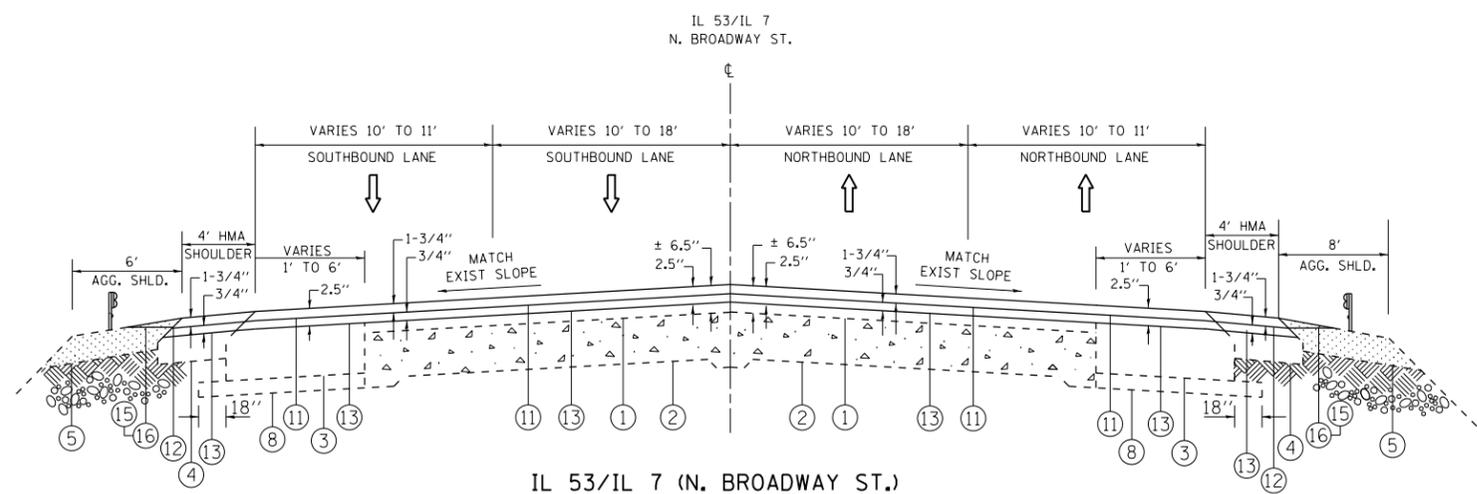
EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
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- ⑮ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
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NOTE:
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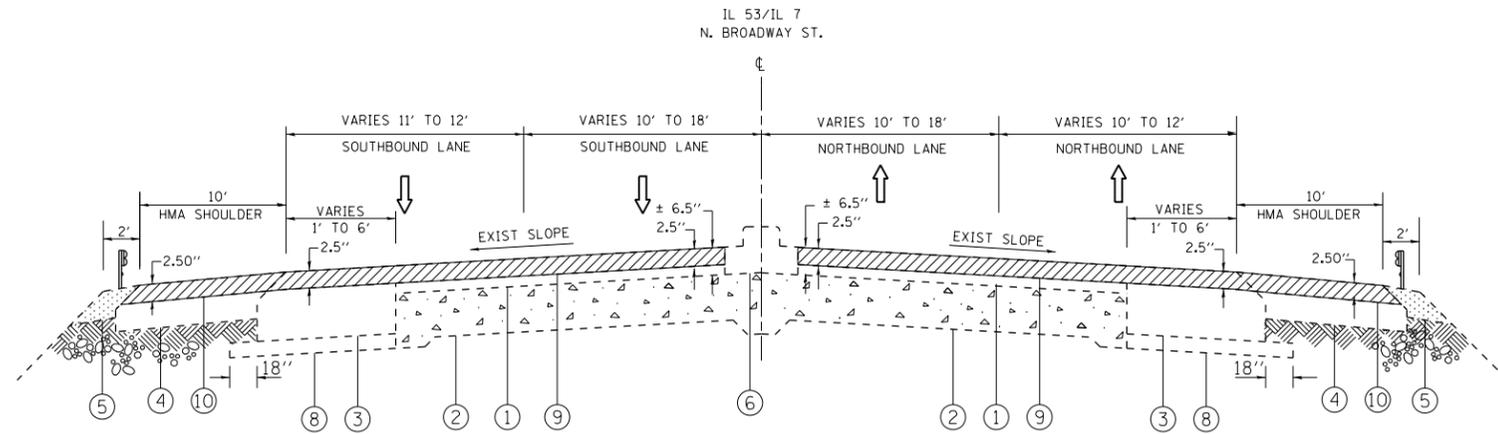


IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 74+65 TO STA. 105+40

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)

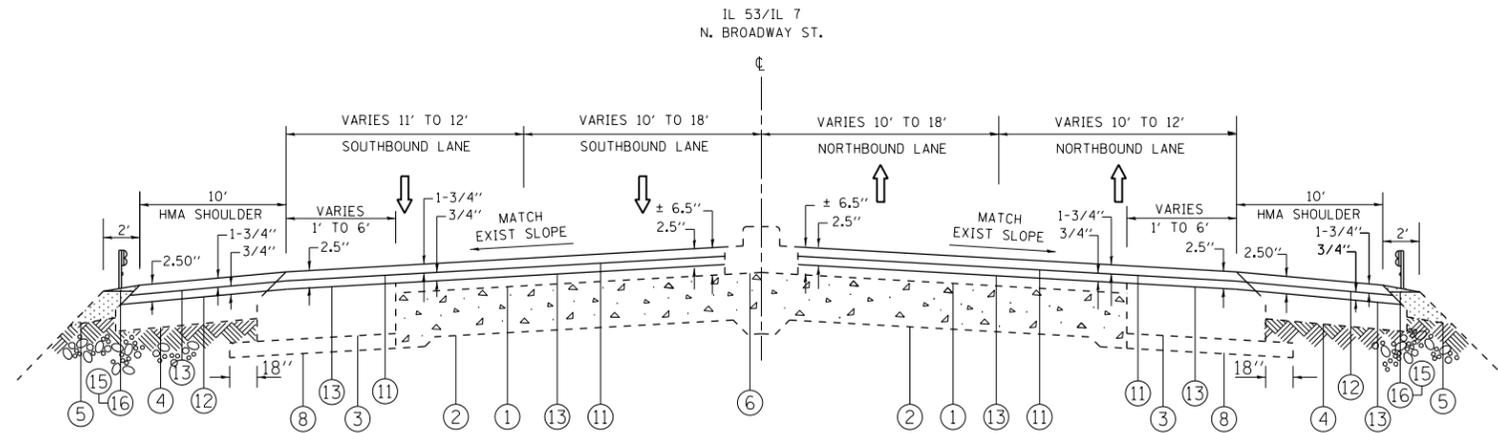
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					112	MY-RS-1	WILL	35	12
PLOT DATE = 5/23/2014	DATE -	REVISED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60N42				
ILLINOIS FED. AID PROJECT												



EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
STA. 105+40 TO STA. 112+85

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)



EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
STA. 105+40 TO STA. 112+85

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)

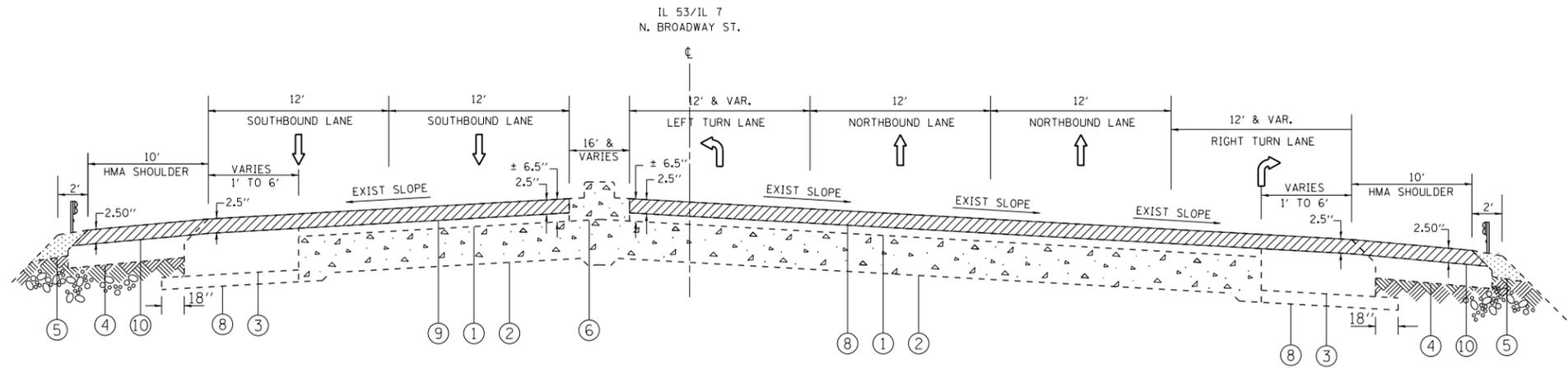
LEGEND

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- ② EXISTING P.C. CONCRETE PAVEMENT, (9"-7"-9")
- ③ EXISTING HMA BASE COURSE, ± 11"
- ④ EXISTING HMA SHOULDER, 8"
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- ⑨ PROPOSED HMA SURFACE REMOVAL, 2-1/2" (MAINLINE)
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- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1-3/4" (MAINLINE)
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					112	MY-RS-1	WILL	35	13
PLOT DATE = 5/23/2014	DATE -	REVISED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60N42				
					ILLINOIS FED. AID PROJECT							



NOTE FOR EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

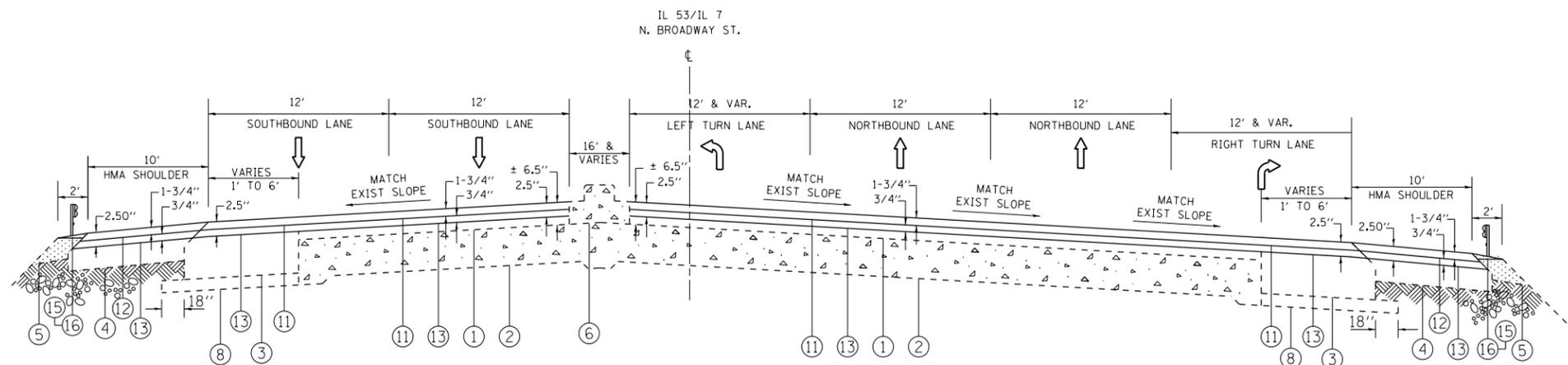
IL 53/IL 7 (N. BROADWAY ST.)
EXISTING TYPICAL SECTION
AT RENWICK RD. (FACING NORTH)

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)

LEGEND

- ① EXISTING HMA SURFACING, ± 6.5"
- ② EXISTING P.C. CONCRETE PAVEMENT, (9'-7"-9')
- ③ EXISTING HMA BASE COURSE, ± 11"
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NOTE:
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" THE CONTRACTOR SHALL MILL FIRST PRIOR TO PATCHING".

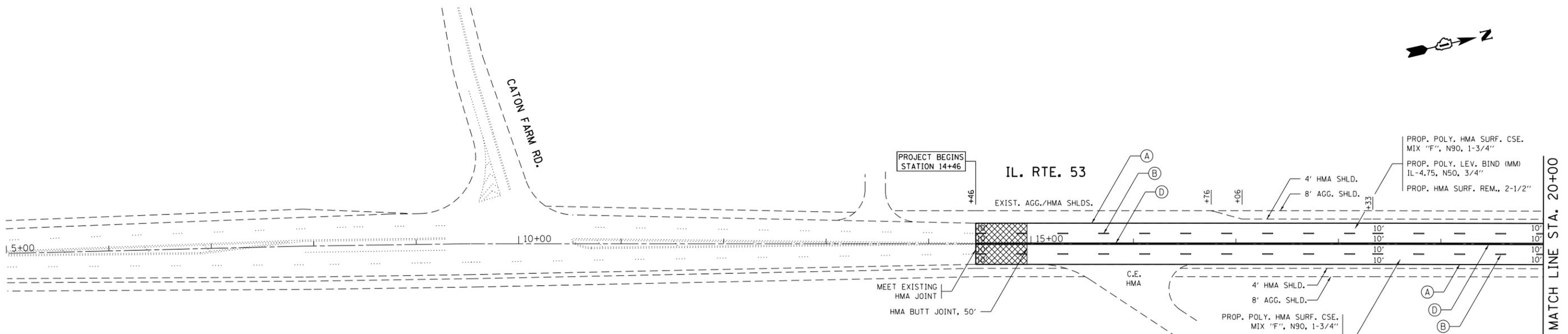


NOTE FOR EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 104+36 TO STA. RENWICK RD. (SB)

IL 53/IL 7 (N. BROADWAY ST.)
PROPOSED TYPICAL SECTION
AT RENWICK RD. (FACING NORTH)

EXISTING GUARDRAIL:
APPROXIMATE LOCATION IS FROM
STA. 105+40 TO RENWICK RD. (NB)

FILE NAME = c:\pwork\pwork\galbenj\d0249591\0131211-sh-t-plan.dgn	USER NAME = galbenj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 /IL 7 (N. BROADWAY STREET) EXISTING AND PROPOSED TYPICAL SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -				112	MY-RS-1	WILL	35	14
PLOT DATE = 5/23/2014	DATE -	REVISED -	REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60N42 ILLINOIS FED. AID PROJECT					



PAVEMENT MARKING LEGEND

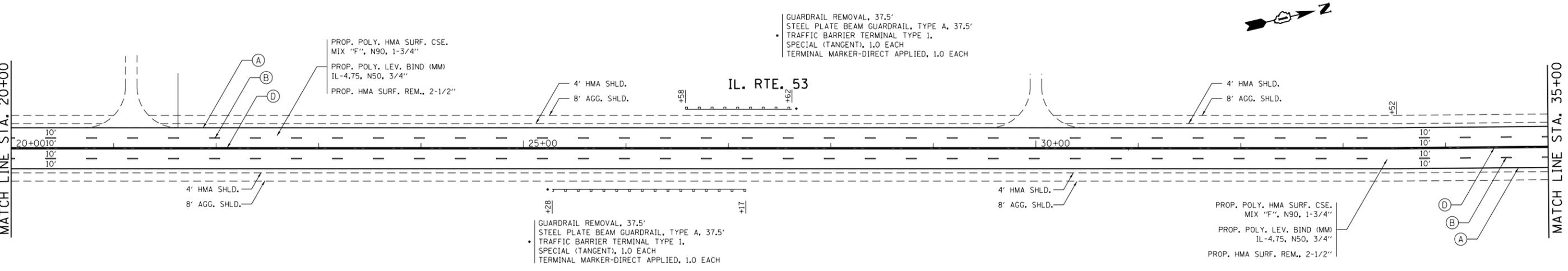
- (A) 4" WHITE - SOLID (EDGE LINE)
- (B) 4" WHITE - 10' DASH / 30' SKIP
- (C) 4" YELLOW - SOLID (EDGE LINE)
- (D) 4" YELLOW - 2 @ 11" C-C
- (E) 6" WHITE - SOLID (TURN LANE LINE)
- (F) 12" YELLOW - RT. 45° DIAGONAL @ 30' C-C (MEDIAN)
- (G) 12" WHITE - RT. 45° DIAGONAL @ 75' C-C (SHOULDER)
- (H) 24" WHITE - SOLID (STOP BAR)
- (I) 8' WHITE - LETTERS AND SYMBOLS

NOTE:

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE AND SHOULD BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

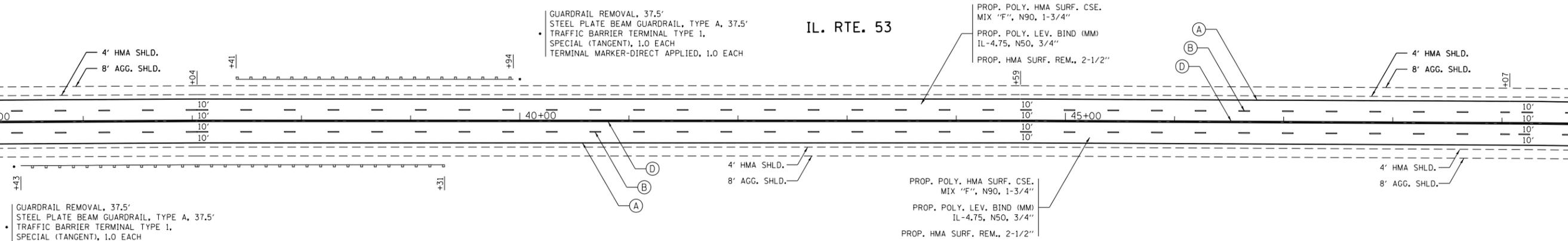
RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT LAWRENCE HILL, TRAFFIC FIELD ENGINEER AT (847) 485-6475 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT MARKINGS.



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. 53 / IL. 7 (N. BROADWAY STREET) EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLANS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 60N42				ILLINOIS FED. AID PROJECT				
PLOT DATE = 5/23/2014		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				

MATCH LINE STA. 35+00



GUARDRAIL REMOVAL, 37.5'
 STEEL PLATE BEAM GUARDRAIL, TYPE A, 37.5'
 TRAFFIC BARRIER TERMINAL TYPE 1,
 SPECIAL (TANGENT), 1.0 EACH
 TERMINAL MARKER-DIRECT APPLIED, 1.0 EACH

GUARDRAIL REMOVAL, 37.5'
 STEEL PLATE BEAM GUARDRAIL, TYPE A, 37.5'
 TRAFFIC BARRIER TERMINAL TYPE 1,
 SPECIAL (TANGENT), 1.0 EACH
 TERMINAL MARKER-DIRECT APPLIED, 1.0 EACH

PROP. POLY. HMA SURF. CSE.
 MIX "F", N90, 1-3/4"
 PROP. POLY. LEV. BIND (MM)
 IL-4.75, N50, 3/4"
 PROP. HMA SURF. REM., 2-1/2"

PAVEMENT MARKING LEGEND

- (A) 4" WHITE - SOLID (EDGE LINE)
- (B) 4" WHITE - 10' DASH / 30' SKIP
- (C) 4" YELLOW - SOLID (EDGE LINE)
- (D) 4" YELLOW - 2 @ 11" C-C
- (E) 6" WHITE - SOLID (TURN LANE LINE)
- (F) 12" YELLOW - RT. 45° DIAGONAL @ 30' C-C (MEDIAN)
- (G) 12" WHITE - RT. 45° DIAGONAL @ 75' C-C (SHOULDER)
- (H) 24" WHITE - SOLID (STOP BAR)
- (I) 8" WHITE - LETTERS AND SYMBOLS

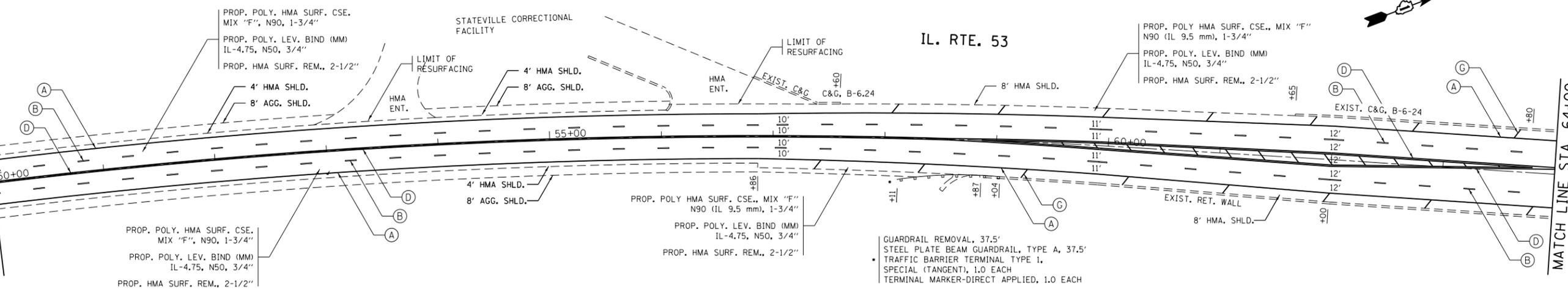
NOTE:

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE AND SHOULD BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT LAWRENCE HILL, TRAFFIC FIELD ENGINEER AT (847) 485-6475 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT MARKINGS.

MATCH LINE STA. 50+00



PROP. POLY. HMA SURF. CSE.
 MIX "F", N90, 1-3/4"
 PROP. POLY. LEV. BIND (MM)
 IL-4.75, N50, 3/4"
 PROP. HMA SURF. REM., 2-1/2"

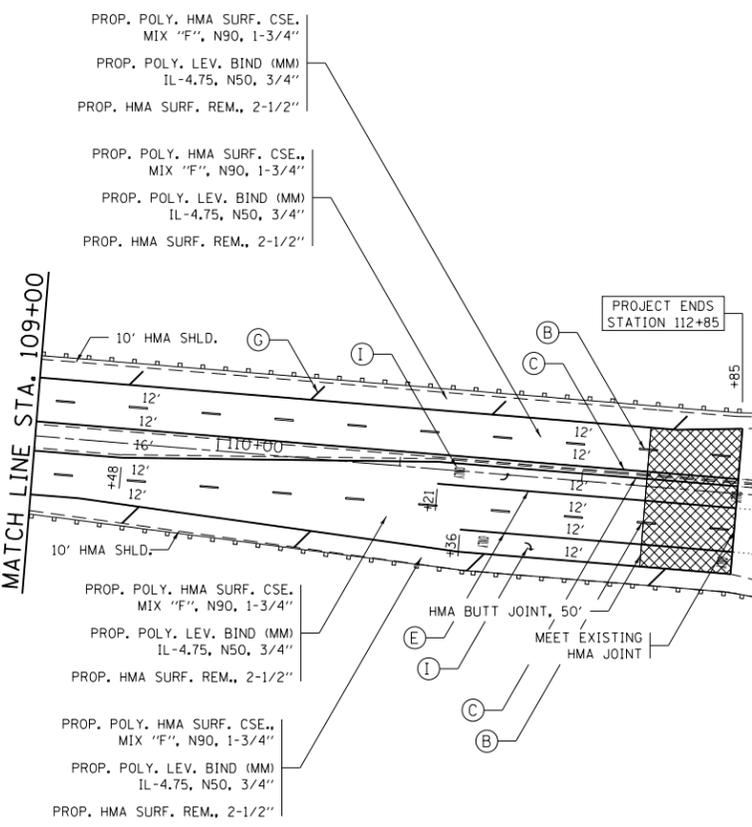
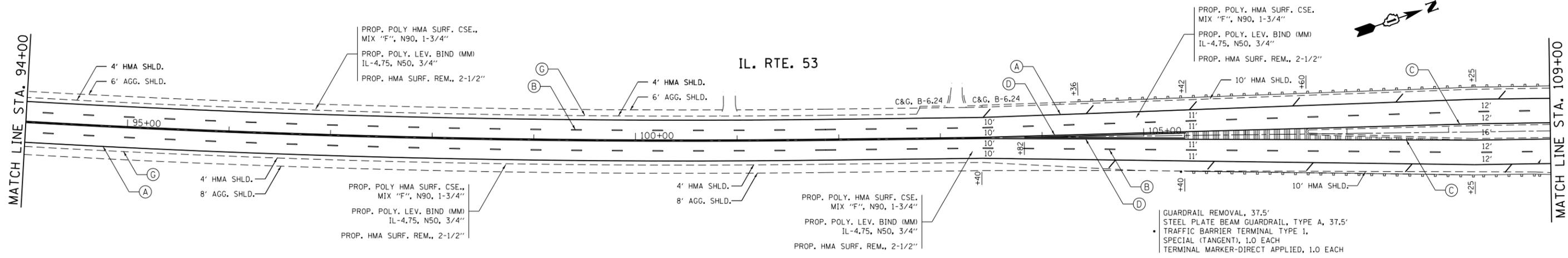
STATEVILLE CORRECTIONAL FACILITY
 LIMIT OF RESURFACING
 HMA ENT.

PROP. POLY. HMA SURF. CSE., MIX "F"
 N90 (IL 9.5 mm), 1-3/4"
 PROP. POLY. LEV. BIND (MM)
 IL-4.75, N50, 3/4"
 PROP. HMA SURF. REM., 2-1/2"

GUARDRAIL REMOVAL, 37.5'
 STEEL PLATE BEAM GUARDRAIL, TYPE A, 37.5'
 TRAFFIC BARRIER TERMINAL TYPE 1,
 SPECIAL (TANGENT), 1.0 EACH
 TERMINAL MARKER-DIRECT APPLIED, 1.0 EACH

PROP. POLY. HMA SURF. CSE., MIX "F"
 N90 (IL 9.5 mm), 1-3/4"
 PROP. POLY. LEV. BIND (MM)
 IL-4.75, N50, 3/4"
 PROP. HMA SURF. REM., 2-1/2"

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL. 53 / IL. 7 (N. BROADWAY STREET) EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLANS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\midot\galbenjr\d0249591\0131211-shit-plan.dgn		DRAWN -	REVISED -		112	MY-RS-1	WILL	35	16				
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 60N42				ILLINOIS FED. AID PROJECT				
PLOT DATE = 5/23/2014		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



PAVEMENT MARKING LEGEND

- (A) 4" WHITE - SOLID (EDGE LINE)
- (B) 4" WHITE - 10' DASH / 30' SKIP
- (C) 4" YELLOW - SOLID (EDGE LINE)
- (D) 4" YELLOW - 2 @ 11" C-C
- (E) 6" WHITE - SOLID (TURN LANE LINE)
- (F) 12" YELLOW - RT. 45° DIAGONAL @ 30' C-C (MEDIAN)
- (G) 12" WHITE - RT. 45° DIAGONAL @ 75' C-C (SHOULDER)
- (H) 24" WHITE - SOLID (STOP BAR)
- (I) 8" WHITE - LETTERS AND SYMBOLS

NOTE:

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE AND SHOULD BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT LAWRENCE HILL, TRAFFIC FIELD ENGINEER AT (847) 485-6475 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT MARKINGS.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
es:\pwwork\pwwid\galbenjr\d0249591\0131211-shit-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/23/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. 53 / IL. 7 (N. BROADWAY STREET)
EXISTING AND PROPOSED ROADWAY AND PAVEMENT MARKING PLANS

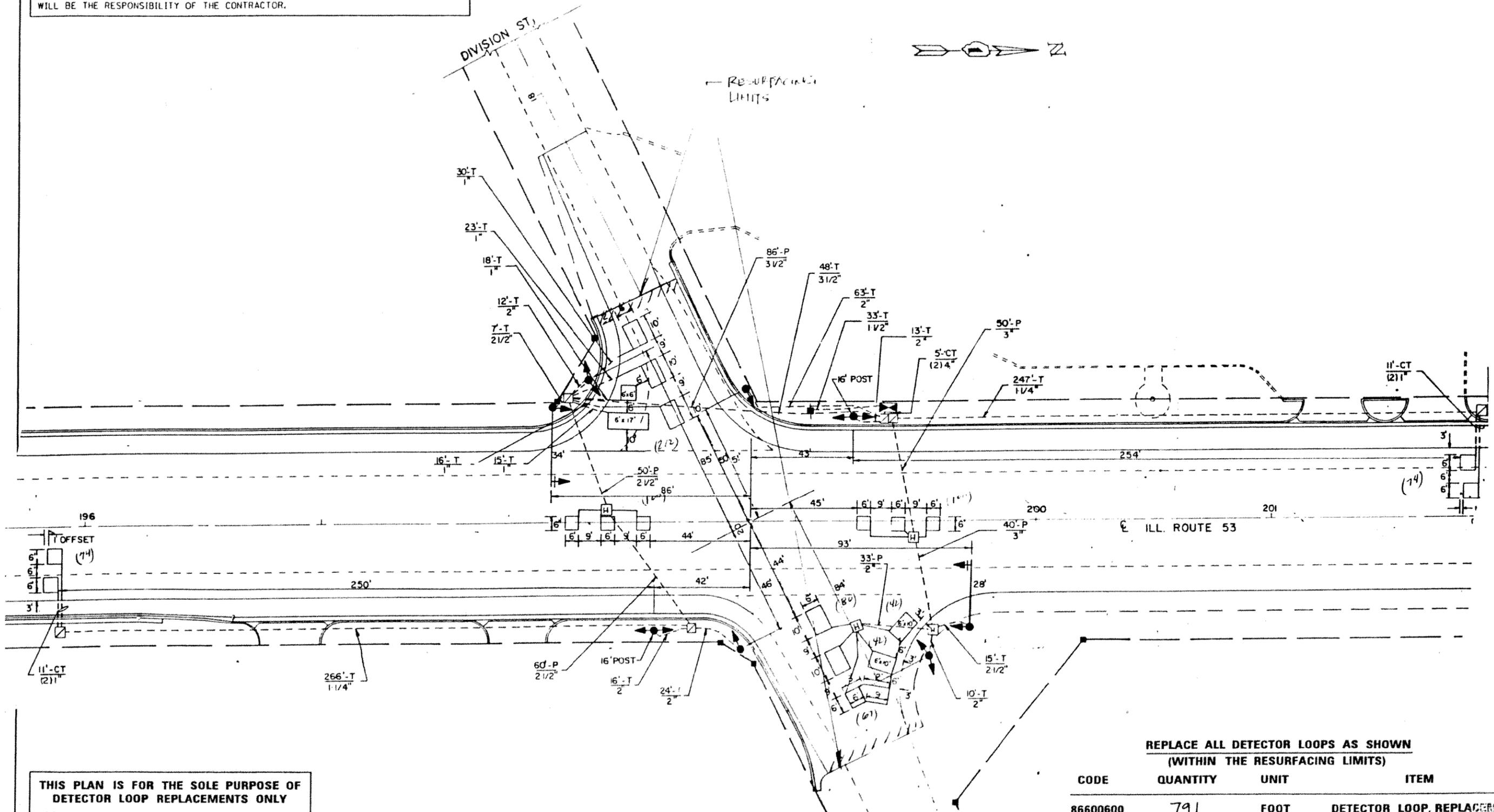
SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	18
CONTRACT NO. 60N42				
ILLINOIS FED. AID PROJECT				

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

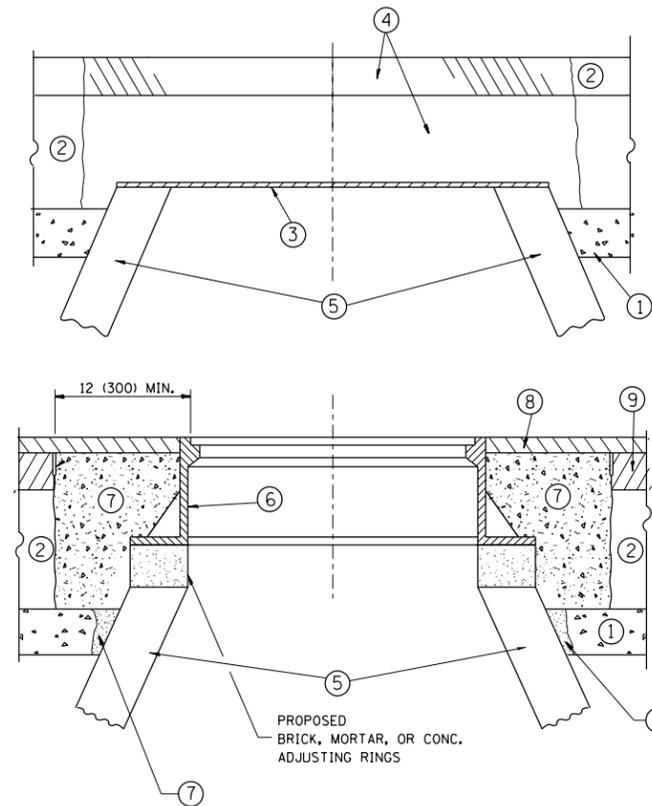
	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE	◀	▶
SIGNAL HEAD	◀	▶
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED	---	---
DETECTOR LOOP	□	□
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		⊥
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		⊥



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	791	FOOT	DETECTOR LOOP REPLACEMENT



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

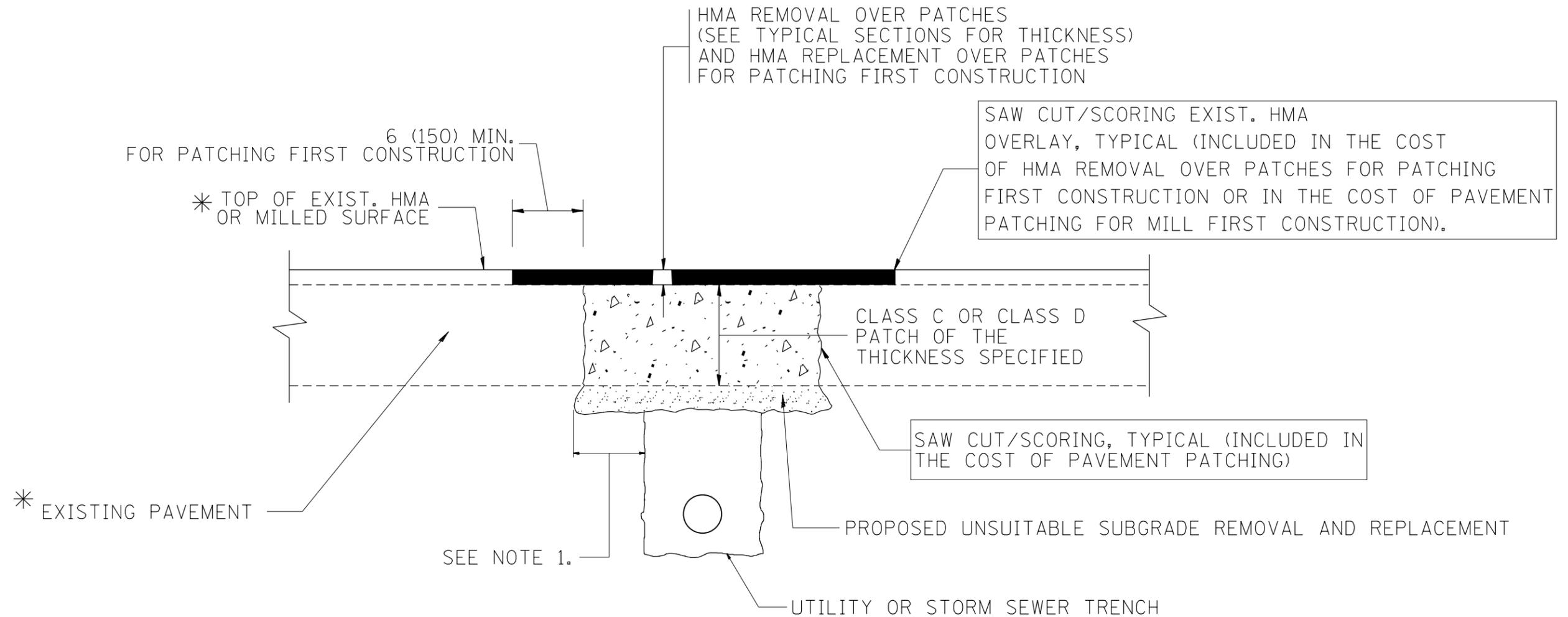
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = galbenjr	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97
et:\pwork\pwork\galbenjr\d0249591\Dist\std.dgn		DRAWN -	REVISED - R. WIEDEMAN 05-14-04
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 5/23/2014	DATE - 10-25-94	REVISED - R. BORO 03-09-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	21
BD600-03 (BD-8)			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
et:\pwork\pwork\galbenjr\d0249591\Dist\d0249591.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 5/23/2014	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	22
BD400-04 (BD-22)			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

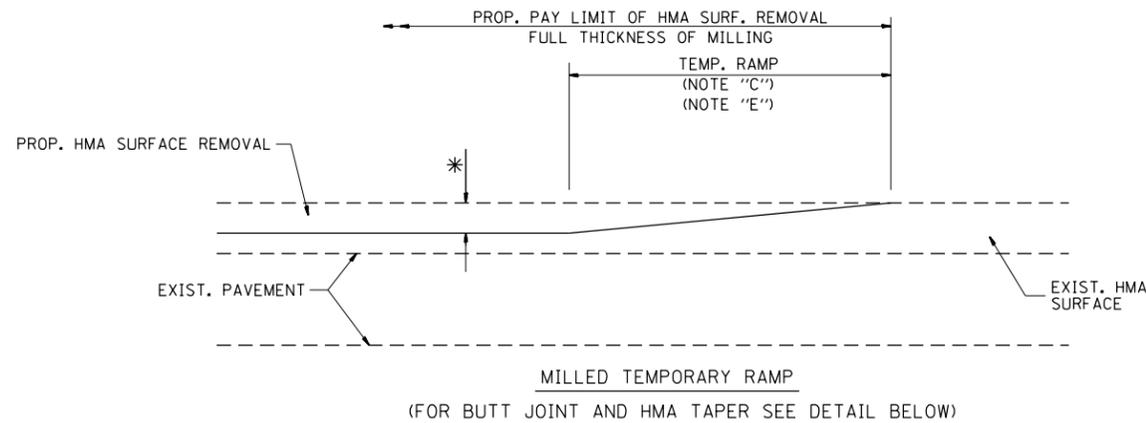
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

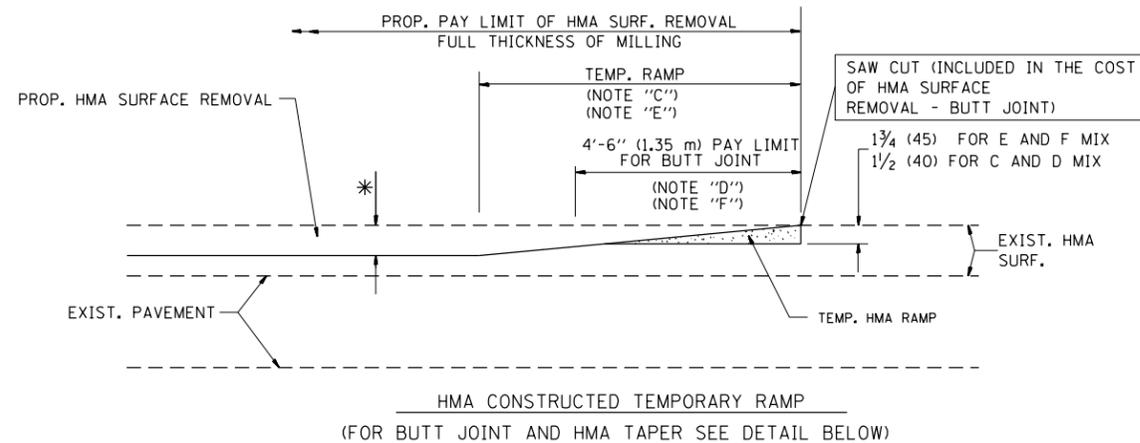
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\galbenjr\d0249591\Dist\std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01					112	MY-RS-1	WILL	35	23
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 12-15-09						BD600-06 (BD-24)		CONTRACT NO. 60N42		
PLOT DATE = 5/23/2014	DATE - 03-11-94							SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

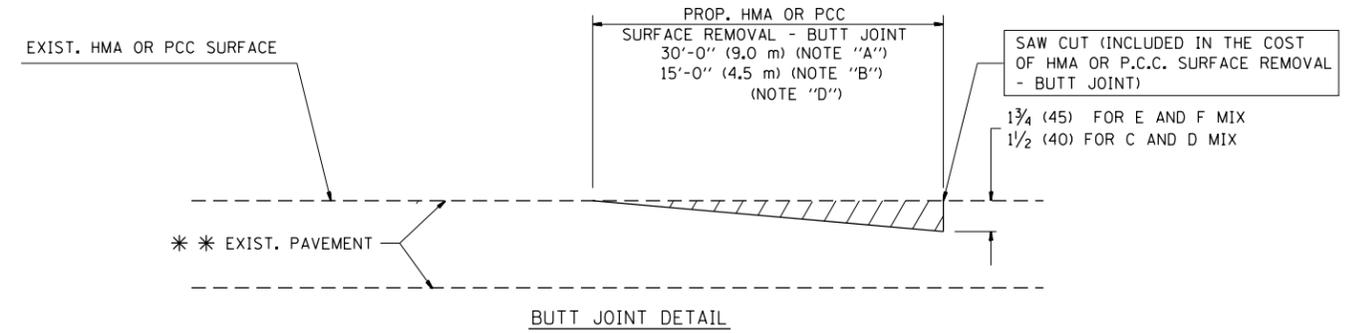


OPTION 1

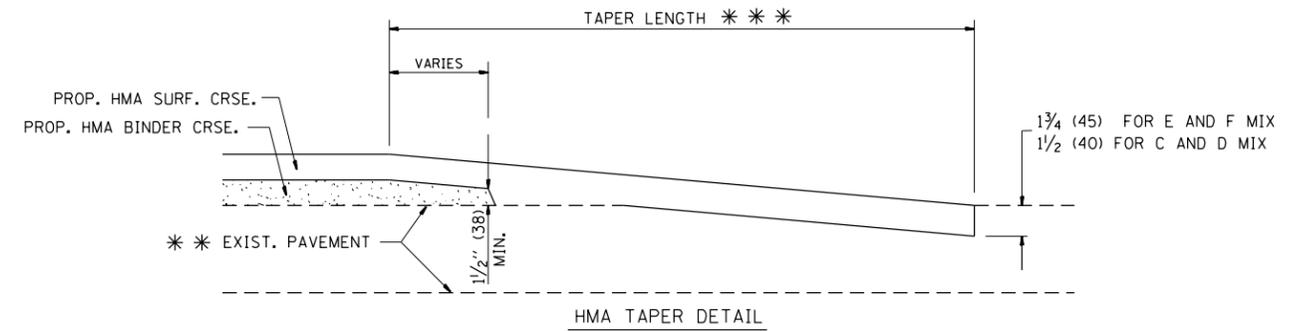


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

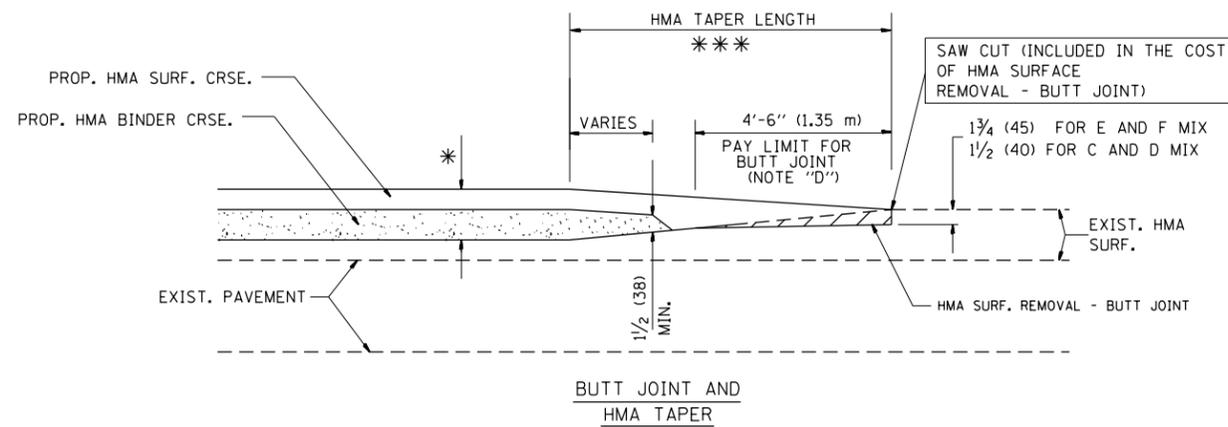
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



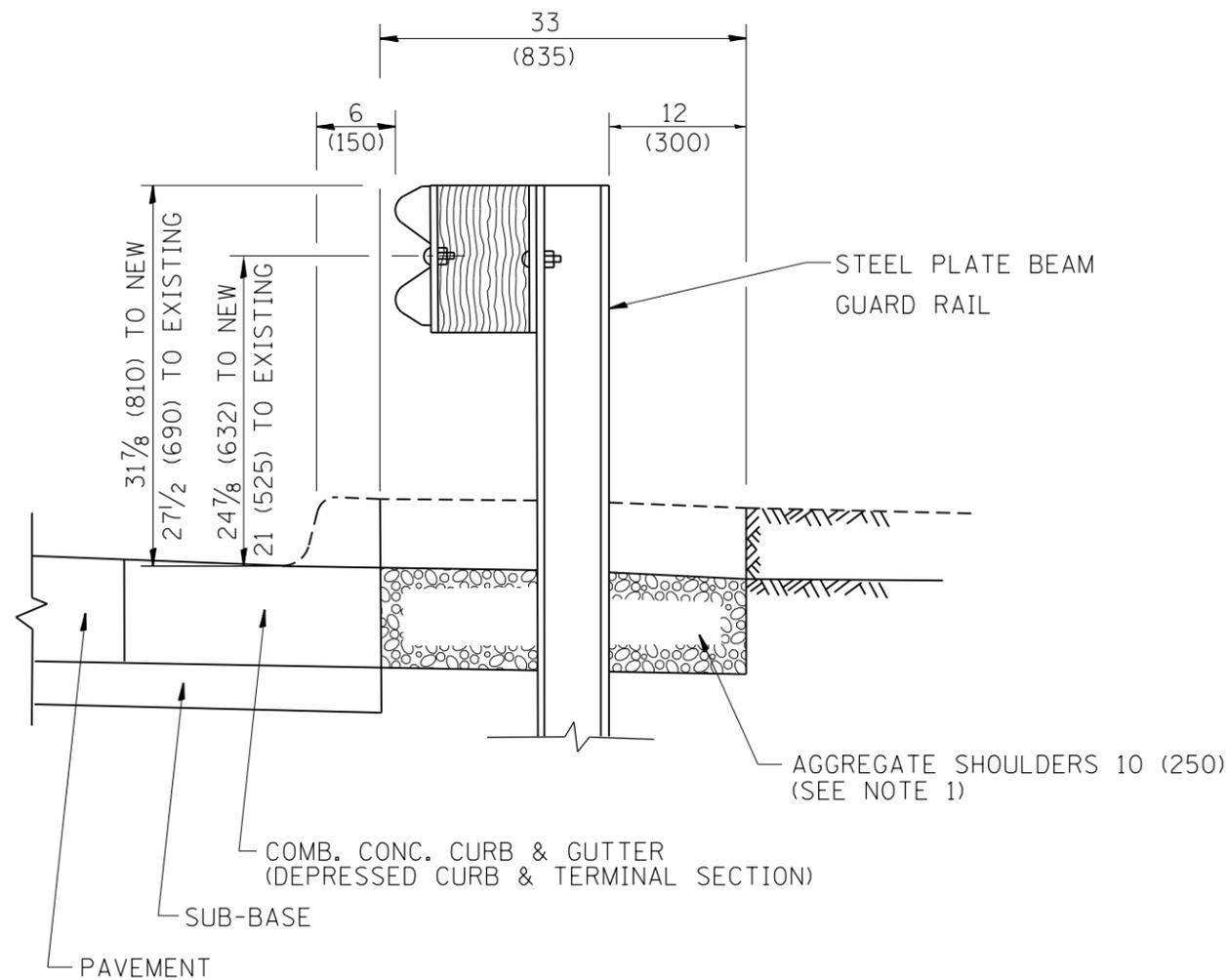
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = galbenjr	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
et:\pw\work\p\dot\galbenjr\d0249591\Dist\std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 5/23/2014	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

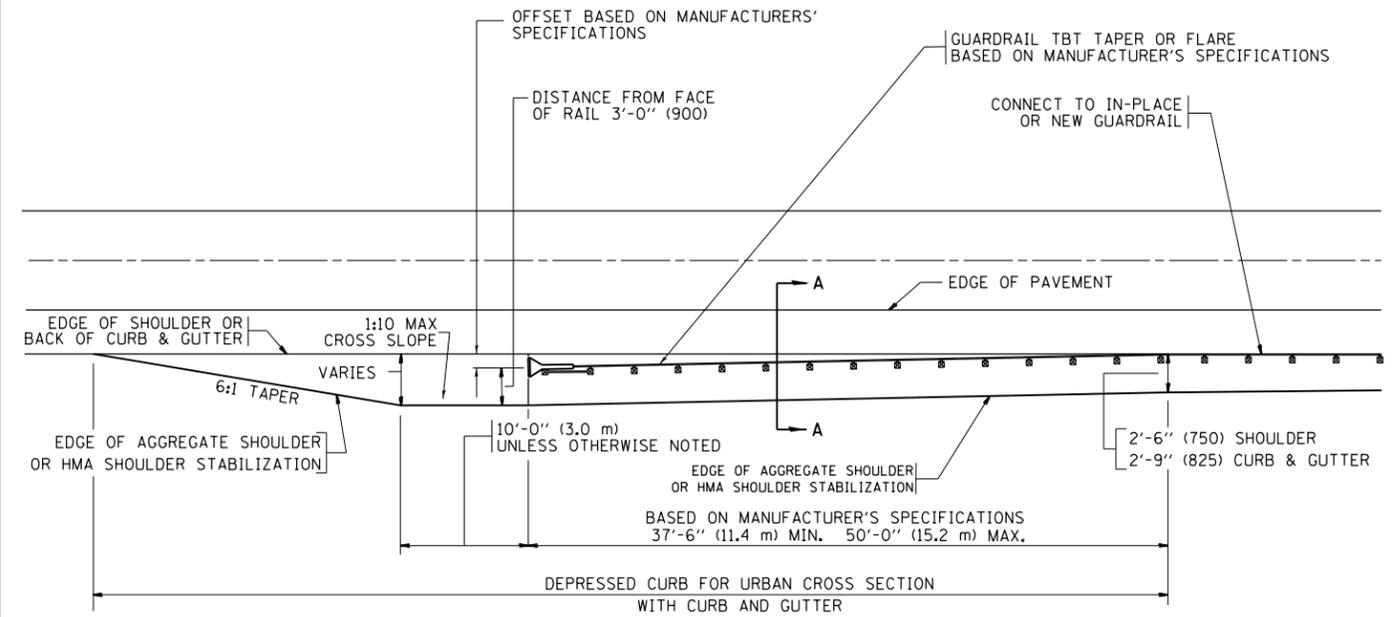
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	24
BD400-05 BD32		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

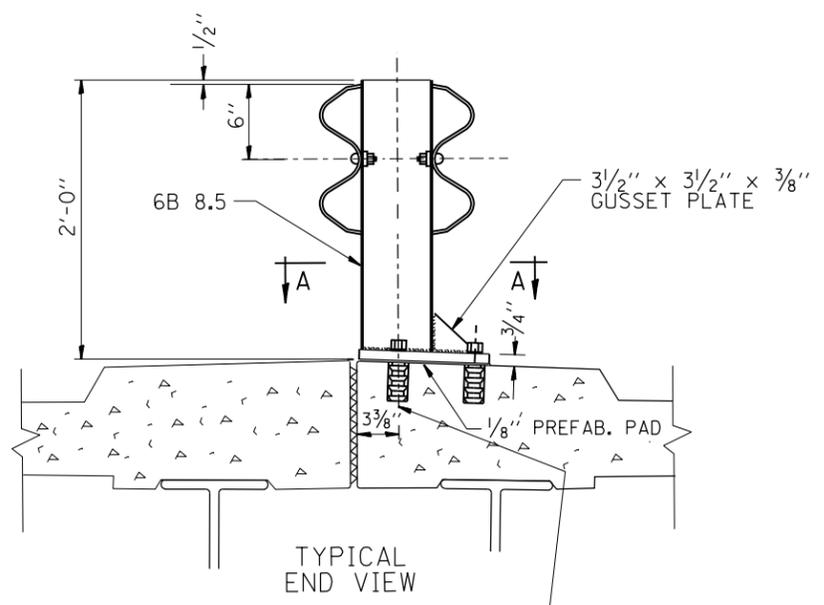
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 5/23/2014	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

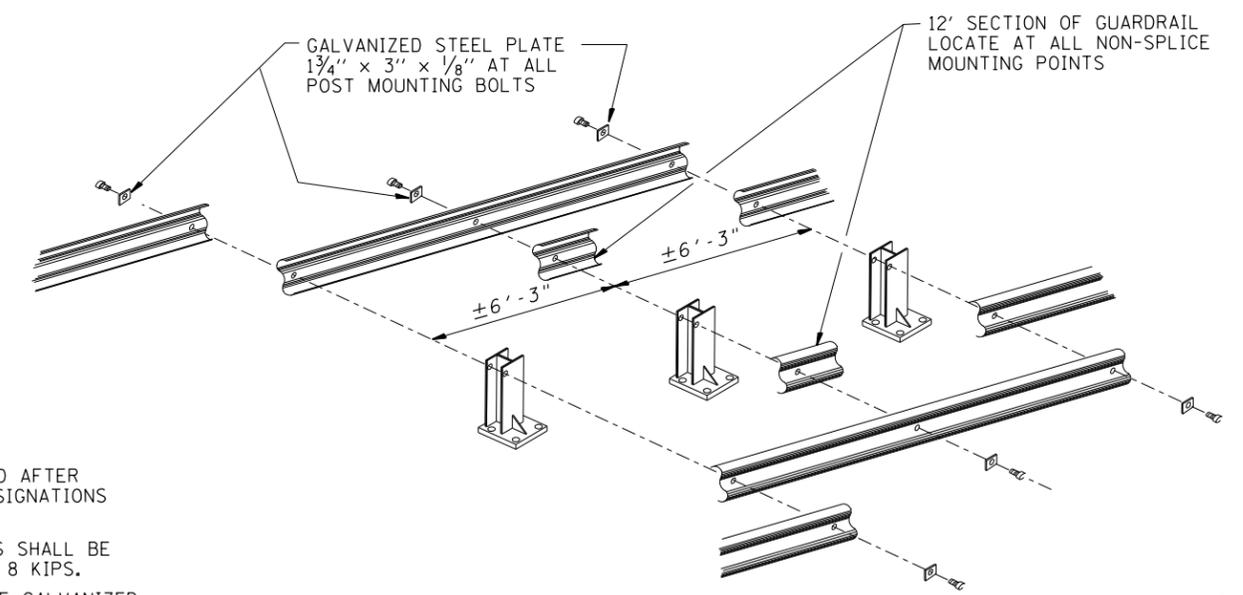
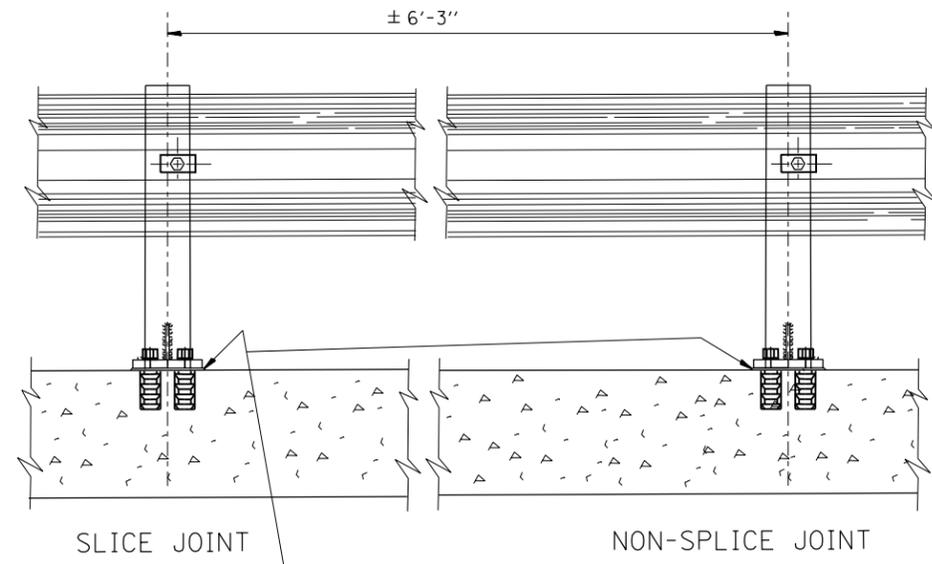
**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	25
BD600-10 (BD 34)		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

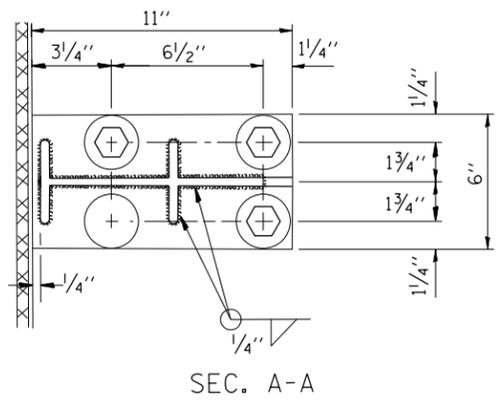


7/8" GALVANIZED ANCHOR BOLTS AND SELF-DRILLING EXPANSION SHELLS OR MULTI UNIT TYPE ANCHORS
1/8" HOLES THRU BASE PLATE



NOTE:

DIAMOND TIPPED DRILL SHALL BE USED TO DRILL THRU REINFORCEMENT BARS

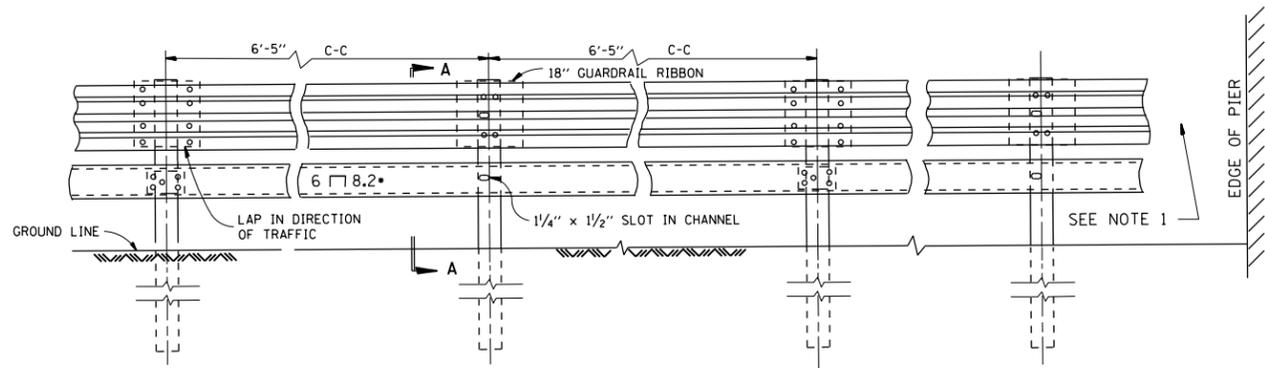


NOTE:

POSTS SHALL BE HOT DIPPED GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM DESIGNATIONS A-123 AND A-385.
TWO PERCENT (2%) OF ALL ANCHOR BOLTS SHALL BE TEST LOADED IN PLACE. MIN. TEST LOAD = 8 KIPS.
ALL BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION A-153.
FOR MATERIAL COMPOSITION OF PREFABRICATED PAD, SEE ARTICLE 54.9(F), (BEARING AND ANCHORAGE), OF THE STANDARD SPECIFICATIONS.
PROVIDE 1-1/8" AND 2-1/16" GALVANIZED STEEL SHIMS FOR 25% OF THE POSTS.
IF ONE ANCHOR BOLT FAILS DURING TEST, TWO OTHER ANCHOR BOLTS SHALL BE TESTED. TESTING EQUIPMENT CAN BE OBTAINED FROM BOLT COMPANIES.

THIS SHEET IS FOR INFORMATION ONLY

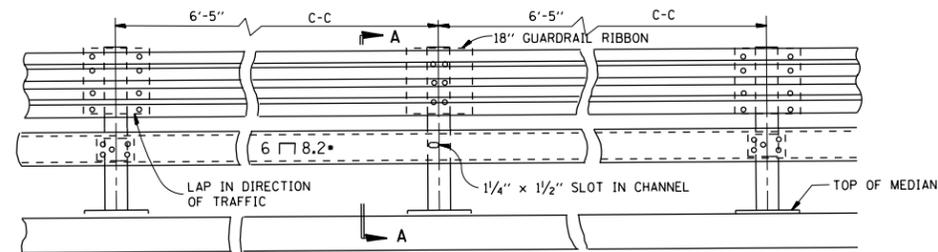
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et:\pwork\pwork\galbenjr\d0249591\Dist\std.dgn		DRAWN - RFL	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	112	MY-RS-1	WILL	35	26
		CHECKED -	REVISED -						BM-05		CONTRACT NO. 60N42		
		DATE - 10-31-88	REVISED -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



ELEVATION

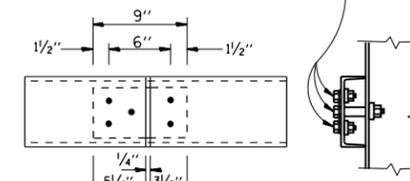
DETAIL OF END SECTION

NOTE 1:
FOR DETAILS OF GUARDRAIL END ANCHORAGE
AT PIERS, SEE SHEET NO. _____

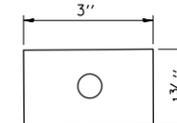


ELEVATION

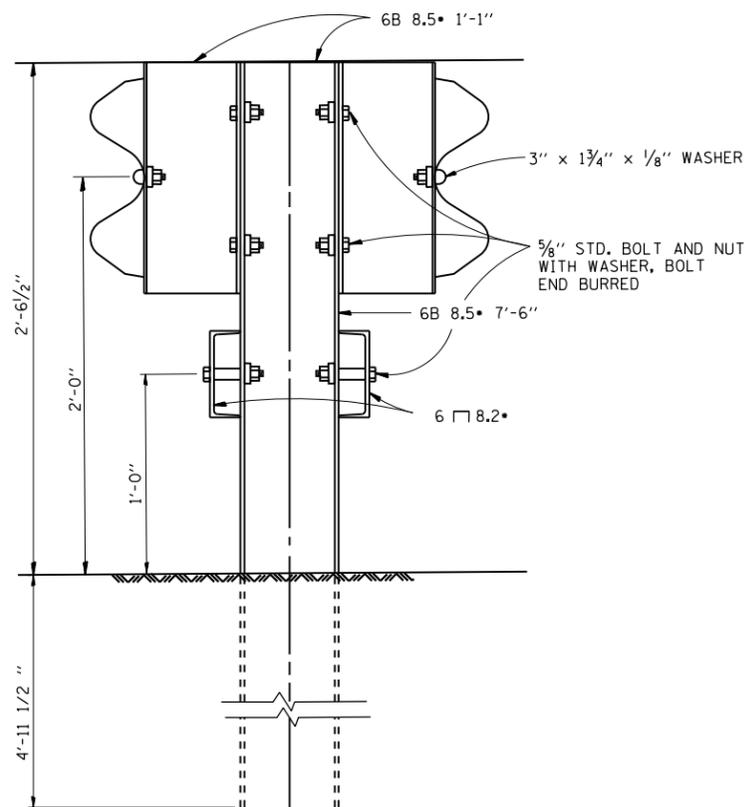
5/8" STD. BOLT AND NUT
WITH WASHER, BOLT
END BURRED



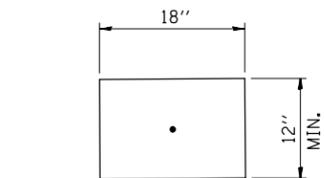
CHANNEL SPLICE
PLATE 8 3/4" x 4 1/2" x 1/4"



WASHER
PLATE 3" x 1 3/4" x 1/8"

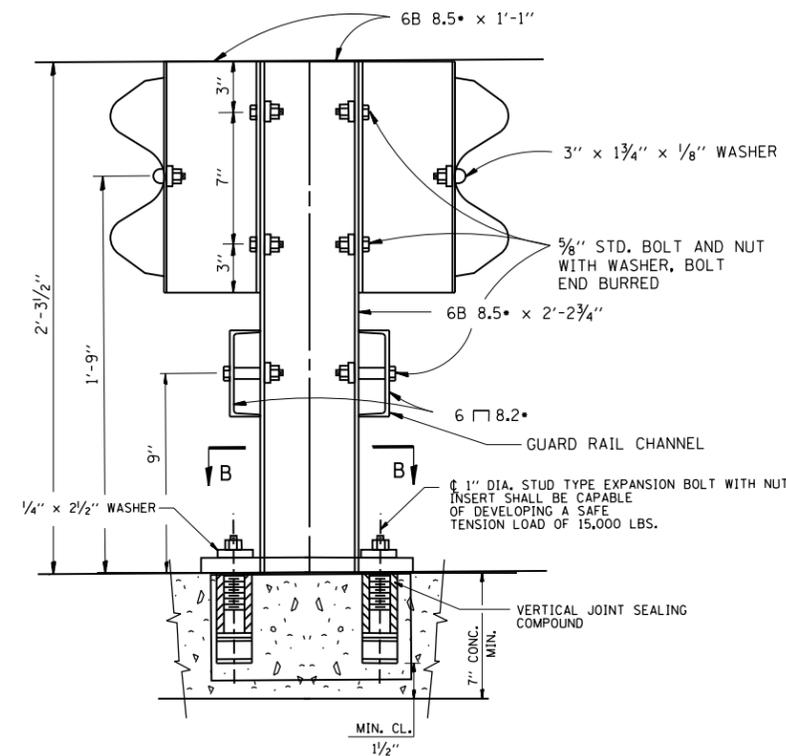


SECTION A-A



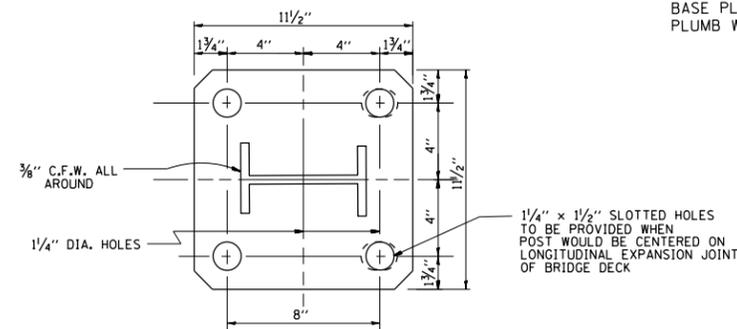
18" GUARDRAIL ELEMENT SPLICE

NOTE:
FOR DETAIL OF RAIL ELEMENT, RAIL SPLICING, BOLT,
AND NUT, SEE STATE STANDARD 2230.
A 1 3/8" x 3" x 1/8" STEEL PLATE WASHER AS SHOWN ON THIS
DRAWING SHALL BE PLACED BETWEEN THE BOLT HEAD
AND THE GUARDRAIL RIBBON AT ALL MOUNTING POINTS.
AN 18" PIECE OF GUARDRAIL RIBBON SHALL BE INSERTED
BETWEEN THE CONTINUOUS RIBBON AND THE POST AT
NON-SLICE MOUNTING POINTS.
RAIL, CHANNELS, POSTS, BASE PLATES, FASTENINGS,
AND ALL OTHER EXPOSED ACCESSORIES SHALL BE
GALVANIZED.



SECTION A-A
ANCHORAGE DETAIL

NOTE:
ALL WELDS SHALL BE GROUND SMOOTH.
POSTS SHALL BE CUT AND WELDED TO THE
BASE PLATE SO THAT THE POSTS WILL BE
PLUMB WHEN ERECTED.



RAIL POST BASE PLATE
R 1 1/2" x 1 1/4" x 1 1/2"

SECTION B-B

THIS SHEET IS FOR INFORMATION ONLY

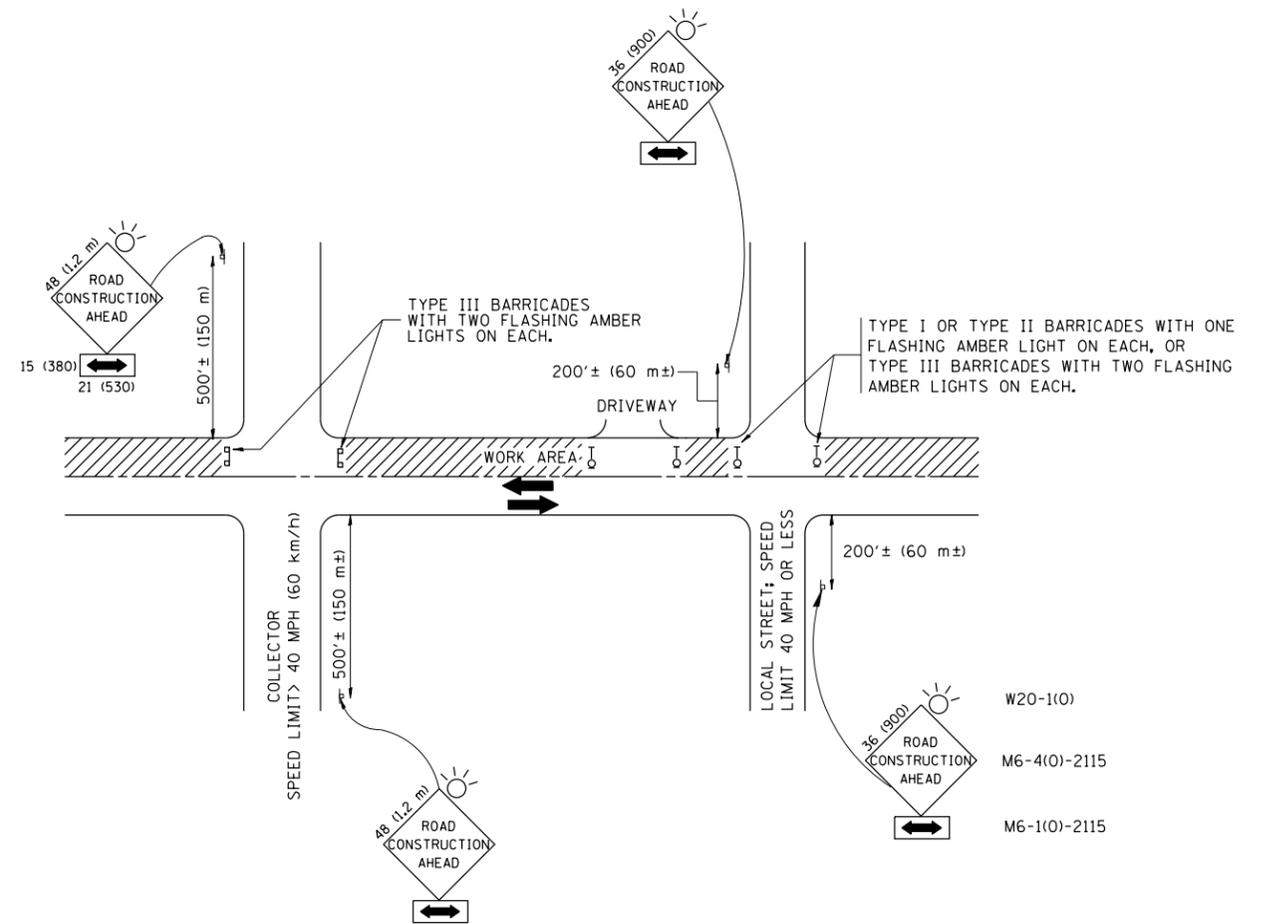
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PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 5/23/2014		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STEEL PLATE BEAM GUARDRAIL
SPECIAL

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	27
BM-06		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

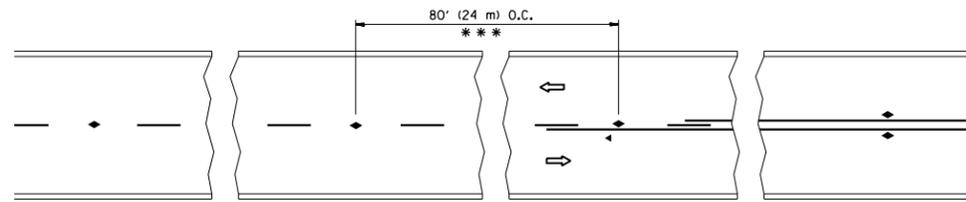
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	PLOT DATE = 5/23/2014	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

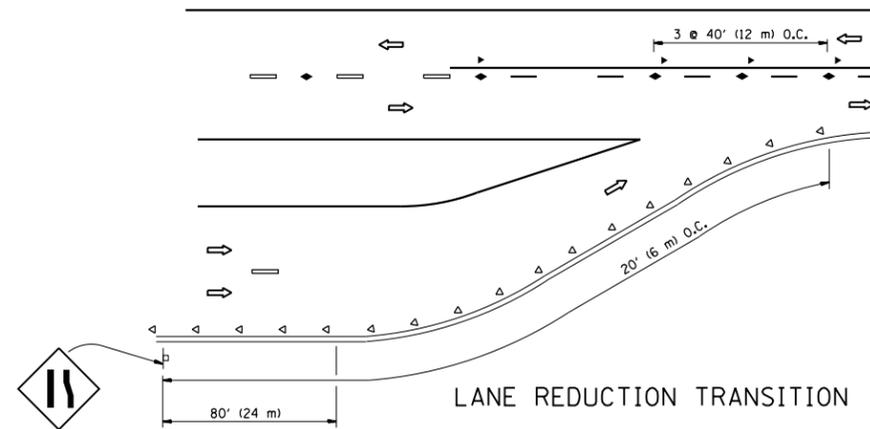
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

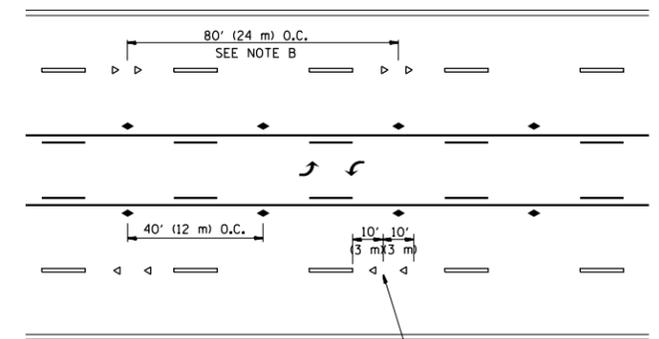


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

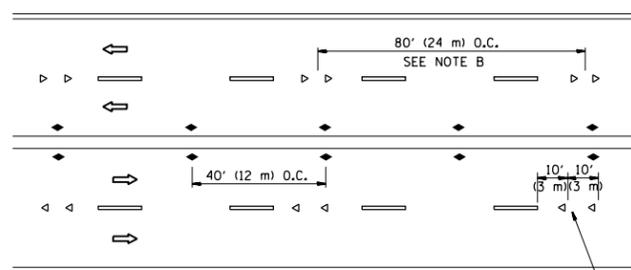
TWO-LANE/TWO-WAY



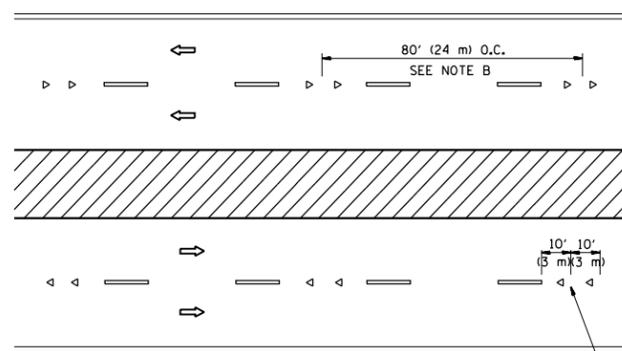
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

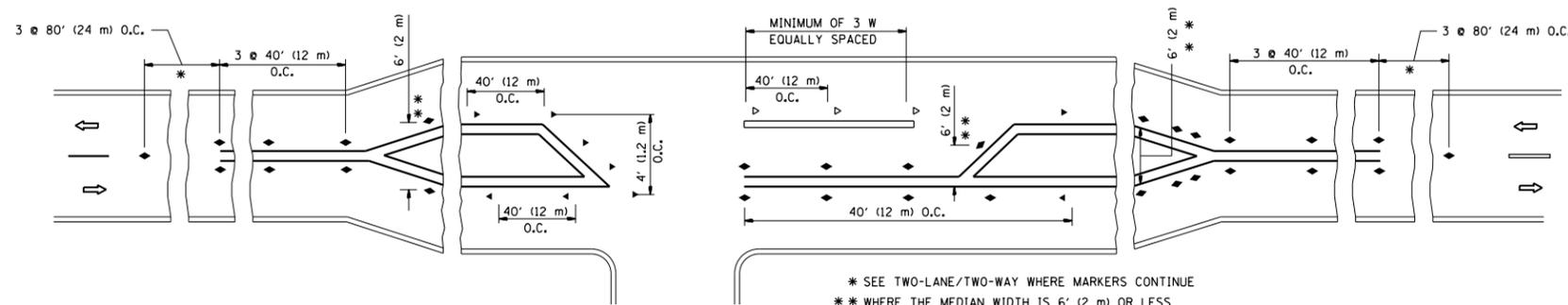
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

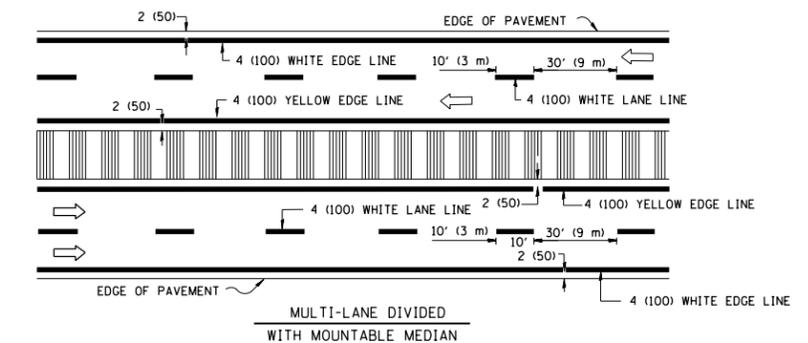
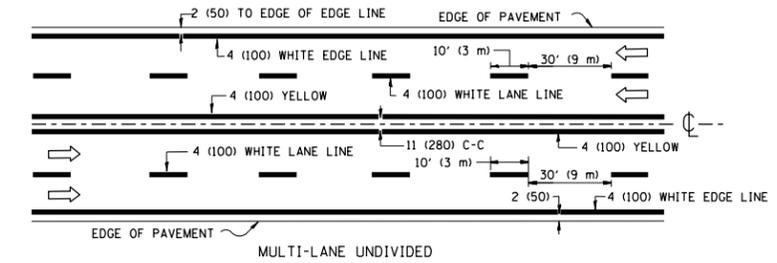
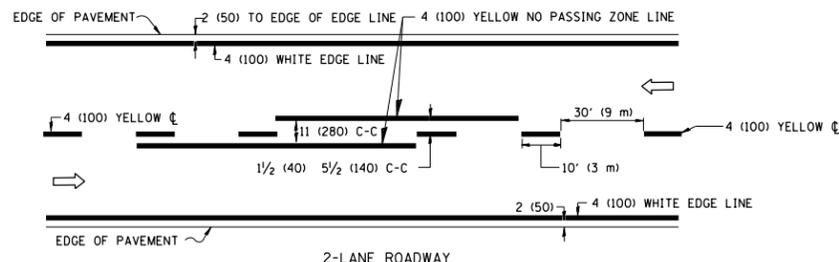
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 5/23/2014	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

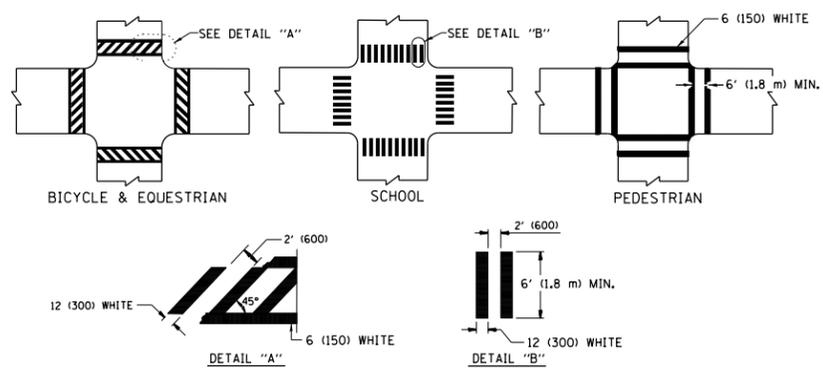
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	29
TC-11			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

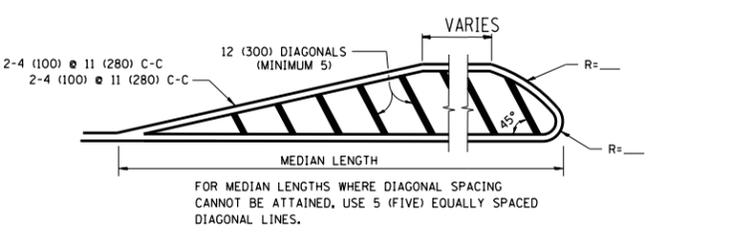
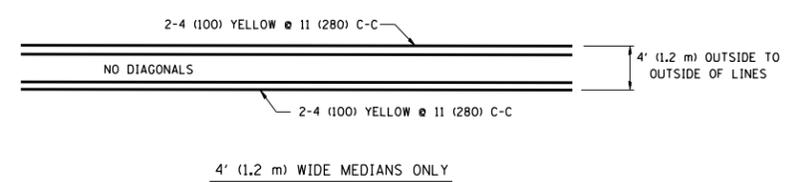


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

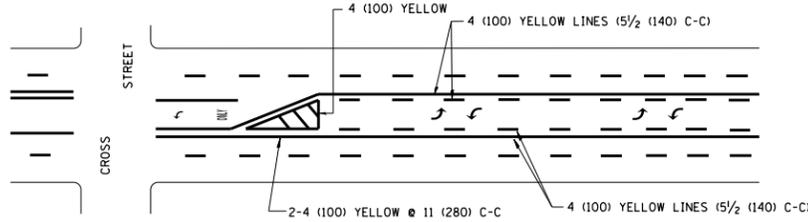


TYPICAL CROSSWALK MARKING

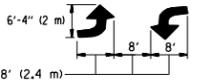


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

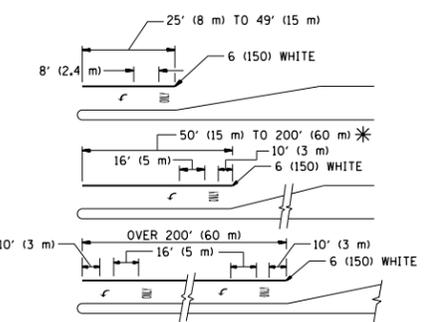


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

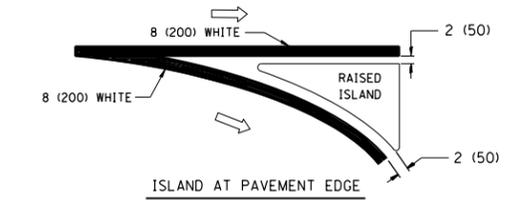
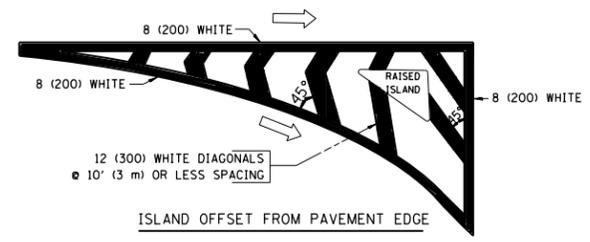


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) MIN. LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) EACH
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

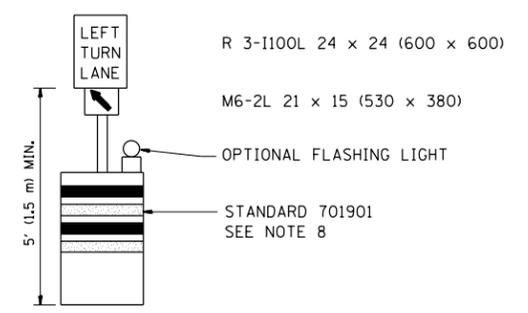
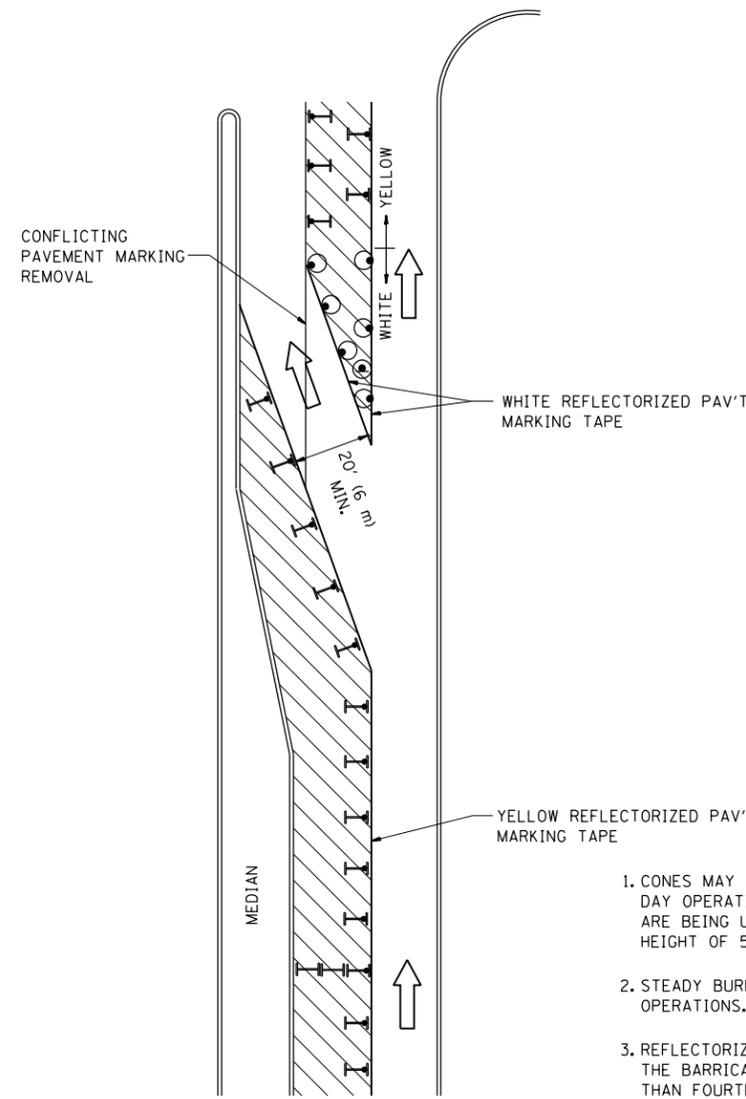
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 5/23/2014	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	30
TC-13		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

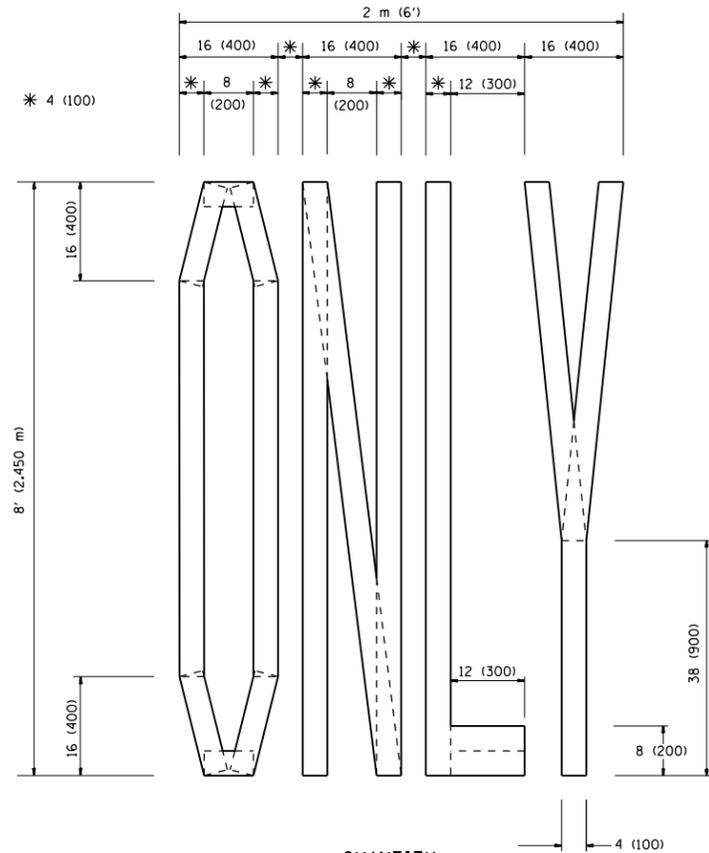
-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = galbenjr	REVISED -T, RAMMACHER 09-08-94	REVISED - R, BORO 09-14-09
et:\pwork\pwork\galbenjr\d0249591\Dist\std.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 5/23/2014	REVISED -T, RAMMACHER 01-06-00	REVISED -

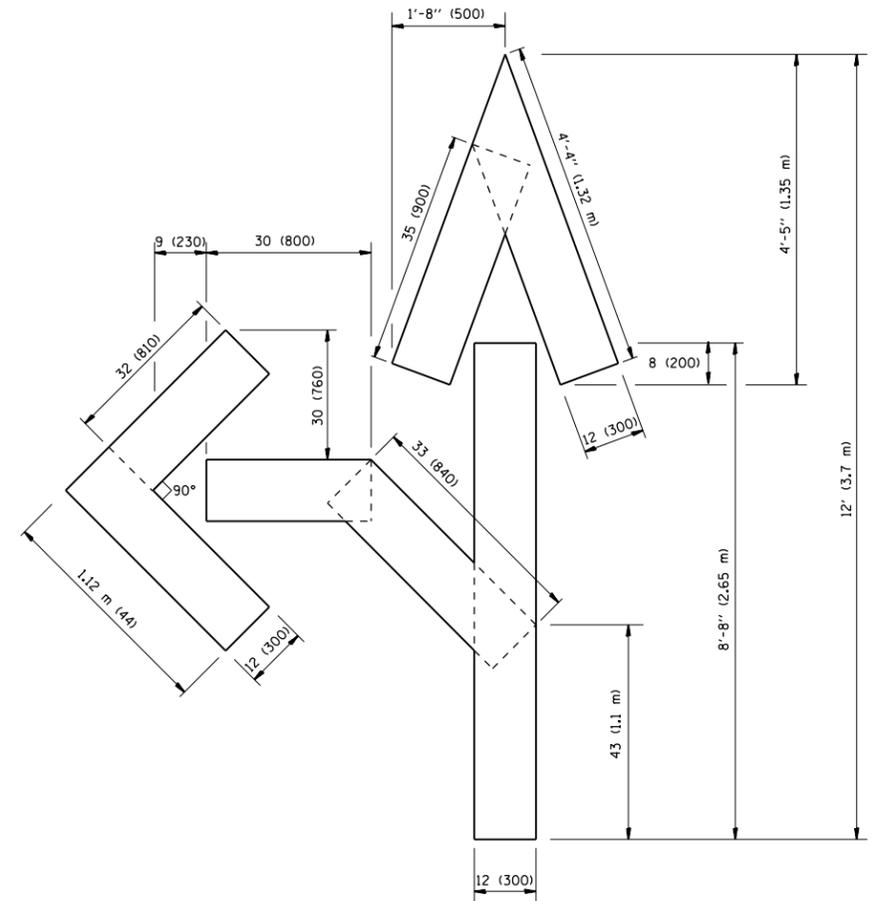
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

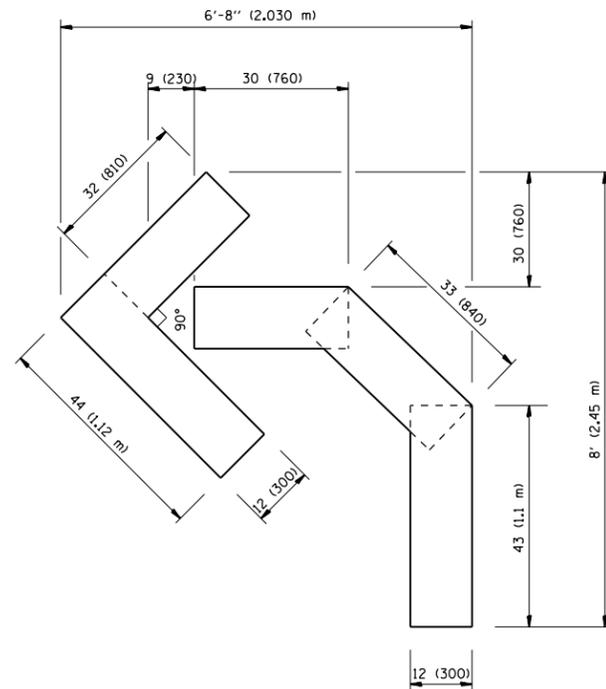
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	31
TC-14			CONTRACT NO. 60N42	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

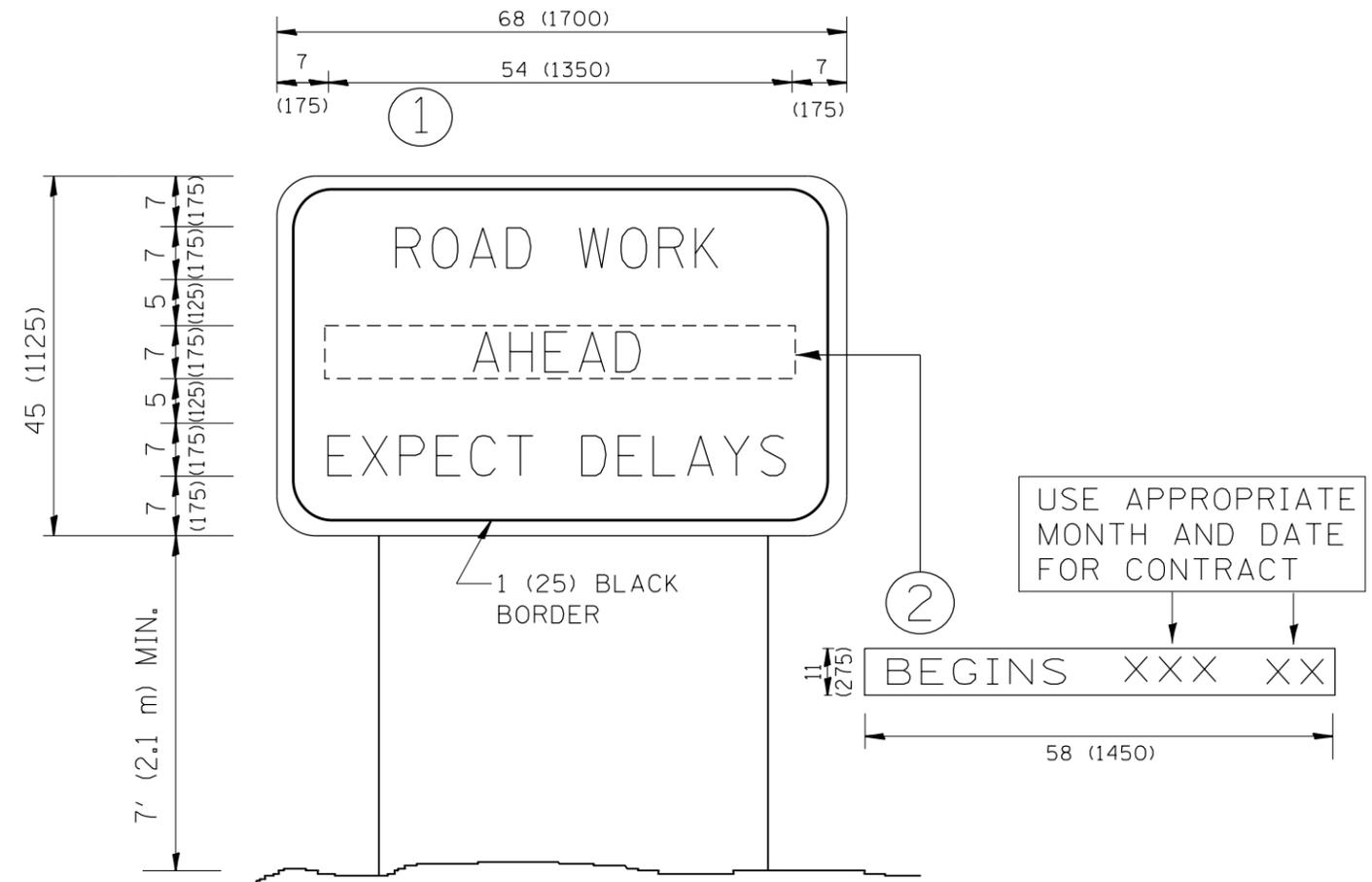
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et:\pwork\pwork\galbenjr\d0249591\Dist\d.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 5/23/2014	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	32
TC-16		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 5/23/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

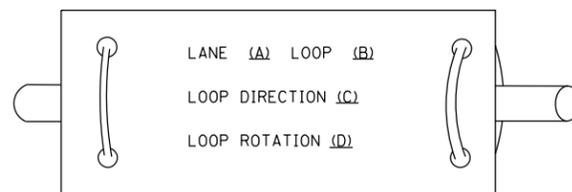
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	33
TC-22			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

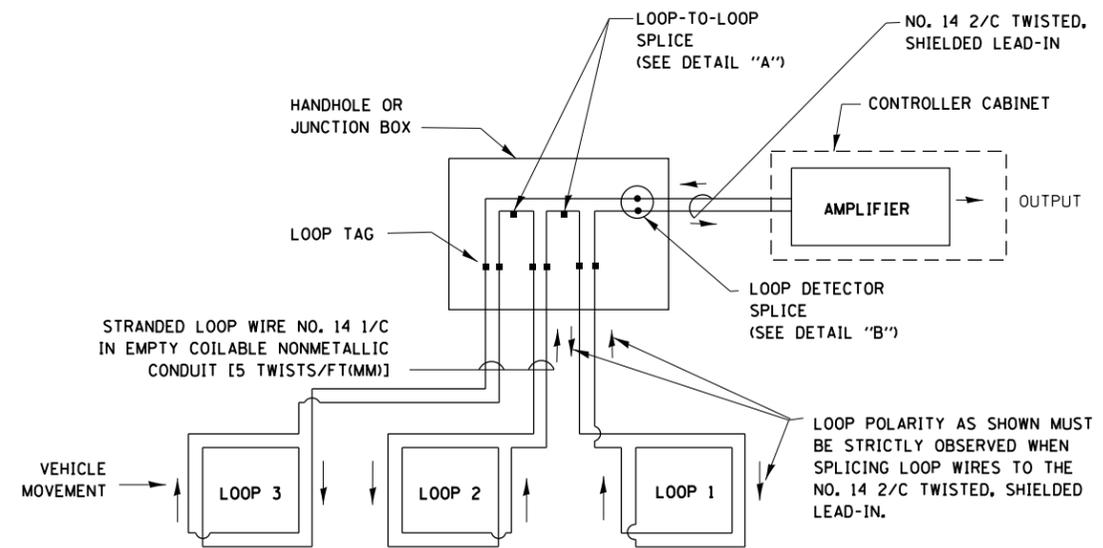
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

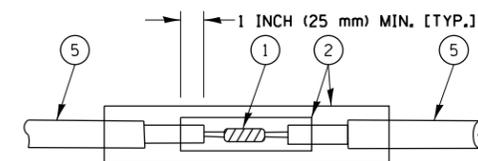


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

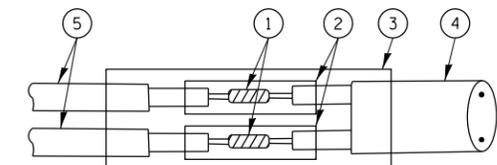


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

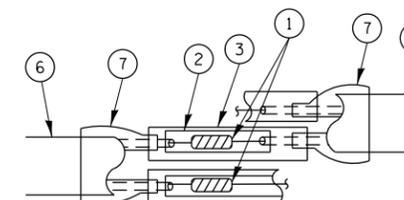


DETAIL "A"
LOOP-TO-LOOP SPLICE

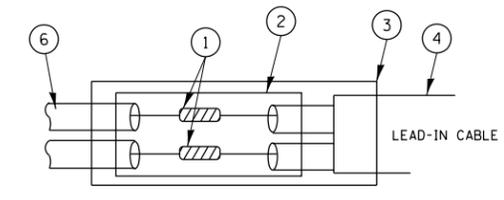


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



PRE-FORMED LOOP
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

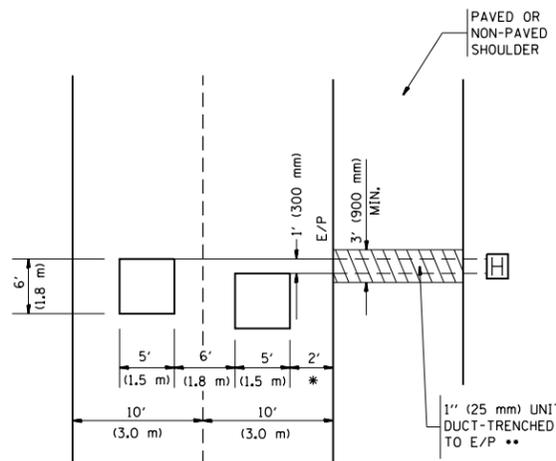
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	34
TS-05			CONTRACT NO. 60N42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



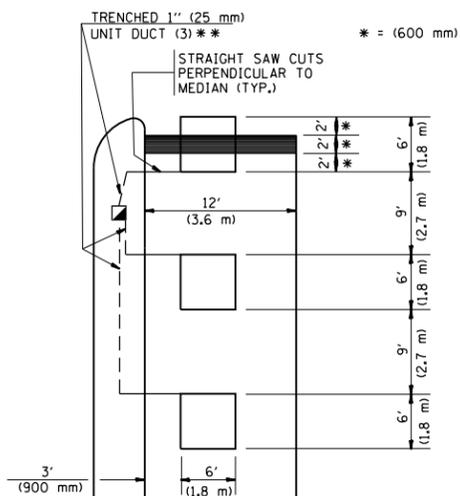
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

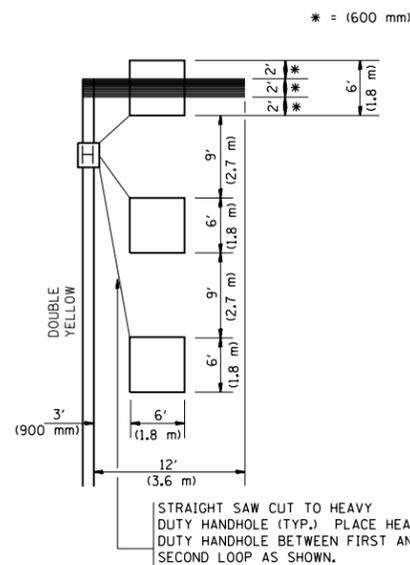


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

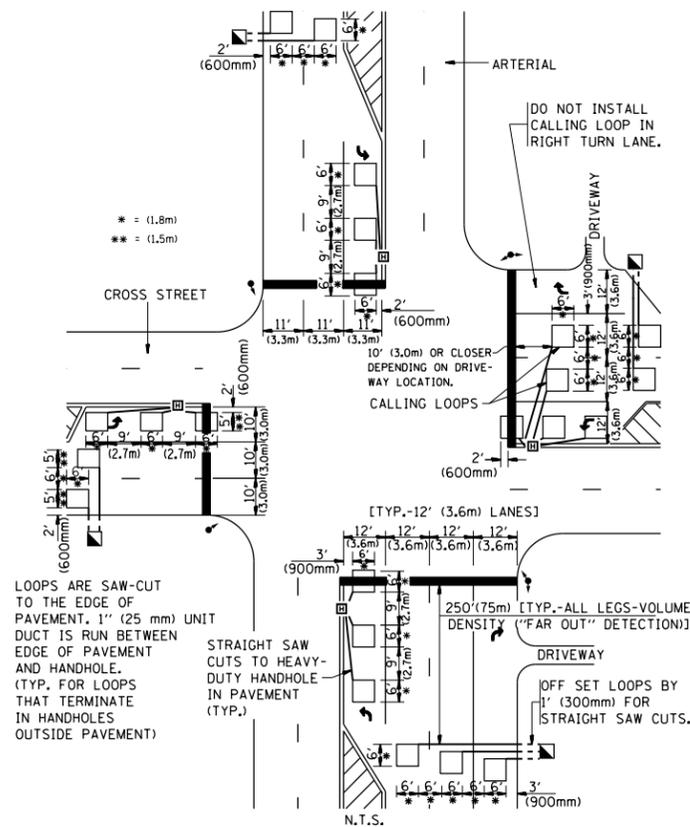
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



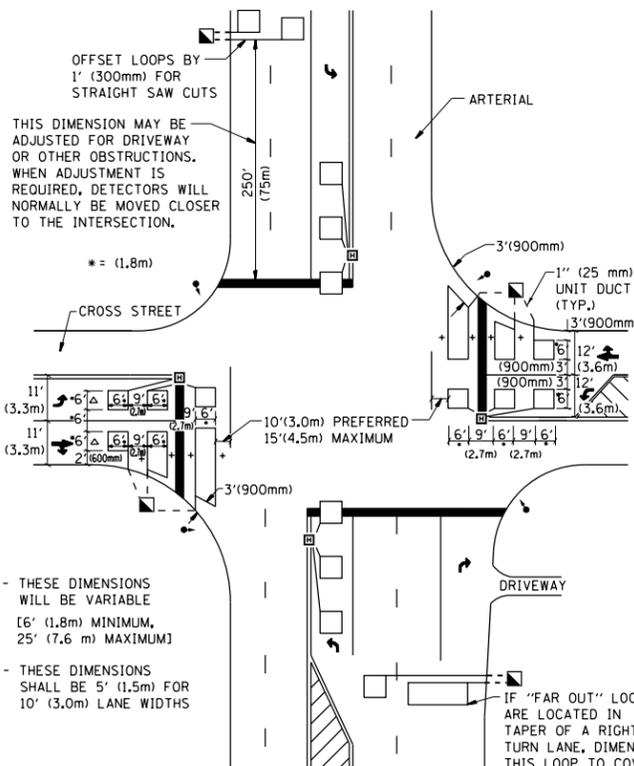
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	MY-RS-1	WILL	35	35
TS-07		CONTRACT NO. 60N42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				