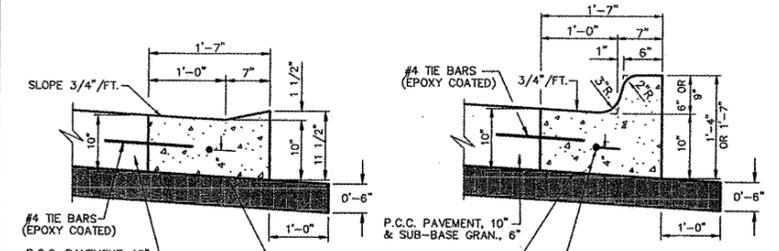


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET
F.A.U. 6755	•	TAZEWELL	39	22
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT	M-5083-112	

04-00114-00-PV  
CONTRACT NO. 89351



**DEPRESSED CURB**

MONOLITHIC CURB WILL NOT BE ALLOWED WITHOUT VILLAGE APPROVAL.

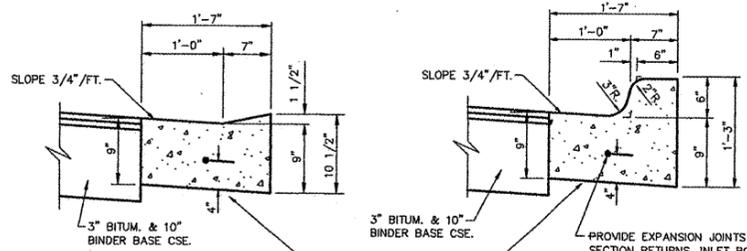
SUBGRADE COMPACTED TO 95% OF MAXIMUM DENSITY, STD. PROCTOR. IF FILL IS REQUIRED, USE COMPACTED C.A. 6 GRAVEL.

PROVIDE EXPANSION JOINTS AT INTER-SECTION RETURNS, INLET BOXOUTS, WORK STOPPAGE POINTS AND AT A MAXIMUM OF 200' O.C. USE 1 1/4" DIA. SMOOTH DOWEL BAR, 18" LONG WITH A METALLIC EXPANSION SLEEVE ON ONE END. GREASE ENTIRE BAR. PROVIDE SAWED CONTRACTION JOINTS AT 20' O.C. FILL WITH JOINT SEALER. EXPANSION JOINTS IN CURB AND GUTTER TO BE SEALED WITH AN APPROVED MASTIC.

TIE BARS BETWEEN CURB & GUTTER AND PAVEMENT SHALL BE IN ACCORDANCE WITH STANDARD 806001.

**GENERAL NOTES :**

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE OF ILLINOIS, LATEST EDITION.
2. FOR CURB & GUTTER ADJACENT TO CONCRETE PAVEMENT, PROVIDE NO. 4 TIE BARS AT 30" O.C., 30" LONG.
3. SEE SPECIAL PROVISION FOR PORTLAND CEMENT CONCRETE.
4. TOP OF CURB TO BE BRANDED WITH EITHER "S", "W" OR "D" AT LOCATIONS OF SEWER, WATER & SUMP DRAIN CROSSINGS. (INCIDENTAL).
5. ANY CONCRETE THAT SPILLS OUT FROM UNDER MULE OR FORM, ON STREET SIDE, SHALL BE REMOVED, PRIOR TO PLACING NEW BASE MATERIAL OR NEW PAVEMENT.
6. CURING COMPOUND SHALL BE APPLIED TO FRONT, BACK, TOP, FACE, AND PAN OF COMBINATION CONCRETE CURB AND GUTTER, AS SOON AS CONCRETE SURFACE HAS A "DRY TO THE TOUCH" CONDITION.
7. ALL RE-BARS, TIE BARS, ETC., SHALL BE EPOXY COATED.
8. EXPANSION JOINT MATERIAL SHALL BE CELLU-CUSHION EXP 200, AS MANUFACTURED BY SEALED AIR CORPORATION, OR APPROVED EQUAL.



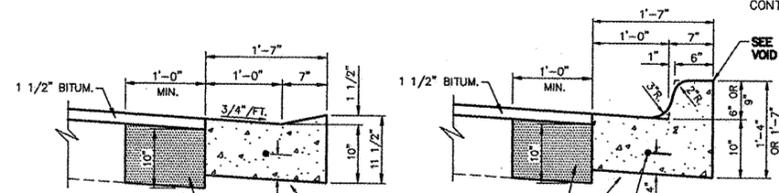
**DEPRESSED CURB**

**GENERAL NOTES :**

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE OF ILLINOIS, LATEST EDITION.
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8. EXPANSION JOINT MATERIAL SHALL BE CELLU-CUSHION EXP 200, AS MANUFACTURED BY SEALED AIR CORPORATION, OR APPROVED EQUAL.

**COMBINATION CONCRETE CURB AND GUTTER**  
**VILLAGE STANDARD TY. B-6.12**

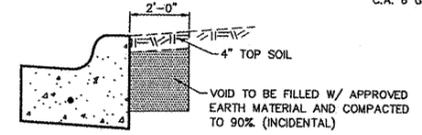
(RT. STA. 19+40.74 TO RT. STA. 22+20.00)  
(LT. STA. 19+40.74 TO LT. STA. 25+63.00)  
(RT. STA. 22+98.00 TO RT. STA. 25+63.00)



**DEPRESSED CURB**

SUBGRADE COMPACTED TO 95% OF MAXIMUM DENSITY, STD. PROCTOR. IF FILL IS REQUIRED, USE COMPACTED C.A. 6 GRAVEL.

PROVIDE EXPANSION JOINTS AT INTER-SECTION RETURNS, INLET BOXOUTS, WORK STOPPAGE POINTS AND AT A MAXIMUM OF 200' O.C. USE 1 1/4" DIA. SMOOTH DOWEL BAR, 18" LONG WITH A METALLIC EXPANSION SLEEVE ON ONE END. GREASE ENTIRE BAR. PROVIDE SAWED CONTRACTION JOINTS AT 20' O.C. FILL WITH JOINT SEALER. EXPANSION JOINTS IN CURB AND GUTTER TO BE SEALED WITH AN APPROVED MASTIC.



**DETAIL "A"**

**GENERAL NOTES :**

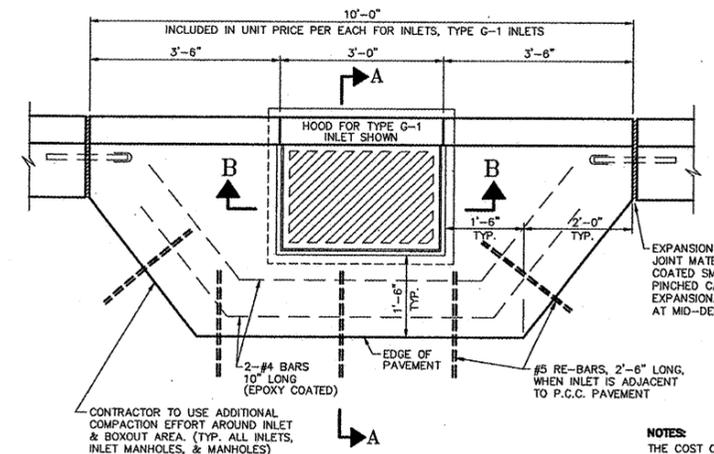
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7. ALL RE-BARS, TIE BARS, ETC., SHALL BE EPOXY COATED.
8. EXPANSION JOINT MATERIAL SHALL BE CELLU-CUSHION EXP 200, AS MANUFACTURED BY SEALED AIR CORPORATION, OR APPROVED EQUAL.

**COMBINATION CONCRETE CURB AND GUTTER**  
**VILLAGE STANDARD TY. B-9.12**

(LT. STA. 10+79.74 TO LT. STA. 32+41.67)  
(RT. STA. 10+79.74 TO RT. STA. 32+41.67)

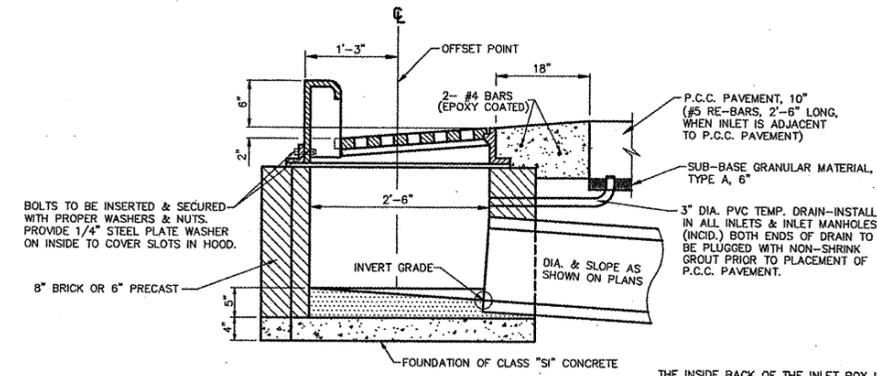
**COMBINATION CONCRETE CURB AND GUTTER**  
**VILLAGE STANDARD TY. B-9.12**

(LT. STA. 32+41.67 TO LT. STA. 34+04.00)



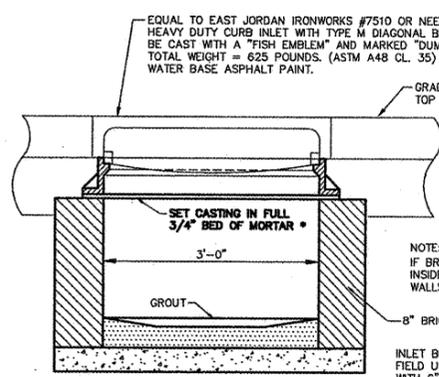
**INLET CASTING BOXOUT FOR G-1 INLETS**

**NOTES:**  
THE COST OF THE INLET CASTING BOXOUT, INCLUDING COMBINATION CONCRETE CURB & APRON, EXPANSION JOINTS, AND NO. 4 RE-BARS SHALL BE INCLUDED IN THE UNIT PRICE PER EACH FOR INLETS, TYPE G-1; INLETS, TYPE G-2; INLETS, DOUBLE G-1; OR INLETS, DOUBLE G-1, SPECIAL.  
INLET BOXES SHALL BE PLACED ACCURATELY. THE INSIDE BACK OF THE INLET BOX IS TO BE IN LINE WITH THE BACK OF THE PROPOSED CURB & GUTTER. INLET BOXES THAT ARE MORE THAN 3" OUT OF ALIGNMENT, SHALL BE REMOVED AND RESET AT THE CONTRACTORS EXPENSE.  
EXPANSION JOINT MATERIAL SHALL BE CELLU-CUSHION EXP 200, AS MANUFACTURED BY THE SEALED AIR CORPORATION, OR APPROVED EQUAL.



**SECTION A-A**

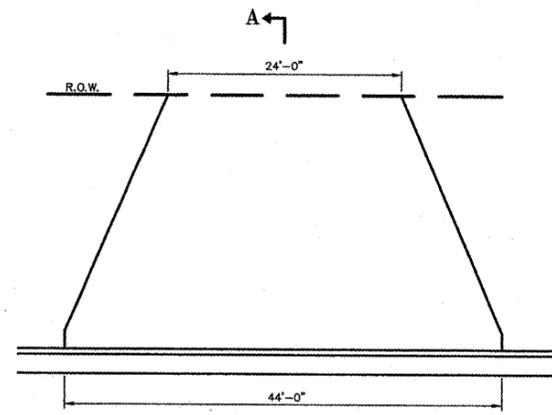
THE INSIDE BACK OF THE INLET BOX IS TO LINE UP WITH THE BACK OF THE PROPOSED CURB AND GUTTER. INLET BOXES THAT ARE MORE THAN 3" OUT OF ALIGNMENT SHALL BE REMOVED AND RESET AT THE CONTRACTOR'S EXPENSE.



**SECTION B-B**

**G-1 INLET DETAILS**

**NOTE:**  
IF BRICK CONSTRUCTION, THE INSIDE AND OUTSIDE OF INLET WALLS SHALL BE MORTAR COATED.  
INLET BOXES MAY BE EITHER CONSTRUCTED IN THE FIELD USING 8" BRICK WALLS OR PRECAST BOXES WITH 6" WALLS. IN EITHER CASE, THE INLET BOXES SHALL BE CONSTRUCTED SO THE MAXIMUM ADJUSTMENT HEIGHT BETWEEN THE TOP OF THE MASONRY AND THE BOTTOM OF THE INLET CASTING SHALL NOT EXCEED 3". PRECAST ADJUSTING RINGS SET IN ROPE MASTIC ARE ALLOWED.



**SECTION A-A**

**FIELD ENTRANCE DETAIL**

DETAIL SHEET W. COURTLAND ST. IMPROVEMENTS		AUSTIN ENGINEERING CO., INC. CIVIL ENGINEERS	
FOR: VILLAGE OF MORTON		PROJ. NO. 184-001143	
DATE: 3/21/08	SCALE: N.T.S.	REVISIONS:	SHEET NO. 22 of 39